

# CONSTRUCTION PLANS FOR ST. LOUIS DOWNTOWN AIRPORT CAHOKIA, ST. CLAIR COUNTY, ILLINOIS RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT

SCOPE OF WORK

BASE BID

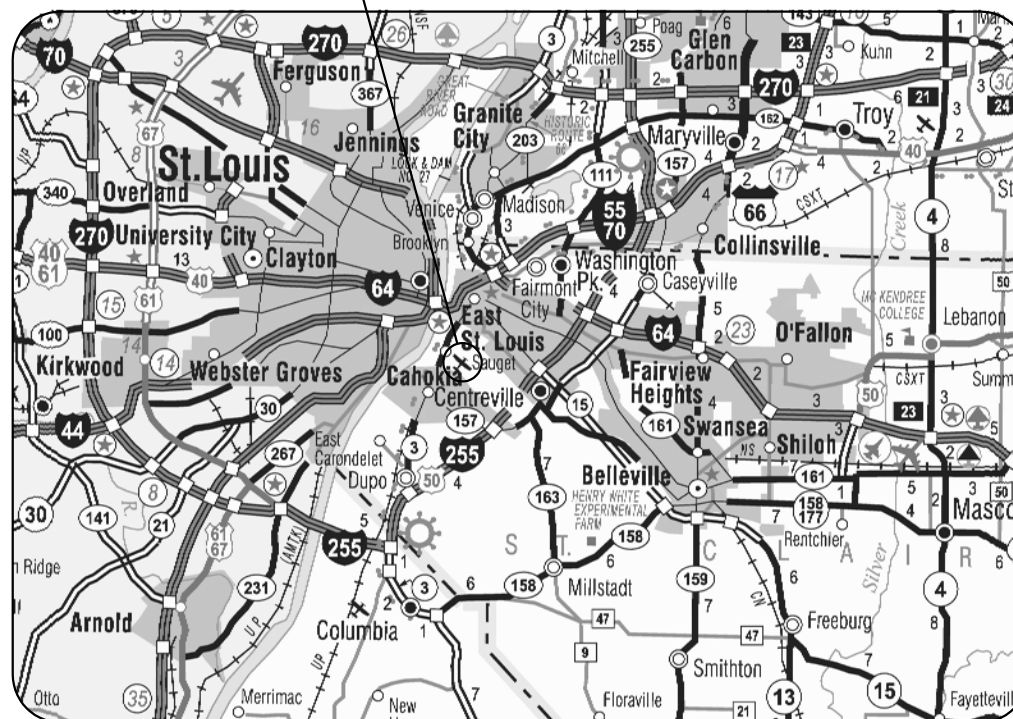
THE PROJECT CONSISTS OF REMOVAL AND REPLACEMENT OF AN EXISTING TAXILANE, INCLUDING DRAINAGE, PAVING AND ASSOCIATED WORK.

ADDITIVE ALTERNATE NO. 1

ADDITIVE ALTERNATE NO. 1 WILL CONSIST OF PARTIAL DEPTH PAVEMENT REPAIRS ON TAXIWAY B1 AND TAXIWAY B7, INCLUDING PAVEMENT REMOVAL, CONCRETE PAVING AND ASSOCIATED WORK.

ILL. PROJ.: CPS-4213  
S.B.G. PROJ.: 3-17-0039-B30  
  
LATITUDE: 38° 34' 14"  
LONGITUDE: 90° 09' 22"  
ELEVATION: 413' M.S.L.  
DATE: MAR 8, 2013

ST. LOUIS DOWNTOWN AIRPORT  
CAHOKIA, ILLINOIS



LOCATION

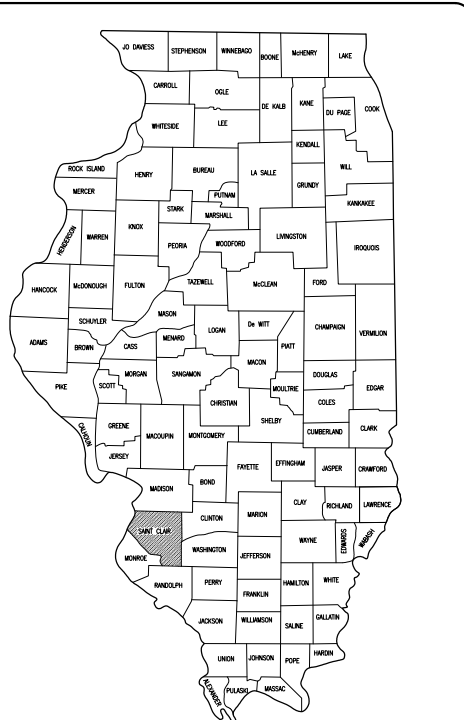


**HANSON**  
Hanson Professional Services Inc.  
CIVIL ENGINEER

Submitted by: *[Signature]* ENGINEER  
Date Submitted: 3/8/2013  
Lic. Exp. Date: 11/30/2013

**BI-STATE DEVELOPMENT AGENCY**

Approved: *[Signature]* DIRECTOR OF THE AIRPORT  
Date: March 7, 2013



LOCATION OF COUNTY

DATE	REVISION

**SAINT LOUIS DOWNTOWN AIRPORT**

A Division of Bi-State Development Agency  
BLOCK GRANT PROJ.: 3-17-0039-B30  
ILL. PROJ.: CPS-4213

Hanson Project No. 12A0069	FILENAME G-001-CVR.dwg	LAYOUT MLH	01/16/13
Scale NONE	Date 03/08/13	DRAWN MLH	01/16/13
		REVIEWED BSS	03/08/13

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Springfield, Illinois 62703-2886

RECONSTRUCT TAXILANE  
IN NORTHWEST QUADRANT

COVER SHEET

1

1 of 18 sheets

SUMMARY OF QUANTITIES - BASE BID				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR150510	FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR150540	HAUL ROUTE	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	1,590	
AR156510	SILT FENCE	LF	325	
AR156520	INLET PROTECTION	EA	4	
AR162900	REMOVE CLASS E FENCE	LF	30	
AR162630	CLASS E GATE-30'	EA	1	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	4,150	
AR401613	BIT. SURF. CSE. - METHOD I, SUPERPAVE	TON	665	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	506	
AR403613	BIT. BASE CSE. - METHOD I, SUPERPAVE	TON	1,195	
AR501508	8" PCC PAVEMENT	SY	815	
AR501530	PCC TEST BATCH	EA	1	
AR602510	BITUMINOUS PRIME COAT	GAL	1,815	
AR603510	BITUMINOUS TACK COAT	GAL	1,088	
AR620520	PAVEMENT MARKING-WATERBORNE	SF	1,320	
AR620525	PAVEMENT MARKING-BLACK BORDER	SF	1,320	
AR701512	12" RCP, CLASS IV	LF	42	
AR752412	PRECAST REINFORCED CONC. FES 12"	EA	2	
AR800372	BITUMINOUS & PCC PAVEMENT REMOVAL	SY	5,806	
AR803015	TEMPORARY PAVEMENT	SY	370	
AR904510	SODDING	SY	160	

**EARTHWORK QUANTITY SUMMARY (BASE BID):**

CUT.....1,590 CY  
 FILL ..... 120 CY  
 FILL + 15% ..... 138 CY

SUMMARY OF QUANTITIES - ADDITIVE ALTERNATE 1				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AS150530	TRAFFIC MAINTENANCE	LS	1	
AS610510	STRUCTURAL PC CONCRETE	CY	8.5	
AS620520	PAVEMENT MARKING-WATERBORNE	SF	30	
AS620525	PAVEMENT MARKING-BLACK BORDER	SF	30	
AS800372	BITUMINOUS & PCC PAVEMENT REMOVAL	SY	41	

**GENERAL NOTES:**

**QUANTITIES**  
 PAYMENT WILL BE MADE UNDER THE ITEM NUMBERS, DESCRIPTIONS AND UNITS NOTED IN THE ABOVE TABLE IN ACCORDANCE WITH THE BASIS OF PAYMENT FOR EACH RESPECTIVE WORK ITEM COMPLETED AND ACCEPTED BY THE ENGINEER.

**CERTIFIED PAYROLLS**  
 THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

**MATERIAL CERTIFICATIONS**  
 COMPLETED WORK CANNOT BE PLACED ON A CONSTRUCTION REPORT UNTIL ALL MATERIAL CERTIFICATIONS FOR THAT PAY ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER.

INDEX TO SHEETS	
SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	PROPOSED SCOPE OF WORK PLAN
4	SURVEY BASE SHEET
5	PROPOSED SAFETY AND PHASING PLAN - WORK AREA 1
6	PROPOSED SAFETY AND PHASING PLAN - WORK AREA 2
7	PROPOSED SAFETY AND PHASING PLAN - WORK AREA 3
8	CONSTRUCTION SAFETY AND PHASING DETAILS AND NOTES
9	EXISTING SITE AND DEMOLITION PLAN
10	PROPOSED TYPICAL SECTIONS
11	PROPOSED CONSTRUCTION PLAN
12	TAXILANE PLAN AND PROFILE
13	TAXILANE CONNECTOR PLAN AND PROFILE
14	PROPOSED JOINTING PLAN AND SPOT ELEVATION PLAN
15	PAVEMENT JOINTING DETAILS
16	PROPOSED MARKING PLAN
17	STORMWATER POLLUTION PREVENTION DETAILS
18	CONSTRUCTION PLAN - ADDITIVE ALTERNATE

REVISION	DATE

**SAINT LOUIS DOWNTOWN AIRPORT**  
 A Division of Bi-State Development Agency  
 BLOCK GRANT PROJ.: 3-17-0039-B30



IL PROJ.: CFS-4213

Hanson Project No.	12A0069	LAYOUT	MLH	02/06/13
Filename	G-002-FLP.dwg	DRAWN	MLH	02/06/13
Scale	NONE	REVIEWED	BSS	03/08/13
Date	03/08/13			



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 Springfield, Illinois 62703-2886

RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT

SUMMARY OF QUANTITIES AND INDEX TO SHEETS

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**UTILITY NOTE**

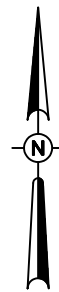
THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

**J.U.L.I.E. INFORMATION**

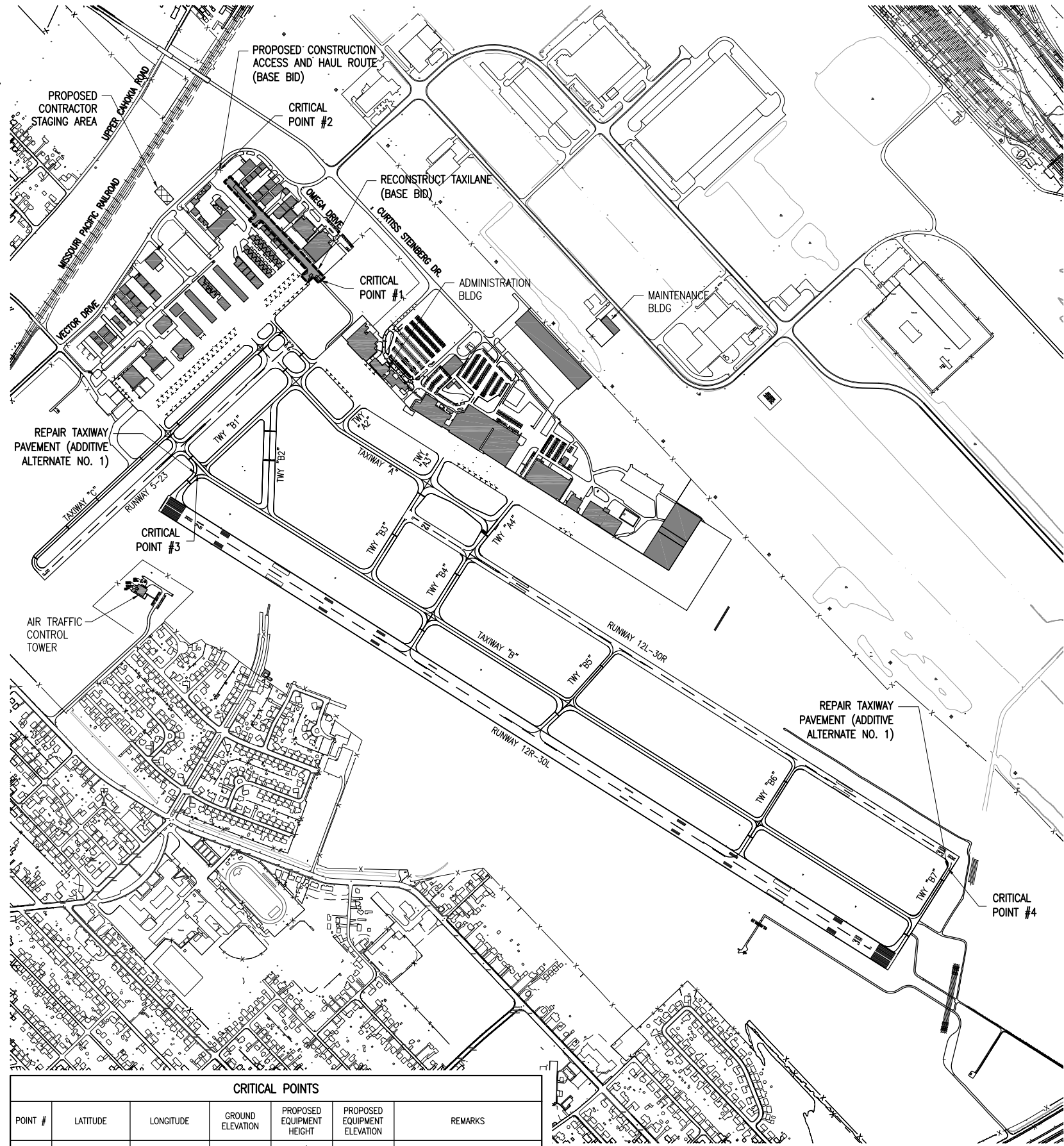
COUNTY ST. CLAIR  
 CITY CAHOKIA  
 TOWNSHIP CENTERVILLE  
 SECTION NO. T.1N. - R.10W.  
 ADDRESS ST. LOUIS DOWNTOWN AIRPORT  
 6100 ARCHVIEW DRIVE  
 CAHOKIA, IL 62206-1445

**LEGEND**

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED IMPROVEMENTS
- PROPOSED CONTRACTOR STAGING AREA
- EXISTING FENCE
- AIRPORT PROPERTY LINE



0' 250' 500' 1000'  
 HALF SIZE SCALE: 1" = 1000'  
 FULL SIZE SCALE: 1" = 500'



CRITICAL POINTS						
POINT #	LATITUDE	LONGITUDE	GROUND ELEVATION	PROPOSED EQUIPMENT HEIGHT	PROPOSED EQUIPMENT ELEVATION	REMARKS
1	N 38° 34' 44.30"	W 90° 09' 38.22"	407.2	25'	432.2	---
2	N 38° 34' 53.10"	W 90° 09' 48.50"	408.7	25'	433.7	---
3	N 38° 34' 28.77"	W 90° 09' 50.78"	411.5	25'	436.5	ADDITIVE ALTERNATE 1
4	N 38° 33' 57.10"	W 90° 08' 34.05"	405.0	25'	430.0	ADDITIVE ALTERNATE 1

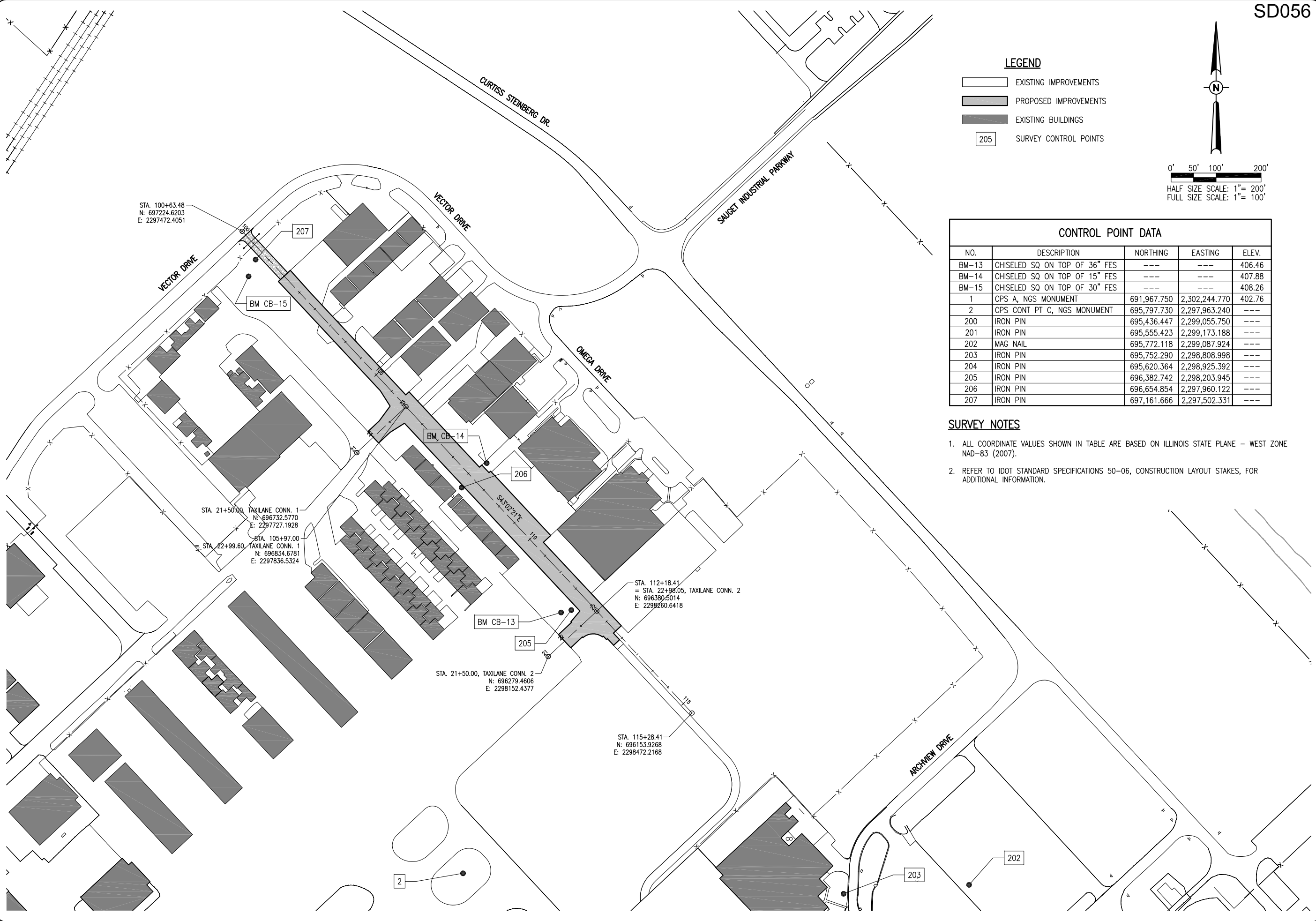
**GENERAL NOTES**

**SD056**





- THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS AND THEIR APPROXIMATE LOCATIONS AND LIMITS, FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. THIS SHEET SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE FOLLOWING PLAN SHEETS FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
- WORK FOR THIS PROJECT (BASE BID) SHALL CONSIST OF, BUT IS NOT LIMITED TO, RECONSTRUCTION OF AN ACCESS TAXILANE, INCLUDING PAVEMENT REMOVAL, AGGREGATE BASE COURSE, BITUMINOUS PAVING, CONCRETE PAVING, PAVEMENT MARKING AND OTHER ASSOCIATED ITEMS. AN ADDITIVE ALTERNATE BID CONSISTS OF TAXIWAY PAVEMENT REPAIRS, INCLUDING PAVEMENT REMOVAL AND CONCRETE PAVING.
- THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN.
- THE CONTRACTOR IS NOT PERMITTED TO USE THE AIRPORT ENTRANCE DRIVE AND AUTO PARKING LOT FOR MATERIAL AND EQUIPMENT HAULING OR STORAGE. THE CONSTRUCTION ENTRANCE AS SHOWN ON THE SCOPE OF WORK AND/OR SAFETY PHASING PLAN ARE ONLY TO BE USED FOR THE PROJECT. ACCESS TO THE PROJECT FOR ALL HAULING OF MATERIALS AND EQUIPMENT SHALL BE RESTRICTED TO THE DESIGNATED CONSTRUCTION ENTRANCE.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT.
- NO EQUIPMENT SHALL BE PERMITTED TO CROSS OR USE ANY EXISTING PAVEMENT OUTSIDE THE CONSTRUCTION LIMITS, GENERAL PROJECT AREA OR HAUL ROUTE.
- CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- UNLESS OTHERWISE NOTED, ALL DISTURBED AREAS OUTSIDE OF THE PROPOSED CONSTRUCTION LIMITS SHALL BE GRADED, SEEDED AND/OR HYDROMULCH SEEDED AT NO ADDITIONAL COST TO THE CONTRACT.
- ALL WASTE MATERIAL SHALL BE HAULED FROM THE AIRPORT AND PROPERLY DISPOSED OF UNLESS OTHERWISE SPECIFIED HEREIN.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
- THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE ONE SET OF REDLINED AS-BUILT DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT.
- CONTRACTOR SHALL NOTE THAT ALL AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING ANY WORK. ALL AREAS WHICH HAVE BEEN FARMED AND OR DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION, AND HAVE BEEN DISTURBED BY CONSTRUCTION ACTIVITY, SHALL BE CHISEL PLOWED (36" MAX.) OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER.)
- CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS TO BE REPAIRED, GRADED, AND MULCHED SEEDED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF THE HAUL ROUTE.
- THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS.
- APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER PERSONS FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE FOR ALL AREAS AFFECTED BY HIS WORK.

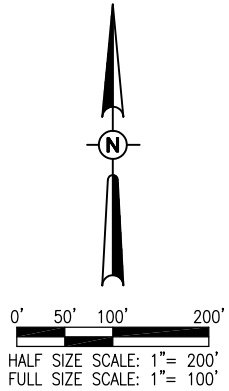
<p><b>SAINT LOUIS DOWNTOWN AIRPORT</b>                  A Division of Bi-State Development Agency                  BLOCK GRANT PROJ.: 3-17-0039-B30</p>	<p>REVISION                  DATE</p> <p>Hanson Project No. 12A0069                  G-003-SOW.dwg                  AS SHOWN                  03/08/13</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>LAYOUT</td> <td>MLH</td> <td>01/16/13</td> </tr> <tr> <td>DRAWN</td> <td>MLH</td> <td>01/16/13</td> </tr> <tr> <td>REVIEWED</td> <td>BSS</td> <td>03/08/13</td> </tr> </table> <p><b>HANSON</b>                  Copyright Hanson Professional Services Inc. 2013                  Hanson Professional Services Inc.                  1525 S. Brentwood Blvd., Suite 200                  Springfield, Illinois 62703-2886</p>	LAYOUT	MLH	01/16/13	DRAWN	MLH	01/16/13	REVIEWED	BSS	03/08/13
LAYOUT	MLH	01/16/13								
DRAWN	MLH	01/16/13								
REVIEWED	BSS	03/08/13								
<p>RECONSTRUCT TAXILANE                  IN NORTHWEST QUADRANT</p>	<p>PROPOSED SCOPE OF                  WORK PLAN</p>									
3										
3 of 18 sheets										

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LEGEND

-  EXISTING IMPROVEMENTS
-  PROPOSED IMPROVEMENTS
-  EXISTING BUILDINGS
-  SURVEY CONTROL POINTS



CONTROL POINT DATA

NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
BM-13	CHISELED SQ ON TOP OF 36" FES	---	---	406.46
BM-14	CHISELED SQ ON TOP OF 15" FES	---	---	407.88
BM-15	CHISELED SQ ON TOP OF 30" FES	---	---	408.26
1	CPS A, NGS MONUMENT	691,967.750	2,302,244.770	402.76
2	CPS CONT PT C, NGS MONUMENT	695,797.730	2,297,963.240	---
200	IRON PIN	695,436.447	2,299,055.750	---
201	IRON PIN	695,555.423	2,299,173.188	---
202	MAG NAIL	695,772.118	2,299,087.924	---
203	IRON PIN	695,752.290	2,298,808.998	---
204	IRON PIN	695,620.364	2,298,925.392	---
205	IRON PIN	696,382.742	2,298,203.945	---
206	IRON PIN	696,654.854	2,297,960.122	---
207	IRON PIN	697,161.666	2,297,502.331	---

SURVEY NOTES

- ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE - WEST ZONE NAD-83 (2007).
- REFER TO IDOT STANDARD SPECIFICATIONS 50-06, CONSTRUCTION LAYOUT STAKES, FOR ADDITIONAL INFORMATION.

STA. 100+63.48  
N: 697224.6203  
E: 2297472.4051

STA. 21+50.00, TAXILANE CONN. 1  
N: 696732.5770  
E: 2297727.1928

STA. 105+97.00  
STA. 22+99.60, TAXILANE CONN. 1  
N: 696834.6781  
E: 2297836.5324

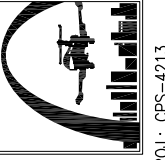
STA. 21+50.00, TAXILANE CONN. 2  
N: 696279.4606  
E: 2298152.4377

STA. 112+18.41  
= STA. 22+98.05, TAXILANE CONN. 2  
N: 696380.5014  
E: 2298260.6418

STA. 115+28.41  
N: 696153.9268  
E: 2298472.2168

REVISION	DATE

**SAINT LOUIS DOWNTOWN AIRPORT**  
A Division of Bi-State Development Agency  
BLOCK GRANT PROJ.: 3-17-0039-B30



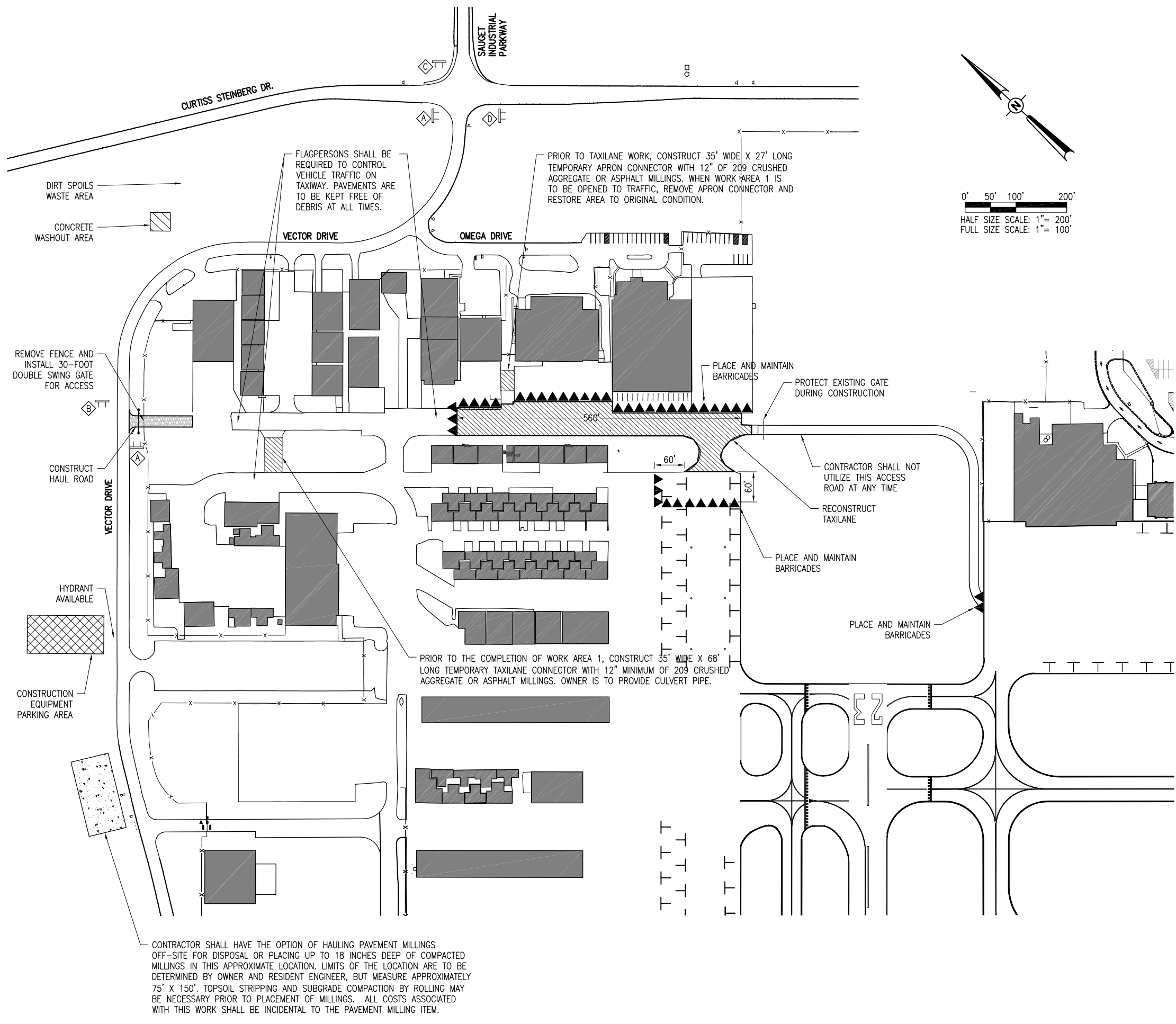
Hanson Project No.	12A0069	LAYOUT	DAW	01/11/13
Filename	C-004-SURV.dwg	DRAWN	DAW	01/11/13
Scale	AS SHOWN	REVIEWED	BSS	03/08/13
Date	03/08/13			

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RECONSTRUCT TAXILANE  
IN NORTHWEST QUADRANT

SURVEY BASE SHEET

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**LEGEND**

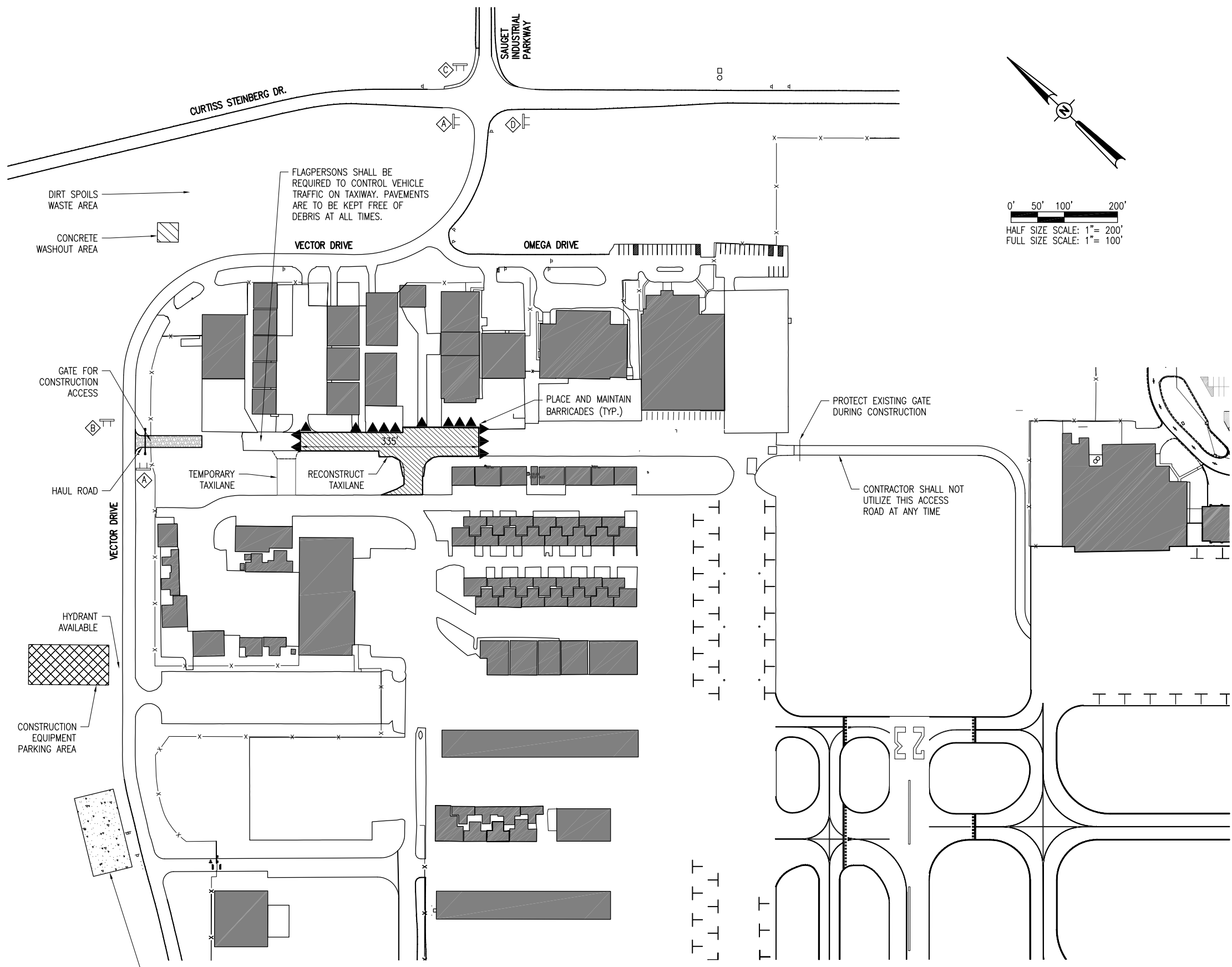
- EXISTING IMPROVEMENTS
- WORK AREA LIMITS
- EXISTING BUILDINGS
- PROPOSED EQUIPMENT PARKING AREA
- EXISTING FENCE
- PROPOSED BARRICADES
- CONSTRUCTION SIGN:
- "CONSTRUCTION TRAFFIC →"
- "← CONSTRUCTION TRAFFIC"
- "CONSTRUCTION TRAFFIC ↑"
- "NO CONSTRUCTION TRAFFIC"

**WORK AREA 1 NOTES**

1. THE WORK ITEMS TO BE COMPLETED IN THIS AREA INCLUDE CONSTRUCTION OF THE HAUL ROAD AND EQUIPMENT PARKING AREA, CONSTRUCTION OF TEMPORARY APRON CONNECTOR, REMOVAL OF THE EXISTING TAXILANE PAVEMENT, CONSTRUCTION OF THE NEW TAXILANE PAVEMENT, INCLUDING GRADING, PAVING, AND EROSION CONTROL.
2. THE CONTRACTOR SHALL HAVE A MAXIMUM OF 22 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK IN THIS AREA. CONCRETE CURE TIME MAY EXTEND BEYOND THIS DURATION IF NECESSARY TO MEET SPECIFICATION REQUIREMENTS TO OPEN PAVEMENT TO TRAFFIC. THE ONLY CONTRACTOR WORK ITEM THAT SHALL BE ALLOWED TO EXTEND BEYOND THE LISTED DURATION IS REMOVAL OF THE BARRICADES TO REOPEN THE PAVEMENT TO TRAFFIC. IF THE CONCRETE HAS NOT ACHIEVED THE STRENGTH REQUIRED TO OPEN PAVEMENT TO TRAFFIC AND THE CONTRACTOR HAS COMPLETED ALL OTHER WORK ITEMS IN THIS AREA, THE PROJECT CONTRACT TIME SHALL BE TEMPORARILY SUSPENDED UNTIL SUCH TIME THAT THE PAVEMENT CAN BE OPENED TO TRAFFIC AND THE CONTRACTOR PROCEEDS TO THE NEXT WORK AREA. THE CONTRACT TIME WILL THEN RESUME WITH THE FIRST DAY OF WORK IN THE NEXT AREA.
3. THE CONTRACTOR MAY NOT PROCEED TO THE NEXT WORK AREA UNTIL THIS AREA IS REOPENED TO AIRCRAFT TRAFFIC PER THE STANDARD SPECIFICATIONS UNLESS APPROVED IN ADVANCE BY THE OWNER.
4. CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
5. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.

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<p>REVISION</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th style="width: 10%;">DATE</th> <th style="width: 10%;">REVISION</th> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> <tr> <td> </td> <td> </td> </tr> </table>	DATE	REVISION							<p style="text-align: center;"><b>SAINT LOUIS DOWNTOWN AIRPORT</b></p> <p style="text-align: center; font-size: small;">A Division of Bi-State Development Agency          BLOCK GRANT PROJ.: 3-17-0039-B30</p> <div style="text-align: center;"> <p style="font-size: x-small;">© Copyright Hanson Professional Services Inc. 2013              Hanson Professional Services Inc.              1525 South Main Street              Springfield, Illinois 62703-2886</p> </div> <table border="1" style="width: 100%; border-collapse: collapse; font-size: x-small;"> <tr> <td style="width: 20%;">Hanson Project No.</td> <td style="width: 20%;">12A0069</td> <td style="width: 20%;">BSS</td> <td style="width: 20%;">01/31/13</td> </tr> <tr> <td>Filename</td> <td>C-005-SFY.dwg</td> <td>MLH</td> <td>01/31/13</td> </tr> <tr> <td>Scale</td> <td>AS SHOWN</td> <td>BSS</td> <td>03/08/13</td> </tr> <tr> <td>Date</td> <td>03/08/13</td> <td>REVIEWED</td> <td> </td> </tr> </table> <p style="text-align: center; font-weight: bold; font-size: 2em;">5</p> <p style="text-align: center; font-size: x-small;">5 of 18 sheets</p>	Hanson Project No.	12A0069	BSS	01/31/13	Filename	C-005-SFY.dwg	MLH	01/31/13	Scale	AS SHOWN	BSS	03/08/13	Date	03/08/13	REVIEWED	
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<p>RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT</p>	<p>PROPOSED SAFETY AND PHASING PLAN - WORK AREA 1</p>																								



**LEGEND**

- EXISTING IMPROVEMENTS
- WORK AREA LIMITS
- EXISTING BUILDINGS
- PROPOSED EQUIPMENT PARKING AREA
- EXISTING FENCE
- PROPOSED BARRICADES
- CONSTRUCTION SIGN:
- "CONSTRUCTION TRAFFIC ←"
- "→ CONSTRUCTION TRAFFIC"
- "CONSTRUCTION TRAFFIC ↑"
- "NO CONSTRUCTION TRAFFIC"

**WORK AREA 2 NOTES**

1. THE WORK ITEMS TO BE COMPLETED IN THIS AREA INCLUDE REMOVAL OF THE EXISTING TAXILANE PAVEMENT, CONSTRUCTION OF THE NEW TAXILANE PAVEMENT, INCLUDING GRADING, PAVING, AND EROSION CONTROL.
2. THE CONTRACTOR SHALL HAVE A MAXIMUM OF 14 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK IN THIS AREA. CONCRETE CURE TIME MAY EXTEND BEYOND THIS DURATION IF NECESSARY TO MEET SPECIFICATION REQUIREMENTS TO OPEN PAVEMENT TO TRAFFIC. THE ONLY CONTRACTOR WORK ITEM THAT SHALL BE ALLOWED TO EXTEND BEYOND THE LISTED DURATION IS REMOVAL OF THE BARRICADES TO REOPEN THE PAVEMENT TO TRAFFIC.
3. THE CONTRACTOR MAY NOT PROCEED TO THE NEXT WORK AREA UNTIL THIS AREA IS REOPENED TO AIRCRAFT TRAFFIC PER THE STANDARD SPECIFICATIONS UNLESS APPROVED IN ADVANCE BY THE OWNER.
4. CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
5. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.

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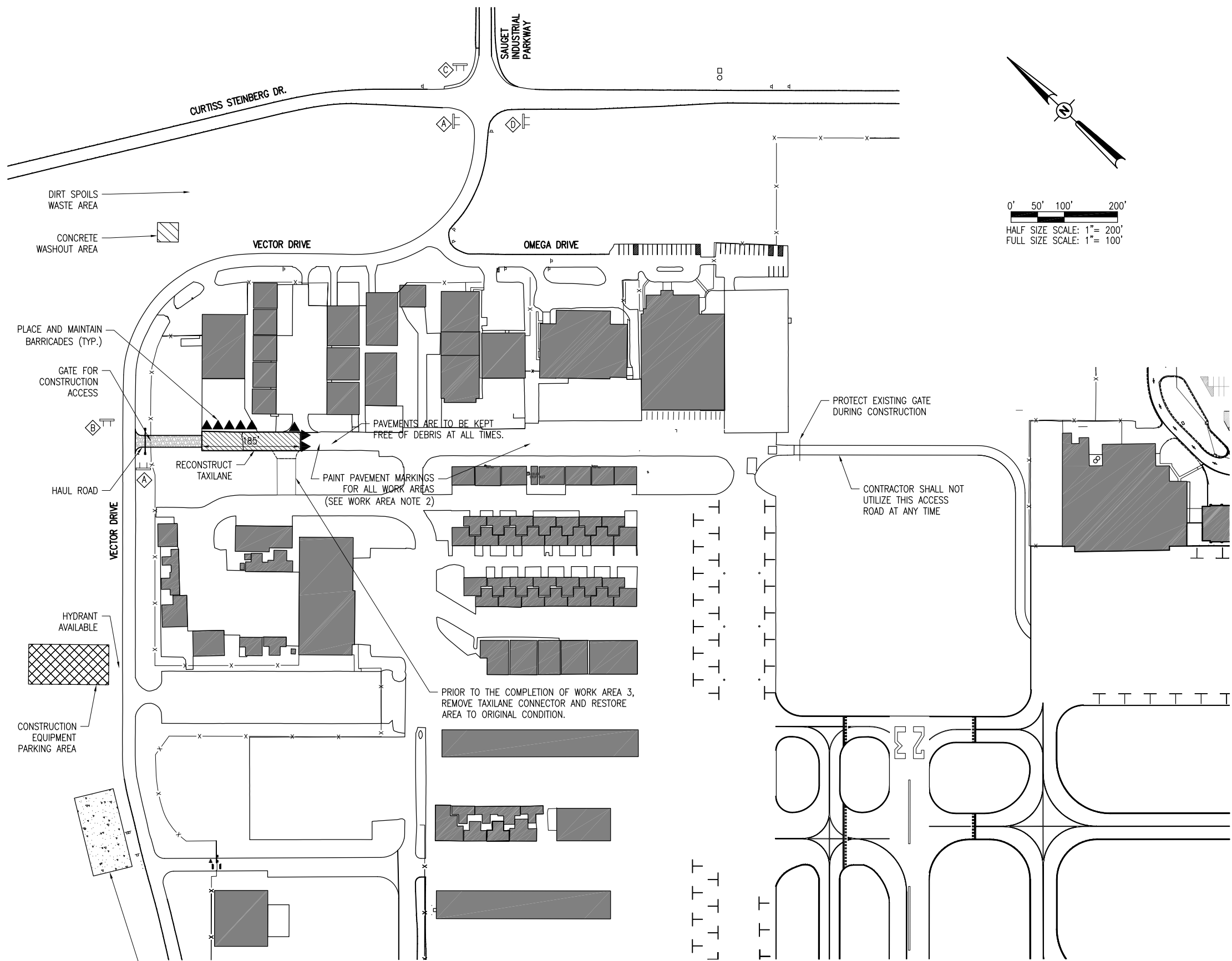
**SAINT LOUIS DOWNTOWN AIRPORT**  
A Division of Bi-State Development Agency  
BLOCK GRANT PROJ.: 3-17-0039-B30  
IL PROJ.: CFS-4213

Hanson Project No.	12A0069	BSS	01/31/13
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RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT  
PROPOSED SAFETY AND PHASING PLAN - WORK AREA 2

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**LEGEND**

- EXISTING IMPROVEMENTS
- WORK AREA LIMITS
- EXISTING BUILDINGS
- PROPOSED EQUIPMENT PARKING AREA
- EXISTING FENCE
- PROPOSED BARRICADES
- CONSTRUCTION SIGN:
  - "CONSTRUCTION TRAFFIC →"
  - "← CONSTRUCTION TRAFFIC"
  - "CONSTRUCTION TRAFFIC ↑"
  - "NO CONSTRUCTION TRAFFIC"

**WORK AREA 3 NOTES**

1. THE WORK ITEMS TO BE COMPLETED IN THIS AREA INCLUDE REMOVAL OF THE EXISTING TAXILANE PAVEMENT, CONSTRUCTION OF THE NEW TAXILANE PAVEMENT, INCLUDING GRADING, PAVING, MARKING, AND EROSION CONTROL.
2. DURING PAVEMENT MARKING OPERATIONS OVER THE ENTIRE PROJECT LIMITS (ALL WORK AREAS) THE ENTIRE TAXILANE SHALL BE CLOSED TO AIRCRAFT TRAFFIC. CONTRACTOR TO COORDINATE PAINTING SCHEDULE WITH THE RESIDENT ENGINEER TO OCCUR DURING NON-PEAK HOURS.
2. THE CONTRACTOR SHALL HAVE A MAXIMUM OF 14 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK IN THIS AREA. CONCRETE CURE TIME MAY EXTEND BEYOND THIS DURATION IF NECESSARY TO MEET SPECIFICATION REQUIREMENTS TO OPEN PAVEMENT TO TRAFFIC. THE ONLY CONTRACTOR WORK ITEM THAT SHALL BE ALLOWED TO EXTEND BEYOND THE LISTED DURATION IS REMOVAL OF THE BARRICADES TO REOPEN THE PAVEMENT TO TRAFFIC.
3. CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
4. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
5. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROAD (BETWEEN VECTOR DRIVE AND THE TAXILANE), CULVERT, AND GATE ARE TO BE LEFT IN PLACE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADDING STONE TO THE HAUL ROAD AS NECESSARY TO LEAVE IT IN A CONDITION SUITABLE FOR AIRPORT USE (INCIDENTAL). THE CONSTRUCTION EQUIPMENT PARKING AREA AND CONCRETE WASH AREA ARE TO BE REMOVED AND THE RESPECTIVE SITES RESTORED PER THE SPECIFICATIONS.

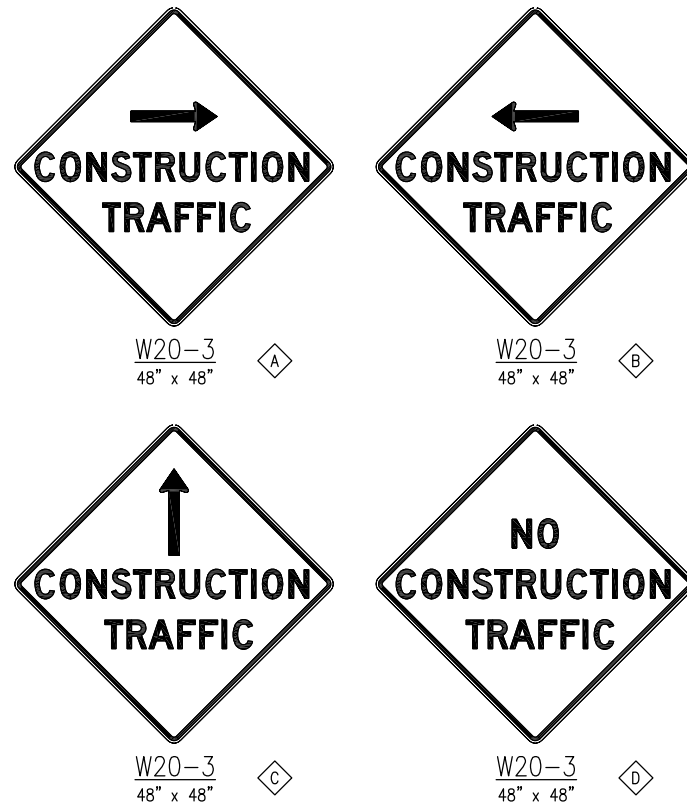
CONTRACTOR SHALL HAVE THE OPTION OF HAULING PAVEMENT MILLINGS OFF-SITE FOR DISPOSAL OR PLACING UP TO 18 INCHES DEEP OF COMPACTED MILLINGS IN THIS APPROXIMATE LOCATION. LIMITS OF THE LOCATION ARE TO BE DETERMINED BY OWNER AND RESIDENT ENGINEER, BUT MEASURE APPROXIMATELY 75' X 150'. TOPSOIL STRIPPING AND SUBGRADE COMPACTION BY ROLLING MAY BE NECESSARY PRIOR TO PLACEMENT OF MILLINGS. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE INCIDENTAL TO THE PAVEMENT MILLING ITEM.

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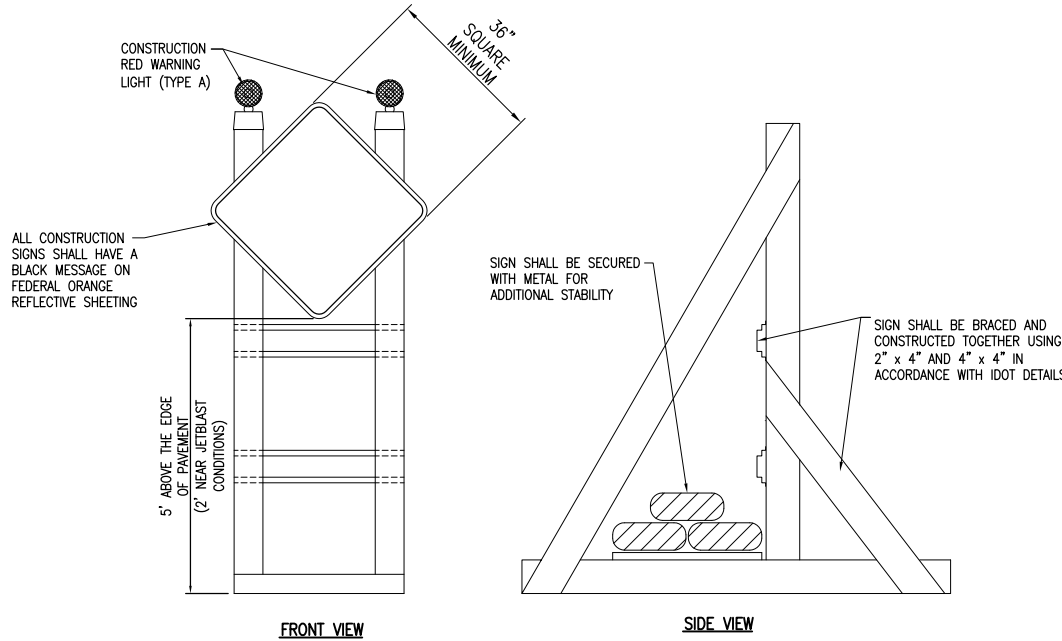
<p><b>SAINT LOUIS DOWNTOWN AIRPORT</b>                  A Division of Bi-State Development Agency                  BLOCK GRANT PROJ.: 3-17-0039-B30</p>	<p>REVISION                  DATE</p>																				
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<p><b>7</b>                  7 of 18 sheets</p>																					

**SAFETY NOTES**

- FOLLOWING ARE THE CONSTRUCTION SAFETY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE SAFETY AND PHASING NOTES AND DETAILS SHEET.
- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE PROJECT SAFETY AND PHASING PLANS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE RESIDENT ENGINEER AND/OR AIRPORT MANAGER RESERVE THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 200' OF ANY ACTIVE RUNWAY, WITHIN 50' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE ENGINEER) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO OPEN TRENCHES WITHIN 200' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 50' OF ANY AIRPORT OPERATIONS AREA WILL BE PERMITTED UNLESS PROPERLY MARKED. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING YELLOW LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT. HOWEVER OTHER EQUIPMENT TALLER THAN 25' MAY BE PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER. NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEP, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED AND YELLOW LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 200' OF ANY ACTIVE RUNWAY CENTERLINE OR WITHIN 50' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 72 HOURS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED BY THE AIRPORT MANAGER TO THE APPROPRIATE FLIGHT SERVICE STATION.
- CONTRACTOR MAY WORK WITHIN OFZ WITHOUT PLACING RUNWAY CLOSED MARKERS IF HE CAN DEMONSTRATE THAT THE OFZ CAN BE CLEARED OF ALL MEN, EQUIPMENT AND OBSTRUCTIONS WITHIN A 15 MINUTE TIME PERIOD BY MONITORING UNICOM FREQUENCY 122.95 AND CLEARING OFZ AT THE REQUEST OF PILOTS. RADIO(S) TO BE PROVIDED BY CONTRACTOR TO ALL CREWS NOT WITHIN MONITORING RANGE OF THE RADIO. (OWNER TO RESTRICT TOUCH AND GO'S DURING CONSTRUCTION WORK HOURS)
- UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.



**CONSTRUCTION SIGNS**  
NOT TO SCALE

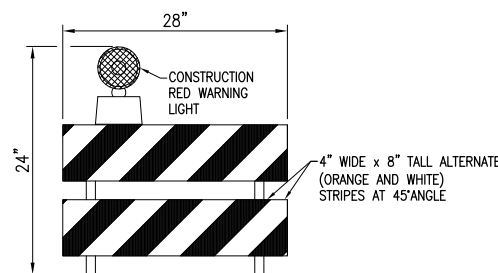


**SIGNAGE NOTES**

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- UNLESS OTHERWISE SPECIFIED, CONSTRUCTION SIGNS SHALL BE MOUNTED ON PORTABLE OR NON-PORTABLE SUPPORTS. A PORTABLE SUPPORT IS DEFINED AS A TYPICAL SIGN STANDARD AS SHOWN ON THIS SHEET, OR A SMALL LIGHT WEIGHT TRAILER. A NON-PORTABLE SUPPORT IS DEFINED AS DRIVEN METAL OR WOOD POST. ALL SIGNS, REGARDLESS OF THE TYPE OF SUPPORTS USED, SHALL BE MOUNTED SUCH THAT THE MESSAGE ON THE SIGN IS LEVEL IN THE HORIZONTAL PLANE AFTER PLACEMENT. THE COST OF CONSTRUCTION WARNING LIGHTS SHALL BE INCLUDED IN THE COST OF THE CONSTRUCTION SIGNS.
- CONSTRUCTION RED WARNING LIGHT- THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY ARE TO BE USED IN A LOW INTENSITY FLASHING MODE (TYPE A).
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- COST FOR PLACING, MAINTAINING, AND REMOVING SIGNS SHALL BE INCLUDED IN OTHER CONTRACT ITEMS.

**BARRICADE NOTES**

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- MODIFIED TYPE II BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT IN 4' INCREMENTS AS DIRECTED BY THE ENGINEER. BARRICADES ARE TO BE SET BACK 66' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT- THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON TYPE II BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PLACING, MAINTAINING, AND REMOVING BARRICADES SHALL BE INCLUDED IN OTHER CONTRACT ITEMS.



**MODIFIED TYPE II BARRICADE**  
NOT TO SCALE

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**SAINT LOUIS DOWNTOWN AIRPORT**  
A Division of Bi-State Development Agency  
BLOCK GRANT PROJ.: 3-17-0039-B30

IL PROJ.: CFS-4213

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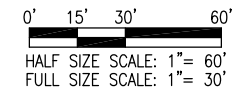
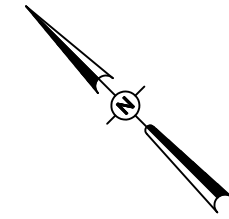
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RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT

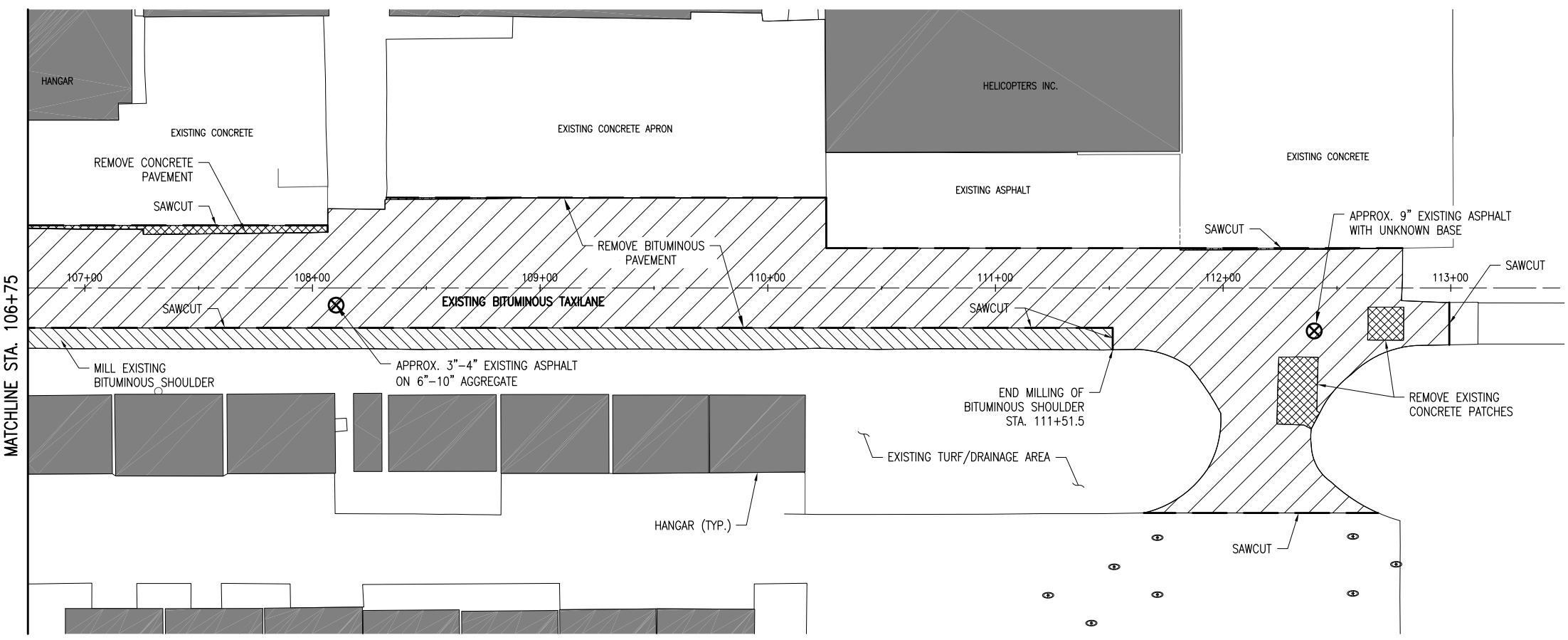
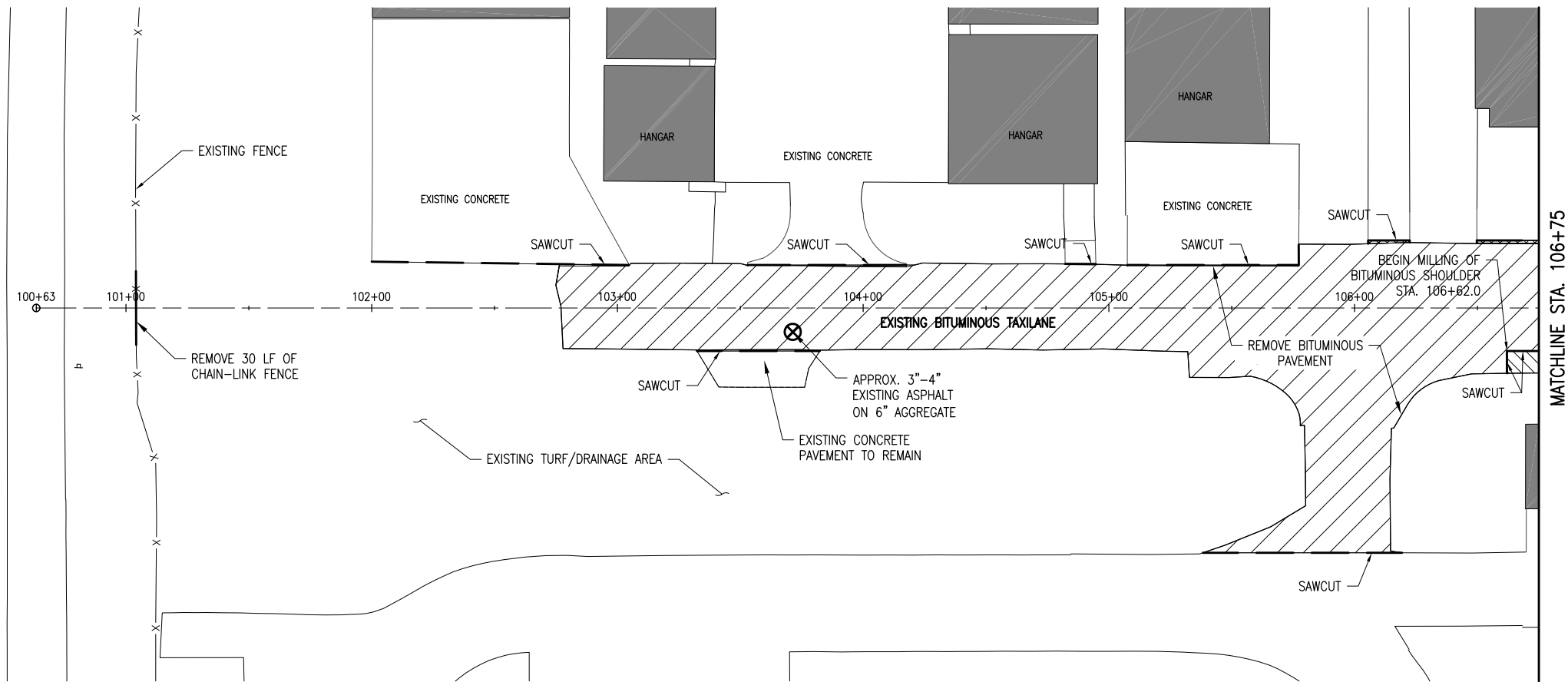
CONSTRUCTION SAFETY AND PHASING DETAILS AND NOTES

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- LEGEND**
- EXISTING PAVEMENT
  - PAVEMENT REMOVAL
  - PAVEMENT MILLING
  - CONCRETE REMOVAL
  - EXISTING BUILDINGS
  - EXISTING FENCE
  - PAVEMENT CORE LOCATION (APPROX.)



REVISION	DATE

**SAINT LOUIS DOWNTOWN AIRPORT**  
 A Division of Bi-State Development Agency  
 BLOCK GRANT PROJ.: 3-17-0039-B30  
 IL PROJ.: CPS-4213

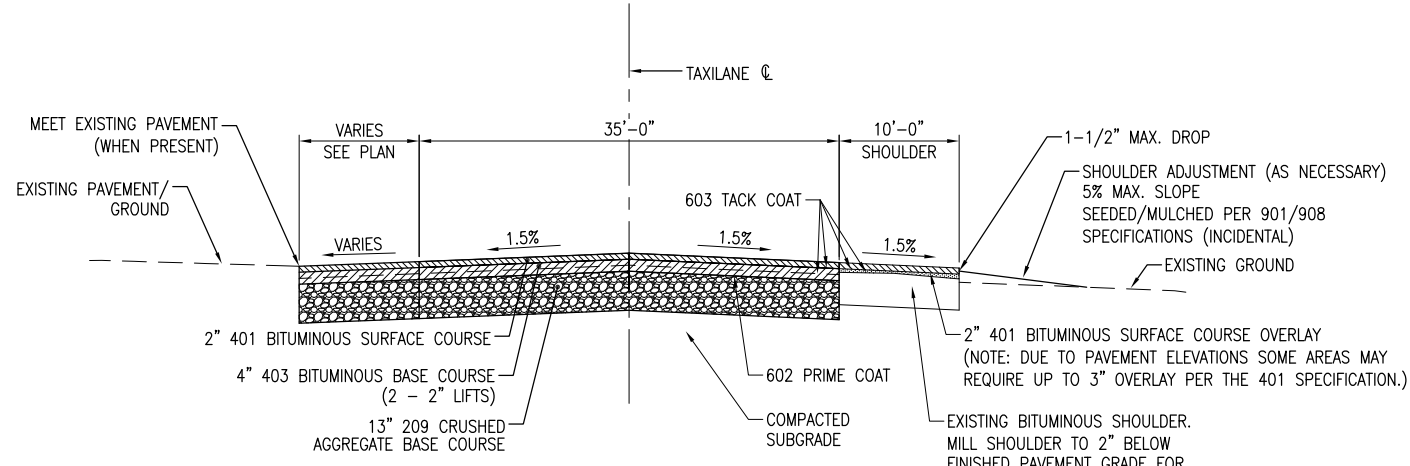
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REVIEWED	BSS 03/08/13

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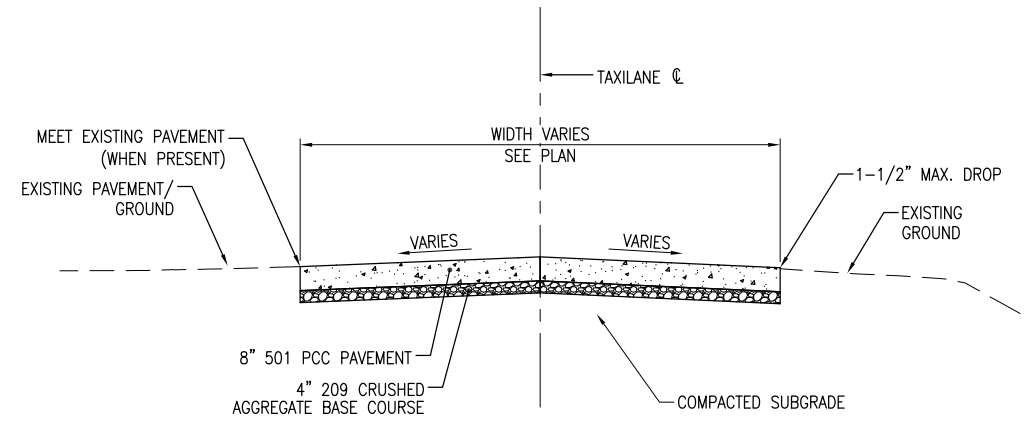
RECONSTRUCT TAXILANE  
 IN NORTHWEST QUADRANT

EXISTING SITE AND  
 DEMOLITION PLAN

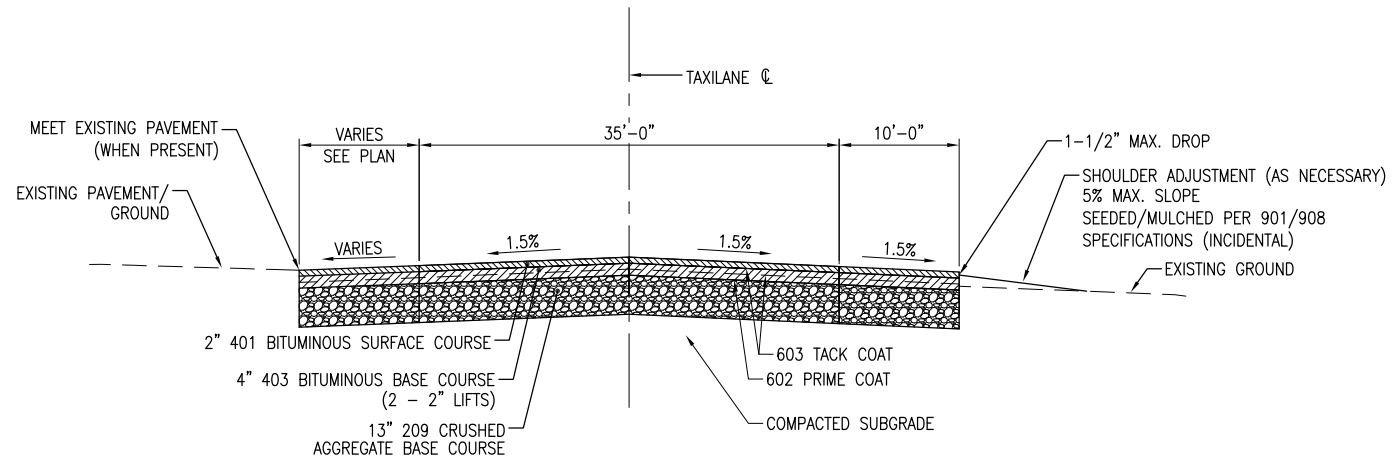
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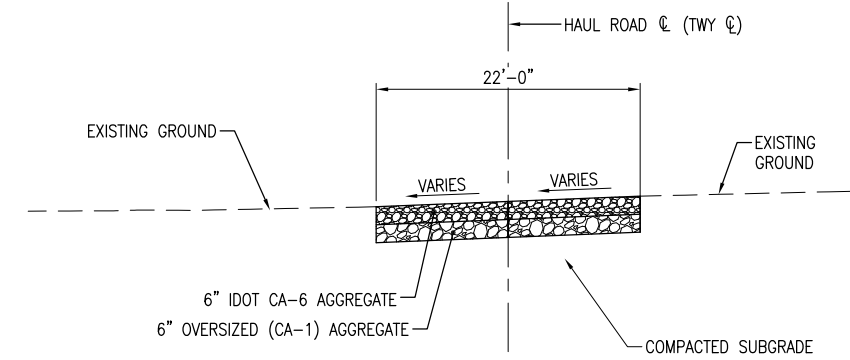
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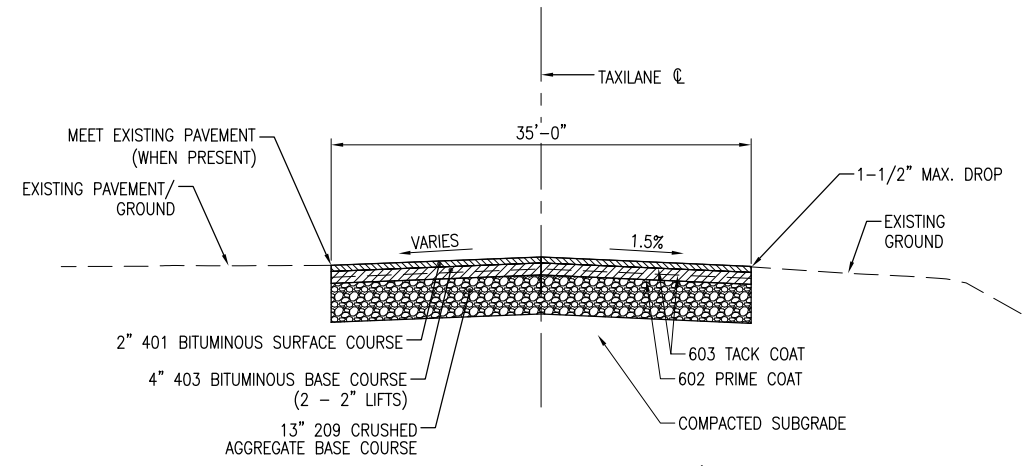
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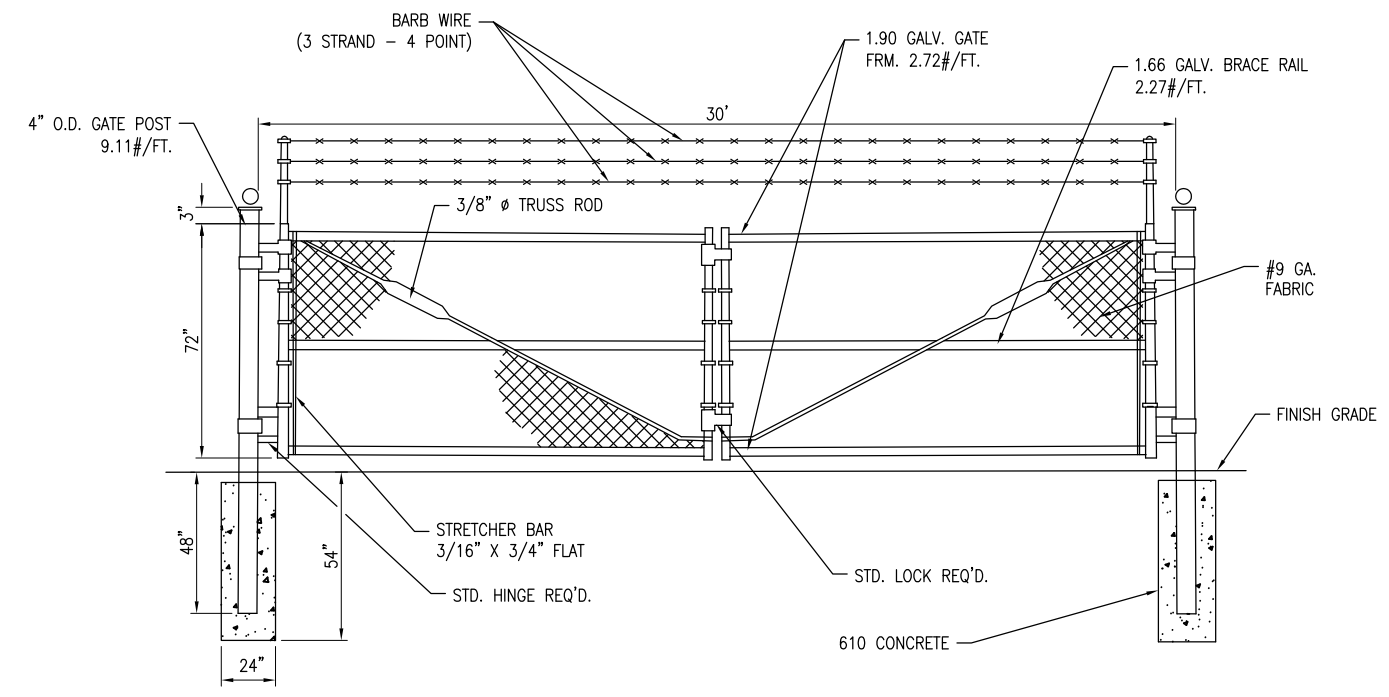
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 STA. 111+51.49 TO STA. 111+98.41  
 (SLOPES AND DIMENSIONS VARY AT INTERSECTIONS)  
 NOT TO SCALE



**TYPICAL SECTION - HAUL ROAD**  
 NOT TO SCALE



**TYPICAL SECTION - TAXILANE/  
 TAXILANE CONNECTOR 1**  
 STA. 102+00 TO STA. 105+77.39  
 (SLOPES AND DIMENSIONS VARY AT INTERSECTIONS)  
 NOT TO SCALE



**MANUAL DOUBLE SWING CHAIN LINK GATE**  
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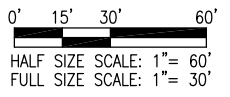
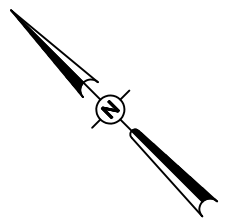
REVISION
DATE

**SAINT LOUIS DOWNTOWN AIRPORT**  
 A Division of Bi-State Development Agency  
 BLOCK GRANT PROJ.: 3-17-0039-B30  
 IL PROJ.: CPS-4213

Hanson Project No.	12A0069	LAYOUT	MLH	02/04/13
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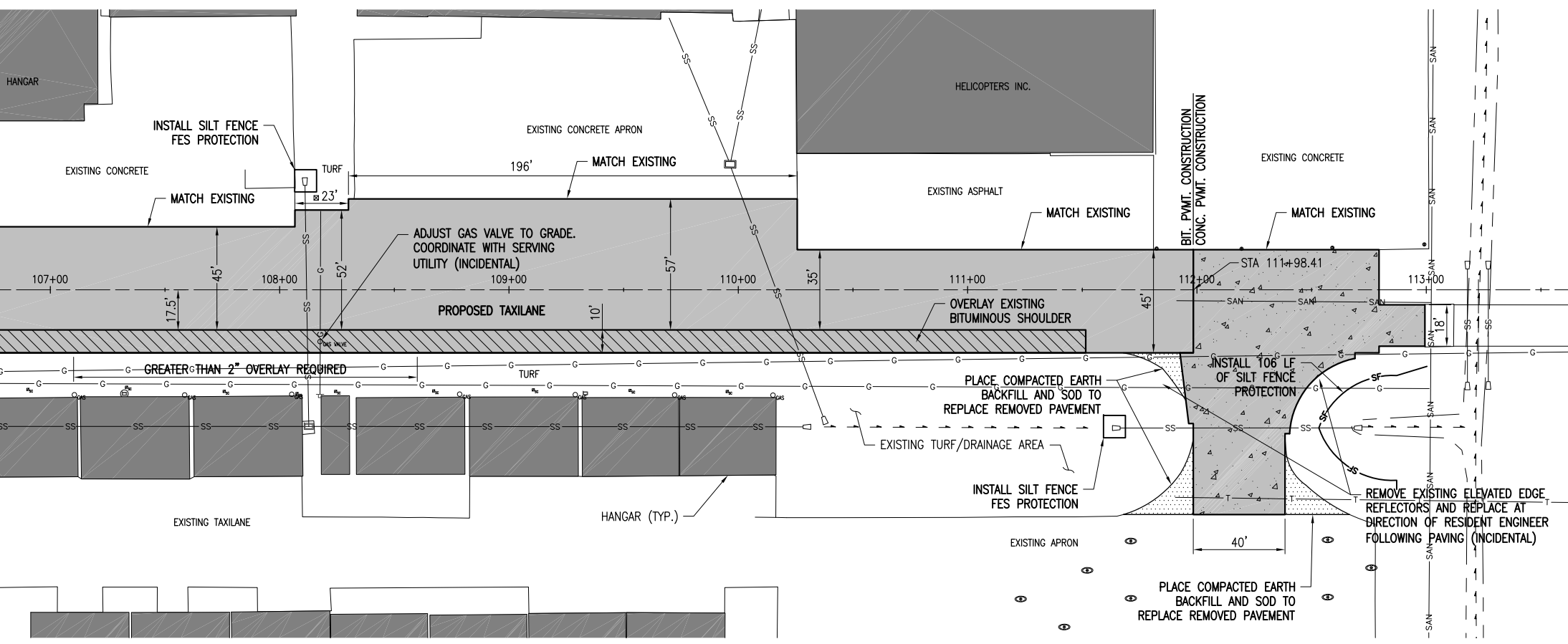
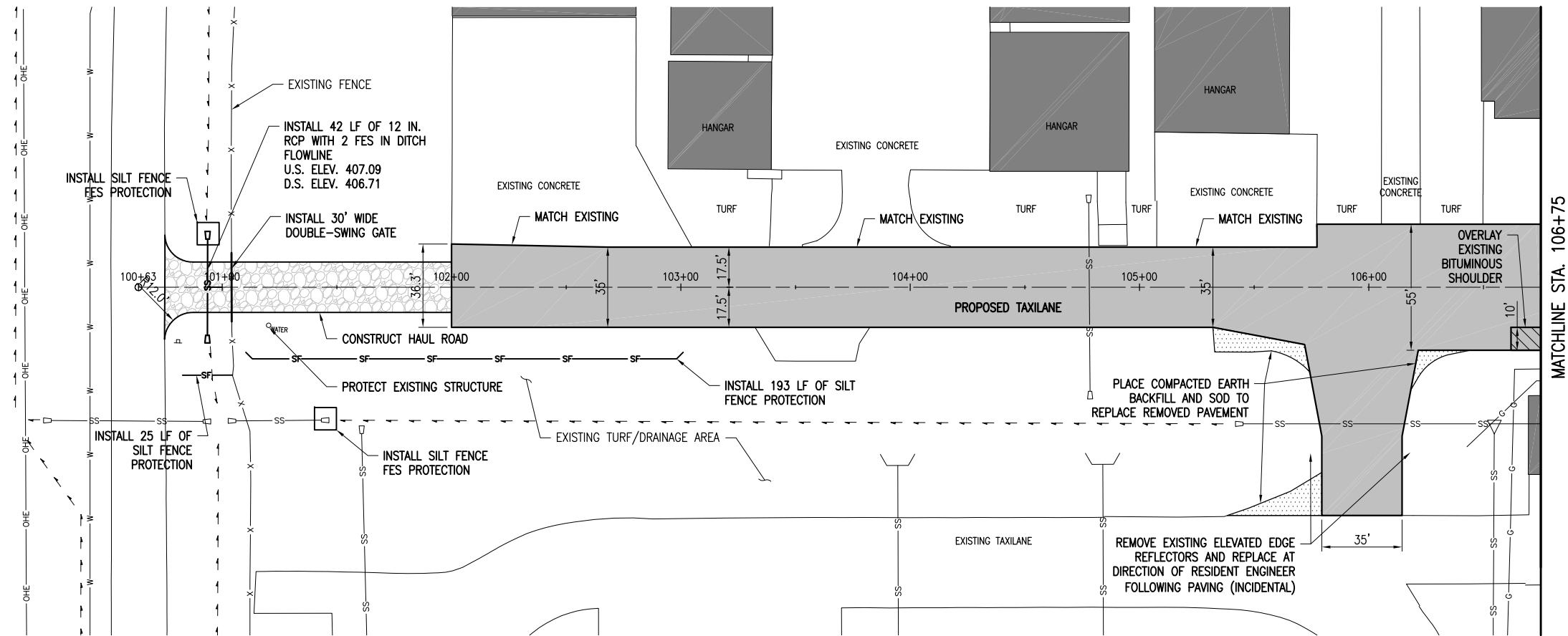
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RECONSTRUCT TAXILANE  
 IN NORTHWEST QUADRANT  
 PROPOSED TYPICAL  
 SECTIONS



**LEGEND**

- EXISTING BUILDING
- PROPOSED BITUMINOUS PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- PROPOSED BITUMINOUS OVERLAY
- PROPOSED AGGREGATE
- PROPOSED SODDING
- EXISTING FENCE
- EXISTING STORM SEWER
- EXISTING SANITARY
- EXISTING DITCH
- EXISTING STORM INLET/MAHOLE
- PROPOSED STORM SEWER



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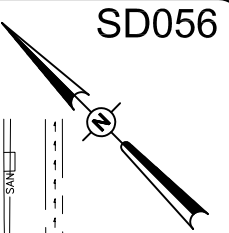
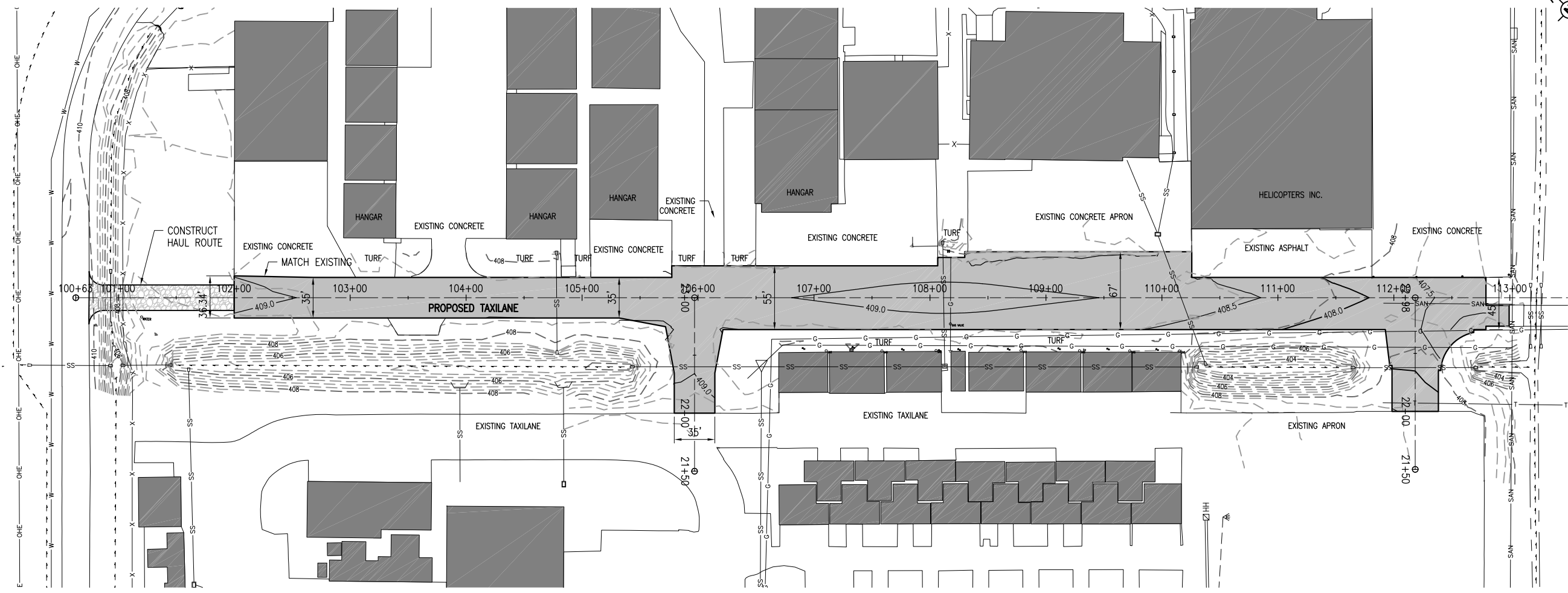
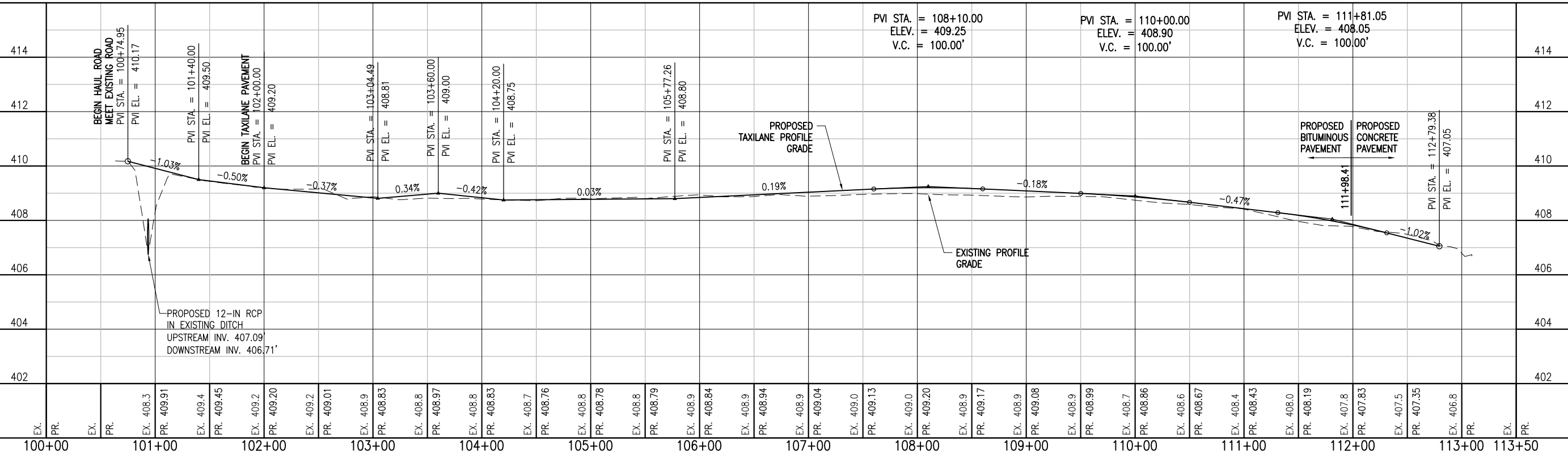
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Filename	C-121-CON.dwg
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Date	03/08/13
LAYOUT	DAW 01/16/13
DRAWN	JRH 01/16/13
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RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT  
 PROPOSED CONSTRUCTION PLAN

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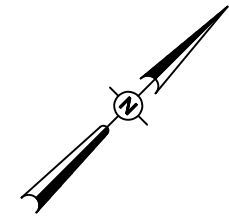
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MLH	01/22/13
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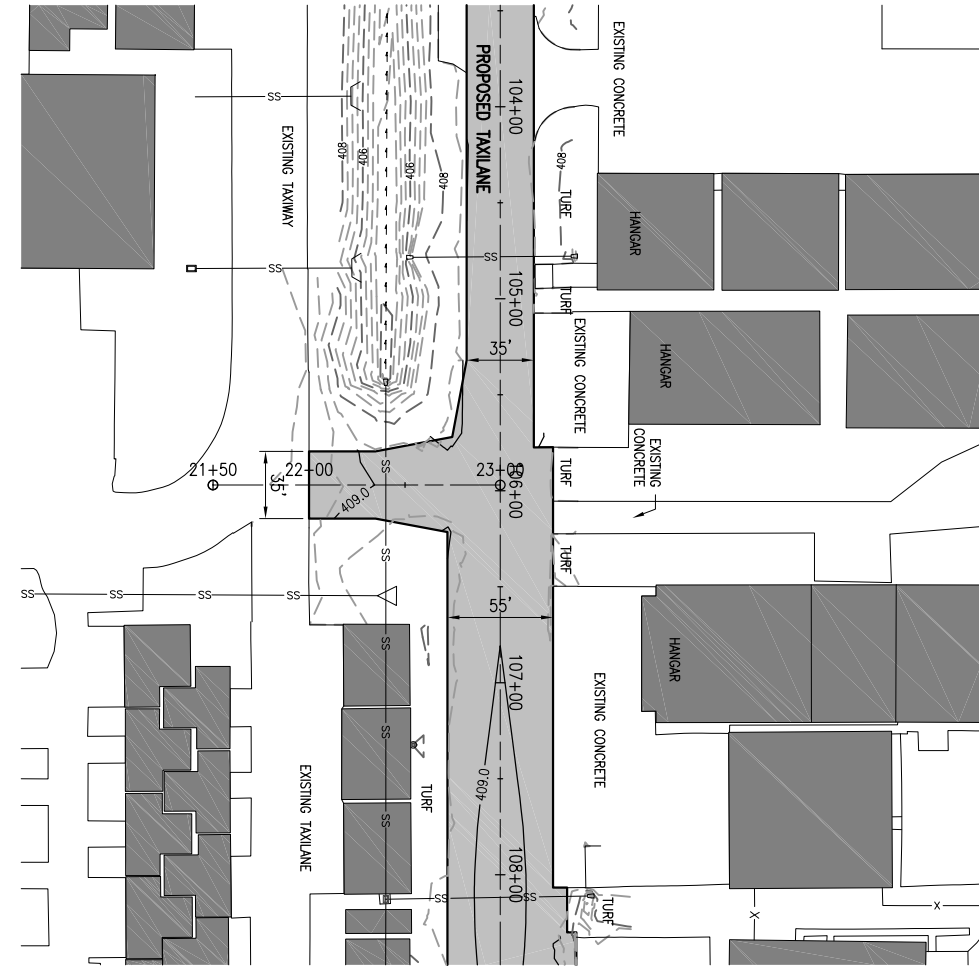
RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT  
 TAXILANE PLAN AND PROFILE



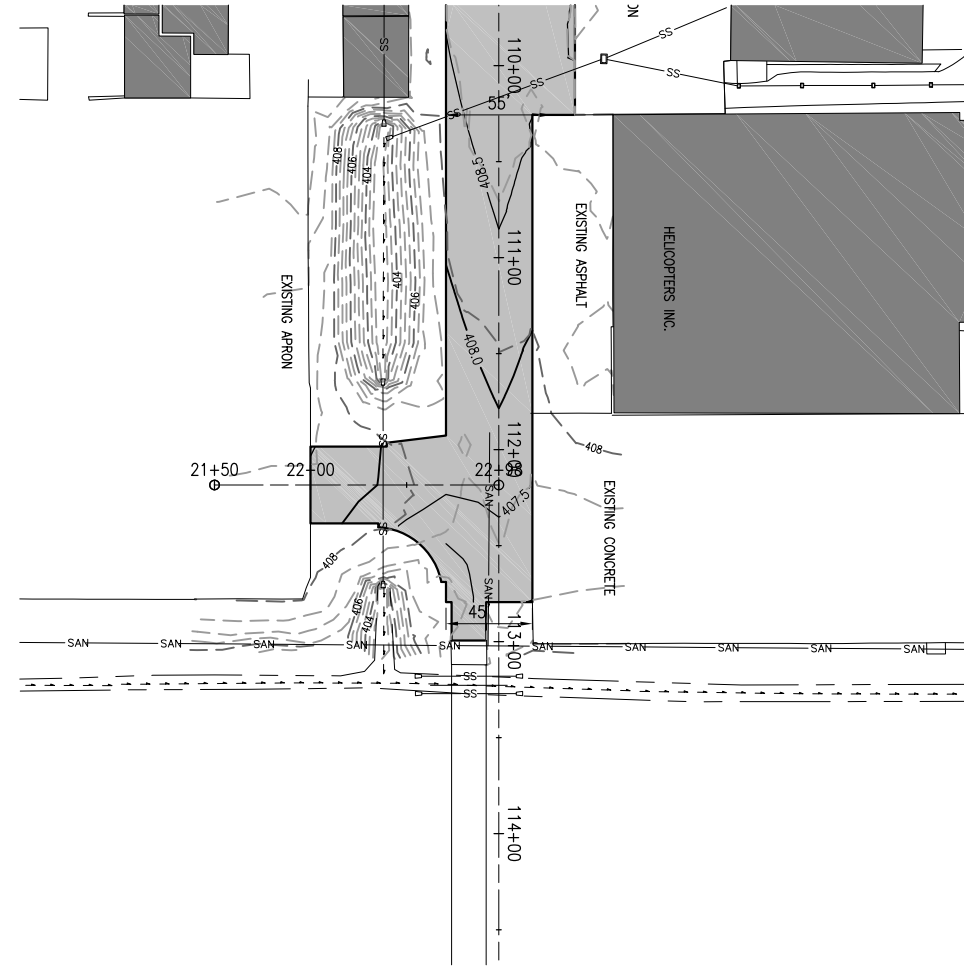
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 FULL SIZE SCALE: 1" = 50'

**LEGEND**

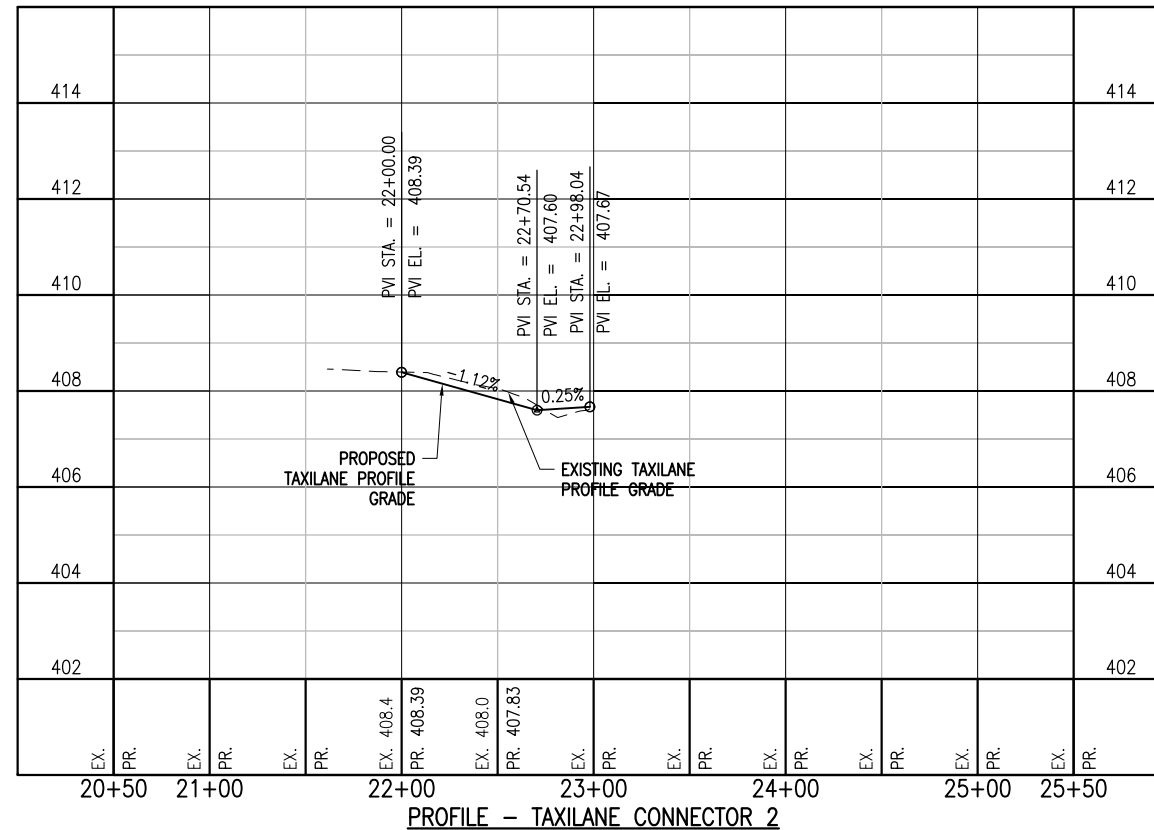
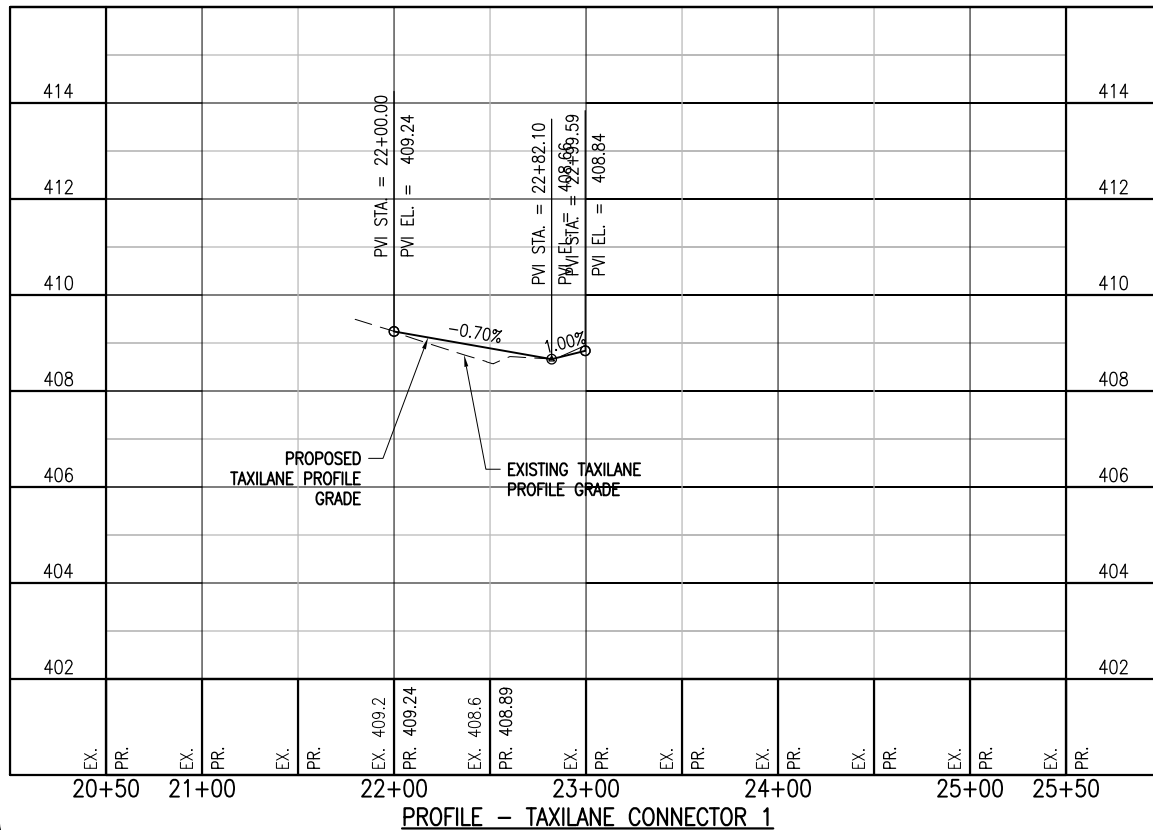
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- EXISTING FENCE
- EXISTING STORM SEWER
- EXISTING SANITARY
- EXISTING DITCH
- EXISTING STORM INLET/MAHOLE



**PLAN - TAXILANE CONNECTOR 1**



**PLAN - TAXILANE CONNECTOR 2**



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AIRPORT

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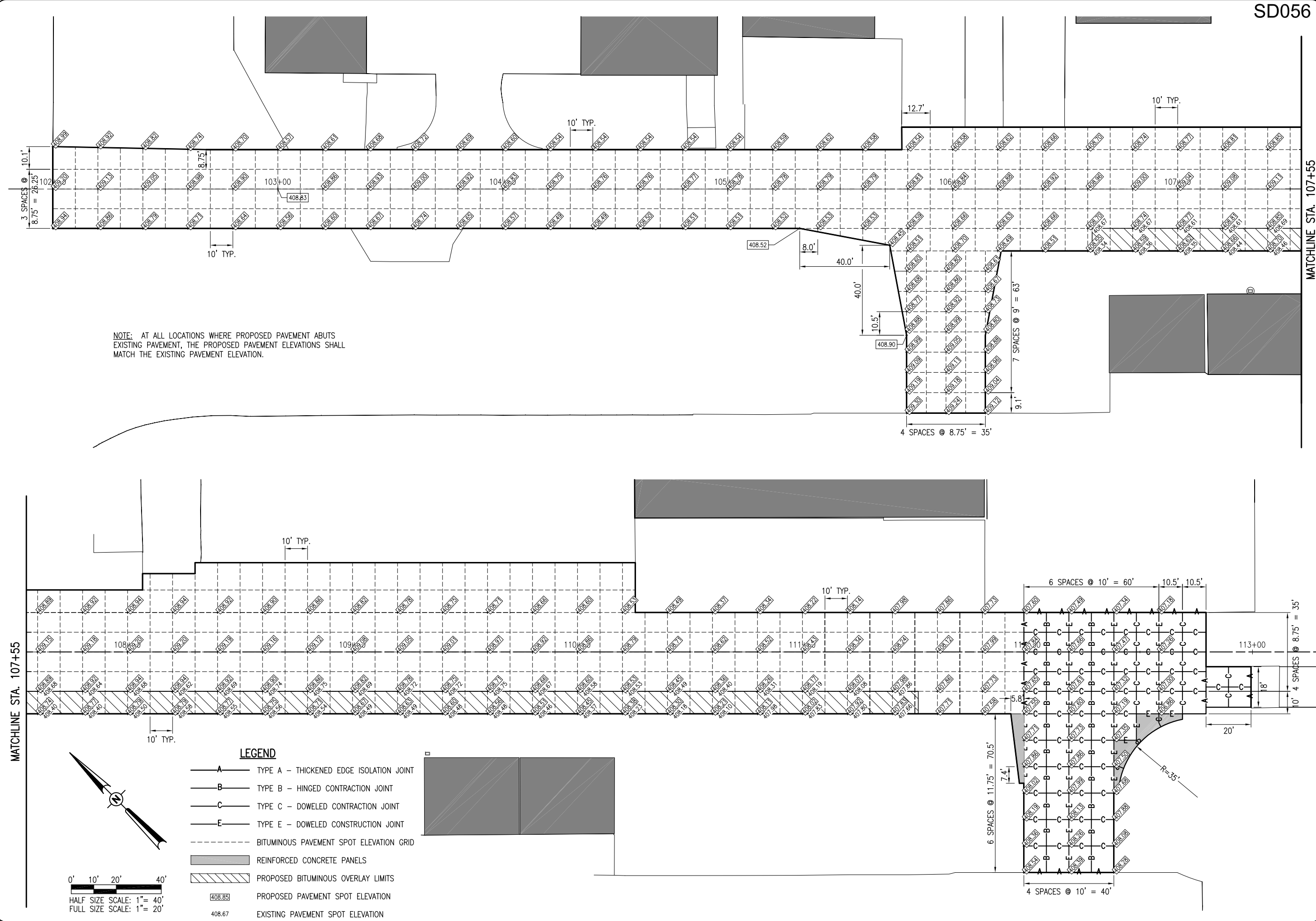
RECONSTRUCT TAXILANE  
IN NORTHWEST QUADRANT

TAXILANE CONNECTOR PLAN  
AND PROFILE

13

13 of 18 sheets

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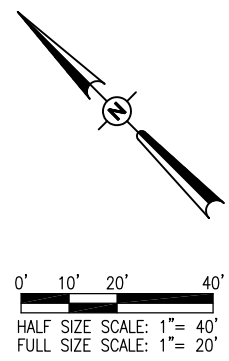


SD056

MATCHLINE STA. 107+55

MATCHLINE STA. 107+55

NOTE: AT ALL LOCATIONS WHERE PROPOSED PAVEMENT ABUTS EXISTING PAVEMENT, THE PROPOSED PAVEMENT ELEVATIONS SHALL MATCH THE EXISTING PAVEMENT ELEVATION.



**LEGEND**

- TYPE A - THICKENED EDGE ISOLATION JOINT
- TYPE B - HINGED CONTRACTION JOINT
- TYPE C - DOWELED CONTRACTION JOINT
- TYPE E - DOWELED CONSTRUCTION JOINT
- BITUMINOUS PAVEMENT SPOT ELEVATION GRID
- REINFORCED CONCRETE PANELS
- PROPOSED BITUMINOUS OVERLAY LIMITS
- PROPOSED PAVEMENT SPOT ELEVATION
- EXISTING PAVEMENT SPOT ELEVATION

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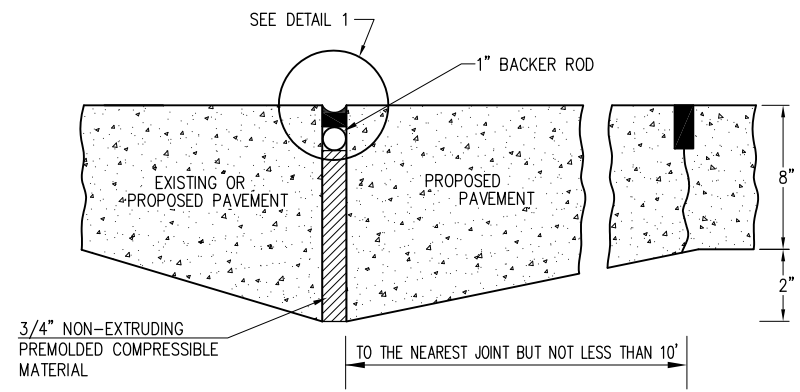
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REVIEWED	BSS 03/08/13

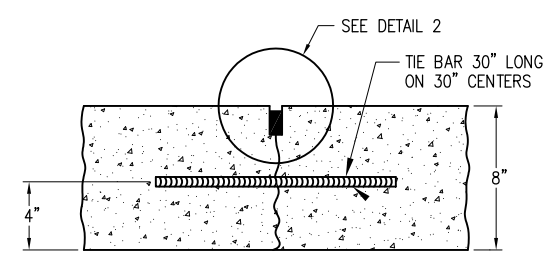
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RECONSTRUCT TAXILANE  
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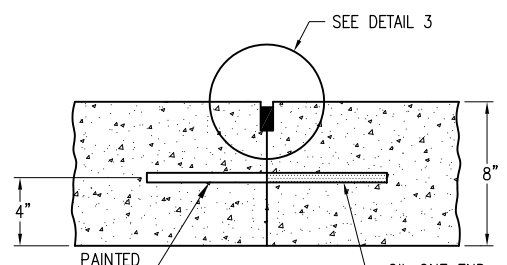
PROPOSED JOINTING PLAN  
 AND SPOT ELEVATION PLAN



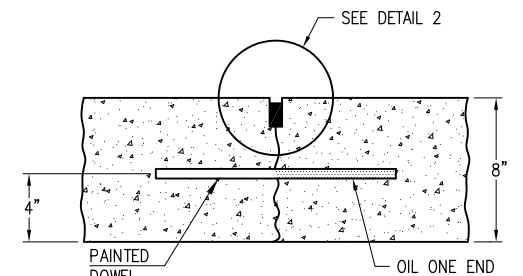
**TYPE-A THICKENED EDGE ISOLATION JOINTS**



**TYPE B HINGED**



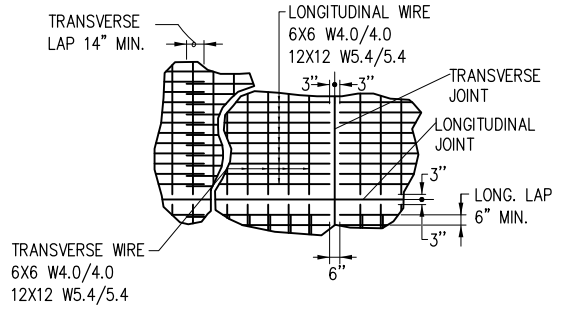
**TYPE E DOWELED**



**TYPE C DOWELED**

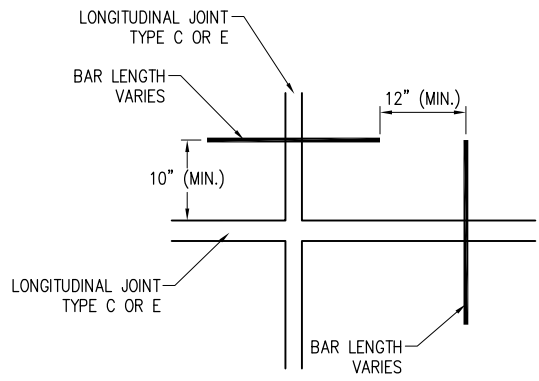
**CONSTRUCTION JOINTS**

**CONTRACTION JOINTS**



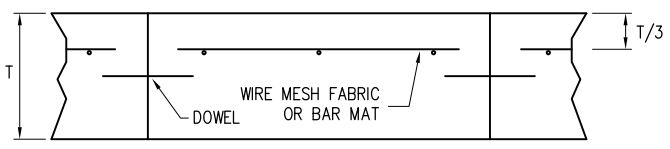
**REINFORCEMENT SHEET WIRE FABRIC DETAIL**

NOTE: DIFFERENT WIRE MESH SIZES/CONFIGURATIONS OR SIZES ARE PERMITTED IF THE MINIMUM CROSS SECTIONAL AREA EQUALS 0.5 OR GREATER.



POSITION OF DOWELS AT EDGE OF JOINT TYPE C OR E

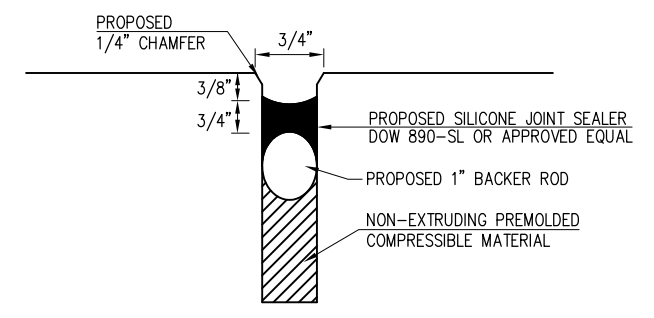
**DOWEL PLAN VIEW**



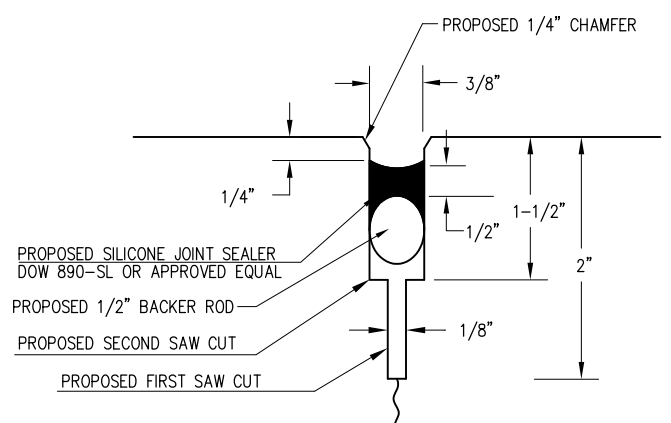
**PAVEMENT REINFORCING DETAIL**

**REINFORCING NOTES:**

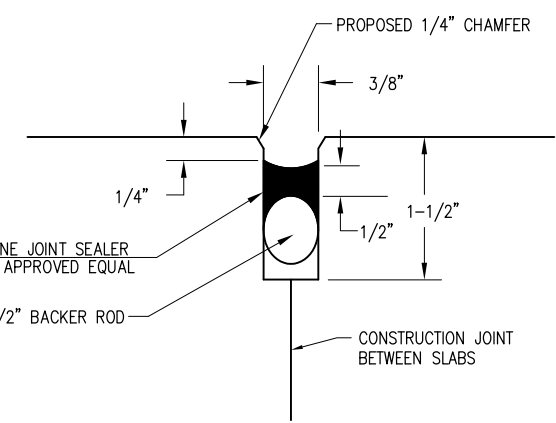
1. END LAPS SHALL BE A MINIMUM OF 12", BUT NOT LESS THAN 30 TIMES THE DIAMETER OF THE LONGITUDINAL WIRE OR BAR.
2. SIDE LAPS SHALL BE A MINIMUM OF 6", BUT NOT LESS THAN 20 TIMES THE DIAMETER OF TRANSVERSE WIRE OR BAR.
3. END AND SIDE CLEARANCES SHALL BE A MAXIMUM OF 6" AND A MINIMUM OF 2".
4. LONGITUDINAL MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 12" APART.
5. TRANSVERSE MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 24" APART.
6. MINIMUM SIZE OF LONGITUDINAL AND TRANSVERSE WIRE SHALL BE W5 OR D5 (NORMAL DIAMETER 0.252").
7. WIRE FABRIC TO BE A.S.T.M. A185 COLD DRAWN WELDED STEEL WIRE FABRIC, WITH MINIMAL SECTIONAL AND AREA IN BOTH DIRECTIONS = 0.09 s.i./Lf.
8. PAVEMENT REINFORCING SHALL BE INCIDENTAL TO 501 PCC PAVEMENT.



**DETAIL 1 NOT TO SCALE**



**DETAIL 2 NOT TO SCALE**



**DETAIL 3 NOT TO SCALE**

**JOINT SEALING DETAILS**

**JOINTING NOTES**

1. ALL JOINT EDGES SHALL BE SAWCUT TO PRODUCE THE 1/8" CHAMFER.
2. ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. DOWELS FOR THE 8" PAVEMENT SHALL BE 1" DIA., 19" LENGTH AND SPACED 12" CENTER TO CENTER.
4. ALL TIE BARS SHALL BE HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.
5. ALL TIE BARS SHALL BE PLACED AT A POINT NOT LESS THAN 6" OR MORE THAN 15" FROM A TRANSVERSE OR EXPANSION JOINT. TIE BARS FOR THE 8" PAVEMENT SHALL BE 5/8" DIA., 20" LENGTH AND SPACED 36" CENTER TO CENTER.
6. DOWELS IN TRANSVERSE CONTRACTION AND LONGITUDINAL CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.
7. ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE ±1/8" FOR CONSTRUCTION JOINTS AND ±1/4" FOR CONTRACTION JOINTS.
8. THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED DOWELS IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE DIVISION OF AERONAUTICS PRIOR TO USE.
9. THE COST OF ALL DOWEL BARS, TIE-BARS, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE P.C.C. PAVEMENT.
10. WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL PROTECT THE TRANSVERSE JOINTS.
11. JOINT SEALANT SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS ITEM 501-2.5.
12. CURING COMPOUND SHALL BE SPECIFIED IN THE SPECIAL PROVISIONS ITEM 501-3.17 AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE ENGINEER.
13. ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.

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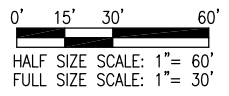
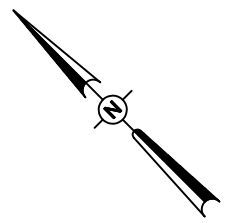
**SAINT LOUIS DOWNTOWN AIRPORT**  
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Hanson Project No.	12A0069
Filename	C-561-JNT.dwg
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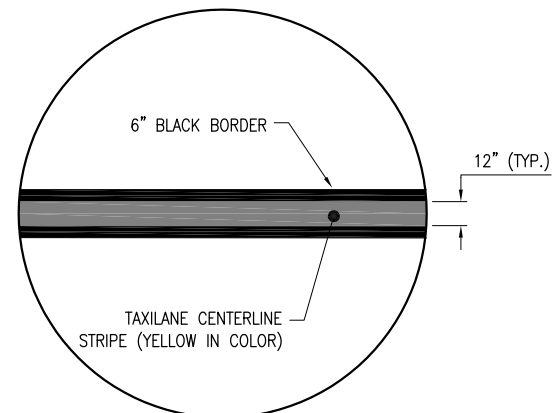
RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT  
PAVEMENT JOINTING DETAILS

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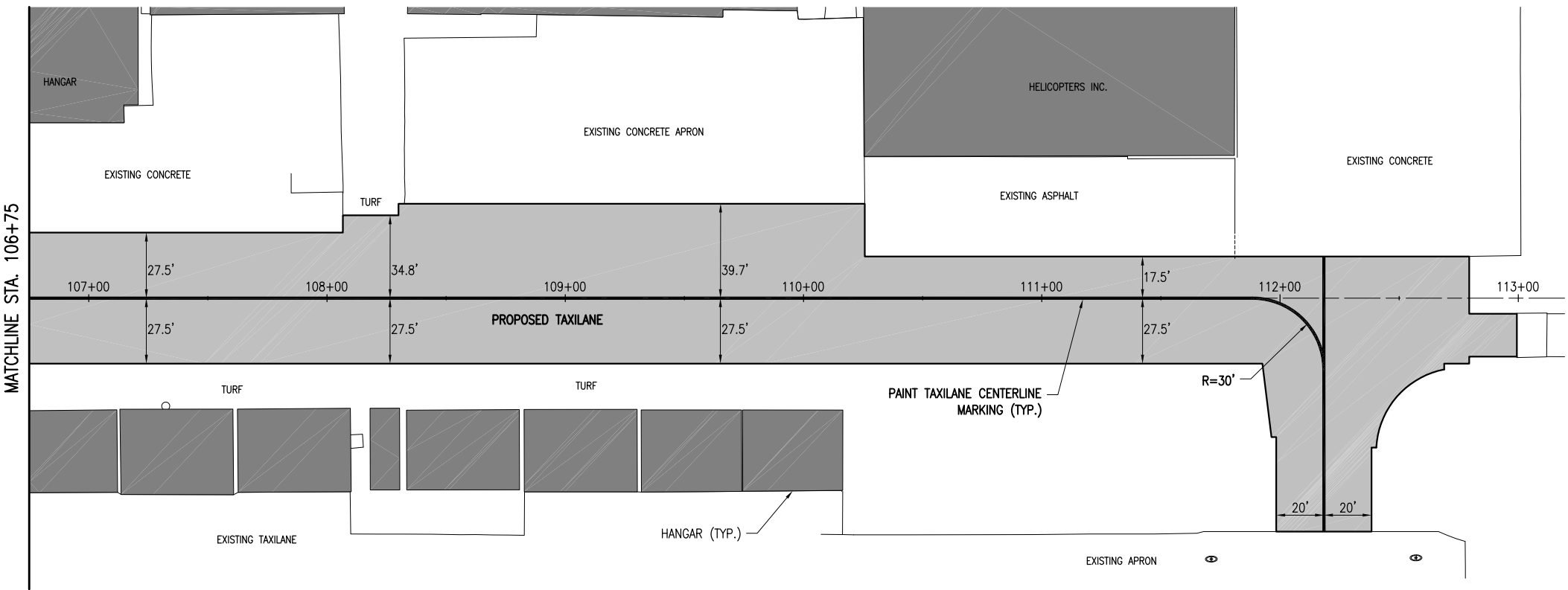
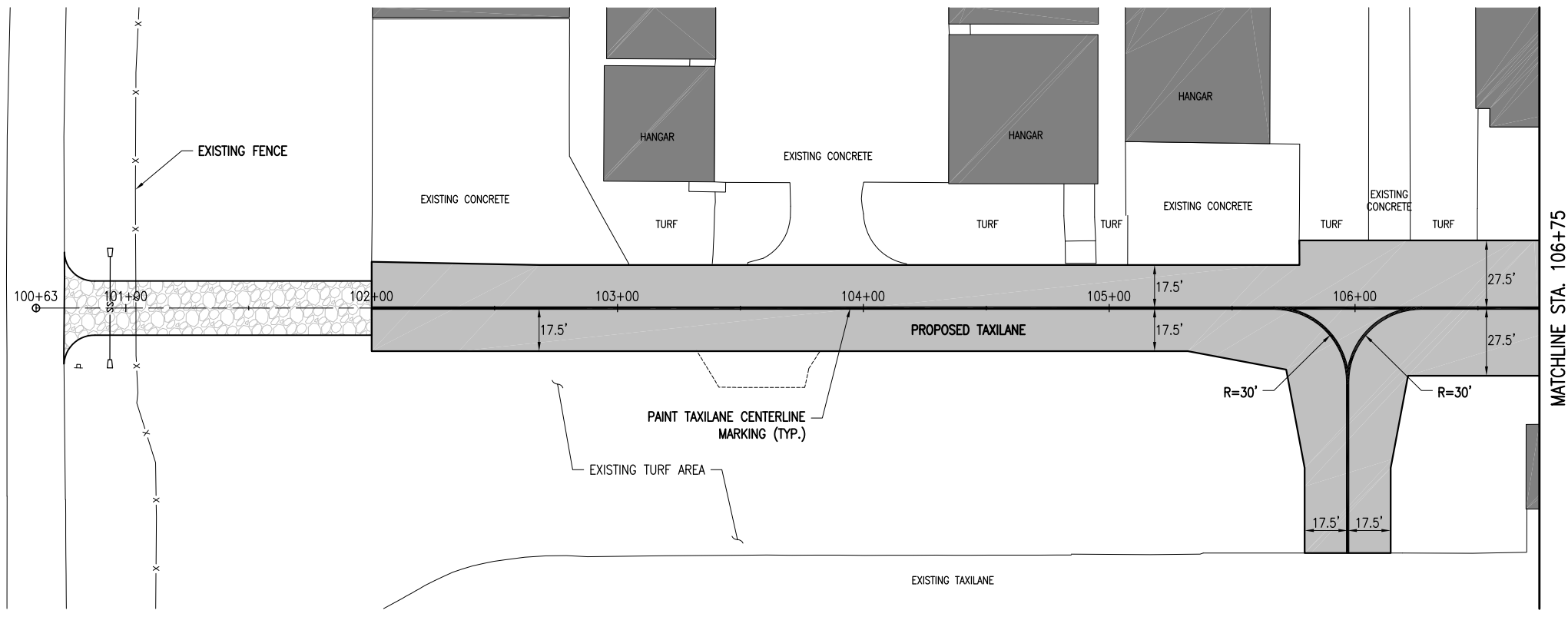
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- PROPOSED IMPROVEMENTS
- EXISTING FENCE
- EXISTING STORM SEWER
- EXISTING SANITARY
- EXISTING DITCH
- EXISTING STORM INLET/MAHOLE



**TAXILANE CENTERLINE DETAIL**  
NOT TO SCALE

**PAVEMENT MARKING NOTES**

1. TYPE B CLASS BEADS SHALL BE REQUIRED FOR ALL YELLOW PERMANENT PAINT MARKINGS. GLASS BEADS ARE NOT REQUIRED FOR TEMPORARY MARKINGS OR BLACK PAINT. REFER TO SPECIFICATION ITEM 620 FOR ADDITIONAL INFORMATION.
2. PAINT SHALL MEET REQUIREMENTS OF TECHNICAL SPECIFICATION ITEM 620, WATERBORNE PAINT.
3. ALL EXISTING MARKINGS NOTED FOR REMOVAL SHALL BE COMPLETED USING HIGH PRESSURE WATER BLASTING OR METHOD APPROVED BY THE ENGINEER. PAINT REMOVAL BY GRINDING OR OTHER METHODS WHICH DAMAGE THE PAVEMENT SURFACES SHALL NOT BE USED. THE MARKINGS SHALL NOT BE PAINTED OVER WITH BLACK PAINT.
4. IMMEDIATELY PRIOR TO THE APPLICATION OF PAINT, ALL SURFACES SHALL BE DRY AND FREE FROM DIRT, GREASE, OIL, LANTANCE, OR OTHER FOREIGN MATERIAL WHICH WOULD REDUCE THE BOND BETWEEN THE PAINT AND THE PAVEMENT. THIS SHALL INCLUDE PAINTED AREAS ON THE EXISTING PAVEMENTS. REFER TO SPECIFICATION ITEM 620-3.3 FOR ADDITIONAL INFORMATION.
5. EXISTING PAVEMENT MARKINGS OUTSIDE THE LIMITS OF THE MARKINGS SHOWN ON THESE PLANS WHICH ARE REMOVED OR WORN DUE TO CONSTRUCTION ACTIVITY SHALL BE REPAINTED. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR THIS WORK.



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Hanson Project No.	12A0069
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RECONSTRUCT TAXILANE  
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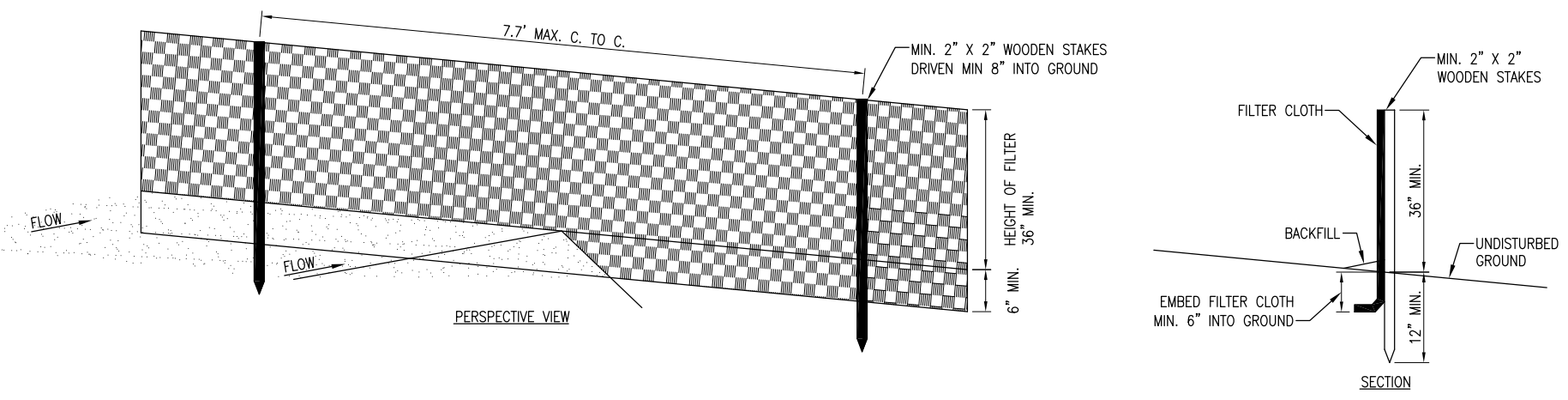
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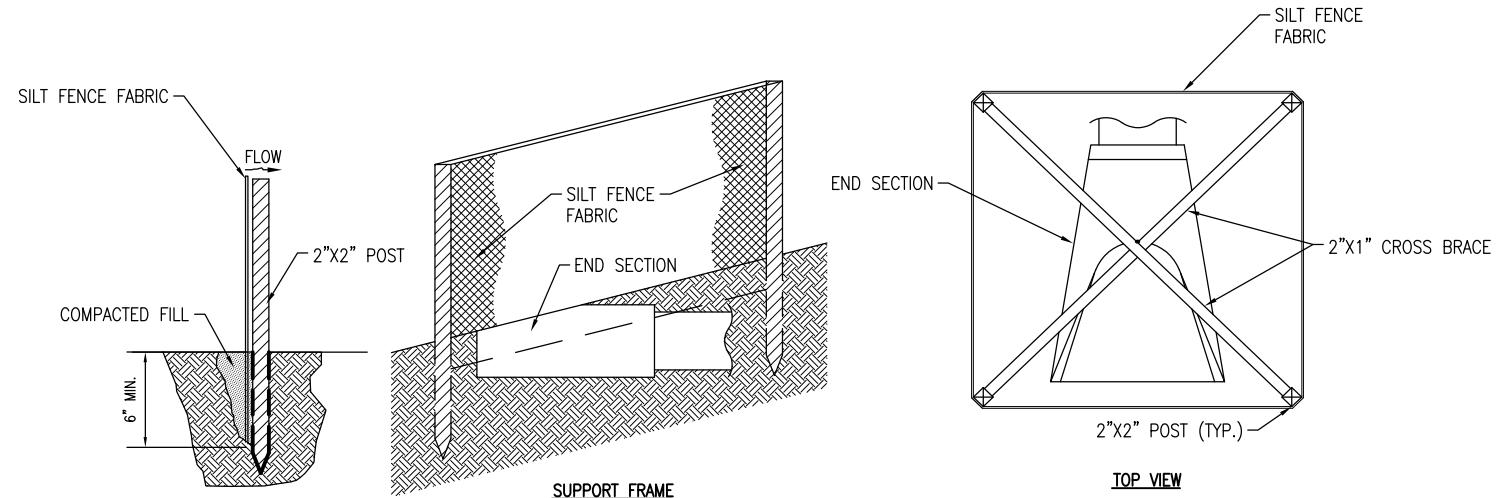


**STORM WATER POLLUTION PREVENTION NOTES**

1. THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.
2. THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHERE EVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.
3. POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.
4. THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.



**SILT FENCE DETAIL**  
NO SCALE



**FABRIC END SECTION PROTECTION**  
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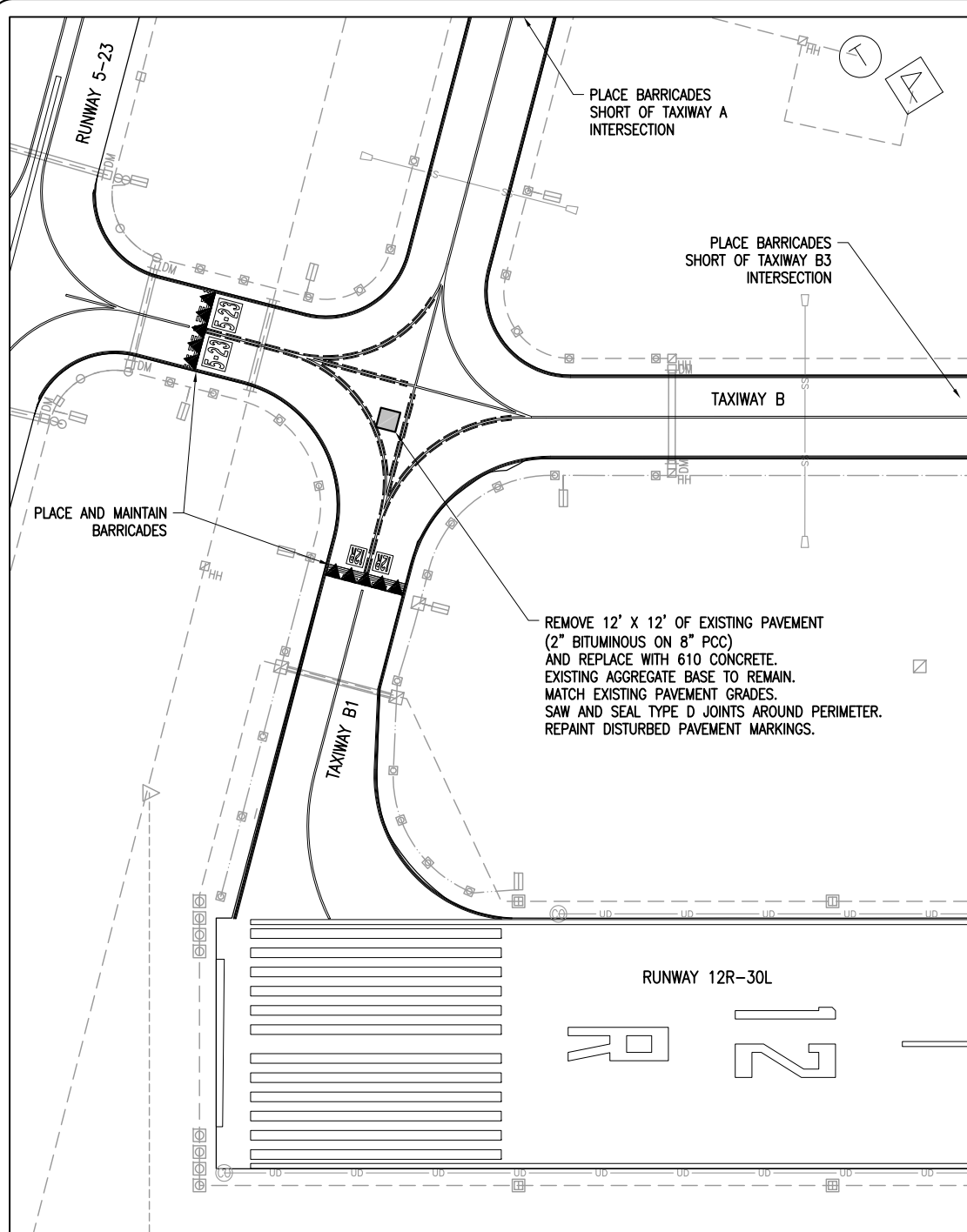
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RECONSTRUCT TAXILANE  
IN NORTHWEST QUADRANT

STORMWATER POLLUTION  
PREVENTION DETAILS

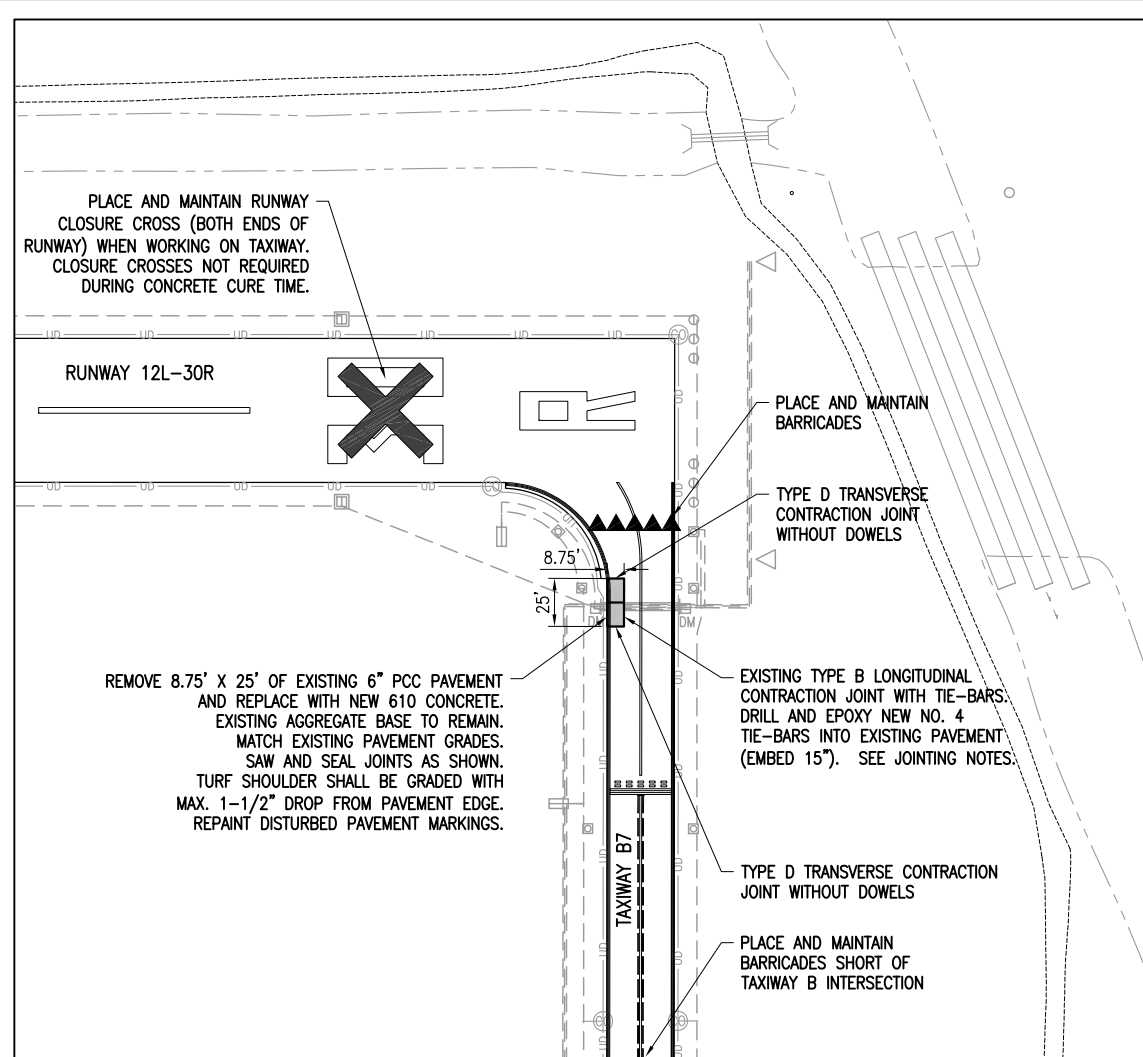
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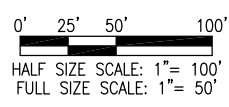
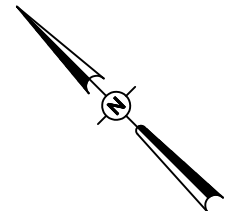
**TAXIWAY B1 PAVEMENT REPAIR PLAN**

**ADDITIVE ALTERNATE PLAN NOTES**

1. THE CONTRACTOR SHALL HAVE A MAXIMUM OF 2 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK ON TAXIWAY B1, EXCLUDING CONCRETE CURE TIME. THE CONTRACTOR SHALL HAVE A MAXIMUM OF 2 CONSECUTIVE CALENDAR DAYS TO COMPLETE THE WORK ON TAXIWAY B7, EXCLUDING CONCRETE CURE TIME.
2. THE CONTRACTOR MAY PERFORM WORK ON BOTH TAXIWAYS WITHIN THE SAME TIME FRAME IF COORDINATED CLOSELY WITH THE OWNER AND RESIDENT ENGINEER IN ADVANCE, HOWEVER CONSTRUCTION TRAFFIC WILL NOT BE PERMITTED TO TRAVEL CONTINUOUSLY BETWEEN THE TWO SITES.
3. CLOSURE CROSSES AND BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION. CLOSURE CROSSES SHALL BE REMOVED UPON COMPLETION OF ACTIVE CONSTRUCTION AT THE SITE, BUT BARRICADES SHALL REMAIN IN PLACE UNTIL THE NEW CONCRETE HAS PROPERLY CURED AND THE PAVEMENTS HAVE BEEN CLEANED TO THE SATISFACTION OF THE OWNER.
4. THE CONTRACTOR SHALL COORDINATE THE SCHEDULE FOR THIS WORK WITH THE OWNER AND RESIDENT ENGINEER IN ADVANCE IN ORDER TO PROPERLY SCHEDULE THE RUNWAY/TAXIWAY CLOSURES AND ALLOW FOR THE WORK TO AVOID CLOSURES DURING CRITICAL AIRCRAFT OPERATIONAL PERIODS. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE CLOSURES.
5. CONTRACTOR ACCESS TO EACH SITE WILL BE COORDINATED PRIOR TO BEGINNING THE WORK. THE CONTRACTOR WILL BE ESCORTED FROM A PERIMETER ACCESS GATE TO EACH SITE.

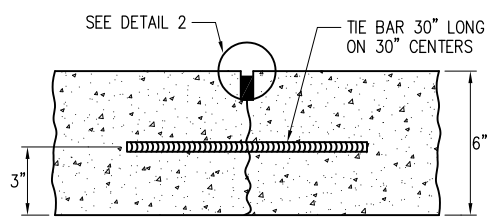


**TAXIWAY B7 PAVEMENT REPAIR PLAN**

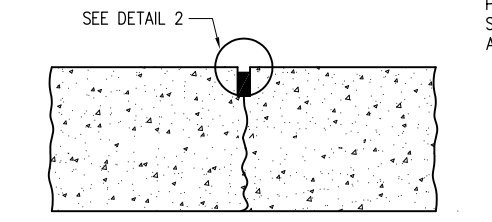


**LEGEND**

- EXISTING PAVEMENT
- PROPOSED PAVEMENT REPLACEMENT
- EXISTING UNDERDRAIN
- EXISTING STORM SEWER
- EXISTING ELECTRIC
- EXISTING ELECTRICAL DUCT
- EXISTING TAXI GUIDANCE SIGN
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED THRESHOLD LIGHT
- EXISTING SPLICE CAN
- EXISTING ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL MANHOLE

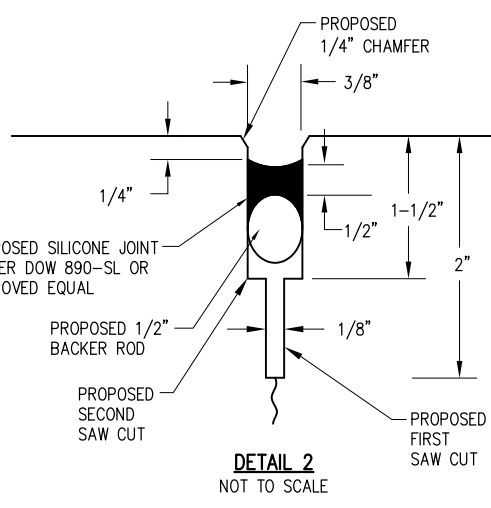


**TYPE B HINGED**



**TYPE D DUMMY**

**CONTRACTION JOINTS**



**DETAIL 2**  
NOT TO SCALE

**JOINT SEALING DETAIL**

**JOINTING NOTES**

1. ALL JOINT EDGES SHALL BE SAWCUT TO PRODUCE THE REQUIRED CHAMFER. ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED.
2. ALL TIE BARS SHALL BE PLACED AT A POINT NOT LESS THAN 6" OR MORE THAN 15" FROM A TRANSVERSE OR EXPANSION JOINT. TIE BARS FOR THE 6" PAVEMENT SHALL BE 1/2" DIA., 30" IN LENGTH AND SPACED 30" CENTER TO CENTER.
3. ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE ±1/8" FOR CONSTRUCTION JOINTS AND ±1/4" FOR CONTRACTION JOINTS.
4. THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED TIE-BARS IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE DIVISION OF AERONAUTICS PRIOR TO USE.
5. THE COST OF ALL DOWEL BARS, TIE-BARS, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE P.C.C. PAVEMENT.
6. REFER TO SPECIFICATION ITEM 610 FOR ADDITIONAL INFORMATION.

REVISION	DATE

**SAINT LOUIS DOWNTOWN AIRPORT**  
A Division of Bi-State Development Agency  
BLOCK GRANT PROJ.: 3-17-0039-B30  
IL PROJ.: CPS-4213

Hanson Project No.	12A0069	DATE	03/08/13
Filename	C-122-CON2.dwg	LAYOUT	DAW 01/28/13
Scale	1"=50'	DRAWN	MLH 01/29/13
Date	03/08/13	REVIEWED	BSS 03/08/13

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1425 North 11th Street  
Springfield, Illinois 62703-2886

RECONSTRUCT TAXILANE IN NORTHWEST QUADRANT  
CONSTRUCTION PLAN - ADDITIVE ALTERNATE

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