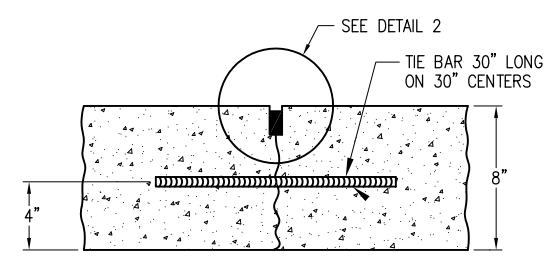
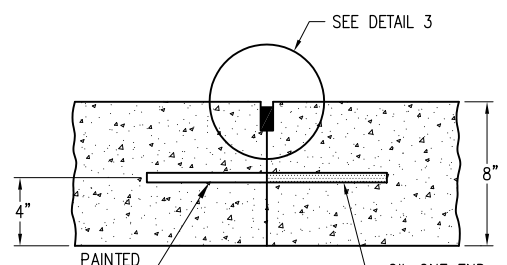


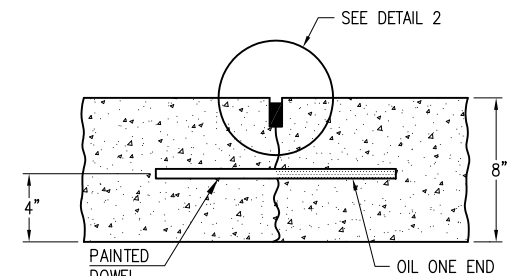
TYPE-A THICKENED EDGE ISOLATION JOINTS



TYPE B HINGED



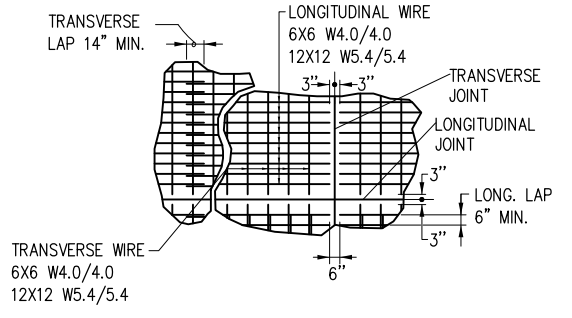
TYPE E DOWELED



TYPE C DOWELED

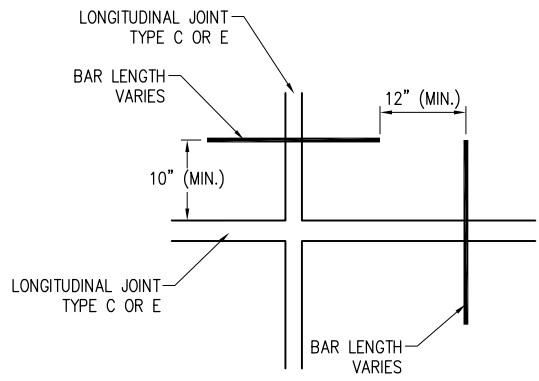
CONSTRUCTION JOINTS

CONTRACTION JOINTS



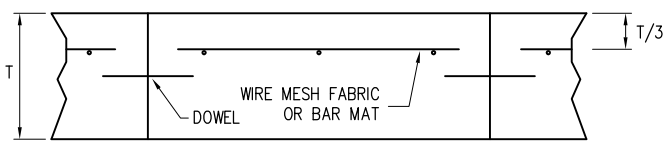
REINFORCEMENT SHEET WIRE FABRIC DETAIL

NOTE: DIFFERENT WIRE MESH SIZES/CONFIGURATIONS OR SIZES ARE PERMITTED IF THE MINIMUM CROSS SECTIONAL AREA EQUALS 0.5 OR GREATER.



POSITION OF DOWELS AT EDGE OF JOINT TYPE C OR E

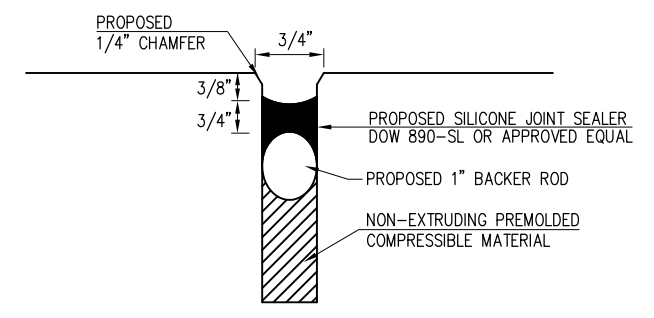
DOWEL PLAN VIEW



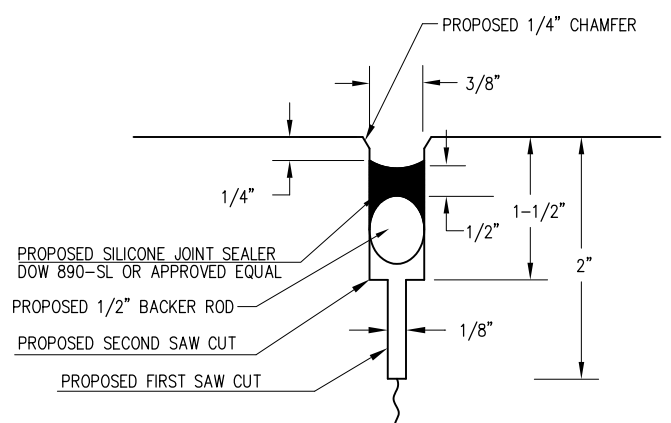
PAVEMENT REINFORCING DETAIL

REINFORCING NOTES:

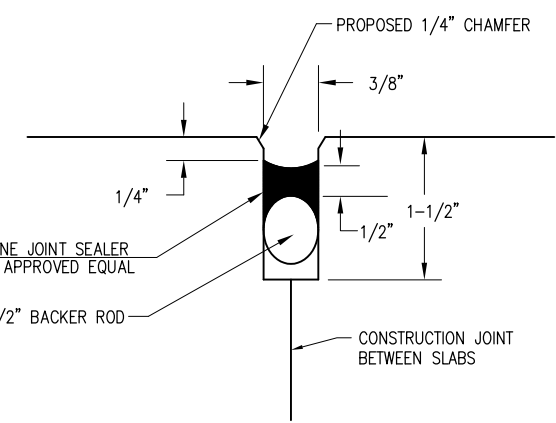
1. END LAPS SHALL BE A MINIMUM OF 12", BUT NOT LESS THAN 30 TIMES THE DIAMETER OF THE LONGITUDINAL WIRE OR BAR.
2. SIDE LAPS SHALL BE A MINIMUM OF 6", BUT NOT LESS THAN 20 TIMES THE DIAMETER OF TRANSVERSE WIRE OR BAR.
3. END AND SIDE CLEARANCES SHALL BE A MAXIMUM OF 6" AND A MINIMUM OF 2".
4. LONGITUDINAL MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 12" APART.
5. TRANSVERSE MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 24" APART.
6. MINIMUM SIZE OF LONGITUDINAL AND TRANSVERSE WIRE SHALL BE W5 OR D5 (NORMAL DIAMETER 0.252").
7. WIRE FABRIC TO BE A.S.T.M. A185 COLD DRAWN WELDED STEEL WIRE FABRIC, WITH MINIMAL SECTIONAL AND AREA IN BOTH DIRECTIONS = 0.09 s.i./Lf.
8. PAVEMENT REINFORCING SHALL BE INCIDENTAL TO 501 PCC PAVEMENT.



DETAIL 1
NOT TO SCALE



DETAIL 2
NOT TO SCALE



DETAIL 3
NOT TO SCALE

JOINT SEALING DETAILS

JOINTING NOTES

1. ALL JOINT EDGES SHALL BE SAWCUT TO PRODUCE THE 1/8" CHAMFER.
2. ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. DOWELS FOR THE 8" PAVEMENT SHALL BE 1" DIA., 19" LENGTH AND SPACED 12" CENTER TO CENTER.
4. ALL TIE BARS SHALL BE HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.
5. ALL TIE BARS SHALL BE PLACED AT A POINT NOT LESS THAN 6" OR MORE THAN 15" FROM A TRANSVERSE OR EXPANSION JOINT. TIE BARS FOR THE 8" PAVEMENT SHALL BE 5/8" DIA., 20" LENGTH AND SPACED 36" CENTER TO CENTER.
6. DOWELS IN TRANSVERSE CONTRACTION AND LONGITUDINAL CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.
7. ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE ±1/8" FOR CONSTRUCTION JOINTS AND ±1/4" FOR CONTRACTION JOINTS.
8. THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED DOWELS IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE DIVISION OF AERONAUTICS PRIOR TO USE.
9. THE COST OF ALL DOWEL BARS, TIE-BARS, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE P.C.C. PAVEMENT.
10. WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL PROTECT THE TRANSVERSE JOINTS.
11. JOINT SEALANT SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS ITEM 501-2.5.
12. CURING COMPOUND SHALL BE SPECIFIED IN THE SPECIAL PROVISIONS ITEM 501-3.17 AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE ENGINEER.
13. ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.

REVISION	DATE

SAINT LOUIS DOWNTOWN AIRPORT
A Division of Bi-State Development Agency
BLOCK GRANT PROJ.: 3-17-0039-B30
ILL PROJ.: CPS-4213

Hanson Project No.	12A0069
Filename	C-561-JNT.dwg
Scale	NOT TO SCALE
Date	03/08/13
LAYOUT	01/31/13
DRAWN	MLH 01/31/13
REVIEWED	BSS 03/08/13

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Springfield, Illinois 62703-2886

RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT

PAVEMENT JOINTING DETAILS

MAR 11, 2013 12:00 PM HARR01115
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