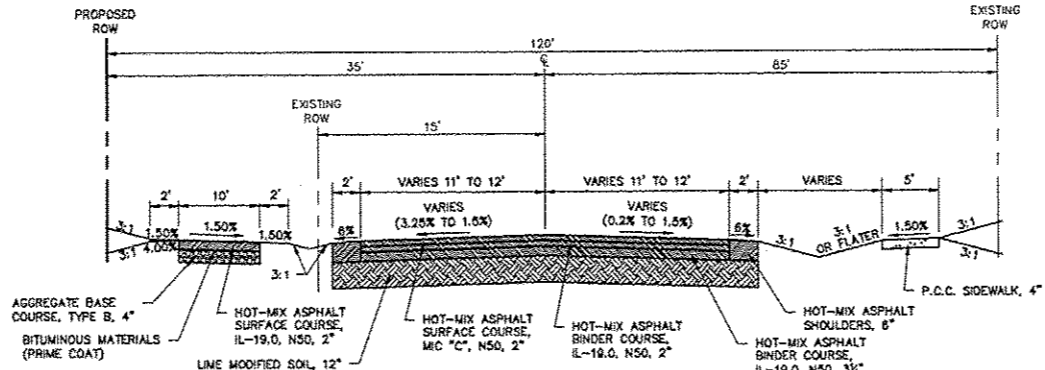
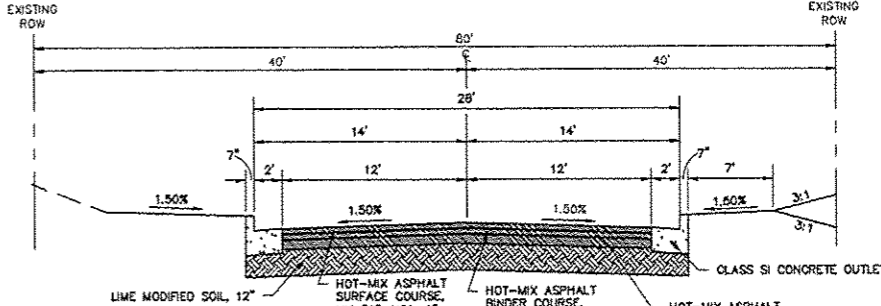


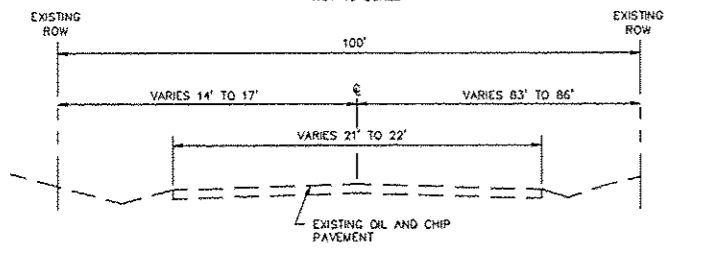
EXISTING TYPICAL SECTION
STA. 16+91.62 TO STA. 27+43
NOT TO SCALE



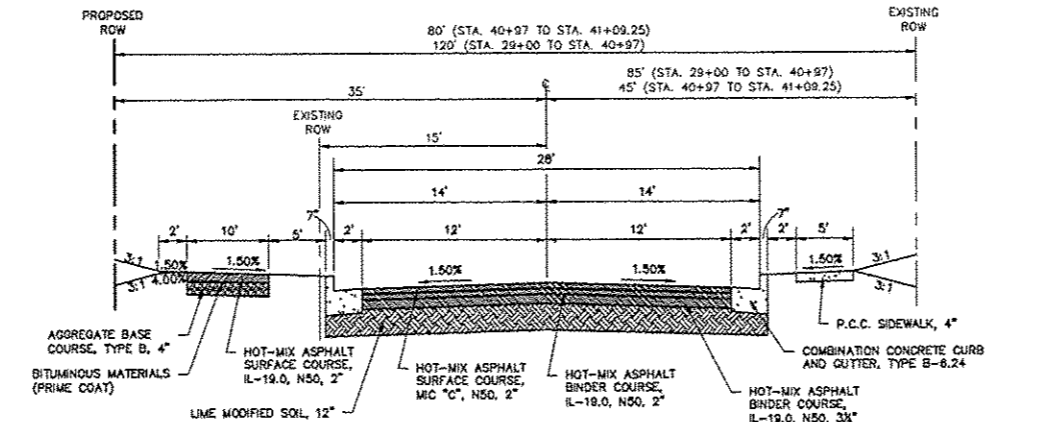
PROPOSED TYPICAL SECTION
STA. 28+13.53 TO STA. 29+00
NOT TO SCALE



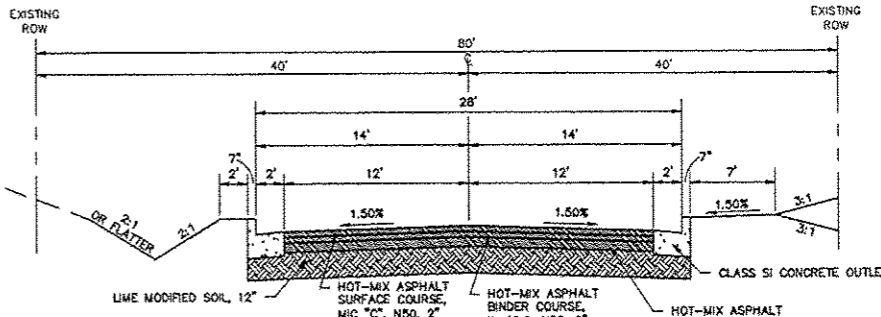
PROPOSED TYPICAL SECTION
STA. 47+24.63 TO STA. 47+37
NOT TO SCALE



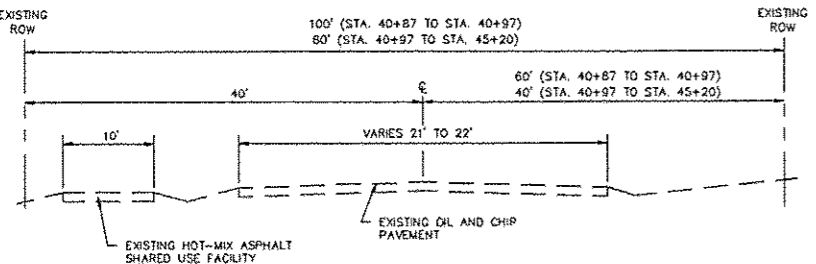
EXISTING TYPICAL SECTION
STA. 27+43 TO STA. 40+87
NOT TO SCALE



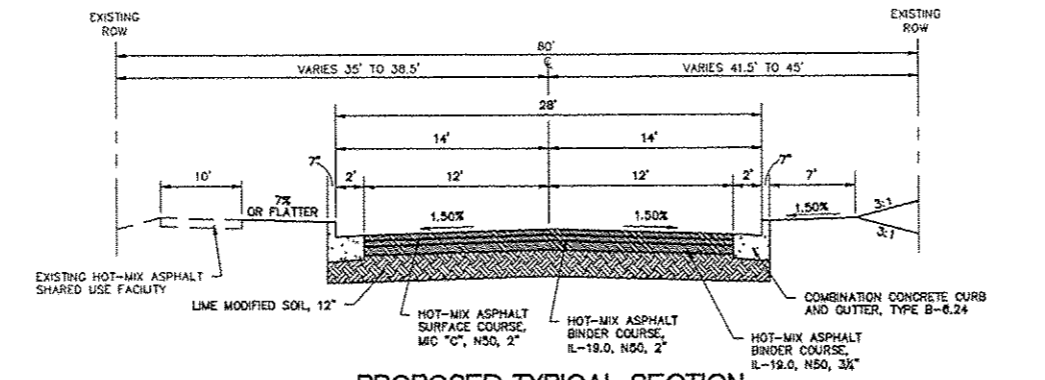
PROPOSED TYPICAL SECTION
STA. 29+00 TO STA. 41+09.25
NOT TO SCALE



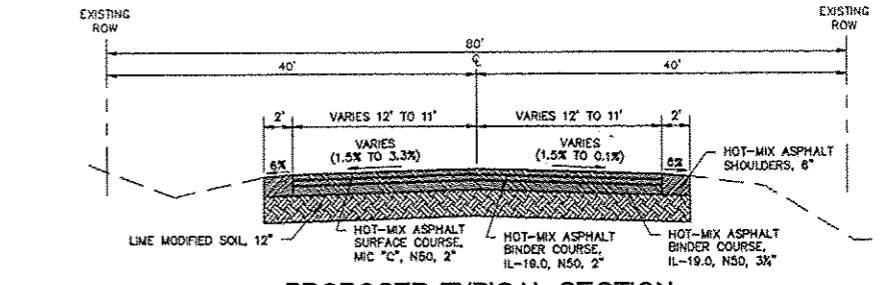
PROPOSED TYPICAL SECTION
STA. 47+37 TO STA. 47+50
NOT TO SCALE



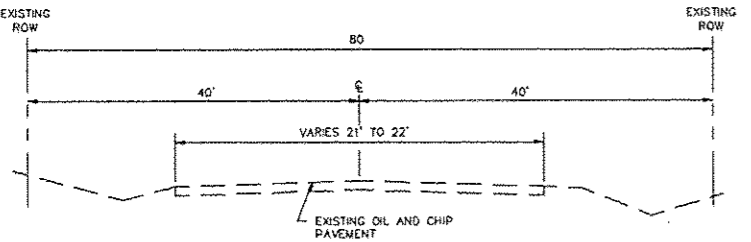
EXISTING TYPICAL SECTION
STA. 40+87 TO STA. 45+20
NOT TO SCALE



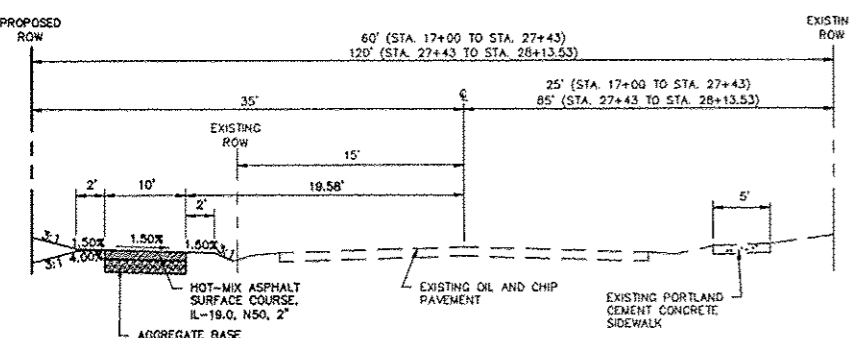
PROPOSED TYPICAL SECTION
STA. 41+09.25 TO STA. 45+20
NOT TO SCALE



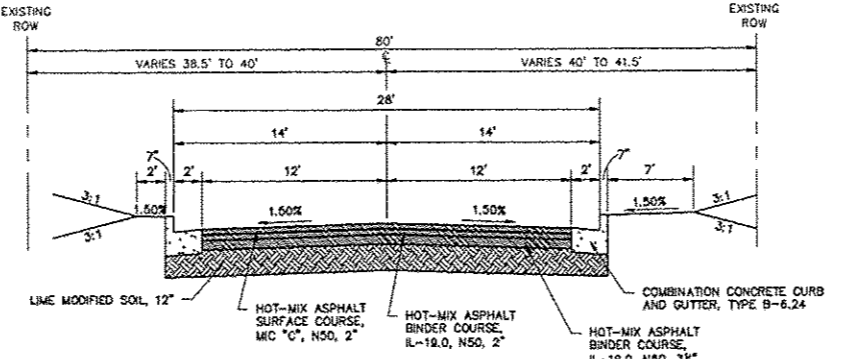
PROPOSED TYPICAL SECTION
STA. 47+50 TO STA. 48+08.59
NOT TO SCALE



EXISTING TYPICAL SECTION
STA. 45+20 TO STA. 48+08.59
NOT TO SCALE



PROPOSED TYPICAL SECTION
STA. 16+91.62 TO STA. 28+13.53
NOT TO SCALE



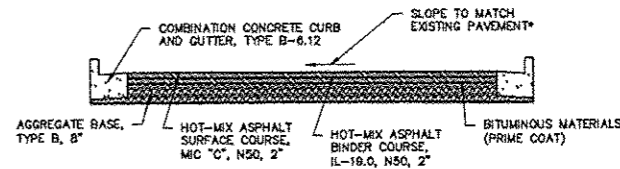
PROPOSED TYPICAL SECTION
STA. 45+20 TO STA. 47+24.63
NOT TO SCALE

PAVEMENT DESIGN DATA

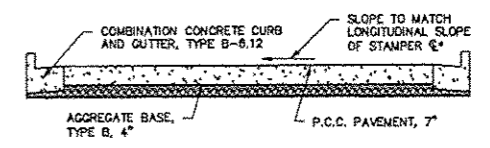
DESIGN SPEED = 30 M.P.H.
CLASS II STREET
2013 AVERAGE DAILY TRAFFIC (A.D.T.) = 4,075
2033 AVERAGE DAILY TRAFFIC (A.D.T.) = 4,500
2023 STRUCTURAL DESIGN TRAFFIC (S.D.T.) = 4,290
PASSENGER VEHICLES (P.V.) = 4,078 (95%)
SINGLE UNITS (S.U.) = 172 (4%)
MULTIPLE UNITS (M.U.) = 42 (1%)

HMA PAVEMENT DESIGN

SSR: POOR
FLEXIBLE TRAFFIC FACTOR: 0.36
SELECTED DESIGN PG BINDER: PG 70-22
DESIGN PAVEMENT HMA TEMP: 82F
DESIGN HMA MODULUS (E_{AC}): 500 ksi
DESIGN HMA MICROSTRAIN: 190
PAVEMENT THICKNESS: 7 1/2"
SUBGRADE: LIME MODIFIED



PROPOSED TYPICAL SECTION
SIDE STREETS
NOT TO SCALE



PROPOSED TYPICAL SECTION
SIDE STREETS
NOT TO SCALE

REVISIONS

SMS ENGINEERS
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CONSULTING ENGINEERS AND LAND SURVEYORS
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DESIGN FIRM # 184-000892

STAMPER LANE - GODFREY, ILLINOIS
FAU ROUTE 8950
10-00022-01-PV
10-00024-00-SW
EXISTING AND PROPOSED TYPICAL SECTIONS

DWG. NO.	10-00022-01-PV
DATE	MARCH 2013
SCALE	NOT TO SCALE
SHEET	3 OF 50

*SEE SHEETS 16 FOR DETAILS