

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	1
		ILLINOIS	CONTRACT NO. 62W74	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

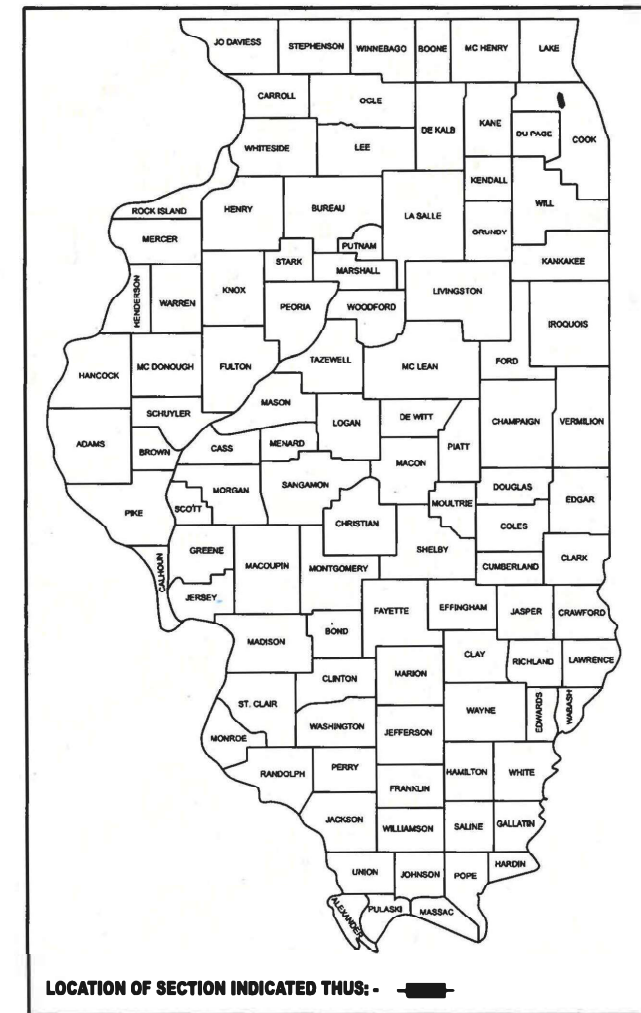
IMPROVEMENT IS LOCATED IN THE VILLAGE OF SCHAUMBURG

TRAFFIC DATA:
I-290 WEST FRONTAGE ROAD
 2022 ADT: 15,700
 POSTED SPEED LIMIT: 45 MPH
IL 72 (HIGGINS RD)
 2021 ADT: 45,300
 POSTED SPEED LIMIT: 40 MPH

PROPOSED HIGHWAY PLANS

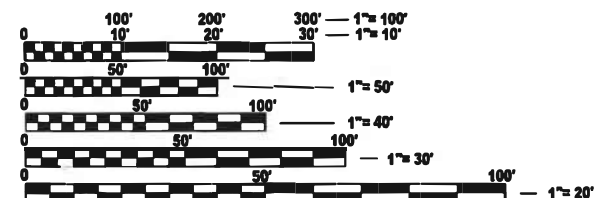
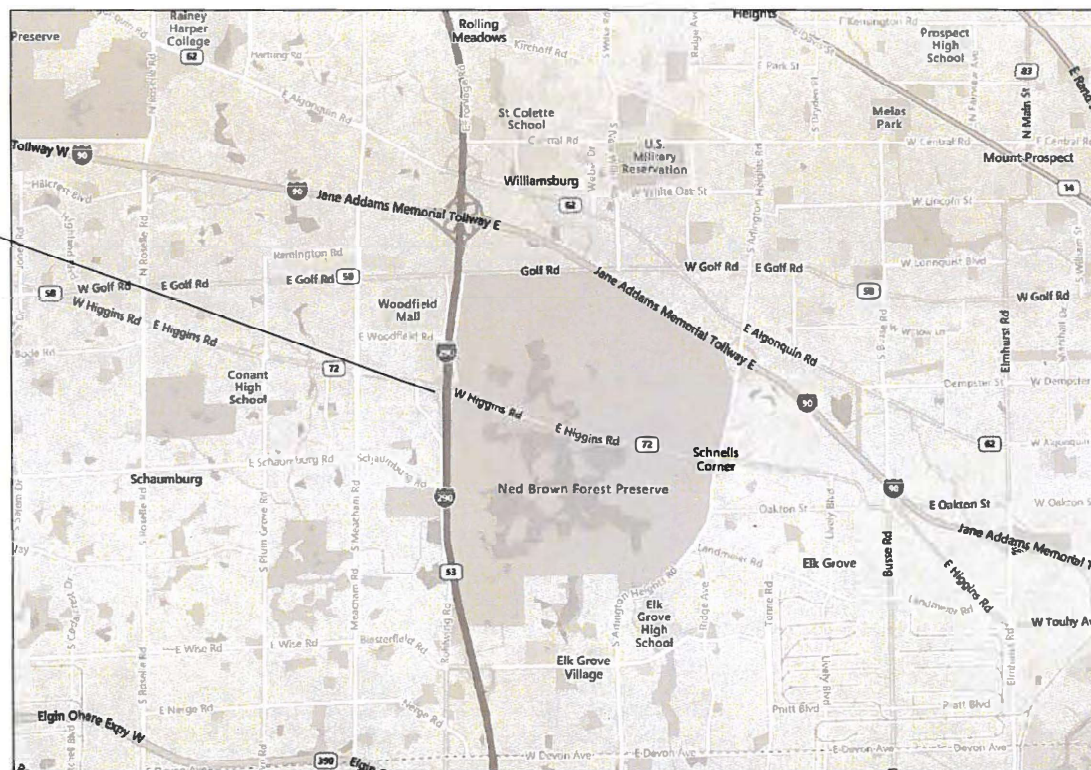
**FAI 290: I-290 WEST FRONTAGE ROAD
AT IL 72 (HIGGINS RD) SB RAMP
SECTION : 2024-955-PP
RESURFACING & PATCHING
COOK COUNTY**

D-91-158-23



IMPROVEMENT LOCATION:
I-290 WEST FRONTAGE ROAD
FROM IL 72 (HIGGINS ROAD) TO
SOUTH OF CORPORATE CROSSING ROAD

C-91-209-24



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: RODRIGO LEDEZMA (847) 705-4580
PROJECT MANAGER: J. ALAIN MIDY (847) 221-3056

CONTRACT NO. 62W74

SCHAUMBURG TOWNSHIP

GROSS LENGTH = 695 FT. = 0.131 MILE
NET LENGTH = 615 FT. = 0.116 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED March 18, 2024

[Signature]
REGIONAL ENGINEER

May 10, 2024 *[Signature]*
ENGINEER OF DESIGN AND ENVIRONMENT

May 10, 2024 *[Signature]*
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.

DESCRIPTION

STANDARD NO.

DESCRIPTION

- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- BUTT JOINTS SHALL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS AREA TRAFFIC FIELD TECHNICIAN, AT PATRICE.HARRIS@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.
- FRAMES AND LIDS/GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE PROJECT LIMITS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.
- RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.
- WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
- PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED OR PLATED STRUCTURES ACCORDING TO THE STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- THE "ARTERIAL ROAD INFORMATION SIGN (TC-22)" IS APPLICABLE ONLY TO ARTERIAL ROADS AND SHALL NOT BE APPLIED TO EXPRESSWAYS/TOLLWAYS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF MAINTENANCE.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4151 FOR EXPRESSWAYS A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR SHALL REQUEST AND GAIN APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT (847-705-4151 OR 4155) OR WWW.IDOTLCS.COM AT LEAST (24) HOURS IN ADVANCE OF ALL DAILY LANE, AND RAMP AND SHOULDER CLOSURES.

1	TITLE SHEET	000001-08	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
2	INDEX OF SHEETS, HIGHWAY STANDARDS & GENERAL NOTES	442201-03	CLASS C AND D PATCHES
3 - 4	SUMMARY OF QUANTITIES	606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
5	TYPICAL SECTIONS	606201-04	TYPE B GUTTER (INLET, OUTLET AND ENTRANCE)
6	ROADWAY PLANS	701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
7	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)	701106-02	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
8	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701400-12	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
9	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	701401-13	LANE CLOSURE, FREEWAY/EXPRESSWAY
10	HMA TAPER AT EDGE OF PCC PAVEMENT (BD-33)	701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > 45 MPH
11	ENTRANCE AND EXIT RAMP CLOSURE DETAILS (TC-08)	701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS > 45 MPH
12	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE LANE AND MULT-LANE WEAVE (TC-09)	701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤ 40 MPH
13	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	701428-01	TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
14	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
15 - 16	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS (TC-12)	701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
17	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)	701901-09	TRAFFIC CONTROL DEVICES
18	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)		
19	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)		
20	TRAFFIC CONTROL FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES (TC-17)		
21	ARTERIAL ROAD INFORMATION SIGN (TC-22)		

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-290 WEST FRONTAGE RD (AT IL 72 (HIGGINS RD) SB RAMP)
 INDEX OF SHEETS, STATE STANDARD AND GENERAL NOTES**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	2
CONTRACT NO. 62W74				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES

ROADWAY
0005
100%
STATE

Code No.	Item	Unit	Total Quantity						
20101700	SUPPLEMENTAL WATERING	UNIT	1	1					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	24	24					
* 25200110	SODDING, SALT TOLERANT	SQ YD	24	24					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	2669	2669					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	166	166					
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	138	138					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	35	35					
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	215	215					
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	383	383					
42001300	PROTECTIVE COAT	SQ YD	25	25					
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	3675	3675					
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	30	30					
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	155	155					
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	20	20					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	30	30					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	50	50					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	30	30					
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	22	22					
60250200	CATCH BASINS TO BE ADJUSTED	EACH	3	3					
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	5	5					
67100100	MOBILIZATION	L SUM	1	1					

SUMMARY OF QUANTITIES

ROADWAY
0005
100%
STATE

Code No.	Item	Unit	Total Quantity						
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	397	397					
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	177	177					
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	39	39					
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	1179	1179					
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	223	223					
70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	115	115					
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	281	281					
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	132	132					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	39	39					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1179	1179					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	223	223					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	115	115					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	281	281					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	14	14					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	14	14					
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	902	902					
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	12	12					
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	112	112					
X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	105	105					

* SPECIALTY ITEM

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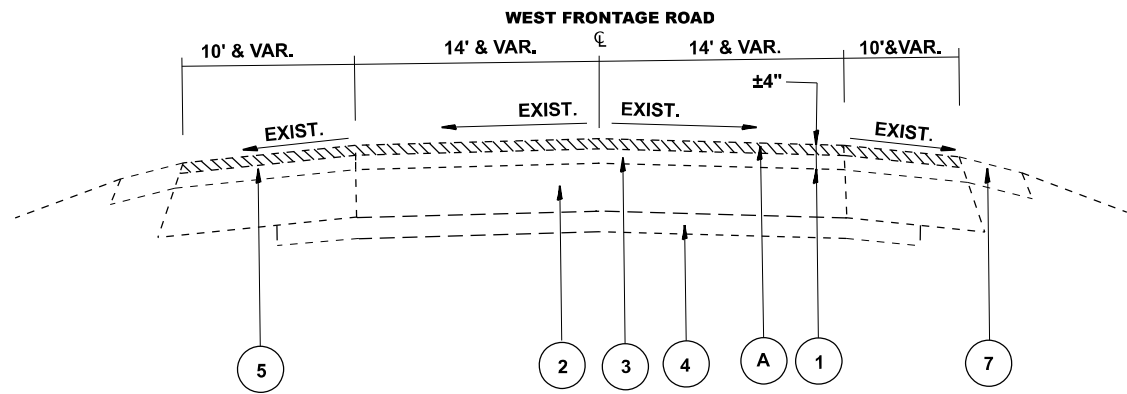
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

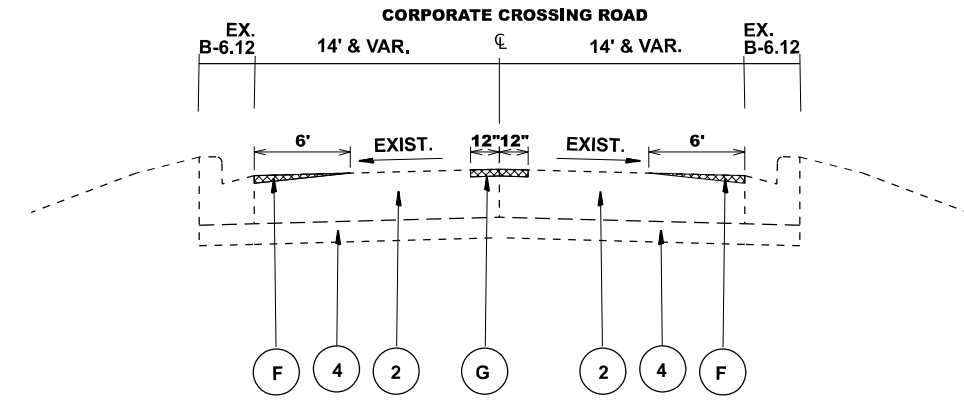
**I-290 WEST FRONTAGE RD (AT IL 72 (HIGGINS RD) SB RAMP)
SUMMARY OF QUANTITIES**

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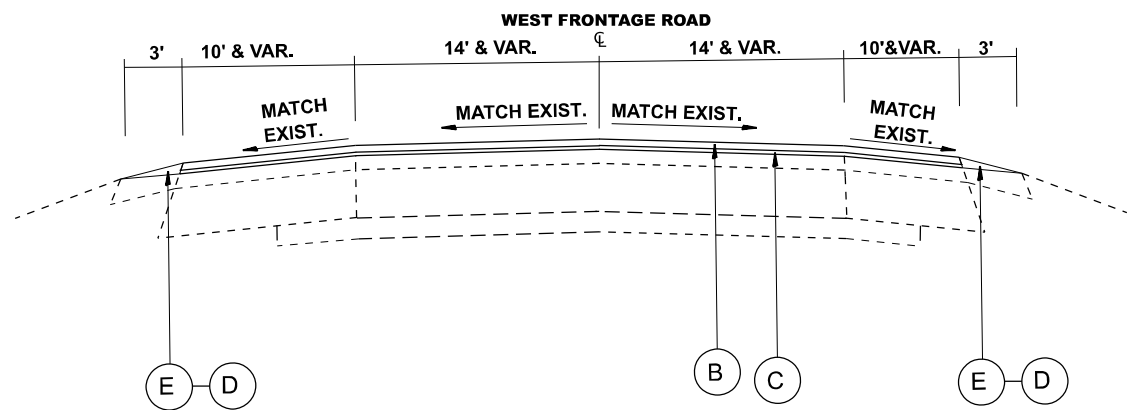
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290	2024-955-PP	COOK	21	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W74	



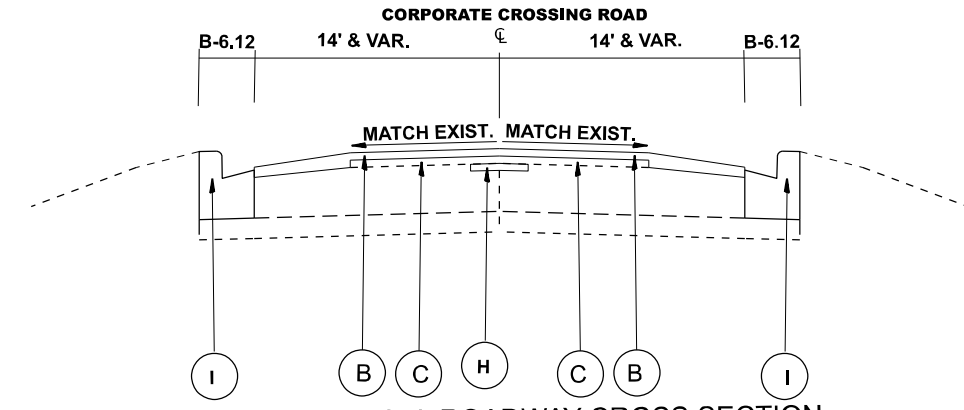
EXISTING TYPICAL ROADWAY CROSS SECTION
I-290 WEST FRONTAGE ROAD
STATION 17+74 TO 23+89



EXISTING TYPICAL ROADWAY CROSS SECTION
CORPORATE CROSSING ROAD
PCC SECTION



PROPOSED TYPICAL ROADWAY CROSS SECTION
I-290 WEST FRONTAGE ROAD
STATION 17+74 TO 23+89



PROPOSED TYPICAL ROADWAY CROSS SECTION
CORPORATE CROSSING ROAD
PCC SECTION

ROADWAY NOTE:

1. CONTRACTOR SHALL PATCH FIRST BEFORE MILLING

NOTE:

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING P.C.C. PAVEMENT, 10"
- ③ EXISTING HOT-MIX ASPHALT SURFACE AFTER MILLING, ±1 1/4"
- ④ EXISTING SUB-BASE GRAN. MATL., 6"
- ⑤ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑥ EXISTING SHOULDER RUMBLE STRIP
- ⑦ EXISTING AGGREGATE SHOULDER TYPE "B"

- A PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"
- B PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"
- C PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"
- D PROPOSED GRADING AND SHAPING SHOULDER
- E PROPOSED AGGREGATE WEDGE SHOULDER, TYPE "B"
- F PROPOSED PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)
- G PROPOSED LONGITUDINAL PARTIAL DEPTH REMOVAL 3"
- H PROPOSED LONGITUDINAL PARTIAL DEPTH PATCHING
- I PROPOSED COMB. CONC. CURB AND GUTTER REMOVAL & REPLACEMENT (AS DETERMINED BY THE ENGINEER)

MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE USES	PERCENT AIR VOIDS @ Ndes	
RESURFACING OVERLAY		
POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1 3/4"	3.5% AT 80 GYR.	QC/QA
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 1"	3.5% AT 50 GYR.	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER, IL-19.0), 10"	4% AT 90 GYR.	QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER, IL-19 mm), 4"	4% AT 90 GYR.	QC/QA
PARTIAL DEPTH PATCHING (HMA BINDER, IL-19 mm), 3"	4% AT 90 GYR.	QC/QA
LONGITUDINAL PARTIAL DEPTH PATCHING (HMA BINDER, IL-19 mm), 3"	4% AT 90 GYR.	QC/QA

QMP Designation: Pay for Performance (PFP); Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP)

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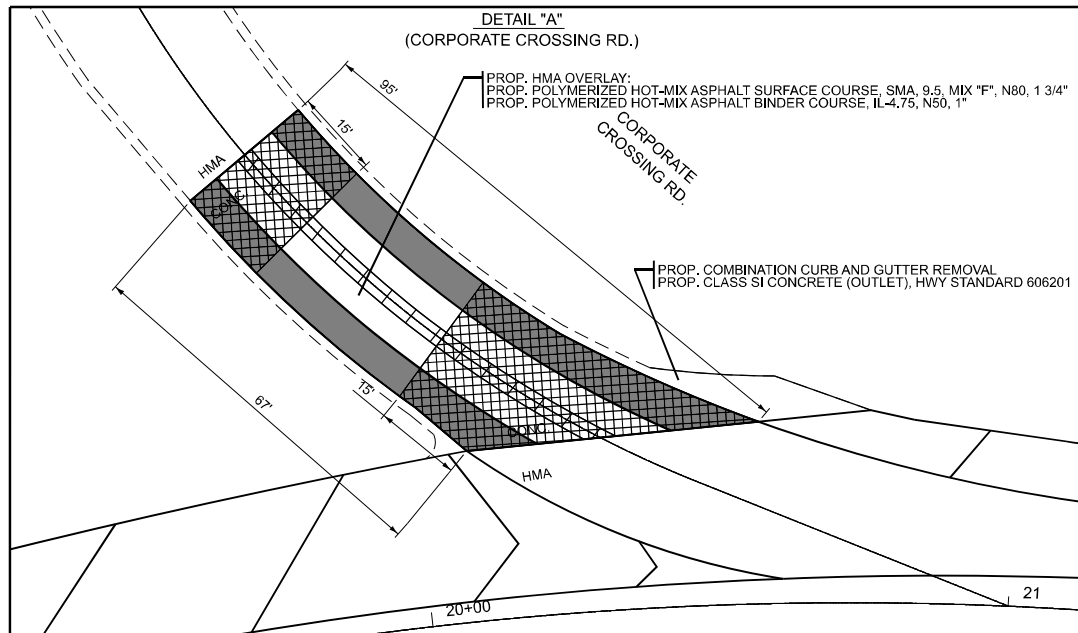
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-290 WEST FRONTAGE RD (AT IL 72 (HIGGINS RD) SB RAMP)
TYPICAL SECTIONS**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

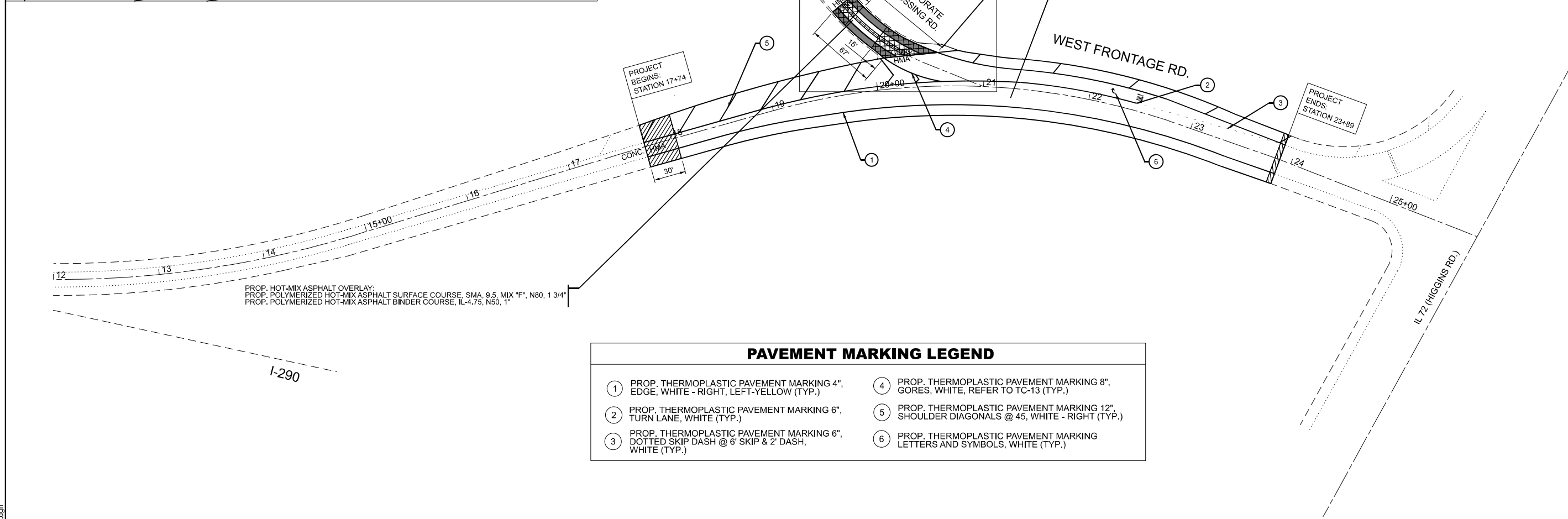
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62W74	

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- ROADWAY LEGEND**
- PROP. HMA SURFACE REMOVAL - BUTT JOINT, 4.5 FT (TYP)
 - PROP. PCC SURFACE REMOVAL - BUTT JOINT
 - PROP. PCC SURFACE REMOVAL (VARIABLE DEPTH)
 - PROP. LONGITUDINAL PARTIAL DEPTH REMOVAL, 3"
 - PROP. LONGITUDINAL PARTIAL DEPTH PATCHING

ROADWAY NOTES
 1. TRANSVERSE JOINT REPAIRS (CS#29) WILL BE DETERMINED BY ENGINEER



PAVEMENT MARKING LEGEND

① PROP. THERMOPLASTIC PAVEMENT MARKING 4", EDGE, WHITE - RIGHT, LEFT-YELLOW (TYP.)	④ PROP. THERMOPLASTIC PAVEMENT MARKING 8", GORES, WHITE, REFER TO TC-13 (TYP.)
② PROP. THERMOPLASTIC PAVEMENT MARKING 6", TURN LANE, WHITE (TYP.)	⑤ PROP. THERMOPLASTIC PAVEMENT MARKING 12", SHOULDER DIAGONALS @ 45, WHITE - RIGHT (TYP.)
③ PROP. THERMOPLASTIC PAVEMENT MARKING 6", DOTTED SKIP DASH @ 6' SKIP & 2' DASH, WHITE (TYP.)	⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS, WHITE (TYP.)

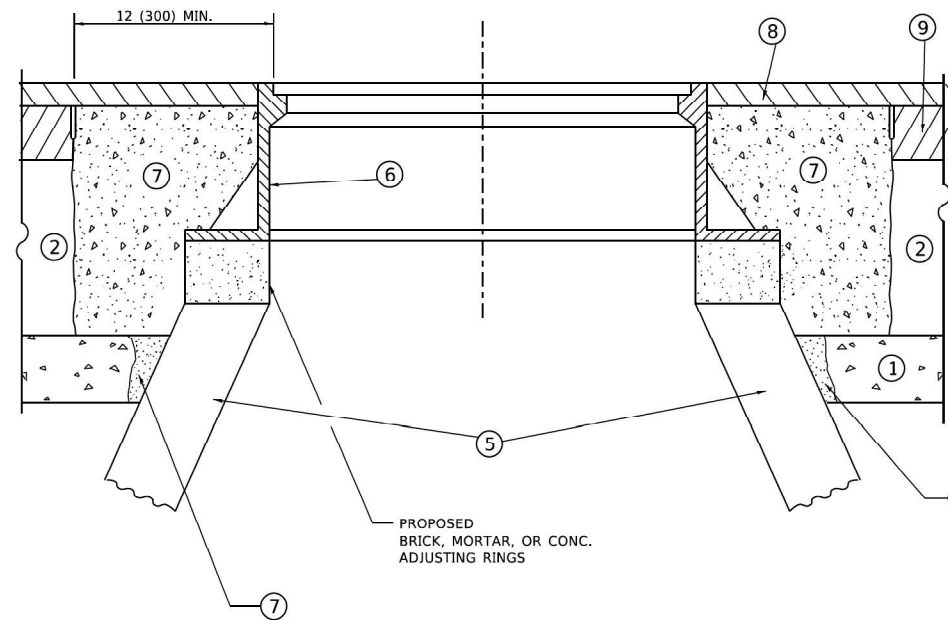
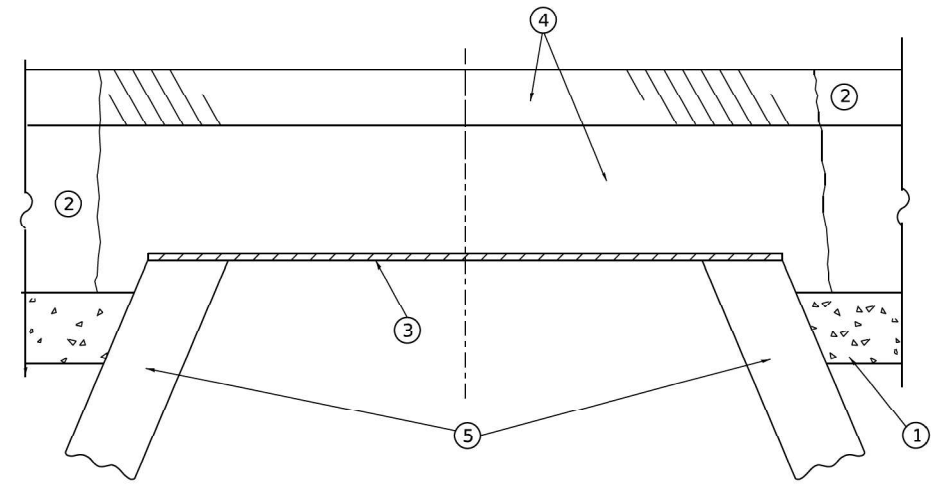
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-290 WEST FRONTAGE RD (AT IL 72 (HIGGINS RD) SB RAMP)
 ROADWAY PLAN**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 12+00.00 TO STA. 27+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	6
CONTRACT NO. 62W74				
ILLINOIS FED. AID PROJECT				



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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	DRAWN -	REVISED - R. BORO 12-06-11
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 3/26/2024	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: SHEET OF SHEETS STA. TO STA.

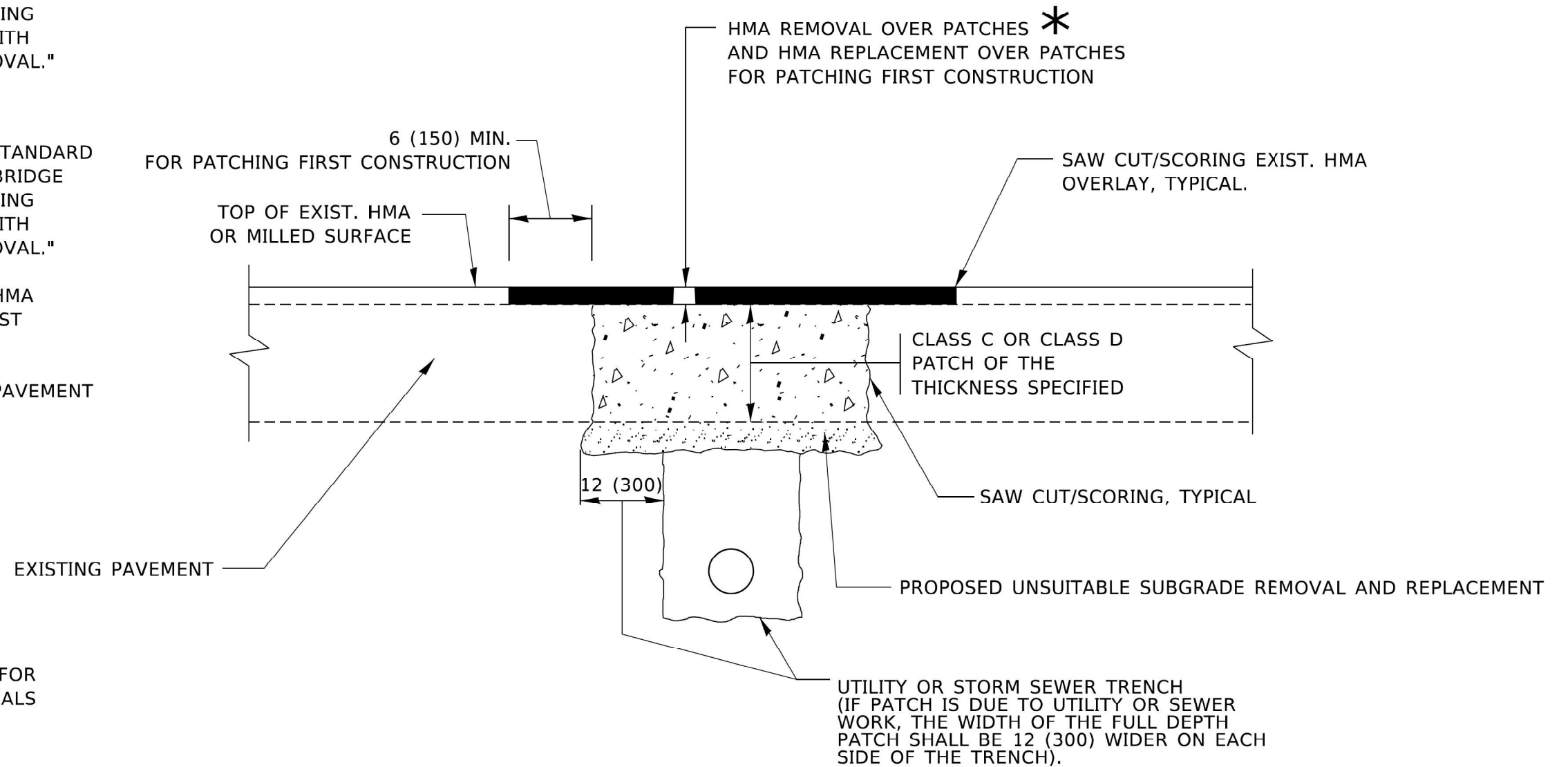
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	7
BD600-03 (BD-08)			CONTRACT NO. 62W74	
ILLINOIS FED. AID PROJECT				

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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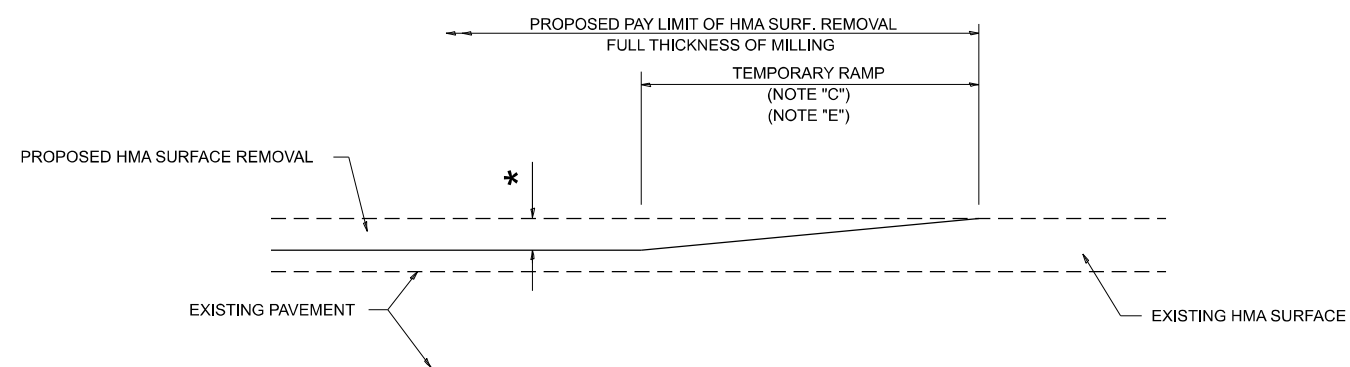
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	DRAWN -	REVISED - R. BORO 09-04-07
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED - K. ENG 10-27-08
PLOT DATE = 3/26/2024	DATE - 10-25-94	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: SHEET OF SHEETS STA. TO STA.

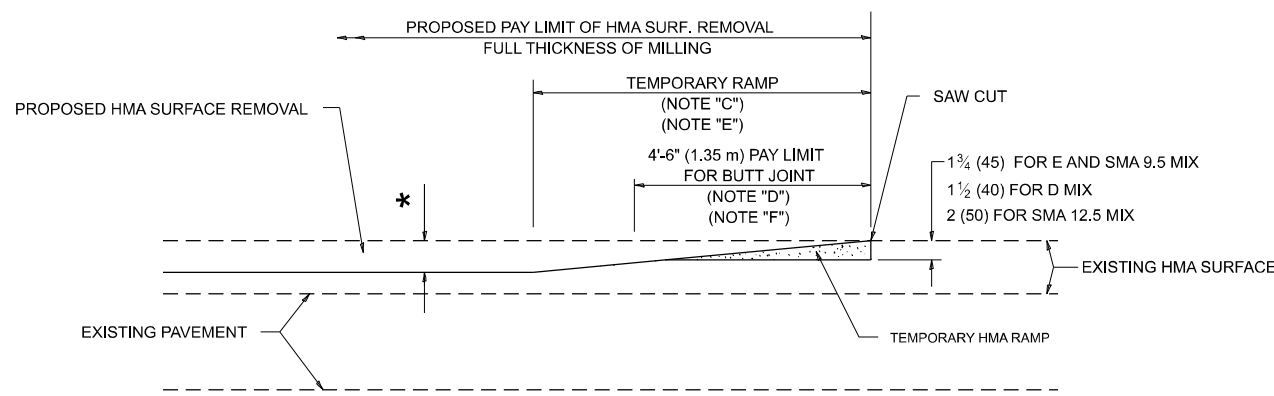
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290	2024-955-PP	COOK	21	8
BD400-04 (BD-22)			CONTRACT NO. 62W74	
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

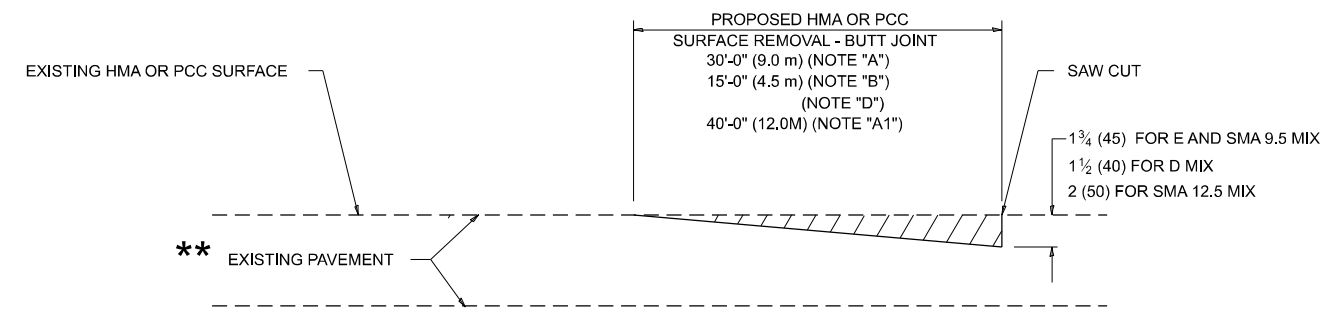


HMA CONSTRUCTED TEMPORARY RAMP

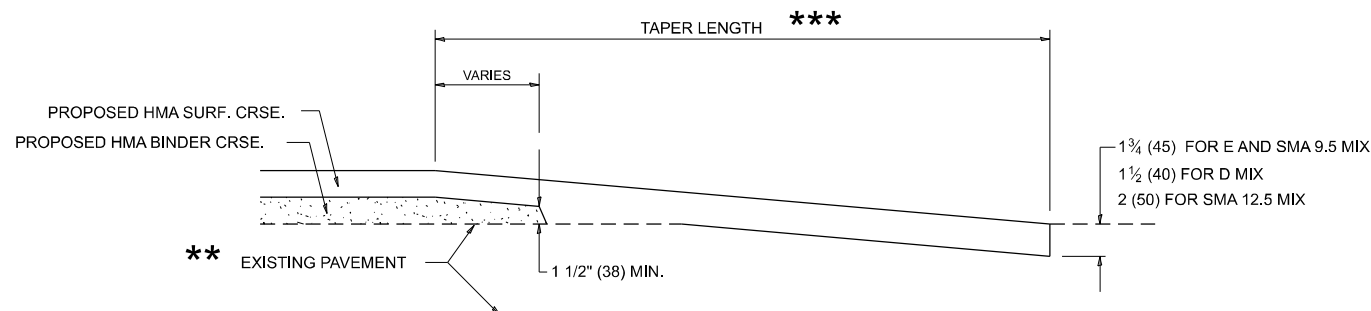
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

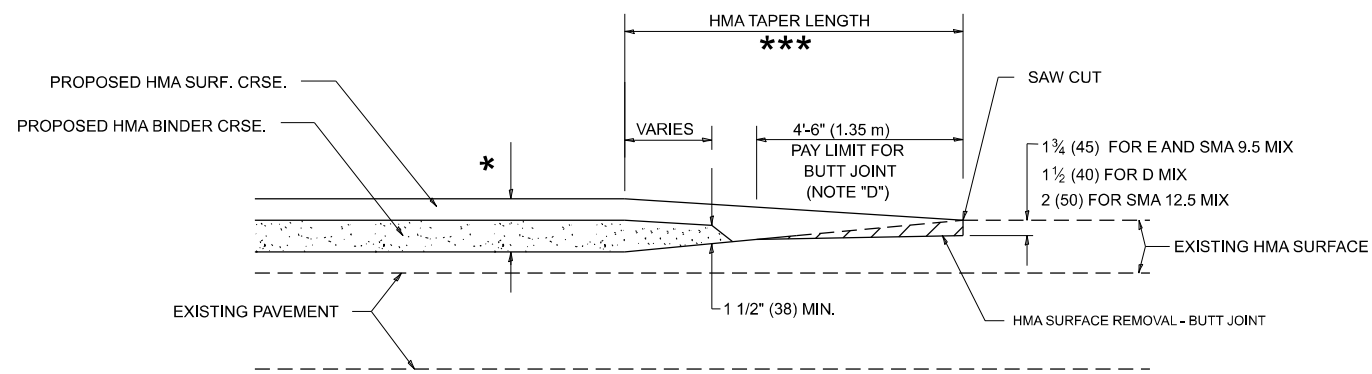
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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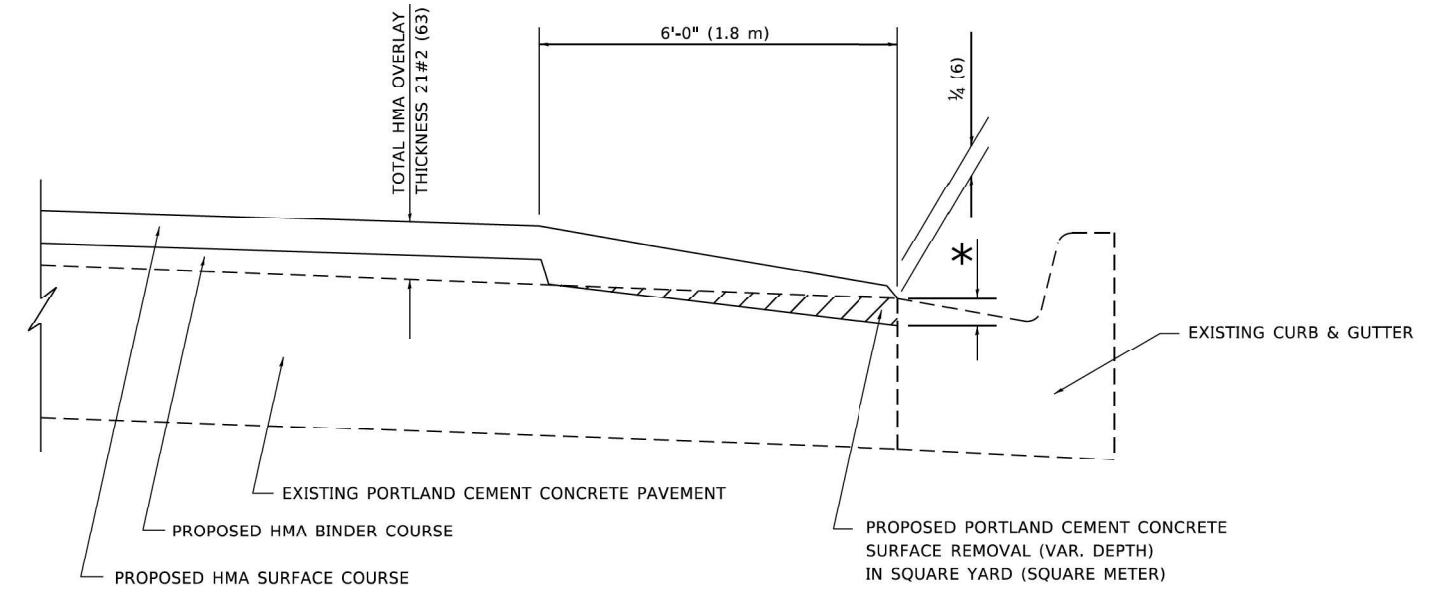
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PLOT SCALE = \$SCALE\$	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 3/26/2024	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	9
BD400-05 BD-32		CONTRACT NO. 62W74		
ILLINOIS FED. AID PROJECT				



**HMA TAPER AT
EDGE OF P.C.C. PAVEMENT**

HMA SURFACE COURSE		HMA BINDER COURSE		* MILLING AT GUTTER FLAG
MIX	THICKNESS	THICKNESS	THICKNESS	
D	1 1/2 (38)	1 (25)	1 1/4 (33)	
E OR SMA 9.5	1 3/4 (44)	3/4 (19)	1 1/2 (38)	

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

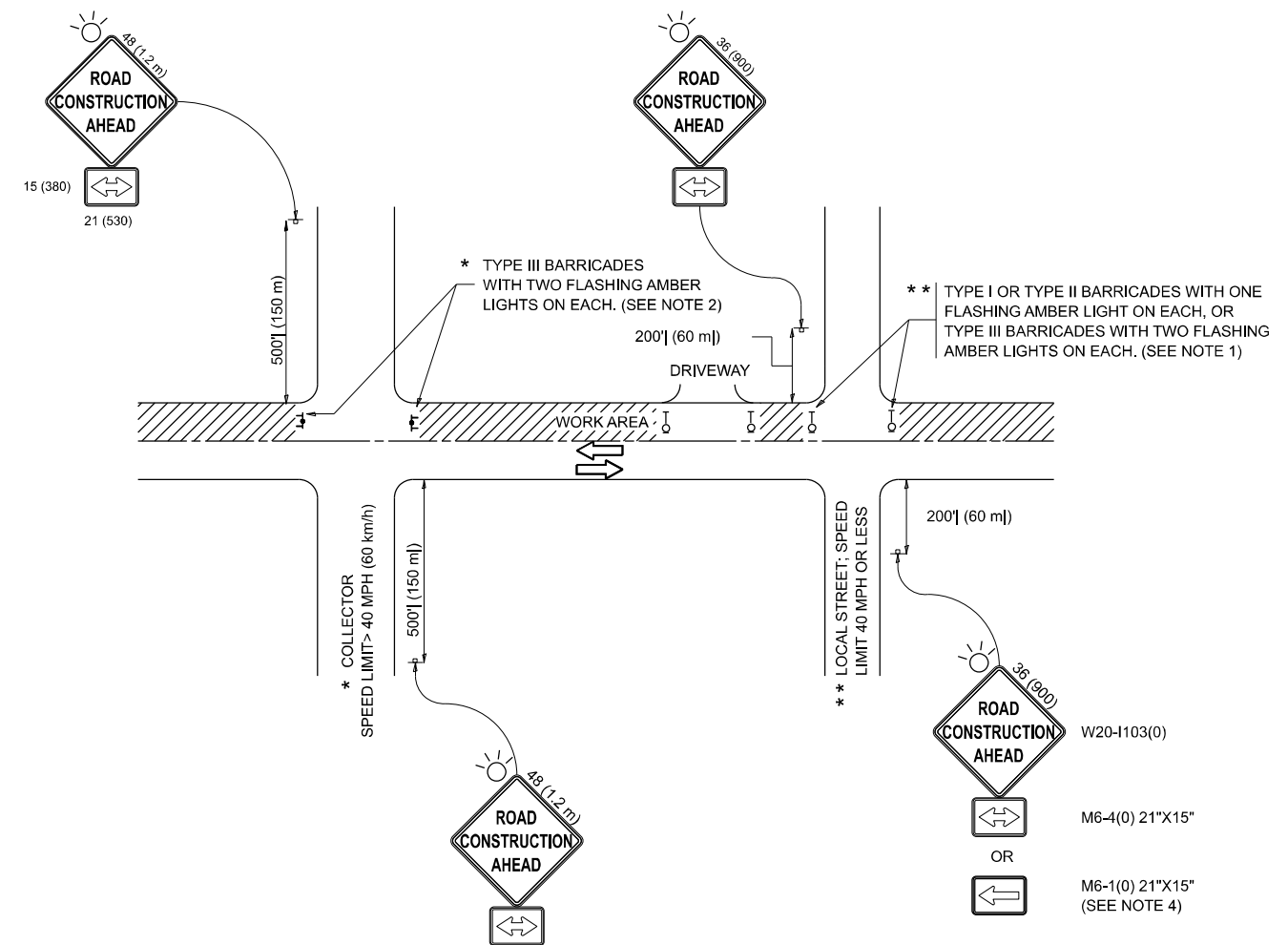
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PLOT SCALE = \$SCALE\$	CHECKED - A. ABBAS	REVISED - JP CHANG 07-08-16
PLOT DATE = 3/26/2024	DATE - 09-10-94	REVISED - K. SMITH 11-18-22

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

HMA TAPER AT EDGE OF P.C.C. PAVEMENT			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	10
BD400-06		BD-33	CONTRACT NO. 62W74	
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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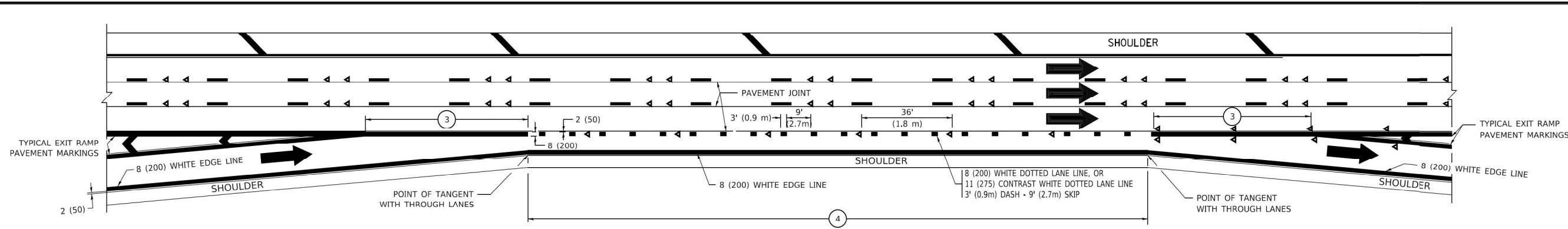
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PLOT DATE = 3/26/2024	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

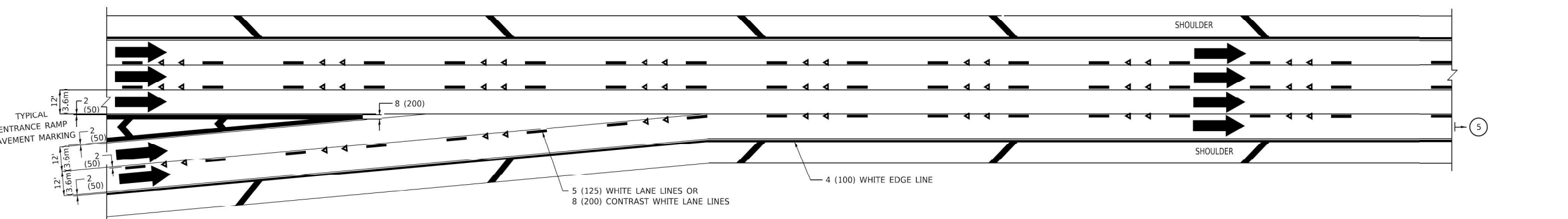
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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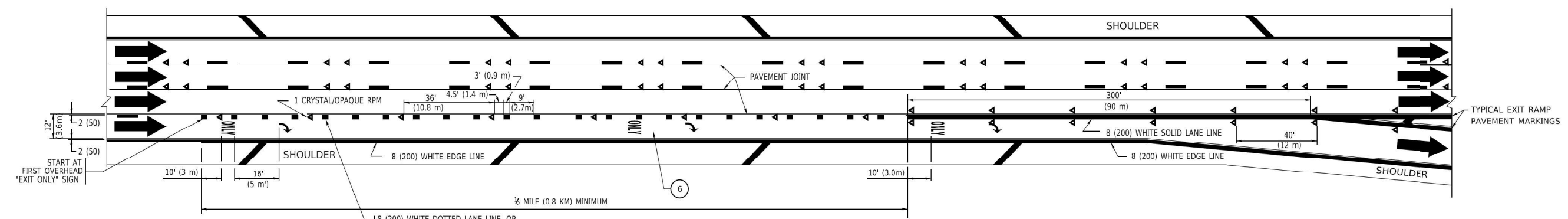
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290	2024-955-PP	COOK	21	13
TC-10			CONTRACT NO. 62W74	
ILLINOIS FED. AID PROJECT				



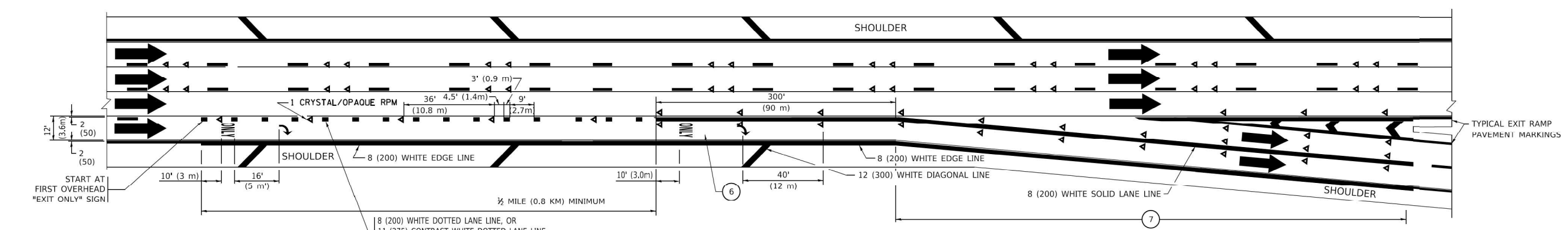
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES:**
- 3 OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - 4 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - 5 FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - 6 ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - 7 CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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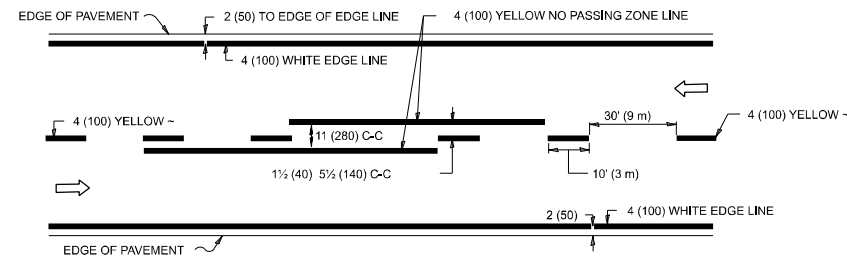
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PLOT DATE = 3/26/2024	DATE - 01-90	REVISED - S.P.B. 01-10
		REVISED - M.D. 09-17

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

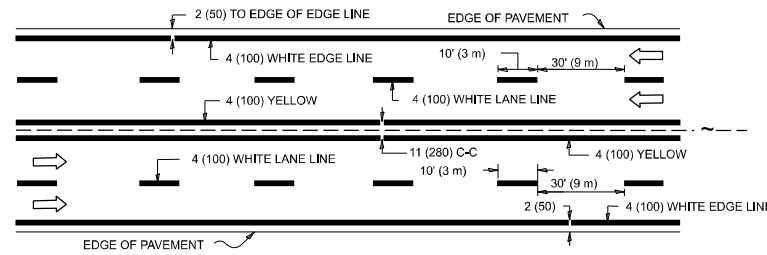
**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

SCALE: SHEET OF SHEETS STA. TO STA.

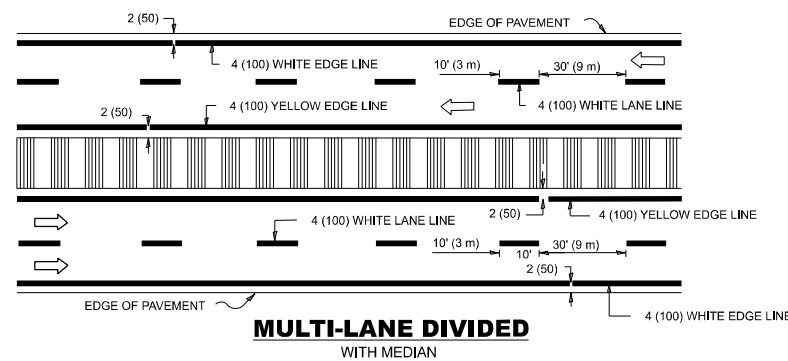
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	16
TC-12		CONTRACT NO. 62W74		
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

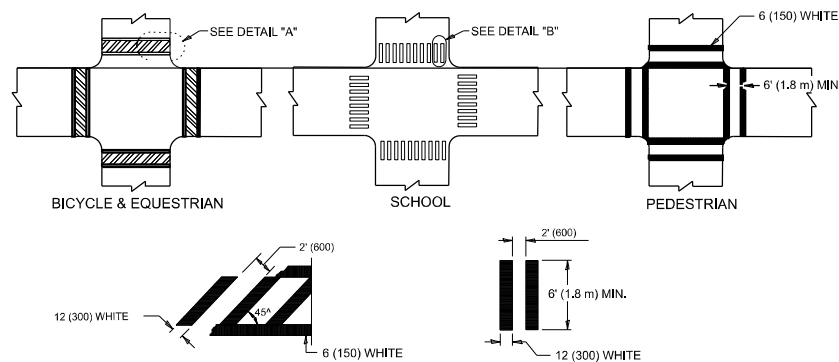


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

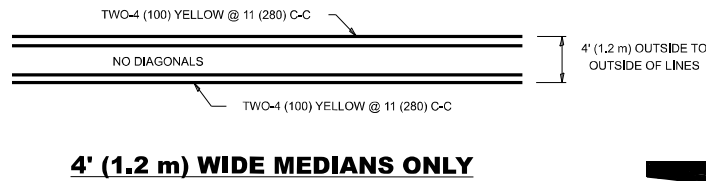


DETAIL "A"

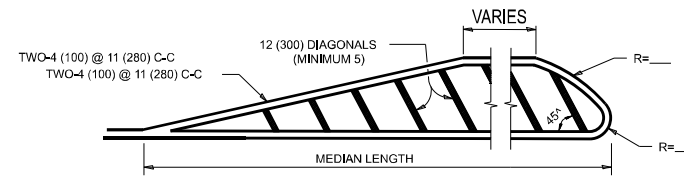
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

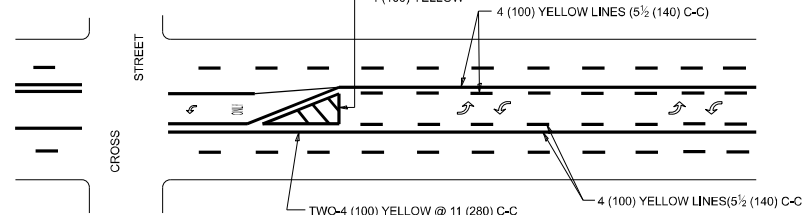


4' (1.2 m) WIDE MEDIANS ONLY



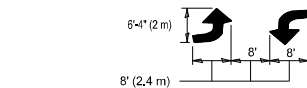
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



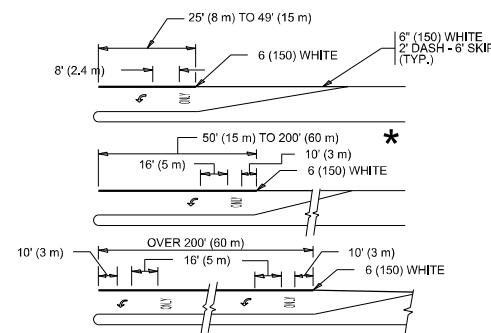
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

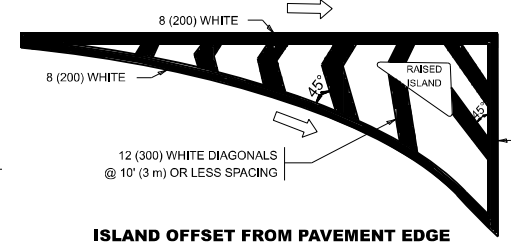


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

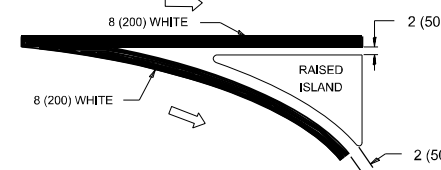
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

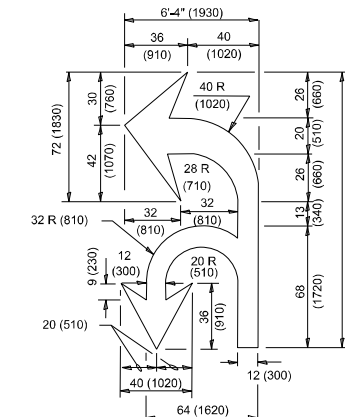


ISLAND OFFSET FROM PAVEMENT EDGE

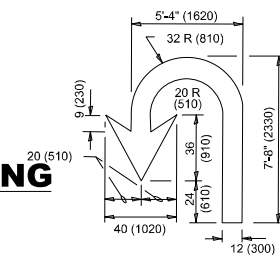


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE-FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK. IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 15 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = \$SCALES	DRAWN - C. JUCIUS 07-01-13	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 3/26/2024	CHECKED -	REVISED - C. JUCIUS 12-21-15
	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	17
TC-13		CONTRACT NO. 62W74		
		ILLINOIS FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

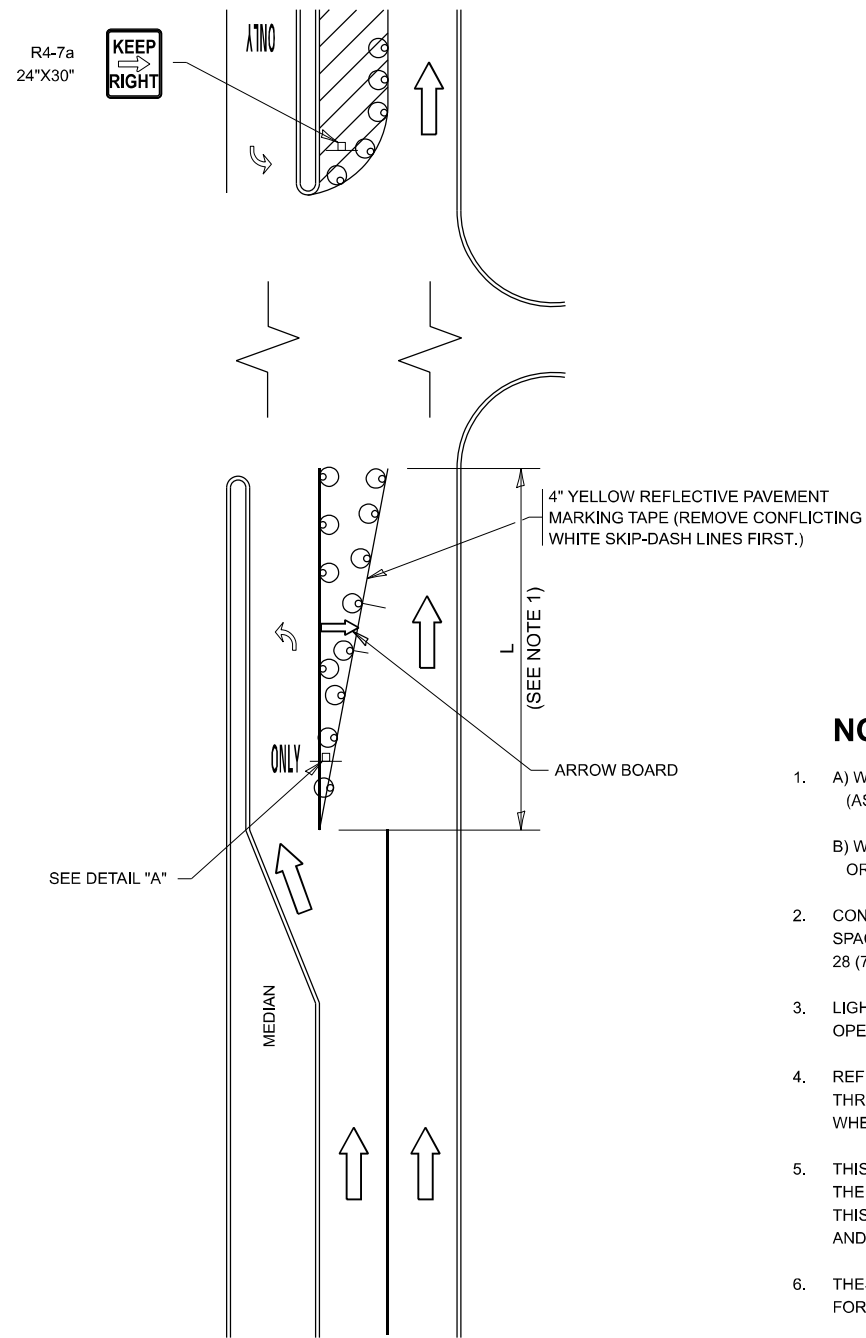


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

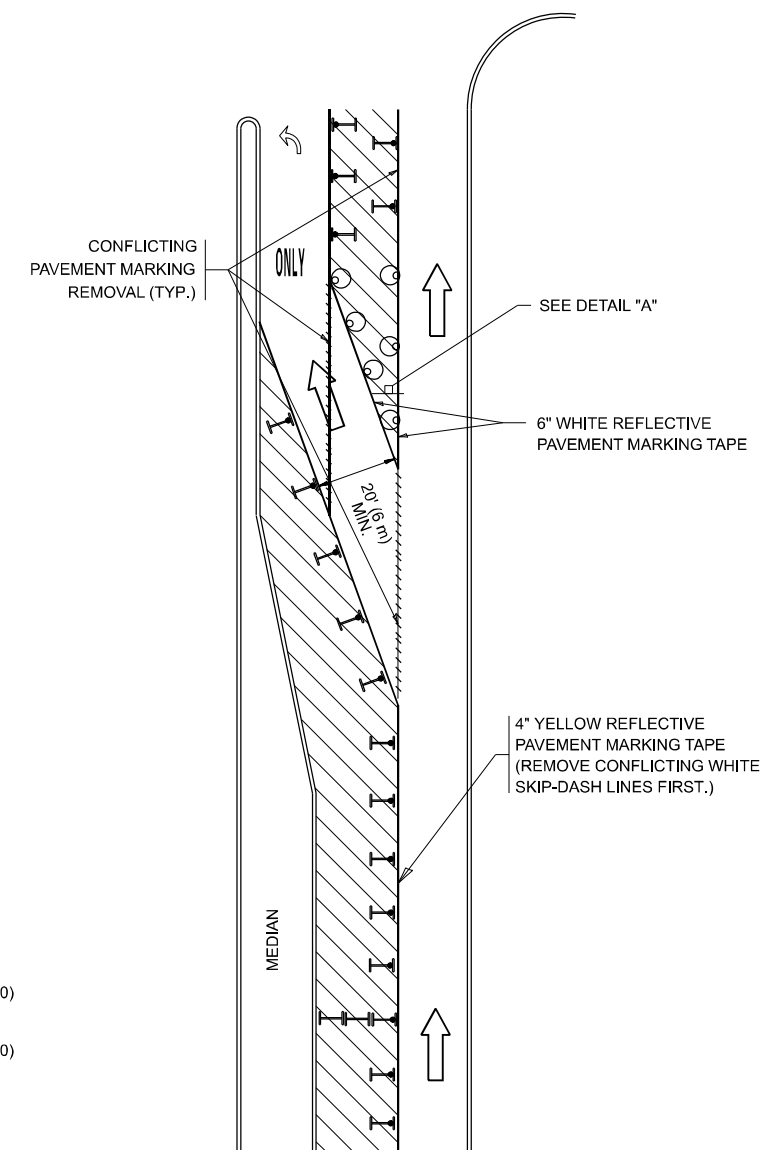









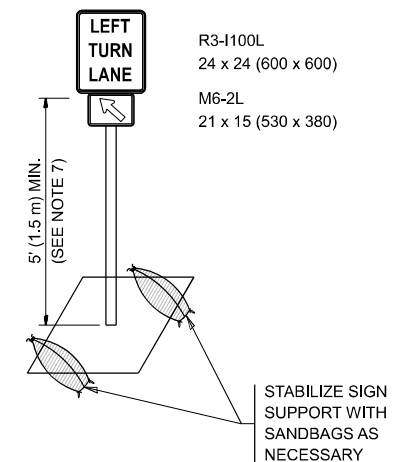
FIGURE 2

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  ARROW BOARD
-  TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  SIGN ASSEMBLY
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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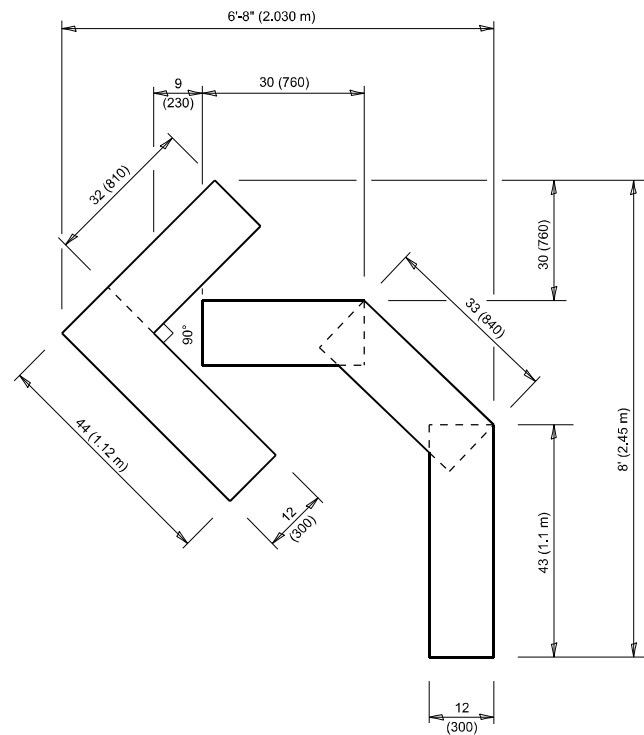
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	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = \$SCALES\$	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 3/26/2024	DATE - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
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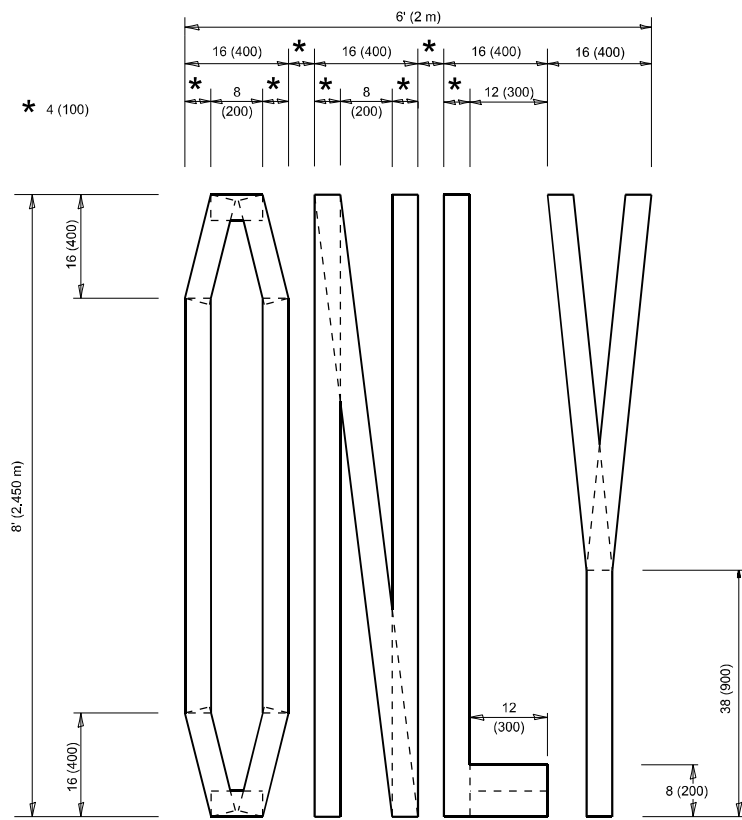
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

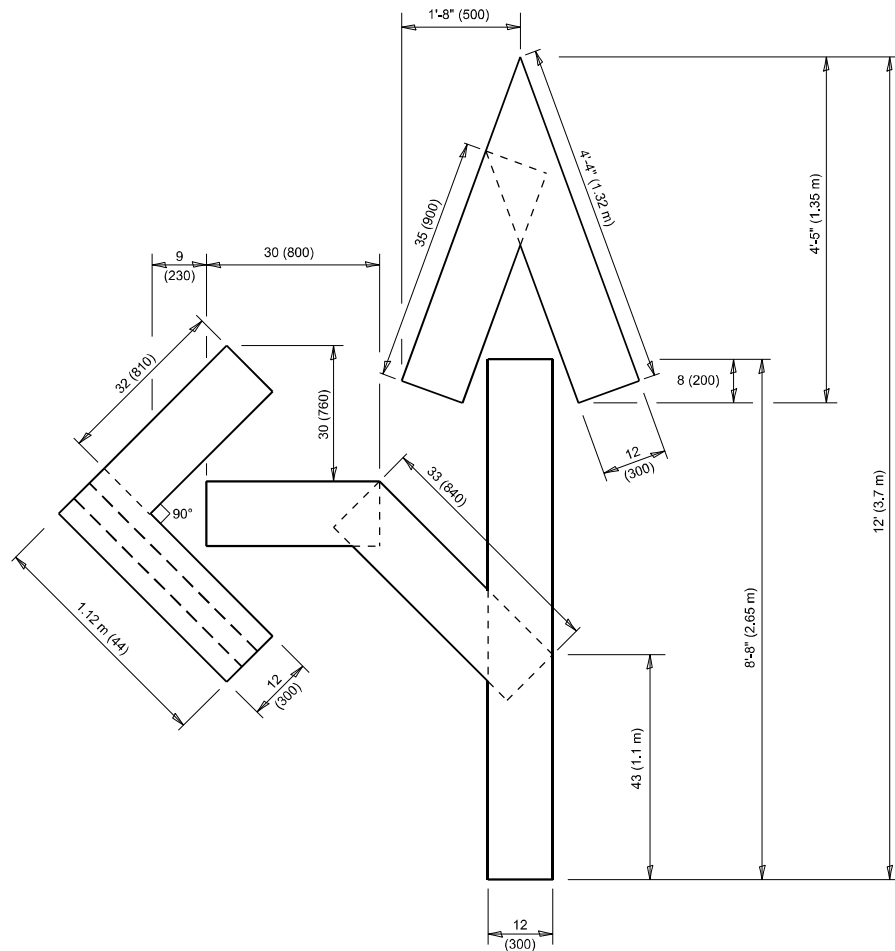
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	18
TC-14		CONTRACT NO. 62W74		
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

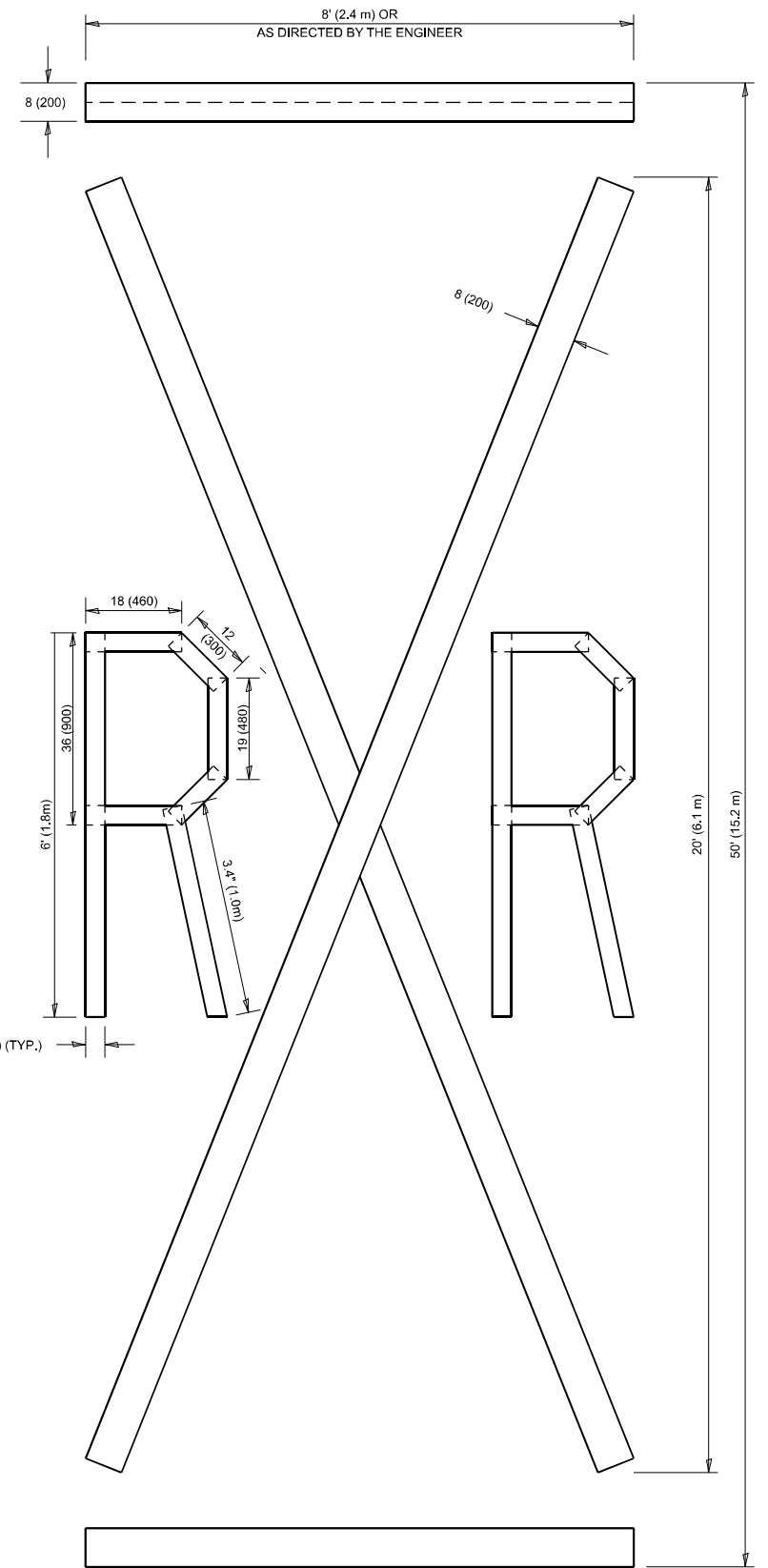


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
 IN LINEAR FEET OF 4" LINES TO MATCH THE
 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
 THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)
 unless otherwise shown.

MODEL: TC-16 (Sheet)
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USER NAME = ivan.diaz	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
PLOT SCALE = \$SCALES\$	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 3/26/2024	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

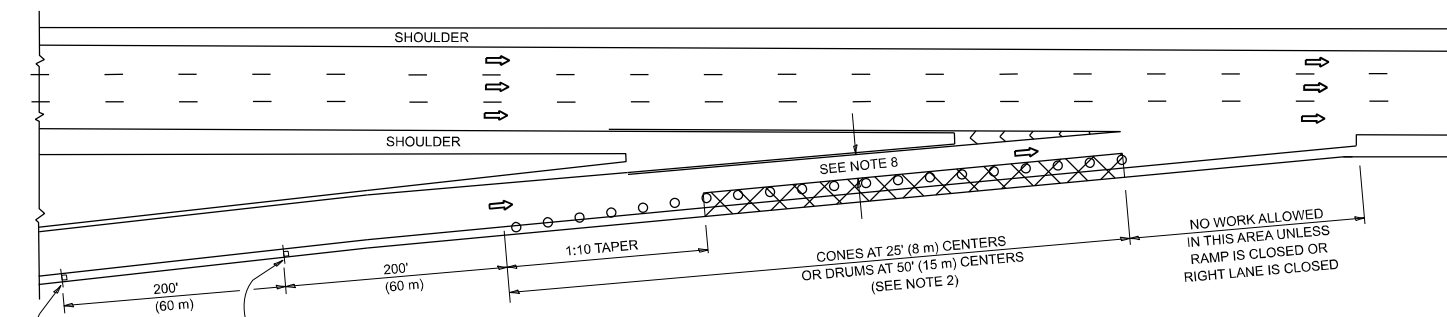
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

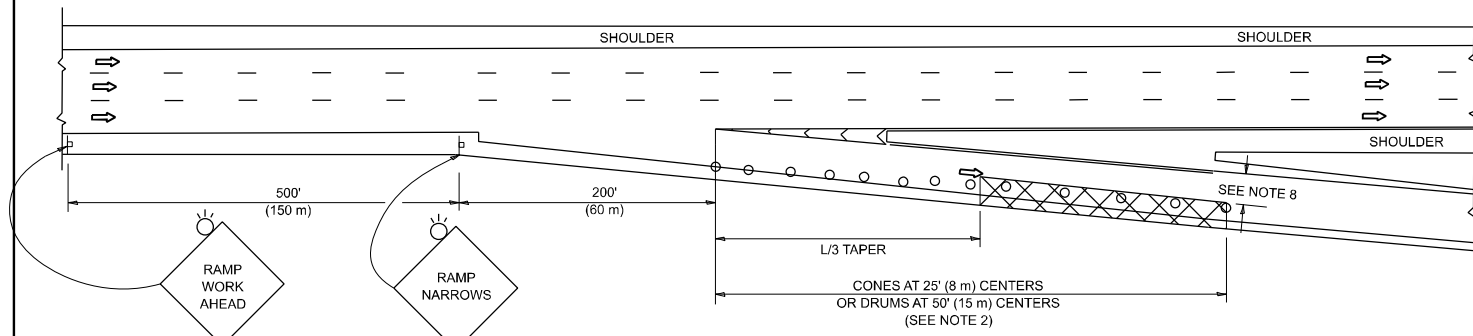
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TC-16		CONTRACT NO. 62W74		
ILLINOIS FED. AID PROJECT				

2024-955-PP

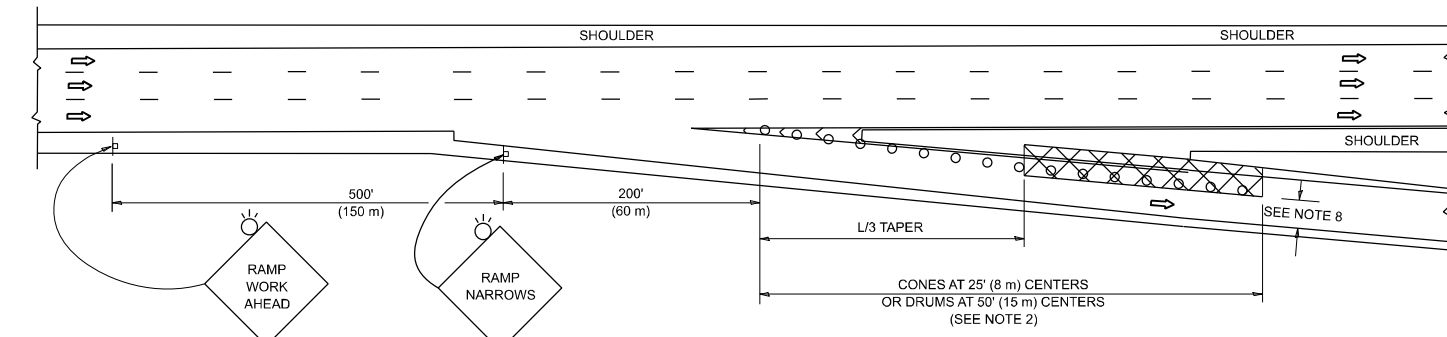
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

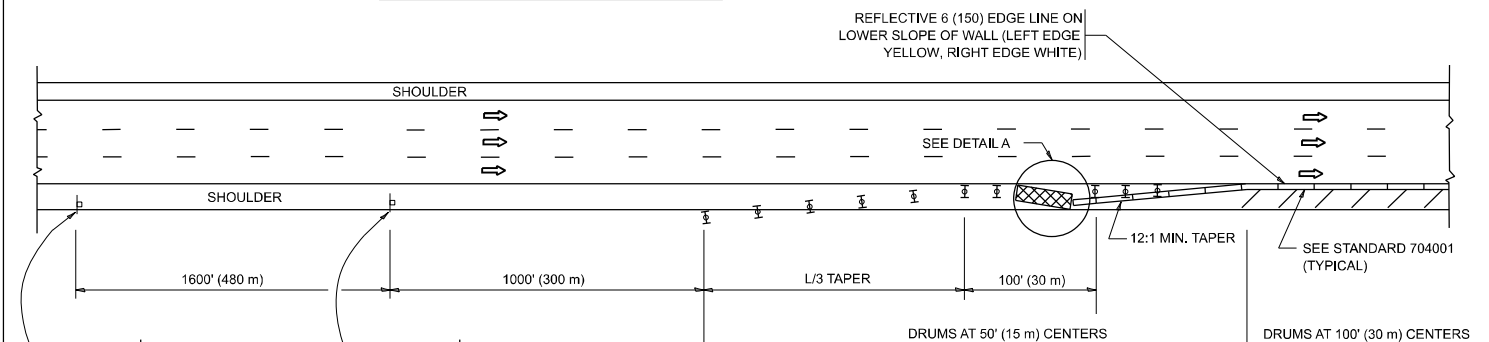
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES:

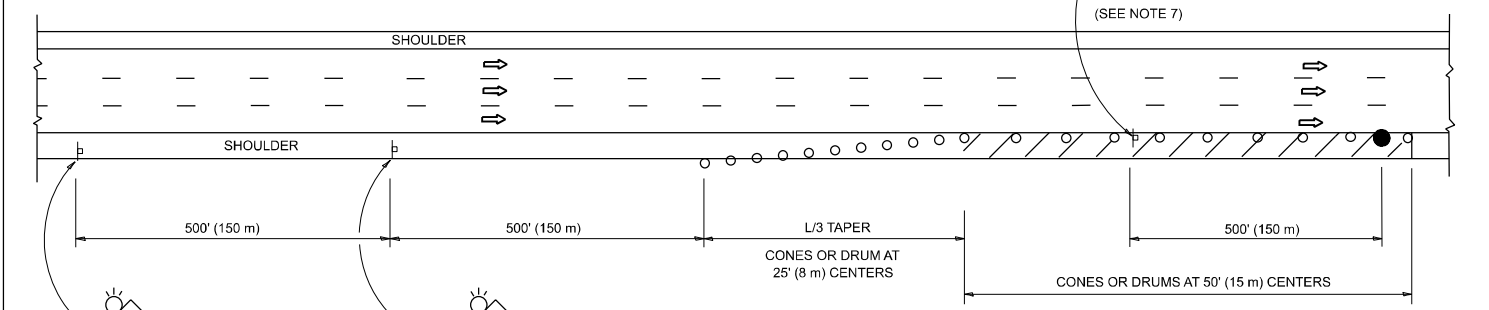
- THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS)	
S = NORMAL POSTED SPEED MPH (KM/H)	
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

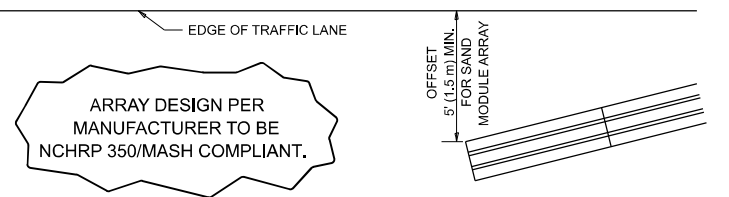


PERMANENT SHOULDER CLOSURE



TEMPORARY SHOULDER CLOSURE

- THIS DETAIL IS USED WHERE:
- VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)**

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - THE WORK ACTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

- 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

- TEMPORARY SHOULDER AND NON-SYSTEM INTERCHANGE PARTIAL CLOSURES ARE ALLOWED WEEKDAYS BETWEEN 9:00 A.M. AND 3:00 P.M. AND BETWEEN 7:00 P.M. AND 5:00 A.M. OR AS APPROVED BY THE EXPRESSWAY TRAFFIC OPERATIONS ENGINEER. PERMANENT SHOULDER AND PARTIAL RAMP CLOSURES WILL ONLY BE PERMITTED IF CALLED FOR IN THE PLANS OR AS APPROVED BY THE EXPRESSWAY OPERATIONS ENGINEER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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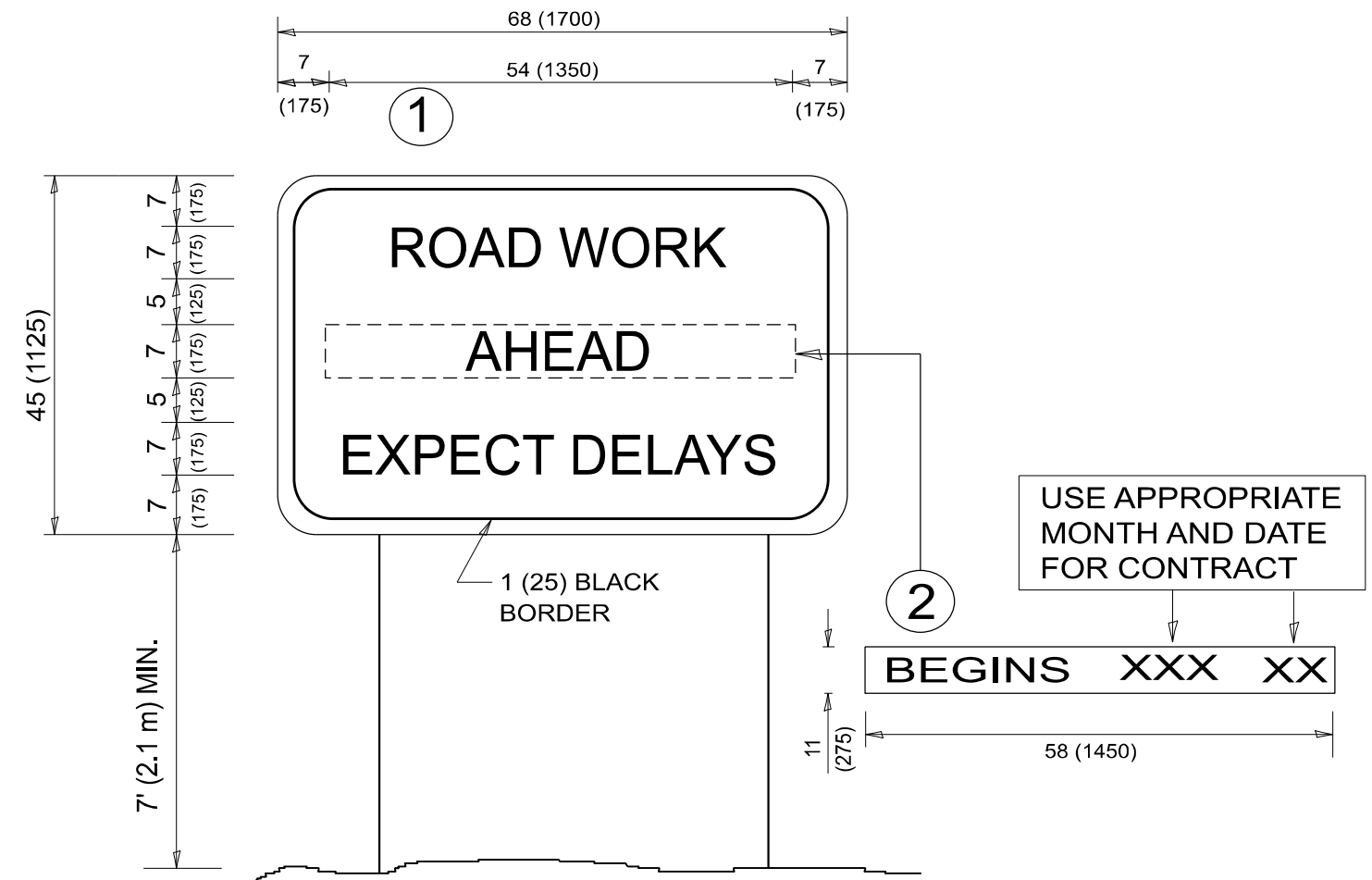
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	DRAWN - D.W.S.	REVISED - M.D. 06-13
PLOT SCALE = \$SCAL\$	CHECKED -	REVISED - M.D. 01-18
PLOT DATE = 3/26/2024	DATE - 11-96	REVISED - M.D. 10-20

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	20
TC-17			CONTRACT NO. 62W74	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

MODEL: TC-22 (Sheet)
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USER NAME = ivan.diaz	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/26/2024	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	2024-955-PP	COOK	21	21
TC-22			CONTRACT NO. 62W74	
ILLINOIS FED. AID PROJECT				