

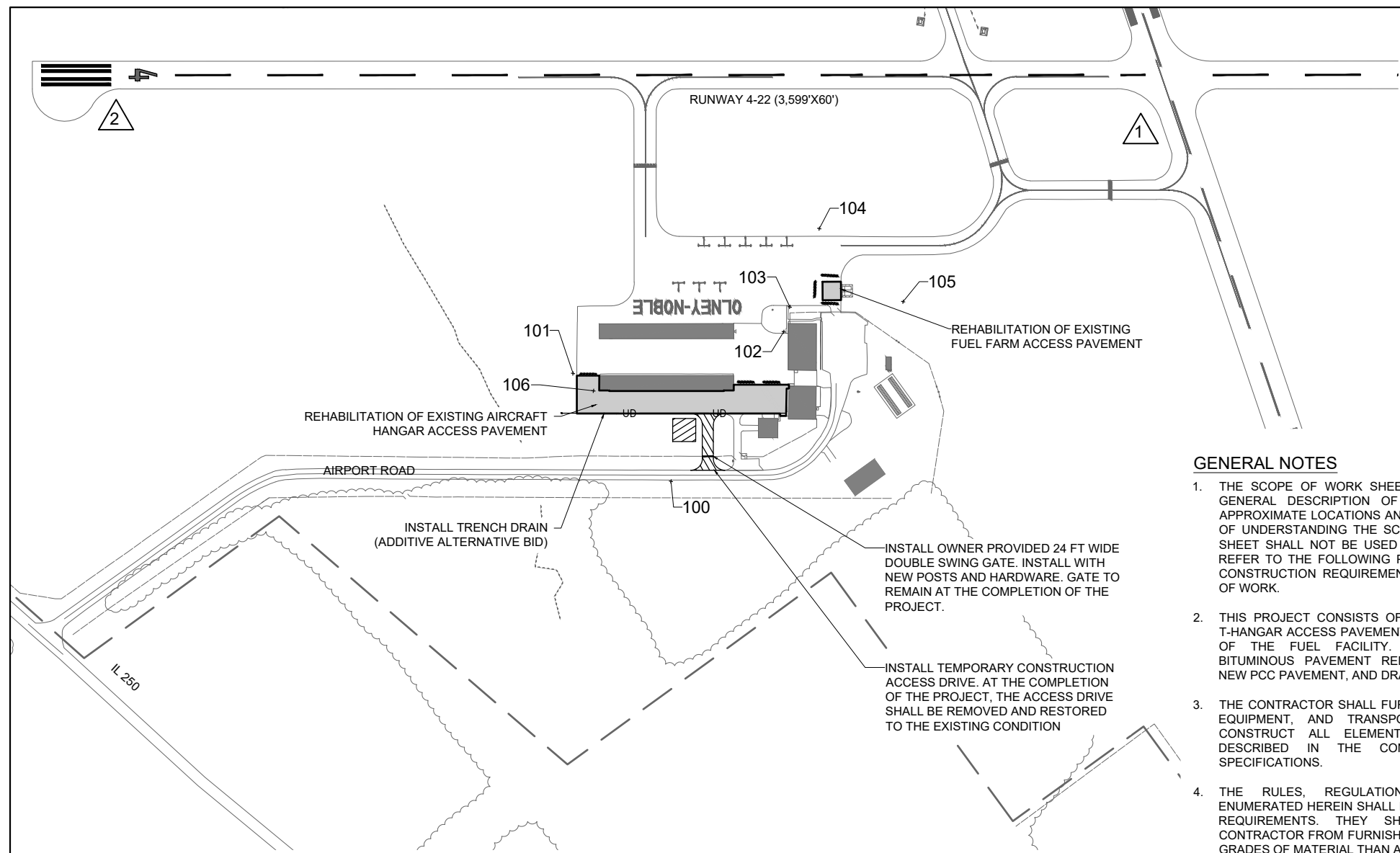


LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDINGS
- PROJECT WORK AREA
- STAGING AREA
- CRITICAL POINT
- NGS SURVEY CONTROL POINT
- SURVEY CONTROL POINT

J.U.L.I.E. INFORMATION

COUNTY _____ RICHLAND
CITY _____ OLNEY
TOWNSHIP _____ NOBLE
SECTION NO. _____ 2
ADDRESS _____ OLNEY-NOBLE AIRPORT
4395 N. AIRPORT RD.
NOBLE, ILLINOIS 62868



GENERAL NOTES

1. THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS AND THEIR APPROXIMATE LOCATIONS AND LIMITS, FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. THIS SHEET SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE FOLLOWING PLAN SHEETS FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
2. THIS PROJECT CONSISTS OF THE RECONSTRUCTION OF T-HANGAR ACCESS PAVEMENTS AND PAVEMENT IN FRONT OF THE FUEL FACILITY. THE PROJECT INCLUDES BITUMINOUS PAVEMENT REMOVAL, CONSTRUCTION OF NEW PCC PAVEMENT, AND DRAINAGE IMPROVEMENTS.
3. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
4. THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN.
5. THE CONTRACTOR IS NOT PERMITTED TO USE THE AIRPORT ENTRANCE DRIVE AND AUTO PARKING LOT FOR MATERIAL AND EQUIPMENT HAULING OR STORAGE. THE CONSTRUCTION ENTRANCE AS SHOWN ON THE PROPOSED SAFETY AND PHASING PLAN WILL BE THE PRIMARY ACCESS TO THE PROPOSED CONSTRUCTION SITE. ALL HAULING OF MATERIALS AND EQUIPMENT SHALL BE RESTRICTED TO THE DESIGNATED CONSTRUCTION ENTRANCE.
6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT. ANY DAMAGE TO FAA CABLES, ACCESS ROADS OR TO FAA FACILITIES DURING CONSTRUCTION WILL REQUIRE THE CONTRACTOR TO REPLACE THE DAMAGED CABLES, ACCESS ROAD OR FAA FACILITIES TO FAA REQUIREMENTS AT THE CONTRACTOR'S EXPENSE. SPLICING OF CABLES IS NOT AN ACCEPTABLE FORM OF REPAIR.
7. NO EQUIPMENT SHALL BE PERMITTED TO CROSS OR USE ANY EXISTING PAVEMENT OUTSIDE THE CONSTRUCTION LIMITS, GENERAL PROJECT AREA OR HAUL ROUTE.
8. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
9. UNLESS OTHERWISE NOTED, ALL DISTURBED AREAS OUTSIDE OF THE PROPOSED CONSTRUCTION LIMITS SHALL BE GRADED, SEEDED AND/OR HYDROMULCH SEEDED AT NO ADDITIONAL COST TO THE CONTRACT.
10. ALL WASTE MATERIAL SHALL BE HAULED FROM THE AIRPORT AND PROPERLY DISPOSED OF UNLESS OTHERWISE SPECIFIED HEREIN.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
12. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
13. THE CONTRACTOR SHALL COORDINATE WITH THE RESIDENT ENGINEER/TECHNICIAN SO THEY MAY DEVELOP ONE SET OF REDLINED AS-BUILT DRAWINGS AT THE COMPLETION OF THE PROJECT.
14. CONTRACTOR SHALL NOTE THAT ALL AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO BEGINNING ANY WORK. ALL AREAS WHICH HAVE BEEN FARMED AND OR DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION, AND HAVE BEEN DISTURBED BY CONSTRUCTION ACTIVITY, SHALL BE CHISEL PLOWED (36" MAX.) OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER.)
15. CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS TO BE REPAIRED, GRADED, AND SEEDED AND MULCHED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF THE HAUL ROUTE.
16. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER/TECHNICIAN IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS.
17. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER PERSONS FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
18. THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE FOR ALL AREAS AFFECTED BY HIS WORK.

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES.

PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF ITS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND THE RESIDENT ENGINEER/TECHNICIAN SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND LOCATED BY THE FAA. ALSO CONTACT AIRPORT MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

CONTROL POINTS				
POINT #	DESCRIPTION	ELEVATION	NORTHING	EASTING
	"OLNEPORT" NGS MONUMENT	469.00	748331.50	1029840.81
	"OLNEPORT AZ MK" NGS MONUMENT	461.00	746543.74	1028518.32
100	847 821 3IN BG	465.07	747074.98	1029868.27
101	847 821 4IN BG	465.24	747039.47	1029554.32
102	847 821 3IN BG	466.98	747463.03	1029748.04
103	847 821 4IN BG	467.04	747505.15	1029712.05
104	847 821 6IN BG	465.40	747656.36	1029611.70
105	847 821 5IN BG	467.69	747711.22	1029846.58
106	824 847	466.41	747053.46	1029610.65

SURVEY NOTES

1. ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD 83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.



DATE SIGNED: 5/3/2024 LICENSE EXPIRES: 11/30/2025

REHABILITATE AIRCRAFT HANGAR ACCESS PAVEMENTS

IDA No: OLY-5017
Contract No. OL018

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
CAD FILE: G-004-SOW.DWG
DESIGN BY: JRH 8/25/2023
DRAWN BY: NLD 8/25/2023
REVIEWED BY: BSS 8/25/2023

SHEET TITLE

SCOPE OF WORK

FOR BID



DATE SIGNED: 5/3/2024 LICENSE EXPIRES: 11/30/2025

REHABILITATE AIRCRAFT
HANGAR ACCESS
PAVEMENTS

IDA No: OLY-5017
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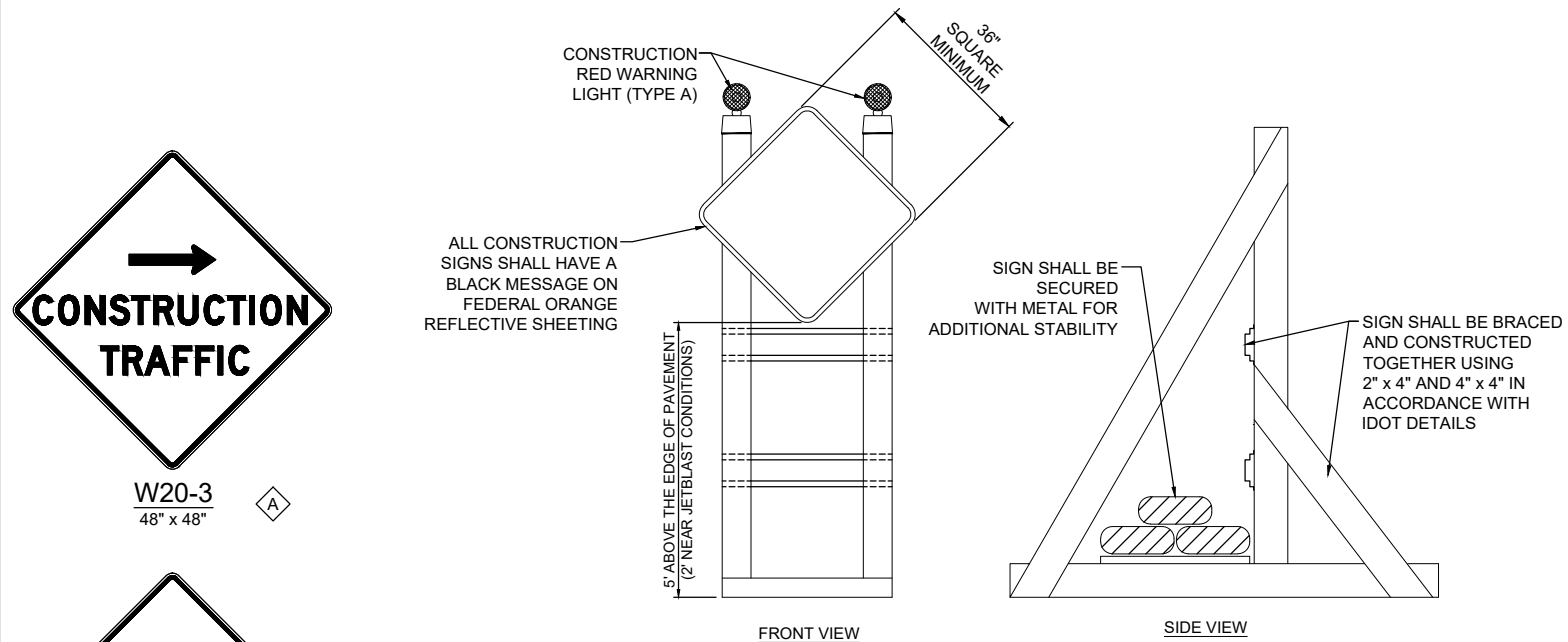
ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
CAD FILE: G-501-SFY.DWG
DESIGN BY: JRH 8/25/2023
DRAWN BY: NLD 8/25/2023
REVIEWED BY: BSS 8/25/2023

SHEET TITLE

CONSTRUCTION
SAFETY NOTES AND
DETAILS

SAFETY NOTES

- THE FOLLOWING NOTES ARE THE CONSTRUCTION SAFETY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET AND THIS SHEET.
- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2G (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5D, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE, WITHIN 62' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA (EX. TAXIWAY), OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2G, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 62' OF ANY TAXIWAY CENTERLINE WILL BE PERMITTED UNLESS APPROPRIATELY BACKFILLED OR COVERED. COVERING FOR OPEN TRENCHES MUST BE DESIGNED TO ALLOW SAFE OPERATIONS OF THE HEAVIEST AIRCRAFT OPERATING ON THE RUNWAY/TAXIWAY ACROSS THE TRENCH WITHOUT DAMAGING THE AIRCRAFT. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT UNLESS PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRACKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE OR WITHIN 62' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 7 DAYS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED BY THE AIRPORT MANAGER TO THE APPROPRIATE FLIGHT SERVICE STATION.
- UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.



ALL CONSTRUCTION SIGNS SHALL HAVE A BLACK MESSAGE ON FEDERAL ORANGE REFLECTIVE SHEETING

SIGN SHALL BE SECURED WITH METAL FOR ADDITIONAL STABILITY

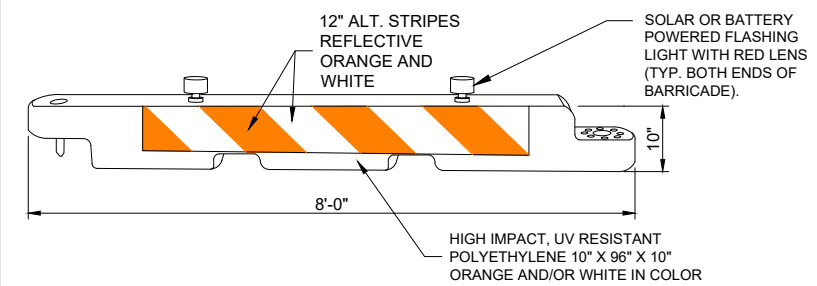
SIGN SHALL BE BRACED AND CONSTRUCTED TOGETHER USING 2" x 4" AND 4" x 4" IN ACCORDANCE WITH IDOT DETAILS

SIGNAGE NOTES

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- UNLESS OTHERWISE SPECIFIED, CONSTRUCTION SIGNS SHALL BE MOUNTED ON PORTABLE OR NON-PORTABLE SUPPORTS. A PORTABLE SUPPORT IS DEFINED AS A TYPICAL SIGN STANDARD AS SHOWN ON THIS SHEET, OR A SMALL LIGHT WEIGHT TRAILER. A NON-PORTABLE SUPPORT IS DEFINED AS DRIVEN METAL OR WOOD POST. ALL SIGNS, REGARDLESS OF THE TYPE OF SUPPORTS USED, SHALL BE MOUNTED SUCH THAT THE MESSAGE ON THE SIGN IS LEVEL IN THE HORIZONTAL PLANE AFTER PLACEMENT. THE COST OF CONSTRUCTION WARNING LIGHTS SHALL BE INCLUDED IN THE COST OF THE CONSTRUCTION SIGNS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY ARE TO BE USED IN A LOW INTENSITY FLASHING MODE (TYPE A).
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING SIGNS SHALL BE INCLUDED IN ITEM AR150540 HAUL ROUTE.

BARRICADE NOTES

- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT IN 4' INCREMENTS. BARRICADES ARE TO BE SET BACK 66' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR BEFORE SUNSET AND 1/2 HOUR AFTER SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING BARRICADES SHALL BE INCLUDED IN ITEM AR150520 MOBILIZATION.



LOW-PROFILE BARRICADE DETAIL
NOT TO SCALE

DETAIL ABOVE REPRESENTS ONE OPTION FOR LOW-PROFILE BARRICADES. OTHER OPTIONS MAY BE UTILIZED AS LONG AS THEY MEET THE REQUIREMENTS OF THE PROJECT, INCLUDING BARRICADE NOTE 1.



DATE: 5/3/2024 LICENSE: 11/30/2025
SIGNED: EXPIRES:

**REHABILITATE AIRCRAFT
HANGAR ACCESS
PAVEMENTS**

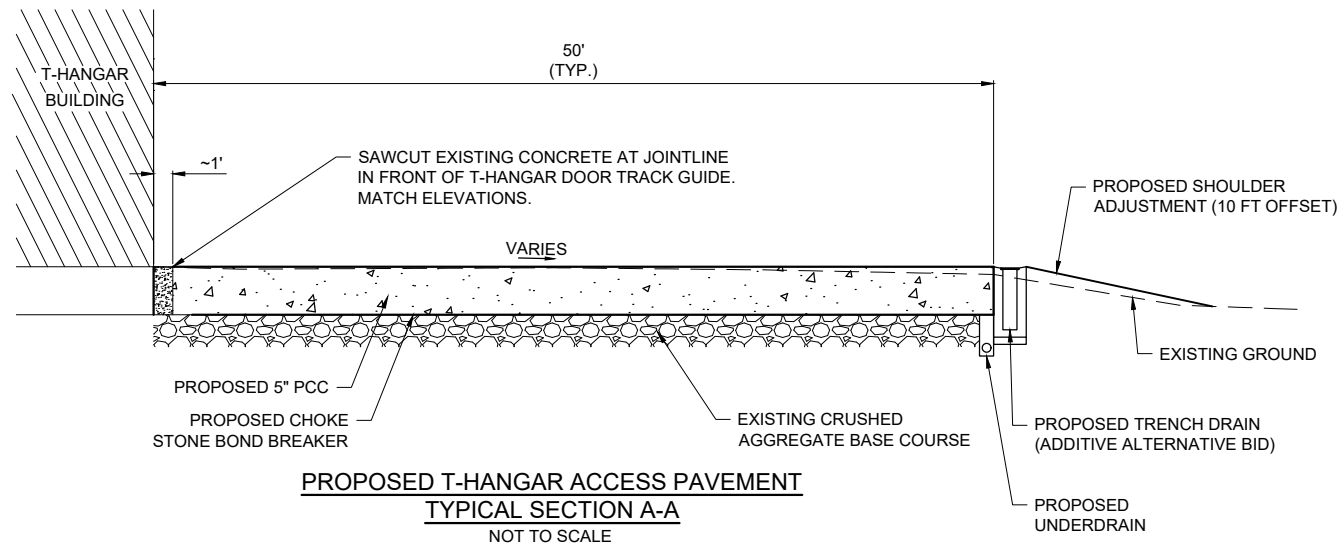
IDA No: OLY-5017
Contract No. OL018

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
CAD FILE: C-302-TYP.DWG
DESIGN BY: JRH 8/25/2023
DRAWN BY: JRH 8/25/2023
REVIEWED BY: BSS 8/25/2023

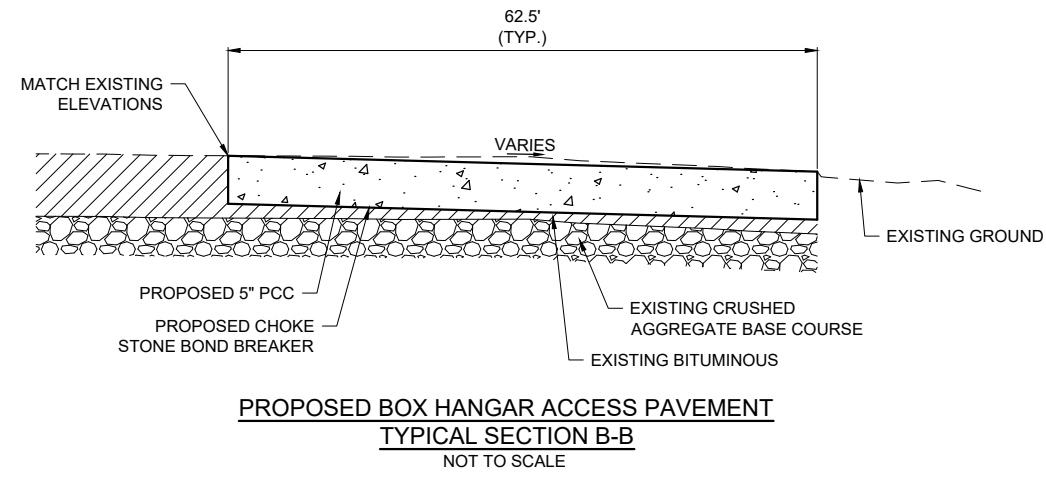
SHEET TITLE

**PROPOSED TYPICAL
SECTIONS**



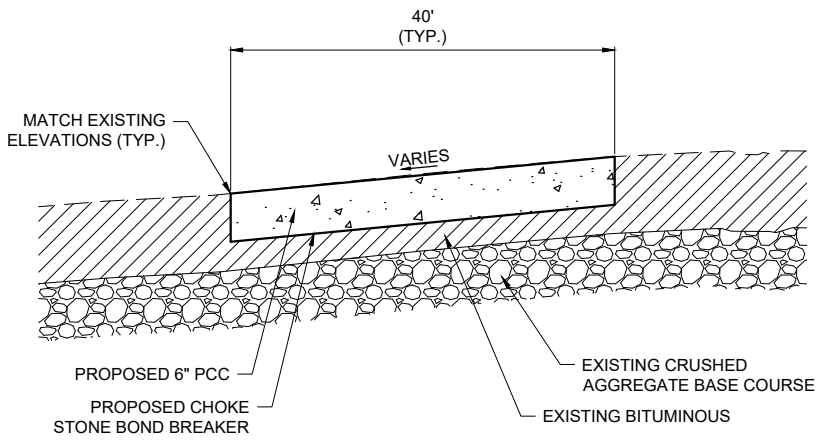
**PROPOSED T-HANGAR ACCESS PAVEMENT
TYPICAL SECTION A-A
NOT TO SCALE**

NOTE:
ADDITIONAL SOIL REMOVAL AND CRUSHED AGGREGATE MAY BE NEEDED TO ACCOMMODATE THE PROPOSED PAVEMENT GEOMETRY. SOIL REMOVAL WILL BE INCIDENTAL TO ITEM AR152480 - SHOULDER ADJUSTMENT. ADDITIONAL CRUSHED AGGREGATE TO REPLACE THE SOIL REMOVAL WILL BE PAID FOR UNDER ITEM AR209606 - CRUSHED AGG. BASE COURSE - 6".



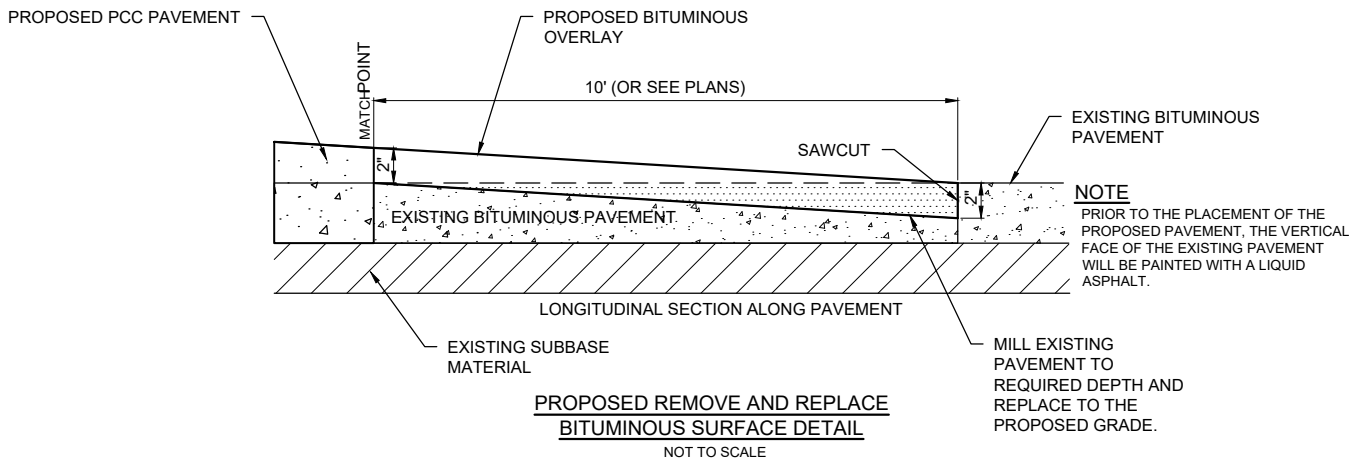
**PROPOSED BOX HANGAR ACCESS PAVEMENT
TYPICAL SECTION B-B
NOT TO SCALE**

NOTE:
ADDITIONAL SOIL REMOVAL AND CRUSHED AGGREGATE MAY BE NEEDED TO ACCOMMODATE THE PROPOSED PAVEMENT GEOMETRY. SOIL REMOVAL WILL BE INCIDENTAL TO ITEM AR152480 - SHOULDER ADJUSTMENT. ADDITIONAL CRUSHED AGGREGATE TO REPLACE THE SOIL REMOVAL WILL BE PAID FOR UNDER ITEM AR209606 - CRUSHED AGG. BASE COURSE - 6".



**PROPOSED FUEL FARM ACCESS PAVEMENT
TYPICAL SECTION C-C
NOT TO SCALE**

- TYPICAL SECTION NOTES**
- EXISTING PAVEMENT SECTION TO BE VARIABLE MILLED IN FRONT OF T-HANGAR IS APPROXIMATELY 3.5" BITUMINOUS ON 8" CRUSHED AGGREGATE.
 - EXISTING PAVEMENT SECTION TO BE VARIABLE MILLED IN FRONT OF THE LARGE BOX HANGARS IS APPROXIMATELY 6.5" BITUMINOUS ON 4" CRUSHED AGGREGATE.
 - EXISTING PAVEMENT SECTION TO BE VARIABLE MILLED IN FRONT OF FUEL FARM IS APPROXIMATELY 8" BITUMINOUS ON 6" CRUSHED AGGREGATE.
 - PRIOR TO THE PLACEMENT OF 501 PCC, A CHOKE STONE LAYER SHALL BE PLACED AS A BOND BREAKER.
 - MILLING OF EXISTING PAVEMENTS SHALL BE VARIABLE DEPTH TO ACCOMMODATE THE PROPOSED PCC PAVEMENT SECTION.



**PROPOSED REMOVE AND REPLACE
BITUMINOUS SURFACE DETAIL
NOT TO SCALE**

NOTE
PRIOR TO THE PLACEMENT OF THE PROPOSED PAVEMENT, THE VERTICAL FACE OF THE EXISTING PAVEMENT WILL BE PAINTED WITH A LIQUID ASPHALT.

REMOVE AND REPLACE BITUMINOUS SURFACE CONSTRUCTION NOTES

- THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH "BUTT JOINT CONSTRUCTION" AS STATED IN THE STANDARD SPECIFICATIONS, ADOPTED MARCH 22, 2023.
- THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED BITUMINOUS OVERLAY AT THE LOCATIONS AS SHOWN ON THE PLANS.
- THE PAVEMENT SURFACE WILL THEN BE MILLED TO A DEPTH OF 2" AT THE BUTT END AND WILL TAPER TO 0 IN. AT THE OPPOSITE END. THE PLANER MUST BE CAPABLE OF MILLING THE SURFACE TO THE DESIRED ELEVATIONS AND GRADES.
- ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.
- ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE CONTRACTOR AND ACCEPTED BY THE RESIDENT ENGINEER.
- ALL ASSOCIATED ITEMS SHALL BE PAID FOR UNDER ITEM AR401911 - REMOVE & REPLACE BIT. SURFACE

FOR BID

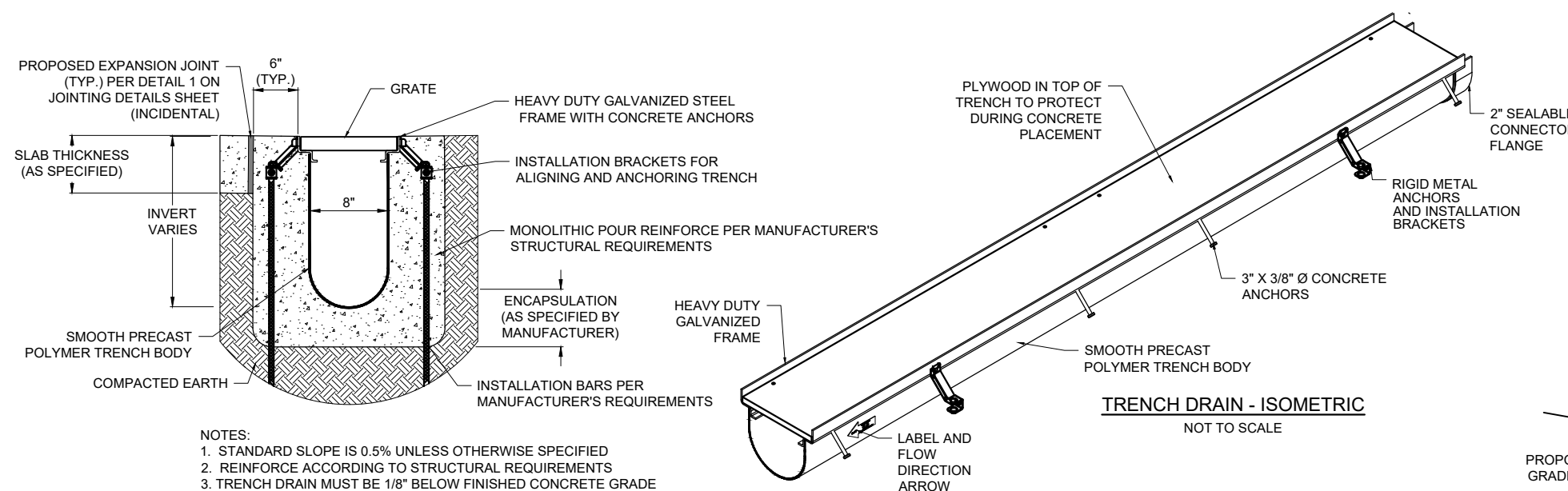


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		DES	DWN	REV

ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
CAD FILE: C-302-TYP.DWG
DESIGN BY: JRH 8/25/2023
DRAWN BY: JRH 8/25/2023
REVIEWED BY: BSS 8/25/2023

SHEET TITLE

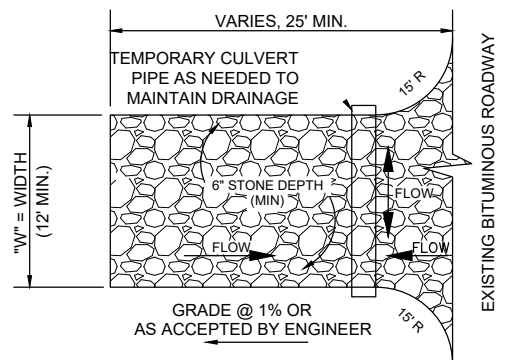
PROPOSED
DRAINAGE DETAILS



TRENCH DRAIN - TYPICAL SECTION
NOT TO SCALE

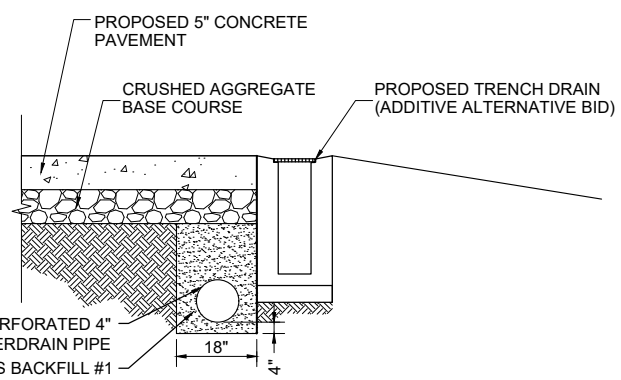
- NOTES:
- STANDARD SLOPE IS 0.5% UNLESS OTHERWISE SPECIFIED
 - REINFORCE ACCORDING TO STRUCTURAL REQUIREMENTS
 - TRENCH DRAIN MUST BE 1/8" BELOW FINISHED CONCRETE GRADE

- NOTES:
- FOR TRENCH DRAIN LOCATION AND INVERT ELEVATIONS, SEE CONSTRUCTION PLAN.
 - TRENCH DRAIN ACCESSORIES, TO INCLUDE BUT NOT LIMITED TO, CHANNEL FRAMES, COUPLERS, AND ADAPTERS ARE THE RESPONSIBILITY OF THE CONTRACTOR AND INCIDENTAL TO TRENCH DRAIN.



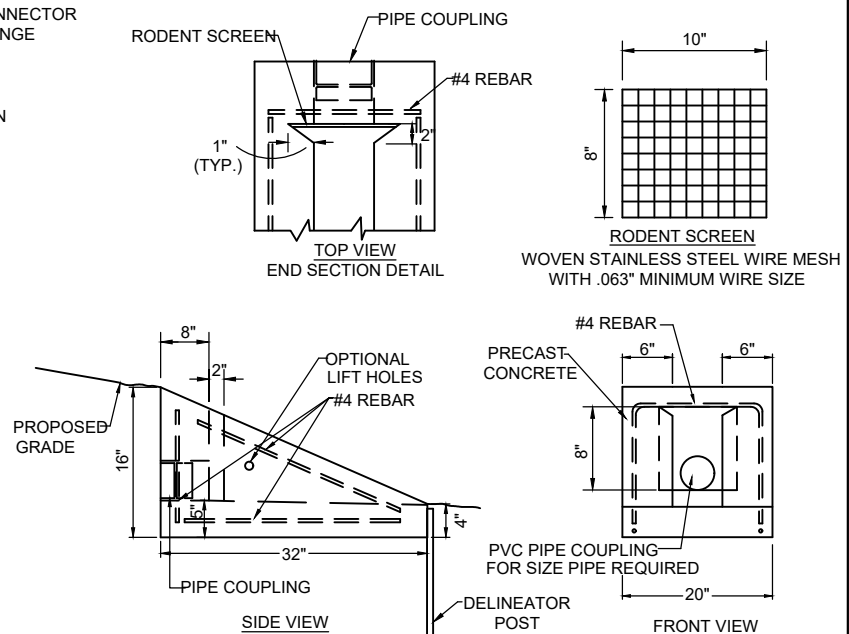
- STONE SIZE SHALL CONFORM TO IDOT CA (2" TO 3" DIA.) STONE SHALL HAVE GEOTEXTILE UNDERLAYMENT.
- CONTRACTOR SHALL PERIODICALLY PLACE 2" STONE TOP DRESSING AND WASH STONE AS REQUIRED BY OWNER/ENGINEER. CONTRACTOR SHALL ENSURE MUD IS NOT TRACKED ONTO PUBLIC ROADS AND SHALL IMMEDIATELY CLEAN ROADS IF TRACKING OCCURS. INCIDENTAL WORK NOT PAID FOR DIRECTLY.
- CONTRACTOR SHALL PROVIDE ADEQUATELY SIZED PIPE AND COVER FOR CONSTRUCTION TRAFFIC VEHICLES TO MAINTAIN PROPER DRAINAGE BELOW TEMPORARY CONSTRUCTION ENTRANCE DRIVE/STAGING AREA.
- CONSTRUCTION ENTRANCE DRIVE/STAGING AREA IS TO BE REMOVED AND THE SITE RESTORED TO PRE-CONSTRUCTION CONDITION AT THE COMPLETION OF THE PROJECT.

TEMPORARY CONSTRUCTION ENTRANCE DRIVE DETAIL
NOT TO SCALE

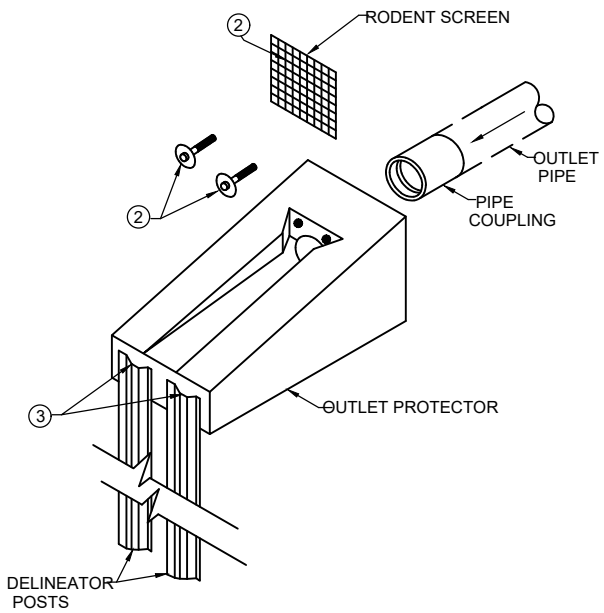


- UNDERDRAIN NOTES**
- THE CONTRACTOR SHALL INSTALL THE PROPOSED 4" P.E. TUBING UNDERDRAINS TO THE DEPTH AND GRADES SHOWN ON THE PLANS. THE UNDERDRAINS SHALL BE INSTALLED AFTER THE LIME SUBGRADE PROCESSING HAS BEEN COMPLETED.
 - THE 4" P.E. TUBING SHALL BE CAPPED AT THE ENDS WHICH DO NOT CONNECT INTO EXISTING STRUCTURES.
 - CONNECTING UNDERDRAINS TO EXISTING STRUCTURES SHALL BE INCLUDED IN THE COST OF THE UNDERDRAINS THEMSELVES, AND MAY INCLUDE CORING INTO THE EXISTING STRUCTURE WALL AND GROUTING THE UNDERDRAIN IN PLACE.
 - THE TRENCH SHALL BE BACKFILLED AND COMPACTED WITH POROUS BACKFILL NO. 1 MATERIAL. THE TRENCH LOCATED IN THE PROPOSED PAVEMENT AREAS WILL BE BACKFILLED AS SHOWN IN THE DETAIL ON THIS SHEET. THE TRENCH LOCATED IN TURF AREAS SHALL BE BACKFILLED UP TO WITHIN 1 FT. OF THE EXISTING GROUND ELEVATION. THE REMAINING 1 FT. OF TRENCH WILL BE BACKFILLED AND COMPACTED WITH EARTH MATERIAL.
 - POROUS BACKFILL SHALL CONFORM TO THE REQUIREMENTS FOR IDOT CA-14 OR IDOT CA-16 AND WILL BE CONSIDERED INCIDENTAL TO AR705504 4" PERFORATED UNDERDRAIN AND NO ADDITIONAL COMPENSATION ALLOWED. CONTRACTOR SHALL PLACE AND CONSOLIDATE THE POROUS BACKFILL TO THE SATISFACTION OF THE RESIDENT ENGINEER/TECHNICIAN.

TYPICAL UNDERDRAIN TRENCH
NOT TO SCALE

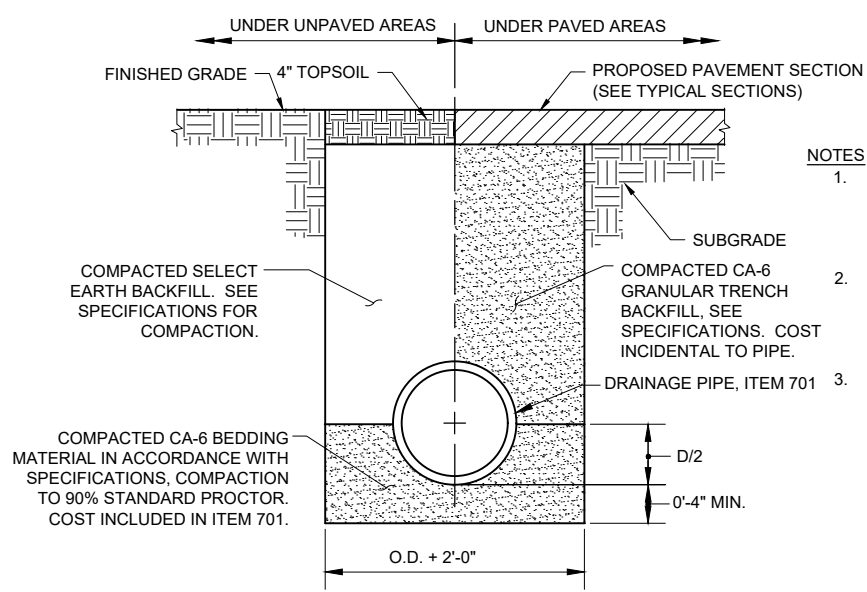


UNDERDRAIN END SECTION
NOT TO SCALE



- UNDERDRAIN END SECTION NOTES:**
- THE CONTRACTOR SHALL INSTALL THE PROPOSED UNDERDRAIN END SECTION AT THE LOCATION AND GRADE SHOWN ON THE PLANS. ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS.
 - THE RODENT SCREEN SHALL BE 1/3" SQUARE (3 OPENINGS PER INCH). IT SHALL BE FASTENED TO THE OUTLET PROTECTOR WITH TWO 1/4" BY 1" HEX HEAD LAG SCREWS WITH FLAT WASHERS AND ANCHORS. THE APPROXIMATE LOCATION OF THE ANCHOR HOLES SHALL BE AS SHOWN ON THE DETAILS.
 - THE DELINEATOR POSTS USED TO ANCHOR THE OUTLET PROTECTOR SHALL BE EMBEDDED A MINIMUM OF 2'. THE EXPOSED END OF THESE DELINEATOR POSTS SHALL NOT PROTRUDE ABOVE THE TOP EDGE OF THE DOWNSTREAM END OF THE OUTLET PROTECTOR.
 - THE PROPOSED UNDERDRAIN END SECTION WILL BE PAID FOR UNDER ITEM AR705620 UNDERDRAIN END SECTION.

UNDERDRAIN END SECTION ISOMETRIC DETAIL
NOT TO SCALE



PIPE TRENCH
NOT TO SCALE

- NOTES**
- UNSUITABLE MATERIAL ENCOUNTERED DURING PLACEMENT OF BEDDING SHALL BE REMOVED AND REPLACED.
 - WITHIN 3 FEET OF PAVED AREA, GRANULAR BACKFILL IS TO BE USED INSTEAD OF EARTH BACKFILL.
 - AT CONTRACTOR'S OPTION IDOT CONTROLLED LOW STRENGTH MATERIAL WITH A HIGH EARLY STRENGTH, "FLASH FILL", MAY BE USED INSTEAD OF GRANULAR TRENCH BACKFILL UNDER PAVEMENTS.

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FOR BID



DATE: 5/3/2024 LICENSE: 11/30/2025
SIGNED: 5/3/2024 EXPIRES: 11/30/2025

**REHABILITATE AIRCRAFT
HANGAR ACCESS
PAVEMENTS**

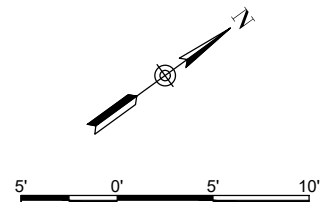
IDA No: OLY-5017
Contract No. OL018

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
CAD FILE: C-122-DEM.DWG
DESIGN BY: JRH 8/25/2023
DRAWN BY: NLD 8/25/2023
REVIEWED BY: BSS 8/25/2023

SHEET TITLE

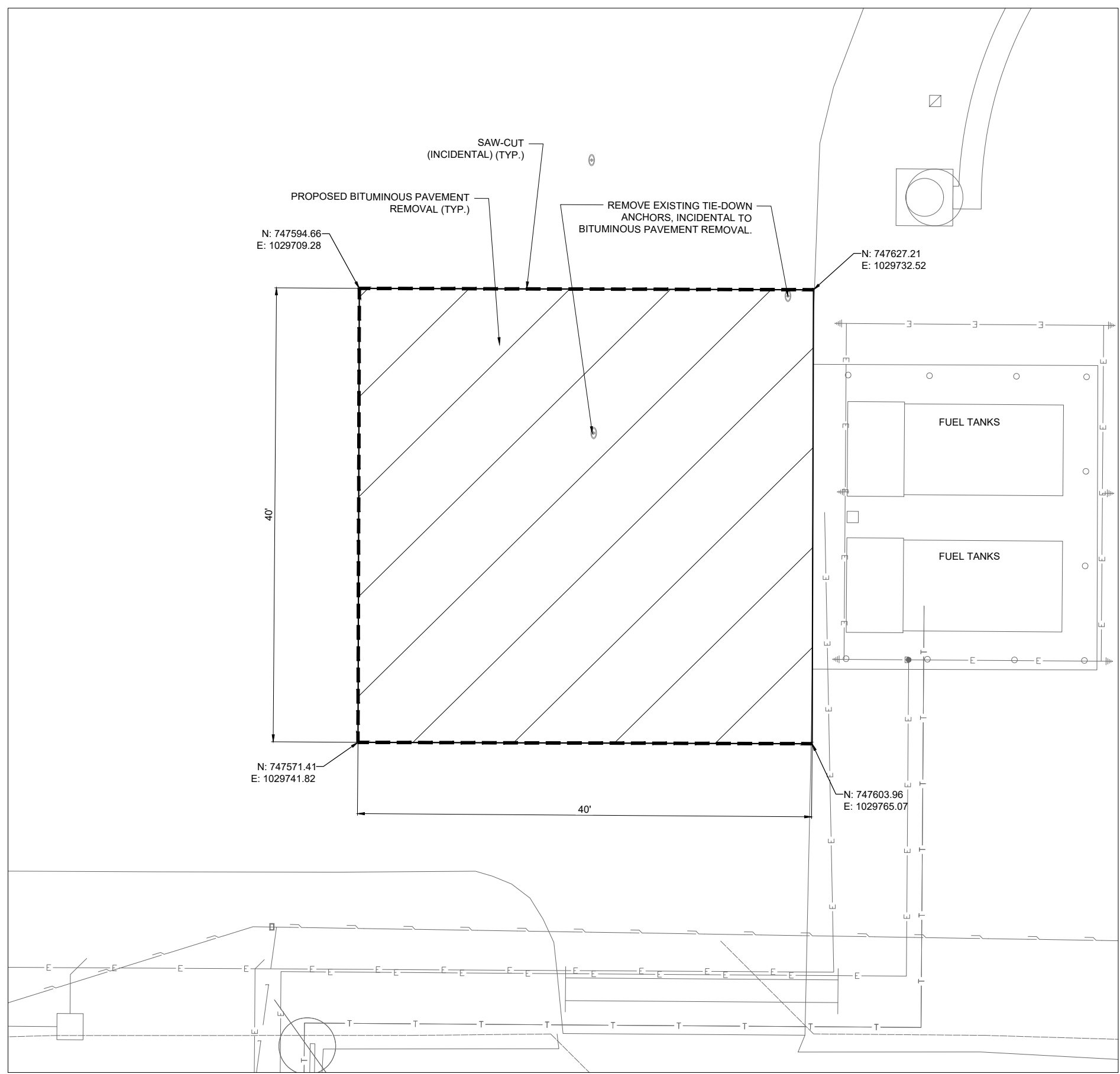
**PROPOSED
DEMOLITION PLAN -
SHEET 1**



- LEGEND**
- EXISTING IMPROVEMENTS
 - EXISTING BUILDINGS
 - PROPOSED BITUMINOUS PAVEMENT REMOVAL
 - PROPOSED SAWCUT (INCIDENTAL)
 - EXISTING STORM SEWER
 - EXISTING TELEPHONE
 - EXISTING ELECTRIC LINE
 - EXISTING BOLLARDS

GENERAL NOTES

1. EXISTING PAVEMENT SECTION TO BE MILLED IN FRONT OF T-HANGAR IS APPROXIMATELY 3.5" BITUMINOUS ON 8" CRUSHED AGGREGATE.
2. EXISTING PAVEMENT SECTION TO BE MILLED IN FRONT OF THE LARGE BOX HANGARS IS APPROXIMATELY 6.5" BITUMINOUS ON 4" CRUSHED AGGREGATE.
3. EXISTING PAVEMENT SECTION TO BE MILLED IN FRONT OF FUEL FARM IS APPROXIMATELY 8" BITUMINOUS ON 6" CRUSHED AGGREGATE.
4. MILLING EQUIPMENT SHALL BE CAPABLE OF ACCURATELY ESTABLISHING THE PROPOSED GRADES TO ACCOMMODATE THE PROPOSED PCC PAVEMENT SECTIONS.
5. ALL PROPOSED MILLINGS ARE TO BE DISPOSED OF OFFSITE.
6. ANY EXCESS DIRT SHALL BE TAKEN OFF-SITE OR PLACED ON AIRPORT PROPERTY AT A LOCATION TO BE COORDINATED WITH THE OWNER.
7. CONTRACTOR SHALL USE EXTREME CAUTION WHEN EXCAVATING AND COMPACTING SUBGRADE OR SUBBASE AROUND EXISTING STORM SEWER PIPES AND INLETS.
8. CONTRACTOR SHALL PROTECT EXISTING HANGARS AND UTILITIES AT ALL TIMES DURING CONSTRUCTION ACTIVITIES.
9. THE RESIDENT ENGINEER WILL MARK THE PROPOSED SAW LINE IN THE FIELD. ANY BITUMINOUS PAVEMENT CUT FROM THE SAWING OPERATIONS WILL BE COLLECTED AND DISPOSED OF OFF THE AIRPORT SITE. THE SAWING AND DISPOSAL OF ANY WASTE MATERIAL WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



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FOR BID



DATE SIGNED: 5/3/2024 LICENSE EXPIRES: 11/30/2025

**REHABILITATE AIRCRAFT
HANGAR ACCESS
PAVEMENTS**

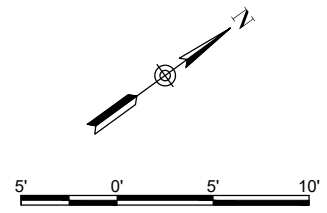
IDA No: OLY-5017
Contract No. OL018

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
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REVIEWED BY: BSS 9/22/2023

SHEET TITLE

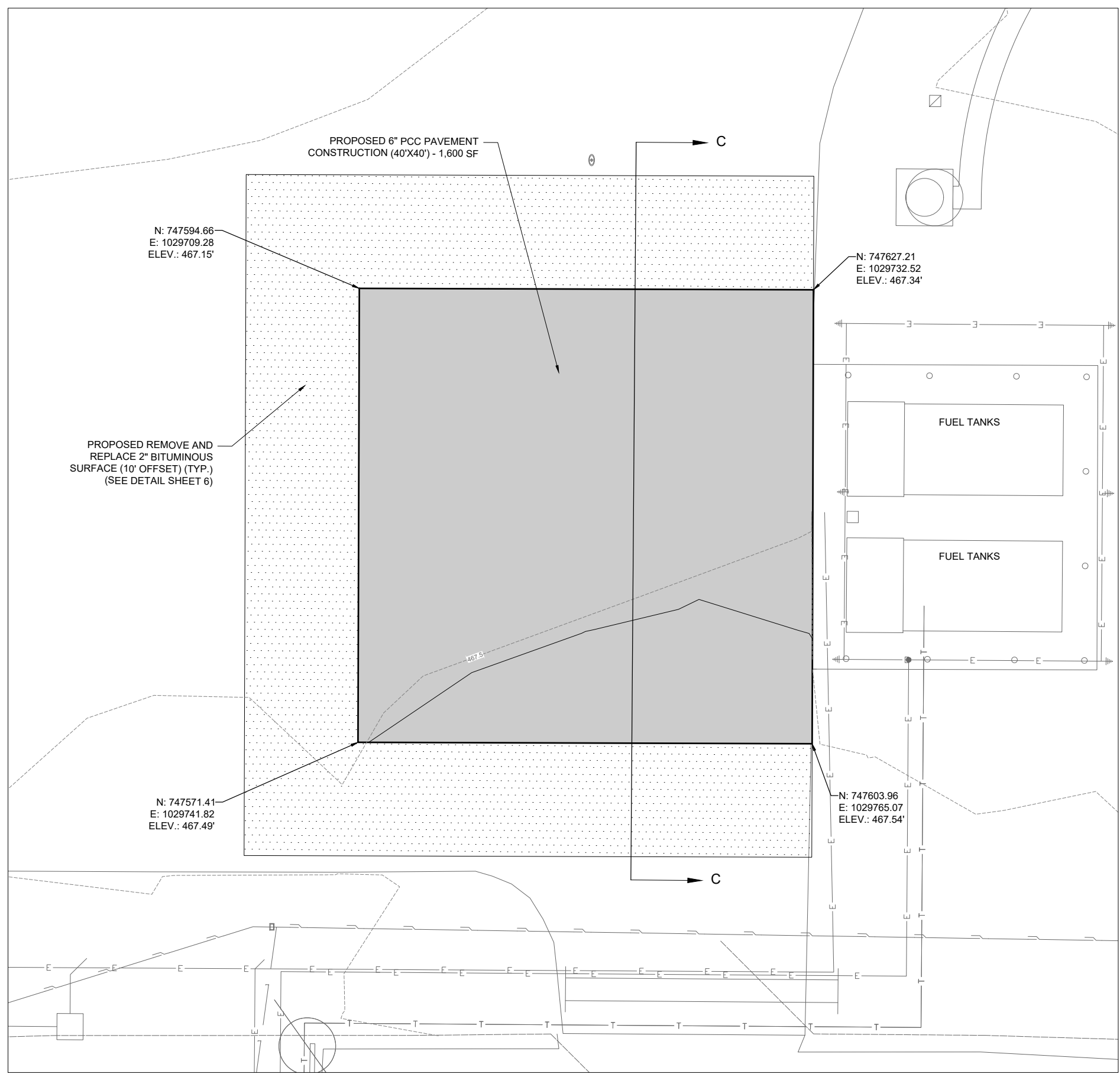
**PROPOSED
CONSTRUCTION
PLAN - SHEET 1**



- LEGEND**
- EXISTING IMPROVEMENTS
 - EXISTING BUILDINGS
 - PROPOSED PCC PAVEMENT
 - PROPOSED 2" BITUMINOUS PAVEMENT MILLING
 - EXISTING STORM SEWER
 - EXISTING TELEPHONE
 - EXISTING ELECTRIC LINE

GENERAL NOTES

1. FOLLOWING THE PROJECT AWARD, THE ENGINEER CAN PROVIDE THE RELEVANT AUTOCAD AND CIVIL 3D SURFACE MODEL FILES TO THE AWARDED CONTRACTOR UPON REQUEST TO ASSIST WITH CONSTRUCTION LAYOUT.
2. ANY EXCESS DIRT SHALL BE TAKEN OFF-SITE OR PLACED ON AIRPORT PROPERTY AT A LOCATION TO BE COORDINATED WITH THE OWNER.
3. CONTRACTOR SHALL USE EXTREME CAUTION WHEN EXCAVATING AND COMPACTING SUBGRADE OR SUBBASE AROUND EXISTING STORM SEWER PIPES AND INLETS.
4. CONTRACTOR SHALL PROTECT EXISTING HANGARS AND UTILITIES AT ALL TIMES DURING CONSTRUCTION ACTIVITIES.
5. THE RESIDENT ENGINEER WILL MARK THE PROPOSED SAW LINE IN THE FIELD. ANY BITUMINOUS PAVEMENT CUT FROM THE SAWING OPERATIONS WILL BE COLLECTED AND DISPOSED OF OFF THE AIRPORT SITE. THE SAWING AND DISPOSAL OF ANY WASTE MATERIAL WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

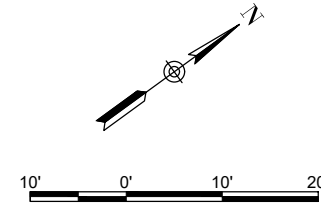


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FOR BID

GENERAL NOTES

1. FOLLOWING THE PROJECT AWARD, THE ENGINEER CAN PROVIDE THE RELEVANT AUTOCAD AND CIVIL 3D SURFACE MODEL FILES TO THE AWARDED CONTRACTOR UPON REQUEST TO ASSIST WITH CONSTRUCTION LAYOUT.
2. ANY EXCESS DIRT SHALL BE TAKEN OFF-SITE OR PLACED ON AIRPORT PROPERTY AT A LOCATION TO BE COORDINATED WITH THE OWNER.
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5. THE RESIDENT ENGINEER WILL MARK THE PROPOSED SAW LINE IN THE FIELD. ANY BITUMINOUS PAVEMENT CUT FROM THE SAWING OPERATIONS WILL BE COLLECTED AND DISPOSED OF OFF THE AIRPORT SITE. THE SAWING AND DISPOSAL OF ANY WASTE MATERIAL WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED
6. ADDITIONAL SOIL REMOVAL TO ACCOMMODATE THE PROPOSED PAVEMENT GEOMETRY SHALL BE INCIDENTAL TO ITEM AR152480 - SHOULDER ADJUSTMENT.
7. ANY ADDITIONAL AGGREGATE TO REPLACE THE SOIL REMOVAL AND ACCOMMODATE THE PROPOSED PAVEMENT GEOMETRY SHALL BE PAID FOR UNDER ITEM AR209606 - CRUSHED AGGREGATE BASE COURSE - 6". AGGREGATE SHALL BE COMPACTED TO THE SATISFACTION OF RESIDENT ENGINEER/TECHNICIAN.



LEGEND

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED PCC PAVEMENT
- PROPOSED 2" BITUMINOUS PAVEMENT MILLING
- PROPOSED UNDERDRAIN
- EXISTING STORM SEWER



Offices Nationwide
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1525 S. 6th Street
Springfield, IL 62703
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fax: 217-788-2503

Illinois Licensed
Professional Service Corporation
#184-001084

OLNEY-NOBLE AIRPORT
AUTHORITY

OLNEY-NOBLE
AIRPORT (OLY)

2632 E. IL 250
NOBLE, RICHLAND COUNTY,
ILLINOIS 62868



DATE: 5/3/2024 LICENSE: 11/30/2025
SIGNED: 5/3/2024 EXPIRES: 11/30/2025

REHABILITATE AIRCRAFT
HANGAR ACCESS
PAVEMENTS

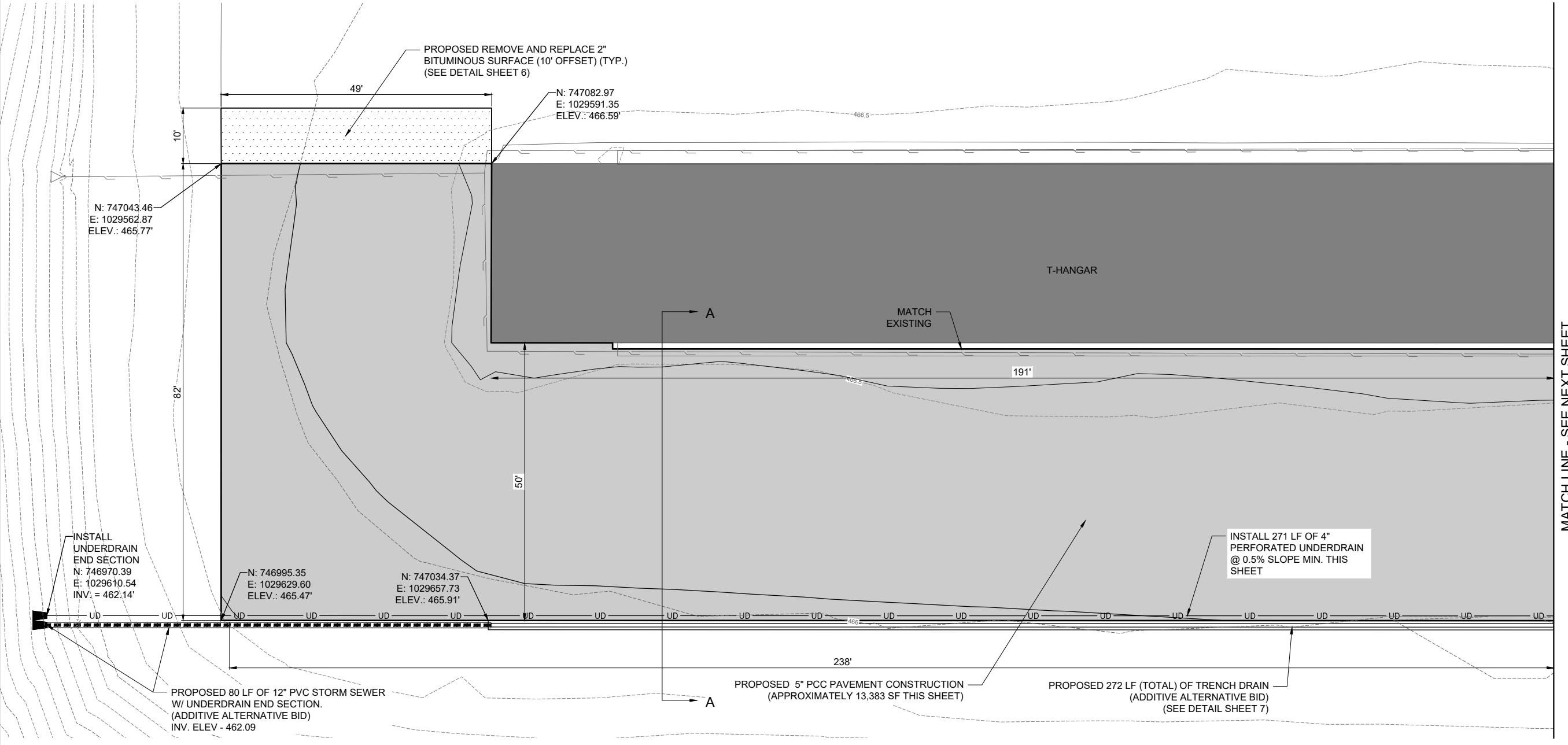
IDA No: OLY-5017
Contract No. OL018

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
CAD FILE: C-121-CON.DWG
DESIGN BY: JRH 9/19/2023
DRAWN BY: NLD 9/19/2023
REVIEWED BY: BSS 9/22/2023

SHEET TITLE

PROPOSED
CONSTRUCTION
PLAN - SHEET 2



MATCH LINE - SEE NEXT SHEET

FOR BID

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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MAY 3, 2024

PROJECT NO: 22A0008D

CAD FILE: C-161-JNT.DWG

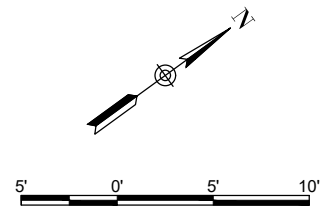
DESIGN BY: JRH 8/25/2023

DRAWN BY: NLD 8/25/2023

REVIEWED BY: BSS 8/25/2023

SHEET TITLE

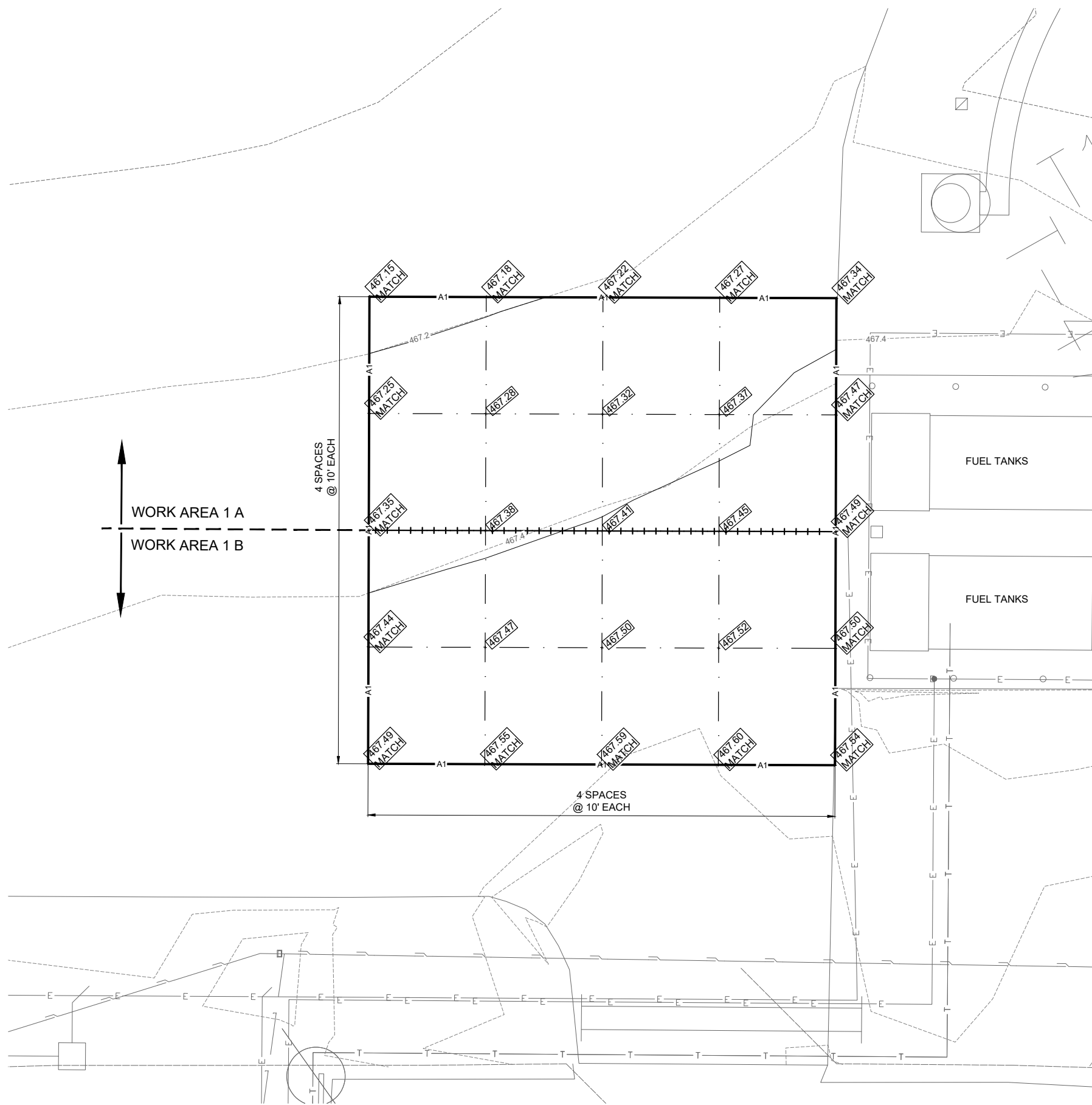
PROPOSED JOINTING
PLAN - SHEET 1



LEGEND

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED REINFORCED PANEL
- TYPE A1, MODIFIED1, MODIFIED ISOLATION JOINT
- TYPE B, CONTRACTION JOINT
- TYPE D, CONTRACTION JOINT
- TYPE E, CONSTRUCTION JOINT
- PROPOSED SPOT ELEVATION

- NOTES:**
- CONTRACTOR MAY PROPOSE ALTERNATIVE PAVING/JOINTING PLAN FOR REVIEW AND APPROVAL FOLLOWING AWARD.
 - CONTRACTOR SHALL VERIFY EXISTING ELEVATIONS AT "MATCH" LOCATIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER/TECHNICIAN IMMEDIATELY TO DETERMINE IF ADJUSTMENTS ARE NECESSARY TO PROPOSED GRADES.



MAY 03, 2024 10:19 AM SCHUB01446 1:22JOBS22A0008D\CAD\AIRPORT\SHHEETC-161-JNT.DWG

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DATE SIGNED: 5/3/2024 LICENSE EXPIRES: 11/30/2025

**REHABILITATE AIRCRAFT
HANGAR ACCESS
PAVEMENTS**

IDA No: OLY-5017
Contract No. OL018

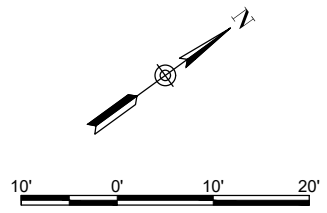
NO.	DATE	DESCRIPTION		
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REVIEWED BY: BSS 8/25/2023

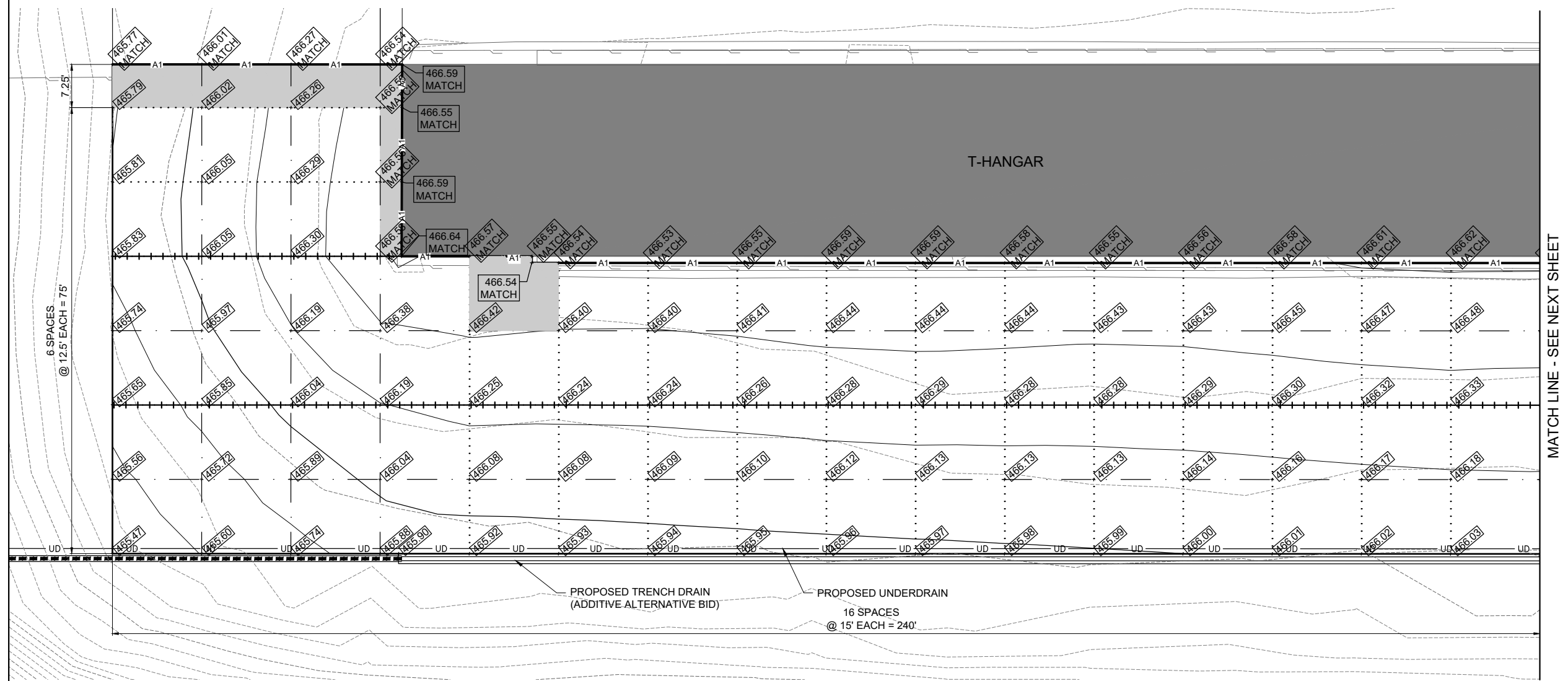
SHEET TITLE

**PROPOSED JOINTING
PLAN - SHEET 2**

- NOTES:**
- CONTRACTOR MAY PROPOSE ALTERNATIVE PAVING/JOINTING PLAN FOR REVIEW AND APPROVAL FOLLOWING AWARD.
 - CONTRACTOR SHALL VERIFY EXISTING ELEVATIONS AT "MATCH" LOCATIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER/TECHNICIAN IMMEDIATELY TO DETERMINE IF ADJUSTMENTS ARE NECESSARY TO PROPOSED GRADES.



- LEGEND**
- EXISTING IMPROVEMENTS
 - EXISTING BUILDINGS
 - PROPOSED REINFORCED PANEL
 - TYPE A1, MODIFIED ISOLATION JOINT
 - TYPE B, CONTRACTION JOINT
 - TYPE D, CONTRACTION JOINT
 - TYPE E, CONSTRUCTION JOINT
 - PROPOSED SPOT ELEVATION



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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

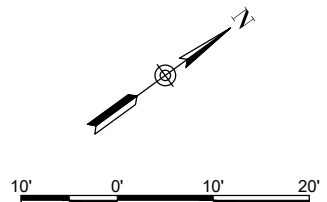
ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
CAD FILE: C-161-JNT.DWG
DESIGN BY: JRH 8/25/2023
DRAWN BY: NLD 8/25/2023
REVIEWED BY: BSS 8/25/2023

SHEET TITLE

PROPOSED JOINTING
PLAN - SHEET 3

NOTES:

1. CONTRACTOR MAY PROPOSE ALTERNATIVE PAVING/JOINTING PLAN FOR REVIEW AND APPROVAL FOLLOWING AWARD.
2. CONTRACTOR SHALL VERIFY EXISTING ELEVATIONS AT "MATCH" LOCATIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER/TECHNICIAN IMMEDIATELY TO DETERMINE IF ADJUSTMENTS ARE NECESSARY TO PROPOSED GRADES.



LEGEND

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED REINFORCED PANEL
- A1 TYPE A1, MODIFIED ISOLATION JOINT
- TYPE B, CONTRACTION JOINT
- TYPE D, CONTRACTION JOINT
- |||||||||| TYPE E, CONSTRUCTION JOINT
- 541.0 PROPOSED SPOT ELEVATION



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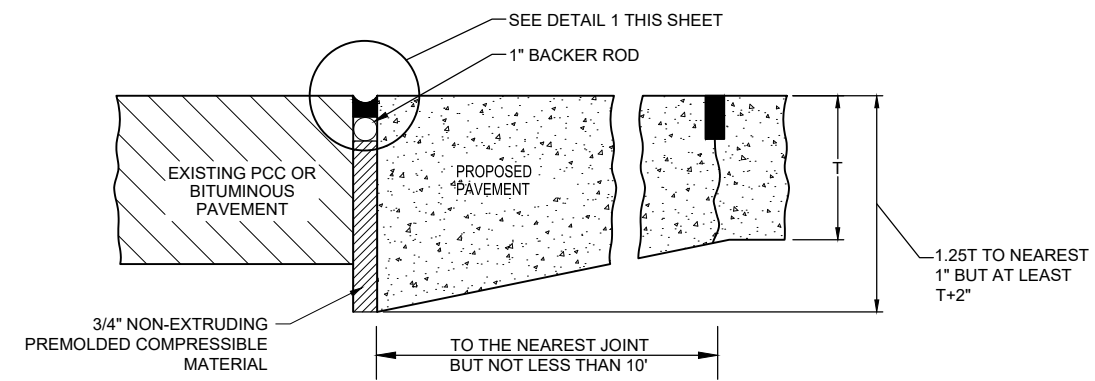


NO.	DATE	DESCRIPTION		
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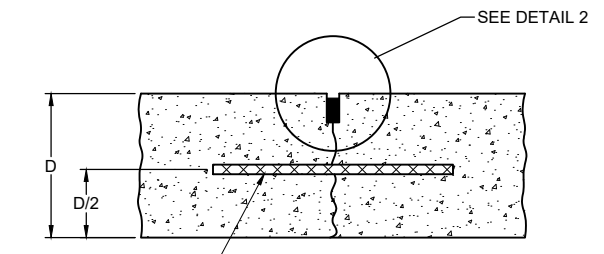
ISSUE: MAY 3, 2024
PROJECT NO: 22A0008D
CAD FILE: C-561-JNT.DWG
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DRAWN BY: NLD 8/25/2023
REVIEWED BY: BSS 8/25/2023

SHEET TITLE

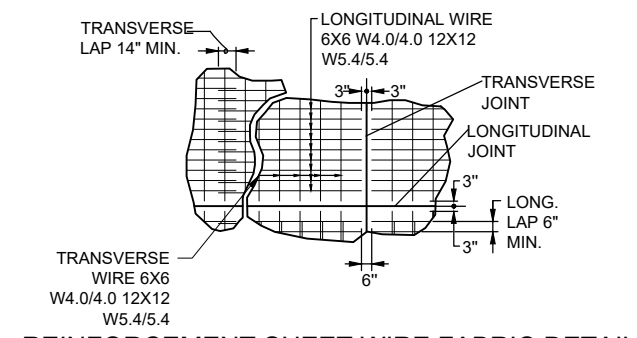
PROPOSED JOINT
DETAILS



TYPE-A1 - MODIFIED THICKENED EDGE ISOLATION JOINT

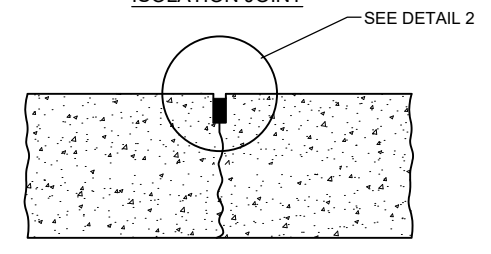


TYPE B - HINGED CONTRACTION JOINT

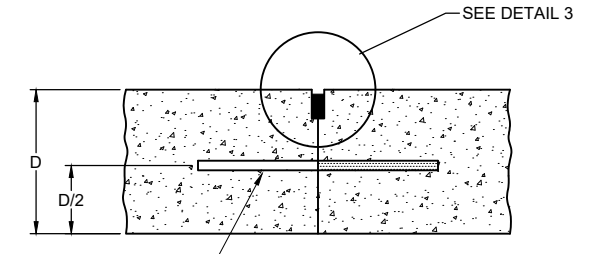


REINFORCEMENT SHEET WIRE FABRIC DETAIL
NOT TO SCALE

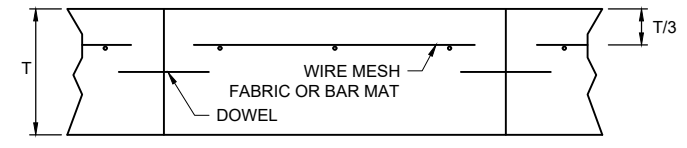
NOTE:
DIFFERENT WIRE MESH SIZES/CONFIGURATIONS ARE PERMITTED TO PROVIDE A MINIMUM OF 0.05% OF THE PANEL CROSS-SECTIONAL AREA IN BOTH DIRECTIONS.



TYPE D DUMMY CONTRACTION JOINT



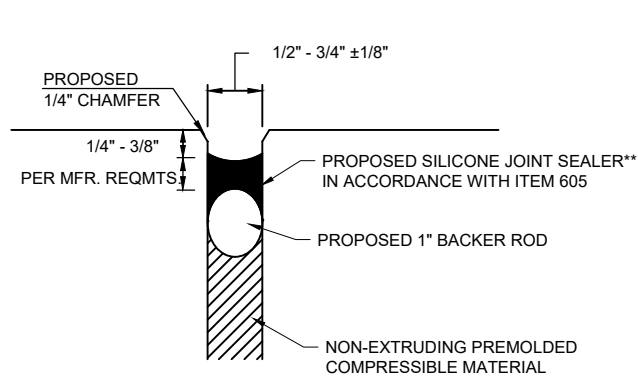
TYPE E DOWELED CONSTRUCTION JOINT



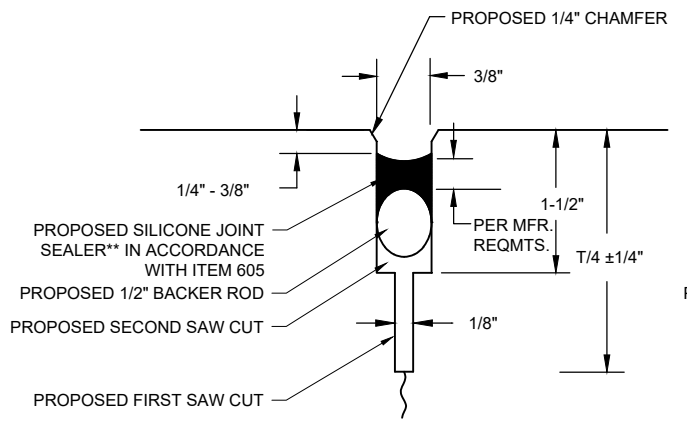
PAVEMENT REINFORCING DETAIL
NOT TO SCALE

REINFORCING NOTES:

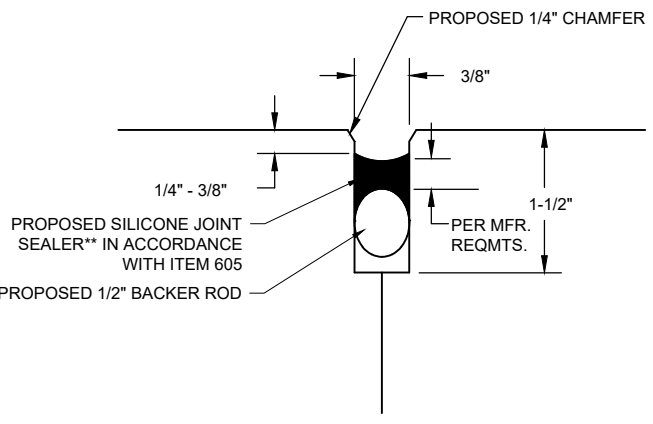
1. END LAPS SHALL BE A MINIMUM OF 12", BUT NOT LESS THAN 30 TIMES THE DIAMETER OF THE LONGITUDINAL WIRE OR BAR.
2. SIDE LAPS SHALL BE A MINIMUM OF 6", BUT NOT LESS THAN 20 TIMES THE DIAMETER OF TRANSVERSE WIRE OR BAR.
3. END AND SIDE CLEARANCES SHALL BE A MAXIMUM OF 6" AND A MINIMUM OF 2".
4. LONGITUDINAL MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 12" APART.
5. TRANSVERSE MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 24" APART.
6. REINFORCING SHALL CONSIST OF WELDED STEEL WIRE FABRIC CONFORMING TO THE REQUIREMENTS OF ASTM A 185 OR BAR MATS CONFORMING TO THE REQUIREMENTS OF ASTM A 184 OR A 704.
7. PAVEMENT REINFORCING SHALL BE INCIDENTAL TO ITEM 501 PCC PAVEMENT.



DETAIL 1
NOT TO SCALE



DETAIL 2
NOT TO SCALE



DETAIL 3
NOT TO SCALE

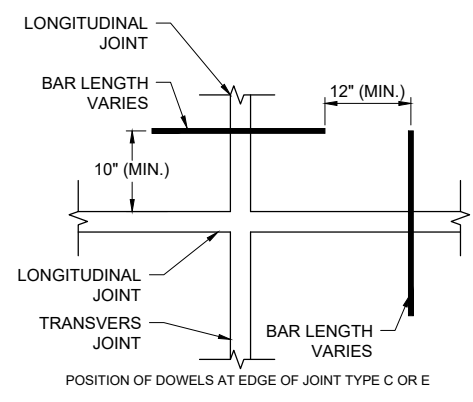
JOINT SEALING DETAILS
NOT TO SCALE

** PROPOSED JOINT SEALANT SHALL BE DOW CORNING 888 NON-SAG SILICONE JOINT SEALER, PECORA 301, OR APPROVED EQUAL.

JOINTING NOTES

1. ALL JOINT EDGES SHALL BE SAWCUT TO PRODUCE THE 1/4" CHAMFER.
2. ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO INSTALLATION.
4. DOWEL BARS FOR 5 IN. AND 6 IN. THICK PCC PAVEMENT SHALL BE 3/4 IN DIAMETER, 18 IN. LONG AT 12 IN. SPACING.
5. ALL TIE BARS SHALL BE HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.
6. ALL TIE BARS FOR 5 IN. AND 6 IN. PCC PAVEMENT SHALL BE 20 IN. LONG NO. 4 BARS SPACED AT 36 IN. ON CENTER.
7. DOWELS IN TRANSVERSE CONTRACTION AND LONGITUDINAL CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.

8. ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE ±1/8" FOR CONSTRUCTION JOINTS AND ±1/4" FOR CONTRACTION JOINTS.
9. THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED DOWELS IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE DIVISION OF AERONAUTICS PRIOR TO USE.
10. THE COST OF ALL DOWEL BARS, TIE-BARS, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE PCC PAVEMENT.
11. WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL PROTECT THE TRANSVERSE JOINTS.
12. JOINT SEALANT SHALL BE AS SPECIFIED IN THE STANDARD SPECIFICATIONS, ITEM 605.
13. CURING COMPOUND SHALL BE AS SPECIFIED IN THE STANDARD SPECIFICATIONS, ITEM 501-2.9, AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE RESIDENT ENGINEER/TECHNICIAN.
14. ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.



DOWEL PLAN VIEW

DOWEL NOTES:

1. DOWEL BARS WITHIN THE STANDARD PCC PAVEMENT (AR501505 & AR501506) SHALL BE STEEL DOWEL BARS CONFORMING TO THE PROJECT SPECIFICATION ITEM 501.
2. CARE SHALL BE TAKEN BY THE CONTRACTOR TO ENSURE THAT THE DOWEL BARS AND BASKET ASSEMBLIES IN CONTRACTION JOINTS ARE ADEQUATELY SECURED AND WILL FLOATING DURING THE CONCRETE VIBRATION PROCESS.
3. ELIMINATE DOWEL, OR TIE BARS, FROM LONGITUDINAL JOINT AS NECESSARY TO MAINTAIN 12 INCH FROM END OF TRANSVERSE DOWEL BARS.

FOR BID



DATE SIGNED: 5/3/2024 LICENSE EXPIRES: 11/30/2025

REHABILITATE AIRCRAFT
HANGAR ACCESS
PAVEMENTS

IDA No: OLY-5017
Contract No. OL018

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MAY 3, 2024

PROJECT NO: 22A0008D

CAD FILE: C-181-SWP.DWG

DESIGN BY: JRH 8/25/2023

DRAWN BY: NLD 8/25/2023

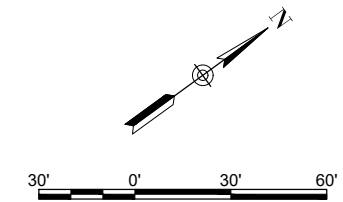
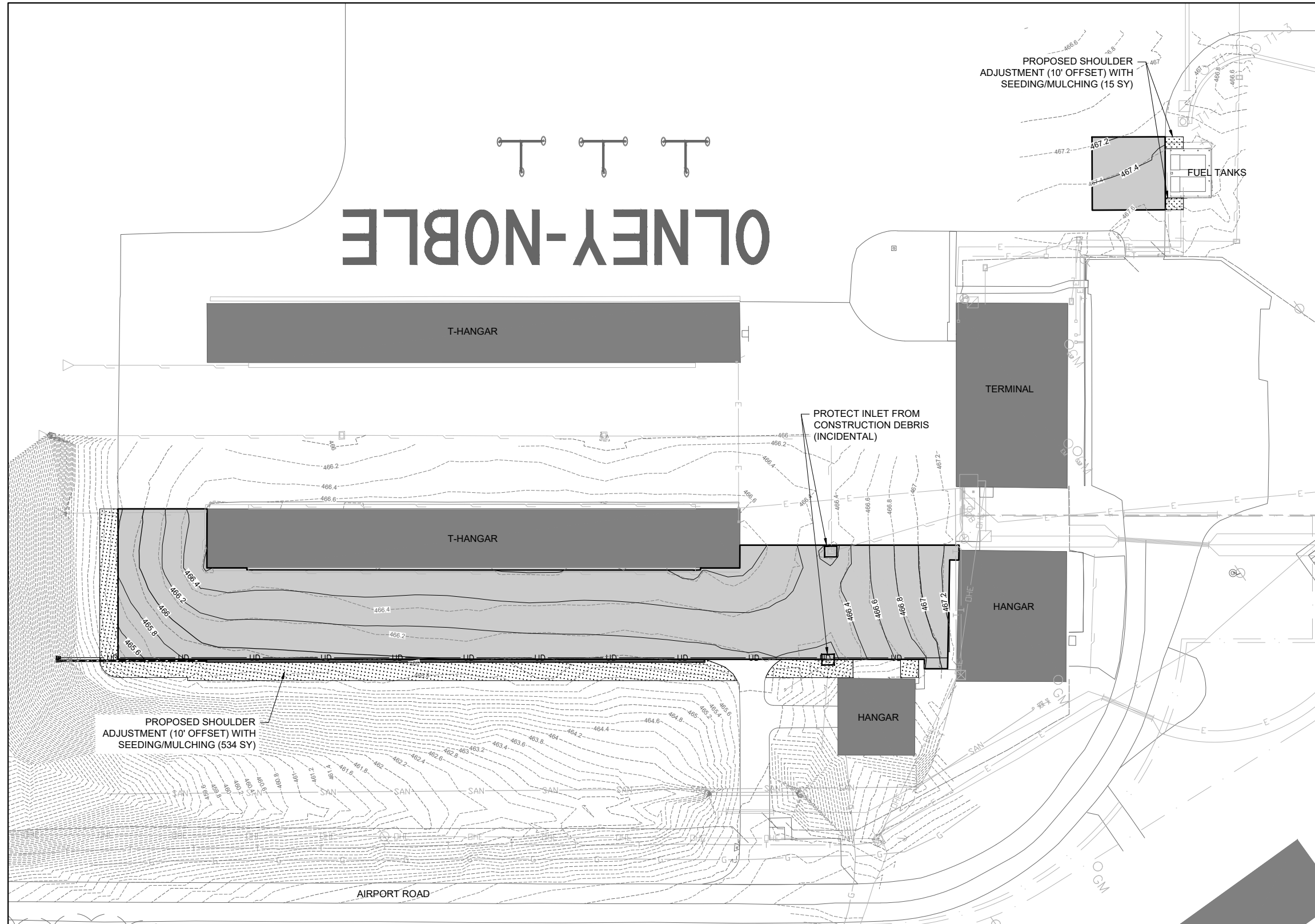
REVIEWED BY: BSS 8/25/2023

SHEET TITLE

PROPOSED
STORMWATER
POLLUTION
PREVENTION PLAN

FOR BID

OLNEY-NOBLE



LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- EXISTING FENCE
- PROPOSED SHOULDER ADJUSTMENT/SEEDING/MULCHING
- 541 - EXISTING CONTOURS
- 541 - PROPOSED CONTOURS
- PROPOSED FENCE
- PROPOSED UNDERDRAIN

STORM WATER POLLUTION PREVENTION NOTES

1. THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.
2. THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHERE EVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.
3. POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.
4. THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.