06-14-2019 LETTING ITEM 106

106

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	1
	ILLINOIS	CONTRACT NO. 72L15		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

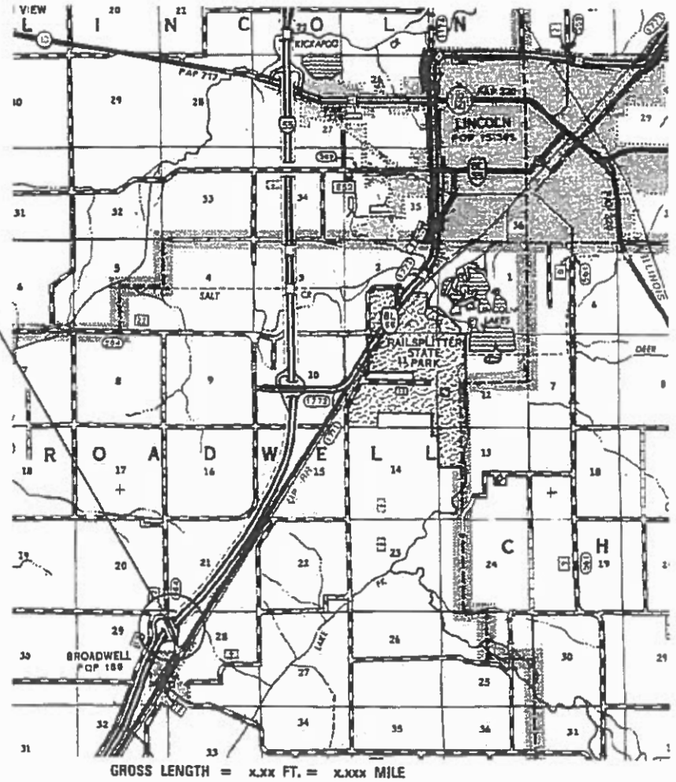
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED
CONTRACT MAINTENANCE

FAI ROUTE 55 (I–55)
SECTION (54–2) BJR

BRIDGE JOINT REPAIR
LOGAN COUNTY

C–96–051–19



PROJECT LOCATION
SN 054-0039
CH 513 OVER I-55
BROADWELL INTERCHANGE

N

D-96-037-19

LOCATION OF SECTION INDICATED THUS:- ▬

100' 200' 300' — 1" = 100'
10' 20' 30' — 1" = 10'
50' 100' — 1" = 50'
50' 100' — 1" = 40'
50' 100' — 1" = 30'
50' 100' — 1" = 20'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1–800–892–0123
OR 811

BRIDGE MAINTENANCE ENGINEER: BRANDON DUDLEY, 217–785–9290

GROSS LENGTH = x.xx FT. = x.xxx MILE
NET LENGTH = x.xx FT. = x.xxx MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED 22 March 2019
REGIONAL ENGINEER
May 10 2019
ENGINEER OF DESIGN AND ENVIRONMENT
May 10 2019
DIRECTOR OF HIGHWAY PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

CONTRACT NO. 72L15

INDEX OF SHEETS

STANDARDS

000001-07
001001-02
001006
701001-02
701006-05
701101-05
701106-02
701301-04
701316-12
701901-08
704001-08

GENERAL NOTES:

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
DISTRICT 6

EXAMINED 15 March 2019
ENGINEER OF OPERATIONS

EXAMINED 15 March 2019
ENGINEER OF PROJECT IMPLEMENTATION

EXAMINED March 5 2019
ENGINEER OF PROGRAM DEVELOPMENT

				0-02174-6002
				SN 054-0039
				100% STATE
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE 0047 LOGAN
50102400	CONCRETE REMOVAL	CU YD	7	7
50300255	CONCRETE SUPERSTRUCTURE	CU YD	7	7
50300300	PROTECTIVE COAT	SQ YD	31	31
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	730	730
50800515	BAR SPLICERS	EACH	8	8
52000110	PREFORMED JOINT STRIP SEAL	FOOT	144	144
67100100	MOBILIZATION	L SUM	1	1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1
70400100	TEMPORARY CONCRETE BARRIER WALL	FOOT	100	100
70400200	RELOCATE TEMPORARY CONCRETE BARRIER WALL	FOOT	100	100
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	1	1
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	1	1
X7010200	TRAFFIC CONTROL AND PROTECTION, STANDARD 701316 (SPECIAL)	EACH	1	1

REV. - MS

USER NAME = dudleyom	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' in.	CHECKED -	REVISED -
PLOT DATE = 3/19/2019	DATE -	REVISED -

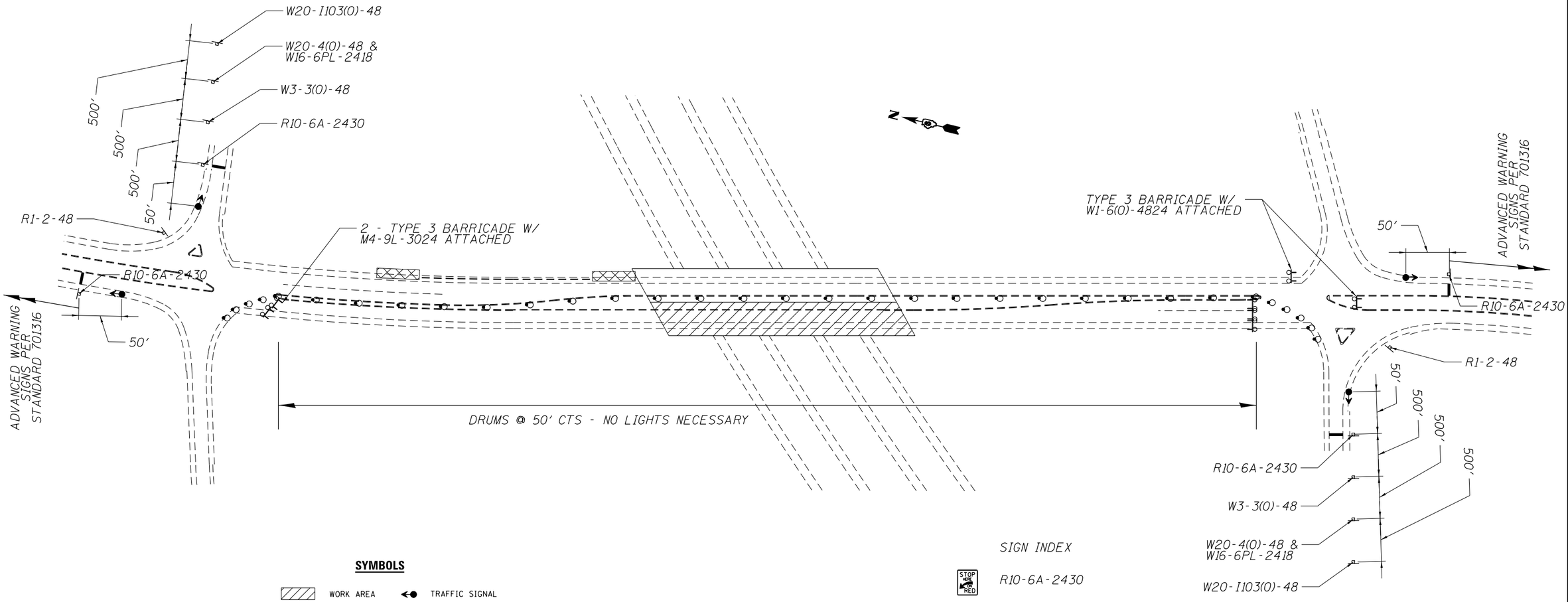
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX, STANDARDS, GENERAL NOTES, SIGNATURES, & SUMMARY OF QUANTITIES

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BIR	LOGAN	10	2

CONTRACT NO. 72L15
ILLINOIS FED. AID PROJECT



SYMBOLS

WORK AREA	TRAFFIC SIGNAL
SIGN	DRUM WITH STEADY BURNING LIGHT
IMPACT ATTENUATOR	TYPE III BARRICADE WITH FLASHING LIGHTS
TEMPORARY CONCRETE BARRIER WALL	

SIGN INDEX

STOP HERE ON RED	R10-6A-2430
	W3-3(0)-48
ONE LANE ROAD AHEAD	W20-4(0)-48
	W16-6PL-2418
ROAD CONSTRUCTION AHEAD	W20-I103(0)-48
YIELD	R1-2-48
	W1-6(0)-4824
DETOUR	M4-9L-3024

TRAFFIC CONTROL NOTES:

1. TRAFFIC CONTROL DEVICES NOT SHOWN IN THIS DETAIL SHALL BE PER STANDARD 701316. DEVICES AND TEMPORARY PAVEMENT MARKINGS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701316 (SPECIAL). IMPACT ATTENUATORS, TEMPORARY TRAFFIC SIGNALS, AND TEMPORARY CONCRETE BARRIER SHALL BE PAID SEPARATELY.

2. ALL EXISTING STOP SIGNS INSIDE THE WORK ZONE SHALL BE COVERED WHILE TRAFFIC SIGNALS ARE ACTIVE.

3. RUMBLE STRIPS SHOWN IN STANDARD 701316 WILL NOT BE REQUIRED.

4. STOP BARS SHOWN SHALL BE 24" WIDE.

5. IMPACT ATTENUATORS AND TEMPORARY CONCRETE BARRIER SHALL BE PLACED SUCH THAT ALL REQUIRED PINNING/SECURING SHALL OCCUR ON THE HMA SHOULDERS AND NOT THE CONCRETE BRIDGE DECK.

MODEL: Default
FILE NAME: G:\OPERATIONS\Bridges\Bridgeplans_CAD\72L13 - 0540039 Johnstplansheet.dgn

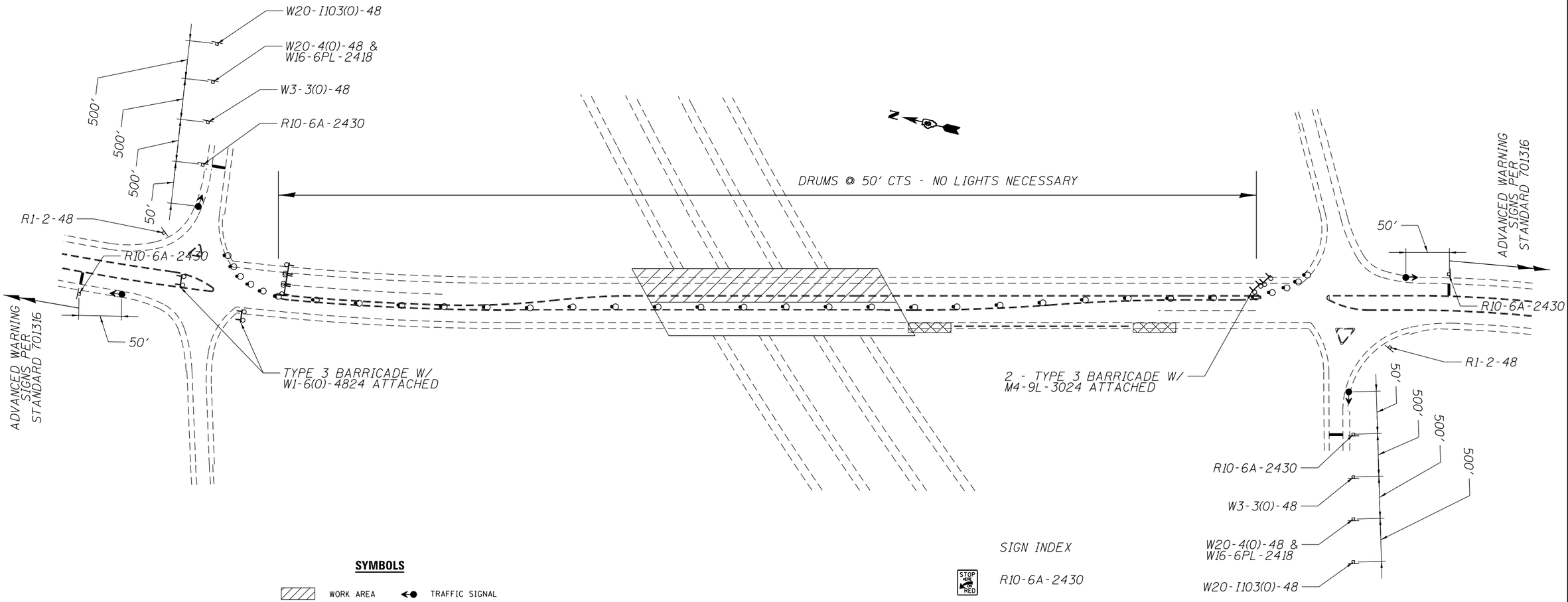
USER NAME = dudleybm	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/19/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE I TRAFFIC CONTROL PLAN

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	3
		CONTRACT NO. 72L15		
	ILLINOIS	FED. AID PROJECT		



SYMBOLS

WORK AREA	TRAFFIC SIGNAL
SIGN	DRUM WITH STEADY BURNING LIGHT
IMPACT ATTENUATOR	TYPE III BARRICADE WITH FLASHING LIGHTS
TEMPORARY CONCRETE BARRIER WALL	

SIGN INDEX

STOP HERE ON RED	R10-6A-2430
	W3-3(0)-48
ONE LANE ROAD AHEAD	W20-4(0)-48
	W16-6PL-2418
ROAD CONSTRUCTION AHEAD	W20-1103(0)-48
YIELD	R1-2-48
	W1-6(0)-4824
DETOUR	M4-9L-3024

TRAFFIC CONTROL NOTES:

1. TRAFFIC CONTROL DEVICES NOT SHOWN IN THIS DETAIL SHALL BE PER STANDARD 701316. DEVICES AND TEMPORARY PAVEMENT MARKINGS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701316 (SPECIAL). IMPACT ATTENUATORS, TEMPORARY TRAFFIC SIGNALS, AND TEMPORARY CONCRETE BARRIER SHALL BE PAID SEPARATELY.

2. ALL EXISTING STOP SIGNS INSIDE THE WORK ZONE SHALL BE COVERED WHILE TRAFFIC SIGNALS ARE ACTIVE.

3. RUMBLE STRIPS SHOWN IN STANDARD 701316 WILL NOT BE REQUIRED.

4. STOP BARS SHOWN SHALL BE 24" WIDE.

5.IMPACT ATTENUATORS AND TEMPORARY CONCRETE BARRIER SHALL BE PLACED SUCH THAT ALL REQUIRED PINNING/SECURING SHALL OCCUR ON THE HMA SHOULDERS AND NOT THE CONCRETE BRIDGE DECK.

MODEL: Default
FILE NAME: G:\OPERATIONS\Bridges\Bridgeplans_CAD\72L13 - 0540039 Johnstplansheet.dgn

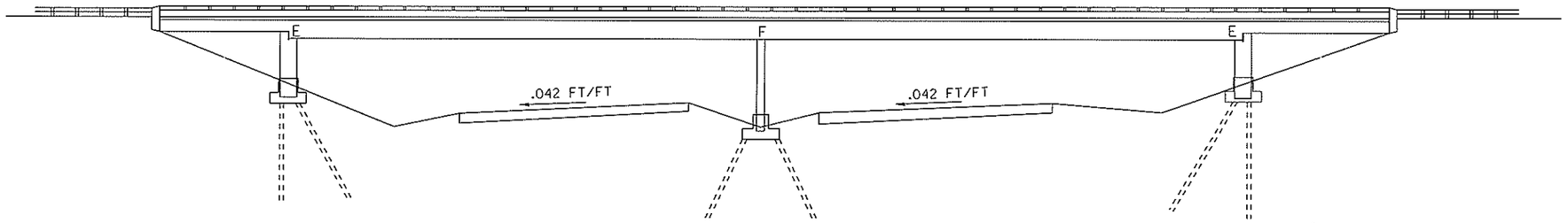
USER NAME = dudleybm	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/19/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

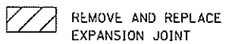
STAGE II TRAFFIC CONTROL PLAN

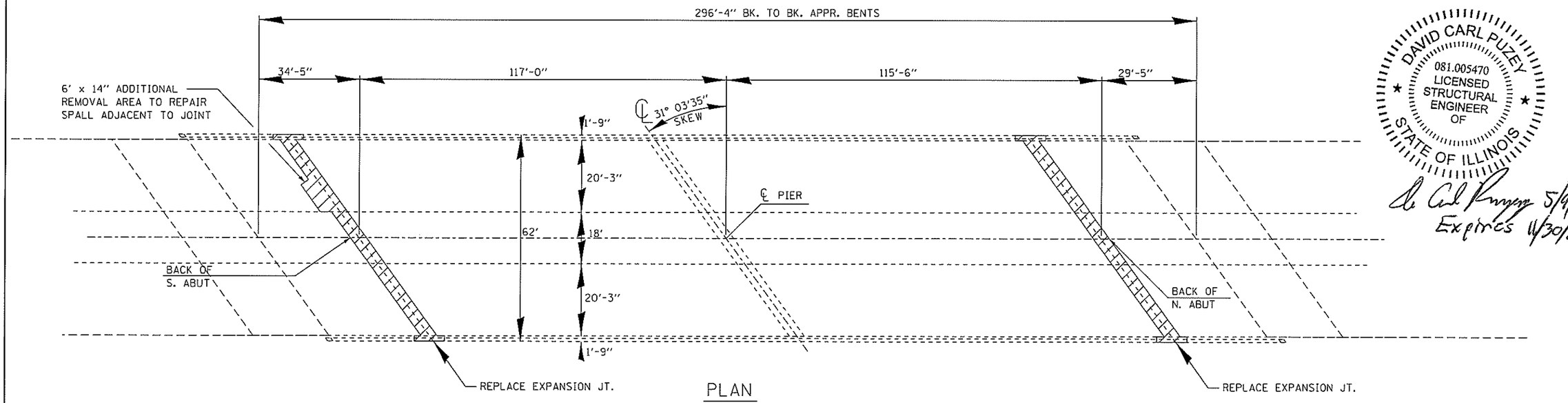
SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	4
		CONTRACT NO. 72L15		
	ILLINOIS	FED. AID PROJECT		



ELEVATION





PLAN

STRUCTURE GENERAL NOTES:

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

PRIOR TO POURING THE NEW CONCRETE DECK, ALL HEAVY OR LOOSE MILL SCALE AND OTHER LOOSE OR POTENTIALLY DETRIMENTAL FOREIGN MATERIAL SHALL BE REMOVED FROM THE SURFACES IN CONTACT WITH CONCRETE. TIGHTLY ADHERED PAINT MAY REMAIN UNLESS OTHERWISE NOTED. REMOVAL SHALL BE ACCOMPLISHED BY METHODS THAT WILL NOT DAMAGE THE STEEL AND THE COST WILL BE INCLUDED IN CONCRETE REMOVAL.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED, AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50°F.

THE ABUTMENT AND DECK SURFACES IN THE AREAS OF EXPANSION JOINT REPLACEMENT SHALL HAVE A TINED FINISH AS PER ARTICLE 420.09(eX1) OF THE STANDARD SPECIFICATIONS. COST INCLUDED WITH CONCRETE SUPERSTRUCTURE.

PROTECTIVE COAT SHALL ONLY BE APPLIED TO THE DECK AND PARAPETS ADJACENT TO THE PROPOSED EXPANSION JOINTS AND NOT THE MICROSILICA OVERLAY.

TOTAL BILL OF MATERIAL (054-0039)

ITEM	*UNIT*	*QUANTITY*
Concrete Removal	*Cu. Yd.*	*7.0*
Concrete Superstructure	*Cu. Yd.*	*7.0*
Reinforcement Bars, Epoxy Coated	*Pound*	*730*
Bar Splicers	*Each*	*8*
Preformed Joint Strip Seal	*Foot*	*144*
Protective Coat	*Sq. Yd.*	*31*

DAVID CARL PUZEY
081.005470
LICENSED STRUCTURAL ENGINEER
OF
STATE OF ILLINOIS
5/9/19
Expires 4/30/20

MODEL: Default
FILE NAME: O:\OPERATIONS\Bridges\BridgesPlans_CAD\72L15 - 054D039 Jointsplansheet.dgn

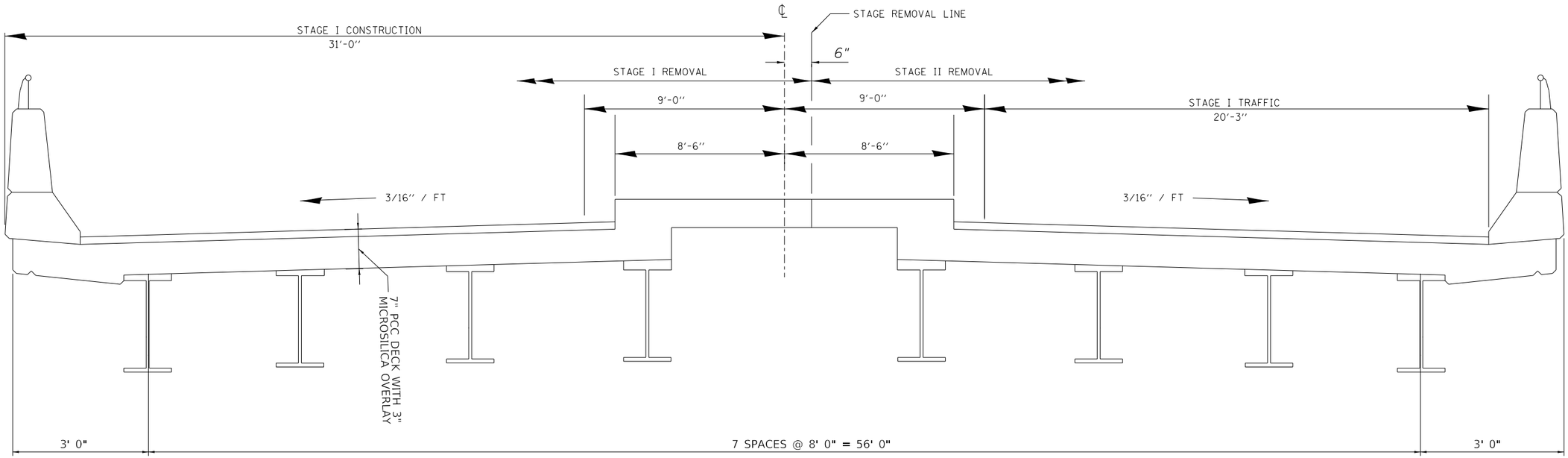
USER NAME = dudleybm	DESIGNED - BMB	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED - AJR	REVISED -	
PLOT DATE = 3/19/2019	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

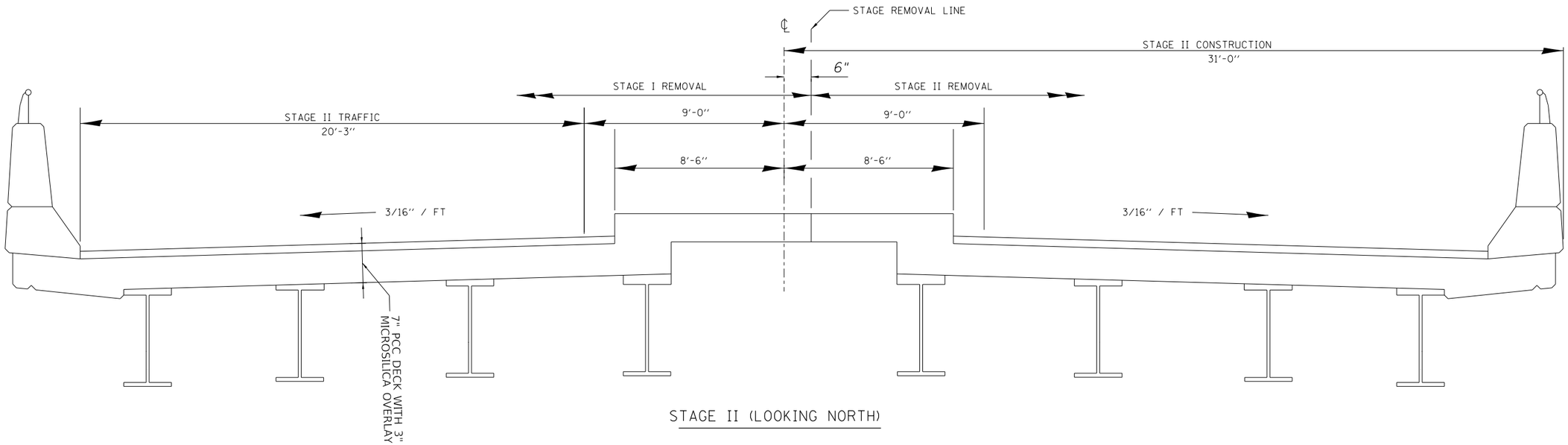
GENERAL PLAN & ELEVATION
SN 054-0039

SCALE: | SHEET 1 OF 6 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	5
		CONTRACT NO. 72L15		
	ILLINOIS FED. AID PROJECT			



STAGE I (LOOKING NORTH)



STAGE II (LOOKING NORTH)

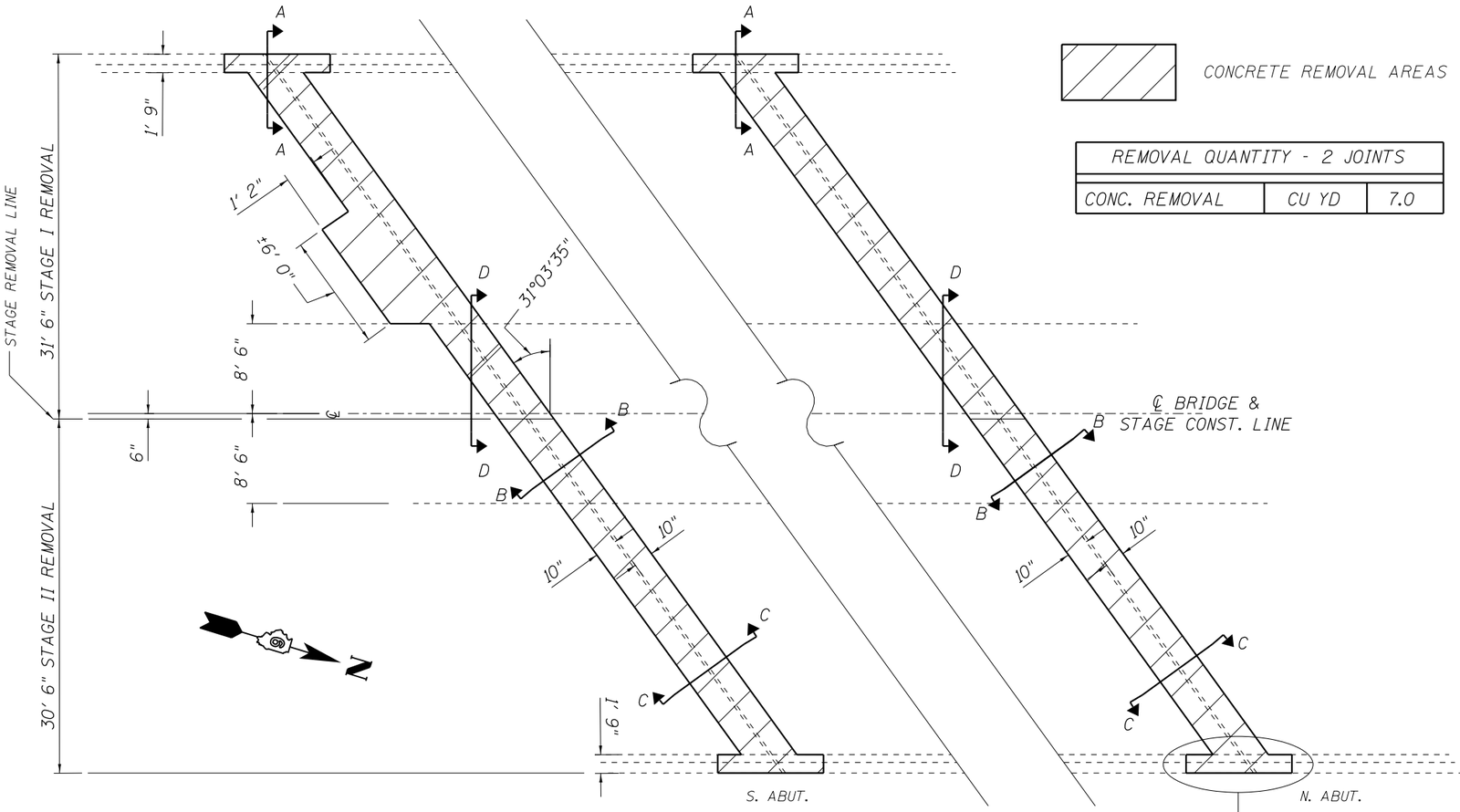
MODEL: Default
FILE NAME: C:\OPERATIONS\Bridges\Bridgeplans_CAD\72L15 - 0540039 Johnsplanssheet.dgn

USER NAME = dudleybm	DESIGNED -	REVISED -	**STATE OF ILLINOIS**
	DRAWN -	REVISED -	**DEPARTMENT OF TRANSPORTATION**
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/19/2019	DATE -	REVISED -	

STAGING DETAIL
SN 054-0039

SCALE: | SHEET 2 OF 6 SHEETS | STA. | TO STA.

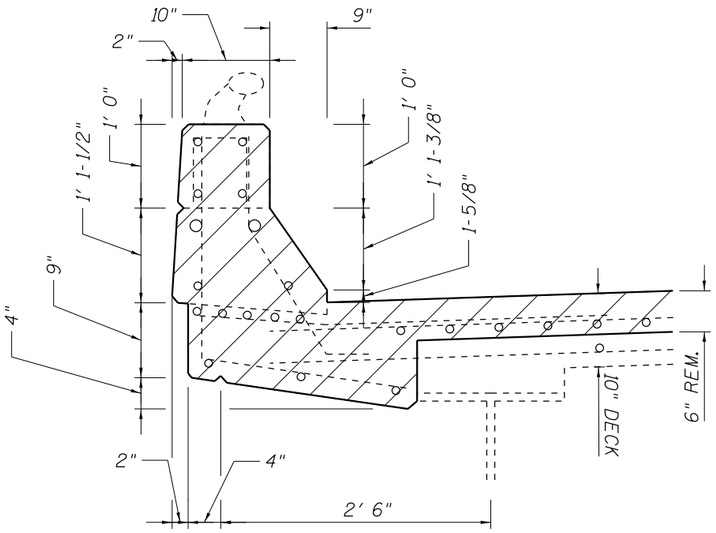
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	6
		CONTRACT NO. 72L15		
	ILLINOIS	FED. AID PROJECT		



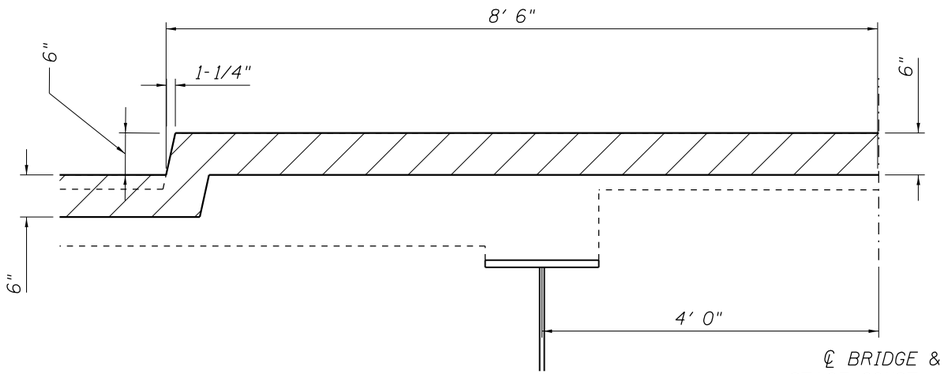
CONCRETE REMOVAL AREAS

REMOVAL QUANTITY - 2 JOINTS		
CONC. REMOVAL	CU YD	7.0

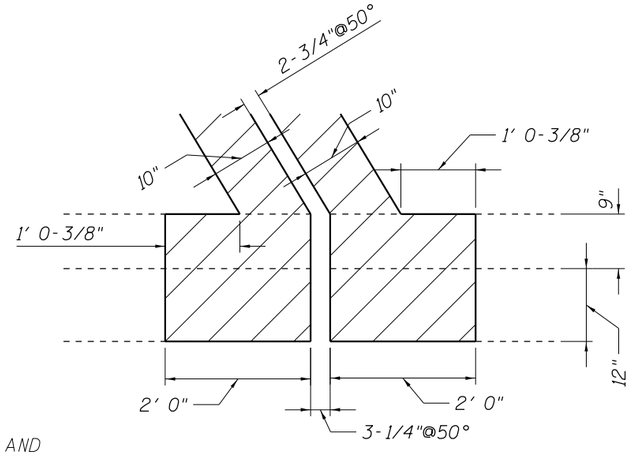
JOINT REMOVAL PLAN



SECTION A-A: PARAPET CROSS SECTION

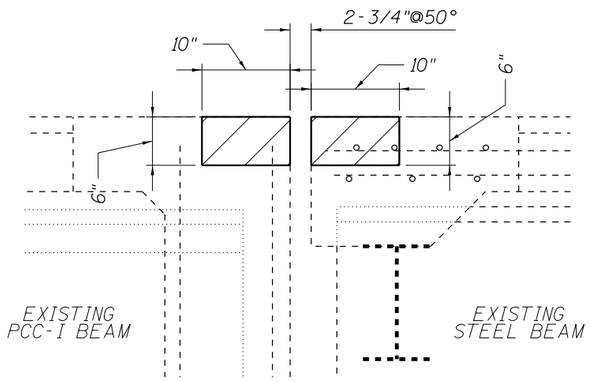


SECTION D-D: MEDIAN CROSS SECTION



PARAPET REMOVAL PLAN (TYPICAL)

SECTION B-B: JOINT CROSS SECTION (MEDIAN)



SECTION C-C: JOINT CROSS SECTION (DECK)

NOTES:

- EXISTING JOINTS CONSIST OF EMBEDDED STEEL PLATES AND PREFORMED RUBBER SEALS. REMOVAL AND DISPOSAL OF THE EXISTING JOINTS SHALL BE INCLUDED IN THE COST OF CONCRETE REMOVAL.
- THE CONTRACTOR SHALL USE EXTEREME CARE DURING CONCRETE REMOVAL SO AS NOT TO DAMAGE PPC-I BEAMS.

MODEL: Default
FILE NAME: c:\OPERATIONS\Bridges\Bridgeplans_CAD\72L15 - 0340039 Jointsplansheet.dgn

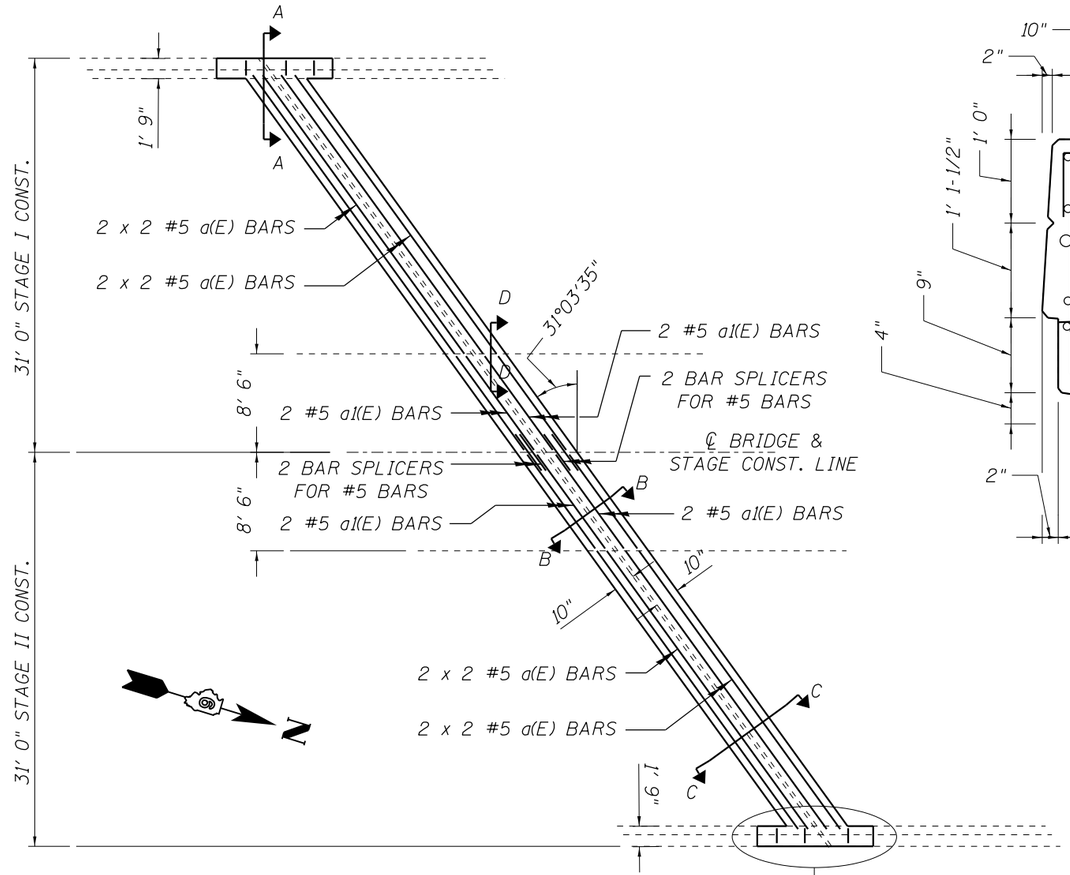
USER NAME = dudleybm	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/19/2019	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

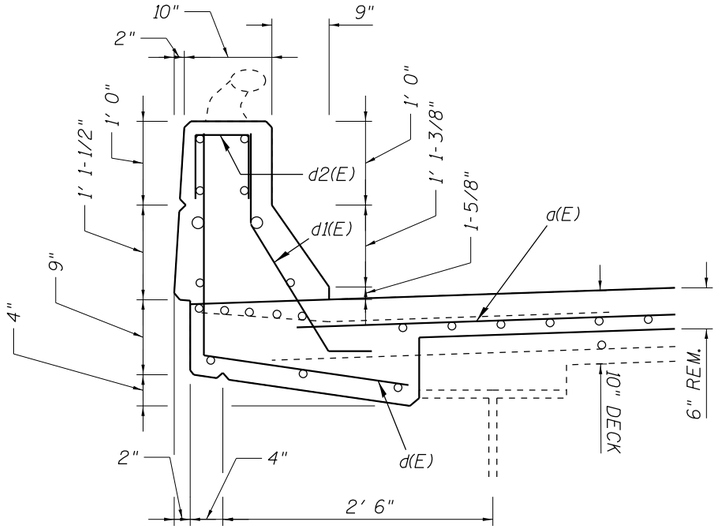
REMOVAL DETAILS
SN 054-0039

SCALE: | SHEET 3 OF 6 SHEETS | STA. | TO STA.

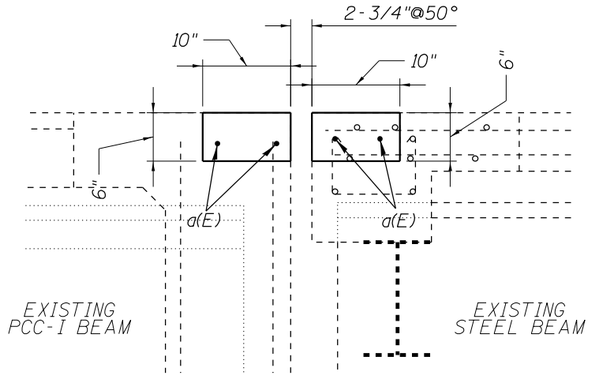
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	7
		CONTRACT NO. 72L15		
	ILLINOIS FED. AID PROJECT			



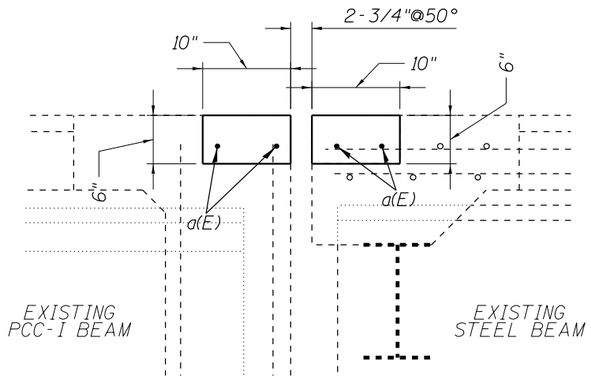
JOINT PLAN (NORTH JT. SHOWN, SOUTH JT. SIMILAR)



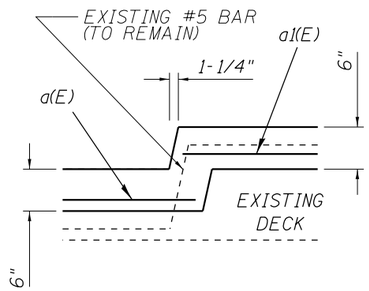
SECTION A-A: PARAPET CROSS SECTION



SECTION B-B: JOINT CROSS SECTION (MEDIAN)

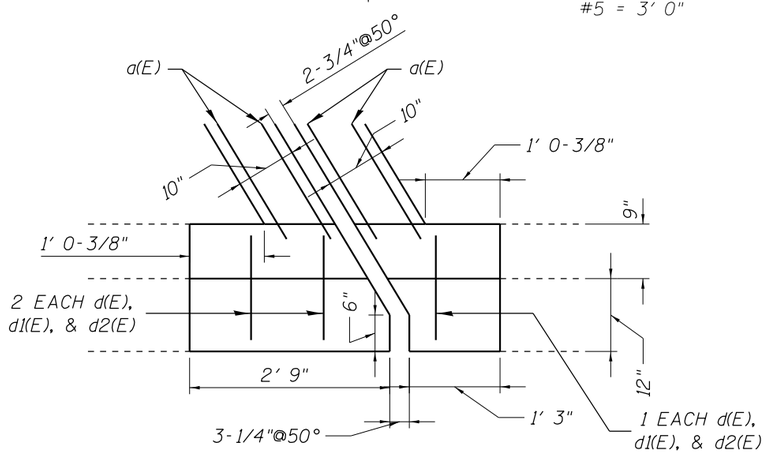


SECTION C-C: JOINT CROSS SECTION (DECK)

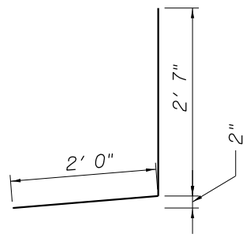


SECTION D-D: MEDIAN CURB CROSS SECTION (TYPICAL)

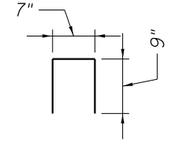
MIN. BAR LAP
#5 = 3' 0"



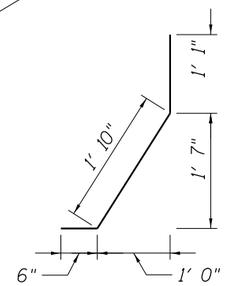
PARAPET PLAN (TYPICAL)



BAR d(E)



BAR d2(E)



BAR d1(E)

BILL OF MATERIAL - 2 JOINTS				
BAR	#	SIZE	LENGTH	SHAPE
a(E)	32	#5	14' 1"	——
a1(E)	16	#5	9' 4"	——
d(E)	12	#4	4' 7"	⌋
d1(E)	12	#5	3' 5"	⌋
d2(E)	12	#4	2' 1"	⊓
REINFORCEMENT BARS (EPOXY COATED)		POUND	730	
CONC. SUPERSTRUCTURE		CU YD	7.0	

MODEL: Default
FILE NAME: G:\OPERATIONS\Bridges\Bridgeplans_CAD\72L15 - 0540039 Jointsplanssheet.dgn

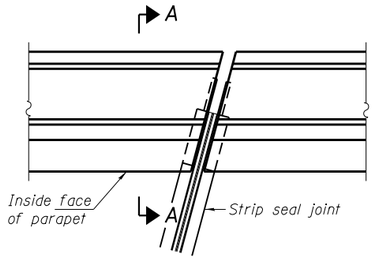
USER NAME = dudleybm	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/19/2019	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

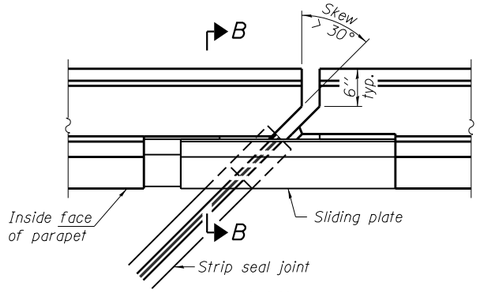
REMOVAL DETAILS
SN 054-0039

SCALE: | SHEET 4 OF 6 SHEETS | STA. | TO STA.

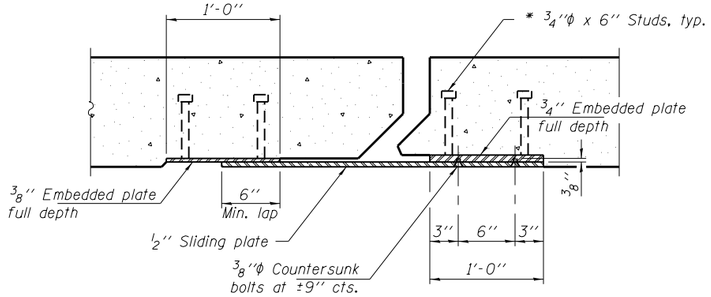
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	8
		CONTRACT NO. 72L15		
	ILLINOIS	FED. AID PROJECT		



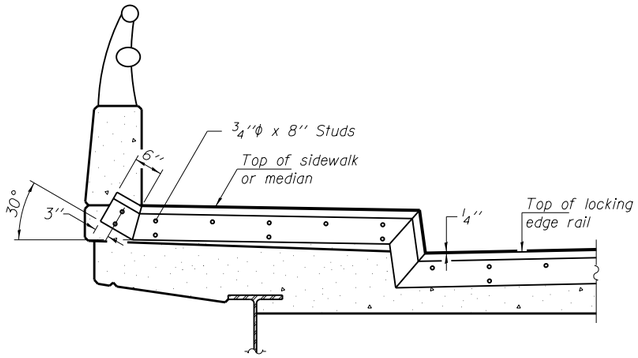
PLAN
(For skews ≤ 30°)



PLAN
(For skews > 30°)
Showing point block

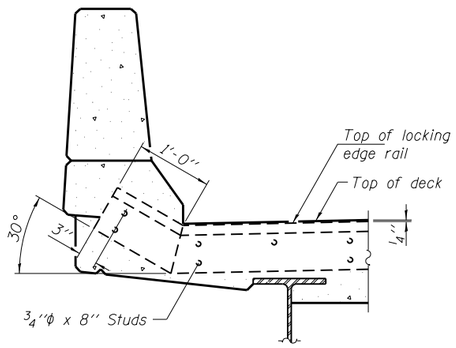


SECTION C-C

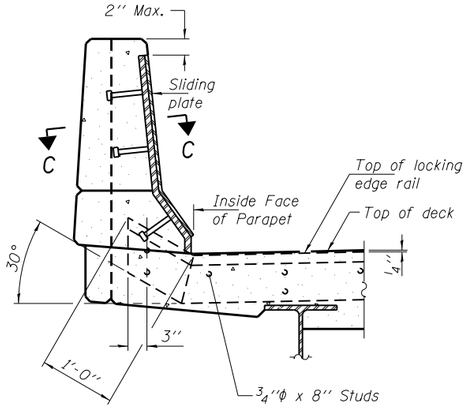


TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

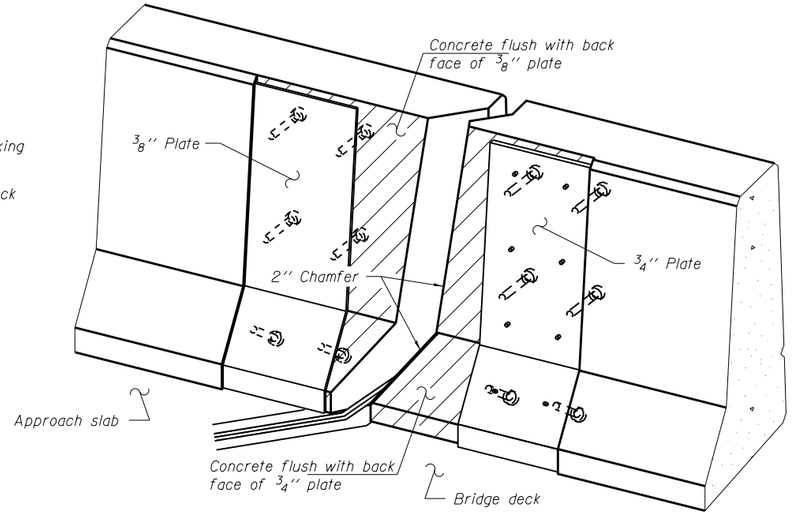
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



SECTION A-A



SECTION B-B



TRIMETRIC VIEW
(Showing back plates only)

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of ¼". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4½" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

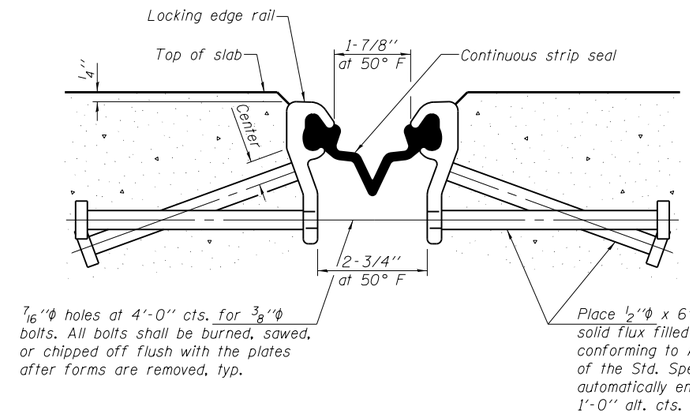
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

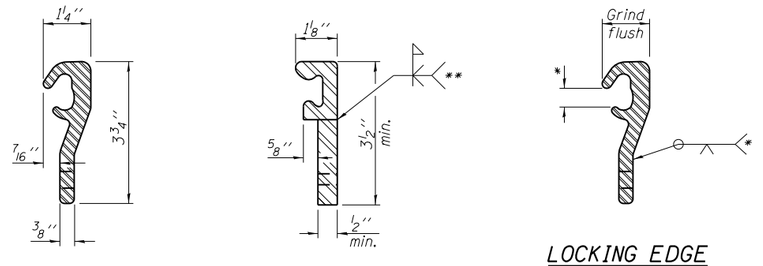
Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

34" F-shape barrier shown, 42" F-shape similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



SECTION THRU STRIP SEAL JOINT



LOCKING EDGE RAIL

* Omit weld at seal opening.

** Back gouge not required if complete joint penetration is verified by mock-up.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	144

MODEL: Default
FILE NAME: D:\OPERATIONS\Bridges\Bridgeplans_CAD\72L15 - 0540039 Jointsplatsheet.dgn

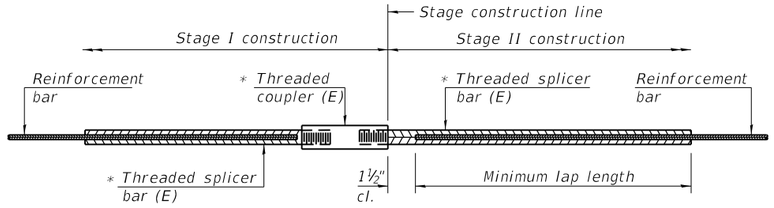
USER NAME = dudleybm	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/19/2019	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS
SN 054-0039

SCALE: | SHEET 5 OF 6 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	9
		CONTRACT NO. 72L15		
	ILLINOIS	FED. AID PROJECT		

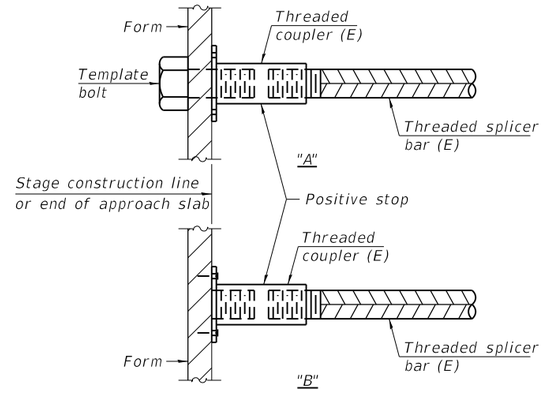


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1½" + thread length

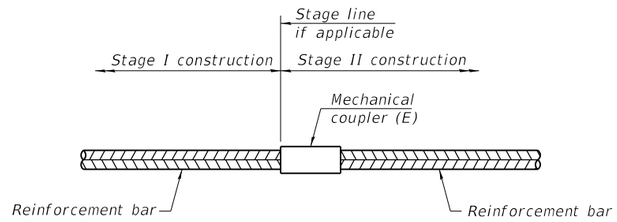
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
stage line (2 jts.)	#5	8	3' 0"



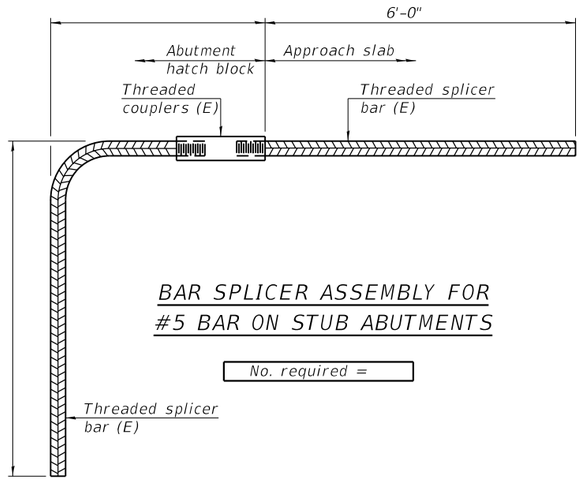
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

2-17-2017

USER NAME = dudleybm	DESIGNED -	REVISED -	
	DRAWN -	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/19/2019	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 054-0039

SCALE: | SHEET 6 OF 6 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	(54-2) BJR	LOGAN	10	10
		CONTRACT NO. 72L15		
	ILLINOIS	FED. AID PROJECT		

MODEL: Default
FILE NAME: G:\OPERATIONS\Bridges\BridgeplansCAD\72L15 - 0540039 (dntsplansheet.dgn