

032

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	1
ILLINOIS			CONTRACT NO. 74703	

STATION EQUATIONS

- STA. 50 + 54.57 BK = STA. 52 + 17.05 AH
- STA. 98 + 83.70 BK = STA. 922 + 10.70 AH
- STA. 967 + 58.21 BK = STA. 967 + 66.00 AH
- STA. 991 + 07.01 BK = STA. 991 + 09.70 AH
- STA. 1003 + 52.02 BK = STA. 1003 + 52.10 AH
- STA. 1023 + 07.00 BK = STA. 1023 + 15.00 AH
- STA. 1052 + 95.27 BK = STA. 1053 + 00.60 AH
- STA. 1064 + 96.00 BK = STA. 1064 + 98.00 AH
- STA. 1091 + 12.50 BK = STA. 1091 + 10.00 AH
- STA. 1141 + 74.97 BK = STA. 1143 + 10.78 AH
- STA. 1174 + 69.45 BK = STA. 1175 + 11.60 AH
- STA. 1224 + 77.97 BK = STA. 1224 + 80.00 AH
- STA. 1231 + 91.50 BK = STA. 1232 + 00.00 AH

PROPOSED  
HIGHWAY PLANS

FAP ROUTE 116 (IL RTE 130)  
SECTION (6,7)RS-3  
PROJECT STP-HSIP-B7EF(776)  
3P RESURFACING  
EDWARDS COUNTY

C-97-012-15

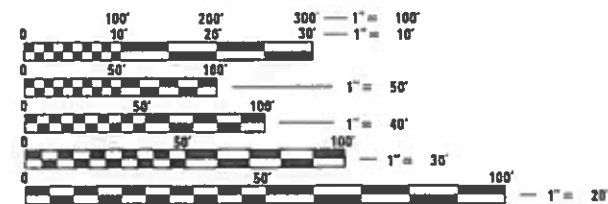


ADT (2017) = 4,200

SECTION (6,7)RS-3  
ENDS STA 1232 + 44.6

BRIDGE OMISSION  
STA 1198 + 15 TO  
STA 1199 + 20

BRIDGE OMISSION  
STA 1089 + 50 TO  
STA 1092 + 95

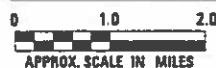
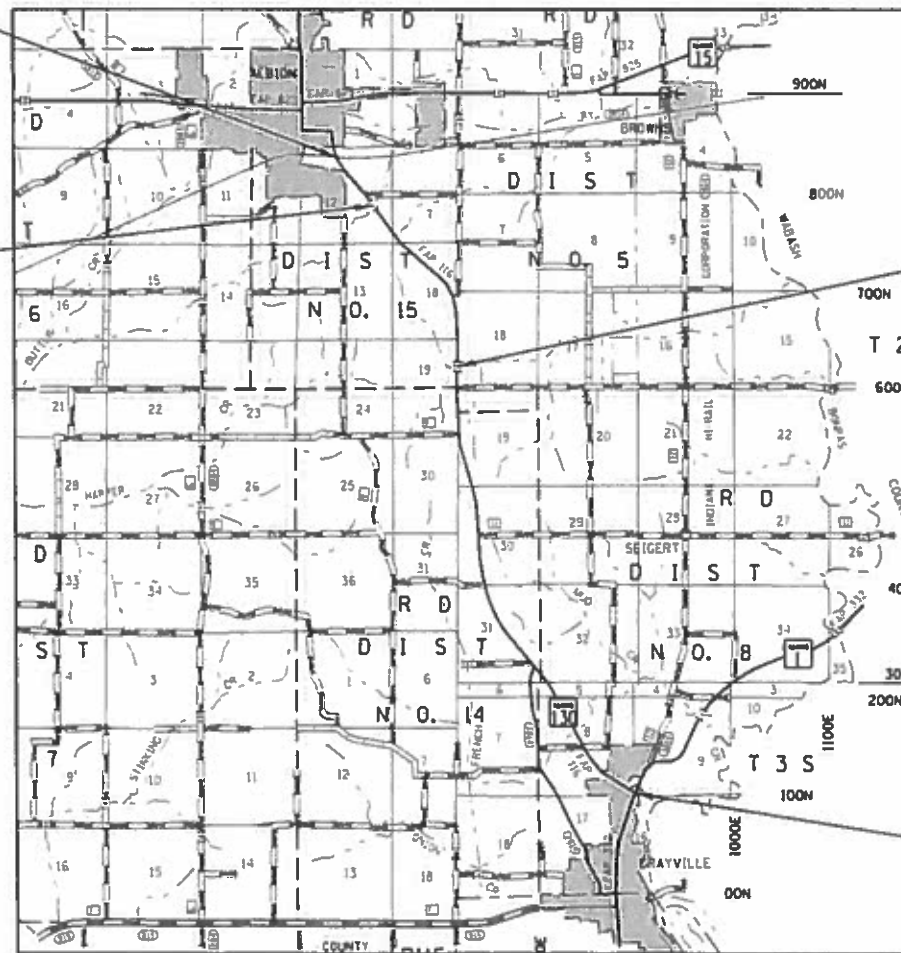


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: DEBRA BARRETT  
PROJECT MANAGER: BRIAN BIERMAN

CONTRACT NO. 74703



GROSS LENGTH = 40,540.7 FT = 7.69 MILE  
NET LENGTH = 40,090.7 FT = 7.59 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED April 1 20 19  
July M South 10  
Regional Engineer

May 10 20 19  
Engineer of Design and Environment

May 10 20 19  
Director of Highways Project Implementation

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OF THE STATE OF ILLINOIS

GENERAL NOTES

THE WORK INCLUDED IN THIS SECTION CONSISTS OF PAVEMENT PATCHING, HMA SURFACE REMOVAL AND RESURFACING, AGGREGATE SHOULDERS, HMA SAFETY SHOULDERS, BRIDGE AND CULVERT REPAIRS AND ANY OTHER WORK NECESSARY TO COMPLETE THIS SECTION.

ALL AREAS DISTURBED DURING CONSTRUCTION SHALL BE SEEDED WITH CLASS 2A SEEDING IN ACCORDANCE WITH SECTION 250 OF THE STANDARD SPECIFICATIONS AFTER APPLICATION OF FERTILIZER NUTRIENTS AT THE RATE OF 90 LB/ACRE FOR NITROGEN, PHOSPHORUS AND POTASSIUM, AS WELL AS AGRICULTURAL LIMESTONE APPLICATION AT THE RATE OF 2 TONS/ACRE. DISTURBED AREAS SHALL THEN BE COVERED WITH MULCH, METHOD 1, IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

TREE REMOVAL, EXCAVATION AND EMBANKMENT NECESSARY FOR CHANNEL GRADING AND PLACEMENT OF FILTER FABRIC AND RIPRAP WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE PER TON FOR RIPRAP, CLASS A5.

ALLOWABLE MATERIALS FOR AGGREGATE SURFACE COURSE, TYPE B SHALL BE LIMITED TO CRUSHED STONE OR CRUSHED CONCRETE.

HMA SHOULDERS 6 1/2" SHALL BE CONSTRUCTED PRIOR TO PAVEMENT PATCHING DUE TO THE POOR CONDITION OF THE EXISTING SHOULDER.

MILLED PAVEMENT SHALL BE RESURFACED WITHIN 5 CALENDAR DAYS.

THE RESIDENT ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE CURING TIME FOR HOT-MIX ASPHALT LIFTS. THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

APPLICATION	PG GRADE	DESIGN AIR VOIDS	MIXTURE COMPOSITION	FRICTION AGGREGATE	QUALITY MANAGEMENT
HMA SURFACE COURSE	PG 64-22	4.0%Ndesign=70	IL-9.5	MIXTURE C	PPF
HMA LEVELING BINDER	PG 64-22	4.0%Ndesign=70	IL-9.5FG	N/A	QCP
HMA SHOULDERS 1.5" (TOP LIFT)	PG 64-22	4.0%Ndesign=70	IL-9.5	MIXTURE C	QC/QA
HMA SHOULDERS 6.5" (BOTTOM LIFTS)	PG 64-22	4.0%Ndesign=70	IL-19.0	N/A	QC/QA
HMA BASE COURSE WIDENING (TOP LIFT)	PG 64-22	4.0%Ndesign=70	IL-9.5	MIXTURE C	QC/QA
HMA BASE COURSE WIDENING (BOTTOM LIFTS)	PG 64-22	4.0%Ndesign=70	IL-19.0	N/A	QC/QA
INCIDENTAL HMA	PG 64-22	4.0%Ndesign=70	IL-9.5	MIXTURE C	QC/QA
SHOULDER REMOVE AND REPLACE	PG 64-22	4.0%Ndesign=70	IL-9.5	MIXTURE C	QC/QA
SHOULDER REMOVE AND REPLACE, SPECIAL	PG 64-22	4.0%Ndesign=70	IL-9.5	MIXTURE C	QC/QA
PAVEMENT PATCHING, CLASS "D"	PG 64-22	4.0%Ndesign=70	IL-19.0	N/A	QC/QA

PAVEMENT PATCHING QUANTITIES PROVIDED ARE ESTIMATES. ACTUAL PAVEMENT PATCHING LOCATIONS AND QUANTITIES WILL BE AS PROVIDED BY THE ENGINEER IN THE FIELD.

ALLOWABLE MATERIALS FOR AGGREGATE SHOULDER WEDGE, TYPE B SHALL BE LIMITED TO CRUSHED STONE, CRUSHED CONCRETE OR RECYCLED ASPHALT PAVEMENT (RAP).

SAWCUTTING, REMOVAL AND/OR REPLACEMENT OF PAVEMENT NECESSARY FOR TYPE A GUTTER REMOVAL WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR GUTTER REMOVAL.

EARTH EXCAVATION AND EMBANKMENT NECESSARY FOR CONSTRUCTION OF CONCRETE GUTTER, TYPE A AND ADJACENT GRADING WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR CONCRETE GUTTER, TYPE A.

EARTH EXCAVATION NECESSARY FOR EARTH SHOULDER REGRADING WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR PIPE UNDERDRAINS, TYPE 2, 6".

BALLAST PROTECTION, IF REQUESTED BY NORFOLK SOUTHERN RAILROAD, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE PER SQUARE YARD FOR PROTECTIVE SHIELD.

PAVEMENT MARKING TAPE SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON HMA SURFACE COURSE. PAINT PAVEMENT MARKINGS SHALL BE PERMISSIBLE FOR SHORT TERM PAVEMENT MARKINGS ON OTHER HMA LIFTS AND MILLED SURFACES.

WHERE DROP-OFFS WILL BE PRESENT NEAR THE EDGE OF PAVEMENT DUE TO CONSTRUCTION OPERATIONS, CLOSURE OF THE ADJACENT LANE WILL BE REQUIRED FOR THE DURATION THAT THE DROP-OFF EXISTS WHEN THE DROP-OFF DEPTH IS IN EXCESS OF 12 INCHES. WHERE DROP-OFFS WILL BE PRESENT THAT ARE LESS THAN 12 INCHES IN DEPTH, A TEMPORARY AGGREGATE WEDGE SHALL BE CONSTRUCTED AT A MINIMUM SLOPE OF 1(V):3(H) WHEN CONSTRUCTION OPERATIONS ARE NOT ACTIVELY BEING PERFORMED. WHEN LANE CLOSURES AND/OR TEMPORARY AGGREGATE WEDGES MUST REMAIN DURING HOURS WHEN THE CONTRACTOR IS NOT ENGAGED IN CONSTRUCTION OPERATIONS, TRAFFIC CONTROL SURVEILLANCE SHALL BE PERFORMED. MITIGATION OF DROP-OFFS, AS HEREIN DESCRIBED (WITH THE EXCEPTION OF TRAFFIC CONTROL SURVEILLANCE) WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

THE FOLLOWING APPLICATION RATES WERE USED IN CALCULATING PLAN QUANTITIES AND HAVE BEEN INCLUDED FOR REFERENCE:

RIPRAP, CLASS A5	0.9167 TON/SQ YD
RIPRAP BEDDING, CLASS A1	0.333 TON/SQ YD
AGGREGATE SURFACE COURSE AND WEDGE SHOULDER	2.05 TON/CU YD
BITUMINOUS MATERIALS (TACK COAT)	0.05 LB/SQ FT (ON MILLED SURFACES)
	0.025 LB/SQ FT (ON HMA LIFTS)
HOT-MIX ASPHALT BINDERS AND SURFACES	112 LB/SQ YD/INCH

INDEX OF SHEETS

SHEET NO	TITLE
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23-31	STRUCTURE REPAIR DETAILS - SN 024-0005
32	CULVERT REPAIR DETAILS - SN 024-7011
33	CULVERT REPAIR DETAILS - SN 024-7013
34	PAVING DETAILS
35-36	RURAL ENTRANCE AND MAILBOX TURNOUT DETAILS
37	TRAFFIC COUNTER DETAILS
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APPLICABLE IDOT HIGHWAY STANDARDS

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED FOLLOWING THE LAST NUMBERED SHEET OF THE PLANS.

000001-07	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
406201-01	MAILBOX TURNOUT
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
601001-05	PIPE UNDERDRAINS
601101-02	CONCRETE HEADWALL FOR PIPE UNDERDRAIN
606101-05	TYPE A GUTTER (INLET, OUTLET & ENTRANCE)
630001-12	STEEL PLATE BEAM GUARDRAIL
630201-07	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
630301-09	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631031-15	TRAFFIC BARRIER TERMINAL, TYPE 6
642006	SHOULDER RUMBLE STRIPS, 8 In.
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-05	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701316-12	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR, FOR SPEEDS >= 45 MPH
701321-17	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701336-07	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES FOR SPEEDS >= 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

REV. - MS

FILE NAME =	USER NAME = steffenk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS &amp; GENERAL NOTES</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 3/29/2019	DATE -	REVISED -		SCALE: N/A	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			

80% FED 90% FED  
20% STATE 10% STATE

80% FED 90% FED  
20% STATE 10% STATE

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005	0021	
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	765		765	
28100209	STONE RIPRAP, CLASS A5	TON	227	227		
28200200	FILTER FABRIC	SO YD	247	247		
35501324	HOT-MIX ASPHALT BASE COURSE, 10"	SO YD	373	373		
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	258	258		
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	65520	65520		
40600637	LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70	TON	6795	6795		
40600990	TEMPORARY RAMP	SO YD	233	233		
40603315	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	TON	8154	8154		
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	146	146		
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	94668	94668		
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SO YD	2007	2007		
44000400	GUTTER REMOVAL	FOOT	533	533		
44004250	PAVED SHOULDER REMOVAL	SO YD	12754		12754	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005	0021	
44201811	CLASS D PATCHES, TYPE I, 14 INCH	SO YD	4	4		
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SO YD	1279	1279		
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SO YD	472	472		
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SO YD	842	842		
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	2681		2681	
48203003	HOT-MIX ASPHALT SHOULDERS, 1 1/2"	SO YD	32845		32845	
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SO YD	92		92	
48203023	HOT-MIX ASPHALT SHOULDERS, 6 1/2"	SO YD	32845		32845	
50102400	CONCRETE REMOVAL	CU YD	8.1	8.1		
50157300	PROTECTIVE SHIELD	SO YD	68	68		
50300255	CONCRETE SUPERSTRUCTURE	CU YD	6.1	6.1		
50300300	PROTECTIVE COAT	SO YD	23	23		
50800105	REINFORCEMENT BARS	POUND	272	272		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1160	1160		

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PLOT DATE = 4/1/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF  
QUANTITIES**

SCALE: SHEET 1 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	3
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				

80% FED 90% FED  
20% STATE 10% STATE

80% FED 90% FED  
20% STATE 10% STATE

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005	0021	
50800515	BAR SPLICERS	EACH	17	17		
52000110	PREFORMED JOINT STRIP SEAL	FOOT	43	43		
54003000	CONCRETE BOX CULVERTS	CU YD	2	2		
54248510	CONCRETE COLLAR	CU YD	2.4	2.4		
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	2	2		
60100070	SHOULDER REMOVAL AND REPLACEMENT	FOOT	600	600		
60108200	PIPE UNDERDRAINS 6" (SPECIAL)	FOOT	50	50		
60108206	PIPE UNDERDRAINS, TYPE 2, 6"	FOOT	600	600		
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	5.9	5.9		
60602500	CONCRETE GUTTER, TYPE A	FOOT	533	533		
61100605	MISCELLANEOUS CONCRETE	CU YD	3.2	3.2		
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	88	88		
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	4		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005	0021	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4		
63200310	GUARDRAIL REMOVAL	FOOT	438	438		
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	76997		76997	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	5	5		
67100100	MOBILIZATION	L SUM	1	1		
70100100	TRAFFIC CONTROL AND PROTECTION, STANDARD 701316	EACH	1	1		
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	LSUM	1	1		
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	LSUM	1	1		
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	LSUM	1	1		
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1		
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	4	4		
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1		
70107005	PAVEMENT MARKING BLACKOUT TAPE, 5"	FOOT	2063	2063		

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\* SPECIALTY ITEM

REV. - MS

USER NAME = steffemk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	F.A.P. RTE. 116	SECTION (6,7)RS-3	COUNTY EDWARDS	TOTAL SHEETS 41	SHEET NO. 4		
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			SCALE:	SHEET 2 OF 4 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
PLOT DATE = 3/29/2019	DATE -	REVISED -									

80% FED 90% FED  
20% STATE 10% STATE

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SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE			SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005	0021		CODE NO	ITEM	UNIT		0005	0021	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	28	28		* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	90	90			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	12121	12121		* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	20	20			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	1337	1337		* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	110842	110842			
70400100	TEMPORARY CONCRETE BARRIER	FOOT	363	363		* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	513	513			
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	305	305		78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	16	16			
70500615	TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1	EACH	1	1		X0323388	TRAFFIC COUNTER	EACH	1	1			
70500665	TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	1	1		X0326440	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)	SO YD	1834	1834			
70600240	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 2	EACH	2	2		X4060995	TEMPORARY RAMP, SPECIAL	SO YD	864	864			
70600340	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 2	EACH	2	2		X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SO YD	391	391			
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4		X4820110	HOT-MIX ASPHALT SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)	SO YD	100	100			
72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	520	520		X7010202	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)	EACH	1	1			
73400100	CONCRETE FOUNDATIONS	CU YD	1.4	1.4		* X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	16	16			
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	16	16		* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	90	90			

\* SPECIALTY ITEM

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PLOT DATE = 4/1/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF  
QUANTITIES

SCALE: SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	5
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				

80% FED     90% FED  
20% STATE     10% STATE

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE			SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005	0021		CODE NO	ITEM	UNIT	TOTAL QUANTITIES	0005	0021	
*X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	20	20									
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	5	5									
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	84	84									
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	39709	39709									
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1									
Z0049799	PROTECTING OR RESETTING SURVEY MARKERS	EACH	6	6									

\* SPECIALTY ITEM

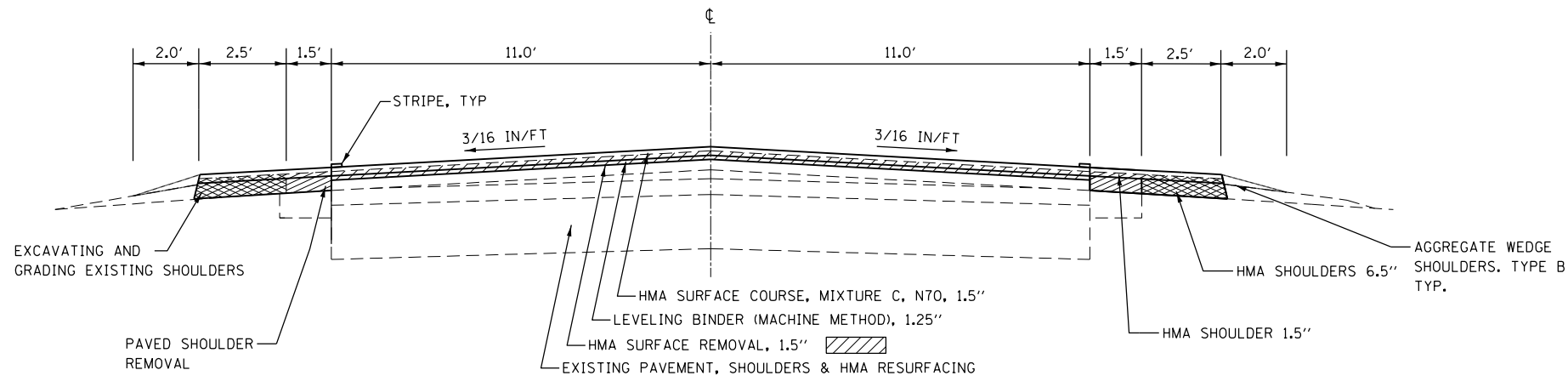
REV. - MS

USER NAME = steffennk DESIGNED - DRAWN - PLOT SCALE = 100.0000 ' / in. CHECKED - PLOT DATE = 3/29/2019 DATE -	REVISED - REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF</b> <b>QUANTITIES</b>	F.A.P. RTE. 116	SECTION (6.7)RS-3	COUNTY EDWARDS	TOTAL SHEETS 41	SHEET NO. 6
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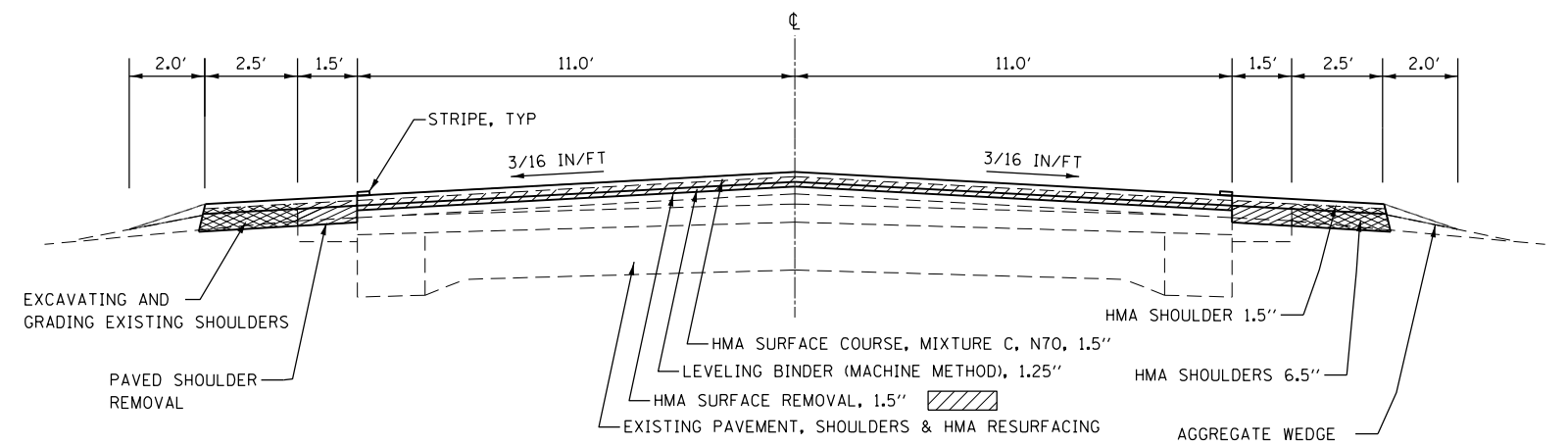
SCALE:     SHEET 4     OF 4     SHEETS     STA.     TO STA.

ILLINOIS     FED. AID PROJECT

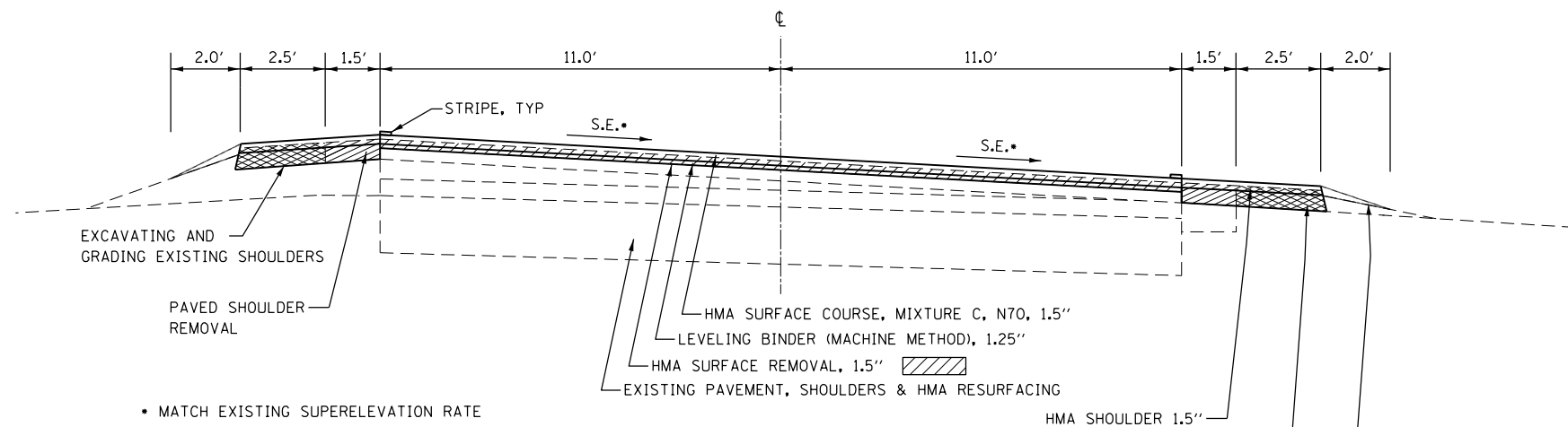
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**TYPICAL SECTION**  
 STA 0+00 TO STA 923+77  
 STA 981+05 TO STA 982+50  
 STA 991+10 TO STA 995+00  
 STA 1026+00 TO STA 1036+75  
 STA 1040+23 TO STA 1042+00  
 STA 1155+00 TO STA 1164+20  
 STA 1207+86 TO STA 1214+39



**TYPICAL SECTION**  
 STA 923+77 TO STA 937+26  
 STA 940.74 TO STA 958+50  
 STA 967+66 TO STA 981+05  
 STA 995+00 TO STA 999+45  
 STA 1003+52 TO STA 1012+00  
 STA 1022+78 TO STA 1026+00  
 STA 1042+00 TO STA 1047+20  
 STA 1053+01 TO STA 1119+81  
 STA 1143+11 TO STA 1155+00  
 STA 1175+12 TO STA 1198+08  
 STA 1224+80 TO STA 1230+29



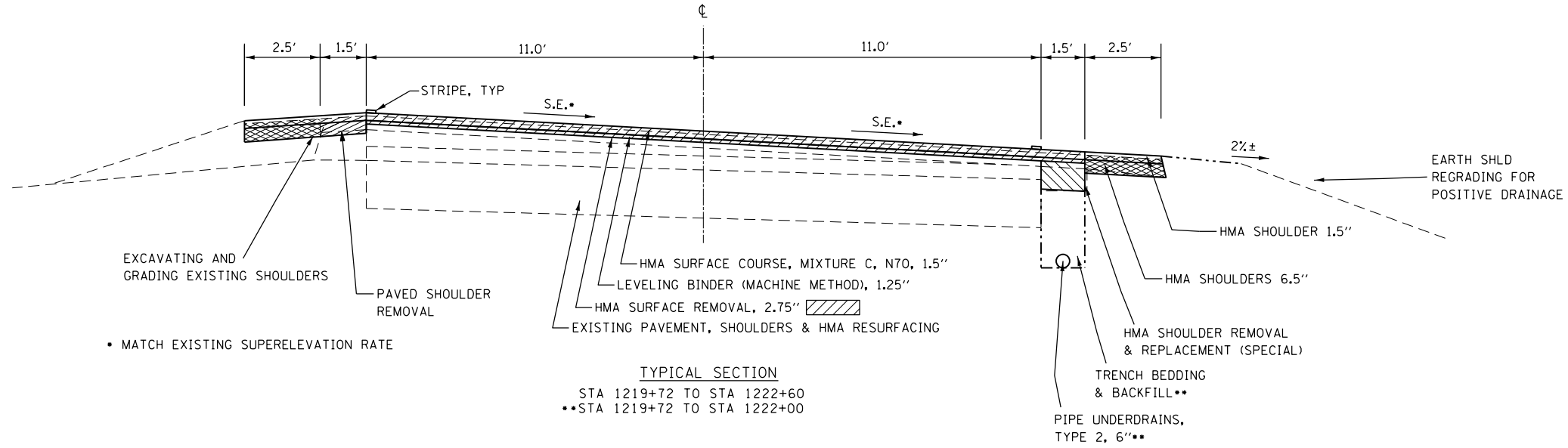
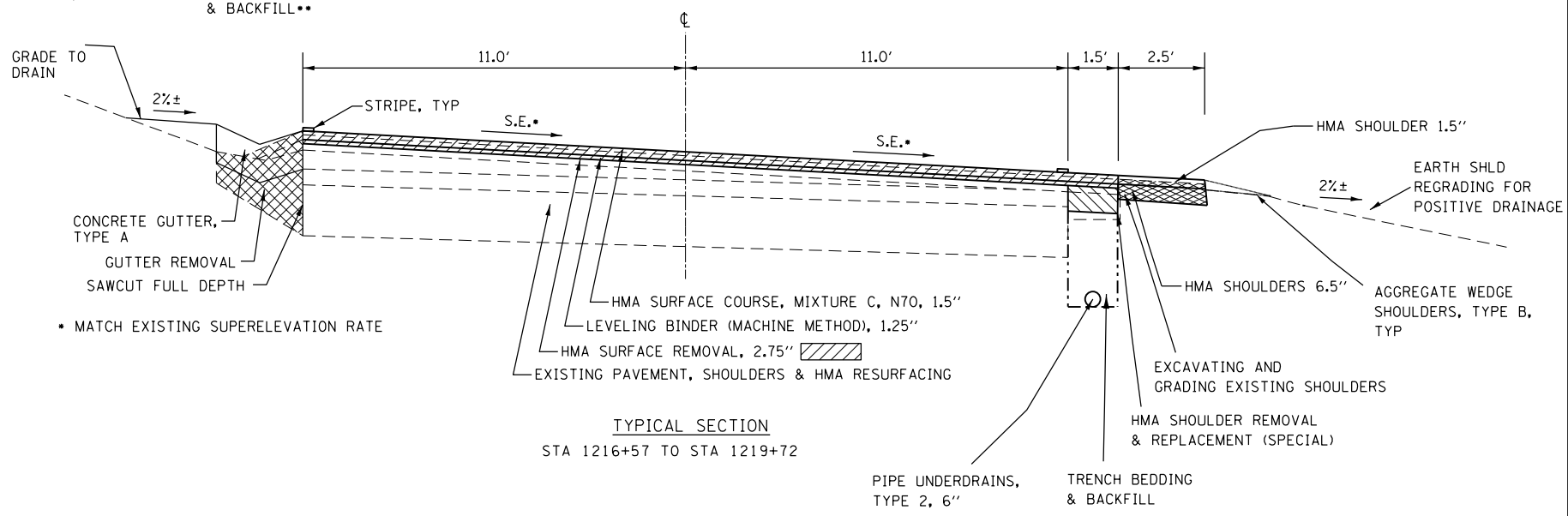
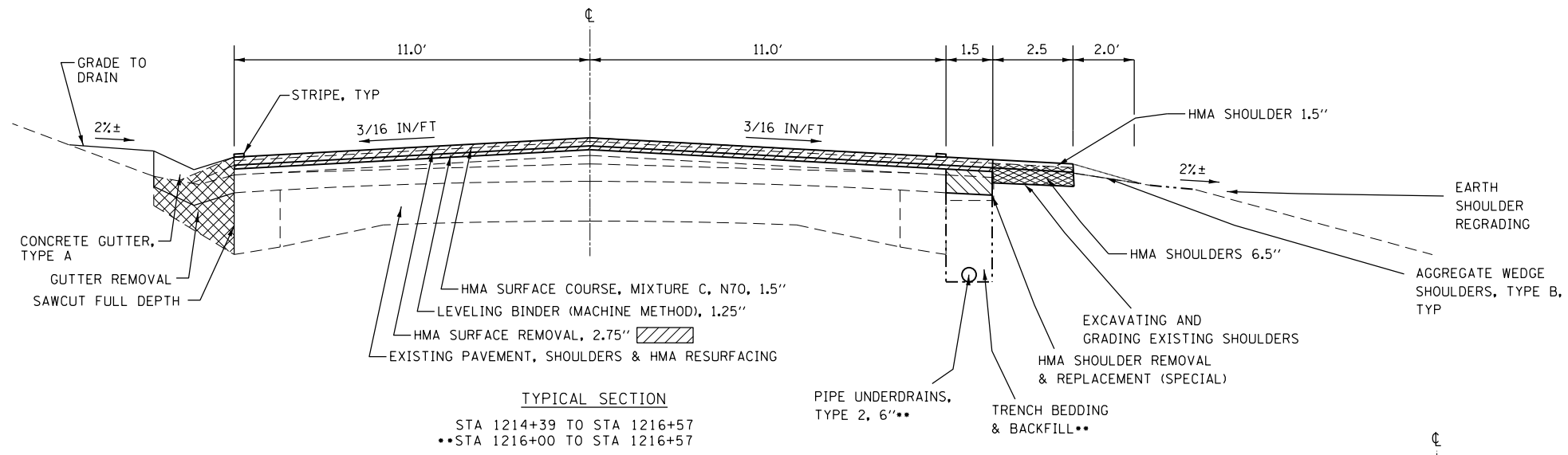
**TYPICAL SECTION**  
 STA 937+26 TO STA 940+74  
 STA 958+50 TO STA 967+66  
 STA 982+50 TO STA 991+10  
 STA 999+45 TO STA 1003+52  
 STA 1012+00 TO STA 1022+78  
 STA 1036+75 TO STA 1040+23  
 STA 1047+20 TO STA 1053+01  
 STA 1119+81 TO STA 1143+11  
 STA 1164+20 TO STA 1175+12  
 STA 1198+08 TO STA 1207+86  
 STA 1222+60 TO STA 1224+80  
 STA 1230+29 TO STA 1232+45

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Default	PLOT DATE = 3/29/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>TYPICAL SECTIONS</b>			
SCALE: N/A	SHEET 1	OF 2 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	7
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISIONS -
	PLOT DATE = 3/29/2019	DATE -	REVISIONS -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>TYPICAL SECTIONS</b>			
SCALE: N/A	SHEET 2	OF 2 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	8
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				



**48102100 AGGREGATE WEDGE SHOULDER, TYPE B**

STATIONING		SIDE	LENGTH (FOOT)	QUANTITY (TONS)
FROM	TO			
0+00	50+55	LT/RT	10,110	352.3
52+17	98+84	LT/RT	9,334	325.3
922+11	967+58	LT/RT	9,094	316.9
967+66	991+07	LT/RT	4,682	163.2
991+10	1003+52	LT/RT	2,484	86.6
1003+52	1023+07	LT/RT	3,910	136.3
1023+15	1052+95	LT/RT	5,960	207.7
1053+01	1064+96	LT/RT	2,390	83.3
1064+98	1089+50	LT/RT	4,904	170.9
1089+50	1092+95		SHOULDER OMISSION	
1092+95	1141+75	LT/RT	9,760	340.1
1143+11	1174+69	LT/RT	6,316	220.1
1175+12	1197+50	LT/RT	4,476	156.0
1197+50	1200+20		SHOULDER OMISSION	
1200+20	1214+20	LT/RT	2,800	97.6
1214+20	1219+72	RT	552	9.6
1219+72	1223+40	RT	368	6.4
1223+40	1230+84	LT/RT	1,488	51.9
DEDUCTION (SEE ENTRANCE SCHEDULE)			-2,508.0	-43.7
TOTAL				<b>2,681</b>

**UNDERDRAIN OUTLET SCHEDULE**

STATION	SIDE (LT/RT)	60100060 CONCRETE HEADWALLS FOR PIPE DRAINS (EACH)	60108200 PIPE UNDERDRAINS 6" (SPECIAL) (FOOT)
1216+00	RT	1	25
1219+00	RT	1	25
TOTALS		<b>2</b>	<b>50</b>

**UNDERDRAIN SCHEDULE**

STATIONING		SIDE (LT/RT)	60100070 SHOULDER REMOVAL AND REPLACEMENT (FOOT)	60108206 PIPE UNDERDRAINS TYPE 2, 6" (FOOT)	x4820110 HMA SHLD. REMOVAL AND REPLACEMENT (SPEC.) (SQ YD)
FROM	TO				
1216+00	1222+00	RT	600.0	600.0	100.0
TOTALS			<b>600</b>	<b>600</b>	<b>100.0</b>

**GUARDRAIL SCHEDULE**

STATIONING		SIDE (LT/RT)	63200310 GUARDRAIL REMOVAL (FOOT)	63000001 STEEL PLATE BEAM GUARDRAIL, TY A, 6 FT POSTS (FOOT)	63100167 TRAFFIC BARRIER TERMINAL, TYPE 1 (SPL), TANG (EACH)	63100085 TRAFFIC BARRIER TERMINAL, TYPE 6 (EACH)	72501000 TERMINAL MARKER - DIRECT APPLIED (EACH)	78200005 GUARDRAIL REFLECTORS, TYPE A (EACH)	48203021 HOT-MIX ASPHALT SHOULDERS, 6" (SQ YD)
FROM	TO								
SN 024-0005									
1230+59	1232+32	RT	180	75.0	1	1	1	4	56.2
1231+48	1232+43	LT	114	12.5	1	1	1	4	35.3
1233+22	1234+10	RT	39	--	1	1	1	4	--
1233+30	1234+18	LT	105	--	1	1	1	4	--
TOTALS			<b>438</b>	<b>88</b>	<b>4</b>	<b>4</b>	<b>4</b>	<b>16</b>	<b>92</b>

NOTE: HMA SHOULDERS 6" IS HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL (SEE HIGHWAY STANDARD 630201).

**40600990 TEMPORARY RAMP**

LOCATION		RAMP WIDTH (FOOT)	DROP-OFF 1 DEPTH (INCHES)	RAMP 1 LENGTH (FOOT)	RAMP 1 AREA (SQ YD)	DROP-OFF 2 DEPTH (INCHES)	RAMP 2 LENGTH (FOOT)	RAMP 2 AREA (SQ YD)	TOTAL RAMP AREA (SQ YD)
STATION	SIDE								
0+00	CL	25.0	2.75	9.0	25.0	1.5	5.0	13.9	38.9
1089+50	CL	25.0	2.75	9.0	25.0	1.5	5.0	13.9	38.9
1092+95	CL	25.0	2.75	9.0	25.0	1.5	5.0	13.9	38.9
1198+15	CL	25.0	2.75	9.0	25.0	1.5	5.0	13.9	38.9
1190+20	CL	25.0	2.75	9.0	25.0	1.5	5.0	13.9	38.9
1232+14	CL	25.0	2.75	9.0	25.0	1.5	5.0	13.9	38.9
TOTAL									<b>233</b>

**Z0049799 PROTECTING OR RESETTING SURVEY MARKERS**

STATION	SIDE (LT/RT)	DESCRIPTION	REPLACEMENT TYPE (IF APPLICABLE)	QUANTITY (EACH)
1185+49.57	CL	MAG NAIL/WASHER	TYPE I	1
1188+99.97	LT	MAG NAIL/WASHER	TYPE I	1
1192+50.27	CL	MAG NAIL/WASHER	TYPE I	1
1198+15.10	CL	MAG NAIL/WASHER	TYPE I	1
1207+92.63	CL	MAG NAIL/WASHER	TYPE I	1
1209+43.39	CL	PK NAIL/WASHER	TYPE I	1
TOTAL				<b>6</b>

**PATCHING SCHEDULE**

LANE	44201811 CLASS D PATCHES, TYPE 1, 14 INCH (SQ YD)	44201815 CLASS D PATCHES, TYPE 2, 14 INCH (SQ YD)	44201819 CLASS D PATCHES, TYPE 3, 14 INCH (SQ YD)	44201821 CLASS D PATCHES, TYPE 4, 14 INCH (SQ YD)
NB	4	612	149	230
SB	--	668	323	612
TOTALS	<b>4</b>	<b>1279</b>	<b>472</b>	<b>842</b>

**PAVEMENT MARKING SCHEDULE**

STATIONING		LENGTH FOOT	CENTER LINE STRIPING TYPE	70300100	70300150	78001110 PAINT PAVEMENT MARKING - LINE 4"				78000500	78000600	78000650	X7830076	X7830078	X7830090	78100100
				SHORT TERM PAVEMENT MARKINGS (3 APPL.) (FOOT)	SHORT TERM PAV'T MARK REMOVAL (1 APPL.) (SQ FT)	4" SOLID WHITE EDGE LINE (FOOT)	4" YELLOW SKIP DASH LINE (FOOT)	4" SOLID YELLOW NB LINE (FOOT)	4" SOLID YELLOW SB LINE (FOOT)	THERM PLAST PAVT MARK - LINE 8" WHITE (FOOT)	THERM PLAST PAVT MARK - LINE 12" WHITE (FOOT)	THERM PLAST PAVT MARK - LINE 24" WHITE (FOOT)	GROOVING FOR RECESSED PAVT MARKING 9" (FOOT)	GROOVING FOR RECESSED PAVT MARKING 13" (FOOT)	GROOVING FOR RECESSED PAVT MARKING 25" (FOOT)	RAISED REFLECTIVE PAV'T MARK (EACH)
0+00	21+00	2,100	SKIP	630	69	4,200	525	--	--	16	90	20	16	90	20	26
21+00	21+87	87	SKIP	26	3	174	22	--	--	--	--	--	--	--	--	1
21+87	32+45	1,058	NBP	317	35	2,116	265	1,058	--	--	--	--	--	--	--	13
32+45	36+69	424	DUAL	127	14	848	--	424	424	--	--	--	--	--	--	5
36+69	46+74	1,005	SBP	302	33	2,010	251	--	1,005	--	--	--	--	--	--	13
46+74	50+55	381	SKIP	114	13	762	95	--	--	--	--	--	--	--	--	5
52+17	84+20	3,203	SKIP	961	106	6,406	801	--	--	--	--	--	--	--	--	40
84+20	92+43	823	NBP	247	27	1,646	206	823	--	--	--	--	--	--	--	10
92+43	97+03	460	DUAL	138	15	920	--	460	460	--	--	--	--	--	--	6
97+03	98+84	181	SBP	54	6	362	45	--	181	--	--	--	--	--	--	2
922+11	929+93	782	SBP	235	26	1,564	196	--	782	--	--	--	--	--	--	10
929+93	948+28	1,835	SKIP	551	61	3,670	459	--	--	--	--	--	--	--	--	23
948+28	956+97	869	NBP	261	29	1,738	217	869	--	--	--	--	--	--	--	11
956+97	966+05	908	DUAL	272	30	1,816	--	908	908	--	--	--	--	--	--	11
966+05	967+58	153	SBP	46	5	306	38	--	153	--	--	--	--	--	--	2
967+58	975+30	764	SBP	229	25	1,528	191	--	764	--	--	--	--	--	--	10
975+30	977+37	207	SKIP	62	7	414	52	--	--	--	--	--	--	--	--	3
977+37	986+40	903	NBP	271	30	1,806	226	903	--	--	--	--	--	--	--	11
986+40	987+97	157	SKIP	47	5	314	39	--	--	--	--	--	--	--	--	2
987+97	991+07	310	SBP	93	10	620	78	--	310	--	--	--	--	--	--	4
991+07	996+05	495	SBP	149	16	990	124	--	495	--	--	--	--	--	--	6
996+05	1003+52	747	SKIP	224	25	1,494	187	--	--	--	--	--	--	--	--	9
1003+52	1022+49	1,897	SKIP	569	63	3,794	474	--	--	--	--	--	--	--	--	24
1022+49	1023+07	58	NBP	17	2	116	15	58	--	--	--	--	--	--	--	1
1023+07	1032+57	942	NBP	283	31	1,884	236	942	--	--	--	--	--	--	--	12
1032+57	1033+41	84	SKIP	25	3	168	21	--	--	--	--	--	--	--	--	1
1033+41	1041+56	815	SBP	245	27	1,630	204	--	815	--	--	--	--	--	--	10
1041+56	1042+62	106	DUAL	32	4	212	--	106	106	--	--	--	--	--	--	1
1042+62	1048+25	563	NBP	169	19	1,126	141	563	--	--	--	--	--	--	--	7
1048+25	1051+55	330	SKIP	99	11	660	83	--	--	--	--	--	--	--	--	4
1051+55	1052+95	140	SBP	42	5	280	35	--	140	--	--	--	--	--	--	2
1052+95	1053+01	547	SBP	164	18	1,094	137	--	547	--	--	--	--	--	--	7
1053+01	1058+48	648	SKIP	194	21	1,296	162	--	--	--	--	--	--	--	--	8
1058+48	1064+96	648	SKIP	194	21	1,296	162	--	--	--	--	--	--	--	--	8
1064+96	1089+50	2,452	SKIP	736	81	4,904	613	--	--	--	--	--	--	--	--	31
1089+50	1092+95	345	SKIP	BRIDGE OMISSION		690	86	--	--	--	--	--	--	--	--	4
1092+95	1115+91	2,296	SKIP	689	76	4,592	574	--	--	--	--	--	--	--	--	29
1115+91	1125+60	969	NBP	291	32	1,938	242	969	--	--	--	--	--	--	--	12
1125+60	1130+34	474	DUAL	142	16	948	--	474	474	--	--	--	--	--	--	6
1130+34	1136+38	604	SBP	181	20	1,208	151	--	604	--	--	--	--	--	--	8
1136+38	1141+75	537	SKIP	161	18	1,074	134	--	--	--	--	--	--	--	--	7
1141+75	1153+90	1,079	SKIP	324	36	2,158	270	--	--	--	--	--	--	--	--	13
1153+90	1164+42	1,052	NBP	316	35	2,104	263	1,052	--	--	--	--	--	--	--	13
1164+42	1166+85	243	DUAL	73	8	486	--	243	243	--	--	--	--	--	--	3
1166+85	1174+69	784	SBP	235	26	1,568	196	--	784	--	--	--	--	--	--	10
1174+69	1176+72	160	SBP	48	5	320	40	--	160	--	--	--	--	--	--	2
1176+72	1198+15	2,143	SKIP	643	71	4,286	536	--	--	--	--	--	--	--	--	27
1198+15	1199+20	105	SKIP	BRIDGE OMISSION		210	26	--	--	--	--	--	--	--	--	1
1199+20	1224+78	2,558	SKIP	767	84	5,116	640	--	--	--	--	--	--	--	--	32
1224+78	1231+92	712	SKIP	316	35	1,424	178	--	--	--	--	--	--	--	--	9
1231+92	1232+00	14	SKIP	4	0	28	4	--	--	--	--	--	--	--	--	0
1232+00	1232+14	14	SKIP	4	0	28	4	--	--	--	--	--	--	--	--	0
1232+14	1237+20	506	SKIP	150	49.5	1,012	127	--	--	--	--	--	--	--	--	6
SUBTOTALS				12,121	1,337	82,030	9,605	9,852	9,355	16	90	20	16	90	20	513
TOTALS						110,842										

PAVING SCHEDULE

STATIONING		LENGTH (FEET)	WIDTH (FEET)	WIDTH SHOULDER (FEET)	TOTAL EOP WIDTH (FEET)	AREA MAINLINE (SQ FT)	AREA SHOULDER (SQ FT)	AREA COMBINED (SQ FT)	40600290	40600637	40603315	48203003	48203023	44000155	44000160	64200108	X4401198	Z0033700	X4060995
FROM	TO								BITUMINOUS MATERIALS (TACK COAT) MAINLINE (POUND)	LEVELING BINDER (MACHINE METHOD), N70 (TON)	HMA SURF COURSE, MIX "C", N70 (TON)	HMA SHOULDER 1.5" (SQ YD)	HMA SHOULDER 6.5" (SQ YD)	HMA SURF REMOVAL, 1.5" (SQ YD)	HMA SURF REMOVAL, 2.75" (SQ YD)	SHOULDER RUMBLE STRIPS 8 INCH (FOOT)	HMA SURF REMOVAL, VARIABLE DEPTH (SQ YD)	LONGITUDINAL JOINT SEALANT (FEET)	TEMPORARY RAMP, SPL (SQ YD)
00+00	50+55	5,055	22.0	4.0	923.0	111,210	40,440	151,650	8,340.8	865.0	1,038.0	4,493.3	4,493.3	12,356.7	--	10,110.0	--	5,055.0	205.1
52+17	98+84	4,667	22.0	4.0	267.0	102,674	37,336	140,010	7,700.6	798.6	958.3	4,148.4	4,148.4	11,408.2	--	9,334.0	--	4,667.0	59.3
922+11	967+58	4,547	22.0	4.0	148.0	100,034	36,376	136,410	7,502.6	778.0	933.7	4,041.8	4,041.8	11,114.9	--	9,094.0	--	4,547.0	32.9
967+66	991+07	2,341	22.0	4.0	129.0	51,502	18,728	70,230	3,862.7	400.6	480.7	2,080.9	2,080.9	5,722.4	--	4,682.0	--	2,341.0	28.7
991+10	1003+52	1,242	22.0	4.0	158.0	27,324	9,936	37,260	2,049.3	212.5	255.0	1,104.0	1,104.0	3,036.0	--	2,484.0	--	1,242.0	35.1
1003+52	1023+07	1,955	22.0	4.0	70.0	43,010	15,640	58,650	3,225.8	334.5	401.4	1,737.8	1,737.8	4,778.9	--	3,910.0	--	1,955.0	15.6
1023+15	1052+95	2,980	22.0	4.0	223.0	65,560	23,840	89,400	4,917.0	509.9	611.9	2,648.9	2,648.9	7,284.4	--	5,960.0	--	2,980.0	49.6
1053+01	1064+96	1,195	22.0	4.0	189.0	26,290	9,560	35,850	1,971.8	204.5	245.4	1,062.2	1,062.2	2,921.1	--	2,390.0	--	1,195.0	42.0
1064+98	1087+70	2,272	22.0	4.0	183.0	49,984	18,176	68,160	3,748.8	388.8	466.5	2,019.6	2,019.6	5,553.8	--	4,544.0	--	2,272.0	40.7
1087+70	1089+50	180	0.0	4.0	0.0	0	1,440	1,440	0.0	0.0	0.0	160.0	160.0	0.0	--	360.0	--	--	0.0
1089+50	1092+95	BRIDGE OMISSION																	
1092+95	1094+65	170	0.0	4.0	0.0	0	1,360	1,360	0.0	0.0	0.0	151.1	151.1	0.0	--	340.0	--	--	0.0
1094+65	1141+75	4,710	22.0	4.0	496.0	103,620	37,680	141,300	7,771.5	805.9	967.1	4,186.7	4,186.7	11,513.3	--	9,420.0	--	4,710.0	110.2
1143+11	1174+69	3,158	22.0	4.0	214.0	69,476	25,264	94,740	5,210.7	540.4	648.4	2,807.1	2,807.1	7,719.6	--	6,316.0	--	3,158.0	47.6
1175+12	1198+15	2,303	22.0	4.0	120.0	50,666	18,424	69,090	3,800.0	394.1	472.9	2,047.1	2,047.1	5,629.6	--	4,606.0	--	2,303.0	26.7
1198+15	1199+20	BRIDGE OMISSION																	
1199+20	1213+59	1,439	22.0	4.0	175.0	31,658	11,512	43,170	2,374.4	246.2	295.5	1,279.1	1,279.1	3,517.6	--	2,878.0	--	1,439.0	38.9
1213+59	1214+39	80	22.0	4.0	0.0	1,760	640	2,400	132.0	13.7	16.4	71.1	71.1	--	--	160.0	195.6	80.0	0.0
1214+39	1219+72	533	22.0	4.0	67.0	11,726	2,132	13,858	879.5	91.2	109.4	236.9	236.9	--	1,302.9	533.0	--	533.0	14.9
1219+72	1222+60	288	22.0	4.0	275.0	6,336	2,304	8,640	475.2	49.3	59.1	256.0	256.0	--	704.0	576.0	--	288.0	61.1
1222+60	1223+40	80	22.0	4.0	0.0	1,760	320	2,080	132.0	13.7	16.4	35.6	35.6	--	--	80.0	195.6	80.0	0.0
1223+40	1224+78	138	22.0	4.0	60.0	3,036	1,104	4,140	227.7	23.6	28.3	122.7	122.7	337.3	--	276.0	--	138.0	13.3
1224+80	1229+12	432	22.0	4.0	80.0	9,504	3,456	12,960	712.8	73.9	88.7	384.0	384.0	1,056.0	--	864.0	--	432.0	17.8
1229+12	1231+92	280	22.0	0.0	110.0	6,160	0	6,160	462.0	47.9	57.5	0.0	0.0	684.4	--	560.0	--	280.0	24.4
1232+00	1232+14	14	22.0	0.0	0.0	308	0	308	23.1	2.4	2.9	0.0	0.0	34.2	--	28.0	--	14.0	0.0
DEDUCTION		2,508		4.0		0	20,064	20,064	0.0	0.0	0.0	2,229.3	2,229.3	--	--	2,508.0	--	--	0.0
TOTALS									65,520	6,795	8,154	32,845	32,845	94,668	2,007	76,997	391	39,709	864

60600095 CLASS SI CONCRETE (OUTLET)

STATION	SIDE (LT/RT)	SECTION F-F LENGTH (FOOT)	SECTION F-F (CU YD)	SECTION A-A TO E-E (CU YD)	TOTAL QUANTITY (CU YD)
1214+39	LT	30.0	2.37	3.53	5.90
TOTAL					5.9

IMPACT ATTENUATOR SCHEDULE

STATIONING		SIDE (LT/RT)	70600240	70600340
FROM	TO		IMPACT ATTENUATORS, TEMPORARY NON-REDIRECTIVE) TEST LEVEL 2 (EACH)	IMPACT ATTENUATORS, RELOCATE NON-REDIRECTIVE) TEST LEVEL 2 (EACH)
STAGE 1				
1230+72	1231+03	RT	1	--
1234+12	1234+43	RT	1	--
STAGE 2				
1230+54	1230+85	LT	--	1
1234+61	1234+92	LT	--	1
TOTALS			2	2

CONCRETE GUTTER SCHEDULE

STATIONING		SIDE (LT/RT)	44000400	60602500
FROM	TO		GUTTER REMOVAL (FOOT)	CONCRETE GUTTER, TYPE A (FOOT)
1214+39	1219+72	LT	533	533
TOTALS			533	533

PAVEMENT MARKING BLACKOUT TAPE, 5"

STATIONING		LENGTH (FOOT)	WIDTH (FOOT)	4" WHITE	4" YELLOW
FROM	TO			SOLID EDGE LINES (2 EACH) (FT)	SKIP DACH CENTER LINE (FT)
STAGE 1					
1227+95	1231+92	397.00	0.333	794.0	99.3
1232+00	1237+20	520.00	0.333	1,040.0	130.0
SUBTOTALS				1,834	229
TOTALS				2,063	

**TEMPORARY CONCRETE BARRIER SCHEDULE**

STATIONING		70400100 TEMPORARY CONCRETE BARRIER	70400200 RELOCATE TEMPORARY CONCRETE BARRIER
FROM	TO	(FOOT)	(FOOT)
<b>STAGE 1</b>			
1231+03	1231+92	89	--
1232+00	1234+12	212	--
<b>STAGE 2</b>			
1230+85	1231+92	--	107
1232+00	1234+61	63	198
<b>TOTALS</b>		<b>363</b>	<b>305</b>

**RIPRAP SCHEDULE**

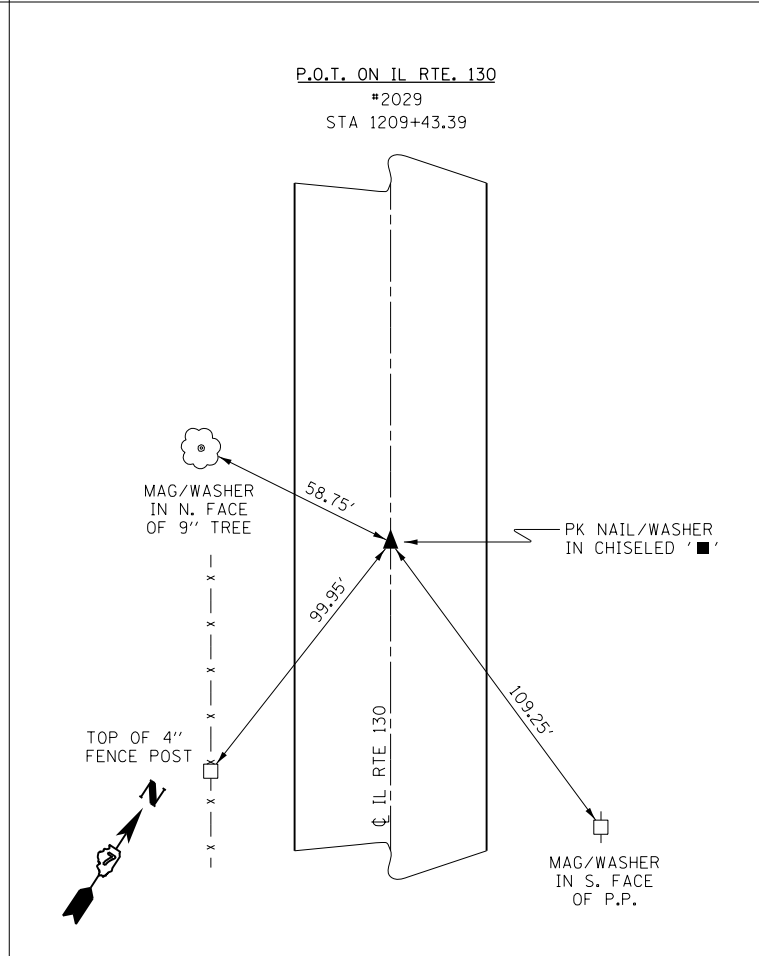
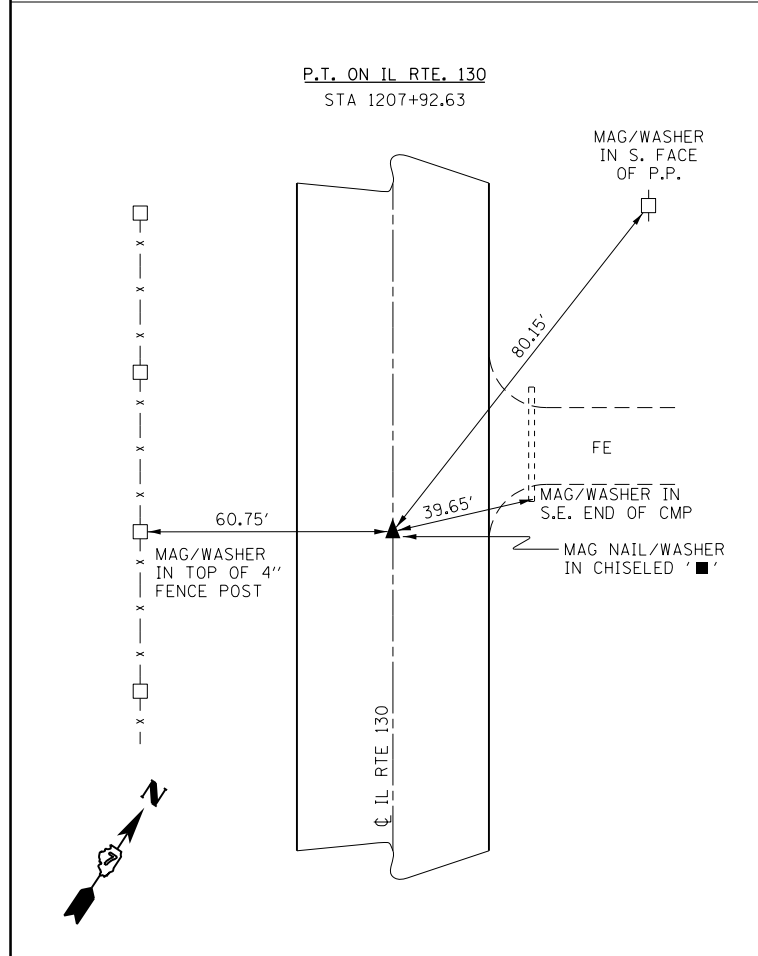
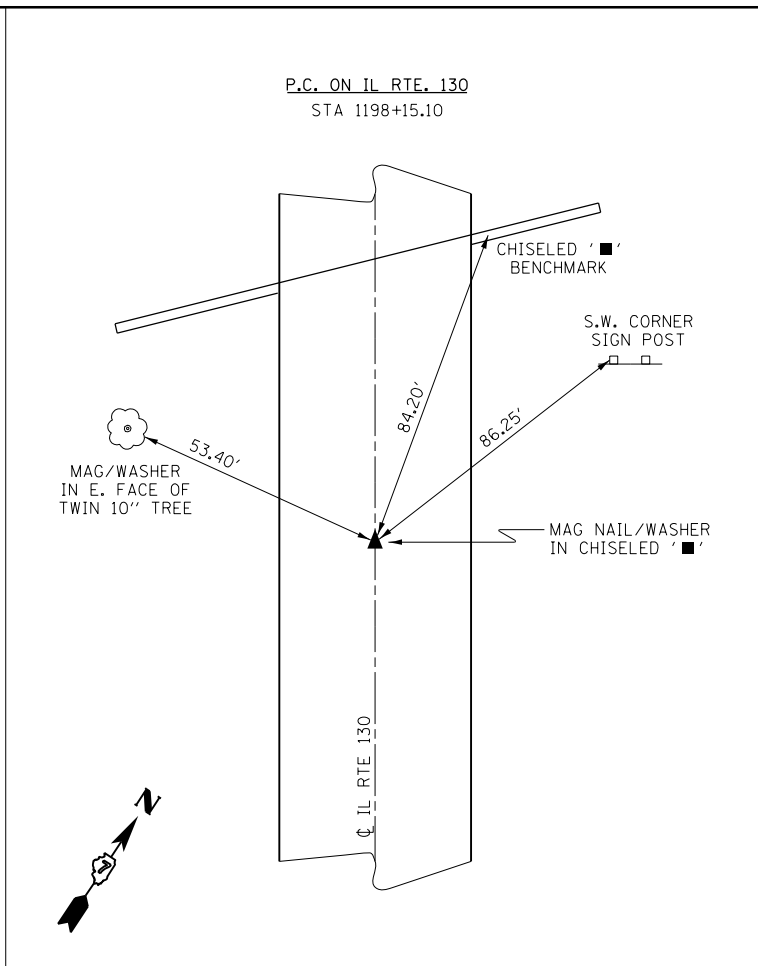
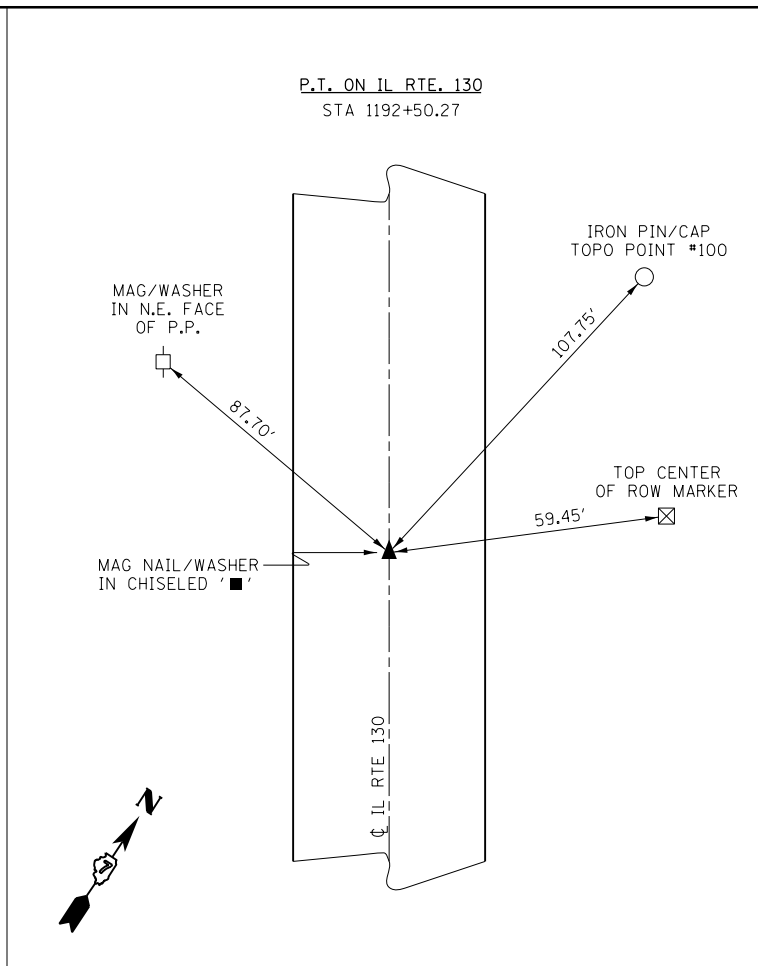
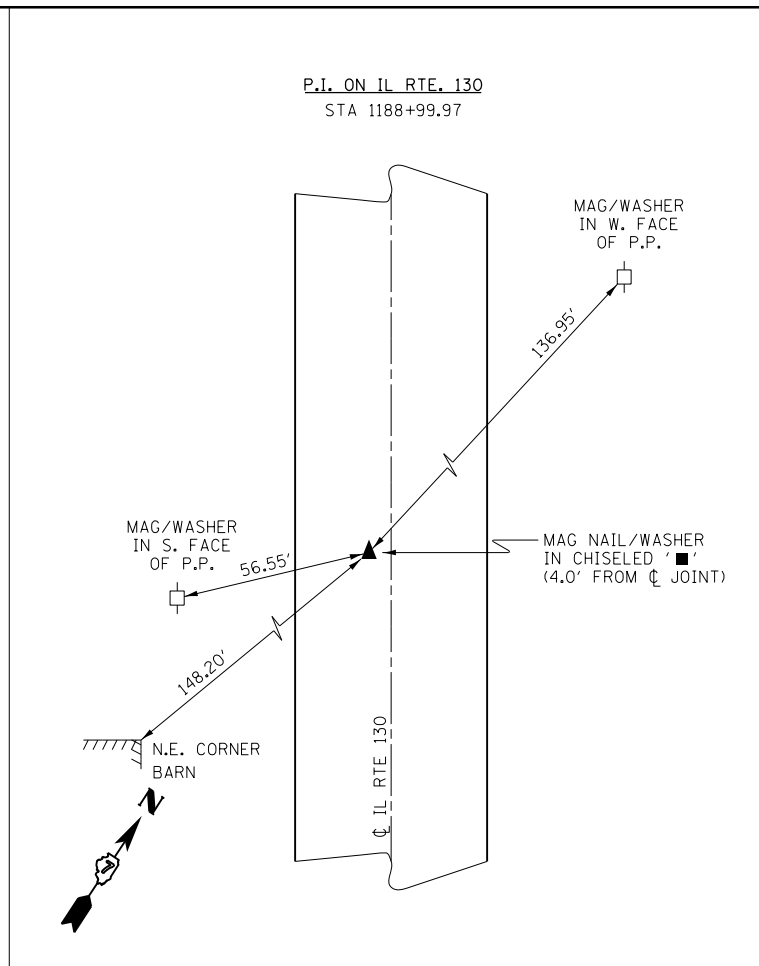
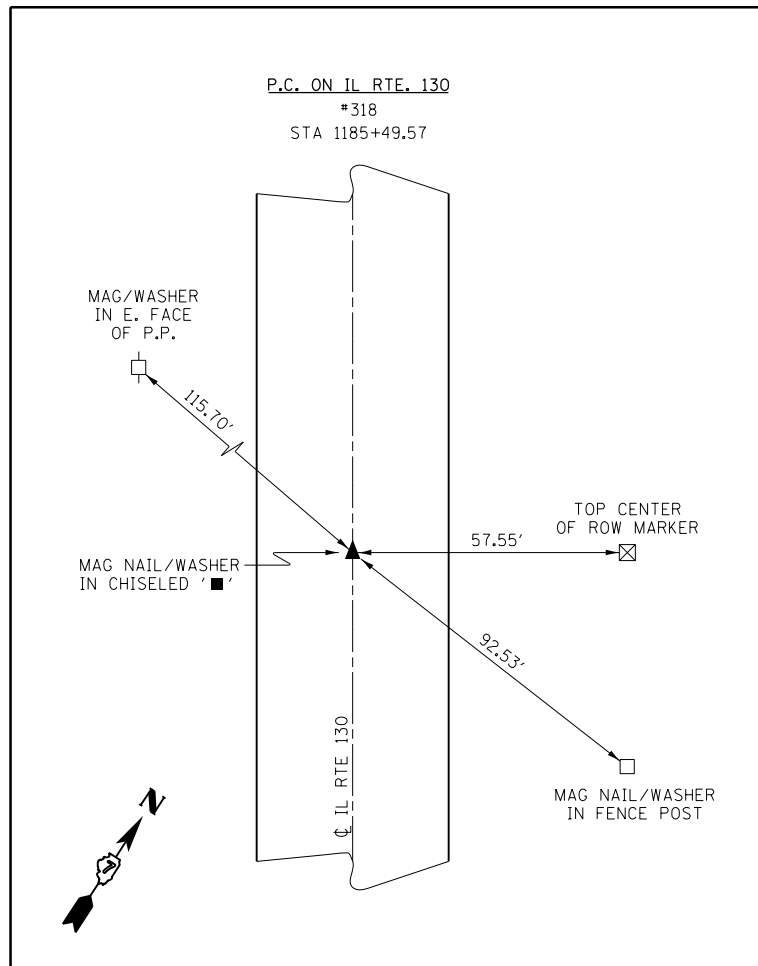
STATION	SIDE (LT/RT)	RIPRAP WIDTH (FOOT)	RIPRAP SIDE 1 LENGTH (FOOT)	RIPRAP SIDE 2 LENGTH (FOOT)	28200200 FILTER FABRIC (SQ YD)	28100209 RIPRAP CLASS A5 (TON)
3+36	RT	35	18	53	138	127
46+84	RT	35	18	38	109	100
<b>TOTALS</b>					<b>247</b>	<b>227</b>

**TEMPORARY GUARDRAIL SCHEDULE**

STATIONING		SIDE (LT/RT)	70500615 TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1 (EACH)	70500665 TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 6 (EACH)
FROM	TO	(LT/RT)	(EACH)	(EACH)
<b>SN 024-0005</b>				
1233+22	1234+10	RT	1	1
<b>TOTALS</b>			<b>1</b>	<b>1</b>

**SHOULDER WIDENING SCHEDULE**

STATIONING		LENGTH (FEET)	WIDTH SHOULDER (FEET)	SIDE LT/RT	BEGIN		END		AVERAGE		20200600 EXCAVATING & GRADING EXISTING SHOULDERS (UNIT)	44004250 PAVED SHOULDER REMOVAL (SQ YD)	35501324 HMA BASE COURSE 10" (SQ YD)
FROM	TO				WIDTH		WIDTH		WIDTH				
FOOT	FOOT				FOOT	FOOT	FOOT	FOOT	FOOT	FOOT			
00+00	50+55	5,055	1.5	LT/RT	--	--	--	--	--	--	101	1685.0	--
52+17	98+84	4,667	1.5	LT/RT	--	--	--	--	--	--	93	1555.7	--
922+11	967+58	4,547	1.5	LT/RT	--	--	--	--	--	--	91	1515.7	--
967+66	991+07	2,341	1.5	LT/RT	--	--	--	--	--	--	47	780.3	--
991+10	1003+52	1,242	1.5	LT/RT	--	--	--	--	--	--	25	414.0	--
1003+52	1023+07	1,955	1.5	LT/RT	--	--	--	--	--	--	39	651.7	--
1023+15	1052+95	2,980	1.5	LT/RT	--	--	--	--	--	--	60	993.3	--
1053+01	1064+96	1,195	1.5	LT/RT	--	--	--	--	--	--	24	398.3	--
1064+98	1087+70	2,272	1.5	LT/RT	--	--	--	--	--	--	45	757.3	--
1087+70	1089+50	180	1.5	LT/RT	--	--	--	--	--	--	4	60.0	--
1089+50	1092+95	BRIDGE OMISSION											
1092+95	1094+65	170	1.5	LT/RT	--	--	--	--	--	--	3	56.7	--
1094+65	1141+75	4,710	1.5	LT/RT	--	--	--	--	--	--	94	1570.0	--
1143+11	1174+69	3,158	1.5	LT/RT	--	--	--	--	--	--	63	1052.7	--
1175+12	1198+15	2,303	1.5	LT/RT	--	--	--	--	--	--	46	767.7	--
1198+15	1199+20	BRIDGE OMISSION											
1199+20	1213+59	1,439	1.5	LT/RT	--	--	--	--	--	--	29	479.7	--
1213+59	1214+39	80	1.5	LT/RT	--	--	--	--	--	--	2	26.7	--
1214+39	1219+72	533	1.5	RT	--	--	--	--	--	--	5	--	--
1219+72	1222+60	288	1.5	LT/RT	--	--	--	--	--	--	6	96.0	--
1222+60	1223+40	80	1.5	RT	--	--	--	--	--	--	1	13.3	--
1223+40	1224+78	138	1.5	LT/RT	--	--	--	--	--	--	3	46.0	--
1224+80	1229+12	432	1.5	LT/RT	--	--	--	--	--	--	9	144.0	--
<b>TOTAL</b>											<b>790</b>	<b>13064.1</b>	
<b>STAGE CONSTRUCTION I/II</b>													
1229+12	1231+92	280	--	LT	1.5	6.5	1.5	6.5	1.5	6.5	--	46.7	202.2
1232+00	1232+31	31	--	LT	1.5	6.5	1.5	6.5	1.5	6.5	--	5.2	22.4
1233+66	1233+85	19	--	LT	6.0	6.0	2.0	2.0	4.0	4.0	--	8.4	8.4
1230+45	1231+92	147	--	RT	1.5	6.5	1.5	6.5	1.5	6.5	--	24.5	106.2
1232+00	1232+19	19	--	RT	1.5	6.5	1.5	6.5	1.5	6.5	--	3.2	13.7
1233+34	1233+50	16	--	RT	6.0	6.0	5.4	5.4	5.7	5.7	--	10.1	10.1
1233+50	1233+71	21	--	RT	5.4	5.4	3.0	3.0	4.2	4.2	--	9.8	9.8
<b>TOTAL</b>											<b>0</b>	<b>108</b>	<b>372.8</b>
<b>DEDUCTION</b>		<b>-2,508</b>	<b>1.5</b>								<b>-25</b>	<b>-418.0</b>	<b>--</b>
<b>GRAND TOTALS</b>											<b>765</b>	<b>12754</b>	<b>373</b>



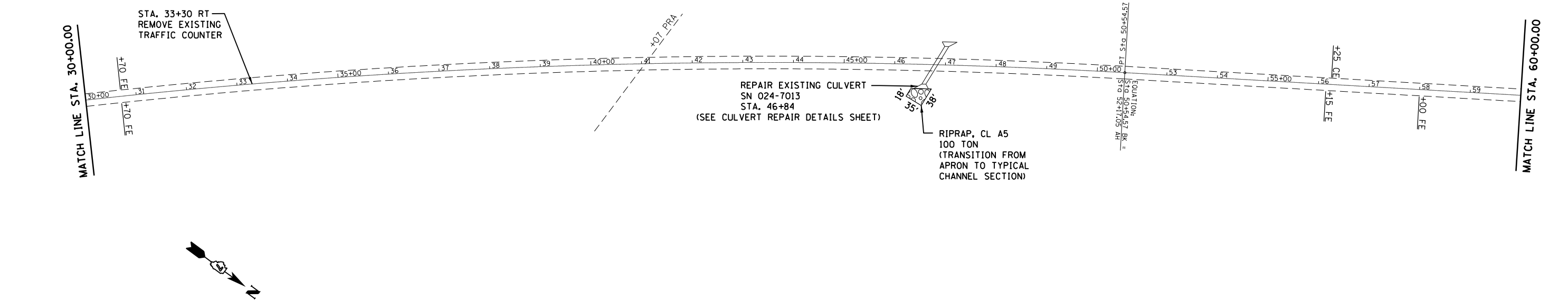
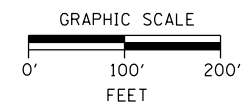
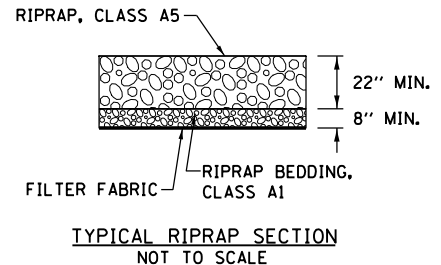
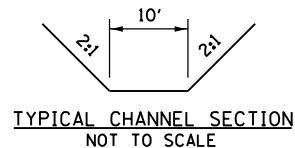
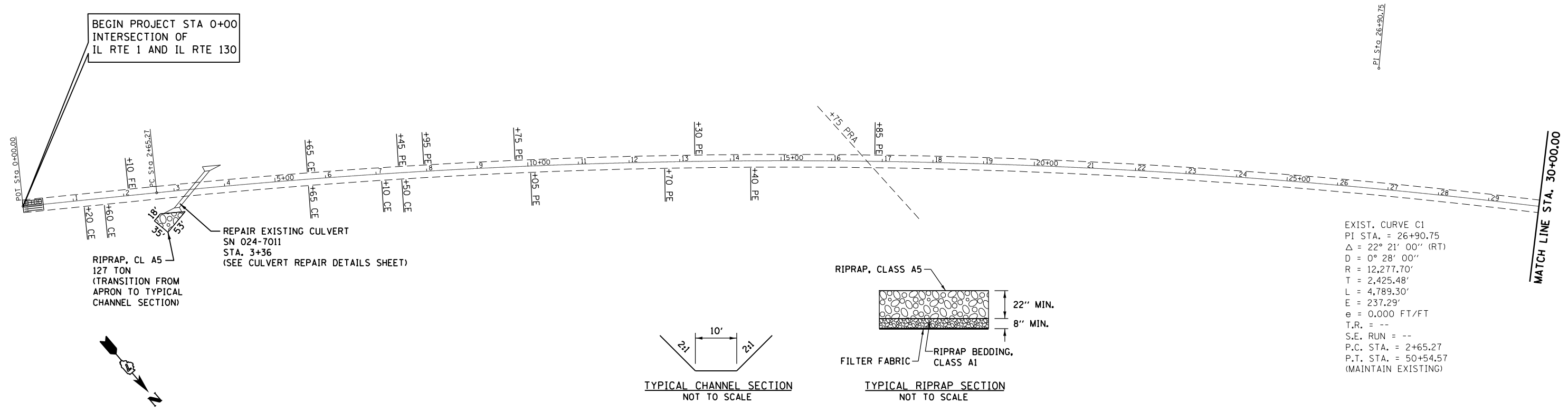
**NOTE:** SEE SCHEDULES OF QUANTITIES FOR ADDITIONAL SURVEY MARKER INFORMATION.

FILE NAME =	USER NAME = steffennk	DESIGNED -	REVISED -
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-sht-atb.dgn		REVISION	REVISION
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/29/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SURVEY MARKER REFERENCE TIES			
SCALE: N/A	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	13
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				

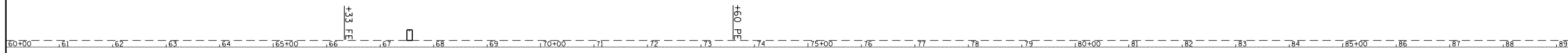


NOTE: REMOVAL OF EXISTING TRAFFIC COUNTER IS INCLUDED IN THE COST OF NEW TRAFFIC COUNTER; SEE SPECIAL PROVISIONS.

FILE NAME =	USER NAME = steffemk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-sht-plan.dgn		REVISIONS	REVISIONS		SCALE: 1"=100'	SHEET 1 OF 7 SHEETS	STA. 0+00.00 TO STA. 60+00.00	116	(6,7)RS-3	EDWARDS	41	14
PLOT SCALE = 200.0000' / in.	CHECKED -	REVISIONS	REVISIONS					CONTRACT NO. 74703				
Default	PLOT DATE = 3/29/2019	DATE -	REVISIONS					ILLINOIS FED. AID PROJECT				

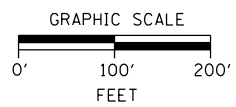
MATCH LINE STA. 60+00.00

MATCH LINE STA. 90+00.00



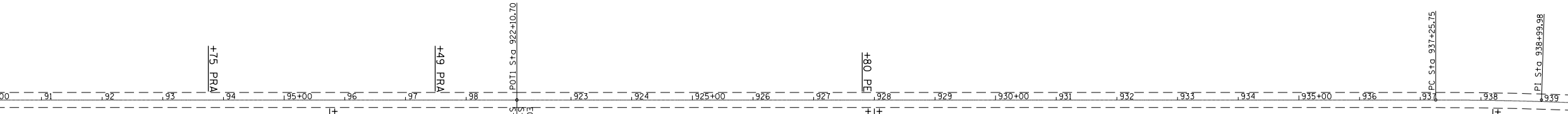
CONSTRUCT  
TRAFFIC COUNTER  
STA. 67+55  
(SEE TRAFFIC COUNTER  
DETAILS SHEET)

NOTE: SEE GENERAL NOTES FOR GUIDANCE WHEN DROP-OFFS DURING  
CONSTRUCTION WILL BE PRESENT ADJACENT TO THE EDGE OF PAVEMENT.



MATCH LINE STA. 90+00.00

MATCH LINE STA. 940+00.00



EXIST. CURVE C2  
PI STA. = 938+99.98  
 $\Delta = 3^{\circ} 29' 00''$  (RT)  
D = 1^{\circ} 00' 00''  
R = 5,729.70'  
T = 174.22'  
L = 348.34'  
E = 2.65'  
e = 0.025 FT/FT  
T.R. = --  
S.E. RUN = --  
P.C. STA. = 937+25.75  
P.T. STA. = 940+74.09  
(MAINTAIN EXISTING)

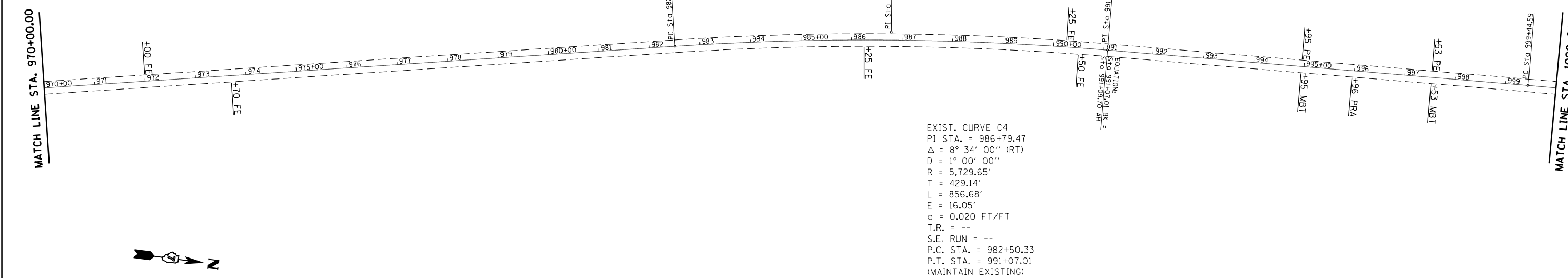
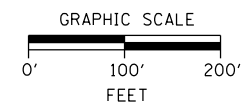
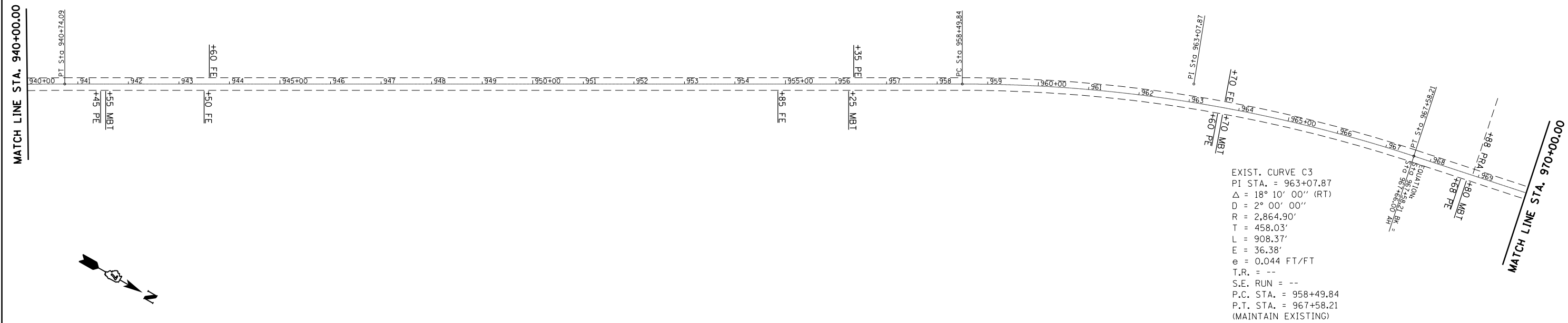
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Default	PLOT DATE = 4/1/2019	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PLAN SHEET

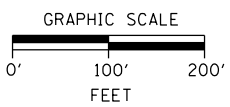
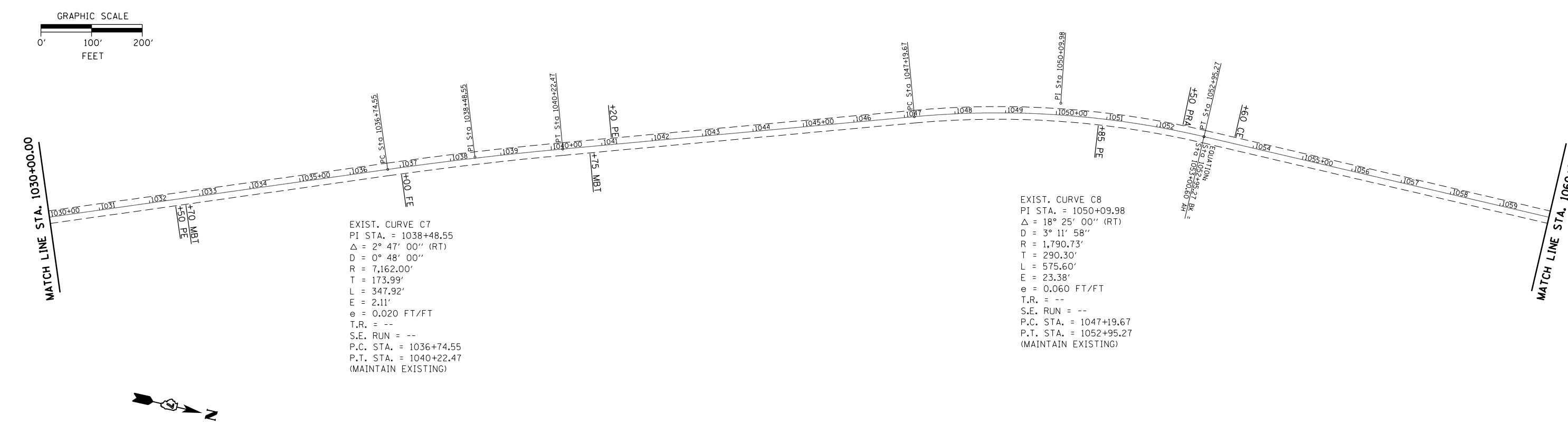
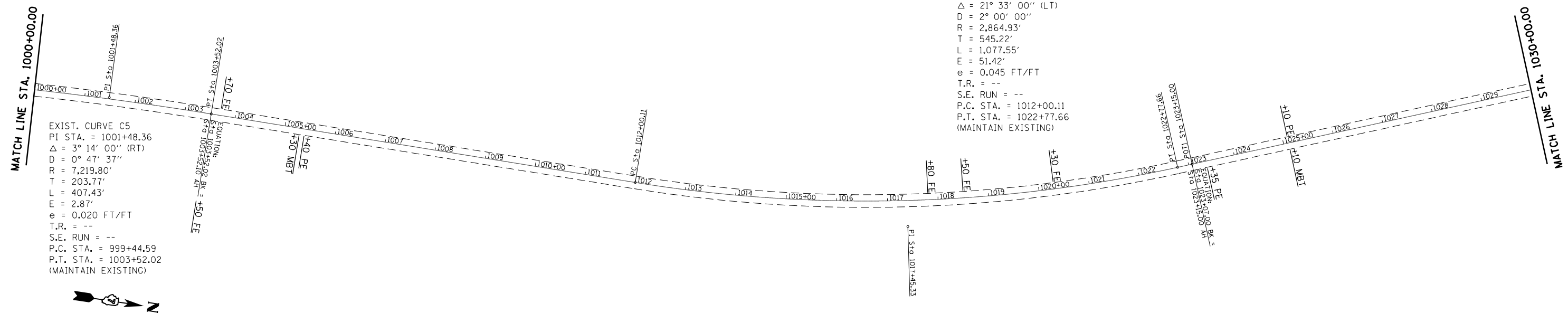
SCALE: 1"=100' SHEET 2 OF 7 SHEETS STA. 60+00.00 TO STA. 940+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	15
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				

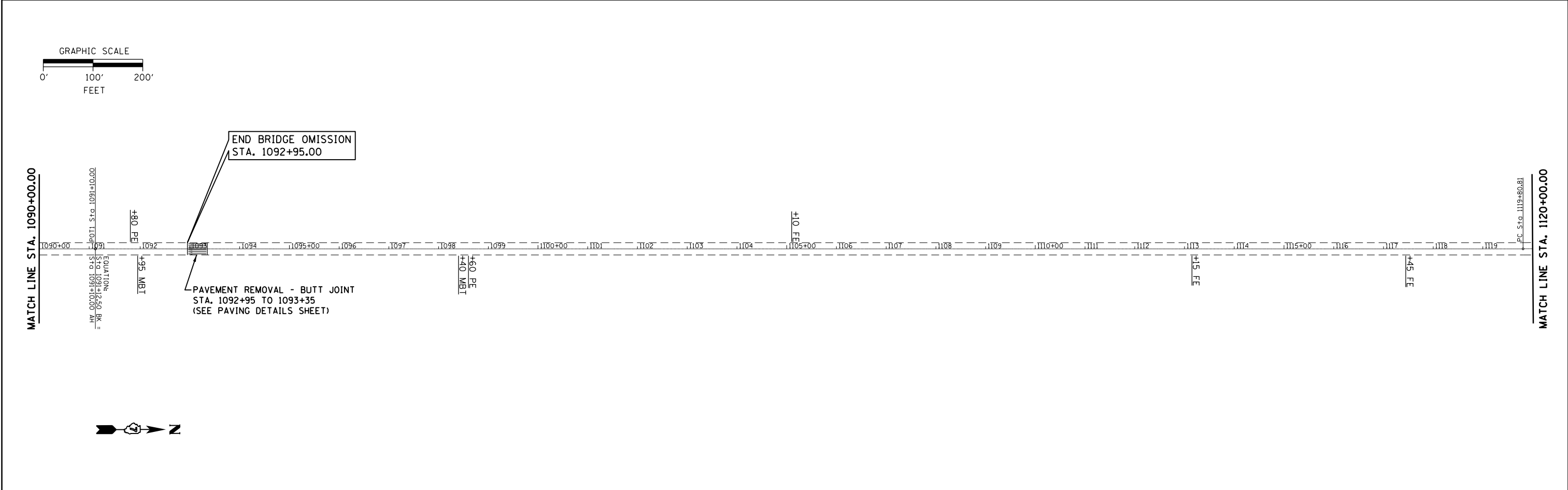
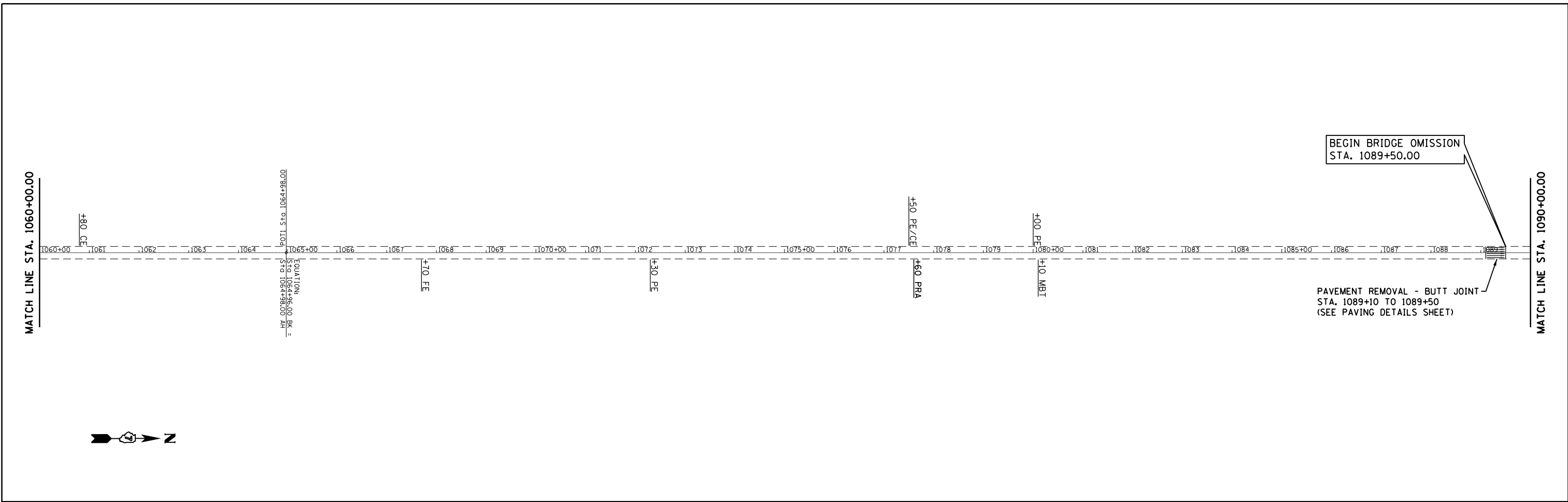


FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 200.0000' / in.		DATE -	REVISED -		CONTRACT NO. 74703							
Default		PLOT DATE = 3/29/2019	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE: 1"=100'		SHEET 3 OF 7 SHEETS		STA. 940+00.00 TO STA. 1000+00.00				





FILE NAME =	USER NAME = steffennk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLOT DATE = 3/29/2019	DATE -	REVISED -			CONTRACT NO. 74703					
						ILLINOIS FED. AID PROJECT					
SCALE: 1"=100'						SHEET 4 OF 7 SHEETS		STA. 1000+00.00 TO STA. 1060+00.00			



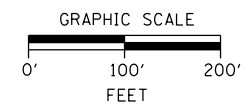
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Default	PLOT DATE = 3/29/2019	DATE -	REVISED -			CONTRACT NO. 74703					
						ILLINOIS FED. AID PROJECT					

SCALE: 1"=100' SHEET 5 OF 7 SHEETS STA. 1060+00.00 TO STA. 1120+00.00

MATCH LINE STA. 1120+00.00

MATCH LINE STA. 1150+00.00

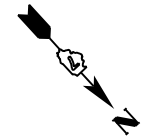
EXIST. CURVE C9  
 PI STA. = 1131+70.14  
 $\Delta$  = 54° 51' 00" (LT)  
 D = 2° 29' 59"  
 R = 2,292.00'  
 T = 1,189.33'  
 L = 2,194.16'  
 E = 290.20'  
 e = 0.052 FT/FT  
 T.R. = --  
 S.E. RUN = --  
 P.C. STA. = 1119+80.81  
 P.T. STA. = 1141+74.97  
 (MAINTAIN EXISTING)



MATCH LINE STA. 1150+00.00

MATCH LINE STA. 1180+00.00

EXIST. CURVE C10  
 PI STA. = 1169+50.79  
 $\Delta$  = 20° 59' 00" (RT)  
 D = 2° 00' 00"  
 R = 2,864.90'  
 T = 530.55'  
 L = 1,049.21'  
 E = 48.71'  
 e = 0.045 FT/FT  
 T.R. = --  
 S.E. RUN = --  
 P.C. STA. = 1164+20.24  
 P.T. STA. = 1174+69.45  
 (MAINTAIN EXISTING)



FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -
p:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-sht-plan.dgn		CHECKED -	REVISED -
Default	PLOT DATE = 3/29/2019	DATE -	REVISED -

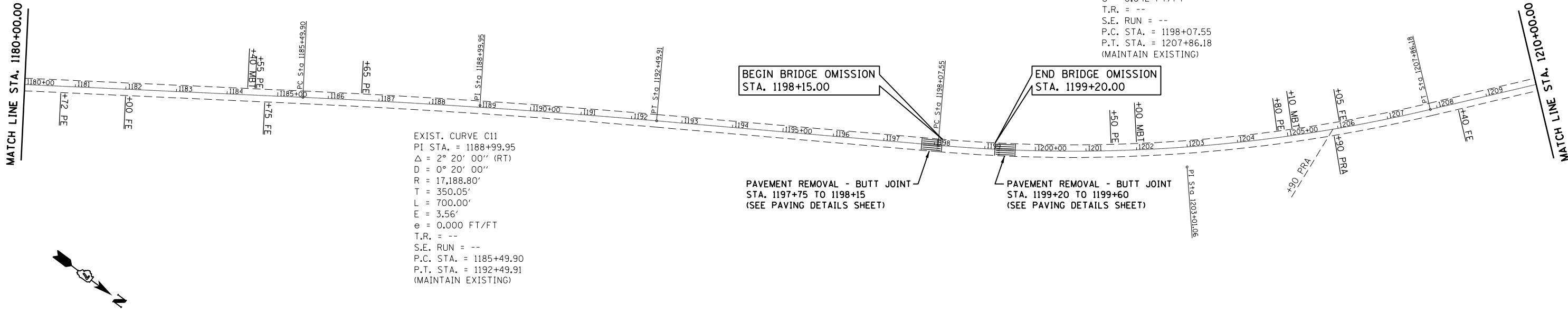
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PLAN SHEET

SCALE: 1"=100' SHEET 6 OF 7 SHEETS STA. 1120+00.00 TO STA. 1180+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	19
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				

EXIST. CURVE C12  
 PI STA. = 1203+01.06  
 $\Delta = 18^\circ 16' 00''$  (LT)  
 $D = 1^\circ 52' 00''$   
 $R = 3,069.60'$   
 $T = 493.50'$   
 $L = 978.63'$   
 $E = 39.42'$   
 $e = 0.042$  FT/FT  
 $T.R. = --$   
 $S.E. RUN = --$   
 $P.C. STA. = 1198+07.55$   
 $P.T. STA. = 1207+86.18$   
 (MAINTAIN EXISTING)



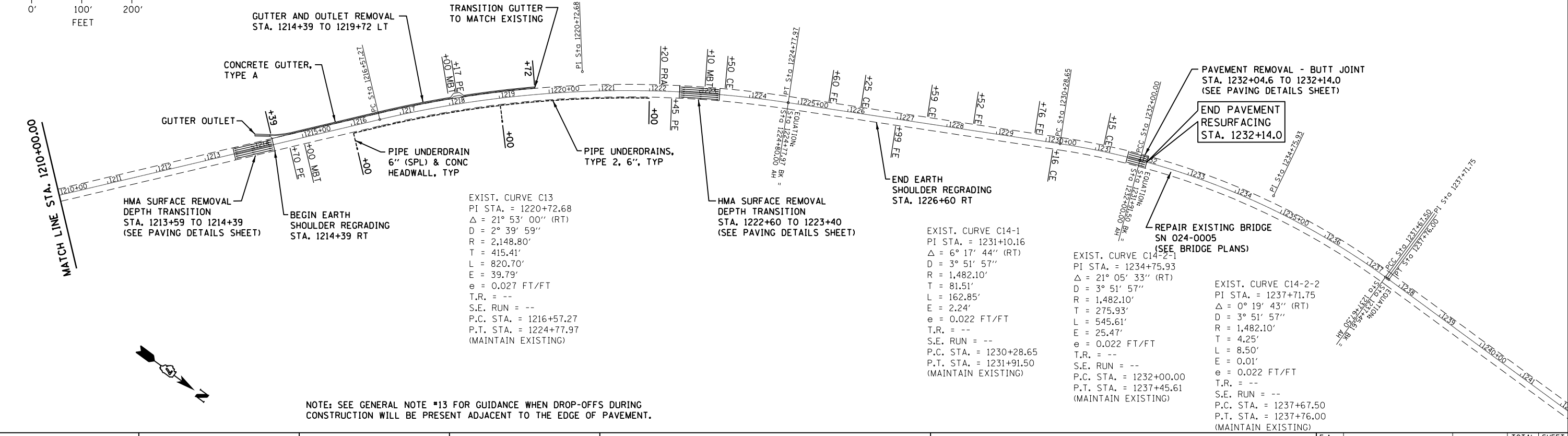
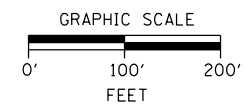
EXIST. CURVE C11  
 PI STA. = 1188+99.95  
 $\Delta = 2^\circ 20' 00''$  (RT)  
 $D = 0^\circ 20' 00''$   
 $R = 17,188.80'$   
 $T = 350.05'$   
 $L = 700.00'$   
 $E = 3.56'$   
 $e = 0.000$  FT/FT  
 $T.R. = --$   
 $S.E. RUN = --$   
 $P.C. STA. = 1185+49.90$   
 $P.T. STA. = 1192+49.91$   
 (MAINTAIN EXISTING)

BEGIN BRIDGE OMISSION  
 STA. 1198+15.00

END BRIDGE OMISSION  
 STA. 1199+20.00

PAVEMENT REMOVAL - BUTT JOINT  
 STA. 1197+75 TO 1198+15  
 (SEE PAVING DETAILS SHEET)

PAVEMENT REMOVAL - BUTT JOINT  
 STA. 1199+20 TO 1199+60  
 (SEE PAVING DETAILS SHEET)



GUTTER AND OUTLET REMOVAL  
 STA. 1214+39 TO 1219+72 LT

CONCRETE GUTTER,  
 TYPE A

GUTTER OUTLET

HMA SURFACE REMOVAL  
 DEPTH TRANSITION  
 STA. 1213+59 TO 1214+39  
 (SEE PAVING DETAILS SHEET)

BEGIN EARTH  
 SHOULDER REGRADING  
 STA. 1214+39 RT

PIPE UNDERDRAIN  
 6" (SPL) & CONC  
 HEADWALL, TYP

PIPE UNDERDRAINS,  
 TYPE 2, 6", TYP

EXIST. CURVE C13  
 PI STA. = 1220+72.68  
 $\Delta = 21^\circ 53' 00''$  (RT)  
 $D = 2^\circ 39' 59''$   
 $R = 2,148.80'$   
 $T = 415.41'$   
 $L = 820.70'$   
 $E = 39.79'$   
 $e = 0.027$  FT/FT  
 $T.R. = --$   
 $S.E. RUN = --$   
 $P.C. STA. = 1216+57.27$   
 $P.T. STA. = 1224+77.97$   
 (MAINTAIN EXISTING)

HMA SURFACE REMOVAL  
 DEPTH TRANSITION  
 STA. 1222+60 TO 1223+40  
 (SEE PAVING DETAILS SHEET)

END EARTH  
 SHOULDER REGRADING  
 STA. 1226+60 RT

EXIST. CURVE C14-1  
 PI STA. = 1231+10.16  
 $\Delta = 6^\circ 17' 44''$  (RT)  
 $D = 3^\circ 51' 57''$   
 $R = 1,482.10'$   
 $T = 81.51'$   
 $L = 162.85'$   
 $E = 2.24'$   
 $e = 0.022$  FT/FT  
 $T.R. = --$   
 $S.E. RUN = --$   
 $P.C. STA. = 1230+28.65$   
 $P.T. STA. = 1231+91.50$   
 (MAINTAIN EXISTING)

EXIST. CURVE C14-2-1  
 PI STA. = 1234+75.93  
 $\Delta = 21^\circ 05' 33''$  (RT)  
 $D = 3^\circ 51' 57''$   
 $R = 1,482.10'$   
 $T = 275.93'$   
 $L = 545.61'$   
 $E = 25.47'$   
 $e = 0.022$  FT/FT  
 $T.R. = --$   
 $S.E. RUN = --$   
 $P.C. STA. = 1232+00.00$   
 $P.T. STA. = 1237+45.61$   
 (MAINTAIN EXISTING)

EXIST. CURVE C14-2-2  
 PI STA. = 1237+71.75  
 $\Delta = 0^\circ 19' 43''$  (RT)  
 $D = 3^\circ 51' 57''$   
 $R = 1,482.10'$   
 $T = 4.25'$   
 $L = 8.50'$   
 $E = 0.01'$   
 $e = 0.022$  FT/FT  
 $T.R. = --$   
 $S.E. RUN = --$   
 $P.C. STA. = 1237+67.50$   
 $P.T. STA. = 1237+76.00$   
 (MAINTAIN EXISTING)

PAVEMENT REMOVAL - BUTT JOINT  
 STA. 1232+04.6 TO 1232+14.0  
 (SEE PAVING DETAILS SHEET)

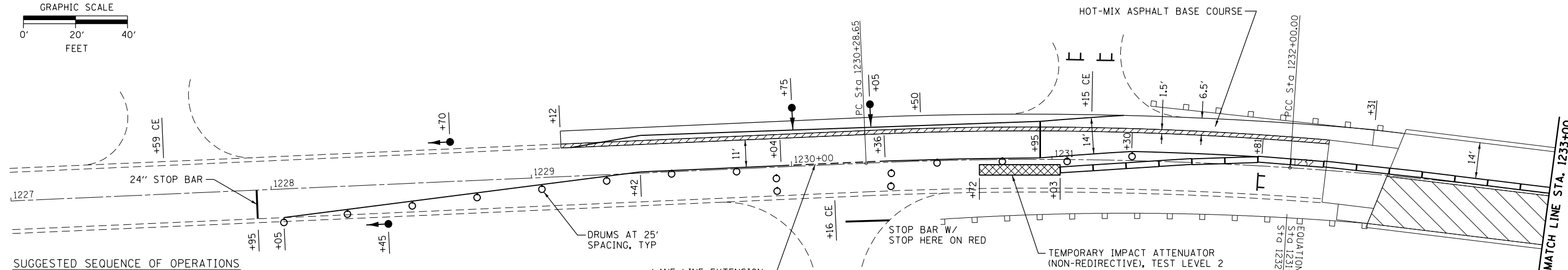
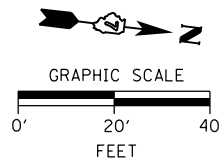
END PAVEMENT  
 RESURFACING  
 STA. 1232+14.0

REPAIR EXISTING BRIDGE  
 SN 024-0005  
 (SEE BRIDGE PLANS)

NOTE: SEE GENERAL NOTE #13 FOR GUIDANCE WHEN DROP-OFFS DURING  
 CONSTRUCTION WILL BE PRESENT ADJACENT TO THE EDGE OF PAVEMENT.

FILE NAME =	USER NAME = steffennk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEET</b>		F.A. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom\dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-sht-plan.dgn		CHECKED -	REVISED -		116	(6,7)RS-3	EDWARDS	41	20		
PLOT SCALE = 200.0000' / in.		DATE -	REVISED -		CONTRACT NO. 74703			ILLINOIS FED. AID PROJECT			
Default		PLOT DATE = 3/29/2019	REVISED -		SCALE: 1"=100'	SHEET 7	OF 7 SHEETS	STA. 1180+00.00	TO STA. 1240+00.00		

REMOVAL OF PAVEMENT MARKINGS CONSTRUCTED AS PART OF STANDARD 701321 SHALL BE INCLUDED IN THE COST OF THIS TRAFFIC CONTROL STANDARD; SEE SPECIAL PROVISIONS. ALL ADVANCE WARNING SIGNAGE FOR THIS STANDARD SHALL BE AT 350'.

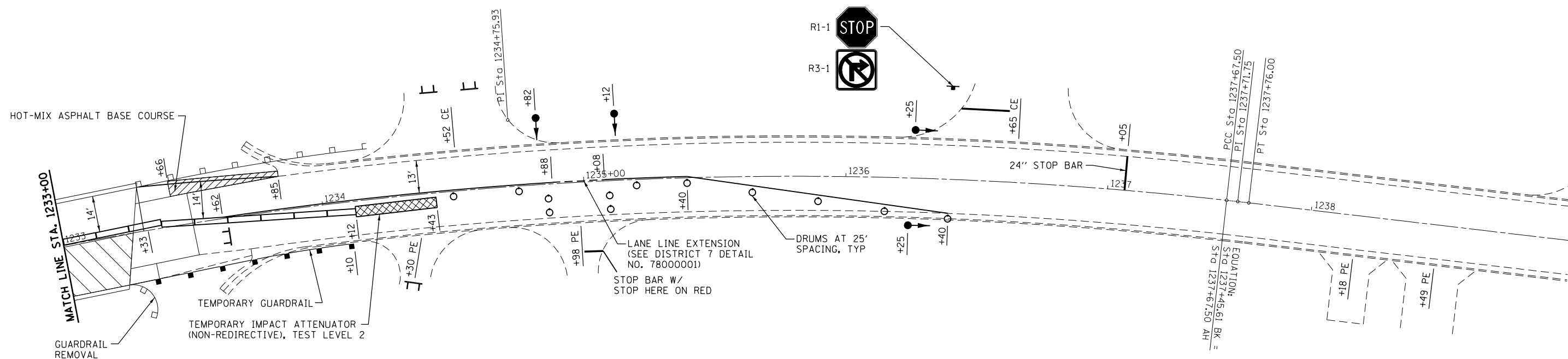


**SUGGESTED SEQUENCE OF OPERATIONS**

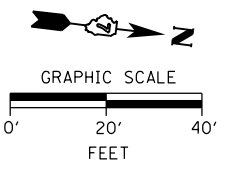
1. PERFORM PAVED SHOULDER REMOVAL AND HOT-MIX ASPHALT BASE COURSE ON SOUTHBOUND LANE USING HIGHWAY STANDARD 701501 FOR TRAFFIC CONTROL.
2. CLOSE ENTRANCES AT STA. 1231+15 LT, STA. 1234+30 RT, AND STA. 1234+52 LT USING TYPE III BARRICADES.
3. ERECT STAGE 1 TRAFFIC CONTROL IN ACCORDANCE WITH HIGHWAY STANDARD 701321 (MODIFIED AS INDICATED ON THIS PLAN). TEMPORARY TRAFFIC SIGNAL PHASES SHALL BE INCLUDED FOR NORTHBOUND AND SOUTHBOUND TRAFFIC AS WELL AS ENTRANCES AT STA. 1230+16 RT AND 1234+98 RT.
4. PERFORM REPAIRS ON STRUCTURE NUMBER 024-0005.
5. CONSTRUCT TEMPORARY GUARDRAIL AT NORTHEAST QUADRANT OF SN 024-0005.
6. REMOVE STAGE 1 TRAFFIC CONTROL.

**LEGEND**

- DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY SIGN
- TEMPORARY CONCRETE BARRIER
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY TRAFFIC SIGNAL
- PAVED SHOULDER REMOVAL
- WORK AREA

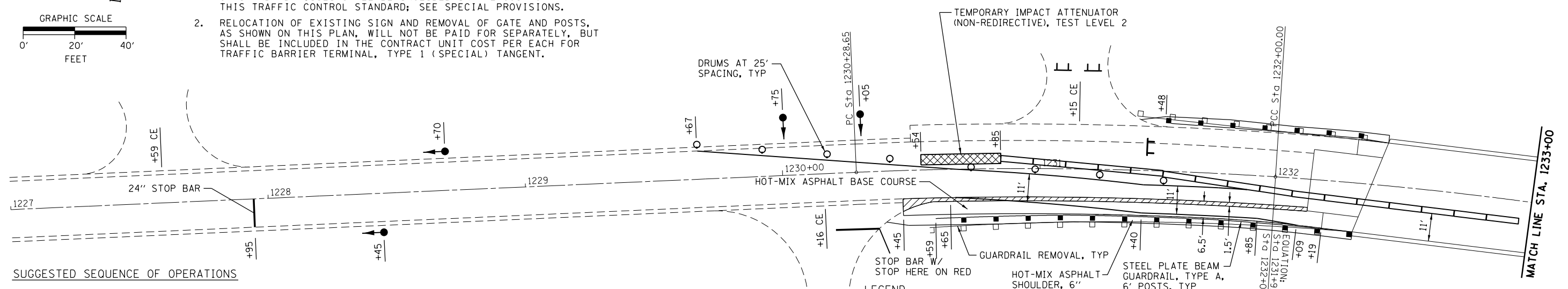


FILE NAME =	USER NAME = steffennk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGE 1 TRAFFIC CONTROL SN 024-0005</b>			F.A. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-sh1-staging.dgn		CHECKED -	REVISED -		116	(6,7)RS-3	EDWARDS	41	21			
Default	PLOT SCALE = 40.0000' / in.	DATE -	REVISED -		CONTRACT NO. 74703			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 3/29/2019		REVISED -		SCALE: 1"=20'	SHEET 1	OF 2 SHEETS	STA. 1227+00	TO STA. 1239+00			



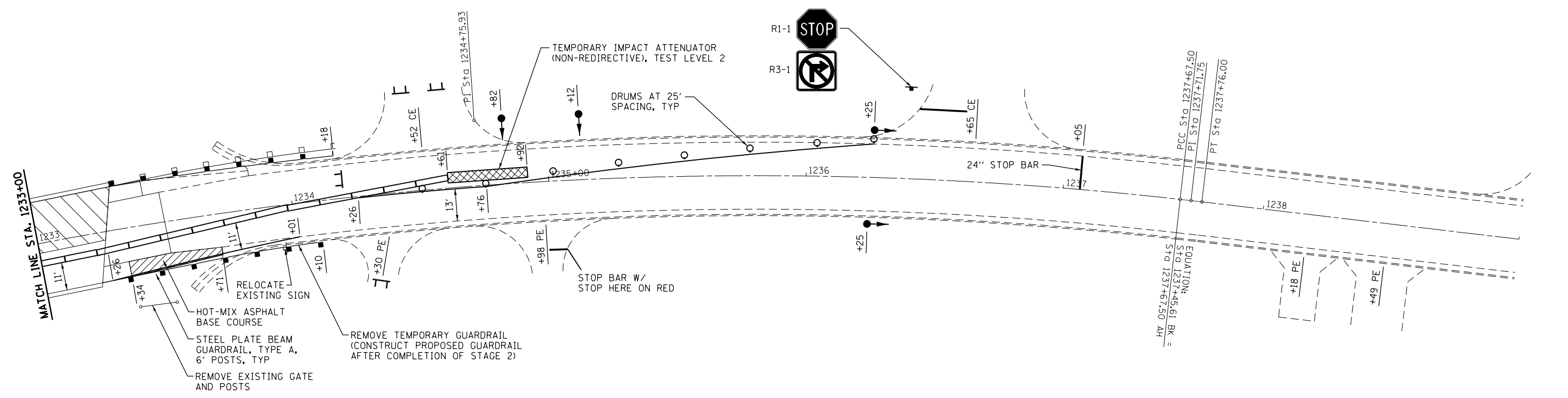
**NOTES**

1. REMOVAL OF PAVEMENT MARKINGS CONSTRUCTED AS PART OF STANDARD 701321 SHALL BE INCLUDED IN THE COST OF THIS TRAFFIC CONTROL STANDARD; SEE SPECIAL PROVISIONS.
2. RELOCATION OF EXISTING SIGN AND REMOVAL OF GATE AND POSTS, AS SHOWN ON THIS PLAN, WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT COST PER EACH FOR TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT.



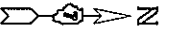
**SUGGESTED SEQUENCE OF OPERATIONS**

1. AT THE END OF STAGE 1 PERFORM PAVED SHOULDER REMOVAL AND HOT-MIX ASPHALT BASE COURSE ON NORTHBOUND LANE USING HIGHWAY STANDARD 701501 FOR TRAFFIC CONTROL.
2. CLOSE ENTRANCES AT STA. 1231+15 LT, STA. 1234+30 RT, AND STA. 1234+52 LT USING TYPE III BARRICADES.
3. ERECT STAGE 2 TRAFFIC CONTROL IN ACCORDANCE WITH HIGHWAY STANDARD 701321 (MODIFIED AS INDICATED ON THIS PLAN). TEMPORARY TRAFFIC SIGNAL PHASES SHALL BE INCLUDED FOR NORTHBOUND AND SOUTHBOUND TRAFFIC AS WELL AS ENTRANCES AT STA. 1230+16 RT AND 1234+98 RT.
4. PERFORM REPAIRS ON STRUCTURE NUMBER 024-0005.
5. REMOVE STAGE 2 TRAFFIC CONTROL.
6. REMOVE EXISTING/TEMPORARY GUARDRAIL AND CONSTRUCT GUARDRAIL USING HIGHWAY STANDARD 701501 FOR TRAFFIC CONTROL.



FILE NAME =	USER NAME = steffennk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGE 2 TRAFFIC CONTROL SN 024-0005</b>			F.A.P. RT. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-sh2-staging.dwg		CHECKED -	REVISED -		116	(6,7)RS-3	EDWARDS	41	22			
Default	PLOT SCALE = 40.0000' / in.	DATE -	REVISED -		CONTRACT NO. 74703			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 3/29/2019		REVISED -		SCALE: 1"=20'	SHEET 2 OF 2 SHEETS	STA. 1227+00 TO STA. 1239+00					

The existing single span steel multi-beam structure was constructed in 1934 as SBI Route 1 section 7VB-NRS. The structure was reconstructed in 1985 at station 1232+78.28. SN. 024-0005 carries FA Route 116 over the Norfolk Southern R.R. The proposed project consists of replacing the south expansion joint with a new strip assembly, structural repair of concrete on both abutments and minor repairs to concrete approaches.



**GENERAL NOTES**

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement Bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system. Cost included with CONCRETE REMOVAL.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

Prior to pouring the new concrete deck, all heavy and loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in CONCRETE REMOVAL.

The contractor shall take all measures necessary to ensure that no debris from concrete removal or structural repair of concrete is allowed to reach the railroad track or ballast. Cost to be included with the appropriate removal pay item.

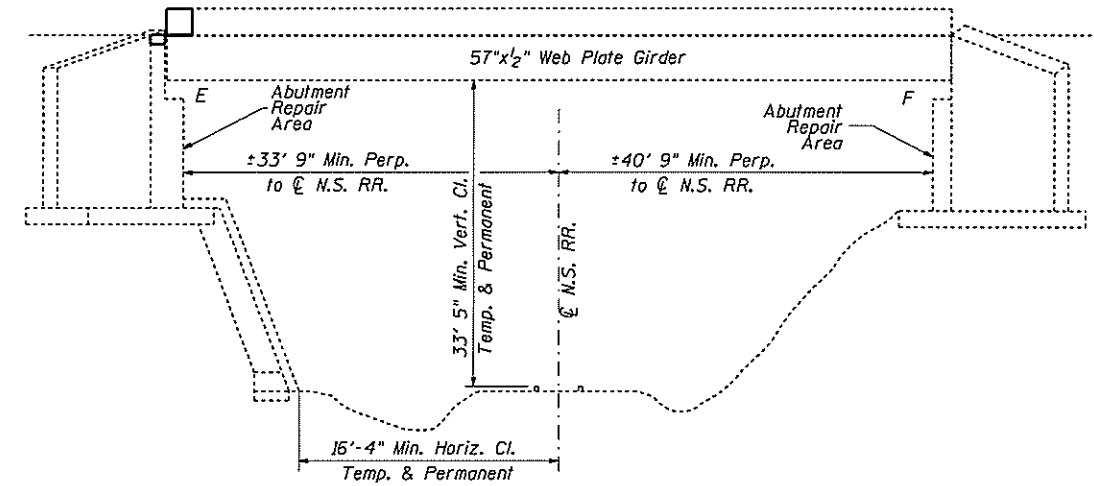
Protective shield shall be provided for the full, out-to-out width of the bridge deck.

Protective Coat to be applied to areas of new concrete only.

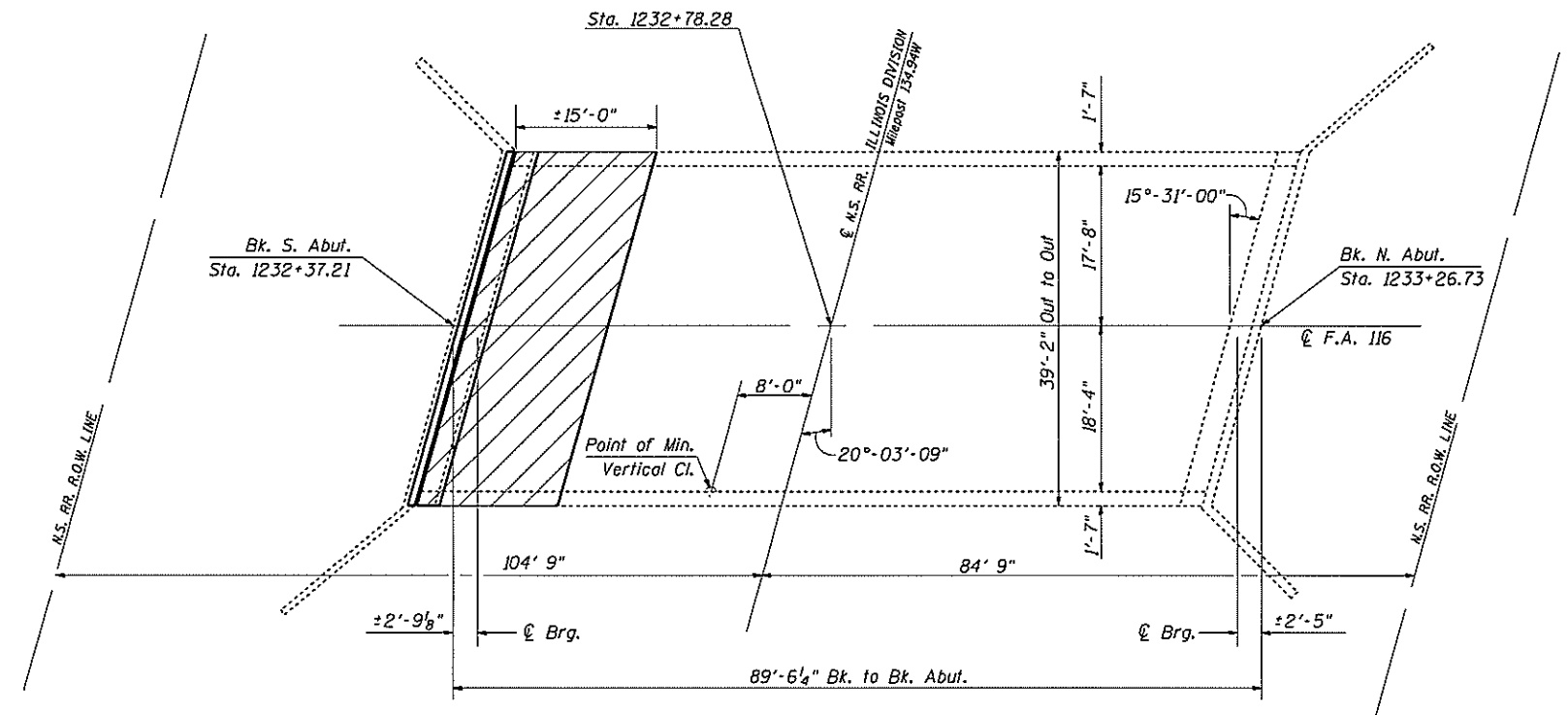
The new deck surface adjacent to the joint shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost to be included with Concrete Superstructure.

The work shall not change the quantity and/or character of flow in the Railway's ditches and/or drainage structures.

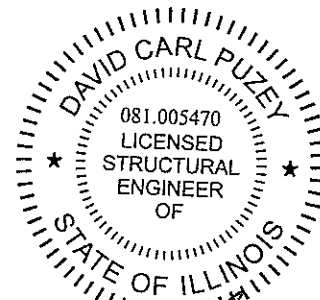
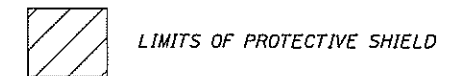
Contact Information for Railway: Mr. Fred Schesny, Engineer Public Improvements, Bridges and Structures  
Norfolk Southern Corporation  
1200 Peachtree Street NE  
Atlanta, Georgia 30309  
Email: frederick.schesny@nscorp.com  
Phone: (404)529-1256 (Office)



**ELEVATION**



**EXISTING DESIGN STRESSES**  
 $f_c = 3,500$  psi.  
 $f_y = 50,000$  psi. (M223 Grade 50)  
 $f_y = 60,000$  psi. (Reinf.)  
 $f_y = 36,000$  psi.



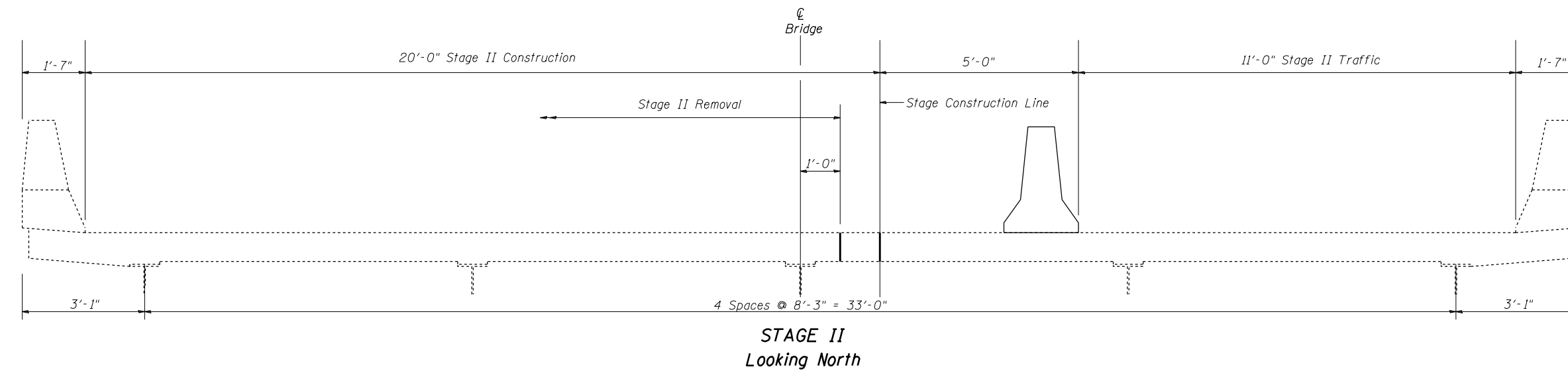
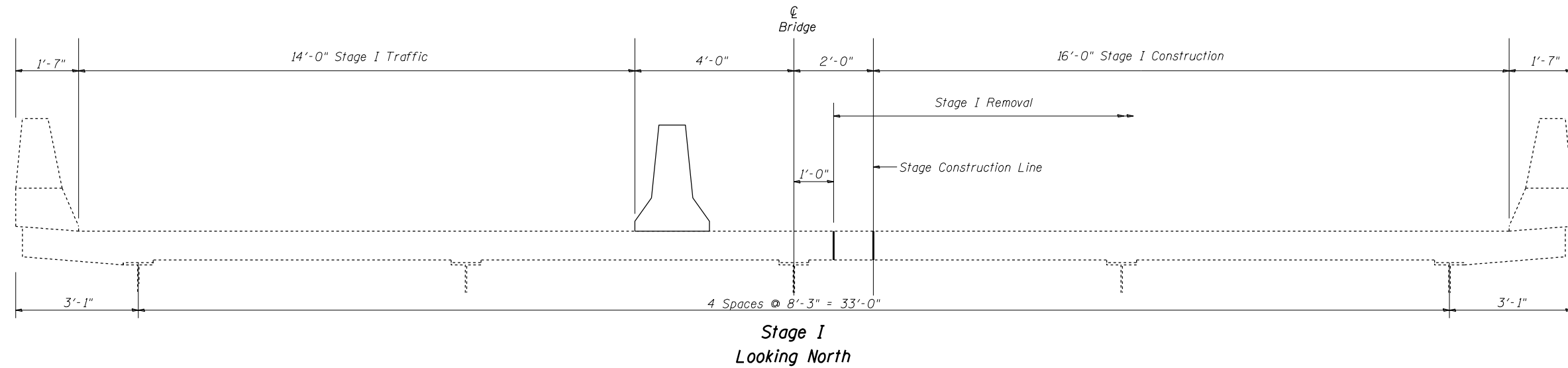
*David Carl Puzey* 5/9/19  
Expires 11/30/20

FILE NAME =	USER NAME = Default	DESIGNED - S. Kassel	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL PLAN AND ELEVATION</b>		F.A.P. RTE. 116	SECTION (6.7) RS-3	COUNTY Edwards	TOTAL SHEETS 41	SHEET NO. 23	
PLLOT SCALE = 40.0000 "/td> <td>PLLOT DATE = 3/12/2019</td> <td>CHECKED - D. Macklin</td> <td>REVISED -</td> <td>SN. 024-0005</td> <td>SCALE:</td> <td>SHEET 1 OF 9 SHEETS</td> <td>STA. _____ TO STA. _____</td> <td colspan="4" style="text-align: center;">CONTRACT NO. 74703</td>	PLLOT DATE = 3/12/2019	CHECKED - D. Macklin	REVISED -		SN. 024-0005	SCALE:	SHEET 1 OF 9 SHEETS	STA. _____ TO STA. _____	CONTRACT NO. 74703			
DATE - 12/20/16	DATE - 12/20/16	REVISI	REVISI		ILLINOIS FED. AID PROJECT							

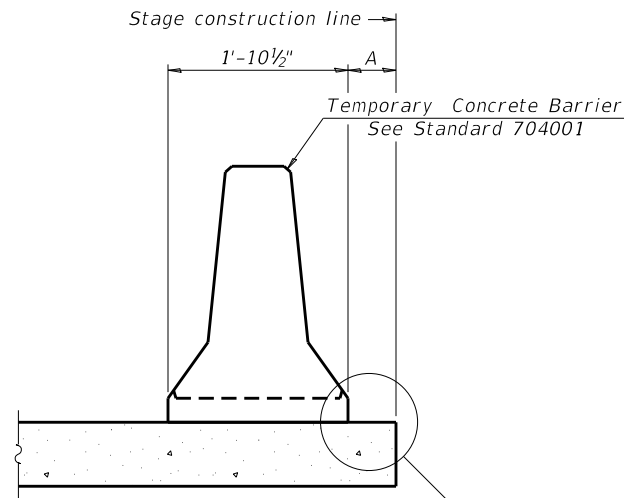
**TOTAL BILL OF MATERIALS**

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	6.1
Concrete Superstructure	Cu. Yd.	6.1
Reinforcement Bars, Epoxy Coated	Pound	960
Bar Splacers	Each	17
Prefomed Joint Strip Seal	Foot	43
Protective Coat *	Sq Yd	23
Protective Shield	Sq Yd	68
Approach Slab Repair (Partial Depth)	Sq Yd	5
Structural Repair of Concrete <5"	Sq Ft	84

\* New concrete areas only.

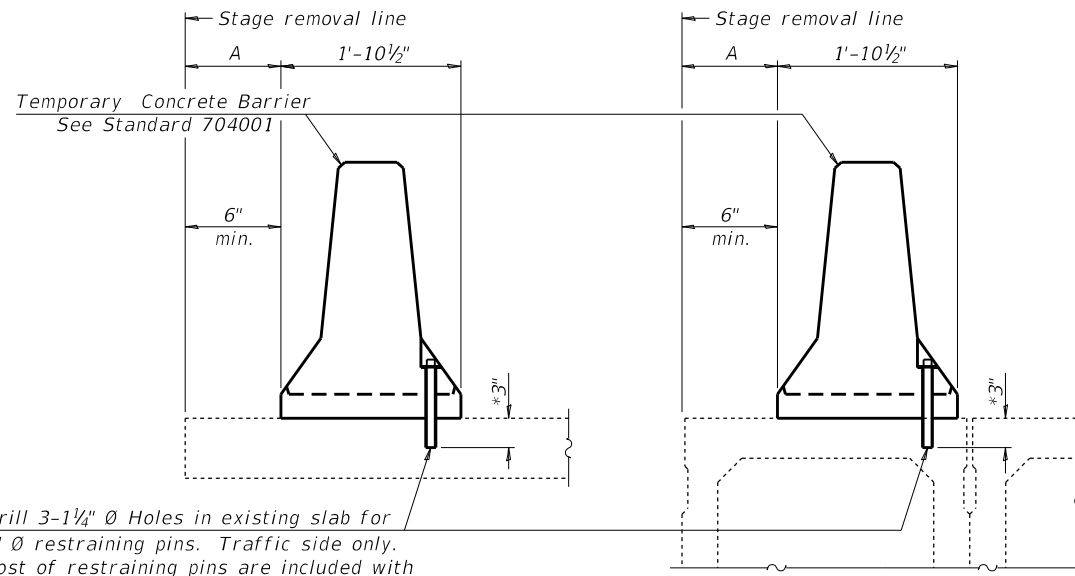






When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



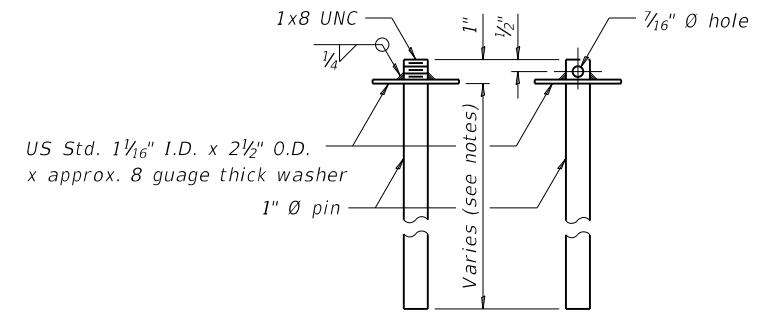
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

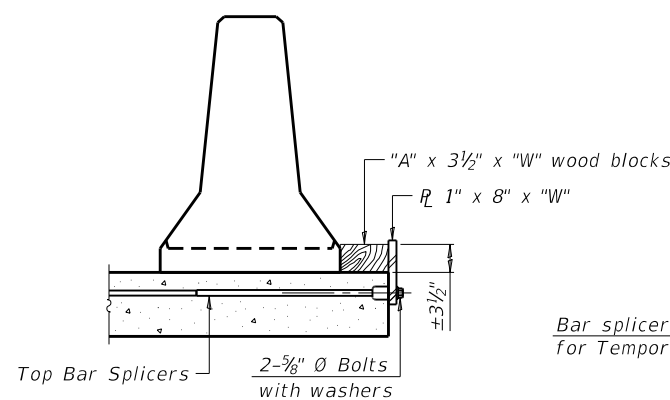
\* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

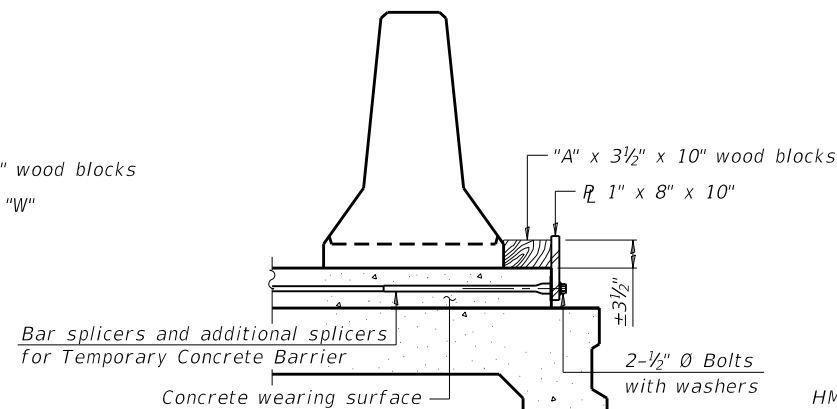
SECTIONS THRU SLAB OR DECK BEAM



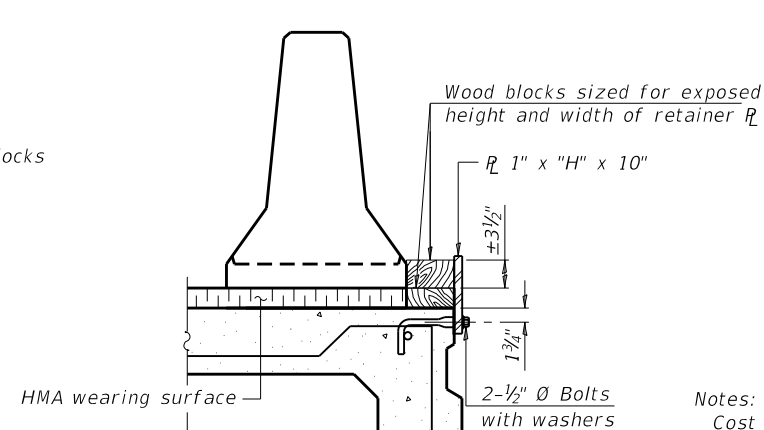
RESTRAINING PIN



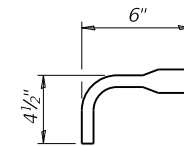
DETAIL I



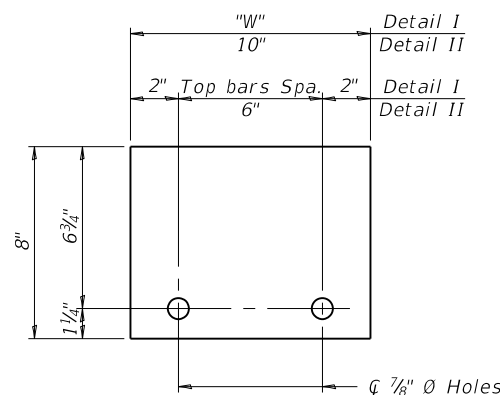
DETAIL II



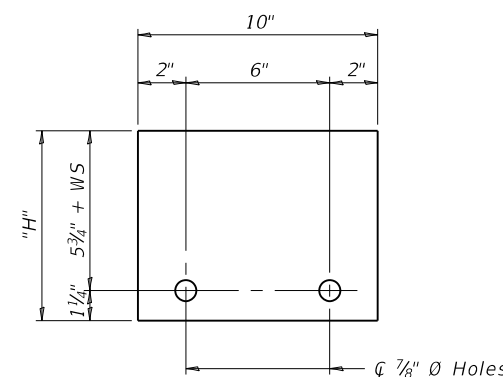
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"  
(Detail I and II)



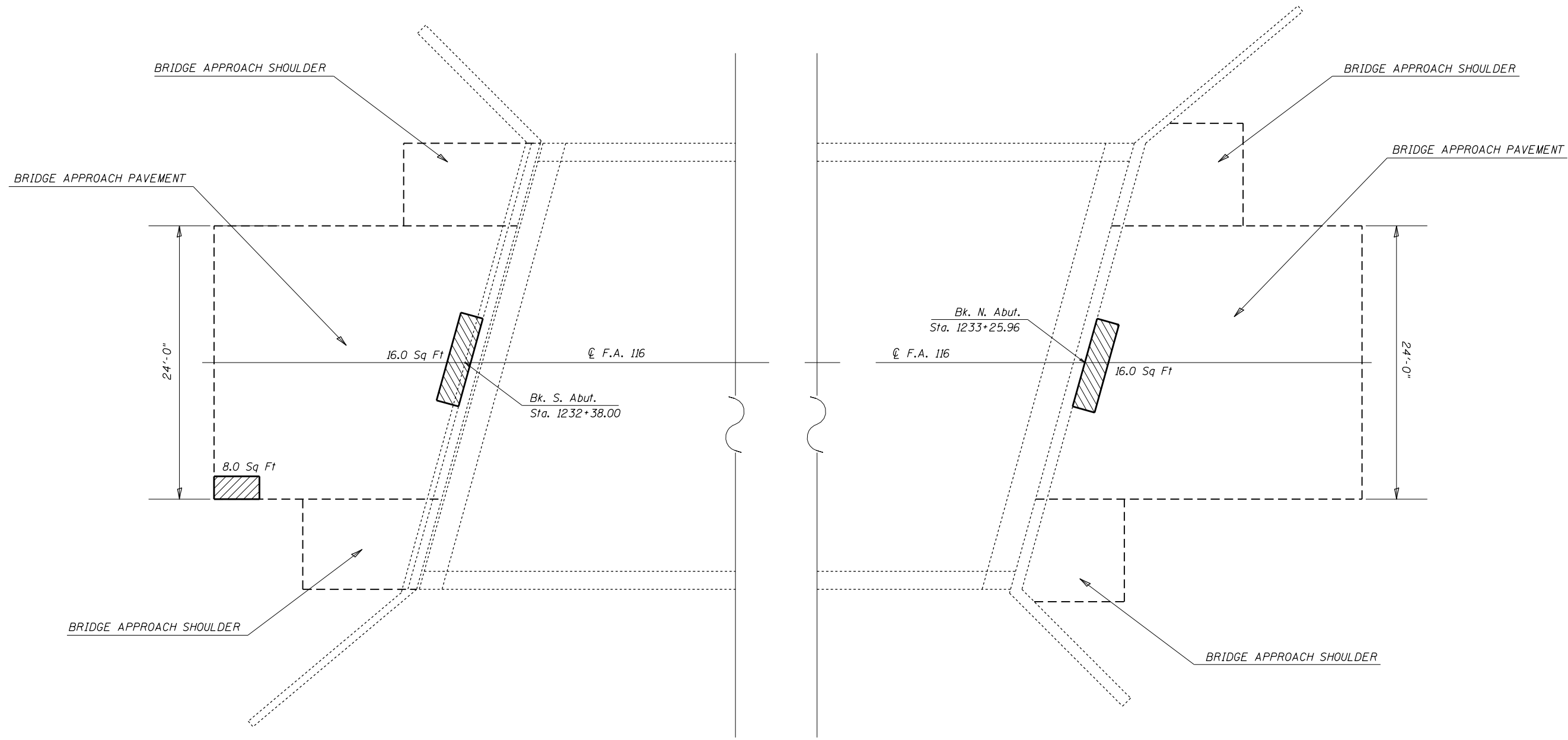
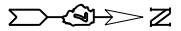
STEEL RETAINER 1" x "H" x 10"  
(Detail III)

Notes:  
 Cost of retainer assembly is included with Temporary Concrete Barrier.  
 A retainer assembly shall be located at the approximate  $\bar{c}$  of each temporary concrete barrier.  
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.  
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.  
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.  
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27 8-11-2017

FILE NAME =	USER NAME = Default	DESIGNED - S. Kassel	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION STRUCTURE NO. 024-0005</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	Default	CHECKED - D. Macklin	REVISED -			116	(6,7) RS-3	EDWARDS	41	25
		DATE - 12/22/16	REVISED -			CONTRACT NO. 74703				



**SOUTH APPROACH PAVEMENT**

**NORTH APPROACH PAVEMENT**

**BILL OF MATERIAL**

Item	Unit	Total
Approach Slab Repair (Partial Depth)	SQ YD	5

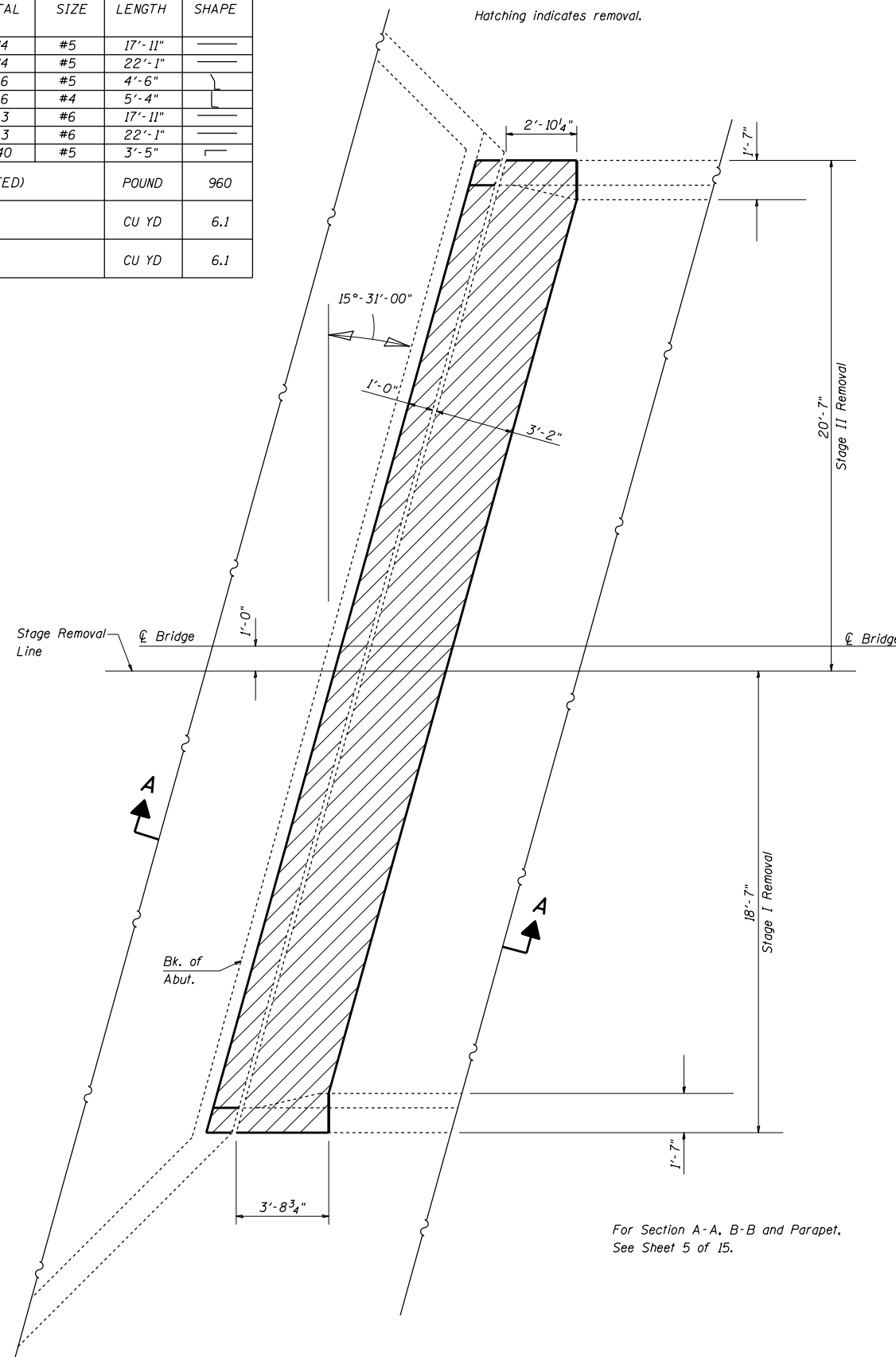
Note: For Information Only. Quantities are estimated. Locations are Approximated. Actual quantities and locations to be determined in the field by the Engineer. (Not to Scale)



APPROACH SLAB REPAIR (PARTIAL DEPTH)

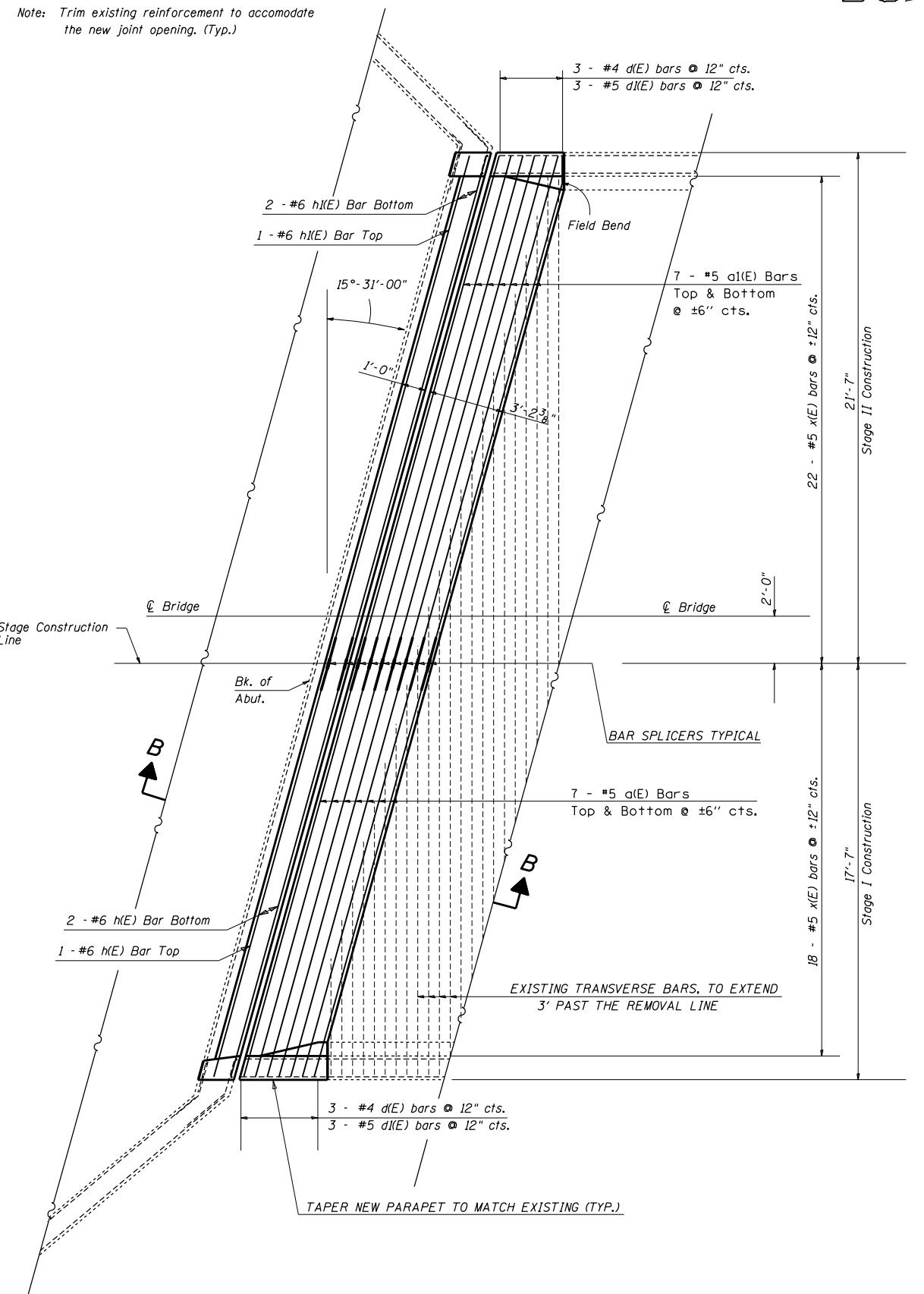
**BILL OF MATERIAL**

BAR	NUMBER OF BARS		TOTAL	SIZE	LENGTH	SHAPE	
	STAGE I	STAGE II					
a (E)	14	0	14	#5	17'-11"	▬▬▬	
a(E)	0	14	14	#5	22'-1"	▬▬▬	
d (E)	3	3	6	#5	4'-6"	┘┐	
d(E)	3	3	6	#4	5'-4"	┘┐	
h (E)	3	0	3	#6	17'-11"	▬▬▬	
h(E)	0	3	3	#6	22'-1"	▬▬▬	
x (E)	18	22	40	#5	3'-5"	▬▬▬	
REINFORCEMENT BARS (EPOXY COATED)						POUND	960
CONCRETE REMOVAL						CU YD	6.1
CONCRETE SUPERSTRUCTURE						CU YD	6.1



**EXISTING PARTIAL PLAN**  
(South Abutment)

For Section A-A, B-B and Parapet, See Sheet 5 of 15.



**PROPOSED PARTIAL PLAN**  
(South Abutment)

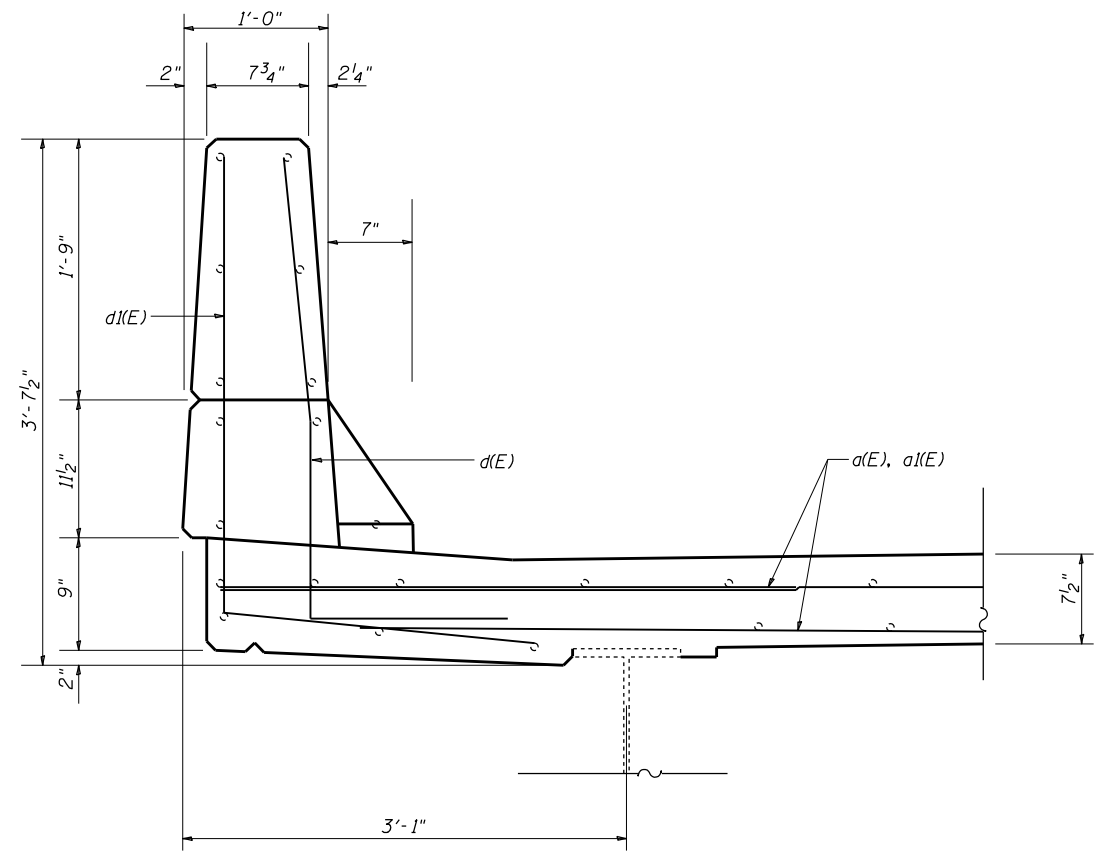
FILE NAME =	USER NAME = Default	DESIGNED - S. Kassel	REVISED -
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Default	PLOT DATE = 3/12/2019	DATE = 12/20/16	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

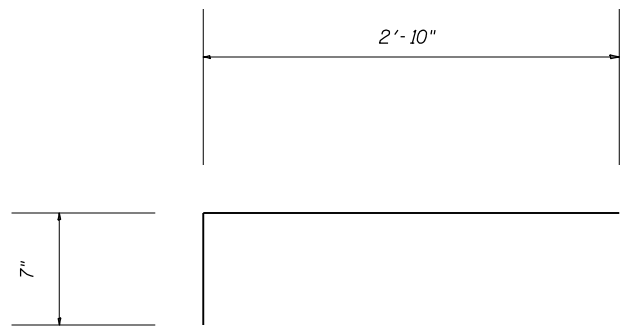
**EXPANSION JOINT REPLACEMENT DETAILS**  
**SN. 024-0005**

SCALE: \_\_\_\_\_ SHEET 5 OF 9 SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

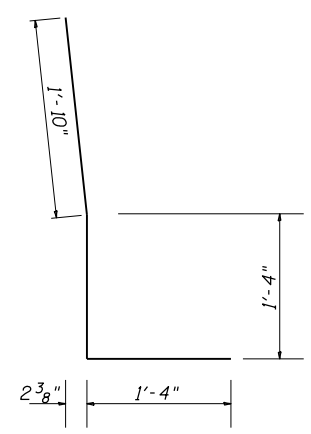
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7) RS-3	Edwards	41	27
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				



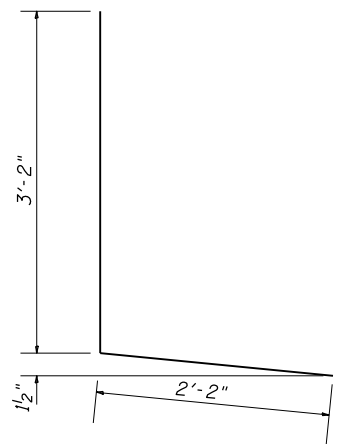
**SECTION THRU PARAPET**



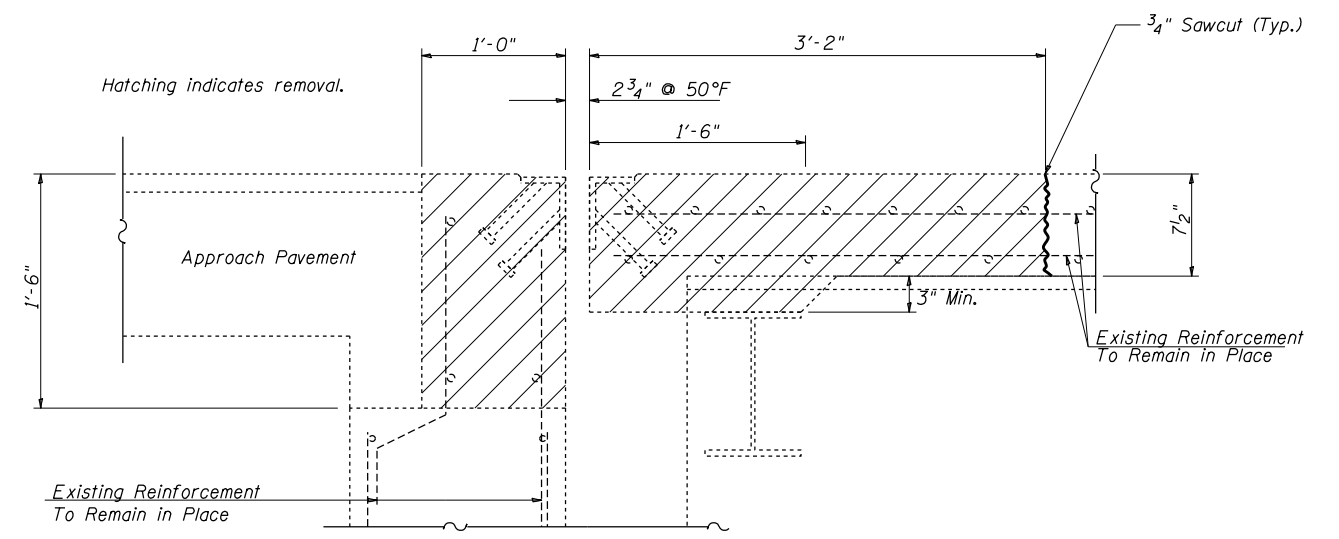
**BAR X(E)**



**BAR d(E)**

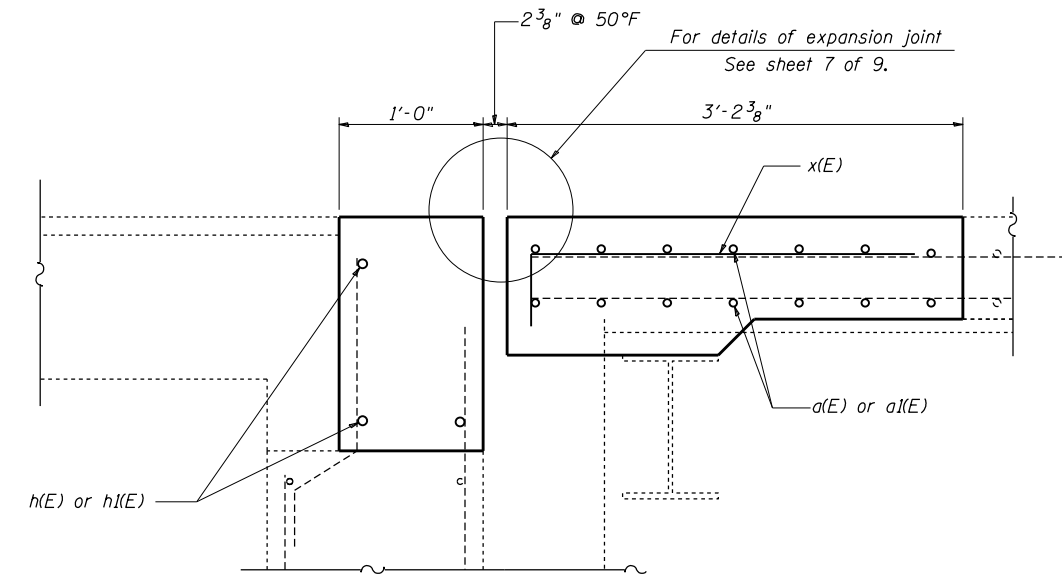


**BAR d(E)**



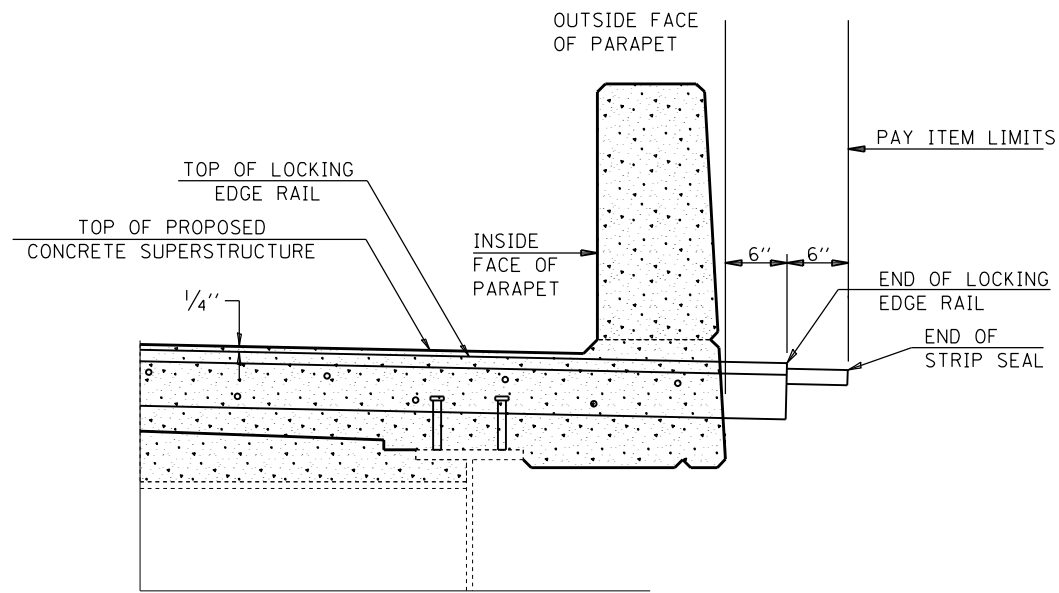
**SECTION A-A**  
(Dimensions at Rt. L's to end of deck)

- Existing Reinforcement
- Proposed Reinforcement

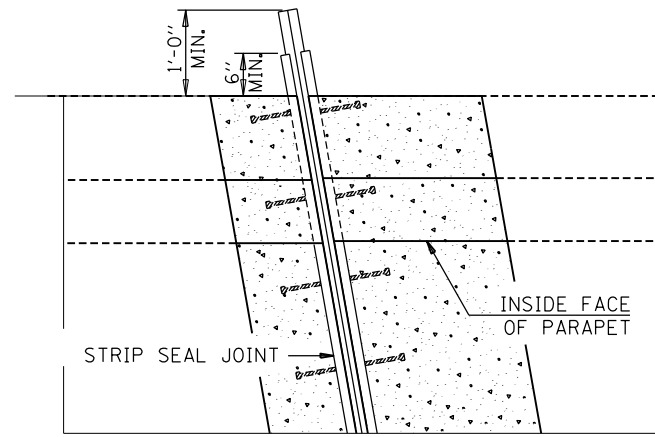


**SECTION B-B**  
(Dimensions at Rt. L's to end of deck)

FILE NAME =	USER NAME = Default	DESIGNED S. Kassel	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXPANSION JOINT REPLACEMENT DETAILS SN. 024-0005</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\planroom.dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWN\Structures\Kassel\Structure-024-0005.dwg	PLLOT SCALE = 40.0000' / in.	CHECKED D. Macklin	REVISED -			116	(6,7) RS-3	Edwards	41	28	
PLLOT DATE = 3/12/2019	DATE 01/11/16	REVISED -	SCALE: _____			SHEET NO. 6 OF 9 SHEETS	STA. _____ TO STA. _____	ILLINOIS FED. AID PROJECT			
CONTRACT NO. 74703											



SECTION THRU PARAPET



PLAN VIEW AT PARAPET

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

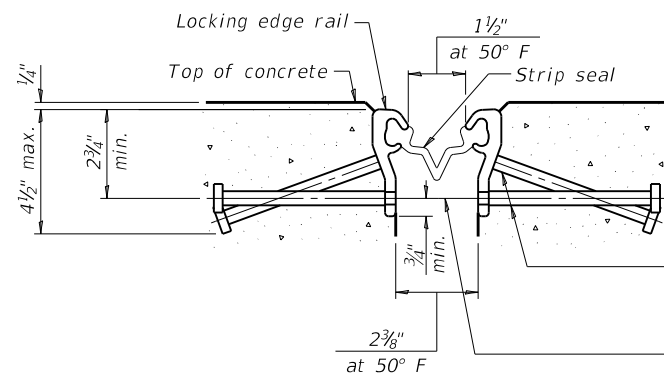
The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

34" F-shape barrier shown, 42" F-shape similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

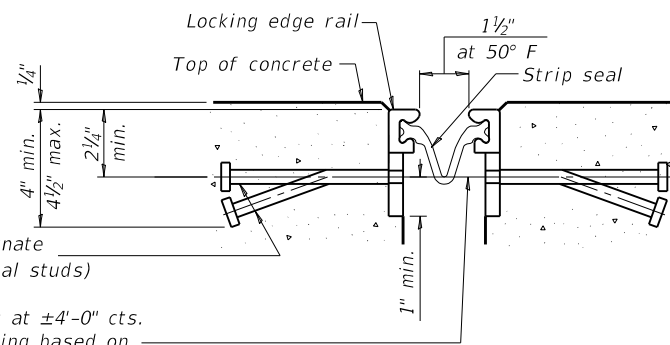


SHOWING ROLLED RAIL JOINT

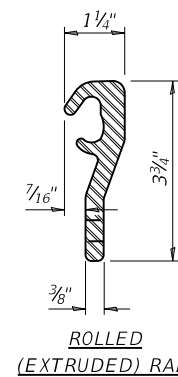
\* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)  
 3/8" Ø threaded rods in 7/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

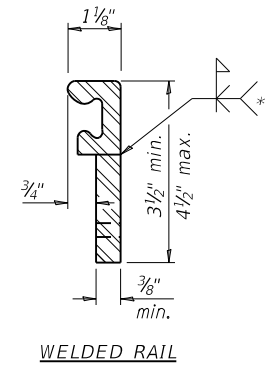
\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



SHOWING WELDED RAIL JOINT



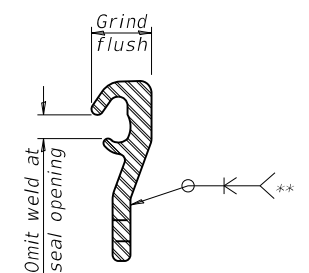
ROLLED (EXTRUDED) RAIL



WELDED RAIL

LOCKING EDGE RAILS

\*\* Back gouge not required if complete joint penetration is verified by mock-up.

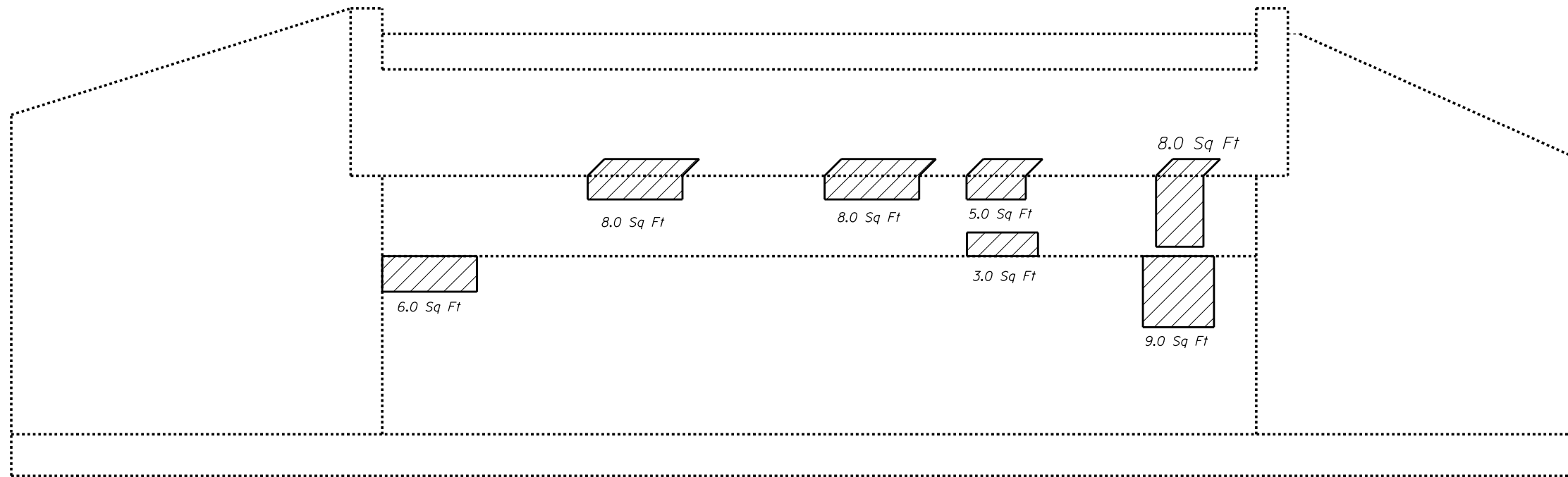


LOCKING EDGE RAIL SPLICE

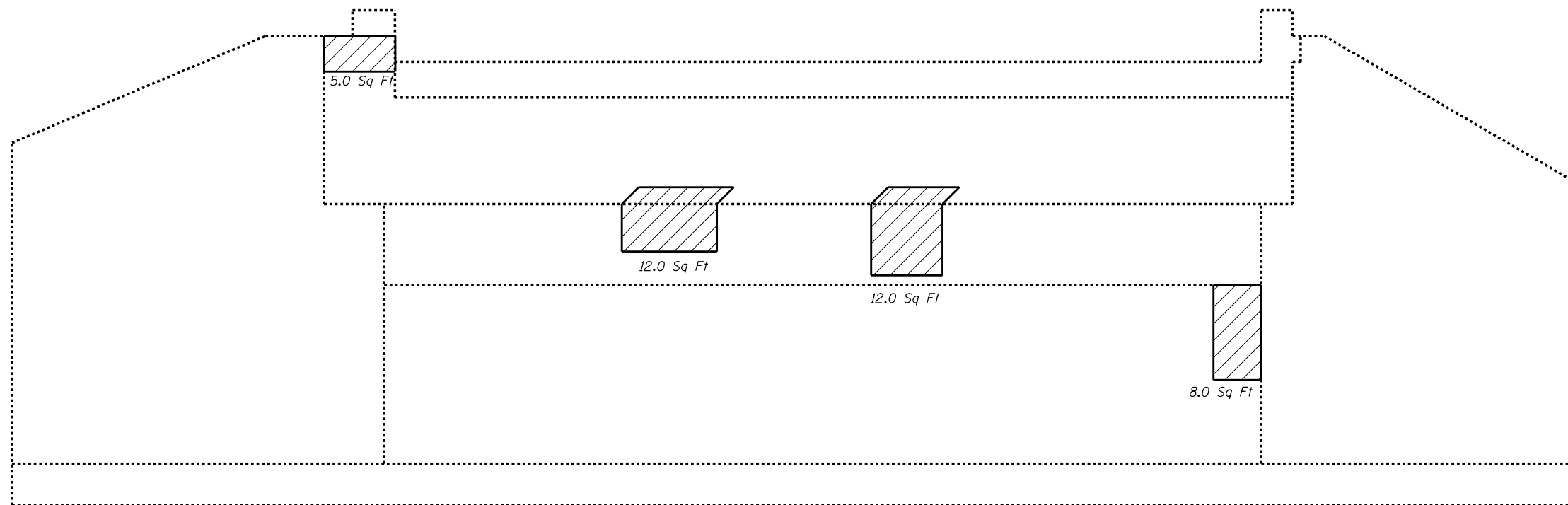
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	43



SN. 024-0005 - SOUTH ABUTMENT



SN. 024-0005 - NORTH ABUTMENT

**BILL OF MATERIAL**

Item	Unit	Total
Structural Repair of Concrete <5"	SQ FT	84

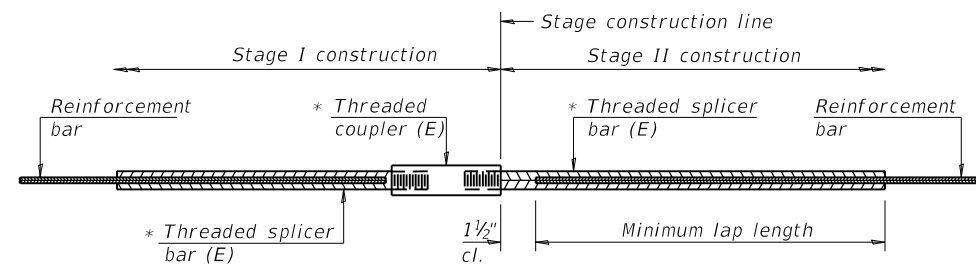
Note: Quantities are estimated. Locations are approximated. Actual quantities and locations to be determined in the field by the Engineer. (Not to Scale)



Structural Repair of Concrete (Depth < 5")

FILE NAME =	USER NAME = Default	DESIGNED - S. Kassel	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRUCTURAL REPAIR OF CONCRETE &lt;5"&gt; SN. 024-0005</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\Structures\024-0005\structure-024-0005.dwg	PLOT SCALE = 40.0000' / in.	CHECKED - D. Macklin	REVISED -			116	(6,7) RS-3	Edwards	41	30
Default	PLOT DATE = 3/12/2019	DATE - 01/05/2017	REVISED -			CONTRACT NO. 74703		ILLINOIS FED. AID PROJECT		

SCALE: \_\_\_\_\_ SHEET 8 OF 9 SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

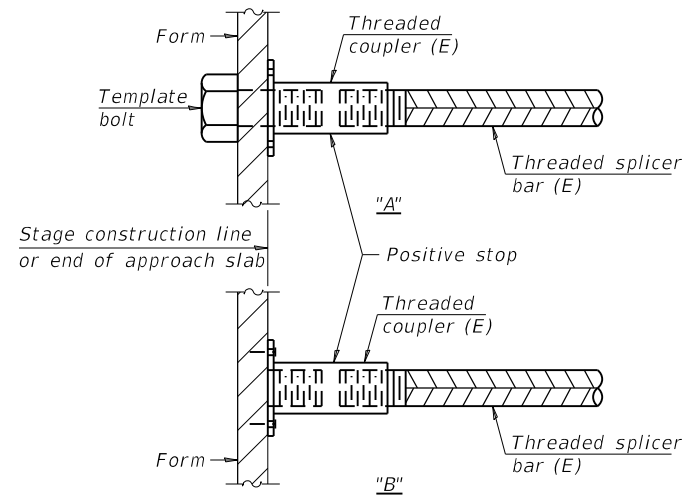


**STANDARD BAR SPLICER ASSEMBLY**

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck	#5	14	3'-6"
Hatch Block	#6	3	4'-0"

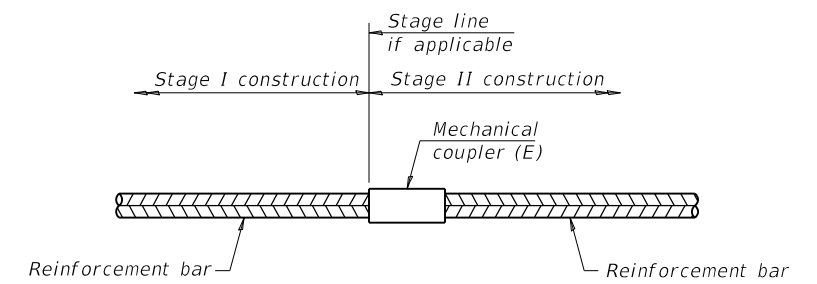


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

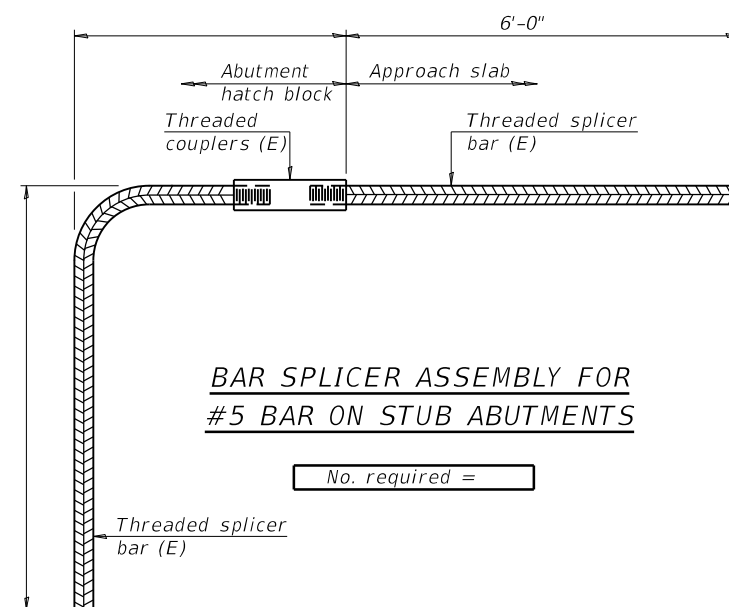
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required



**BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS**

No. required =

**NOTES**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

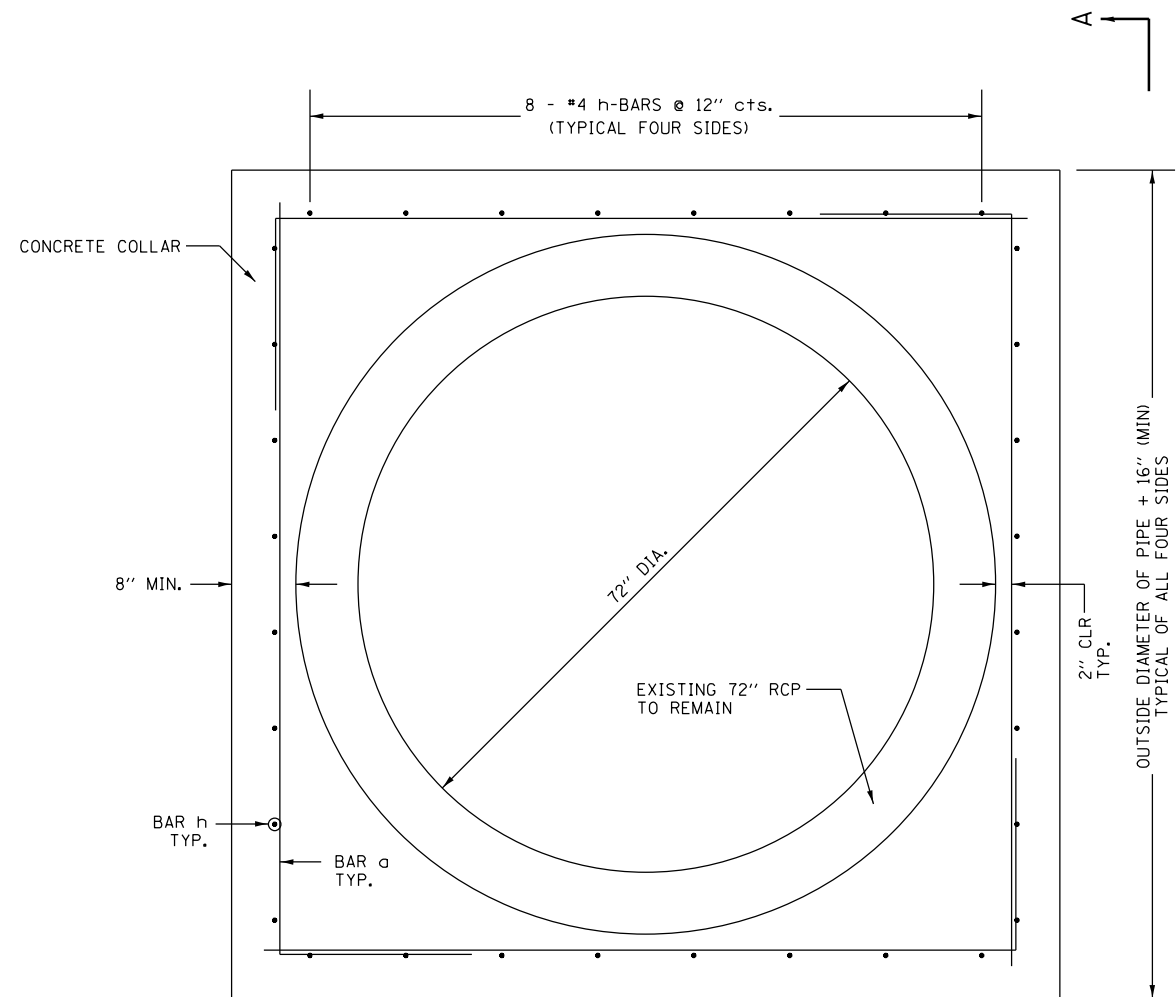
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 2-17-2017

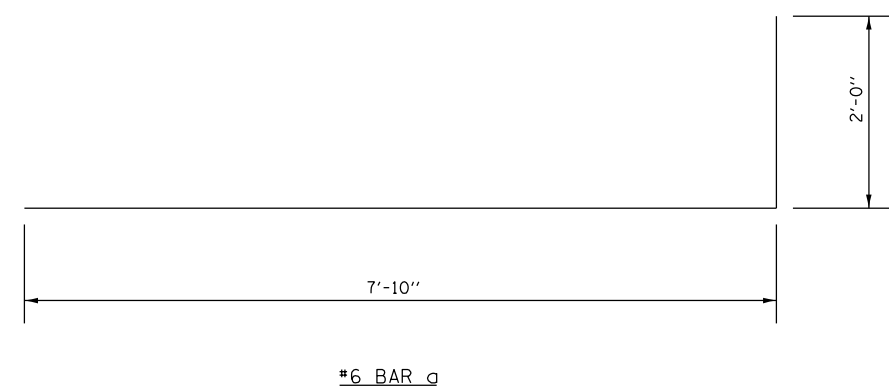
FILE NAME =	USER NAME = Default	DESIGNED - S. Kassel	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS STRUCTURE NO. 024-0005</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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SCALE: SHEET 9 OF 9 SHEETS STA. TO STA.

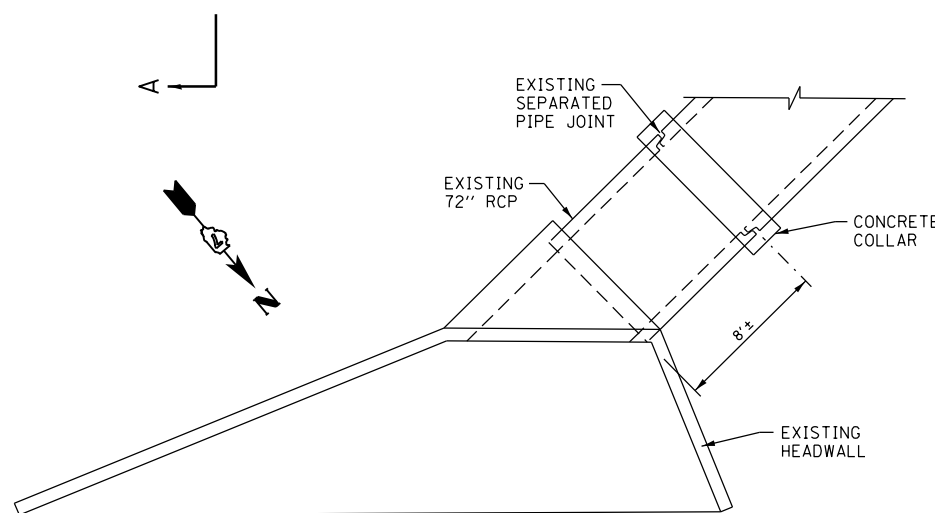
ILLINOIS FED. AID PROJECT



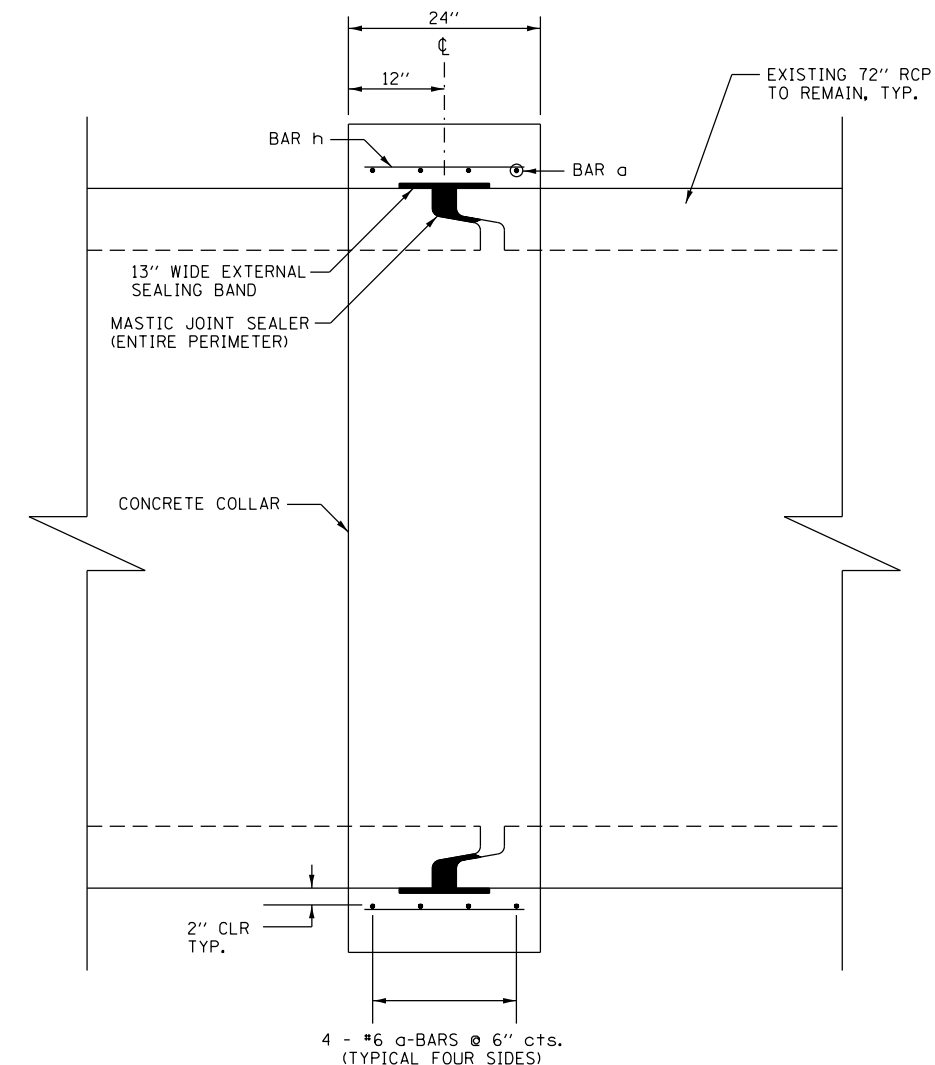
ELEVATION VIEW



BAR	QTY	SIZE	LENGTH	SHAPE
a	16	#6	9' - 10"	—
h	32	#4	1' - 8"	—



LOCATION SKETCH



SECTION A-A

NOTES:

- GAP IN EXISTING JOINT SHALL BE FILLED WITH MASTIC JOINT SEALER AROUND THE ENTIRE PIPE PERIMETER.
- EXTERNAL SEALING BAND SHALL BE INSTALLED PRIOR TO PLACEMENT OF REINFORCEMENT BAR AND CONCRETE.
- INSTALLATION OF MASTIC AND EXTERNAL SEALING BAND SHALL BE COMPLETED TO THE SATISFACTION OF THE ENGINEER PRIOR TO REINFORCEMENT AND CONCRETE FABRICATION.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL MASTIC AND EXTERNAL SEALING BAND AS INDICATED AND AS DIRECTED BY THE ENGINEER. THIS WORK, AS WELL AS NECESSARY EARTH EXCAVATION AND EMBANKMENT, WILL NOT BE PAID FOR SEPERATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE PER CUBIC YARD FOR CONCRETE COLLAR.

BILL OF MATERIALS

ITEM	UNIT	QUANTITY
REINFORCEMENT BARS	POUND	271.9
CONCRETE COLLAR	CU YD	2.4

FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\KWP\1024\20024-7011.dwg		CHECKED -	REVISED -
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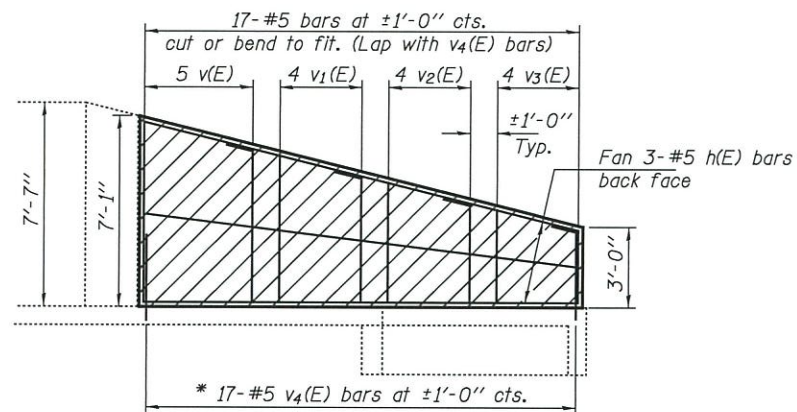
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CULVERT REPAIR DETAILS SN 024-7011  
FAP 116 (IL 130)

SCALE: N/A SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	32
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				

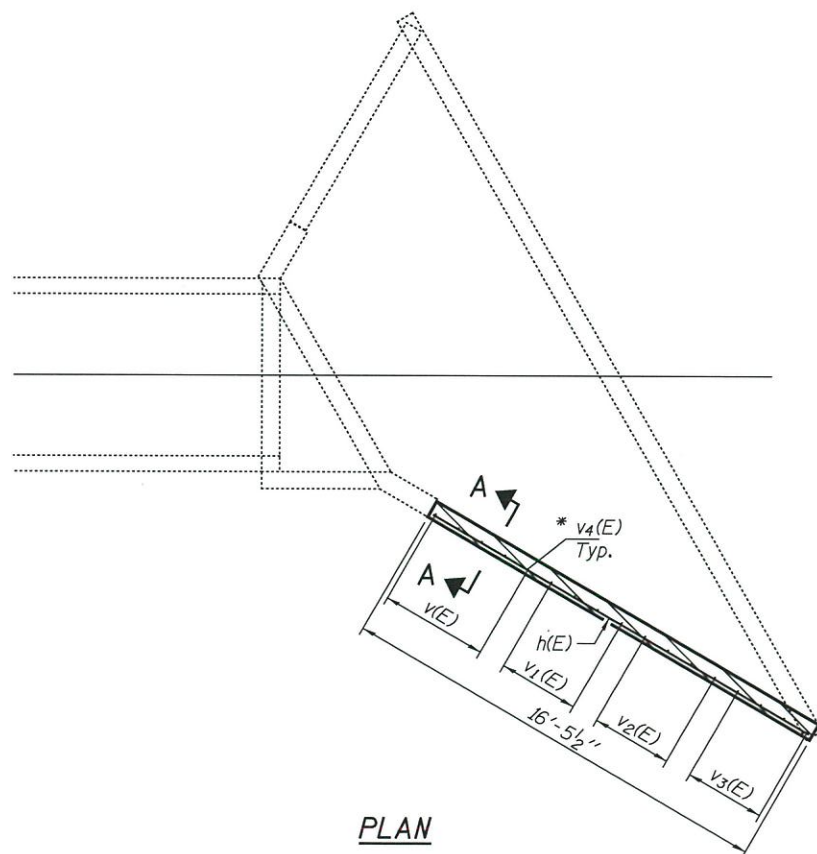




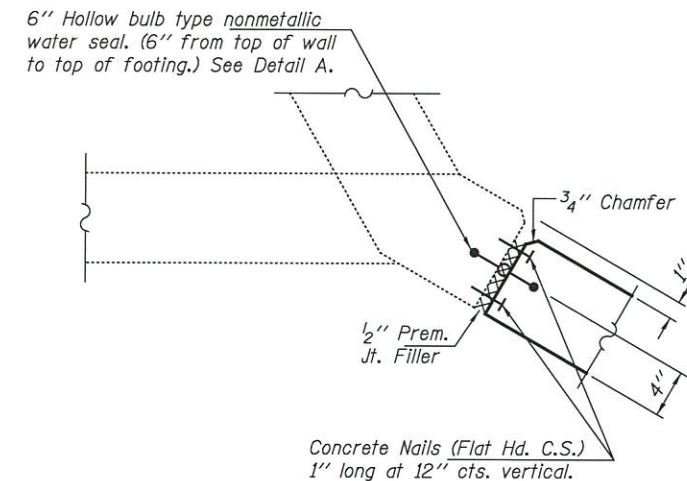
**ELEVATION**

\* Epoxy grout v<sub>4</sub>(E) bars in 6" deep holes according to Article 584 of the Standard Specifications.

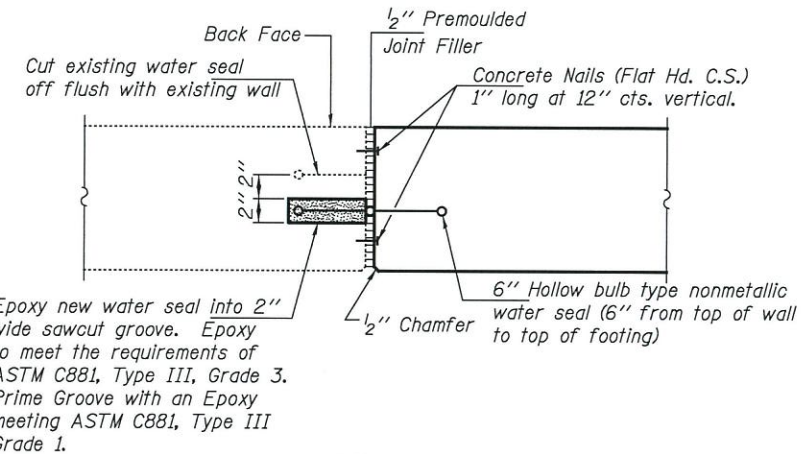
Hatched areas indicate Concrete Removal and Concrete Box Culverts



**PLAN**

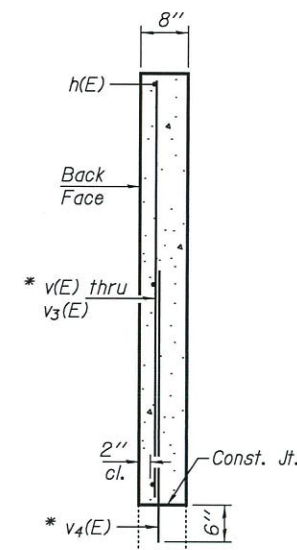


**CORNER DETAIL**



**DETAIL A**

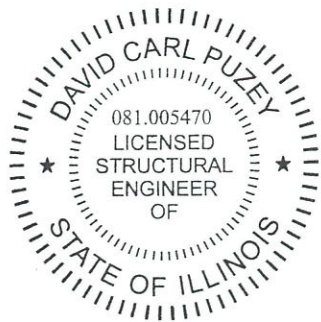
Cost of water seal materials and installation is included in the cost of Concrete Box Culverts.



**SECTION A-A**

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h(E)	3	#5	16'-1"	—
v(E)	5	#5	6'-8"	—
v <sub>1</sub> (E)	4	#5	5'-5"	—
v <sub>2</sub> (E)	4	#5	4'-5"	—
v <sub>3</sub> (E)	4	#5	3'-5"	—
v <sub>4</sub> (E)	17	#5	3'-2"	—
Concrete Removal				Cu. Yd. 2.0
Concrete Box Culverts				Cu. Yd. 2.0
Reinforcement Bars, Epoxy Coated				Pound 200



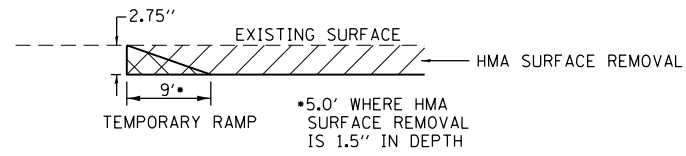
EXPIRES 11-30-2020

DESIGNED	Victor H. Decic	EXAMINED	Tim A. [Signature]	DATE	APRIL 26, 2019
CHECKED	[Signature]	PASSED	[Signature]	REVISIONS	
DRAWN	ballva				
CHECKED	[Signature]				

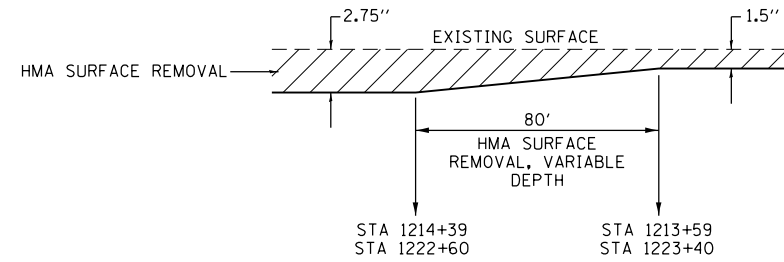
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CULVERT REPAIR DETAILS  
FAP 116 (IL 130)  
SN 024-7013  
SHEET NO. 1 OF 1 SHEETS

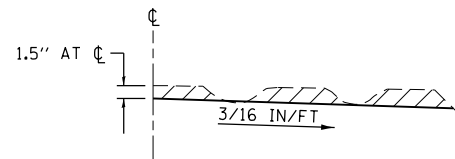
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	33
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				



TEMPORARY RAMP DETAIL



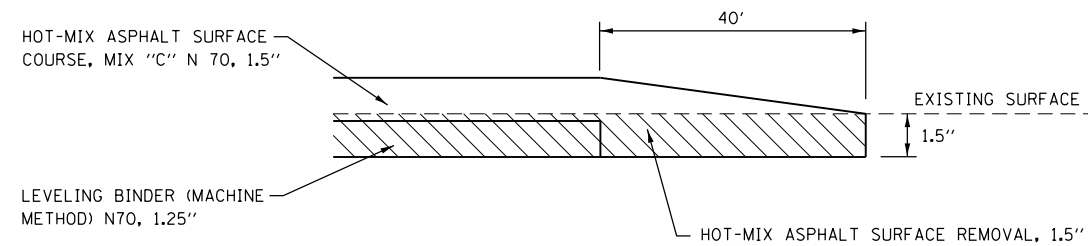
MILLING DEPTH TRANSITION DETAIL



**NOTES:**

1. MILLING SHALL BE PERFORMED IN A MANNER TO ATTAIN A 3/16 IN/FT SLOPE IN CROWN SECTIONS.
2. EXISTING SUPERELEVATIONS AND SUPERELEVATION TRANSITIONS SHALL BE MAINTAINED UNLESS OTHERWISE INDICATED ON THE PLANS.
3. MILLING TO THE BOTTOM OF WHEEL RUTS SHALL NOT BE NECESSARY UNLESS REQUIRED TO ATTAIN SLOPE OR THE DESIGN DEPTH AT CENTERLINE.
4. THE AVERAGE DEPTH OF MILLING IS ESTIMATED TO BE 1.5 INCHES BUT MAY VARY IN ISOLATED LOCATIONS.

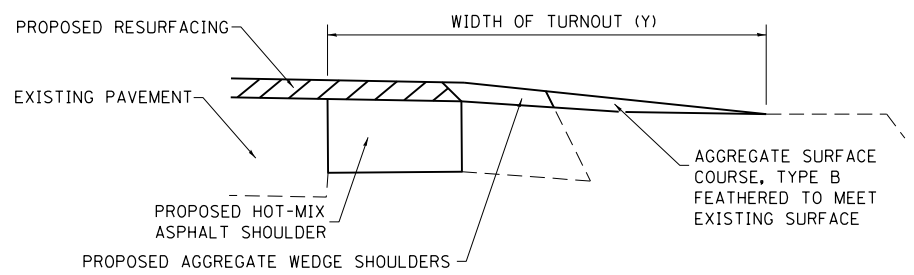
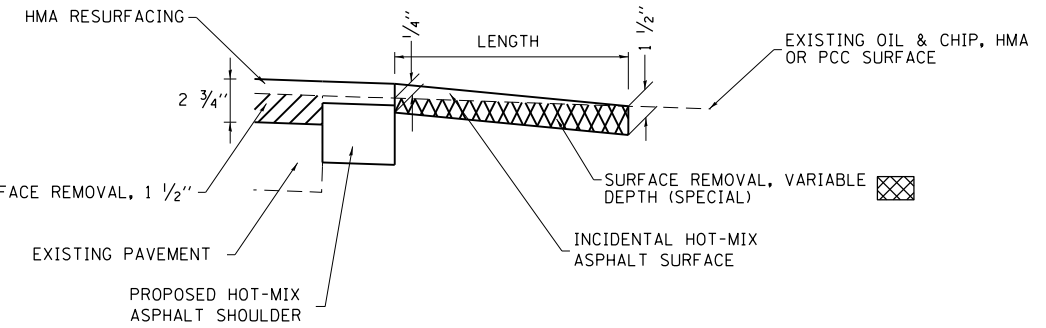
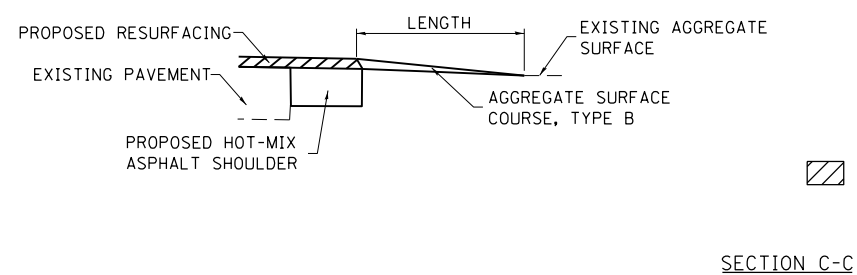
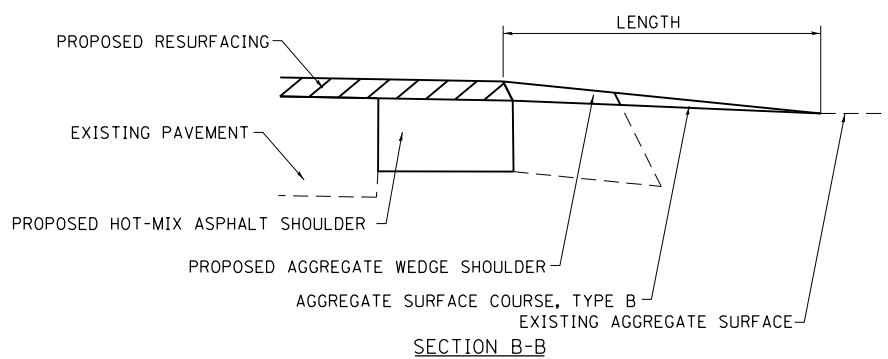
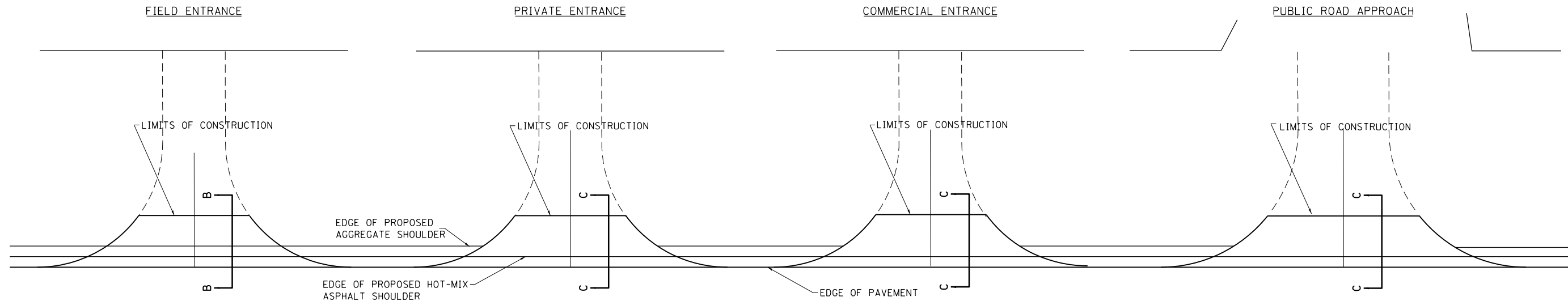
HMA SURFACE REMOVAL, 1.5" DETAIL



**BUTT JOINT DETAIL**

- STA 0+00 TO STA 0+40
- STA 1089+10 TO STA 1089+50
- STA 1092+95 TO STA 1093+35
- STA 1197+75 TO STA 1198+15
- STA 1199+20 TO STA 1199+60
- STA 1232+05 TO STA 1232+45

FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVING DETAILS</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-sht-details.dgn		REVISIONS	REVISIONS					116	(6,7)RS-3	EDWARDS	41	34
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 74703			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 4/1/2019	DATE -	REVISED -		SCALE: N/A	SHEET 1	OF 1 SHEETS	STA.	TO STA.			



TYPICAL SECTION AT MAILBOX TURNOUT  
NOTE: SEE STANDARD 406201 FOR MAILBOX TURNOUT DETAILS

**NOTES**  
LENGTH = 10' UNLESS OTHERWISE NOTED ON PLANS  
THE THICKNESS OF THE HOT-MIX ASPHALT SHOULDERS THROUGH AGGREGATE COMMERCIAL ENTRANCES AND PUBLIC ROADS SHALL BE 10". THE COST OF THE EXTRA THICKNESS SHALL BE INCLUDED WITH THE HOT-MIX ASPHALT SHOULDERS PAY ITEM.  
THE COST OF THE BITUMINOUS MATERIALS (TACK COAT) FOR ENTRANCES AND PUBLIC ROAD APPROACHES SHALL BE INCLUDED IN THE PAY ITEM INCIDENTAL HOT-MIX ASPHALT SURFACING.

SIDE	STATION	TYPE	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)		INCIDENTAL HOT-MIX ASPHALT SURFACING	AGGREGATE SURFACE COURSE, TYPE B
			SO	YD		
RT	1+20	CE	--	--	--	3.2
RT	1+60	CE	--	--	--	3.4
LT	2+10	FE	--	--	--	2.3
LT	5+65	CE	45.0	3.2	--	--
RT	5+65	CE	41.7	2.9	--	--
RT	7+10	CE	41.1	2.9	--	--
LT	7+45	PE	27.8	1.9	--	--
RT	7+50	CE	34.4	2.4	--	--
LT	7+95	PE	28.9	2.0	--	--
LT	9+75	PE	--	--	3.5	--
RT	10+05	PE	53.9	3.8	--	--
RT	12+70	PE	28.9	2.0	--	--
LT	13+30	PE	46.7	3.3	--	--
RT	14+40	PE	33.3	2.3	--	--
LT	16+75	PRA	86.1	6.0	--	--
RT	16+75	PRA	76.1	5.3	--	--
LT	16+85	PE	36.1	2.5	--	--
LT	30+70	FE	--	--	1.7	--
RT	30+70	FE	--	--	1.7	--
LT	41+07	PRA	--	--	3.6	--
RT	41+07	PRA	53.3	4.5	--	--

SIDE	STATION	TYPE	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)		INCIDENTAL HOT-MIX ASPHALT SURFACING	AGGREGATE SURFACE COURSE, TYPE B
			SO	YD		
RT	56+15	FE	--	--	--	1.7
LT	56+25	CE	--	--	--	3.2
RT	58+00	FE	--	--	--	1.7
LT	66+33	FE	--	--	--	1.7
RT	67+23	FE	--	--	--	1.7
LT	73+60	PE	55.6	4.7	--	--
RT	85+30	FE	--	--	1.7	--
LT	93+75	PRA	92.2	7.7	--	--
RT	95+75	FE	--	--	2.4	--
LT	97+49	PRA	--	--	3.6	--
LT	927+80	PE	--	--	2.4	--
RT	928+00	FE	--	--	2.0	--
RT	928+00	MBT	--	--	3.6	--
RT	938+20	FE	--	--	2.0	--
RT	941+45	PE	31.7	2.7	--	--
RT	941+55	MBT	--	--	3.6	--
RT	943+50	FE	--	--	2.0	--
LT	943+60	FE	--	--	2.0	--
RT	954+85	FE	--	--	2.0	--
RT	956+25	MBT	--	--	3.6	--
LT	956+35	PE	--	--	2.2	--

SIDE	STATION	TYPE	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)		INCIDENTAL HOT-MIX ASPHALT SURFACING	AGGREGATE SURFACE COURSE, TYPE B
			SO	YD		
RT	963+60	PE	--	--	--	2.0
RT	963+70	MBT	--	--	--	3.6
LT	963+70	FE	--	--	--	1.7
RT	968+68	PE	--	--	--	2.8
RT	968+80	MBT	--	--	--	3.6
LT	968+88	PRA	81.1	6.8	--	--
LT	972+00	FE	--	--	--	1.7
RT	973+70	FE	--	--	--	2.2
RT	986+25	FE	--	--	--	2.0
LT	990+25	FE	--	--	--	2.8
RT	990+50	FE	--	--	--	1.7
RT	994+95	MBT	--	--	--	3.6
LT	994+95	PE	--	--	--	2.4
RT	995+96	PRA	71.1	6	--	--
LT	997+53	PE	--	--	--	2.4
RT	997+53	MBT	--	--	--	3.6
RT	1003+50	FE	--	--	--	1.7
LT	1003+70	FE	--	--	--	2.4
RT	1005+30	MBT	--	--	--	3.6
RT	1005+40	PE	--	--	--	2.0
LT	1017+80	FE	--	--	--	1.7

SIDE	STATION	TYPE	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)		INCIDENTAL HOT-MIX ASPHALT SURFACING	AGGREGATE SURFACE COURSE, TYPE B
			SO	YD		
LT	1018+50	FE	--	--	--	2.0
LT	1020+30	FE	--	--	--	1.7
RT	1023+35	PE	--	--	--	2.0
RT	1025+10	MBT	--	--	--	3.6
LT	1025+10	PE	--	--	--	2.2
RT	1032+50	PE	--	--	--	2.4
RT	1032+70	MBT	--	--	--	3.6
RT	1037+00	FE	--	--	--	1.7
RT	1040+75	MBT	--	--	--	3.6
LT	1041+20	PE	--	--	--	2.4
RT	1050+85	PE	--	--	--	2.0
LT	1052+50	PRA	64.4	5.4	--	--
LT	1053+60	CE	82.8	7	--	--
LT	1060+80	CE	81.1	6.8	--	--
RT	1067+70	FE	--	--	--	2.4
RT	1072+30	PE	--	--	--	2.0
LT	1077+50	PE/CE	--	--	--	3.6
RT	1077+60	PRA	54.4	4.6	--	--
LT	1080+00	PE	--	--	--	2.0
RT	1080+10	MBT	--	--	--	3.6
LT	1091+80	PE	--	--	--	--

FE=FIELD ENTRANCE PRA - PUBLIC ROAD APPROACH  
PE=PRIVATE ENTRANCE MBT - MAILBOX TURNOUT  
CE=COMMERCIAL ENTRANCE

MODEL: Default FILE: 11/15/19 10:41:00 AM PROJECT: 7/2019/ROADWAY/DOCUMENTS/1001 OFFICE: DISTRICT 7 PROJECT: 7/2019/ROADWAY/DOCUMENTS/1001 SHEET: 7/2019-SP-ENR-05.DWG

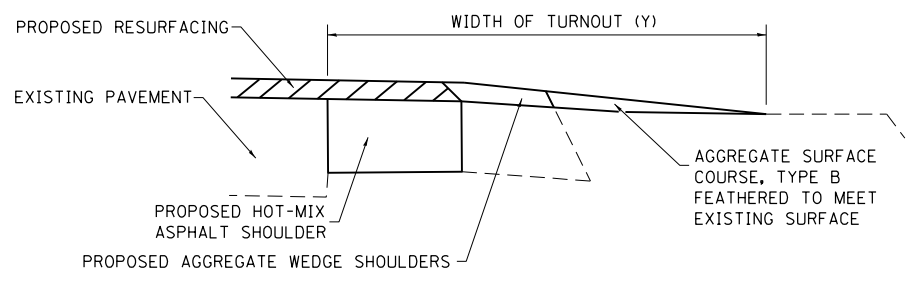
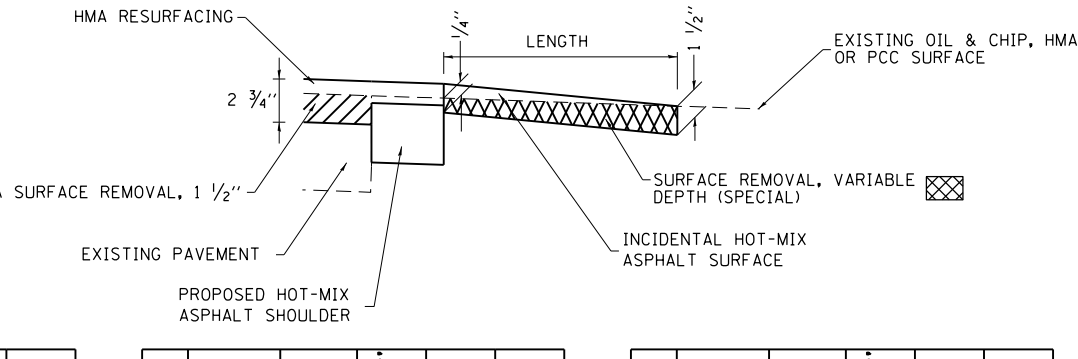
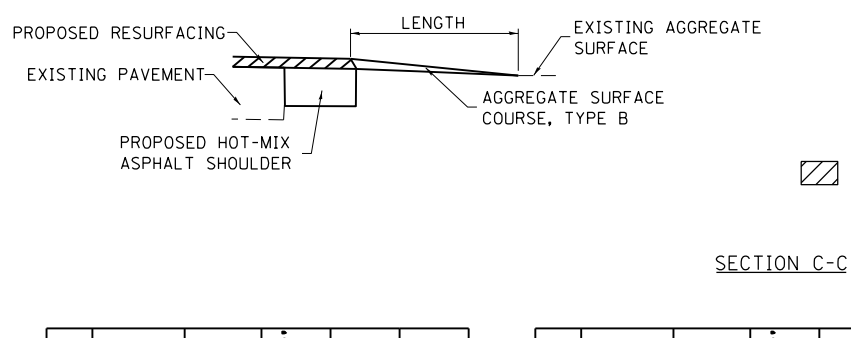
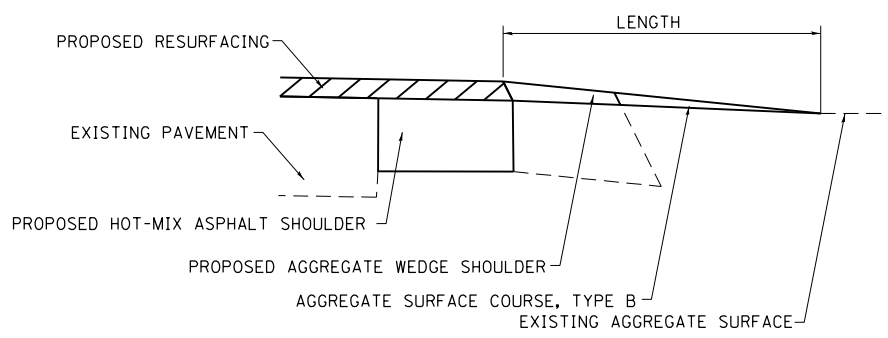
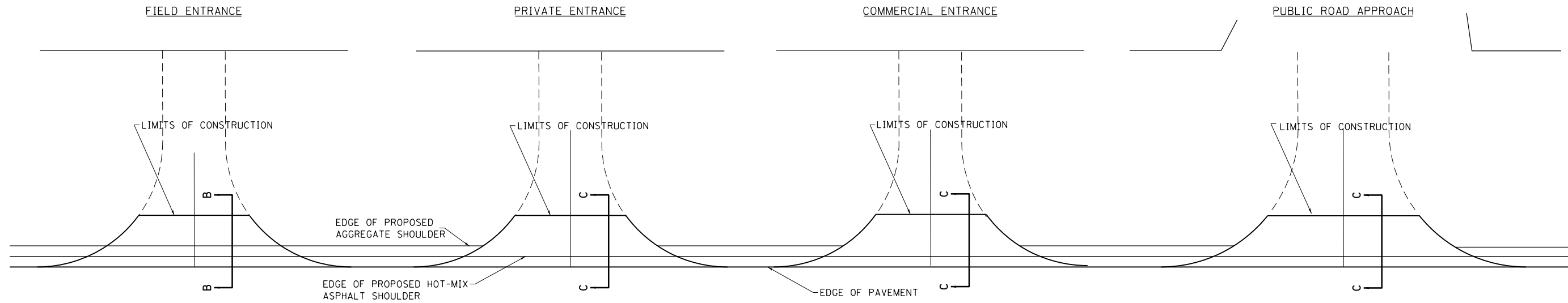
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	DRAWN -	REVISED -
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PLOT DATE = 3/29/2019	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ENTRANCE SCHEDULE AND MAILBOX TURNOUT DETAILS  
SHOULDERS (PROPOSED)

SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	35
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				



TYPICAL SECTION AT MAILBOX TURNOUT  
NOTE: SEE STANDARD 406201 FOR MAILBOX TURNOUT DETAILS

**NOTES**  
LENGTH = 10' UNLESS OTHERWISE NOTED ON PLANS

THE THICKNESS OF THE HOT-MIX ASPHALT SHOULDERS THROUGH AGGREGATE COMMERCIAL ENTRANCES AND PUBLIC ROADS SHALL BE 10". THE COST OF THE EXTRA THICKNESS SHALL BE INCLUDED WITH THE HOT-MIX ASPHALT SHOULDERS PAY ITEM.

THE COST OF THE BITUMINOUS MATERIALS (TACK COAT) FOR ENTRANCES AND PUBLIC ROAD APPROACHES SHALL BE INCLUDED IN THE PAY ITEM INCIDENTAL HOT-MIX ASPHALT SURFACING.

SIDE	STATION	TYPE	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)		INCIDENTAL HOT-MIX ASPHALT SURFACING	AGGREGATE SURFACE COURSE, TYPE B
			SO YD	TON		
RT	1091+95	MBT	--	--	--	--
RT	1098+40	MBT	--	--	--	3.6
RT	1098+60	PE	--	--	--	2.4
LT	1105+10	FE	--	--	--	2.4
RT	1113+15	FE	--	--	--	2.0
RT	1117+45	FE	--	--	--	2.2
LT	1121+00	PE	--	--	--	2.2
LT	1121+70	PE	31.1	2.6	--	--
RT	1127+00	PRA	96.7	8.1	--	--
RT	1129+04	CE	123.3	10.4	--	--
RT	1133+96	CE	126.7	10.6	--	--
RT	1144+70	FE	--	--	--	1.7
LT	1145+80	PE	--	--	--	1.7
LT	1154+50	PE	--	--	--	2.2
LT	1155+00	MBT	--	--	--	3.6
RT	1160+75	FE	--	--	--	2.2
LT	1162+80	PE	--	--	--	4.0
LT	1162+90	MBT	--	--	--	3.6
LT	1165+10	MBT	--	--	--	3.6
LT	1165+30	PE	37.8	3.2	--	--
LT	1170+75	MBT	--	--	--	3.6

SIDE	STATION	TYPE	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)		INCIDENTAL HOT-MIX ASPHALT SURFACING	AGGREGATE SURFACE COURSE, TYPE B
			SO YD	TON		
LT	1171+00	PE	--	--	--	2.4
LT	1175+85	FE	--	--	--	1.7
RT	1176+60	FE	--	--	--	2.0
RT	1180+72	PE	37.8	3.2	--	--
RT	1182+00	FE	--	--	--	1.7
LT	1184+40	MBT	--	--	--	4.4
LT	1184+55	PE	--	--	--	2.2
RT	1184+75	FE	--	--	--	2.0
LT	1186+65	PE	--	--	--	2.2
LT	1201+50	PE	--	--	--	3.2
LT	1202+00	MBT	--	--	--	3.6
LT	1204+80	PE	--	--	--	2.4
LT	1205+10	MBT	--	--	--	3.6
RT	1205+90	PRA	76.1	6.4	--	--
LT	1206+05	FE	--	--	--	1.7
RT	1208+40	FE	--	--	--	1.7
RT	1214+70	PE	--	--	--	2.0
RT	1215+00	MBT	--	--	--	3.6
LT	1218+00	MBT	--	--	--	3.6
LT	1218+17	PE	--	--	--	--
LT	1222+20	PRA	--	--	--	--

SIDE	STATION	TYPE	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)		INCIDENTAL HOT-MIX ASPHALT SURFACING	AGGREGATE SURFACE COURSE, TYPE B
			SO YD	TON		
RT	1222+45	PE	--	--	--	--
LT	1223+10	MBT	--	--	--	3.6
LT	1223+50	CE	--	--	--	4.0
LT	1225+60	FE	--	--	--	1.7
LT	1226+25	CE	--	--	--	2.4
RT	1226+99	FE	--	--	--	1.7
LT	1227+59	CE	--	--	--	2.4
LT	1228+52	FE	--	--	--	1.7
LT	1229+76	FE	--	--	--	1.7
RT	1230+16	CE	57.2	4.8	--	--
LT	1231+15	CE	--	--	--	2.8
		TOTALS	1834	146	258	

SIDE	STATION	TYPE	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)		INCIDENTAL HOT-MIX ASPHALT SURFACING	AGGREGATE SURFACE COURSE, TYPE B
			SO YD	TON		

FE=FIELD ENTRANCE PRA - PUBLIC ROAD APPROACH  
PE=PRIVATE ENTRANCE MBT - MAILBOX TURNOUT  
CE=COMMERCIAL ENTRANCE

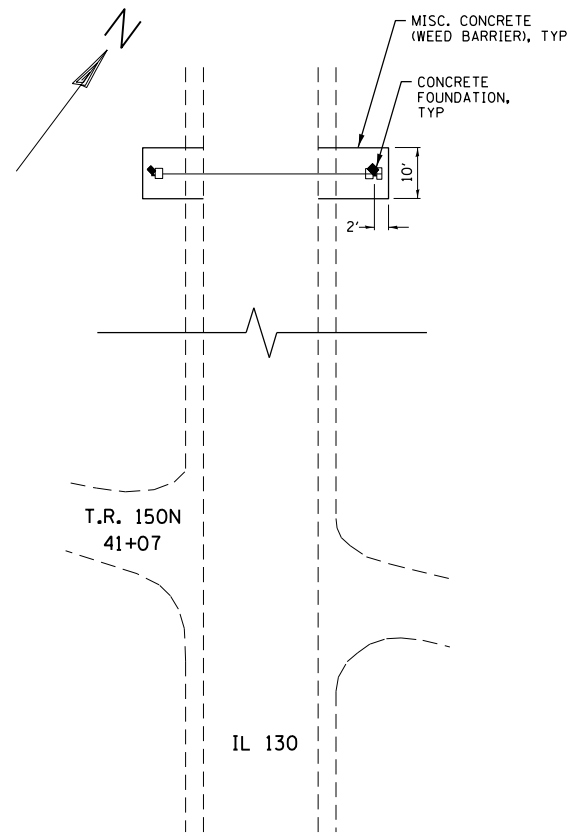
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USER NAME = stefenmk	DESIGNED - 08-03-99	REVISED - 03-13-07
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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 3/29/2019	DATE -	REVISED -

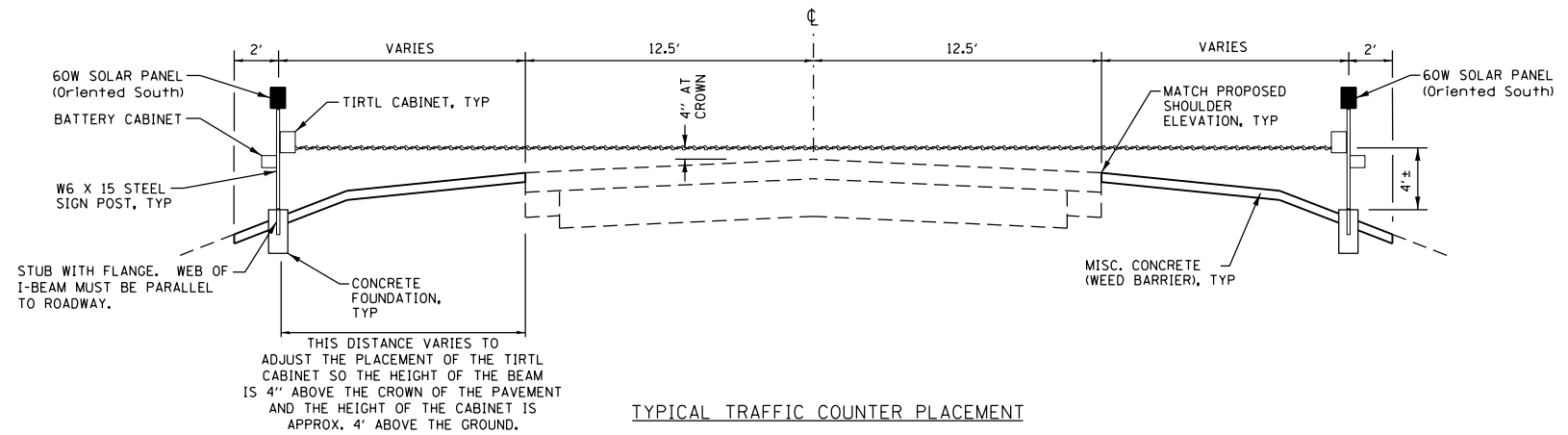
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>ENTRANCE SCHEDULE AND MAILBOX TURNOUT DETAILS SHOULDERS (PROPOSED)</b>			
SCALE:	SHEET 2 OF 2 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	36
CONTRACT NO. 74703				
ILLINOIS FED. AID PROJECT				



LOCATION: APPROX. 0.5 MILE NORTH OF PRA 150N AT STATION •67+55•



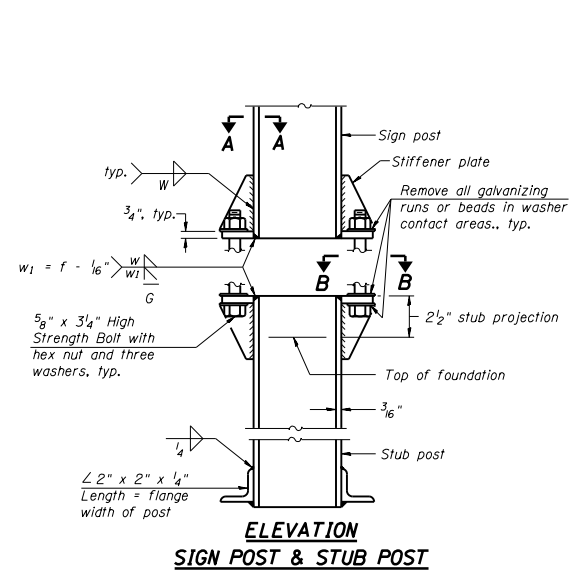
TYPICAL TRAFFIC COUNTER PLACEMENT

NOTES:

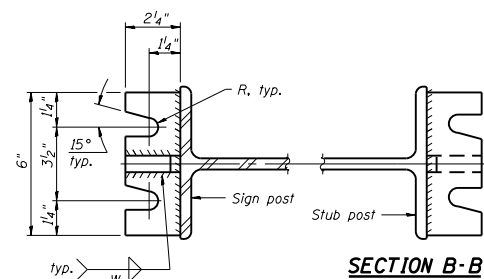
1. SYSTEM CONSISTS OF TWO W6 X 15 I-BEAM SIGNPOSTS WITH CONCRETE FOUNDATION AND A FLANGE WITH BREAKAWAY BOLTS
2. THE I-BEAMS ARE 12' LONG AND DRILLED AS PER THE I-BEAM DETAIL TO ACCOMMODATE A PIPE TO ALLOW THE MOUNTING OF THE SOLAR PANEL.
3. THE WEB OF THE I-BEAM IS TO BE PLACED PARALLEL TO THE LANES.
4. THE TWO POSTS MUST BE PLACED DIRECTLY ACROSS FROM EACH OTHER AND PERPENDICULAR TO THE LANES.
5. TWO POSTS ARE REQUIRED LOCATED AS PER THE ENGINEER AND DRAWING.
6. THE CABINETS ARE ATTACHED TO THE I-BEAM USING 8" GALVANIZED J-BOLTS.
7. THE BOTTOM OF THE CABINET HEIGHT MUST BE NO LOWER THAN 4' ABOVE THE GROUND AND BE ABLE TO VIEW ACROSS ALL LANES AT THE CROWN OF THE PAVEMENT.
8. THE IDOT OFFICE OF PLANNING AND PROGRAMMING DATA MANAGEMENT LAB RICH MARX (217-782-2065) MUST BE CONTACTED 2 WEEKS PRIOR TO ANY WORK BEING DONE TO ENSURE PROPER POST PLACEMENT.

TIRTL TRAFFIC COUNTER SCHEDULE OF QUANTITIES (FOR INFORMATION ONLY)

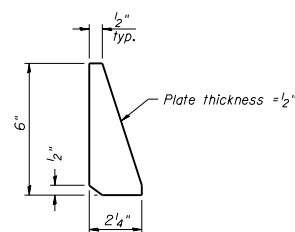
CODE NUMBER	ITEM	UNIT	QUANTITY
61100605	MISCELLANEOUS CONCRETE	CU YD	3.2
72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	520
X0323388	TRAFFIC COUNTER SYSTEM (INCLUDES BELOW)	EACH	1
TIRTL TRAFFIC DATA COLLECTION SYSTEM INCLUDES:			
	Sierra Wireless Modem RV-50	EACH	1
	Panorama Low Profile LTE MiMo LGAM-BC3G-26-3SP Antenna	EACH	1
	TIRTL CABINET 15" X 27" X 16"	EACH	2
	BATTERY CABINET 16.5" X 16" X 11.5" BBA1M w/ #2 Police Lock	EACH	1
	SOLAR PANEL 60 W 12 VDC with Bracket and junction box	EACH	2
	TIRTL TRAFFIC COUNTER	EACH	1
	SUNSAVER SOLAR CHARGE REGULATOR 10 AMP 12 VDC	EACH	2
	BATTERY - 33 AH ABSORBED ELECTROLYTE 8" X 5.5" X 7" (DEEP CYCLE)	EACH	1
	BATTERY -104AH ABSORBED ELECTROLYTE 10" X 6" X 8" (DEEP CYCLE)	EACH	1



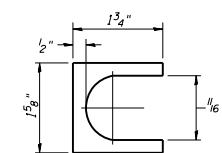
ELEVATION SIGN POST & STUB POST



SECTION A-A



STIFFENER PLATE DETAIL



SHIM DETAIL  
Furnish two 0.01" thick and two 0.03" thick stainless steel or brass (ASTM B36) shims per post.

GENERAL NOTES

Posts shall be plumbed by using shims with post-to-stub post connection bolts snug tight only. Final tightening of all High Strength Bolts shall be in accordance with Article 727.05 and threads at the junction of the bolt and nut shall be burred or center punched to prevent the nut from loosening.

LOADING: 80 m.p.h. wind with 30% gust factor, normal to sign.

DESIGN STRESSES:

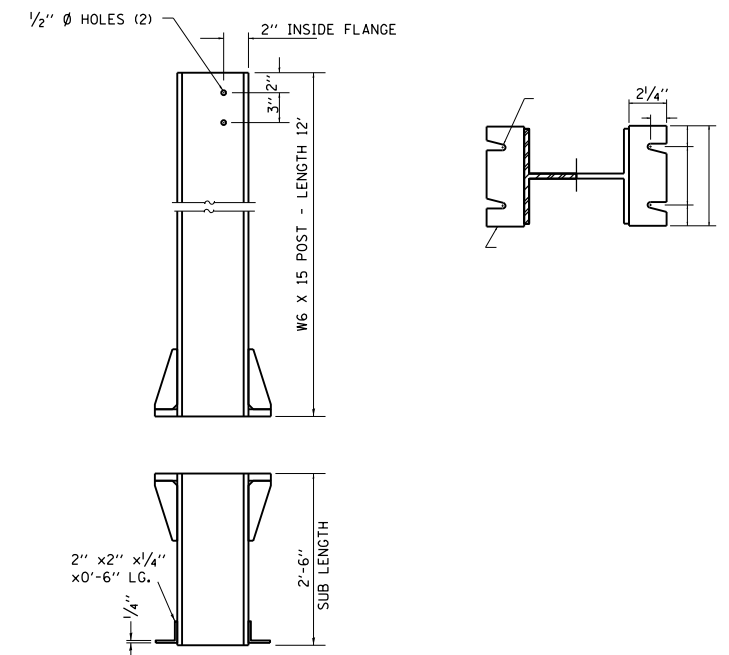
Structural steel - 20,000 p.s.i.

Reinforcing steel - 20,000 p.s.i.

Concrete - 1,400 p.s.i.

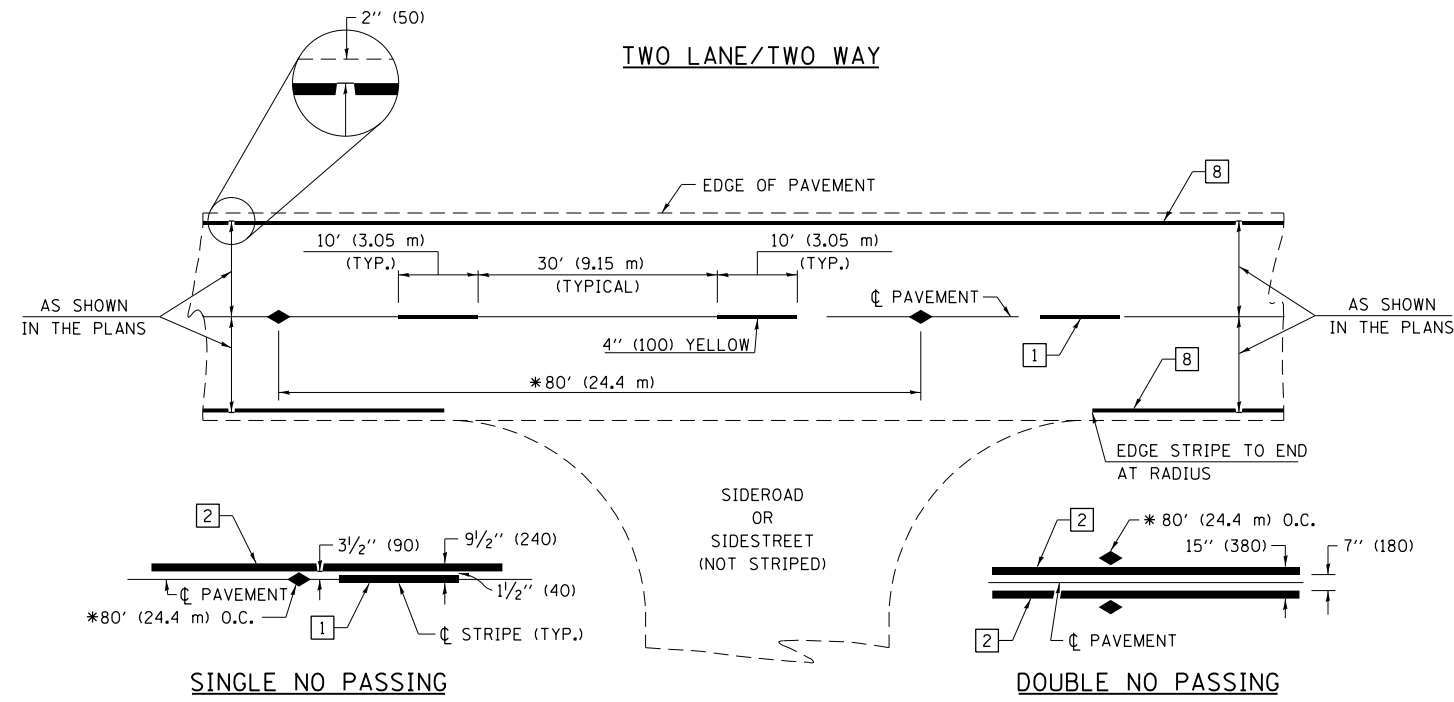
Footing soil pressure - 2,000 p.s.f.

After fabrication, the post and upper 6", min. of the stub post shall be hot-dip galvanized in accordance with AASHTO M111. All bolts, nuts and washers shall be hot-dip galvanized in accordance with AASHTO M232.



BREAK-AWAY WIDE FLANGE STEEL POST DETAILS

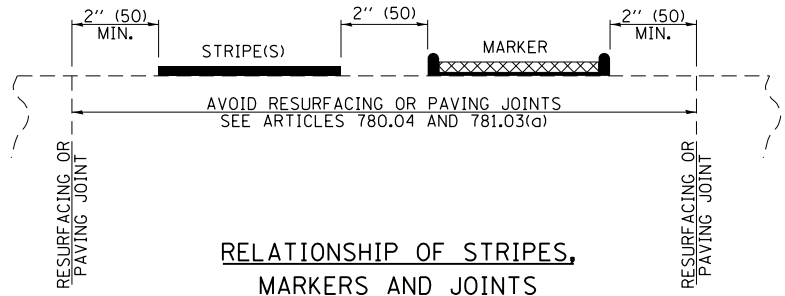
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PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -							CONTRACT NO. 74703				
Default	PLOT DATE = 4/1/2019	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				
SCALE:					SHEET	OF	SHEETS	STA.	TO	STA.			



\* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 12" (300) SOLID (WHITE)
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) PARKING (WHITE)

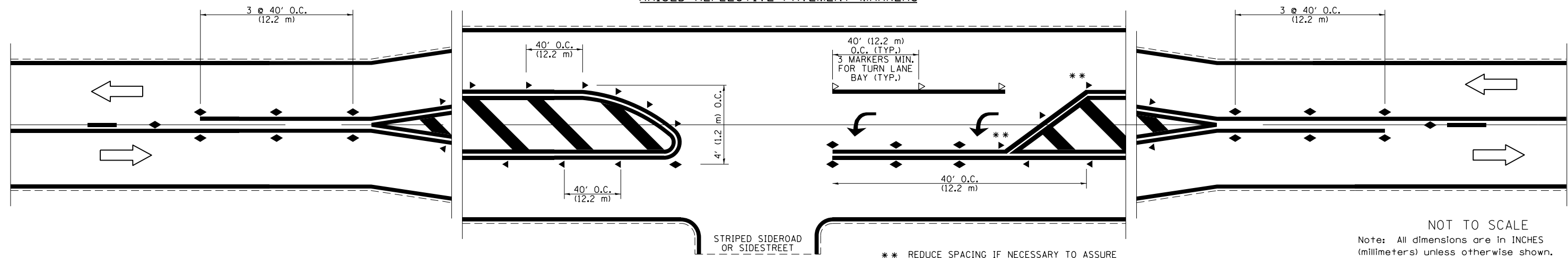


RELATIONSHIP OF STRIPES, MARKERS AND JOINTS

TYPICAL PAVEMENT MARKERS LEGEND

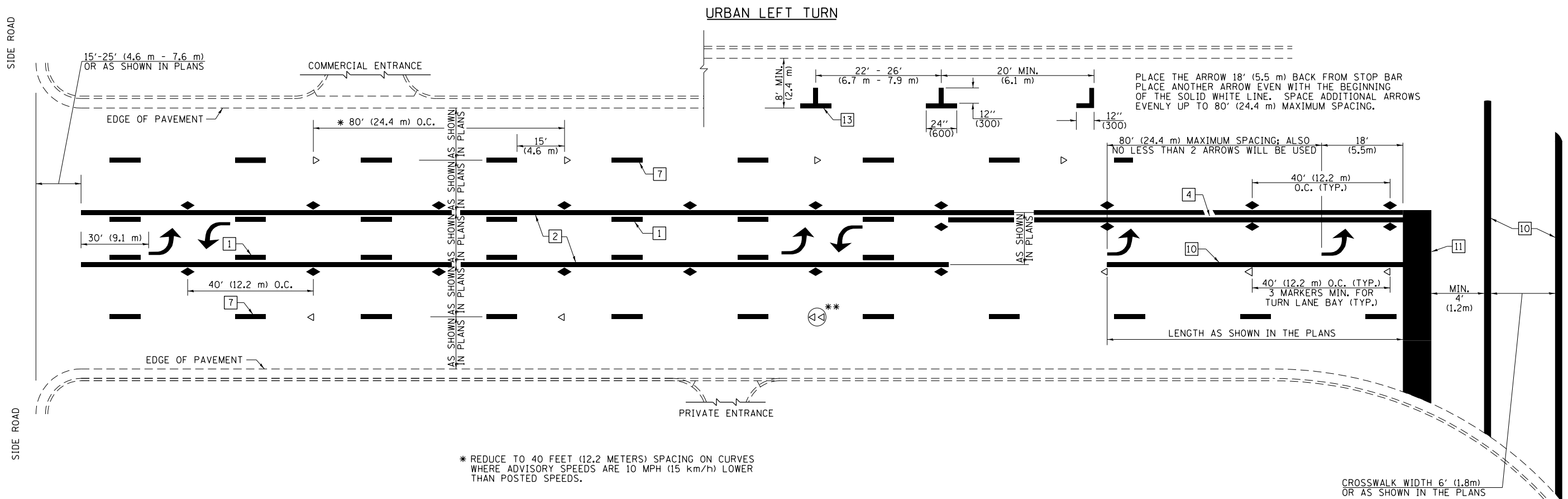
- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RAISED REFLECTIVE PAVEMENT MARKERS



\*\* REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

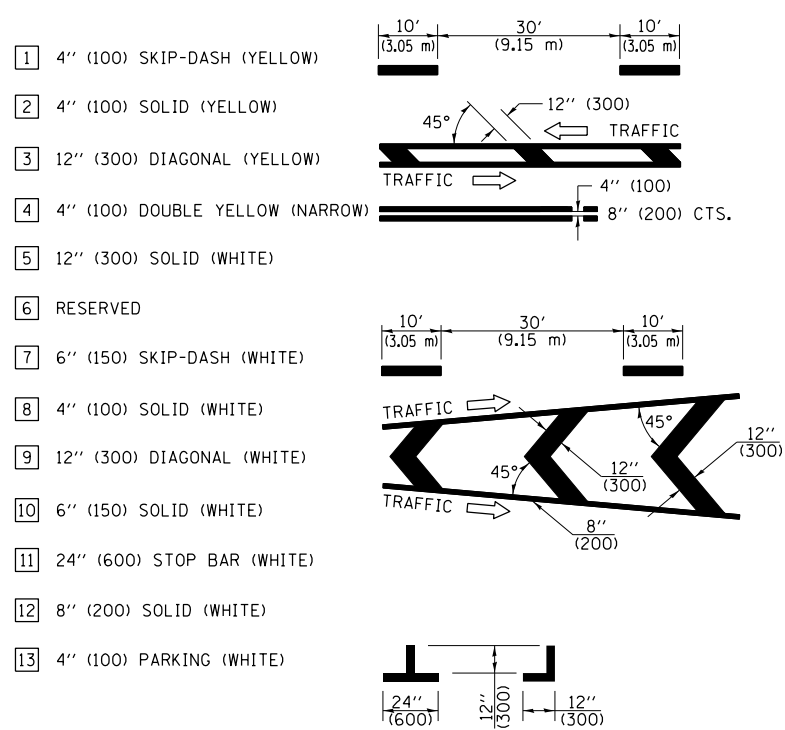
NOT TO SCALE  
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.



\* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

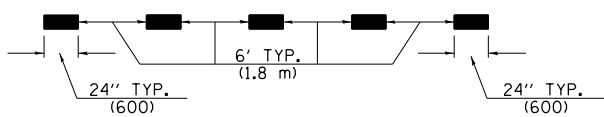
\*\* DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

**PAVEMENT MARKING LEGEND**

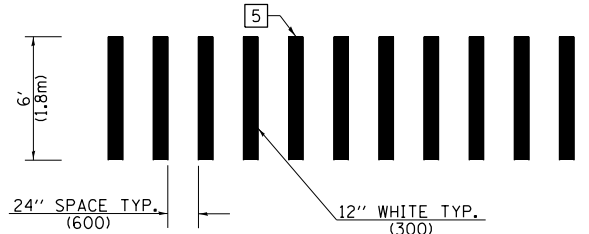


**GENERAL NOTES**

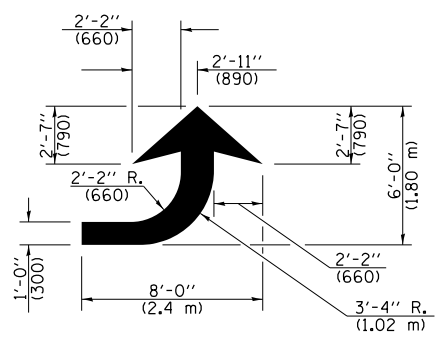
- TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
- THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
- USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)
- LANE LINE EXTENSIONS SHALL BE THE SAME COLOR AND WIDTH AS THE LANE LINE BEING EXTENDED.



**LANE LINE EXTENSIONS**

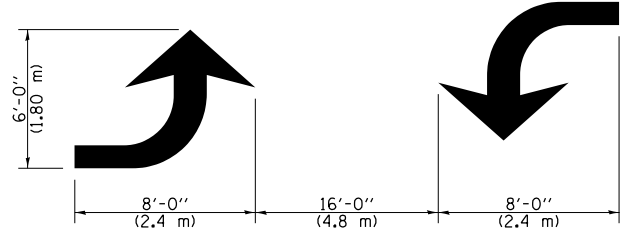


**CROSSWALK DETAIL (DECATUR CITY LIMITS ONLY)**



**LEFT ARROW**

REVERSE FOR RIGHT ARROW  
AREA = 15.6 SQ. FT. (1.47 m<sup>2</sup>)  
(WHITE)

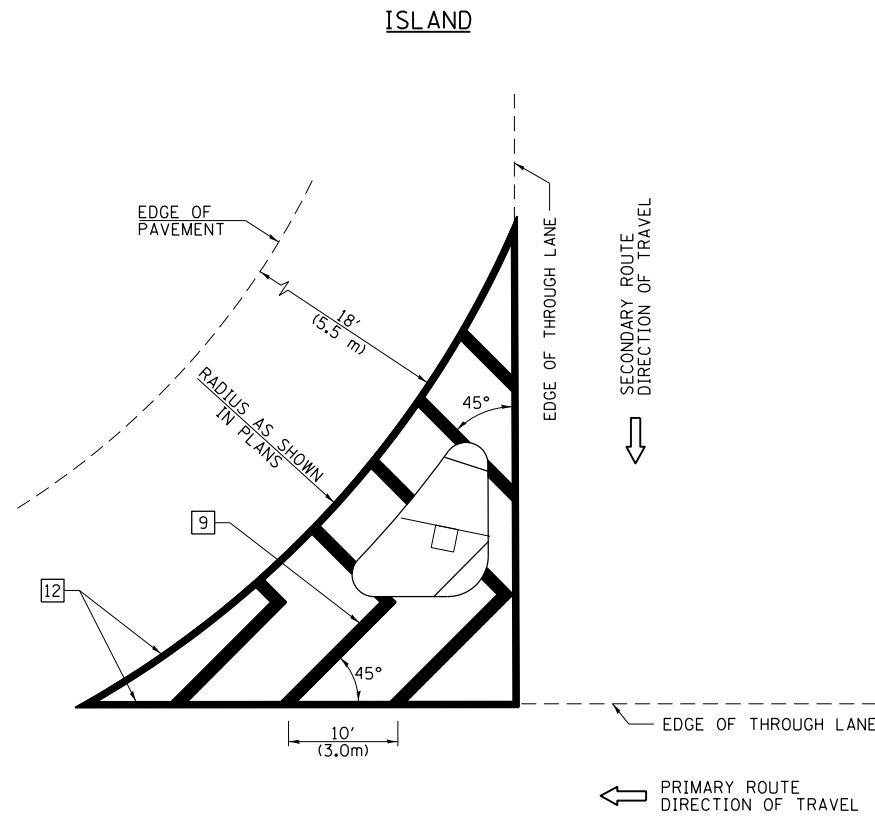


**TYPICAL DOUBLE TURN ARROWS (WHITE)**

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL &amp; URBAN APPLICATIONS)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-shd-details.dwg	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					116	(6,7)RS-3	EDWARDS	41	39
PLOT DATE = 3/29/2019	DATE -	REVISED -	REVISED -		SCALE: N/A SHEET NO. 2 OF 4 SHEETS STA. TO STA.			CONTRACT NO. 74703				
										ILLINOIS FED. AID PROJECT		

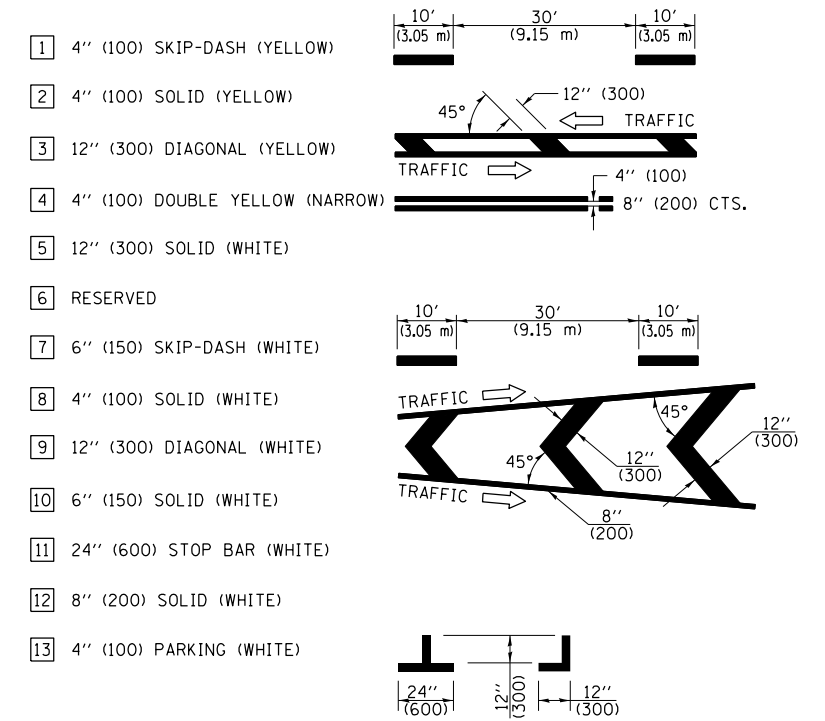


**GENERAL NOTES**

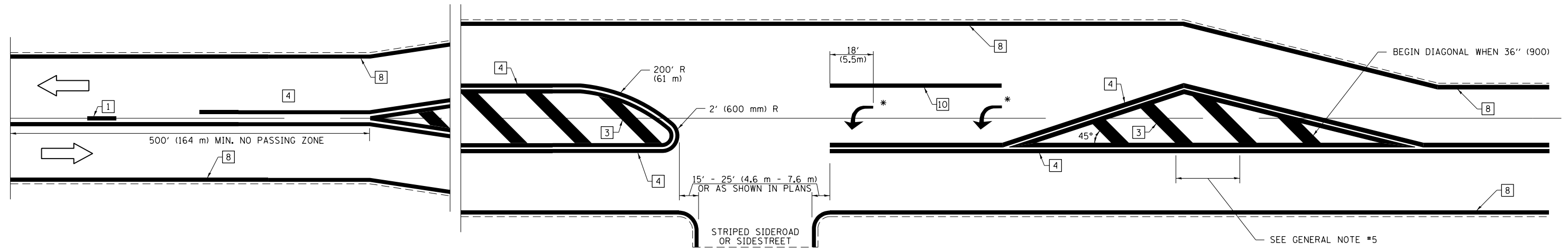
1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

< 30 MPH (< 50 km/h)	15' (4.5 m)
30-45 MPH (50-75 km/h)	20' (6.0 m)
>45 MPH (>75 km/h)	30' (9.0 m)

**PAVEMENT MARKING LEGEND**



**RURAL LEFT TURN STRIPING**



\* PLACE AN ARROW 18' (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

NOT TO SCALE  
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 7 DETAIL NO. 7800001**

FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\0774703-shd-details.dwg		REVISIONS	REVISIONS
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/29/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

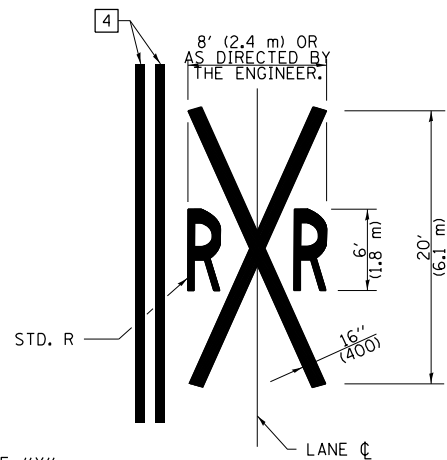
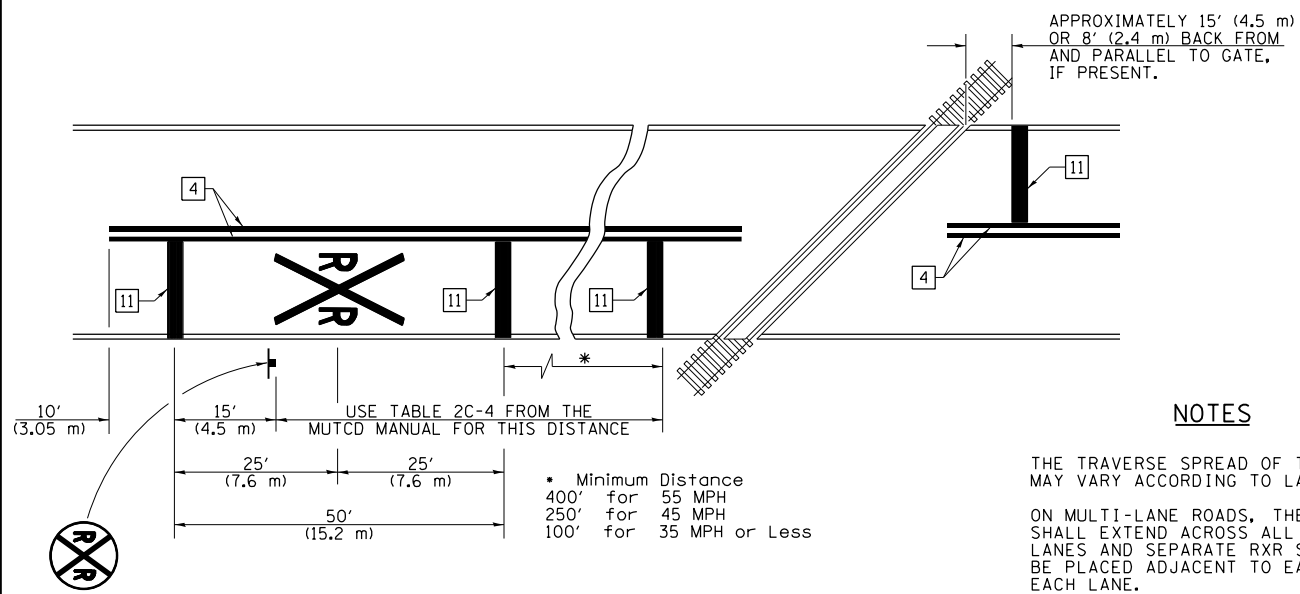
**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS  
(RURAL & URBAN APPLICATIONS)**

SCALE: N/A SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
116	(6,7)RS-3	EDWARDS	41	40
				CONTRACT NO. 74703
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING



NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

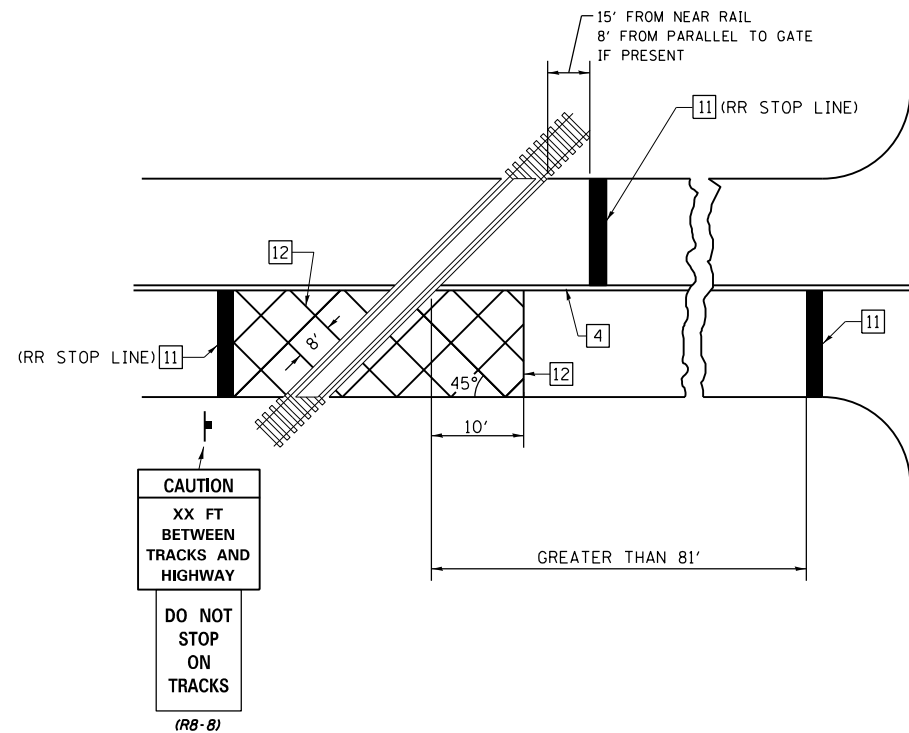
ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE R X R SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

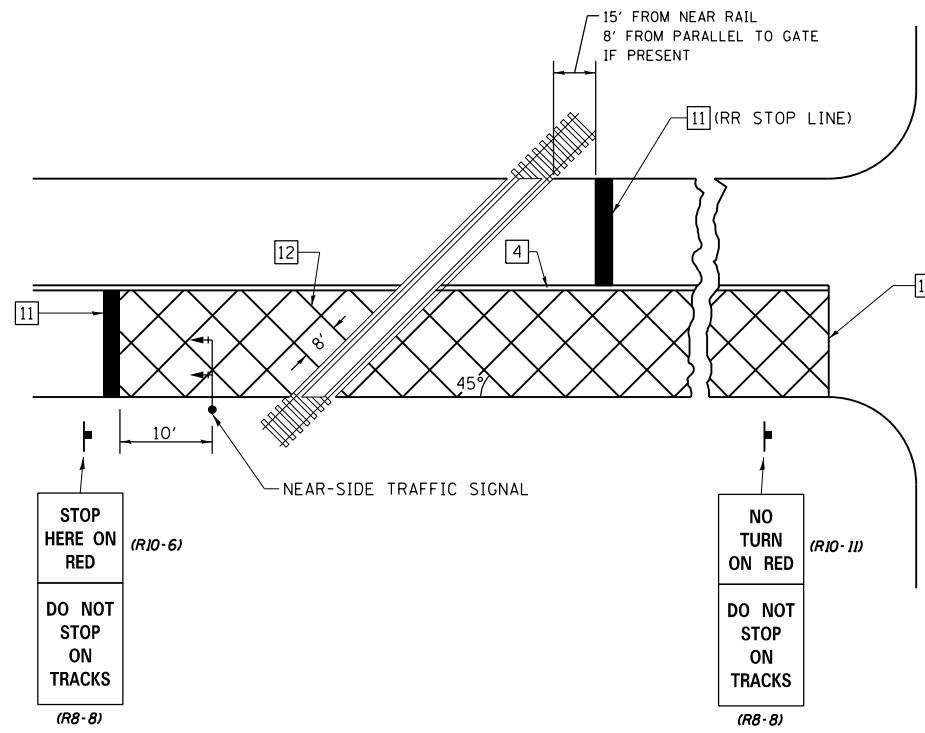
PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 12" (300) SOLID (WHITE)
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) PARKING (WHITE)

RAILROAD CROSSING WITH INTERCONNECT ONLY



RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.

SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

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pw:\planroom.dot.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 7\Projects\74703\DRAWING\CADsheets\D774703-sht-details.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			116	(6,7)RS-3	EDWARDS	41	41	
PLOT DATE = 3/29/2019	DATE -	REVISED -	REVISED -			CONTRACT NO. 74703		ILLINOIS FED. AID PROJECT			
						SCALE: N/A	SHEET NO. 4 OF 4 SHEETS	STA.	TO STA.		