

LETTING ITEM NO. 29A
LETTING DATE: JUNE 14, 2013

**CONSTRUCTION PLANS
 FOR**

PEO96
TOTAL SHEETS: 56

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

A.I.P. PROJECT NO.: 3-17-0080-XX
ILLINOIS PROJECT NO.: PIA-4361

RECONSTRUCT GENERAL AVIATION APRON

APRIL 26, 2013

GEOMETRIC CRITERIA
 AIRPLANE DESIGN GROUP II
 TAXIWAY DESIGN GROUP 3/4

PAVEMENT DESIGN CRITERIA
 LIGHT PAVEMENT:
 48,200# DUAL WHEEL, 1,265
 HEAVY DEPARTURES
 120,000# DUAL WHEEL, 40 ANNUAL
 DEPARTURES

MAXIMUM EQUIPMENT HEIGHT = 25'
GROUND FREQUENCY 121.85

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

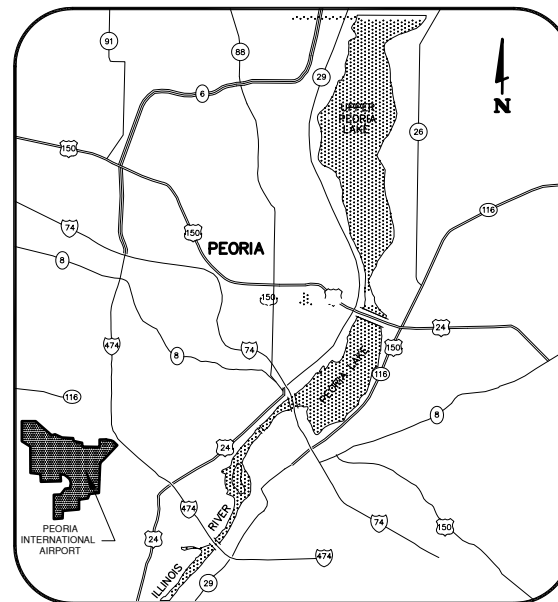
**TOWNSHIP: 8 NORTH
 RANGE: 7 EAST
 COUNTY: PEORIA
 TOWNSHIP: LIMESTONE**

811 Know what's below.
 Call before you dig.

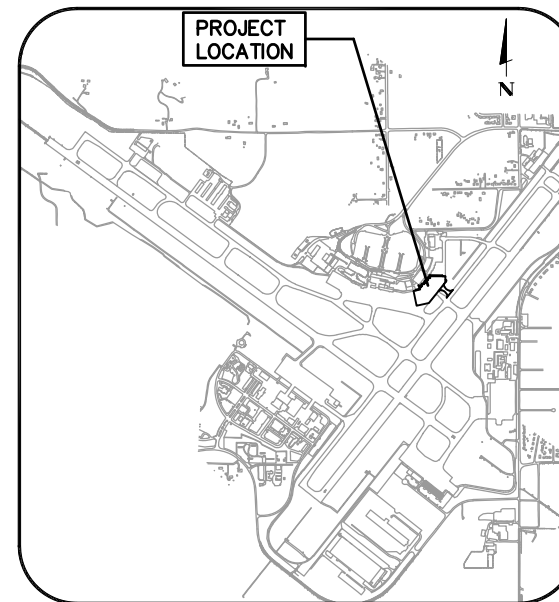
COMMON GROUND ALLIANCE
 www.call811.com or
 Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

**CALL 911 IN THE EVENT IN WHICH DAMAGE
 RESULTS IN THE RELEASE OF NATURAL GAS.**



LOCATION MAP



SITE PLAN



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

APPROVED *[Signature]*

DATE April 26, 2013

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

SUBMITTED BY *[Signature]*

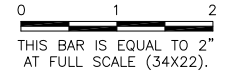
DATE April 26, 2013

CM&T JOB NUMBER 12061-03-00

PE096

REVISIONS

NUMBER	BY	DATE



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS

RECONSTRUCT GENERAL AVIATION APRON
INDEX TO SHEETS AND SUMMARY OF
QUANTITIES

DESIGN BY:	CET/TJH
DRAWN BY:	CMT
CHECKED BY:	TJH
APPROVED BY:	CET
DATE:	APRIL 26, 2013
JOB No:	12061-03
IL PROJ. NO.	PIA-4361
AIP PROJ. NO.	3-17-0080-XX
SHEET	02 OF 56 SHEETS

SHEET LIST TABLE

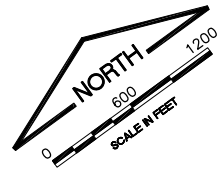
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SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AR108706	1/C #6 COUNTERPOISE	L.F.	790
AR110504	4-WAY CONCRETE ENCASED DUCT	LF	790
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	4,260
AR162510	CLASS E FENCE 10'	L.F.	30
AR162716	ELECTRIC GATE - 16'	EACH	1
AR162900	REMOVE CLASS E FENCE	L.F.	30
AR162908	REMOVE ELECTRIC GATE	EACH	1
AR209612	CRUSHED AGGREGATE BASE COURSE - 12"	S.Y.	25,050
AR401610	BITUMINOUS SURFACE COURSE	TON	43
AR403610	BITUMINOUS BASE COURSE	TON	86
AR501509	9" PCC PAVEMENT	S.Y.	12,325
AR501515	15" PCC PAVEMENT	S.Y.	12,320
AR501530	PCC TEST BATCH	EACH	1
AR501900	REMOVE PCC PAVEMENT	S.Y.	24,900
AR501921	FULL DEPTH PCC PATCHING	C.F.	250
AR510510	TIE DOWN	EACH	144
AR510515	GROUND ROD	EACH	22
AR602510	BITUMINOUS PRIME COAT	GAL	154
AR603510	BITUMINOUS TACK COAT	GAL	93
AR605541	CLEAN & SEAL CRACKS	L.F.	2,500
AR620510	PAVEMENT MARKING	S.F.	4,210
AR701512	12" RCP, CLASS IV	L.F.	22
AR705524	4" PERFORATED UNDERDRAIN W/SOCK	L.F.	619
AR751415	INLET - SPECIAL	EACH	2
AR751605	CATCH BASIN - SPECIAL	EACH	1
AR751900	REMOVE INLET	EACH	2
AR751940	ADJUST INLET	EACH	2
AR751949	ADJUST INSPECTION HOLE	EACH	1
AR800293	DUCT MARKER	EACH	4
AR901510	SEEDING	ACRE	0.2
AR908520	EXCELSIOR BLANKET	S.Y.	803

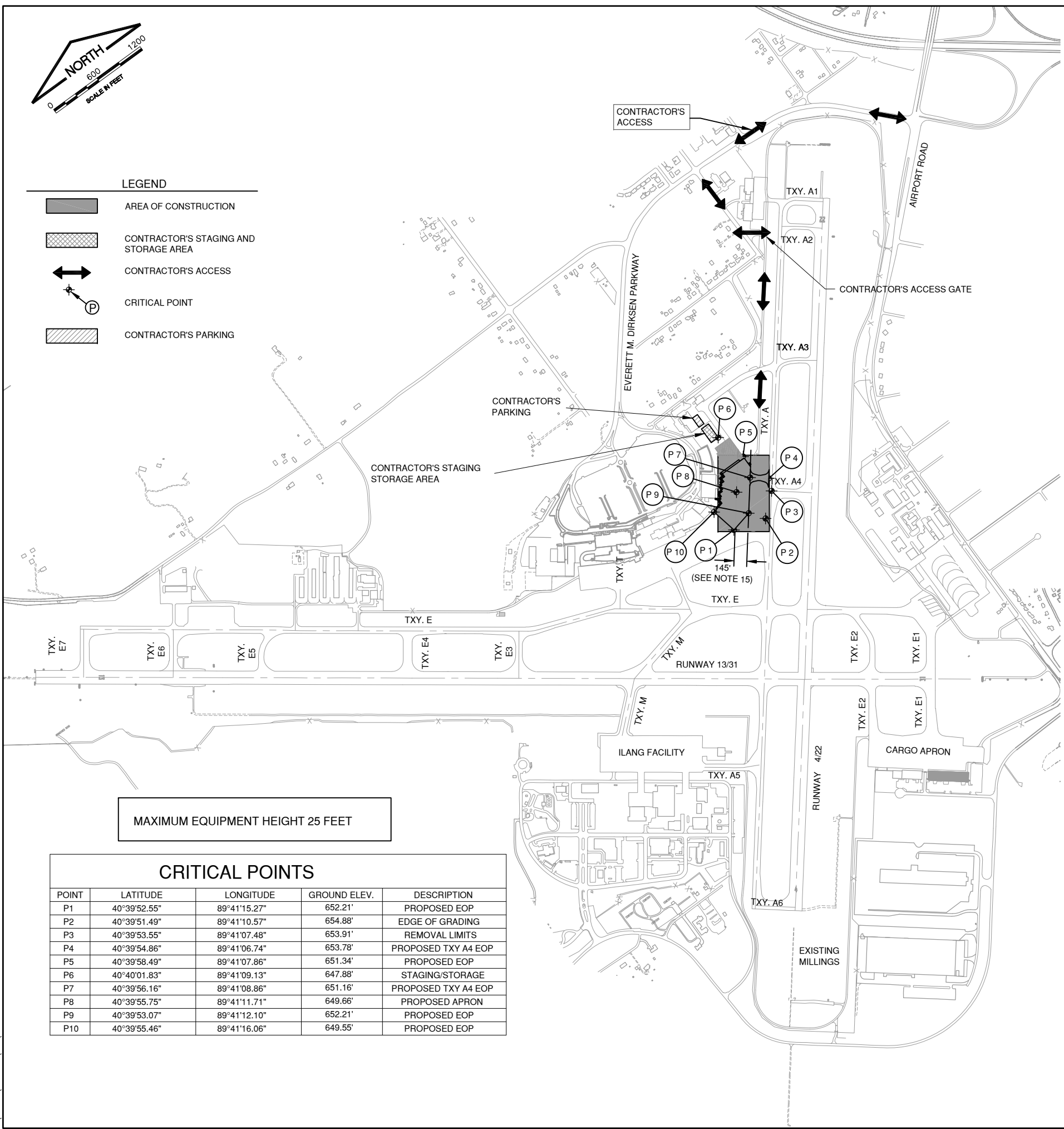
ADDITIVE ALTERNATE 1

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
AS108108	1/C #8 5 KV UG CABLE	L.F.	3,152
AS108706	1/C #6 COUNTERPOISE	L.F.	1,810
AS110012	2" DIRECTIONAL BORE	L.F.	170
AS110215	2" PVC CONDUIT, DIRECT BURIAL	L.F.	1,860
AS110504	4-WAY CONCRETE ENCASED DUCT	L.F.	170
AS125415	MITL - BASE MOUNTED	EACH	14
AS125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	2
AS125470	MODIFY EXISTING SIGN PANEL	EACH	2
AS125962	RELOCATE BASE MOUNTED LIGHT	EACH	25
AS125964	RELOCATE TAXI GUIDANCE SIGN	EACH	2
AS152410	UNCLASSIFIED EXCAVATION	CY	2,155
AS209612	CRUSHED AGGREGATE BASE COURSE - 12"	S.Y.	2,085
AS501509	9" PCC PAVEMENT	S.Y.	2,065
AS501900	REMOVE PCC PAVEMENT	S.Y.	2,225
AS620510	PAVEMENT MARKING	S.F.	1,600
AS620595	TEMPORARY MARKING & REMOVAL	S.F.	224
AS620900	PAVEMENT MARKING REMOVAL	S.F.	1,245
AS701512	12" RCP, CLASS IV	L.F.	117
AS705524	4" PERFORATED UNDERDRAIN W/SOCK	L.F.	490
AS705635	UNDERDRAIN COLLECTION STRUCTURE	EACH	1
AS705640	UNDERDRAIN CLEANOUT	EACH	2
AS705900	REMOVE UNDERDRAIN	L.F.	320
AS751410	INLET	EACH	1
AS800293	DUCT MARKER - IN PAVEMENT	EACH	8
AS901510	SEEDING	ACRE	2
AS908510	MULCHING	ACRE	2
AS908520	EXCELSIOR BLANKET	S.Y.	1,014



LEGEND

- AREA OF CONSTRUCTION
- CONTRACTOR'S STAGING AND STORAGE AREA
- CONTRACTOR'S ACCESS
- CRITICAL POINT
- CONTRACTOR'S PARKING



MAXIMUM EQUIPMENT HEIGHT 25 FEET

CRITICAL POINTS

POINT	LATITUDE	LONGITUDE	GROUND ELEV.	DESCRIPTION
P1	40°39'52.55"	89°41'15.27"	652.21'	PROPOSED EOP
P2	40°39'51.49"	89°41'10.57"	654.88'	EDGE OF GRADING
P3	40°39'53.55"	89°41'07.48"	653.91'	REMOVAL LIMITS
P4	40°39'54.86"	89°41'06.74"	653.78'	PROPOSED TXY A4 EOP
P5	40°39'58.49"	89°41'07.86"	651.34'	PROPOSED EOP
P6	40°40'01.83"	89°41'09.13"	647.88'	STAGING/STORAGE
P7	40°39'56.16"	89°41'08.86"	651.16'	PROPOSED TXY A4 EOP
P8	40°39'55.75"	89°41'11.71"	649.66'	PROPOSED APRON
P9	40°39'53.07"	89°41'12.10"	652.21'	PROPOSED EOP
P10	40°39'55.46"	89°41'16.06"	649.55'	PROPOSED EOP

GENERAL NOTES

1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF AN ESCORT OR A FLAGMAN IN RADIO CONTACT WITH FAA AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED THE APPROVED MAAP/FAA SAFETY COURSE MAY OPERATE THESE RADIOS.
3. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT OPERATIONS DIRECTOR.
4. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
5. EXCESS EARTH, BROKEN ASPHALT AND CONCRETE SHALL BE DISPOSED OF BY THE CONTRACTOR OFF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
6. VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 160' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
7. ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
8. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
9. THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
10. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
11. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS.
12. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE LIGHTED, MARKED, AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
13. COST OF TEMPORARY EDGE LIGHTING AND CABLING IN ORDER TO MAINTAIN AIRFIELD CIRCUITS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES PRIOR TO COMPLETING EXCAVATION OR TRENCHING ITEMS. ANY UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT HIS/HER OWN COST.
15. WHEN CONSTRUCTION EQUIPMENT IS OPERATING ON THE GENERAL AVIATION APRON FROM THE EDGE OF PAVEMENT NEAREST TAXIWAY A UP TO 145' FROM THE EDGE OF THE APRON, CERTAIN CONSTRUCTION EQUIPMENT MAY CAUSE A LINE OF SITE SHADOW FOR THE ATCT ON TAXIWAY A. PRIOR TO THE COMMENCEMENT OF PHASES 2 AND 3 IN WHICH TAXIWAY A IS TO REMAIN OPEN, THE CONTRACTOR AND RESIDENT ENGINEER MAY BE REQUIRED TO MEET WITH THE ATCT TO DISCUSS TESTING THE POSSIBILITY OF LINE OF SITE SHADOWS IMPOSED BY CONSTRUCTION EQUIPMENT. WHILE IN CONSTANT COMMUNICATION WITH THE ATCT, THE CONTRACTOR SHALL POSITION HIS MOST CRITICAL EQUIPMENT WITHIN THE 145' ZONE SHOWN AND UNDER THE DIRECTION OF THE ATCT REPOSITION THE EQUIPMENT TO THE LOCATIONS DIRECTED BY THE ATCT TO ALLOW THEM THE OPPORTUNITY TO DETERMINE IF LINE OF SITE SHADOWS WILL BE IMPOSED. THE CONTRACTOR SHALL DELINEATE THE LOCATIONS OF EACH INTERFERENCE AND COOPERATE WITH THE TOWER WHEN OPERATING WITHIN THESE CONSTRAINTS. AT THE TOWER'S DISCRETION, WORK IN THESE AREAS MAY BE COMPLETED USING ONE OF THE FOLLOWING OPTIONS: 1) WITH NO CLOSURES SO LONG AS CONSTANT COMMUNICATION IS MAINTAINED WITH THE TOWER; 2) TEMPORARY CLOSURES OF TAXIWAY A WHILE CONSTRUCTION EQUIPMENT IS WITHIN THE DELINEATED AREA; 3) CLOSURE OF TAXIWAY A WITHIN THE LIMITS OF THE LINE OF SITE SHADOW FOR THE DURATION OF THE PHASE.

FILE: SITE PLAN.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 9:32 PM

Pia-Base_Updated
 Pia-Base-Existing
 BASE_PROP_GEO

PE096

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 SITE PLAN**

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**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT**

DESIGN BY: CET/TJH

DRAWN BY: CMT

CHECKED BY: TJH

APPROVED BY: CET

DATE: APRIL 26, 2013

JOB No: 12061-03

IL. PROJ. NO. PIA-4361
 AIP PROJ. NO. 3-17-0080-XX

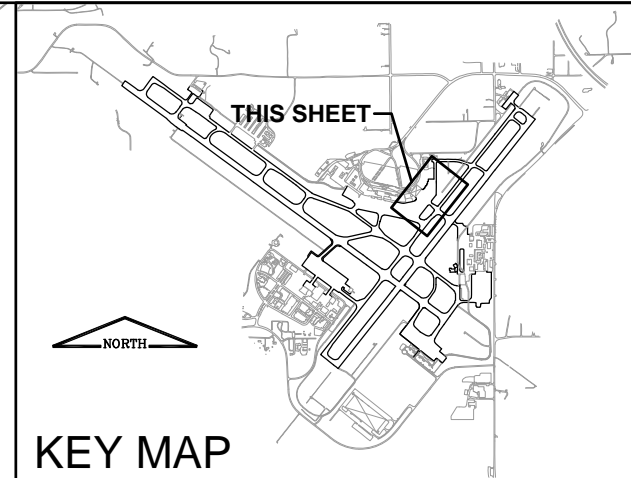
SHEET 03 OF 56 SHEETS

PE096

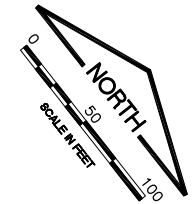
REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



KEY MAP



LEGEND

- EXISTING PAVEMENT
- PROPOSED GEOMETRY
- CP-1 CONTROL POINT

NOTES:

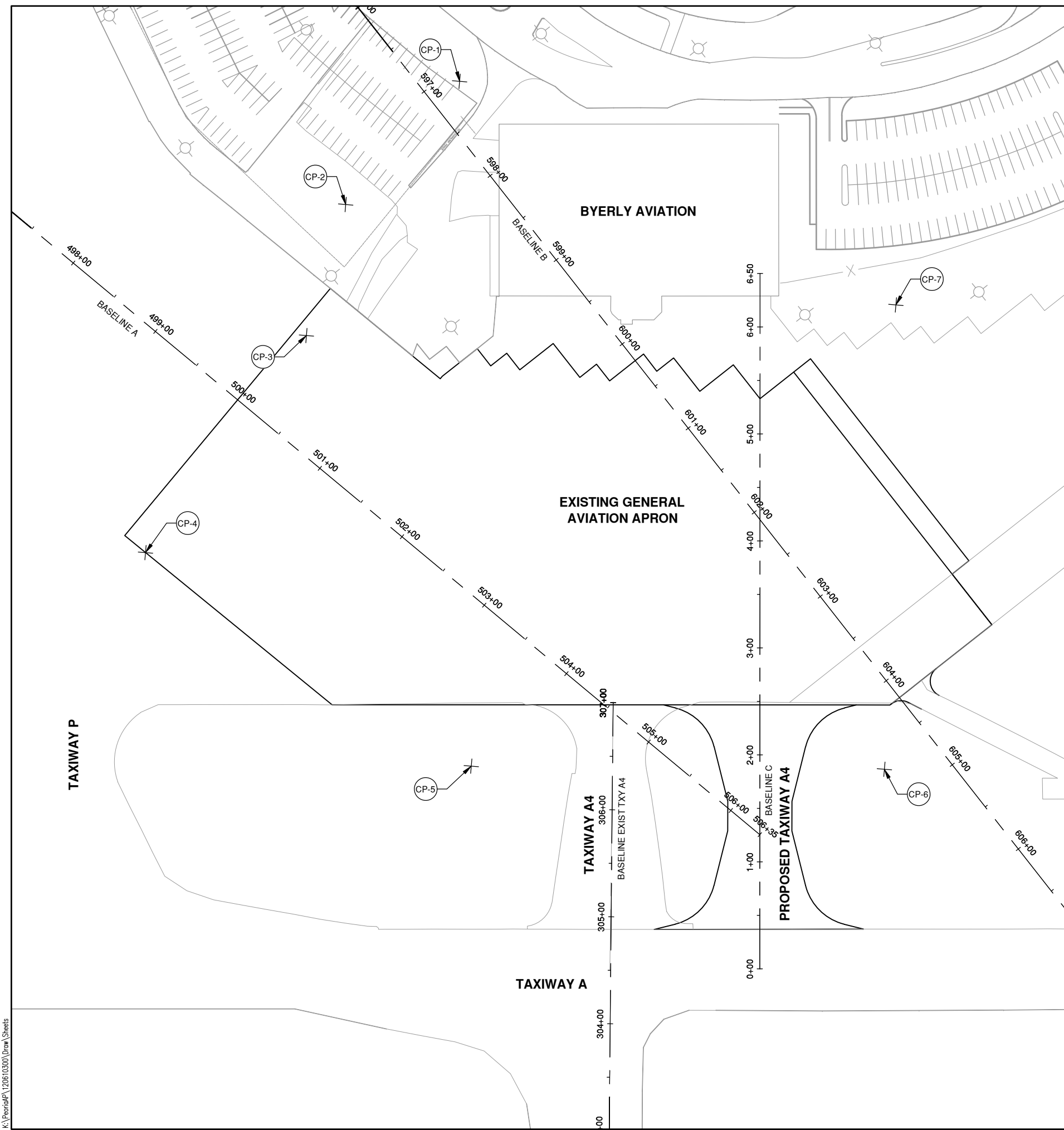
SEE CONSTRUCTION ACTIVITY PLAN 4 FOR CONTROL POINTS FOR THE TERMINAL APRON EXPANSION CONSTRUCTION.

CONTROL POINTS

POINT	OBJECT	BASELINE	STATION	OFFSET	ELEVATION	NORTHING	EASTING
CP-1	IRON PIN	BASELINE B	597+13	32.1' L	649.71	1456842.35	2429349.91
CP-2	60D NAIL	BASELINE B	597+38	122.8' R	649.25	1456687.38	2429374.26
CP-3	SCRIBE X	BASELINE A	500+11	87.4' L	649.21	1456582.68	2429447.60
CP-4	SCRIBE X	BASELINE A	500+25	164.9' R	652.29	1456338.83	2429513.58
CP-5	IRON PIN	BASELINE A	503+87	123.4' R	650.85	1456454.18	2429858.99
CP-6	IRON PIN	BASELINE B	604+64	52.5' R	650.78	1456755.53	2430100.73
CP-7	60D NAIL	BASELINE B	601+29	224.0' L	649.56	1457032.98	2429766.75
CP-8	IRON PIN	BASELINE B	602+59	440.2' L	649.91	1457248.80	2429897.11

ALIGNMENT DATA

ALIGNMENT	BEGIN STATION	NORTHING/ EASTING	END STATION	NORTHING/ EASTING
BASELINE EXIST TXY A4	STA. 300+00.00	N:1456155.9991 E:2430440.0787	STA. 307+00.25	N: 1456595.0965 E: 2429894.6012
BASELINE A	STA. 495+00.00	N:1456391.5922 E:2428965.7050	STA. 506+35.25	N: 1456626.2128 E: 2430076.4447
BASELINE B	STA. 595+00.00	N:1456810.9348 E:2429136.8186	STA. 607+42.43	N: 1456807.1621 E: 2430379.2379
BASELINE C	STA. 0+00.00	N:1456548.4024 E:2430174.8811	STA. 6+50.00	N: 1456951.4823 E: 2429664.9531



**GENERAL WAYNE A. DOWNING
 INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 CONTROL POINTS**

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SHEET 04 OF 56 SHEETS	

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GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

1. TOTAL CONTRACT TIME SHALL BE 108 CALENDAR DAYS IF ONLY THE BASE BID IS AWARDED. HOWEVER, IF THE ADDITIVE ALTERNATE IS AWARDED IN CONJUNCTION WITH THE BASE BID, THE CONTRACT TIME SHALL BE 138 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATES SHOWN.
3. CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATIONS AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. ONLY THOSE INDIVIDUALS WHO RECEIVE THIS DESIGNATION WILL BE PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT. ALL COSTS ASSOCIATED WITH THE DRIVER TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS.
7. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN.
8. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
9. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
10. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
11. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
12. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
13. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
14. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
15. THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRPORT OPERATIONS
 GREG HUSER - MAINTENANCE SUPERVISOR (309) 303-1005

AIRPORT MAINTENANCE
 GREG HUSER - MAINTENANCE SUPERVISOR (309) 303-1005

ENGINEER
 CHUCK TAYLOR P.E. - PROJECT ENGINEER (217) 787-8050
 CMT - RESIDENT ENGINEER (217) 787-8050

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL ATTEND A FINAL INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

1. NO RUNWAY CLOSURES WILL BE REQUIRED FOR THIS PROJECT. HOWEVER, A TAXIWAY CLOSURE IS REQUIRED FOR THIS PROJECT. WHEN THE TAXIWAY CLOSURE IS COORDINATED AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2F.
2. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET.

15. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5C AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

16. PROTECTION

1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY SAFETY AREA, WHICH EXTENDS 160' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

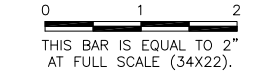
17. OTHER LIMITATIONS ON CONSTRUCTION

1. IF DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

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NUMBER	BY	DATE



**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 CONSTRUCTION SAFETY PHASING PLAN
 NOTES**

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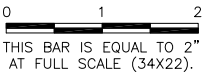
**GENERAL WAYNE A. DOWNING
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SHEET 05 OF 56 SHEETS	

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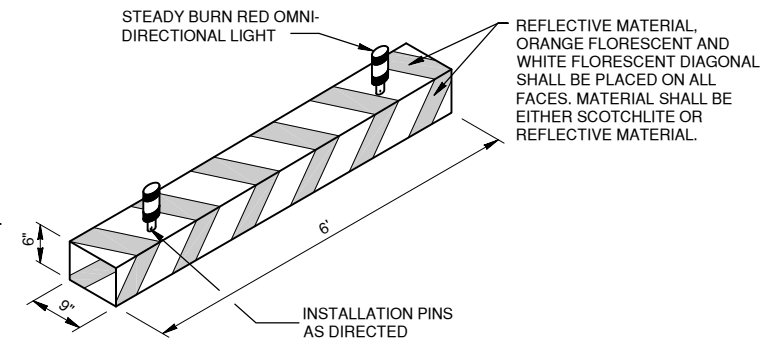


CONSTRUCTION ACTIVITY PLAN GENERAL NOTES

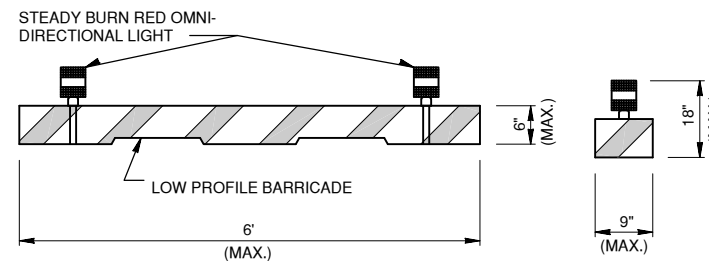
1. THE CONTRACTOR SHALL PLAN AND CONDUCT HIS WORK SO AS TO NOT INTERFERE OR HINDER THE PROGRESS OR WORK BEING PERFORMED BY OTHER CONTRACTORS.
2. THE TIMELY PROSECUTION OF THE OVERALL PROJECT IS DEPENDENT UPON THE PROPER COORDINATION BETWEEN CONTRACTORS.
3. IT SHALL BE FULLY UNDERSTOOD BY THE CONTRACTOR THAT THE PROSECUTION OF THE OVERALL PROJECT ARE THE GOVERNING CRITERIA FOR RESOLVING CONFLICTS WHICH MAY ARISE BETWEEN HIS SCHEDULE AND THE SCHEDULE OF OTHER CONTRACTORS.
4. WHEN CONFLICTS ARISE, RESOLUTION OF SUCH CONFLICTS WILL BE MADE BY THE AIRPORT THROUGH THE RESIDENT ENGINEER IN THE BEST INTEREST OF THE AIRPORT AND THE PROJECT.
5. DELAYS, CHANGES IN SCHEDULING OR THE EXPEDITION OF WORK UNDER THIS CONTRACT TO PROVIDE FOR THE TIMELY PROSECUTION OF THE PROJECT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. VEHICLE ACCESS ON PERIMETER ROADS SHALL BE MAINTAINED THROUGH OUT THE PROJECT.
7. THE CONTRACTOR SHALL BE AWARE THAT DAILY OPERATION OF THE AIRPORT WILL CONTINUE THROUGHOUT THIS PROJECT. AS A RESULT, DAILY COORDINATION WILL BE NECESSARY TO LIMIT DISRUPTION TO AIRPORT/TENANT AND CONTRACTOR OPERATIONS.
8. CONTRACTOR'S STAGING, STORAGE, AND PARKING WILL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
9. THE CONTRACTOR SHALL PLACE ALL BARRICADES, CONSTRUCTION SETBACK LINES, AND EROSION CONTROL ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
10. CONSTRUCTION PHASING IS OF CRITICAL IMPORTANCE TO THE AIRPORT FOR THIS PROJECT. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO CLOSE BOTH RUNWAYS AT THE SAME TIME.
11. CONSTRUCTION RELATED ITEMS REQUIRING THE CLOSURE OF RUNWAYS AND/OR TAXIWAYS SHALL REQUIRE A MINIMUM OF 72 HOURS NOTICE TO COORDINATION WITH THE AIRPORT AND FAA. NO EXTENSION TO CONTRACT TIME WILL BE GIVEN FOR DELAYS CAUSED BY LACK OF ADEQUATE COORDINATION.
12. CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A. THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - B. THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE AIRPORT OPERATIONS BY THE CONTRACTOR.
 - C. THE CONTRACTOR SHALL USE AN EXISTING GATE(S), OR NEW GATES AS CALLED OUT IN THE PLANS, FOR ACCESS TO THE AIRFIELD.
 - D. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - E. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - G. THE CONTRACTOR SHALL CLOSE AND SECURE THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I. THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - J. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
 - K. THE CONTRACTOR SHALL EVALUATE THE PRESERVE THE CONDITION OF THE EXISTING AIRPORT PERIMETER ROAD AND ALL OTHER ACCESS ROUTES UTILIZED BY THE CONTRACTOR FOR ACCESS TO THE SITE. THE CONTRACTOR SHALL DOCUMENT THE CONDITION OF THE EXISTING PAVEMENT IN THE PRESENCE OF THE AIRPORT AND RESIDENT ENGINEER PRIOR TO THE INITIATION OF CONSTRUCTION. AT THE COMPLETION OF CONSTRUCTION, ANY PAVEMENT USED BY THE CONTRACTOR THAT IS SAID TO BE DAMAGED BY HIS OPERATIONS SHALL BE REPAIRED TO IT'S ORIGINAL CONDITION AND/OR TO THE SATISFACTION AT THE CONTRACTOR'S EXPENSE.

BEAM BARRICADE NOTES

- 1.) BARRICADE SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
- 2.) BARRICADE SHALL BE EASILY COLLAPSIBLE UPON CONTACT WITH AIRCRAFT.
- 3.) PLACE AT 10' INTERVALS.
- 4.) NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.



ISOMETRIC



FRONT ELEVATION

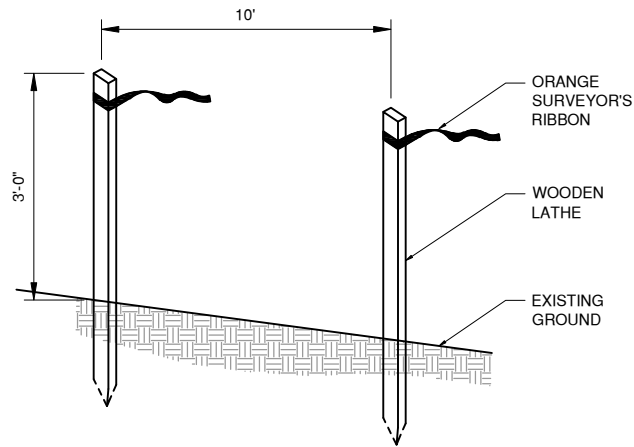
SIDE ELEVATION

LOW PROFILE - BEAM BARRICADE DETAILS

N.T.S.

SECURITY NOTES

1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. AT NO TIME ARE UNSUPERVISED GATE OPENINGS PERMITTED AT THE AIRPORT. IF THE PROPOSED WORK REQUIRES GATE OPENING, THE CONTRACTOR SHALL PROVIDE A GUARD TO PREVENT UNAUTHORIZED ACCESS. THE CONTRACTOR SHALL SECURE THE GATE AT THE COMPLETION OF THE WORK DAY.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES DURING THE COURSE OF THE WORK.
4. FINES CAN BE LEVIED AGAINST THE CONTRACTOR BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) FOR NEGLIGENCE IF THE AIRPORT SECURITY IS COMPROMISED AND THE AIRPORT PERIMETER FENCE LINE IS NOT MAINTAINED AS SPECIFIED ABOVE. FINES CAN ALSO BE LEVIED AGAINST THE CONTRACTOR FOR FAILURE TO COOPERATE WITH THE AIRPORT MANAGEMENT AS REQUIRED TO MAINTAIN AIRPORT SECURITY.



CONSTRUCTION SETBACK LINE DETAIL

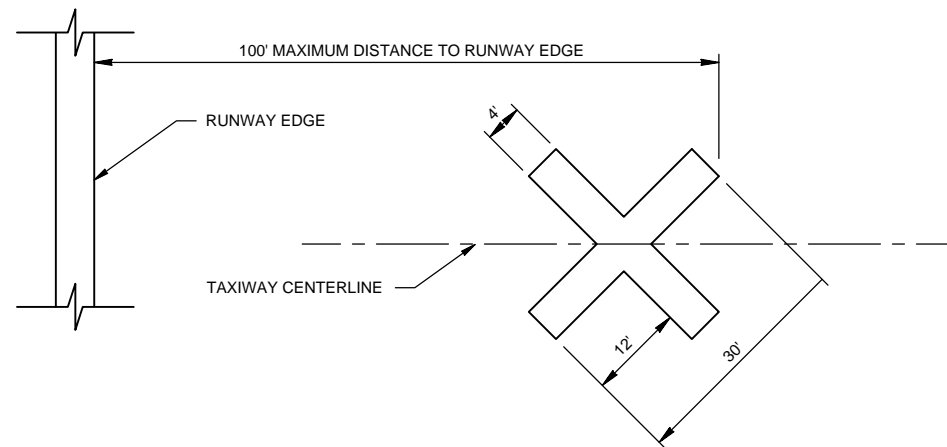
N.T.S.

FLASHER BARRICADE DETAIL-IDOT TYPE 1

N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 10' INTERVALS.



TYPICAL TAXIWAY CLOSURE MARKER DETAIL

N.T.S.

NOTES

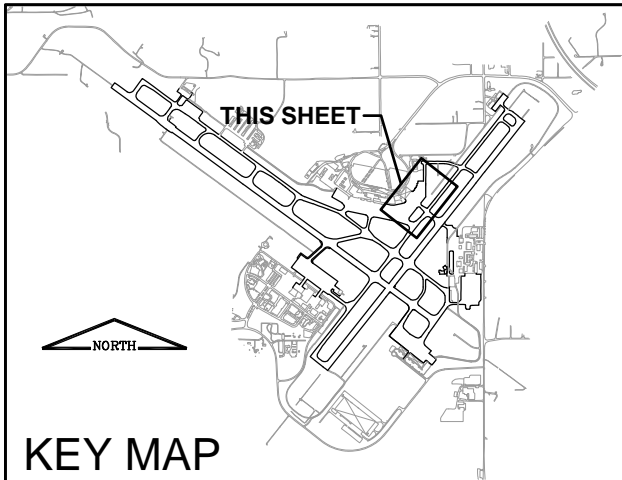
- 1.) THE MARKING SHALL BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING, OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OFA CRITERIA AND ARE APPROVED BY THE AIRPORT
- 2.) PLACE OVER TAXIWAY CENTERLINE.
- 3.) YELLOW X SHALL BE ADEQUATELY SECURED TO WITHSTAND JET BLAST OF 100 MPH WINDS.

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 CONSTRUCTION ACTIVITY PLAN NOTES AND
 DETAILS**

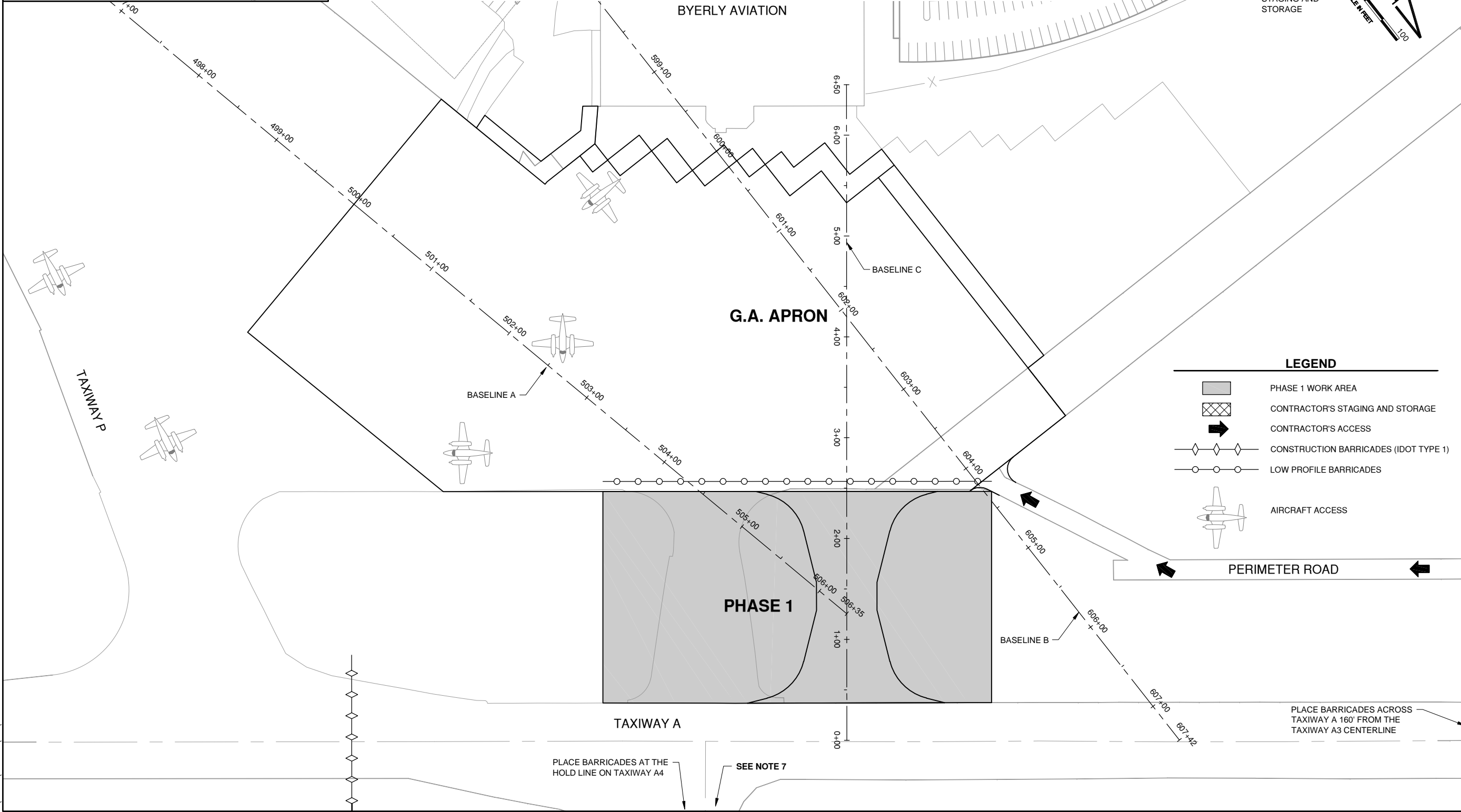
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PHASE 1 NOTES

1. THE PHASE 1 WORK AREA SHALL CONSIST OF THE ADDITIVE ALTERNATE CONSTRUCTION. SHOULD THE ADDITIVE ALTERNATE NOT BE AWARDED AS PART OF THE CONSTRUCTION PROJECT, THIS PHASE SHALL BE ELIMINATED.
2. THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT AN ESTIMATED PROGRESS SCHEDULE TO THE RESIDENT ENGINEER TWO (2) WEEKS PRIOR TO THE INITIATION OF PHASE 1. THE PROGRESS SCHEDULE SHALL BE REVIEWED BY THE AIRPORT AND BYERLY AVIATION TO ENSURE CONFLICTS BETWEEN CONSTRUCTION AND AIRPORT OPERATIONS ARE MINIMIZED. SHOULD THE PROPOSED PROGRESS SCHEDULE RESULT IN CONFLICTS, THE CONTRACTOR SHALL SUBMIT A REVISED SCHEDULE. AFTER THE COMMENCEMENT OF PHASE 1, THE CONTRACTOR SHALL SUBMIT UPDATED PROGRESS REPORTS ON A WEEKLY BASIS.
3. THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR 7 DAYS PRIOR TO THE INITIATION OF PHASE 1.
4. ALL BARRICADES SHALL BE SET UP PRIOR TO THE INITIATION OF THIS PHASE AND SHALL REMAIN IN PLACE UNTIL THE TAXIWAY A CLOSURE IS NO LONGER REQUIRED.
5. TEMPORARY "JUMPER" CABLES SHALL BE INSTALLED AS SHOWN ON THE CABLE AND CONDUIT PLAN TO MAINTAIN A CONTINUOUS EDGE LIGHT CIRCUIT FOR THE PORTIONS OF CIRCUIT 1 AND CIRCUIT 2 TO REMAIN OPEN.
6. PAVEMENT MARKING FOR PHASE 1 UP TO THE NON-MOVEMENT AREA BOUNDARY MARKING SHALL BE COMPLETED AT THE END OF PHASE 1. A SECOND MOBILIZATION SHALL BE REQUIRED FOR THE MARKING IN PHASES 2 & 3.
7. THE CONTRACTOR SHALL MARK A TEMPORARY TAXIWAY CLOSURE MARKER ON TAXIWAY A4 AT THE INTERSECTION OF RUNWAY 4/22. SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS FOR THE TEMPORARY CLOSURE MARKING DETAIL.



LEGEND

- PHASE 1 WORK AREA
- CONTRACTOR'S STAGING AND STORAGE
- CONTRACTOR'S ACCESS
- CONSTRUCTION BARRICADES (IDOT TYPE 1)
- LOW PROFILE BARRICADES
- AIRCRAFT ACCESS

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 CONSTRUCTION ACTIVITY PLAN 1**

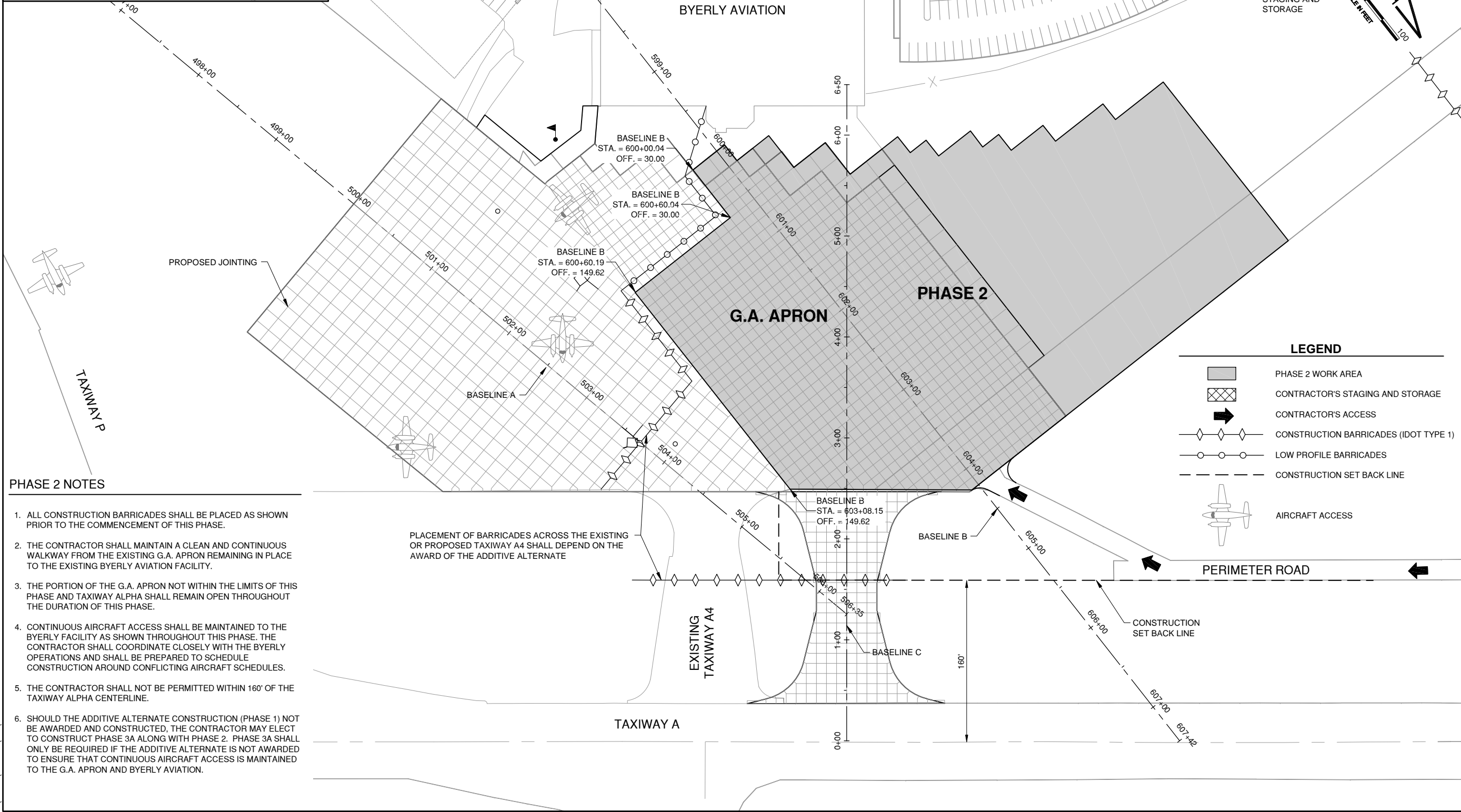
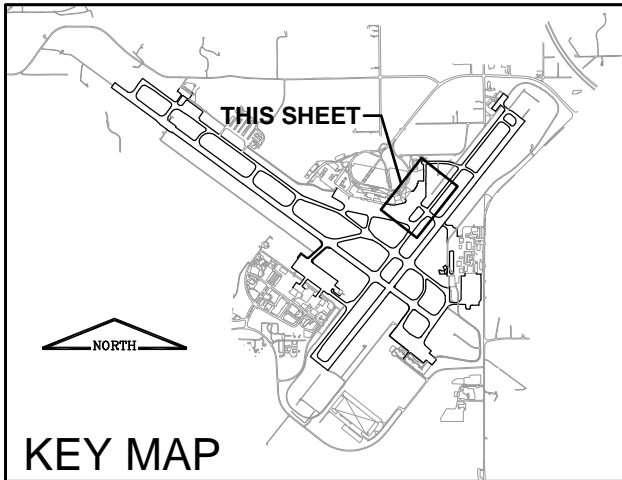
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- PHASE 2 NOTES**
1. ALL CONSTRUCTION BARRICADES SHALL BE PLACED AS SHOWN PRIOR TO THE COMMENCEMENT OF THIS PHASE.
 2. THE CONTRACTOR SHALL MAINTAIN A CLEAN AND CONTINUOUS WALKWAY FROM THE EXISTING G.A. APRON REMAINING IN PLACE TO THE EXISTING BYERLY AVIATION FACILITY.
 3. THE PORTION OF THE G.A. APRON NOT WITHIN THE LIMITS OF THIS PHASE AND TAXIWAY ALPHA SHALL REMAIN OPEN THROUGHOUT THE DURATION OF THIS PHASE.
 4. CONTINUOUS AIRCRAFT ACCESS SHALL BE MAINTAINED TO THE BYERLY FACILITY AS SHOWN THROUGHOUT THIS PHASE. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE BYERLY OPERATIONS AND SHALL BE PREPARED TO SCHEDULE CONSTRUCTION AROUND CONFLICTING AIRCRAFT SCHEDULES.
 5. THE CONTRACTOR SHALL NOT BE PERMITTED WITHIN 160' OF THE TAXIWAY ALPHA CENTERLINE.
 6. SHOULD THE ADDITIVE ALTERNATE CONSTRUCTION (PHASE 1) NOT BE AWARDED AND CONSTRUCTED, THE CONTRACTOR MAY ELECT TO CONSTRUCT PHASE 3A ALONG WITH PHASE 2. PHASE 3A SHALL ONLY BE REQUIRED IF THE ADDITIVE ALTERNATE IS NOT AWARDED TO ENSURE THAT CONTINUOUS AIRCRAFT ACCESS IS MAINTAINED TO THE G.A. APRON AND BYERLY AVIATION.

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 CONSTRUCTION ACTIVITY PLAN 2**

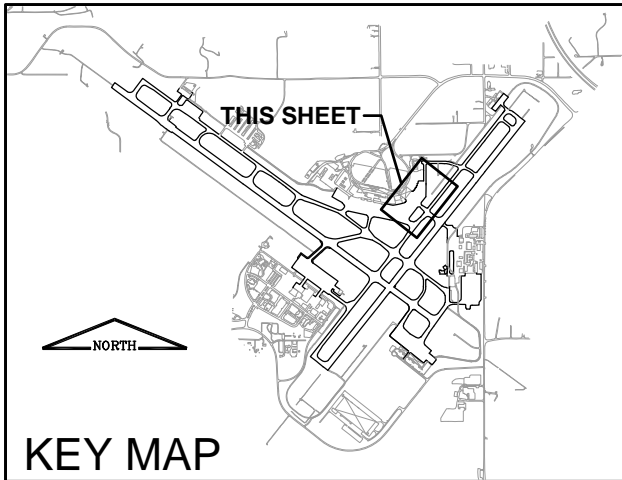
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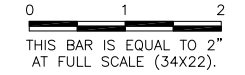


PHASE 3 NOTES

1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT 7 DAYS PRIOR TO THE INITIATION OF PHASE 3.
2. ALL BARRICADES SHALL BE PLACED AS SHOWN PRIOR TO STARTING WORK IN THIS PHASE.
3. TAXIWAY P SHALL REMAIN OPEN THROUGHOUT THE DURATION OF THIS PHASE.
4. ACCESS TO THE BYERLY FACILITY SHALL REMAIN AS SHOWN THROUGHOUT THE DURATION OF THIS PHASE.
5. SHOULD THE ADDITIVE ALTERNATE (PHASE 1) NOT BE AWARDED AND CONSTRUCTED, THE ADDITION OF PHASE 3A SHALL BE REQUIRED TO ALLOW ACCESS TO THE G.A. APRON/BYERLY AVIATION. DURING THE CONSTRUCTION OF PHASE 3A, BARRICADES SHALL BE PLACED ACROSS TAXIWAY A4 AS SHOWN ON CONSTRUCTION ACTIVITY PLAN 2.
6. PHASE 3A SHALL BE ELIMINATED AND CONSTRUCTED AS PART OF PHASE 3 SHOULD THE ADDITIVE ALTERNATE CONSTRUCTION BE AWARDED AND CONSTRUCTED AS PER CONSTRUCTION ACTIVITY PLAN 1.
7. ALL CONSTRUCTION EQUIPMENT AND PERSONNEL OPERATING WITHIN THE TAXILANE OFA SHALL BE IN CONTINUOUS RADIO CONTACT WITH AND OPERATING UNDER THE DIRECTION OF THE OFA. ALL WORK WITHIN THESE LIMITS SHALL BE EXPEDITED AND THE CONTRACTOR SHALL BE PREPARED TO IMMEDIATELY EXIT THE OFA WHEN DIRECTED BY THE ATCT.

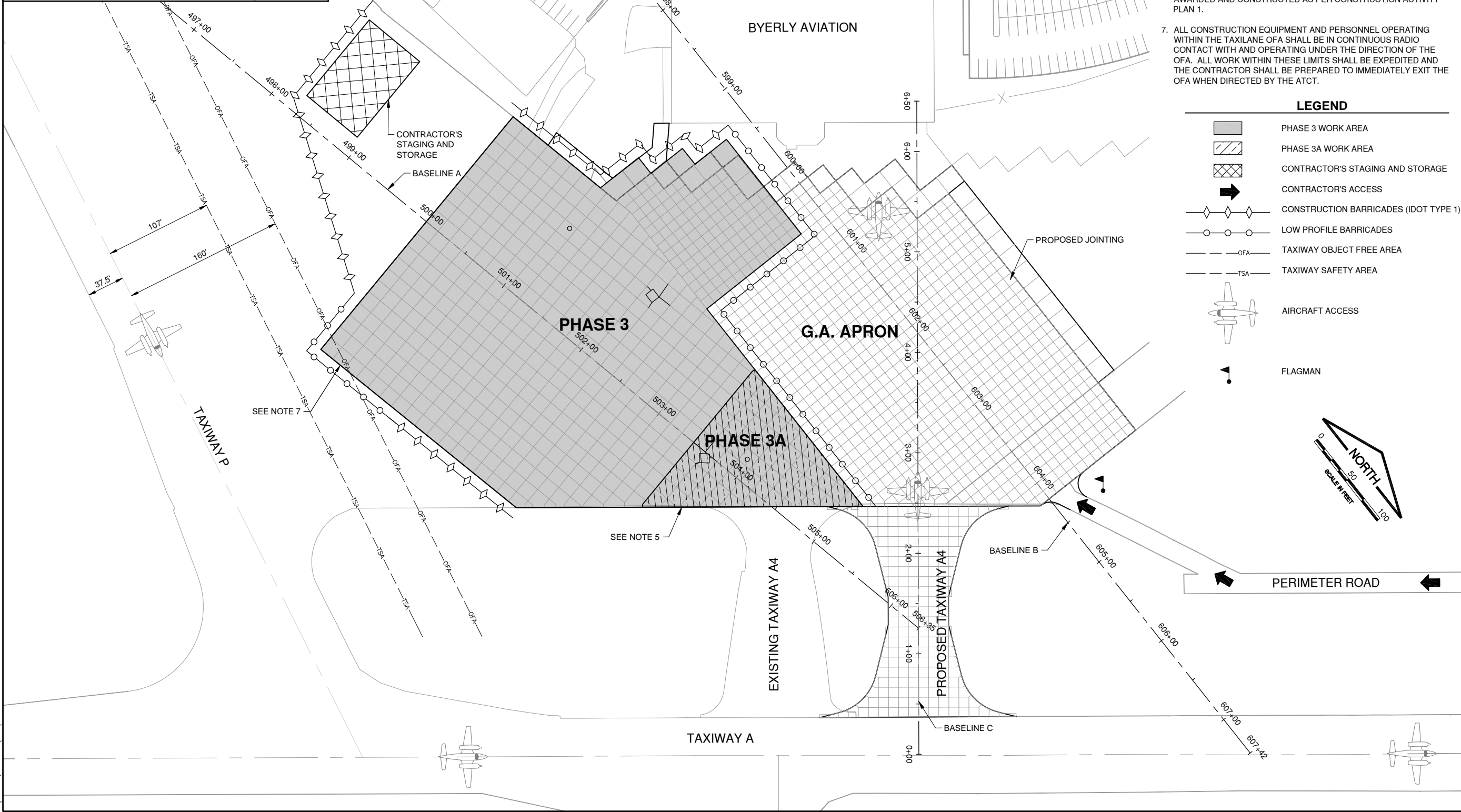
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LEGEND

- PHASE 3 WORK AREA
- PHASE 3A WORK AREA
- CONTRACTOR'S STAGING AND STORAGE
- CONTRACTOR'S ACCESS
- CONSTRUCTION BARRICADES (IDOT TYPE 1)
- LOW PROFILE BARRICADES
- OFA TAXIWAY OBJECT FREE AREA
- TSA TAXIWAY SAFETY AREA
- AIRCRAFT ACCESS
- FLAGMAN



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**RECONSTRUCT GENERAL AVIATION APRON
 CONSTRUCTION ACTIVITY PLAN 3**

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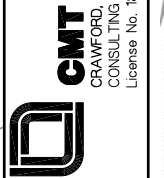
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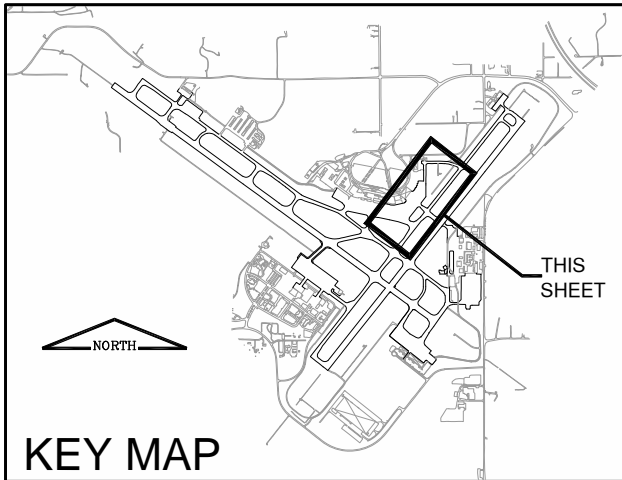
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**RECONSTRUCT GENERAL AVIATION APRON
 EXISTING PAVEMENT STRUCTURES**

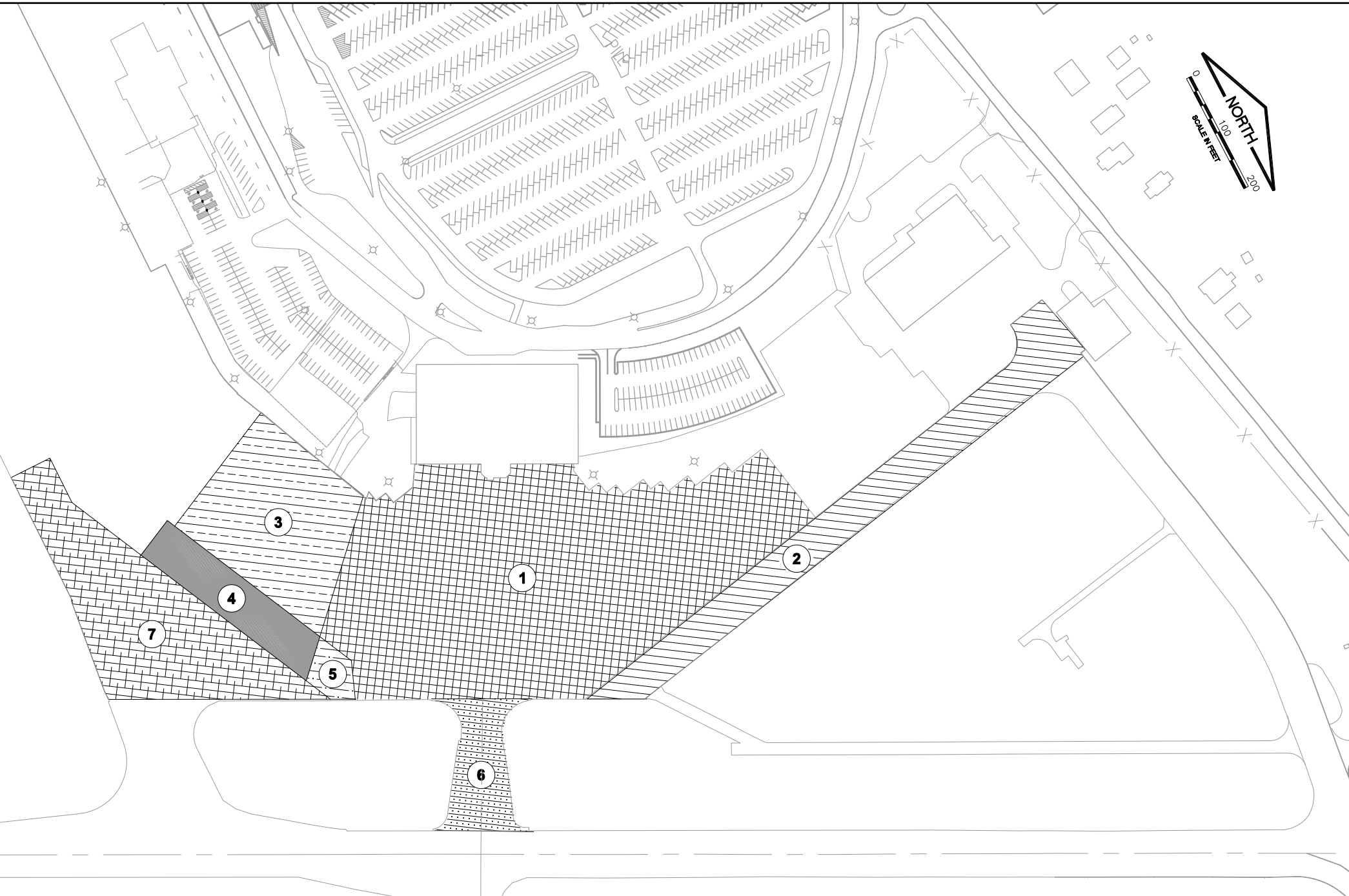
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KEY MAP



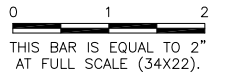
LEGEND

	1	9" PCC PAVEMENT (501) 6" LIME AND FLYASH TREATED SUBGRADE (155)
	2	3" BITUMINOUS SURFACE COURSE (401) 10" PCC PAVEMENT (501)
	3	14" PCC PAVEMENT (501) 7" BITUMINOUS BASE COURSE (201) 6" TREATED LIME SUBGRADE (155)
	4	4" AND VARIABLE THICKNESS BITUMINOUS SURFACE COURSE (401) 10" PCC PAVEMENT (501)
	5	VARIABLE THICKNESS BITUMINOUS SURFACE (401) 10" PCC PAVEMENT (501)
	6	3" BITUMINOUS SURFACE COURSE (401) VARIABLE DEPTH BITUMINOUS BASE COURSE (403) 6" PCC PAVEMENT (501)
	7	18" PCC PAVEMENT (501) 8" CRUSHED AGGREGATE BASE COURSE (209)

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NOTES

- DEMOLITION AND REMOVALS SHALL BE COMPLETED ONLY WITHIN THE LIMITS OF EACH PHASE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN.
- ALL MOORING EYES (TIE DOWNS) AND GROUND RODS WITHIN THE LIMITS OF THE PAVEMENT DEMOLITION SHALL BE REMOVED. REMOVAL OF THE MOORING EYES AND TIE DOWNS SHALL BE CONSIDERED INCIDENTAL TO THE PAVEMENT REMOVAL AND SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT.
- THE CONTRACTOR SHALL USE EXTREME CAUTION TO PROTECT ALL EXISTING UNDERGROUND UTILITIES AS WELL AS STRUCTURES IN THE PAVEMENT THAT ARE TO REMAIN IN PLACE. ANY DAMAGE TO THESE ITEMS SHALL BE REPAIRED BY THE CONTRACTOR WITH ALL COSTS TO BE BORNE BY THE CONTRACTOR.
- AN EXISTING DUCT WITH FAA UTILITY CABLES RUNS UNDER THE REMOVAL AREA. THE LOCATION OF THE DUCT/CABLE SHOWN IS APPROXIMATE. THE CONTRACTOR SHALL COORDINATE WITH THE FAA PRIOR TO ANY REMOVAL OPERATIONS TO JOINTLY LOCATE THE LOCATION OF THE EXISTING UTILITY. THE CONTRACTOR SHALL PROTECT THE EXISTING UTILITY THROUGHOUT THE DURATION OF THE PROJECT. SHOULD THE UTILITY BECOME DAMAGED BY THE CONTRACTOR'S OPERATIONS, THE PROJECT SHALL BE IMMEDIATELY SUSPENDED UNTIL THE UTILITY IS REPAIRED AT THE DIRECTION AND SATISFACTION OF THE FAA. ALL COSTS OF THE REPAIR AND ANY COSTS RELATED TO THE PROJECT DELAYS SHALL BE BORNE BY THE CONTRACTOR.
- ADJACENT TO THE EXISTING FAA DUCT IS AN EXISTING ELECTRICAL DUCT CONTAINING 13.5KV PRIMARY POWER. THE TWO DUCTS ENTER A COMMON MANHOLE AS SHOWN ON THE EXISTING CONDITIONS AND REMOVAL SHEETS. THE CONTRACTOR SHALL USE EXTREME CAUTION WHEN WORKING ADJACENT TO THE HIGH VOLTAGE CABLE.
- AS SHOWN ON EXISTING CONDITIONS AND REMOVALS, FOR A WIDTH OF 10' (5 FEET ON EITHER SIDE OF THE EXISTING FAA AND UTILITY POWER DUCTS) THE EXISTING PAVEMENT STRUCTURE SHALL BE REMOVED BY USE OF HAND METHODS. HEAVY EQUIPMENT THAT MAY DAMAGE THE EXISTING UTILITIES SHALL NOT BE ALLOWED FOR DEMOLITION OPERATIONS WITHIN THE LIMITS SHOWN OVER THE EXISTING DUCTS.
- WHERE SAWCUTTING IS SHOWN ADJACENT TO EXISTING PAVEMENT TO REMAIN IN PLACE, A SECOND SAWCUT LINE SHALL BE CONSTRUCTED ON THE PAVEMENT TO BE REMOVED TO PREVENT DAMAGE TO THE EXISTING PAVEMENT DURING BREAKING OPERATIONS.
- EXISTING RECORDS IDENTIFY AN EXISTING EDGE DRAIN INSPECTION HOLE FOR THE EXISTING 6" EDGE DRAIN PIPE AT THE APPROXIMATE LOCATION SHOWN. FIELD/VISUAL SURVEYS HAVE INDICATED THAT THE EDGE DRAIN HAS EITHER BEEN REMOVED OR IS BURIED. SHOULD THE EDGE DRAIN BE DISCOVERED DURING CONSTRUCTION, IT SHALL BE REMOVED AND THE END OF THE LINE SHALL BE CAPPED. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE PAVEMENT REMOVAL OPERATIONS.
- SEE DRAINAGE PLAN AND UNDERDRAIN PLAN FOR UNDERDRAIN/EDGE DRAIN PIPE REMOVAL LIMITS AND NOTES. UNDERDRAIN/EDGE PIPE REMOVAL NOT SHOWN EXISTING CONDITIONS AND REMOVALS TO IMPROVE SHEET CLARITY.
- DURING THE CONSTRUCTION OF THE G.A. APRON PAVEMENT, THE CONTRACTOR SHALL USE CAUTION TO PROTECT THE EXISTING EDGE LIGHTS OFF THE EDGE OF PAVEMENT. AS NECESSARY TO PROTECT THE EDGE LIGHTS, THE CONTRACTOR SHALL REMOVE THE LIGHT FIXTURE AND STORE IN A SAFE LOCATION. THE CIRCUIT SHALL BE TEMPORARILY RECONNECTED INSIDE OF THE LIGHT CAN BASE AND A BASE PLATE SHALL BE BOLTED TO THE TOP OF THE CAN TO PREVENT WATER FROM FLOWING IN. THE CONTRACTOR MAY ELECT TO CONSTRUCT TEMPORARY BASE PLATES OUT OF PLYWOOD AND BOLT THEM TO THE CAN PROVIDED THAT THEY ARE SECURELY ATTACHED AND PREVENT WATER FROM ENTERING THE CAN. PROTECTION OF THE EDGE LIGHTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT.

LEGEND

	EXISTING PAVEMENT
	EXISTING BASE MOUNTED TAXIWAY LIGHT
R	TO BE REMOVED
A	TO BE ADJUSTED
RR	TO BE REMOVED AND RELOCATED
P	PROTECT
	EXISTING TAXIWAY GUIDANCE SIGN
	EXISTING PAVEMENT MARKING
	EXISTING MOORING EYE/GROUND ROD
	EXISTING INLET
	EXISTING DUCT
	EXISTING STORM SEWER
	EXISTING DOWN SPOUT
	EXISTING FENCE
	EXISTING HANDHOLE/INSPECTION HOLE
	EXISTING EDGE LIGHT CIRCUIT 1
	EXISTING EDGE LIGHT CIRCUIT 2
	EXISTING JOINT
	EXISTING WATER LINE
	EXISTING ELECTRICAL MANHOLE
	EXISTING APRON LIGHTING
	EXISTING FAA CABLE
	PROPOSED FULL DEPTH SAW CUT
	HAND REMOVAL LIMITS

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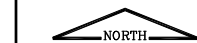
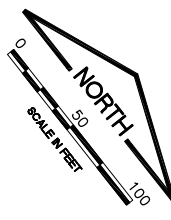
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

GENERAL WAYNE A. DOWNING
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RECONSTRUCT GENERAL AVIATION APRON
EXISTING CONDITION LEGEND AND NOTES

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KEY MAP

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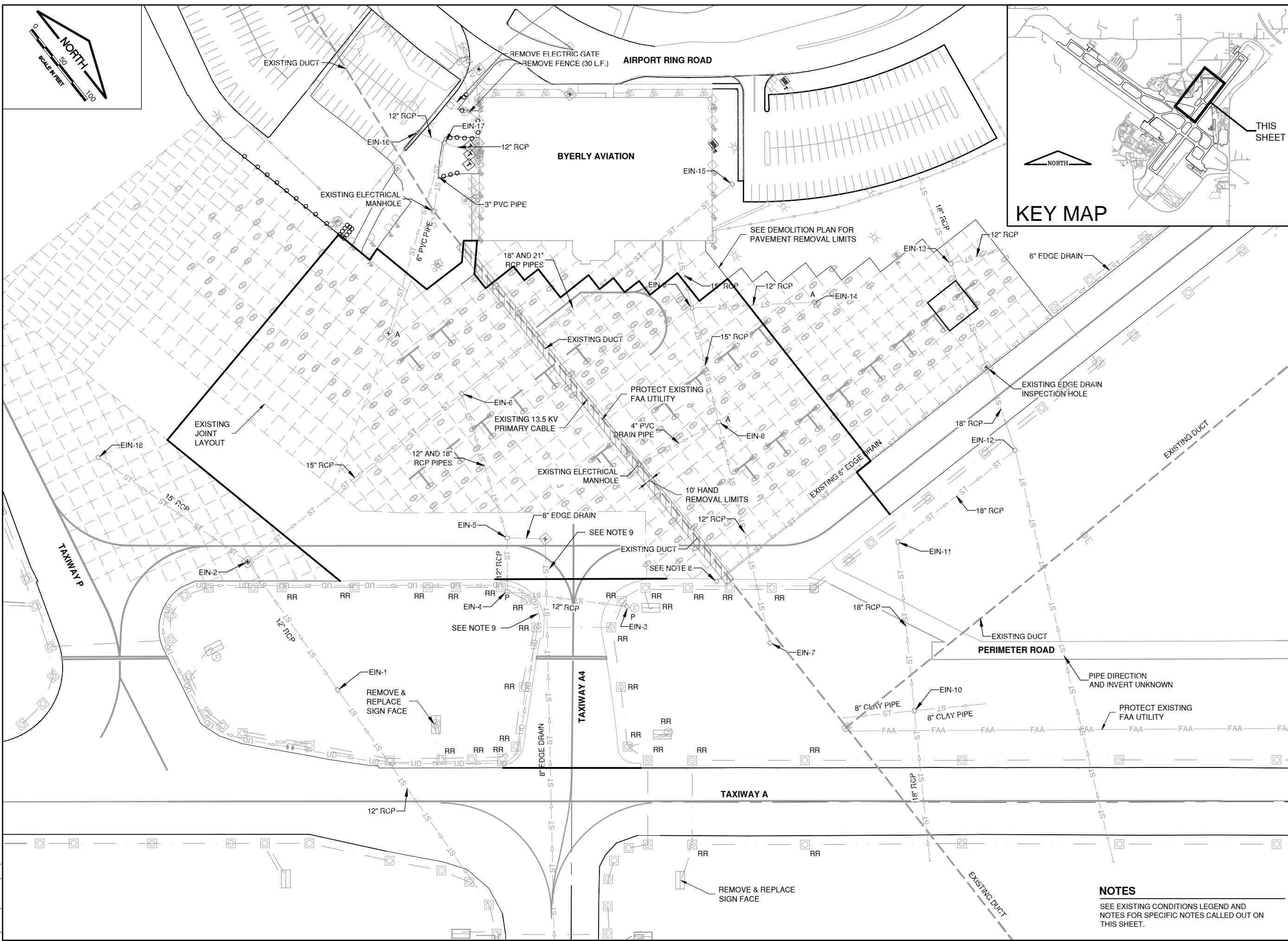
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 KEYMAP
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 BASE_PROP_GEO

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**RECONSTRUCT GENERAL AVIATION APRON
 EXISTING CONDITIONS AND REMOVALS**

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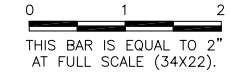
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NOTES
 SEE EXISTING CONDITIONS LEGEND AND
 NOTES FOR SPECIFIC NOTES CALLED OUT ON
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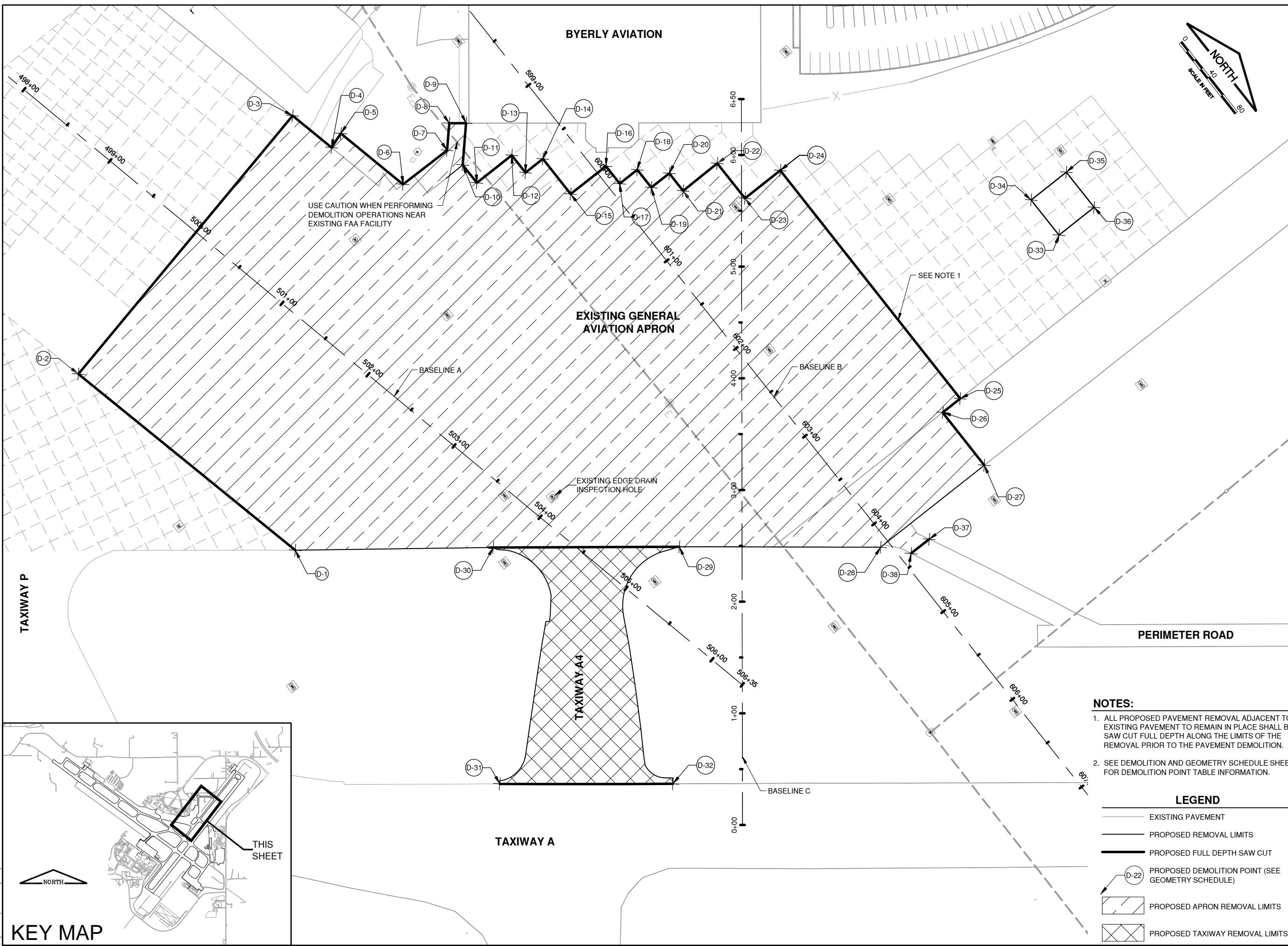
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**RECONSTRUCT GENERAL AVIATION APRON
 DEMOLITION PLAN**

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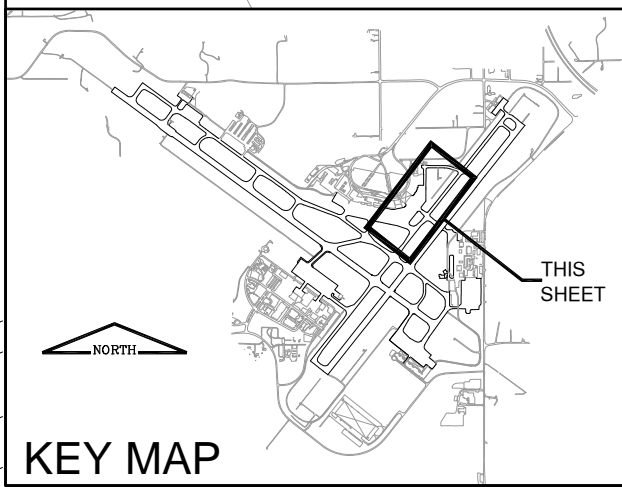
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- NOTES:**
- ALL PROPOSED PAVEMENT REMOVAL ADJACENT TO EXISTING PAVEMENT TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH ALONG THE LIMITS OF THE REMOVAL PRIOR TO THE PAVEMENT DEMOLITION.
 - SEE DEMOLITION AND GEOMETRY SCHEDULE SHEET FOR DEMOLITION POINT TABLE INFORMATION.

LEGEND

	EXISTING PAVEMENT
	PROPOSED REMOVAL LIMITS
	PROPOSED FULL DEPTH SAW CUT
	PROPOSED DEMOLITION POINT (SEE GEOMETRY SCHEDULE)
	PROPOSED APRON REMOVAL LIMITS
	PROPOSED TAXIWAY REMOVAL LIMITS



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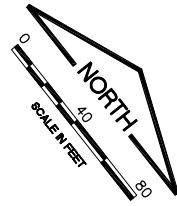
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BYERLY AVIATION

EXISTING GENERAL AVIATION APRON

REMOVE & REPLACE PANELS

LEGEND

- EXISTING PAVEMENT
- PROPOSED 9" PCC PAVEMENT (501)
 PROPOSED 12" CRUSHED AGGREGATE BASE COURSE (209)
- PROPOSED 15" PCC PAVEMENT (501)
 PROPOSED 12" CRUSHED AGGREGATE BASE COURSE (209)
- PROPOSED SELECT PAVEMENT REPAIRS
- PROPOSED 2" BITUMINOUS SURFACE COURSE (401)
 PROPOSED 4" BITUMINOUS BASE COURSE (403)
 PROPOSED 12" CRUSHED AGGREGATE BASE COURSE (209)
- PROPOSED PCC PANEL REMOVAL AND REPLACEMENT
 PROPOSED 9" PCC PAVEMENT (501)
 PROPOSED 12" CRUSHED AGGREGATE BASE COURSE (209)

NOTES:

- TIE DOWNS WITHIN THE LIMITS OF THE PANELS TO BE REMOVED AND REPLACED SHALL BE REPLACED IN THEIR ORIGINAL LOCATION. LAYOUT FOR THESE TIE DOWNS ARE NOT SHOWN ON THE TIE DOWN AND GROUNDING PLAN.
- PANELS TO BE REMOVED AND REPLACED SHALL BE REINFORCED AND DOWELED TO EXISTING PAVEMENT AND ALL NEW JOINTS SHALL BE DOWELED. JOINT SPACING SHALL MATCH THE EXISTING.
- SELECT PAVEMENT REPAIRS SHALL INCLUDE CLEANING AND SEALING CRACKS AND FULL DEPTH PATCHING FOR JOINT AND CORNER SPALLS. AREAS TO RECEIVE THE SELECT PAVEMENT REPAIRS SHALL BE IDENTIFIED BY THE RESIDENT ENGINEER WITHIN THE LIMITS OF THE ESTIMATED QUANTITY. ADDITIONAL REPAIRS EXCEEDING PLAN QUANTITIES SHALL NOT BE COMPLETED WITHOUT THE WRITTEN APPROVAL FROM THE DIVISION OF AERONAUTICS.
- THE CONTRACTOR SHALL REMOVE A 20' SECTION OF THE PERIMETER ROAD FOR CONSTRUCTION OF THE PCC APRON. THE PERIMETER ROAD TO REMAIN IN PLACE SHALL BE PROTECTED DURING CONSTRUCTION. THE 20' SECTION SHALL BE RECONSTRUCTED AFTER THE CONSTRUCTION OF THE PCC APRON.

SEE NOTE 4

EXISTING TAXIWAY A4
 TO BE REMOVED
 (ADDITIVE ALTERNATE)

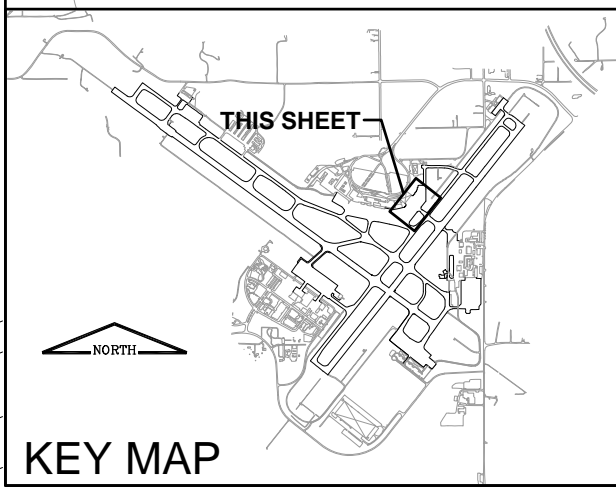
TAXIWAY A4

PROPOSED TAXIWAY A4

PROPOSED TAXIWAY A4
 (ADDITIVE ALTERNATE)

TAXIWAY A

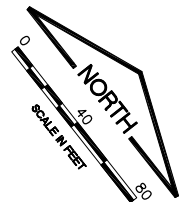
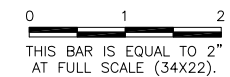
TAXIWAY P



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BYERLY AVIATION

EXISTING GENERAL AVIATION APRON

LEGEND

- EXISTING PAVEMENT
- PROPOSED GEOMETRY
- G-22 PROPOSED GEOMETRY POINT
- R-1 PROPOSED RADIUS POINT

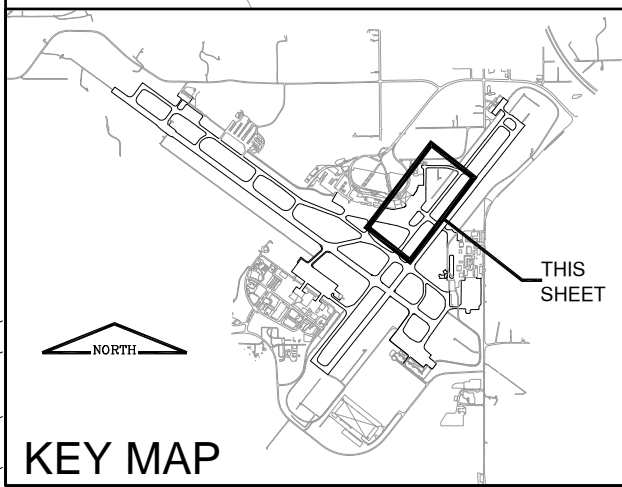
PERIMETER ROAD

TAXIWAY A4

PROPOSED TAXIWAY A4

TAXIWAY A


TAXIWAY P



KEY MAP

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RECONSTRUCT GENERAL AVIATION APRON
 PROPOSED GEOMETRY

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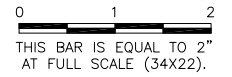
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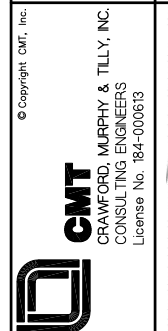
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POINT	BASELINE	STATION	OFFSET	NORTHING	EASTING	NOTES
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D-2	BASELINE A	500+00.0	165.0' R	1456333.49	2429489.01	DEMOLITION POINT
D-3	BASELINE A	500+00.0	135.5' L	1456627.50	2429426.91	DEMOLITION POINT
D-4	BASELINE A	500+45.0	135.8' L	1456637.08	2429470.88	DEMOLITION POINT
D-5	BASELINE A	500+43.4	150.8' L	1456651.42	2429466.23	DEMOLITION POINT
D-6	BASELINE A	501+15.3	151.2' L	1456666.75	2429536.51	DEMOLITION POINT
D-7	BASELINE A	501+26.3	199.9' L	1456716.59	2429537.13	DEMOLITION POINT
D-8	BASELINE A	501+12.3	219.8' L	1456733.16	2429519.36	DEMOLITION POINT
D-9	BASELINE B	598+92.0	64.7' R	1456745.03	2429528.63	DEMOLITION POINT
D-10	BASELINE B	599+19.6	90.2' R	1456719.42	2429556.10	DEMOLITION POINT
D-11	BASELINE B	599+40.0	90.2' R	1456719.38	2429576.52	DEMOLITION POINT
D-12	BASELINE B	599+39.9	50.4' R	1456759.18	2429576.60	DEMOLITION POINT
D-13	BASELINE B	599+59.7	50.4' R	1456759.13	2429596.41	DEMOLITION POINT
D-14	BASELINE B	599+59.7	30.5' R	1456779.00	2429596.42	DEMOLITION POINT
D-15	BASELINE B	599+99.8	30.5' R	1456778.91	2429636.51	DEMOLITION POINT
D-16	BASELINE B	599+99.8	9.8' L	1456819.20	2429636.60	DEMOLITION POINT
D-17	BASELINE B	600+19.7	9.8' L	1456819.13	2429656.57	DEMOLITION POINT
D-18	BASELINE B	600+19.7	29.8' L	1456839.16	2429656.60	DEMOLITION POINT
D-19	BASELINE B	600+39.9	29.8' L	1456839.10	2429676.84	DEMOLITION POINT
D-20	BASELINE B	600+39.9	50.3' L	1456859.64	2429676.88	DEMOLITION POINT
D-21	BASELINE B	600+59.9	50.3' L	1456859.57	2429696.91	DEMOLITION POINT
D-22	BASELINE B	600+59.5	89.6' L	1456898.83	2429696.60	DEMOLITION POINT
D-23	BASELINE B	600+99.5	89.8' L	1456898.86	2429736.62	DEMOLITION POINT
D-24	BASELINE B	600+99.4	130.0' L	1456939.11	2429736.64	DEMOLITION POINT
D-25	BASELINE B	603+59.6	130.0' L	1456938.32	2429996.79	DEMOLITION POINT
D-26	BASELINE B	603+59.7	110.1' L	1456918.45	2429996.81	DEMOLITION POINT
D-27	BASELINE B	604+19.8	110.0' L	1456918.14	2430056.99	DEMOLITION POINT
D-28	BASELINE B	604+20.0	8.3' R	1456799.85	2430056.80	DEMOLITION POINT
D-29	BASELINE C	2+49.2	56.2' L	1456658.86	2429944.51	DEMOLITION POINT
D-30	BASELINE C	2+48.8	222.8' L	1456527.89	2429841.51	DEMOLITION POINT
D-31	BASELINE C	0+37.2	217.3' L	1456400.97	2430010.98	DEMOLITION POINT
D-32	BASELINE C	0+37.0	62.3' L	1456522.44	2430107.23	DEMOLITION POINT
D-33	BASELINE B	602+99.4	290.1' L	1457098.57	2429937.07	DEMOLITION POINT
D-34	BASELINE B	602+59.2	290.0' L	1457098.62	2429896.94	DEMOLITION POINT
D-35	BASELINE B	602+59.2	330.1' L	1457138.73	2429896.97	DEMOLITION POINT
D-36	BASELINE B	602+99.3	330.2' L	1457138.73	2429937.09	DEMOLITION POINT
D-37	BASELINE B	604+41.3	30.3' L	1456838.36	2430078.24	DEMOLITION POINT
D-38	BASELINE B	604+41.4	9.6' L	1456817.65	2430078.20	DEMOLITION POINT

GEOMETRY POINTS						
POINT	BASELINE	STATION	OFFSET	NORTHING	EASTING	NOTES
G-1	BASELINE A	502+50.2	162.6' R	1456387.57	2429733.27	GEOMETRY POINT
G-2	BASELINE A	500+00.0	165.0' R	1456333.49	2429489.01	GEOMETRY POINT
G-3	BASELINE A	500+00.0	135.5' L	1456627.50	2429426.91	GEOMETRY POINT
G-4	BASELINE B	598+34.6	172.8' R	1456637.08	2429470.88	GEOMETRY POINT
G-5	BASELINE B	598+29.9	158.5' R	1456651.42	2429466.23	GEOMETRY POINT
G-6	BASELINE A	501+33.0	136.4' L	1456655.83	2429556.85	GEOMETRY POINT
G-7	BASELINE B	599+00.1	143.0' R	1456666.75	2429536.51	GEOMETRY POINT
G-8	BASELINE B	598+82.8	76.6' R	1456733.16	2429519.36	GEOMETRY POINT
G-9	BASELINE B	599+00.6	93.1' R	1456716.59	2429537.13	GEOMETRY POINT
G-10	BASELINE B	599+19.8	150.8' R	1456658.83	2429556.20	GEOMETRY POINT
G-11	BASELINE B	599+19.8	109.3' R	1456700.38	2429556.27	GEOMETRY POINT
G-12	BASELINE B	598+92.0	64.7' R	1456745.03	2429528.63	GEOMETRY POINT
G-13	BASELINE B	599+19.6	90.2' R	1456719.42	2429556.10	GEOMETRY POINT
G-14	BASELINE B	599+40.1	109.3' R	1456700.30	2429576.62	GEOMETRY POINT
G-15	BASELINE B	599+40.1	90.2' R	1456719.38	2429576.66	GEOMETRY POINT
G-16	BASELINE B	599+59.8	90.2' R	1456719.34	2429596.38	GEOMETRY POINT
G-17	BASELINE B	599+39.9	50.4' R	1456759.18	2429576.60	GEOMETRY POINT
G-18	BASELINE B	599+59.7	50.4' R	1456759.13	2429596.41	GEOMETRY POINT
G-19	BASELINE B	599+59.7	30.5' R	1456779.00	2429596.42	GEOMETRY POINT
G-20	BASELINE B	600+00.1	50.4' R	1456759.02	2429636.73	GEOMETRY POINT
G-21	BASELINE B	600+00.0	30.5' R	1456778.91	2429636.76	GEOMETRY POINT
G-22	BASELINE B	600+19.8	30.5' R	1456778.87	2429656.51	GEOMETRY POINT
G-23	BASELINE B	599+99.7	9.8' L	1456819.20	2429636.56	GEOMETRY POINT
G-24	BASELINE B	600+19.7	9.8' L	1456819.13	2429656.57	GEOMETRY POINT
G-25	BASELINE B	600+40.0	9.8' L	1456819.06	2429676.80	GEOMETRY POINT
G-26	BASELINE B	600+19.7	29.8' L	1456839.16	2429656.60	GEOMETRY POINT
G-27	BASELINE B	600+39.9	29.8' L	1456839.10	2429676.84	GEOMETRY POINT
G-28	BASELINE B	600+39.9	50.3' L	1456859.64	2429676.88	GEOMETRY POINT
G-29	BASELINE B	600+79.5	29.8' L	1456838.98	2429716.43	GEOMETRY POINT
G-30	BASELINE B	600+59.7	50.3' L	1456859.57	2429696.70	GEOMETRY POINT
G-31	BASELINE B	600+79.4	69.7' L	1456878.92	2429716.44	GEOMETRY POINT
G-32	BASELINE B	600+59.7	89.6' L	1456898.83	2429696.74	GEOMETRY POINT
G-33	BASELINE B	601+19.7	69.8' L	1456878.85	2429756.71	GEOMETRY POINT
G-34	BASELINE B	600+99.5	89.8' L	1456898.86	2429736.62	GEOMETRY POINT
G-35	BASELINE B	600+99.4	130.0' L	1456939.11	2429736.64	GEOMETRY POINT
G-36	BASELINE B	601+19.5	130.0' L	1456939.05	2429756.75	GEOMETRY POINT
G-37	BASELINE B	603+59.6	130.0' L	1456938.32	2429996.79	GEOMETRY POINT
G-38	BASELINE B	603+59.7	110.0' L	1456918.32	2429996.81	GEOMETRY POINT
G-39	BASELINE B	604+19.8	110.0' L	1456918.14	2430056.99	GEOMETRY POINT
G-40	BASELINE B	604+20.0	11.3' R	1456796.85	2430056.79	GEOMETRY POINT
G-41	BASELINE B	599+37.8	150.0' R	1456659.61	2429574.17	BORDER BETWEEN 9" AND 15" PCC
G-42	BASELINE B	603+10.6	150.0' R	1456658.47	2429947.00	BORDER BETWEEN 9" AND 15" PCC
G-43	BASELINE C	2+46.7	90.2' R	1456772.12	2430037.24	GEOMETRY POINT/ CURVE PC/PT
G-44	BASELINE C	2+46.7	90.2' L	1456630.67	2429925.43	GEOMETRY POINT/ CURVE PC/PT
G-45	BASELINE C	2+43.9	79.1' L	1456637.62	2429934.45	GEOMETRY POINT
G-46	BASELINE C	2+07.6	42.7' L	1456643.60	2429985.55	CURVE PC/PT
G-47	BASELINE C	1+56.7	30.0' L	1456622.02	2430033.37	CURVE PC/PT
G-48	BASELINE C	1+29.0	30.0' L	1456604.84	2430055.11	GEOMETRY POINT
G-49	BASELINE C	0+78.1	42.7' L	1456563.29	2430087.15	CURVE PC/PT
G-50	BASELINE C	0+41.7	79.1' L	1456512.19	2430093.13	CURVE PC/PT

GEOMETRY POINTS						
POINT	BASELINE	STATION	OFFSET	NORTHING	EASTING	NOTES
G-51	BASELINE C	0+36.8	98.6' L	1456493.84	2430084.85	GEOMETRY POINT
G-52	BASELINE C	0+37.2	97.0' R	1456647.54	2430205.81	GEOMETRY POINT
G-53	BASELINE C	0+41.7	79.1' R	1456636.31	2430191.24	CURVE PC/PT
G-54	BASELINE C	0+78.1	42.7' R	1456630.33	2430140.14	CURVE PC/PT
G-55	BASELINE C	1+29.0	30.0' R	1456651.91	2430092.32	GEOMETRY POINT
G-56	BASELINE C	1+56.7	30.0' R	1456669.09	2430070.58	GEOMETRY POINT
G-57	BASELINE C	2+07.6	42.7' R	1456710.64	2430038.54	CURVE PC/PT
G-58	BASELINE C	2+43.9	79.1' R	1456761.74	2430032.56	CURVE PC/PT
G-59	BASELINE B	602+59.2	330.1' L	1457138.73	2429937.07	GEOMETRY POINT
G-60	BASELINE B	602+99.3	330.2' L	1457138.73	2429937.09	GEOMETRY POINT
G-61	BASELINE B	602+99.4	290.1' L	1457098.57	2429937.07	GEOMETRY POINT
G-62	BASELINE B	602+59.2	290.0' L	1457098.62	2429896.94	GEOMETRY POINT
G-63	BASELINE B	604+41.4	9.6' L	1456817.65	2430078.20	GEOMETRY POINT
G-64	BASELINE B	604+41.3	30.2' L	1456838.30	2430078.24	GEOMETRY POINT
R-1	BASELINE C	1+95.4	91.2' R	1456741.17	2430078.13	RADIUS POINT
R-2	BASELINE C	1+95.4	91.2' L	1456598.03	2429964.98	RADIUS POINT
R-3	BASELINE C	0+90.2	91.2' R	1456675.90	2430160.70	RADIUS POINT
R-4	BASELINE C	0+90.2	91.2' L	1456532.76	2430047.55	RADIUS POINT
R-5	BASELINE B	604+35.0	9.9' R	1456798.17	2430071.80	RADIUS POINT
R-6	BASELINE B	604+34.9	43.8' L	1456851.89	2430071.89	RADIUS POINT

**GENERAL WAYNE A. DOWNING
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 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 DEMOLITION AND GEOMETRY SCHEDULE**

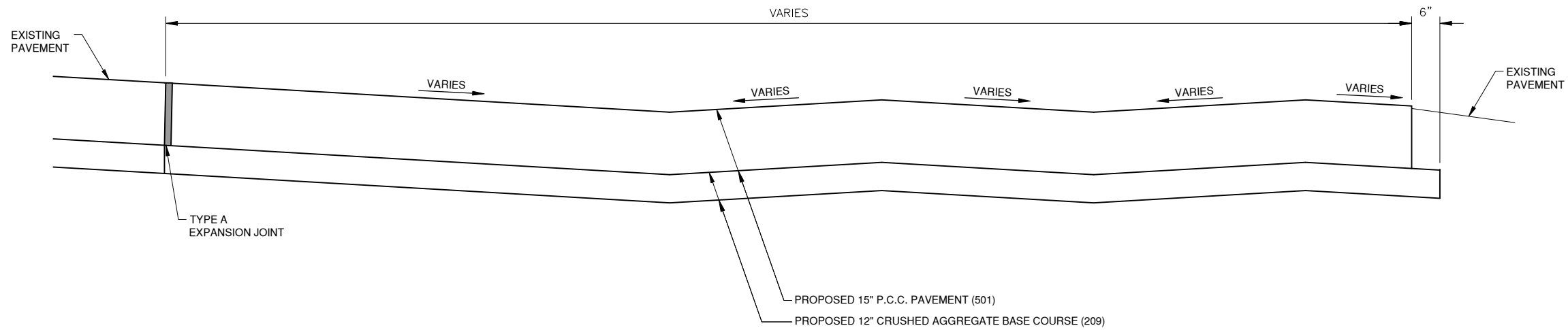


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JOB No:	12061-03
IL. PROJ. NO. PIA-4361 AIP PROJ. NO. 3-17-0080-XX	
SHEET 16 OF 56 SHEETS	

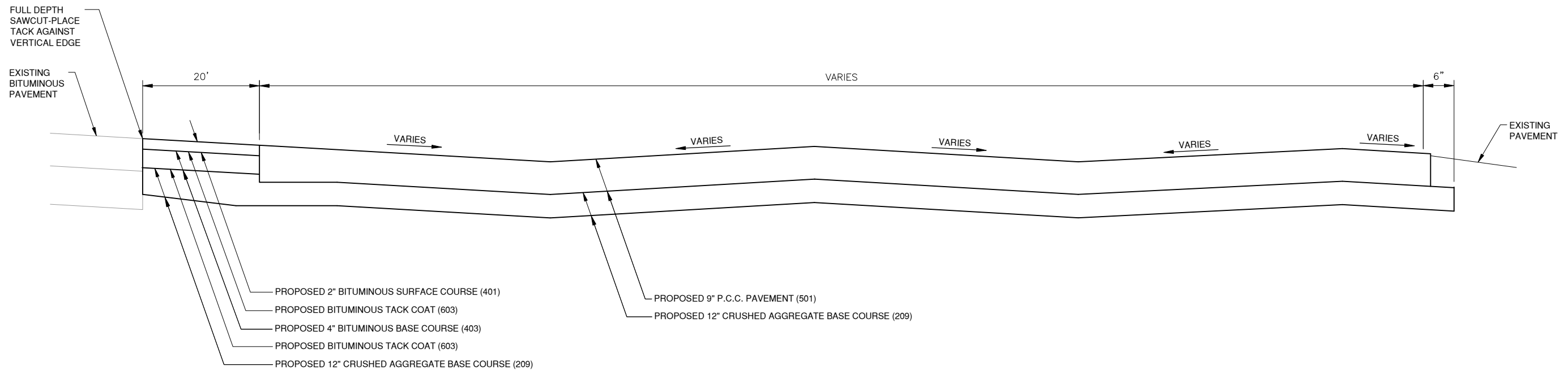
PE096

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



A PROPOSED 15" APRON PAVEMENT **A**
 N.T.S.




B PROPOSED 9" APRON PAVEMENT **B**
 N.T.S.

NOTES:

- CONSTRUCTION OF THE 4" BITUMINOUS BASE COURSE SHALL BE PLACED IN 2" LIFTS.
- BITUMINOUS TACK COAT SHALL BE PLACED BETWEEN ALL LIFTS.
- SEE JOINTING PLAN FOR JOINT SPACING AND JOINT TYPES.

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 PEORIA, ILLINOIS**
**RECONSTRUCT GENERAL AVIATION APRON
 TYPICAL SECTIONS 1**

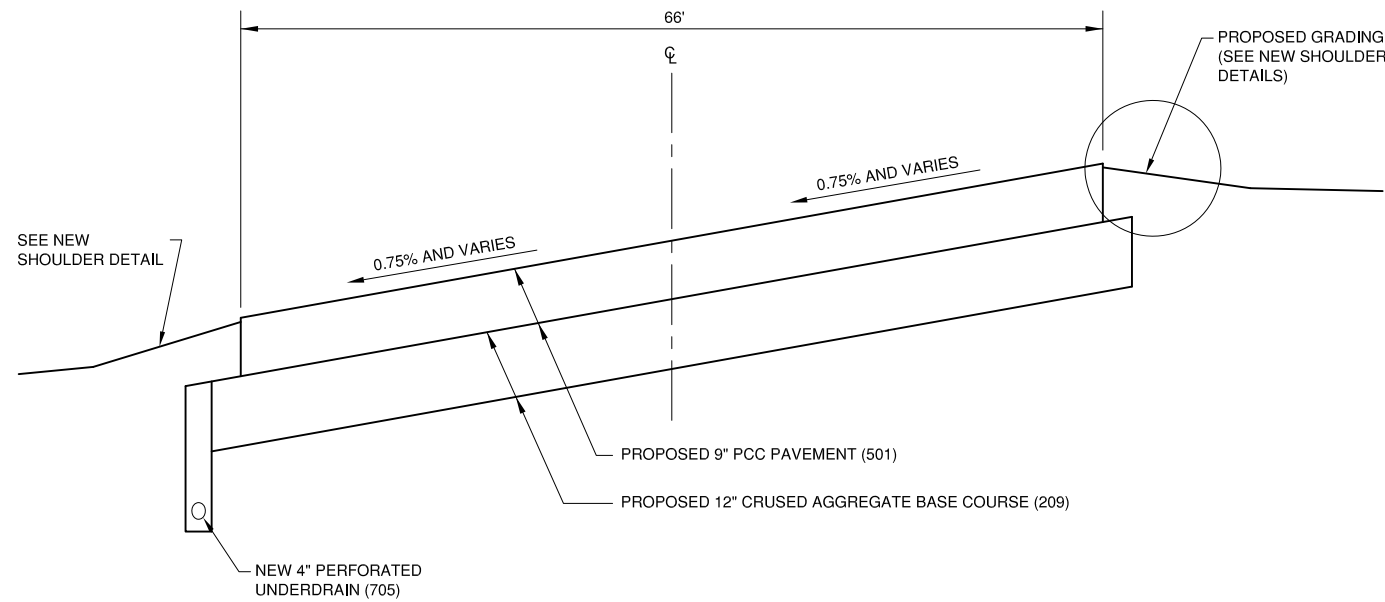
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SHEET 17 OF 56 SHEETS	

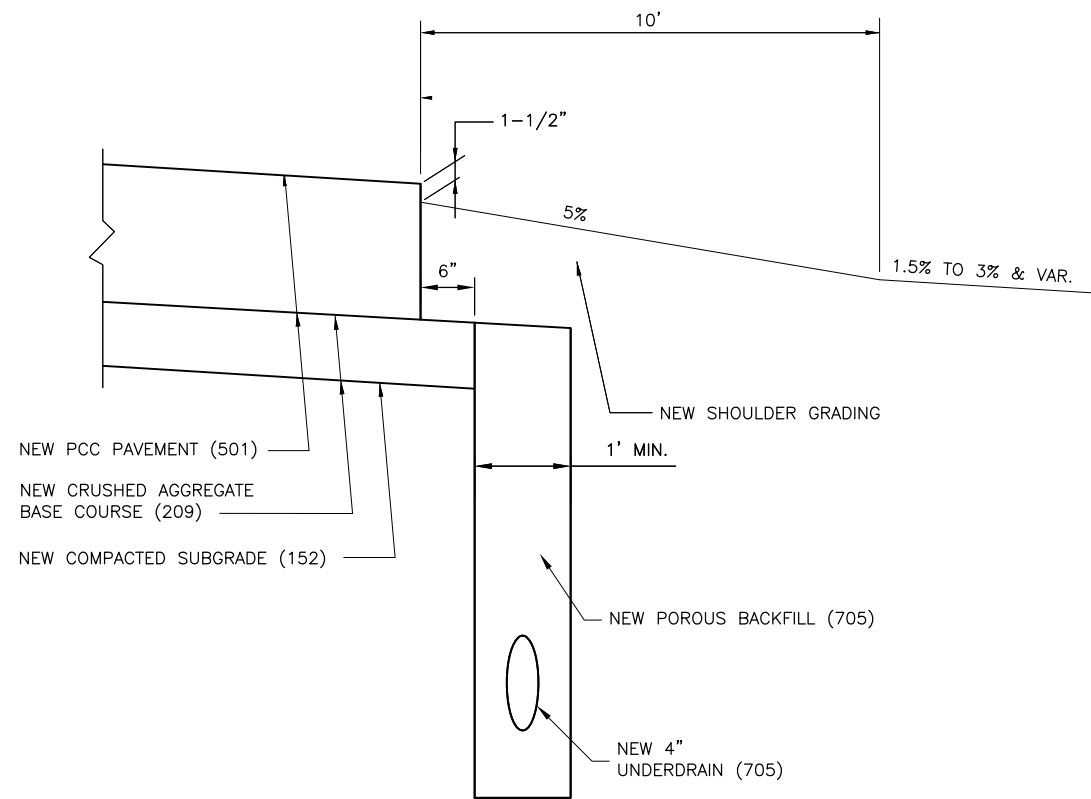
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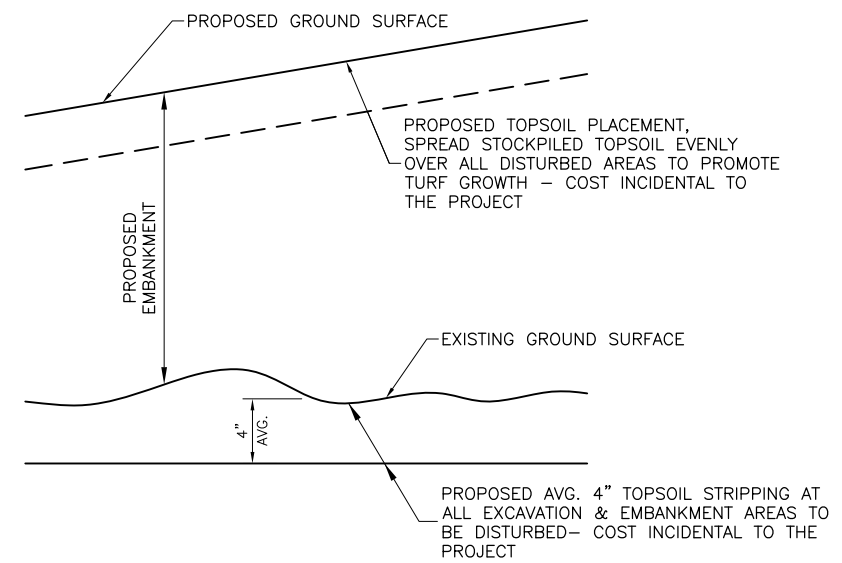
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



C PROPOSED TAXIWAY A4 **C**
 N.T.S.



NEW SHOULDER DETAIL "A"
 N.T.S.



TOPSOIL STRIPPING DETAIL
 N.T.S.

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RECONSTRUCT GENERAL AVIATION APRON

TYPICAL SECTIONS 2

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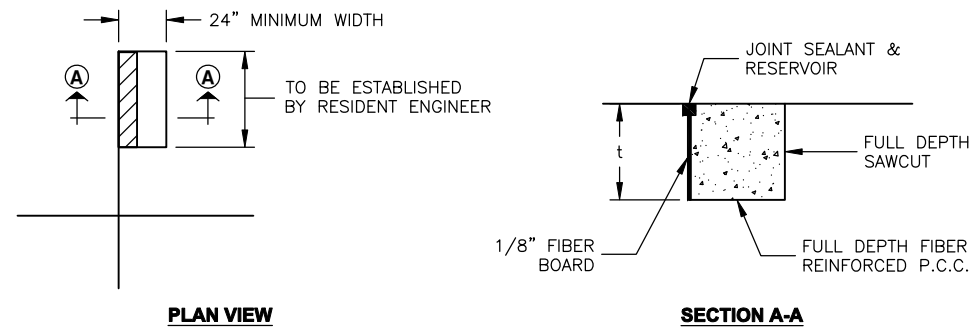
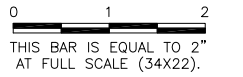
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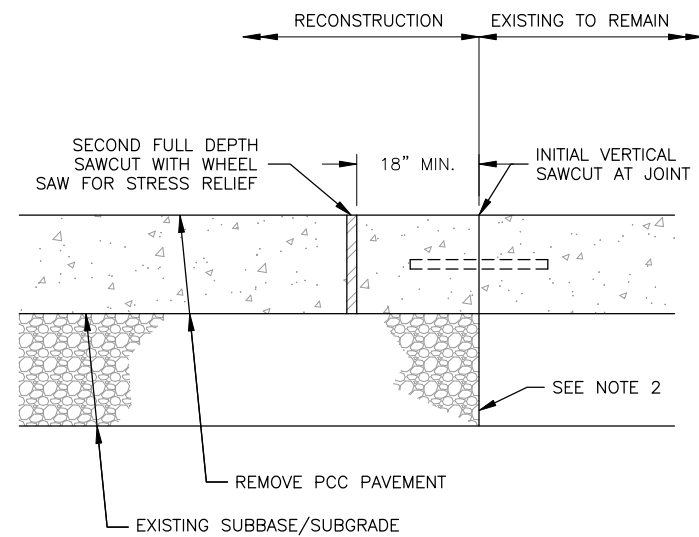
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SHEET	18 OF 56 SHEETS

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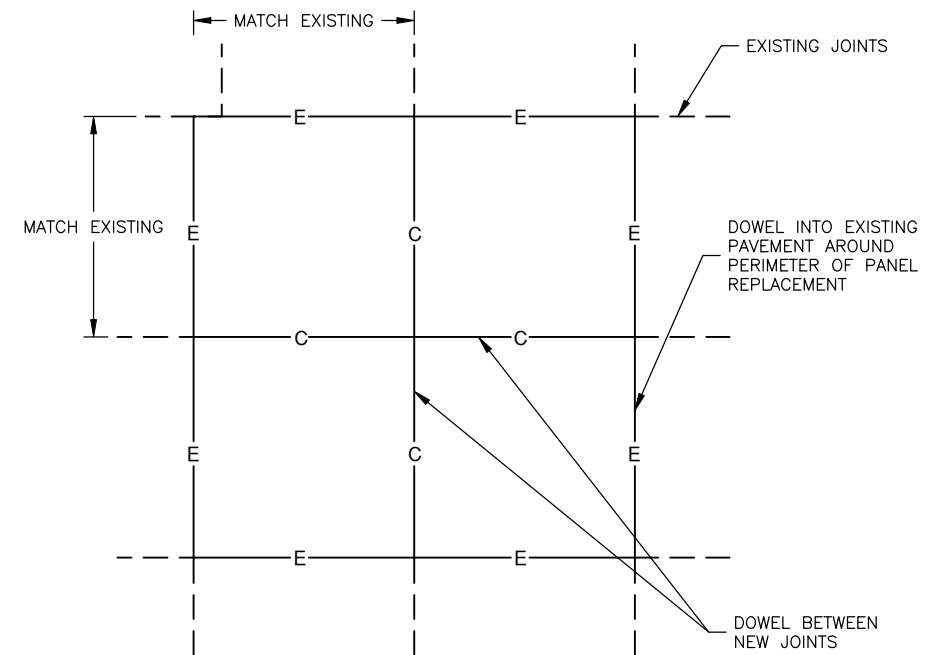
FULL DEPTH P.C.C. PATCHING
 N.T.S.



PCC PAVEMENT REMOVAL DETAIL
 N.T.S.

NOTES:

1. TWO VERTICAL SAWCUTS SHALL BE MADE AS INDICATED PRIOR TO STARTING ANY PAVEMENT REMOVAL ADJACENT TO EXISTING PAVEMENTS TO REMAIN IN PLACE.
2. CONTRACTOR SHALL TAKE ACTION AS NEEDED TO PREVENT LOSS OF AGGREGATE FROM UNDERNEATH PAVEMENT TO REMAIN IN PLACE.



REMOVE AND REPLACE PANELS
 N.T.S.

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RECONSTRUCT GENERAL AVIATION APRON
TYPICAL SECTIONS 3

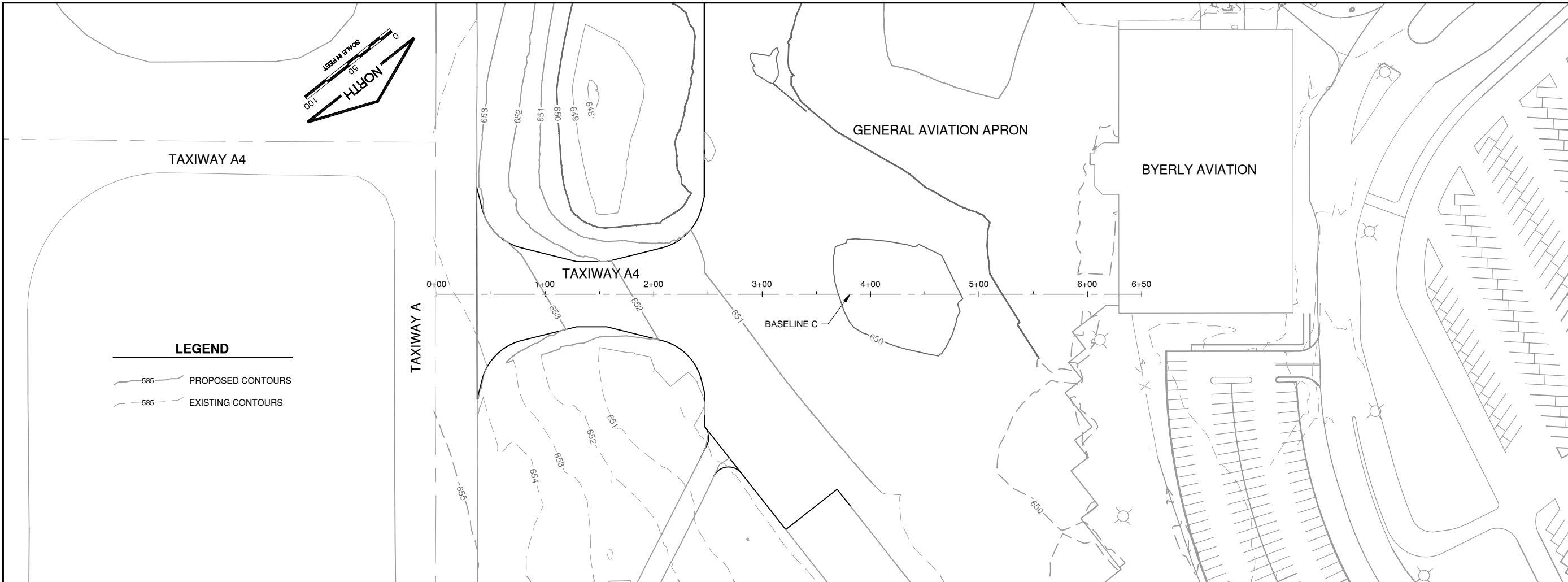
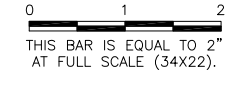
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SHEET 19 OF 56 SHEETS	

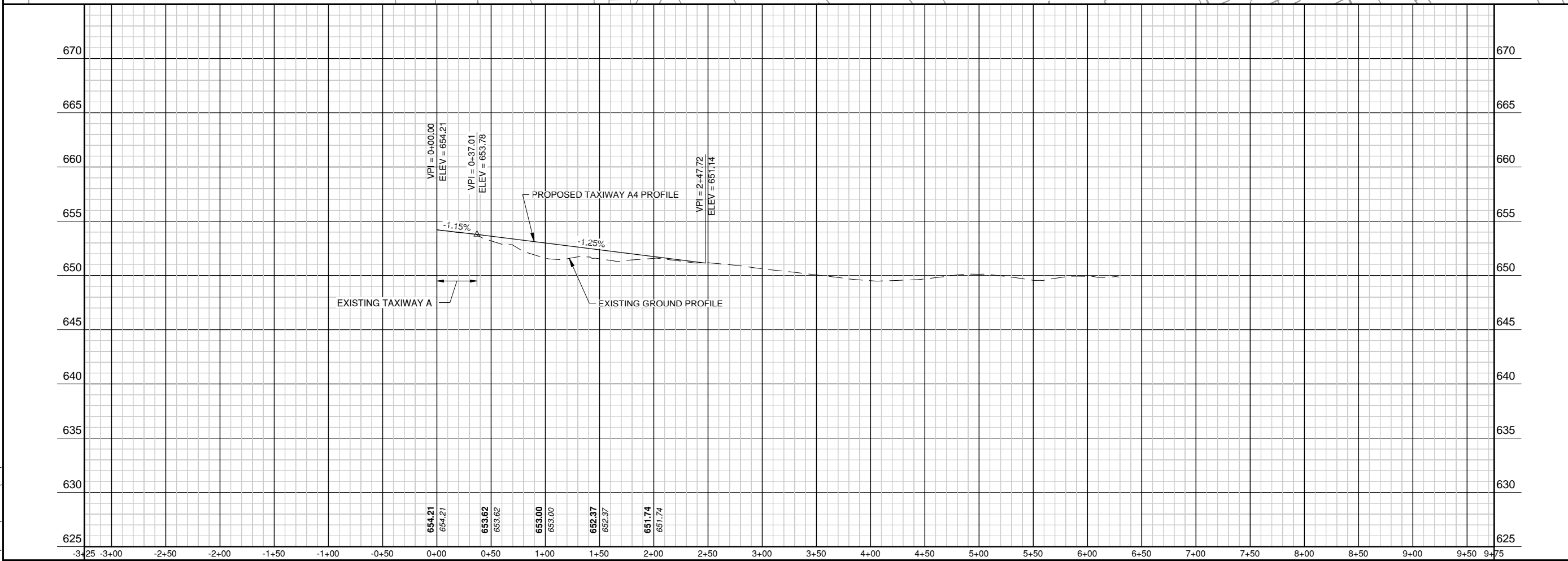
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NUMBER	BY	DATE



LEGEND


— 585 — PROPOSED CONTOURS
 - - - 585 - - - EXISTING CONTOURS




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**RECONSTRUCT GENERAL AVIATION APRON
 TAXIWAY A4 PLAN AND PROFILE**

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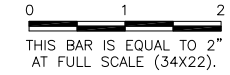


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SHEET	20 OF 56 SHEETS

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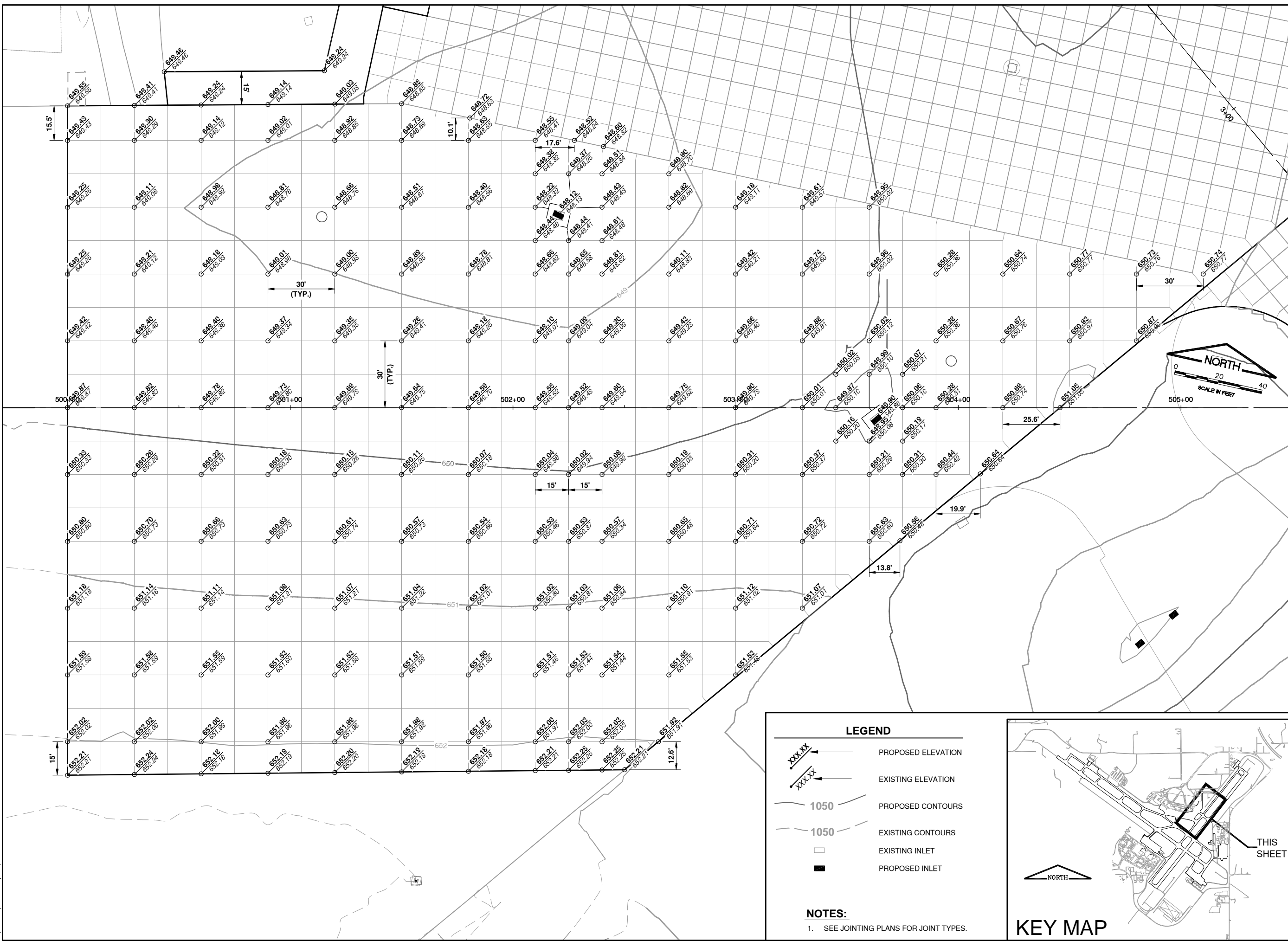


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**RECONSTRUCT GENERAL AVIATION APRON
 STAKING PLAN 1**

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SHEET	21 OF 56 SHEETS

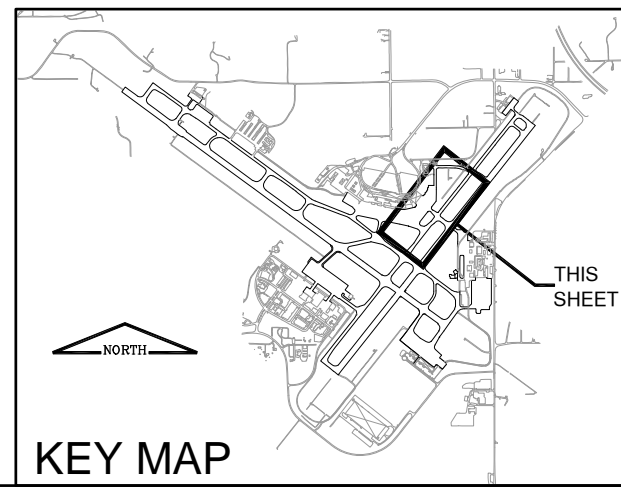


LEGEND

- PROPOSED ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS
- EXISTING CONTOURS
- EXISTING INLET
- PROPOSED INLET

NOTES:

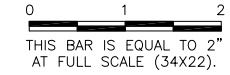
- SEE JOINTING PLANS FOR JOINT TYPES.



PE096

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NUMBER	BY	DATE

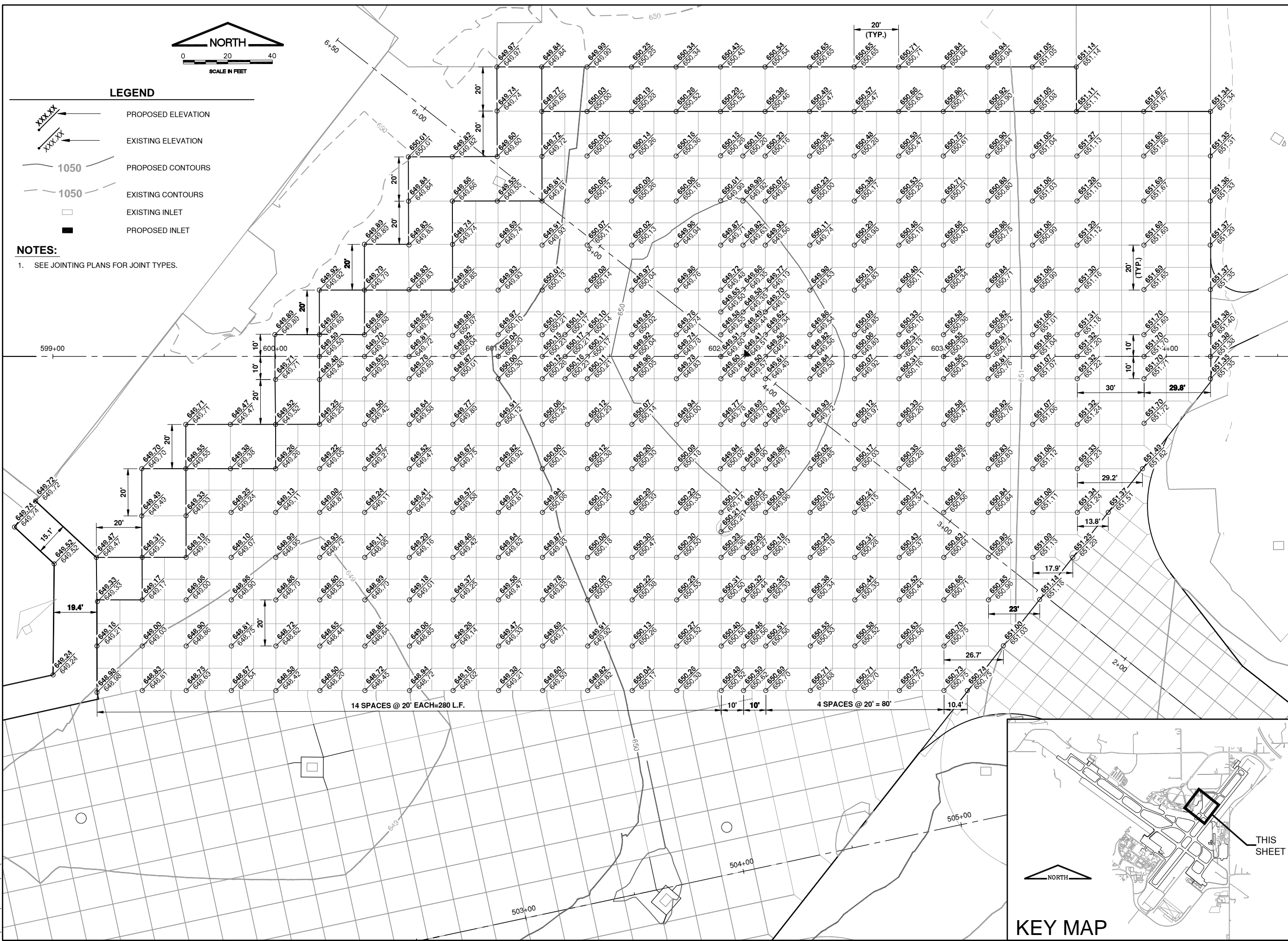


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**RECONSTRUCT GENERAL AVIATION APRON
 STAKING PLAN 2**

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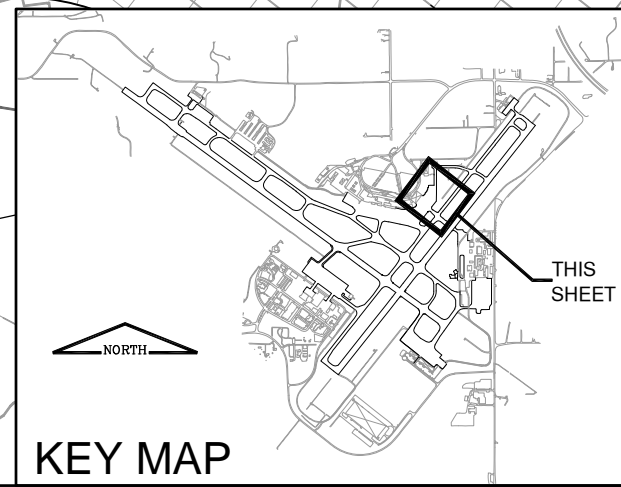
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JOB No:	12061-03
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AIP PROJ. NO. 3-17-0080-XX	
SHEET 22 OF 56 SHEETS	



LEGEND

- PROPOSED ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS
- EXISTING CONTOURS
- EXISTING INLET
- PROPOSED INLET

NOTES:
 1. SEE JOINTING PLANS FOR JOINT TYPES.

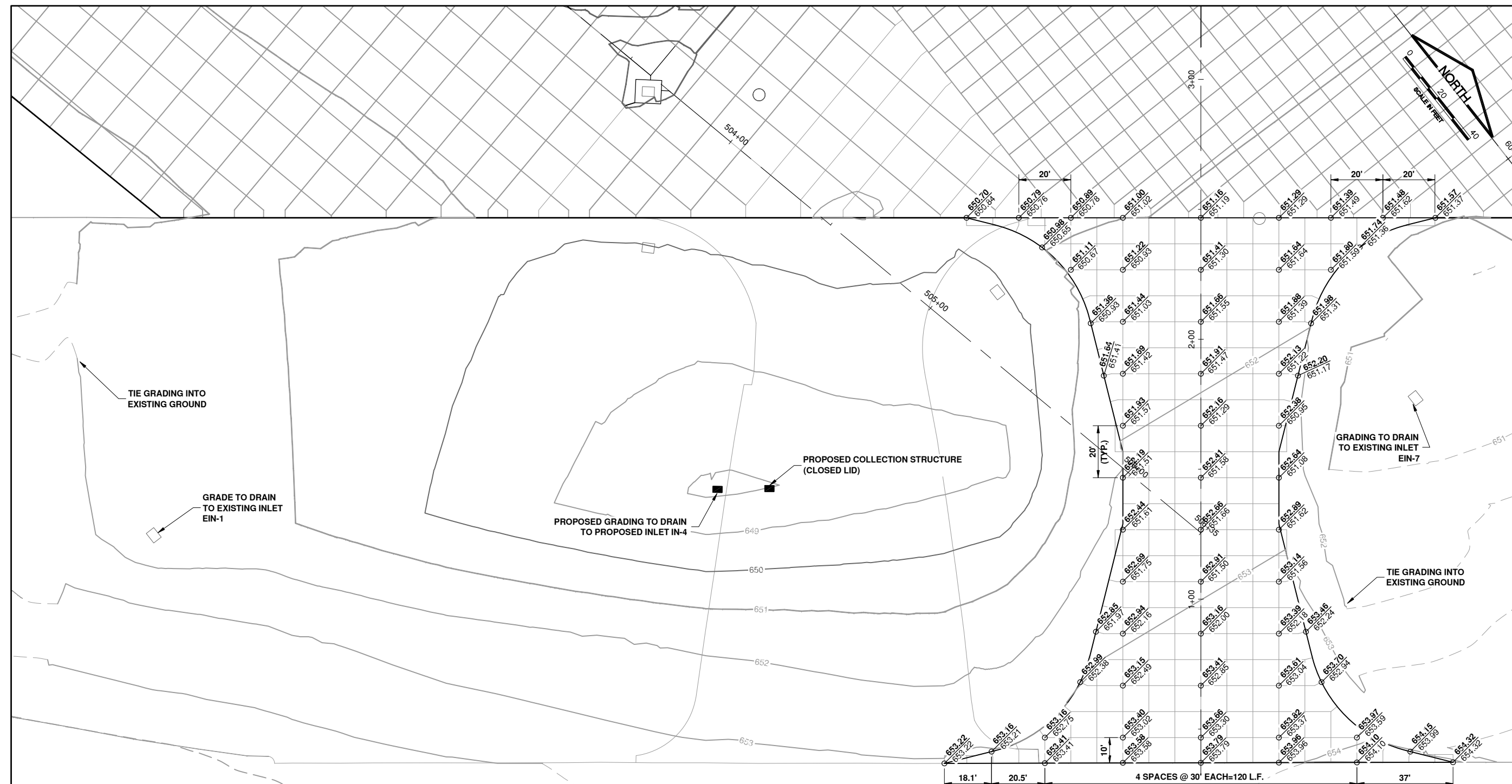
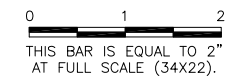


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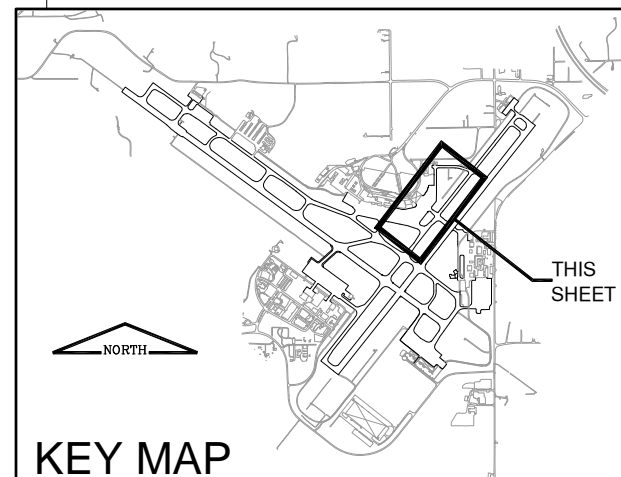
18.1' 20.5' 4 SPACES @ 30' EACH=120 L.F. 37'

LEGEND

- PROPOSED ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS
- EXISTING CONTOURS
- EXISTING INLET
- PROPOSED INLET

NOTES:

- SEE JOINTING PLANS FOR JOINT TYPES.



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RECONSTRUCT GENERAL AVIATION APRON
STAKING PLAN 3

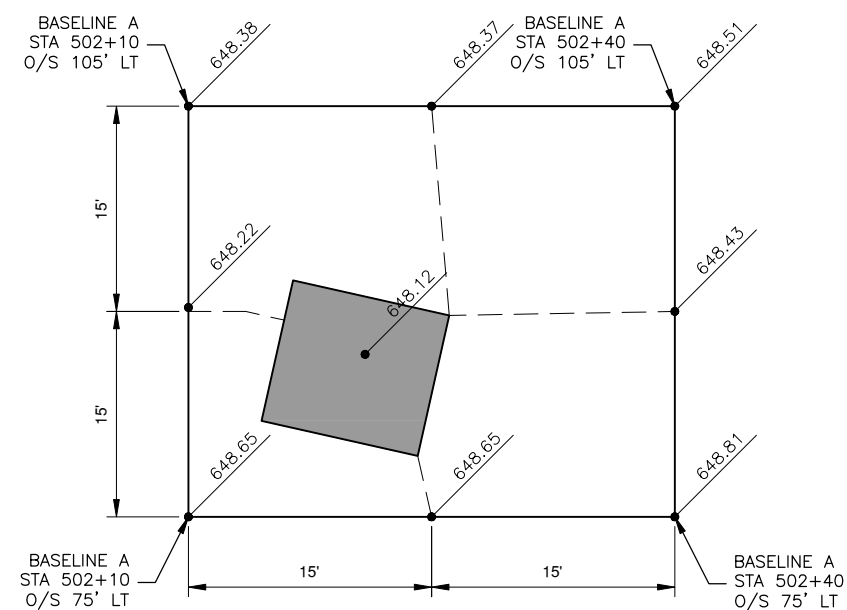
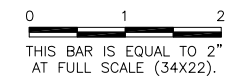
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SHEET 23 OF 56 SHEETS	

PE096

REVISIONS

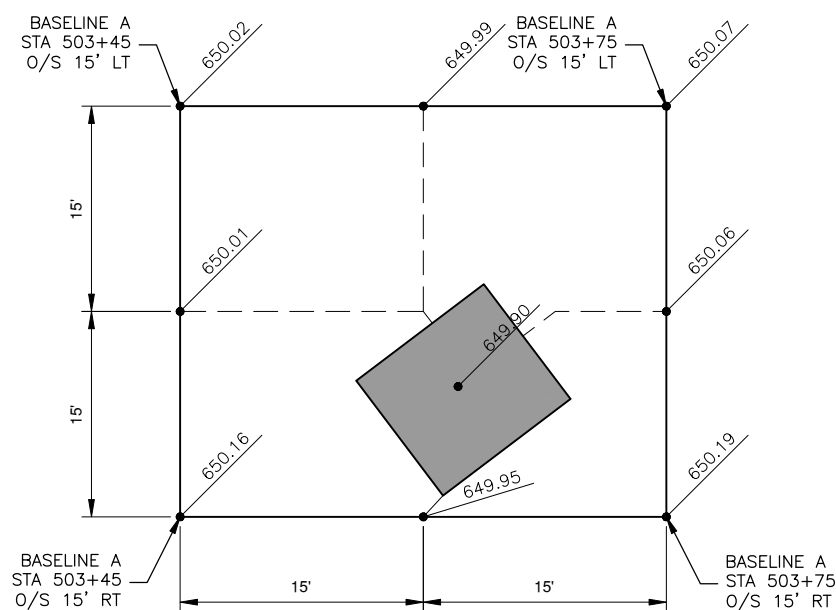
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(IN-2)

INLET STAKING PLAN

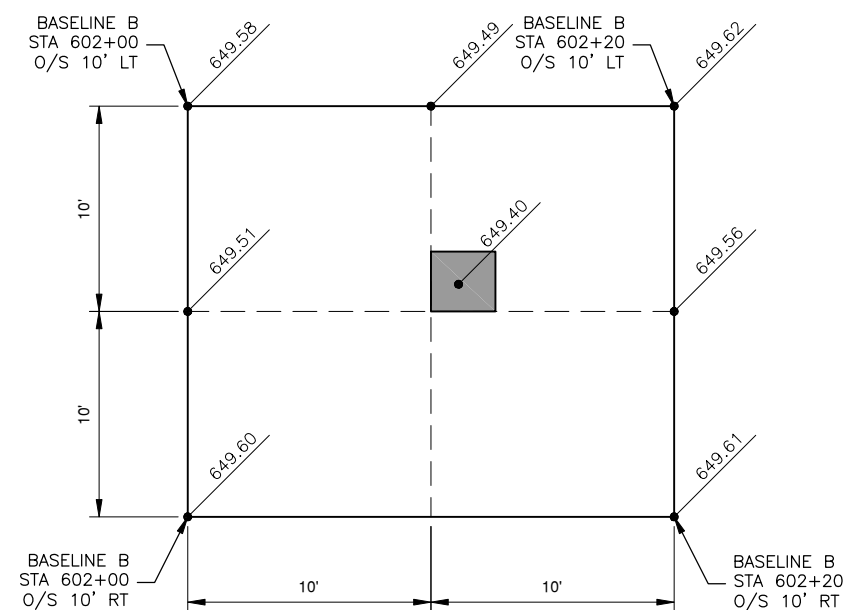
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(IN-3)

INLET STAKING PLAN

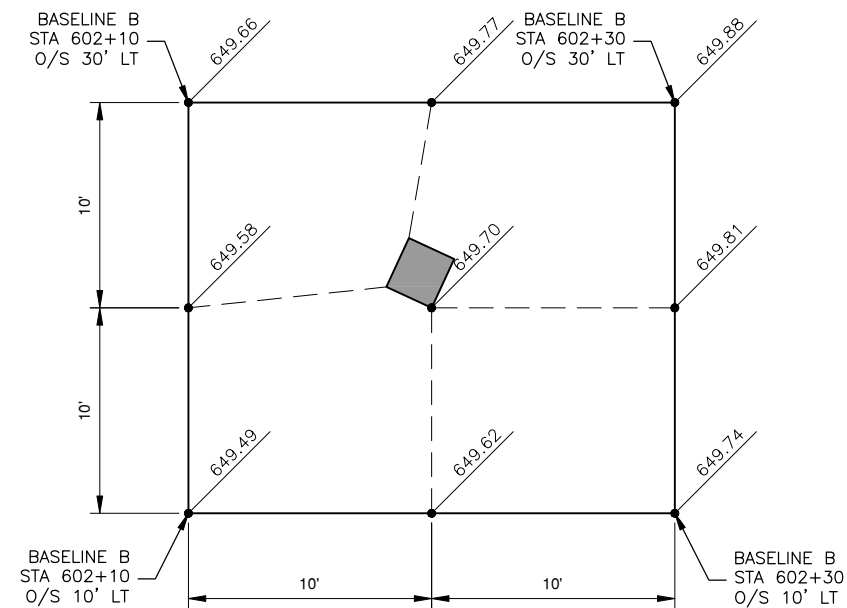
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(IN-1)

INLET STAKING PLAN

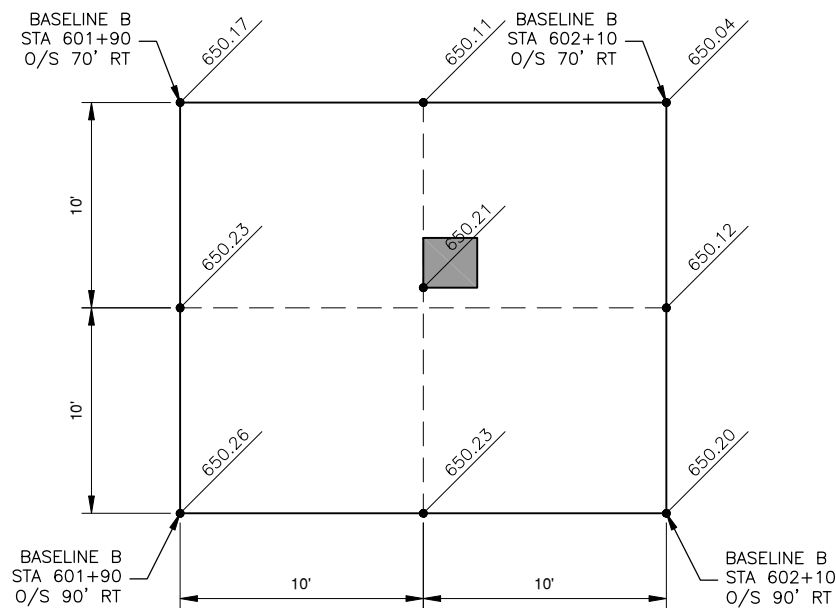
N.T.S.



(EIN-8)

INLET STAKING PLAN

N.T.S.



(EXISTING ELECTRICAL MANHOLE)

STRUCTURE STAKING PLAN

N.T.S.

NOTES:

1. IN ADDITION TO THE MESH REINFORCING IN THE PANELS AROUND PAVEMENT PENETRATIONS, THE CONTRACTOR SHALL PLACE 3 - #5 BARS OFF THE CORNERS OF ALL INLETS OR OTHER PAVEMENT PENETRATIONS AS SHOWN IN THE "PAVEMENT PENETRATION DETAIL" ON THE PAVING AND MISCELLANEOUS DETAILS SHEET.
2. THE CONTRACTOR SHALL CONSTRUCT AND SEAL AN EXPANSION JOINT AROUND THE PROPOSED AND EXISTING INLETS TO REDUCE THE POSSIBILITY OF SYMPATHETIC CRACKING.
3. THE PAVEMENT ELEVATION AROUND THE EXISTING ELECTRICAL MANHOLE SHALL MATCH THE EXISTING ELECTRICAL MANHOLE ELEVATION.

GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

RECONSTRUCT GENERAL AVIATION APRON
 INLET STAKE PLAN

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CHECKED BY: TJH

APPROVED BY: CET

DATE: APRIL 26, 2013

JOB No: 12061-03

IL PROJ. NO. PIA-4361
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SHEET 24 OF 56 SHEETS

PE096

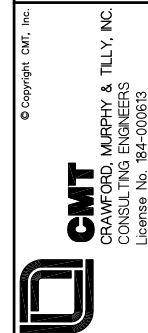
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

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**RECONSTRUCT GENERAL AVIATION APRON
 JOINTING PLAN 1**

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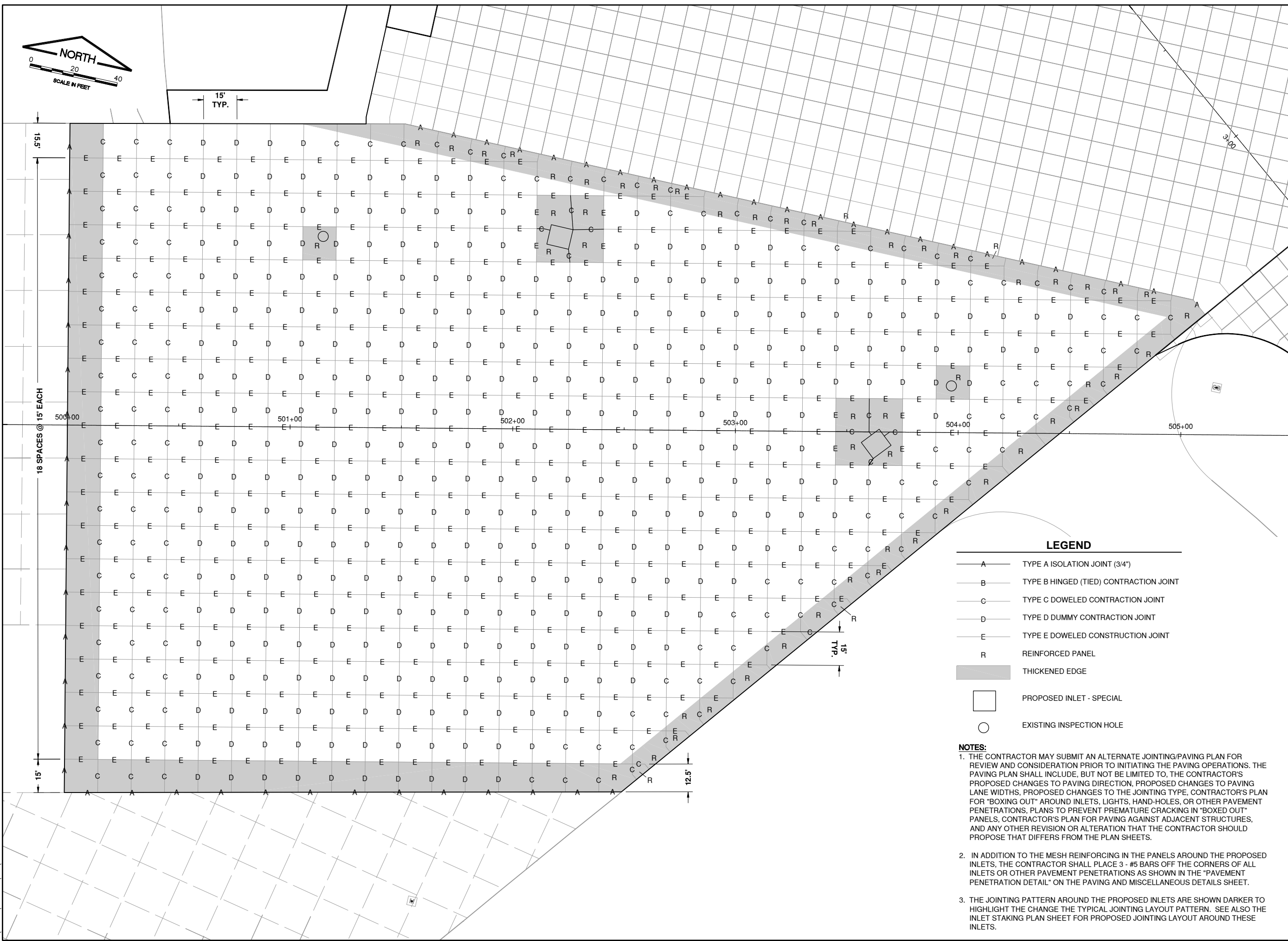
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AIP PROJ. NO.	3-17-0080-XX
SHEET	25 OF 56 SHEETS



15'
 TYP.

15'
 18 SPACES @ 15' EACH
 500+00
 501+00
 502+00
 503+00
 504+00
 505+00

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LEGEND

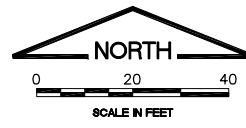
- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- R — REINFORCED PANEL
- █ THICKENED EDGE
- PROPOSED INLET - SPECIAL
- EXISTING INSPECTION HOLE

- NOTES:**
1. THE CONTRACTOR MAY SUBMIT AN ALTERNATE JOINTING/PAVING PLAN FOR REVIEW AND CONSIDERATION PRIOR TO INITIATING THE PAVING OPERATIONS. THE PAVING PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO, THE CONTRACTOR'S PROPOSED CHANGES TO PAVING DIRECTION, PROPOSED CHANGES TO PAVING LANE WIDTHS, PROPOSED CHANGES TO THE JOINTING TYPE, CONTRACTOR'S PLAN FOR "BOXING OUT" AROUND INLETS, LIGHTS, HAND-HOLES, OR OTHER PAVEMENT PENETRATIONS, PLANS TO PREVENT PREMATURE CRACKING IN "BOXED OUT" PANELS, CONTRACTOR'S PLAN FOR PAVING AGAINST ADJACENT STRUCTURES, AND ANY OTHER REVISION OR ALTERATION THAT THE CONTRACTOR SHOULD PROPOSE THAT DIFFERS FROM THE PLAN SHEETS.
 2. IN ADDITION TO THE MESH REINFORCING IN THE PANELS AROUND THE PROPOSED INLETS, THE CONTRACTOR SHALL PLACE 3 - #5 BARS OFF THE CORNERS OF ALL INLETS OR OTHER PAVEMENT PENETRATIONS AS SHOWN IN THE "PAVEMENT PENETRATION DETAIL" ON THE PAVING AND MISCELLANEOUS DETAILS SHEET.
 3. THE JOINTING PATTERN AROUND THE PROPOSED INLETS ARE SHOWN DARKER TO HIGHLIGHT THE CHANGE THE TYPICAL JOINTING LAYOUT PATTERN. SEE ALSO THE INLET STAKING PLAN SHEET FOR PROPOSED JOINTING LAYOUT AROUND THESE INLETS.

LEGEND

- A — TYPE A ISOLATION JOINT (3/4")
- B — TYPE B HINGED (TIED) CONTRACTION JOINT
- C — TYPE C DOWELED CONTRACTION JOINT
- D — TYPE D DUMMY CONTRACTION JOINT
- E — TYPE E DOWELED CONSTRUCTION JOINT
- R — REINFORCED PANEL
- THICKENED EDGE

NOTE:
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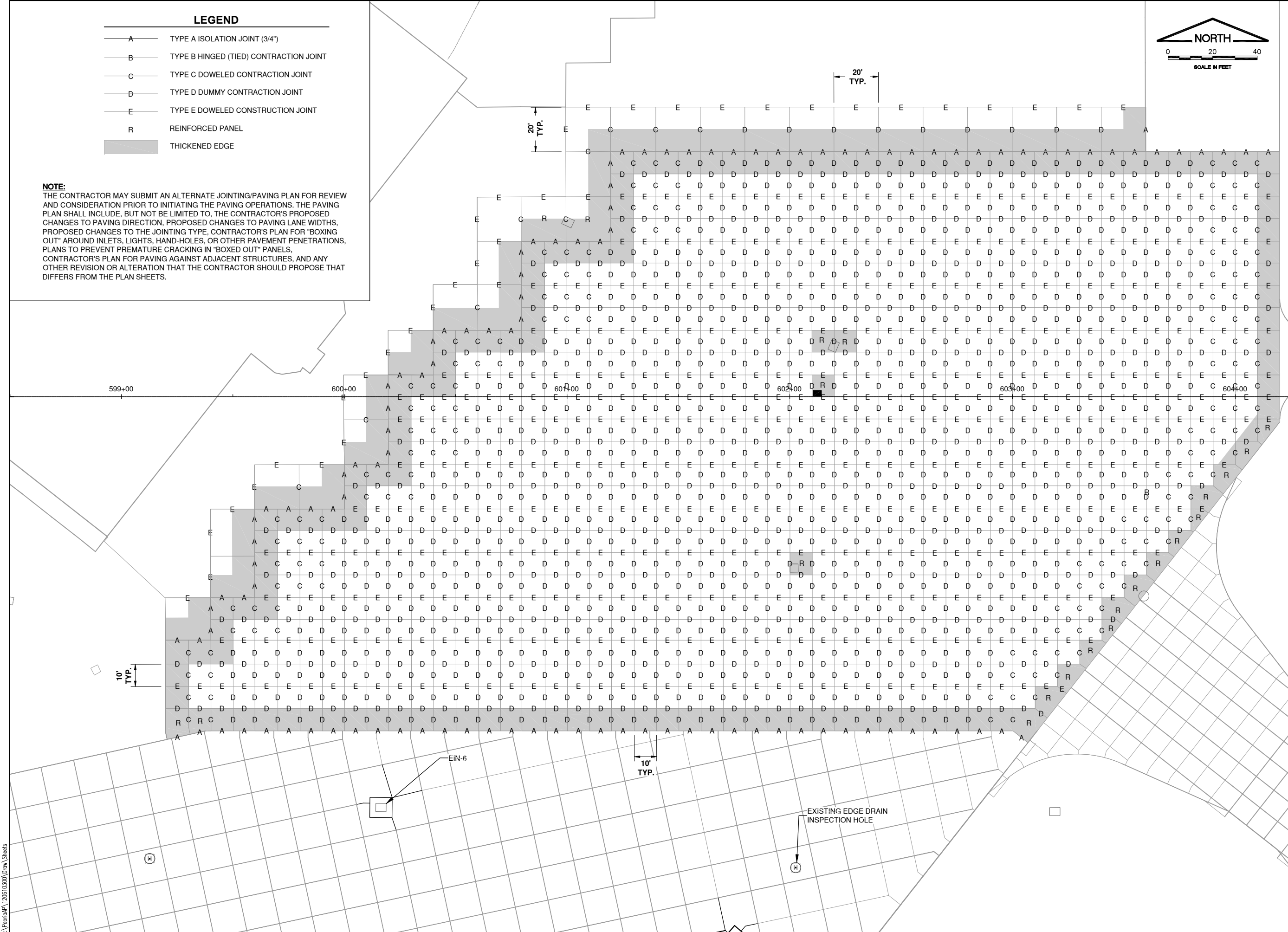
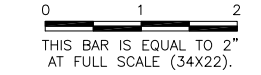


FILE: JOINT PLAN 2.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 9:37 PM

PIA-Base-Existing
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 BASE_PROP_JOINT
 BASE_PROP_GEO
 1206103-V-UP

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**RECONSTRUCT GENERAL AVIATION APRON
 JOINTING PLAN 2**

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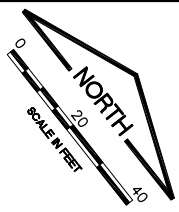


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AIP PROJ. NO. 3-17-0080-XX	
SHEET 26 OF 56 SHEETS	

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LEGEND

— A —	TYPE A ISOLATION JOINT (3/4")
— B —	TYPE B HINGED (TIED) CONTRACTION JOINT
— C —	TYPE C DOWELED CONTRACTION JOINT
— D —	TYPE D DUMMY CONTRACTION JOINT
— E —	TYPE E DOWELED CONSTRUCTION JOINT
R	REINFORCED PANEL
■	THICKENED EDGE

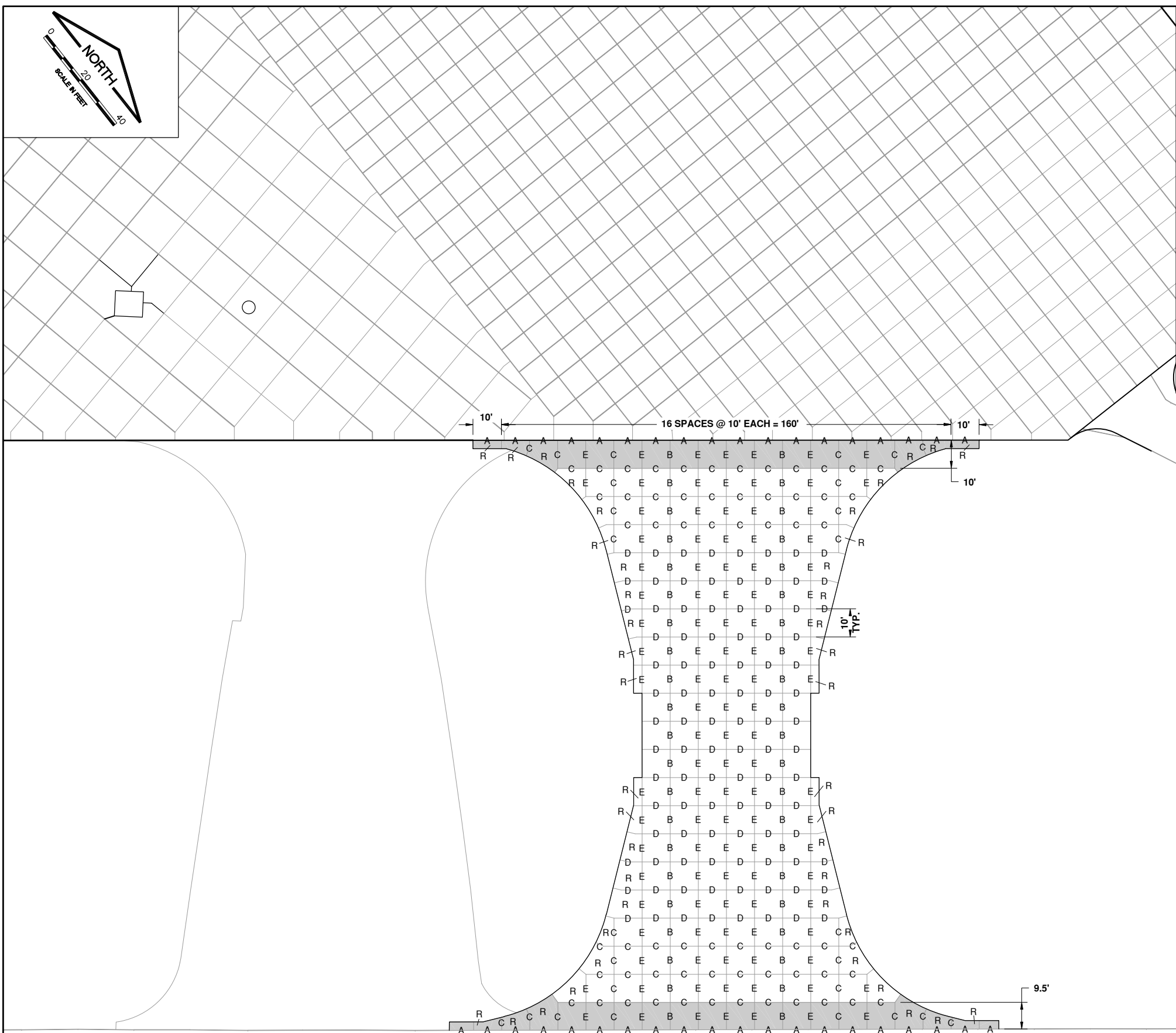
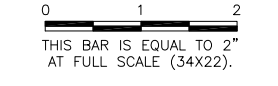
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FILE: JOINT PLAN 3.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 9:37 PM

PIA-Base-Existing
 1206103-V-VF2D
 BASE_PROP_JOINT
 BASE_PROP_GEO

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**RECONSTRUCT GENERAL AVIATION APRON
 JOINTING PLAN 3**

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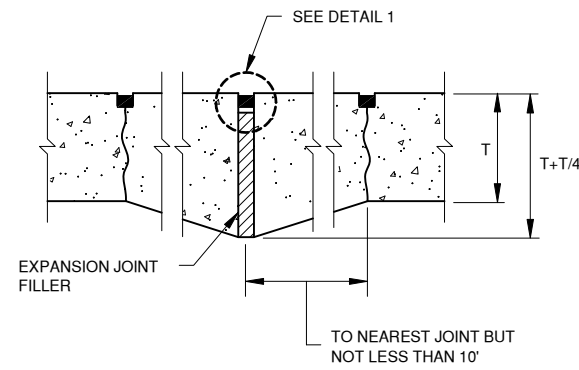
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AIP PROJ. NO.	3-17-0080-XX
SHEET	27 OF 56 SHEETS

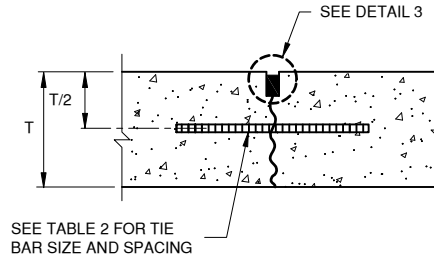
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TABLE 1	
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"



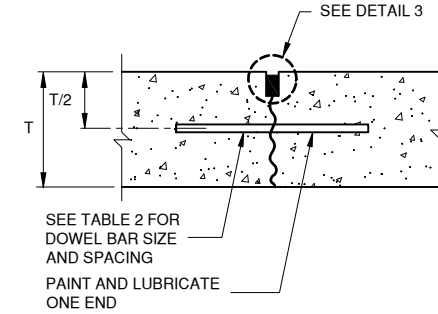
TYPE A THICKENED ISOLATION

SYMBOL 



TYPE B HINGED (TIED) CONTRACTION

SYMBOL 



TYPE C DOWELED CONTRACTION

SYMBOL 

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REVISIONS

NUMBER	BY	DATE

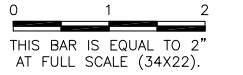
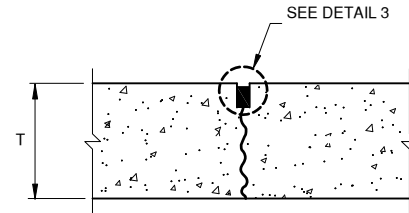
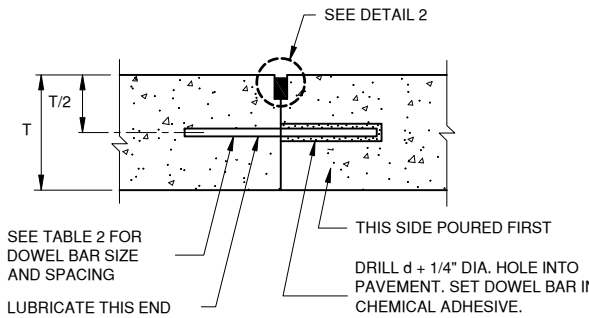


TABLE 2						
PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"



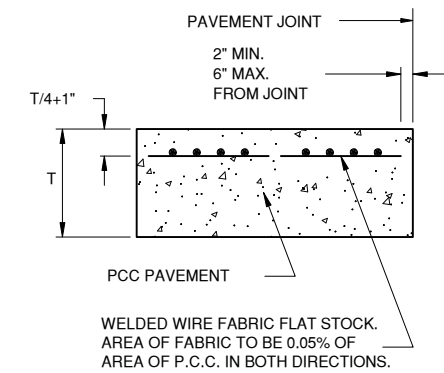
TYPE D DUMMY CONTRACTION

SYMBOL 



TYPE E DOWELED CONSTRUCTION

SYMBOL 



ODD SHAPED PANEL REINFORCEMENT

SYMBOL 

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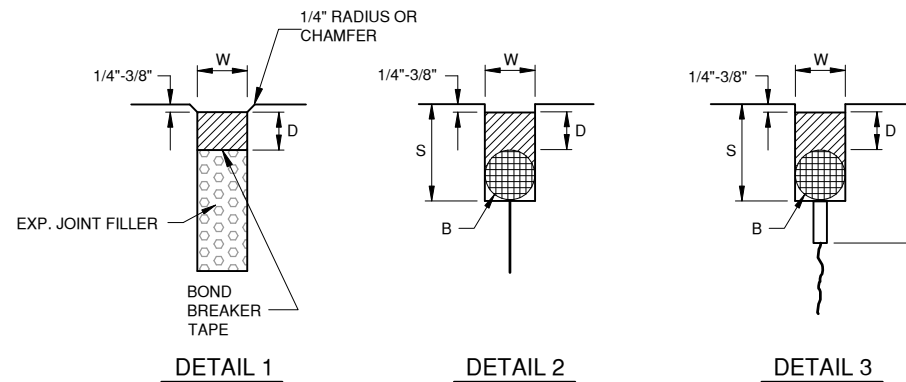
**RECONSTRUCT GENERAL AVIATION APRON
 JOINTING DETAILS**

JOINT SEALING DIMENSIONS			
	DETAIL 1	DETAIL 2	DETAIL 3
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/8	1/4	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-1/8	1-1/8

JOINT NOTES

- 1.) ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- 2.) THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- 3.) ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- 4.) TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- 5.) THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

JOINT SEALING DETAILS



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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

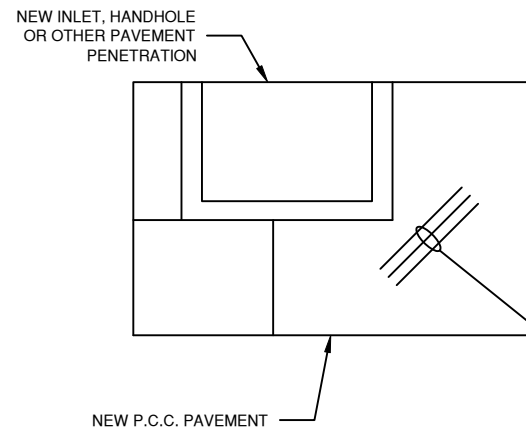
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RECONSTRUCT GENERAL AVIATION APRON
 PAVING AND MISCELLANEOUS DETAILS

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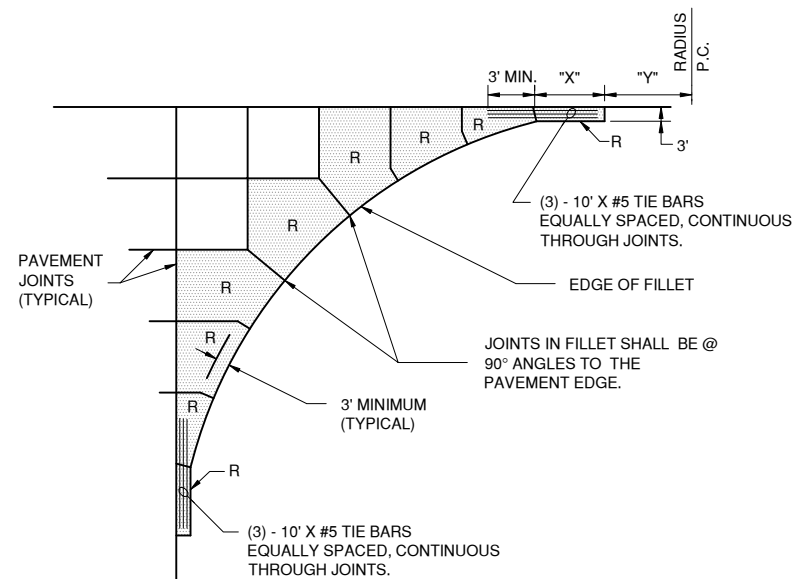


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SHEET	29 OF 56 SHEETS



PAVEMENT PENETRATION DETAIL
 N.T.S.

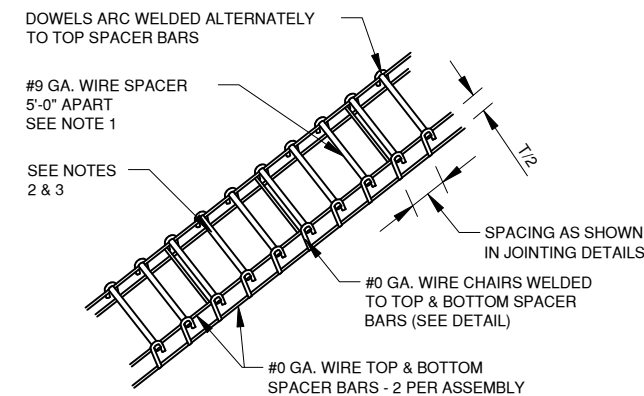
PLACE (3)- #5 TIE BARS IN NEW P.C.C. PAVEMENT ADJACENT TO CORNER OF PAVEMENT PENETRATION (TYPICAL OF ALL CORNERS)



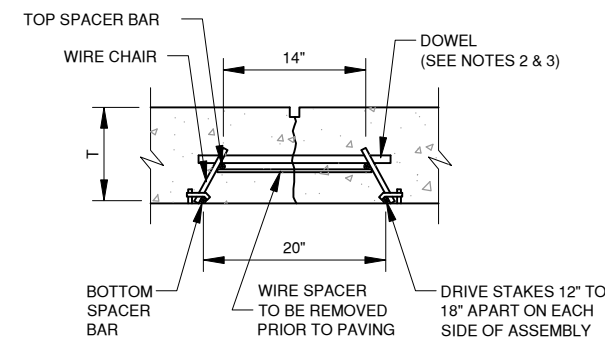
Ⓡ DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN ON JOINTING DETAILS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN)

FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98

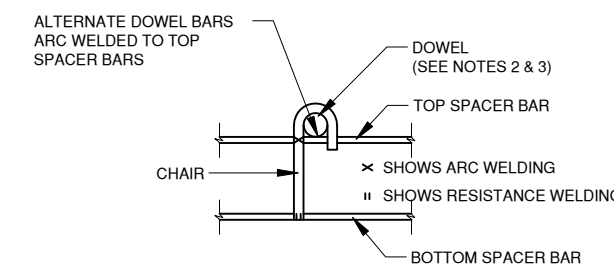
FILLET DETAIL & FILLET REINFORCING LAYOUT
 N.T.S.



DOWEL BASKET ASSEMBLY DETAIL



DOWEL BAR INSTALLATION DETAIL

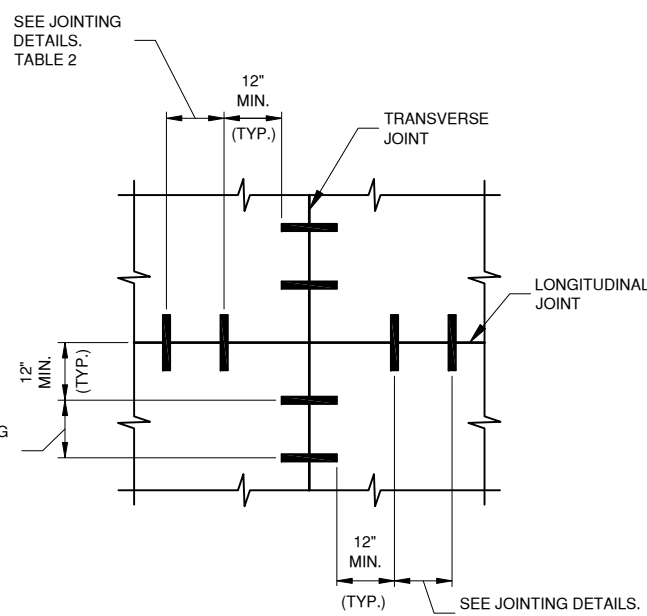


TYPICAL DOWEL BASKET ELEVATION DETAIL SHOWING CHAIR

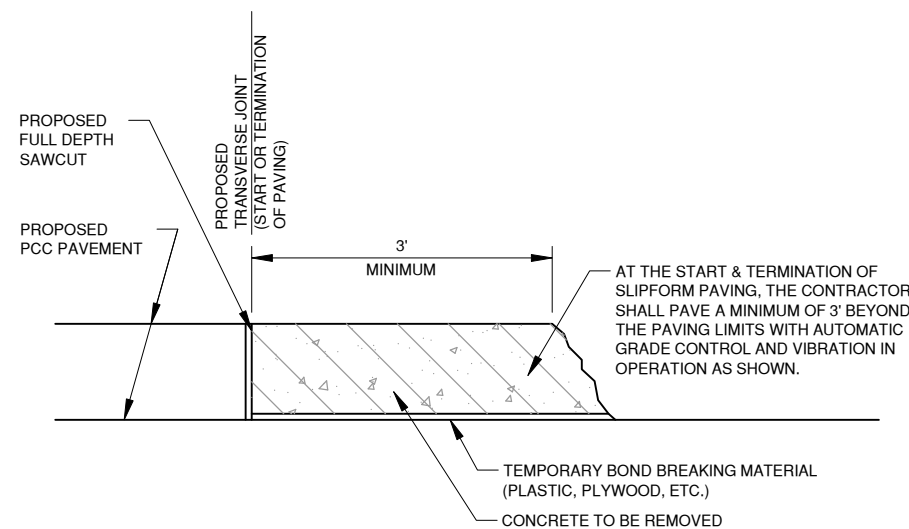
DOWEL BASKET DETAILS
 N.T.S.

DOWEL BASKET NOTES

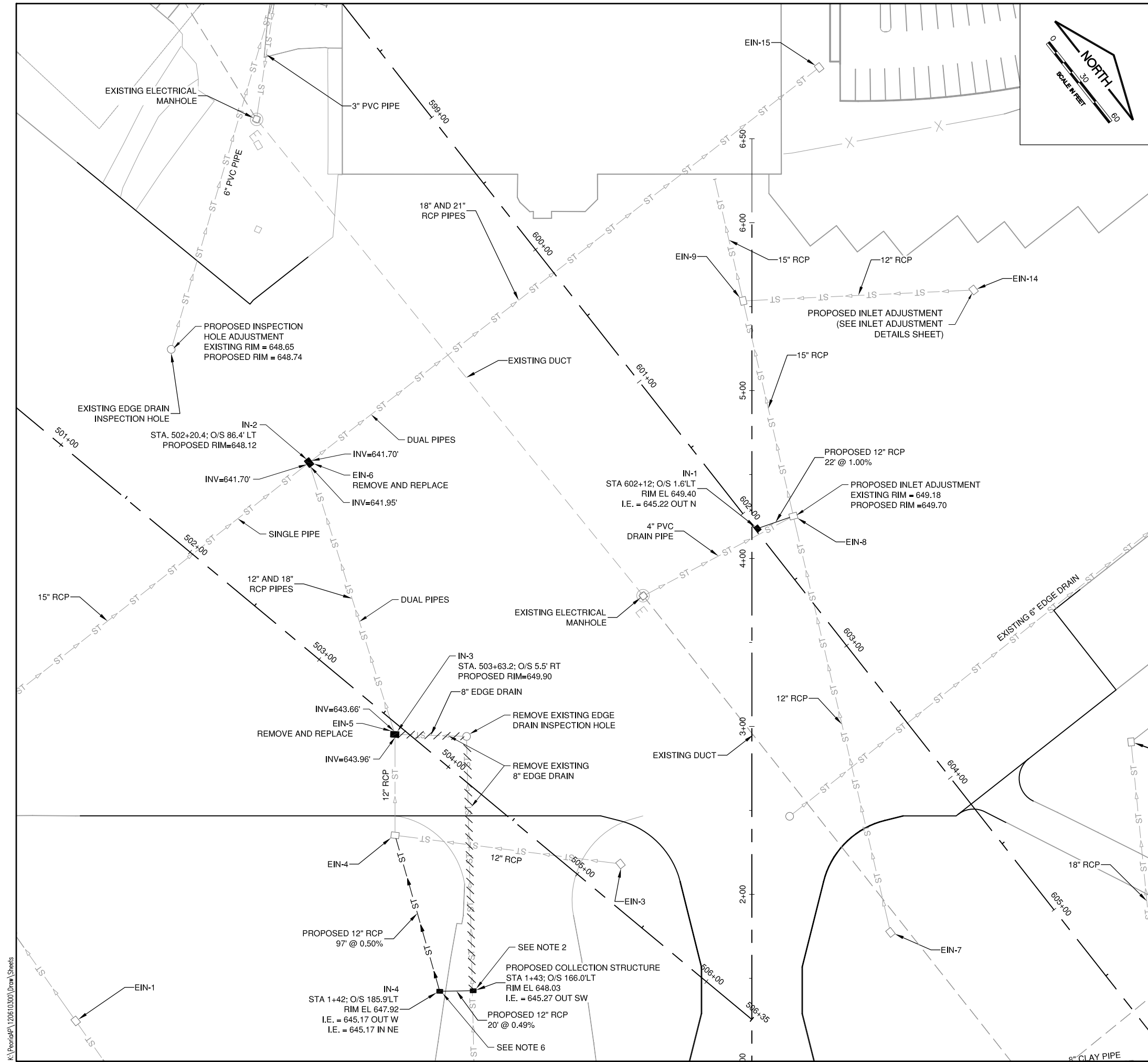
- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



DOWEL PLACEMENT DETAIL
 N.T.S.



REQUIREMENTS AT START & TERMINATION OF SLIPFORM PAVING
 N.T.S.



NOTES:

1. REMOVAL OF THE 8" EDGE DRAIN, REMOVAL OF THE INSPECTION HOLE, AND THE CONSTRUCTION OF THE COLLECTION STRUCTURE, IN-4, AND THE CONNECTING 12" PIPES SHALL BE PART OF THE ADDITIVE ALTERNATE.
2. AFTER THE REMOVAL OF TAXIWAY A4, THE EXISTING EDGE DRAIN SHALL BE POTHOLED AND LOCATED. THE PROPOSED COLLECTION STRUCTURE SHALL BE RELOCATED AS NECESSARY TO CONNECT TO THE EXISTING EDGE DRAIN. IF THE EXISTING EDGE DRAIN CANNOT BE LOCATED, THE PROPOSED COLLECTION STRUCTURE AND CONNECTING 12" RCP SHALL NOT BE INSTALLED.
3. THE CONTRACTOR SHALL USE CAUTION WHEN REMOVING THE EXISTING EDGE DRAIN NOT TO DISTURB THE EXISTING SUBGRADE BEYOND THE WIDTH OF THE TRENCH AND SHALL NOT CONSTRUCT THE TRENCH WIDER THAN NECESSARY.
4. SHOULD THE ADDITIVE ALTERNATE NOT BE AWARDED, THE EXISTING EDGE DRAIN AND INSPECTION HOLE SHALL REMAIN IN PLACE AND THE EXISTING EDGE DRAIN SHALL BE CONNECTED TO THE PROPOSED INLET IN-3 AT THE EXISTING INVERT ELEVATION OF 644.13.
5. THE REMOVAL OF THE EDGE DRAIN PIPE SHALL BE PAID FOR AS AN ADDITIVE ALTERNATE UNDER ITEM AR705900 - REMOVE UNDERDRAIN.
6. THE PROPOSED INLET IN-4 SHALL BE USED TO DRAIN THE EXISTING UNDERDRAIN AS SHOWN ON THE UNDERDRAIN PLAN. THE EXISTING UNDERDRAIN SHALL BE LOCATED PRIOR TO THE CONSTRUCTION OF IN-4 TO VERIFY THAT THE INLET LOCATION INTERSECTS THE EXISTING UNDERDRAIN.
7. SEE STAKING PLANS 1 THROUGH 3 FOR EXISTING AND PROPOSED CONTOURS AND FOR INFIELD GRADING AFTER THE REMOVAL OF EXISTING TAXIWAY A4 (ADDITIVE ALTERNATE).
8. INLET IN-1 SHALL BE PAID FOR UNDER ITEM AR751605 - CATCH BASIN - SPECIAL. SEE CATCH BASIN - SPECIAL DETAILS SHEET FOR IN-1 DETAILS.
9. INLETS IN-2 AND IN-3 SHALL BE PAID FOR UNDER ITEM AR751415 - INLET - SPECIAL. SEE INLET - SPECIAL DETAILS SHEET FOR IN-2 AND IN-3 DETAILS.
10. THE ADDITIVE ALTERNATE INLET IN-4 SHALL BE PAID FOR UNDER ITEM AS751410 - INLET. SEE INLET DETAILS SHEET FOR IN-4 DETAILS.

LEGEND

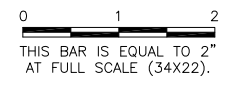
- ST — ST — PROPOSED STORM SEWER
- - - ST - - - EXISTING STORM SEWER/EDGE DRAIN
- ||||| ST ||||| EXISTING STORM/EDGE DRAIN TO BE REMOVED
- PROPOSED INLET
- EXISTING INLET

FILE: DRAINAGE PLAN.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/2/2013 8:19 AM

PIA-Bose-Existing
 BASE_PROP_GEO
 1206103-V-UP
 BASE_PROP_JOINT

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 PEORIA, ILLINOIS**
**RECONSTRUCT GENERAL AVIATION APRON
 DRAINAGE PLAN**

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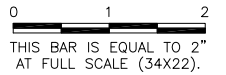


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SHEET	30 OF 56 SHEETS

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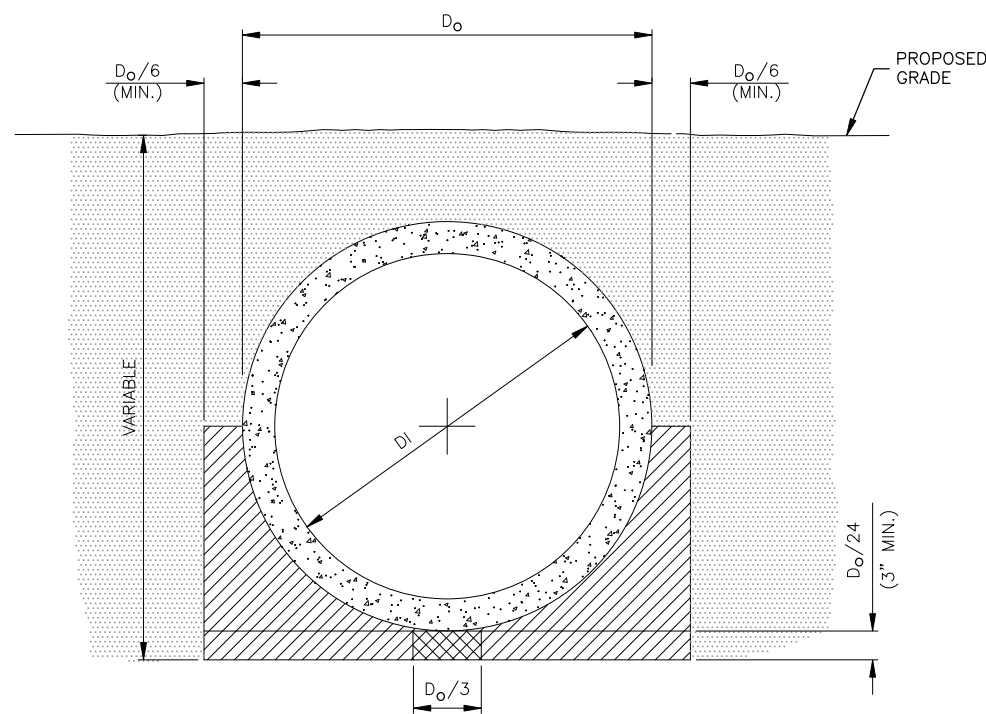


TRENCH INSTALLATION LEGEND

- DRAINAGE CONDUIT MATERIAL—CONCRETE
- MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
- HAUNCH AND OUTER BEDDING COMPACTION— TO ENGINEER'S SATISFACTION OR 95% STANDARD PROCTOR
- LOWER SIDE AND OVERFILL COMPACTION— SAME AS EMBANKMENT REQUIREMENTS
- D_o PIPE OUTSIDE DIAMETER
- D_i PIPE INSIDE DIAMETER

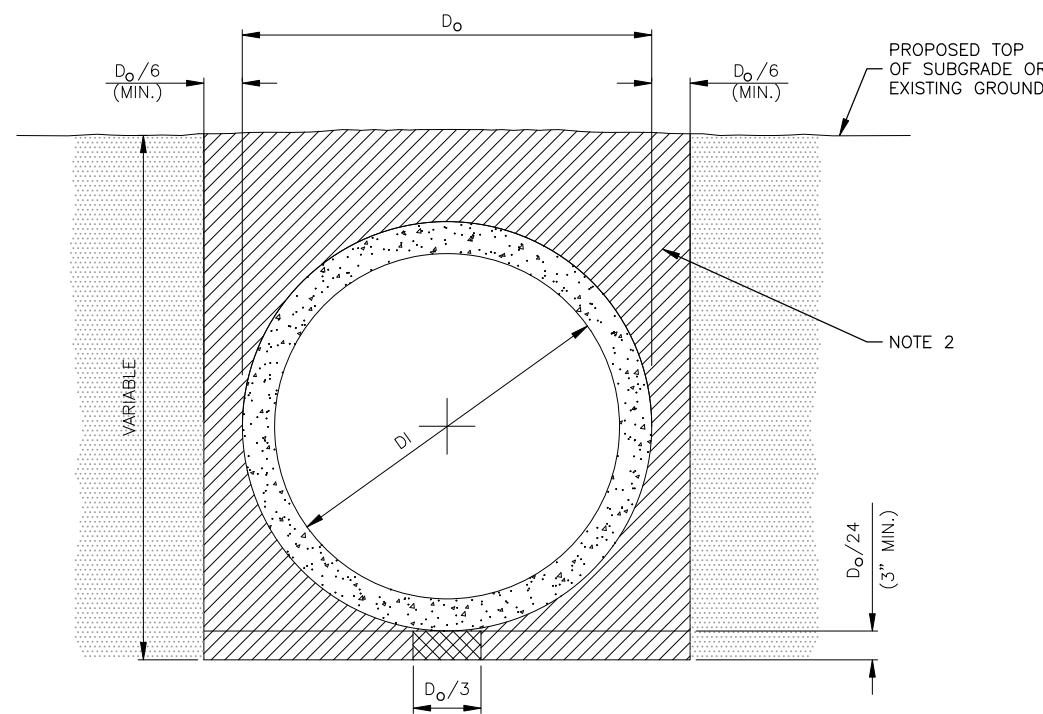
TRENCH INSTALLATION NOTES

- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
- BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.



**STANDARD TRENCH INSTALLATION
NON-PAVED AREA**

N.T.S.



**STANDARD TRENCH INSTALLATION
PROPOSED PAVED AREA**

N.T.S.

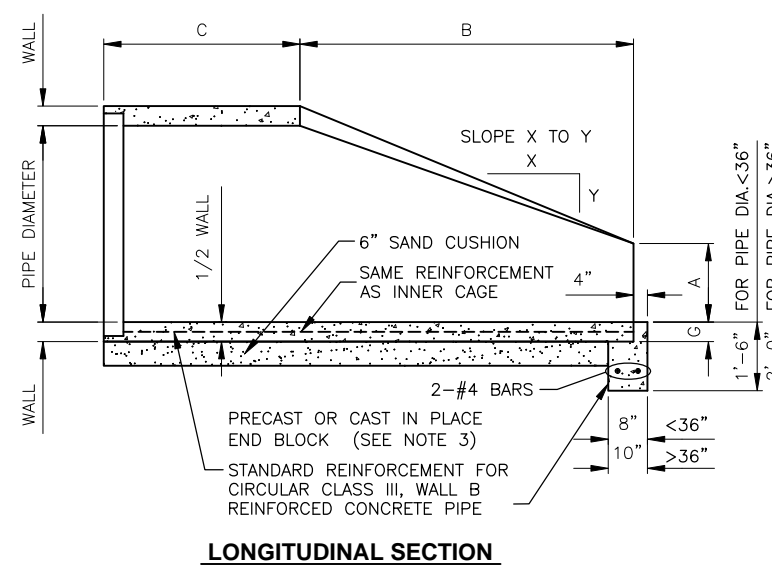
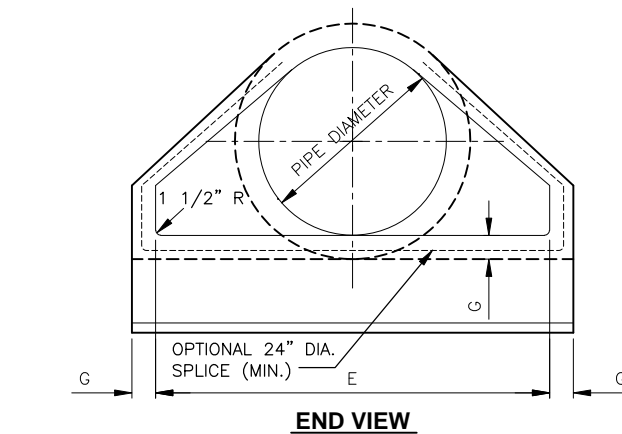
DIMENSIONS - TABLE 1

PIPE DIA.	APPROX WT.(lbs.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	2"	9"	3:1
15"	740	2 1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2 1/4"	11"	3:1
18"	990	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2 1/2"	12"	3:1
21"	1280	2 3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2 3/4"	13"	3:1
24"	1520	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3"	14"	3:1
27"	1930	3 1/4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	4'-6"	3 1/4"	14 1/2"	3:1
30"	2190	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	3 1/2"	15"	3:1
33"	3200	3 3/4"	1'-1 1/2"	4'-10 1/2"	3'-3 1/4"	8'-1 3/4"	5'-6"	3 3/4"	17 1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	4"	20"	3:1
42"	5380	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4 1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5 1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5 1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6 1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5 1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7 1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6 1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6 1/2"	1'-9"	9'-3 1/2"	10'-0"	6 1/2"	*	1.5:1

* RADIUS AS FURNISHED BY MANUFACTURER.

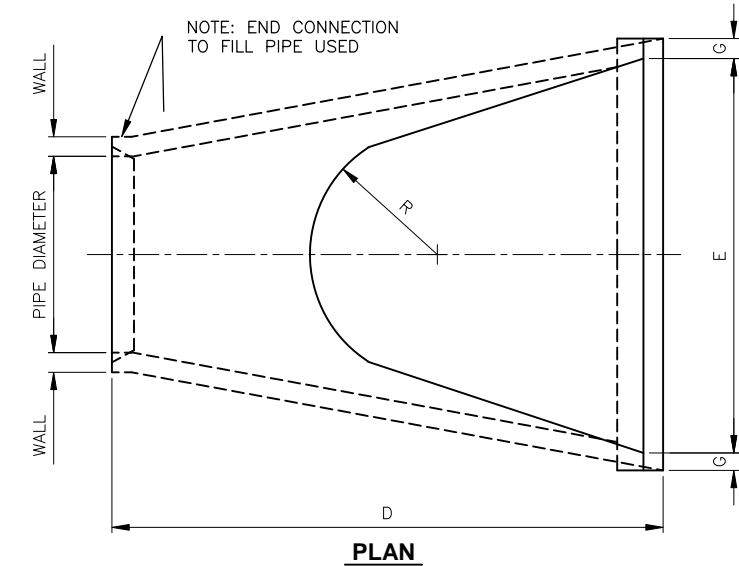
PRECAST FLARE SECTION NOTES

- PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
- PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
- THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 701.



PRECAST REINFORCED CONCRETE FLARED END SECTION
(I.D.O.T. STD. NO. 542301)

N.T.S.



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PEORIA, ILLINOIS
RECONSTRUCT GENERAL AVIATION APRON
DRAINAGE DETAILS

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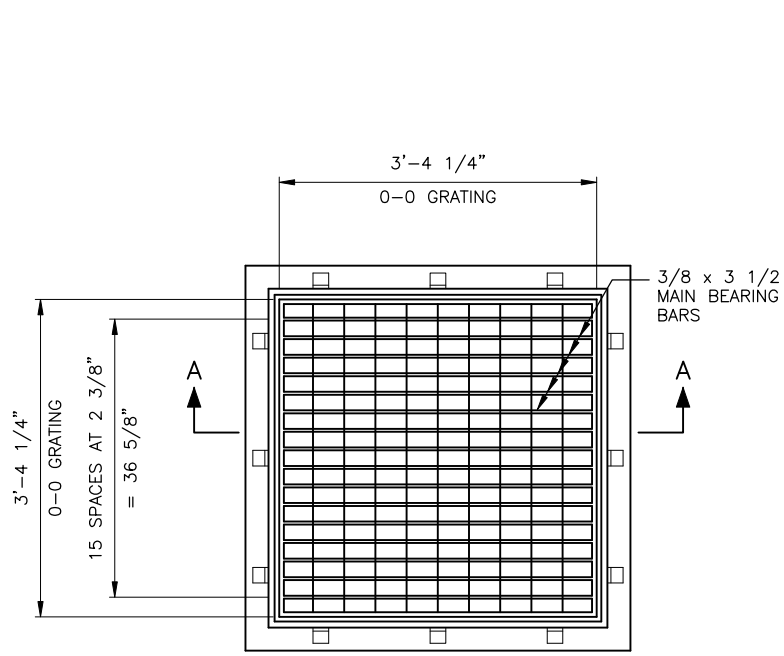
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IL PROJ. NO.	PIA-4361
AIP PROJ. NO.	3-17-0080-XX
SHEET	31 OF 56 SHEETS

PE096

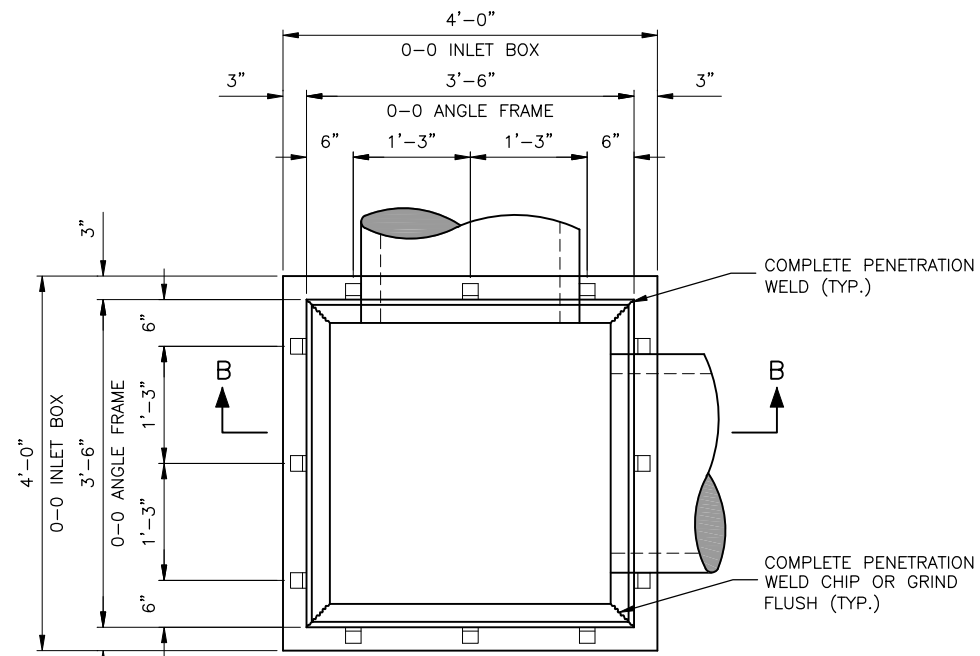
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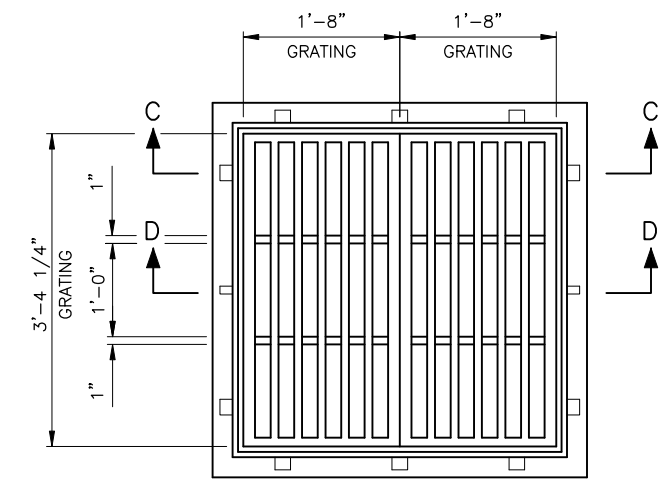
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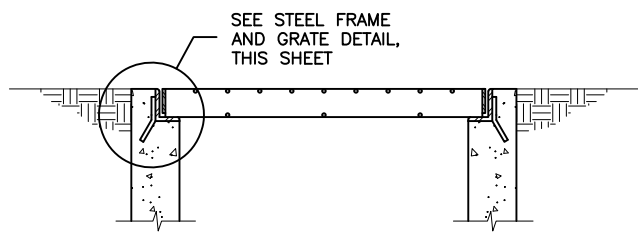
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N.T.S.



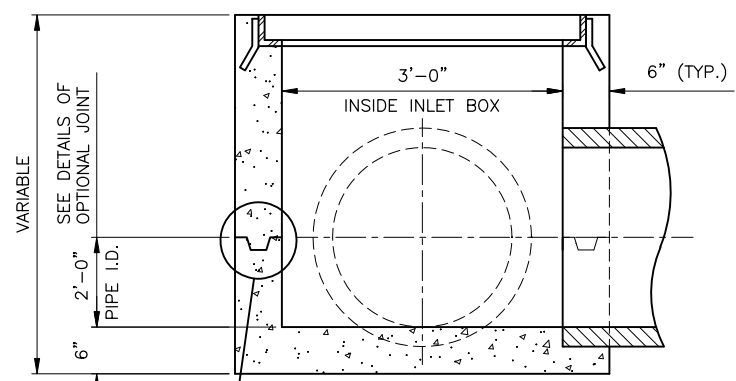
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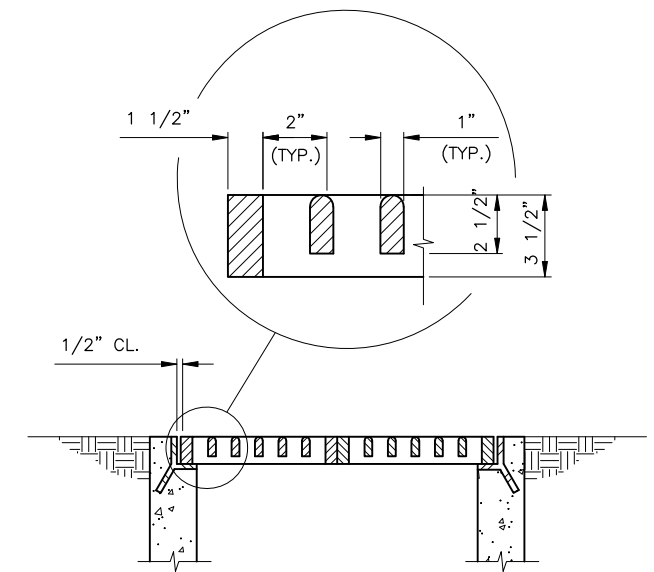
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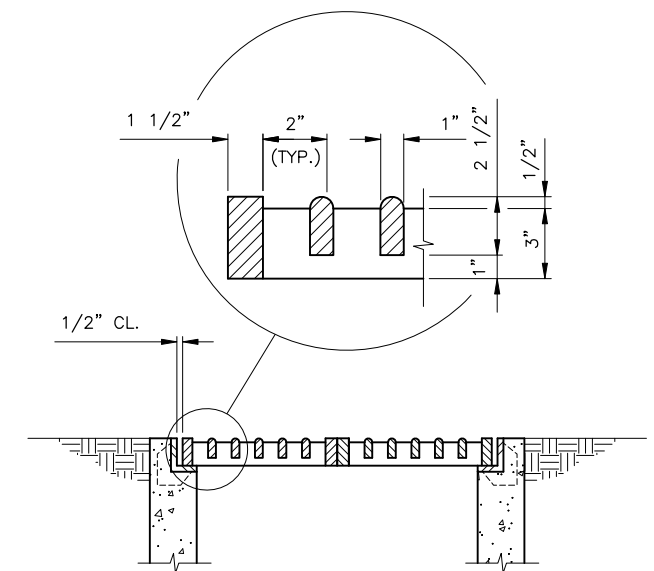
SECTION A-A
N.T.S.



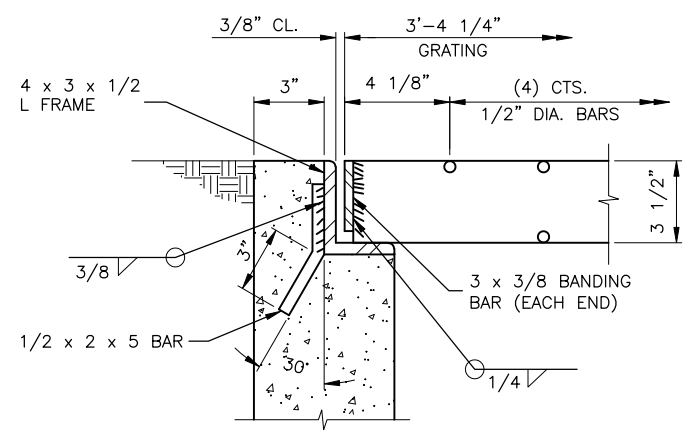
SECTION B-B
N.T.S.



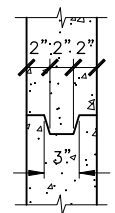
SECTION C-C
N.T.S.



SECTION D-D



STEEL FRAME & GRATE DETAIL
N.T.S.



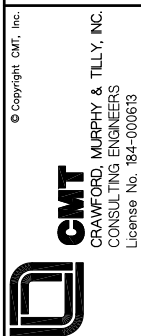
OPTIONAL JOINT DETAIL
N.T.S.

INLET NOTE
 INLET IS AN IDOT STD. 542546, FLUSH INLET BOX FOR MEDIAN. FRAME & GRATES ARE NEENAH R-3807 OR APPROVED EQUAL.

CAST FRAME & GRATE DETAILS
N.T.S.

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 INLET DETAILS**



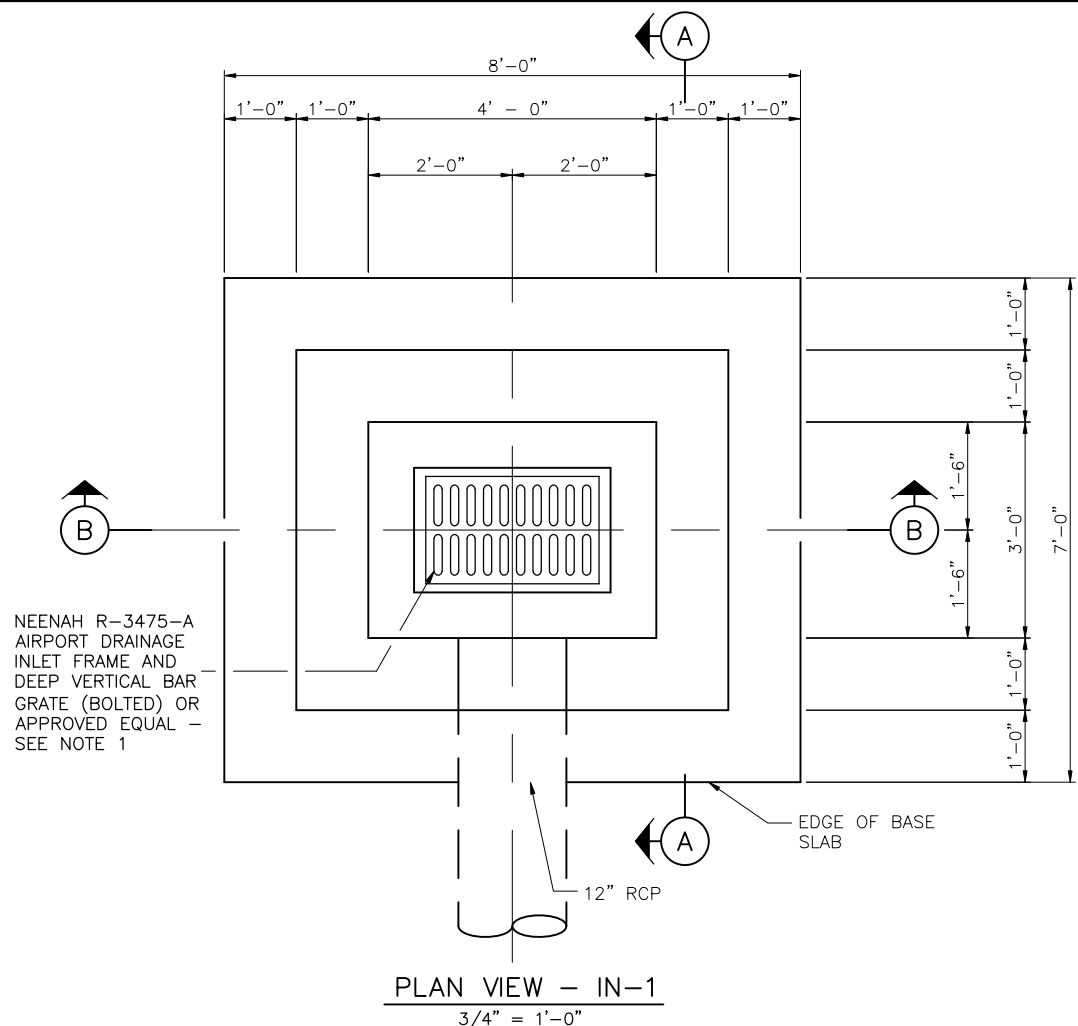
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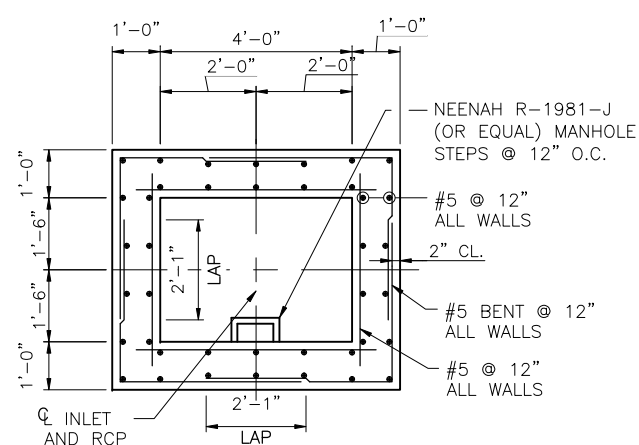
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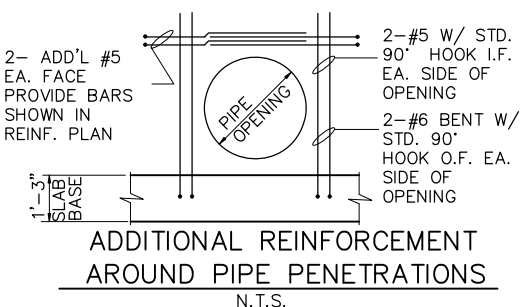
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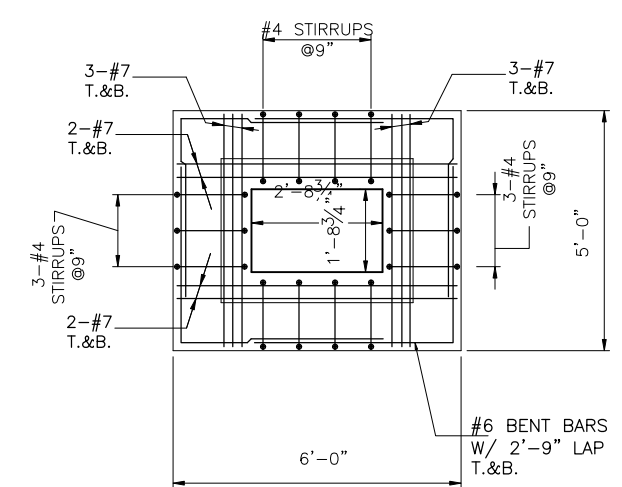
PLAN VIEW - IN-1
 3/4" = 1'-0"



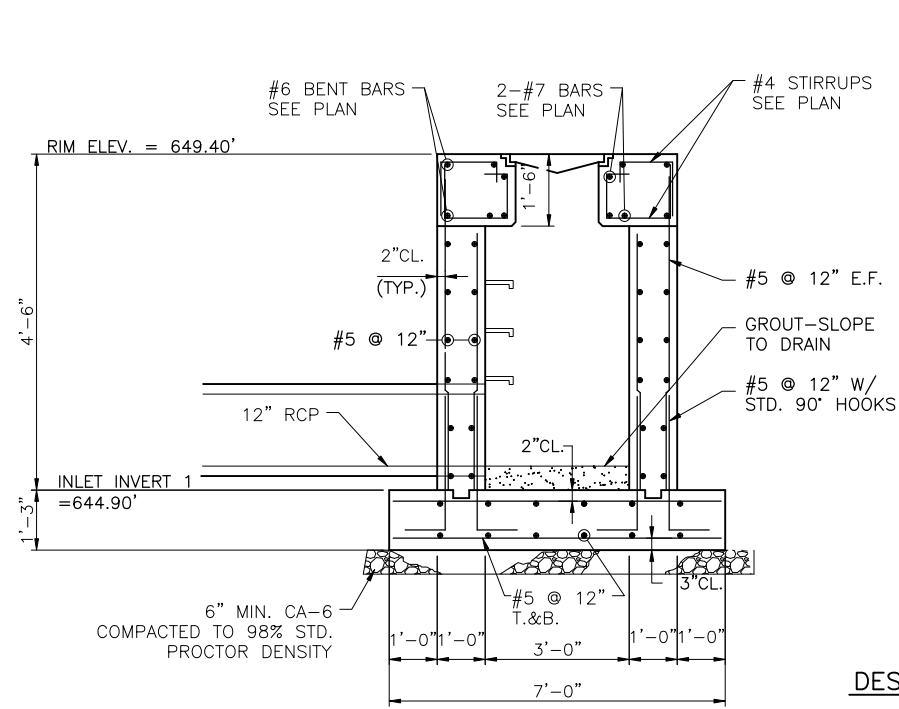
WALL REINFORCEMENT PLAN
 1/2" = 1'-0"



ADDITIONAL REINFORCEMENT AROUND PIPE PENETRATIONS
 N.T.S.

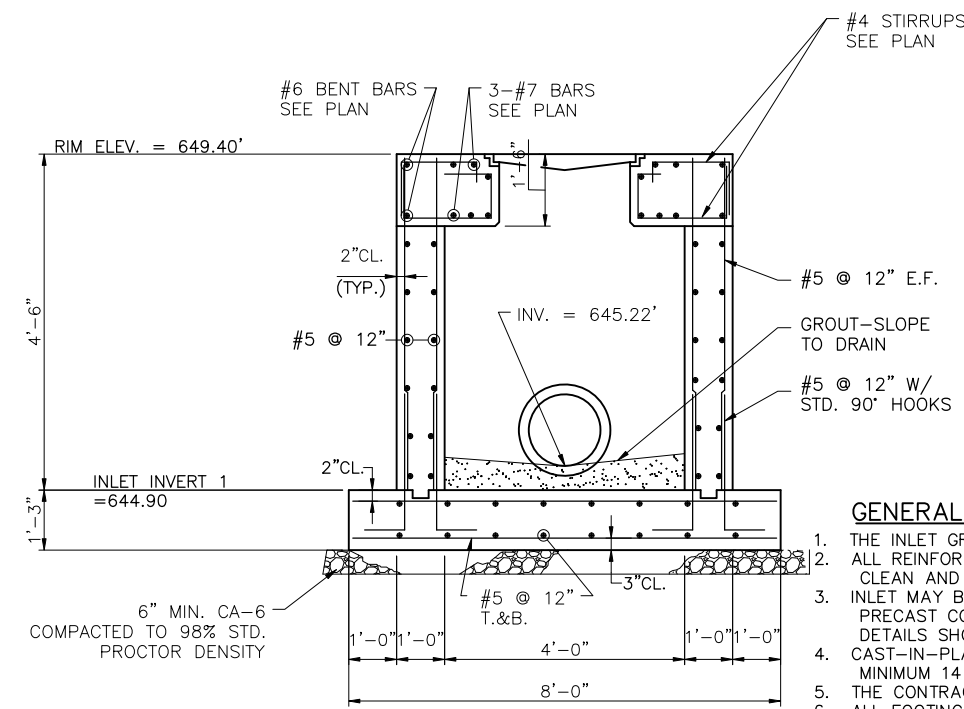


TOP REINFORCEMENT PLAN



SECTION A
 1/2" = 1'-0"

DESIGN CRITERIA
 L.L. = BOEING AIRCRAFT 757-300
 WATER TABLE AT TOP OF INLET
 NET ALLOWABLE SOIL BEARING
 PRESSURE = 2500 PSF



SECTION B
 1/2" = 1'-0"

GENERAL NOTES

1. THE INLET GRATE SHALL BE SUPPORTED ON ALL FOUR SIDES.
2. ALL REINFORCEMENT BARS SHALL CONFORM TO ASTM A615 GRADE 60 AND SHALL BE CLEAN AND FREE OF GREASE, SCALING RUST AND OTHER FOREIGN MATERIALS.
3. INLET MAY BE CONSTRUCTED BY CAST-IN-PLACE CONCRETE OR PRECAST CONCRETE. PRECAST CONCRETE INLETS SHALL BE CONSTRUCTED TO THE LINES, DIMENSIONS AND DETAILS SHOWN ON THIS SHEET.
4. CAST-IN-PLACE CONCRETE AND PRECAST CONCRETE FOR THE INLETS SHALL HAVE A MINIMUM 14 DAYS COMPRESSIVE STRENGTH OF 4000 PSI.
5. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS AND SIZE OF PIPE OPENINGS.
6. ALL FOOTING EXCAVATIONS SHALL BE CLEAN FREE OF DEBRIS, STANDING WATER AND LOOSE SOIL AND SHALL BE INSPECTED BY THE ENGINEER PRIOR TO THE PLACEMENT OF CONCRETE OR SUBBASE.
7. CONCRETE SHALL NOT BE PLACED OVER FROZEN OR MUDDY SOIL.
8. THIS ITEM SHALL BE PAID FOR UNDER ITEM AR751605 - CATCH BASIN-SPECIAL.

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RECONSTRUCT GENERAL AVIATION APRON
CATCH BASIN - SPECIAL DETAILS

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0 1 2
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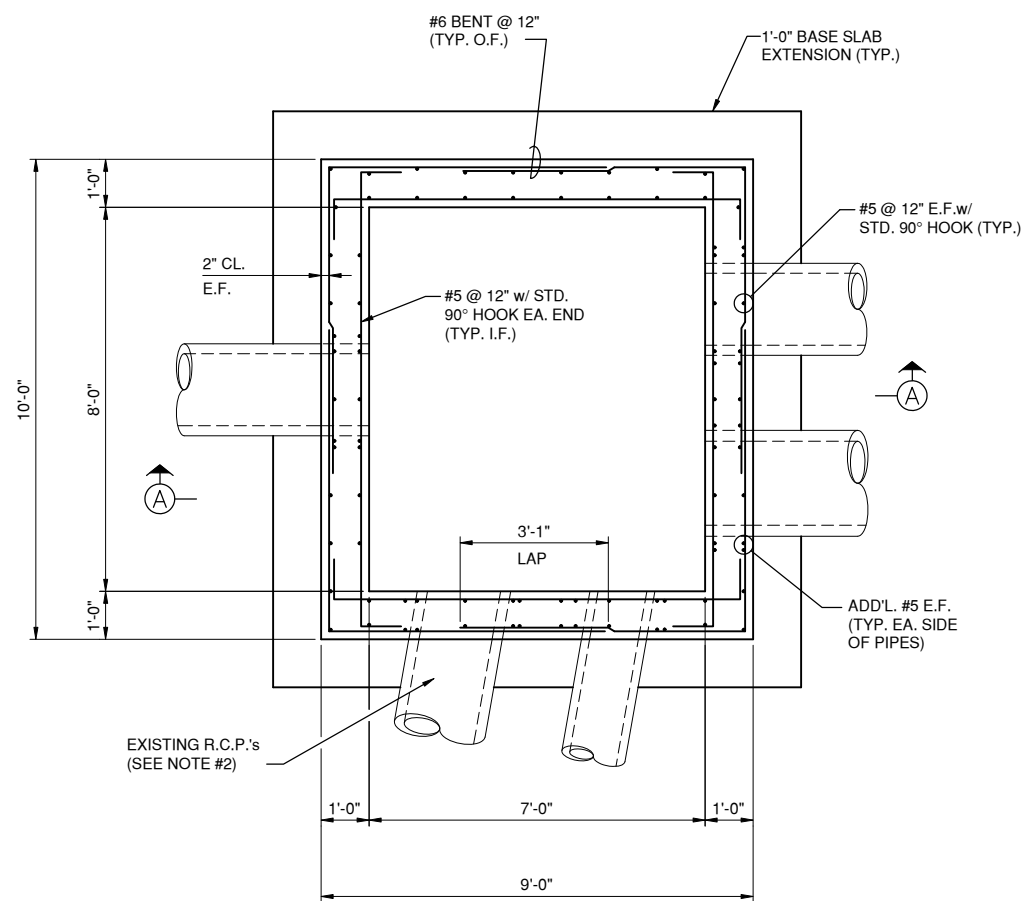
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 INLET - SPECIAL DETAILS

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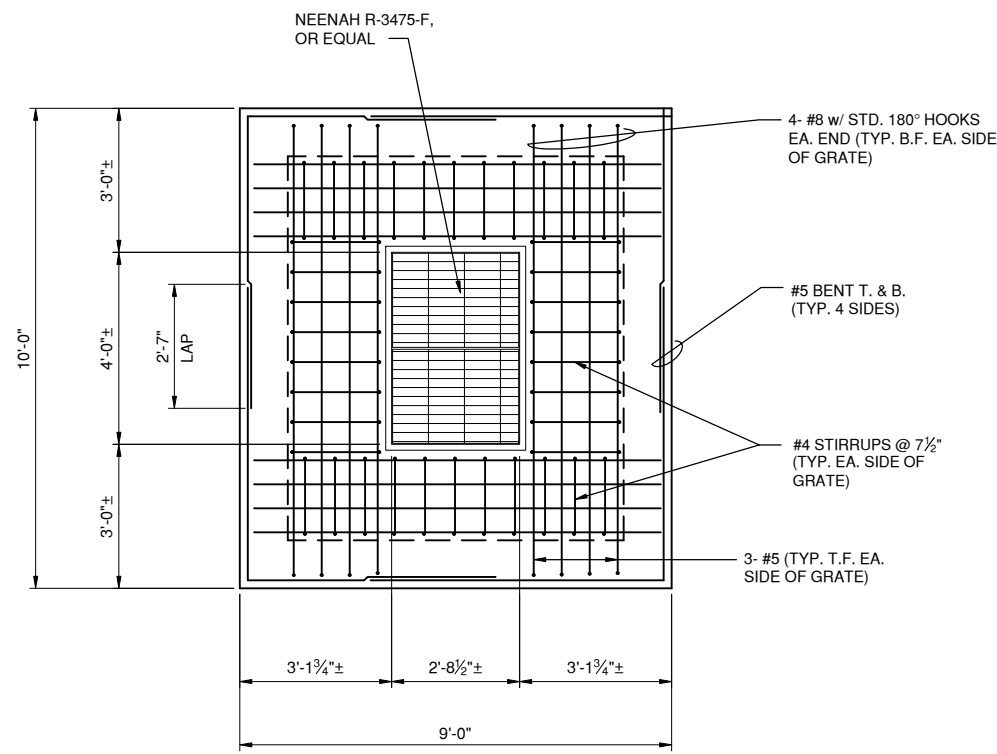
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AIP PROJ. NO.	3-17-0080-XX
SHEET	34 OF 56 SHEETS



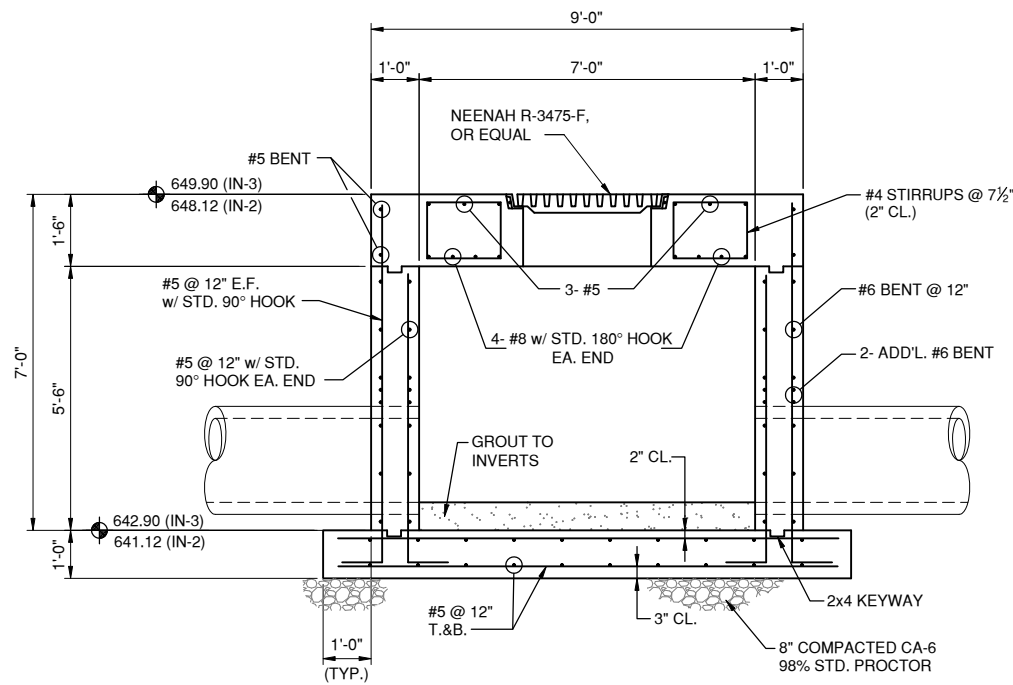
WALL PLAN

1/2"=1'-0"



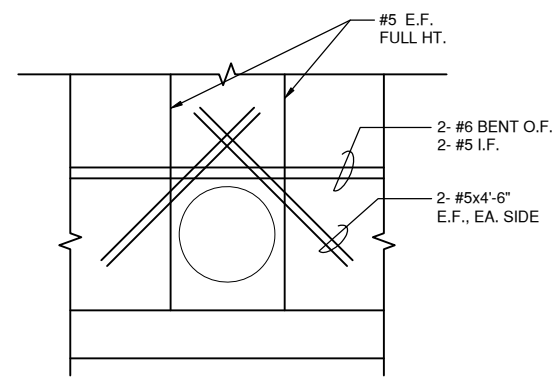
TOP SLAB PLAN

1/2"=1'-0"



SECTION A

1/2"=1'-0"



ADDITIONAL REINFORCING AROUND PIPES

1/2"=1'-0"

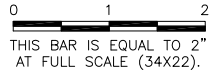
NOTES:

- SEE SHEET 30 FOR LOCATIONS OF IN-3 AND IN-2.
- IN-2 SHOWN, IN-3 SIMILAR. SEE SHEET 30 FOR PIPE SIZES, LOCATIONS AND INVERTS.

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**RECONSTRUCT GENERAL AVIATION APRON
 INLET ADJUSTMENT DETAILS**

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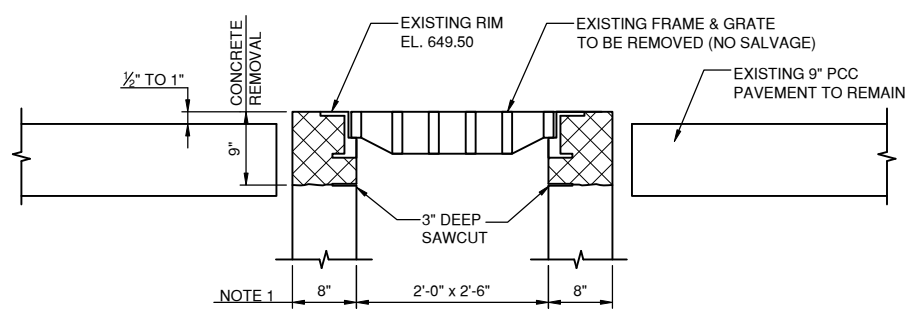


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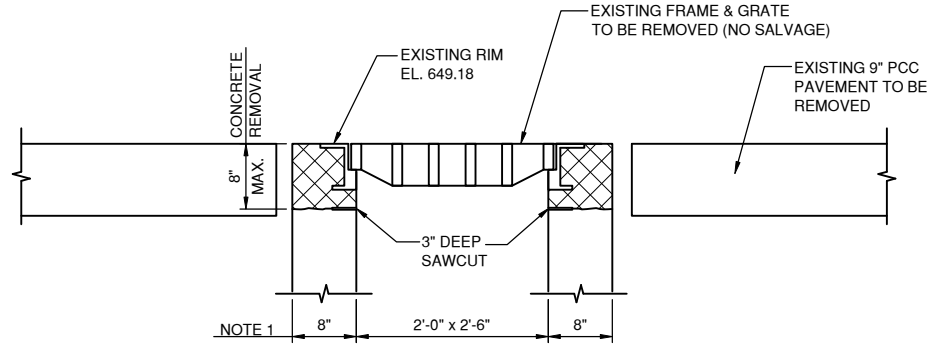


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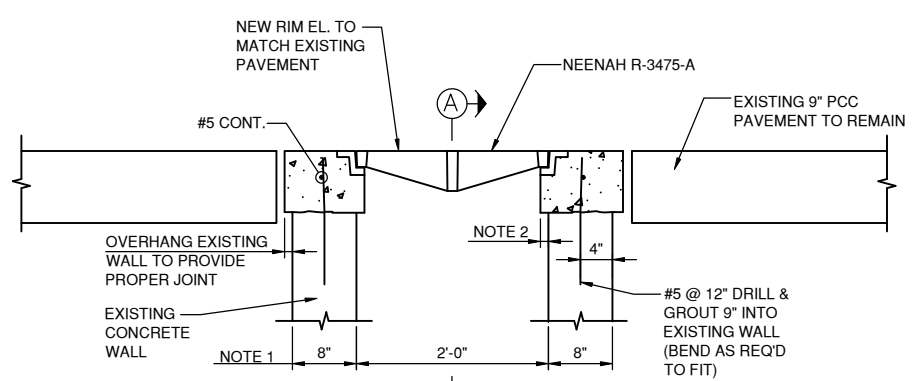
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SHEET 35 OF 56 SHEETS	



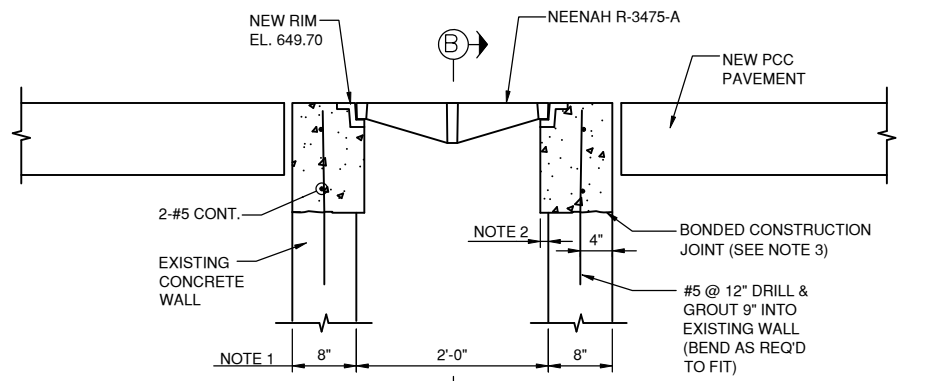
REMOVAL



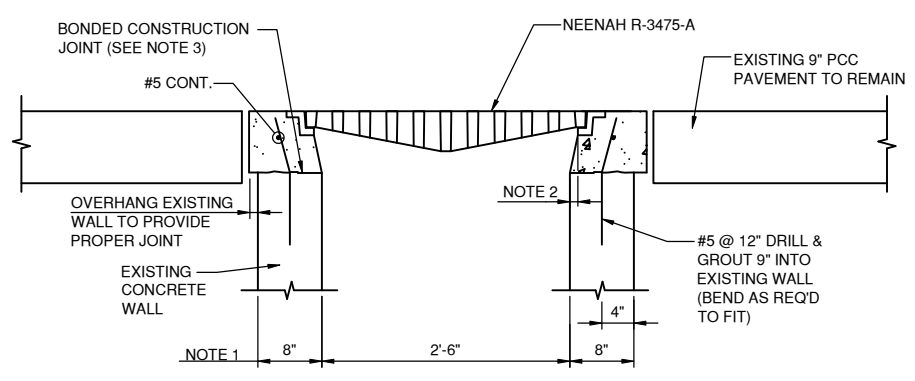
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REPLACEMENT

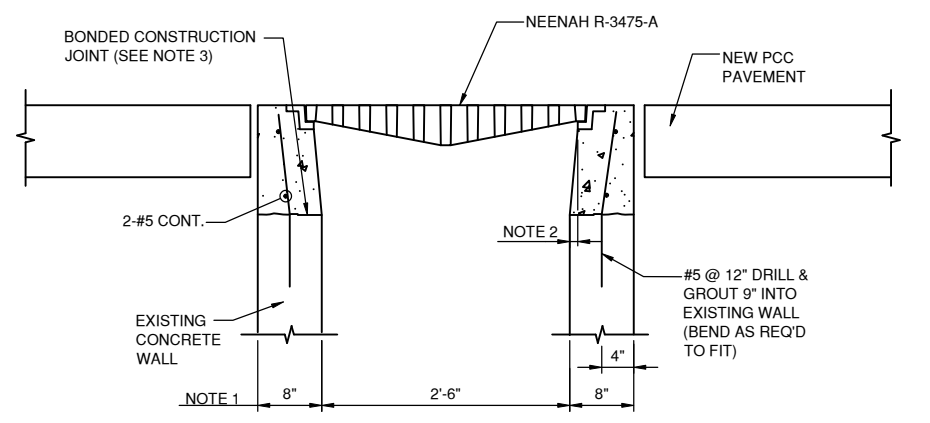


REPLACEMENT



SECTION A

EIN-14 MODIFICATIONS
1"=1'-0"



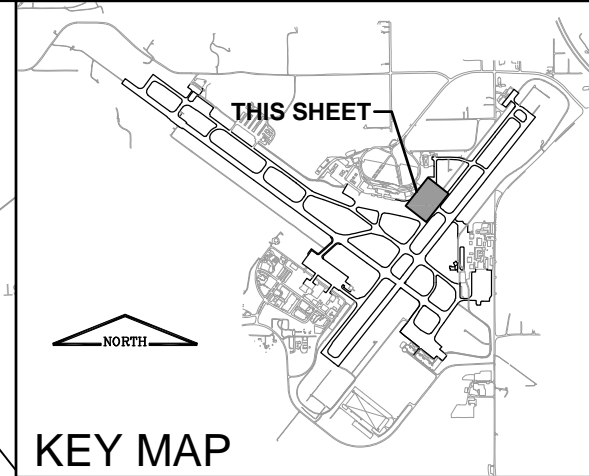
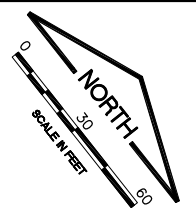
SECTION B

EIN-8 MODIFICATIONS
1"=1'-0"

NOTES

- EXISTING STRUCTURE DIMENSIONS ARE ESTIMATED FROM TOPOGRAPHIC SURVEYS. ALL DIMENSIONS SHALL BE MEASURED IN FIELD BY CONTRACTOR PRIOR TO SUBMITTING SHOP DRAWINGS.
- NEW CONCRETE WALL REPLACEMENT SHALL BE TAPERED OR OVERHANG EXISTING WALL TO ACCOMMODATE NEW GRATE FRAME.
- EXISTING CONCRETE SURFACES WHICH ARE TO RECEIVE NEW CONCRETE SHALL BE CLEANED PRIOR TO APPLYING BONDING AGENT.

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KEY MAP

- NOTES**
1. CAP UNDERDRAIN AT LOCATIONS SHOWN.
 2. SEE UNDERDRAIN SCHEDULE FOR LOCATIONS AND ELEVATIONS.
 3. UNDERDRAINS INSTALLED OFF THE EDGE OF PAVEMENT SHALL BE INSTALLED AT THE OFFSET DISTANCE SHOWN IN THE UNDERDRAIN DETAILS.
 4. CLEANOUT RIM ELEVATIONS SHALL NOT BE ABOVE THE PROPOSED TURF ELEVATION.

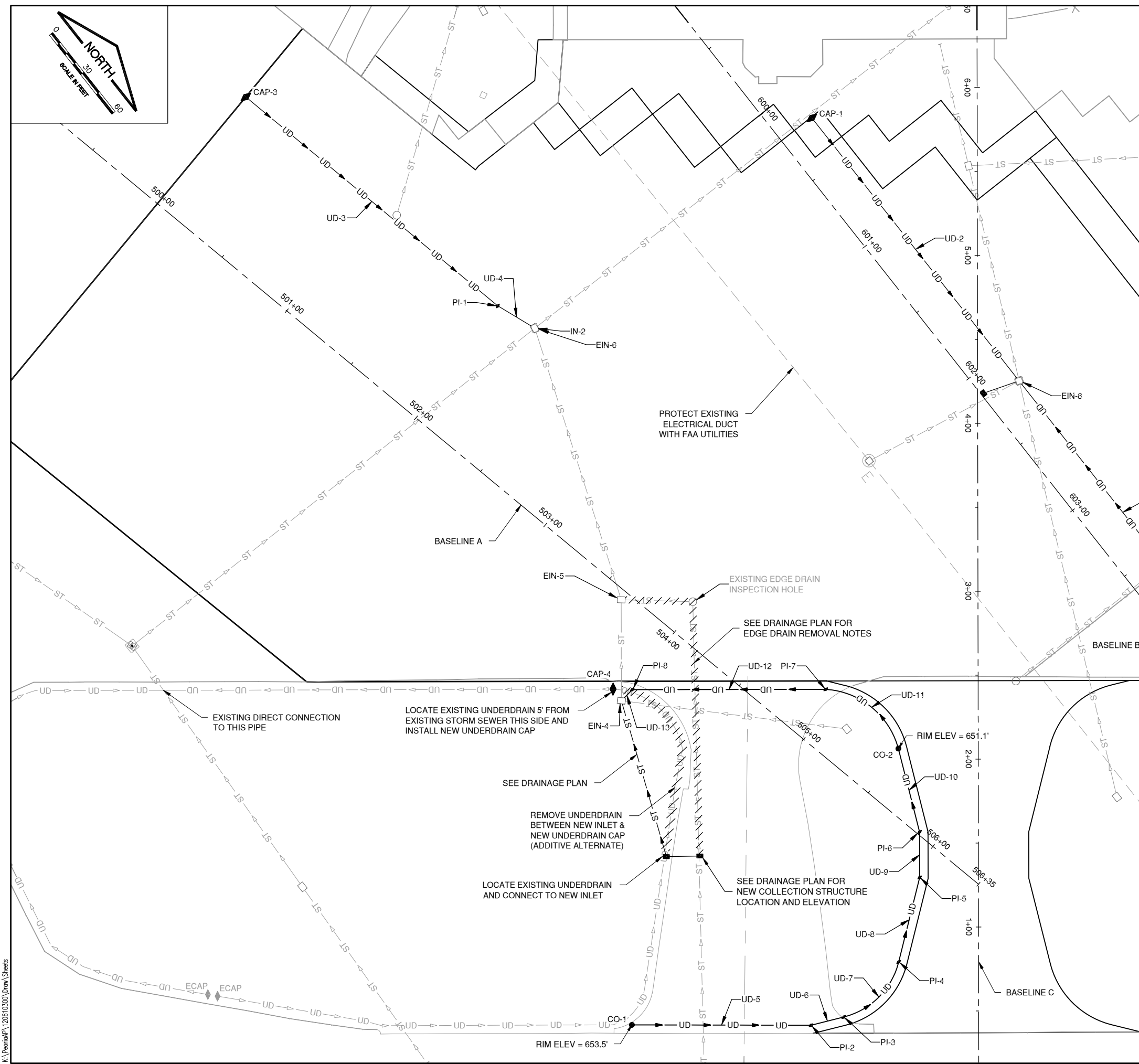
FILE: UNDERDRAIN PLAN.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 9:38 PM

BASE_PROP_GEO
 PIA-Base-Existing
 1206103-V-UP
 1206103-C-UPDR
 KEYMAP

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**RECONSTRUCT GENERAL AVIATION APRON
 UNDERDRAIN PLAN**

LEGEND

— UD —	NEW UNDERDRAIN
- - - UD - - -	EXISTING UNDERDRAIN
- - - DUCT - - -	EXISTING DUCT
UD	EXISTING UNDERDRAIN TO BE REMOVED
■	NEW INLET
●	NEW CLEANOUT
●	NEW COLLECTION STRUCTURE
◆	NEW UNDERDRAIN END CAP
◆	NEW DIRECT CONNECTION
□	EXISTING INLET
⊙	EXISTING MANHOLE
○	EXISTING CLEANOUT
○	EXISTING COLLECTION STRUCTURE
◆	EXISTING UNDERDRAIN END CAP

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SHEET	36 OF 56 SHEETS

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STRUCTURE SCHEDULE (BASE BID)

STRUCTURE	ALIGNMENT	STATION	OFFSET	INVERT	TYPE
CAP-1	BASELINE B	600+20.3	22.7' L	OUT 647.31 (E)	UNDERDRAIN CAP
CAP-2	BASELINE B	604+19.1	22.7' L	OUT 648.88 (W)	UNDERDRAIN CAP
CAP-3	BASELINE A	500+00.0	82.5' L	OUT 646.81 (E)	UNDERDRAIN CAP
EIN-8	BASELINE B	602+19.7	22.7' L	IN 645.74 (E) IN 645.74 (W)	EXISTING INLET
IN-2	BASELINE A	502+20.4	86.4' L	IN 645.80 (W)	NEW INLET
PI-1	BASELINE A	501+95.0	82.5' L	IN 645.93 (W) OUT 645.93 (E)	POINT OF INTERSECTION

PIPE SCHEDULE (BASE BID)

LINE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	UPSTREAM INVERT	DOWNSTREAM INVERT	LENGTH (FT)	SLOPE	TYPE
UD-1	CAP-2	EIN-8	648.88	645.74	199	1.57%	4" PERFORATED PIPE
UD-2	CAP-1	EIN-8	647.31	645.74	199	0.78%	4" PERFORATED PIPE
UD-3	CAP-3	PI-1	646.81	645.93	195	0.45%	4" PERFORATED PIPE
UD-4	PI-1	IN-2	645.93	645.80	26	0.50%	4" PERFORATED PIPE

STRUCTURE SCHEDULE (ADDITIVE ALTERNATE)

STRUCTURE	ALIGNMENT	STATION	OFFSET	INVERT	TYPE
CO-1	BASELINE C	0+38.0	206.6 L	OUT 650.80 (NE)	CLEANOUT STRUCTURE
CO-2	BASELINE C	2+07.3	43.7 L	IN 649.21 (SE) OUT 649.21 (NW)	CLEANOUT STRUCTURE
EIN-4	BASELINE C	2+35.1	212.5 L	IN 646.81 (N)	EXISTING INLET
PI-2	BASELINE C	0+37.3	99.9 L	IN 650.25 (SW) OUT 650.25 (NE)	POINT OF INTERSECTION
PI-3	BASELINE C	0+42.2	79.2 L	IN 650.15 (SW) OUT 650.15 (NE)	POINT OF INTERSECTION
PI-4	BASELINE C	0+78.2	43.2 L	IN 649.90 (SE) OUT 649.90 (NW)	POINT OF INTERSECTION
PI-5	BASELINE C	1+28.5	30.6 L	IN 649.64 (SE) OUT 649.64 (NW)	POINT OF INTERSECTION
PI-6	BASELINE C	1+56.7	30.5 L	IN 649.50 (SE) OUT 649.50 (NW)	POINT OF INTERSECTION
PI-7	BASELINE C	2+46.2	90.8 L	IN 647.44 (NE) OUT 647.44 (SW)	POINT OF INTERSECTION
PI-8	BASELINE C	2+46.2	206.1 L	IN 646.86 (NE) OUT 646.86 (S)	POINT OF INTERSECTION

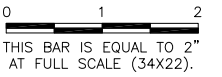
PIPE SCHEDULE (ADDITIVE ALTERNATE)

LINE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	UPSTREAM INVERT	DOWNSTREAM INVERT	LENGTH (FT)	SLOPE	TYPE
UD-5	CO-1	PI-2	650.80	650.25	107	0.51%	PVC PIPE
UD-6	PI-2	PI-3	650.25	650.15	20	0.51%	PVC PIPE
UD-7	PI-3	PI-4	650.15	649.90	49	0.51%	PVC PIPE
UD-8	PI-4	PI-5	649.90	649.64	52	0.51%	PVC PIPE
UD-9	PI-5	PI-6	649.64	649.50	27	0.51%	PVC PIPE
UD-10	PI-6	CO-2	649.50	649.21	51	0.57%	PVC PIPE
UD-11	CO-2	PI-7	649.21	647.44	60	2.97%	PVC PIPE
UD-12	PI-7	PI-8	647.44	646.86	115	0.50%	PVC PIPE
UD-13	PI-8	EIN-4	646.86	646.81	9	0.50%	PVC PIPE

PE096

REVISIONS

NUMBER	BY	DATE



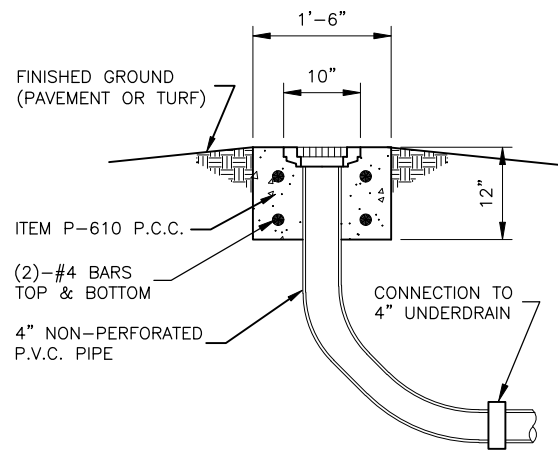
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 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 UNDERDRAIN SCHEDULE**

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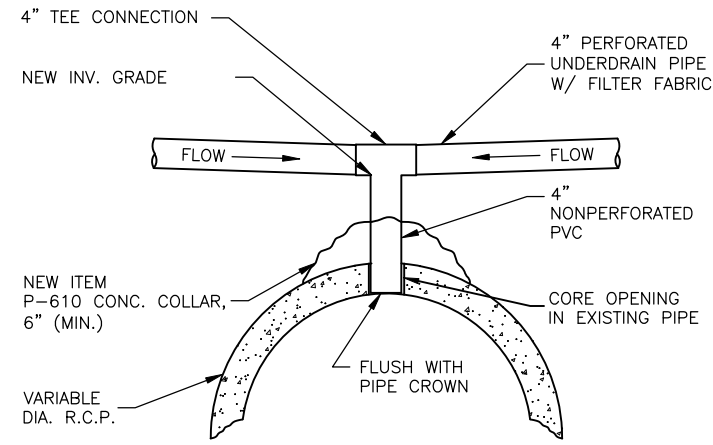
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CHECKED BY:	TJH
APPROVED BY:	CET
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JOB No:	12061-03
IL. PROJ. NO. PIA-4361 AIP PROJ. NO. 3-17-0080-XX	
SHEET 37 OF 56 SHEETS	



SIDE VIEW

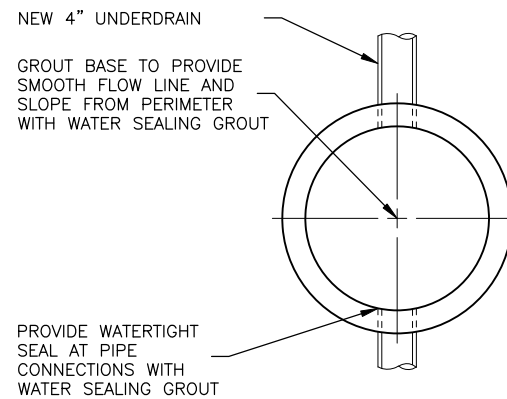
UNDERDRAIN CLEAN-OUT DETAIL (TYPE 1)

N.T.S.



UNDERDRAIN DIRECT TOP CONNECTION DETAIL

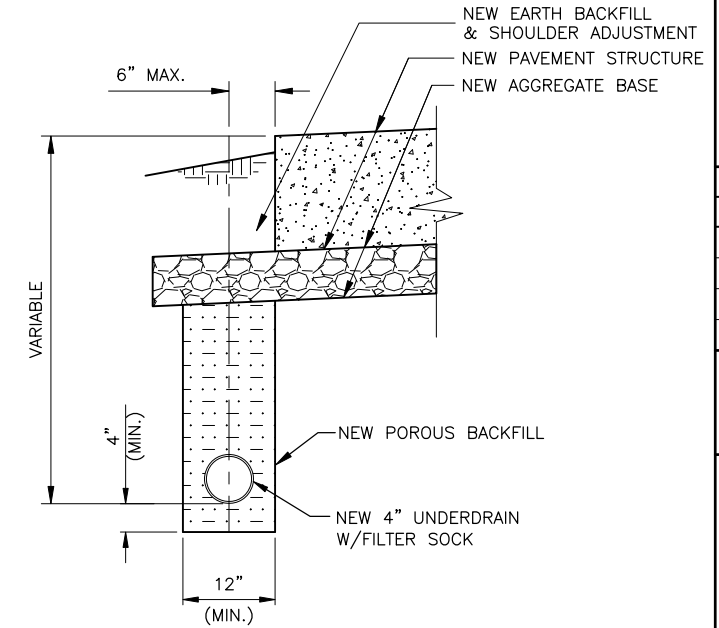
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PLAN

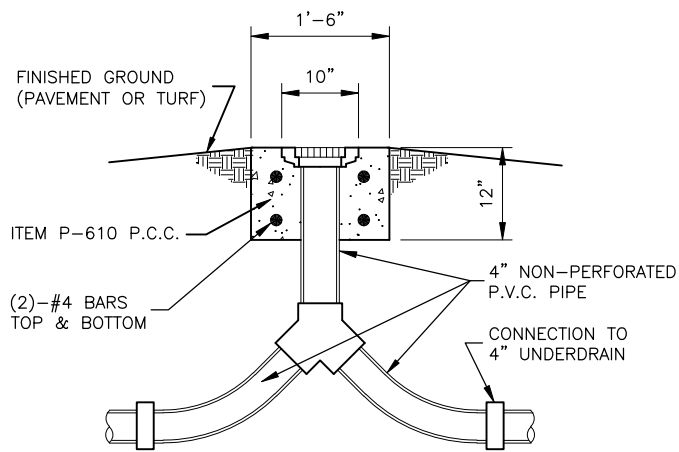
UNDERDRAIN CLEAN-OUT DETAIL

N.T.S.



TYPICAL UNDERDRAIN DETAIL PAVEMENT EDGE

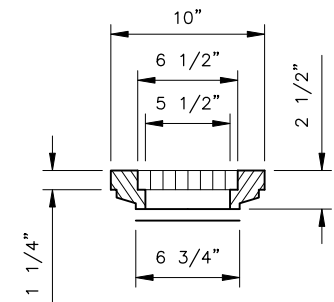
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SIDE VIEW

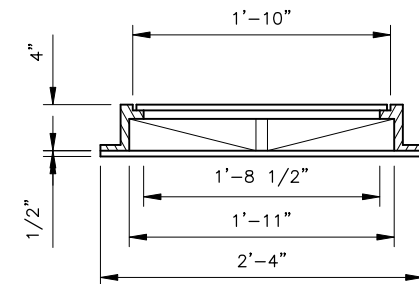
UNDERDRAIN CLEAN-OUT DETAIL (TYPE 2)

N.T.S.



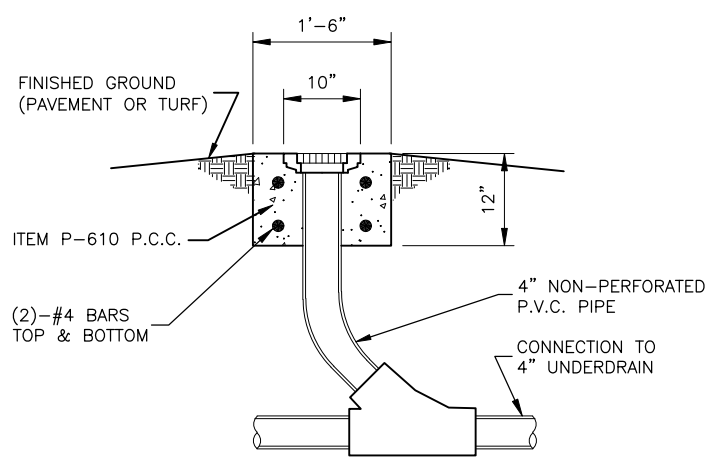
FRAME & COVER

CAST IRON FRAME & COVER (SIMILAR TO NEENAH R-6013 OR APPROVED EQUAL)



FRAME & LID

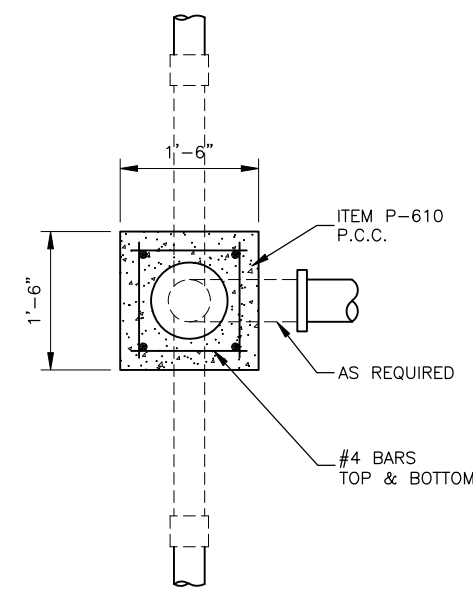
CAST IRON FRAME & LID (SIMILAR TO NEENAH R-1690-A HEAVY DUTY OR APPROVED EQUAL)



SIDE VIEW

UNDERDRAIN CLEAN-OUT DETAIL (TYPE 3)

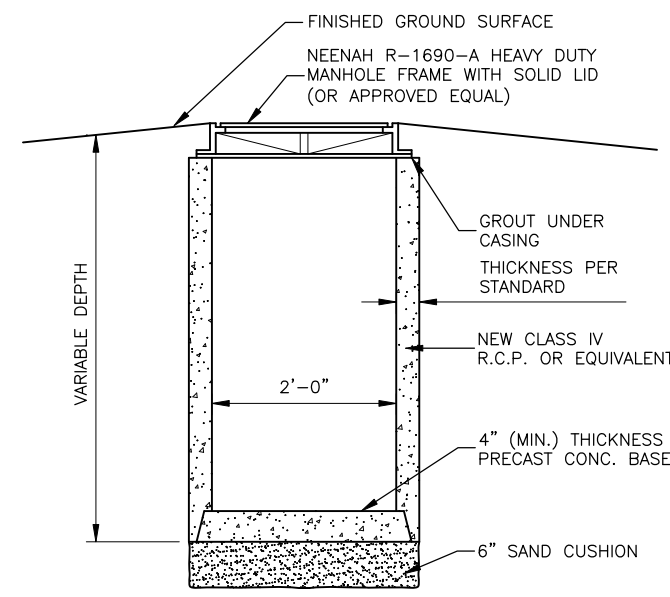
N.T.S.



PLAN

UNDERDRAIN CLEAN-OUT DETAIL

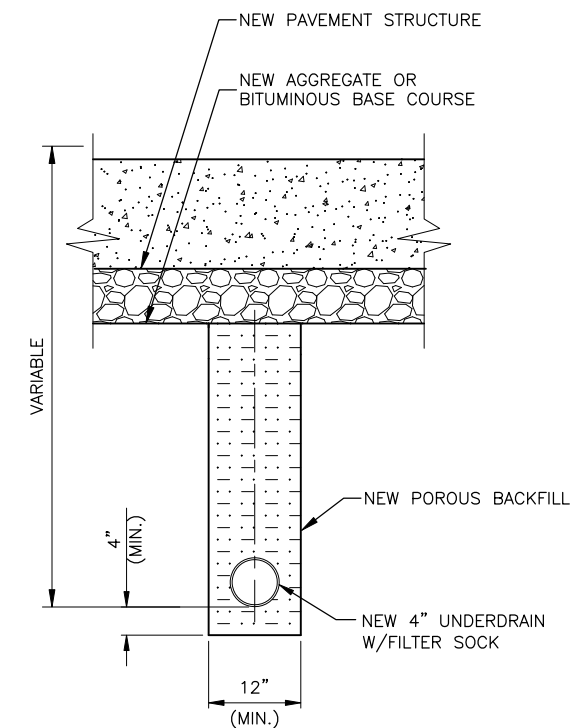
N.T.S.



SIDE VIEW

UNDERDRAIN COLLECTION STRUCTURE DETAIL

N.T.S.



TYPICAL UNDERDRAIN DETAIL BELOW PAVEMENT

N.T.S.

UNDERDRAIN NOTE

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

PE096

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
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**RECONSTRUCT GENERAL AVIATION APRON
 UNDERDRAIN DETAILS**

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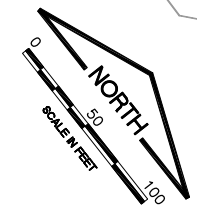
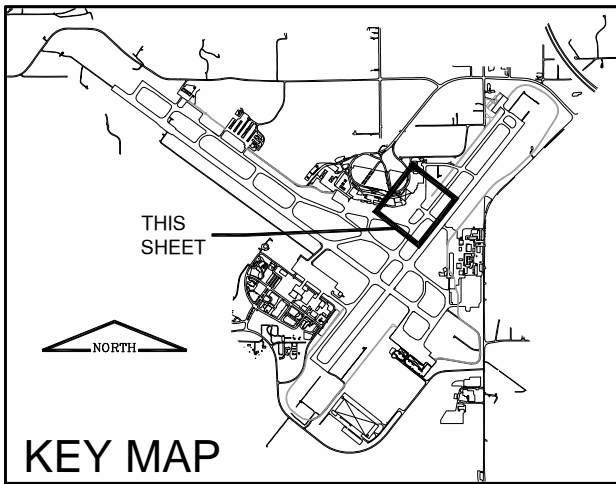
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CHECKED BY:	TJH
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DATE:	APRIL 26, 2013
JOB No:	12061-03
IL PROJ. NO. PIA-4361	
AIP PROJ. NO. 3-17-0080-XX	
SHEET	38 OF 56 SHEETS

PE096

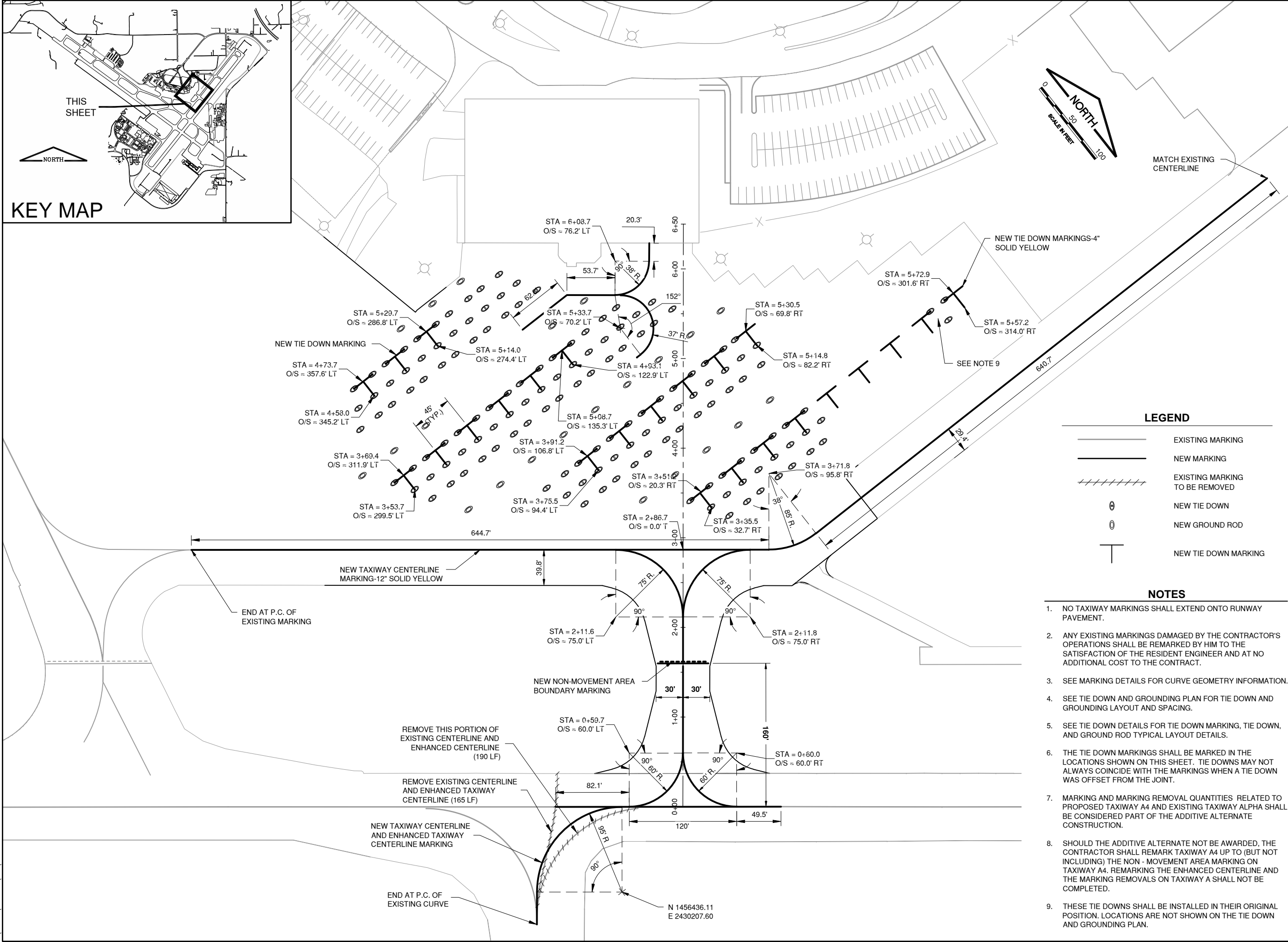
REVISIONS

NUMBER	BY	DATE

0 1 2
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KEY MAP



LEGEND

- EXISTING MARKING
- NEW MARKING
- EXISTING MARKING TO BE REMOVED
- NEW TIE DOWN
- NEW GROUND ROD
- NEW TIE DOWN MARKING

NOTES

1. NO TAXIWAY MARKINGS SHALL EXTEND ONTO RUNWAY PAVEMENT.
2. ANY EXISTING MARKINGS DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REMARKED BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AT NO ADDITIONAL COST TO THE CONTRACT.
3. SEE MARKING DETAILS FOR CURVE GEOMETRY INFORMATION.
4. SEE TIE DOWN AND GROUNDING PLAN FOR TIE DOWN AND GROUNDING LAYOUT AND SPACING.
5. SEE TIE DOWN DETAILS FOR TIE DOWN MARKING, TIE DOWN, AND GROUND ROD TYPICAL LAYOUT DETAILS.
6. THE TIE DOWN MARKINGS SHALL BE MARKED IN THE LOCATIONS SHOWN ON THIS SHEET. TIE DOWNS MAY NOT ALWAYS COINCIDE WITH THE MARKINGS WHEN A TIE DOWN WAS OFFSET FROM THE JOINT.
7. MARKING AND MARKING REMOVAL QUANTITIES RELATED TO PROPOSED TAXIWAY A4 AND EXISTING TAXIWAY ALPHA SHALL BE CONSIDERED PART OF THE ADDITIVE ALTERNATE CONSTRUCTION.
8. SHOULD THE ADDITIVE ALTERNATE NOT BE AWARDED, THE CONTRACTOR SHALL REMARK TAXIWAY A4 UP TO (BUT NOT INCLUDING) THE NON - MOVEMENT AREA MARKING ON TAXIWAY A4. REMARKING THE ENHANCED CENTERLINE AND THE MARKING REMOVALS ON TAXIWAY A SHALL NOT BE COMPLETED.
9. THESE TIE DOWNS SHALL BE INSTALLED IN THEIR ORIGINAL POSITION. LOCATIONS ARE NOT SHOWN ON THE TIE DOWN AND GROUNDING PLAN.

**GENERAL WAYNE A. DOWNING
 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 MARKING PLAN**

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AIP PROJ. NO. 3-17-0080-XX	
SHEET	39 OF 56 SHEETS

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PE096

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
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 PEORIA, ILLINOIS**

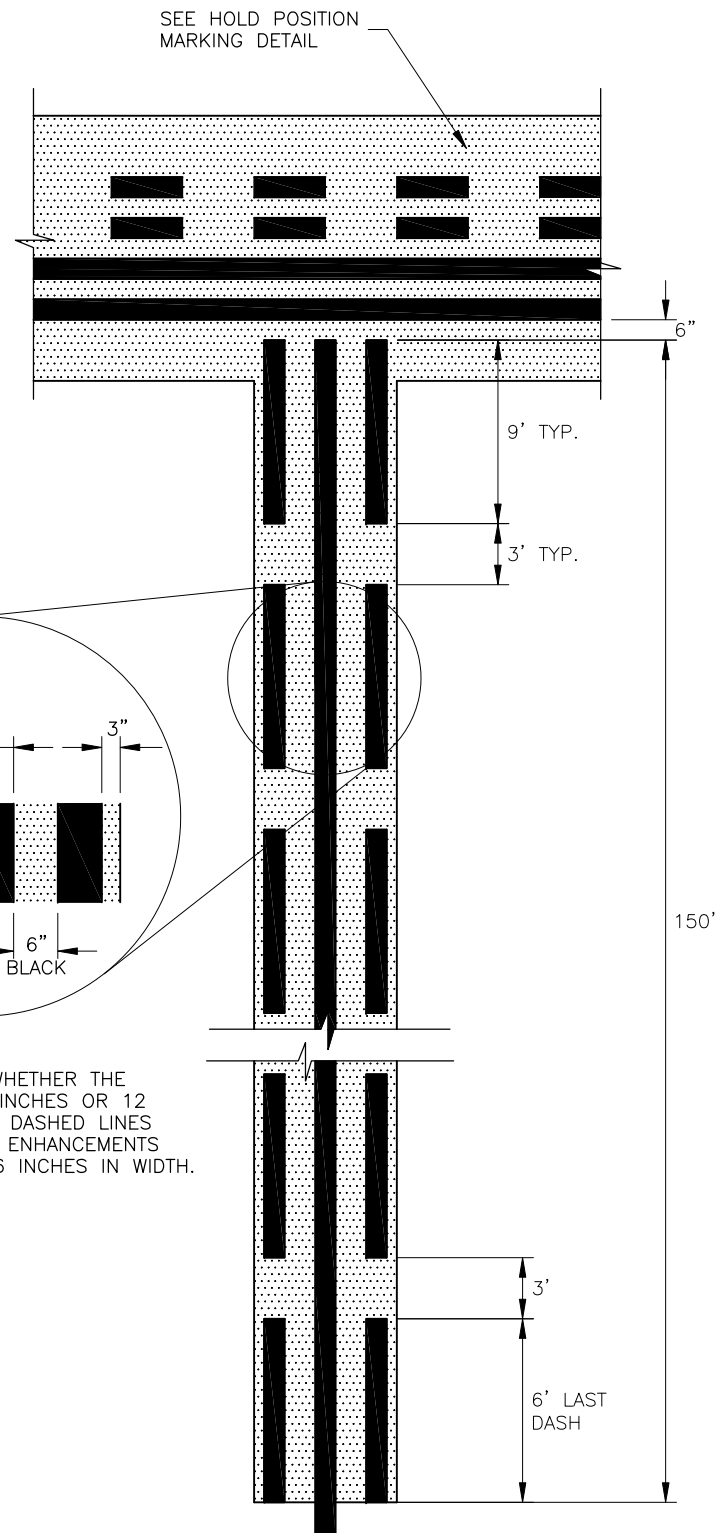
**RECONSTRUCT GENERAL AVIATION APRON
 MARKING DETAILS**

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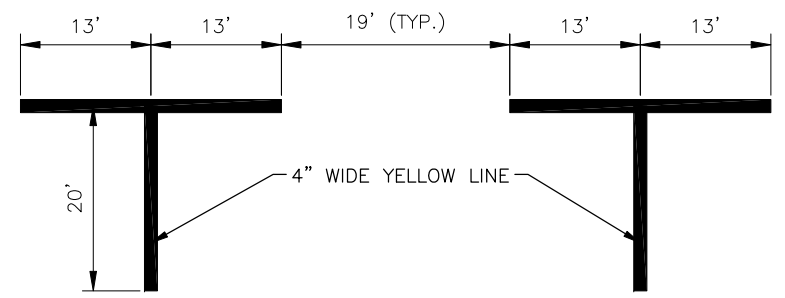
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APPROVED BY:	CET
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JOB No:	12061-03
IL PROJ. NO.	PIA-4361
AIP PROJ. NO.	3-17-0080-XX
SHEET	40 OF 56 SHEETS



**ENHANCED TAXIWAY
 CENTERLINE MARKING**
 N.T.S.

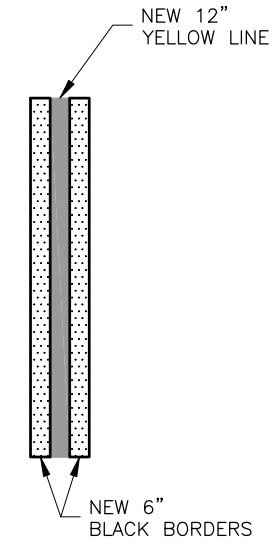
MARKING NOTE

1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER.
2. REFLECTIVE BEADS ARE NOT TO BE PLACED IN THE BLACK BORDER.

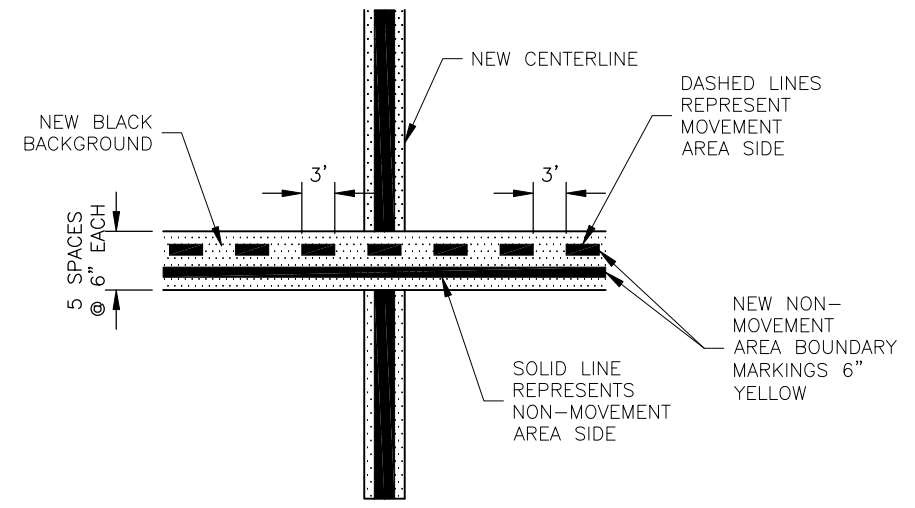


TIE DOWN DETAIL
 N.T.S.

- NOTE:**
1. ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE MEDIA & 6" BLACK BORDER.
 2. BLACK BORDER SHALL NOT REQUIRE REFLECTIVE MEDIA.



**TAXIWAY
 CENTERLINE
 CONTINUOUS
 (YELLOW)**
 N.T.S.

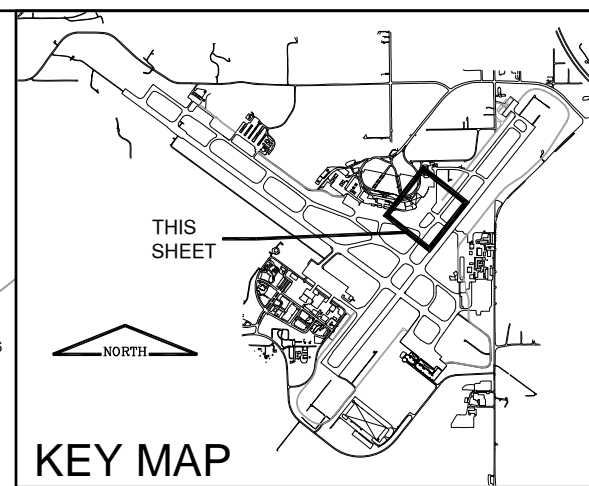
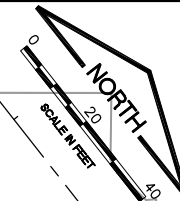


**NON-MOVEMENT AREA
 BOUNDARY MARKING**
 N.T.S.

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NOTES:

1. SEE TIE DOWN DETAILS FOR TIE DOWN MARKING, TIE DOWN, AND GROUND ROD TYPICAL LAYOUT DETAILS, AND NOTES REGARDING TIE DOWN CONSTRUCTION.
2. SEE MARKING PLAN FOR MARKING LAYOUT
3. THREE TIE DOWNS NOT SHOWN ON THIS SHEET SHALL BE PLACED IN THEIR ORIGINAL POSITIONS WHICH SHALL BE MEASURED PRIOR TO THE PANEL REMOVAL AND REPLACEMENT SHOWN ON THE EXISTING CONDITIONS AND REMOVALS SHEET.



FILE: TIE DOWN AND GROUNDING PLAN 1.d
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 9:39 PM

BASE_PROP_GEO
 PIA-Base-Existing
 KEYMAP
 BASE_PROP_JOINT
 1206103-VF2D

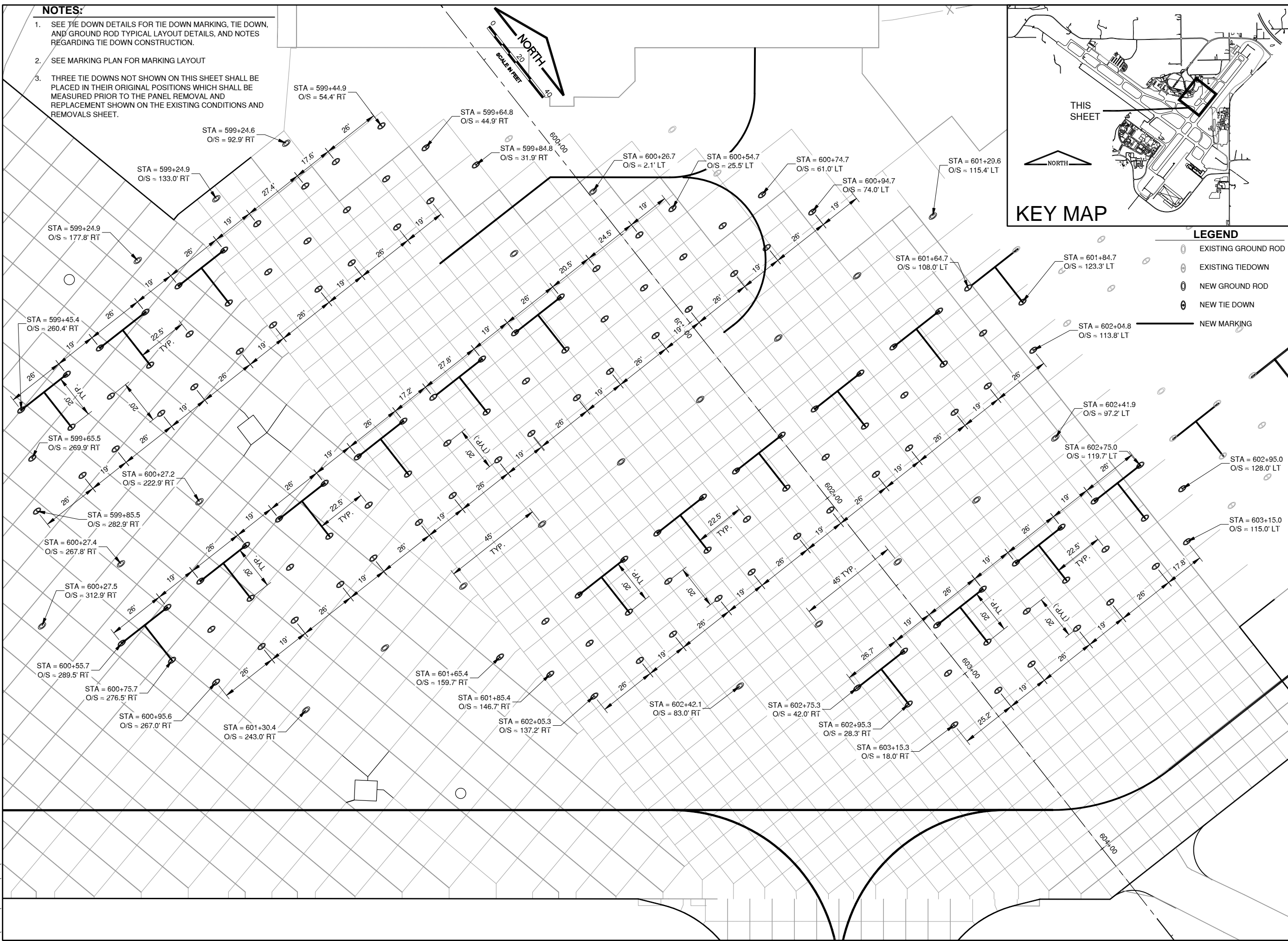
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REVISIONS		
NUMBER	BY	DATE



LEGEND

- EXISTING GROUND ROD
- EXISTING TIEDOWN
- NEW GROUND ROD
- NEW TIE DOWN
- NEW MARKING



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 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 TIE DOWN AND GROUNDING PLAN**

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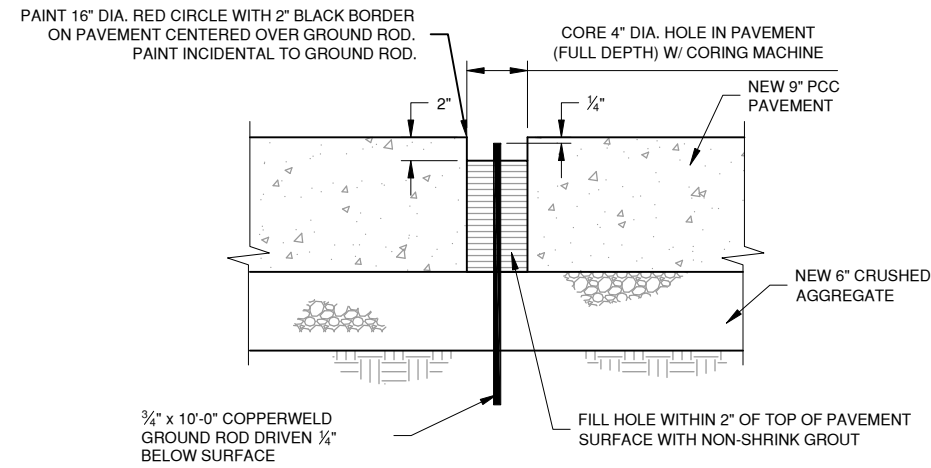
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JOB No:	12061-03
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AIP PROJ. NO. 3-17-0080-XX	
SHEET	41 OF 56 SHEETS

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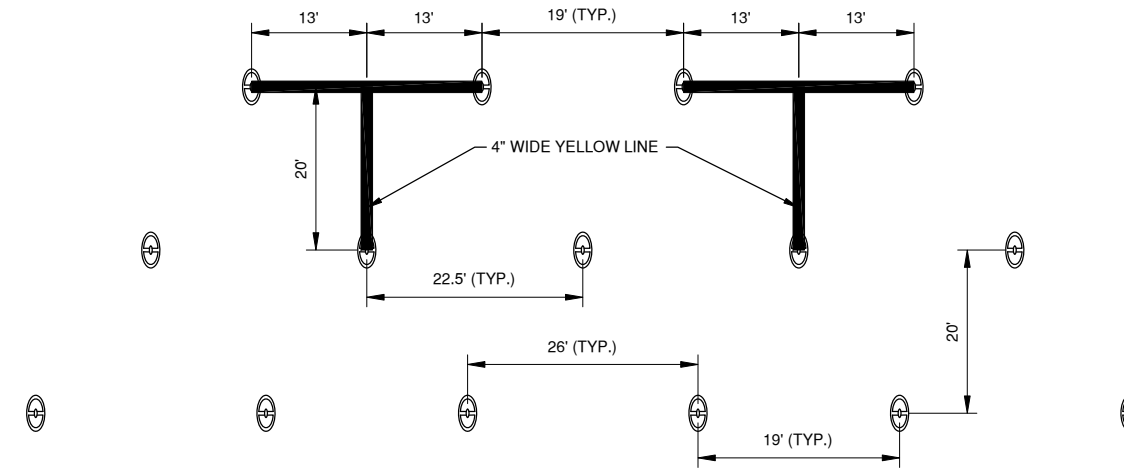
REVISIONS		
NUMBER	BY	DATE

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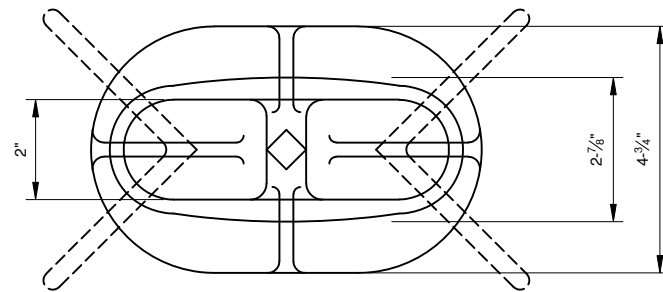
GROUNDING POINT DETAIL

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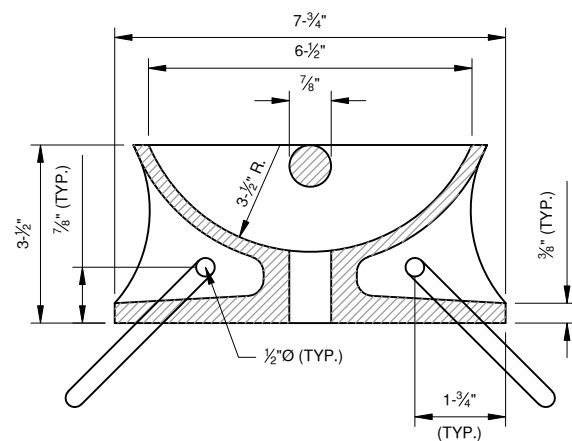


TIE DOWN DETAIL

N.T.S.



PLAN



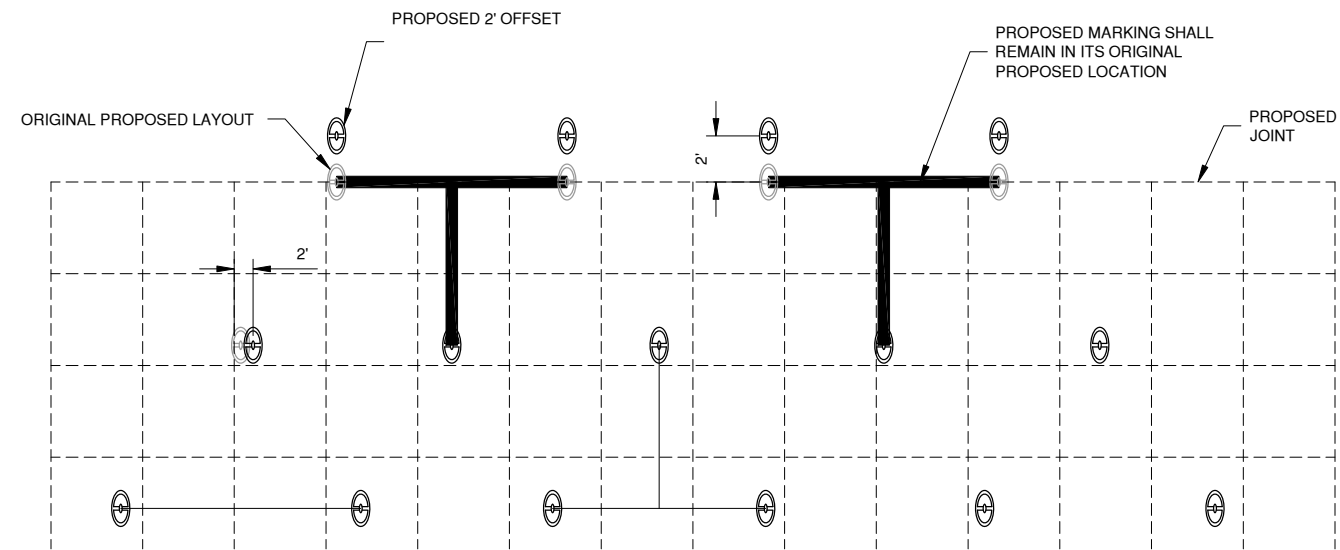
SECTION

MOORING EYE/TIE DOWN DETAIL

N.T.S.

NOTES:

1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
2. ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45°. TWO ANCHOR BARS PER MOORING EYE ARE REQUIRED.
3. MOORING EYES SHALL BE INSTALLED DURING THE PAVING OPERATION AND SHALL NOT BE CORED AFTER PAVING.
4. THE PLACEMENT OF THE TIE DOWNS SHALL BE AN INTEGRAL PART OF THE PAVING PROCESS. THE CONTRACTOR SHALL IDENTIFY ALL TIE DOWNS AND GROUND RODS THAT FALL WITHIN 2' OF A JOINT PRIOR TO BEGINNING PAVING OPERATIONS.



TIE DOWN JOINT DETAIL

N.T.S.

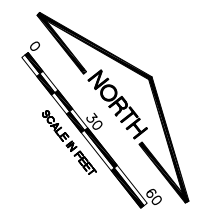
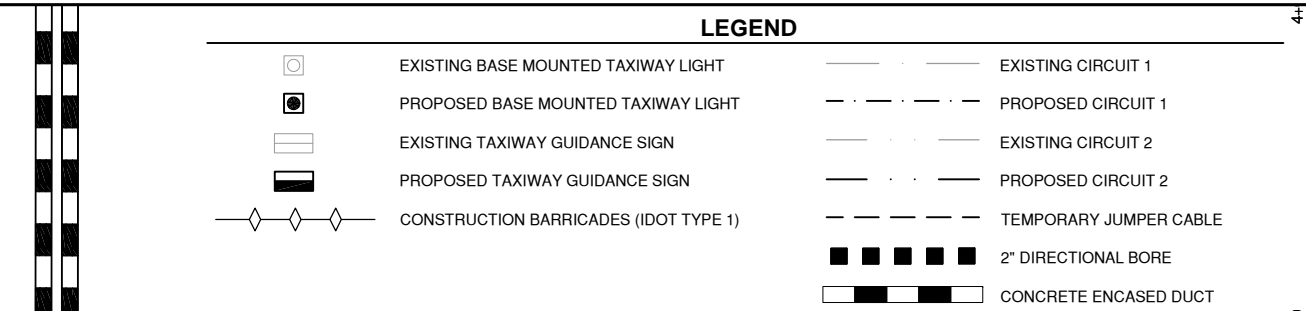
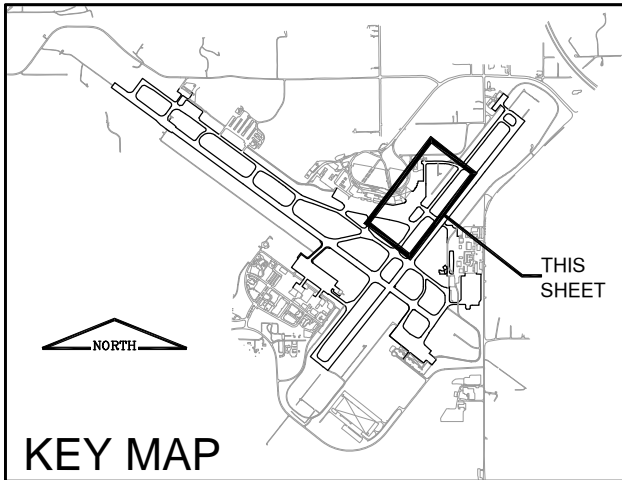
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**RECONSTRUCT GENERAL AVIATION APRON
 TIE DOWN DETAILS**

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SHEET 42 OF 56 SHEETS	



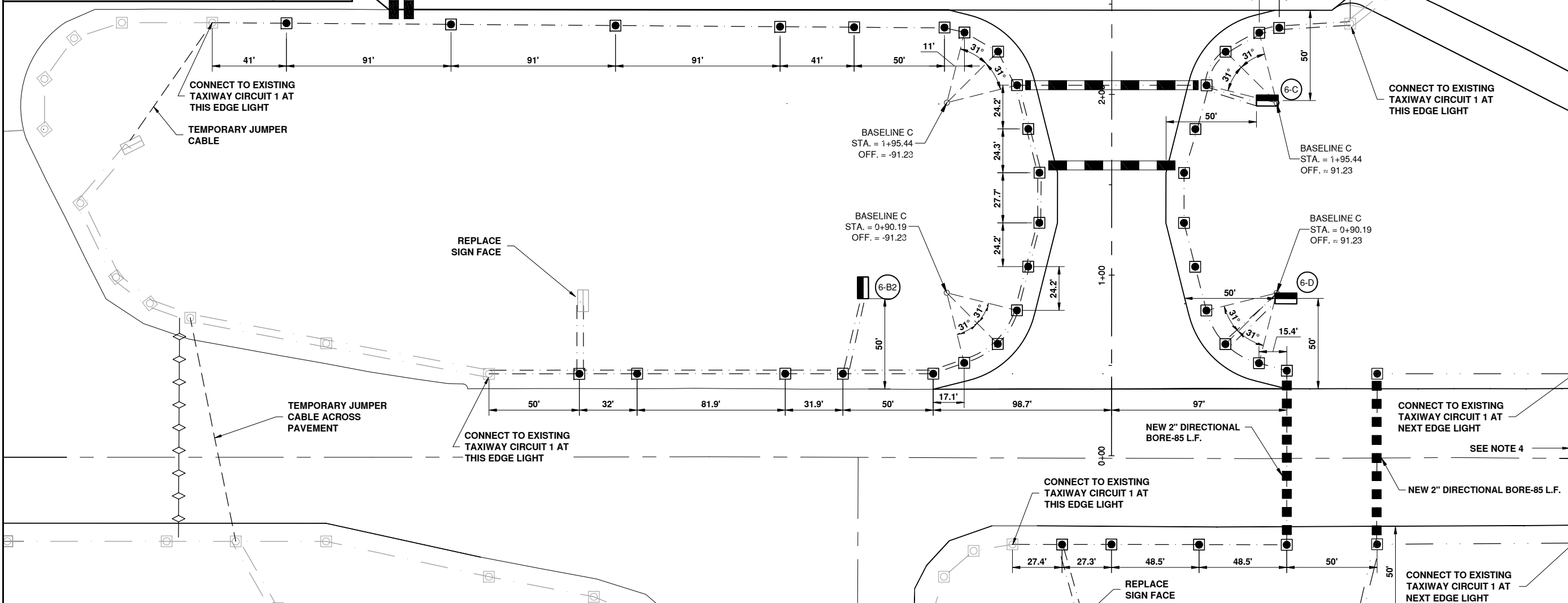
FILE: LIGHTING PLAN 1.dwg
 UPDATE BY: TJ Heaviesides
 PLOT DATE: 5/1/2013 9:39 PM

PIA-Base-Existing
 BASE_PROP_GEO
 KEYMAP
 1206103-E-AL
 1106107-E-AL

PE096

REVISIONS		
NUMBER	BY	DATE

0 1 2
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**RECONSTRUCT GENERAL AVIATION APRON
 LIGHTING PLAN**

SIGNAGE SCHEDULE

SIGN #	SIDE	NEW SIGN LEGEND	WHITE ON RED BACKGROUND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	NUMBER OF CHARACTERS	NEW POWER CIRCUIT	NOTES
6-B	NE SW	A A A4 →			A4 →	A	4	CKT 1	NE FACE TO REMAIN REPLACE SW FACE
6-J	NE SW	A ← A4 22↑			← A4 22↑	A	4	CKT 1	REPLACE NE FACE SW FACE TO REMAIN
6-C	NW	A4↑			A4↑	A4	3	CKT 1	RELOCATED SIGN
6-D	NW SE	A4 ← A → A4			← A →	A4 A4	5	CKT 1	RELOCATED SIGN
6-B2	NE SW	A A ← A4			← A4	A A	4	CKT 1	NEW SIGN
6-J2	NE SW	A A4 → A			A4 →	A A	4	CKT 2	NEW SIGN

NOTES:

- NEW LIGHTS SHALL BE INSTALLED 10' FROM THE EDGE OF PAVEMENT WHEN MEASURED PERPENDICULAR TO THE CENTER LINE.
- SEE CABLE AND CONDUIT LAYOUT PLAN FOR COUNTERPOISE LAYOUT.
- THE CONTRACTOR SHALL MAINTAIN THE PORTIONS OF THE EXISTING TAXIWAY EDGE CIRCUITS 1 AND 2 THAT ARE TO REMAIN OPEN TO AIRCRAFT TRAFFIC THROUGH THE USE OF TEMPORARY JUMPER CABLES. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE TEMPORARY JUMPER CABLE ACROSS TAXIWAY A AT EDGE LIGHTS NEAREST TO THE PLACED BARRICADES AND WITHIN THE LIMITS OF THE CLOSED TAXIWAY.
- TEMPORARY JUMPER CABLE TO BE INSTALLED IN RIGID METAL CONDUIT PER AC 150/5370-2F ACROSS THE PAVEMENT AND BETWEEN THE EXISTING LIGHTS AS SHOWN. TEMPORARY JUMPER CABLES SHALL BE INSTALLED PRIOR TO REMOVING EXISTING LIGHTS TO MAINTAIN SERVICE TO THE REST OF THE TAXIWAY CIRCUIT. **COST TO INSTALL, MAINTAIN, & REMOVE TEMPORARY JUMPER CABLES SHALL BE INCIDENTAL TO THE PROJECT.**

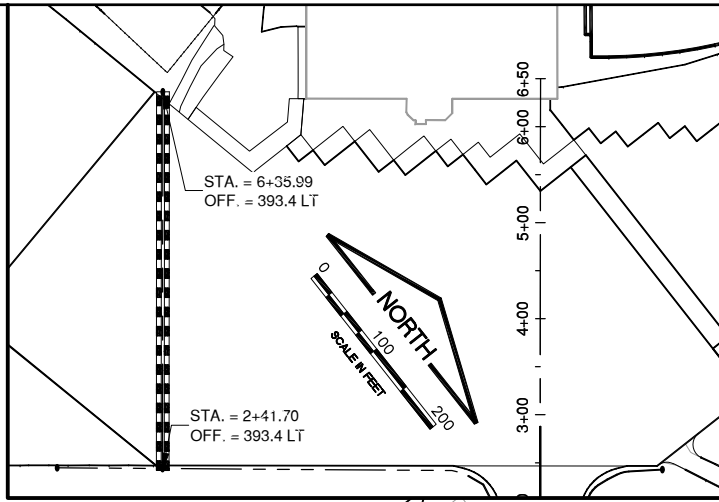
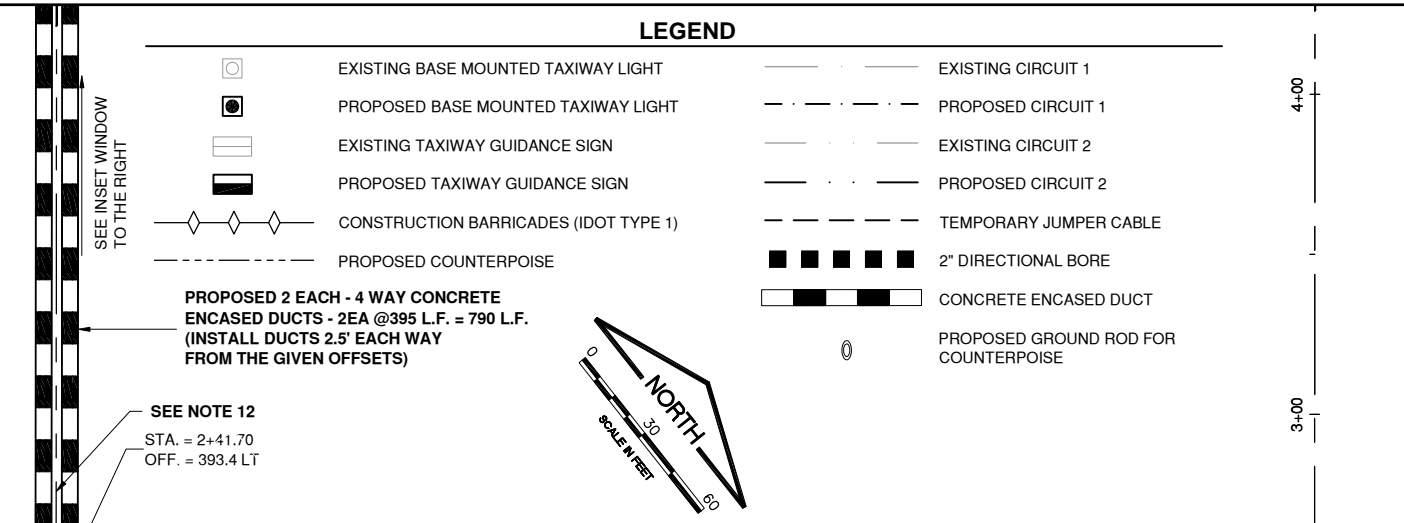
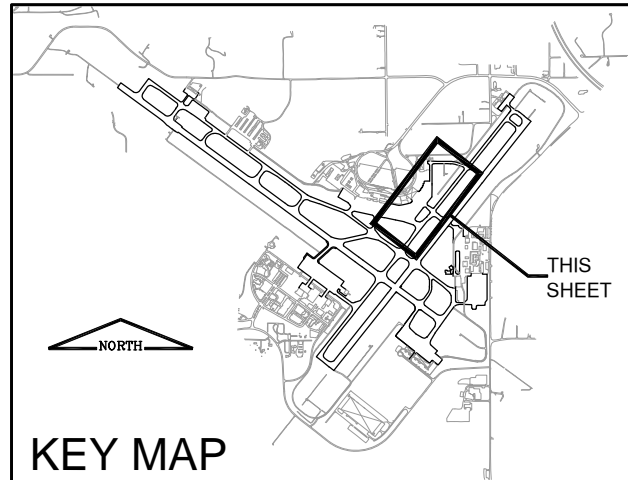
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 CHECKED BY: TJH
 APPROVED BY: CET
 DATE: APRIL 26, 2013
 JOB No: 12061-03
 IL. PROJ. NO. PIA-4361
 AIP PROJ. NO. 3-17-0080-XX
 SHEET 43 OF 56 SHEETS

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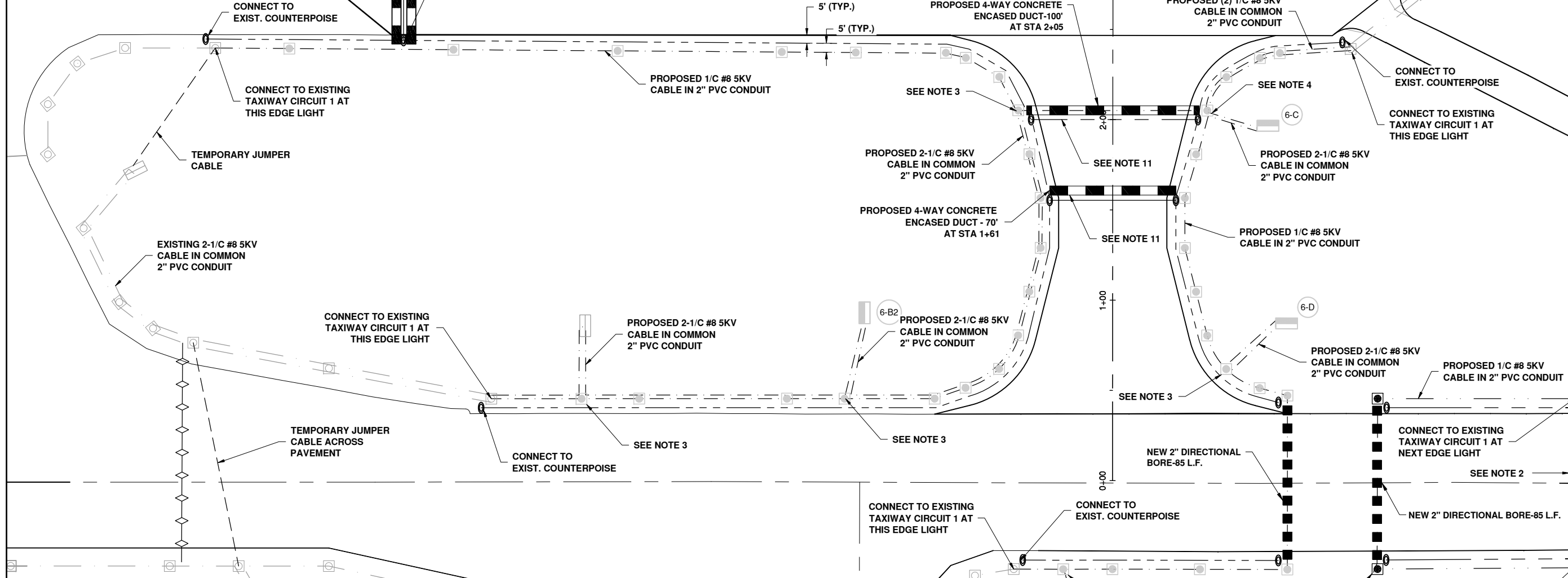
FILE: CONDUIT PLAN 1.dwg
UPDATE BY: TJ Heavisides
PLOT DATE: 5/1/2013 9:39 PM

PIA-Base-Existing
BASE_PROP_GEO
KEYMAP
1206103-E-AL
1106107-E-AL

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REVISIONS		
NUMBER	BY	DATE

0 1 2
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NOTES:

- THE CONTRACTOR SHALL MAINTAIN THE PORTIONS OF THE EXISTING TAXIWAY EDGE CIRCUITS 1 AND 2 THAT ARE TO REMAIN OPEN TO AIRCRAFT TRAFFIC THROUGH THE USE OF TEMPORARY JUMPER CABLES. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE TEMPORARY JUMPER CABLE ACROSS TAXIWAY A AT EDGE LIGHTS NEAREST TO THE PLACED BARRICADES AND WITHIN THE LIMITS OF THE CLOSED TAXIWAY.
- THIS LIGHT BASE CAN SHALL HAVE (3) 2" CONDUIT HUBS.
- THIS LIGHT BASE CAN SHALL HAVE (4) 2" CONDUIT HUBS.
- NEW 4-WAY CONCRETE ENCASED DUCT SHALL CONSIST OF (4) 2" PVC CONDUITS.
- NEW 2" DIRECTIONAL BORE SHALL BE CONNECTED TO NEW 2" PVC CONDUIT FOR EDGE LIGHT CIRCUITS WITH A 2" PVC COUPLING.
- THE ENDS OF ALL UNUSED CONDUITS SHALL BE PLUGGED FOR FUTURE USE.
- NEW 2 - 4 WAY CONCRETE ENCASED DUCT SHALL BE INSTALLED AT THE MINIMUM DEPTH SHOWN ON THE DUCT BANK DETAILS SHEET. THE CONTRACTOR SHALL USE CAUTION NOT TO CONFLICT WITH EXISTING UTILITIES OR THE PROPOSED UNDERDRAIN.
- NEW CONCRETE ENCASED DUCT AND DIRECTIONAL BORE SHALL EXTEND 5' BEYOND THE EDGE OF PAVEMENT.
- NEW IN-PAVEMENT DUCT MARKERS SHALL BE PLACED IN THE PROPOSED AND EXISTING PAVEMENT ON BOTH ENDS OF THE EXISTING DUCT.
- INSTALL COUNTERPOISE OVER PROPOSED CONCRETE ENCASED DUCT AS SHOWN ON THE DUCT BANK DETAILS SHEET.
- COUNTERPOISE FOR THE 2 - 4 WAY DUCTS SHALL BE INSTALLED OVER EACH DUCT.

**GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
CABLE AND CONDUIT LAYOUT PLAN**

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JOB No:	12061-03
IL PROJ. NO. PIA-4361 AIP PROJ. NO. 3-17-0080-XX	

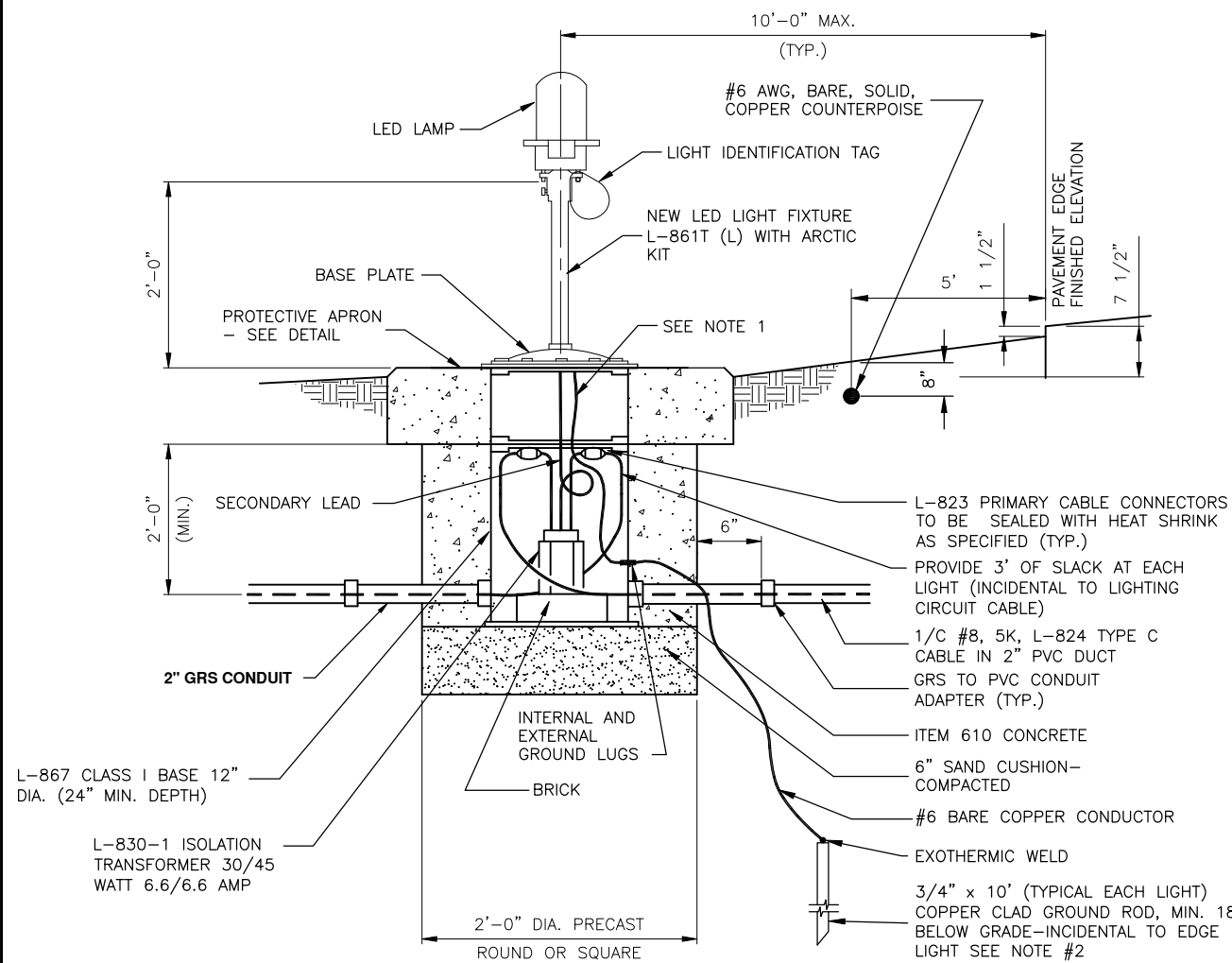
SHEET 44 OF 56 SHEETS

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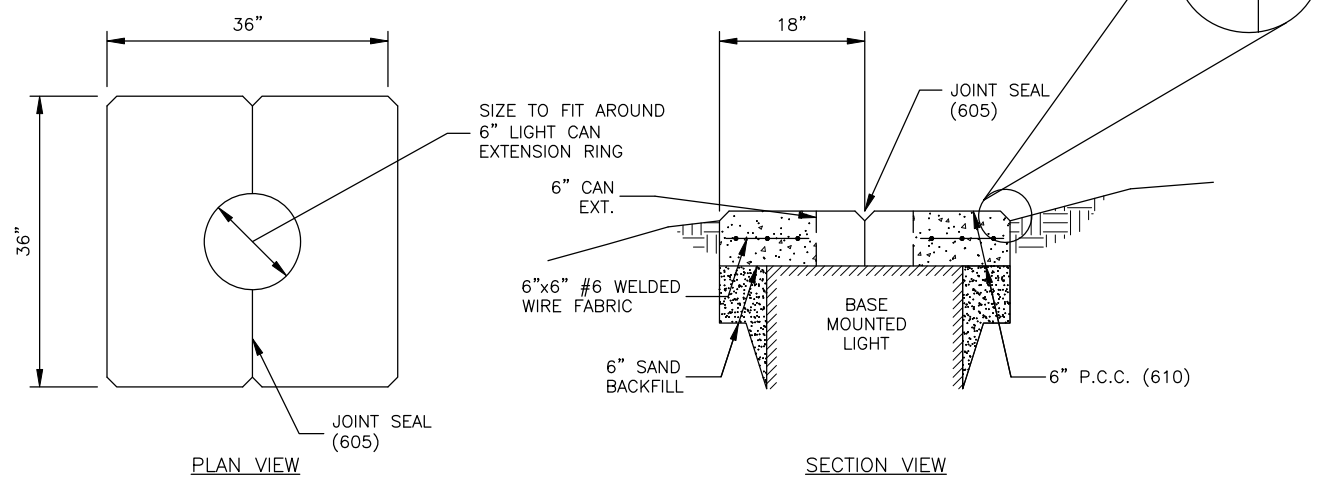
REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).



BASE MOUNTED EDGE LIGHTS
 N.T.S.

L-867 CLASS I BASE 12" DIA. (24" MIN. DEPTH)
 L-830-1 ISOLATION TRANSFORMER 30/45 WATT 6.6/6.6 AMP



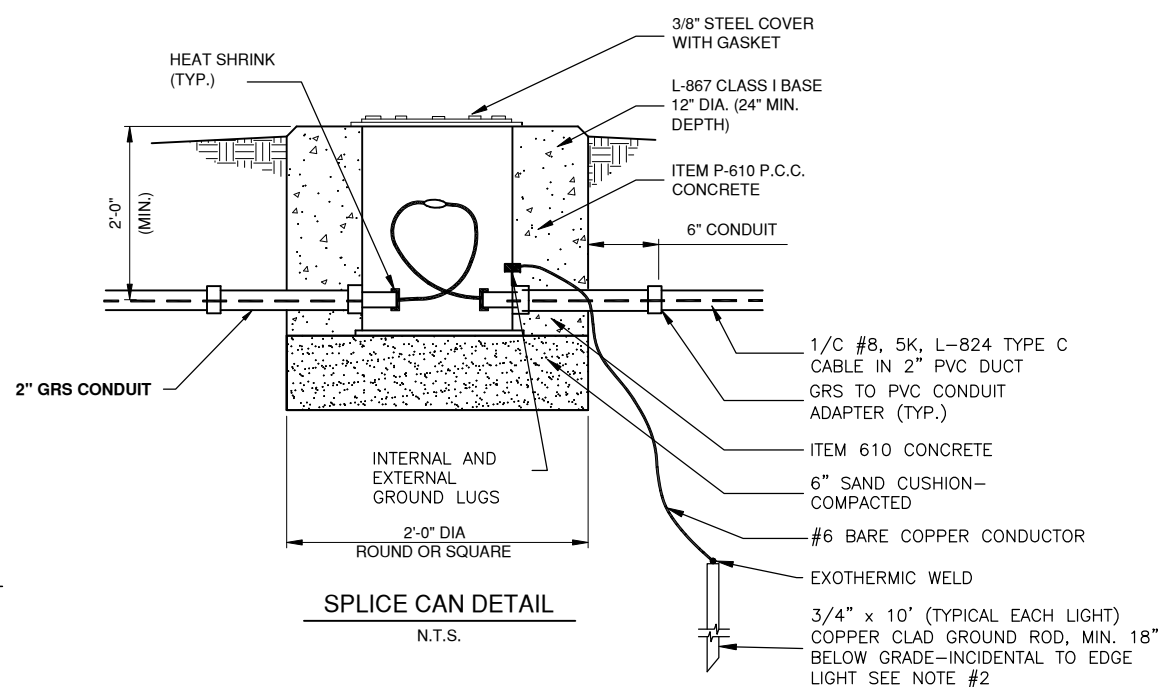
PREFORMED PROTECTIVE APRON DETAIL
 N.T.S.

LIGHT NOTES

1. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.

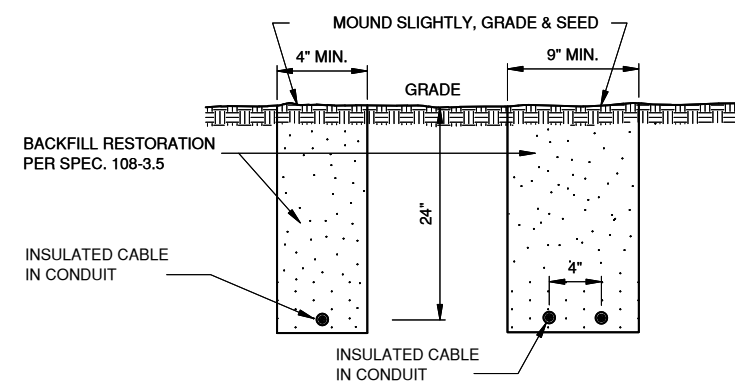
NOTES

1. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
2. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
3. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.



SPLICE CAN DETAIL
 N.T.S.

UNIT DUCT SHALL BE TERMINATED AT THE L-823 CONNECTORS AND SEALED WITH HEAT SHRINK AS SPECIFIED.



TRENCH DETAIL
 N.T.S.

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PEORIA, ILLINOIS
RECONSTRUCT GENERAL AVIATION APRON
ELECTRICAL DETAILS 1

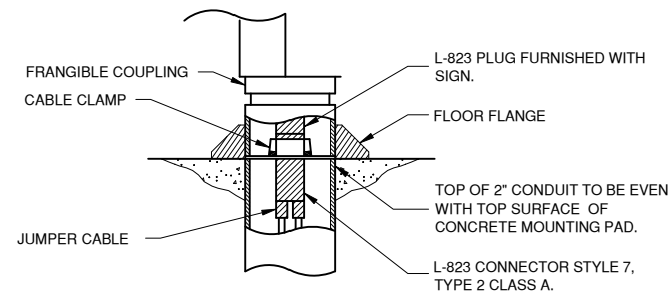
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SHEET 45 OF 56 SHEETS	

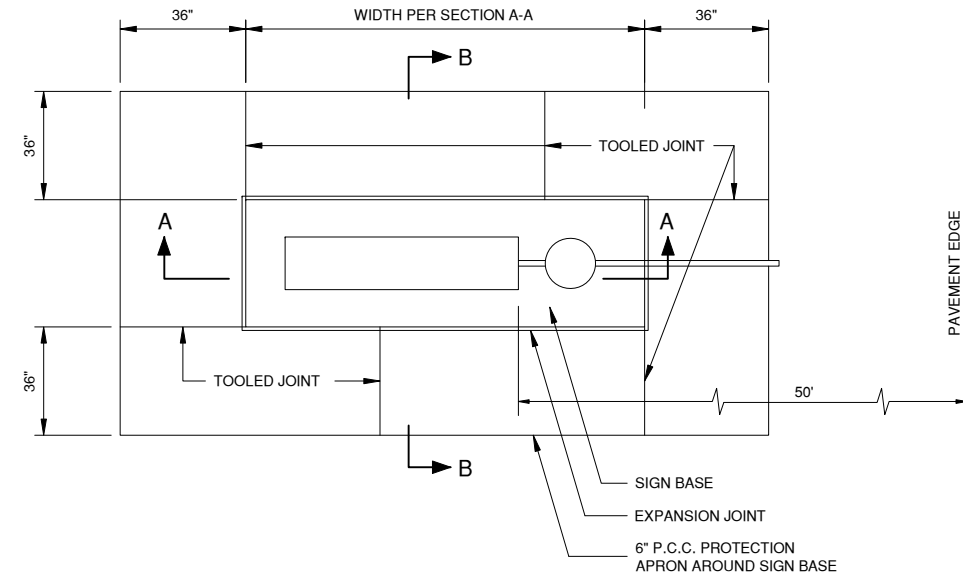
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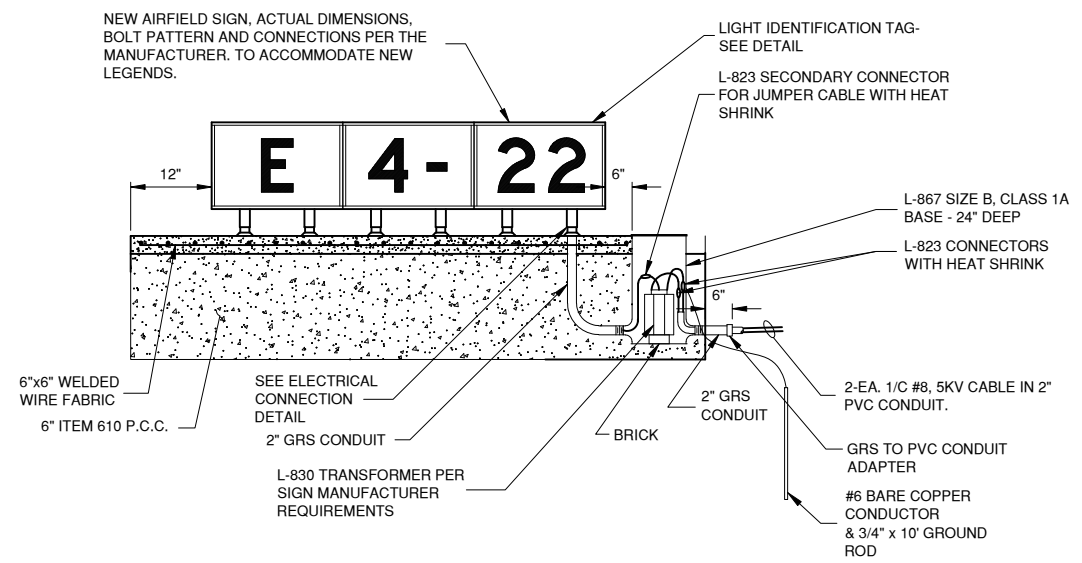
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 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



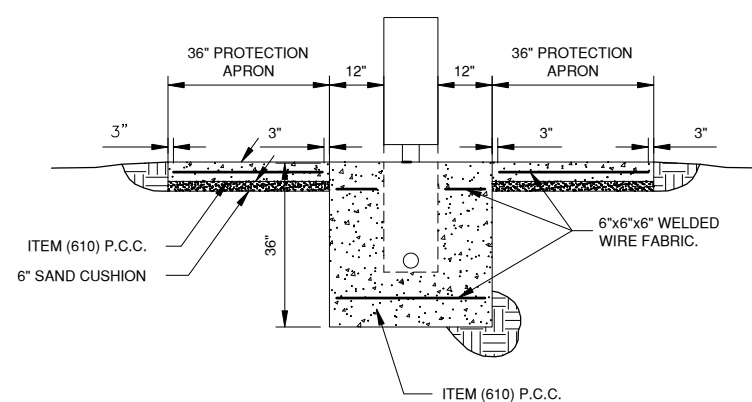
ELECTRICAL CONNECTION DETAIL
N.T.S.



PLAN

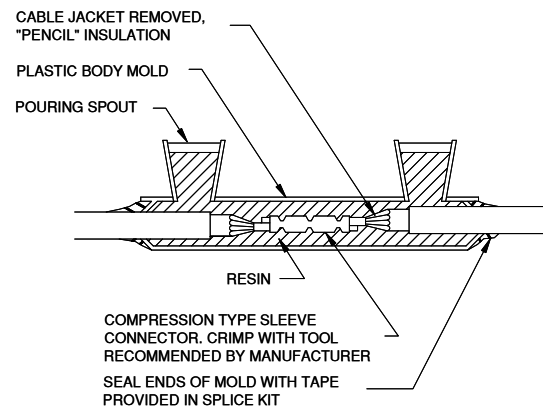


SECTION A-A



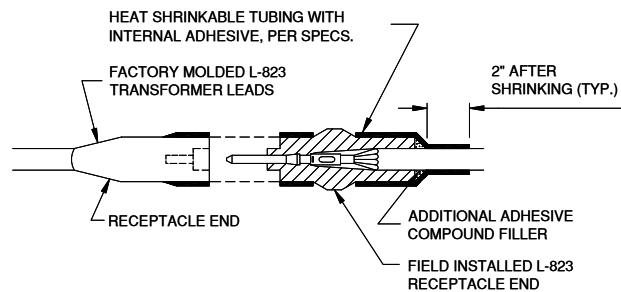
SECTION B-B

L-858 AIRFIELD SIGN DETAILS
N.T.S.



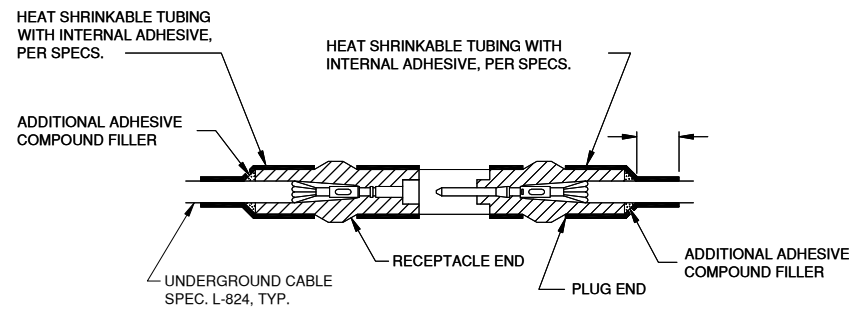
FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTIONS.

TYPE A



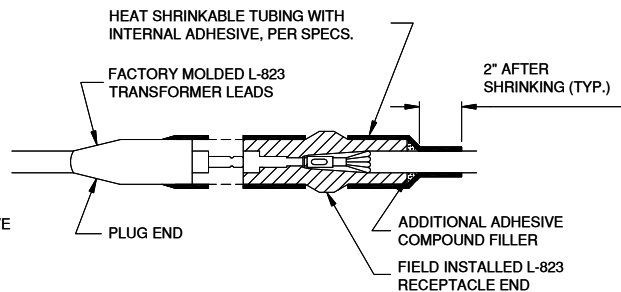
FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

TYPE C



NOT TO BE USED IN THIS PROJECT UNLESS OTHERWISE DIRECTED BY ENGINEER

TYPE B



FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

TYPE D

CABLE SPLICES

N.T.S.
NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

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RECONSTRUCT GENERAL AVIATION APRON

ELECTRICAL DETAILS 2

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0 1 2
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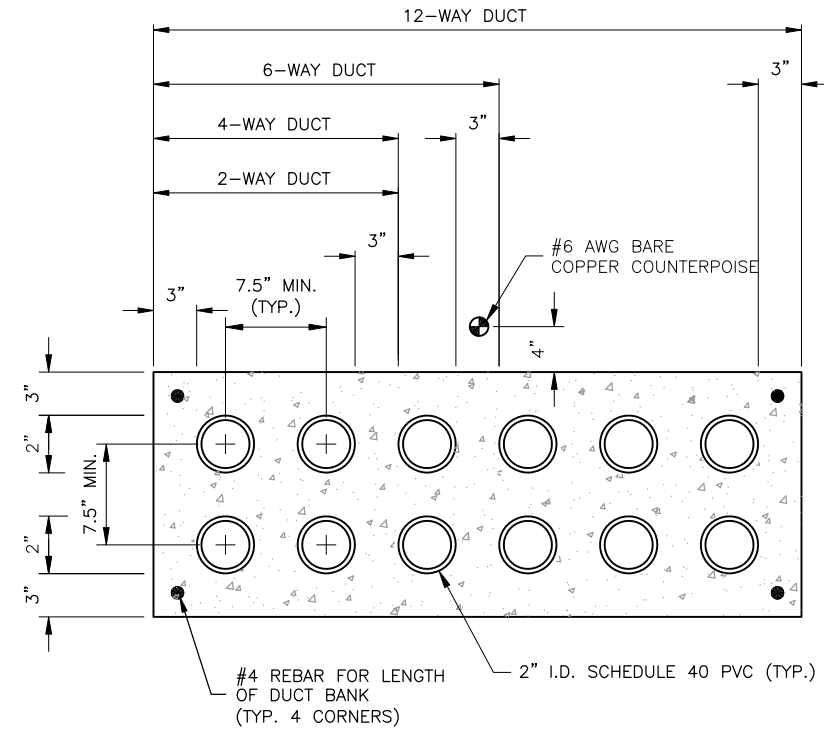
RECONSTRUCT GENERAL AVIATION APRON
 DUCT BANK DETAILS

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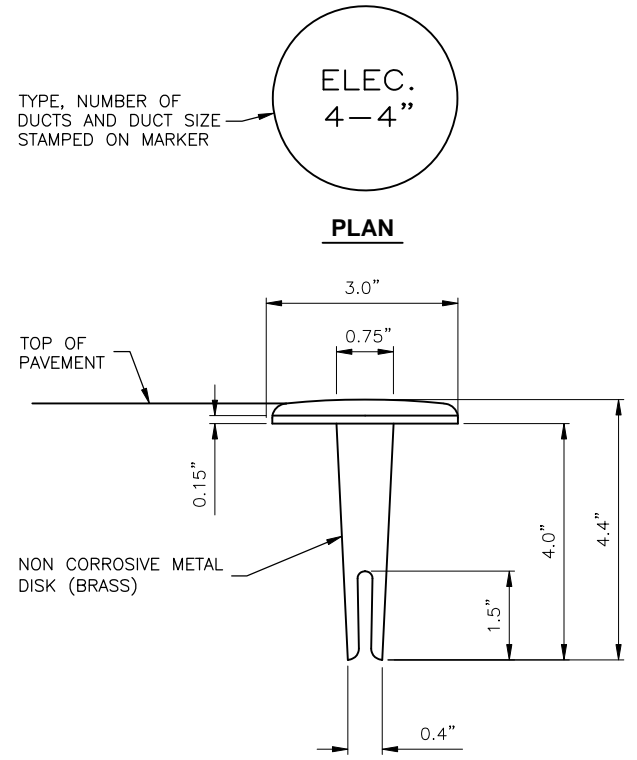
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SHEET	47 OF 56 SHEETS



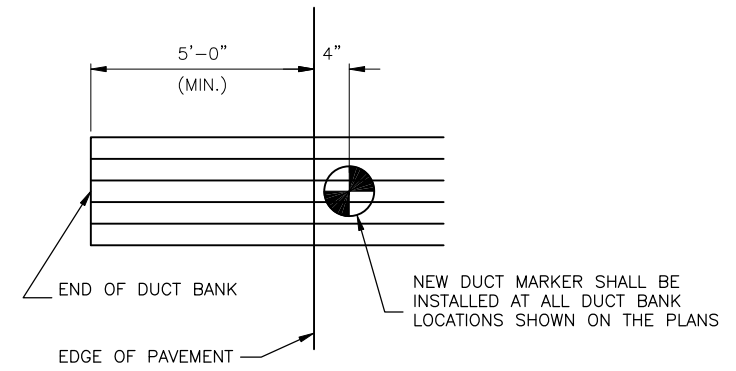
DUCT BANK DETAIL
 N.T.S.

DUCT BANK NOTES

- DIMENSIONS SHOWN ARE MINIMUM.
- TOP OF CONCRETE ENCASUREMENT TO BE NOT LESS THAN 24" BELOW FINISHED SUBGRADE.
- DUCT CONCRETE SHALL BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
- ALL DUCT SHALL BE 2" INSIDE DIAMETER AS NOTED IN PLANS.
- LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS SHALL BE INSTALLED AT LOCATIONS DESIGNATED BY THE ENGINEER.
- CONTRACTOR SHALL INSTALL DUCT BANKS AT A DEPTH WHICH WILL NOT CONFLICT WITH ELEVATION SENSITIVE UTILITIES.
- A PULL WIRE SHALL BE PROVIDED IN EACH UNUSED CONDUIT. UNUSED DUCT SHALL BE SEALED WITH PVC PLUGS/CAPS TO THE SATISFACTION OF THE ENGINEER.
- DUCT BANKS SHALL EXTEND A MINIMUM OF 5' BEYOND THE NEW EDGE OF PAVEMENT.
- BOTH ENDS OF THE DUCT BANK SHALL BE CONSTRUCTED WITH THE BELL END FLUSH WITH THE FACE OF THE PCC DUCT BANK.
- COUNTERPOISE SHALL BE INSTALLED FOR THE FULL LENGTH OF NEW DUCT AT 4" ABOVE THE TOP OF THE CONCRETE. THE COUNTERPOISE SHALL CONNECT TO GROUNDS AT EACH END OF THE DUCT INSTALLED 5' AWAY FROM THE DUCT OPENING SO AS NOT TO INTERFERE WITH THE DUCT OPENING.



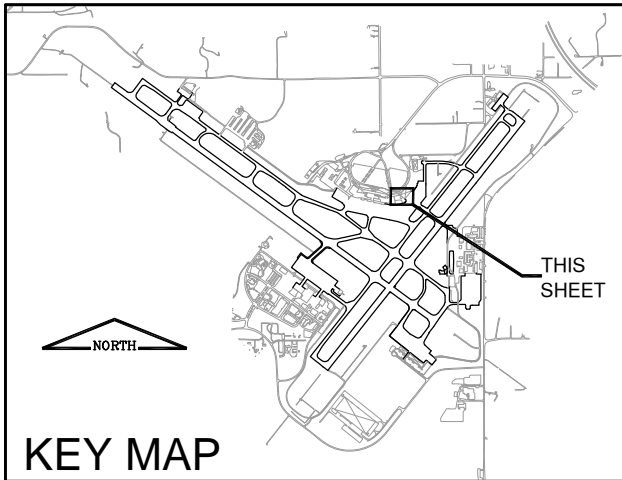
DUCT MARKER DETAILS
 N.T.S.



MARKER PLACEMENT

DUCT MARKERS SHALL BE RECESSED AND GROUTED INTO THE PAVEMENTS.

NEW DUCT MARKER SHALL BE INSTALLED AT ALL DUCT BANK LOCATIONS SHOWN ON THE PLANS



AIRPORT RING ROAD

THIS SHEET

KEY MAP

LEGEND

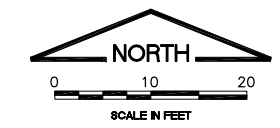
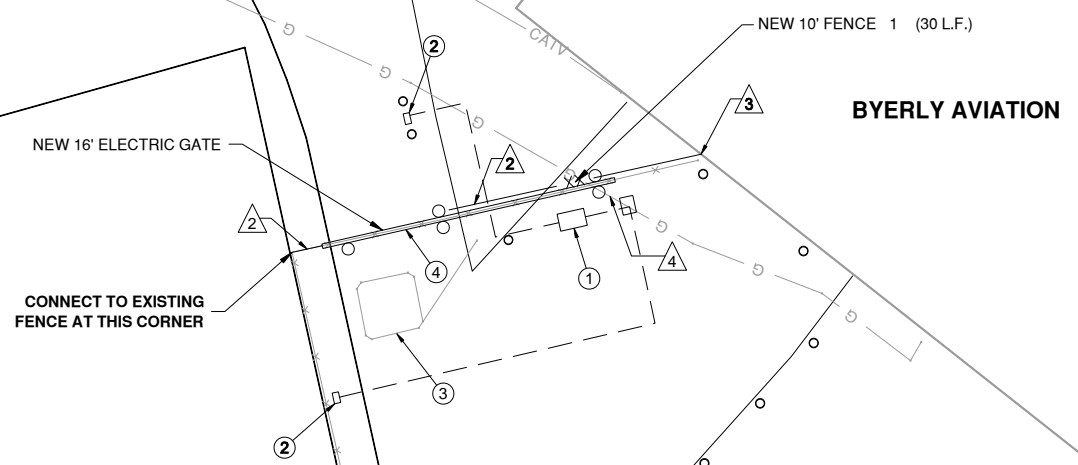
- XX — NEW 10' FENCE
- x - x - x - EXISTING FENCE
- ==== NEW ELECTRIC GATE
- CATV — EXISTING CATV CABLES
- G — EXISTING GAS LINE
- EXISTING BOLLARD

FENCE NOTES

- ① FENCING QUANTITIES SHOWN ARE FROM NEW GATE POST TO NEW GATE POST OR FENCE CHANGE.
- ② NEW FENCE/GATE SHALL BE PLACED IN APPROXIMATELY THE SAME LOCATION AS THE EXISTING FENCE, UNLESS OTHERWISE SHOWN.
- ③ TERMINATE/BEGIN FENCE AT EXISTING BUILDING.
- ④ USE EXTREME CAUTION WHEN INSTALLING FENCE POSTS NEAR EXISTING UTILITIES

GATE NOTES

- ① EXISTING GATE OPERATOR.
- ② EXISTING EXIT READER.
- ③ EXISTING LOOP DETECTOR.
- ④ THE EXISTING BYERLY SIGNS ON EACH SIDE OF THE GATE SHALL BE REMOVED AND INSTALLED ON THE PROPOSED GATE. THE CONTRACTOR SHALL, IN THE PRESENCE OF THE RESIDENT ENGINEER, DOCUMENT THE CONDITION OF THE SIGNS. ANY DAMAGE TO THE SIGNS AS A RESULT OF THE CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT HIS OWN COST AND TO THE SATISFACTION OF THE RESIDENT ENGINEER.



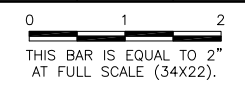
FILE: FENCE PLAN.dwg
 UPDATE BY: TJ Heavisides
 PLOT DATE: 5/1/2013 9:40 PM

PIA-Base-Existing
 KEYMAP
 1206103-V-VF2D

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 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 FENCE PLAN**

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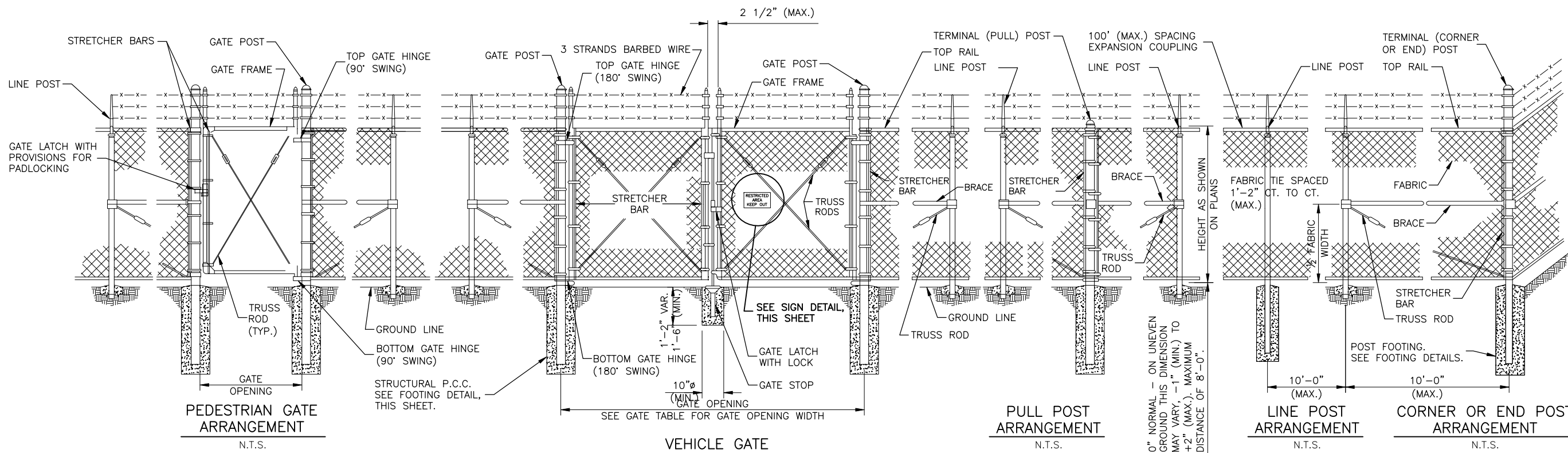
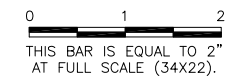


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SHEET	48 OF 56 SHEETS

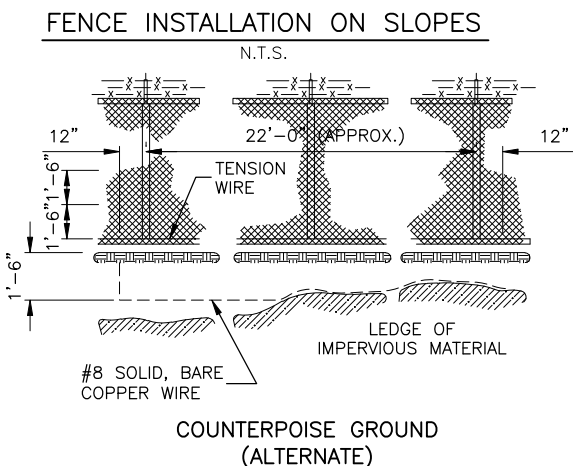
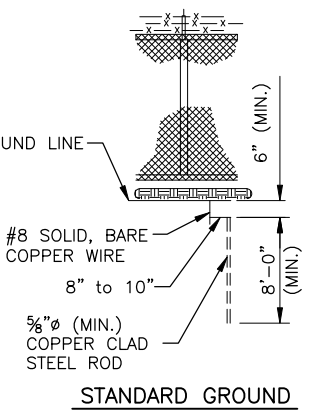
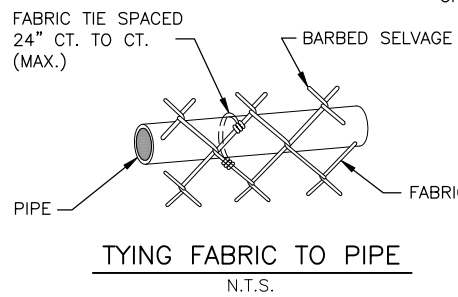
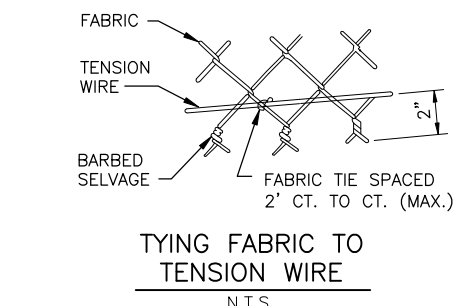
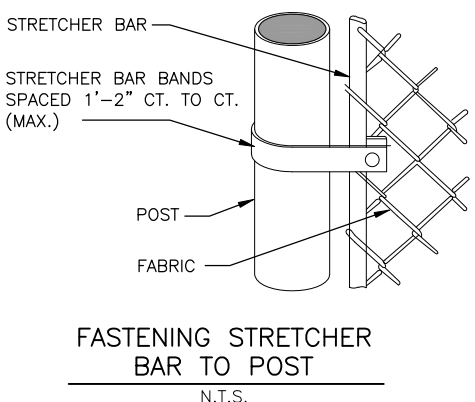
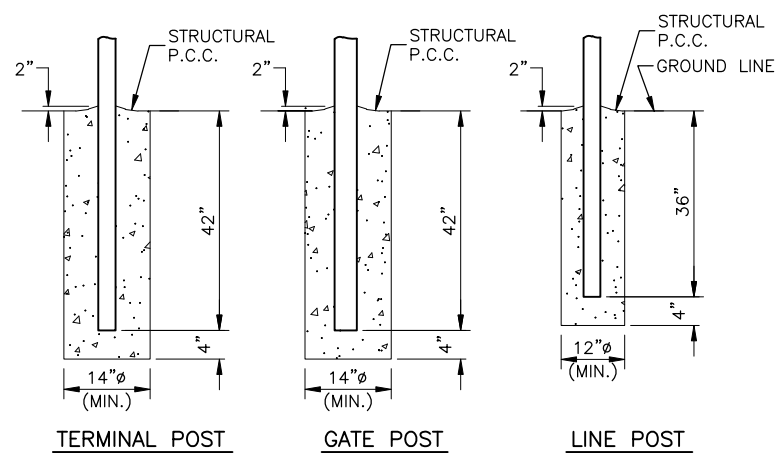
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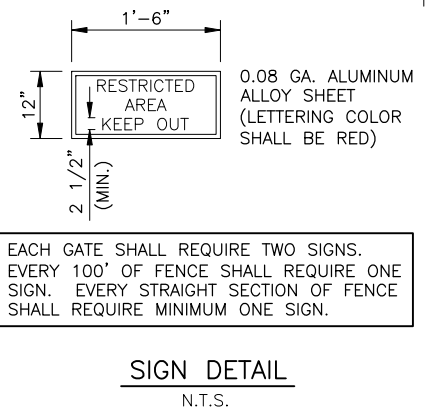
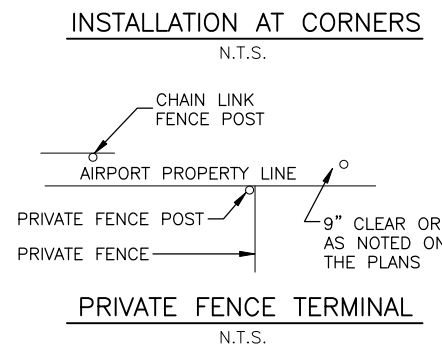
PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660 FT. INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1320 FT. AND GREATER THAN 660 FT.



PROTECTIVE ELECTRICAL GROUND DETAILS
N.T.S.

- NOTES**
1. CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 500 FT. EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
 2. FENCE UNDER POWER LINES SHALL BE GROUNDED BY 3 GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSINGS.
 3. THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF IMPERVIOUS EARTH STRUCTURES.
 4. THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.
 5. ALL PROPOSED CLASS E FENCE SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 664001-01.

WHERE FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE. WHERE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE ENGINEER.



GENERAL WAYNE A. DOWNING
PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
RECONSTRUCT GENERAL AVIATION APRON
FENCE DETAILS 1

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SHEET	49 OF 56 SHEETS

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0 1 2
 THIS BAR IS EQUAL TO 2"
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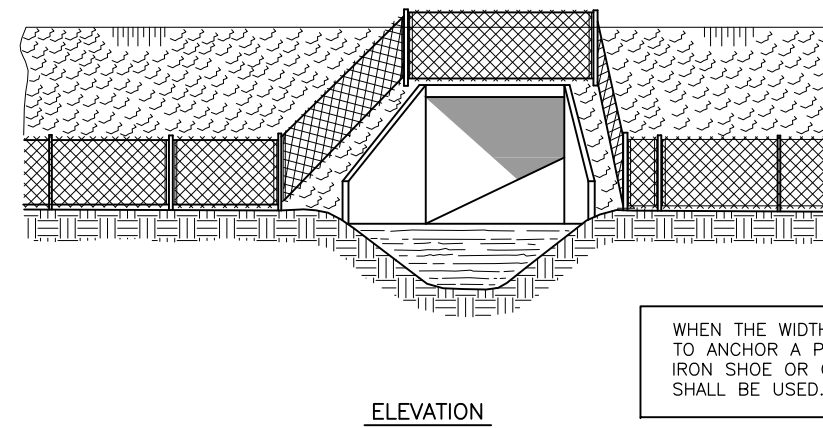
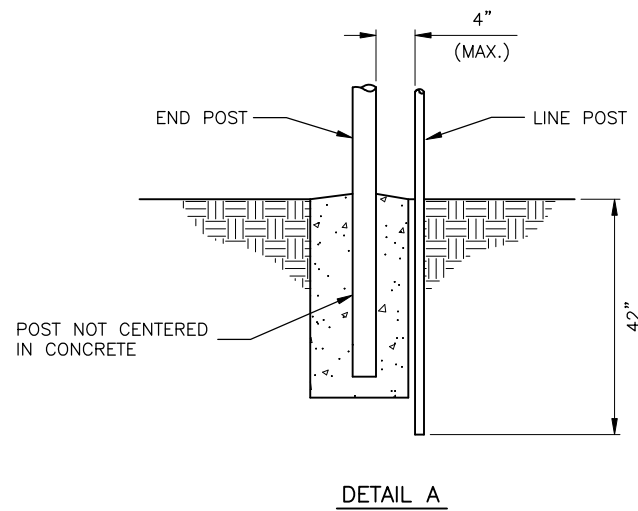
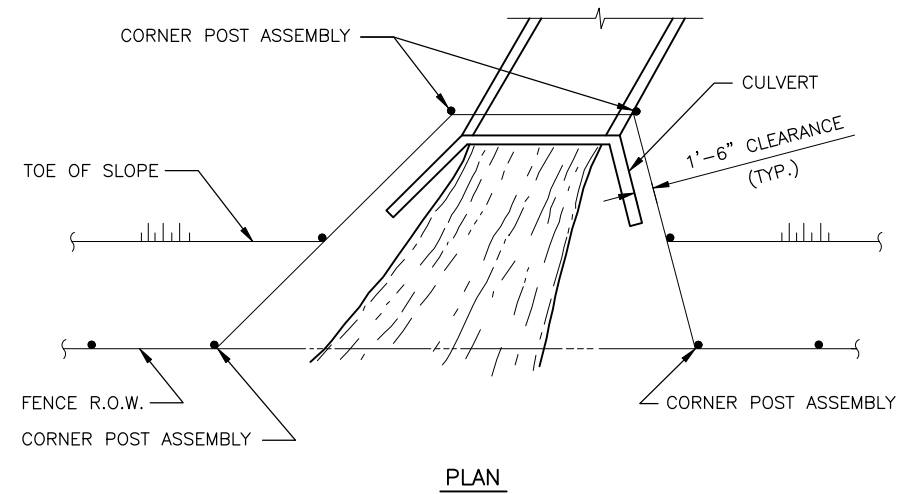
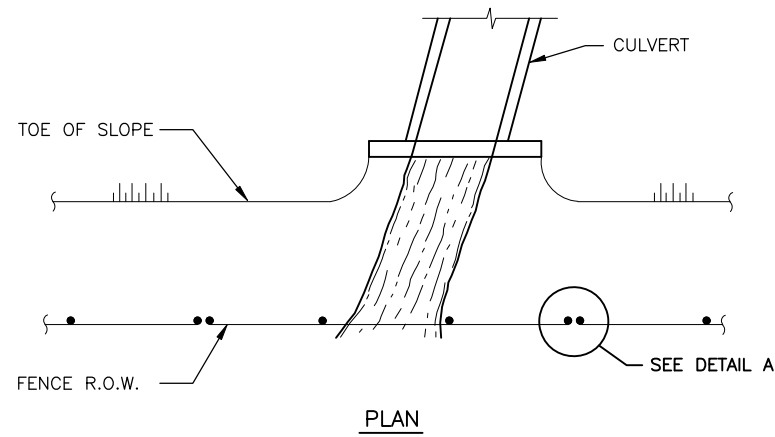
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**RECONSTRUCT GENERAL AVIATION APRON
 FENCE DETAILS 2**

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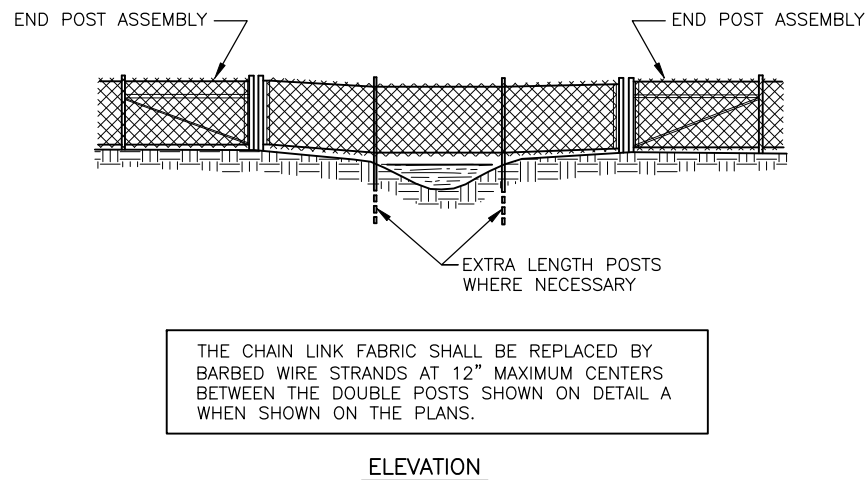
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SHEET	50 OF 56 SHEETS



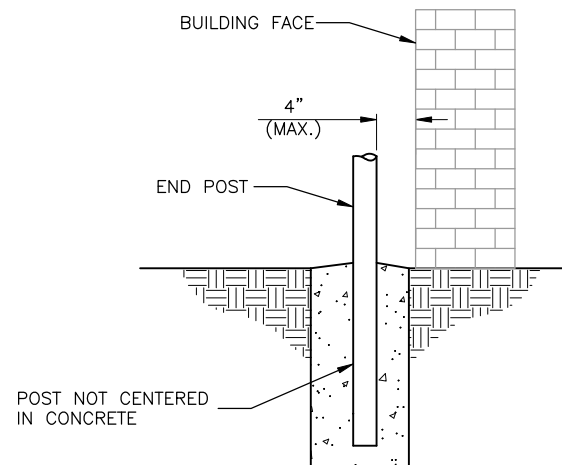
WHEN THE WIDTH OF THE CULVERT MAKES IT NECESSARY TO ANCHOR A POST TO THE TOP OF THE CULVERT, A CAST IRON SHOE OR OTHER DEVICE APPROVED BY THE ENGINEER SHALL BE USED.

FENCE INSTALLATION AROUND HEADWALL DETAILS
 N.T.S.



THE CHAIN LINK FABRIC SHALL BE REPLACED BY BARBED WIRE STRANDS AT 12" MAXIMUM CENTERS BETWEEN THE DOUBLE POSTS SHOWN ON DETAIL A WHEN SHOWN ON THE PLANS.

FENCE INSTALLATION OVER STREAM DETAILS
 N.T.S.

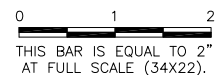


FENCE INSTALLATION AT BUILDING FACE
 N.T.S.

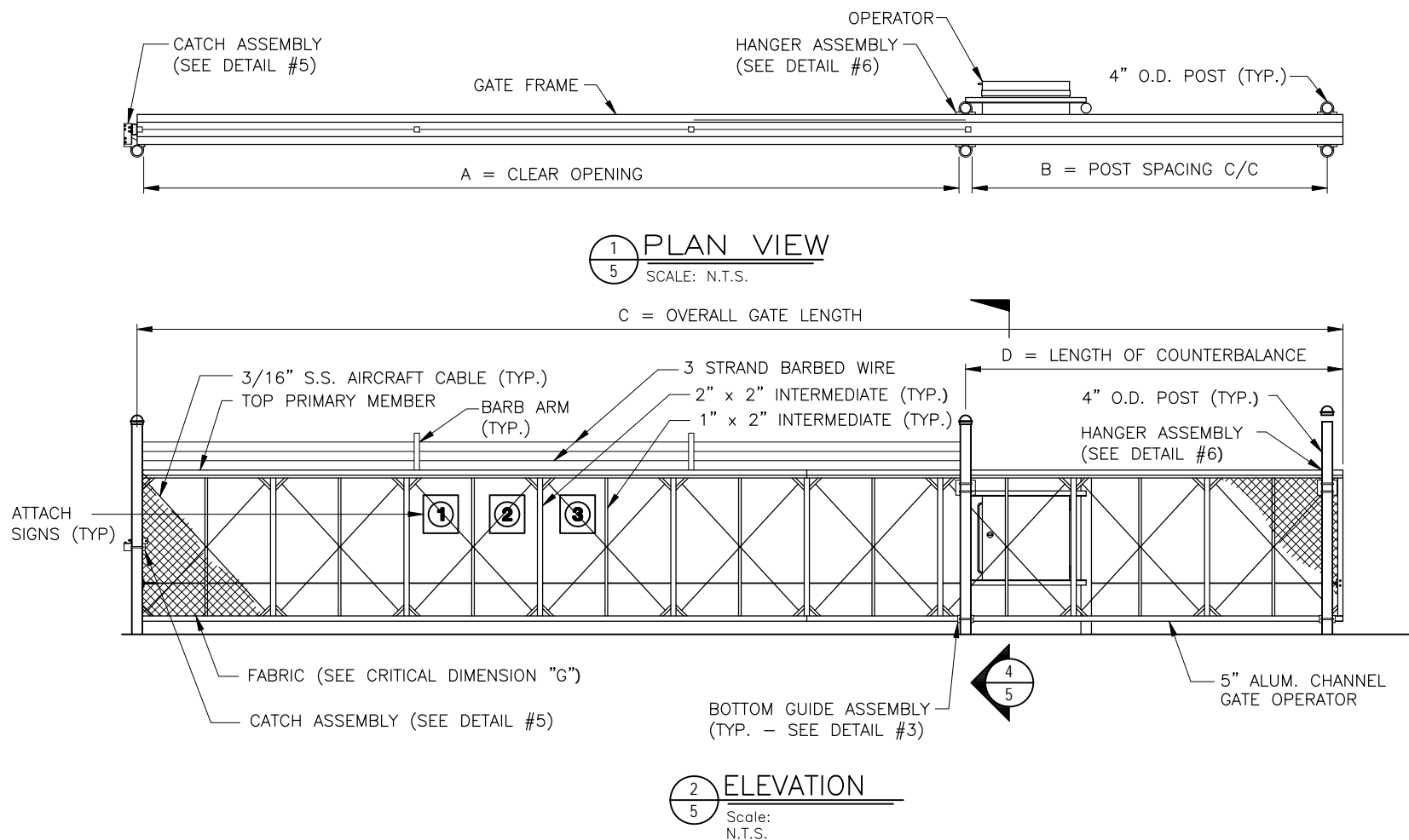
POST TYPE	FENCE POST SHAPE, SIZE AND WEIGHT					
	FENCE POST MATERIAL					
	ROUND STEEL PIPE SCH. 40		ROUND STEEL PIPE SS 40		ROLL FORMED C SECTION	
	DIAMETER	WEIGHT	DIAMETER	WEIGHT	SIZE	WEIGHT
CORNER, END, PULL	4.0" O.D.	9.11 LB/FT	4.0" O.D.	6.57 LB/LF	3.25" X 2.5"	4.50 LB/FT
LINE POST	2.875" O.D.	5.79 LB/FT	2.875" O.D.	4.64 LB/LF	3.25" X 2.5"	4.50 LB/FT
TOP RAIL	1.66" O.D.	2.77 LB/FT	1.66" O.D.	1.84 LF/LF	1.625" X 1.25"	1.35 LB/FT
GATE POST	6.25" O.D.	18.97 LB/FT	N/A	N/A	N/A	N/A

PE096

REVISIONS		
NUMBER	BY	DATE



CANTILEVER SLIDE GATE CRITICAL DIMENSION CHART		
DESIGNATION	DESCRIPTION	GATE
A	CLEAR OPENING	16'-0"
B	COUNTER BALANCE POST SPACING C/C	8'-11"
C	OVERALL GATE LENGTH	24'-0"
D	COUNTER BALANCE LENGTH	8'-0"
E	NOMINAL GATE HEIGHT	10'-0"
F	POST HEIGHT	11'-6"
G	FABRIC HEIGHT	9'-0"



UL 235 COMPLIANCE NOTES

- GATE INSTALLATION SHALL COMPLY WITH ALL REQUIREMENTS OF UL 235, INCLUDING, BUT NOT LIMITED TO:
1. ALL OPENINGS OF THE SLIDE GATE ARE GUARDED OR SCREENED FROM THE BOTTOM OF THE GATE TO A MINIMUM OF 4 FEET ABOVE GROUND TO OPENINGS ANYWHERE IN THE GATE, AND IN THAT PORTION OF THE ADJACENT FENCE THAT THE GATE COVERS IN THE OPEN POSITION.
 2. ALL EXPOSED PINCH POINTS ARE ELIMINATED OR GUARDED AND GUARDING IS SUPPLIED FOR ALL EXPOSED ROLLERS.
 3. A WARNING SIGN (SEE DETAIL, THIS SHEET) MUST BE AFFIXED TO BOTH SIDES OF THE SLIDE GATE.
 4. FOR ADDITIONAL UL 235 REQUIREMENTS FOR THIS GATE INSTALLATION, SEE GATE OPERATOR DETAILS SHEET.

NOTES

1. CANTILEVERED GATE SHALL BE SUFFICIENTLY RIGID TO WITHSTAND FLEXING OR BENDING DURING WINDY CONDITIONS. CONTRACTOR SHALL PROVIDE STIFFENERS, STRUCTURAL SHAPES IN EXCESS OF THE MINIMUM SPECIFIED DIMENSIONS OR ADDITIONAL ROLLERS AND POSTS SUFFICIENT TO PREVENT DISPLACEMENT OF THE GATE BY WIND OR BY UNAUTHORIZED PERSONNEL.
 2. THE FABRIC TYPE AND FINISH OF THE GATE, MATCH WITH PROPOSED FENCE OR AS DIRECTED BY THE ENGINEER.
- ALL SLIDING GATES SHALL HAVE ALL ROLLERS ENCLOSED IN STEEL OR PLASTIC SHROUDS TO PREVENT ACCIDENTAL INJURY.

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET IS GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.

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PEORIA INTERNATIONAL AIRPORT
PEORIA, ILLINOIS
RECONSTRUCT GENERAL AVIATION APRON
ELECTRICAL GATE DETAILS 1

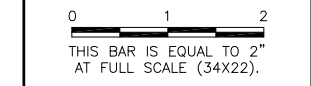
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JOB No:	12061-03
IL PROJ. NO.	PIA-4361
AIP PROJ. NO.	3-17-0080-XX
SHEET	51 OF 56 SHEETS

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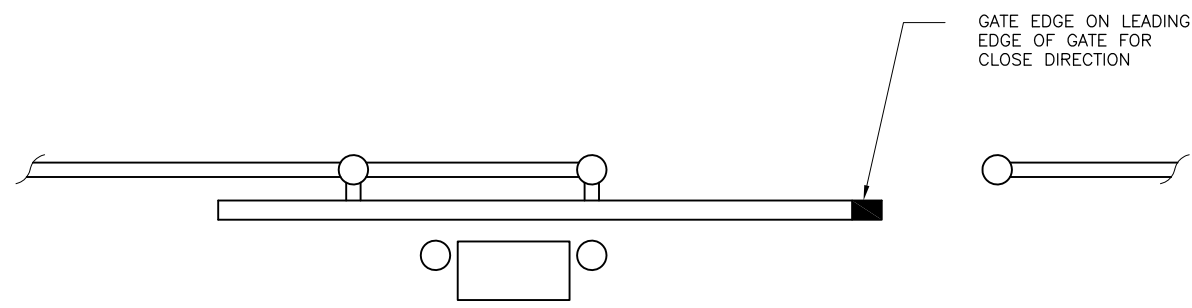
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ELECTRICAL GATE DETAILS 2

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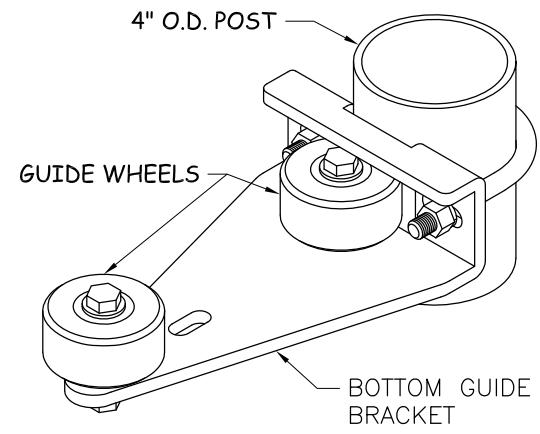
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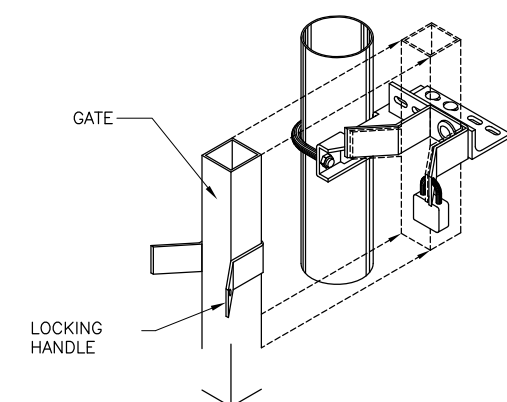
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SHEET	52 OF 56 SHEETS



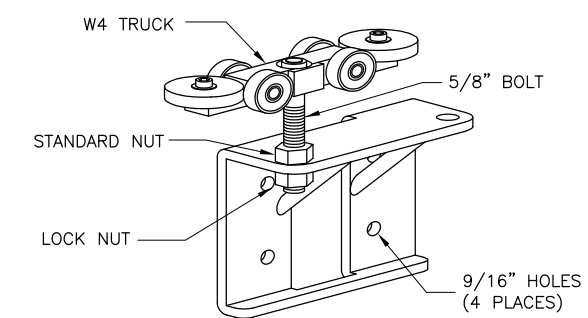
GATE OPERATOR CONTACT SENSOR
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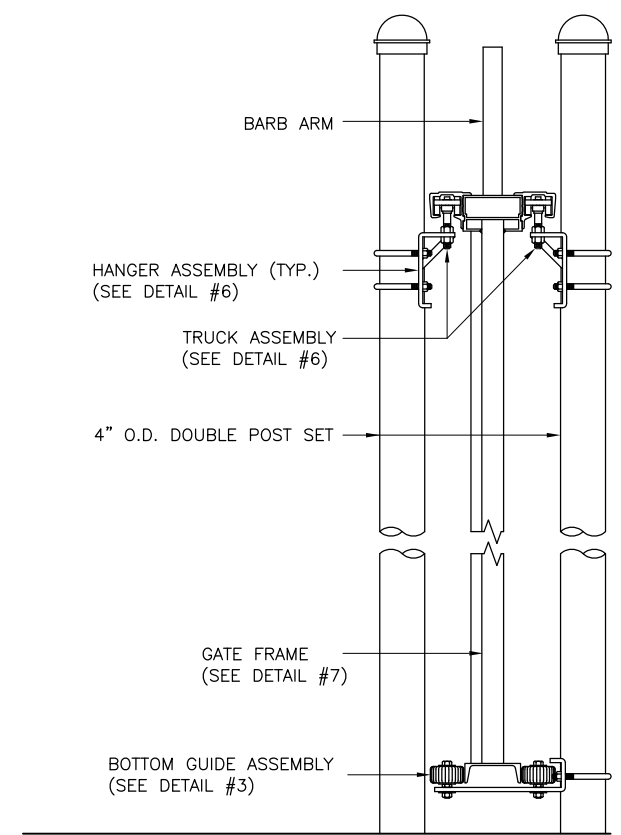
3
5 BOTTOM GUIDE
 N.T.S.



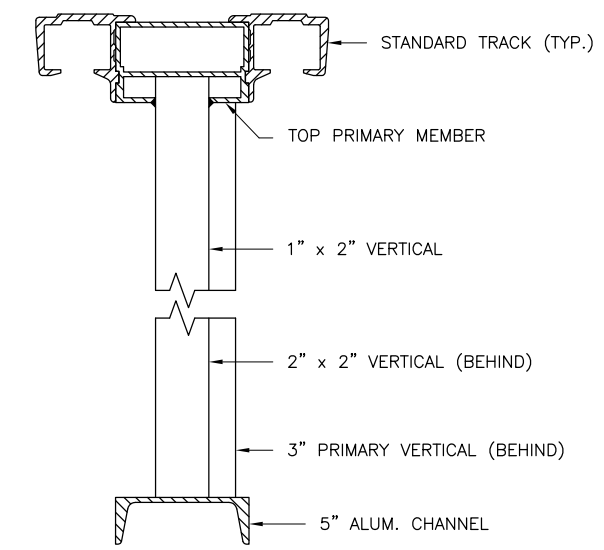
5
5 CATCH ASSEMBLY
 N.T.S.



6
5 GATE HANGER ASSEMBLY
 N.T.S.

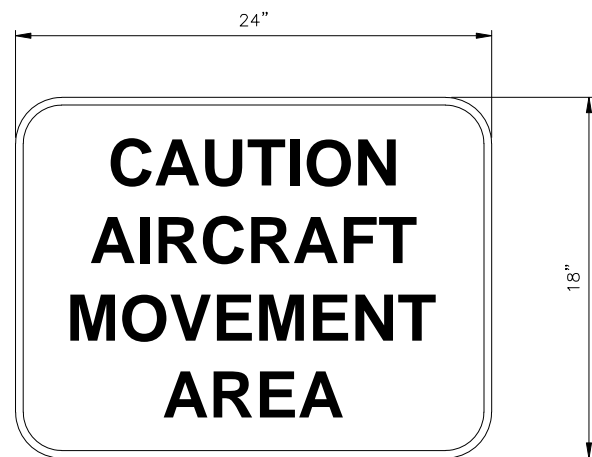


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5 ASSEMBLY SECTION
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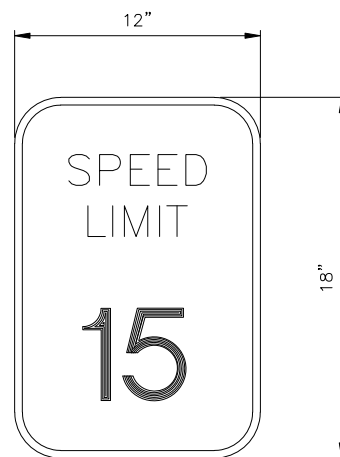


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5 GATE FRAME SECTION
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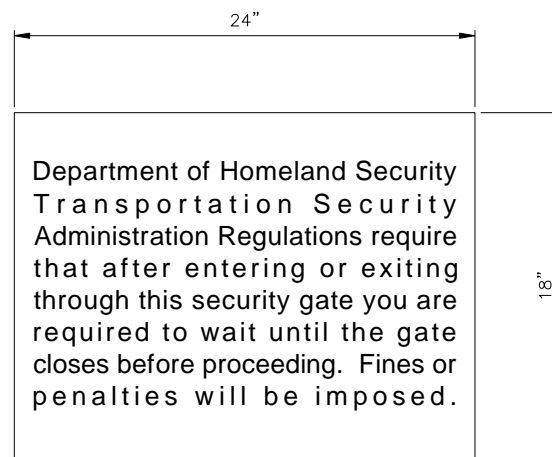
CANTILEVER SLIDE GATE DETAILS



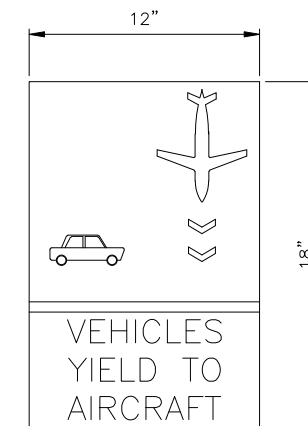
CAUTION SIGN



SPEED LIMIT SIGN



TSA SIGN

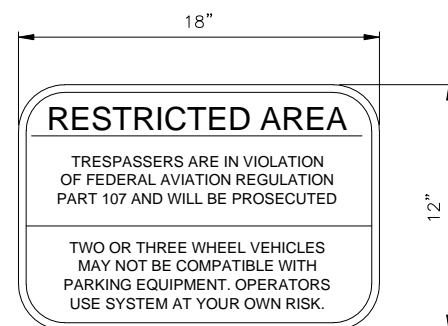


YIELD SIGN

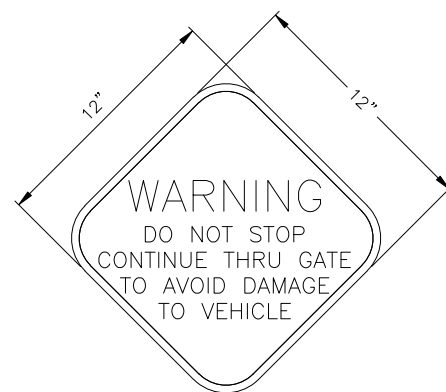
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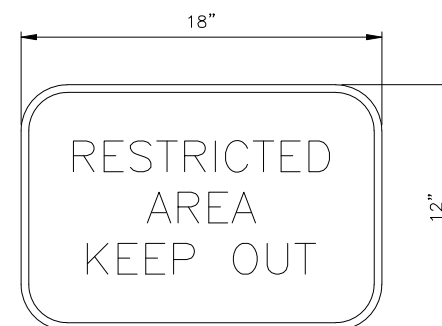
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



RESTRICTED AREA



CONTINUE THRU GATE



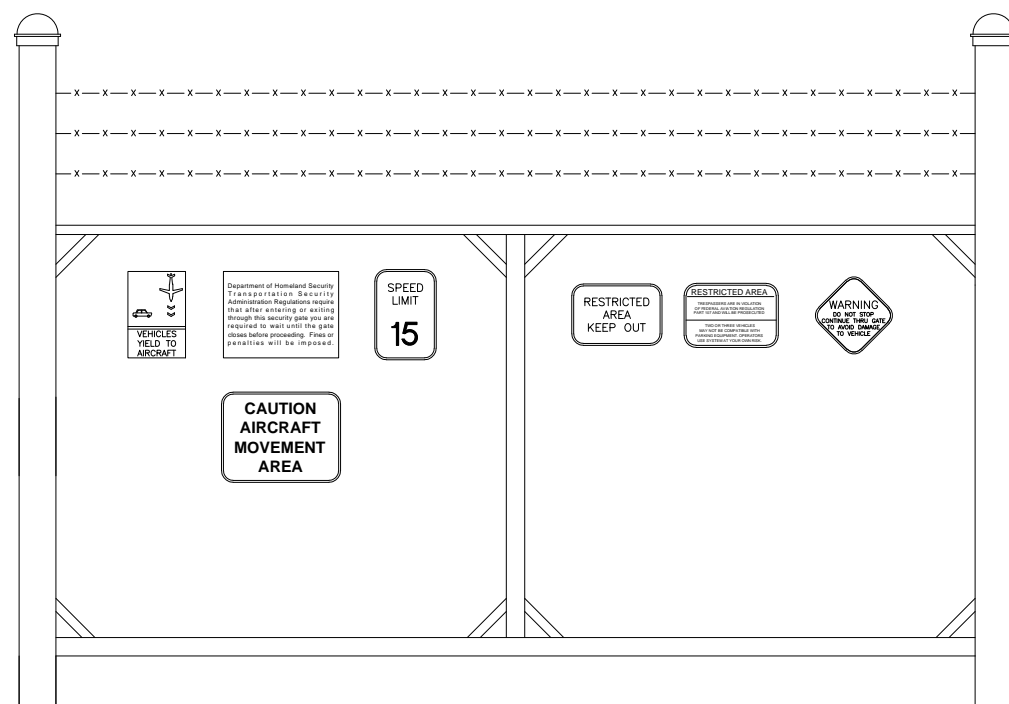
KEEP OUT SIGN



WARNING SIGN DETAIL

NOTES:

- SEE PROPOSED GATE PLAN FOR TYPES OF SIGNS REQUIRED AT EACH GATE.
- GATE NUMBER AND RESTRICTED AREA SIGNS SHALL BE INSTALLED ON ALL NEW GATES. COST TO PROVIDE AND INSTALL SIGNS SHALL BE INCIDENTAL TO THE COST OF THE ASSOCIATED GATE. COORDINATE WITH AIRPORT ON NUMBER.
- CONTRACTOR SHALL PROVIDE SHOP DRAWINGS OF EACH SIGN. SHOP DRAWINGS SHALL MEET THE APPROVAL OF THE AIRPORT AND ENGINEER PRIOR TO MANUFACTURE AND INSTALLATION.
- LETTERING COLOR SHALL BE RED ON WHITE BACKGROUND, WITH THE EXCEPTION OF THE SPEED LIMIT SIGN WHICH SHALL BE BLACK LETTERING ON WHITE BACKGROUND.
- EVERY 100' OF FENCE SHALL REQUIRE ONE RESTRICTED AREA SIGN. EVERY STRAIGHT SECTION OF FENCE SHALL REQUIRE MINIMUM ONE RESTRICTED AREA SIGN.
- COSTS TO PROVIDE AND INSTALL SIGNS SHALL BE INCIDENTAL TO CONTRACT PAY ITEMS.
- ALL SIGNS SHALL BE FASTENED TO THE INSIDE OF THE FENCE.
- THE TSA SIGN, CONTINUE THRU GATE, AND WARNING SIGN SHALL BE INSTALLED FACING BOTH THE AIRSIDE AND LANDSIDE.

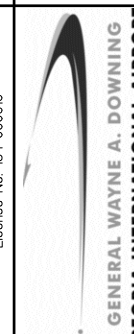


ELECTRIC VEHICLE GATE

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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS

RECONSTRUCT GENERAL AVIATION APRON
 GATE SIGNS DETAILS

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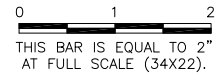


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SHEET 53 OF 56 SHEETS	

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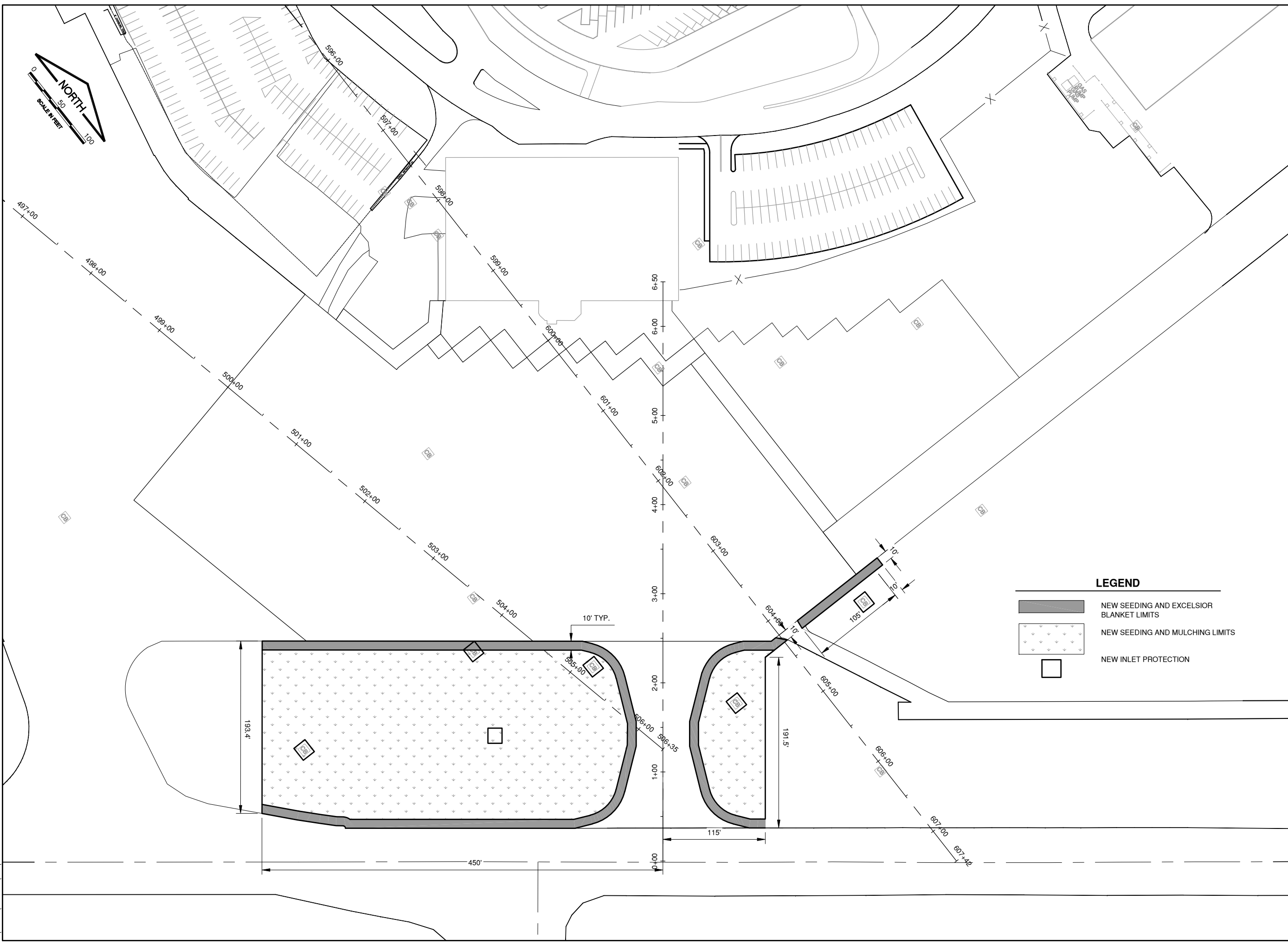
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 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 TURFING AND EROSION CONTROL PLAN**

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SHEET	54 OF 56 SHEETS



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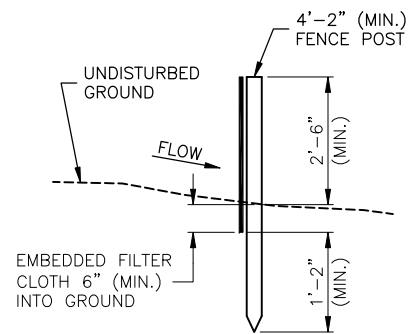
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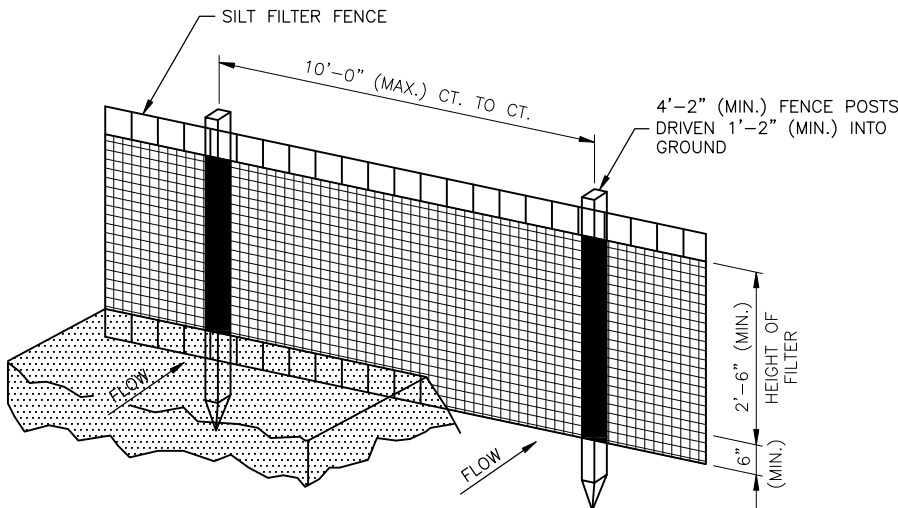
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**

**RECONSTRUCT GENERAL AVIATION APRON
 EROSION CONTROL DETAILS**



SECTION

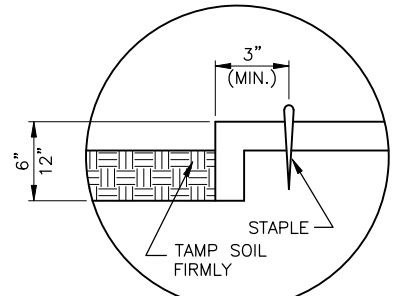


PERSPECTIVE VIEW

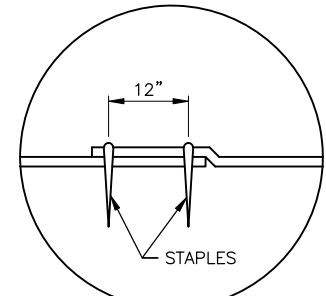
EROSION CONTROL FABRIC FENCE DETAILS
 N.T.S.

EROSION CONTROL FABRIC FENCE NOTES

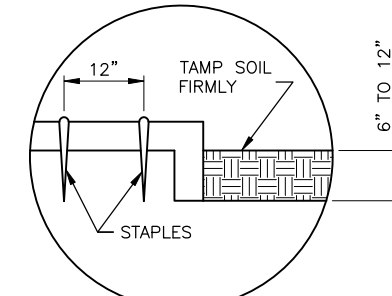
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



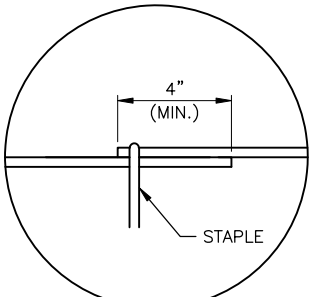
DETAIL 1 - TERMINAL FOLD



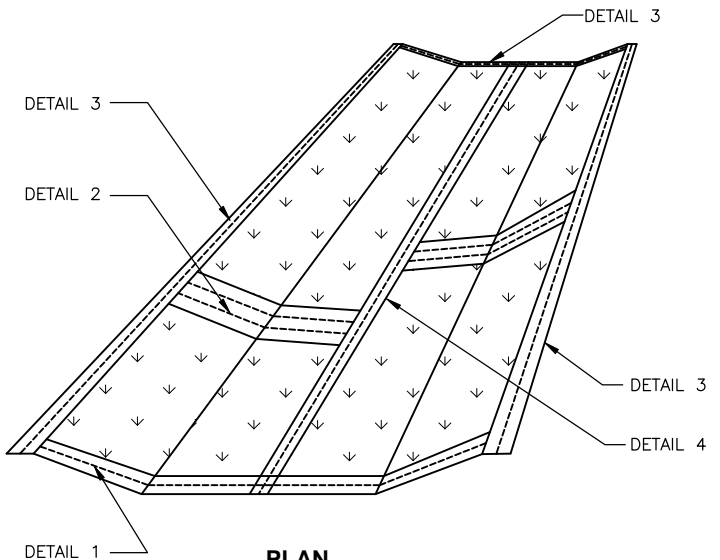
DETAIL 2 - JUNCTION SLOT



DETAIL 3 - ANCHOR SLOT



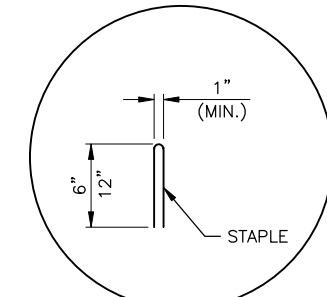
DETAIL 4 - LAP JOINT



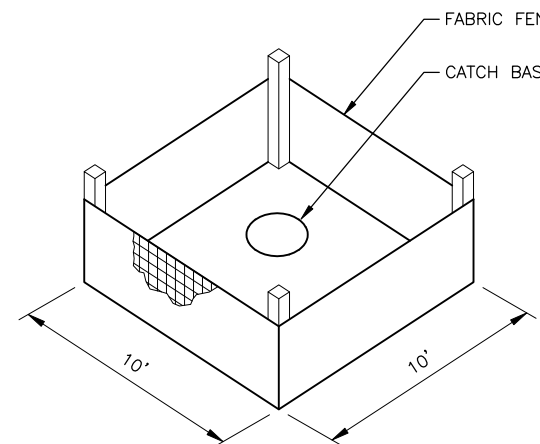
EXCELSIOR BLANKET DETAILS
 N.T.S.

EXCELSIOR BLANKET NOTES

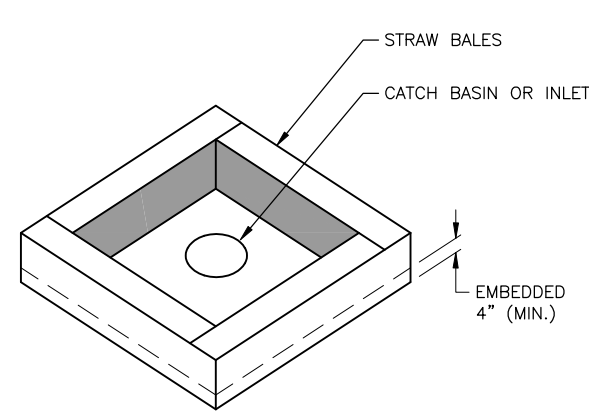
1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



DETAIL 5 - STAPLE DETAIL



INLET PROTECTION WITH FABRIC
 N.T.S.



INLET PROTECTION WITH STRAW BALES
 N.T.S.

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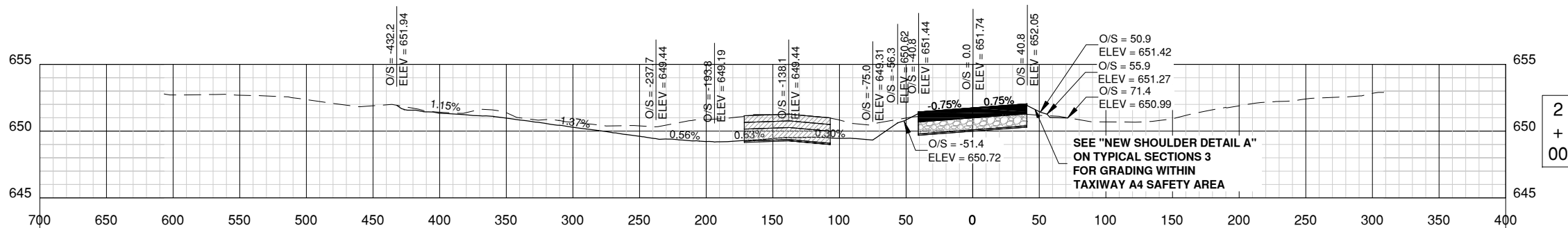
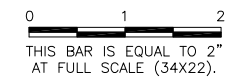
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SHEET	55 OF 56 SHEETS

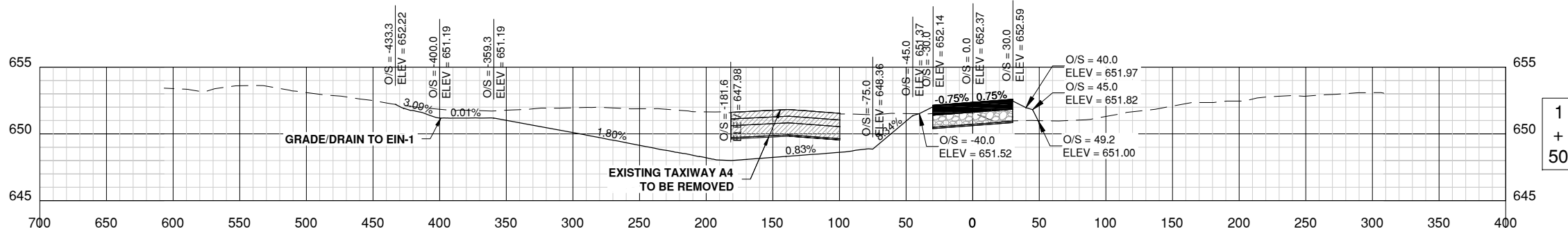
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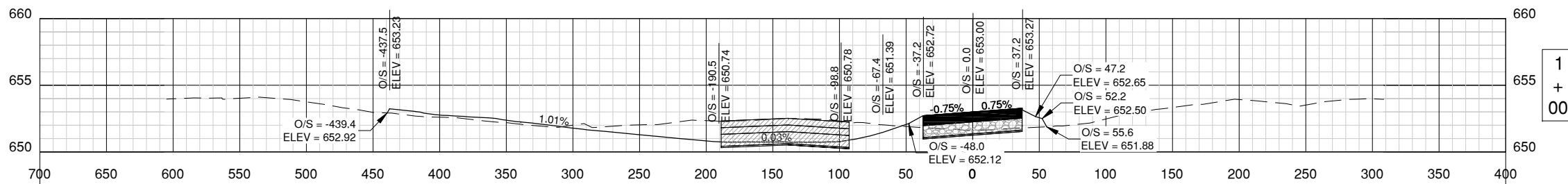
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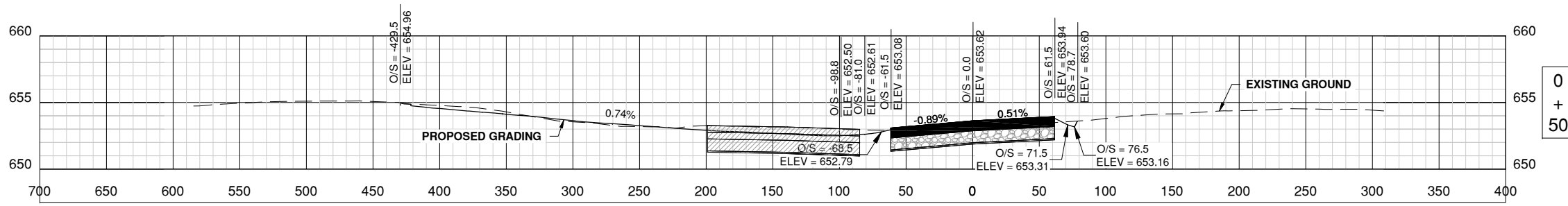
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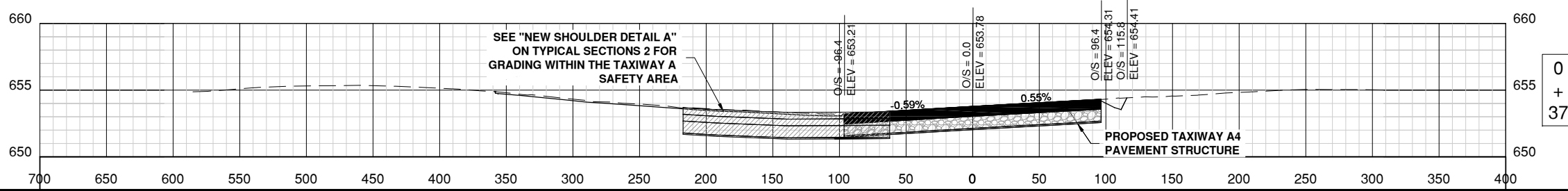
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 PEORIA INTERNATIONAL AIRPORT
 PEORIA, ILLINOIS**
**RECONSTRUCT GENERAL AVIATION APRON
 TAXIWAY A4 CROSS SECTIONS**

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