

IDOT PROJECT LABOR AGREEMENT DETERMINATION

To: Ann L. Schneider, Secretary

From: Omer Osman, Director

Date: May 8, 2013

Re: FAI Rte. 270 (I-270), Contract Number 76F97, Madison County

{June 14, 2013 Letting}

In accordance with the Project Labor Agreement Act (30 ILCS 571), it is recommended that a project labor agreement (PLA) be utilized for the above-captioned Project. This recommendation is based on the considerations indicated below.

- 1) The Project is being awarded and administered by IDOT (i.e., not by another governmental agency).
- 2) The Project is being constructed using state or local funds only (i.e., no federal funds).
- 3) The overall size, scope, sequencing, logistics or other aspects of the Project make it particularly challenging to manage, and use of a PLA is expected to help assure that the construction work is performed properly and efficiently under the circumstances.
- 4) The duration of construction activity on the Project is expected to exceed one construction season (i.e., 110 or more working days), or the nature of the Project results in a heightened need for labor force continuity and stability over a substantial period of time.
- 5) There is a firm construction completion date established for the Project thereby increasing the adverse consequences of any work stoppage or other labor disruption.
- 6) The time required to complete the Project is expected to extend beyond the expiration date of one or more existing collective bargaining agreements covering trades likely to be involved in the Project, thereby increasing the likelihood of work stoppage(s) or other labor disruption(s) during construction of the Project.
- 7) In the absence of a PLA, there is an increased likelihood of jurisdictional disputes among unions or of conflict between unionized and non-unionized workers on the Project that could have a potentially material adverse effect on the time, cost, or quality of work performed on the Project.

- 8) This project presents specific safety concerns to the traveling public and a PLA will ensure labor force continuity and stability, decreasing the length of the safety concern
- 9) Use of a PLA is expected to result in improved access to skilled labor, improved efficiency, or improved safety performance on the Project
- 10) Use of a PLA on the Project is not expected to have a material adverse effect on the competitive bidding process
- 11) Use of a PLA on the Project is not expected to have a material adverse effect on the ability of the Department to achieve other Departmental goals (e.g., utilization of disadvantaged businesses, utilization of Illinois domiciled businesses, development of competitive vendor alternatives over time, etc.)
- 12) There are other material considerations favoring or disfavoring use of a PLA on this Project as follows:

Based upon the identified considerations, we recommend that you approve use of a PLA on this Project. Upon your approval, the Department shall undertake to negotiate in good faith a PLA with the relevant labor organization(s), and shall include in all necessary bid specifications and other documents information regarding the actual or form of PLA that is to binding upon all contractors and their employees

Agreed: Asman 6/03/13
{Division Chief} (Date)

Agreed: [Signature] 5/20/13
{Bureau of Design & Environment} (Date)

Agreed: Jeffrey Z. Kerin 5/13/13
{Regional Engineer} (Date)

Approved: Ann L. Schneider 6/4/13
Ann L. Schneider, Secretary (Date)
Sarah Hermann, Ex Assistant

FHWA concurrence in the PLA for the above mentioned contract

Gregory Nadeau, Deputy Administrator of FHWA
See Approval Dated 5/09/13

ATTACHMENT A:

JUSTIFICATION FOR USE OF PROJECT LABOR AGREEMENT ON FAI RTE. 270 (I-270), CONTRACT NUMBER 76F97, MADISON COUNTY WHICH INVOLVES THE REPLACEMENT OF STEEL PLATE BEAM GUARDRAIL AND IMPACT ATTENUATORS ON I-270 FROM IL-3 TO I-55. THE LENGTH OF THIS PROJECT IS APPROXIMATELY 12.41 MILES.

ITEM 2: This project is federally funded.

ITEM 3: Estimated project cost is \$2,200,000. The project length is approximately 12.41 miles

The overall project scope consists of.

- The removal of existing steel plate beam guardrail, and the installation of proposed steel plate beam guardrail and impact attenuators on approximately 12.4 miles of I-270 in Madison County.
- The work will be constructed utilizing highway standards for single and double lane closures utilizing changeable message signs. This work must also be done outside of peak hours, which may entail having workers present during nighttime hours.

Although this project may not be complicated with regard to the scope of work, any disruption of this project due to labor issues could result in either:

1. Maintaining traffic indefinitely in areas in which existing steel plate beam guardrail has been removed, thus exposing the traveling public to unprotected hazards.
2. Creating scheduling problems with several other projects on I-270 within these limits. i.e. micro-surfacing, milling and resurfacing, and ITS.

In order to avoid either 1 or 2 above, maintaining a steady workforce is necessary.

It is anticipated that lapsing trade agreements on this project would be addressed through the use of a Project Labor Agreement. It is the Illinois Department of Transportation's finding that the workforce needed for this project can be provided by the union trades involved.

ITEM 4: This project has 175 working days, which exceeds one construction season

ITEM 6: The following collective bargaining agreements will expire during the construction of this project

<u>Local</u>	<u>Current Contract Expiration</u>
Carpenters D C.	04/30/2013
IBEW 309 (Lineman)	12/01/2013
Iron Workers 392	07/31/2013
Laborers D C.	07/31/2013
Teamsters 525	07/31/2014

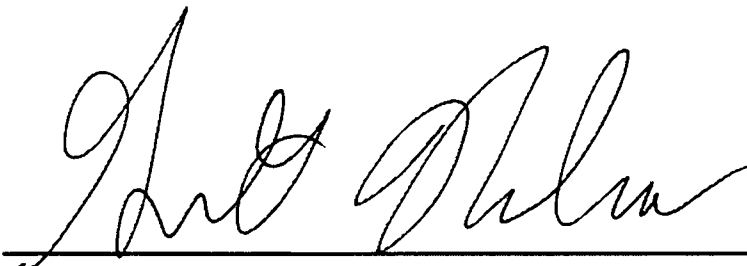
ITEM 8: Part of the scope of work for this project is to remove existing steel plate beam guardrail within a 12.41 mile stretch of urbanized interstate area with ADTs varying from 28,800 to 55,200. If there should be a work stoppage due to lapsing trade agreements prior to the installation of the proposed steel plate beam guardrail or impact attenuators, the traveling public may be at an increased risk for injury due to prolonged exposure to unprotected safety hazards.

PLA Request


Approval of Project Labor Agreement

Disapproval of Project Labor Agreement

Reason for disapproval:



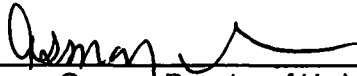
Signature



Date

Execution Page

Illinois Department of Transportation




Omer Osman, Director of Highways



Matthew R. Hughes, Director Finance & Administration



Michael A. Forti, Chief Counsel

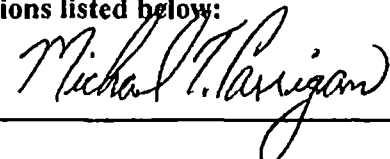


Ann L. Schneider, Secretary
SARAH KUWANO
Ex. Assistant

6/4/13

(Date)

Illinois AFL-CIO Statewide Project Labor Agreement Committee, representing the local unions listed below:



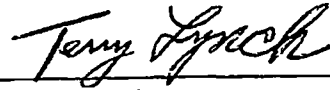
May 20, 2013

(Date)

List Union Locals:



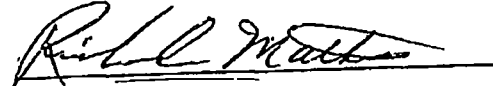
Jim Allen
Bricklayers



Terry Lynch
Heat & Frost Insulators & Allied
Workers



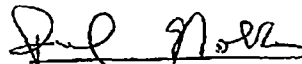
Curtis Cade
United Association



Richard Mathis
Roofers

*

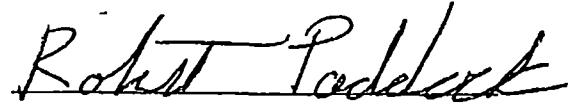
Ed Christensen, Elevator
Constructors



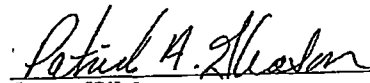
Paul Noble
IBEW



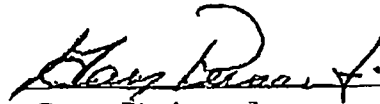
Terry Fitzmaurice
Painters



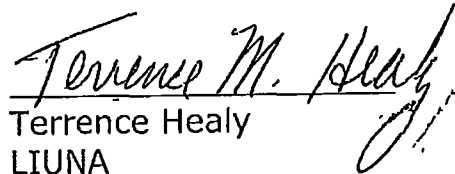
Robert Paddock
IUOE



Pat Gleason
Teamsters



Gary Perinar Jr.
Carpenters



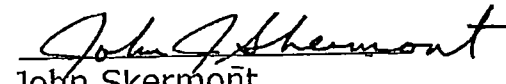
Terrence Healy
LIUNA



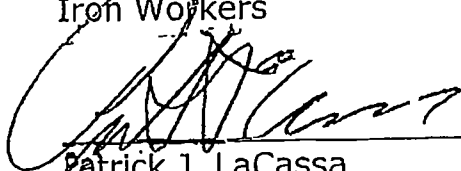
Brian Mulheran
Sheet Metal Workers



Tadas Kicielinski
Iron Workers



John Skermont
Boilermakers



Patrick J. LaCassa
OPCMIA

*only if Elevator Constructors master agreement
language is attached to PLA