

# KANKAKEE VALLEY AIRPORT AUTHORITY KANKAKEE, ILLINOIS

## CONSTRUCTION PLANS FOR GREATER KANKAKEE AIRPORT

### REHABILITATE TERMINAL APRON PHASE 3

ILLINOIS PROJECT: IKK-4246  
S.B.G. PROJECT: 3-17-0057-B21



*D. Kyle Peabody*  
3-13-13  
EXP. 11-30-13

12075-03

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SUBMITTED BY *D. Kyle Peabody*  
D. KYLE PEABODY, P.E.

DATE March 13 2013

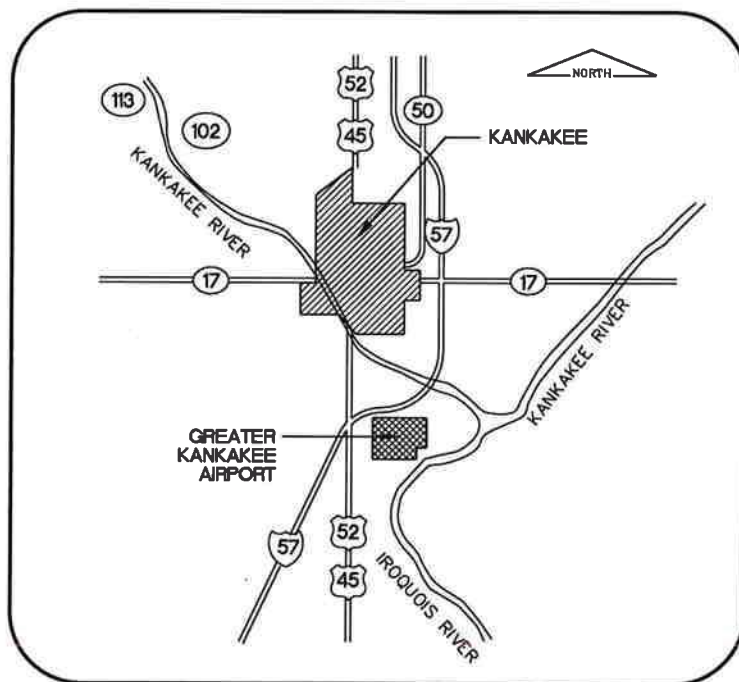
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KANKAKEE VALLEY AIRPORT AUTHORITY  
GREATER KANKAKEE AIRPORT

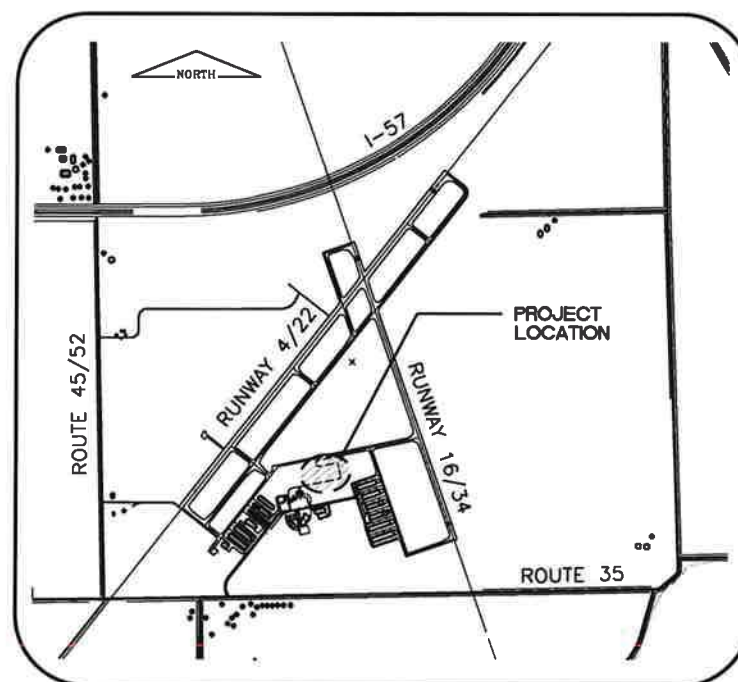
SECTION: 21 COUNTY: KANKAKEE  
RANGE: R 12 E TOWNSHIP: OTTO

UNICOM RADIO FREQUENCY - 123.0

MARCH 13, 2013



SITE PLAN



LOCATION MAP

DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY D  
DESIGN AIRCRAFT GROUP II

KANKAKEE VALLEY  
AIRPORT AUTHORITY

APPROVED *David J. ...*  
AIRPORT MANAGER

DATE 3/15/13 2013

**SUMMARY OF QUANTITIES**

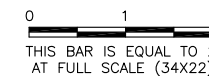
<b>BASE BID - Rehabilitate Terminal Apron (Phase 3)</b>				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	35	
AR152540	SOIL STABILIZATION FABRIC	SY	50	
AR156531	EROSION CONTROL BLANKET	SY	570	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	2,000	
AR208515	POROUS GRANULAR EMBANKMENT	CY	17	
AR209612	CRUSHED AGG. BASE COURSE - 12"	SY	50	
AR401610	BITUMINOUS SURFACE COURSE	TON	775	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	6,630	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SY	300	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SY	100	
AR403610	BITUMINOUS BASE COURSE	TON	1,100	
AR510510	TIE DOWN	EACH	21	
AR510900	REMOVE TIE DOWN	EACH	37	
AR602510	BITUMINOUS PRIME COAT	GAL	15	
AR603510	BITUMINOUS TACK COAT	GAL	2,050	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	770	
AR901510	SEEDNG	ACRE	0.12	
AR905530	TOPSOILING	SY	570	
<b>ADDITIVE ALTERNATE #1 - SW Hangar 7 Pvmt.</b>				
AS152410	UNCLASSIFIED EXCAVATION	CY	235	
AS152540	SOIL STABILIZATION FABRIC	SY	466	
AS208515	POROUS GRANULAR EMBANKMENT	CY	156	
AS209606	CRUSHED AGG. BASE COURSE - 6"	SY	466	
AS401610	BITUMINOUS SURFACE COURSE	TON	42	
AS401900	REMOVE BITUMINOUS PAVEMENT	SY	466	
AS403610	BITUMINOUS BASE COURSE	TON	70	
AS501606	6" PCC SIDEWALK	SF	65	
AS501690	PCC SIDEWALK REMOVAL	SF	65	
AS602510	BITUMINOUS PRIME COAT	GAL	140	
AS603510	BITUMINOUS TACK COAT	GAL	70	

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- 6 TYPICAL SECTIONS
- 7 EXISTING CONDITIONS/PROPOSED REMOVALS
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**REVISIONS**


NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON  
 PHASE 3**

**INDEX TO SHEETS / SUMMARY OF QUANTITIES**

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DATE:	03/13/13
JOB No:	12075-03-00
IL. PROJECT: IKK-4246 S.B.G. PROJECT: 3-17-0057-B21 <b>FINAL</b>	
SHEET	2 OF 11 SHEETS

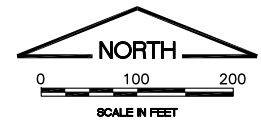
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HORIZONTAL LAYOUT			
POINT	DESCRIPTION	NORTHING	EASTING
1	BEGIN PROPOSED IMPROVEMENTS - TERMINAL APRON - STA. 14+50.00 @ TAXIWAY H	1603146.54	1118088.36
2	BEGIN PROPOSED IMPROVEMENTS - TERMINAL APRON - STA. 14+50.00 @ TAXIWAY H	1602926.45	1118134.72
3	END PROPOSED IMPROVEMENTS - TERMINAL APRON - STA. 11+82.78 @ TAXIWAY H	1603194.79	1118351.51
4	END PROPOSED IMPROVEMENTS - TERMINAL APRON - STA. 11+82.78 @ TAXIWAY H	1603198.72	1118350.68
5	BEGIN PROPOSED IMPROVEMENTS - SW HANGAR 7 - STA. 1+47.05 @ T-HANGAR TAXIWAY	1602054.47	1116863.46
6	BEGIN PROPOSED IMPROVEMENTS - SW HANGAR 7 - STA. 1+95.49 @ T-HANGAR TAXIWAY	1602024.94	1116901.86
7	END PROPOSED IMPROVEMENTS - SW HANGAR 7	1601956.72	1116848.91
8	END PROPOSED IMPROVEMENTS - SW HANGAR 7	1601986.37	1116810.95

**LEGEND**

-  BASE BID:  
 NEW 2" BITUMINOUS SURFACE COURSE (401)  
 NEW 2"-4" BITUMINOUS BASE COURSE (403)  
 NEW 2" (NOMINAL) BITUMINOUS MILLING (401)
-  ADDITIVE ALTERNATE #1:  
 NEW 1.5" BITUMINOUS SURFACE COURSE (401)  
 NEW 2.5" BITUMINOUS BASE COURSE (403)  
 NEW 6" CRUSHED AGGREGATE BASE COURSE (209)  
 EXISTING BITUMINOUS PAVEMENT TO BE REMOVED (401)
-  BM#1  
 EXISTING BENCH MARK

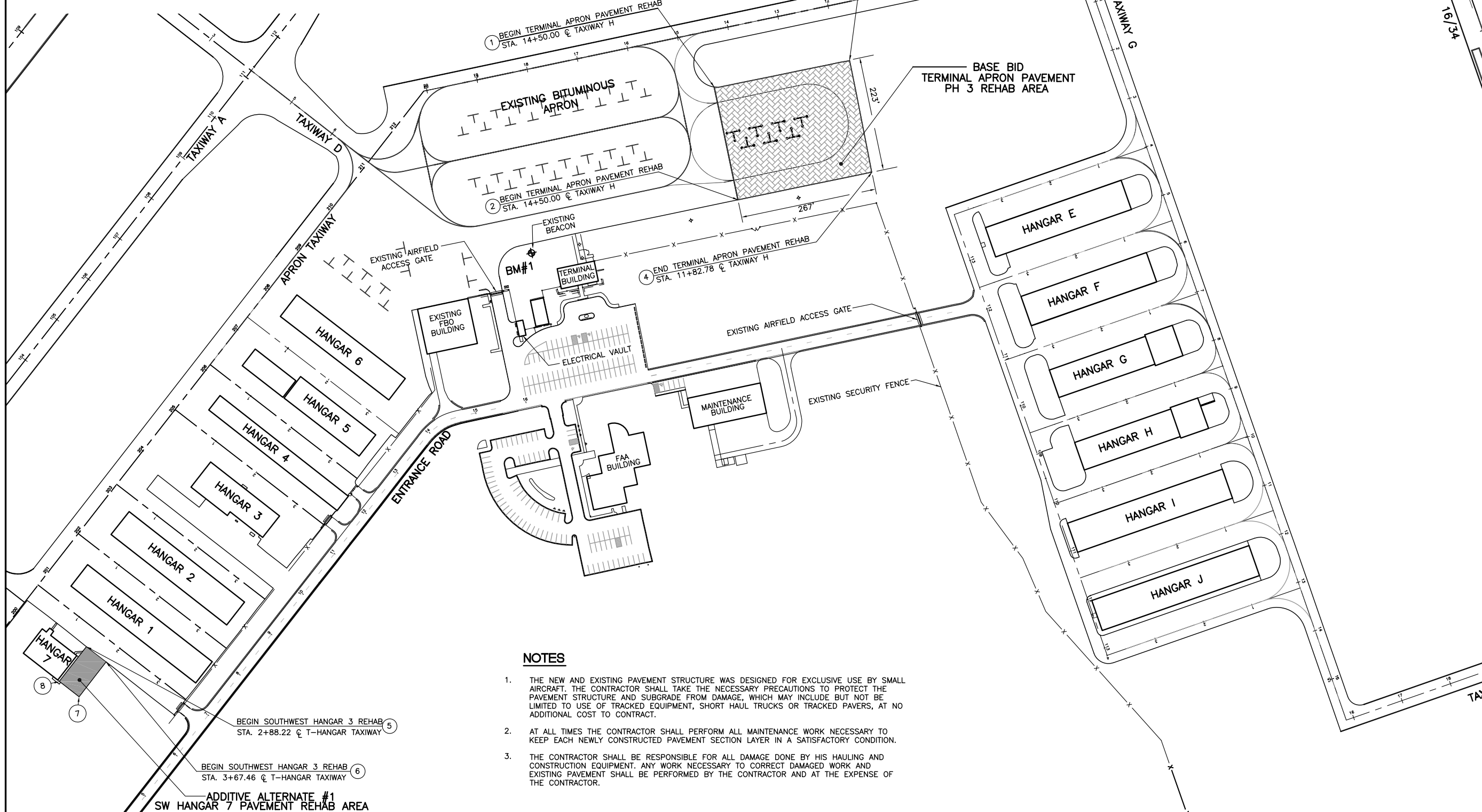


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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

CONTRACTOR SHALL VERIFY ALL HORIZONTAL CONTROL POINTS BEFORE BEGINNING CONSTRUCTION

VERTICAL CONTROL		
BENCHMARK	DESCRIPTION	ELEVATION
BM #1	N.W. CORNER OF BEACON FOUNDATION	622.41



**GREATER KANKAKEE AIRPORT  
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 REHABILITATE TERMINAL APRON  
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**SITE PLAN / PROJECT CONTROL PLAN**

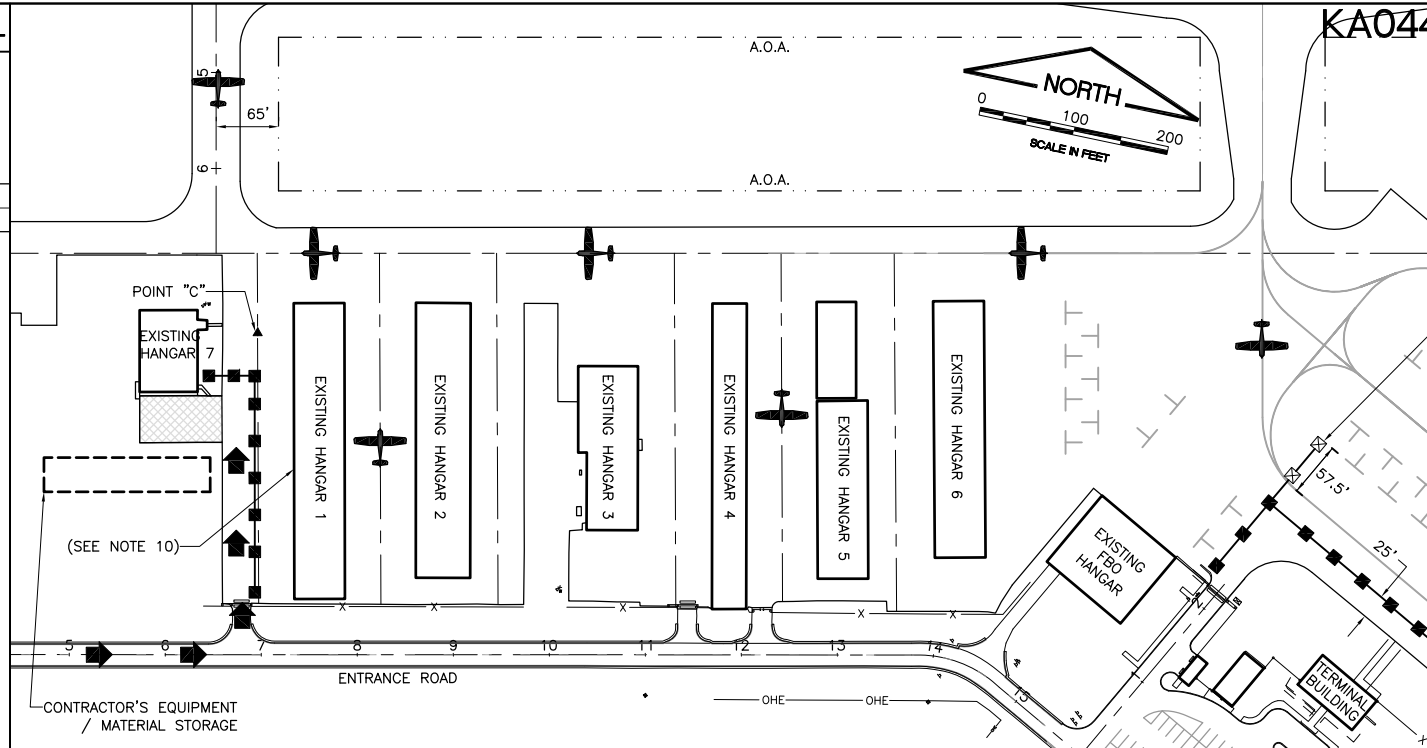
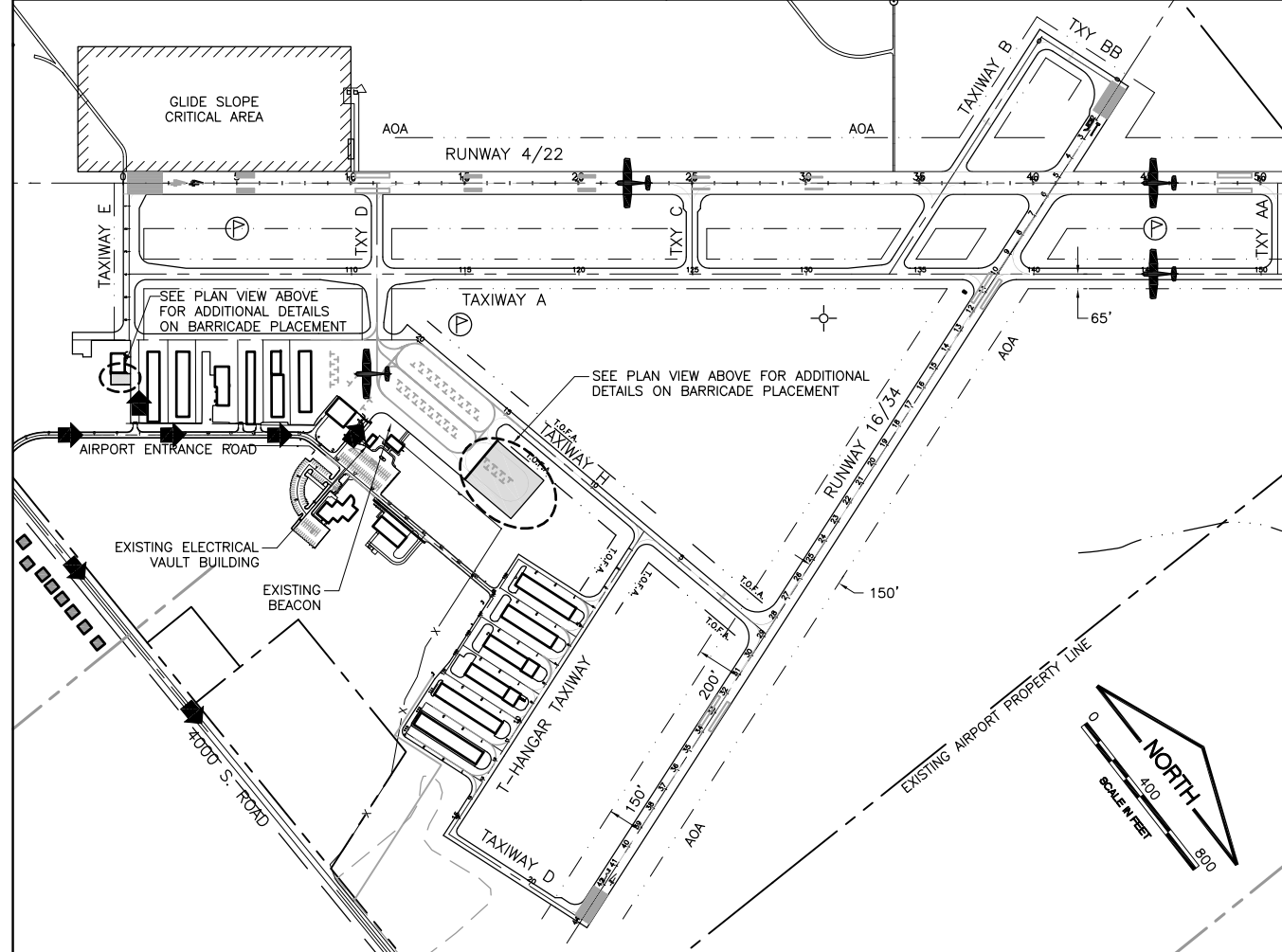
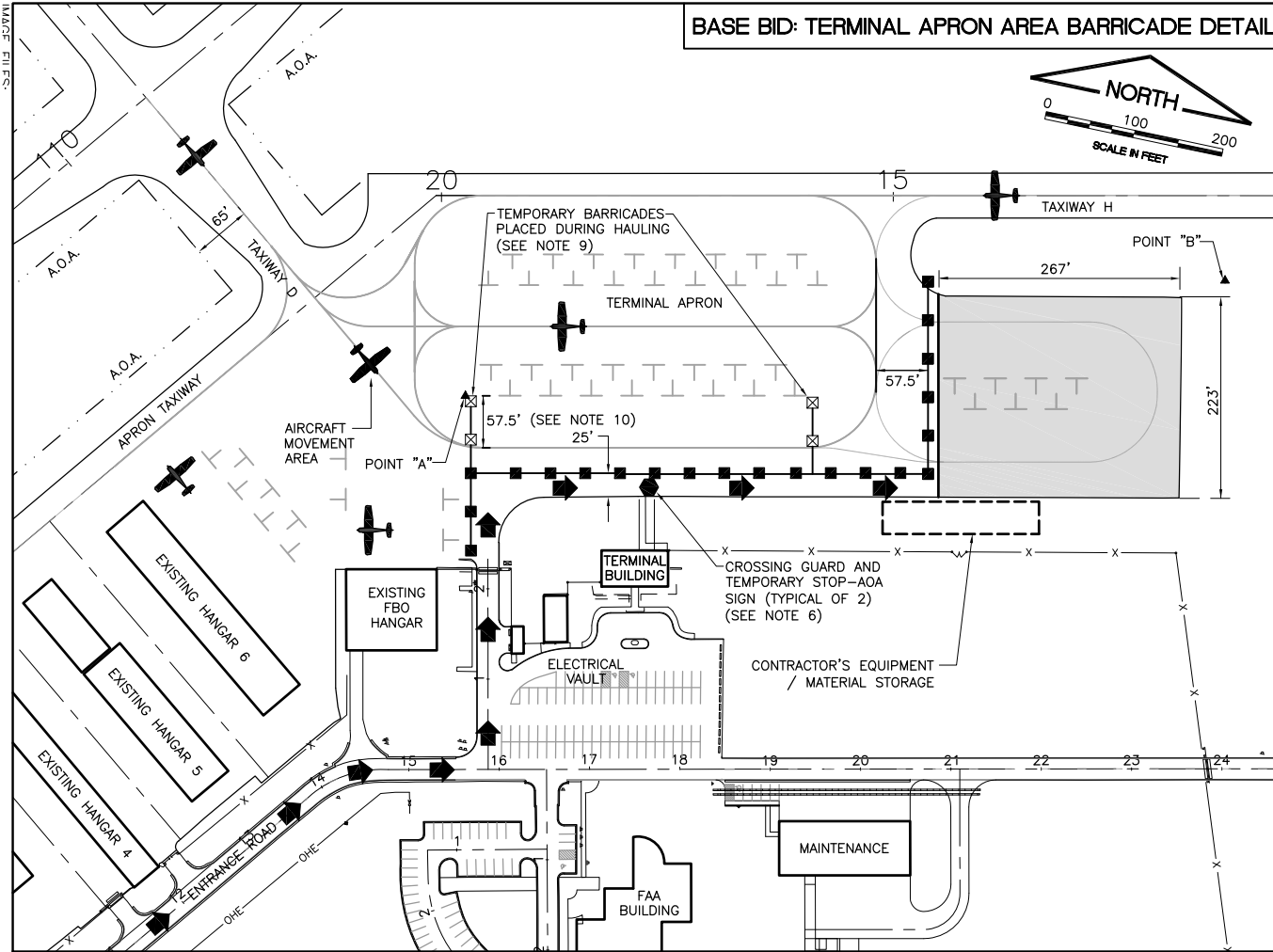
**NOTES**

- THE NEW AND EXISTING PAVEMENT STRUCTURE WAS DESIGNED FOR EXCLUSIVE USE BY SMALL AIRCRAFT. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO CONTRACT.
- AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.

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SHEET	3 OF 11 SHEETS



ADDITIVE ALTERNATE #1: HANGAR 7 PVT AREA BARRICADE DETAIL (SEE NOTE 3)

**SUGGESTED SEQUENCE OF CONSTRUCTION**

- LEGEND**
- WORK LIMITS (BASE BID)
  - WORK LIMITS (ADDITIVE ALTERNATE #1)
  - EXISTING HANGAR/BUILDING
  - EXISTING AIRPORT PROPERTY LINE
  - LOW PROFILE BARRICADES
  - TEMPORARY LOW PROFILE BARRICADES (SEE NOTE 9)
  - AIRCRAFT MOVEMENT AREA
  - CONTRACTOR'S ACCESS/HAUL ROUTE
- COORDINATE RELOCATION OF ANY AIRCRAFT IN WORK AREA (BY OTHERS) WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
  - PLACE BARRICADES AS SHOWN.
  - REMOVE EXISTING AIRCRAFT TIEDOWNS PER PLAN. (BASE BID ONLY)
  - REMOVE EXISTING PAVEMENT/MILLING PER PLAN.
  - PERFORM CRACK SEALING AND ANY NECESSARY PAVEMENT REPAIRS.
  - CONSTRUCT GRADE PER PLAN IN FULL-DEPTH PAVEMENT AREAS.
  - CONSTRUCT BITUMINOUS PAVEMENTS.
  - INSTALL AIRCRAFT TIEDOWNS. (BASE BID ONLY)
  - INSTALL PAVEMENT MARKINGS PER PLAN. (BASE BID ONLY)
  - REMOVE MISCELLANEOUS DEBRIS FROM CONSTRUCTION AREA AND CLEAN PAVEMENTS.
  - REMOVE BARRICADES.
  - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER SO THAT DISPLACED AIRCRAFT CAN BE RELOCATED BACK TO THEIR ORIGINAL STORAGE AREAS (BY OTHERS).

POINT "A" CLOSEST POSSIBLE POINT TO RUNWAY 4/22  
 EXISTING GROUND ELEV: 621.6  
 LATITUDE: 41°04'00.51"  
 LONGITUDE: 87°50'59.07"

POINT "B" CLOSEST CONSTRUCTION POINT TO RUNWAY 16/34  
 EXISTING GROUND ELEV: 617.8  
 LATITUDE: 41°04'03.42"  
 LONGITUDE: 87°50'48.65"

POINT "C" CLOSEST CONSTRUCTION POINT TO RUNWAY 4/22  
 EXISTING GROUND ELEV: 620.7  
 LATITUDE: 41°03'52.61"  
 LONGITUDE: 87°51'09.12"

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25 FEET

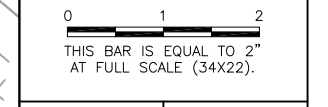
DESIGN AIRCRAFT APPROACH CATEGORY: D  
 DESIGN AIRPORT GROUP: II  
 TAXILANE CENTERLINE TO OBJECT SEPARATION (TOFA) = 57.5'

**NOTES**

1. THE CONTRACTOR SHALL, IF REQUIRED BY THE CITY, COMPLETE PERMIT APPLICATION AND OBTAIN A CITY OF KANKAKEE BUILDING PERMIT FOR THIS PROJECT BEFORE THE NOTICE TO PROCEED IS ISSUED.
2. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK OR AS DIRECTED BY THE RESIDENT ENGINEER.
3. IF ADDITIVE ALTERNATE #1 IS AWARDED, THE CONTRACTOR SHALL COMPLETE THE REQUIRED WORK IN THAT AREA WITHIN 10 CALENDAR DAYS.
4. THE AIRPORT WILL REQUIRE TWO (2) WEEKS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY WORK TO COORDINATE WITH THE TENANTS.
5. AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL RETURN THE EQUIPMENT TO THE STAGING AND STORAGE AREA SHOWN.
6. WHEN HAUL ROUTE IS IN USE, THE CONTRACTOR WILL BE REQUIRED TO HAVE A CROSSING GUARD FOR AIRPORT PERSONNEL, AIRPORT TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM AIRPORT TERMINAL BUILDING. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
7. CONTRACTOR SHALL HAVE A DEDICATED FLAGGER ONSITE WHEN WORKING ADJACENT TO ACTIVE TAXIWAYS AND YIELD TO ALL ONCOMING AIRCRAFT. COST SHALL BE INCIDENTAL TO THE CONTRACT.
8. CONTRACTOR SHALL MAINTAIN ALL CONSTRUCTION EQUIPMENT AND VEHICLES WITHIN THE LIMITS OF IMPROVEMENTS. AREAS OUTSIDE LIMITS OF IMPROVEMENTS ARE AIRCRAFT MOVEMENT AREAS. NO VEHICLE IS ALLOWED PASSED THESE LIMITS.
9. DURING HAULING OPERATIONS, CONTRACTOR SHALL PLACE TEMPORARY BARRICADES AT THE LOCATION SHOWN. ONCE THE HAULING IS DONE EACH DAY, THE CONTRACTOR SHALL REMOVE BARRICADES TO ALLOW AIRPORT / AIRCRAFT USAGE OF TAXILANE.
10. CONTRACTOR TO MAINTAIN AIRCRAFT ACCESS AT ALL TIME TO PAVEMENTS / SECTION OUTSIDE THE CONSTRUCTION LIMITS. CONTRACTOR SHALL COORDINATE THIS WITH ENGINEER AND AIRPORT. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.

**KA044**  
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**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON  
 PHASE 3**

**SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS**

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SHEET 4 OF 11 SHEETS	

**GENERAL NOTES**

1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
2. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2F (LATEST EDITION) **OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION**.
3. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
4. THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
7. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
8. THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
9. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
11. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES WITH TWO ORANGE FLAGS (20" x 20") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.
12. THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER FOURTEEN (14) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
16. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
18. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
19. THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
21. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

22. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
23. COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
24. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
25. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
26. CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS / TAXILANES AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.
27. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
28. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
29. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
30. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

**CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)**

32. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
33. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
34. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

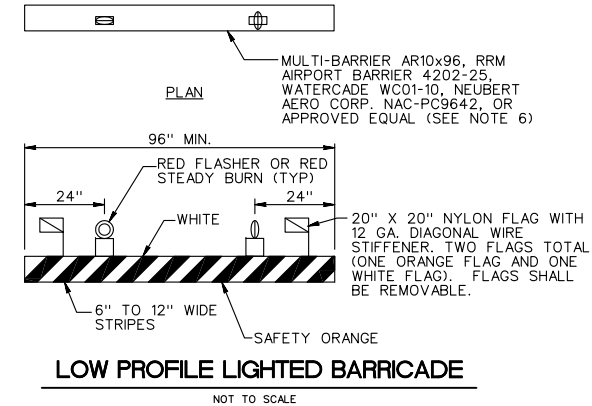
**LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY SAFETY AREA (RSA) AND TAXIWAY OBJECT FREE AREA (TOFA)**

**RUNWAYS:**

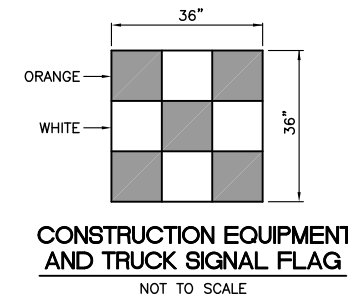
THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON PREVIOUS SHEET FOR SAFETY AREA WIDTHS.

**TAXIWAYS / TAXILANES:**

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON PREVIOUS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.



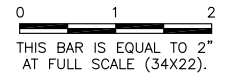
- BARRICADE NOTES:**
1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
  2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
  3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 10' SPACING CENTER TO CENTER BETWEEN RED LIGHTS ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
  4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
  5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
  6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN.



CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

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**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON  
 PHASE 3**

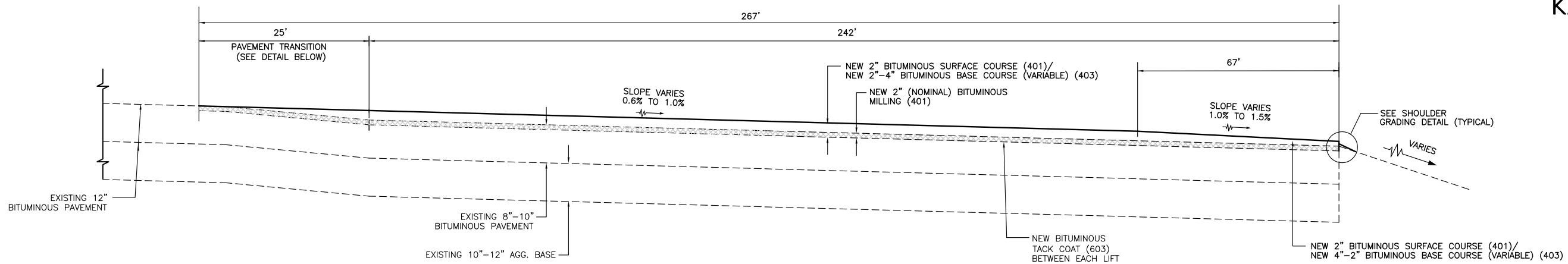
**SEQUENCE OF CONSTRUCTION  
 PER AC 150/5370-2F (LATEST EDITION)**

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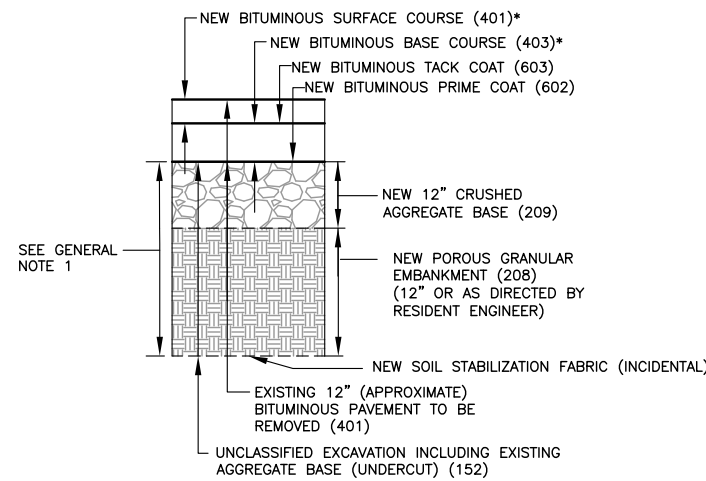
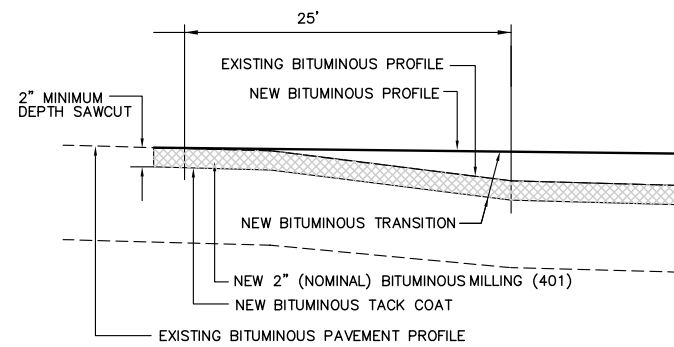
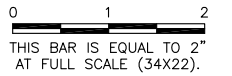


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CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/13/13
JOB No:	12075-03-00

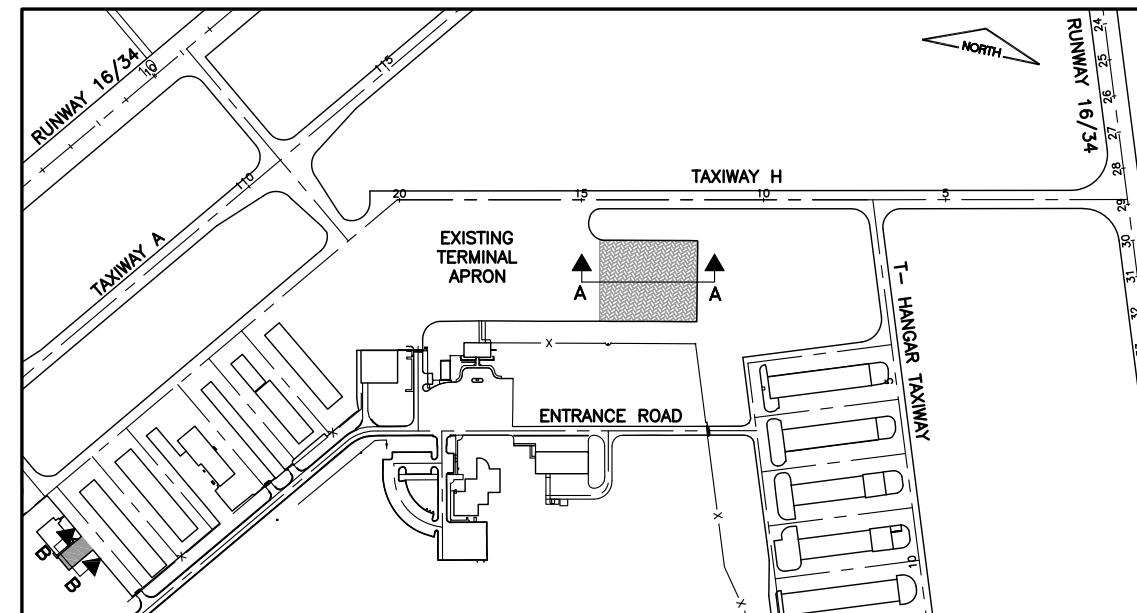
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 S.B.G. PROJECT: 3-17-0057-B21  
**FINAL**



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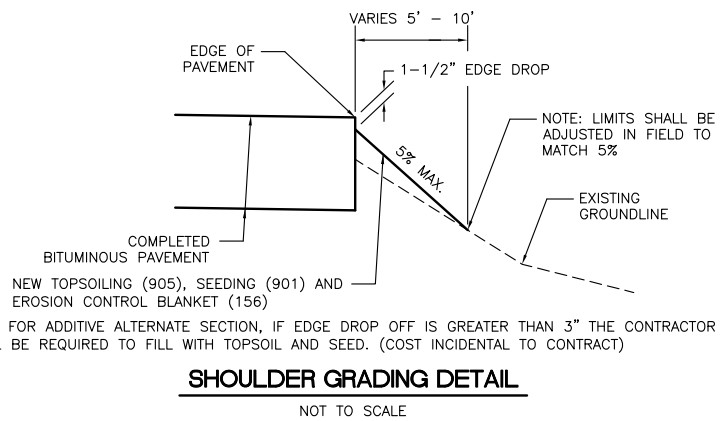
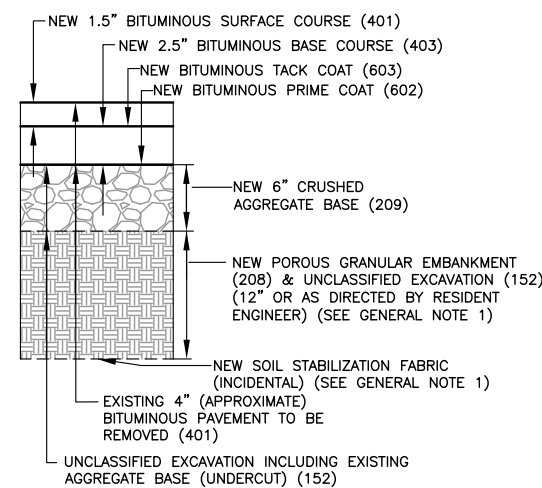
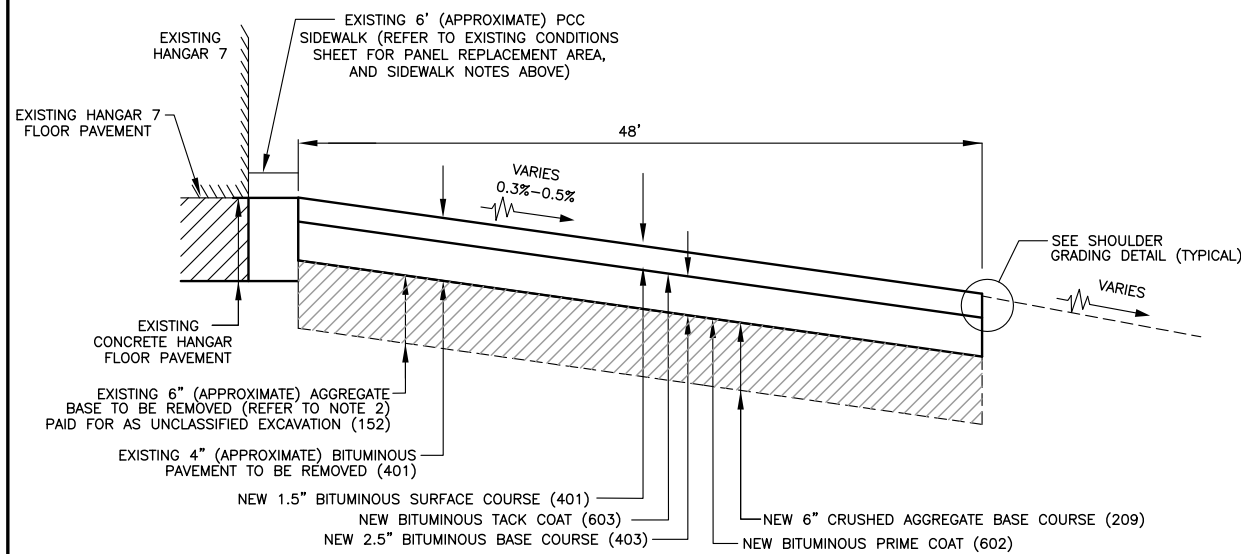


\* REFER TO REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B ON SHEET 10.



**SIDEWALK NOTES**

1. SIDEWALK JOINTS SHALL BE SAW CUT AS SHOWN ON EXISTING CONDITIONS SHEET AND AS LAID OUT BY ENGINEER IN FIELD. CONTRACTION JOINTS SHALL BE FILLED WITH GRAY NP1 OR EQUIVALENT. THE COST OF THE JOINTING AND SEALING SHALL BE CONSIDERED INCIDENTAL TO THE SIDEWALK.
2. CONTRACTOR SHALL INSTALL 6"x6" / W4 X W4 WIRE MESH IN CONFORMANCE TO ASTM A185 IN ALL SIDEWALK PAVEMENT. THE COST OF THE WIRE MESH SHALL BE INCIDENTAL TO THE SIDEWALK.
3. CONTRACTOR SHALL DOWEL INTO EXISTING SIDEWALK PAVEMENT AS DIRECTED BY ENGINEER. DOWEL BARS SHALL BE A MINIMUM OF 3/4" IN DIA, 18" IN LENGTH AND SPACED A 12". THE COST OF THE DOWEL BARS SHALL BE INCIDENTAL TO THE SIDEWALK.



**GENERAL NOTES:**

1. LOCATIONS FOR FURNISHING AND PLACING POROUS GRANULAR EMBANKMENT SHALL BE AS THE FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION. THIS MATERIAL IS INTENDED TO REPAIR SOFT SUBGRADE AS DIRECTED BY THE RESIDENT ENGINEER. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. EXCAVATION OF THE SOFT SUBGRADE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. (REFER TO STABILIZATION TYPICAL SECTION)

**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON  
 PHASE 3**

**TYPICAL SECTIONS**

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IL PROJECT:	IKK-4246
S.B.G. PROJECT:	3-17-0057-B21
<b>FINAL</b>	
SHEET	6 OF 11 SHEETS

**ADDITIVE ALTERNATE #1: SW HANGAR 7 PAVEMENT**

**KA044**

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 UPDATE BY: Adham Odeh  
 SURVEY BOOK #  
 DATE: Thursday, March 14, 2013 8:57:47 AM  
 XREF DWG: T-Hangar-base.dwg  
 tbclnt.dwg  
 tb.dwg  
 etopo.DWG



**NOTES**

1. SEE ENGINEERING INFORMATION SHEETS FOR PAVEMENT CORE DATA AND BORING LOGS.
2. THE EXISTING TERMINAL APRON PAVEMENT TO BE REMOVED SHALL BE SAWED 2" DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
3. CONTRACTOR TO TAKE MEASURES TO PROTECT ALL UNDERGROUND UTILITIES INCLUDING, BUT NOT LIMITED TO, STORM SEWER PIPE AND UNDERDRAIN FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.
4. REMOVAL OF EXISTING AGGREGATE BASE BENEATH HMA PAVEMENT TO BE REMOVED SHALL BE PAID FOR AS UNCLASSIFIED EXCAVATION.
5. PAVEMENT REMOVAL AND REPLACEMENT AND CRACK SEALING QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAYOUT THESE AREAS IN THE FIELD DURING CONSTRUCTION.
6. REMOVAL OF EXISTING GROUND RODS SHALL BE INCIDENTAL TO PAVEMENT MILLING
7. LIMITS FOR THE REMOVAL & REPLACEMENT OF THE PCC SIDEWALK PANELS SHOWN SHALL BE PER ENGINEER FIELD LAYOUT.
8. THE EXISTING SW HANGAR 7 PAVEMENT TO BE REMOVED SHALL BE SAWCUT AT FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.

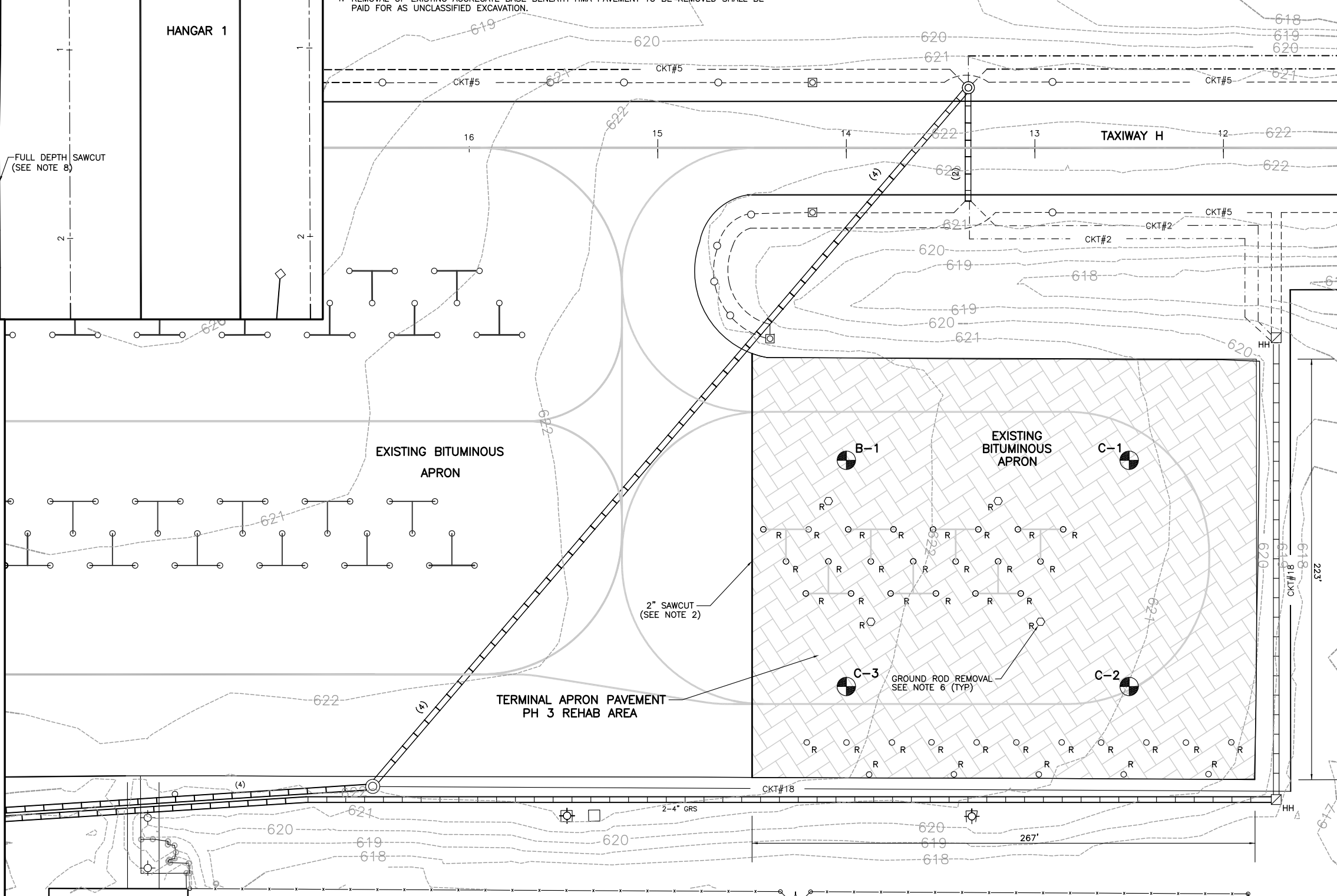
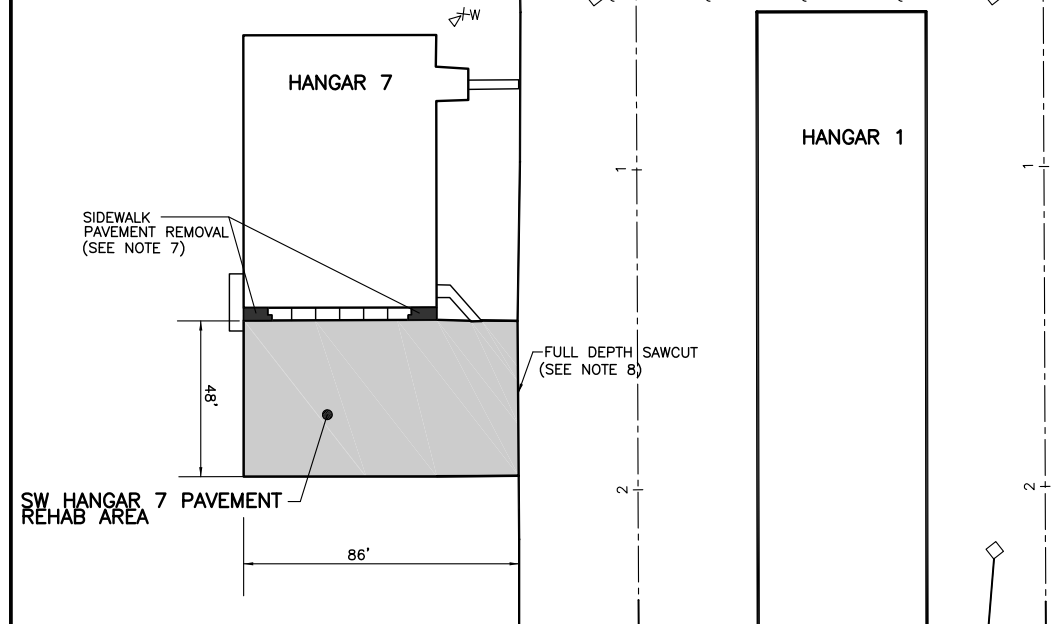
**REVISIONS**

NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

**LEGEND**

- TERMINAL APRON PAVEMENT NEW 2" (NOMINAL) BITUMINOUS MILLING (401)
- SOUTHWEST HANGAR 7 PAVEMENT EXISTING BITUMINOUS PAVEMENT TO BE REMOVED (401)
- EXISTING UNDERDRAIN
- EXISTING INLET/MANHOLE/HEADWALL
- EXISTING STORM SEWER
- EXISTING SANITARY MANHOLE
- EXISTING SANITARY CLEANOUT
- EXISTING UNDERDRAIN CLEANOUT /INSPECTION HOLE
- EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- SOIL BORING
- PAVEMENT CORE
- EXISTING FLARED END SECTION
- EXISTING CONCRETE ENCASED DUCT BANK (NUMBER OF WAYS)
- EXISTING GRS CONDUIT
- EXISTING ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL SPLICE CAN
- EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING TAXIWAY CIRCUIT
- EXISTING TAXIWAY CIRCUIT
- EXISTING ELECTRICAL CABLE
- EXISTING TELEPHONE CABLE
- EXISTING GAS LINE
- EXISTING WATERMAIN
- EXISTING WATER VALVE
- EXISTING FIRE HYDRANT
- EXISTING CONTOURS
- EXISTING TIE DOWN ANCHOR/MOORING EYE
- EXISTING GROUND ROD
- DENOTES ADJACENT ITEM TO BE REMOVED



**GEOTECHNICAL LAYOUT TABLE**

BORING NO.	STATION/OFFSET	ELEVATION
B-1	STA. 14+00, 165' LT. CENTERLINE TAXIWAY H	622.4
C-1	STA. 12+50, 165' LT. CENTERLINE TAXIWAY H	621.2
C-2	STA. 12+50, 285' LT. CENTERLINE TAXIWAY H	621.0
C-3	STA. 14+00, 285' LT. CENTERLINE TAXIWAY H	622.2

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON  
 PHASE 3  
 EXISTING CONDITIONS/  
 PROPOSED REMOVALS**

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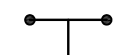

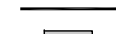

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 DRAWN BY: AAO  
 CHECKED BY: DKP  
 APPROVED BY: DKP  
 DATE: 03/13/13  
 JOB No: 12075-03-00  
 I.L. PROJECT: IKK-4246  
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**FINAL**  
 SHEET 7 OF 11 SHEETS





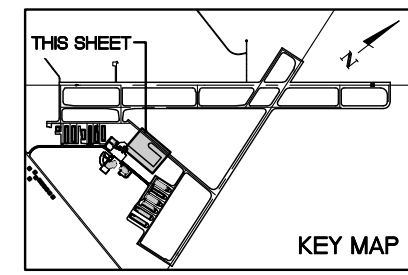
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 DATE: Thursday, March 14, 2013 9:24:44 AM  
 XREF DWG: T-Hangar-base.dwg  
 tbclnt.dwg  
 tb.dwg  
 etopo.DWG

**LEGEND**

-  NEW AIRCRAFT TIEDOWN (WITH 3 MOORING EYES)
-  EXISTING PAVEMENT MARKING
-  NEW PAVEMENT MARKING
-  LIMITS OF BASE BID CONSTRUCTION

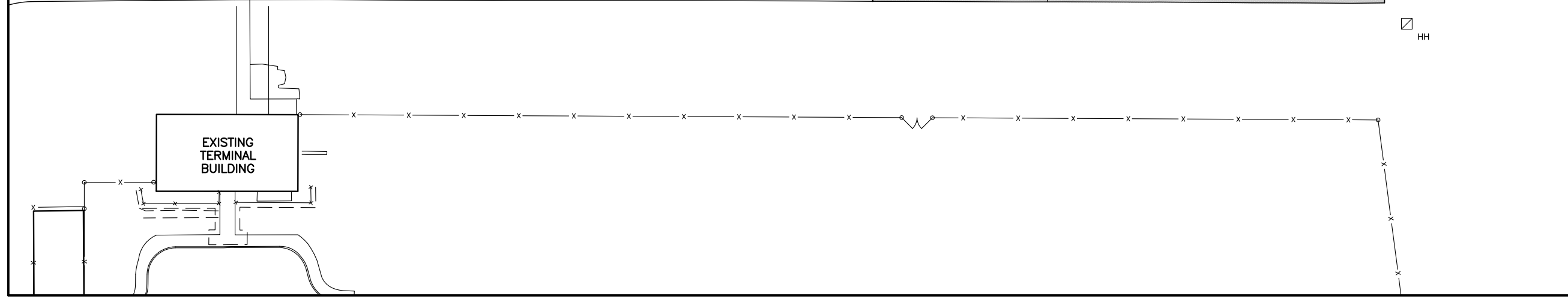
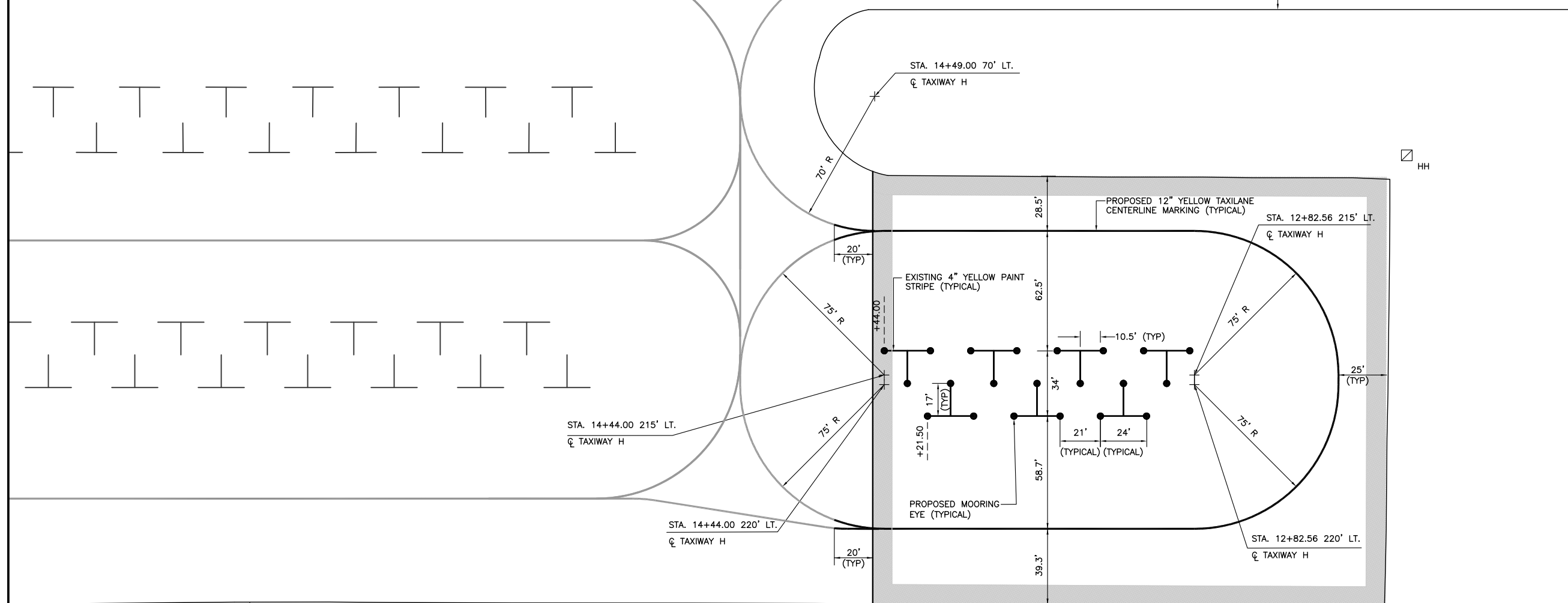
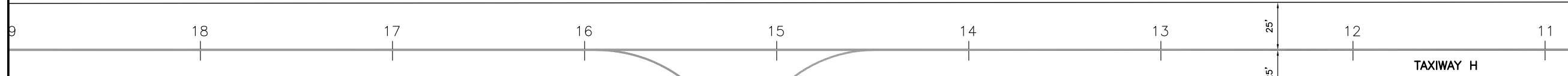
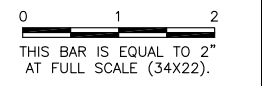
**PAVEMENT MARKING NOTES**

1. ALL NEW AND EXISTING PAVEMENTS TO BE MARKED OR RE-MARKED SHALL BE CLEAN AND DRY PRIOR TO MARKING.



**REVISIONS**

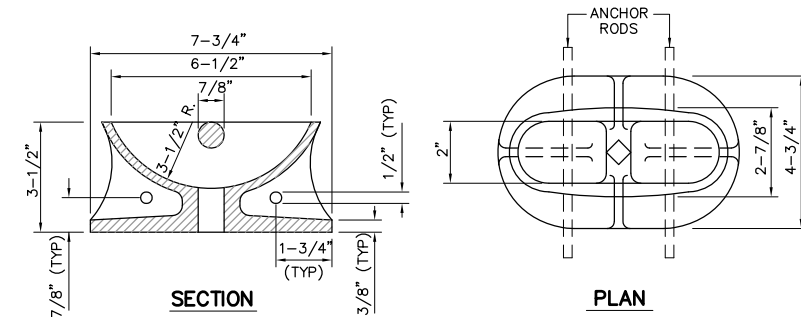
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**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON  
 PHASE 3  
 TIEDOWN/PAVEMENT MARKING PLAN**

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SHEET	9 OF 11 SHEETS

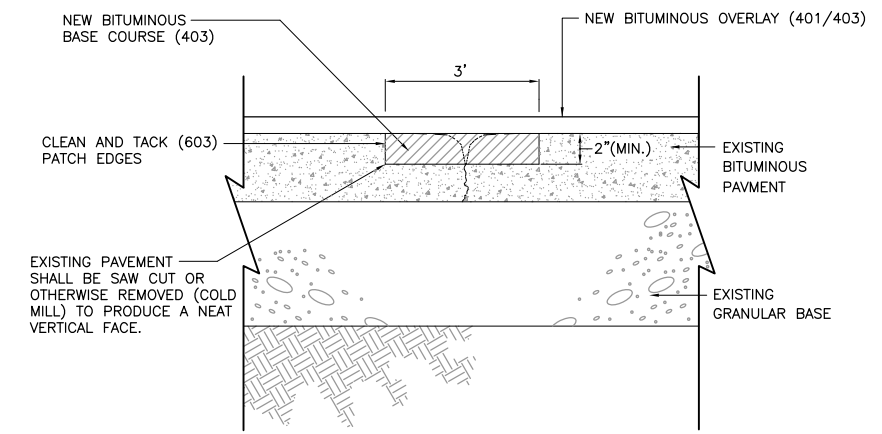
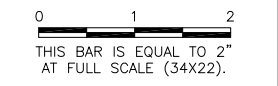


**MOORING EYE DETAIL**  
 NOT TO SCALE

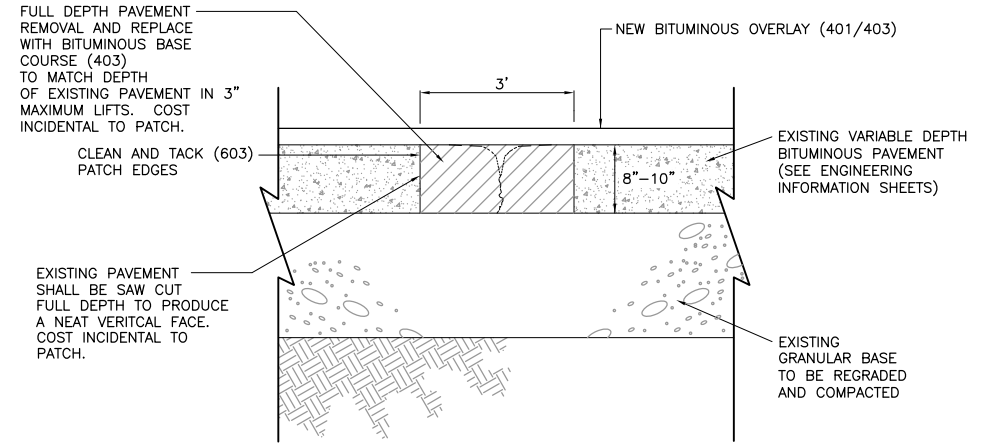
- NOTES:**
1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
  2. ANCHOR RODS TO BE NO. 3 DEFORMED REINFORCING STEEL 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.

REVISIONS

NUMBER	BY	DATE



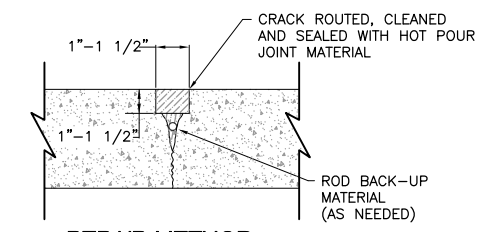
**REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A**  
 NOT TO SCALE



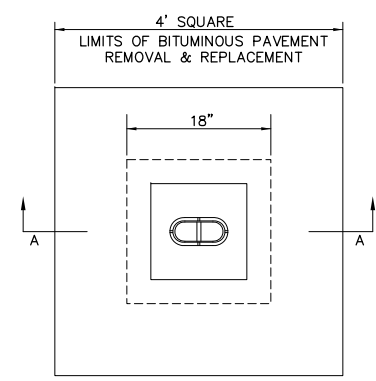
**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B**  
 NOT TO SCALE

**PAVEMENT REMOVAL AND REPLACEMENT NOTES**

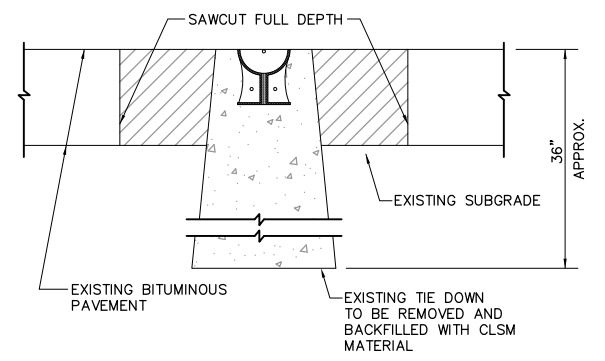
1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
3. AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT SHALL BE DONE AT SECTIONS WHERE THE REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B WAS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF GEOTECHNICAL TESTING OR PROOF ROLL AS DEFINED IN SECTION 209. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES.



**REPAIR METHOD - CRACK ROUTING AND SEALING**  
 NOT TO SCALE



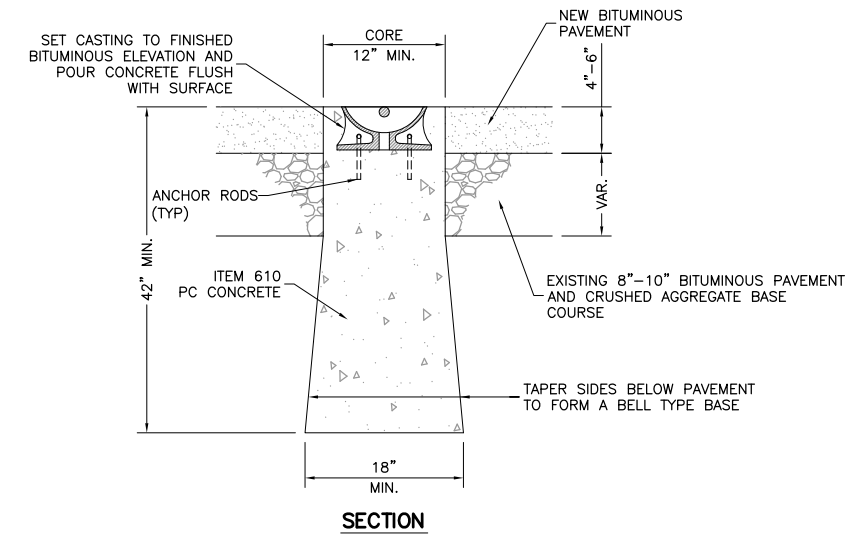
**PLAN**



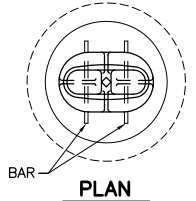
**TIE DOWN REMOVAL DETAIL**  
 NOT TO SCALE

**NOTES:**

1. THE SAWCUT AND BITUMINOUS PAVEMENT REMOVAL AND REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE MOORING EYE REMOVAL.
2. THE VOID LEFT BY THE REMOVAL SHALL BE BACKFILLED TO 9" BELOW EXISTING PAVEMENT SURFACE WITH CLSM MATERIAL. THE FINAL 9" SHALL BE FILLED WITH BITUMINOUS SURFACE COURSE TO MATCH THE MILLED SURFACE.



**SECTION**



**PLAN**

**MOORING EYE INSTALLATION DETAIL**  
 NOT TO SCALE

**NOTE:**

1. THE CONTRACTOR SHALL CORE OR SAW THROUGH BITUMINOUS OVERLAY AND EXISTING BITUMINOUS PAVEMENT AND ENLARGE HOLE TO DIMENSIONS SHOWN.

**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON  
 PHASE 3**

**MISCELLANEOUS DETAILS**

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DESIGN BY:	AAO
DRAWN BY:	AAO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	03/13/13
JOB No:	12075-03-00
IL PROJECT:	IKK-4246
S.B.G. PROJECT:	3-17-0057-B21
<b>FINAL</b>	
SHEET	10 OF 11 SHEETS

**Boring 1**

- 1.1" Surface Course
- 1.9" Surface Course— Deteriorated
- 1.5" Surface Course
- 1.3" Surface Course
- 1.7" Bituminous Base Course
- 2.2" Bituminous Base Course
- 9.7" Total Pavement Thickness**

12" Crushed Limestone (Maximum size- 2 1/2 inch, little fines)

**Core C1**

- 1.4" Surface Course
- 1.8" Surface Course
- 1.0" Surface Course
- 1.1" Surface Course--- Not Bonded
- 3.0" Bituminous Base Course
- 8.3" Total Pavement Thickness**

5" Crushed Limestone (Maximum size- 1 1/2 inches)  
 10" Crushes Limestone, trace clay (Maximum size- 3 inches to fines)

**Core C2**

- 1.3" Surface Course
- 1.7" Surface Course
- 0.8" Surface Course
- 2.3" Surface Course--- Not Bonded
- 2.3" Bituminous Base Course
- 8.4" Total Asphalt Thickness**

7.5" Crushed Limestone (Maximum size- 1 1/2 inches)  
 3" Crushes Limestone, trace clay (Maximum size- 2 1/2 inches to fines)

**Core C3**

- 1.2" Surface Course
- 2.0" Surface Course--- Not Bonded
- 1.0" Surface Course
- 1.3" Surface Course--- Not Bonded
- 1.5" Surface Course
- 2.9" Bituminous Base Course
- 9.9" Total Pavement Thickness**

13" Crushed Limestone (Maximum size- 1 3/4 inch to fines)

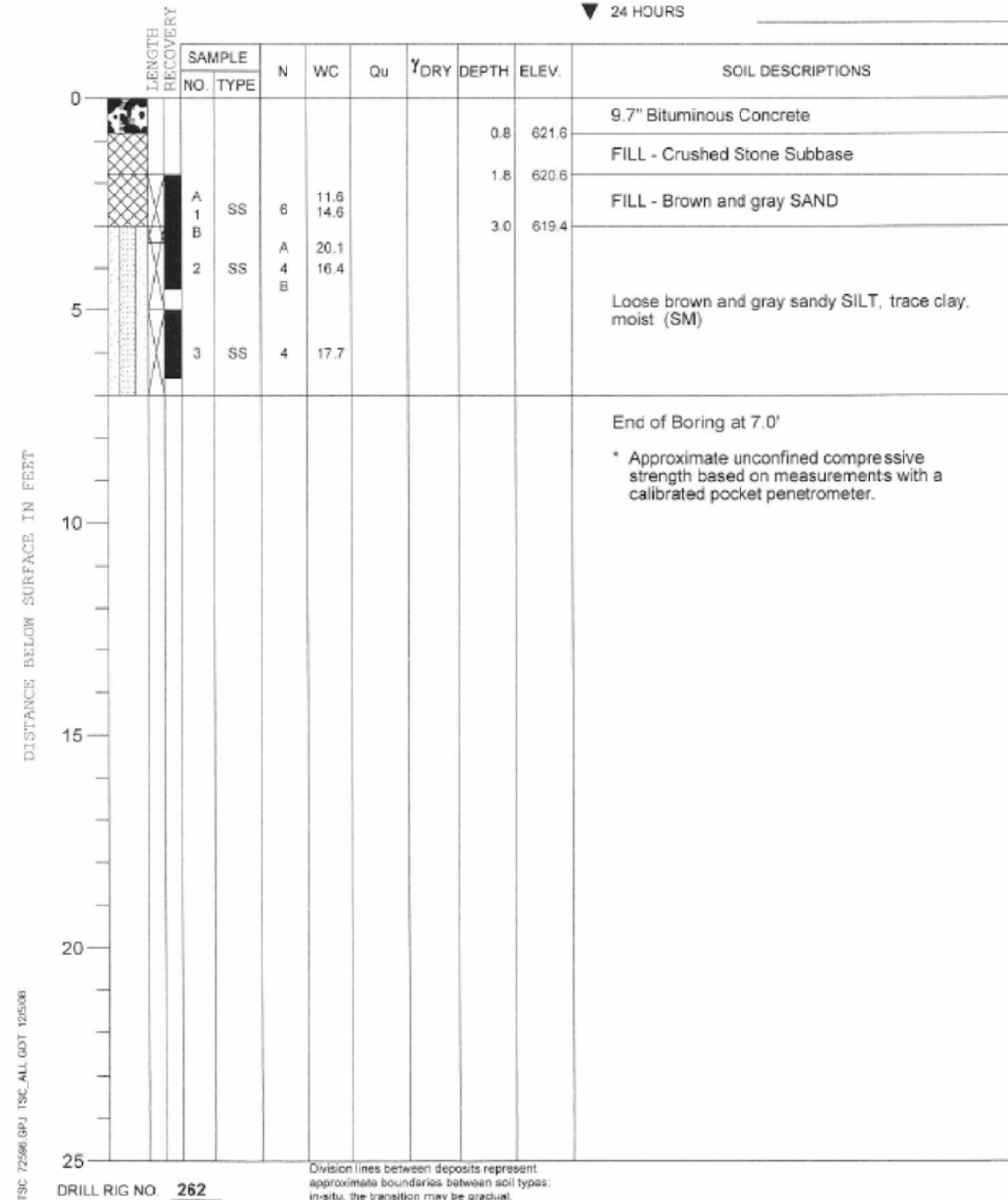
PROJECT Rehailitate Terminal Apron - Phase I, Greater Kankakee Airport

CLIENT Crawford, Murphy & Tilly

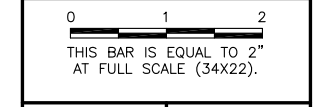


BORING 1 DATE STARTED 10-31-08 DATE COMPLETED 10-31-08 JOB L-72,596

ELEVATIONS		WATER LEVEL OBSERVATIONS	
GROUND SURFACE	<u>622.4</u>	▽ WHILE DRILLING	<u>Dry</u>
END OF BORING	<u>615.4</u>	▽ AT END OF BORING	<u>Dry</u>
		▽ 24 HOURS	



REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT  
 KANKAKEE, ILLINOIS  
 REHABILITATE TERMINAL APRON  
 PHASE 3  
 ENGINEERING INFORMATION**

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