

ITEM NO. 02A

IDOT LETTING: JUNE 14, 2024

CN028
TOTAL SHEETS = 31

CONSTRUCTION PLANS

RESURFACE RUNWAY 18/36

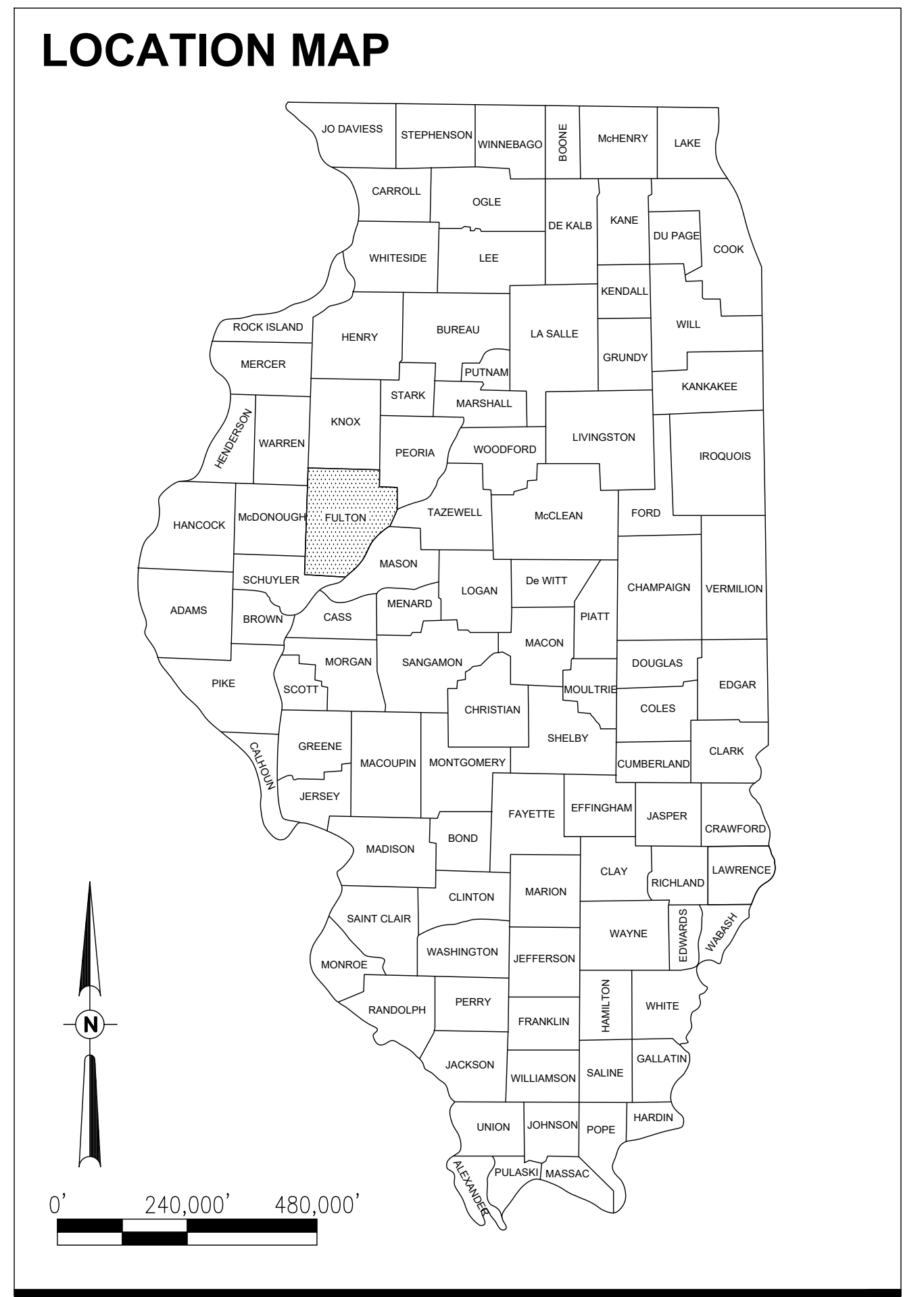
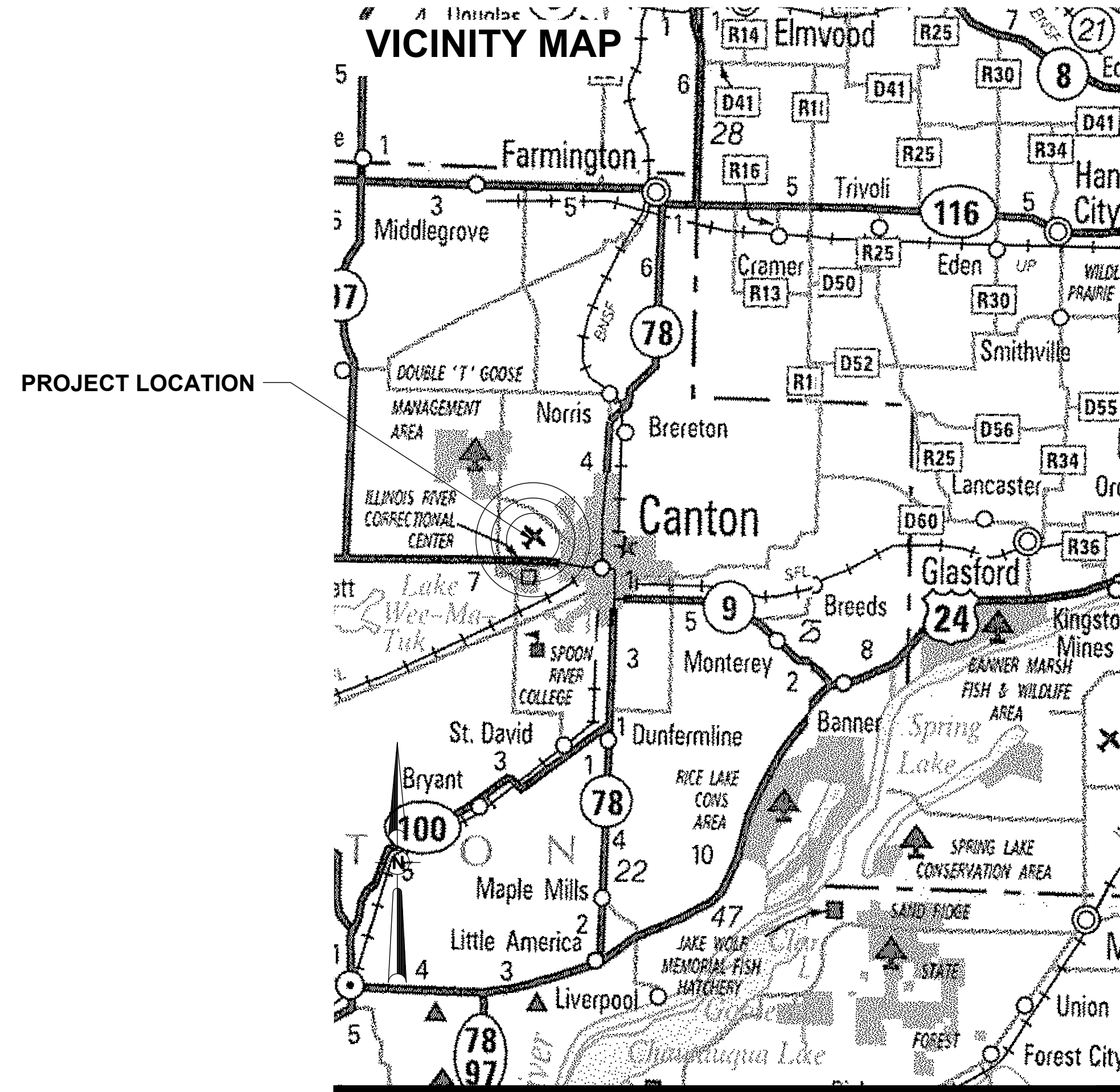
CANTON PARK DISTRICT
INGERSOLL AIRPORT (CTK)
CANTON, FULTON COUNTY, ILLINOIS

IDA PROJECT NO. CTK-5121
SBGP PROJECT NO. 3-17-SBGP-TBD

APRIL 19, 2024
(100% SUBMITTAL)

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

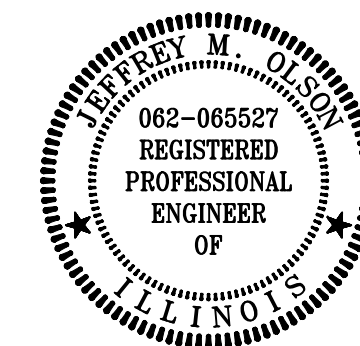


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No.	Issue/Description	Sheets Changed	Date	By



HANSON PROFESSIONAL SERVICES INC.
1525 S. 6th Street
Springfield, IL 62703
Telephone: 217-788-2450
Fax: 217-788-2503



EXPIRES 11/30/25

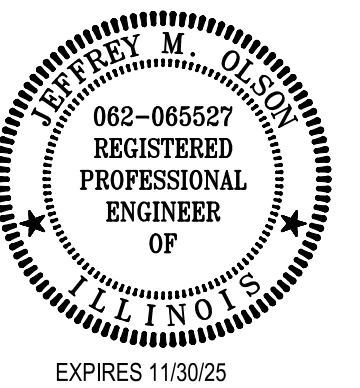
(Signature)
Jeffrey M. Olson, P.E.
Project Manager

04/19/2024
Date

INGERSOLL AIRPORT
CANTON PARK DISTRICT
250 S. Avenue D
Canton, Illinois 61520
Telephone 309 647 1345

(Signature)
Jon Johnson, Airport Manager

04/19/2024
Date



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: G-002-SQ0.DWG
DESIGN BY: AJL 03/21/24
DRAWN BY: AJL 03/21/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

**SHEET INDEX AND
SUMMARY OF
QUANTITIES**

GENERAL NOTES

- THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
- ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- THE LOCATION OF THE ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.
- THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
- THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
- APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- NPDES PERMIT - THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS NOT REQUIRED.
- MATERIAL CERTIFICATIONS - MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
- CERTIFIED PAYROLLS - THE RESIDENT ENGINEER CANNOT FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.

INDEX OF SHEETS	
SHEET NUMBER	SHEET TITLE
1	COVER SHEET
2	SHEET INDEX AND SUMMARY OF QUANTITIES
3	SCOPE OF WORK
4	SITE AND SAFETY PLAN - PHASE 1
5	SITE AND SAFETY PLAN - PHASE 2
6	SAFETY NOTES AND DETAILS
7	STORMWATER POLLUTION PREVENTION PLAN (SWPPP)
8	TYPICAL SECTIONS
9	CLEAN AND SEAL BITUMINOUS CRACKS SCHEDULE
10	PLAN AND PROFILE - SHEET 1
11	PLAN AND PROFILE - SHEET 2
12	PLAN AND PROFILE - SHEET 3
13	PLAN AND PROFILE - SHEET 4
14	MARKING PLAN - SHEET 1
15	MARKING PLAN - SHEET 2
16	MARKING PLAN - SHEET 3
17	MARKING DETAILS
18	CROSS-SECTION 1
19	CROSS-SECTION 2
20	CROSS-SECTION 3
21	CROSS-SECTION 4
22	CROSS-SECTION 5
23	CROSS-SECTION 6
24	CROSS-SECTION 7
25	CROSS-SECTION 8
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27	CROSS-SECTION 10
28	CROSS-SECTION 11
29	CROSS-SECTION 12
30	CROSS-SECTION 13
31	CROSS-SECTION 14

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AR150520	MOBILIZATION	L SUM	1	
AR150530	TRAFFIC MAINTENANCE	L SUM	1	
AR152480	SHOULDER ADJUSTMENT	SQ YD	4,866	
AR156510	SILT FENCE	FOOT	249	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	FOOT	19,385	
AR401614	BIT. SURF. CSE-METHOD II, SUPERPAVE	TON	6,533	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	672	
AR603510	BITUMINOUS TACK COAT	GALLON	9,468	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	17,445	
AR620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	5,426	
AR901510	SEEDING	ACRE	1.00	
AR908510	MULCHING	ACRE	1.00	

J.U.L.I.E. INFORMATION

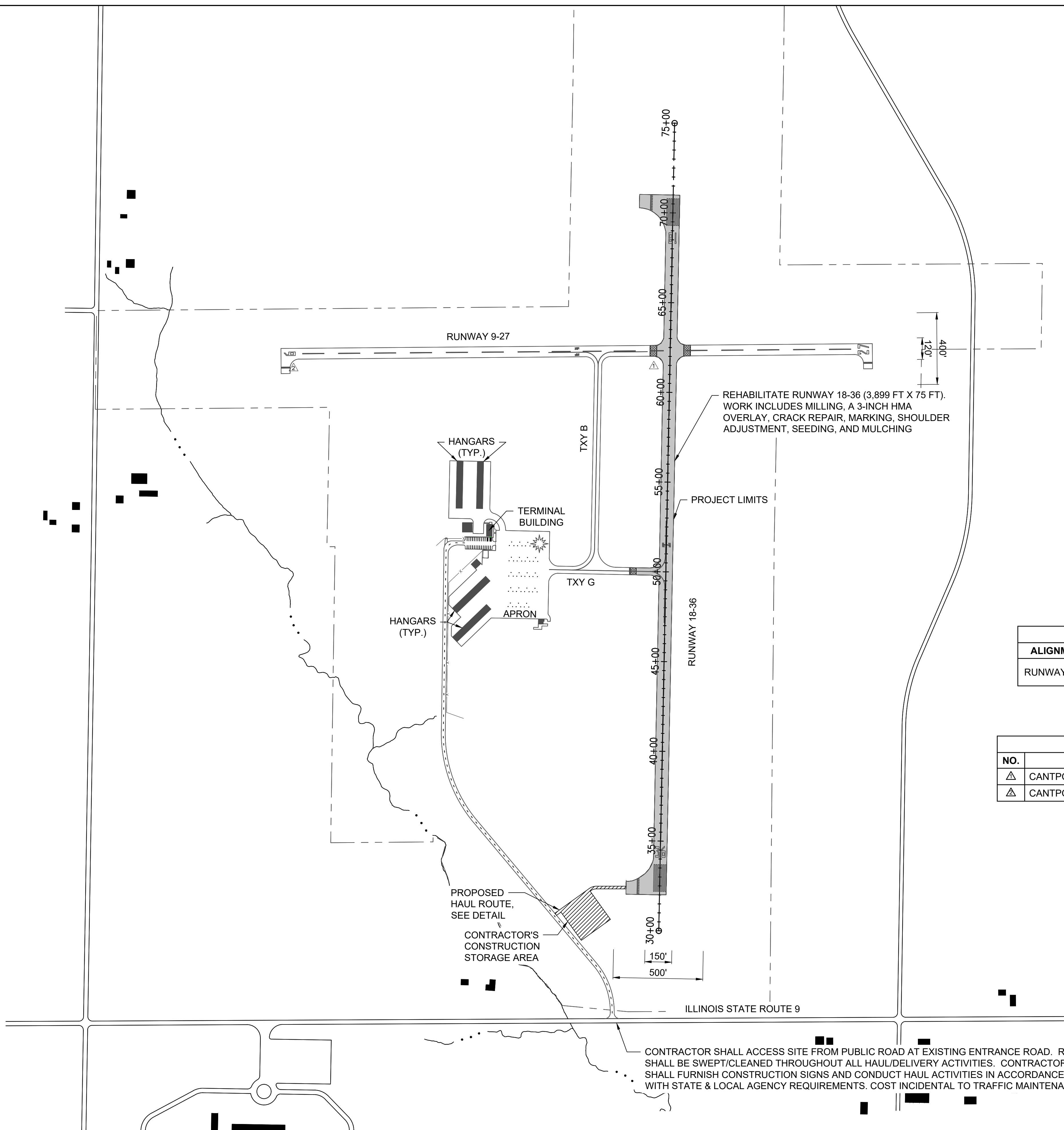
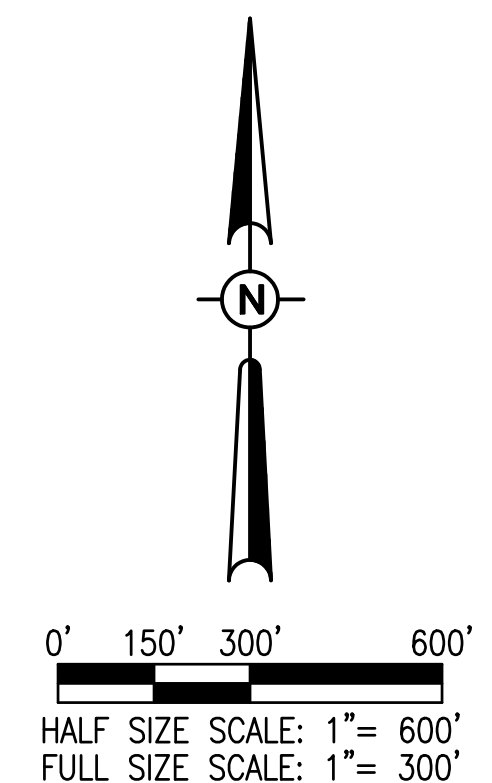
COUNTY _____ FULTON
CITY _____ CANTON
TOWNSHIP _____ 7N
RANGE _____ 4E
SECTION NO. _____ 20 & 29
ADDRESS _____ CANTON PARK DISTRICT
250 S. AVENUE D
CANTON, IL 61520

RESPONSIBILITY FOR EXISTING UTILITIES

THE LOCATION, SIZE AND/OR TYPE OF MATERIAL OF EXISTING UNDERGROUND OR OVERHEAD UTILITIES AS MAY BE INDICATED ON THESE CONSTRUCTION PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE PROJECT ENGINEER HAVE INDEPENDENTLY VERIFIED THIS INFORMATION AND NEITHER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, SUFFICIENCY OR COMPLETENESS OF THE INFORMATION AND GIVE NO EXPRESSED OR IMPLIED GUARANTEE THAT ANY CONDITIONS INDICATED ARE REPRESENTATIVE OF ACTUAL CONDITIONS TO BE ENCOUNTERED.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AND AGENCIES OF HIS CONSTRUCTION PLANS AND SHALL OBTAIN FROM EACH PARTY DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF ALL UTILITIES AND THE WORKING SCHEDULE OF ANY REMOVALS OR ADJUSTMENTS REQUIRED OF THE UTILITY. THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (PHONE 800-892-0123) TO ASSIST IN THE ABOVE.

THE CONTRACTOR SHALL PROTECT ANY FACILITIES TO THE SATISFACTION OF THE UTILITY OR OWNING-AGENCY WITH THE COST OF ANY REQUIRED PROTECTION TO BE INCIDENTAL TO THE CONTRACT. IN THE EVENT A UTILITY LINE OR SERVICE IS UNEXPECTEDLY ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE AND THE UTILITY COMPANY OR AGENCY OF JURISDICTION. ANY SUCH UTILITIES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO SERVICE AT ONCE.



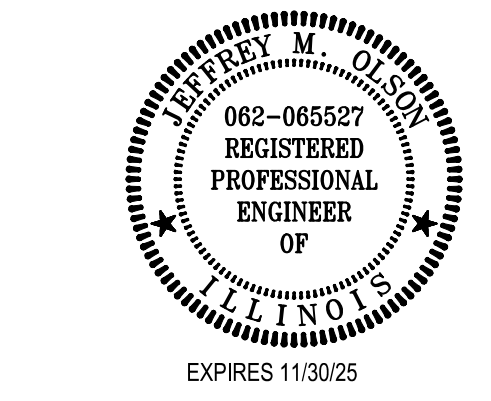
REHABILITATE RUNWAY 18-36 (3,899 FT X 75 FT). WORK INCLUDES MILLING, A 3-INCH HMA OVERLAY, CRACK REPAIR, MARKING, SHOULDER ADJUSTMENT, SEEDING, AND MULCHING

NOTES

1. VERTICAL COORDINATES ARE IN NGVD 29. HORIZONTAL COORDINATES ARE IN STATE PLANE NAD 83 ILLINOIS WEST.
2. STATIONS, OFFSETS AND ELEVATIONS SHOWN ARE IN FEET.
3. THE AIRPORT REFERENCE CODE FOR RUNWAY 18-36 IS B-II. RUNWAY 18-36 HAS A NON-PRECISION APPROACH WITH VISIBILITY NOT LOWER THAN 1 MILE.

ALIGNMENT DATA					
ALIGNMENT	LOCATION	STATION	NORTHING	EASTING	DESCRIPTION
RUNWAY 18/36	BEGIN	32+01.09	1418745.57	2322284.58	IRON END ROD
	END	70+99.30	1422643.04	2322361.00	IRON END ROD

CONTROL POINTS					
NO.	NAME	NORTHING	EASTING	ELEV.	DESCRIPTION
△	CANTPORT	1421688.774	2322253.202	677.59	DISC IN CONC. MONUMENT
△	CANTPORT AZ MK	1421677.720	2320246.077	682.00	DISC IN CONC. MONUMENT



RESURFACE RUNWAY 18-36

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

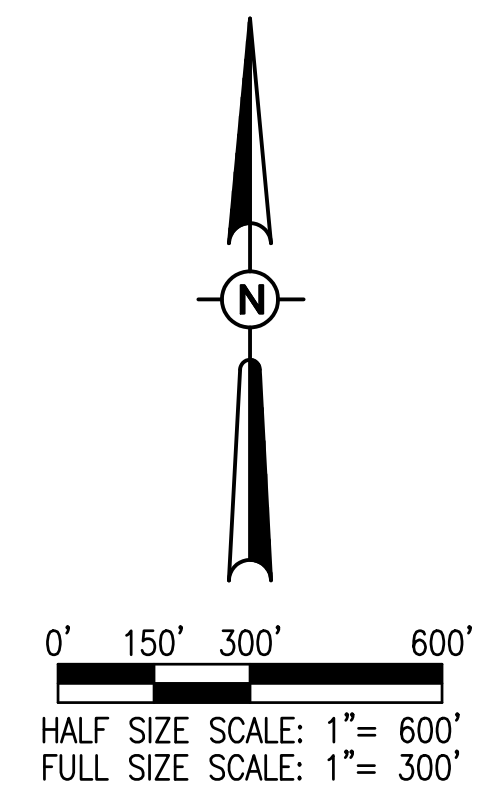
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-101-SOW.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

SCOPE OF WORK

FOR BID



LEGEND

- WORK AREA
- HAUL ROUTE TO BE REMOVED
- CLOSURE CROSS
- LOW PROFILE BARRICADE
- ACCESS ROUTE
- FLAGGER
- CRITICAL POINT
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- TAXIWAY OBJECT FREE AREA
- APPROACH SURFACE

PHASE 1 NOTES

WORK TO BE COMPLETED

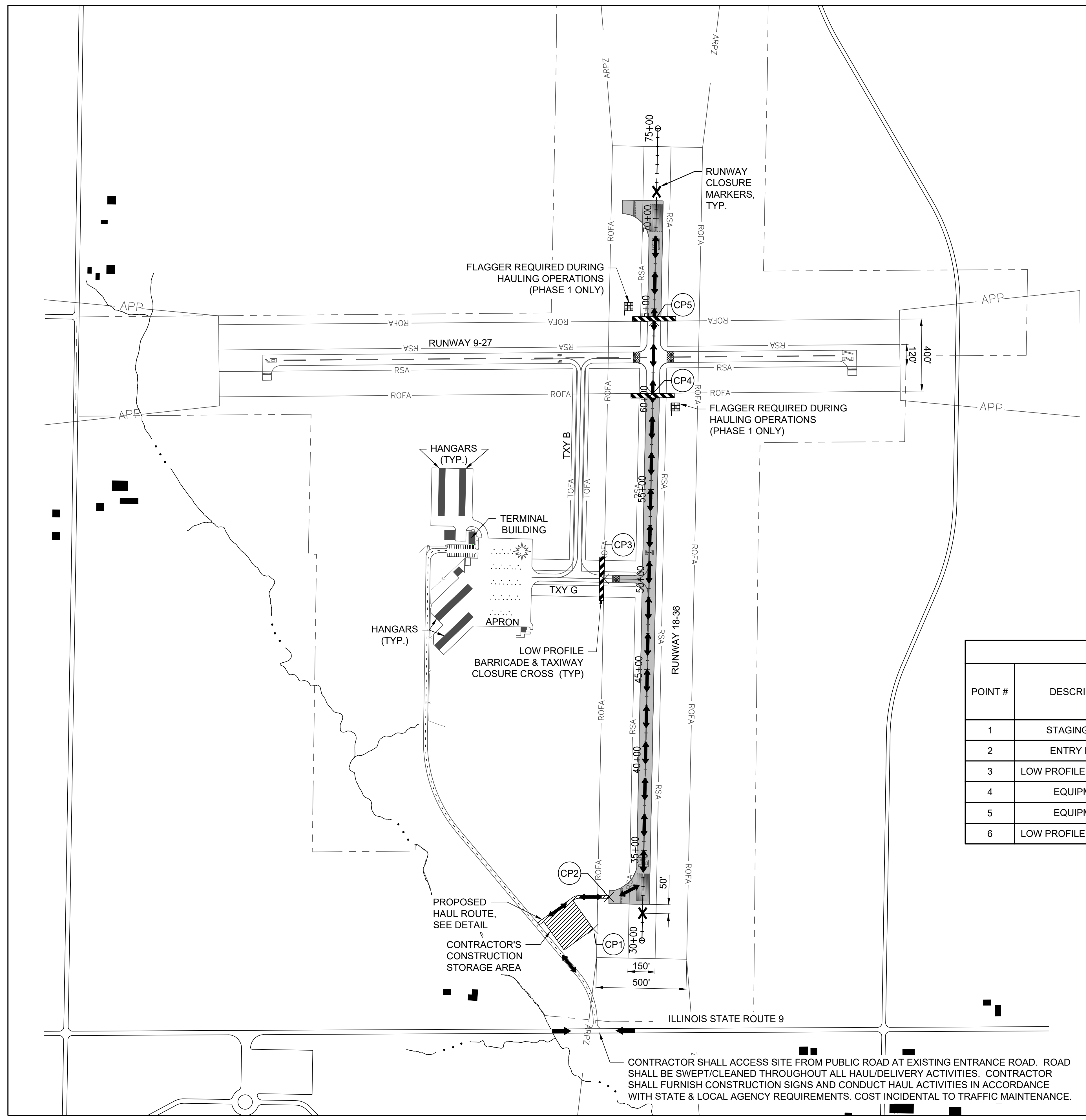
- MILLING, CRACK SEALING, AND PAVING SHALL BE COMPLETED IN PHASE 1.
- MARKING, SHOULDER ADJUSTMENT, SEEDING, AND MULCHING WITHIN PHASE 1 PROJECT LIMITS SHALL BE COMPLETED IN EITHER PHASE 1 OR 2.

AIRFIELD CLOSURES AND CHANGES

- RUNWAY 18/36 WILL BE CLOSED AND RUNWAY 9/27 WILL REMAIN OPEN. A PORTION OF TAXIWAY G WILL BE CLOSED.

CTK CTAF/UNICOM FREQUENCY = 122.80 MHz

CONTROL POINTS						
POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GROUND ELEVATION (MSL)	MAX. EQUIP. HEIGHT (FT)	TOP ELEV. GND + EQUIP. HEIGHT (MSL)
1	STAGING AREA	040° 33' 43.20"	-090° 04' 30.52"	663	25	688
2	ENTRY POINT	040° 33' 44.95"	-090° 04' 29.40"	679	25	704
3	LOW PROFILE BARRICADE	040° 34' 02.38"	-090° 04' 29.74"	672	25	697
4	EQUIPMENT	040° 34' 12.53"	-090° 04' 26.22"	679	25	704
5	EQUIPMENT	040° 34' 16.48"	-090° 04' 26.11"	680	25	705
6	LOW PROFILE BARRICADE	040° 34' 12.50"	-090° 04' 31.40"	678	3	681



CONTRACTOR SHALL ACCESS SITE FROM PUBLIC ROAD AT EXISTING ENTRANCE ROAD. ROAD SHALL BE SWEEP/CLEANED THROUGHOUT ALL HAUL/DELIVERY ACTIVITIES. CONTRACTOR SHALL FURNISH CONSTRUCTION SIGNS AND CONDUCT HAUL ACTIVITIES IN ACCORDANCE WITH STATE & LOCAL AGENCY REQUIREMENTS. COST INCIDENTAL TO TRAFFIC MAINTENANCE.

RESURFACE RUNWAY 18-36

SBGP No:
3-17-SBGP-TBD

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Contract No. CN028

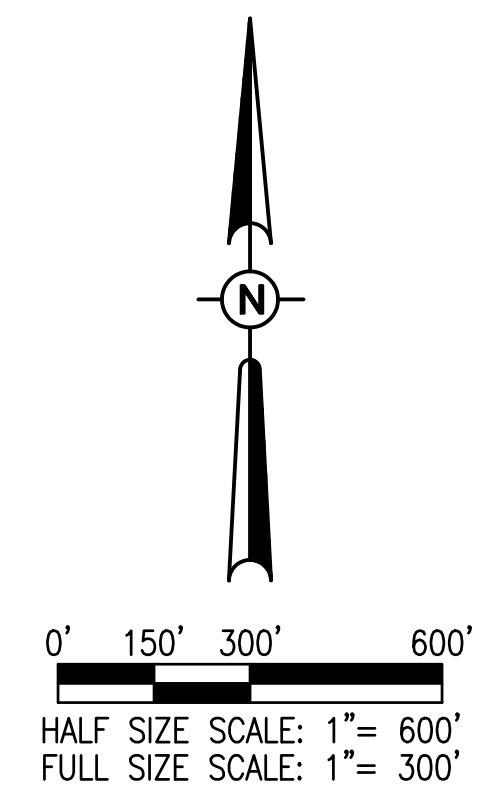
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-103-CSPP.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 04/16/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

SITE AND SAFETY PLAN - PHASE 1

FOR BID



LEGEND

- WORK AREA
- HAUL ROUTE TO BE REMOVED
- CLOSURE CROSS
- LOW PROFILE BARRICADE
- ACCESS ROUTE
- FLAGGER
- CRITICAL POINT
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- TAXIWAY OBJECT FREE AREA
- APPROACH SURFACE

PHASE 2 NOTES

WORK TO BE COMPLETED

- MILLING, CRACK SEALING, PAVING, MARKING, SHOULDER ADJUSTMENT, SEEDING, AND MULCHING SHALL BE COMPLETED.

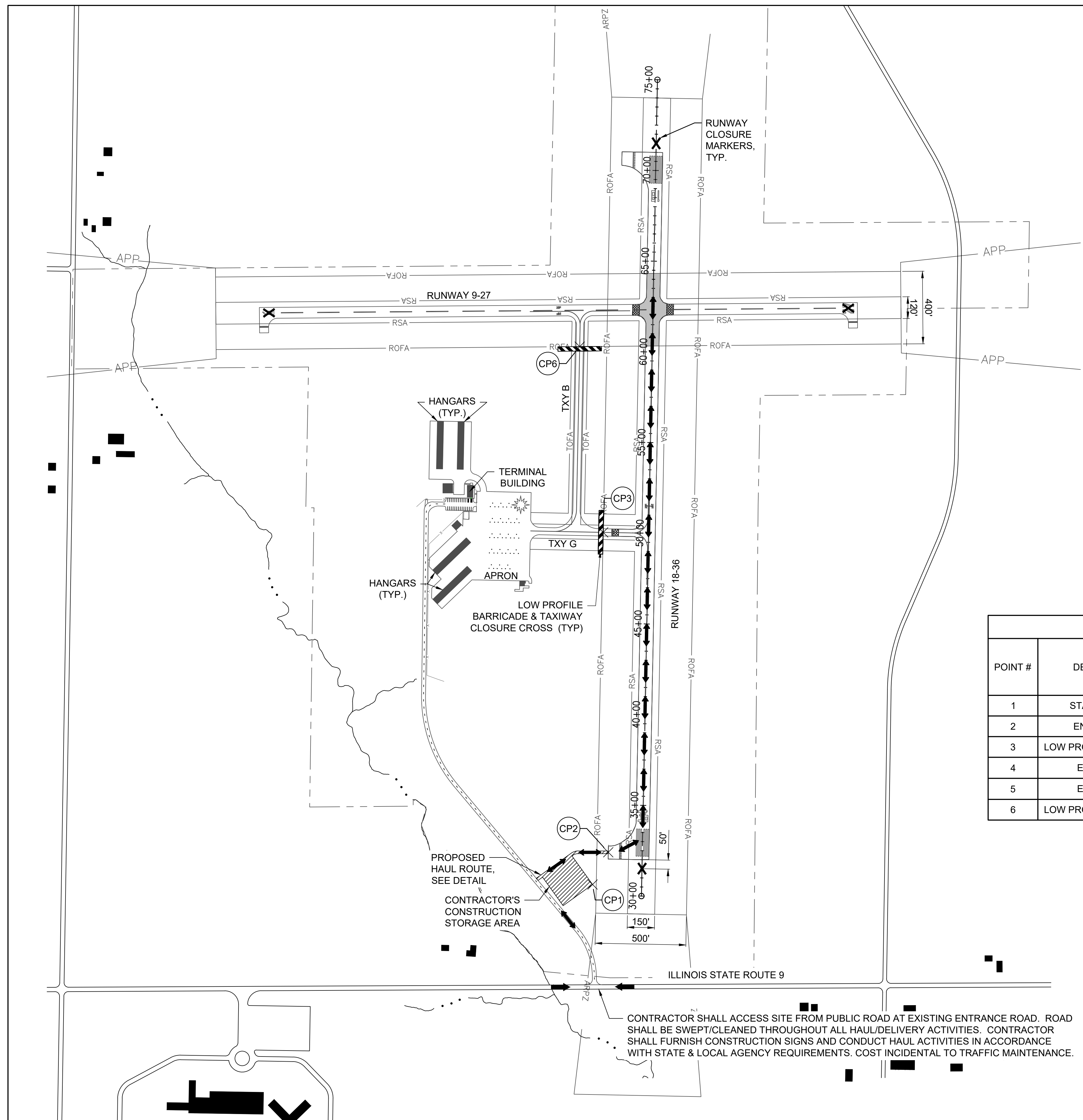
AIRFIELD CLOSURES AND CHANGES

- BOTH RUNWAY 18/36 AND RUNWAY 9/27 WILL BE CLOSED. ALL TAXIWAYS WILL BE CLOSED.

CTK CTAF/UNICOM FREQUENCY = 122.80 MHz

CONTROL POINTS

POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GROUND ELEVATION (MSL)	MAX. EQUIP. HEIGHT (FT)	TOP ELEV. GND + EQUIP. HEIGHT (MSL)
1	STAGING AREA	040° 33' 43.20"	-090° 04' 30.52"	663	25	688
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3	LOW PROFILE BARRICADE	040° 34' 02.38"	-090° 04' 29.74"	672	25	697
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6	LOW PROFILE BARRICADE	040° 34' 12.50"	-090° 04' 31.40"	678	3	681



CONTRACTOR SHALL ACCESS SITE FROM PUBLIC ROAD AT EXISTING ENTRANCE ROAD. ROAD SHALL BE SWEEP/CLEANED THROUGHOUT ALL HAUL/DELIVERY ACTIVITIES. CONTRACTOR SHALL FURNISH CONSTRUCTION SIGNS AND CONDUCT HAUL ACTIVITIES IN ACCORDANCE WITH STATE & LOCAL AGENCY REQUIREMENTS. COST INCIDENTAL TO TRAFFIC MAINTENANCE.

RESURFACE RUNWAY 18-36

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

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ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-103-CSPP.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 04/16/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

SITE AND SAFETY PLAN - PHASE 2

FOR BID

SAFETY NOTES

- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- PRIOR TO ACCESSING THE AIRFIELD, ANY DESIGNATED CONTRACTOR OR SUBCONTRACTOR EMPLOYEES WHO WILL BE OPERATING OR ESCORTING A VEHICLE ON AN ACTIVE AREA OF THE AIRFIELD MUST BE FAMILIAR WITH THE "FAA GUIDE TO GROUND VEHICLE OPERATIONS", AND KEEP A HARD COPY IN THE VEHICLE FOR REFERENCE. THE GUIDE CAN BE FOUND AT: https://www.faa.gov/airports/runway_safety/media/Ground_Vehicle_Guide_Proof_Final.pdf
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE (OPEN) AIRFIELD PAVEMENT AREA WITHOUT AN APPROPRIATE ESCORT. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF RWY 9/27 AND/OR 125' OF RWY 18/36 (DISTANCES MEASURED FROM ACTIVE CENTERLINES) UNLESS CLOSED OR OTHERWISE NOTED. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL ALSO NOT BE PERMITTED WITHIN WITHIN 62' OF ANY ACTIVE AIRPORT TAXIWAY CENTERLINE OR APRON UNLESS OTHERWISE NOTED.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRE-CONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 62' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION", LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND ADDITIONAL AIRSPACE APPROVAL BY THE FAA. AIRSPACE APPROVALS REQUIRE CONSIDERABLE LEAD TIME AND SHOULD BE REQUESTED WELL IN ADVANCE.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEP, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- CONTRACTOR SHALL TAKE MEASURES TO AVOID TRACKING BITUMINOUS TACK COAT ASSOCIATED WITH PAVING PROJECTS ONTO ADJACENT PAVEMENT AREAS, ESPECIALLY GROOVED RUNWAY PAVEMENTS, UNLESS SUFFICIENT PROTECTION HAS BEEN APPLIED. HEAVY TRACKING OR DAMAGE TO ADJACENT PAVEMENTS AND GROOVED SURFACES MAY BE CAUSE FOR STOPPING THE WORK UNTIL ACCEPTABLE PROTECTION OR CHANGE IN WORK METHODS HAS BEEN PROVIDED.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.

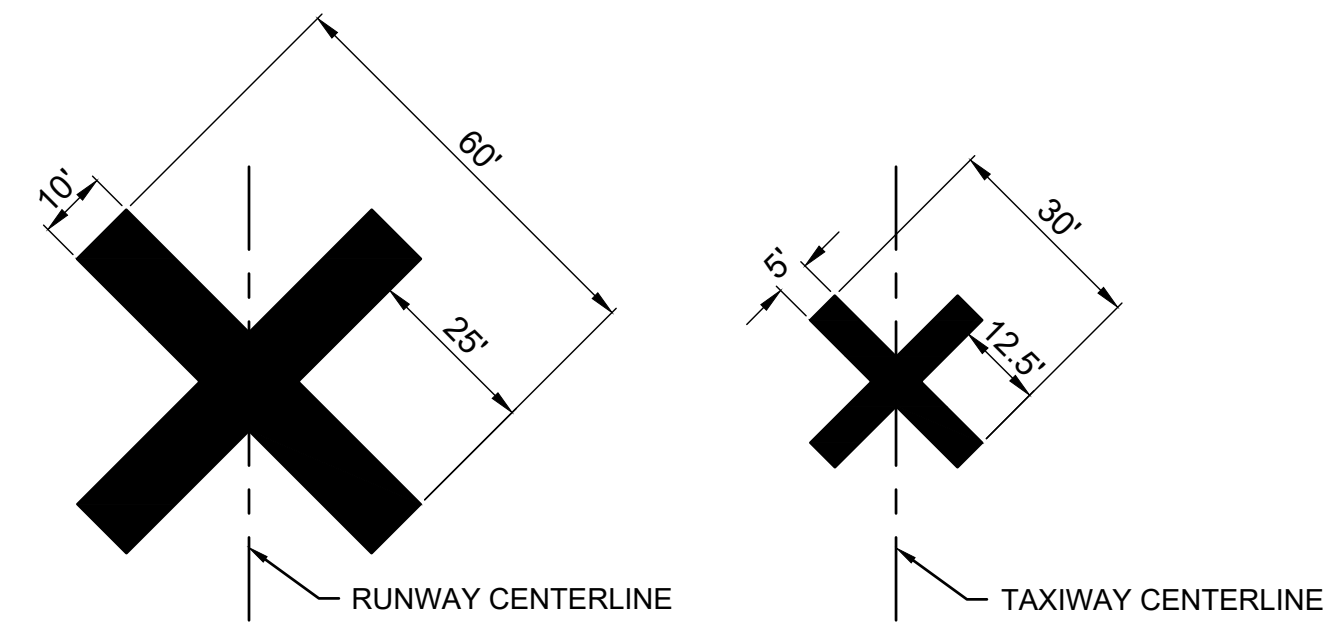
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR IMPLEMENTING MEASURES TO CONTROL OR AVOID CREATING ATTRACTANTS TO WILDLIFE. MEASURES MAY INCLUDE CONTINUOUSLY REMOVING ANY WASTE OR LOOSE MATERIALS, PLACEMENT OF MATERIALS IN APPROPRIATE STORAGE CONTAINERS, PROPERLY MAINTAINING FENCES AND GATES TO PREVENT ACCESS, AND PREVENTING PONDING OF WATER THROUGHOUT THE SITE.
- UNLESS SPECIFIED OTHERWISE, COST FOR SAFETY, STAGING, AND TRAFFIC MAINTENANCE ITEMS IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.
- THE CONTRACTOR SHALL HAVE THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS DETAILED IN THE SPECIAL PROVISIONS, SUBMITTED AND APPROVED PRIOR TO BEING ISSUED "NOTICE TO PROCEED".
- ALL RUNWAY/TAXIWAY CLOSURES SHALL BE COORDINATED WITH AIRPORT MANAGEMENT A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME TO ALLOW FOR THE PROPER COORDINATION. AIRPORT MANAGEMENT HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.
- RUNWAY/TAXIWAY CLOSURE PROCEDURES:
 - CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME.
 - ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
 - PLACEMENT OF CROSSES AND BARRICADES.
 - ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.

RUNWAY/TAXIWAY RE-OPENING PROCEDURES:

- ENSURE ALL PERSONNEL, EQUIPMENT AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
- INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES.
- CONTACT AIRPORT MANAGEMENT OR REPRESENTATIVE FOR FINAL INSPECTION OF THE AREA.
- REMOVE BARRICADES AND CROSSES.
- ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.

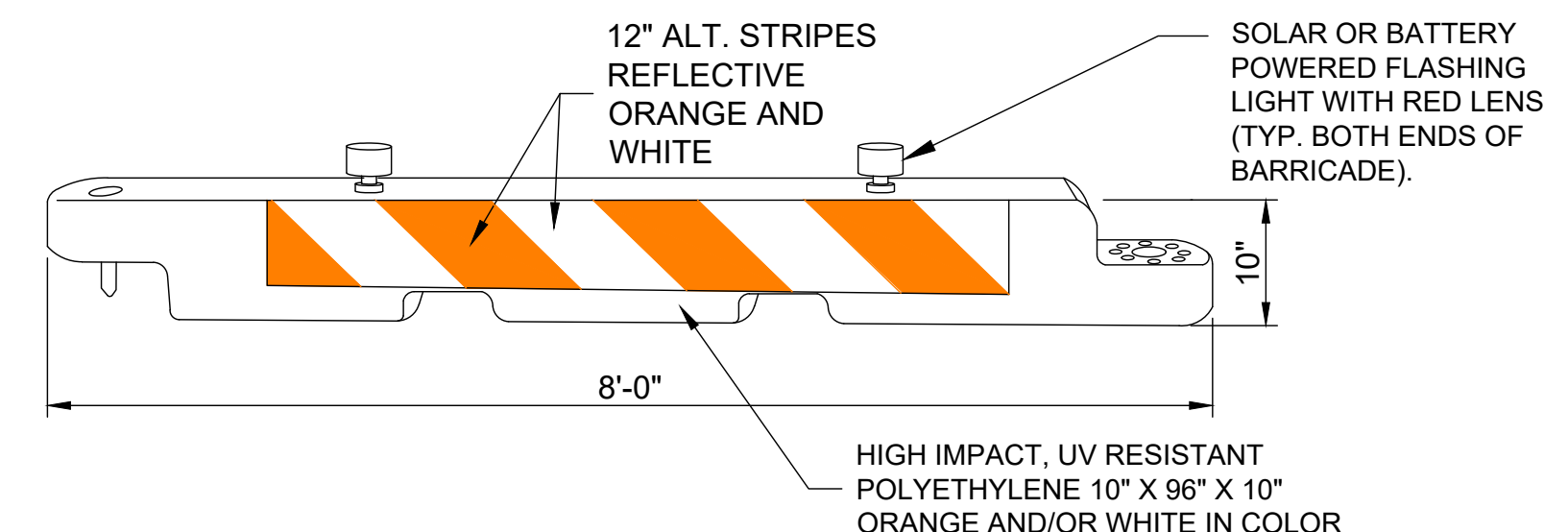
BARRICADE NOTES

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- BARRICADES SHALL BE INTERLOCKED END TO END OVER THE LENGTH OF THE PAVEMENT WHERE PROTECTING OPEN RUNWAYS, AND SPACED END TO END A MAXIMUM OF 4 FEET IN OTHER ALL OTHER AREAS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS OTHERWISE NOTED.



TEMPORARY CLOSURE CROSS DETAIL
NOT TO SCALE

- TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE "AVIATION YELLOW"
- TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
- TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS OTHERWISE NOTED.



LOW-PROFILE BARRICADE DETAIL
NOT TO SCALE

DETAIL ABOVE REPRESENTS ONE OPTION FOR LOW-PROFILE BARRICADES. OTHER OPTIONS MAY BE UTILIZED AS LONG AS THEY MEET THE REQUIREMENTS OF THE PROJECT, INCLUDING BARRICADE NOTE 1.

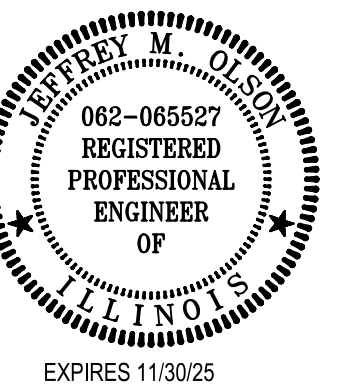


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26013 N. Ingersoll Rd.
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RESURFACE RUNWAY 18-36

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
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SHEET TITLE

SAFETY NOTES AND DETAILS

FOR BID

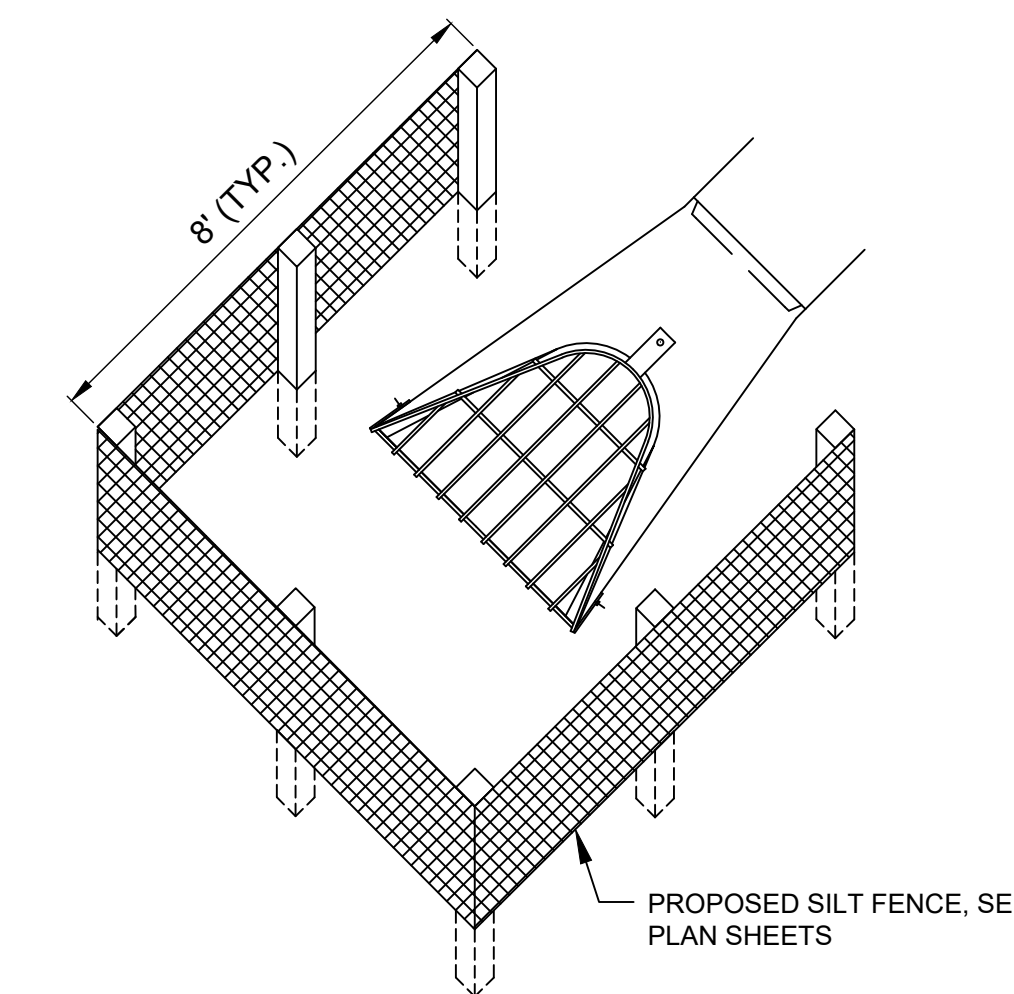
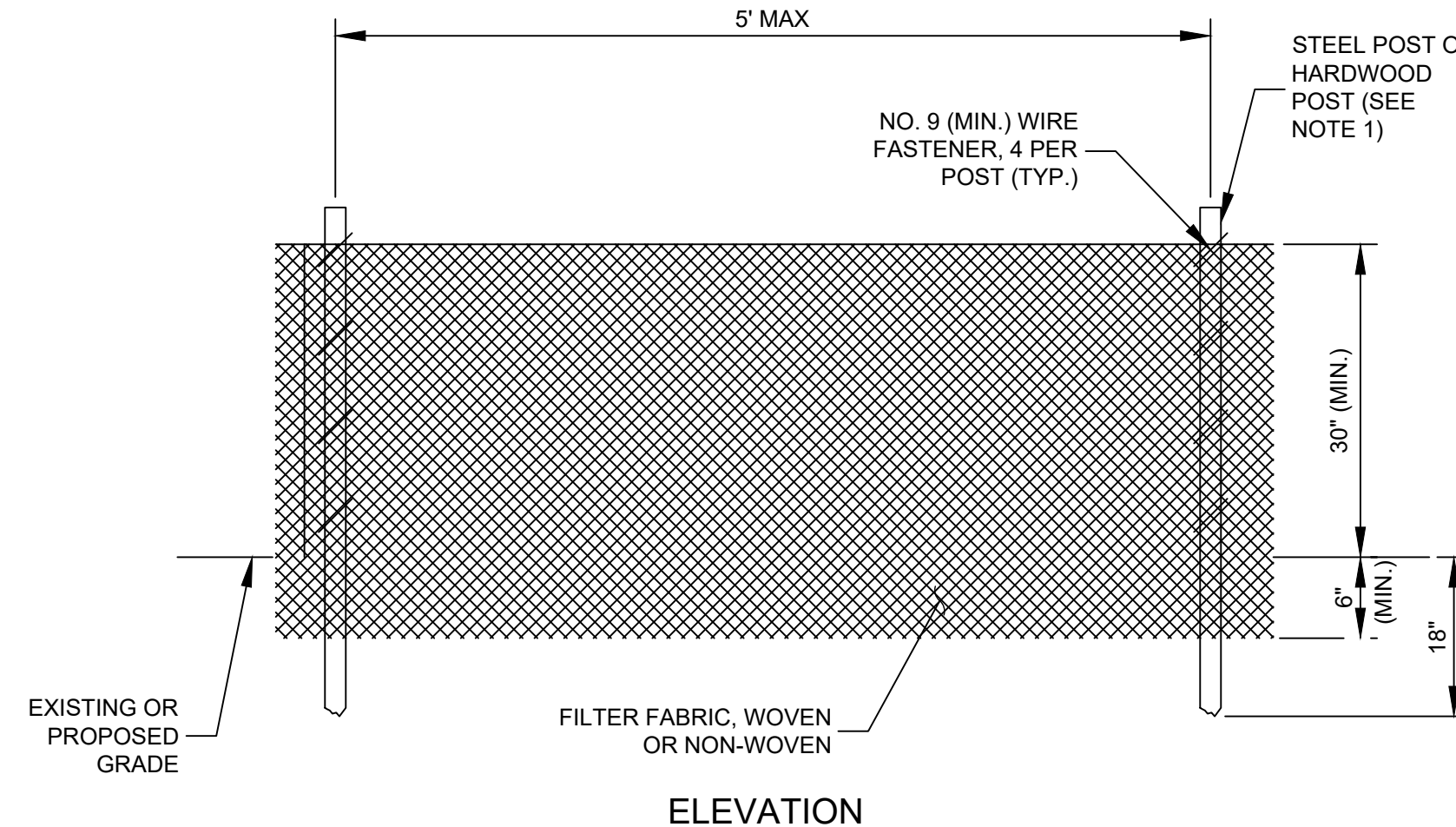
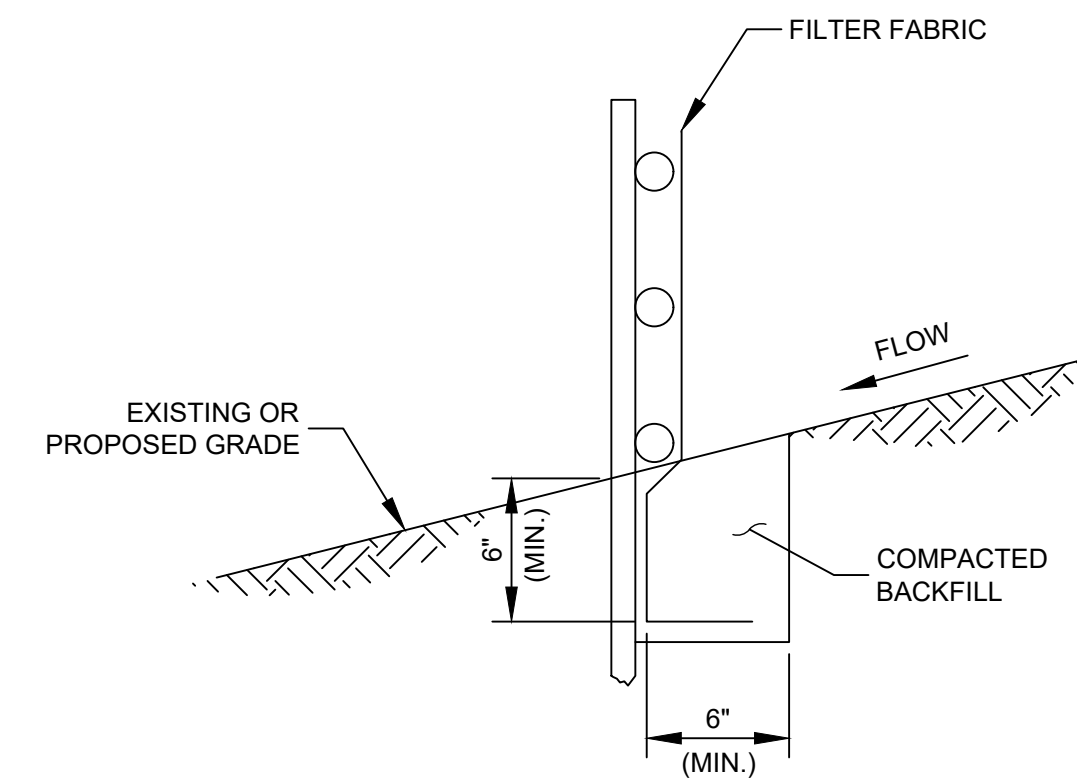
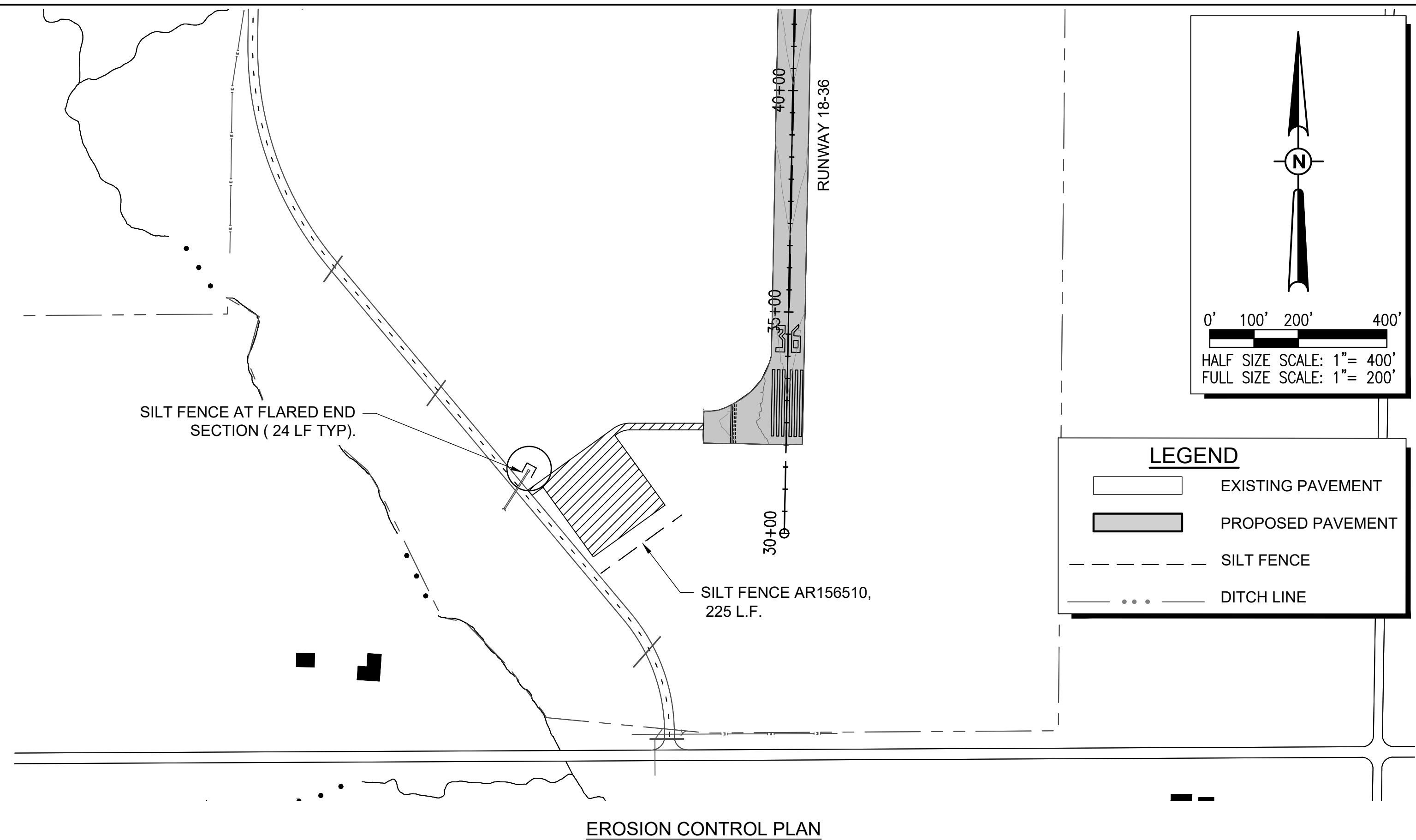
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SEDIMENTATION AND EROSION CONTROL NOTES:

- A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- B. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:
 - UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.
 - AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- D. A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- E. TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.
- F. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
- G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.
- I. APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- J. STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- K. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- L. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- N. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- O. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.

NOTES:

- 1. FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS.
- 2. FILTER FABRIC SHALL BE SECURELY FASTENED TO POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
- 3. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
- 4. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (ARTICLE IV, SECTION B.1.j.1.f.i, AS AMENDED), OR EQUIVALENT.
- 5. A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
- 6. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- 7. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- 8. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
- 9. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
- 10. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
- 11. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
- 12. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- SILT FENCE
- DITCH LINE



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**RESURFACE
RUNWAY 18-36**

**SBGP No:
3-17-SBGP-TBD**

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
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SHEET TITLE

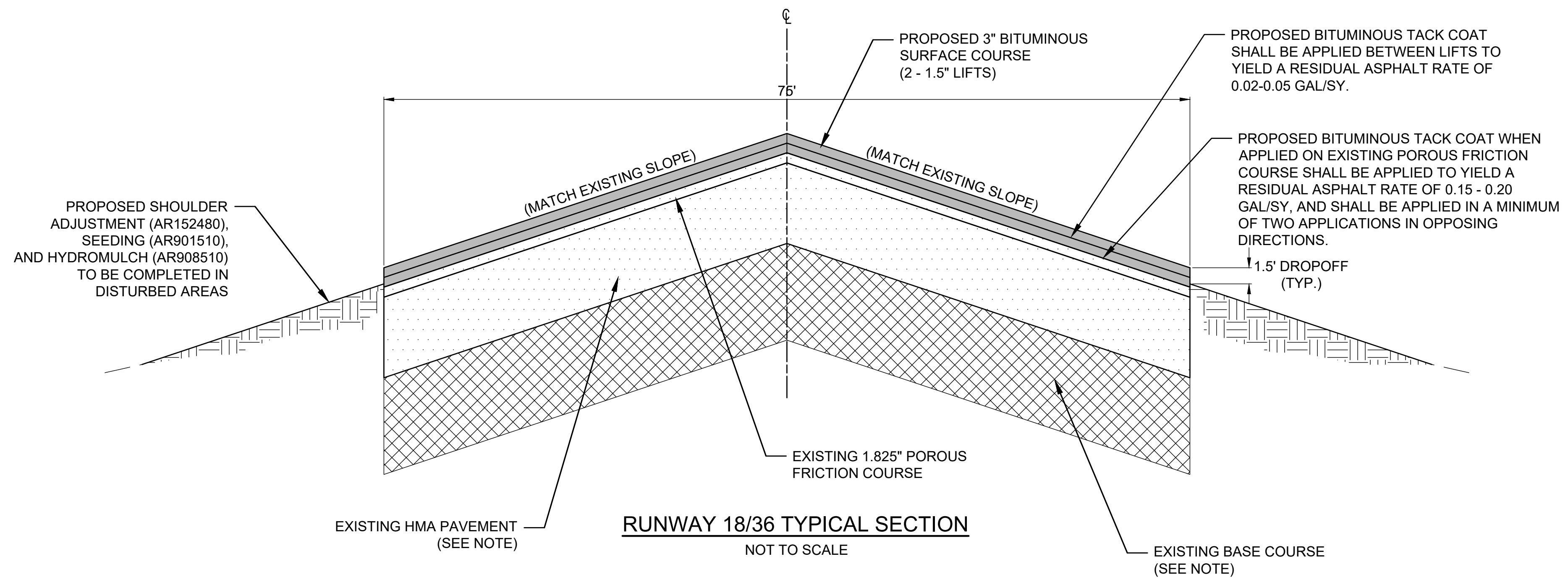
**STORMWATER
POLLUTION
PREVENTION PLAN
(SWPPP)**

FOR BID

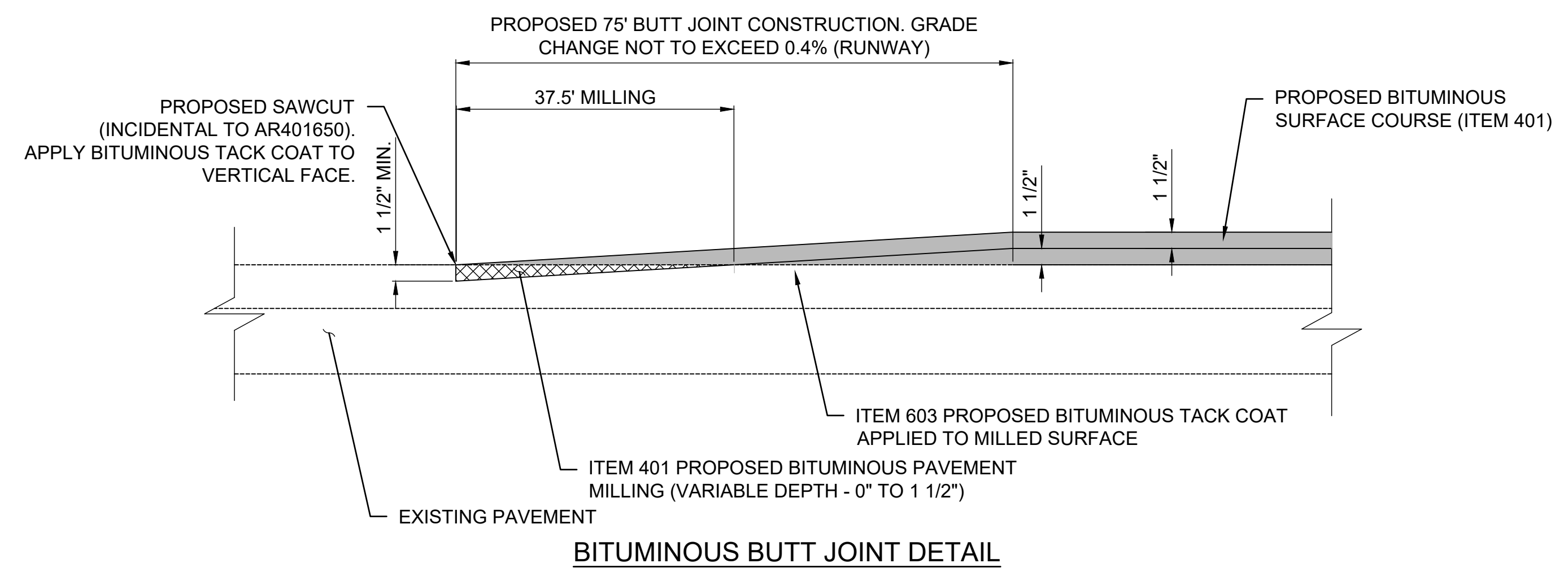
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EXISTING	PROPOSED	LEGEND
		BIT. SURF. CSE.-METHOD II, SUPERPAVE
		SHOULDER ADJUSTMENT (FILL)
		BITUMINOUS PAVEMENT
		CRUSHED AGGREGATE



NOTE:
FROM RECORD DRAWINGS, THE EXISTING PAVEMENT CONFIGURATION IS, FROM STA. 32+01 TO STA. 71+00.5, IS 0.10\"/>



- BUTT JOINT CONSTRUCTION NOTES**
- ALL BITUMINOUS PAVEMENT MILLING AREAS FOR PROPOSED BUTT JOINTS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER/TECHNICIAN.
 - THE PAVEMENT SURFACE WILL THEN BE MILLED TO A DEPTH OF 1.5 INCHES AT THE BUTT END AND WILL TAPER TO A 0 INCH DEPTH WHEN MEASURED FROM THE PROPOSED PAVEMENT SURFACE AT THE OPPOSITE END. THE PLANER MUST BE CAPABLE OF MILLING THE SURFACE TO THE DESIRED ELEVATIONS AND GRADES.
 - ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.
 - BUTT JOINTS WILL BE PAVED WITH THE RESPECTIVE LIFTS OF SURFACE COURSE ON THE RUNWAY.
 - THE MILLING, TACK COAT AND BITUMINOUS PAVING SHALL BE PAID FOR UNDER THEIR RESPECTIVE PAY ITEMS. THE SAW-CUT AND OTHER ASSOCIATED WORK SHALL BE INCIDENTAL.

RESURFACE RUNWAY 18-36

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TYPICAL SECTIONS

FOR BID



RESURFACE
RUNWAY 18-36

SBGP No:
3-17-SBGP-TBD

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SHEET TITLE

CLEAN AND SEAL
BITUMINOUS CRACKS
SCHEDULE

ESTIMATED CLEAN AND SEAL BITUMINOUS CRACKS SCHEDULE (AR201661)		
CRACK NO.	STATION (APPROXIMATE)	CRACK LENGTH (FT)
CENTERLINE LONGITUDINAL JOINT	32+01 TO 71+00	3899
MISCELLANEOUS	32+01 TO 71+00	8526
36 TURNAROUND	32+01 TO 34+01	250
18 TURNAROUND	68+94 TO 71+00	250
1	32+80	75
2	32+88	37.5
3	33+68	37.5
4	34+68	75
5	35+76	37.5
6	36+07	37.5
7	36+45	37.5
8	36+57	75
9	36+58	37.5
10	36+87	75
11	36+98	37.5
12	37+27	37.5
13	37+29	37.5
14	37+37	37.5
15	37+73	37.5
16	37+82	75
17	37+94	37.5
18	38+26	37.5
19	38+54	75
20	38+85	37.5
21	39+04	75
22	39+30	37.5
23	39+75	75
24	40+02	37.5
25	40+22	37.5
26	40+42	37.5
27	41+10	75
28	41+70	37.5
29	41+76	37.5
30	42+00	37.5
31	42+03	75
32	42+07	37.5
33	42+24	37.5
34	42+82	37.5
35	43+14	75
36	43+66	75
37	44+26	37.5
38	44+78	37.5
39	44+98	37.5
40	45+09	37.5
41	45+78	75
42	46+27	37.5
43	46+69	37.5
44	47+11	75
45	47+41	37.5
46	48+04	37.5
47	48+09	37.5
48	48+75	75
49	48+86	75
50	48+88	75
51	49+27	75
52	49+35	75
53	49+56	37.5
54	49+63	37.5
55	49+79	75
56	49+84	37.5

ESTIMATED CLEAN AND SEAL BITUMINOUS CRACKS SCHEDULE (AR201661)		
CRACK NO.	STATION (APPROXIMATE)	CRACK LENGTH (FT)
57	50+26	37.5
58	50+68	75
59	51+16	75
60	51+94	75
61	52+43	75
62	52+71	37.5
63	52+86	75
64	53+09	75
65	53+11	75
66	53+63	37.5
67	53+67	37.5
68	53+91	37.5
69	54+18	37.5
70	54+27	37.5
71	54+63	75
72	54+65	75
73	55+18	37.5
74	55+60	75
75	56+11	75
76	56+65	75
77	57+10	37.5
78	57+34	75
79	57+75	75
80	57+77	75
81	58+48	75
82	58+54	37.5
83	58+92	75
84	59+21	37.5
85	60+01	75
86	60+28	75
87	60+81	37.5
88	61+91	62
89	62+54	58
90	63+05	37.5
91	63+34	37.5
92	63+78	37.5
93	63+91	75
94	64+29	37.5
95	64+35	37.5
96	64+50	75
97	65+05	75
98	65+38	37.5
99	65+58	75
100	65+64	75
101	65+80	37.5
102	66+12	75
103	66+14	75
104	66+28	37.5
105	66+62	75
106	67+18	37.5
107	67+41	75
108	67+43	75
109	68+18	75
110	68+46	75
111	68+61	75
112	68+96	37.5
113	69+13	75
114	69+30	37.5
115	69+52	75
116	70+09	37.5
TWY G	-	40

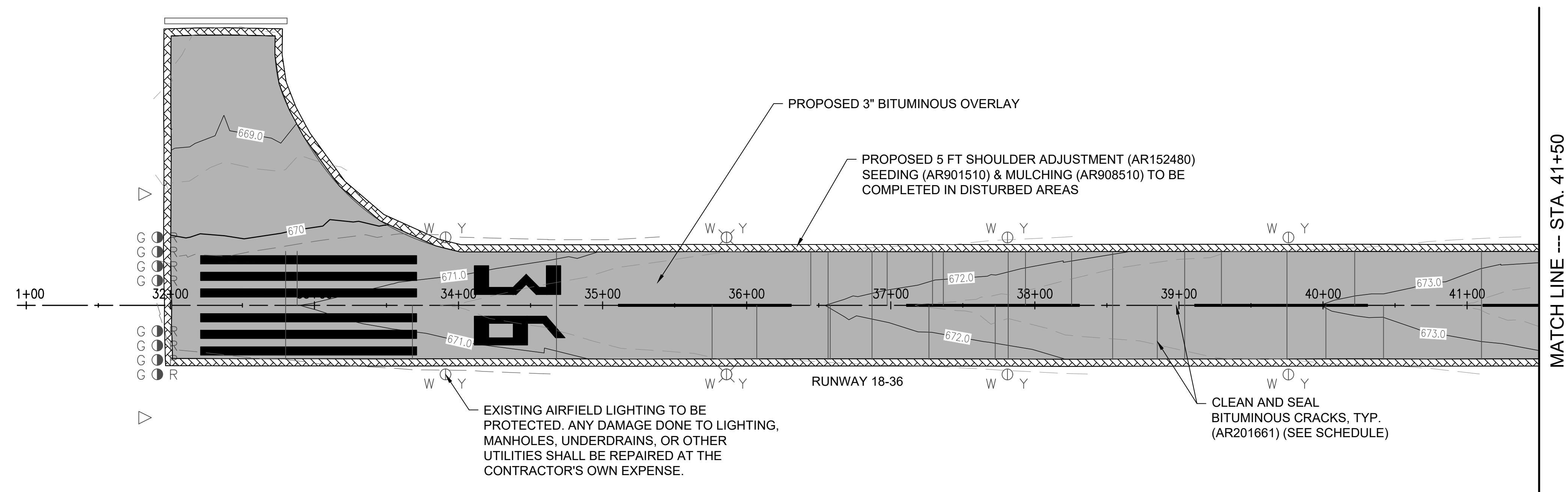
CLEAN AND SEAL BITUMINOUS CRACKS NOTES

- THE PROVIDED SCHEDULE IS NOT ENTIRELY LAID OUT ON THE FOLLOWING PLAN AND PROFILE SHEETS. MISCELLANEOUS, 36 TURNAROUND, AND 18 TURNAROUND ARE ESTIMATED QUANTITIES TO BE LOCATED AND MARKED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CRACKS 1 THROUGH 116 IN THE SCHEDULE HAVE BEEN FIELD VERIFIED AS TRANSVERSE CRACKS TO BE CLEANED AND SEALED. ADDITIONAL ESTIMATED QUANTITY IS PROVIDED FOR AREAS SUCH AS TURNAROUNDS, AND MISCELLANEOUS RUNWAY CRACKING. FINAL LOCATION OF CRACKS SHALL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER/TECHNICIAN AND ESTIMATED PLAN QUANTITY ADJUSTED ACCORDINGLY.



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- PROPOSED SHOULDER WORK
- PROPOSED SAW & SEAL
- EXISTING CONTOUR
- PROPOSED CONTOUR



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

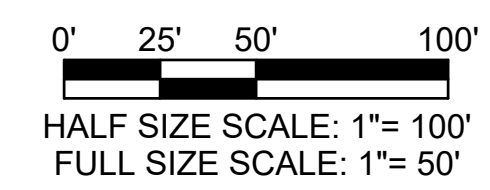
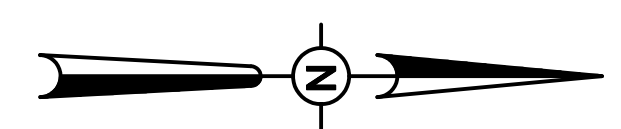
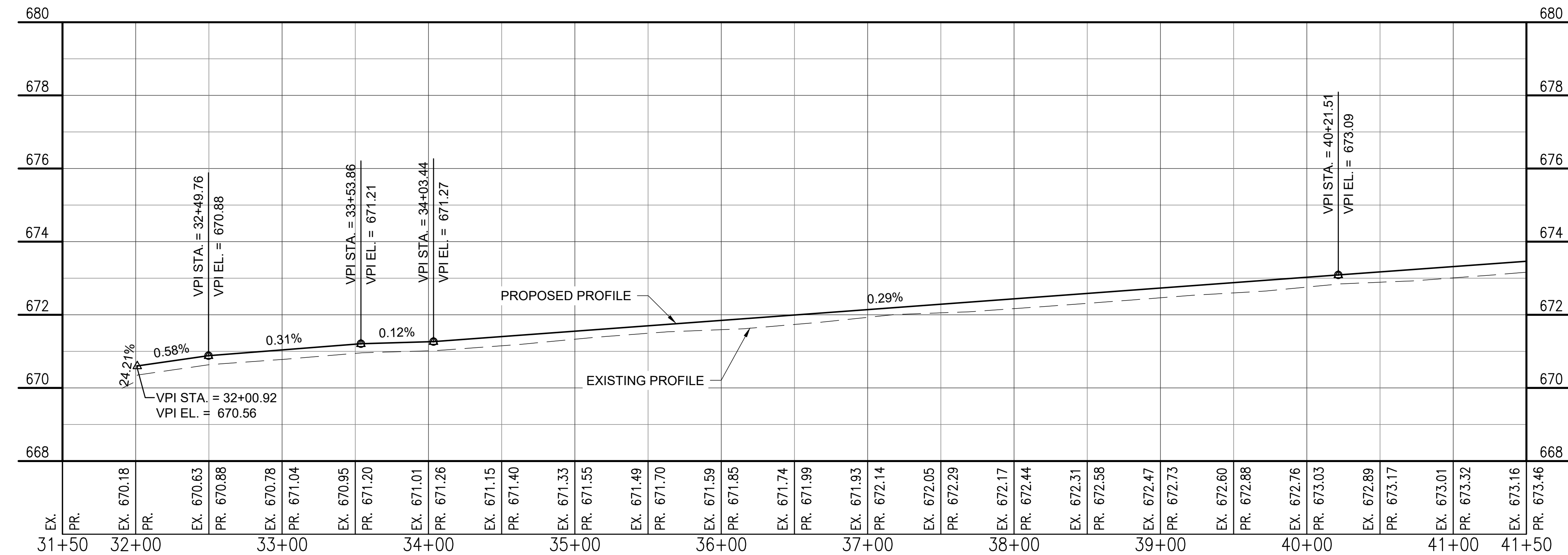
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SHEET TITLE

**PLAN AND PROFILE -
SHEET 1**

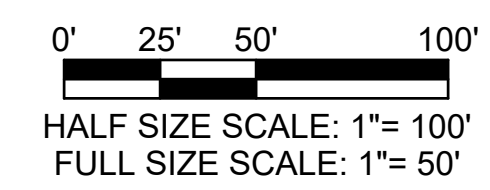
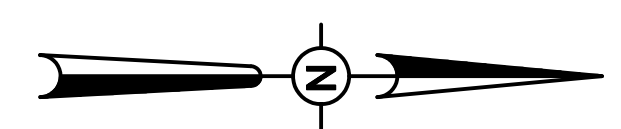
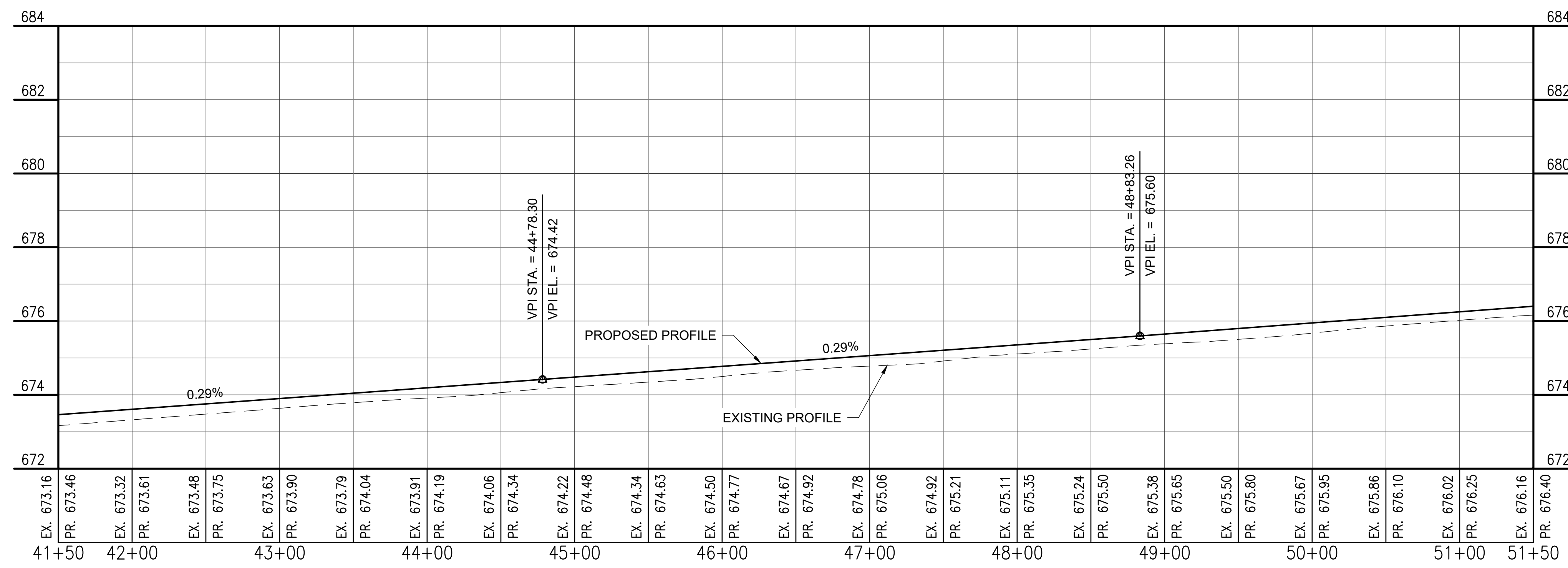
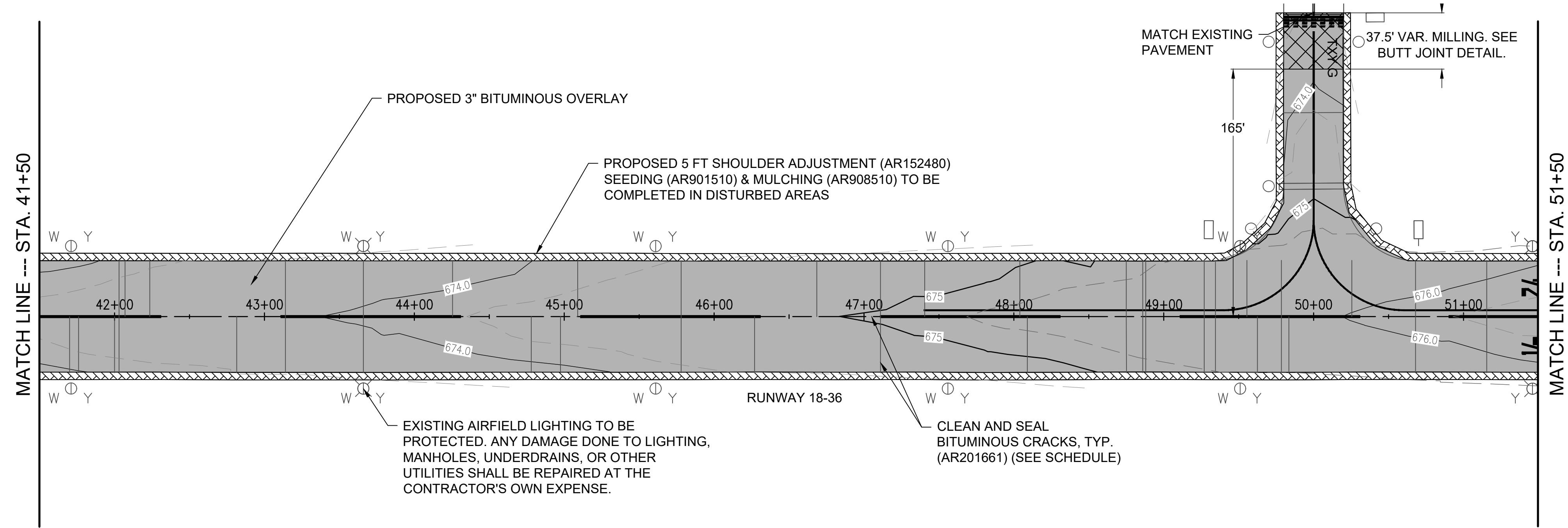


FOR BID



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- PROPOSED SHOULDER WORK
- PROPOSED SAW & SEAL
- EXISTING CONTOUR
- PROPOSED CONTOUR



FOR BID

**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-701-PNP.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 04/16/24
REVIEWED BY: JMO 04/19/2024

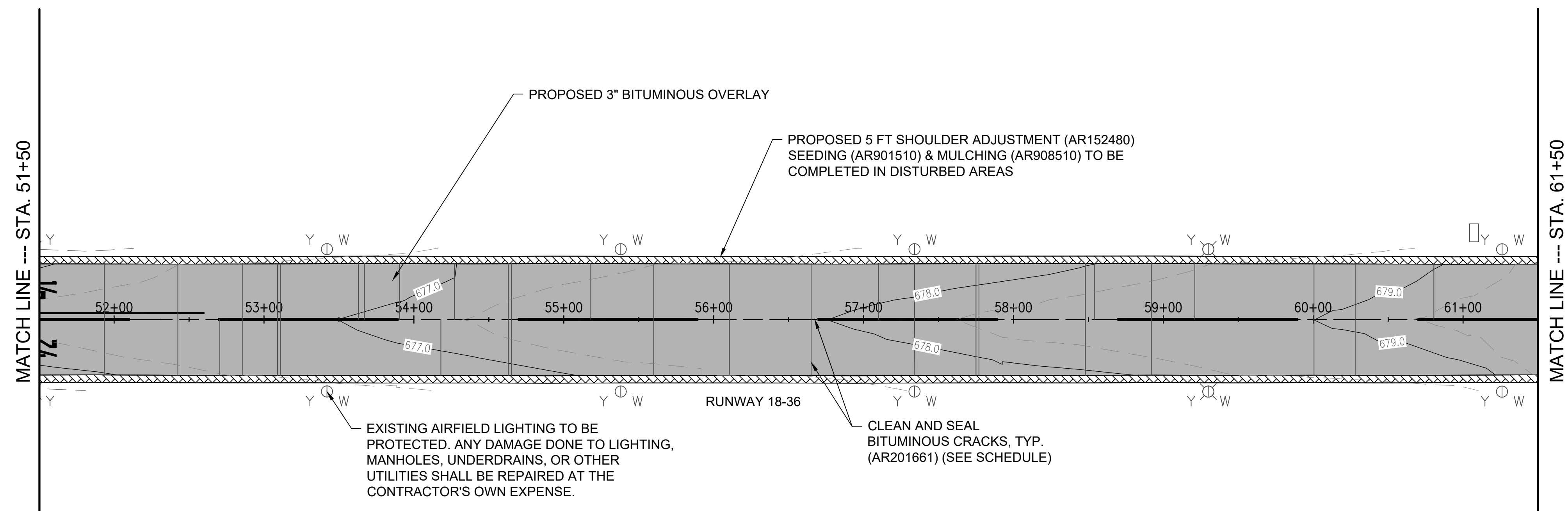
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**PLAN AND PROFILE -
SHEET 2**



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- PROPOSED SHOULDER WORK
- PROPOSED SAW & SEAL
- EXISTING CONTOUR
- PROPOSED CONTOUR



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

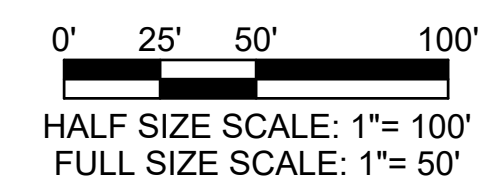
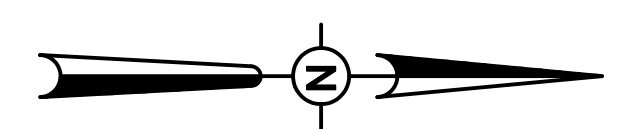
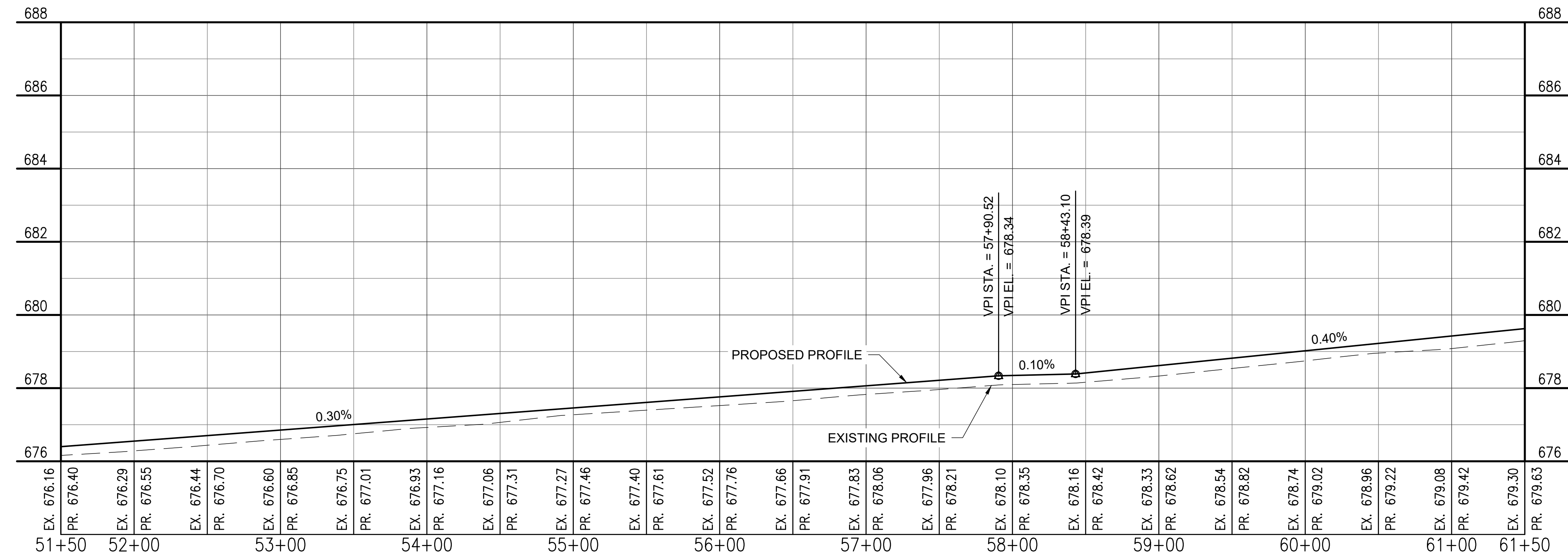
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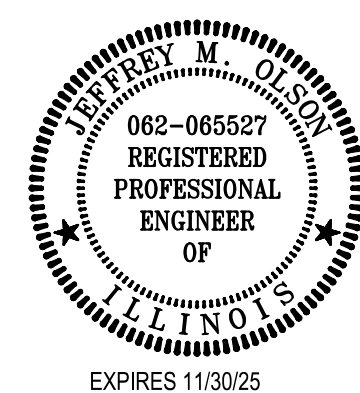
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SHEET TITLE

**PLAN AND PROFILE -
SHEET 3**

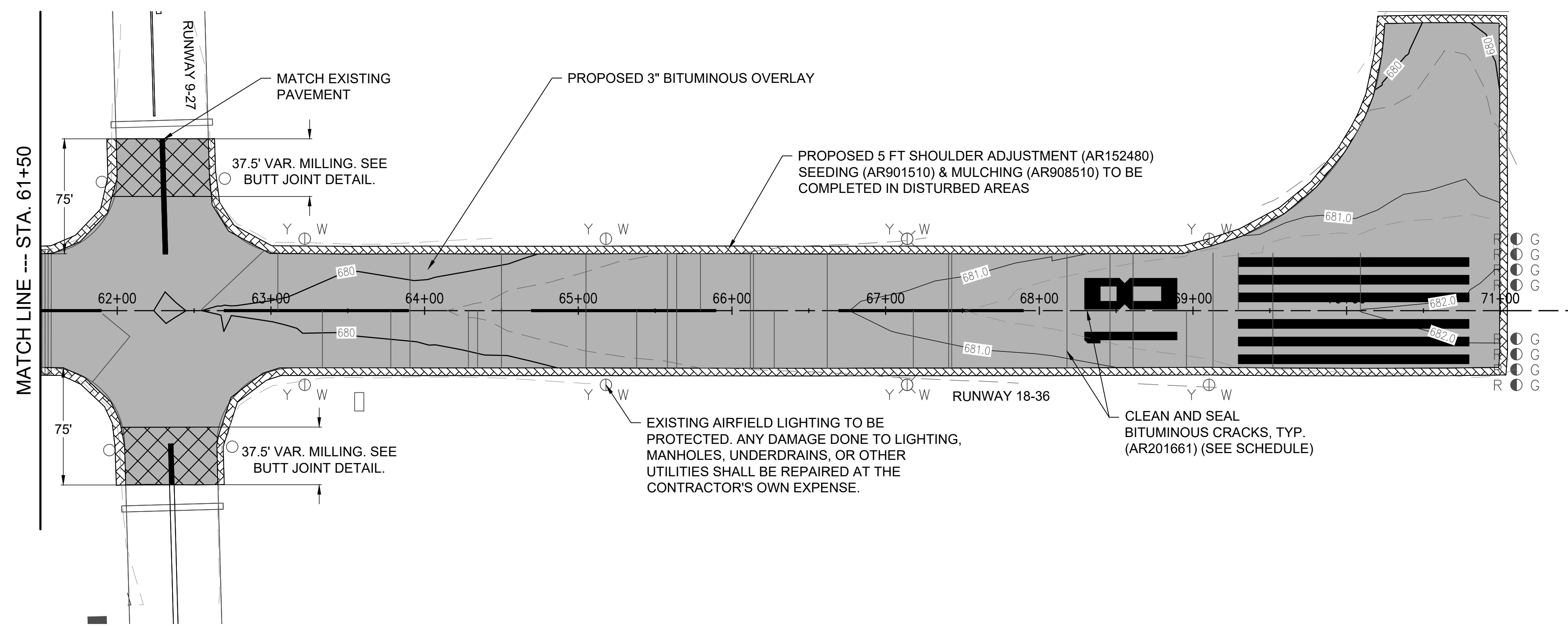


FOR BID



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- PROPOSED SHOULDER WORK
- PROPOSED SAW & SEAL
- EXISTING CONTOUR
- PROPOSED CONTOUR



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

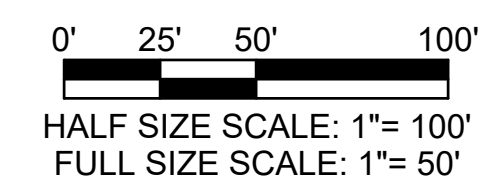
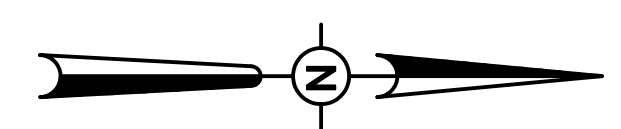
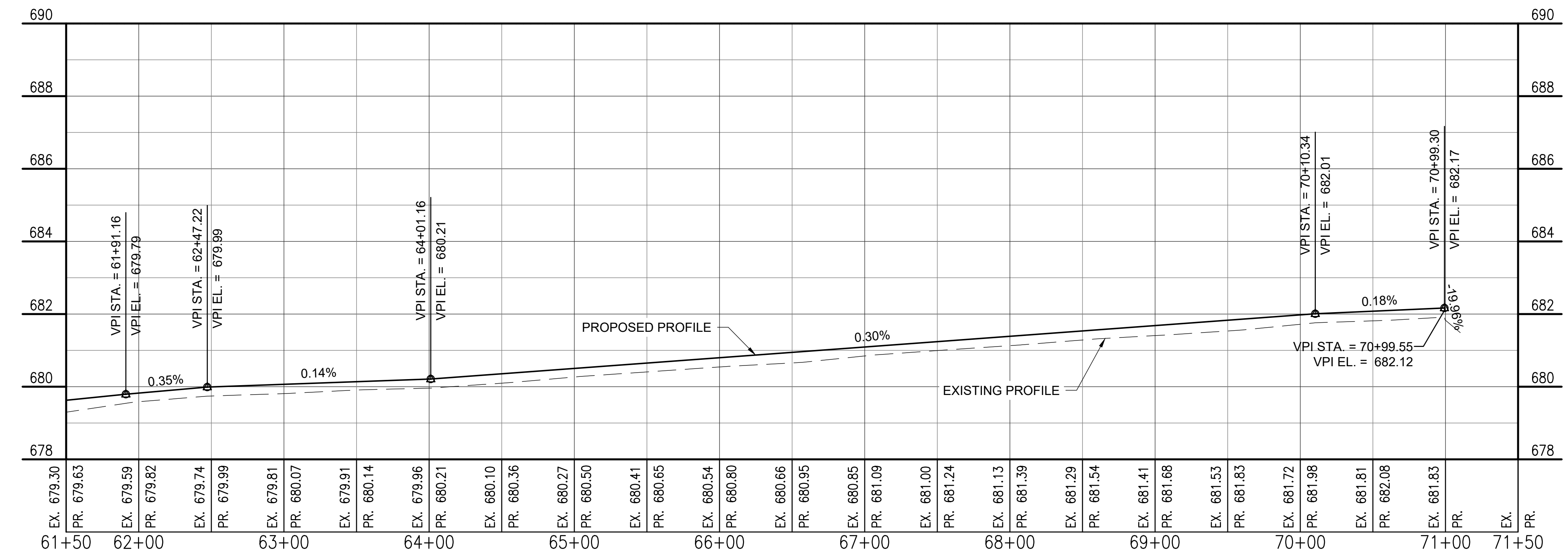
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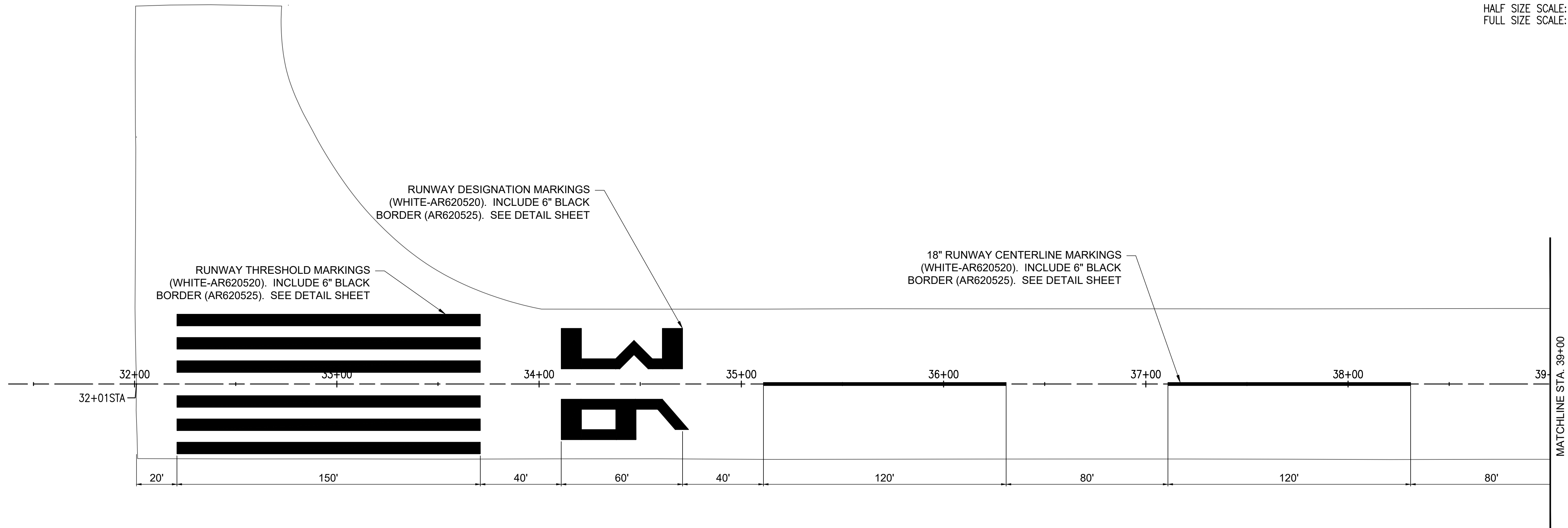
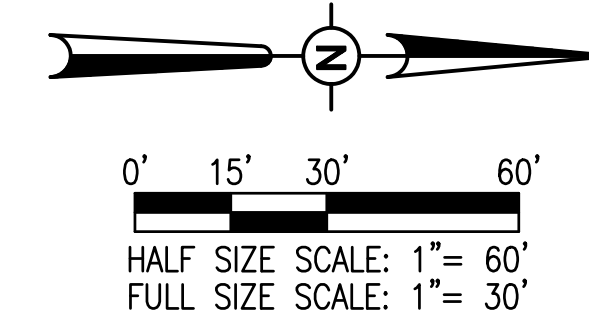
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**PLAN AND PROFILE -
SHEET 4**



FOR BID



WATERBORNE PAVEMENT MARKING NOTES

1. A 30-DAY WAITING PERIOD IS REQUIRED BETWEEN THE PLACEMENT OF BITUMINOUS SURFACE COURSE AND THE FINAL APPLICATION OF WATERBORNE PAINT. AFTER THE PLACEMENT OF SURFACE COURSE AND PRIOR TO THE 30-DAY CURE PERIOD AN INITIAL COAT OF MARKINGS SHALL BE PLACED AT 50% OF THE NORMAL APPLICATION RATE. BLACK MARKINGS SHALL NOT BE PLACED DURING THE INITIAL COAT. THE REMAINDER OF THE MARKINGS, INCLUDING THE BLACK BORDERS, SHALL BE PLACED AFTER THE 30-DAY CURE PERIOD HAS ELAPSED. (IF ONLY MARKING REMAINS, THE CALENDAR DAYS MAY BE SUSPENDED DURING THE 30 DAY CURE PERIOD.)
2. WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS AND WILL BE APPLIED IN ONE APPLICATION.
3. GLASS BEADS SHALL BE APPLIED ON THE SECOND COAT OF MARKING. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
4. CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.
5. THE CONTRACTOR WILL BE RESPONSIBLE FOR RE-MARKING ANY MARKINGS DAMAGED BY CONSTRUCTION ACTIVITIES THAT ARE OUTSIDE OF THE PROPOSED WORK LIMITS.

MARKING SCHEDULE			
AR620520 - PAVEMENT MARKING - WATERBORNE (WHITE)			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
THRESHOLD MARKING	862.5	12	10,350.0
DESIGNATION MARKING			
1	318.0	1	318.0
3	636.0	1	636.0
6	712.0	1	712.0
8	876.0	1	876.0
DESIGNATION MARKING			
1/2	177.0	2	354.0
RUNWAY 18/36 CENTERLINE MARKING	180.0	17	3,060.0
RUNWAY 9/27 CENTERLINE MARKING (27')	81.0	1	81.0
RUNWAY 9/27 CENTERLINE MARKING (75')	225.0	1	225.0
AR620520 - PAVEMENT MARKING - WATERBORNE (YELLOW)			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
12" TAXIWAY LEAD IN LINES - TWY G	711.0	1	711.0
RUNWAY HOLDING POSITION	122.0	1	122.0
AR620525 PAVEMENT MARKING - BLACK BORDER			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
THRESHOLD MARKING	156.8	12	1,881.0
DESIGNATION MARKING			
1	68.0	1	68.0
3	103.0	1	103.0
6	114.0	1	114.0
8	132.0	1	132.0
DESIGNATION MARKING			
1/2	69.0	2	138.0
RUNWAY 18/36 CENTERLINE MARKING	122.5	17	2,083.0
6" TAXIWAY LEAD IN LINES - TWY G	709.0	1	709.0
RUNWAY HOLDING POSITION	198.0	1	198.0

RESURFACE RUNWAY 18-36

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

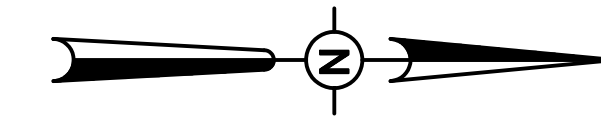
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ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-110-MARK.DWG
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SHEET TITLE

MARKING PLAN - SHEET 1

FOR BID



0' 15' 30' 60'
 HALF SIZE SCALE: 1" = 60'
 FULL SIZE SCALE: 1" = 30'



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 Telephone 309 647 2072



**RESURFACE
 RUNWAY 18-36**

SBGP No:
 3-17-SBGP-TBD

IDA No: CTK-5121

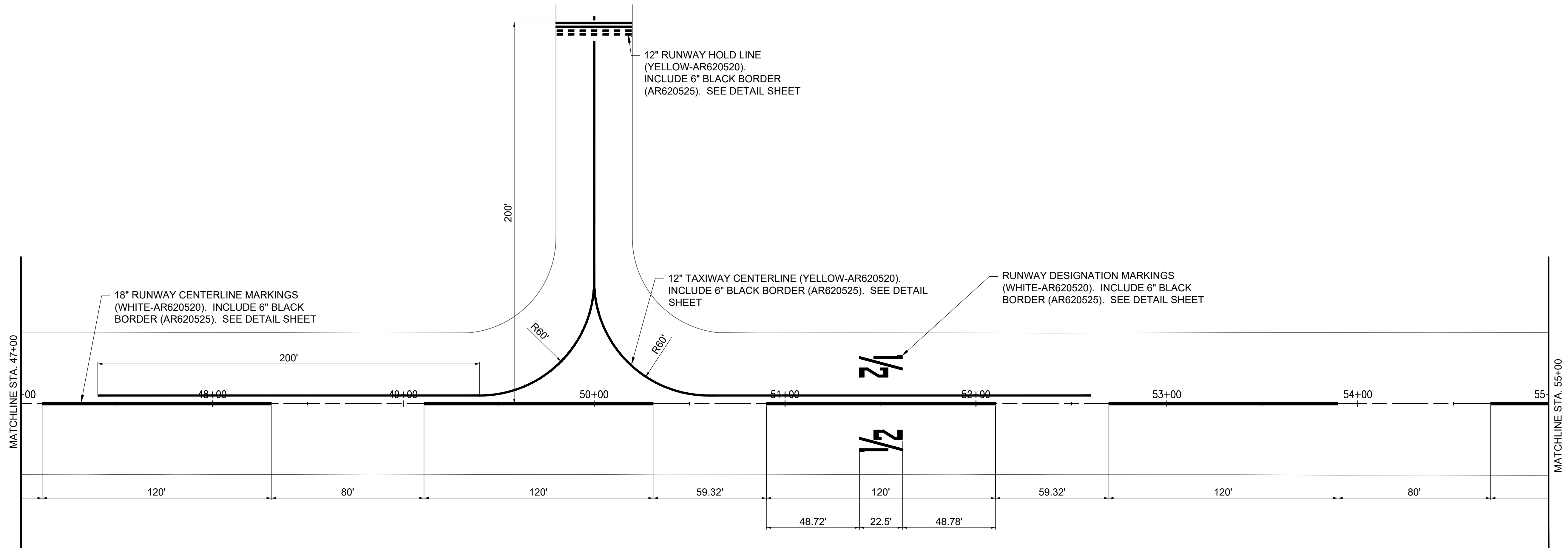
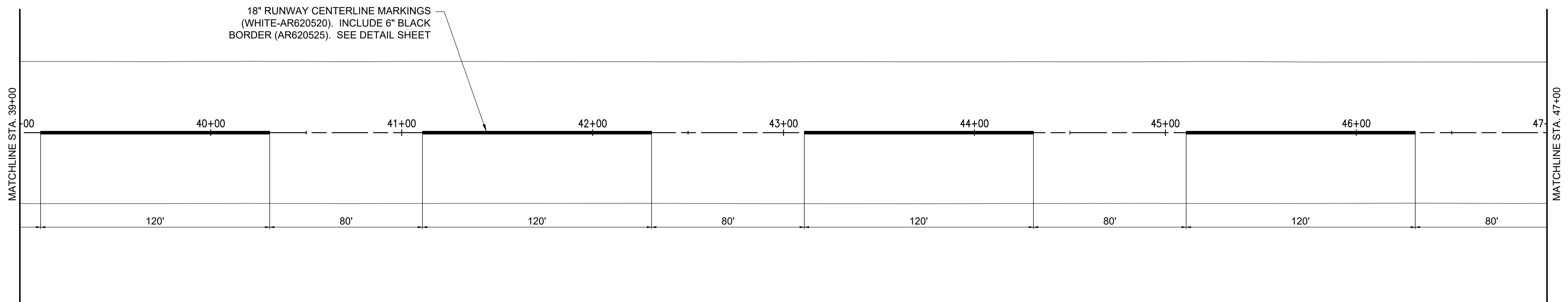
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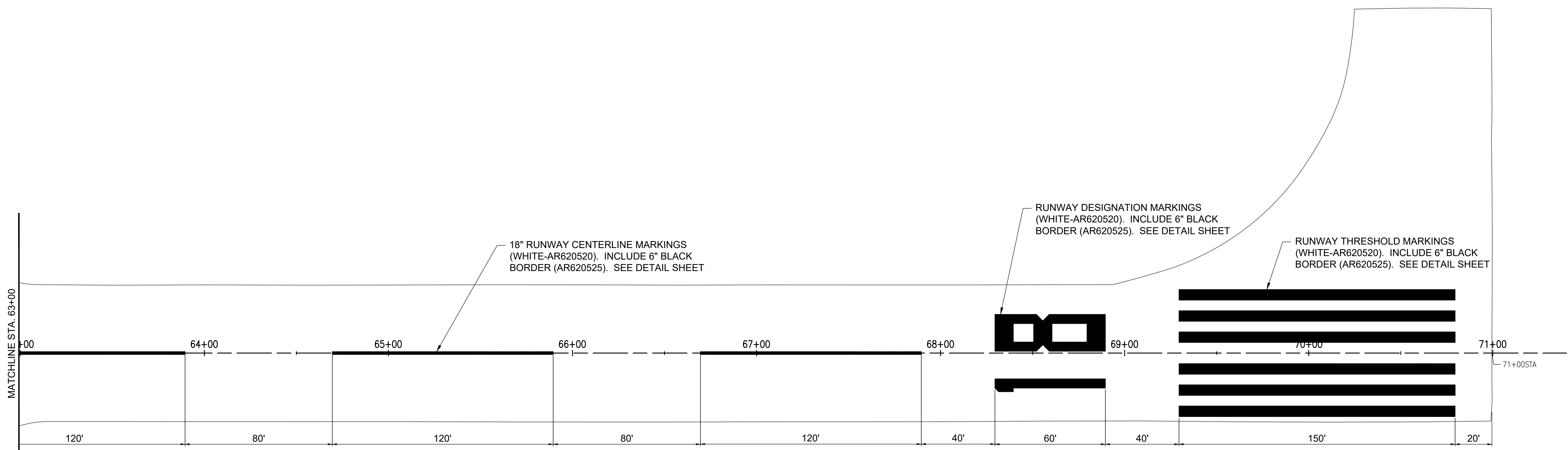
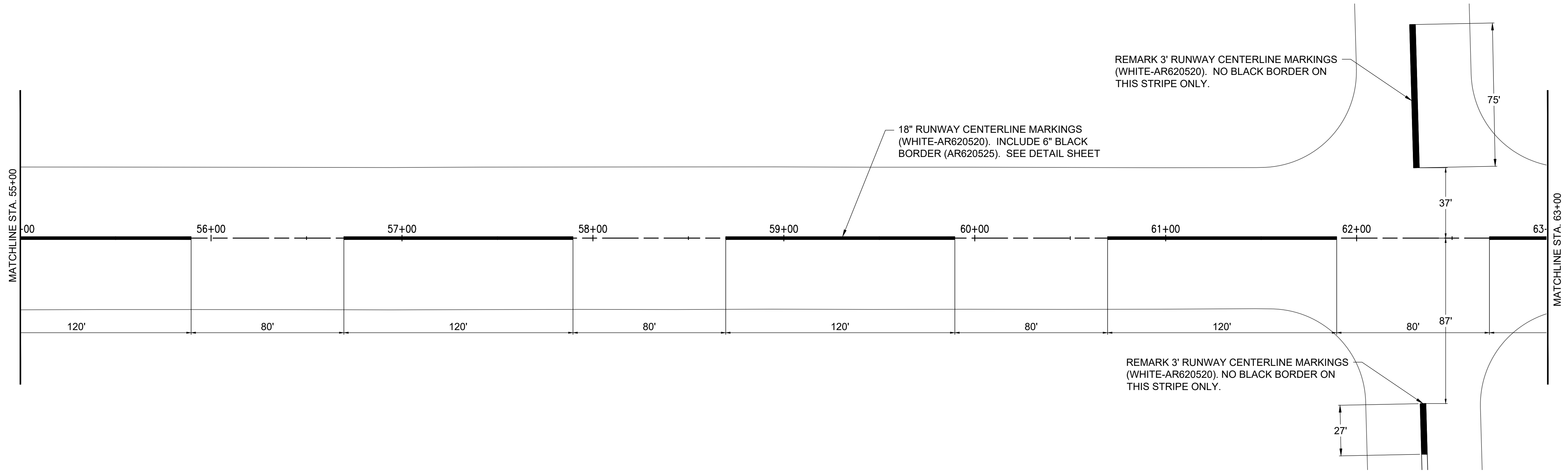
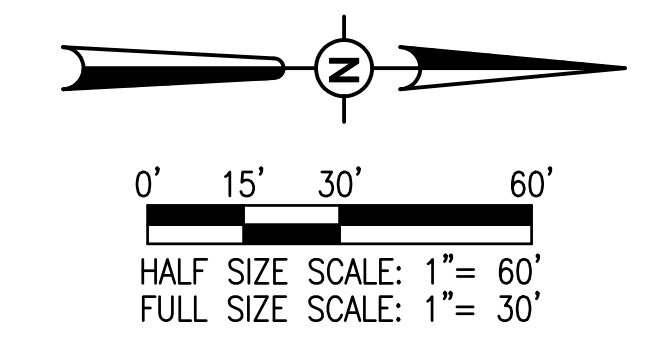
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**MARKING PLAN -
 SHEET 2**



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**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

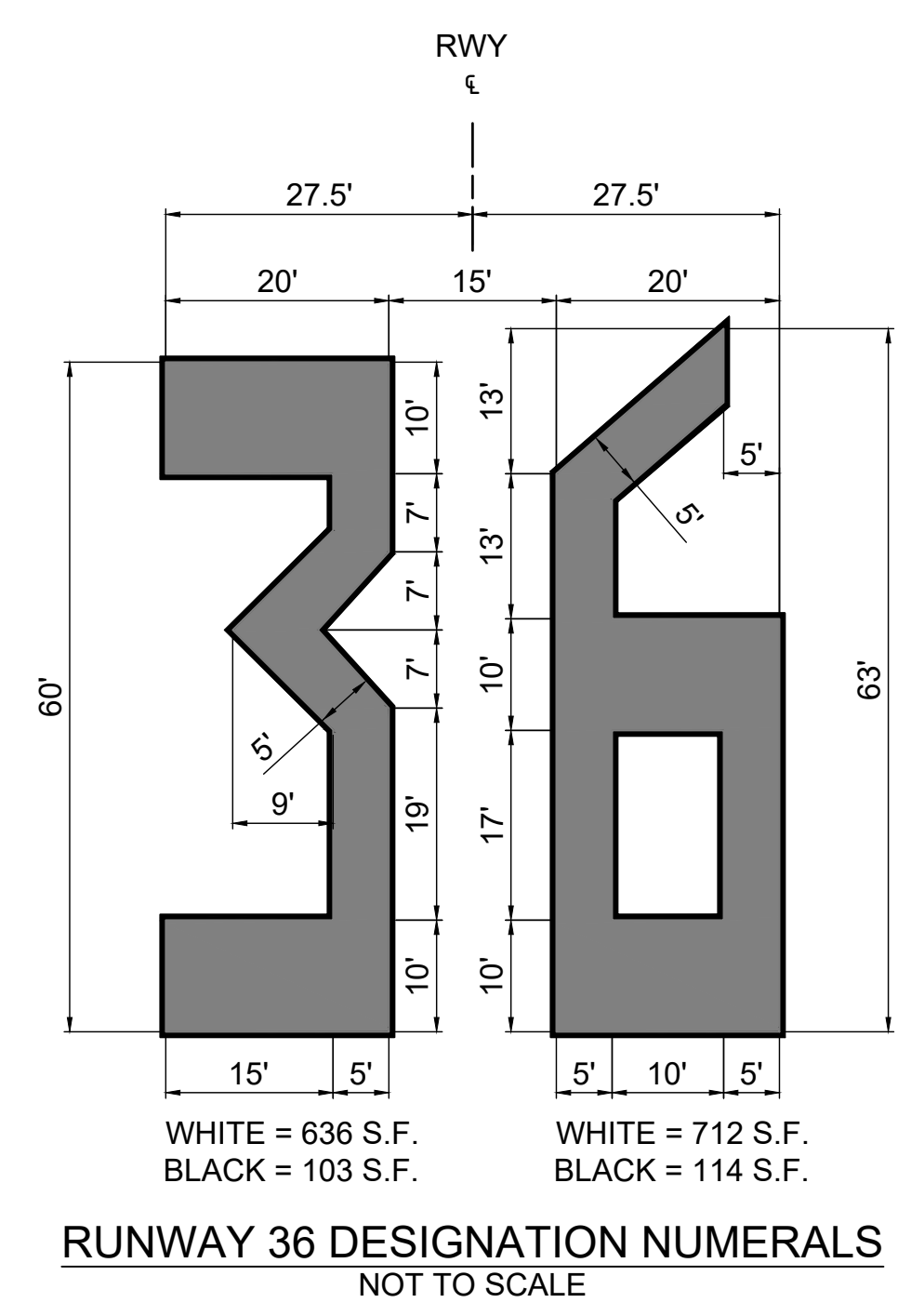
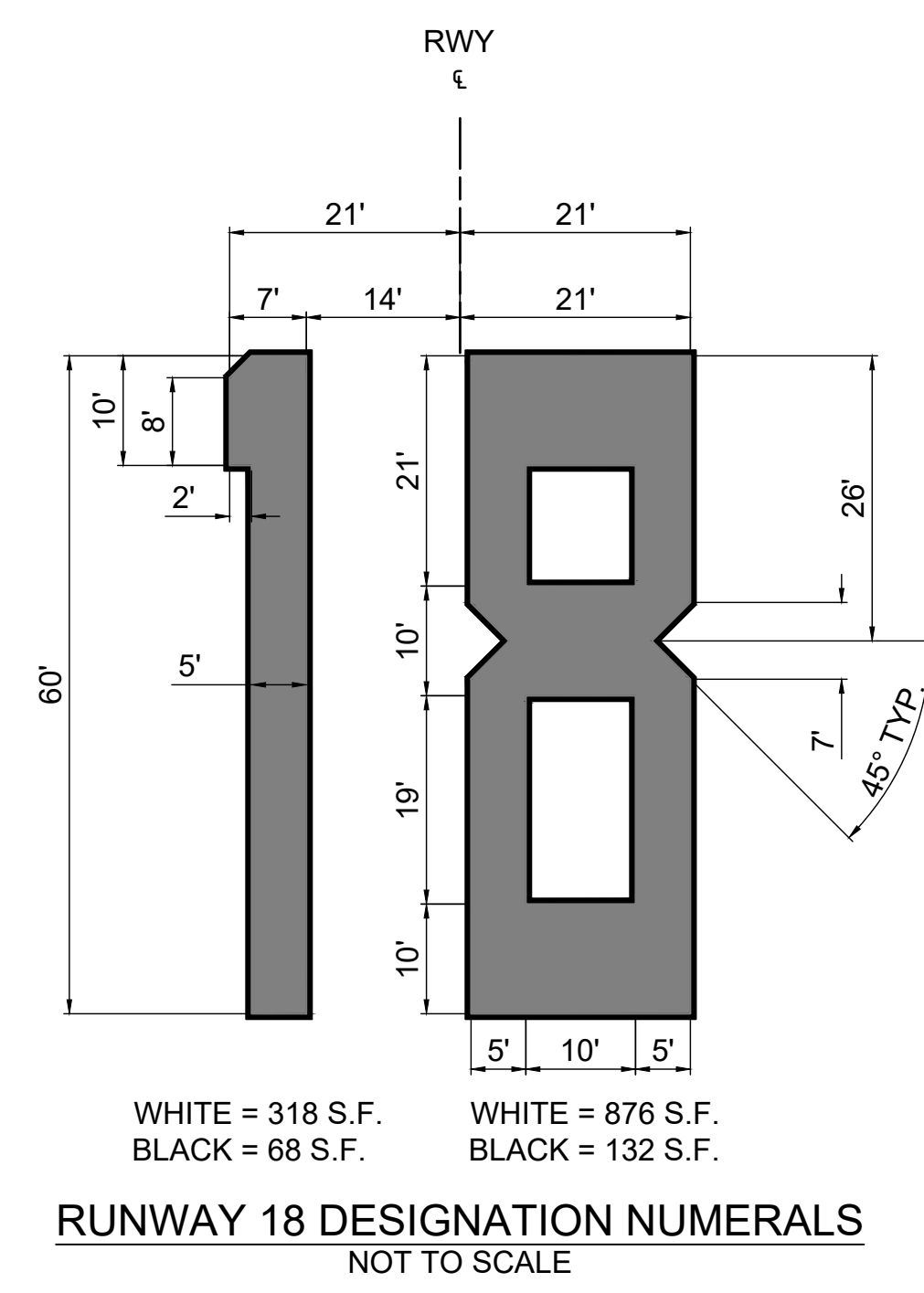
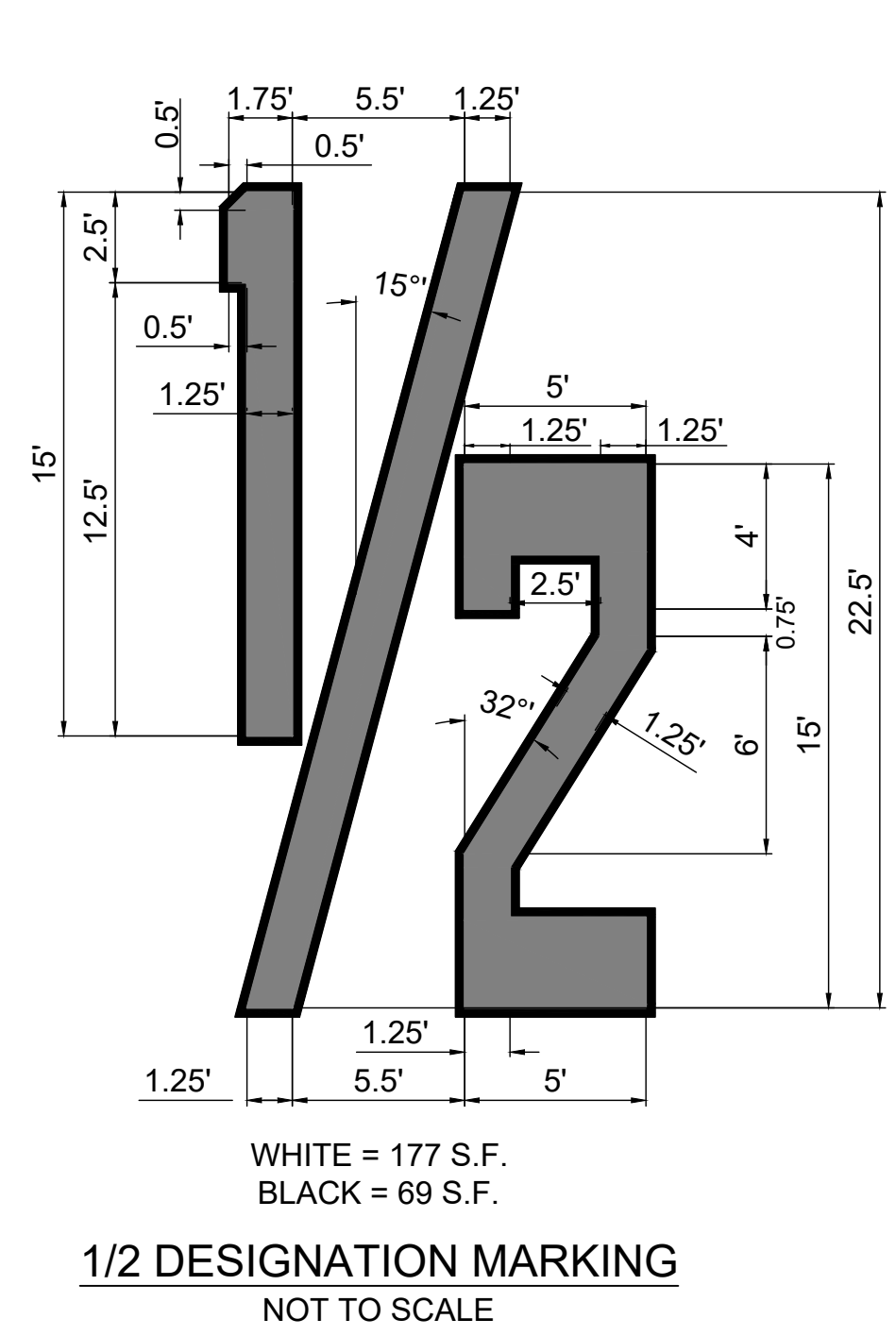
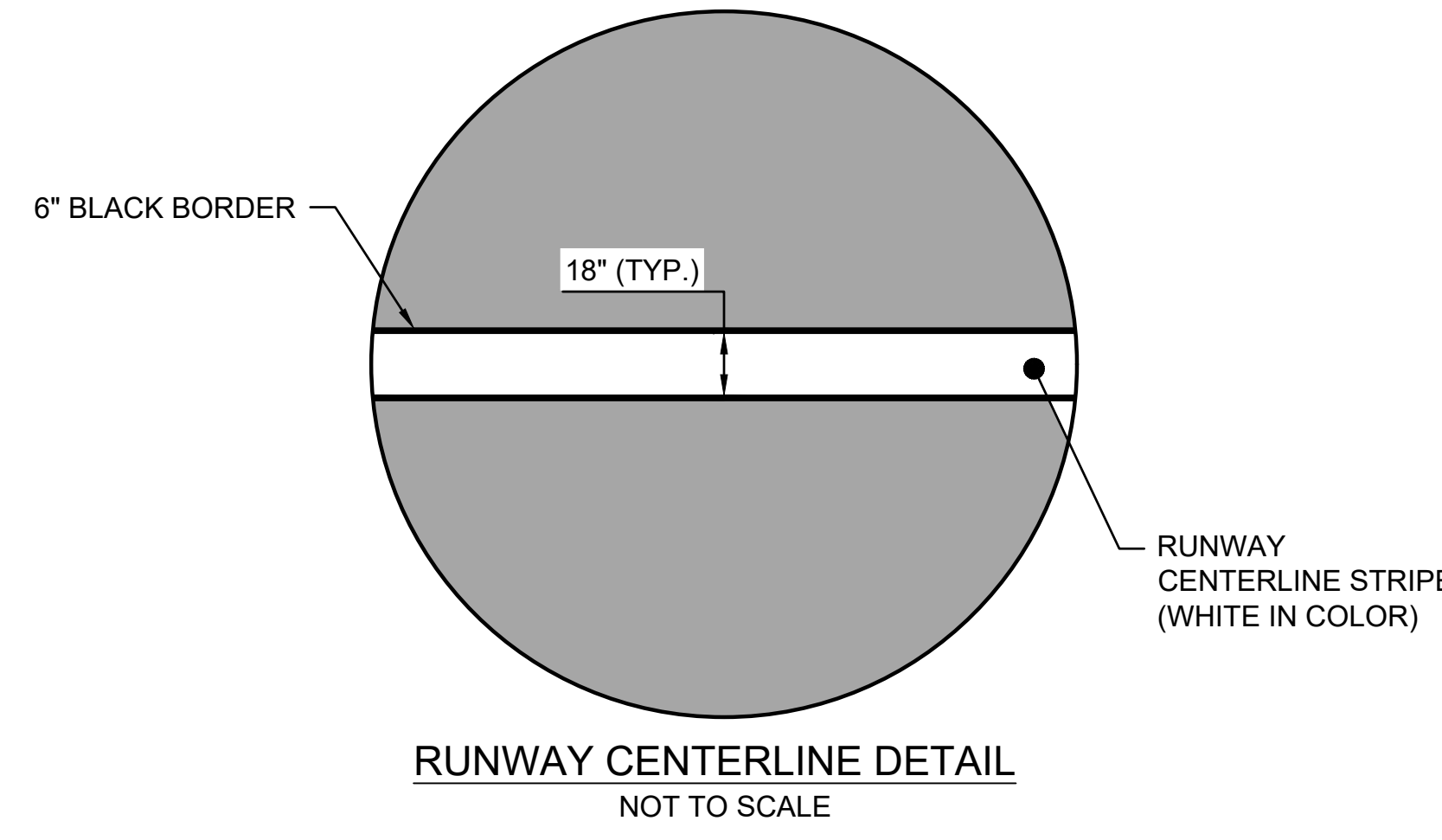
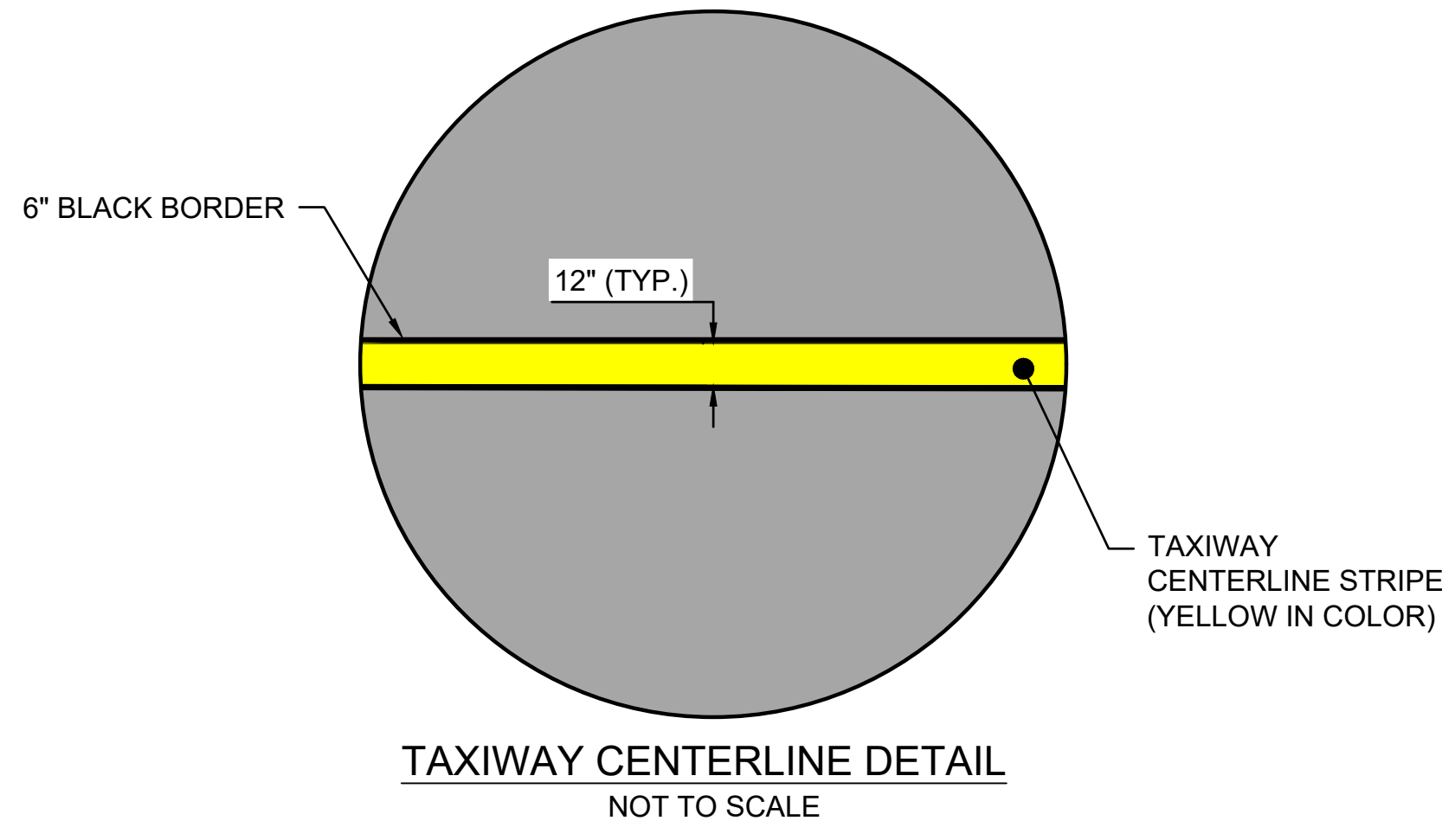
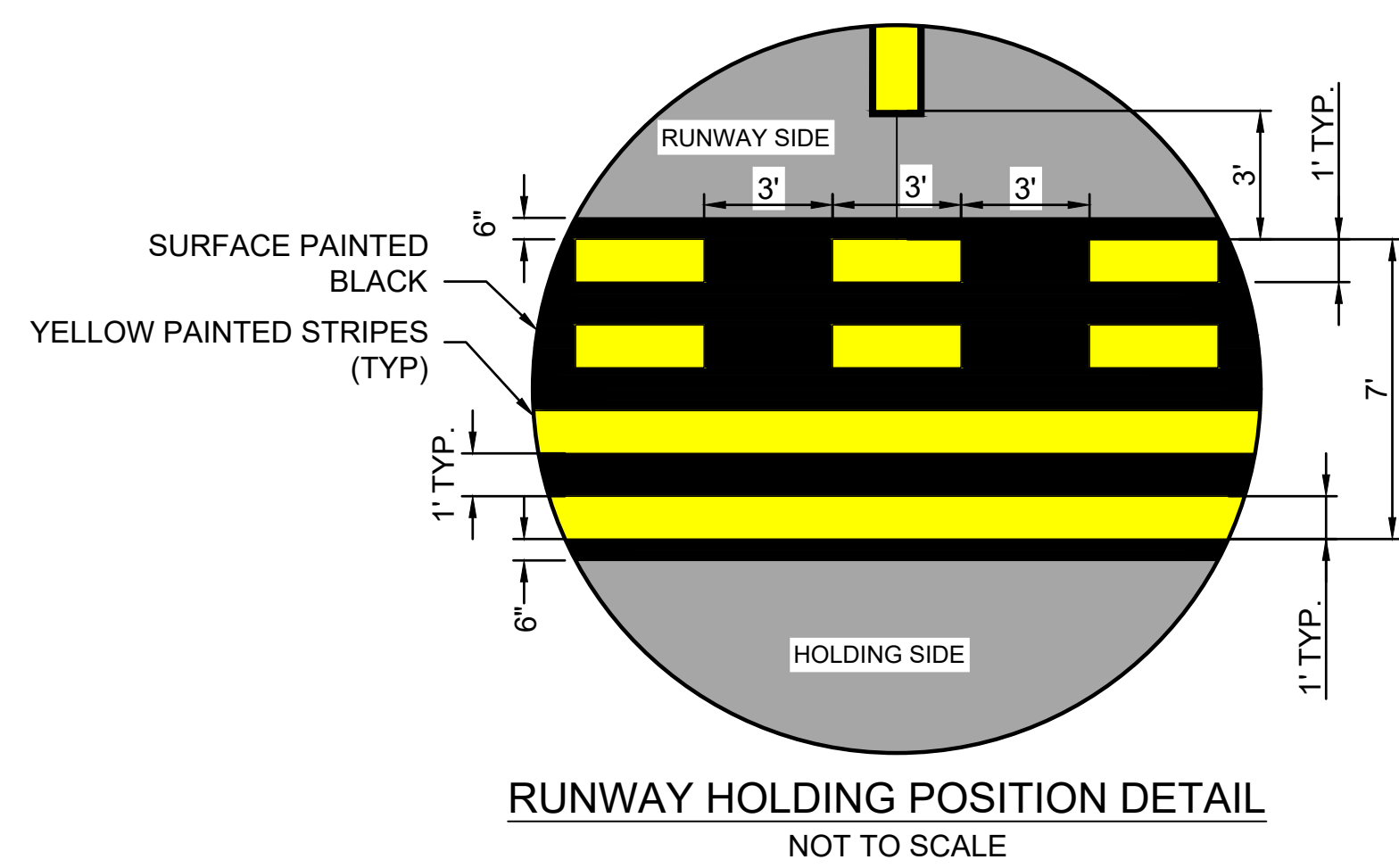
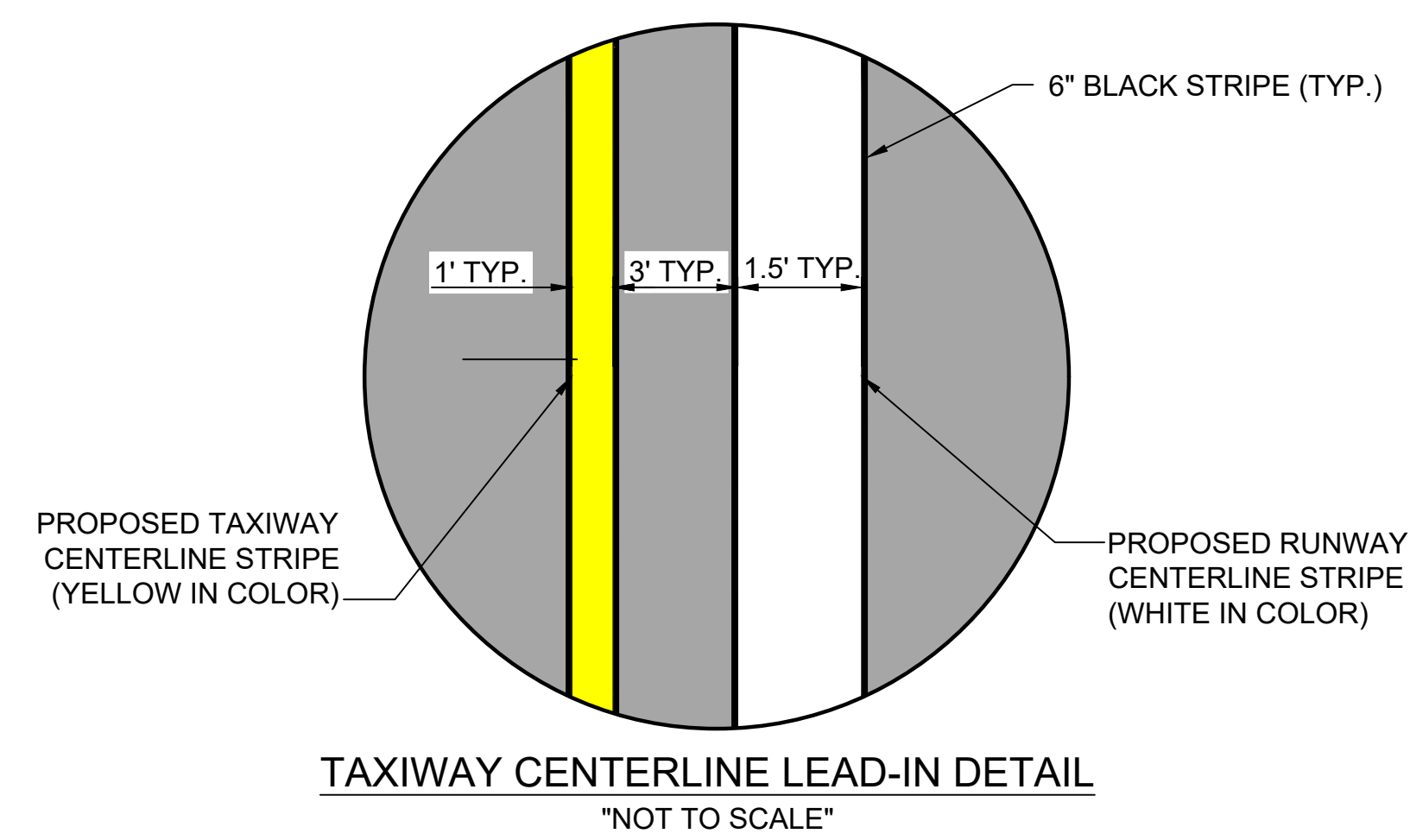
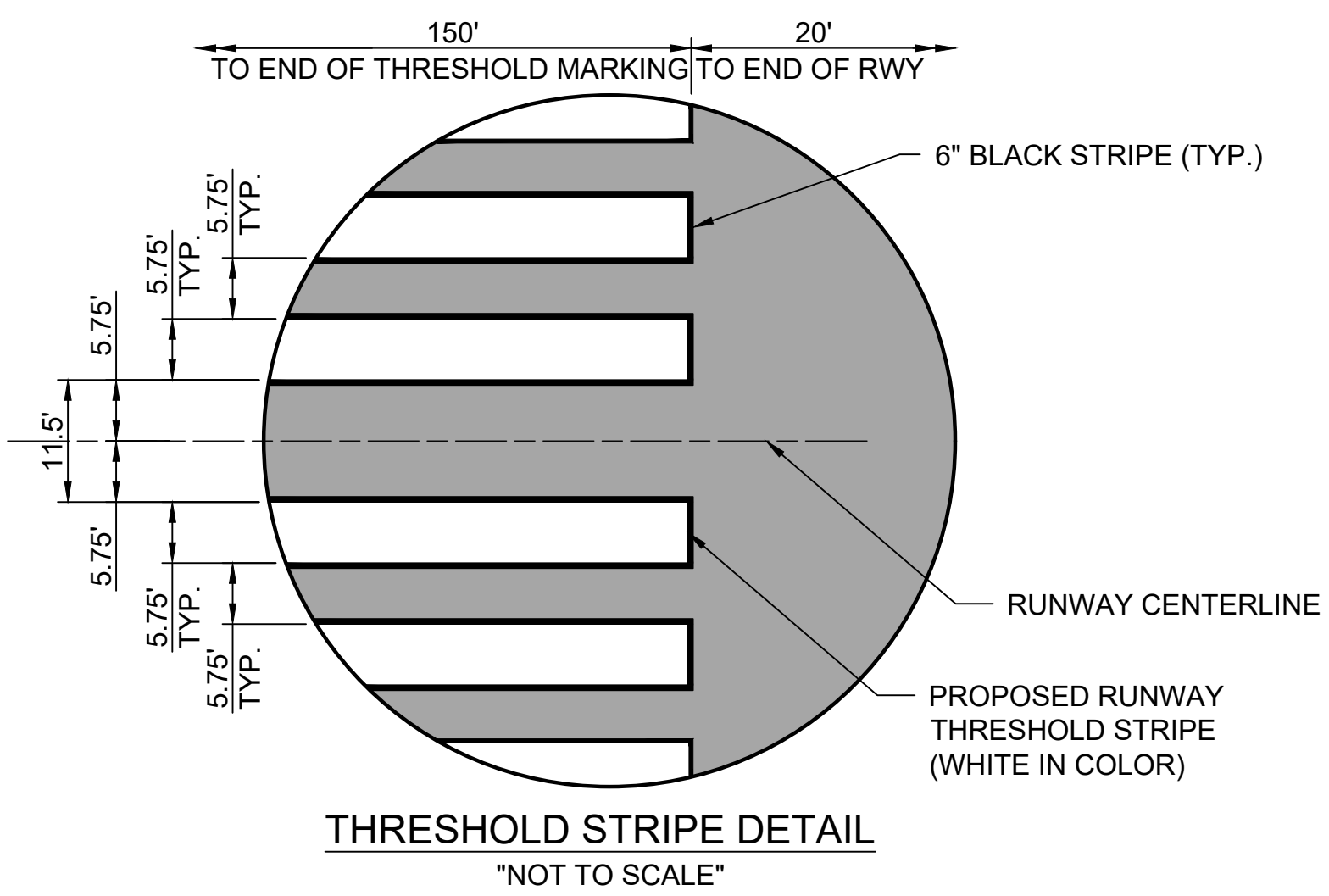
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PROJECT NO: 23A1039_00
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DESIGN BY: JAP 02/28/24
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SHEET TITLE

**MARKING PLAN -
SHEET 3**

FOR BID

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- RUNWAY DESIGNATION MARKING NOTES:**
- ALL NUMERAL MARKING WILL BE WHITE IN COLOR WITH A 6" BLACK OUTLINE.
 - NUMERALS ARE HORIZONTALLY SPACED 15 FEET APART.
 - DOUBLE DIGIT NUMERAL DESIGNATIONS ARE CENTERED ON THE RUNWAY PAVEMENT CENTERLINE BASED ON THE CENTER OF THE OUTER EDGES OF THE TWO NUMERALS.
 - ALL DIMENSIONS SHOWN ARE FOR WHITE MARKINGS.

**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

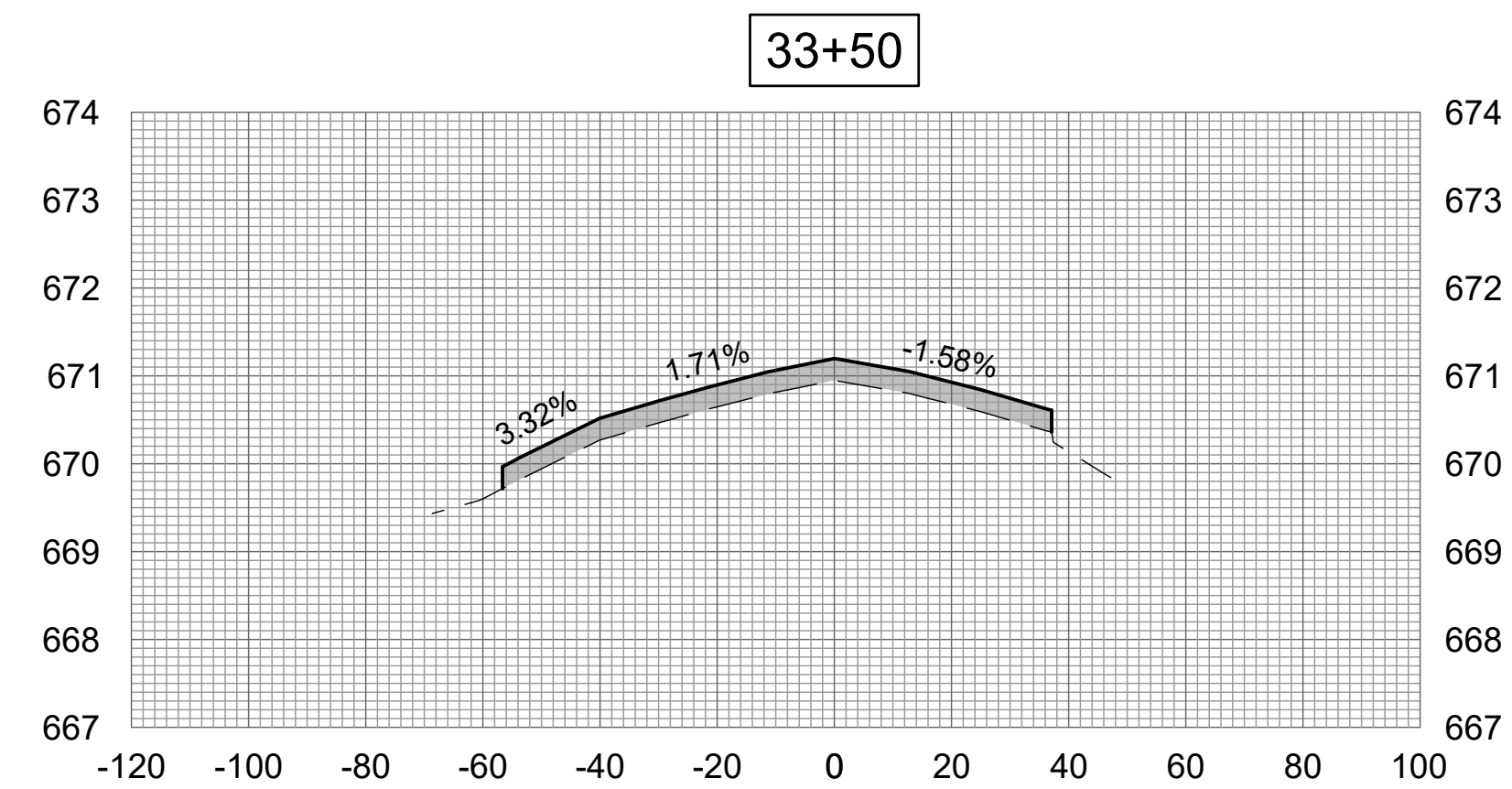
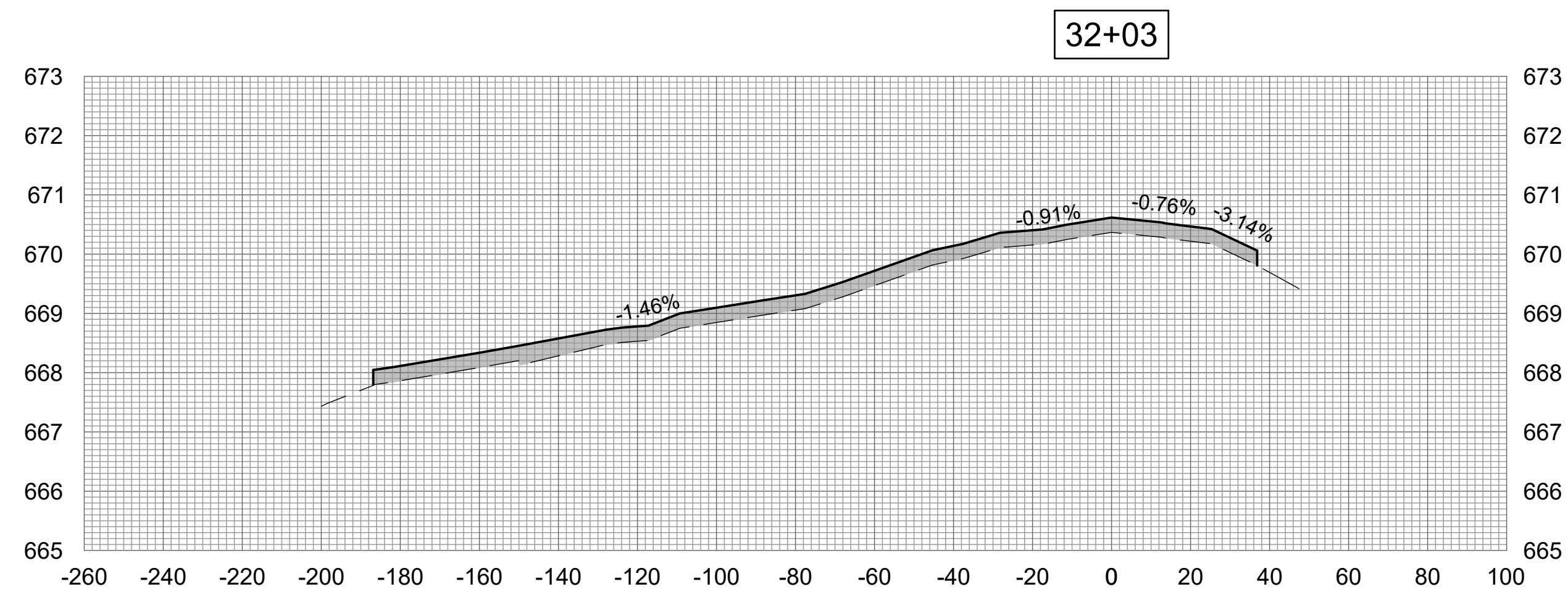
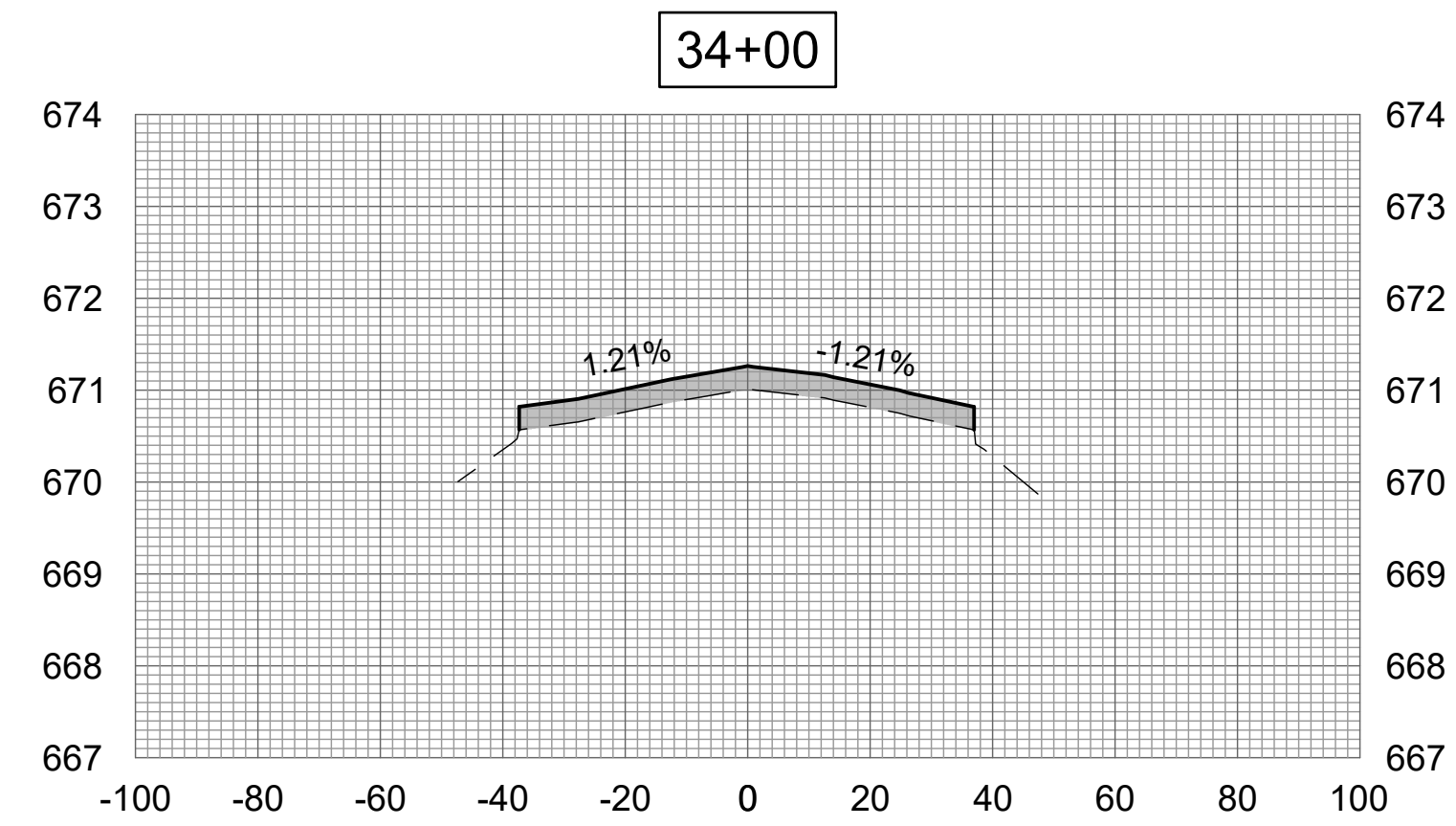
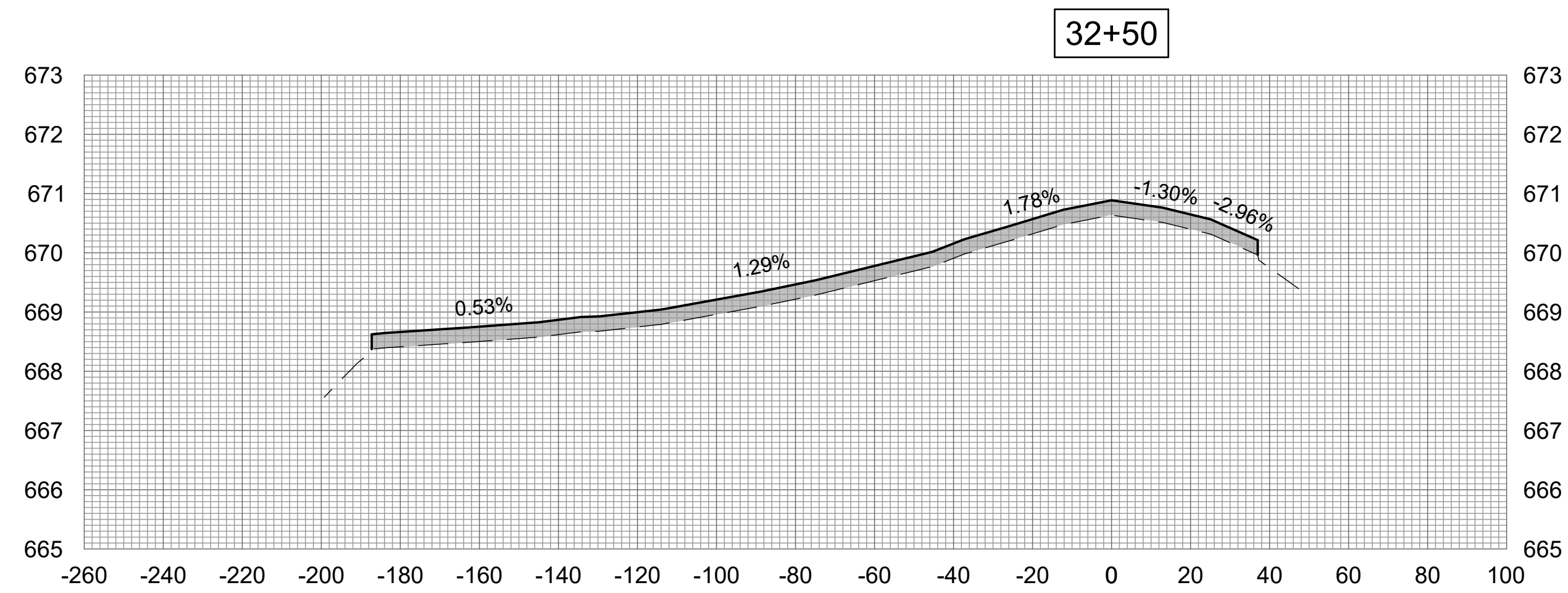
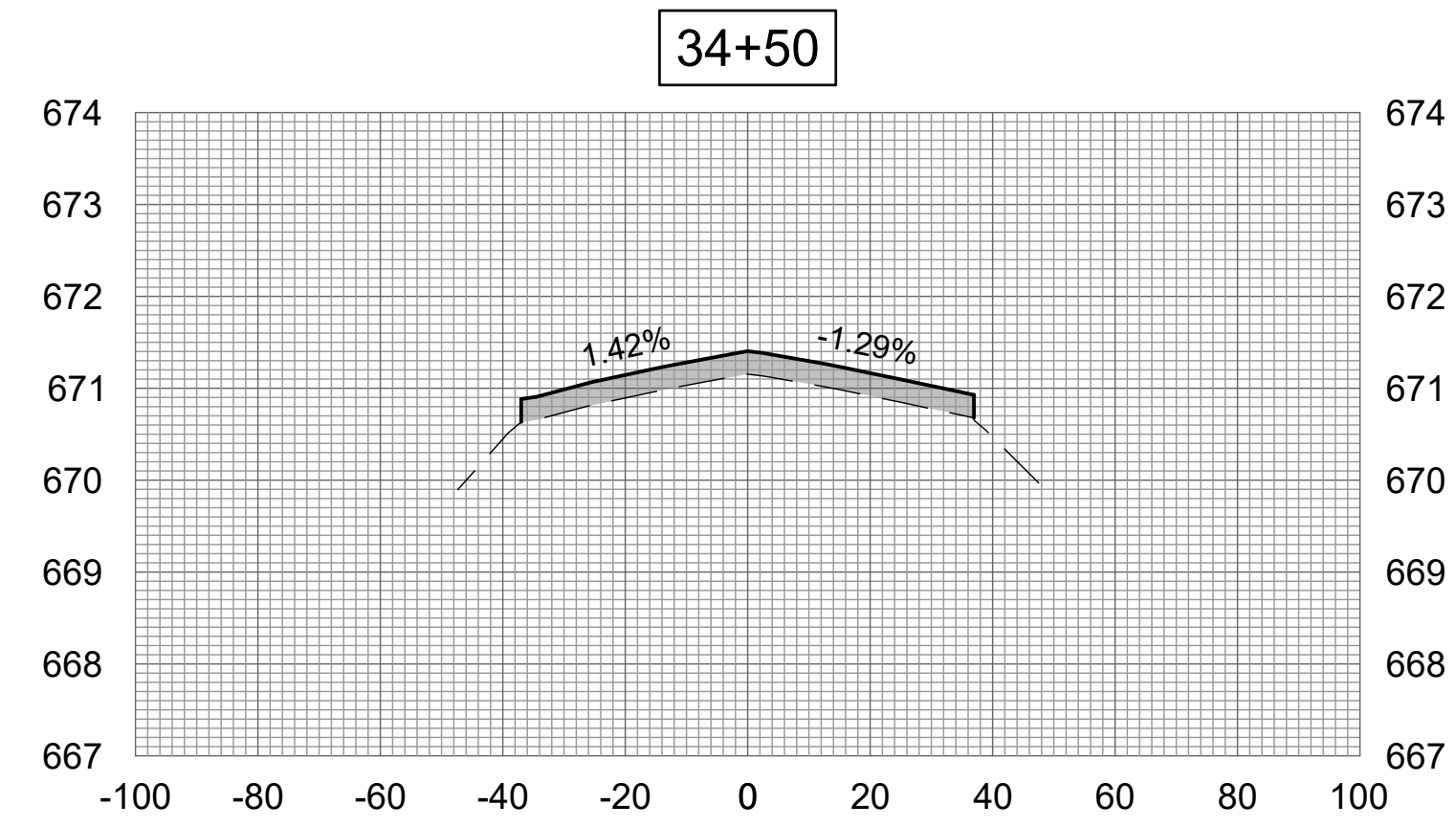
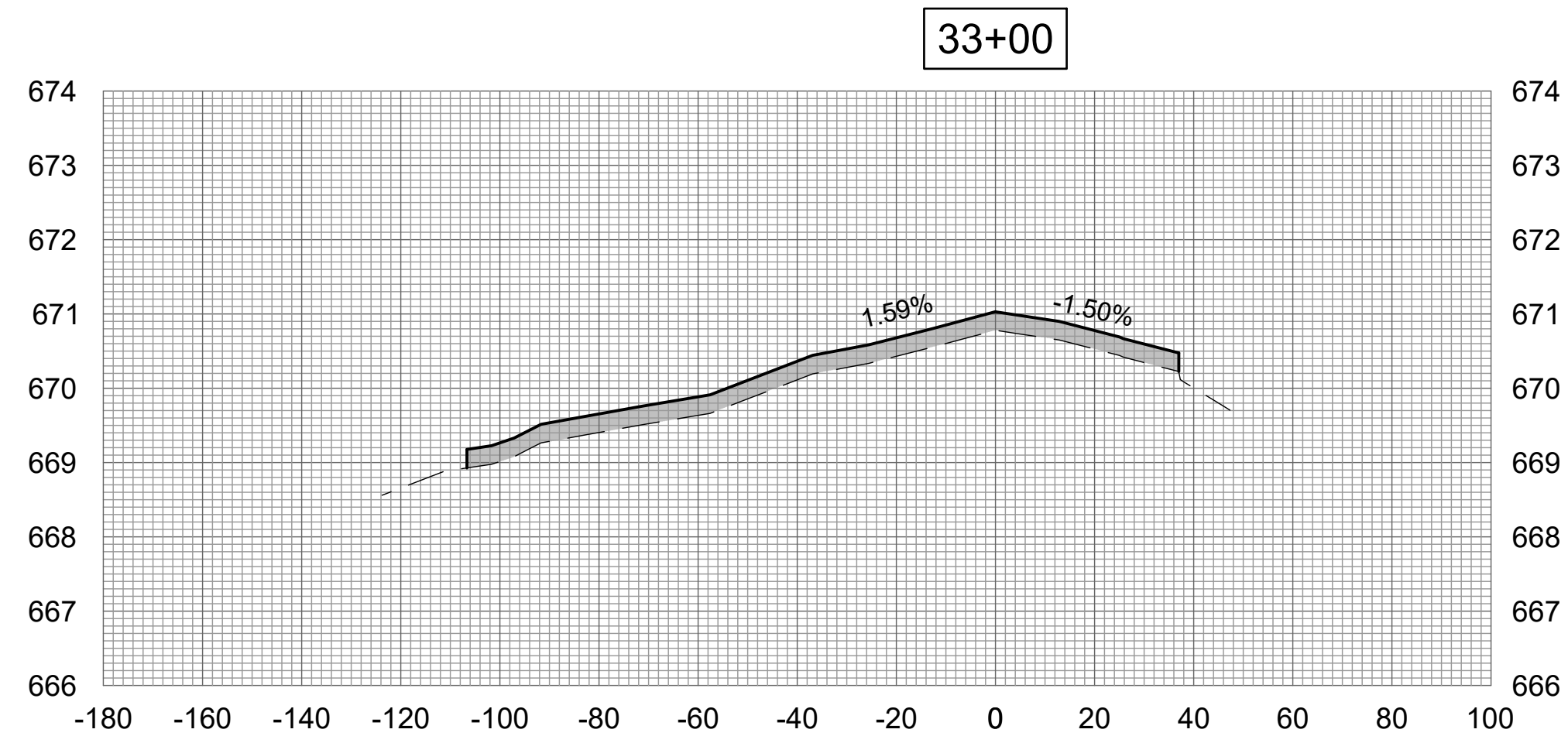
Contract No. CN028

NO.	DATE	DESCRIPTION

NO.	DATE	DESCRIPTION

MARKING DETAILS

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**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

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Contract No. CN028

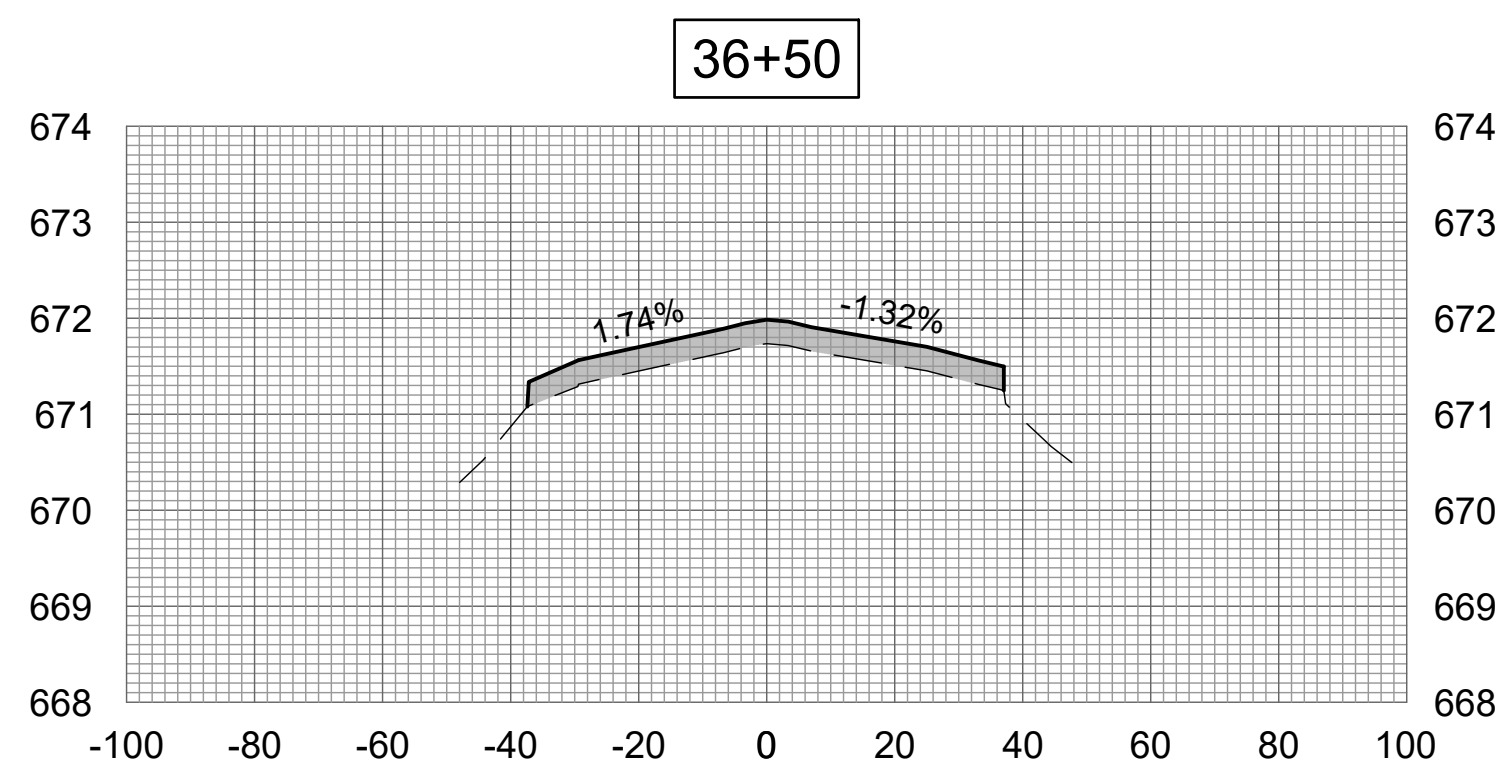
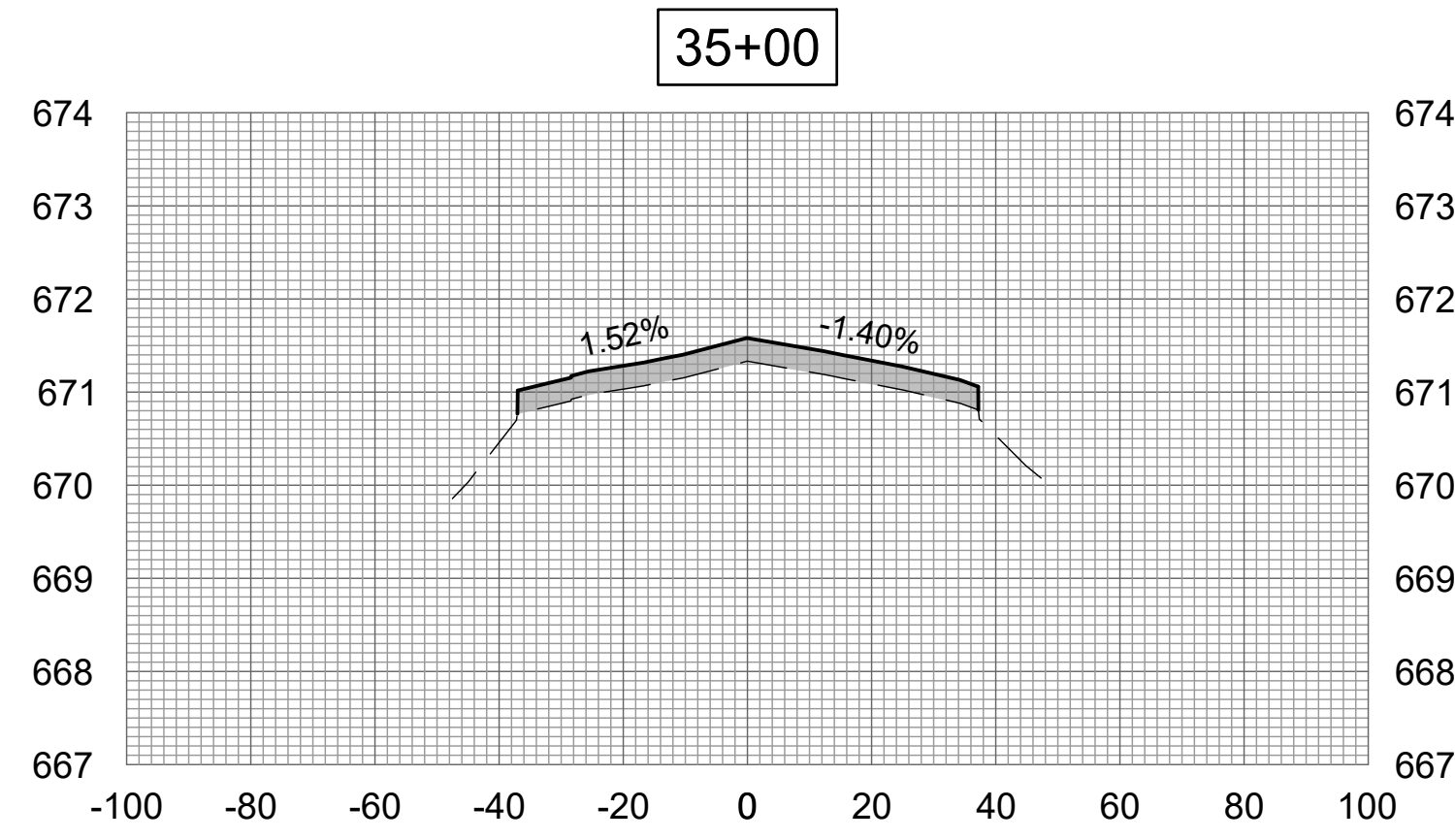
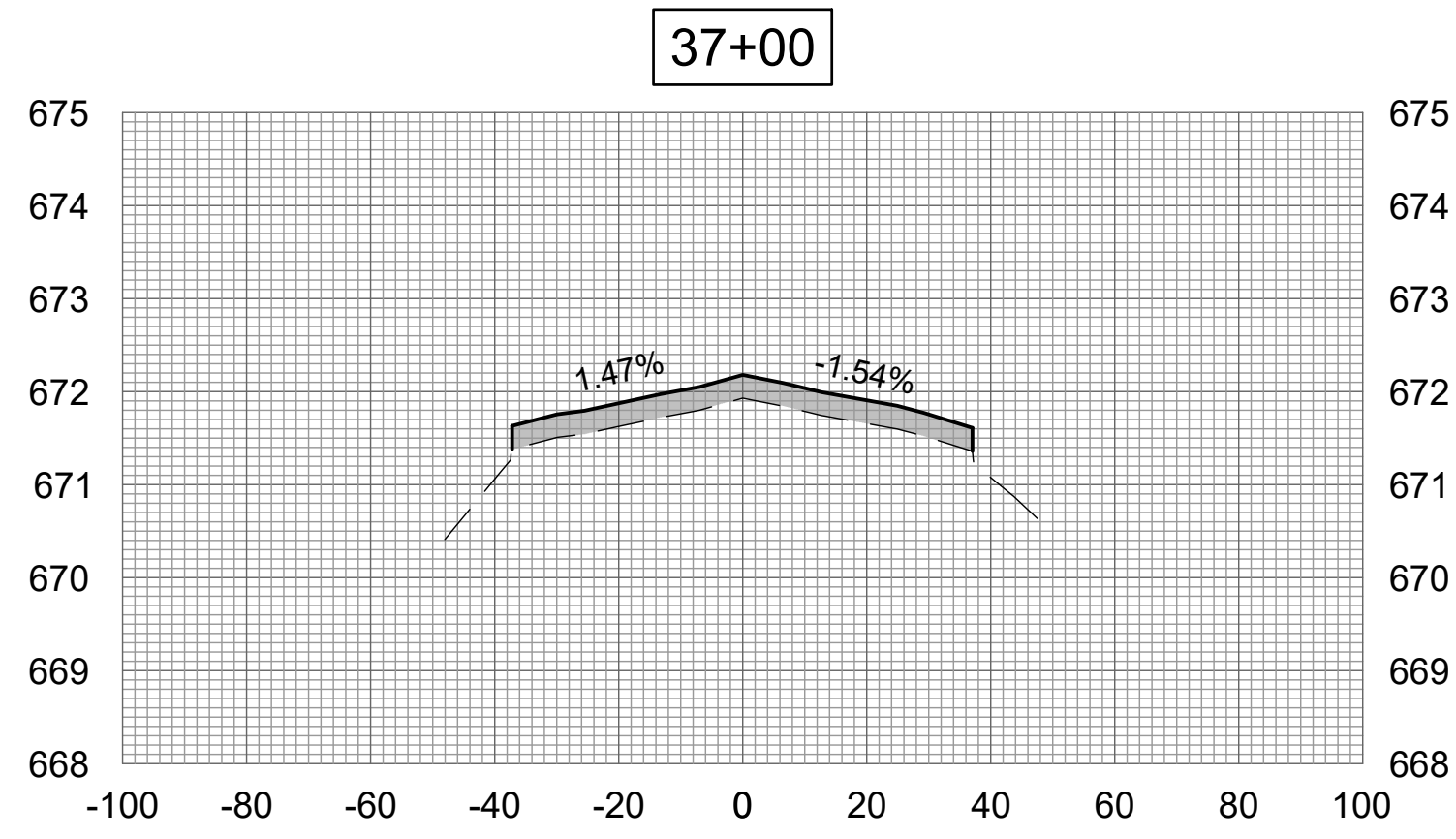
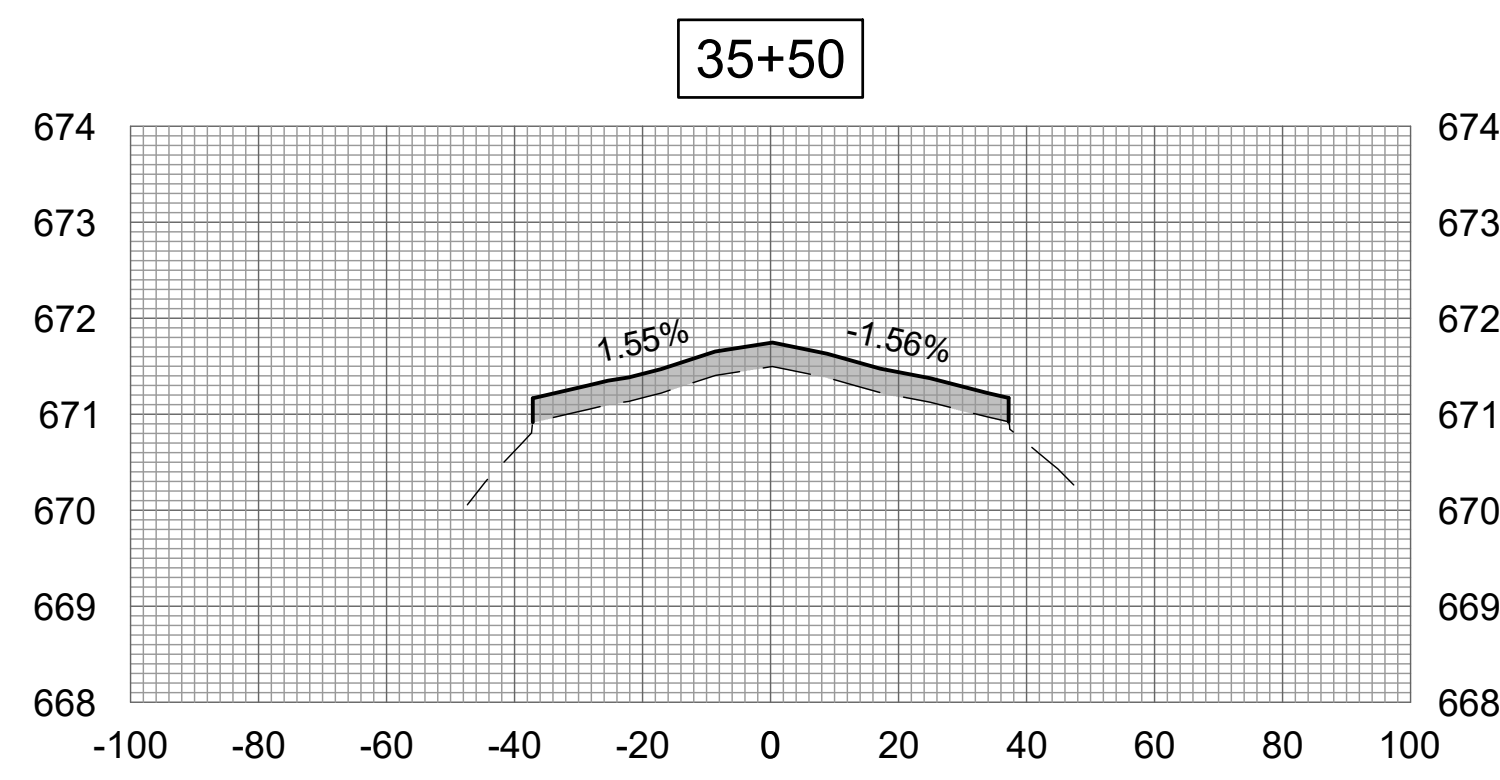
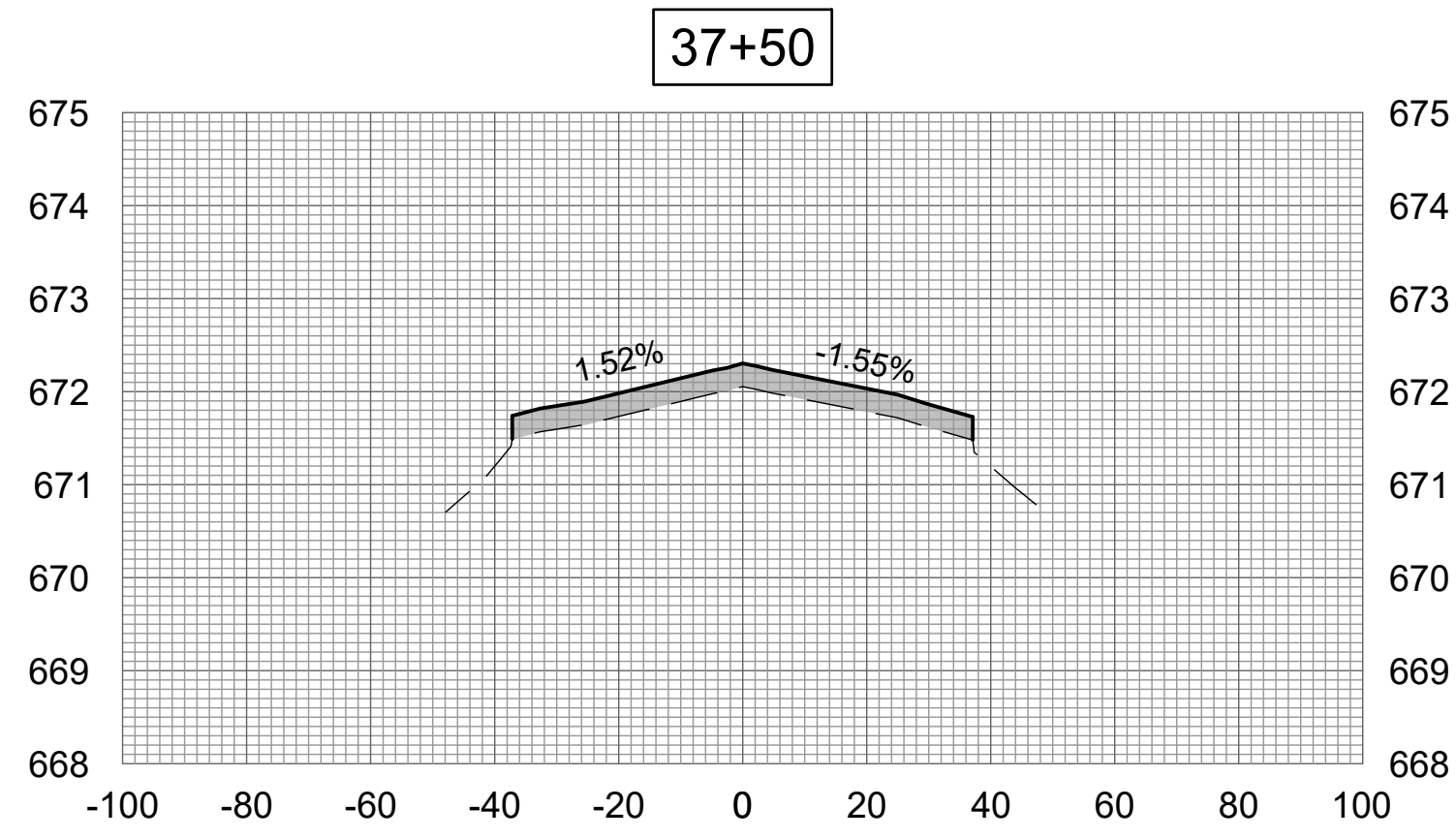
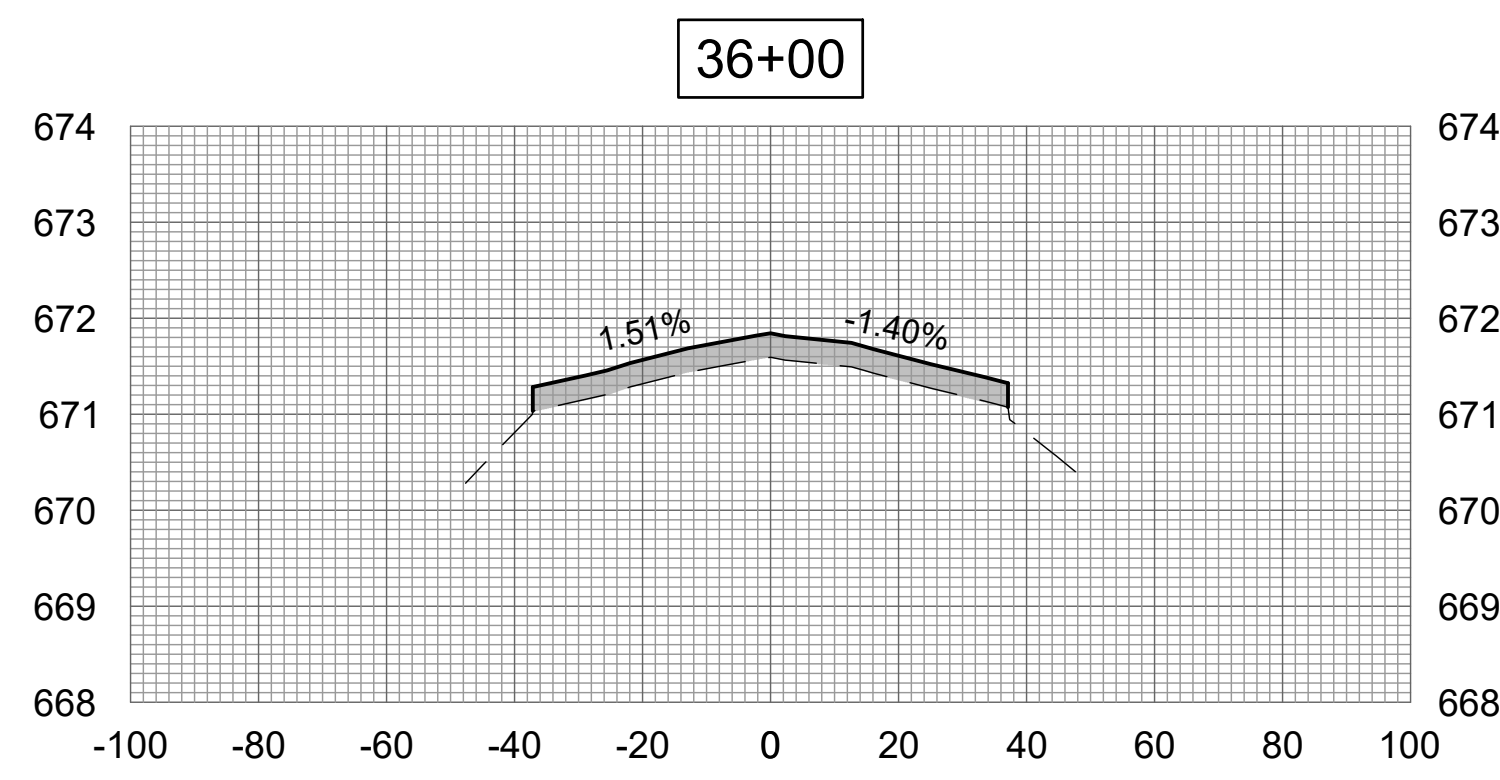
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CROSS-SECTION 1

FOR BID



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

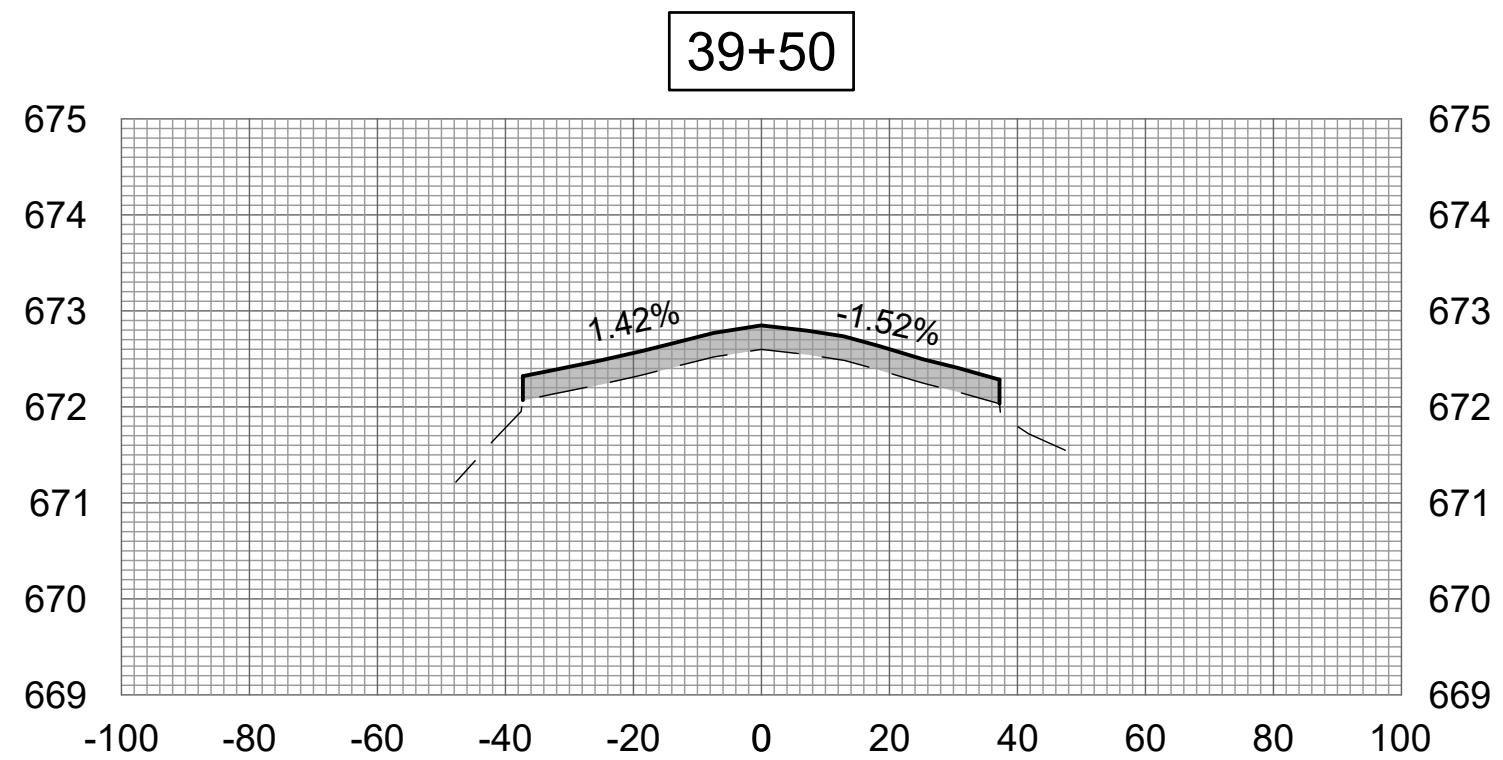
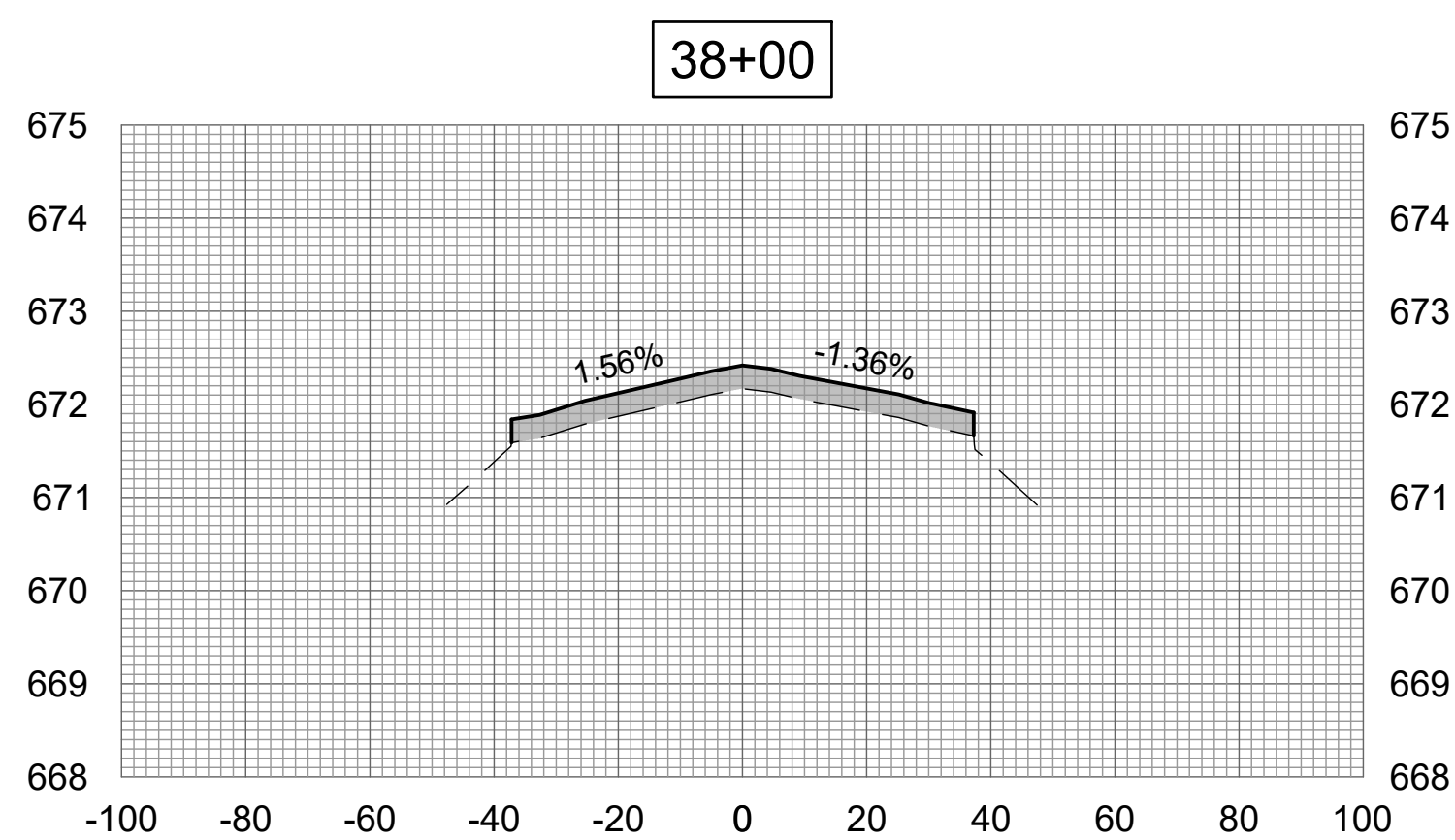
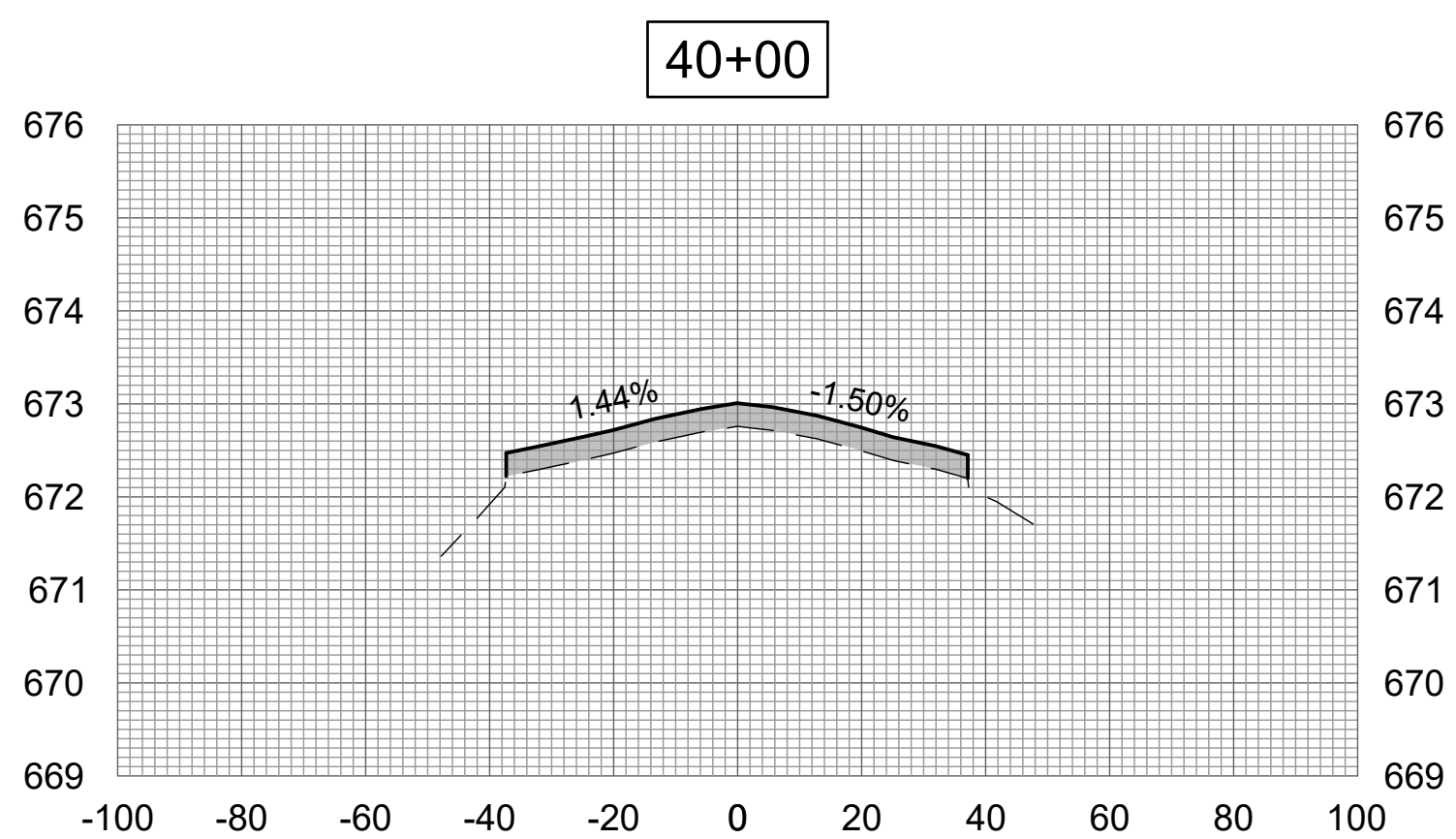
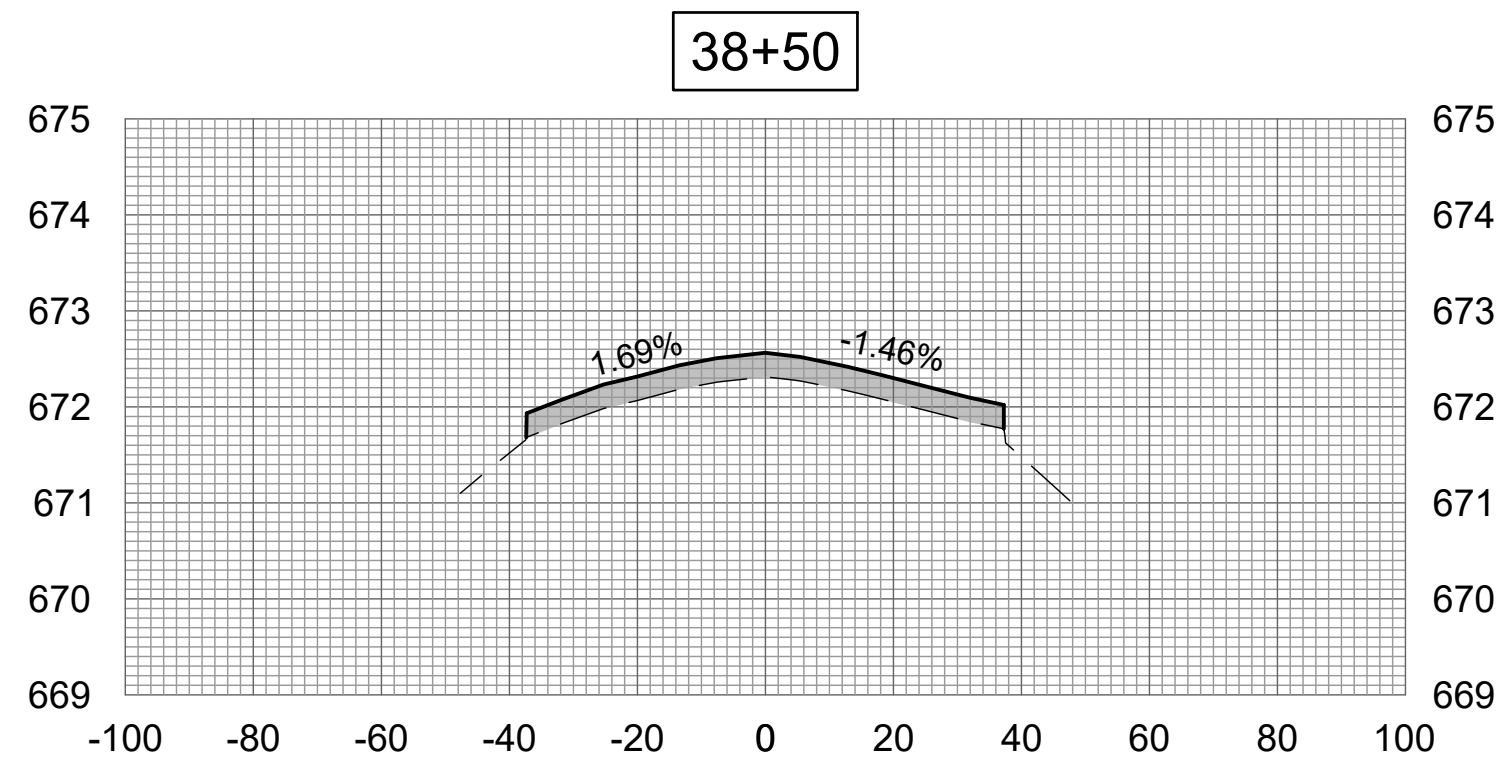
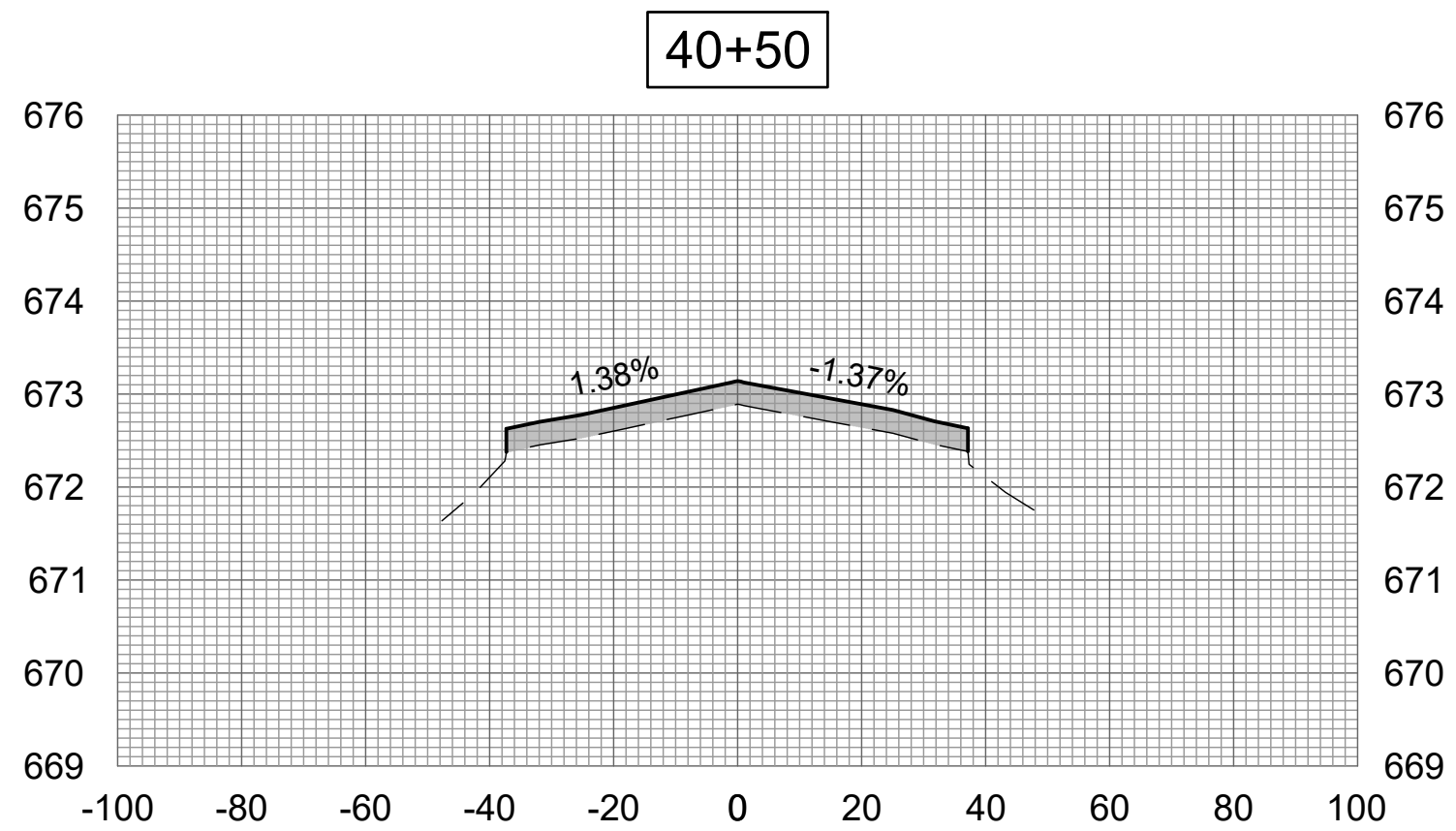
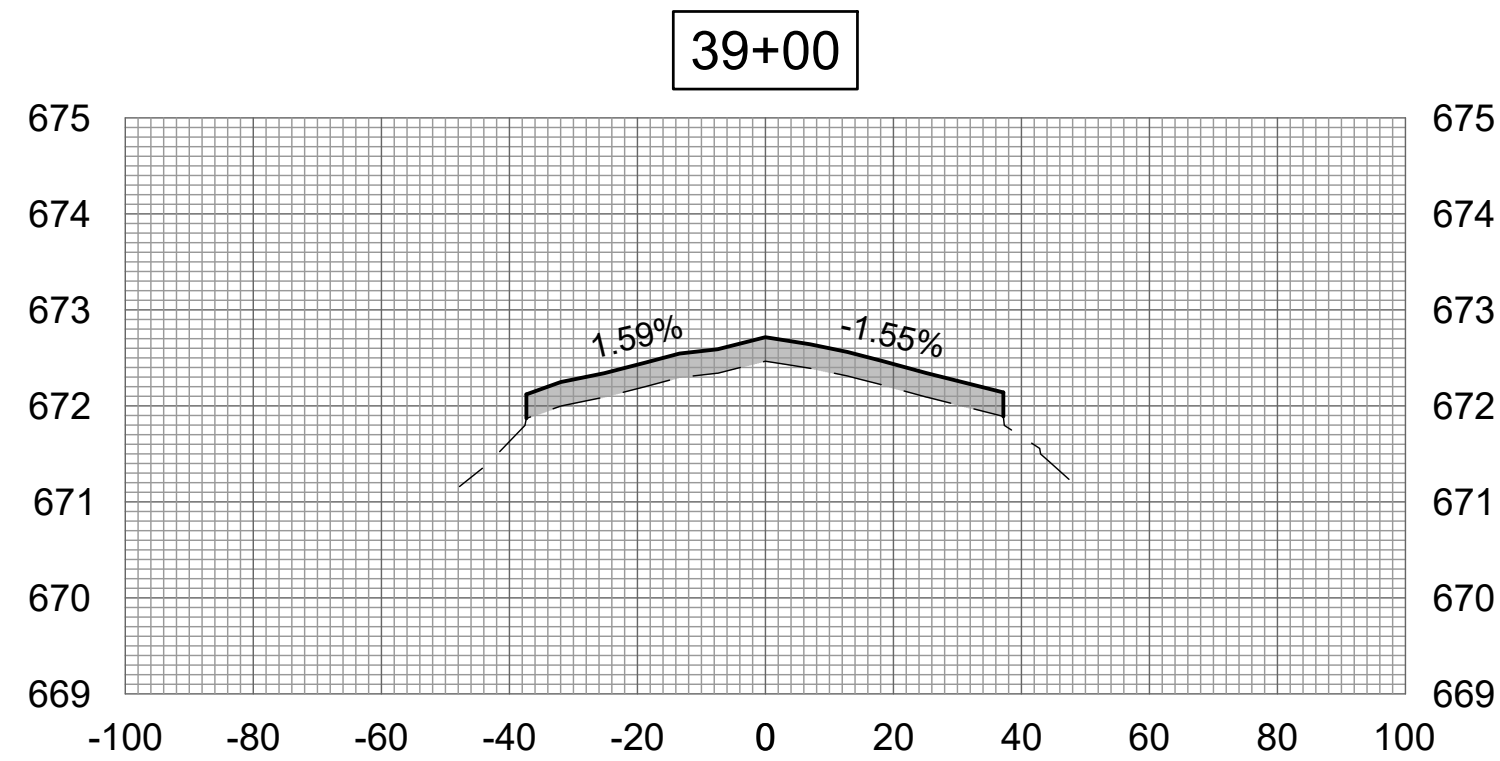
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SHEET TITLE

CROSS-SECTION 2

FOR BID



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

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SHEET TITLE

CROSS-SECTION 3

FOR BID



**RESURFACE
RUNWAY 18-36**

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IDA No: CTK-5121

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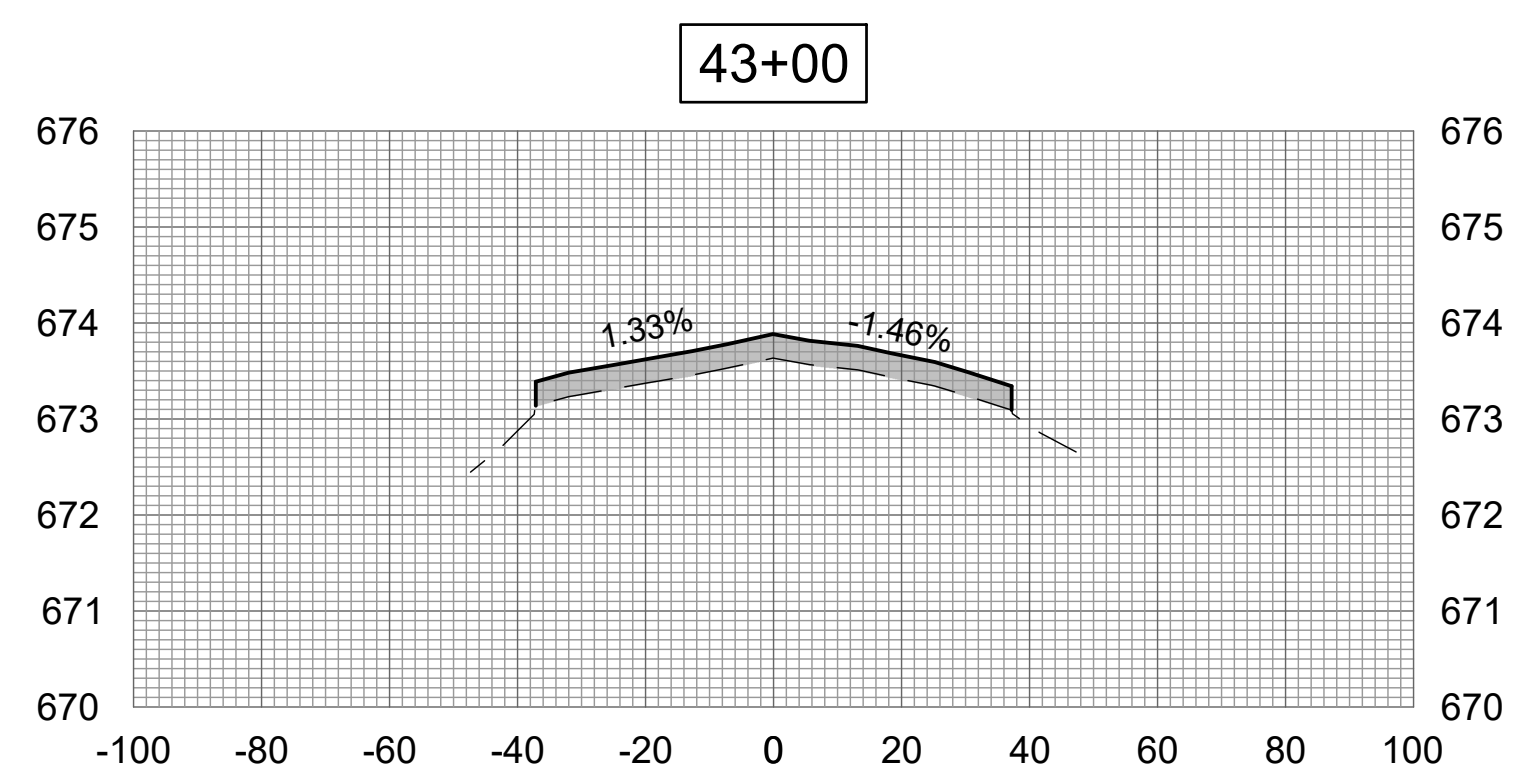
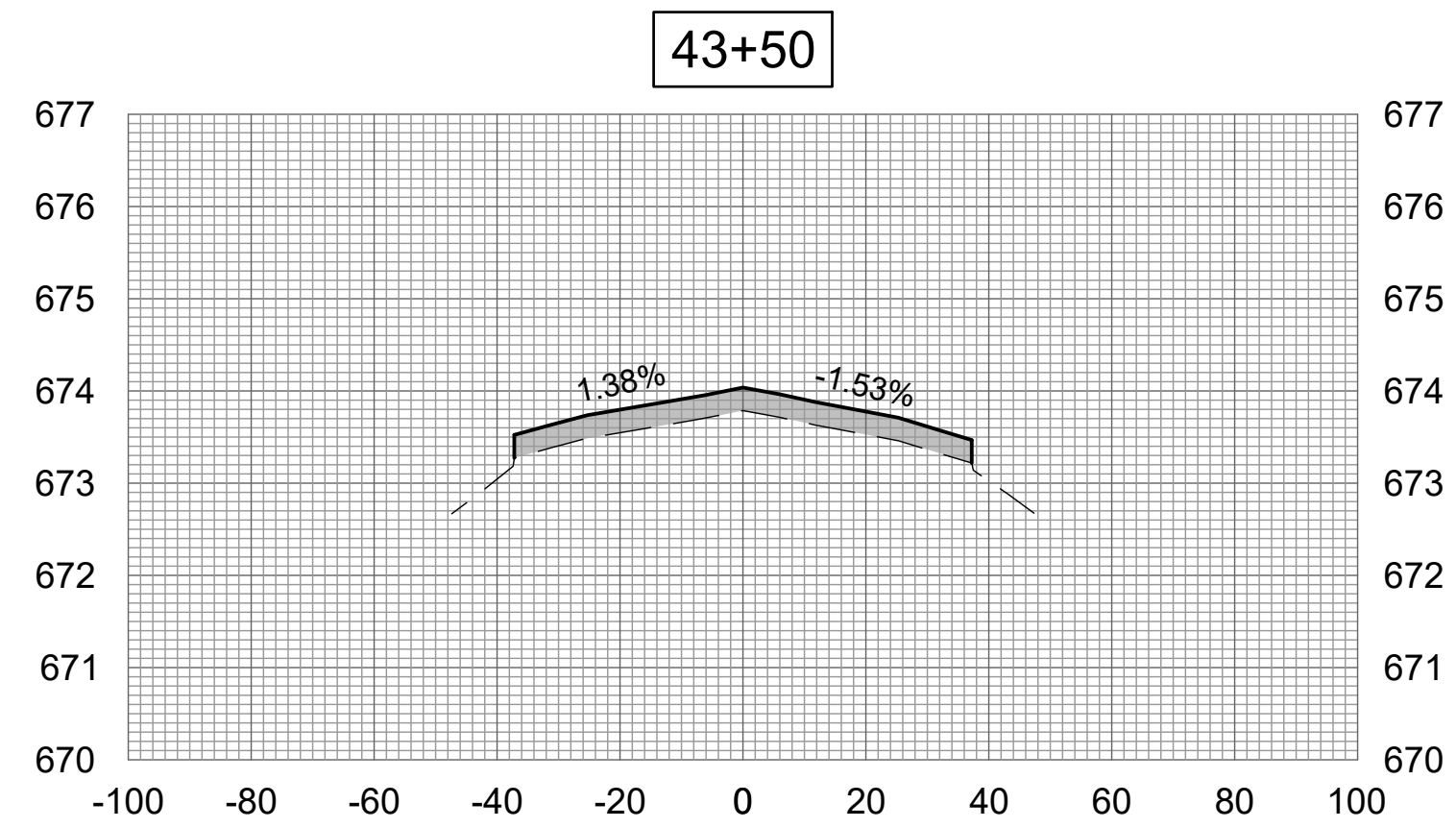
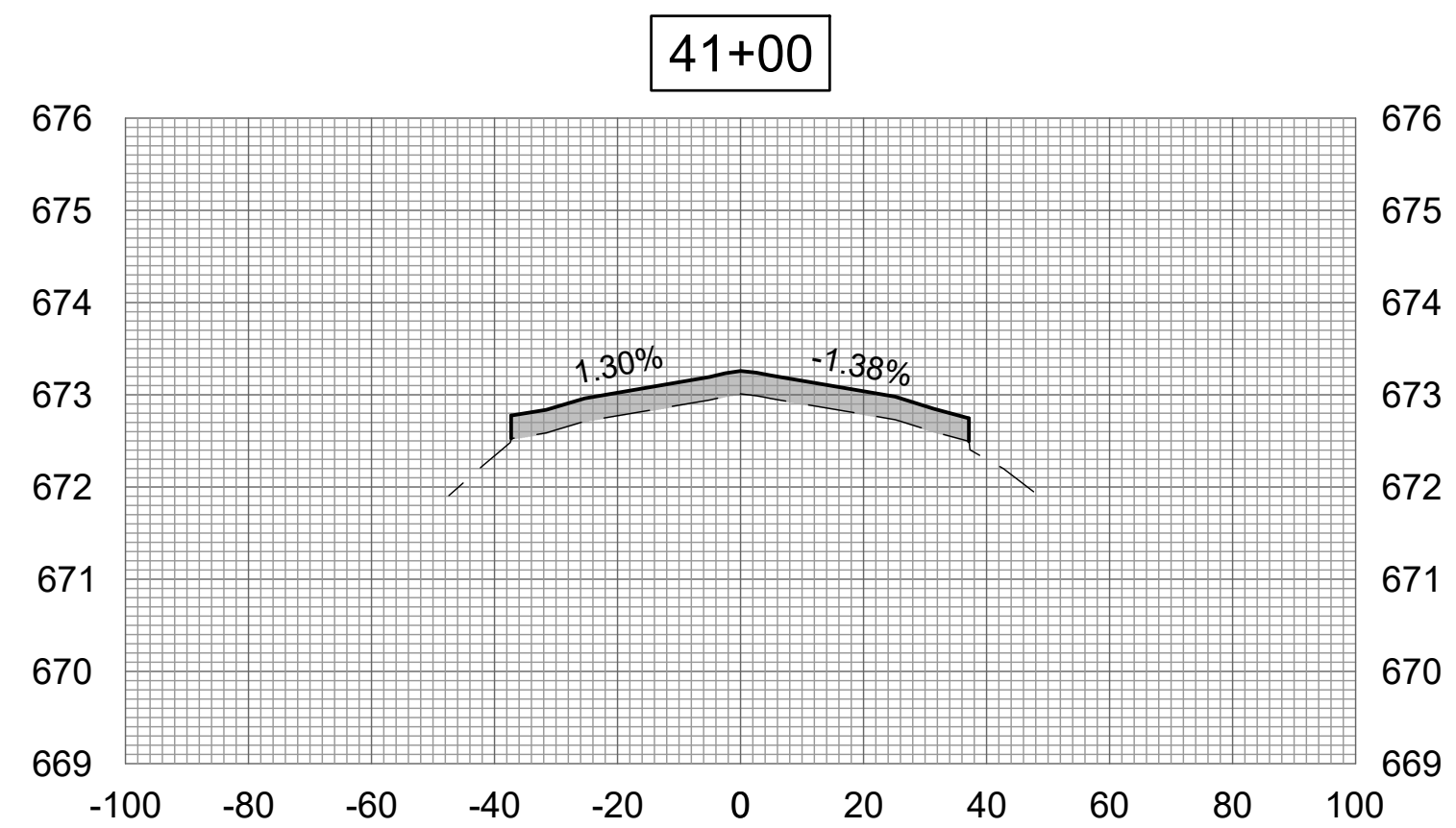
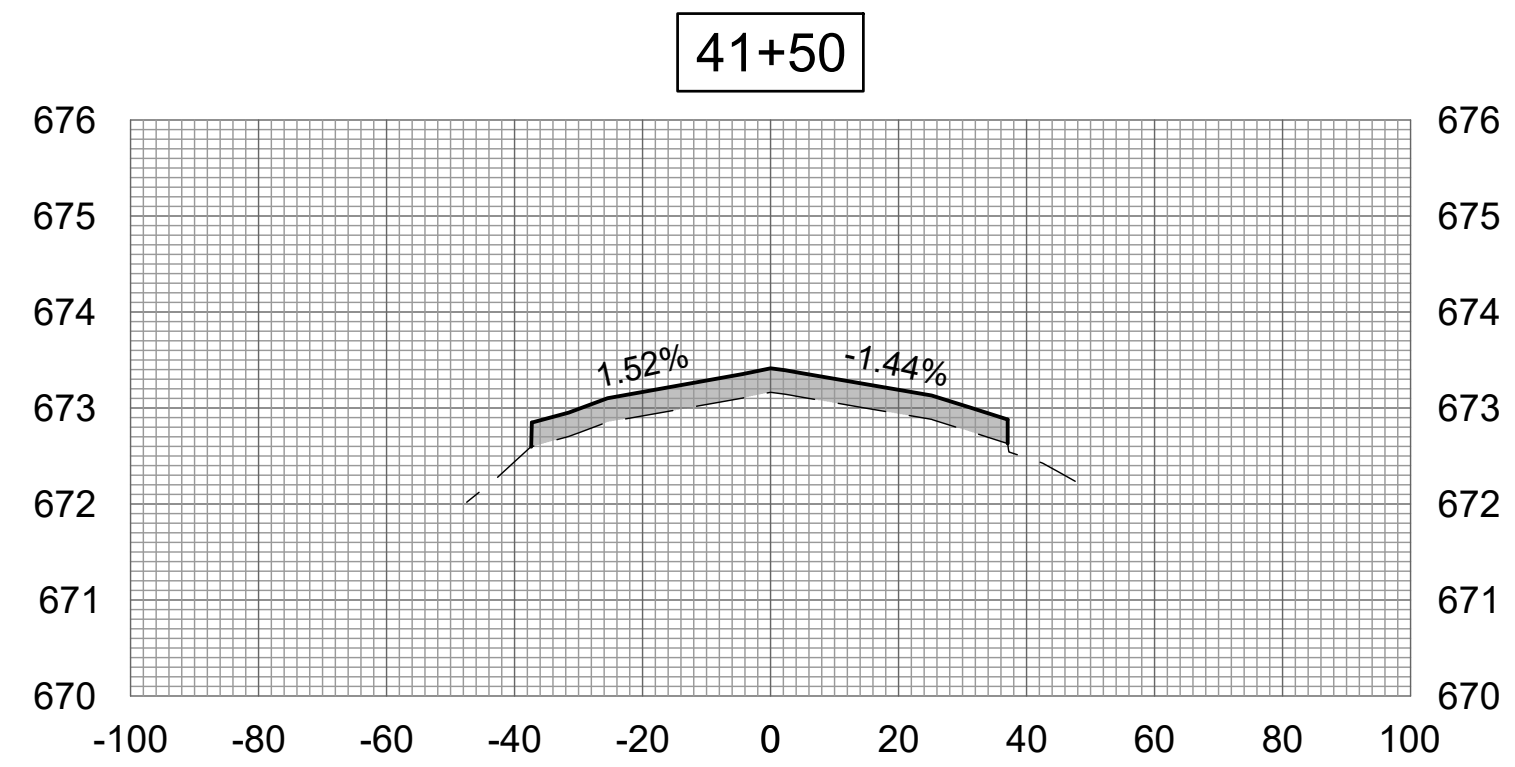
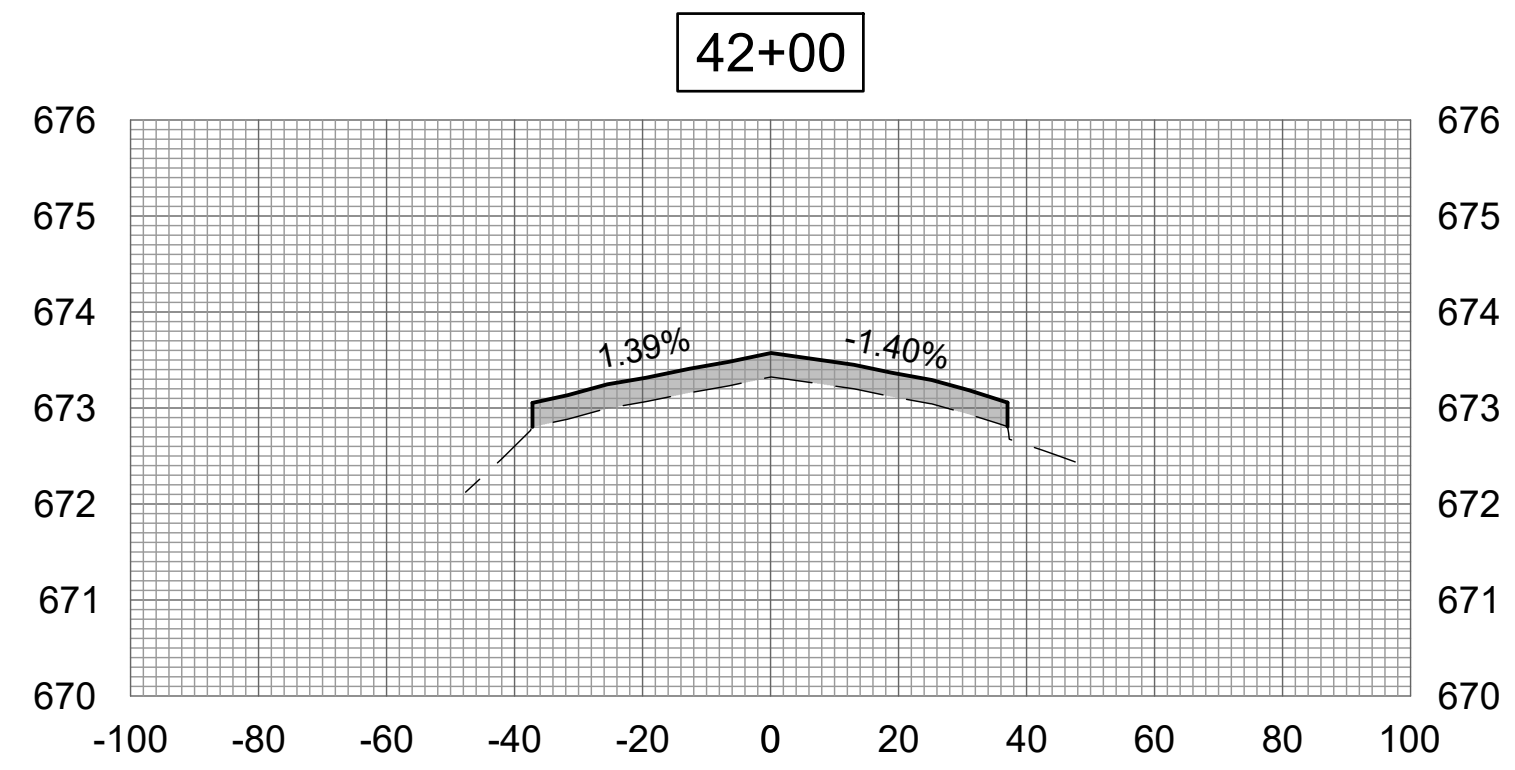
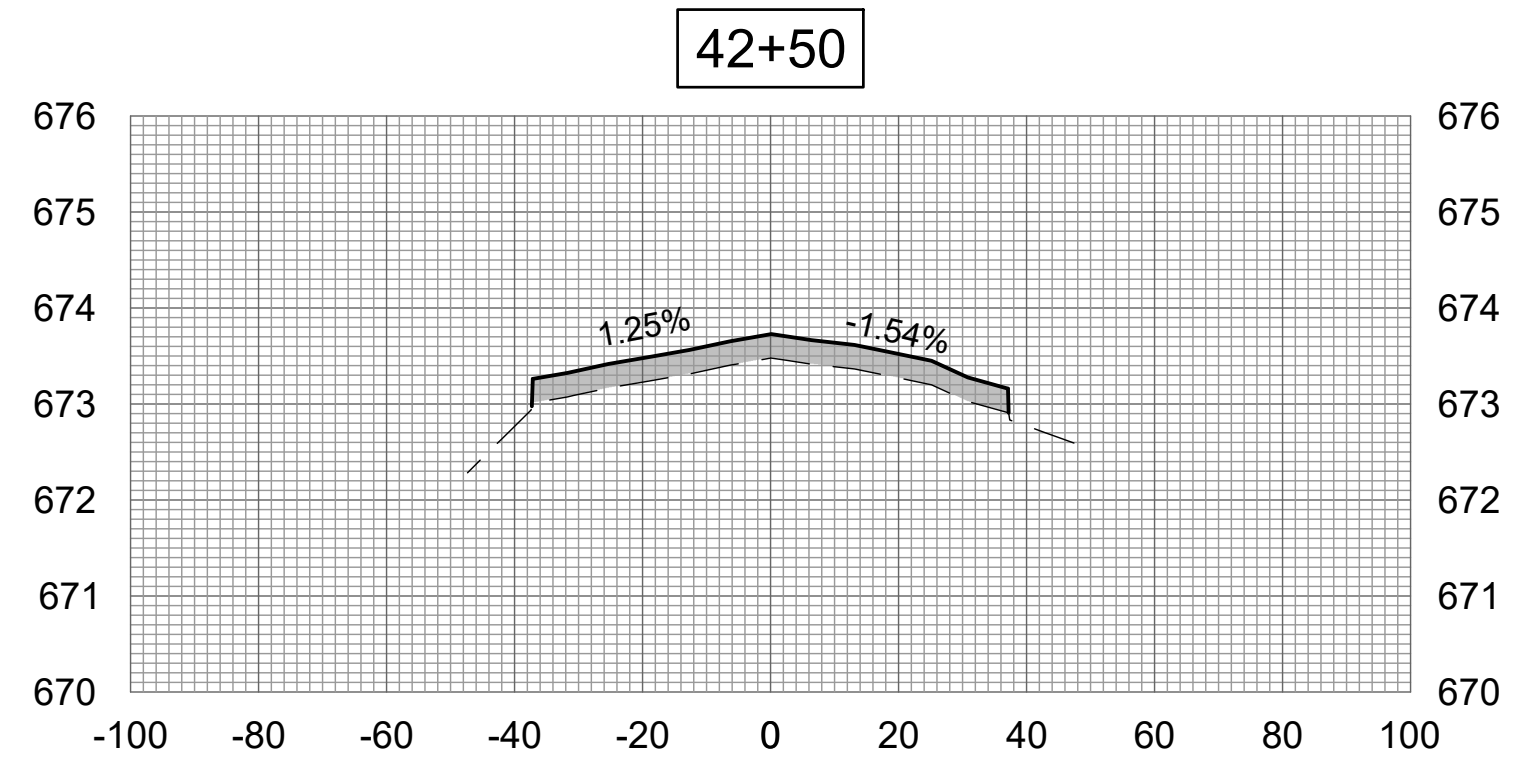
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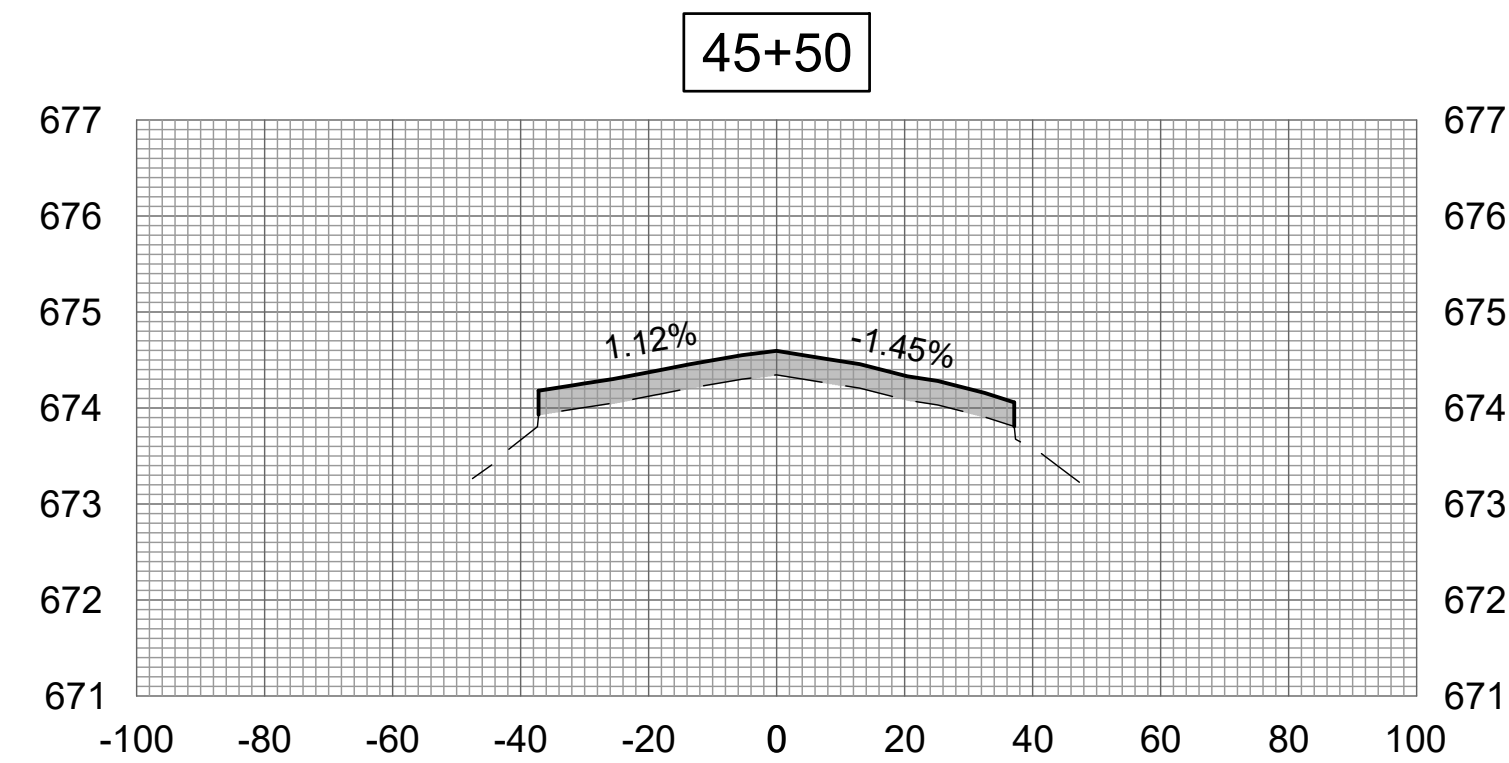
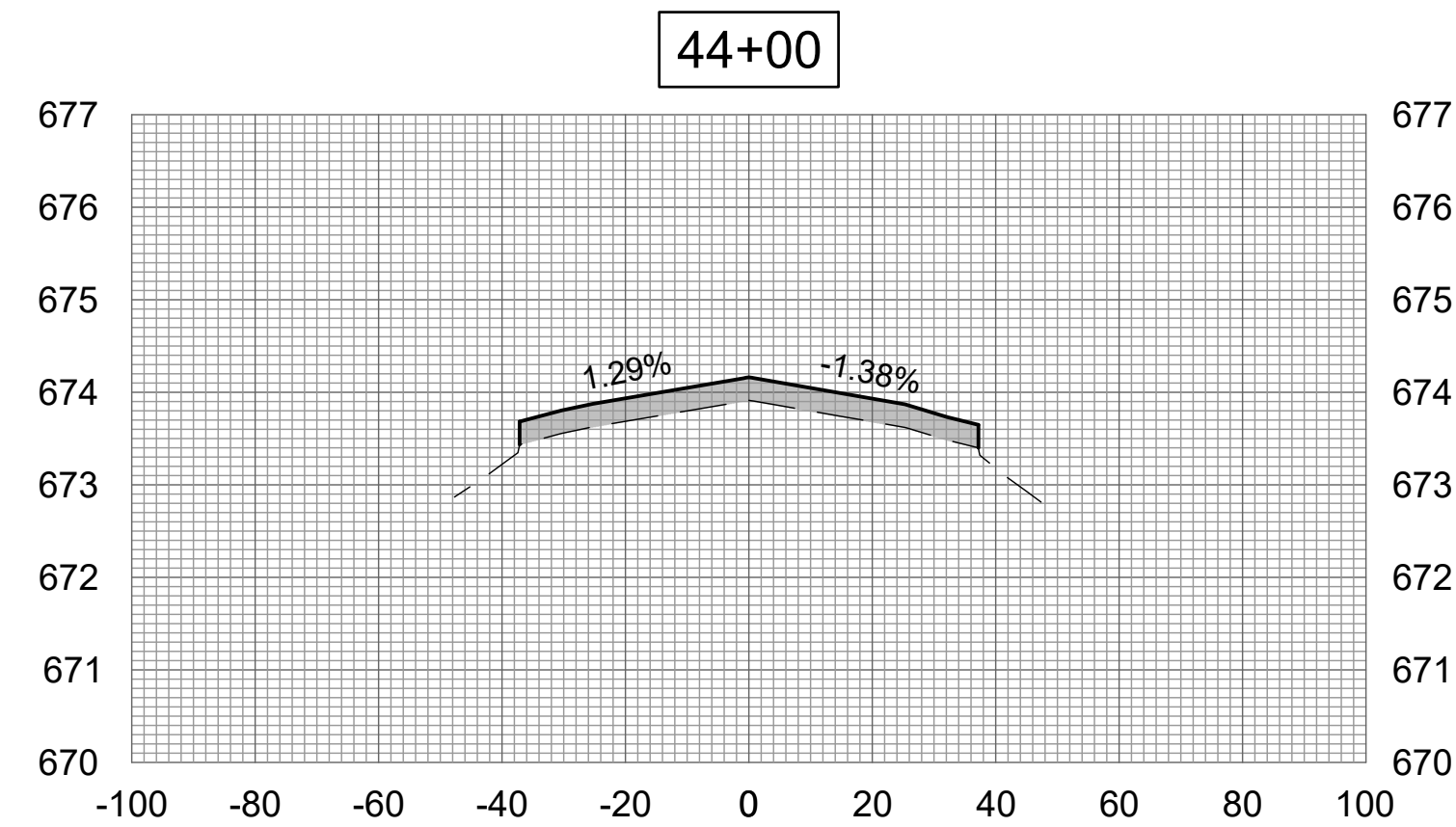
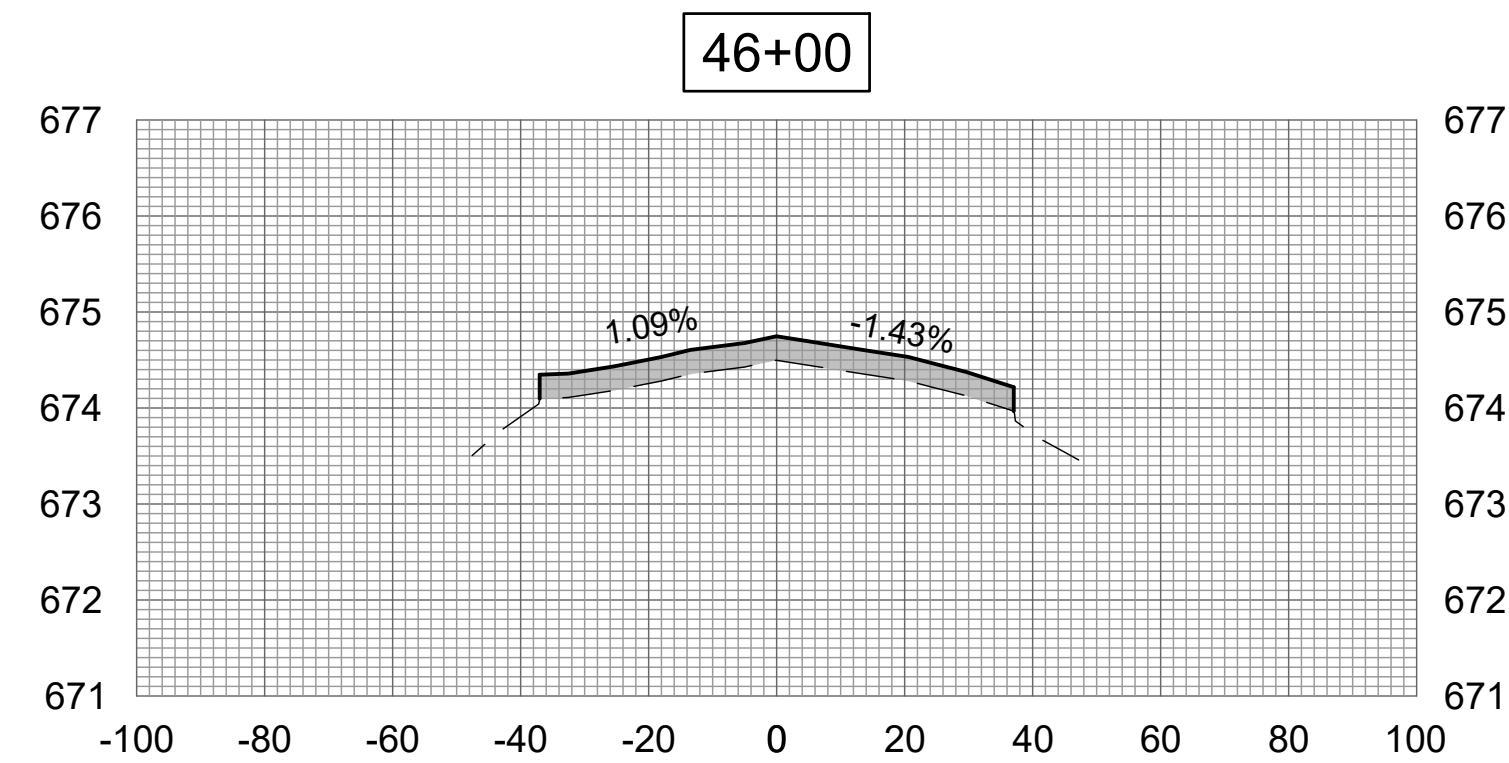
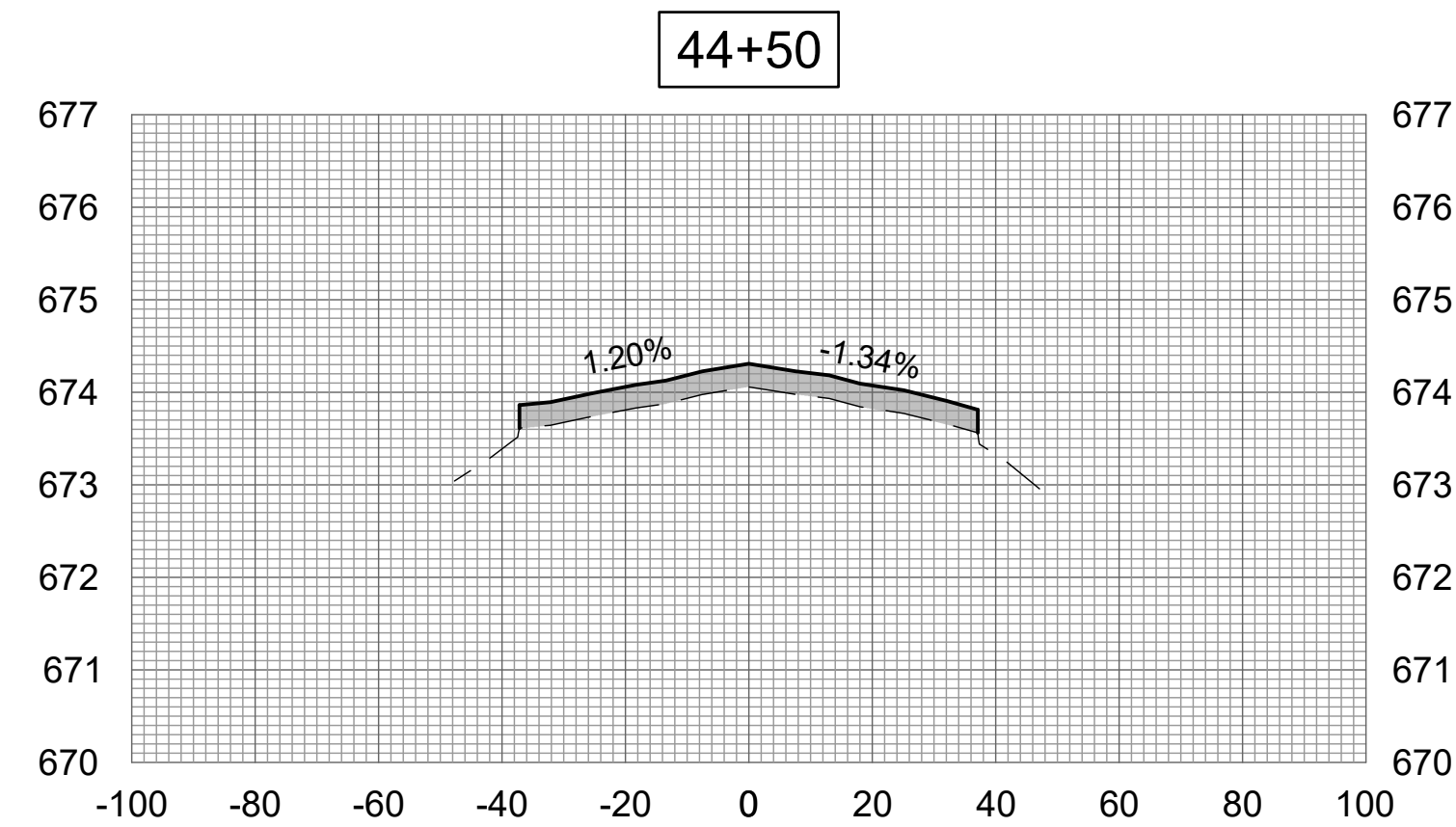
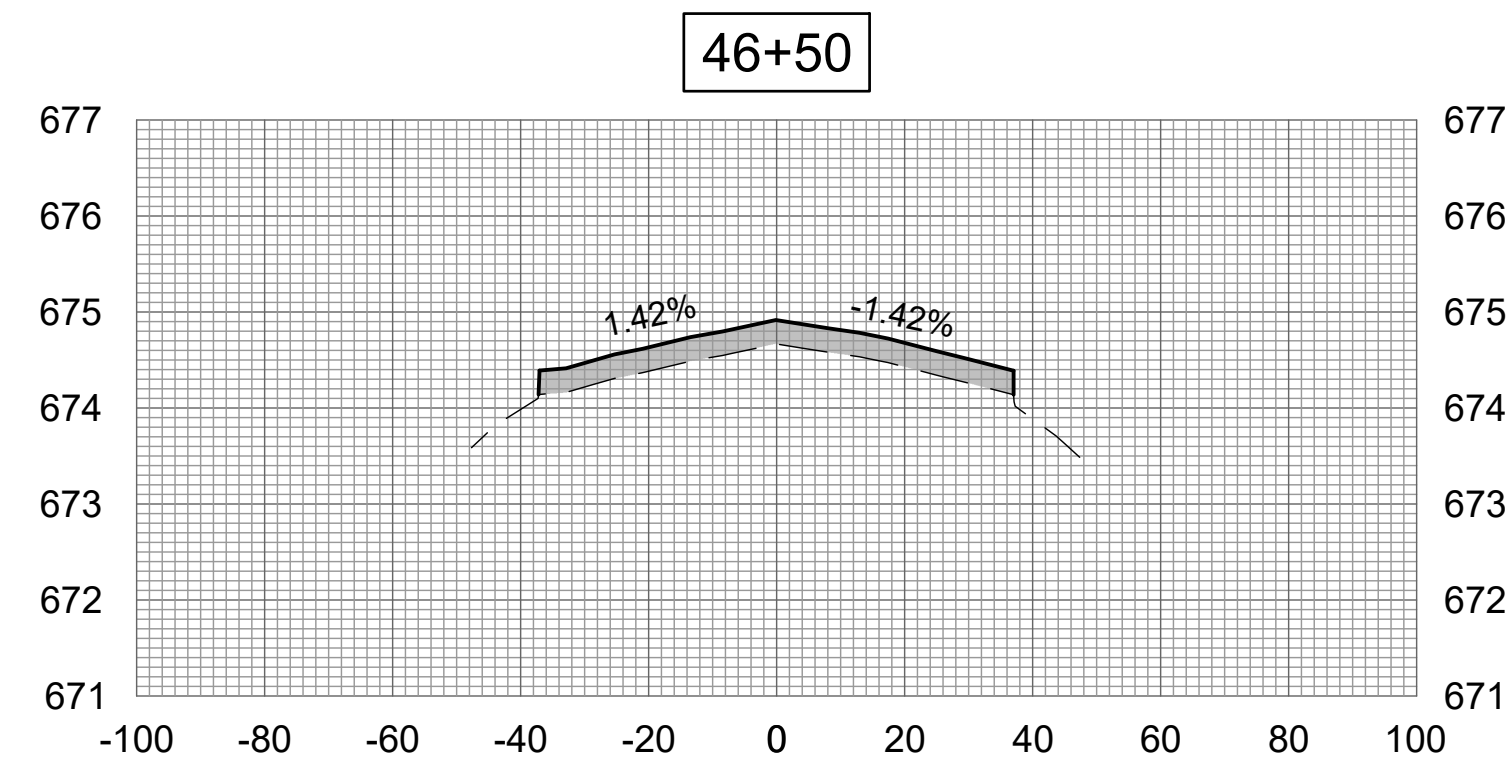
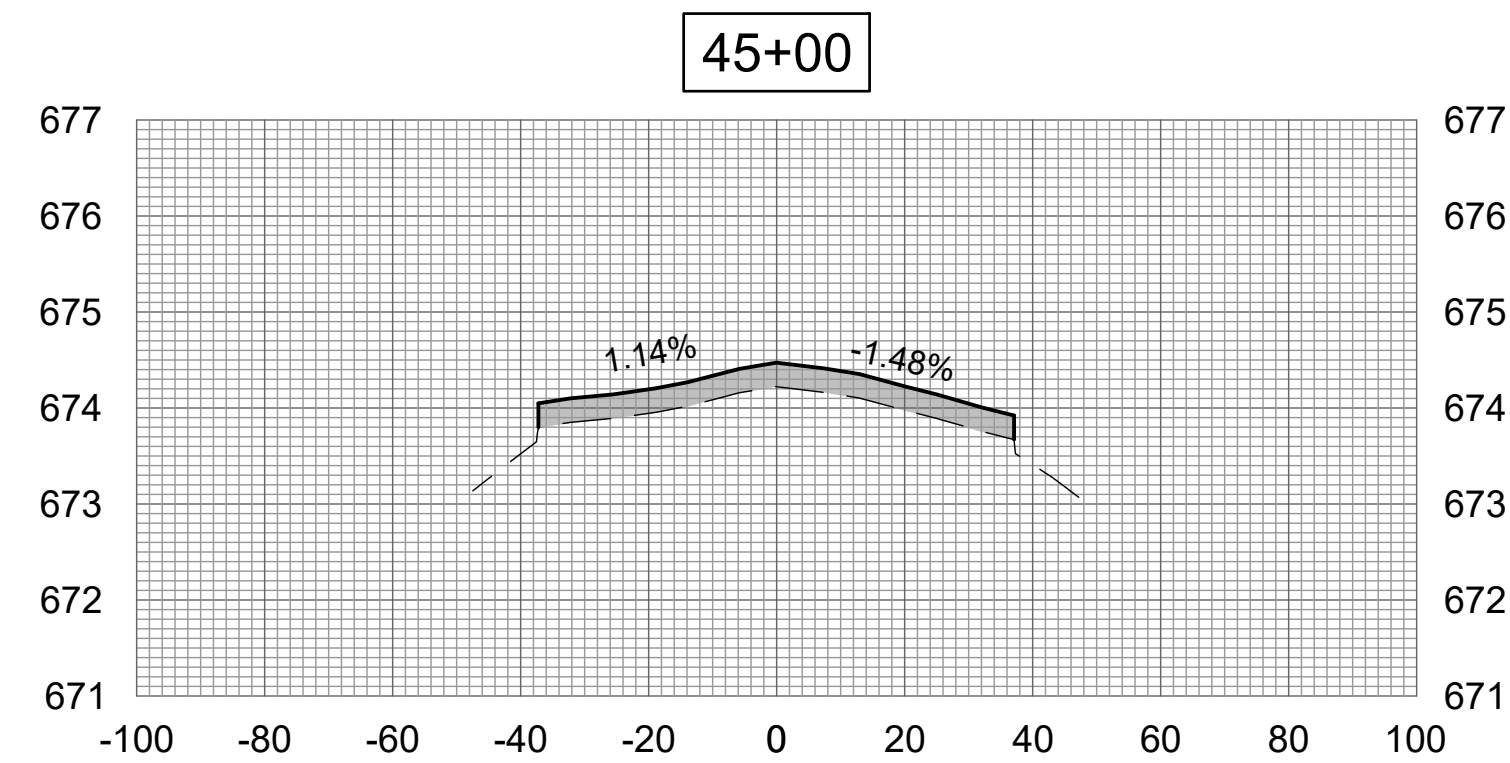
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CROSS-SECTION 4





**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

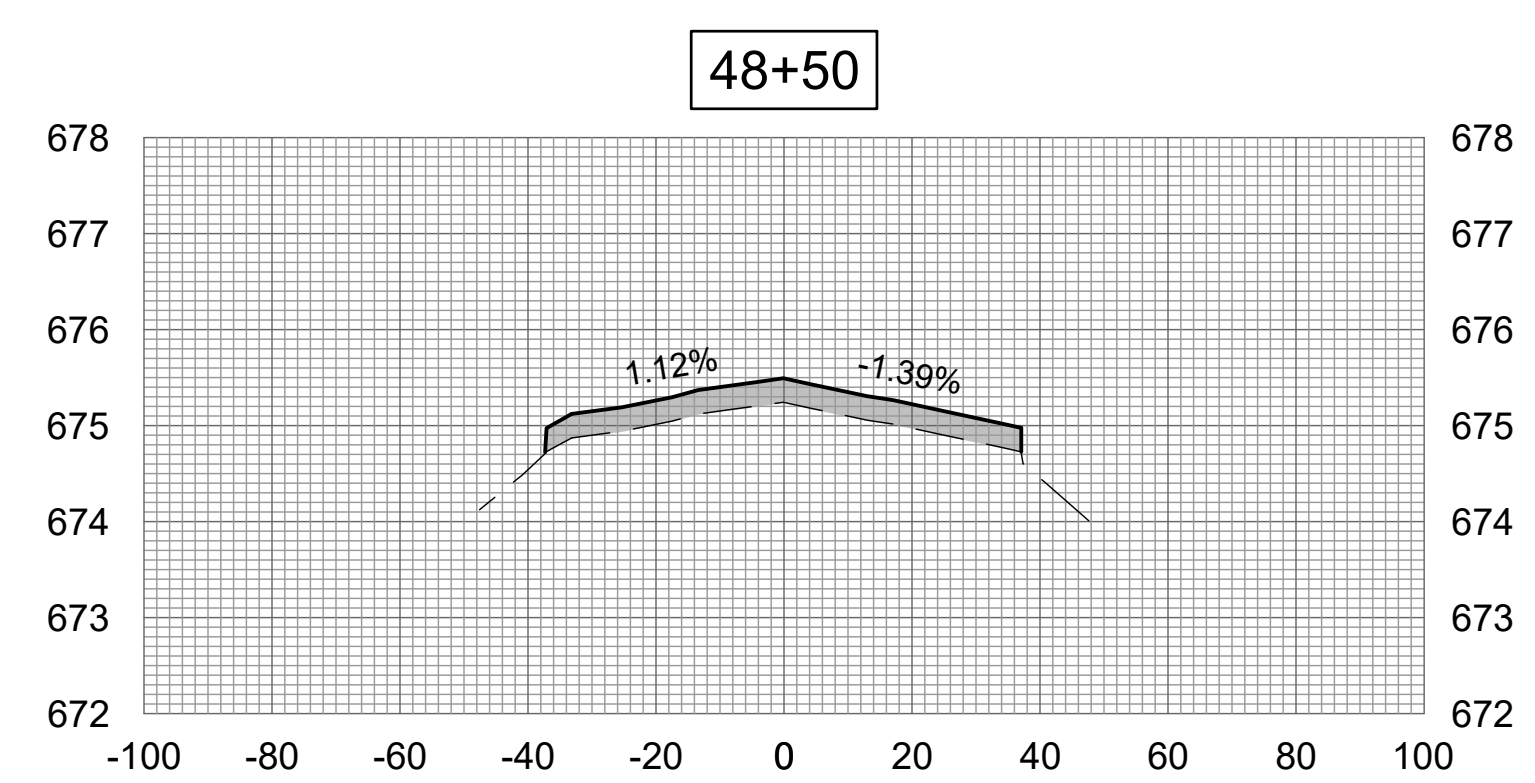
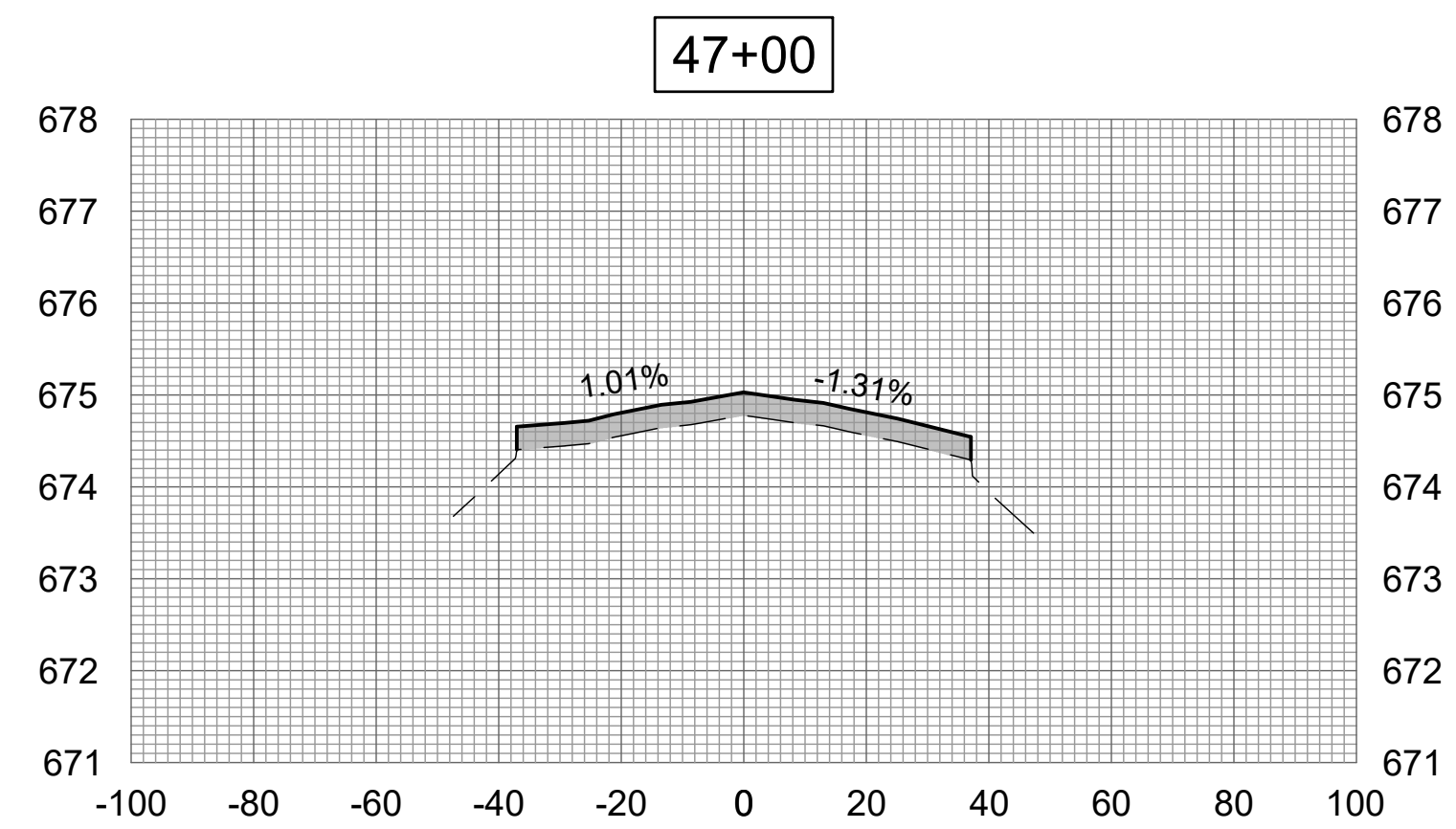
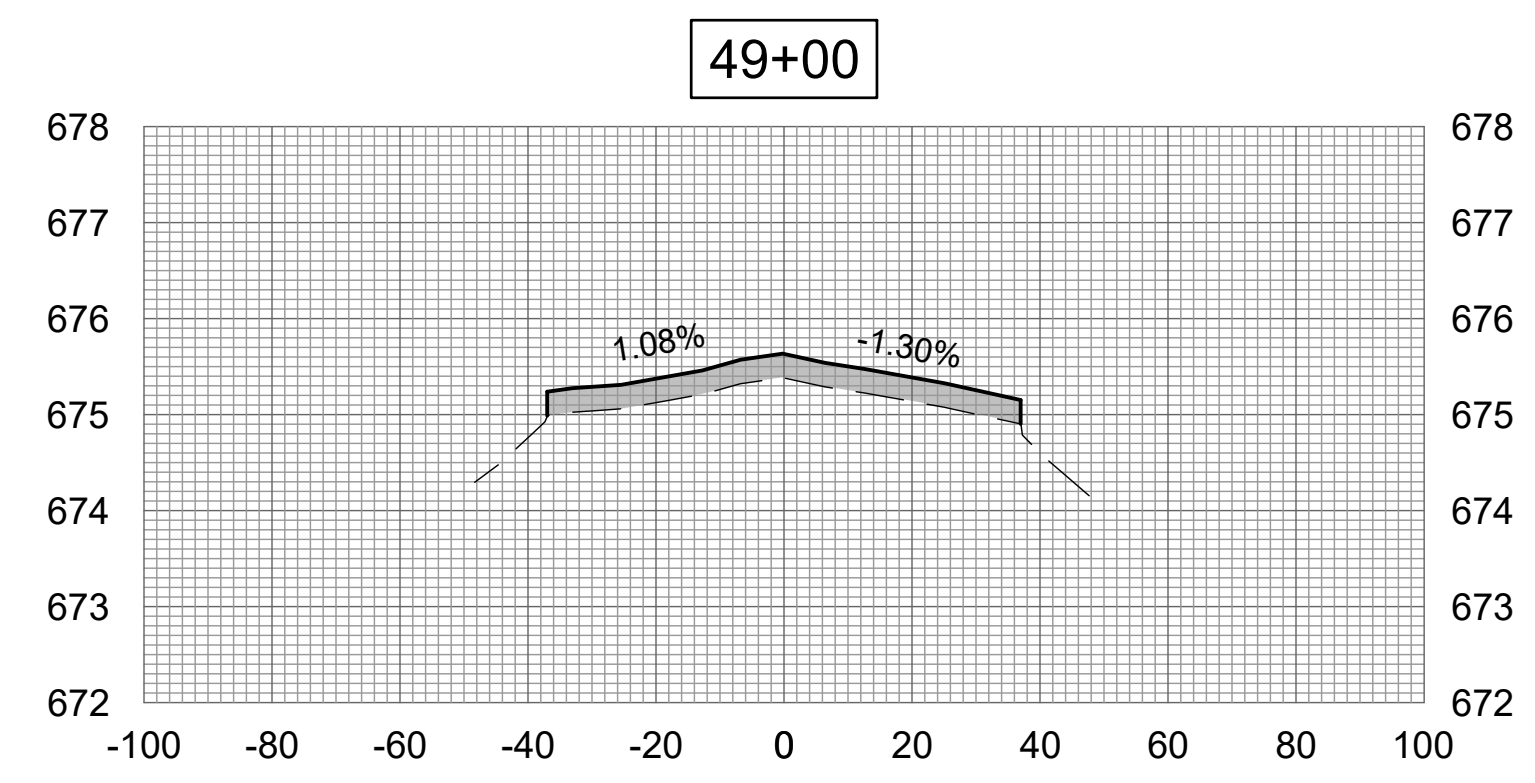
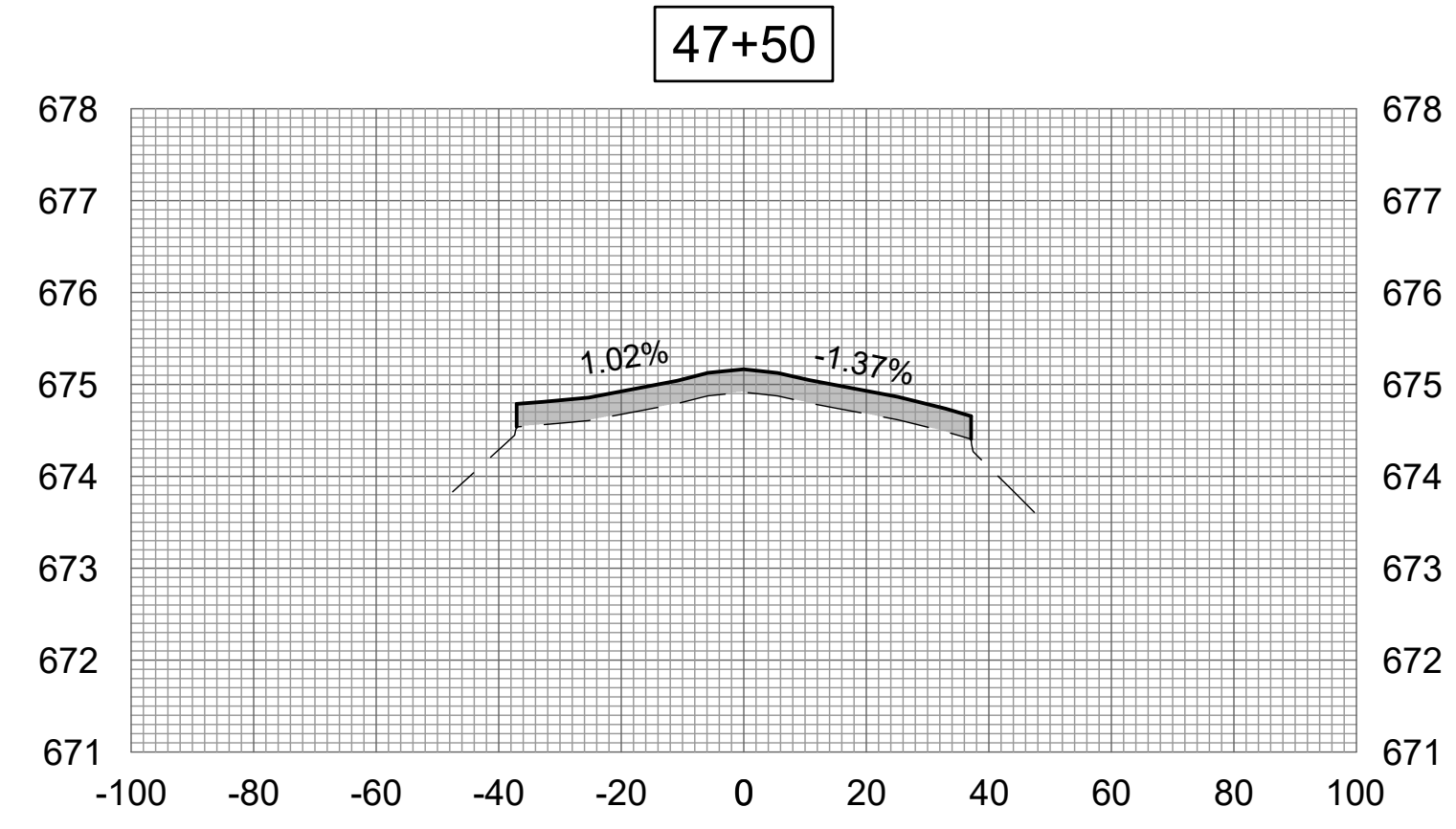
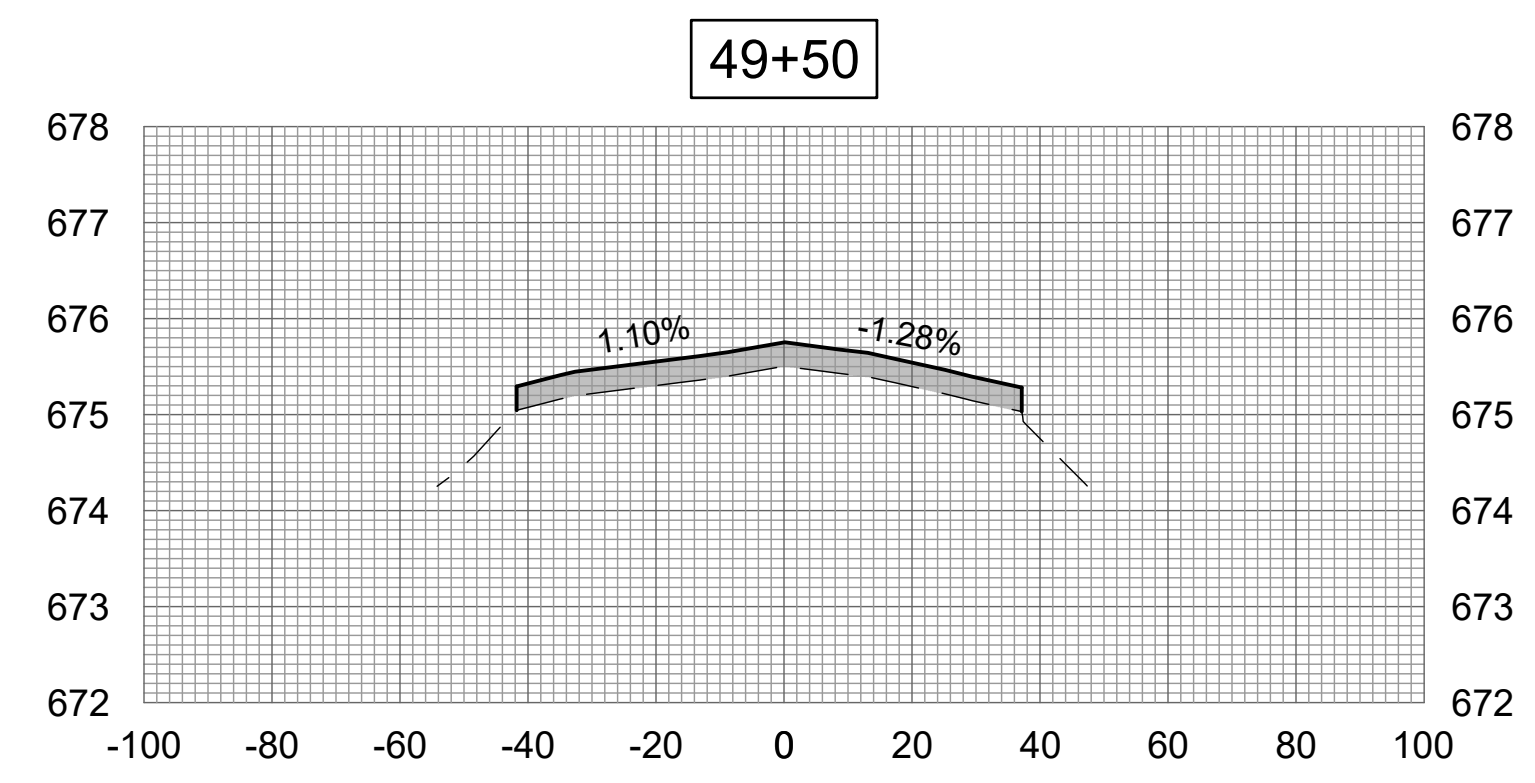
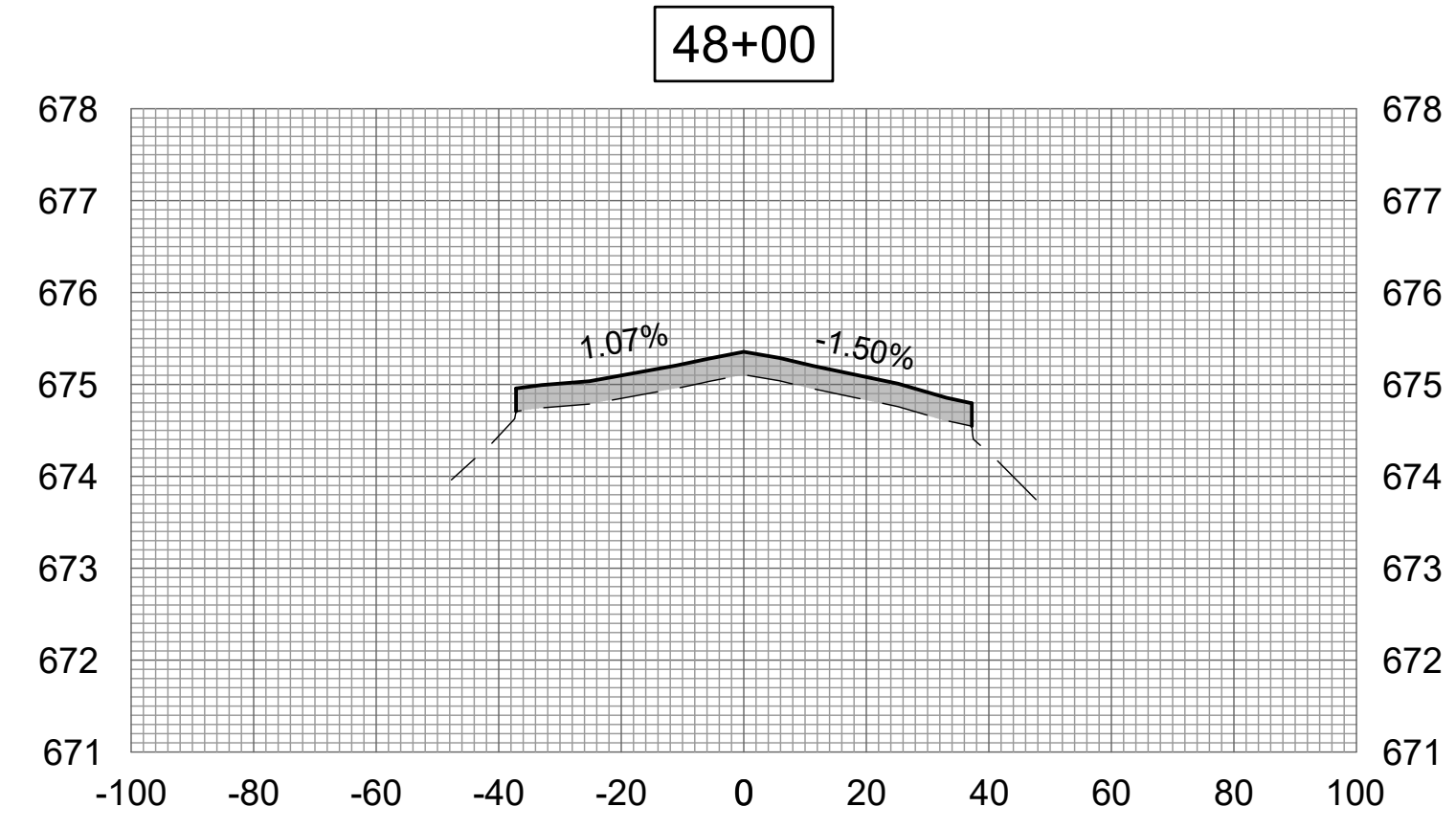
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 5

FOR BID



RESURFACE
RUNWAY 18-36

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

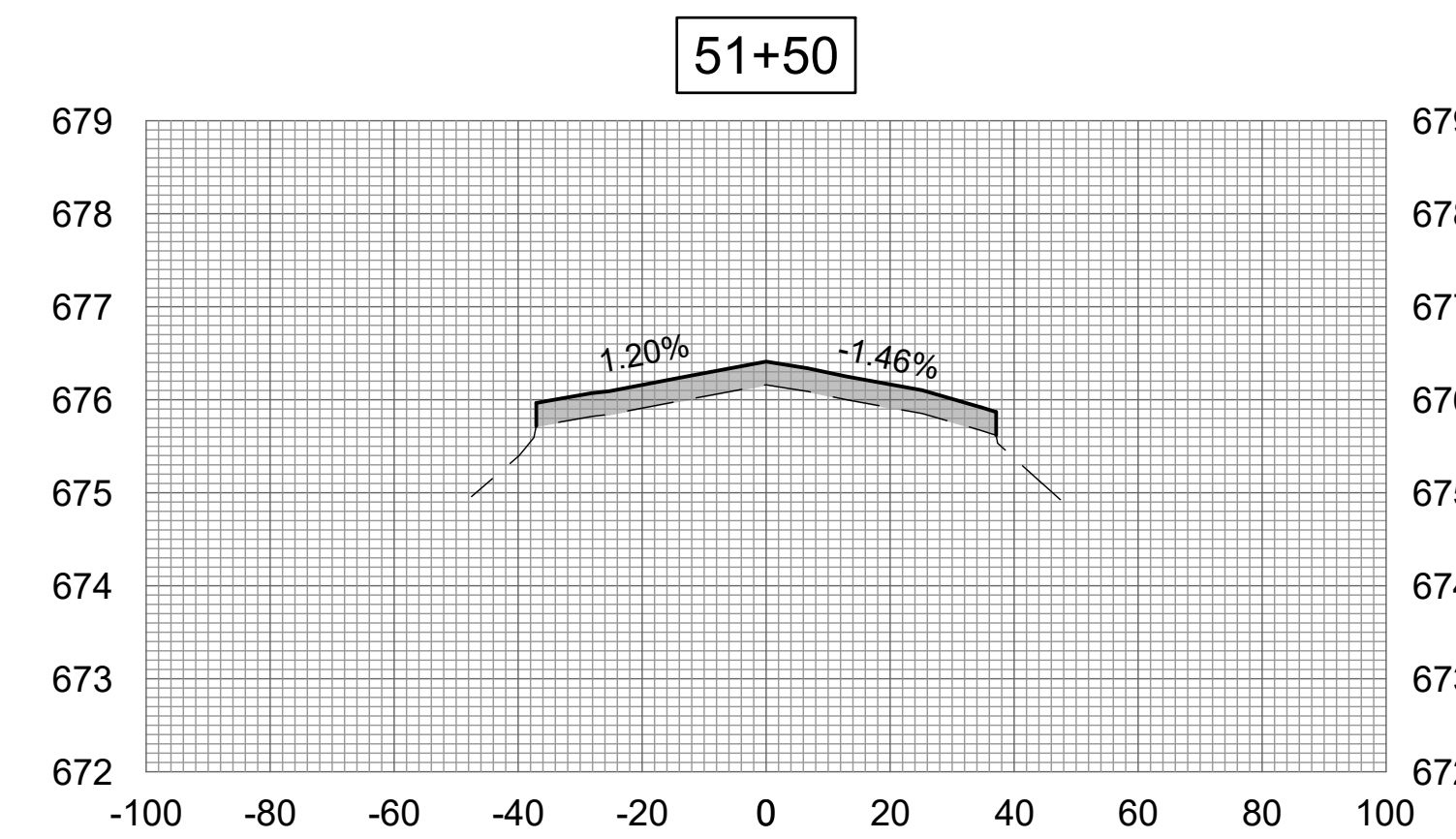
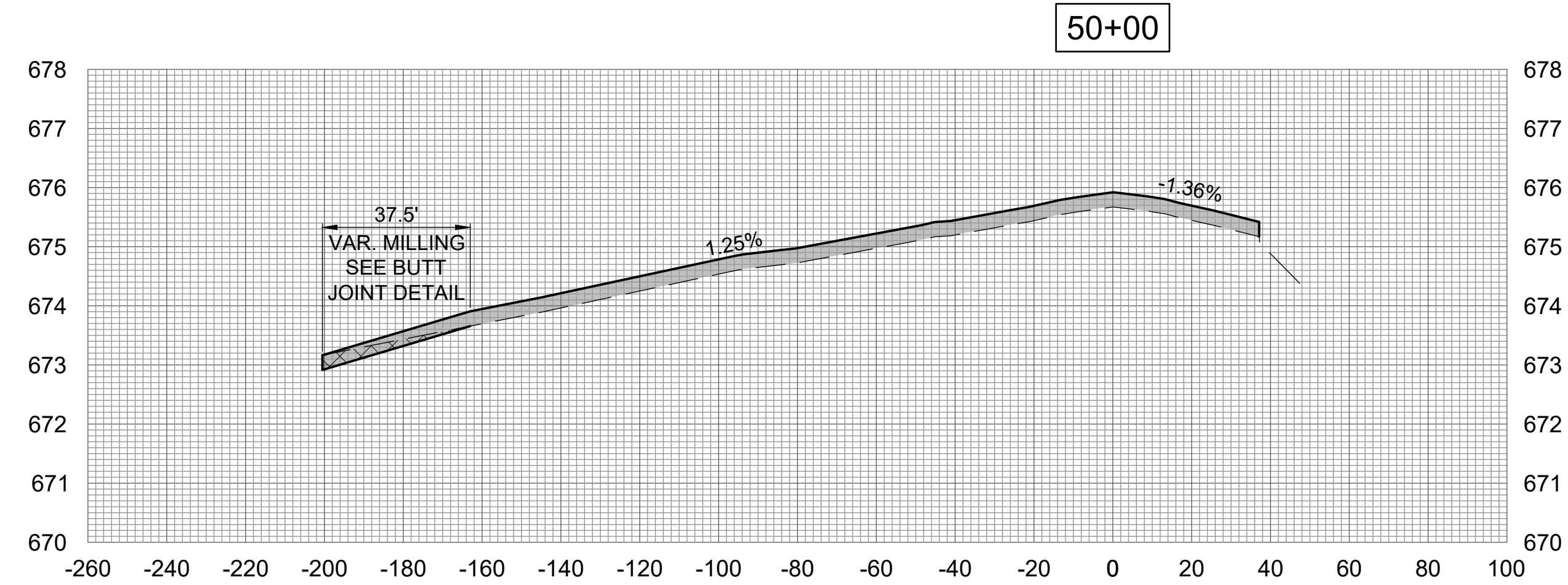
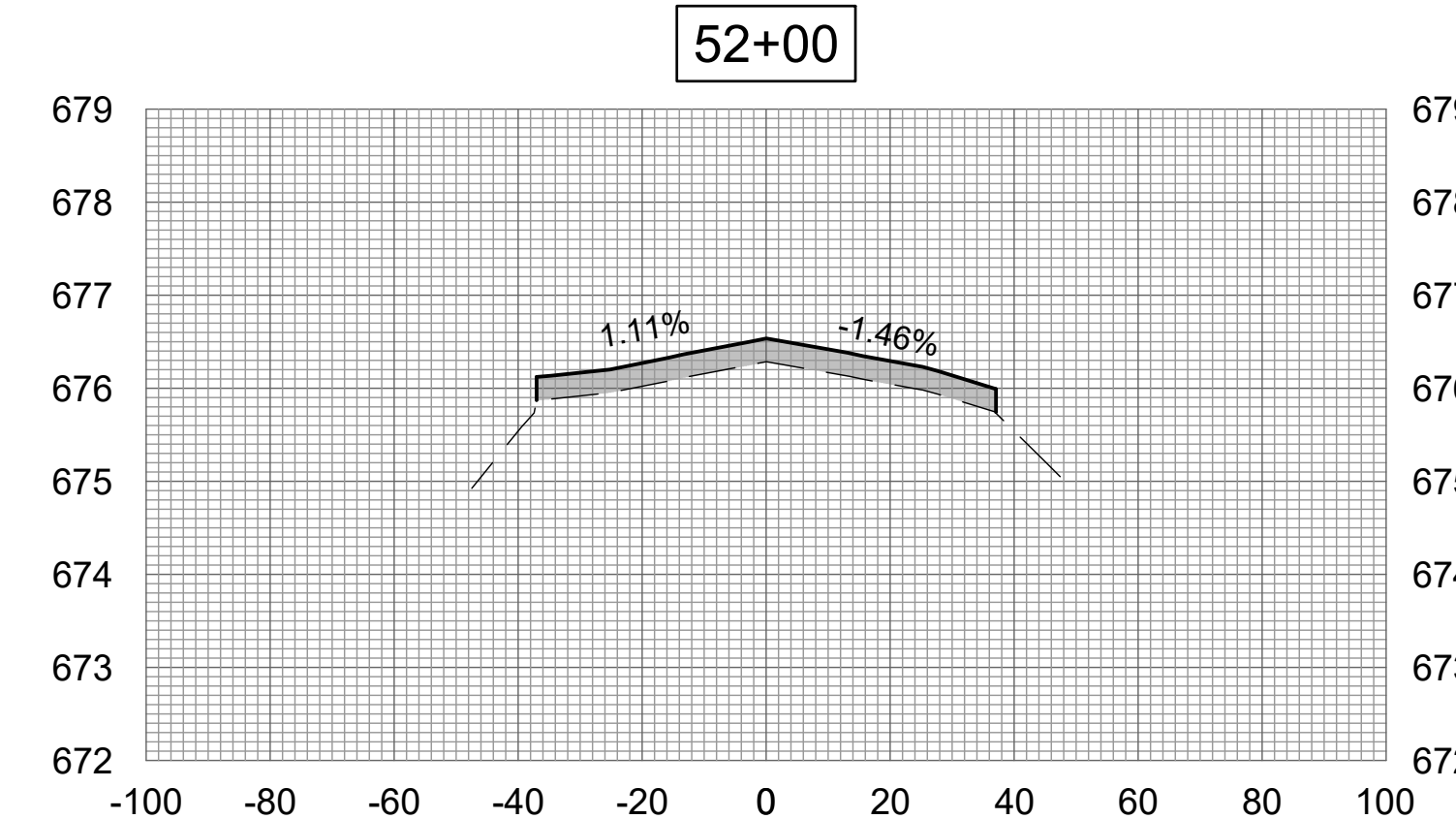
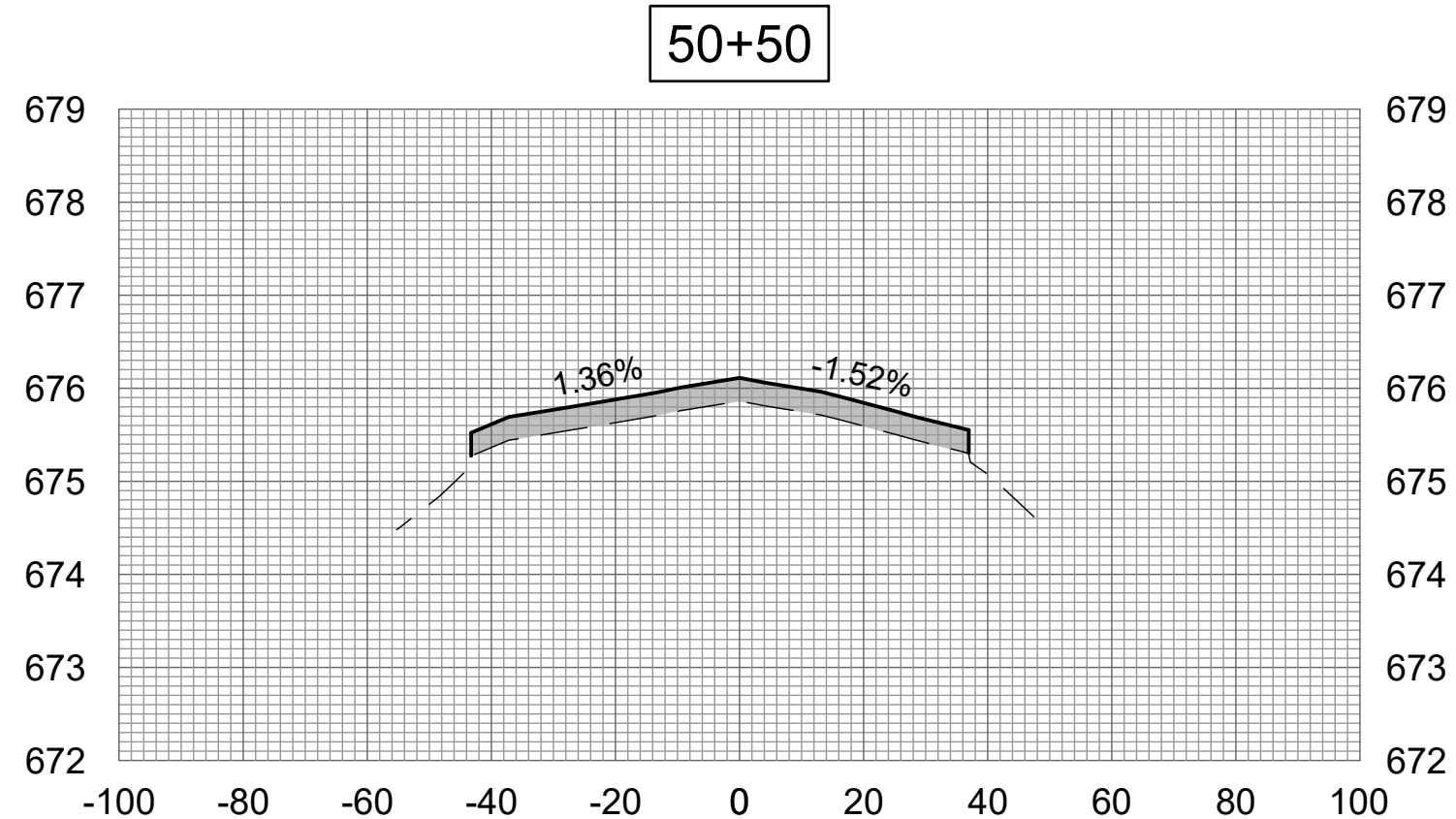
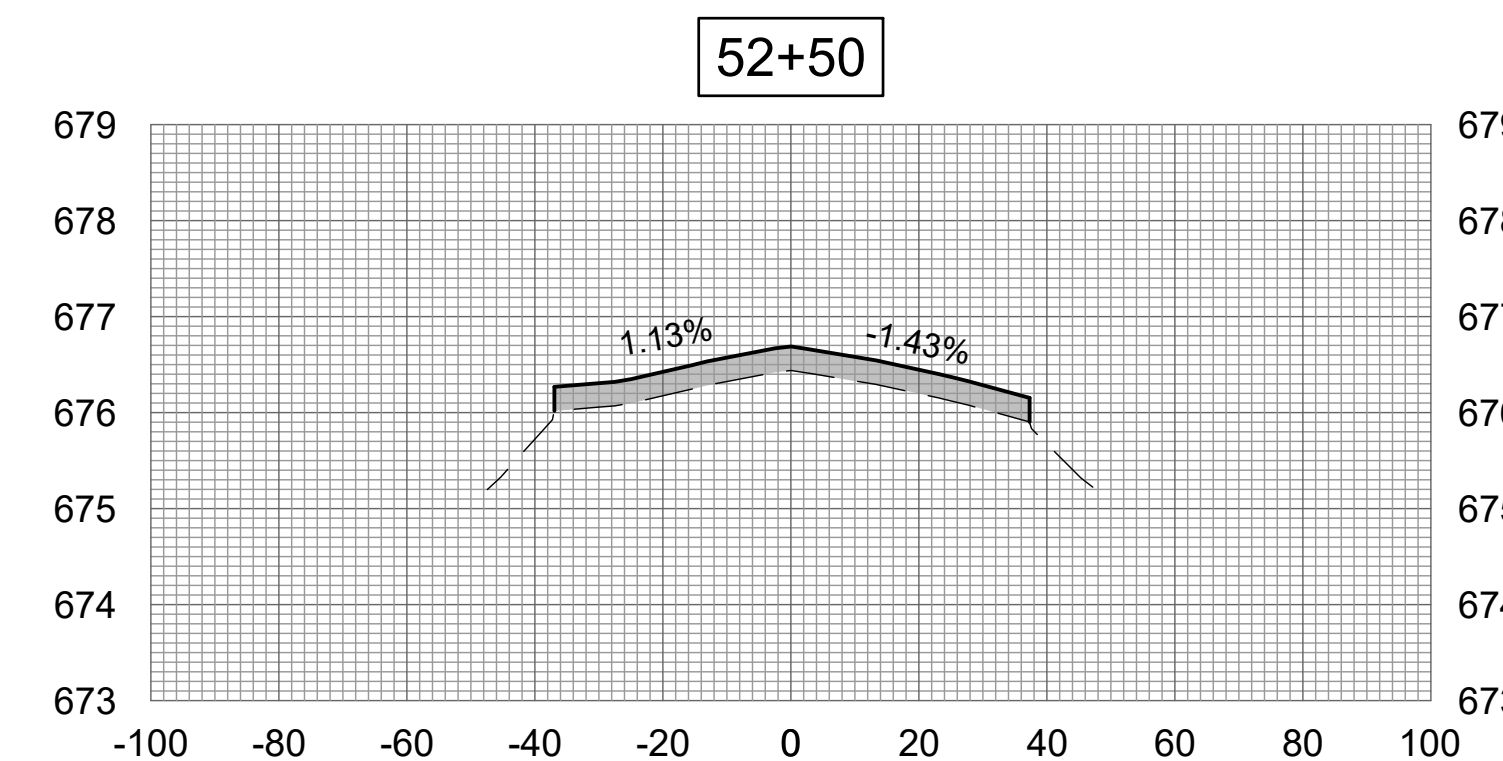
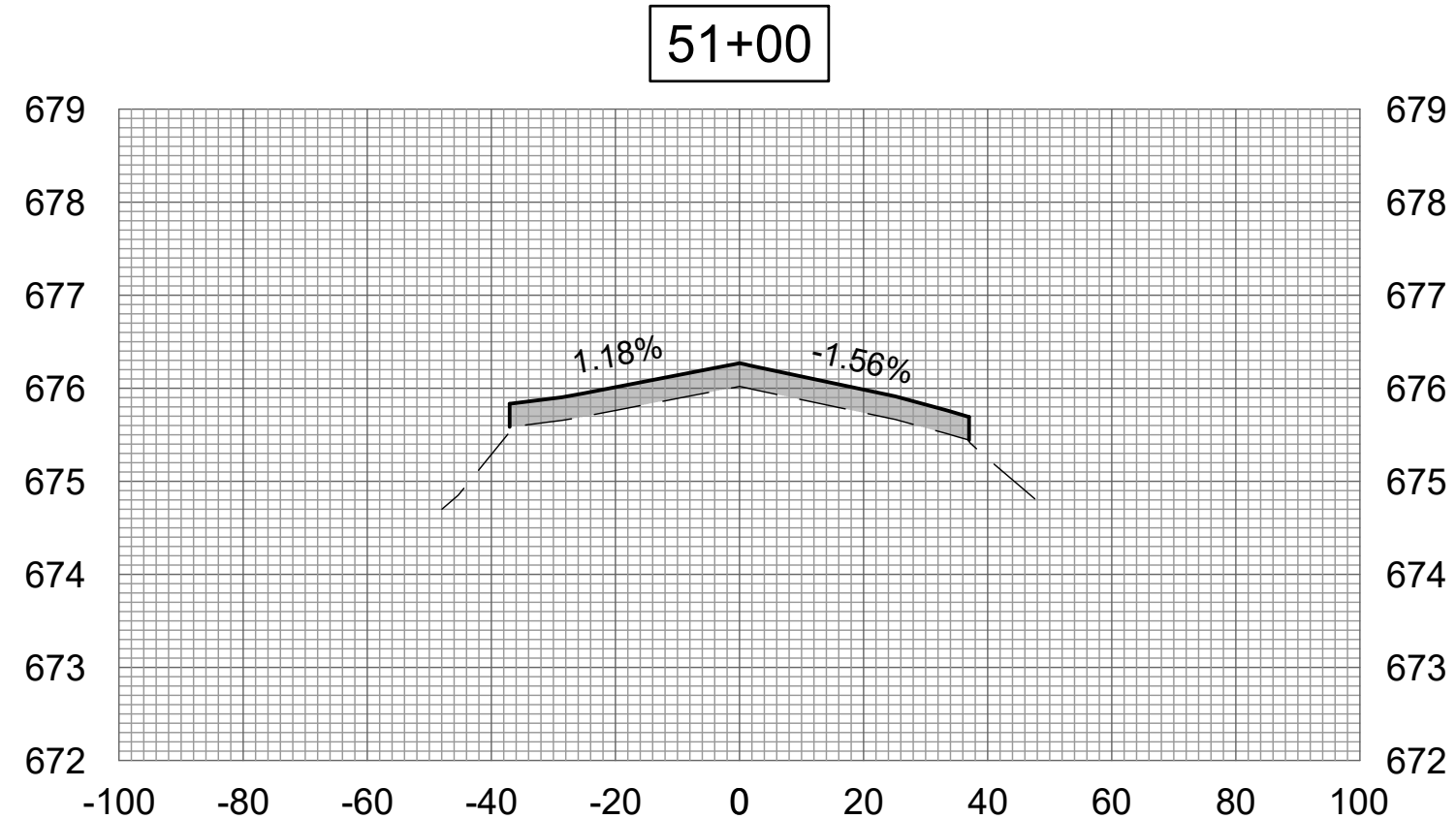
ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 6

APR 19, 2024 3:09 PM L:\AMB02387\1\23\JOBS\23A1039_00\CAD\AIRPORT\SHHEET\301-XS.DWG

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**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 7

FOR BID



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

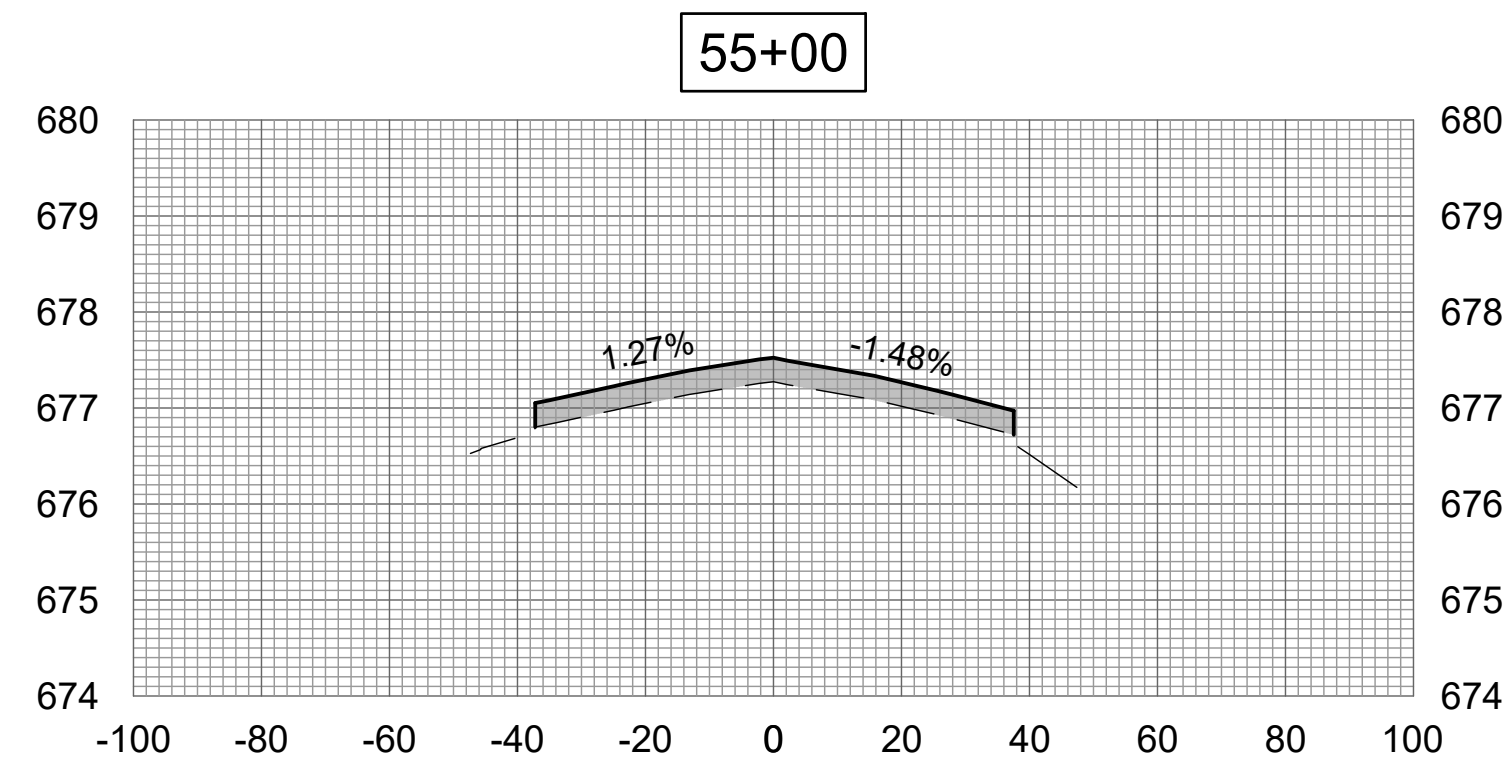
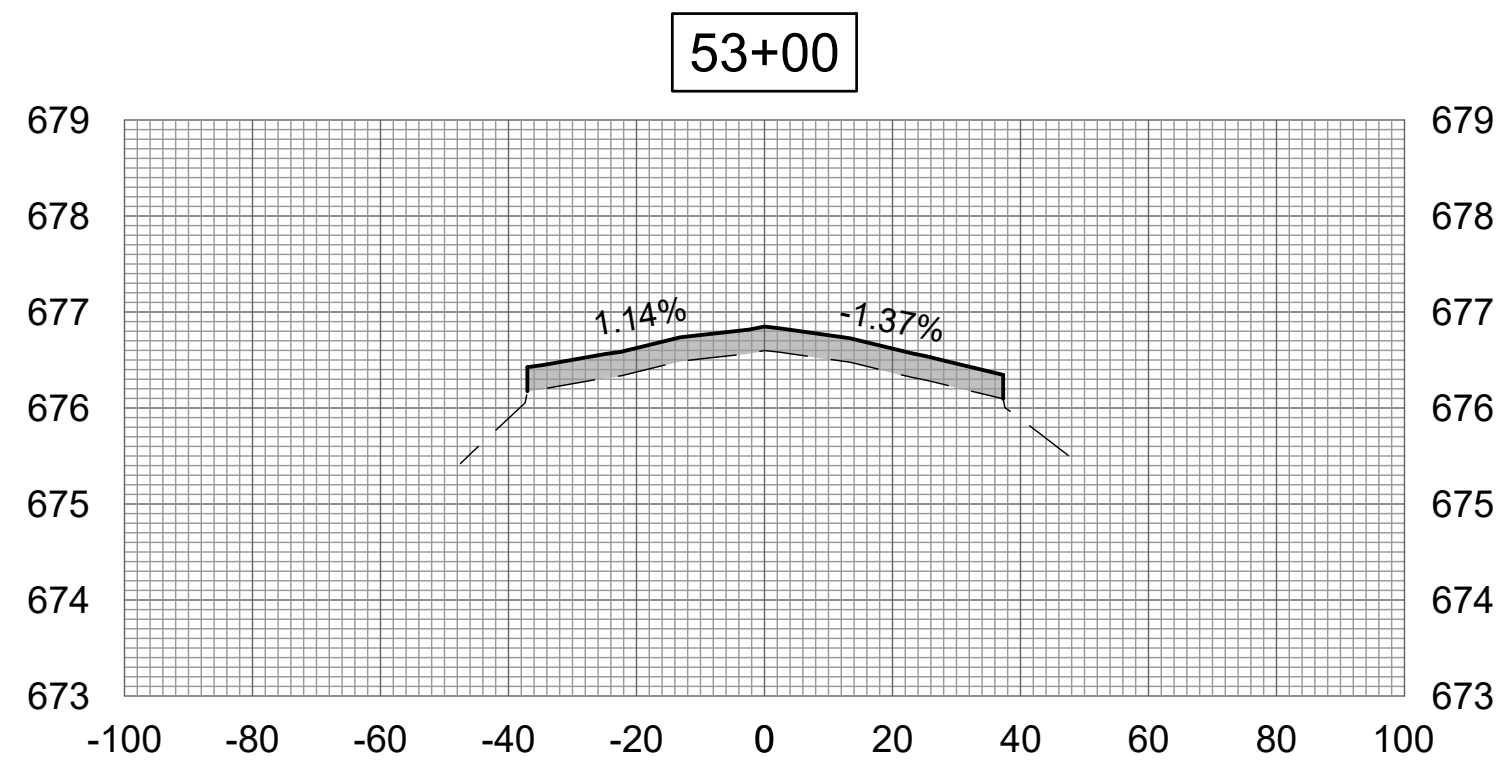
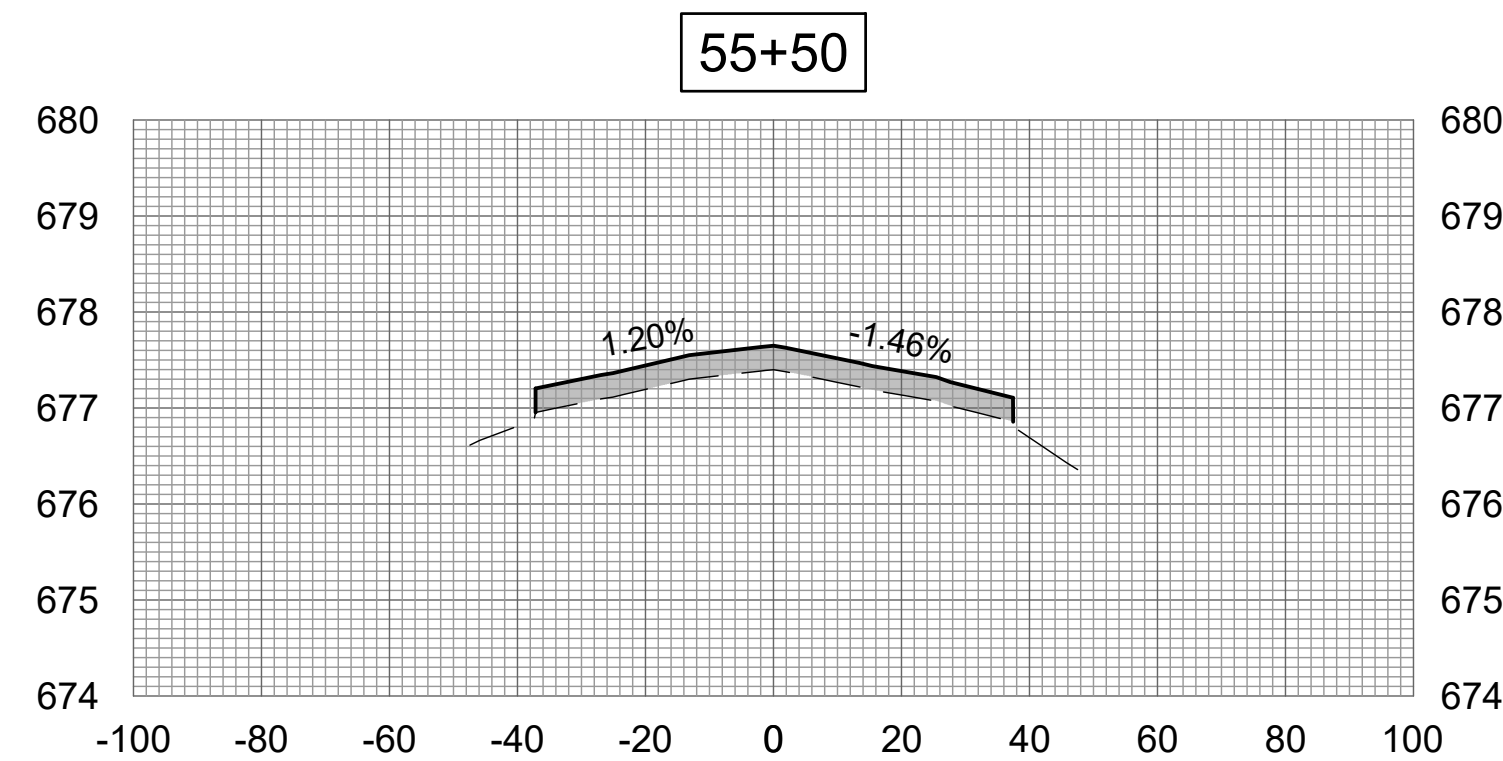
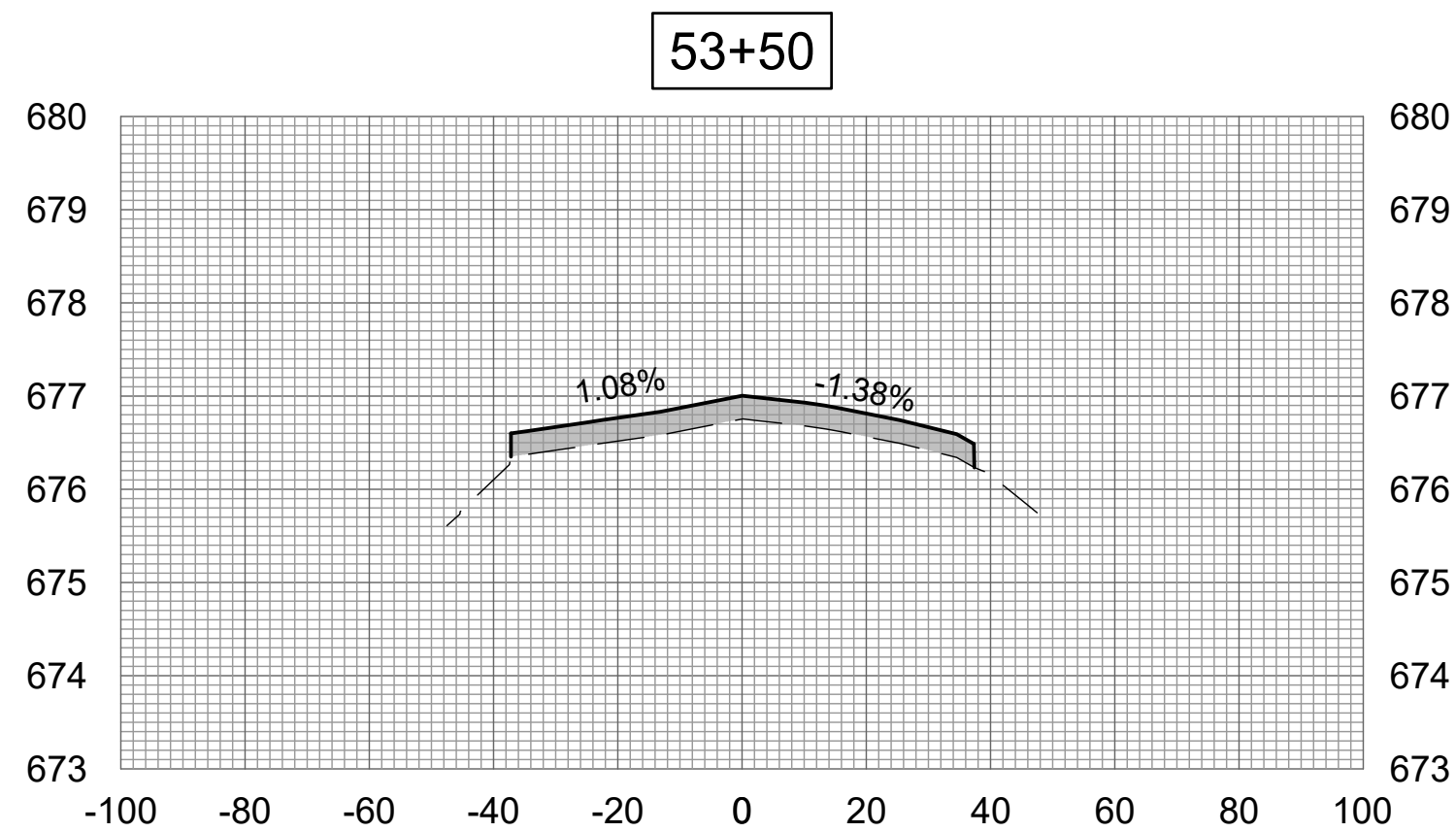
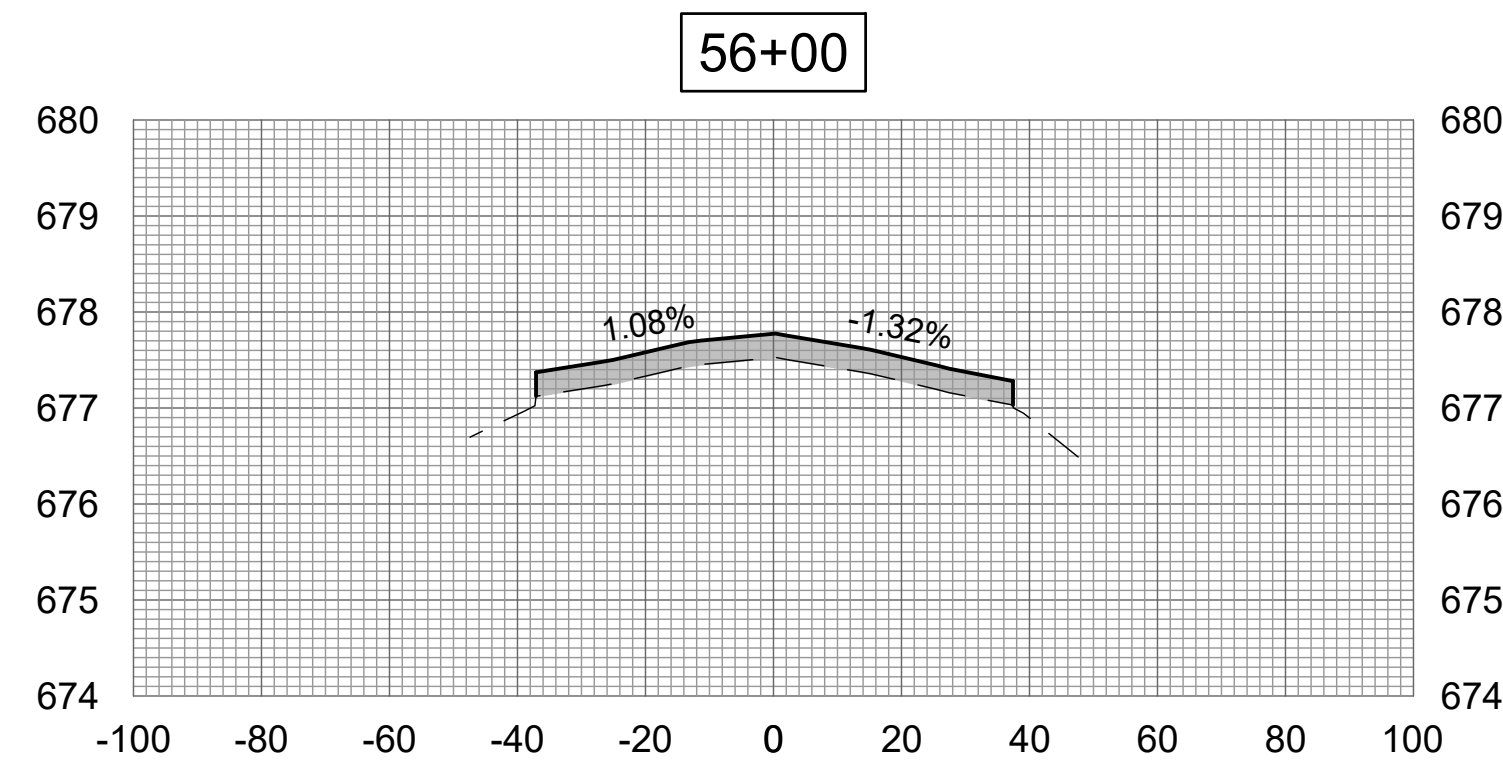
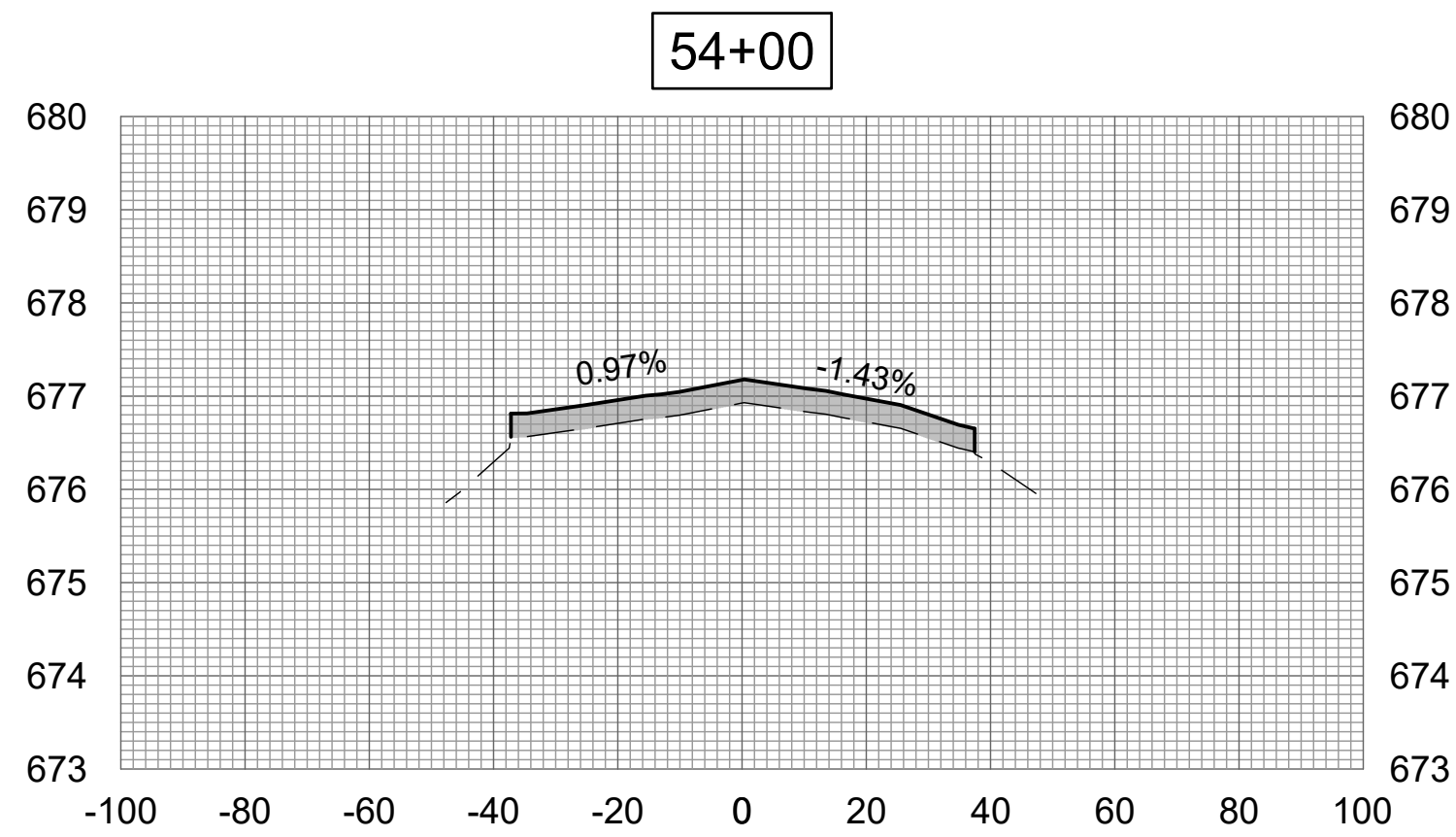
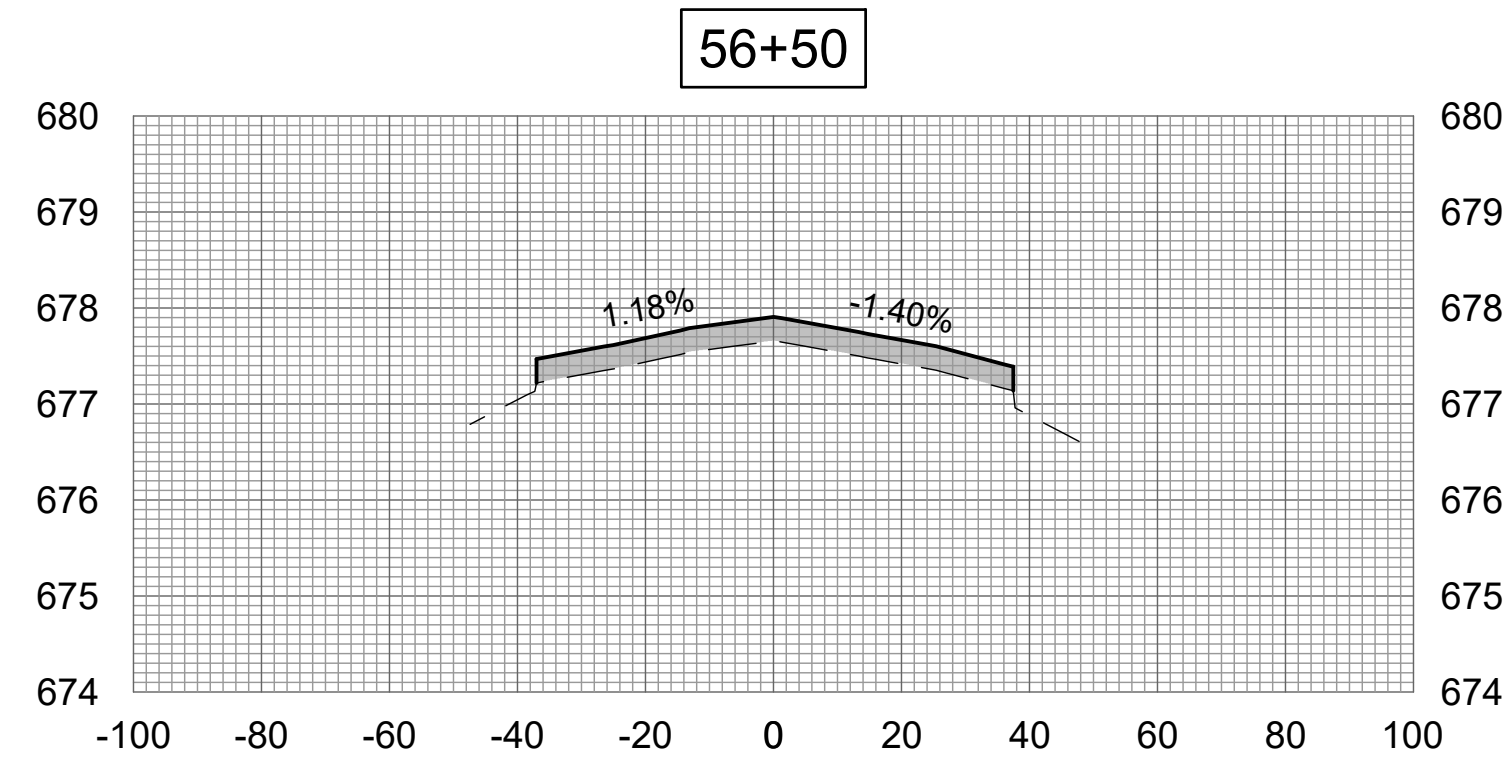
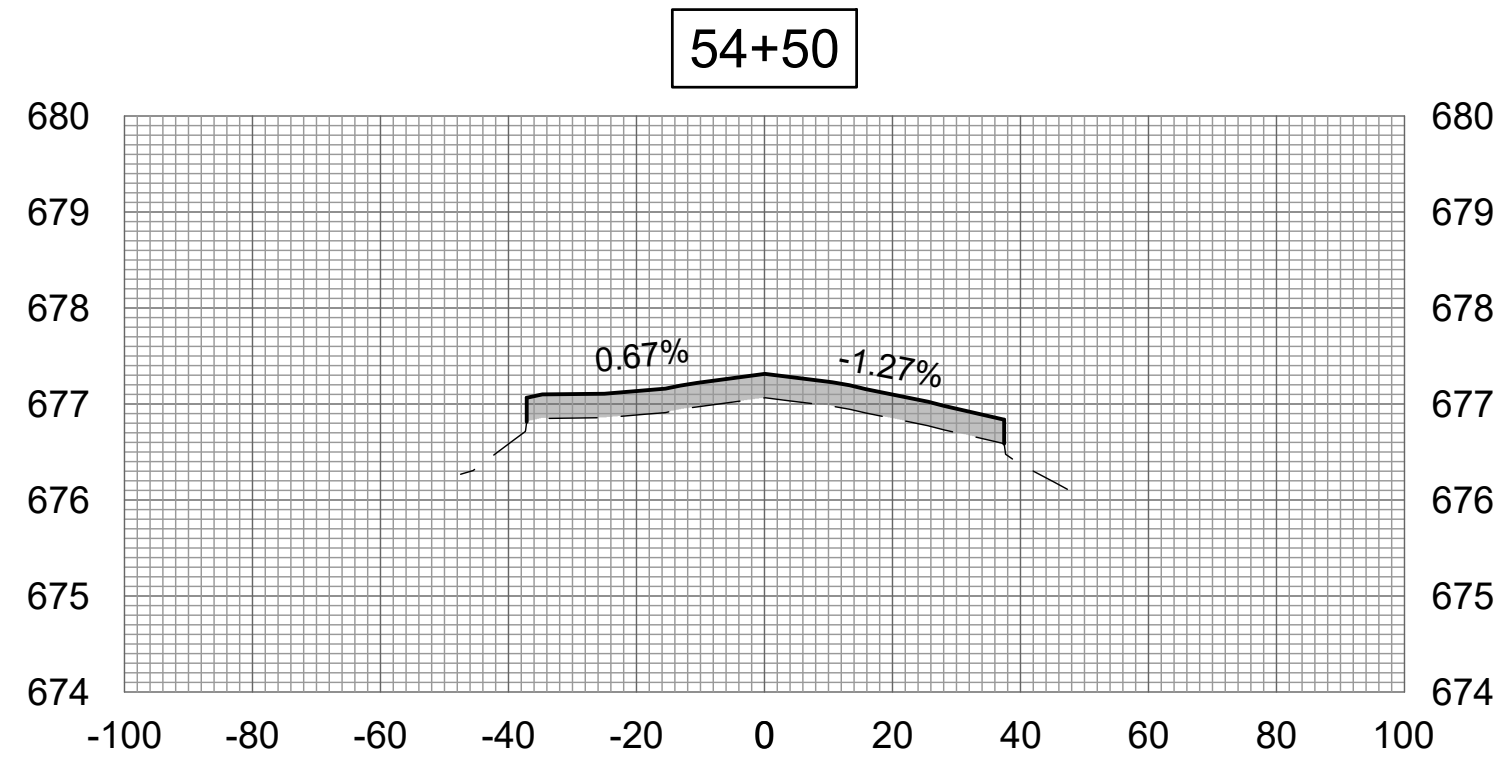
IDA No: CTK-5121
Contract No. CN028

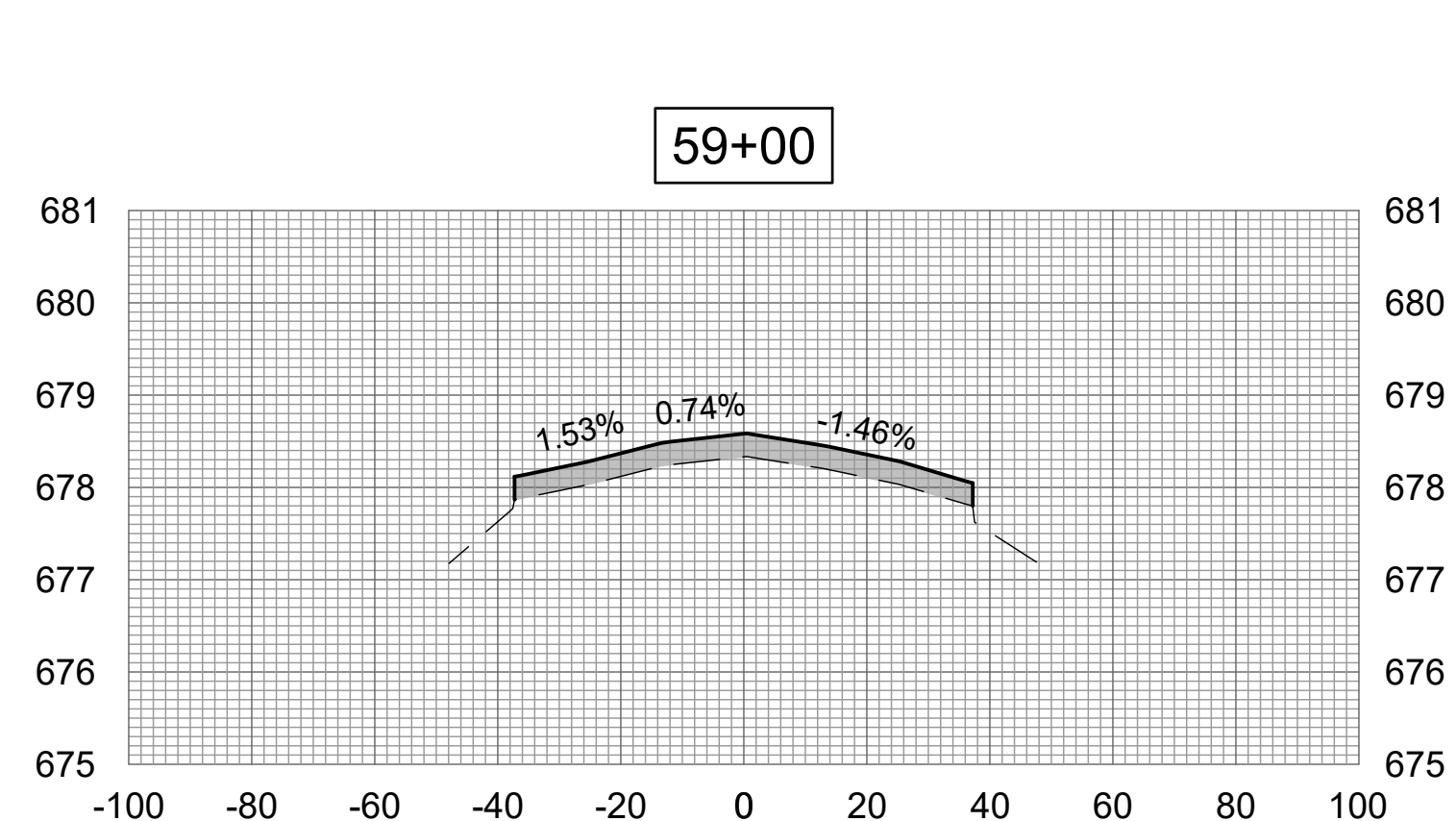
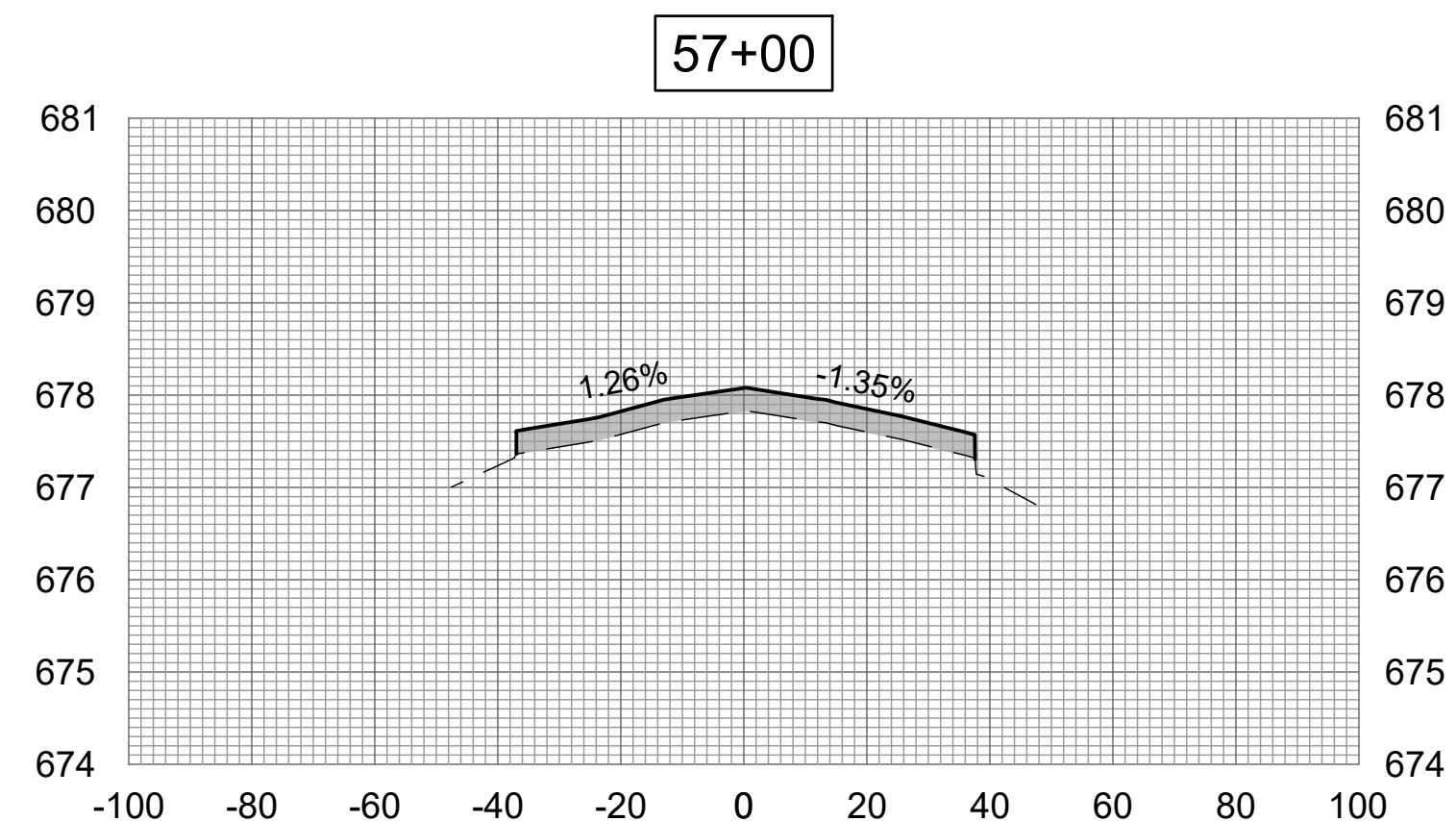
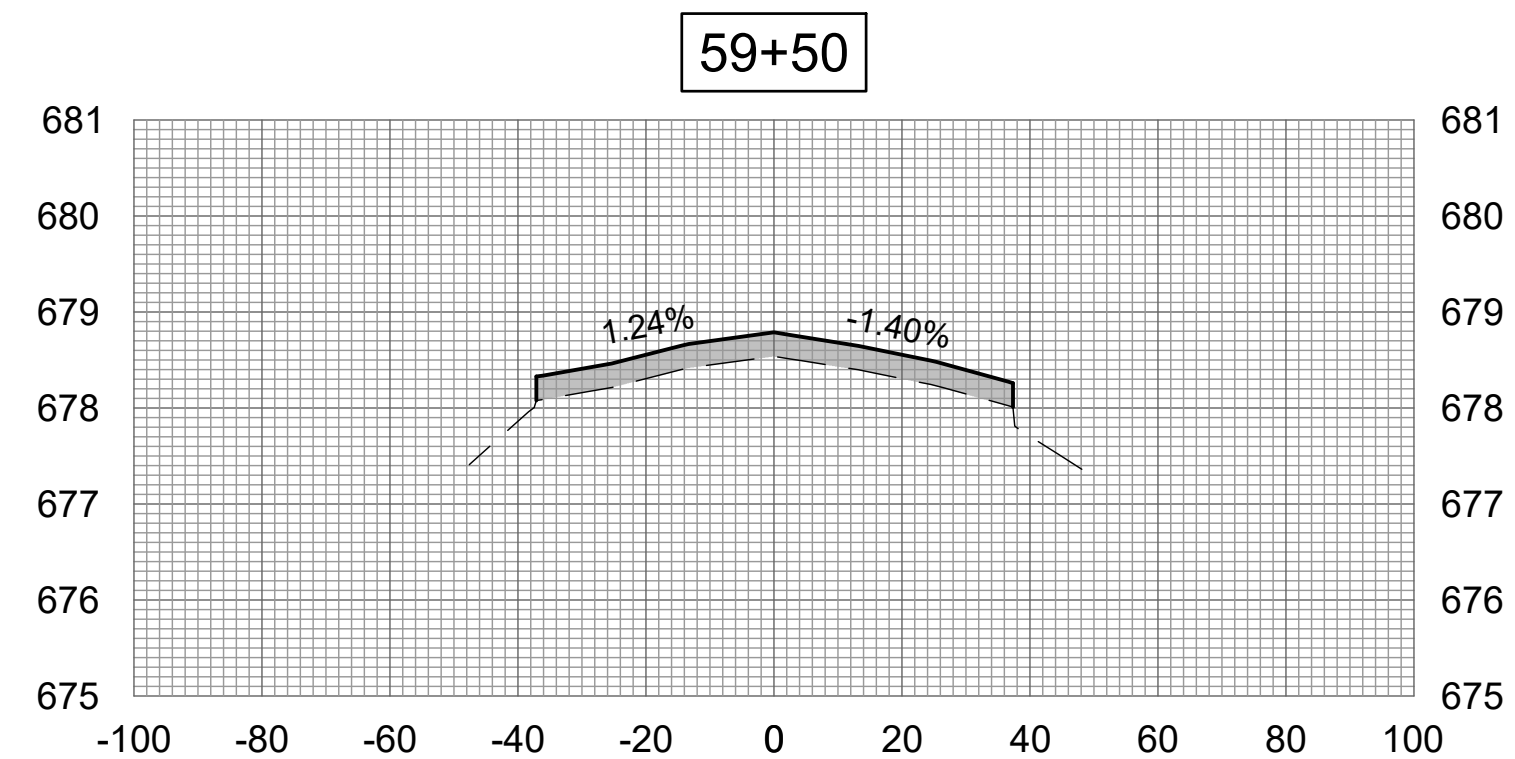
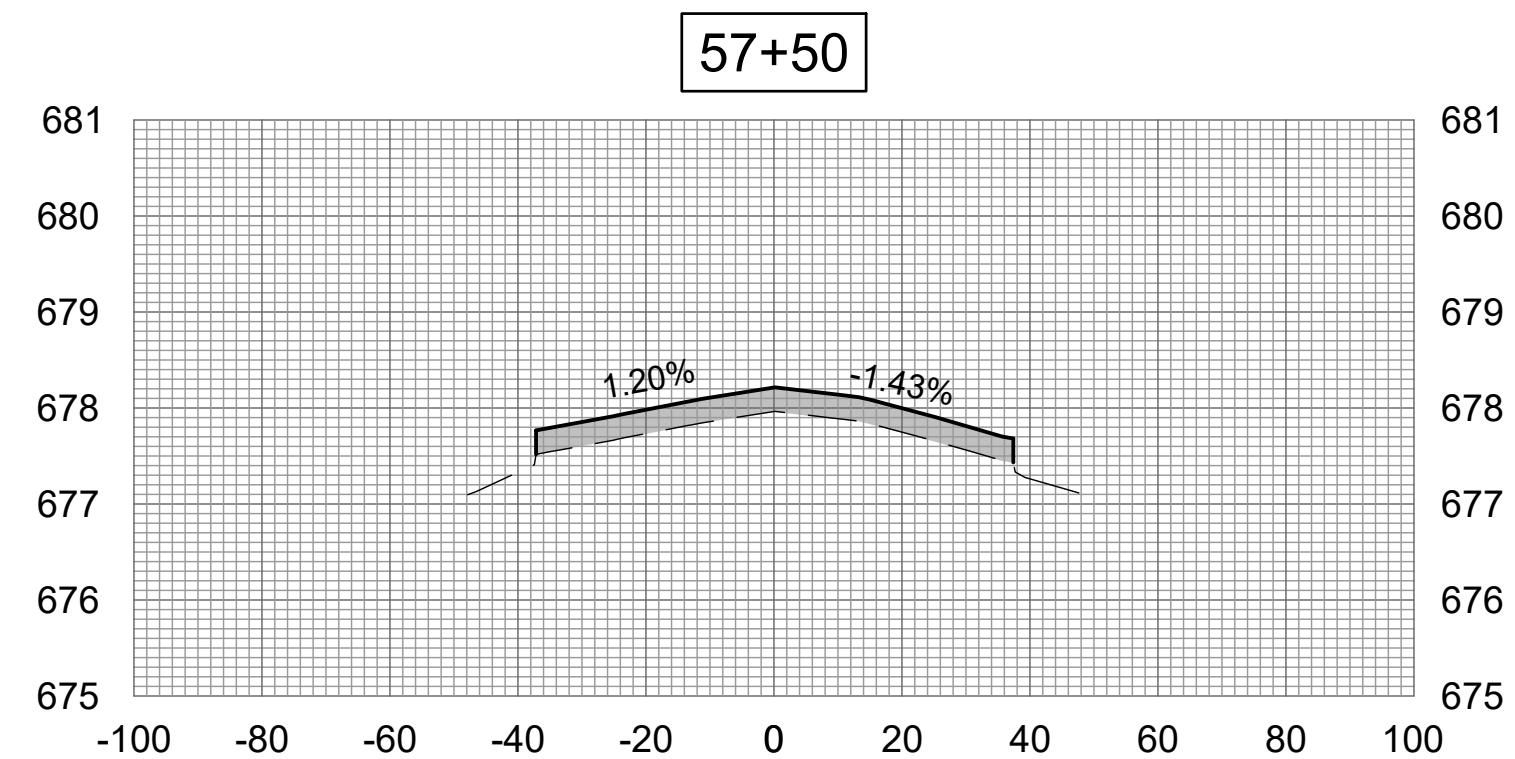
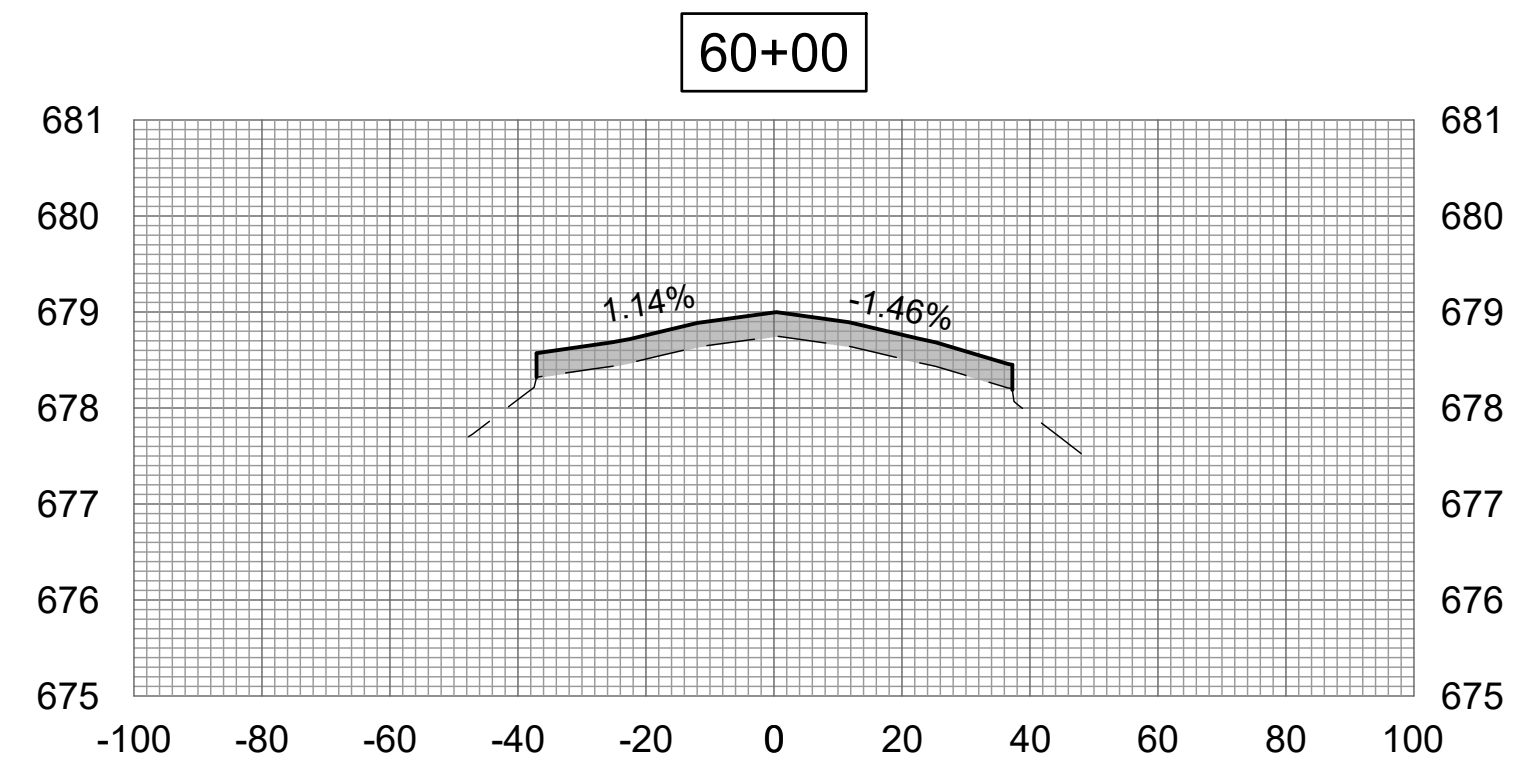
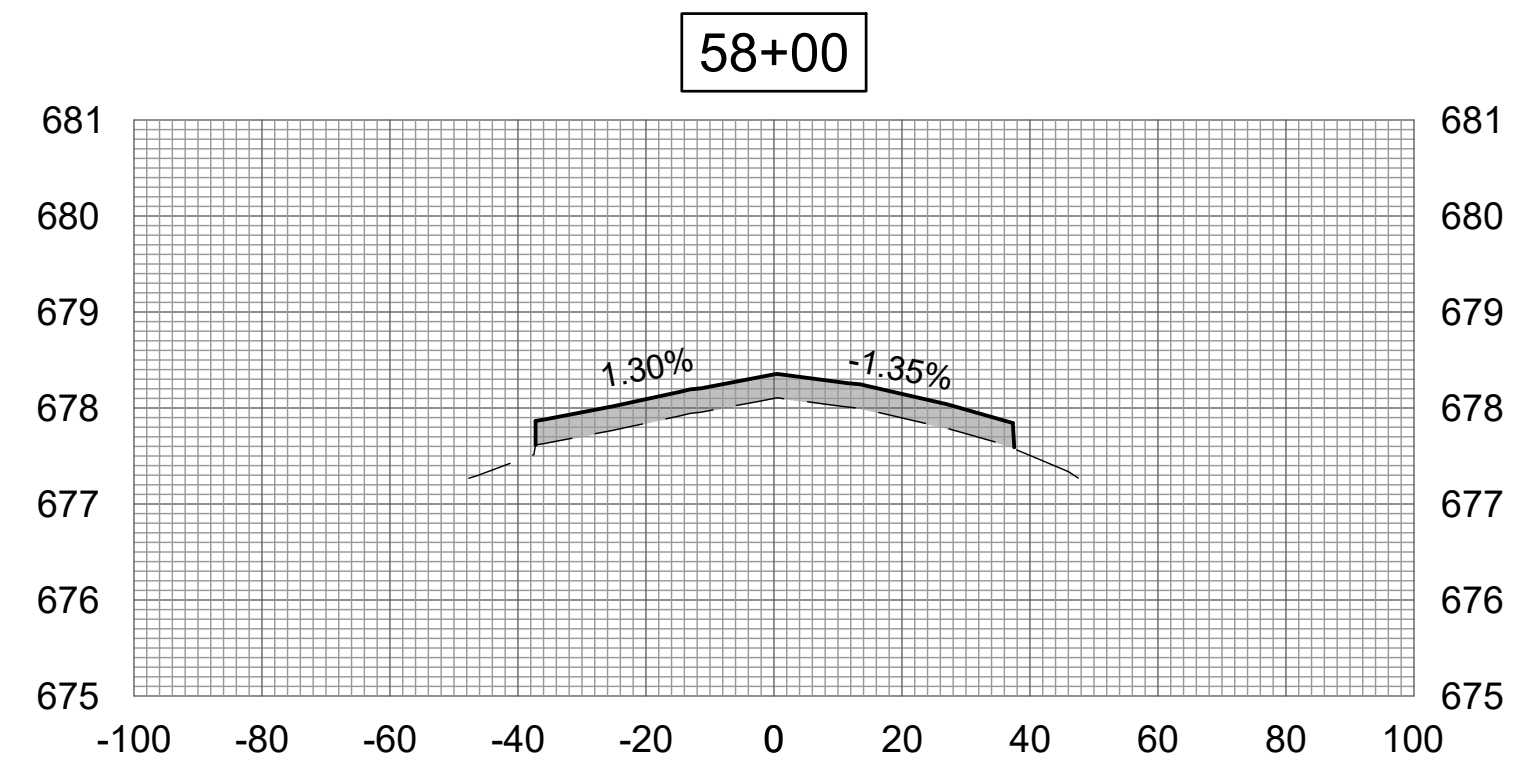
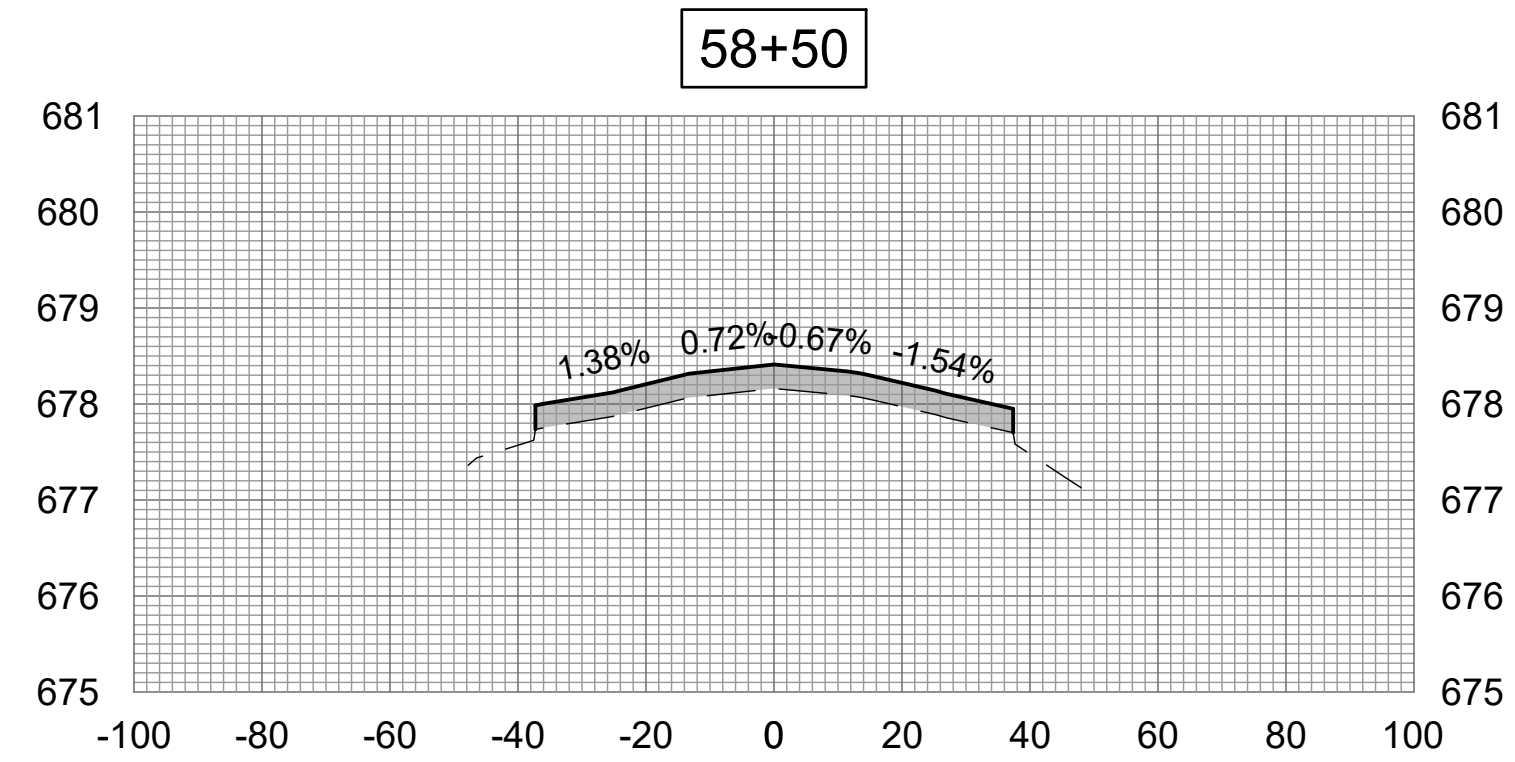
NO.	DATE	DESCRIPTION		
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ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 8





**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 9

FOR BID



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

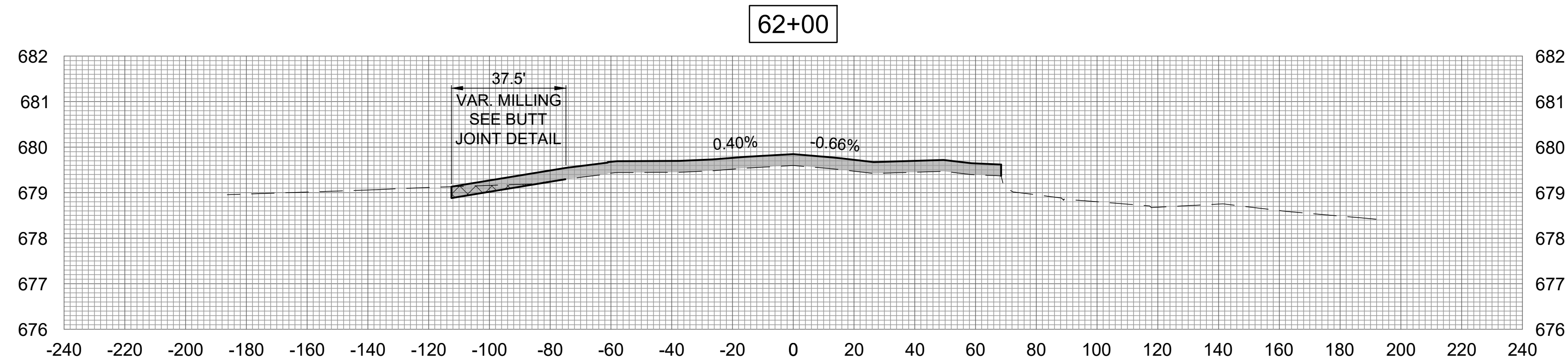
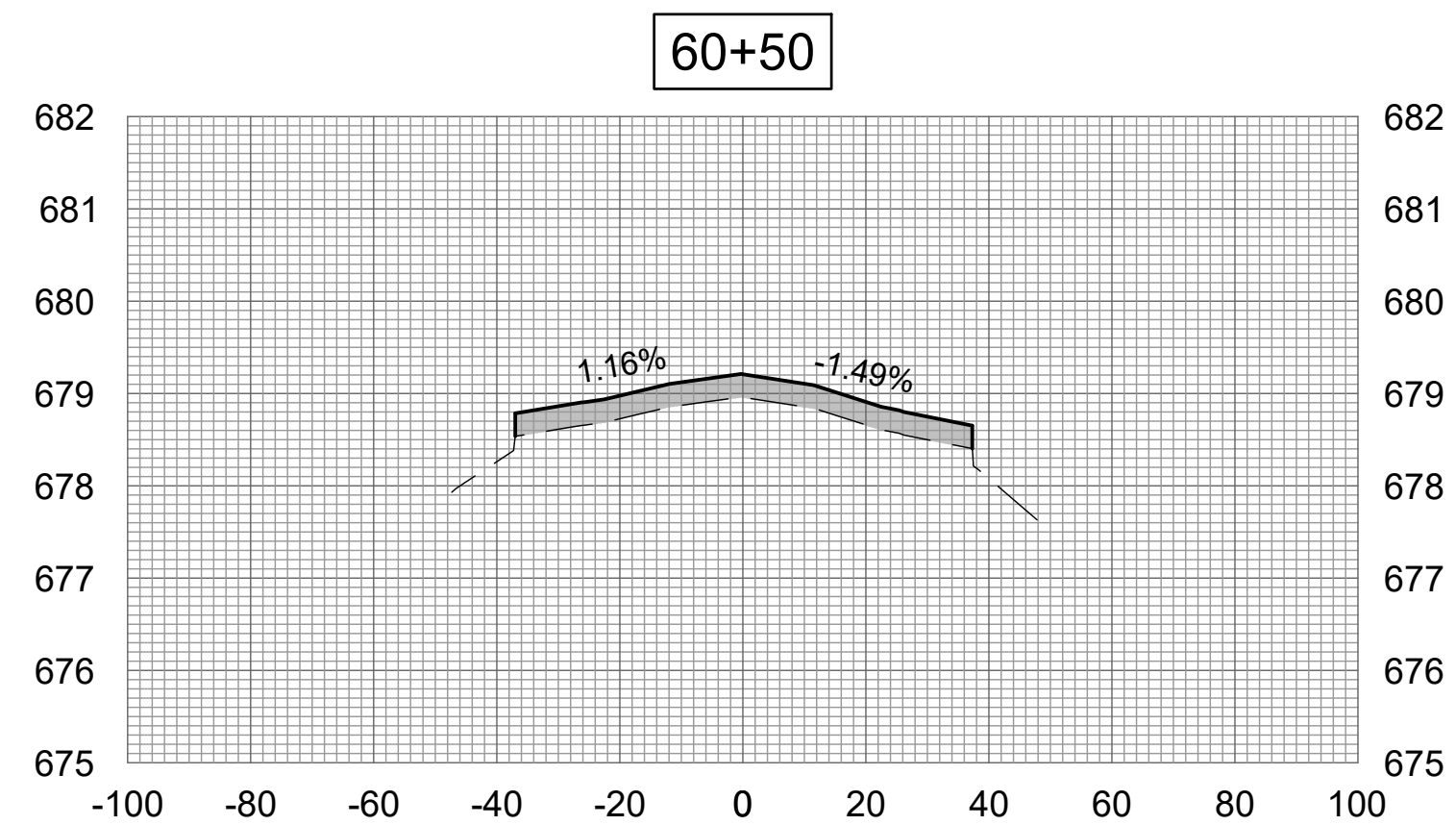
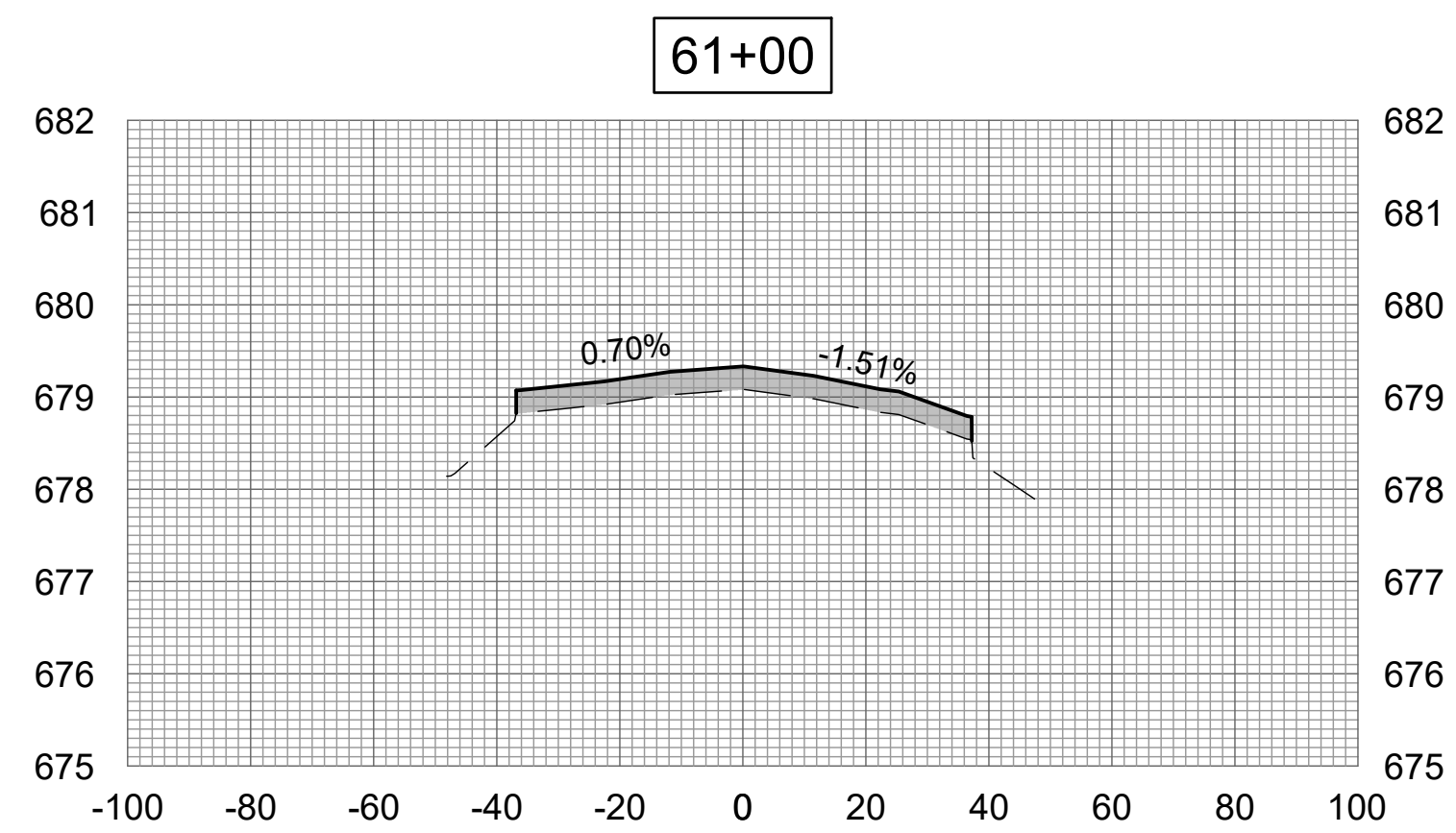
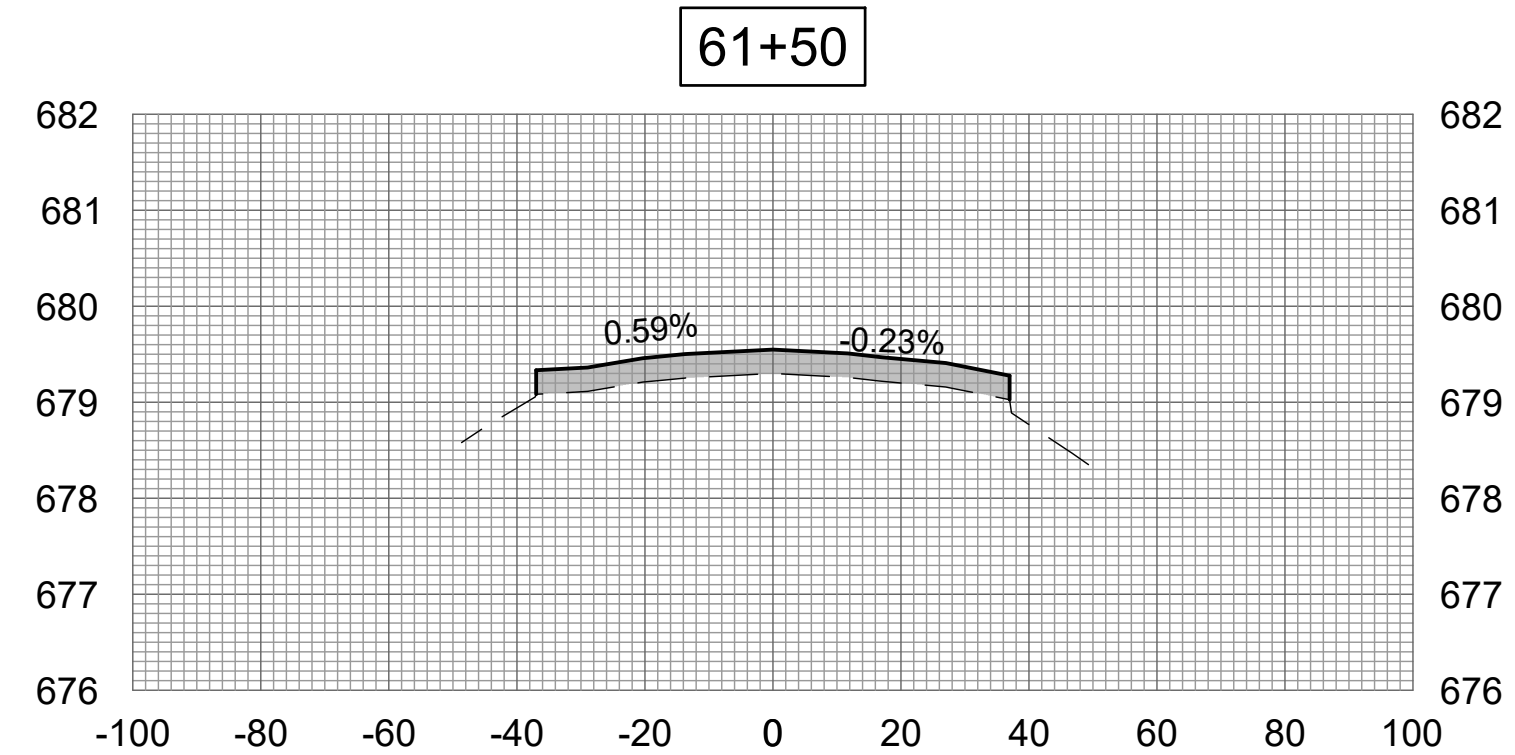
Contract No. CN028

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ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 10



FOR BID



**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

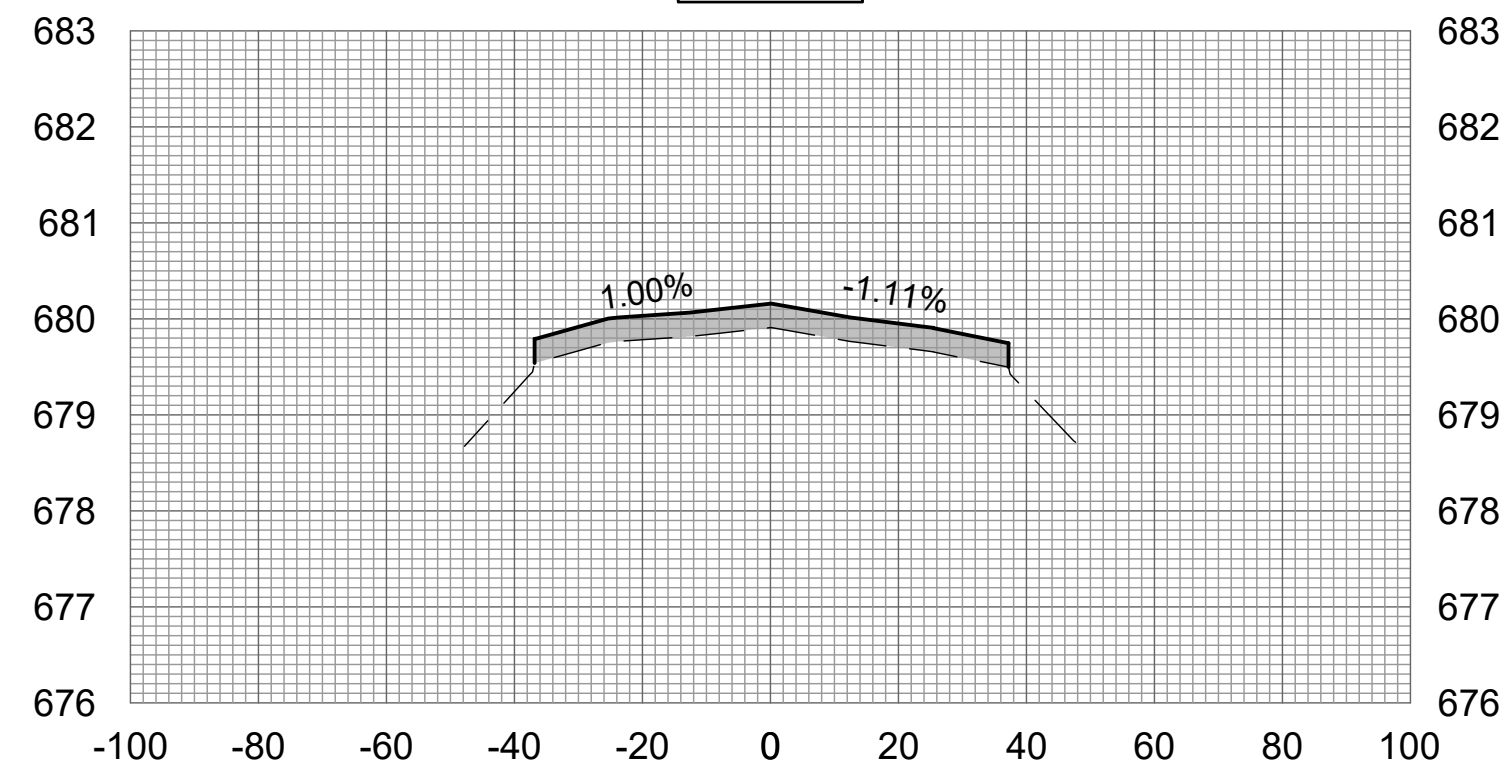
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ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

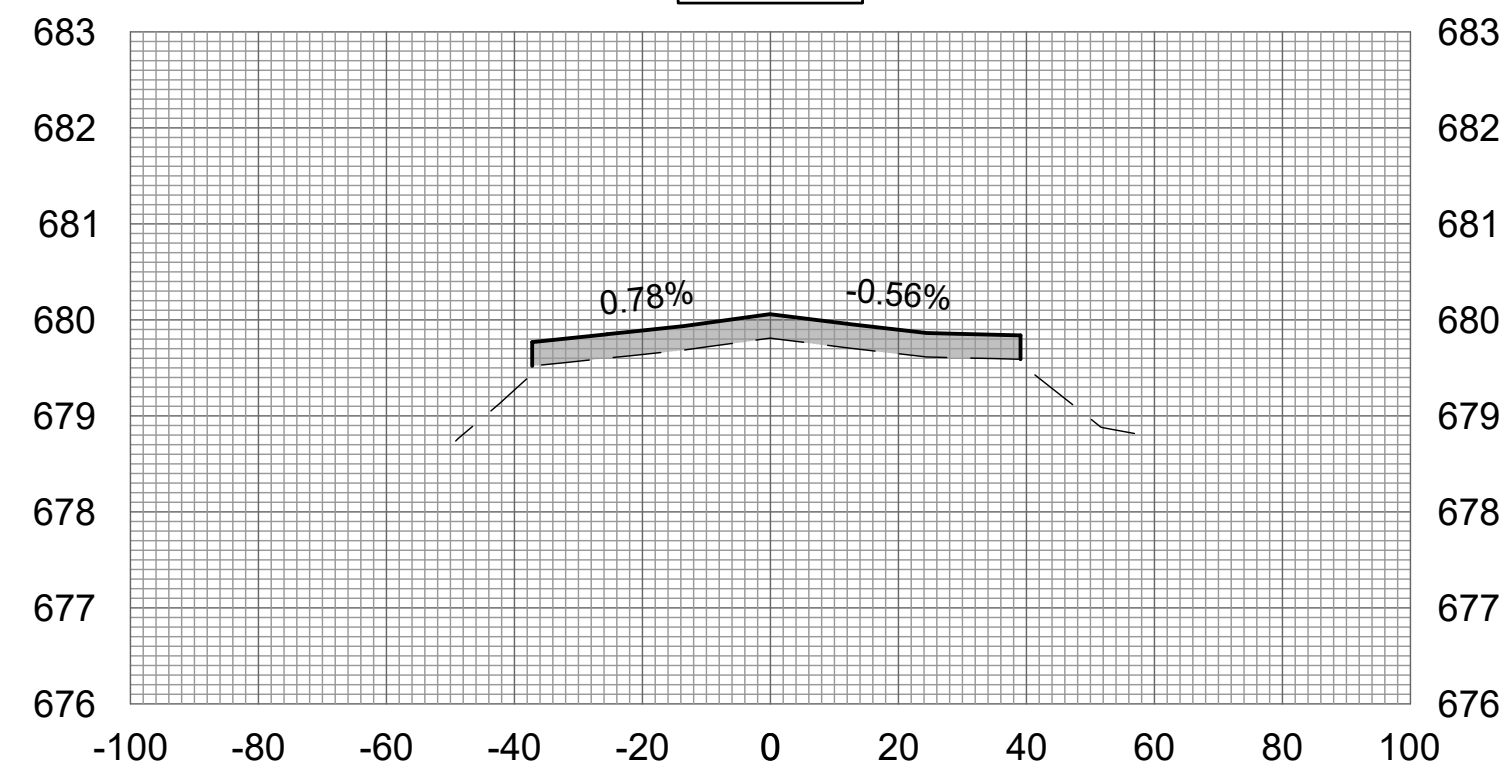
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CROSS-SECTION 11

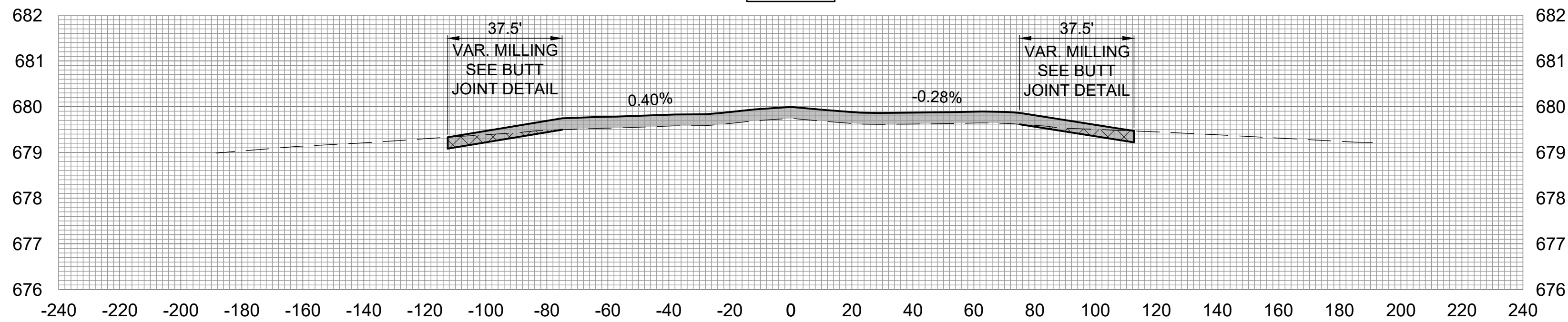
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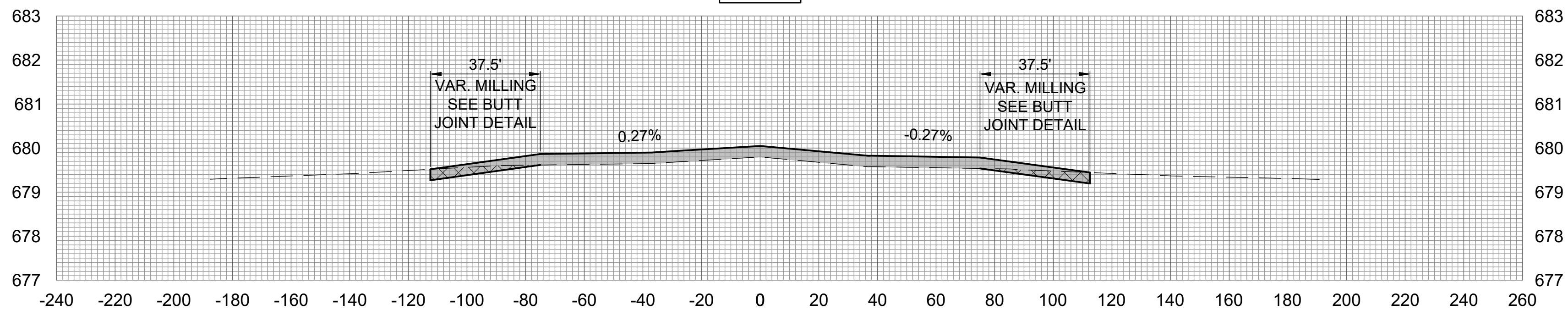
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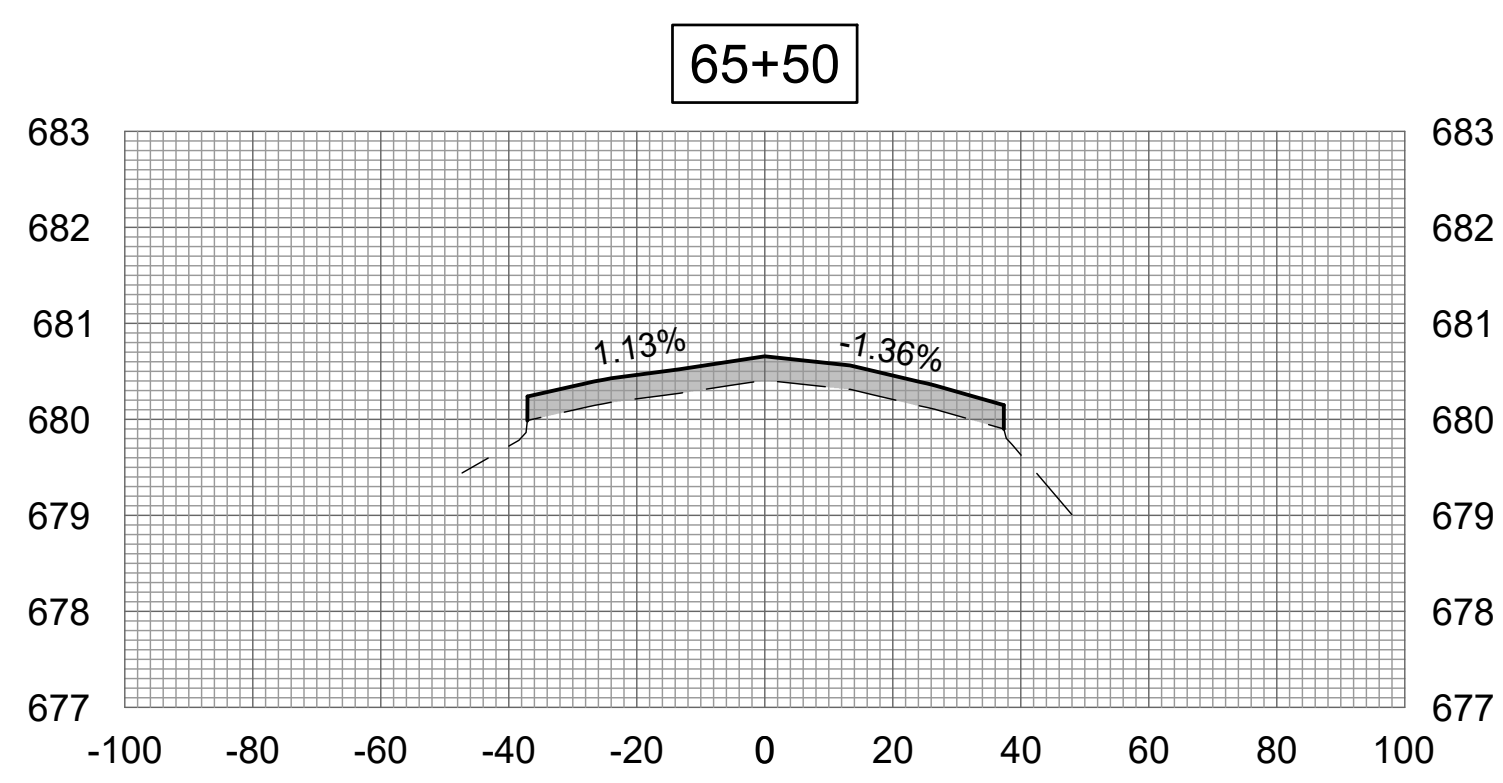
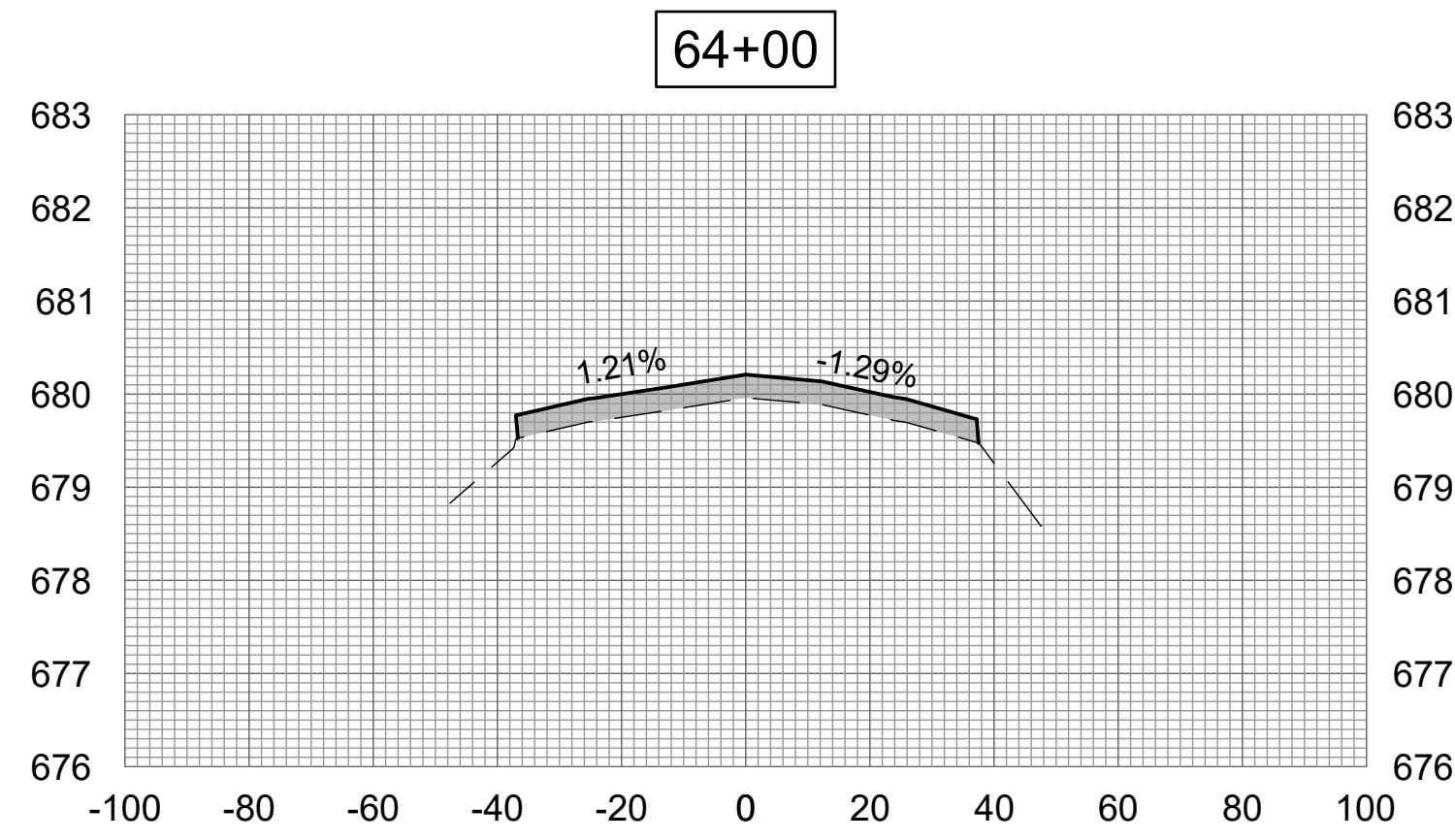
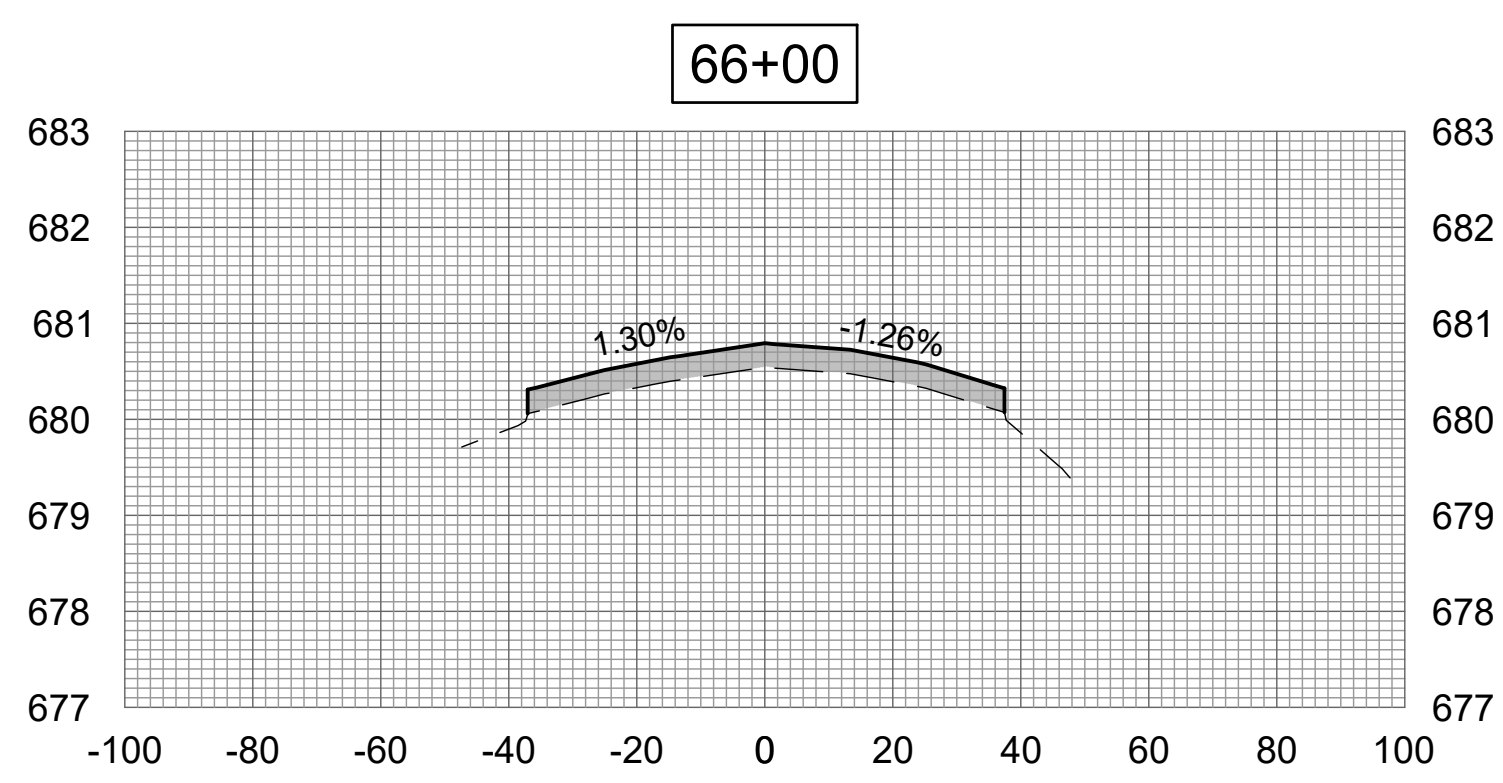
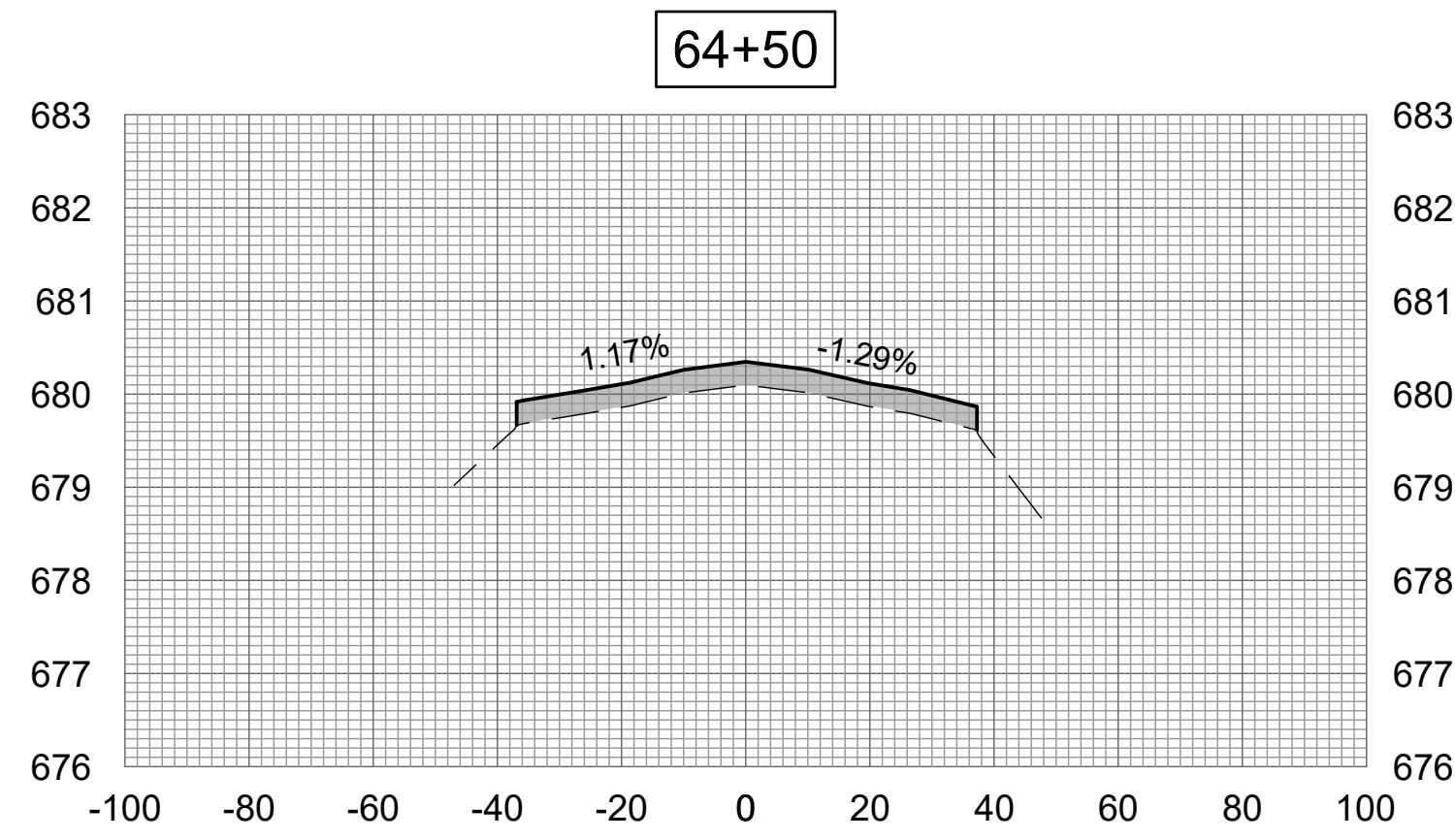
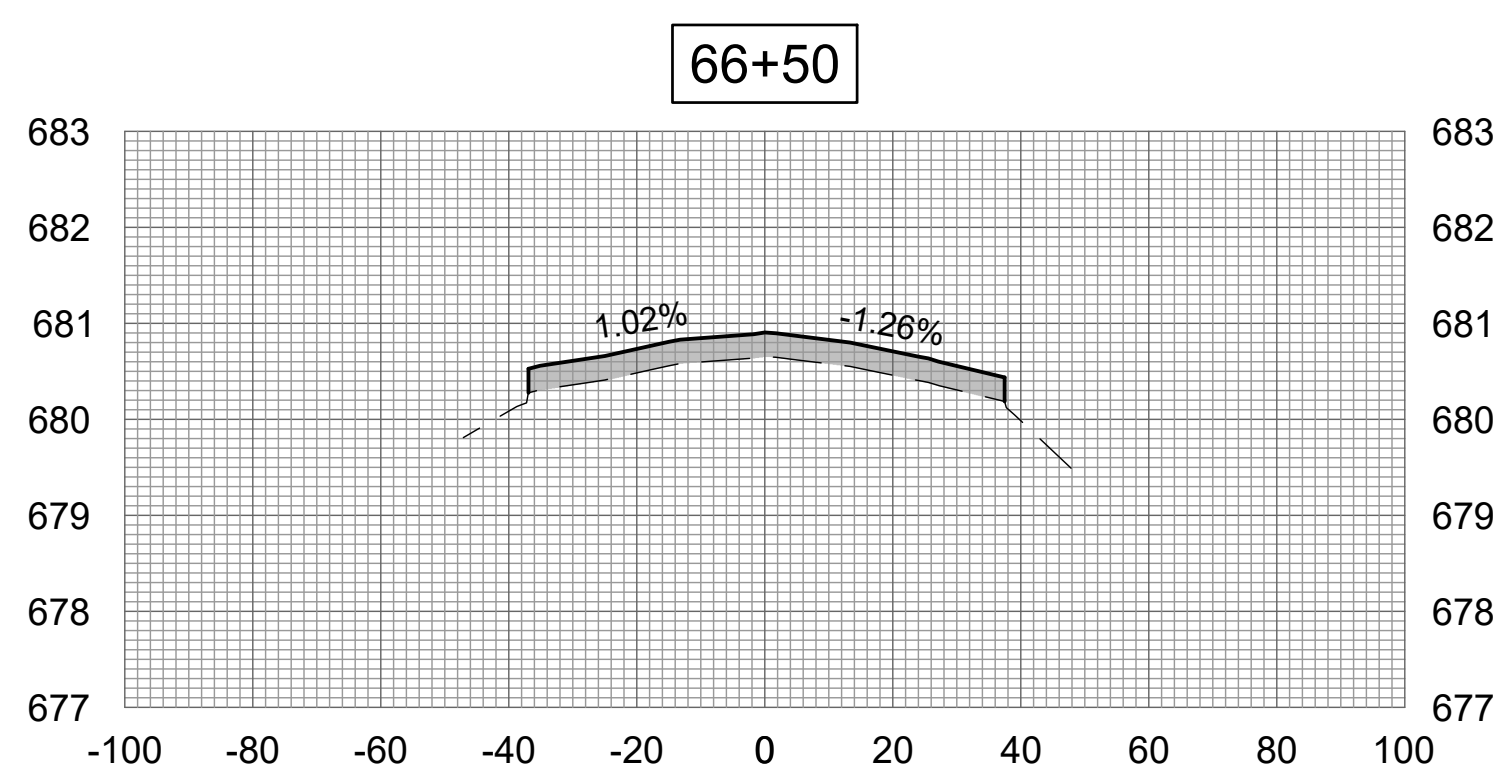
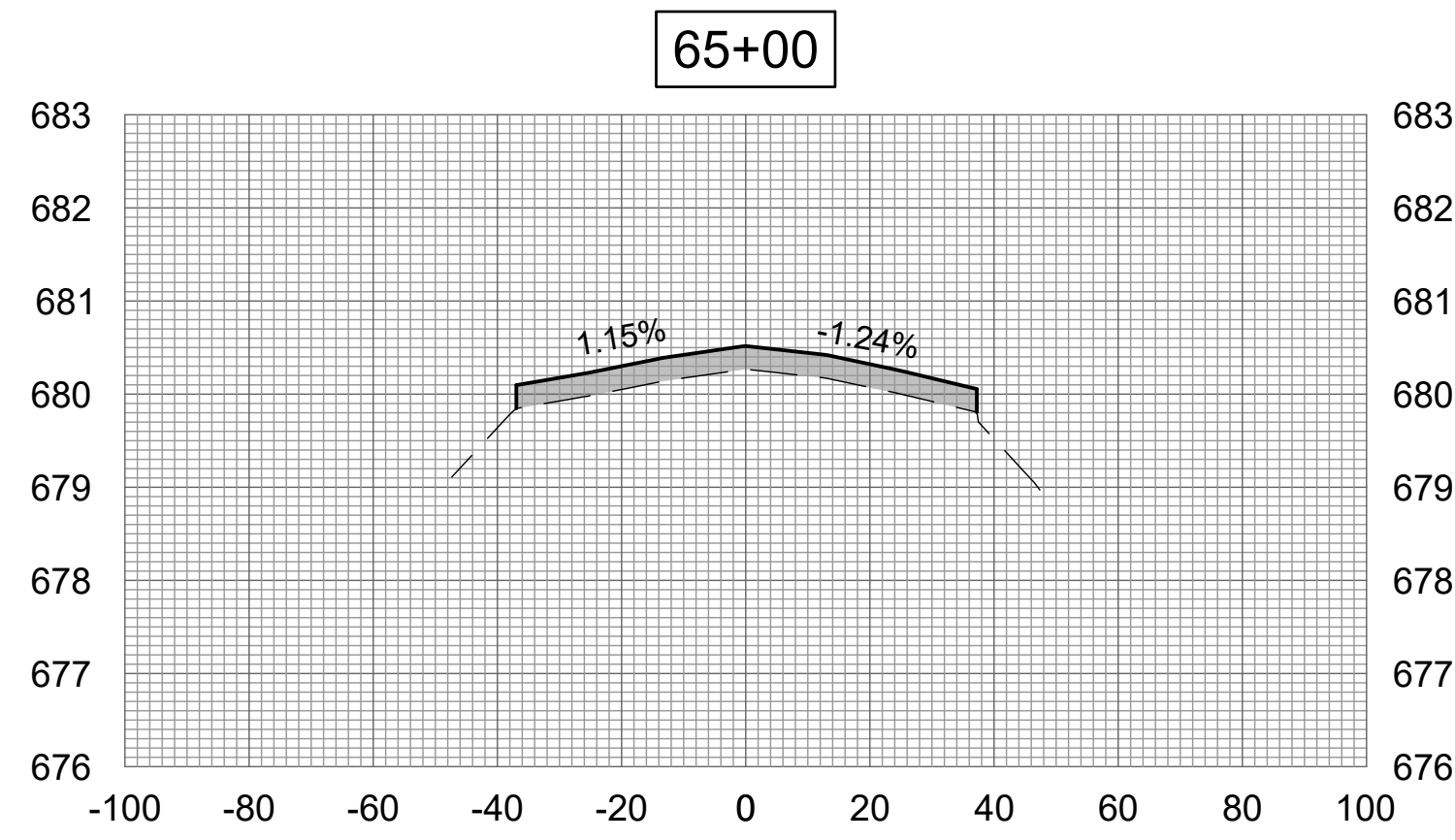
62+50



62+31



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**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

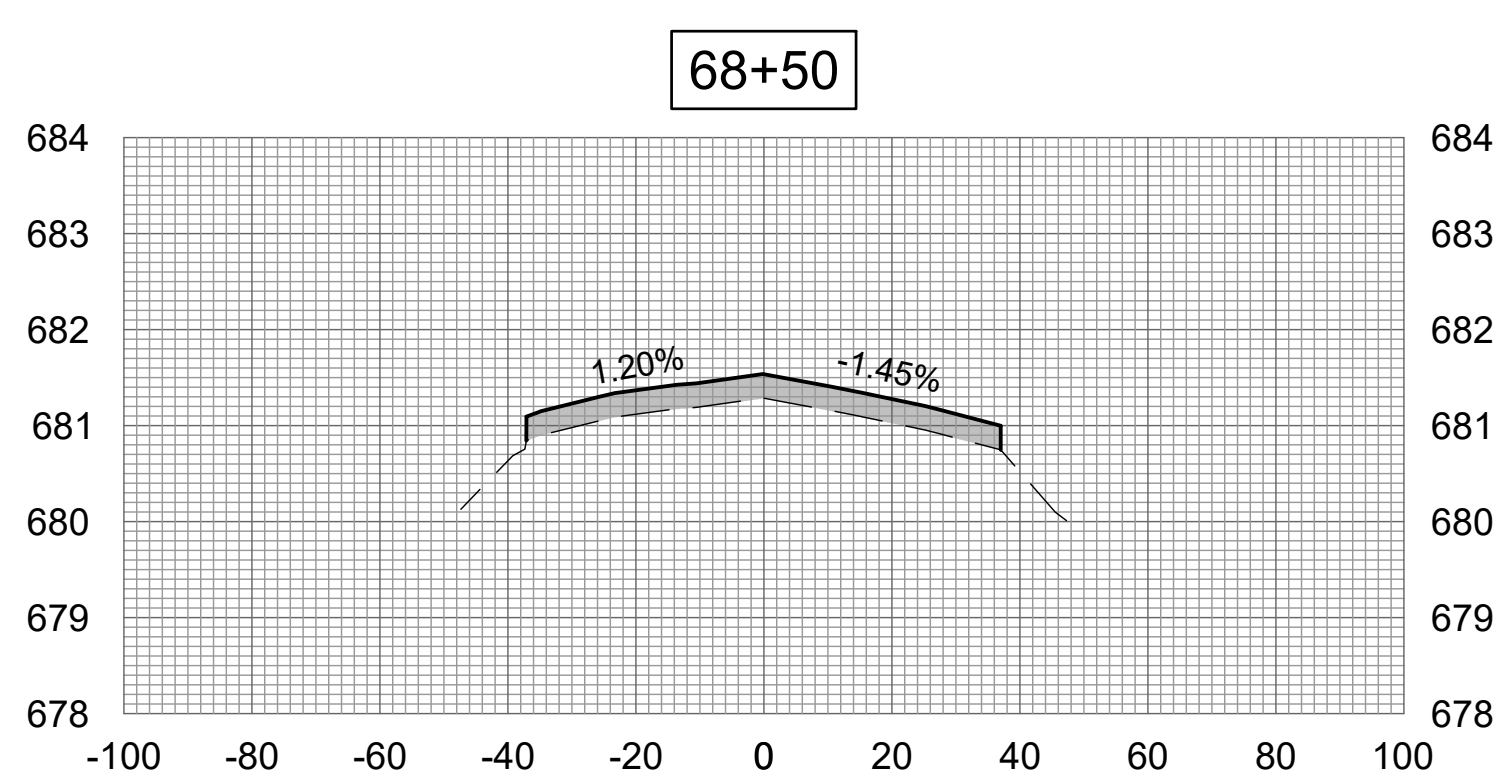
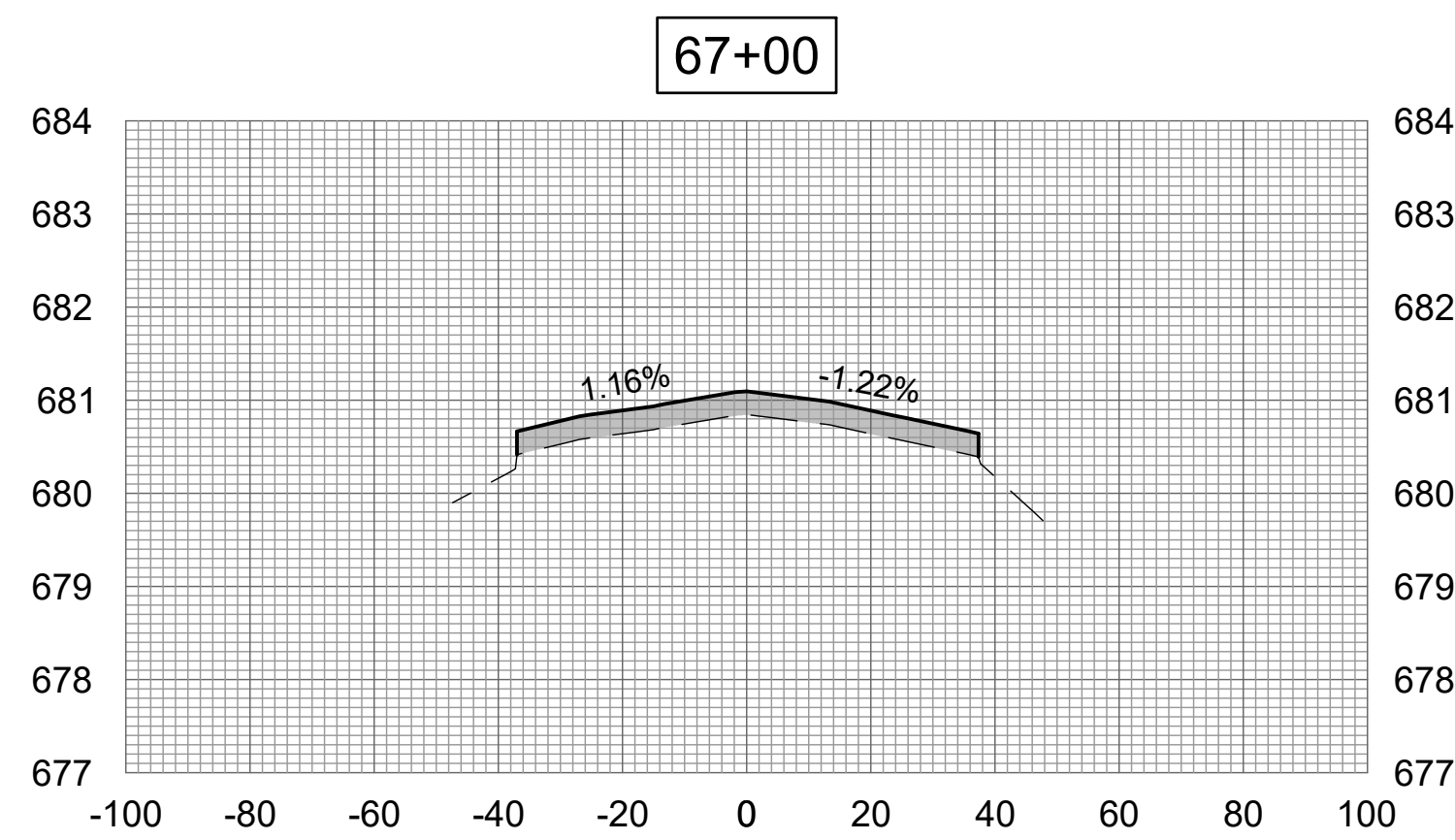
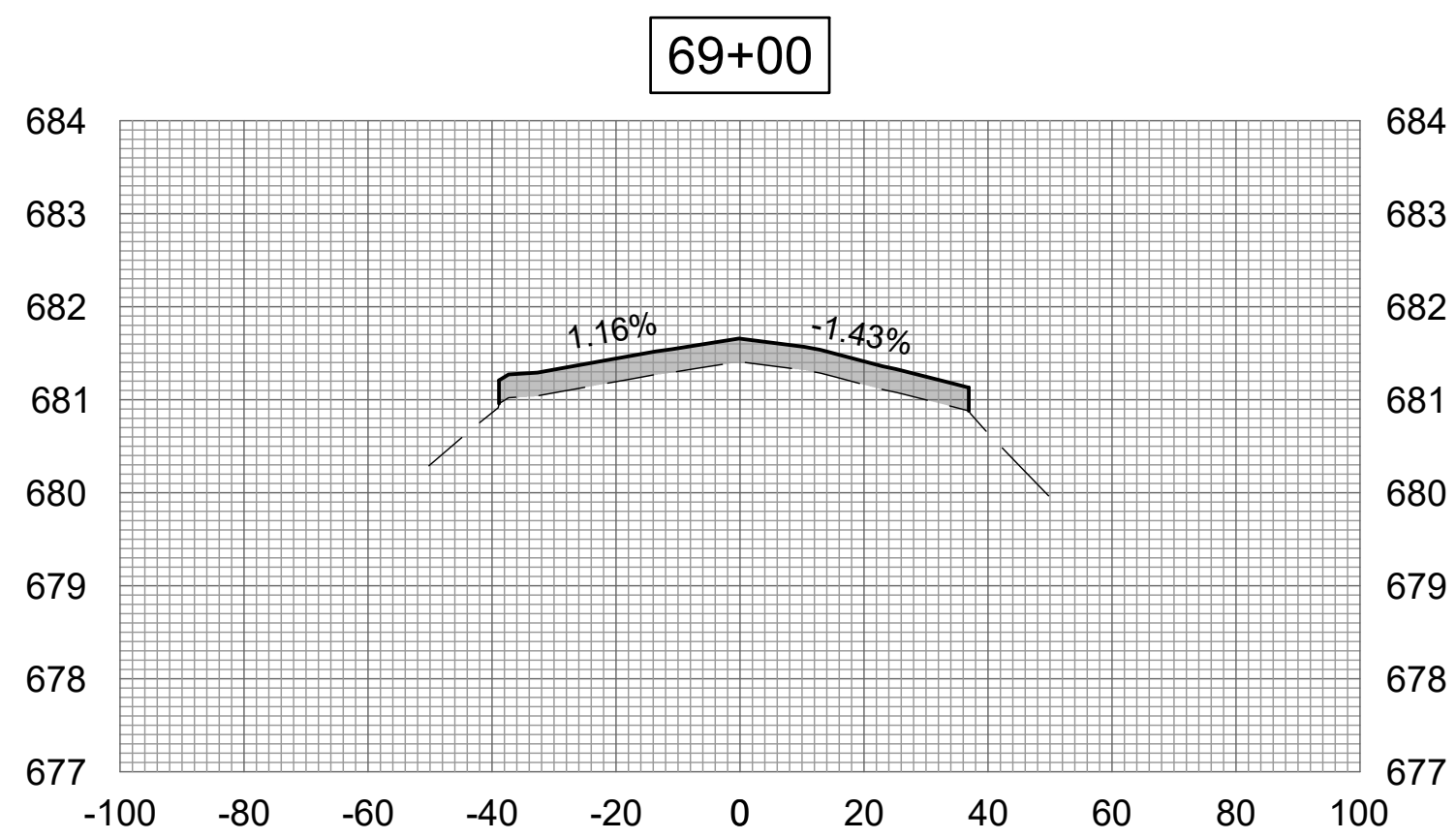
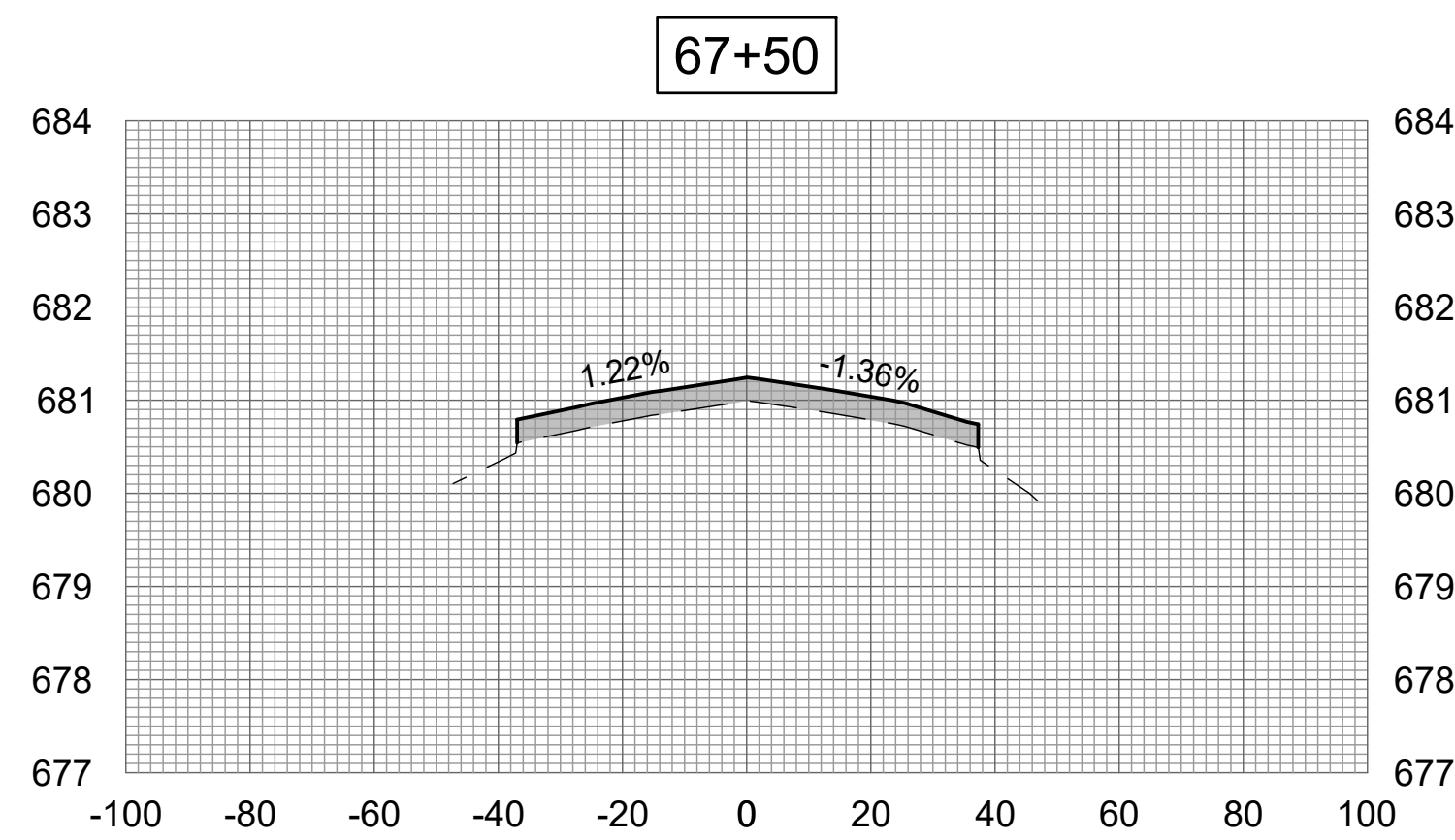
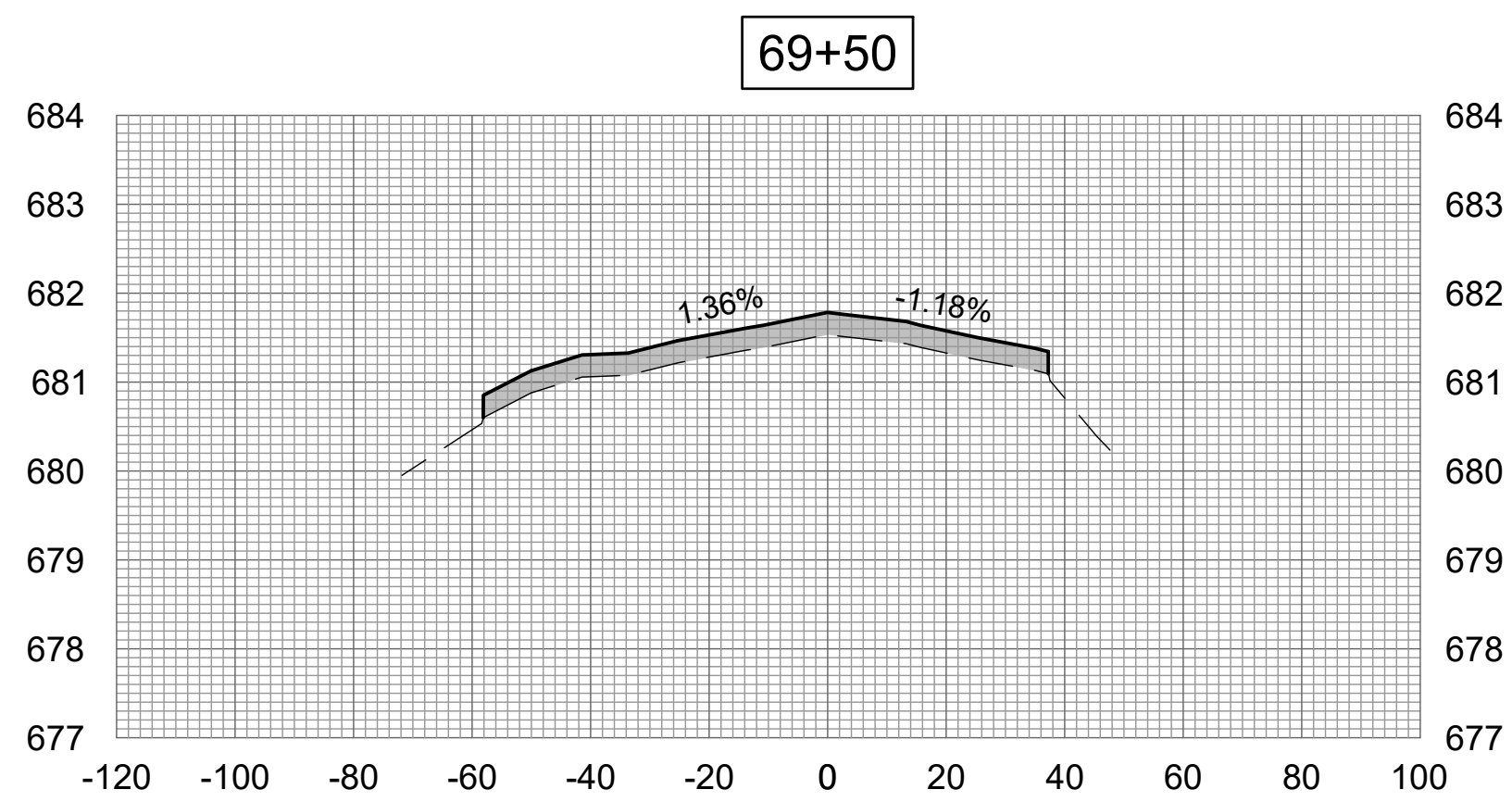
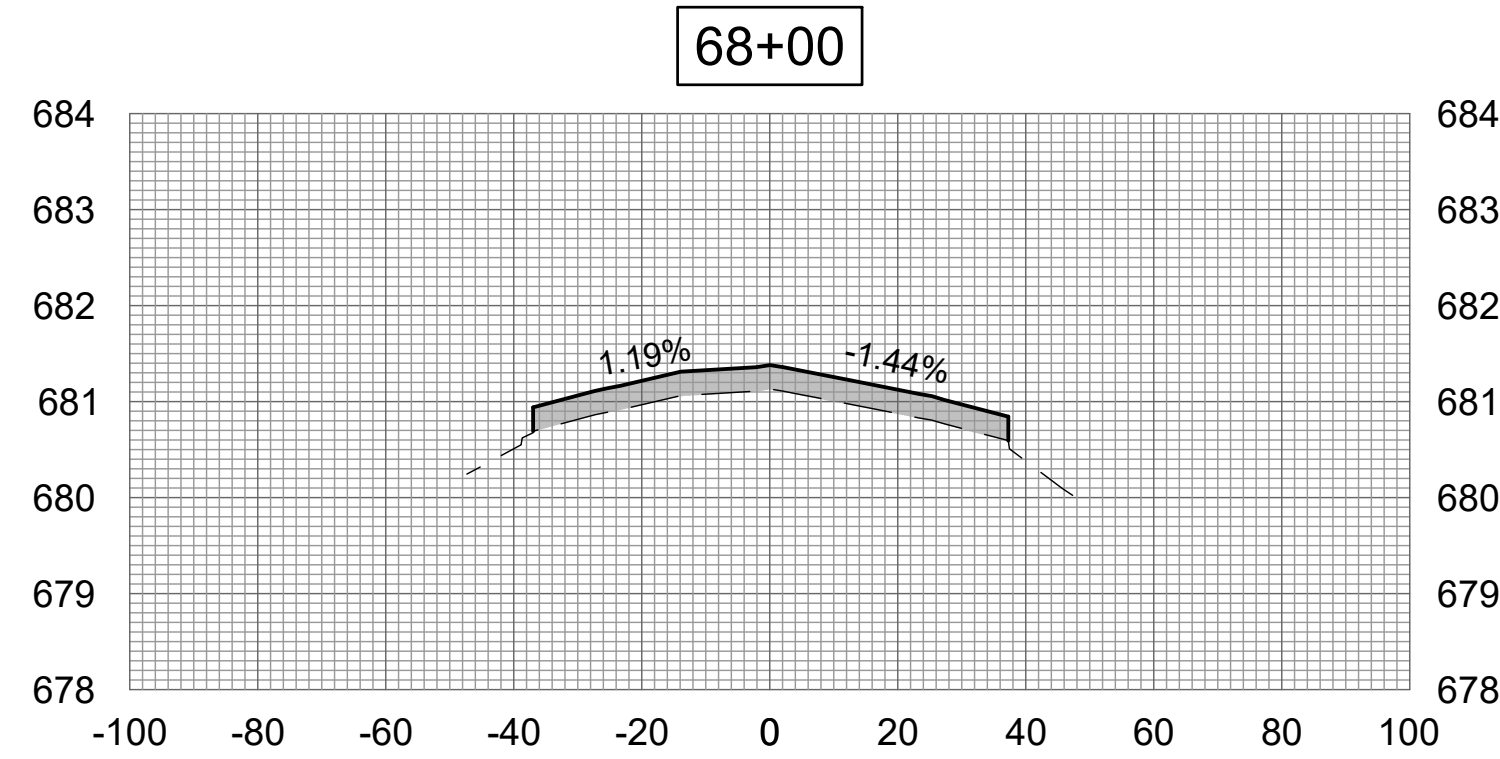
ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
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DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 12

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**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 13

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**RESURFACE
RUNWAY 18-36**

SBGP No:
3-17-SBGP-TBD

IDA No: CTK-5121

Contract No. CN028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: APRIL 19, 2024
PROJECT NO: 23A1039_00
CAD FILE: C-301-XS.DWG
DESIGN BY: JAP 02/28/24
DRAWN BY: KCB 02/28/24
REVIEWED BY: JMO 04/19/2024

SHEET TITLE

CROSS-SECTION 14

