

60A25

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------|--------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 1 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS**

**PROPOSED
HIGHWAY PLANS**

**FAP 577: ILL. RTE. 171 (ARCHER AVE.)
AT 131ST ST. AND ASHBURY DR.**

**SECTION: 3114 N-1
CHANNELIZATION AND
TRAFFIC SIGNAL INSTALLATION**

**PROJECT NO.: HSIP-0577(218)
COOK COUNTY
C-91-008-06**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

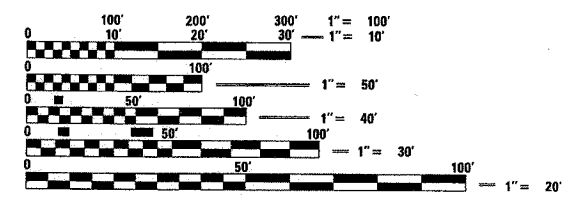
PROJECT IS LOCATED IN
THE VILLAGE OF LEMONT IN COOK COUNTY

TRAFFIC DATA

2020 ADT = 15500
2016 ADT = 14587
2003 ADT = 11392
POSTED SPEED LIMIT = 50 MPH



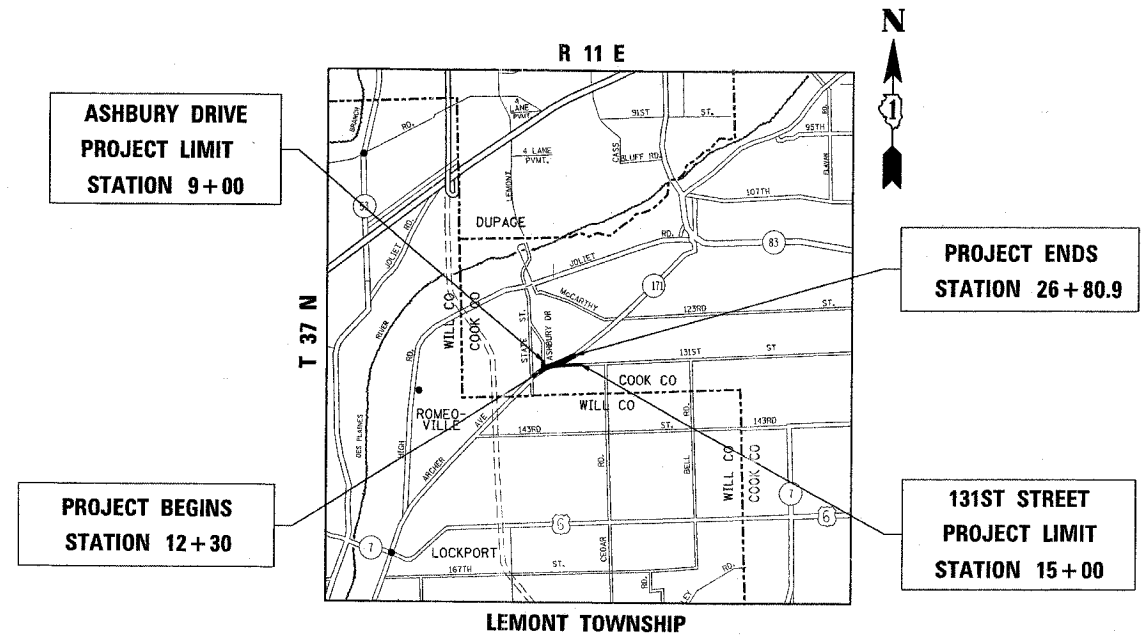
DISTRICT ONE - DESIGN AND PLAN PREPERATION ENGINEER - KEN ENG/J. P. CHANG (847) 705-4432



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 60A25



GROSS & NET LENGTH OF PROJECT = 1450.9 FEET = 0.27 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED April 25 20 07

Diane O'Keefe / KV
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 11, 20 07
Eric E. Haral / ED
INTERIM ENGINEER OF DESIGN AND ENVIRONMENT

May 11, 20 07
Milton R. See / P. RD
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------|----------|------------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 2 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 60A25

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| 50 | STEEL PLATE BEAM GUARDRAIL ADJACENT TO CURB AND GUTTER AND STABILIZATION AT TBT TY 1 SPL. |
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| SHEET NO. | DESCRIPTION: |
|-----------|---|
| 000001-04 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
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| 424001-04 | CURB RAMPS FOR SIDEWALKS |
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| 482001-01 | BITUMINOUS SHOULDER DETAIL - ADJACENT TO FLEXIBLE PAVEMENT |
| 542301-01 | PRECAST REINFORCED CONCRETE FLARED END SECTION |
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| 604001-02 | FRAME AND LIDS TYPE 1 |
| 604086-01 | FRAME AND GRATE TYPE 23 |
| 606001-03 | CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER |
| 630001-07 | STEEL PLATE BEAM GUARDRAIL |
| 606301-03 | PC CONCRETE ISLANDS AND MEDIANS |
| 630201-04 | PCC/BITUMINOUS STABILIZATION AT STEEL PLATE BEAM GUARDRAIL |
| 630301-04 | SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS |
| 635006-02 | REFLECTOR AND TERMINAL MARKER PLACEMENT |
| 635011-01 | REFLECTOR AND MOUNTING DETAILS |
| 701201-02 | LANE CLOSURE, 2L, 2W, DAY ONLY FOR SPEED > 45 MPH |
| 701306-01 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY FOR SPEEDS > 45 MPH |
| 701326-02 | LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING FOR SPEEDS > 45 MPH |
| 701501-03 | URBAN LANE CLOSURE 2L, 2W UNDIVIDED |
| 702001-06 | TRAFFIC CONTROL DEVICES |
| 704001-03 | TEMPORARY CONCRETE BARRIER |
| 780001-01 | TYPICAL PAVEMENT MARKINGS |

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF LEMONT

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT

3 METER (10 FEET) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 40 MM (1 1/2 INCHES) WHERE THE SPEED LIMIT IS 80 KM/H (45 MPH) OR LESS AND 25 MM (1 INCH) WHERE THE SPEED LIMIT IS GREATER THAN 80 KM/H (45 MPH). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 75 MM (3 INCHES) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING

THE RESIDENT ENGINEER SHALL CONTACT Ms. PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER AT (708) 597-9800, A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL CONTACT THE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE RESIDENT ENGINEER SHALL CONTACT MR. STEVEN HOOGHKIRK, BUREAU OF MAINTENANCE SUPPORT SECTION AT (847) 705-4177 FOR AVAILABILITY OF TEMPORARY CONCRETE BARRIER, STATE OWNED. IF TEMPORARY CONCRETE BARRIER, STATE OWNED, IS AVAILABLE, IT SHALL BE UTILIZED AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL LOAD THE TEMPORARY CONCRETE BARRIER FROM THE STATE MAINTENANCE YARD, TRANSPORT, UNLOAD AND PLACE THE TEMPORARY CONCRETE BARRIER IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND THE DETAILS SHOWN IN THE PLAN. AT THE CONCLUSION OF WORK, REMOVE, TRANSPORT AND UNLOAD THE BARRIER UNITS AT THE SPECIFIED STATE MAINTENANCE YARD AS DIRECTED BY THE ENGINEER. IF TEMPORARY CONCRETE BARRIER, STATE OWNED, IS NOT AVAILABLE, THE CONTRACTOR SHALL PROVIDE TEMPORARY CONCRETE BARRIER. THE CONTRACTOR SHALL FURNISH AND INSTALL TEMPORARY CONCRETE BARRIER TERMINAL SECTIONS. AT THE COMPLETION OF THE CONTRACT, THE CONTRACTOR SHALL REMOVE, TRANSPORT AND UNLOAD THE TEMPORARY BARRIER TERMINAL SECTIONS AT THE SPECIFIED STATE MAINTENANCE YARD AND THE TERMINAL SECTIONS SHALL BECOME THE PROPERTY OF THE DEPARTMENT OF TRANSPORTATION.

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
ILL 171 (ARCHER AVE)
AT ASHBURY DR & 131ST STREET
INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
 DRAWN BY
 CHECKED BY
 DATE: 5/2/2007

5/2/2007 c:\p\project\p113602\des\ignoa.dgn

| SUMMARY OF QUANTITIES | | | | 901.FED./101.STATE | | | | CONSTRUCTION TYPE CODE | | | | SUMMARY OF QUANTITIES | | | | 901.FED./101.STATE | | | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|---|-------|------------------------|--------------------|---------|---------|--|------------------------|--|--|------|------------------------|--------------|---------|---------|--------------------|--|--|--|------------------------|--|--|--|
| CODE NO | ITEM | UNIT | URBAN TOTAL QUANTITIES | 100% VILLAGE | | | | | | | | URBAN TOTAL QUANTITIES | 100% VILLAGE | | | | | | | | | | |
| | | | | I000-1A | Y031-1F | Y031-3D | | | | | | | I000-1A | Y031-1F | Y031-3D | | | | | | | | |
| X0325774 | RELOCATE TEMPORARY IMPACT ATTENUATOR | EACH | 14 | 14 | | | | | | | 415 | 415 | | | | | | | | | | | |
| X0325503 | REMOVAL OF TEMPORARY SOIL RETENTION SYSTEM | FOOT | 1150 | 1150 | | | | | | | 175 | 175 | | | | | | | | | | | |
| X0325772 | IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE), TEST LEVEL 2 | EACH | 14 | 14 | | | | | | | 272 | 272 | | | | | | | | | | | |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 543 | 543 | | | | | | | 4 | 4 | | | | | | | | | | | |
| 20100210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 280 | 280 | | | | | | | 18.5 | 18.5 | | | | | | | | | | | |
| 20101100 | TREE TRUNK PROTECTION | EACH | 15 | 15 | | | | | | | 3 | 3 | | | | | | | | | | | |
| 20101200 | TREE ROOT PRUNING | EACH | 58 | 58 | | | | | | | 198 | 198 | | | | | | | | | | | |
| 20200100 | EARTH EXCAVATION | CU YD | 11910.20 | 11910.20 | | | | | | | 2 | 2 | | | | | | | | | | | |
| 20201006 | GRADING AND SHAPING SHOULDERS | UNIT | 16 | 16 | | | | | | | 45 | 45 | | | | | | | | | | | |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 222 | 222 | | | | | | | 16 | 16 | | | | | | | | | | | |
| 20800150 | TRENCH BACKFILL | CU YD | 160.5 | 160.5 | | | | | | | 48 | 48 | | | | | | | | | | | |
| * 21101625 | TOPSOIL FURNISH AND PLACE, 6" | SQ YD | 6292 | 6292 | | | | | | | 298 | 298 | | | | | | | | | | | |
| * 21101805 | COMPOST FURNISH AND PLACE, 2" | SQ YD | 280 | 280 | | | | | | | 760 | 760 | | | | | | | | | | | |
| * 21101815 | COMPOST FURNISH AND PLACE, 4" | SQ YD | 5324 | 5324 | | | | | | | 4645 | 4645 | | | | | | | | | | | |
| * 25000210 | SEEDING, CLASS 2A | ACRE | 0.5 | 0.5 | | | | | | | 490 | 490 | | | | | | | | | | | |
| * 25000312 | SEEDING, CLASS 4A | ACRE | 1.1 | 1.1 | | | | | | | 5456 | 5456 | | | | | | | | | | | |
| * 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 144 | 144 | | | | | | | 2990 | 2990 | | | | | | | | | | | |
| * 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 144 | 144 | | | | | | | 390 | 390 | | | | | | | | | | | |
| * 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 144 | 144 | | | | | | | 740 | 740 | | | | | | | | | | | |
| * 25100630 | EROSION CONTROL BLANKET | SQ YD | 7744 | 7744 | | | | | | | 106 | 106 | | | | | | | | | | | |
| * 25200110 | SODDING, SALT TOLERANT | SQ YD | 2170 | 2170 | | | | | | | 110 | 110 | | | | | | | | | | | |
| * 25200200 | SUPPLEMENTAL WATERING | UNIT | 78 | 78 | | | | | | | 1410 | 1410 | | | | | | | | | | | |
| 25400200 | SELECTIVE MOWING STAKES | EACH | 7 | 7 | | | | | | | 925 | 925 | | | | | | | | | | | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 150 | 150 | | | | | | | 60 | 60 | | | | | | | | | | | |
| 28000300 | TEMPORARY DITCH CHECKS | EACH | 14 | 14 | | | | | | | 50 | 50 | | | | | | | | | | | |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 3158 | 3158 | | | | | | | 622 | 622 | | | | | | | | | | | |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 13 | 13 | | | | | | | | | | | | | | | | | | | |
| 28100105 | STONE RIPRAP, CLASS A3 | SQ YD | 530 | 530 | | | | | | | | | | | | | | | | | | | |
| 28200200 | FILTER FABRIC | SQ YD | 530 | 530 | | | | | | | | | | | | | | | | | | | |

* SPECIALTY ITEMS

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION | |
|-----------|------|---------------------------------------|--|
| NAME | DATE | ILL 171 (ARCHER AVE.) | |
| | | AT ASHBURY DR. AND 131ST ST. | |
| | | SUMMARY OF QUANTITIES | |

5/3/2007 10:00:00 AM

| | | | | |
|-----------------------|----------|----------|-----------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 4 |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | HIGHWAY PROJECT | |

90% FED. / 10% STATE CONTRACT NO. 60A25

| SUMMARY OF QUANTITIES | | | URBAN TOTAL QUANTITIES | CONSTRUCTION TYPE CODE | | |
|-----------------------|--|--------|------------------------|------------------------|---------|---------|
| CODE NO | ITEM | UNIT | | 1000-1A | Y031-1F | Y031-3D |
| 48203029 | HOT-MIX ASPHALT SHOULDERS, 8" | SQ YD | 1287 | 1287 | | |
| 50105220 | PIPE CULVERT REMOVAL | FOOT | 455 | 455 | | |
| 54213660 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15" | EACH | 10 | 10 | | |
| 542A0220 | PIPE CULVERTS, CLASS A, TYPE 1 15" | FOOT | 208.15 | 208.15 | | |
| 550A0070 | STORM SEWERS, CLASS A, TYPE 1 15" | FOOT | 452 | 452 | | |
| 60107600 | PIPE UNDERDRAINS 4" | FOOT | 355.5 | 355.5 | | |
| 60201330 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 23 FRAME AND GRATE | EACH | 2 | 2 | | |
| 60252800 | CATCH BASINS TO BE RECONSTRUCTED | EACH | 2 | 2 | | |
| 60257900 | MANHOLES TO BE RECONSTRUCTED | EACH | 4 | 4 | | |
| 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 415 | 415 | | |
| 60604400 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 | FOOT | 722 | 722 | | |
| 60618300 | CONCRETE MEDIAN SURFACE, 4 INCH | SQ FT | 940 | 940 | | |
| * 63000000 | STEEL PLATE BEAM GUARD RAIL, TYPE A | FOOT | 331.25 | 331.25 | | |
| * 63100167 | TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT) | EACH | 5 | 5 | | |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 755.5 | 755.5 | | |
| 66410302 | CHAIN LINK FENCE REMOVAL, SPECIAL | FOOT | 173 | 173 | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 6 | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 0.5 | 0.5 | |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 | 1 | | |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 1 | | |
| 70100500 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701326 | L SUM | 1 | 1 | | |
| 70101800 | TRAFFIC CONTROL AND PROTECTION (SPECIAL) | L SUM | 1 | 1 | | |
| 70102620 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501 | L SUM | 1 | 1 | | |
| 70102625 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701606 | L SUM | 1 | | 1 | |
| 70102630 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701601 | L SUM | 1 | | 1 | |

| SUMMARY OF QUANTITIES | | | URBAN TOTAL QUANTITIES | CONSTRUCTION TYPE CODE | | |
|-----------------------|--|--------|------------------------|------------------------|---------|---------|
| CODE NO | ITEM | UNIT | | 1000-1A | Y031-1F | Y031-3D |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1 | | 1 | |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 4 | 4 | | |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 1 | | 1 | |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 326 | 326 | | |
| 70300210 | TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 184 | 184 | | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 8836 | 8836 | | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 710 | 710 | | |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 242 | 242 | | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 105 | 105 | | |
| 70300500 | PAVEMENT MARKING TAPE, TYPE III | FOOT | 11600 | 11600 | | |
| 70300520 | PAVEMENT MARKING TAPE, TYPE III 4" | FOOT | 105 | 105 | | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 4555 | 4555 | | |
| 70400500 | TEMPORARY CONCRETE BARRIER (STATE OWNED) | FOOT | 987.5 | 987.5 | | |
| 70400600 | RELOCATE TEMPORARY CONCRETE BARRIER (STATE OWNED) | FOOT | 987.5 | 987.5 | | |
| * 72000100 | SIGN PANEL - TYPE 1 | SQ FT | 28.5 | | 28.5 | |
| * 72000200 | SIGN PANEL - TYPE 2 | SQ FT | 25 | | 25 | |
| * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 184 | 184 | | |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 8836 | 8836 | | |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 710 | 710 | | |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 242 | 242 | | |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 105 | 105 | | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 88 | 88 | | |
| * 78200410 | GUARDRAIL MARKERS, TYPE A | EACH | 11 | 11 | | |
| * 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 4 | 4 | | |

* SPECIALTY ITEMS

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
ILL 171 (ARCHER AVE.)
AT ASHBURY DR. AND 131ST ST.
SUMMARY OF QUANTITIES

Rev.

PLOT DATE: 5/3/2007

5/3/2007

| SUMMARY OF QUANTITIES | | | 90% FED./10% STATE | | | | | SUMMARY OF QUANTITIES | | | 90% FED./10% STATE | | | | | | | | |
|-----------------------|---|------|------------------------|------------------------|---------|---------|--|-----------------------|---------|------------|--|------------------------|------------------------|---------|---------|---------|--|--|--|
| CODE NO | ITEM | UNIT | URBAN TOTAL QUANTITIES | CONSTRUCTION TYPE CODE | | | | | CODE NO | ITEM | UNIT | URBAN TOTAL QUANTITIES | CONSTRUCTION TYPE CODE | | | | | | |
| | | | | I000-1A | Y031-1F | Y031-3D | | | | | | | | I000-1A | Y031-1F | Y031-3D | | | |
| * 81000600 | CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL | FOOT | 499 | | 499 | | | | | * 88030050 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 2 | | 2 | | | | |
| * 81000700 | CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL | FOOT | 34 | | 34 | | | | | * 88030110 | SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED | EACH | 1 | | 1 | | | | |
| * 81001000 | CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL | FOOT | 10 | | 10 | | | | | * 88030210 | SIGNAL HEAD, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 1 | | 1 | | | | |
| * 81018500 | CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL | FOOT | 69 | | 69 | | | | | * 88030240 | SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED | EACH | 1 | | 1 | | | | |
| * 81018900 | CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL | FOOT | 349 | | 349 | | | | | * 88200210 | TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 8 | | 8 | | | | |
| * 81400100 | HANDHOLE | EACH | 3 | | 3 | | | | | * 88500100 | INDUCTIVE LOOP DETECTOR | EACH | 5 | | 5 | | | | |
| * 81400200 | HEAVY-DUTY HANDHOLE | EACH | 5 | | 5 | | | | | * 88600100 | DETECTOR LOOP, TYPE I | FOOT | 512 | | 512 | | | | |
| * 81400300 | DOUBLE HANDHOLE | EACH | 1 | | 1 | | | | | * 88700200 | LIGHT DETECTOR | EACH | 4 | | | 4 | | | |
| * 81900200 | TRENCH AND BACKFILL FOR ELECTRICAL WORK | FOOT | 538 | | 538 | | | | | * 88700300 | LIGHT DETECTOR AMPLIFIER | EACH | 1 | | | 1 | | | |
| * 85700200 | FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 | | 1 | | | | | * A2002916 | TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 2" CALIPER, BALLED AND BURLAPPED | EACH | 5 | 5 | | | | | |
| * 87301225 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 612 | | 612 | | | | | * A2005616 | TREE, OSTRYA VIRGINIANA (AMERICAN HOPHORNBEAM), 2" CALIPER, BALLED AND BURLAPPED | EACH | 9 | 9 | | | | | |
| * 87301245 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 2175 | | 2175 | | | | | * A2007114 | TREE, QUERCUS RUBRA (RED OAK), 1-3/4" CALIPER, BALLED AND BURLAPPED | EACH | 6 | 6 | | | | | |
| * 87301255 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 218 | | 218 | | | | | * A2007920 | TREE, TILIA AMERICANA REDMOND (REDMOND AMERICAN LINDEN), 2-1/2" CALIPER, BALLED AND BURLAPPED | EACH | 7 | 7 | | | | | |
| * 87301305 | ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 1359 | | 1359 | | | | | * B2000764 | TREE, AMELANCHIER X GRANDIFLORA AUTUMN BRILLIANCE (AUTUMN BRILLIANCE SERVICE BERRY), 5' HEIGHT, SHRUB FORM, BALLED AND BURLAPPED | EACH | 45 | 45 | | | | | |
| * 87301805 | ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C | FOOT | 54 | | 54 | | | | | * B2002166 | TREE, CRATAEGUS VIRDIS (GREEN HAWTHORN), 6' HEIGHT, MULTI-STEM, BALLED AND BURLAPPED | EACH | 7 | 7 | | | | | |
| * 87700130 | STEEL MAST ARM ASSEMBLY AND POLE, 18 FT. | EACH | 1 | | 1 | | | | | * C2001536 | SHRUB, CORNUS RACEMOSA (GREY DOGWOOD), 3' HEIGHT, BALLED AND BURLAPPED | EACH | 100 | 100 | | | | | |
| * 87700190 | STEEL MAST ARM ASSEMBLY AND POLE, 30 FT. | EACH | 1 | | 1 | | | | | * C2008218 | SHRUB, ROSA NEARLY WILD (NEARLY WILD SHRUB ROSE), 18" HEIGHT, CONTAINER | EACH | 45 | 45 | | | | | |
| * 87700210 | STEEL MAST ARM ASSEMBLY AND POLE, 34 FT. | EACH | 1 | | 1 | | | | | * K0026850 | PERENNIAL PLANT CARE | SQ YD | 600 | 600 | | | | | |
| * 87700240 | STEEL MAST ARM ASSEMBLY AND POLE, 40 FT. | EACH | 1 | | 1 | | | | | * K0029000 | PERENNIAL PLANTS, WOODLAND TYPE | UNIT | 11 | 11 | | | | | |
| * 87800150 | CONCRETE FOUNDATION, TYPE C | FOOT | 4 | | 4 | | | | | * K0036120 | MULCH PLACEMENT 4" | SQ YD | 1850 | 1850 | | | | | |
| * 87800400 | CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER | FOOT | 60 | | 60 | | | | | X0322256 | TEMPORARY INFORMATION SIGNING | SQ FT | 102.8 | 102.8 | | | | | |
| * 88030020 | SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED | EACH | 7 | | 7 | | | | | | | | | | | | | | |

* SPECIALTY ITEMS

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 ILL 171 (ARCHER AVE.)
 AT ASHBURY DR. AND 131ST ST.
 SUMMARY OF QUANTITIES

PLOT DATE: 5/3/2007

5/3/2007

| SUMMARY OF QUANTITIES | | | 90% FED. / 10% STATE | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|--|--------|------------------------|--------------|------------------------|---------|--|--|
| CODE NO | ITEM | UNIT | URBAN TOTAL QUANTITIES | | | | | |
| | | | | 100% VILLAGE | | | | |
| | | | | I000-1A | Y031-1F | Y031-3D | | |
| * X0322323 | WEED CONTROL, TEASEL | GALLON | 1 | 1 | | | | |
| * X0322859 | WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE | POUND | 12 | 12 | | | | |
| X0323988 | TEMPORARY SOIL RETENTION SYSTEM | SQ FT | 6880 | 6880 | | | | |
| X0324097 | COARSE SAND PLACEMENT, 2" | SQ YD | 280 | 280 | | | | |
| X0342600 | MANHOLES, TYPE A, 5' -DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE | EACH | 1 | 1 | | | | |
| X0656100 | DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT | SQ YD | 50 | 50 | | | | |
| X0712400 | TEMPORARY PAVEMENT | SQ YD | 2365 | 2365 | | | | |
| X4021000 | TEMPORARY ACCESS (PRIVATE ENTRANCE) | EACH | 6 | 6 | | | | |
| * X8050015 | SERVICE INSTALLATION - POLE MOUNTED | EACH | 1 | | 1 | | | |
| * X8620020 | UNINTERRUPTIBLE POWER SUPPLY | EACH | 1 | | 1 | | | |
| * X8730027 | ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C | FOOT | 446 | | 446 | | | |
| * X8730250 | ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED | FOOT | 614 | | | 614 | | |
| XX001306 | SIDEWALK REMOVAL AND REPLACEMENT | SQ FT | 300 | 300 | | | | |
| Z0001050 | AGGREGATE SUBGRADE 12" | SQ YD | 5635 | 5635 | | | | |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 | 1 | | | | |
| Z0030240 | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 2 | EACH | 4 | 4 | | | | |
| Z0030340 | IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 2 | EACH | 4 | 4 | | | | |
| Z0064800 | SELECTIVE CLEARING | UNIT | 7 | 7 | | | | |

| SUMMARY OF QUANTITIES | | | | | CONSTRUCTION TYPE CODE | | | |
|-----------------------|------|------|------------------|--------------|------------------------|---------|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | | | | | |
| | | | | 100% VILLAGE | | | | |
| | | | | I000-1A | Y031-1F | Y031-1D | | |
| | | | | | | | | |

* SPECIALTY ITEMS

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 ILL 171 (ARCHER AVE.)
 AT ASHBURY DR. AND 131ST ST.
 SUMMARY OF QUANTITIES

5/3/2007 10:00:00 AM

LEGEND

- ① EXISTING HMA SURFACING
- ② EXISTING HMA PAVEMENT, 9" (±)
- ③ EXISTING PC CONC. PAVEMENT, 9" & VARIES
- ④ EXISTING HMA BASE COURSE WIDENING, 9"
- ⑤ EXISTING COMB. CONC. CURB & GUTTER, TYPE B-6.12
- ⑥ EXISTING PC CONC. SIDEWALK, 4" OR 5"
- ⑦ EXISTING CORRUGATED PC CONCRETE MEDIAN (TO BE REMOVED)
- ⑧ EXISTING PC CONCRETE MEDIAN SURFACE, 4"
- ⑨ EXISTING HMA SHOULDER
- ⑩ EXISTING AGGREGATE SHOULDER (TO BE REMOVED AND PAID AS UNSUITABLE MATERIAL)
- ⑪ EXISTING SUB BASE GRANULAR MATERIAL, TYPE B
- ⑫ PROPOSED LEVELING BINDER (MM), N70, 1"
- ⑬ PROPOSED HMA PAVEMENT (FULL DEPTH), 9"
A. HMA SURFACE COURSE, MIX "D", N70, 2"
B. HMA BASE COURSE, 7"
- ⑭ PROPOSED HMA PAVEMENT (FULL DEPTH), 13-1/4"
A. HMA SURFACE COURSE, MIX "D", N70, 2"
B. HMA BASE COURSE, 11-1/4"
- ⑮ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
- ⑯ PROPOSED HMA SHOULDER, 8"
- ⑰ PROPOSED COMB. CONC. CURB & GUTTER, TYPE B-6.18
- ⑱ PROPOSED COMB. CONC. CURB & GUTTER, TYPE B-6-12
- ⑲ PROPOSED AGGREGATE SUBGRADE, 12"
- ⑳ PROPOSED AGGREGATE SHOULDER, TYPE B, 6"
- ㉑ PROPOSED TOPSOIL PLACEMENT, 6" & SODDING OR SEEDING
- ㉒ PROPOSED PC CONC. MEDIAN SURFACE, 4"
- ㉓ PROPOSED HMA BASE COURSE WIDENING, 10-3/4"
- ㉔ PROPOSED HMA BASE COURSE WIDENING, 6-1/2"

NOTES:
ADDITIONAL SUB-BASE GRANULAR MATERIAL UNDER PROPOSED CURB AND GUTTER SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF "SUB-BASE GRANULAR MATERIAL, TYPE B 4 INCHES.

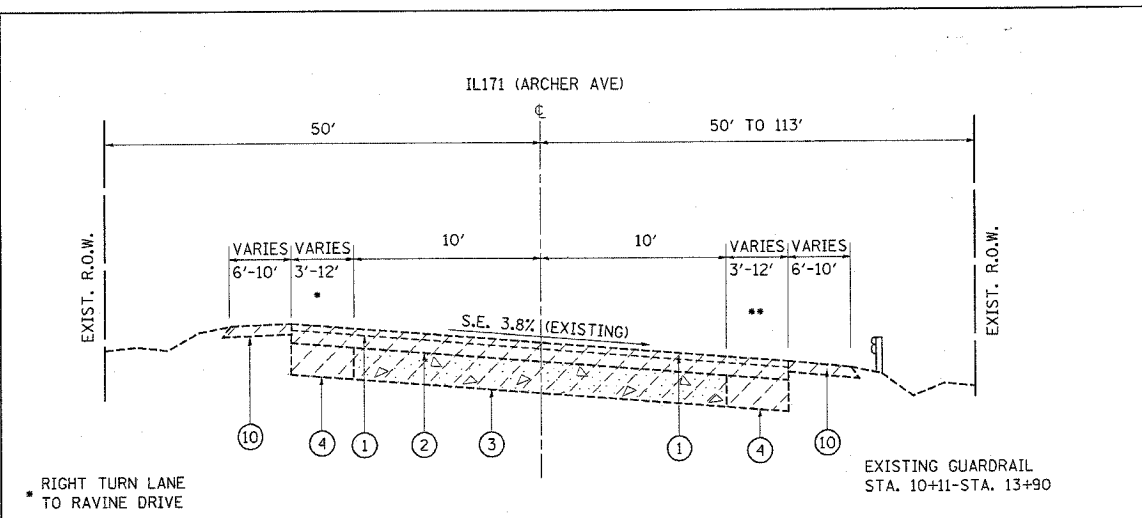
ANY SAW CUTTING REQUIRED TO REMOVE ANY ITEM ADJACENT TO AN ITEM TO BE SAVED WILL BE CONSIDERED AS PART OF THE REMOVAL ITEM AND WILL NOT BE PAID FOR SEPARATELY.

THE HMA CONCRETE SURFACE REMOVAL SHALL BE PERFORMED AT THE PROPOSED CROSS-SLOPE SHOWN ON THE PROPOSED TYPICAL SECTIONS, AND NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR THIS REQUIREMENT.

MIXTURES FOR THE WIDENING AREA SHALL BE THE SAME MIXTURE USED IN THE RECONSTRUCTION AREA.

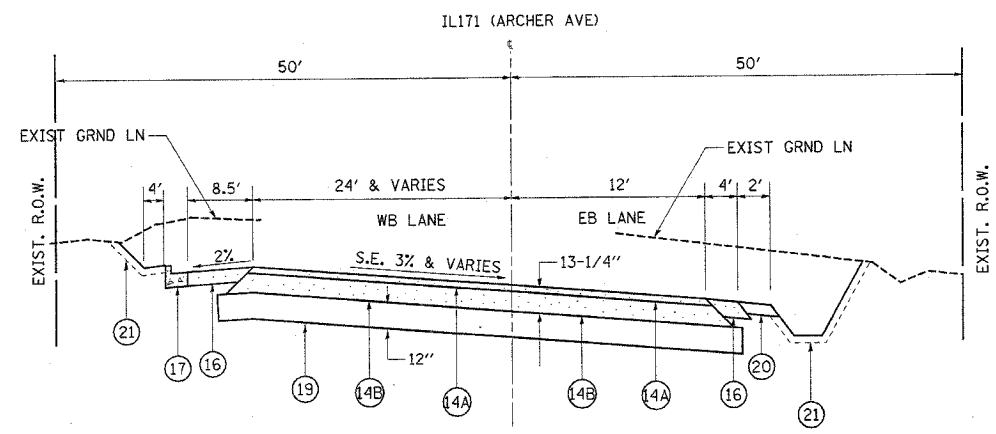
| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
**ILL 171 (ARCHER AVE)
AT ASHBURY DR & 131ST STREET**
EXISTING AND PROPOSED
TYPICAL SECTION
SCALE: VERT. _____
HORIZ. _____
DATE _____ DRAWN BY _____
CHECKED BY _____

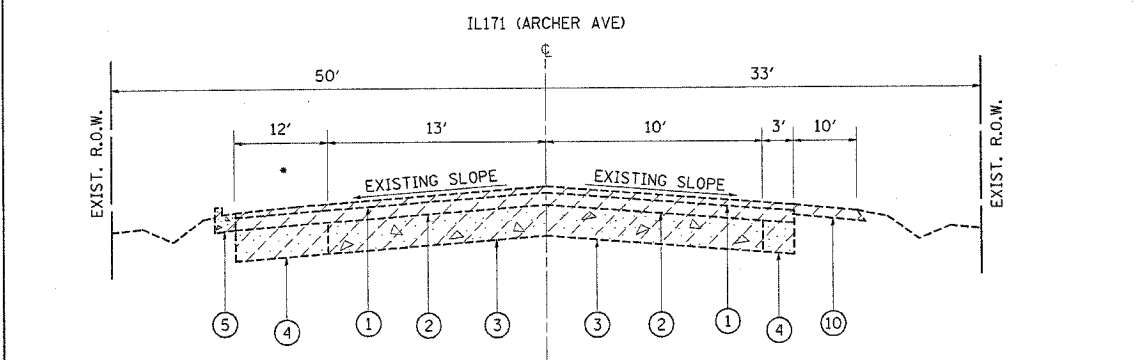


**ILL 171 (ARCHER AVE)
EXISTING TYPICAL SECTION
STA. 11+14 TO STA. 19+50**

AREAS OF REMOVAL ITEMS VARIES
SEE PLAN SHEETS FOR DETAILED LOCATIONS.

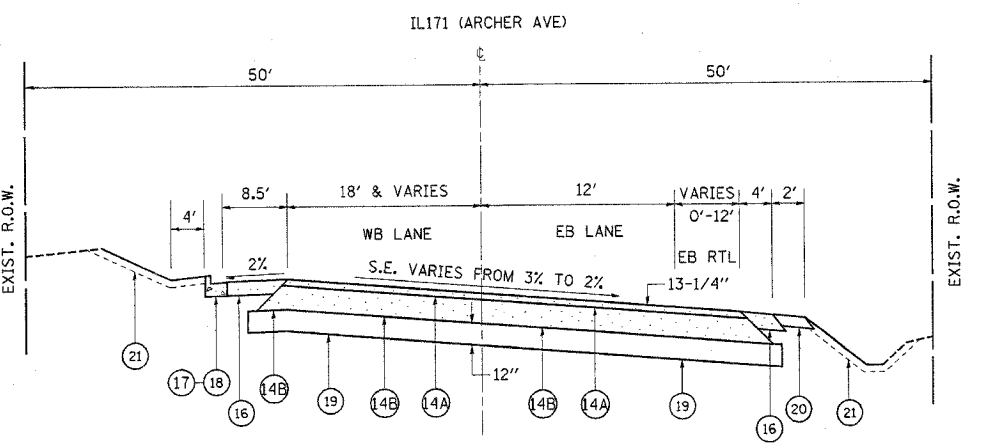


**ILL 171 (ARCHER AVE)
PROPOSED TYPICAL SECTION
STA. 12+30 TO STA. 14+00
(RECONSTRUCTION)**



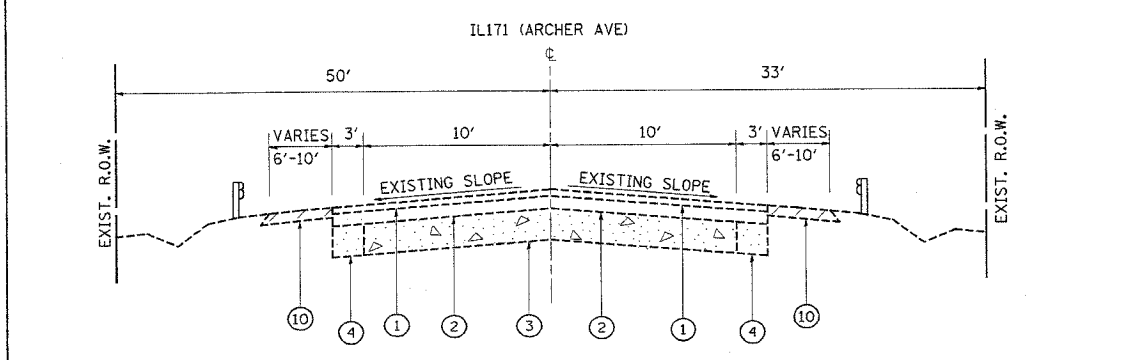
**ILL 171 (ARCHER AVE)
EXISTING TYPICAL SECTION
STA. 20+50 TO STA. 21+50**

AREAS OF REMOVAL ITEMS VARIES
SEE PLAN SHEETS FOR DETAILED LOCATIONS.



**ILL 171 (ARCHER AVE)
PROPOSED TYPICAL SECTION
STA. 14+00 TO STA. 21+31
(RECONSTRUCTION)**

NOTES:
FOR SUPERELEVATION AND PROFILE
CORRECTION USE LEVELING BINDER

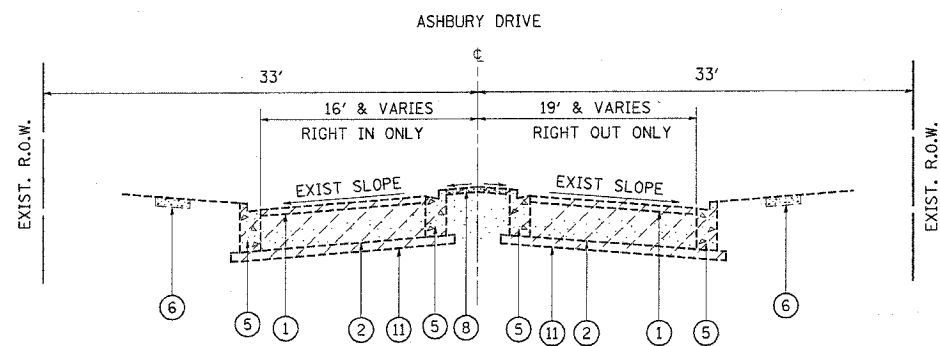


**ILL 171 (ARCHER AVE)
EXISTING TYPICAL SECTION
STA. 21+50 TO STA. 28+00**

EXISTING GUARDRAIL
STA. 21+50-STA. 30+30

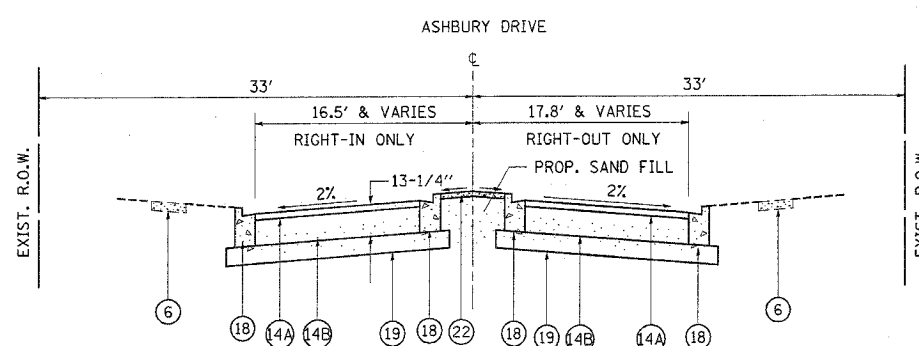
AREAS OF REMOVAL ITEMS VARIES
SEE PLAN SHEETS FOR DETAILED LOCATIONS.

PLOT DATE = 5/2/2007
 PLOT SCALE = 50.000' / IN.
 USER NAME = galbarrn



**ASHBURY DRIVE
EXISTING TYPICAL SECTION
STA. 9+000 TO STA. 9+80**

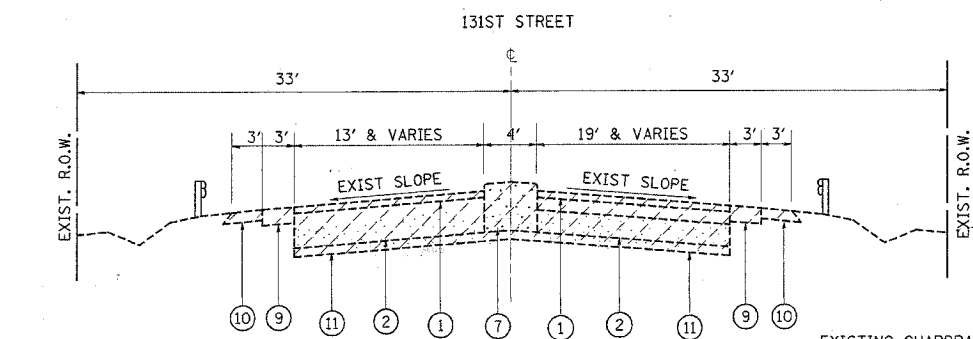
AREAS OF REMOVAL ITEMS VARIES
SEE PLAN SHEETS FOR DETAILED LOCATIONS.



**ASHBURY DRIVE
PROPOSED TYPICAL SECTION
STA. 9+000 TO STA. 9+80
(RECONSTRUCTION)**

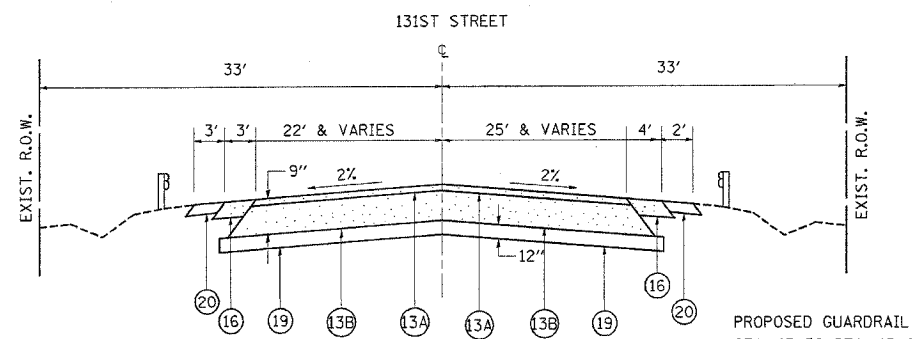
LEGEND

- ① EXISTING HMA SURFACING
- ② EXISTING HMA PAVEMENT, 9" (±)
- ③ EXISTING PC CONC. PAVEMENT, 9" & VARIES
- ④ EXISTING HMA BASE COURSE WIDENING, 9"
- ⑤ EXISTING COMB. CONC. CURB & GUTTER, TYPE B-6.12
- ⑥ EXISTING PC CONC. SIDEWALK, 4" OR 5"
- ⑦ EXISTING CORRUGATED PC CONCRETE MEDIAN (TO BE REMOVED)
- ⑧ EXISTING PC CONCRETE MEDIAN SURFACE, 4"
- ⑨ EXISTING HMA SHOULDER
- ⑩ EXISTING AGGREGATE SHOULDER (TO BE REMOVED AND PAID AS UNSUITABLE MATERIAL)
- ⑪ EXISTING SUB BASE GRANULAR MATERIAL, TYPE B
- ⑫ PROPOSED LEVELING BINDER (MM), N70, 1"
- ⑬ PROPOSED HMA PAVEMENT (FULL DEPTH), 9"
A. HMA SURFACE COURSE, MIX "D", N70, 2"
B. HMA BASE COURSE, 7"
- ⑭ PROPOSED HMA PAVEMENT (FULL DEPTH), 13-1/4"
A. HMA SURFACE COURSE, MIX "D", N70, 2"
B. HMA BASE COURSE, 11-1/4"
- ⑮ PROPOSED HMA SURFACE COURSE, MIX "D", N70, 1-1/2"
- ⑯ PROPOSED HMA SHOULDER, 8"
- ⑰ PROPOSED COMB. CONC. CURB & GUTTER, TYPEB-6.18
- ⑱ PROPOSED COMB. CONC. CURB & GUTTER, TYPE B-6-12
- ⑲ PROPOSED AGGREGATE SUBGRADE, 12"
- ⑳ PROPOSED AGGREGATE SHOULDER, TYPE B, 6"
- ㉑ PROPOSED TOPSOIL PLACEMENT, 6" & SODDING OR SEEDING
- ㉒ PROPOSED PC CONC. MEDIAN SURFACE, 4"
- ㉓ PROPOSED HMA BASE COURSE WIDENING, 10-3/4"
- ㉔ PROPOSED HMA BASE COURSE WIDENING, 6-1/2"

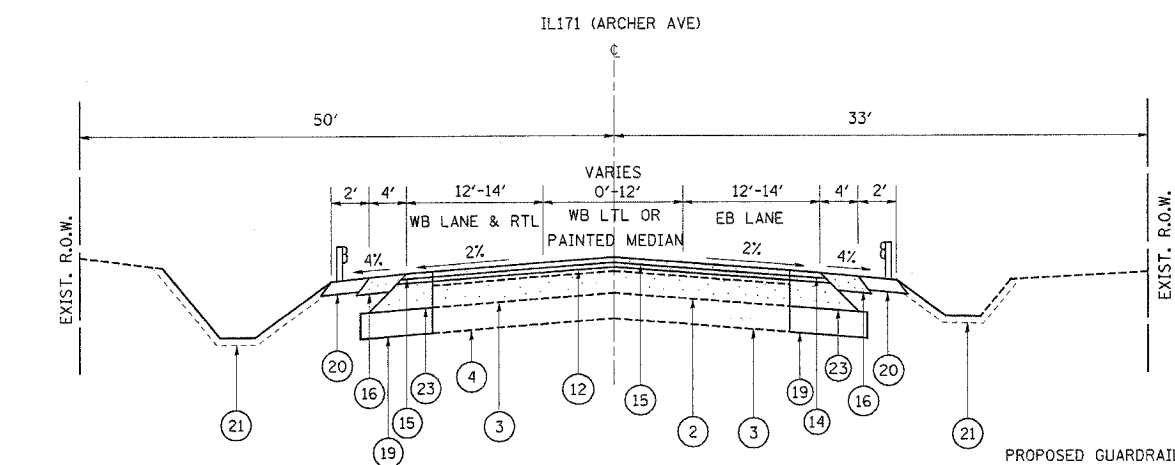


**131ST STREET
EXISTING TYPICAL SECTION
STA. 10+50 TO STA. 15+00**

AREAS OF REMOVAL ITEMS VARIES
SEE PLAN SHEETS FOR DETAILED LOCATIONS.

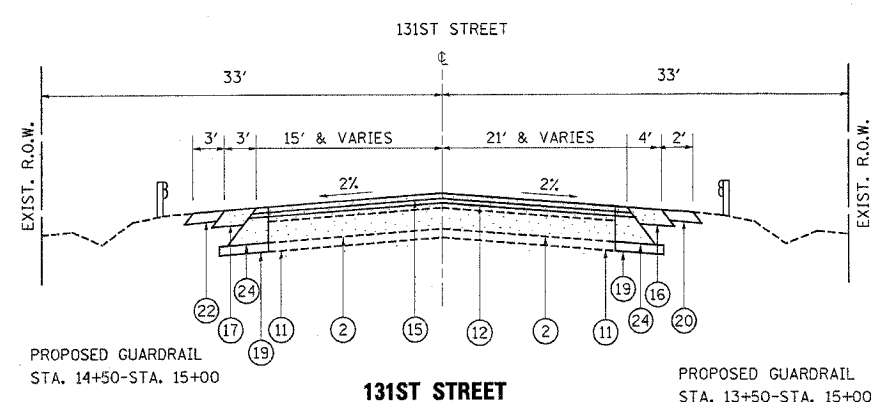


**131ST STREET
PROPOSED TYPICAL SECTION
STA. 10+00 TO STA. 11+40
(RECONSTRUCTION)**



**ILL 171 (ARCHER AVE)
PROPOSED TYPICAL SECTION
STA. 21+31 TO STA. 26+80.0
(WIDENING/RESURFACING)**

NOTES:
FOR SUPERELEVATION AND PROFILE
CORRECTION USE LEVELING BINDER



**131ST STREET
PROPOSED TYPICAL SECTION
STA. 11+40 TO STA. 15+00
(WIDENING RESURFACING)**

NOTES:

ADDITIONAL SUB-BASE GRANULAR MATERIAL UNDER PROPOSED CURB AND GUTTER SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF "SUB-BASE GRANULAR MATERIAL, TYPE B 4 INCHES.

ANY SAW CUTTING REQUIRED TO REMOVE ANY ITEM ADJACENT TO AN ITEM TO BE SAVED WILL BE CONSIDERED AS PART OF THE REMOVAL ITEM AND WILL NOT BE PAID FOR SEPARATELY.

THE HMA CONCRETE SURFACE REMOVAL SHALL BE PERFORMED AT THE PROPOSED CROSS-SLOPE SHOWN ON THE PROPOSED TYPICAL SECTIONS, AND NO ADDITIONAL COMPENSATION WILL BE PROVIDED FOR THIS REQUIREMENT.

MIXTURES FOR THE WIDENING AREA SHALL BE THE SAME MIXTURE USED IN THE RECONSTRUCTION AREA.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION ILL 171 (ARCHER AVE) AT ASHBURY DR & 131ST STREET EXISTING AND PROPOSED TYPICAL SECTION |
|-----------|------|---|
| NAME | DATE | |
| | | SCALE: VERT. HORIZ. DATE |
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 PLOT SCALE = 60.0000 / IN.
 USER NAME = gabrielsb

HOT-MIX ASPHALT MIXTURE REQUIREMENTS
THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT.

| MIXTURE TYPE | AC/PG | AIR VOIDS (%) | THICKNESS |
|--|-----------------------|---------------|-------------------|
| PAVEMENT RESURFACING | | | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) | PG 64-22 | 4%±70 GYR | 1-1/2" |
| LEVELING BINDER (MACHINE METHOD), N70, IL-9.5mm, | PG 64-22 58-22 | 4%±70 GYR | 1 " |
| WIDENING SECTION (RESURFACING AREA) | | | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) | PG 64-22 | 4%±70 GYR | 1-1/2" |
| LEVELING BINDER (MACHINE METHOD), N70, IL-9.5mm | PG 64-22 58-22 | 4%±70 GYR | 1 " |
| HOT-MIX ASPHALT BASE COURSE WIDENING (HMA BINDER IL-19mm) | PG 64-22 58-22 | 4%±50 GYR | 6-1/2" 10-3/4" |
| FULL DEPTH PAVEMENT | | | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm) | PG 64-22 | 4%±70 GYR | 2" |
| HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm) | PG 64-22/ PG 58-22 | 4%±50 GYR | 11-1/4" & 7" |
| ENTRANCES (PE) | | | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5 mm) | PG 64-22 | 4%±50 GYR | 2" |
| HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm) | PG 64-22/ PG 58-22 | 4%±50 GYR | 6" |
| SHOULDER | | | |
| HOT-MIX ASPHALT SHOULDER | PG 64-22/ PG 58-22 | 2%±30 GYR | 8" |
| TEMPORARY PAVEMENT | | | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) | PG 64-22 | 4%±50 GYR | 1-3/4" |
| TEMPORARY PAVEMENT (HMA BINDER IL-19 mm) | PG 64-22/ PG 58-22 | 4%±50 GYR | 8-1/4" |
| HOT-MIX ASPHALT PATCHING | | | |
| CLASS D PATCHES (HMA BINDER IL-19 mm) | PG 64-22/ PG 58-22 | 4%±70 GYR | 17" |
| HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm) | PG 64-22/ PG 58-22 | 4%±70 GYR | 2-1/2" |

NOTE:
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 POUNDS PER SQUARE YARD PER INCH.
• WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

PERENNIAL PLANT CARE SCHEDULE

| ACTIVITY | TIME |
|---|---|
| PLANT PERENNIALS AS PER PLAN | MAY 1 - JUNE 15 AUGUST 15 - SEPTEMBER 15 |
| MULCH PERRENIAL BEDS | 24 HOURS AFTER PLANTING |
| INSTALL SELECTIVE MOW STAKES AS PER PLAN OR DIRECTION OF R.E. | PRIOR TO PERIOD OF ESTABLISHMENT INSPECTION |
| PERENNIAL PLANT PERIOD OF ESTABLISHMENT WATER ONCE EVERY 7 DAYS FOR 4 WEEKS | PRIOR TO PERIOD OF ESTABLISHMENT INSPECTION |
| REPLACE DEAD PLANTS | AFTER PERIOD OF ESTABLISHMENT INSPECTION |
| PERENNIAL PLANT CARE (FIRST CYCLE) | 30 DAYS AFTER PERIOD OF ESTABLISHMENT INSPECTION |
| PERENNIAL PLANT CARE (SECOND CYCLE) | 60 DAYS AFTER PERIOD OF ESTABLISHMENT INSPECTION |
| PERENNIAL PLANT CARE (THIRD CYCLE) | 90 DAYS AFTER PERIOD OF ESTABLISHMENT INSPECTION |
| SUPPLEMENTAL WATERING | USE AFTER PERIOD OF EST. INSP. AS DIRECTED BY THE RESIDENT ENGINEER |

NOTE:
ALL PERENNIAL PLANTS SHALL BE STAGGERED AND SPACED ON 12" (300 mm) CENTERS UNLESS OTHERWISE NOTED.

SCHEDULE FOR PIPE UNDERDRAINS, 4" DIA.
ILLINOIS 171 (ARCHER AVE.) AT ASHBURY DR. AND 131ST. STREET

| LOCATION (STATIONS) | LENGTH (FEET) | REMARKS |
|------------------------------|---------------|--------------|
| ILLINOIS 171 (ARCHER AVENUE) | | |
| STA. 13+10 | 58.00 | TRANSVERSE |
| STA. 15+30 | 64.00 | TRANSVERSE |
| STA. 21+25 | 65.50 | TRANSVERSE |
| • STA. 25+00 - STA. 25+50 | 50.00 | LONGITUDINAL |
| •• STA. 25+00 - STA. 25+50 | 50.00 | LONGITUDINAL |
| 131ST STREET | | |
| STA. 11+35 | 68.00 | TRANSVERSE |

- NORTH SIDE
- SOUTH SIDE

EARTHWORK SCHEDULE

ILLINOIS 171 (ARCHER AVE.) AT ASHBURY DR. AND 131ST. STREET

| LOCATION | EARTH EXCAVATION | EARTH EXCAVATION ADJUSTED FOR SHRINKAGE | EMBANKMENT | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) |
|----------|------------------|---|------------|---|
| STATIONS | CUBIC YARD | CUBIC YARD | CUBIC YARD | CUBIC YARD |

| ILLINOIS 171 (ARCHER AVENUE) | | | | |
|------------------------------|---------|---------|--------|----------|
| STA. 12+00 - STA. 21+00 | 9134.56 | 7764.38 | 156.48 | +7607.90 |
| STA. 21+00 - STA. 24+00 | 621.30 | 528.11 | 66.67 | +461.44 |
| STA. 24+00 - STA. 25+00 | 91.34 | 77.64 | 1.22 | +76.42 |
| STA. 25+00 - STA. 25+75 | 32.88 | 27.95 | 0.00 | +27.95 |

| ASHBURY DRIVE/131ST STREET | | | | |
|----------------------------|----------|----------|--------|----------|
| STA. 09+00 - STA. 09+80 | 507.78 | 431.61 | 0.93 | +430.68 |
| STA. 09+80 - STA. 11+00 | 1141.29 | 970.10 | 0.41 | +969.69 |
| STA. 11+00 - STA. 15+00 | 381.05 | 334.28 | 25.72 | +298.56 |
| TOTAL | 11910.20 | 10134.07 | 251.43 | +9872.64 |

COLUMN 1: LOCATION FROM PLANS
COLUMN 2: QUANTITY OF EARTH EXCAVATION (CUT) FROM CROSS SECTIONS
COLUMN 3: QUANTITY OF EARTH EXCAVATION (CUT) ADJUSTED FOR A SHRINKAGE FACTOR OF 15%
COLUMN 4: QUANTITY OF EMBANKMENT (FILL) FROM CROSS SECTIONS
COLUMN 5: (COLUMN 3 - COLUMN 4)
(+) = QUANTITY OF FURNISHED EXCAVATION NEEDED
(-) = QUANTITY TO BE WASTED

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
ILL 171 (ARCHER AVE)
AT ASHBURY DR & 131ST STREET
SCHEDULE OF QUANTITIES
SCALE: VERT. _____
HORIZ. _____
DATE _____
DRAWN BY _____
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PLOT DATE = 5/2/2007
 FILE NAME = I:\11582\designs.dgn
 PLOT SCALE = 1/8" = 1'-0"
 USER NAME = gellbamb

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|----------|------------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 11 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

EXIST. CURVE ADDIT-1
 PI STA. = 9+94.54
 $\Delta = 20^\circ 38' 49''$ (RT)
 $D = 1^\circ 45' 110''$
 $R = 3,268.40'$
 $T = 595.35'$
 $L = 1,177.79'$
 $E = 53.78'$
 $\theta = 2.87'$
 S.E. RUN = 3+99.19
 P.C. STA. = 15+76.98
 P.T. STA. = 15+76.98

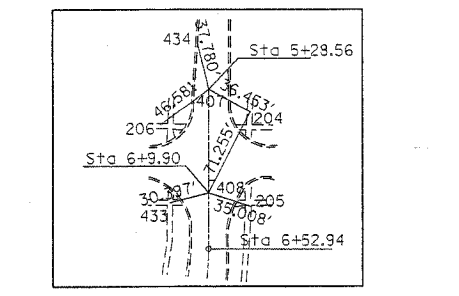
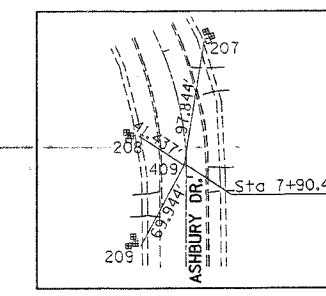
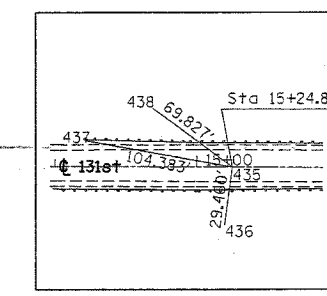
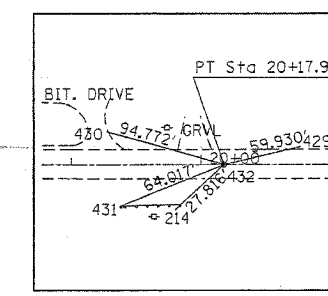
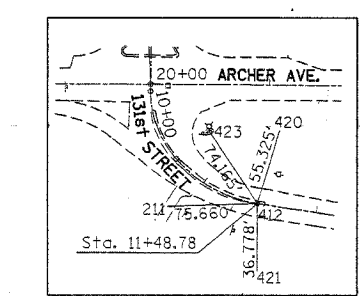
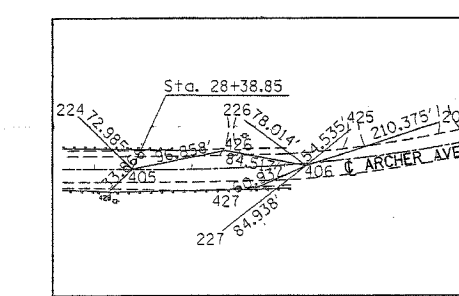
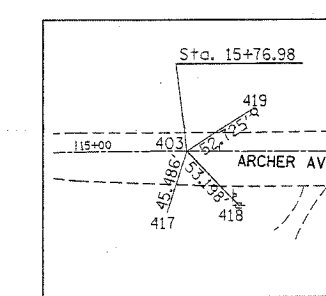
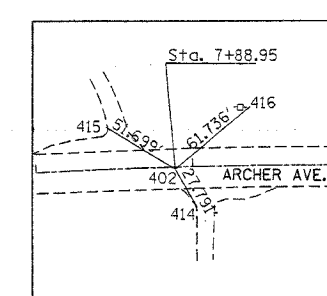
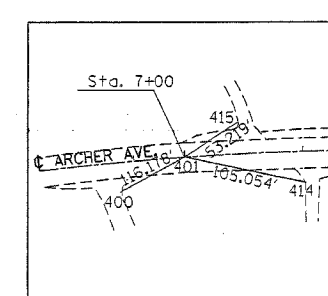
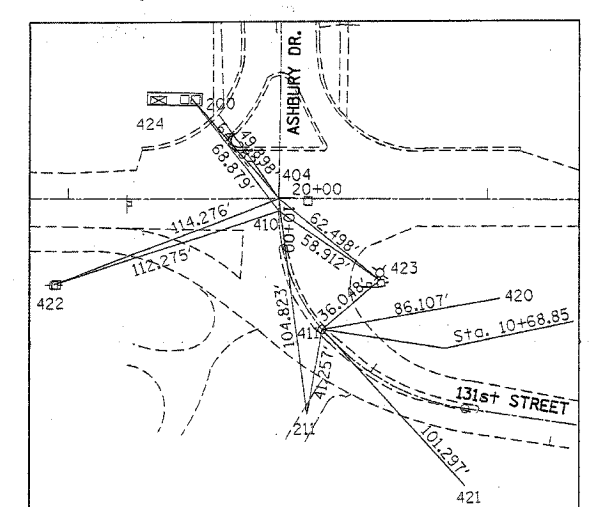
EXIST. CURVE CURVES
 PI STA. = 7+23.98
 $\Delta = 31^\circ 30' 46''$ (RT)
 $D = 22^\circ 55' 06''$
 $R = 250.00'$
 $T = 131.50'$
 $L = 9.76'$
 S.E. RUN = 6+52.94
 P.C. STA. = 7+30.44
 P.T. STA. = 7+30.44

EXIST. CURVE CURVE2
 PI STA. = 29+27.94
 $\Delta = 4^\circ 11' 25''$ (LT)
 $D = 2^\circ 21' 110''$
 $R = 2,435.00'$
 $T = 89.08'$
 $L = 178.08'$
 $E = 1.63'$
 $\theta =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 28+38.85$
 $P.T. STA. = 30+16.94$

EXIST. CURVE CURVE4
 PI STA. = 10+39.02
 $\Delta = 40^\circ 28' 59''$ (LT)
 $D = 64^\circ 52' 23''$
 $R = 88.32'$
 $T = 32.57'$
 $L = 62.40'$
 $E = 5.81'$
 $\theta =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 10+06.45$
 $P.T. STA. = 10+68.86$

EXIST. CURVE CURVES
 PI STA. = 11+10.60
 $\Delta = 40^\circ 53' 04''$ (LT)
 $D = 51^\circ 09' 25''$
 $R = 112.00'$
 $T = 41.75'$
 $L = 79.92'$
 $E = 7.53'$
 $\theta =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 10+68.86$
 $P.T. STA. = 11+48.78$

EXIST. CURVE CURVE6
 PI STA. = 17+71.39
 $\Delta = 0^\circ 24' 54''$ (LT)
 $D = 0^\circ 05' 03''$
 $R = 68,052.38'$
 $T = 246.53'$
 $L = 493.06'$
 $E = 0.45'$
 $\theta =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 15+24.86$
 $P.T. STA. = 20+17.92$



BENCHMARKS:
 TP 19, SPIKE NAIL SET NORTH FACE POWER POLE SW CORNER OF 131st ST. AND DAVINCI ST. ELEVATION = 710.096
 TP 23, RR SPIKE SET WEST FACE OF WOOD LIGHT POLE, SIDE OF IL. 171 (ARCHER AVE.), FIRST LIGHT POLE N.E. OF 131ST ST. AT INTERSECTION AT SOUTHEASTERLY CORNER. ELEVATION = 709.963
 TP 50, N. FLANGE OF HYDRANT NORTHERLY SIDE OF IL. 171 (ARCHER AVE.), FIRST HYDRANT WESTERLY OF 131ST ST. ELEVATION = 715.11

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ILLINOIS DEPARTMENT OF TRANSPORTATION
**IL. RTE. 171 (ARCHER AVE.)
 AT ASHBURY DRIVE AND
 131st STREET
 ALIGNMENT & TIES PLAN**
 SCALE: VERT. _____
 HORIZ. _____
 DATE _____ DRAWN BY _____
 CHECKED BY _____

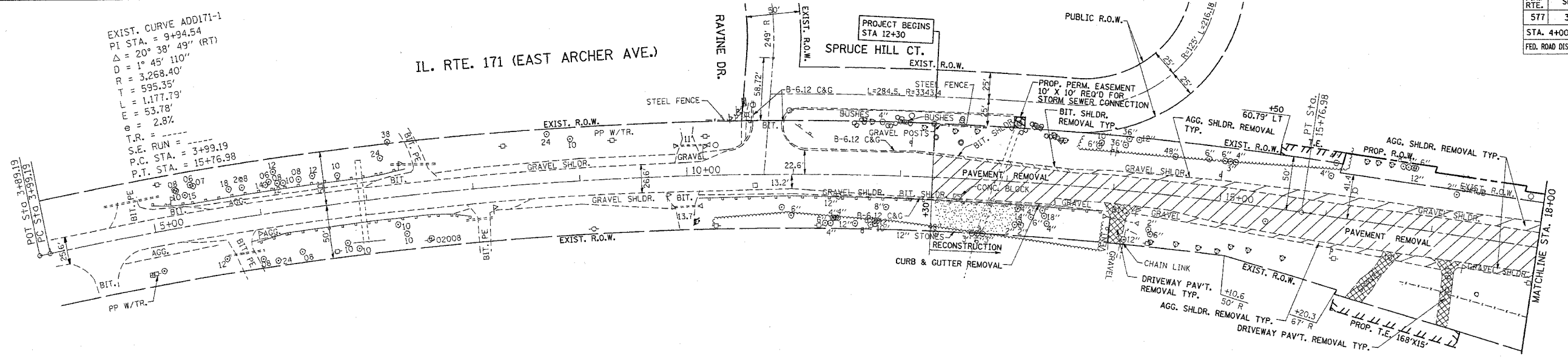
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 12 |
| STA. 4+00 | | TO STA. 18+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

EXIST. CURVE ADD171-1
 PI STA. = 9+94.54
 $\Delta = 20^\circ 38' 49''$ (RT)
 $D = 1^\circ 45' 110''$
 $R = 3,268.40'$
 $T = 595.35'$
 $L = 1,177.79'$
 $E = 53.78'$
 $\theta = 2.8\%$
 $T.R. =$
 $S.E. RUN = 3+99.19$
 $P.C. STA. = 15+76.98$
 $P.T. STA. = 15+76.98$

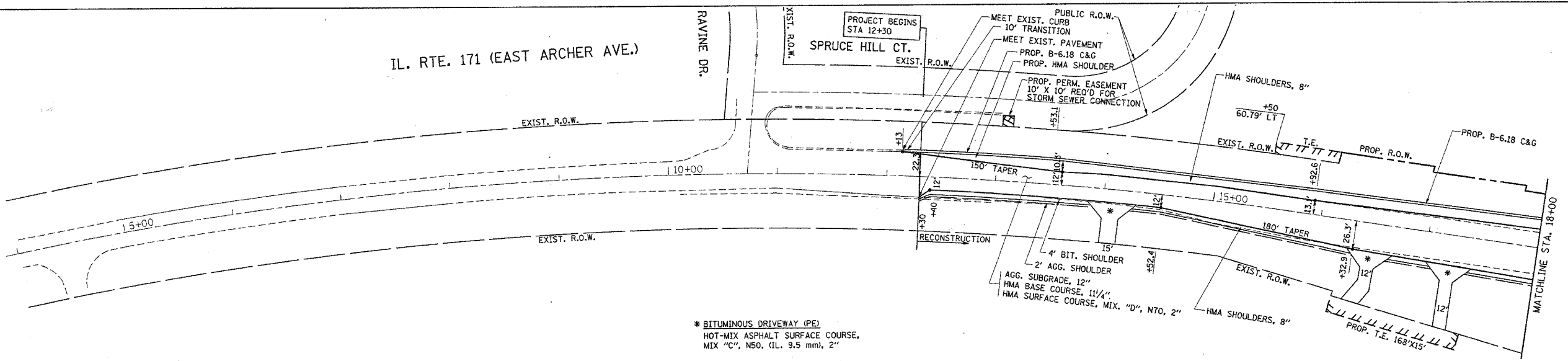
IL. RTE. 171 (EAST ARCHER AVE.)

PROJECT BEGINS STA 12+30
 SPRUCE HILL CT.
 EXIST. R.O.W.

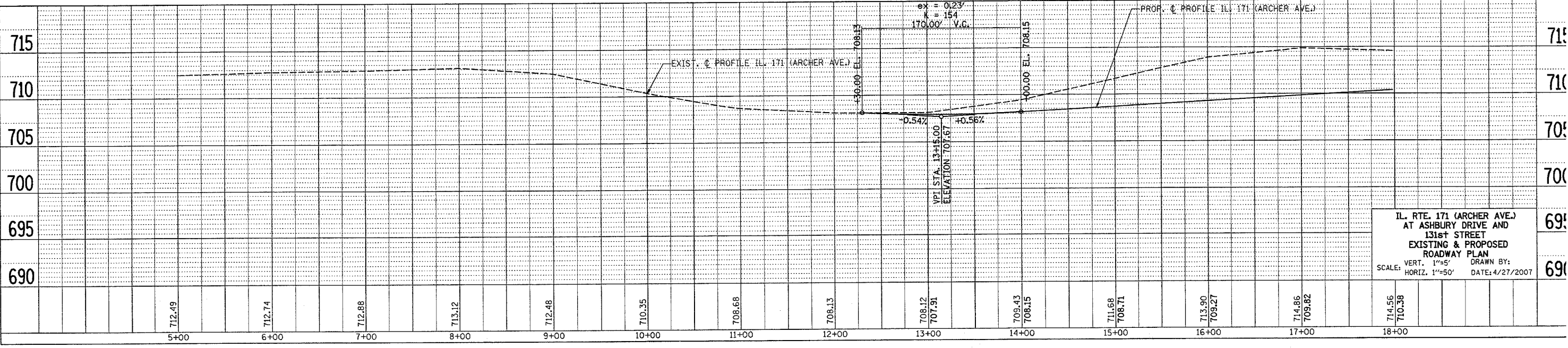


IL. RTE. 171 (EAST ARCHER AVE.)

PROJECT BEGINS STA 12+30
 SPRUCE HILL CT.
 EXIST. R.O.W.



* BITUMINOUS DRIVEWAY (PE)
 HOT-MIX ASPHALT SURFACE COURSE,
 MIX "C", NS0, (IL. 9.5 mm), 2"
 HOT-MIX ASPHALT BASE COURSE,
 (HMA BINDER IL-19 mm), 6"



IL. RTE. 171 (ARCHER AVE.)
 AT ASHBURY DRIVE AND
 131st STREET
 EXISTING & PROPOSED
 ROADWAY PLAN
 SCALE: VERT. 1"=5'
 HORIZ. 1"=50' DATE: 4/27/2007

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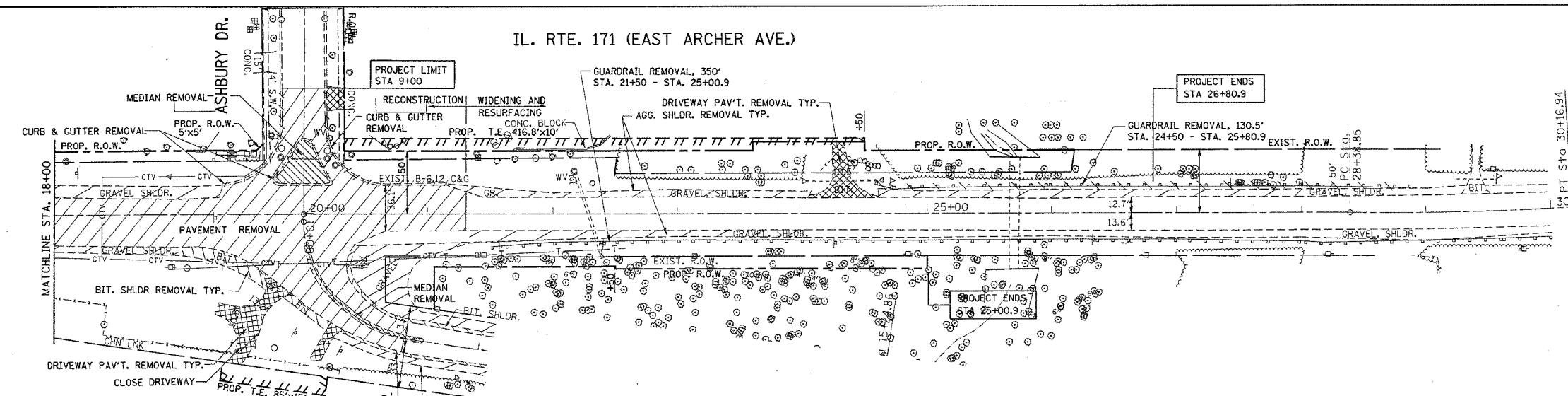
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 USER NAME = abauerl

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 13 |
| STA. 18+00 | | TO STA. 30+00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

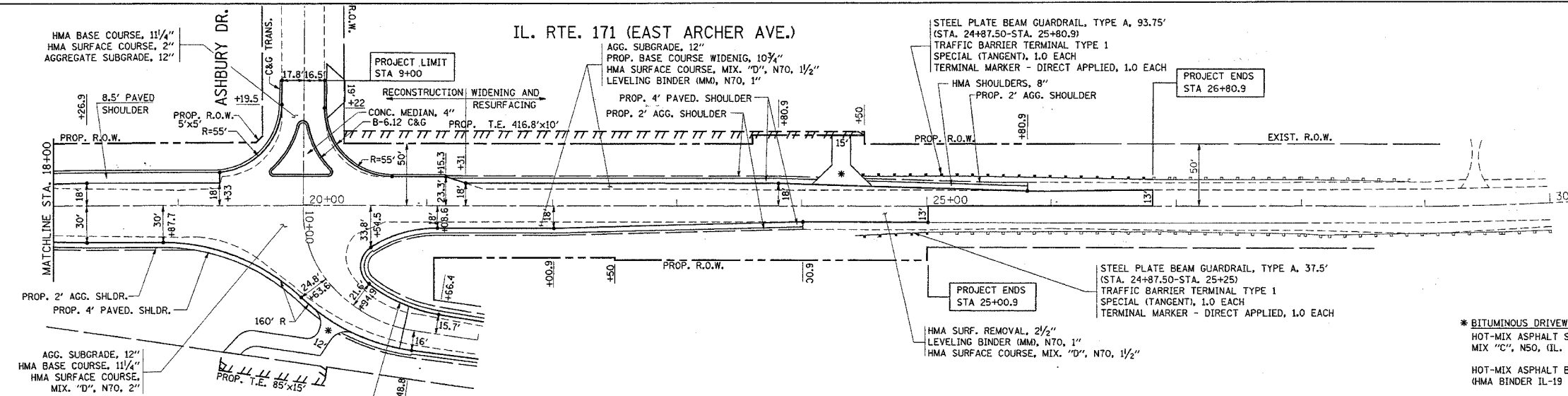
EXIST. CURVE CURVE2
 PI STA. = 29+27.94
 $\Delta = 4^\circ 11' 25''$ (LT)
 $D = 2^\circ 21' 11''$
 $R = 2,435.00'$
 $T = 89.08'$
 $L = 178.08'$
 $E = 1.63'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 28+38.85$
 $P.T. STA. = 30+16.94$



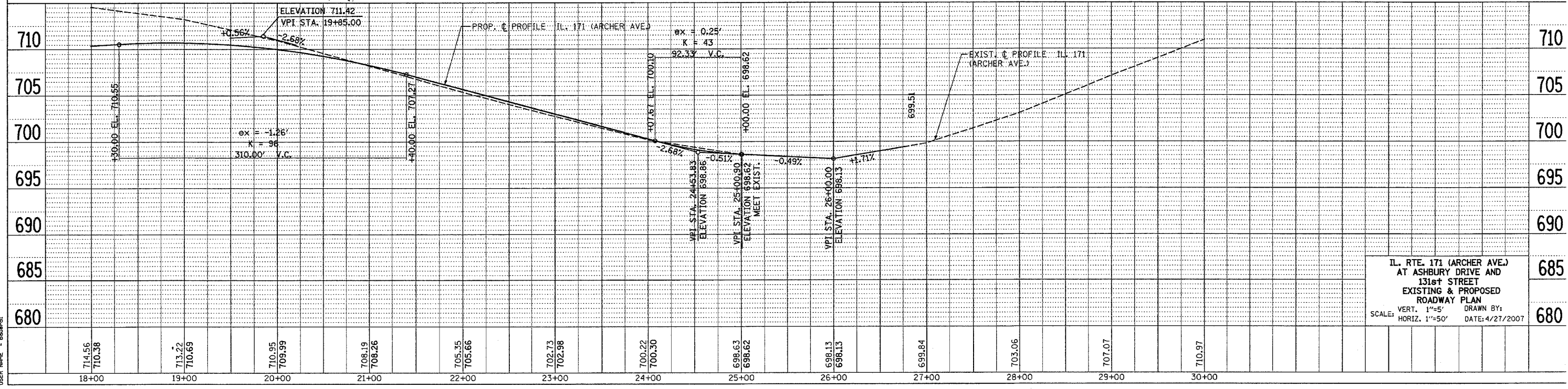
IL. RTE. 171 (EAST ARCHER AVE.)



IL. RTE. 171 (EAST ARCHER AVE.)



* BITUMINOUS DRIVEWAY (PE)
 HOT-MIX ASPHALT SURFACE COURSE,
 MIX "C", NSO, (1L 9.5 mm), 2"
 HOT-MIX ASPHALT BASE COURSE,
 (HMA BINDER 1L-19 mm), 6"



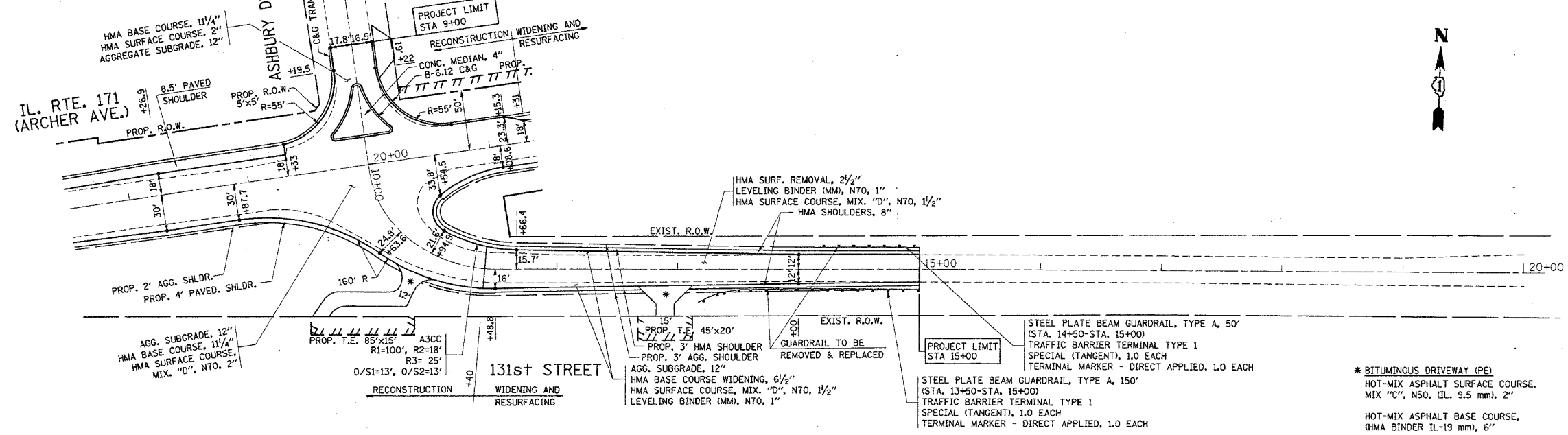
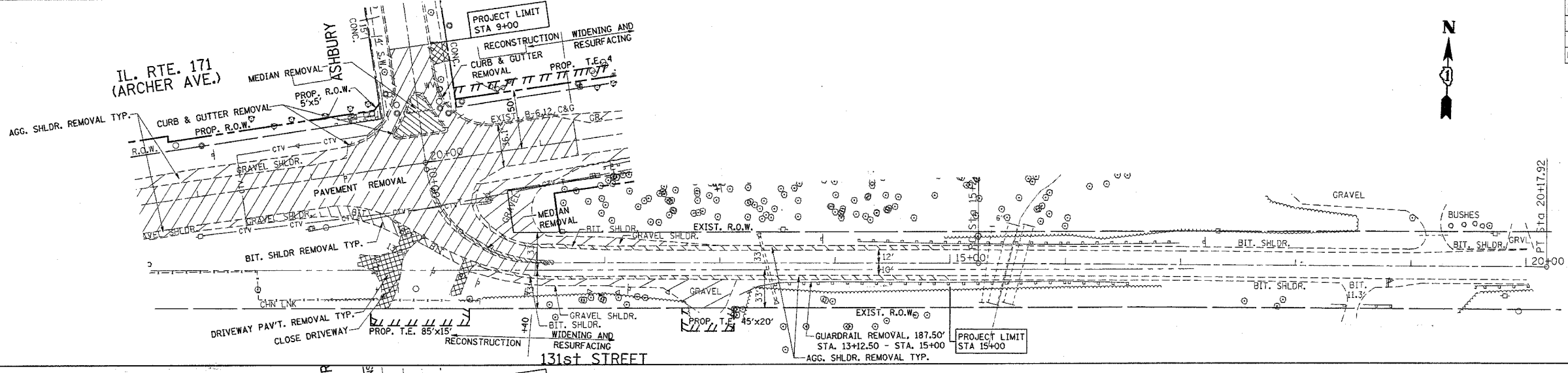
IL. RTE. 171 (ARCHER AVE.)
 AT ASHBURY DRIVE AND
 131st STREET
 EXISTING & PROPOSED
 ROADWAY PLAN
 SCALE: VERT. 1"=5' HORIZ. 1"=50' DRAWN BY: DATE: 4/27/2007

| DATE | BY | REVISION |
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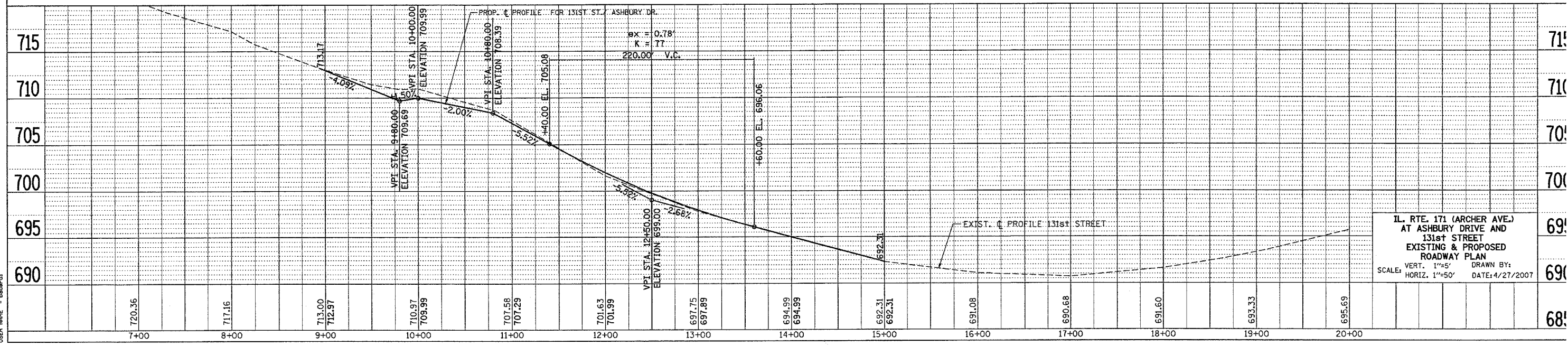
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PLOT DATE = 4/27/2007
 PLOT SCALE = 1"=50'
 USER NAME = bauerl

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 14 |
| STA. 9+00 | | TO STA. 18+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



* BITUMINOUS DRIVEWAY (PE)
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, (IL. 9.5 mm), 2"
HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19 mm), 6"



IL. RTE. 171 (ARCHER AVE.)
AT ASHBURY DRIVE AND
131st STREET
EXISTING & PROPOSED
ROADWAY PLAN
SCALE: VERT. 1"=5'
HORIZ. 1"=50'
DRAWN BY:
DATE: 4/27/2007

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PLOT DATE = 4/27/2007
PLOT SCALE = 50.0000 / IN.
USER NAME = bward1

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------|---------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 15 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| CONTRACT NO. 60A25 | | | | |

TRAFFIC CONTROL AND PROTECTION GENERAL NOTES

- THE SITE CONDITIONS AS WELL AS THE SUGGESTED TRAFFIC STAGING INVOLVES CONSTRICTED WORK AREAS ESPECIALLY DURING PAVING OPERATIONS. THE CONTRACTOR SHALL AVOID ENCROACHMENTS ON THE ADJACENT DRIVING LANES TO ENSURE PUBLIC SAFETY.
- THE TRAFFIC CONTROL PLANS AND THE CONSTRUCTION STAGING SHOWN FOR THIS CONTRACT SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. THE CONTRACTOR MAY IMPROVE OR MODIFY THE TRAFFIC CONTROL PLANS FOR CONSTRUCTION NEEDS. THE CONTRACTOR'S PROPOSED TRAFFIC CONTROL PLANS/REVISIONS SHALL BE SUBMITTED IN ADVANCE FOR THE APPROVAL OF THE ENGINEER.
- ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE STATE OF ILLINOIS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, AND SHALL BE IN PLACE BEFORE ANY WORK IS STARTED.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES MAY BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. EXISTING STOP SIGNS AND HIGHWAYS ALONG ILLINOIS 171 (ARCHER AVE) AND CROSS STREETS SHALL BE RELOCATED TO CONTROL THE TRAFFIC FOR THE VARIOUS STAGES OF CONSTRUCTION AS SHOWN ON THE STAGING PLANS OR AS DIRECTED BY THE ENGINEER. THE COST IS INCLUDED AS PART OF THE TRAFFIC CONTROL AND PROTECTION (SPECIAL) PAY ITEM.
- ALL DRIVEWAYS AND SIDEWALKS TO HOUSES AND BUSINESSES AND CROSSROADS SHALL REMAIN IN SERVICE THROUGHOUT THE LIFE OF THIS CONTRACT AND SHALL BE CONSTRUCTED IN STAGES, AS SHOWN ON THE CONSTRUCTION STAGING PLANS AND DISTRICT DETAILS FOR TEMPORARY ACCESS DRIVES AND SIDE STREETS. NO RESIDENTIAL OR COMMERCIAL DRIVEWAYS SHALL BE CLOSED OR REMOVE FROM SERVICE WITHOUT PRIOR APPROVAL OF ENGINEER. STORM SEWER SYSTEMS AND DITCHES SHALL BE MAINTAINED AT ALL TIMES EXCEPT AT PERIODS OF CONNECTING TO THE PROPOSED STORM SEWER LINES.
- ONE 11 FEET LANE (MIN. LANE WIDTH) IN EACH DIRECTION SHALL BE KEPT OPEN TO THROUGH TRAFFIC ALONG ILLINOIS 171 (ARCHER AVE) AND SIDE STREETS.
- THIS PROJECT INVOLVES CONSIDERABLE PUBLIC AND MUNICIPAL UTILITY RELOCATIONS AND OR ADJUSTMENTS. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES THROUGH J.U.L.I.E. AND COORDINATION WITH LOCAL MUNICIPAL AUTHORITY AND/OR PUBLIC UTILITY COMPANIES TO RELOCATE WATERMANS, FIRE HYDRANTS, GAS LINES, TELEPHONE LINES, ELECTRIC LINES, ETC. THAT ARE IN CONFLICT WITH THE PROPOSED STORM SEWERS AND PAVEMENT RECONSTRUCTION.
- THE CONTRACTOR SHALL NOT STORE EQUIPMENT, VEHICLES OR MATERIALS ON ADJACENT STREETS BEYOND THE PROJECT LIMIT WITHOUT SPECIFIC WRITTEN APPROVAL FROM THE ENGINEER.
- REMOVE EXISTING PAVEMENT MARKINGS IF IN CONFLICT WITH THE TEMPORARY PAVEMENT MARKINGS FOR TRAFFIC CONTROL AND PROTECTION PLANS. TEMPORARY PAVEMENT MARKINGS SHALL BE PROVIDED FOR ALL LOCATIONS AS SHOWN ON THE CONSTRUCTION STAGING PLANS OR AS DIRECTED BY THE ENGINEER. TEMPORARY PAVEMENT MARKING TAPES, TYPE III SHALL BE USED ON FINISHED SURFACES AND ON APPROACHES TO THE CONSTRUCTION AREA
- TAPER LENGTH FOR TRAFFIC CONTROL DEVICES IS DEFINED BY:

$$L = \frac{WS^2}{60}$$

WHERE L = TAPER LENGTH IN FEET
W = WIDTH OF OFFSET IN FEET
S = POSTED SPEED IN MPH
- ADEQUATE TURNING RADII FOR VEHICLES, INCLUDING TRUCKS AND BUSES SHALL BE MAINTAINED AT ALL INTERSECTIONS DURING CONSTRUCTION OPERATIONS.
- PAYMENT FOR TRAFFIC CONTROL AND PROTECTION (STAGES I, IA, II, & IIA) AND ALL OTHER STAGING OPERATIONS FOR THE ENTIRE CONTRACT WILL BE PAID FOR ON A LUMP BASIS AS "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".
- TEMPORARY CONCRETE BARRIERS AND EROSION CONTROL SYSTEM SHALL BE PLACED WHERE THE TRAVEL LANE IS ADJACENT TO A DROP OF 3 FEET OR GREATER AND AT LOCATIONS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE IN ACCORDANCE WITH SECTION 704 OF THE STANDARD SPECIFICATIONS. PAYMENT WILL BE ACCORDING TO APPLICABLE ITEMS IN THE BILL OF QUANTITIES.
- TEMPORARY PAVEMENT IS REQUIRED TO MAINTAIN THE REQUIRED TRAFFIC LANES ON IL 171 (ARCHER AVE) AND THE SIDE STREETS AS SHOWN ON THE STAGING PLANS. TEMPORARY PAVEMENT CONSISTS OF HMA BASE COURSE AND HMA SURFACE COURSE.
- THE CONTRACTOR WILL GIVE AT LEAST 48 HOURS PRIOR NOTICE TO THE VILLAGE ENGINEER OF LEMONT BEFORE CLOSING ANY SIDE STREET DURING CONSTRUCTION.
- THE CONTRACTOR WILL BE REQUIRED TO VERIFY/CONFIRM/LOCATE AND IDENTIFY ALL EXISTING UNDERGROUND UTILITY LOCATIONS PRIOR TO THE START OF CONSTRUCTION OPERATIONS.
- TRAFFIC SURVEILLANCE WILL BE REQUIRED FOR THIS CONTRACT AND WILL BE PAID FOR UNDER APPLICABLE PAY ITEMS.
- BARRICADES/DRUMS SPACING TO BE AS FOLLOWS:
TAPER = 25 FT. C-C SPACING
MAINLINE = 50 FT. C-C SPACING
DRIVEWAY RETURNS = 10 FT C-C SPACING
- ALL TEMPORARY PAVEMENT MARKINGS (PAINT AND OR TYPE III TAPE) WILL BE PAID FOR SEPARATELY ACCORDING TO THE APPLICABLE PAY ITEMS SHOWN IN THE SUMMARY OF QUANTITIES.
- VERTICAL PANELS WITH STEADY BURN LIGHT MAY BE SUBSTITUTED IN PLACE OF TYPE I OR TYPE II BARRICADES OR REFLECTORIZED DRUMS WHERE SPACE IS LIMITED.

STAGING SEQUENCE FOR IL. 171 (ARCHER AVE.)
(RECONSTRUCTION AREA)

PRESTAGE 1:

- INSTALL SIGNS SHOWN ON THE DETAIL " TEMPORARY INFORMATION SIGNING" PLACED PRIOR TO THE START OF CONSTRUCTION ACTIVITIES ON ILLINOIS 171 (ARCHER AVE), ASHBURY DRIVE AND 131ST STREET.
- REMOVE A PORTION OF THE CURB AND GUTTER ON THE EAST SIDE OF RAVINE DRIVE.
- CONSTRUCT TEMPORARY PAVEMENT FROM RAVINE DRIVE TO ASHBURY DRIVE AND FROM ASHBURY DRIVE TO IL. 171 (STA. 23+68.70).
- INSTALL TEMPORARY CONCRETE BARRIER WALL.
- ANY OPEN HOLES RESULTING FROM THE CONTRACTOR'S OPERATION SHALL BE COMPLETELY BACKFILLED TO TOP OF THE ADJACENT ROADWAY SURFACE ELEVATION WHENEVER THE CONTRACTOR IS NOT WORKING. PRIOR TO OPENING THE ROADWAY TO TRAFFIC, TEMPORARY PAVEMENT SHALL BE INSTALLED IN THE TRENCHES. ALL MAJOR INTERSECTIONS MUST BE KEPT OPEN AT ALL TIMES.

STAGE I:
(TRAFFIC ON THE NORTH SIDE OF IL. 171 PAVEMENT)

- REMOVE SOUTH PORTION OF PAVEMENT AT THE EAST END OF THE JOB AT STA. 12+30 AND PROCEED EASTERLY TOWARDS STA. 19+14 AND CONTINUE FROM 131ST STREET INTERSECTION TO STA. 21+31.
- REMOVE SOUTHEAST PORTION OF PAVEMENT ON 131ST STREET FROM STA. 10+00 TO STA. 11+40 SOUTHEASTERLY OF THE INTERSECTION.
- INSTALL/ERECT TEMPORARY EROSION CONTROL SYSTEM RIGHT UPON COMPLETION OF PAVEMENT REMOVAL ON THE LOCATIONS SPECIFIED ABOVE.
- REMOVE SOUTHWEST PORTION OF PAVEMENT ON 131ST STREET FROM STA. 10+00 TO STA. 11+40 SOUTHEASTERLY OF THE INTERSECTION.
- CONSTRUCT SOUTH SIDE DRAINAGE SYSTEM STARTING AT THE WEST LIMITS OF CONSTRUCTION PORTION OF PROJECT.
- CONSTRUCT SOUTH SIDE PAVEMENT AND RELATED ITEMS.
- CONSTRUCT TEMPORARY PAVEMENT PRIOR IN SWITCHING TRAFFIC TO THE SOUTH SIDE OF IL 171.
- THE CONTRACTOR WILL BE REQUIRED TO MAINTAINED ACCESS TO ALL DRIVEWAYS 18 FT OR WIDER.
- ADDITIONAL SUBSTAGING NOT SHOWN WILL BE REQUIRED TO MAINTAINED TRAFFIC AT THE CROSS STREETS AND DRIVEWAYS.

STAGE II:
(TRAFFIC ON THE SOUTH SIDE OF IL. 171 PAVEMENT)

- RELOCATE TEMPORARY CONCRETE BARRIER WALL.
- REMOVE NORTH PORTIONS OF THE EXISTING PAVEMENT OF IL 171 FROM STA. 12+30 TO STA. 19+30 AND FROM ASHBURY DR TO STA.21+31
- REMOVE EAST PORTIONS OF THE EXISTING ISLAND AND PAVEMENT ON ASHBURY DRIVE.
- CONSTRUCT THE REMAINING PORTIONS OF DRAINAGE SYSTEM ON THE NORTH SIDE STARTING AT THE EAST LIMITS OF THE RECONSTRUCTION PORTION OF PROJECT (STA. 12+30). REMOVE AND OR PLUG REMAINING PORTIONS OF THE EXISTING DRAINAGE SYSTEM AS IT BECOMES ABANDONED.
- CONSTRUCT THE NORTH SIDE PAVEMENT AND RELATED ITEMS. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN ACCESS TO ALL DRIVEWAYS 18 FT OR WIDER.

- SWITCH TRAFFIC AFTER COMPLETING THE EAST LEG OF THE INTERSECTION OF ASHBURY DR. AND IL 171. THEREAFTER FINISHING THE REMAINING PORTION OF THE INTERSECTION.
- ADDITIONAL SUBSTAGING NOT SHOWN WILL BE REQUIRED TO MAINTAIN TRAFFIC AT THE CROSS STREET AND DRIVEWAYS.

STAGE III:
(TRAFFIC ON THE SOUTH SIDE OF IL. 171 PAVEMENT)

- CONSTRUCT THE RIGHT IN RIGHT OUT ISLAND ON ASHBURY DRIVE.
- REMOVE TEMPORARY PAVEMENT ON THE NORTH SIDE AND CONSTRUCT PC CONCRETE CURB AND GUTTER.
- CONSTRUCT BITUMINOUS AND AGGREGATE SHOULDERS AND RELATED ITEMS ON THE NORTH SIDE OF IL 171.
- REMOVE TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY PAVEMENT ON THE SOUTH SIDE OF IL 171 UPON COMPLETION OF STAGE III.
- CONSTRUCT BITUMINOUS SHOULDER AND AGGREGATE SHOULDER AND RELATED ITEMS ON THE SOUTH SIDE OF IL. 171.

STAGE IV:
(TRAFFIC ON NEWLY CONSTRUCTED PAVEMENT)

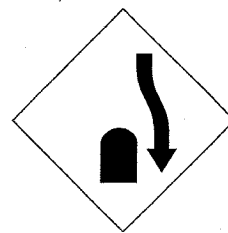
- COMPLETE ALL MISCELLANEOUS WORK ITEMS USING DAYTIME LANE CLOSURES.

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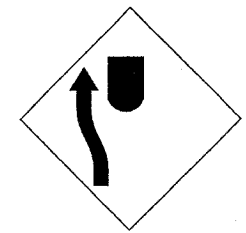
ILLINOIS DEPARTMENT OF TRANSPORTATION
ILL 171 (ARCHER AVE)
AT ASHBURY DR & 131ST STREET
STAGING SEQUENCE AND
GENERAL NOTES

DRAWN BY
CHECKED BY

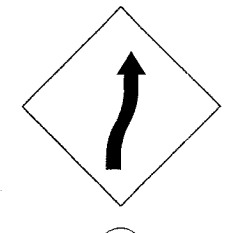
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USER NAME = galbreth



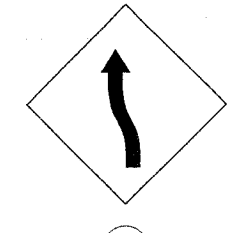
①
W 6-1
(36 x 36 in)



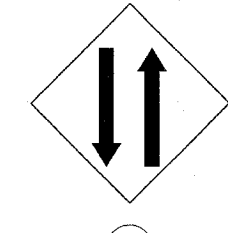
②
W 6-1
(36 x 36 in)



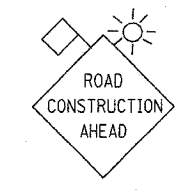
③
W- 14aR
(48 x 48 in)



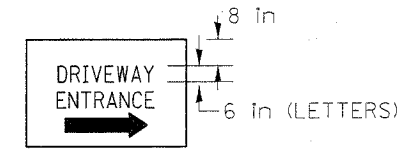
④
W- 14dL
(48 x 48 in)



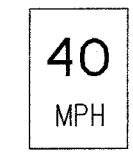
⑤
W 6-3
(48 x 48 in)



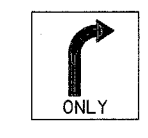
⑥
W 20-1
48 x 48 in



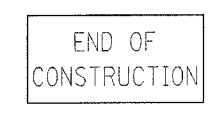
⑦
BLACK ON WHITE
REFLECTIVE BACK
GROUND, 1/2" BORDER



⑧
W13-1
(24 x 24 in)



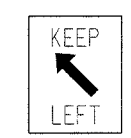
⑨
R3-5
30 x 36 in
RIGHT TURN ONLY



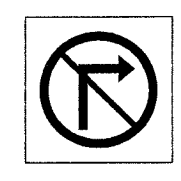
⑩
G 20-2
60 x 24 in



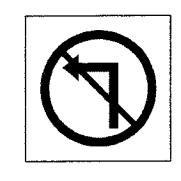
⑪
R4-7b
24 x 24 in



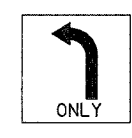
⑫
R4-7b
24 x 24 in



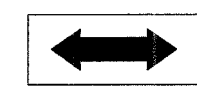
⑬
R3-1
(24 x 24 in)
NO RIGHT TURN



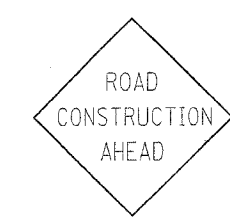
⑭
R3-2
(24 x 24 in)
NO LEFT TURN



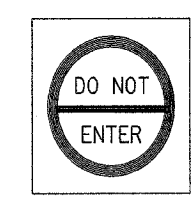
⑮
R3-5
30 x 36 in
LEFT TURN ONLY



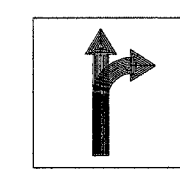
⑯
W1-7
48 x 24 in



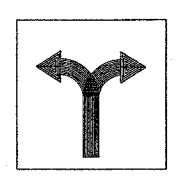
⑰
W20-1(0)
(48 x 48 in)



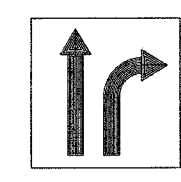
⑱
W 6-3
(48 x 48 in)



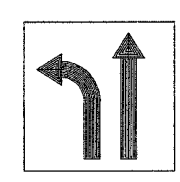
⑲
R3-6
30 x 36 in
STRAIGHT RIGHT TURN ARROW



⑳
R3-6c
30 x 36 in
RIGHT/LEFT TURN ARROW



㉑
R3-6a
30 x 36 in
RIGHT TURN THRU STAGE



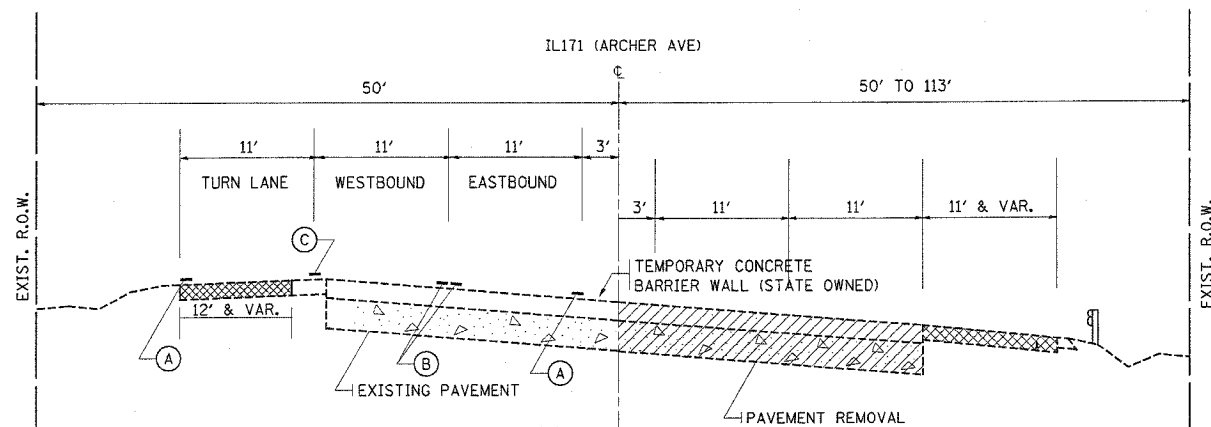
㉒
R3-6b
30 x 36 in
LEFT TURN THRU STAGE

PLOT DATE = 5/2/2007
 PLOT SCALE = 1/8" = 1'-0"
 USER NAME = galbamb

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
ILL 171 (ARCHER AVE)
SIGN LEGEND /DETAILS
 FOR
SUGGESTED STAGES OF CONSTRUCTION
AND TRAFFIC CONTROL
 DRAWN BY
 CHECKED BY

| | | | | |
|--------------------|----------|---|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 17 |
| STA. TO STA. | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 60A25 | | | | |



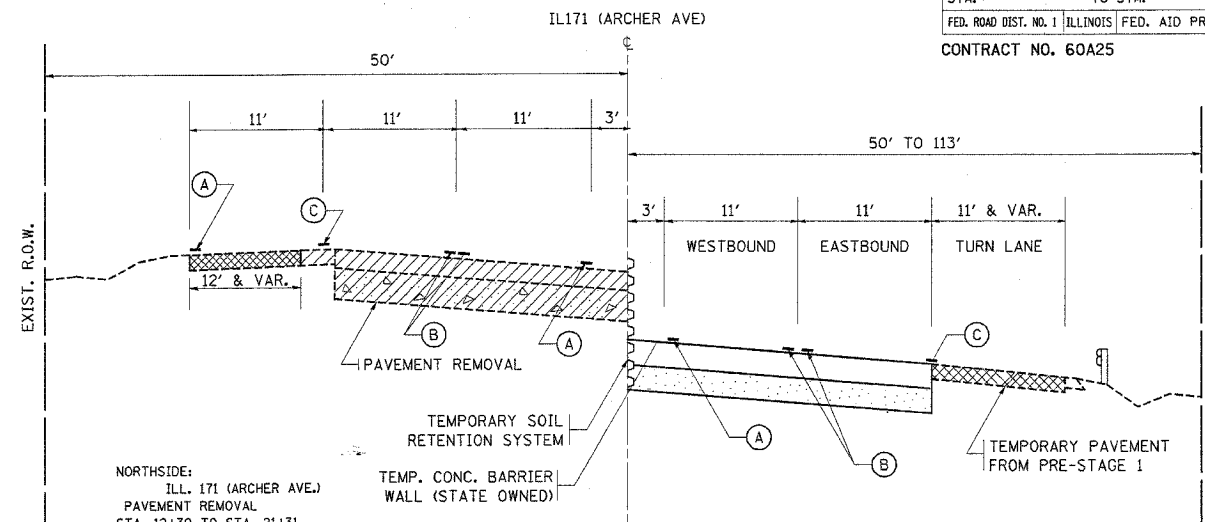
NORTHSIDE:
ILL. 171 (ARCHER AVE.)
TEMPORARY PAVEMENT (12' & VAR.)
RAVINE DR. (RR) TO STA. 17+53 &
ASHBURY DR. TO STA. 23+69

**ILL 171 (ARCHER AVE)
PRE-STAGE 1
STA 11+14 TO STA 28+00**

SOUTHSIDE:
ILL. 171 (ARCHER AVE.)
PAVEMENT REMOVAL
STA. 12+30 TO STA. 21+31
131ST STREET
PAVEMENT REMOVAL
STA. 10+00 TO STA 11+40

LEGEND

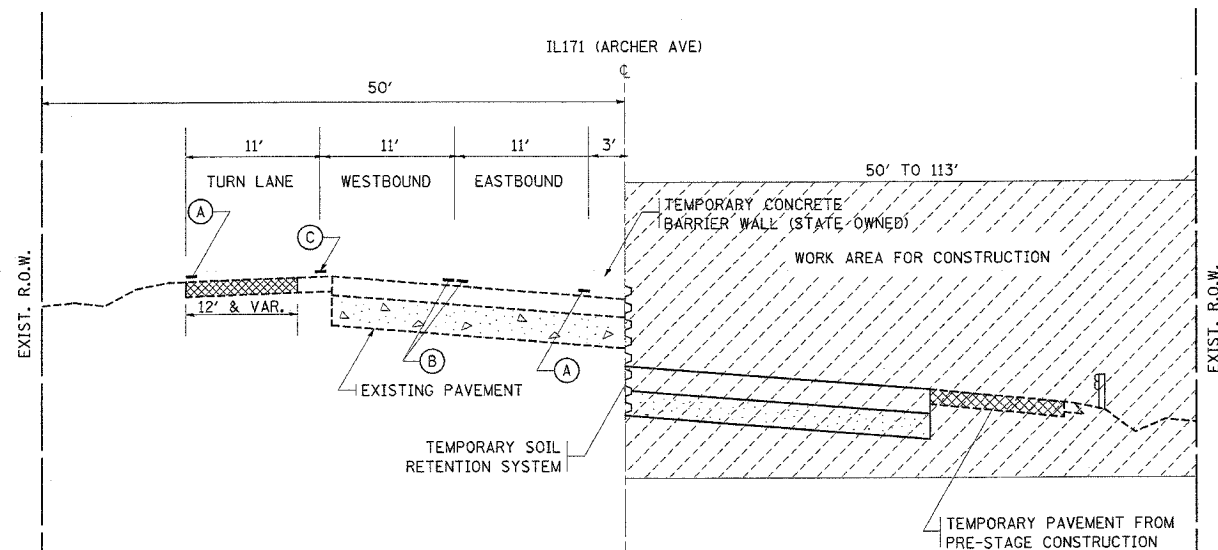
- (A) TEMPORARY PAVEMNET MARKING, TYPE 111
4" SOLID WHITE EDGE LINE
- (B) TEMPORARY PAVEMENT MARKING, TYPE 111
4" SOLID DOUBLE YELLOW
- (C) TEMPORARY PAVEMENT MARKING, TYPE 111
6" SOLID WHITE (TURN LANE)
- PAVEMENT REMOVAL
- TEMPORARY PAVEMENT, 10"
- WORK ZONE



NORTHSIDE:
ILL. 171 (ARCHER AVE.)
PAVEMENT REMOVAL
STA. 12+30 TO STA. 21+31
ASHBURY DRIVE
PAVEMENT REMOVAL
STA. 09+00 TO STA. 10+00

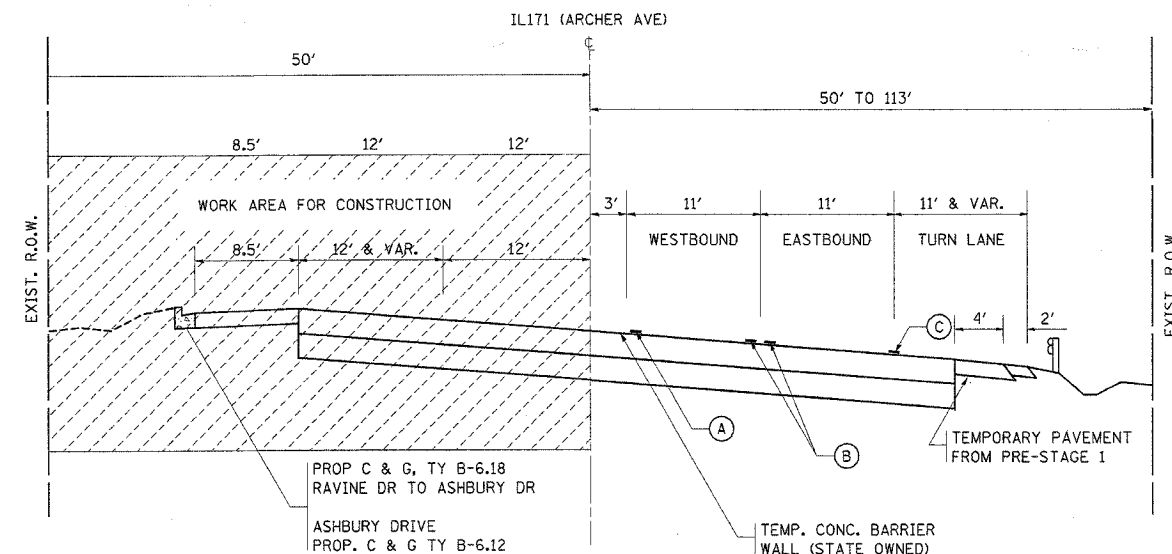
**ILL 171 (ARCHER AVE)
PRE-STAGE 1A
STA. 11+14 TO STA 28+00**

SOUTHSIDE:
ILL. 171 (ARCHER AVE.)
PAVEMENT REMOVAL
STA. 12+30 TO STA. 21+ 31
131ST STREET
PAVEMENT REMOVAL
IL. 171 (EOP) TO STA. 11+40



NORTHSIDE:
ILL. 171 (ARCHER AVE.)
TEMPORARY PAVEMENT (12' & VAR.)
RAVINE DR. (RR) TO STA. 17+53 &
ASHBURY DR. TO STA. 23+69

**ILL 171 (ARCHER AVE)
STAGE 1 AND STAGE 1A CONSTRUCTION
STA. 11+14 TO STA 28+00**



PROP C & G, TY B-6.18
RAVINE DR TO ASHBURY DR
ASHBURY DRIVE
PROP. C & G TY B-6.12

**ILL 171 (ARCHER AVE)
STAGE II AND IIA CONSTRUCTION
STA. 11+14 TO STA 28+00**

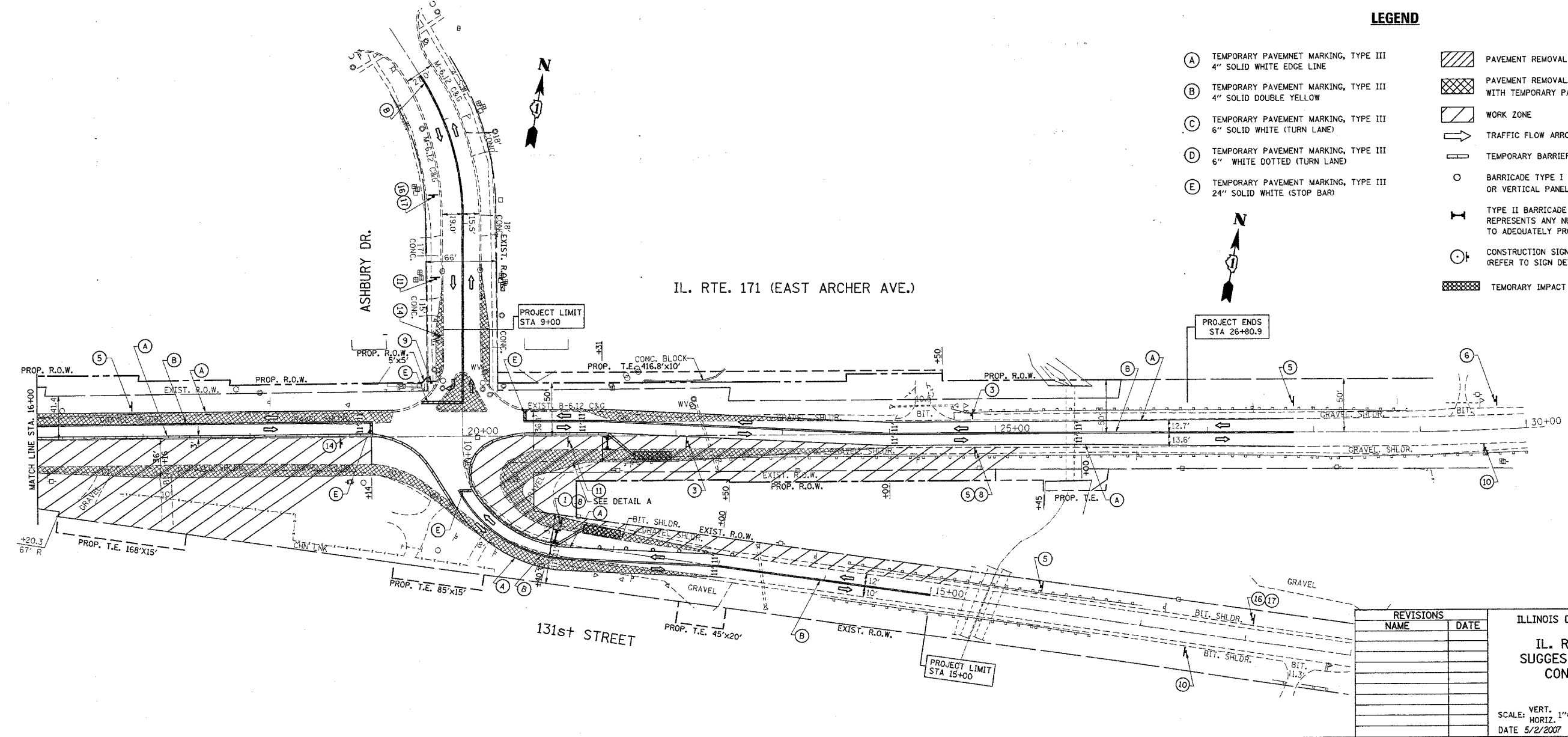
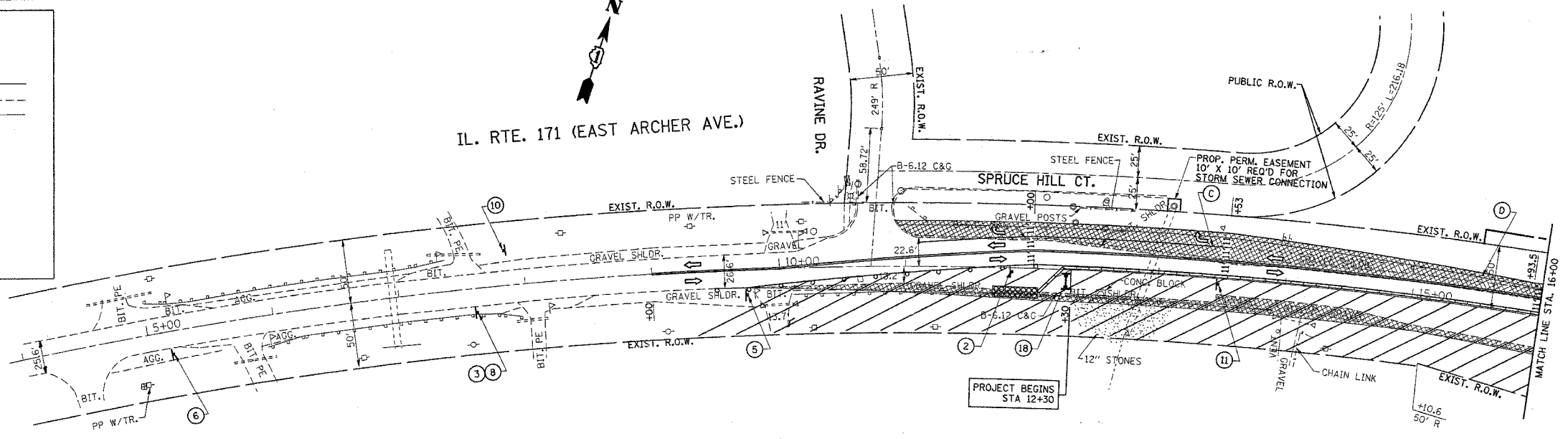
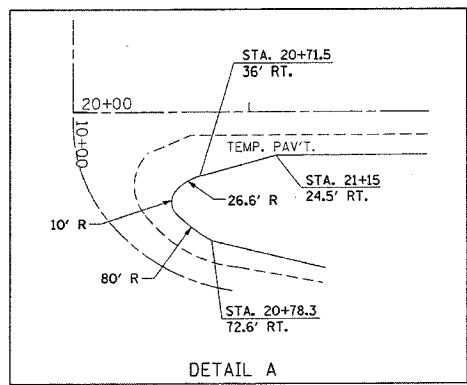
| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
**ILL 171 (ARCHER AVE)
AT ASHBURY DR & 131ST STREET**
**TYPICAL SECTIONS FOR
MAINTENANCE OF TRAFFIC**

SCALE: VERT. DRAWN BY
 HORIZ. CHECKED BY
DATE

PLOT DATE = 5/2/2007
PLOT SCALE = 500.000' / IN.
USER NAME = galbennb

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|---------------------|----------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 18 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



LEGEND

- (A) TEMPORARY PAVEMNT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
- (B) TEMPORARY PAVEMNT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
- (C) TEMPORARY PAVEMNT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
- (D) TEMPORARY PAVEMNT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
- (E) TEMPORARY PAVEMNT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
- [Hatched Box] PAVEMENT REMOVAL
- [Cross-hatched Box] PAVEMENT REMOVAL/MEDIAN REMOVAL, REPLACEMENT WITH TEMPORARY PAVEMENT, 10'
- [Diagonal Lines] WORK ZONE
- [Arrow] TRAFFIC FLOW ARROW
- [Barrier Symbol] TEMPORARY BARRIER WALL (STATE OWNED)
- [Barricade Symbol] BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT
- [Barricade Symbol] TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
- [Sign Symbol] CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)
- [Dashed Box] TEMPORARY IMPACT ATTENUATOR

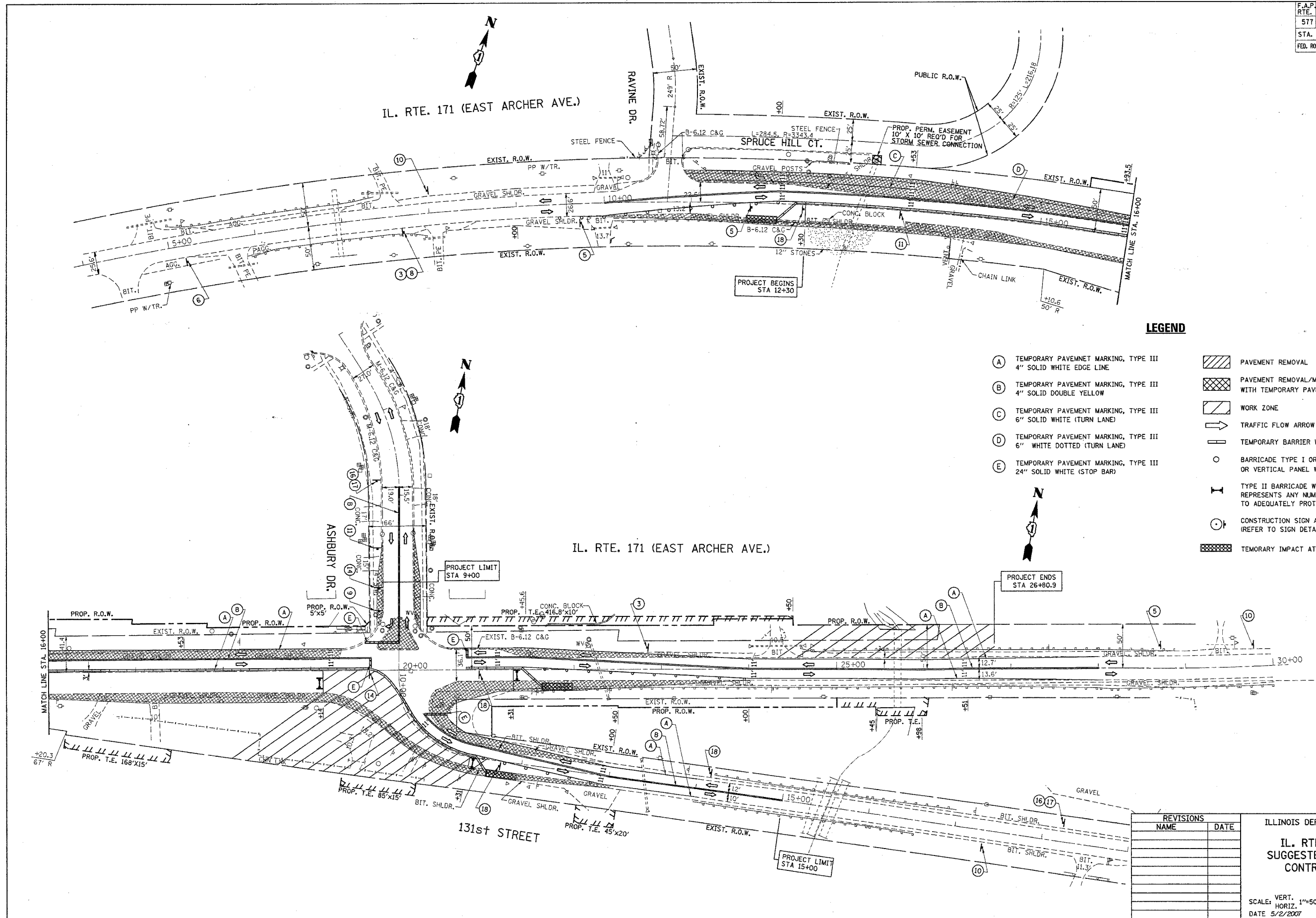
| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 IL. RTE. 171 (ARCHER AVE.)
 SUSTAINED STAGING & TRAFFIC CONTROL PLAN: STAGE I

VERT. SCALE: 1"=50'
 HORIZ. SCALE: 1"=50'
 DATE 5/2/2007
 DRAWN BY
 CHECKED BY

PLOT DATE = 5/2/2007
 FILE NAME = c:\projects\60a25\stage1.dgn
 USER NAME = gsg12007

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 19 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



- LEGEND**
- (A) TEMPORARY PAVENMET MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - (B) TEMPORARY PAVENMET MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - (C) TEMPORARY PAVENMET MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - (D) TEMPORARY PAVENMET MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - (E) TEMPORARY PAVENMET MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
- [Hatched Box] PAVEMENT REMOVAL
 - [Cross-hatched Box] PAVEMENT REMOVAL/MEDIAN REMOVAL, REPLACEMENT WITH TEMPORARY PAVEMENT, 10"
 - [Diagonal Line Box] WORK ZONE
 - [Arrow] TRAFFIC FLOW ARROW
 - [Barrier Wall Symbol] TEMPORARY BARRIER WALL (STATE OWNED)
 - [Barricade Symbol] BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT
 - [Barricade Symbol] TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
 - [Sign Symbol] CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)
 - [Impact Attenuator Symbol] TEMPORARY IMPACT ATTENUATOR

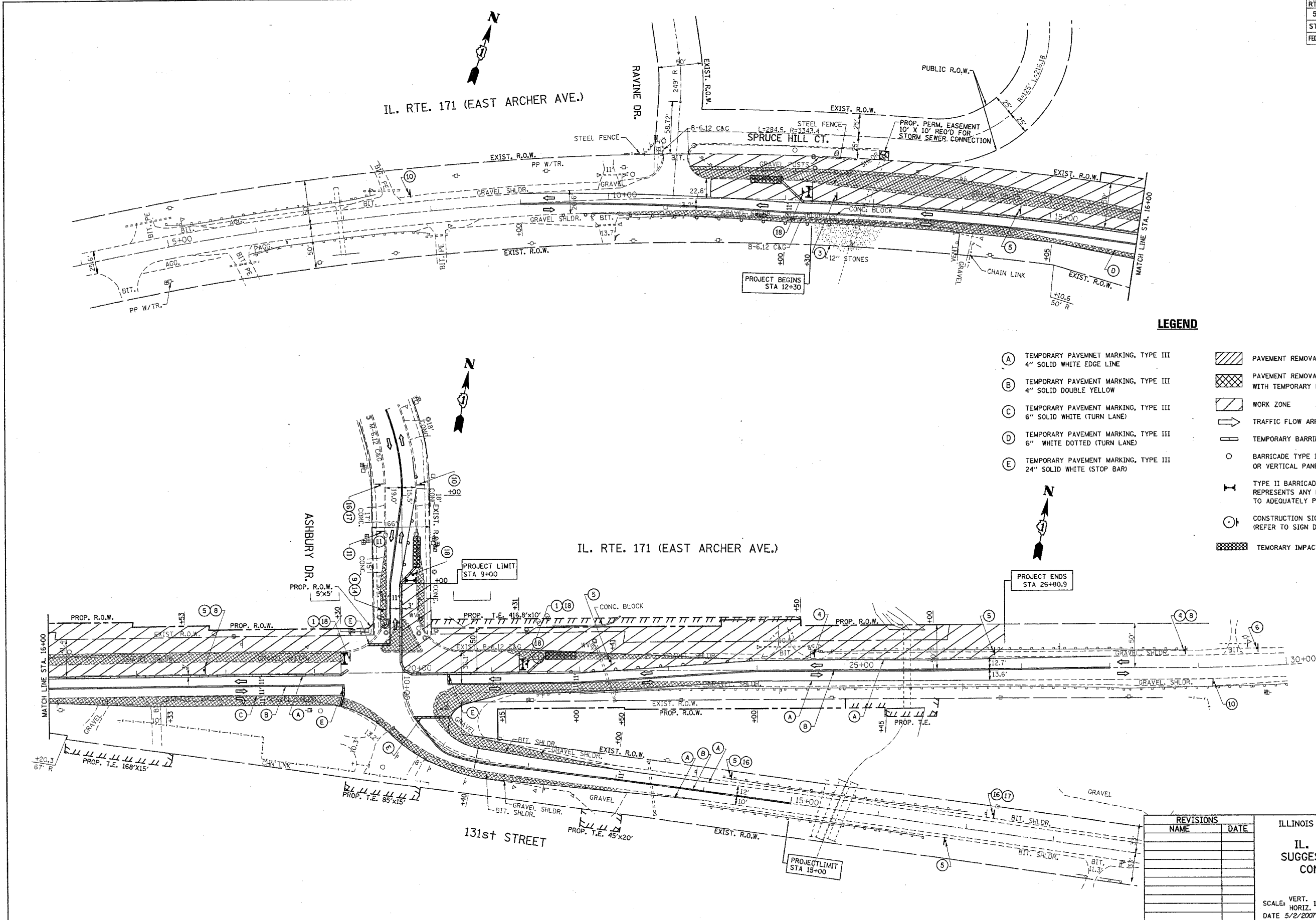
PLOT DATE = 5/2/2007
 FILE NAME = c:\proje\mva\p13882\shstage1a.dgn
 PLOT SCALE = 50.00000 / IN.
 USER NAME = gagliardi

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 IL. RTE. 171 (ARCHER AVE.)
 SUGGESTED STAGING & TRAFFIC CONTROL PLAN: STAGE IA

SCALE: VERT. 1"=50'
 DATE 5/2/2007
 DRAWN BY
 CHECKED BY

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 20 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



LEGEND

- (A) TEMPORARY PAVEMNET MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - (B) TEMPORARY PAVEMNET MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - (C) TEMPORARY PAVEMNET MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - (D) TEMPORARY PAVEMNET MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - (E) TEMPORARY PAVEMNET MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
- PAVEMENT REMOVAL
 - PAVEMENT REMOVAL/MEDIAN REMOVAL, REPLACEMENT WITH TEMPORARY PAVEMENT, 10"
 - WORK ZONE
 - TRAFFIC FLOW ARROW
 - TEMPORARY BARRIER WALL (STATE OWNED)
 - BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT
 - TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
 - CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)
 - TEMPORARY IMPACT ATTENUATOR

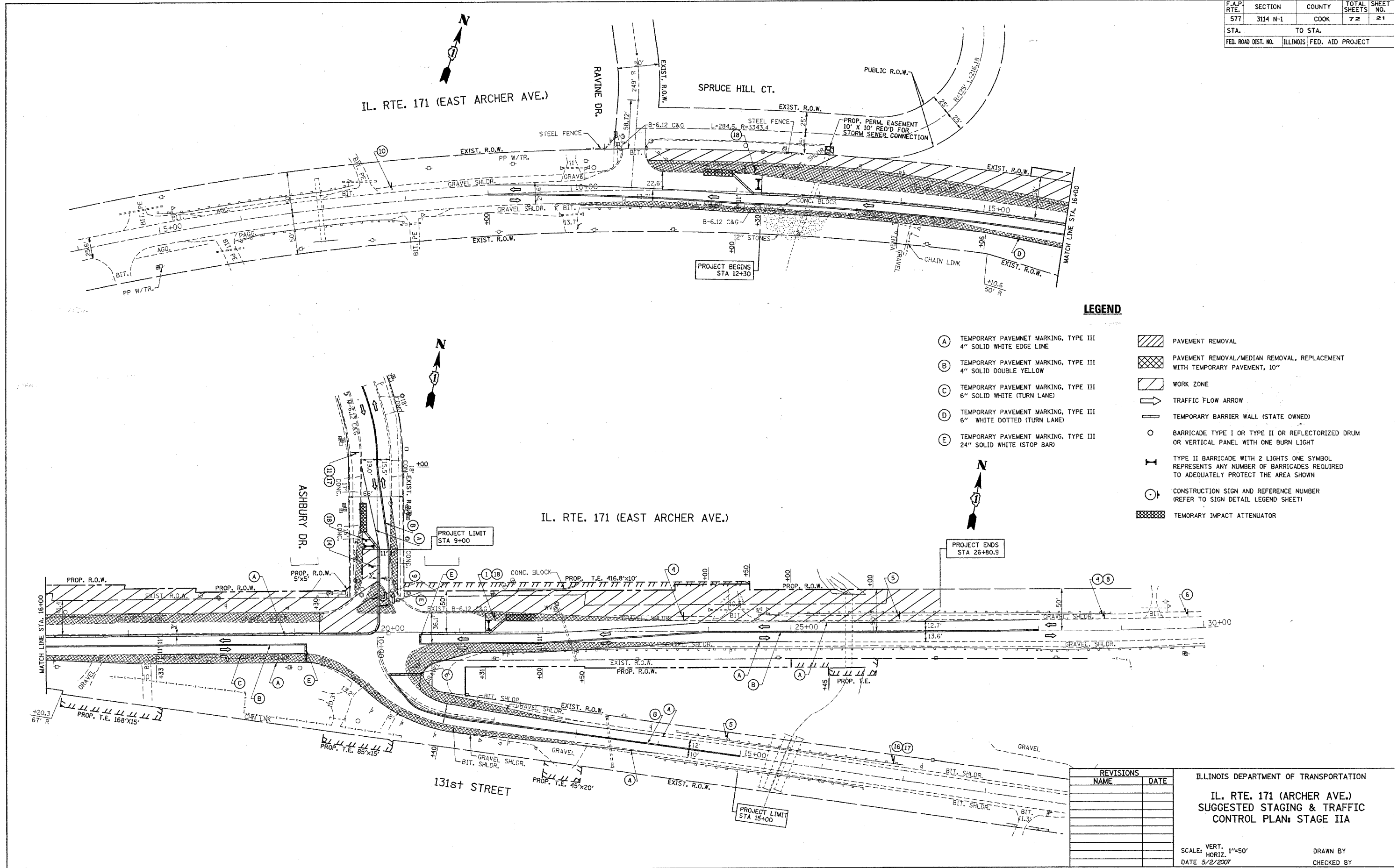
PLOT DATE = 5/2/2007
 FILE NAME = c:\pvc\j\mets\pl13882\ah_stage2.dgn
 PLOT SCALE = 0.0000 / 1" = 50'
 USER NAME = gsg10end05

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ILLINOIS DEPARTMENT OF TRANSPORTATION
IL. RTE. 171 (ARCHER AVE.)
SUGGESTED STAGING & TRAFFIC CONTROL PLAN: STAGE II

SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE 5/2/2007
 DRAWN BY
 CHECKED BY

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 21 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



LEGEND

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - (E) TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
- [Hatched Box] PAVEMENT REMOVAL
 - [Cross-hatched Box] PAVEMENT REMOVAL/MEDIAN REMOVAL, REPLACEMENT WITH TEMPORARY PAVEMENT, 10"
 - [Diagonal Line Box] WORK ZONE
 - [Arrow] TRAFFIC FLOW ARROW
 - [Barrier Wall Symbol] TEMPORARY BARRIER WALL (STATE OWNED)
 - [Barricade Symbol] BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT
 - [Barricade with Lights Symbol] TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
 - [Sign Symbol] CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)
 - [Impact Attenuator Symbol] TEMPORARY IMPACT ATTENUATOR

DATE = 5/2/2007
 FILE NAME = c:\p01\stg\113602\stg-2a.dgn
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = gaganob

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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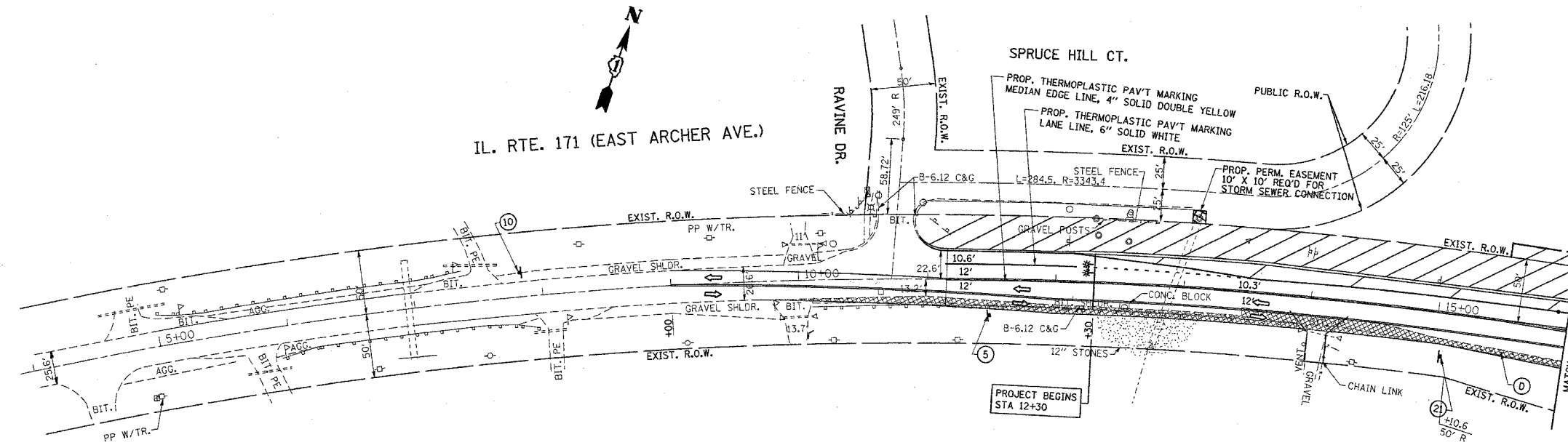
ILLINOIS DEPARTMENT OF TRANSPORTATION

**IL. RTE. 171 (ARCHER AVE.)
SUGGESTED STAGING & TRAFFIC CONTROL PLAN: STAGE IIA**

SCALE: VERT. 1"=50'
HORIZ. 1"=50'
DATE 5/2/2007

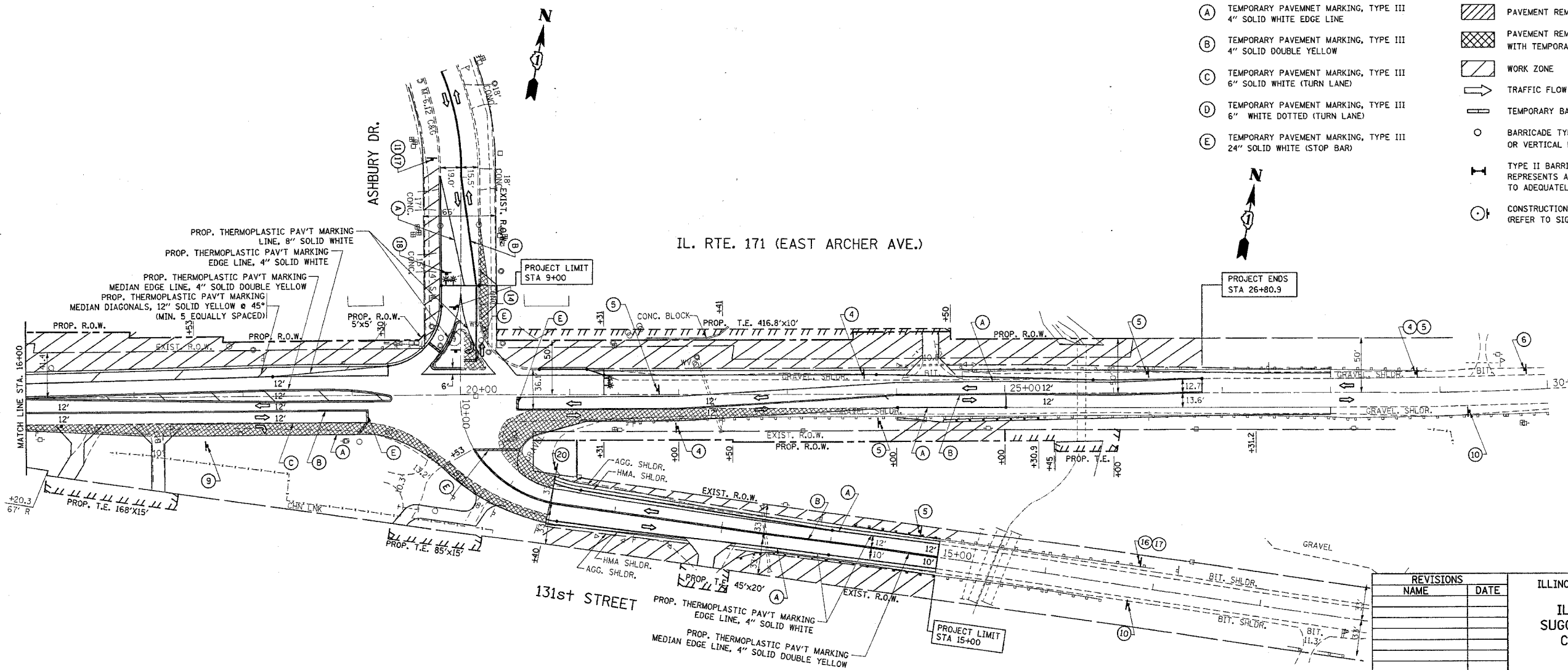
DRAWN BY
CHECKED BY

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 22 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



LEGEND

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - (E) TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
- [Hatched Box] PAVEMENT REMOVAL
 - [Cross-hatched Box] PAVEMENT REMOVAL/MEDIAN REMOVAL, REPLACEMENT WITH TEMPORARY PAVEMENT, 10"
 - [Diagonal Line Box] WORK ZONE
 - [Arrow] TRAFFIC FLOW ARROW
 - [Vertical Line] TEMPORARY BARRIER WALL (STATE OWNED)
 - [Circle with X] BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT
 - [T-shaped Symbol] TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
 - [Circle with Number] CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)



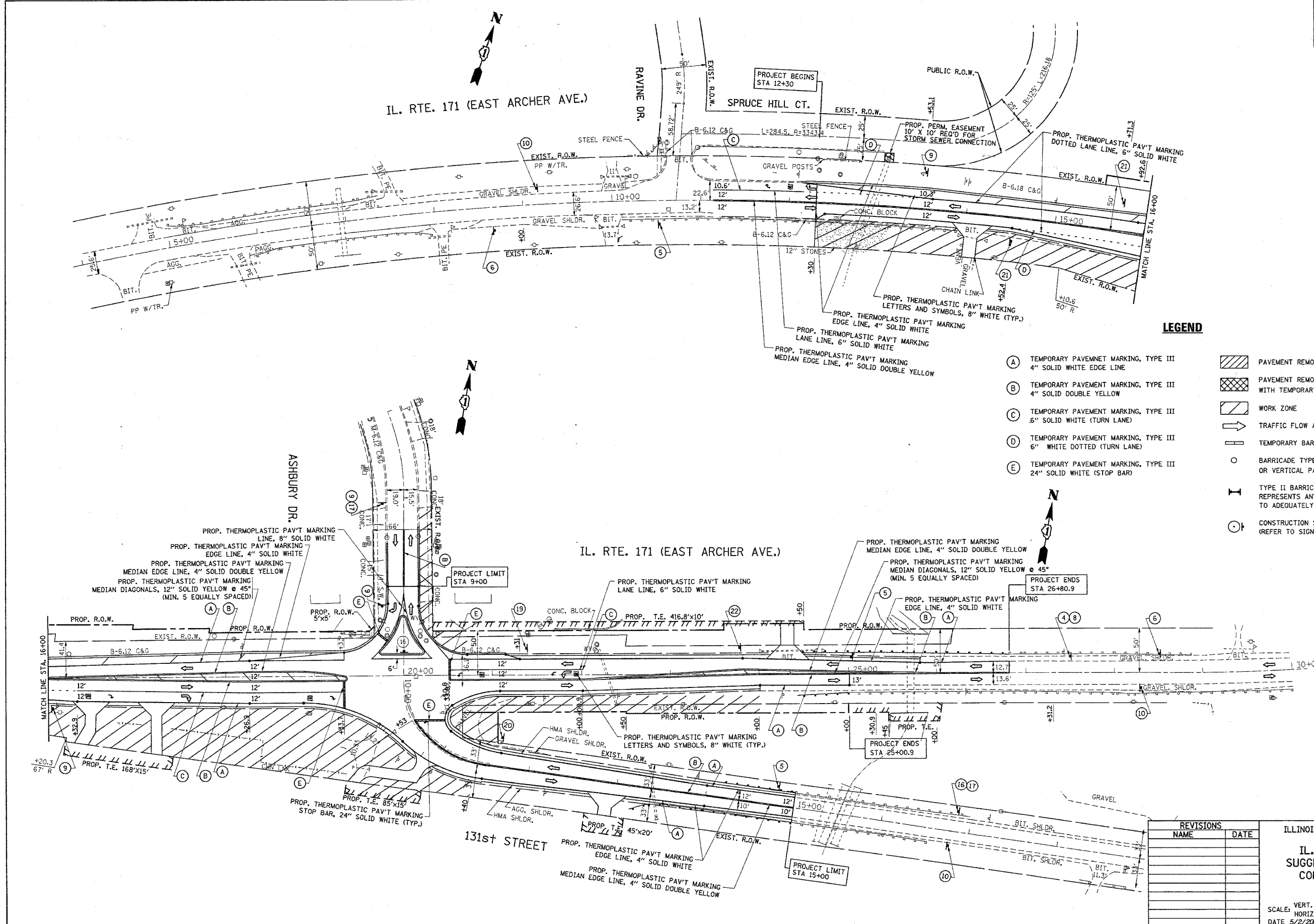
| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
**IL. RTE. 171 (ARCHER AVE.)
 SUGGESTED STAGING & TRAFFIC
 CONTROL PLAN: STAGE III**

SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE 5/2/2007
 DRAWN BY
 CHECKED BY

PLT DATE = 5/2/2007
 PLOT SCALE = 50/2000 / IN.
 USER NAME = gaglianobst

| | | | | |
|---------------------|----------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 23 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



LEGEND

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - (E) TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
- PAVEMENT REMOVAL
 - PAVEMENT REMOVAL/MEDIAN REMOVAL, REPLACEMENT WITH TEMPORARY PAVEMENT, 10'
 - WORK ZONE
 - TRAFFIC FLOW ARROW
 - TEMPORARY BARRIAGE WALL (STATE OWNED)
 - BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT
 - TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
 - CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)

PLOT DATE = 5/27/2007
 FILE NAME = c:\pva\pva\131892\111\stage_3a.dgn
 PLOT SCALE = 50.0000 / 1" IN.
 USER NAME = gmg\jgmbst

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

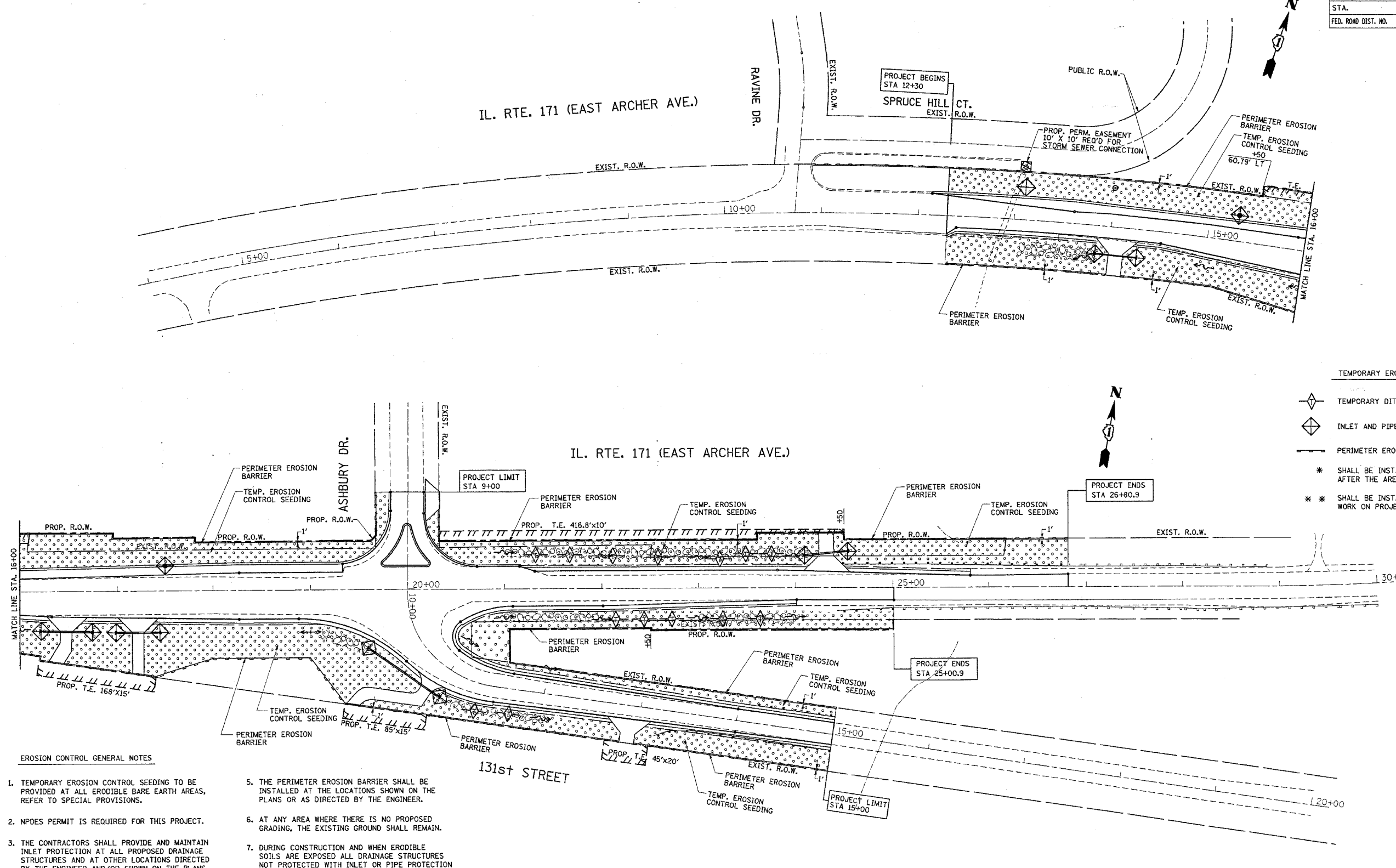
**IL. RTE. 171 (ARCHER AVE.)
SUGGESTED STAGING & TRAFFIC
CONTROL PLAN: STAGE IIIA**

SCALE: VERT. 1"=50'
HORIZ. 1"=50'

DATE 5/2/2007

DRAWN BY _____
CHECKED BY _____

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 24 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



TEMPORARY EROSION CONTROL LEGEND

- TEMPORARY DITCH CHECK *
- INLET AND PIPE PROTECTION *
- PERIMETER EROSION BARRIER
- * SHALL BE INSTALLED IMMEDIATELY AFTER THE AREA IS DISTURBED
- ** SHALL BE INSTALLED BEFORE ANY WORK ON PROJECT CAN BEGIN

EROSION CONTROL GENERAL NOTES

1. TEMPORARY EROSION CONTROL SEEDING TO BE PROVIDED AT ALL ERODIBLE BARE EARTH AREAS, REFER TO SPECIAL PROVISIONS.
2. NPDES PERMIT IS REQUIRED FOR THIS PROJECT.
3. THE CONTRACTORS SHALL PROVIDE AND MAINTAIN INLET PROTECTION AT ALL PROPOSED DRAINAGE STRUCTURES AND AT OTHER LOCATIONS DIRECTED BY THE ENGINEER AND/OR SHOWN ON THE PLANS. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE AREA INVOLVED HAS BEEN DISTURBED.
4. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND OF THE APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT, OR UNTIL SUCH A TIME AS DIRECTED BY THE ENGINEER.
5. THE PERIMETER EROSION BARRIER SHALL BE INSTALLED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
6. AT ANY AREA WHERE THERE IS NO PROPOSED GRADING, THE EXISTING GROUND SHALL REMAIN.
7. DURING CONSTRUCTION AND WHEN ERODIBLE SOILS ARE EXPOSED ALL DRAINAGE STRUCTURES NOT PROTECTED WITH INLET OR PIPE PROTECTION SHALL BE KEPT SEALED TO PREVENT ACCESS FROM EROSION. THIS WORK SHALL BE CONSIDERED AS PART OF THE COST OF THE VARIOUS DRAINAGE STRUCTURES.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
**IL. RTE. 171 (ARCHER AVE.)
 AT ASHBURY DRIVE AND
 131st STREET
 EROSION CONTROL PLAN**

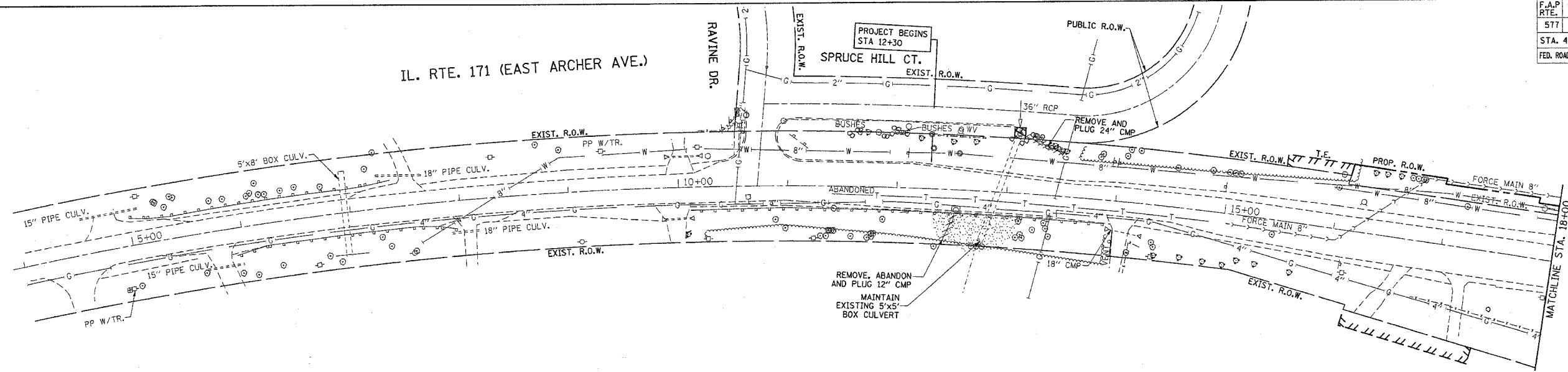
SCALE: VERT. _____
 HORIZ. _____
 DATE _____

DRAWN BY _____
 CHECKED BY _____

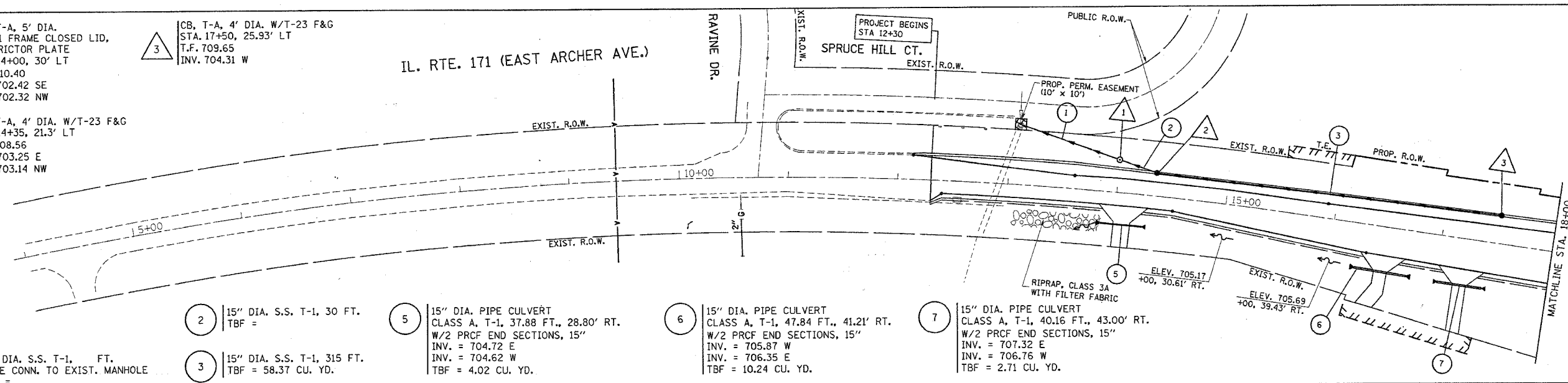
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 PLOT SCALE = 50.0000' / 1" IN.
 USER NAME = jhrick

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 25 |
| STA. 4+00 | | TO STA. 18+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

IL. RTE. 171 (EAST ARCHER AVE.)

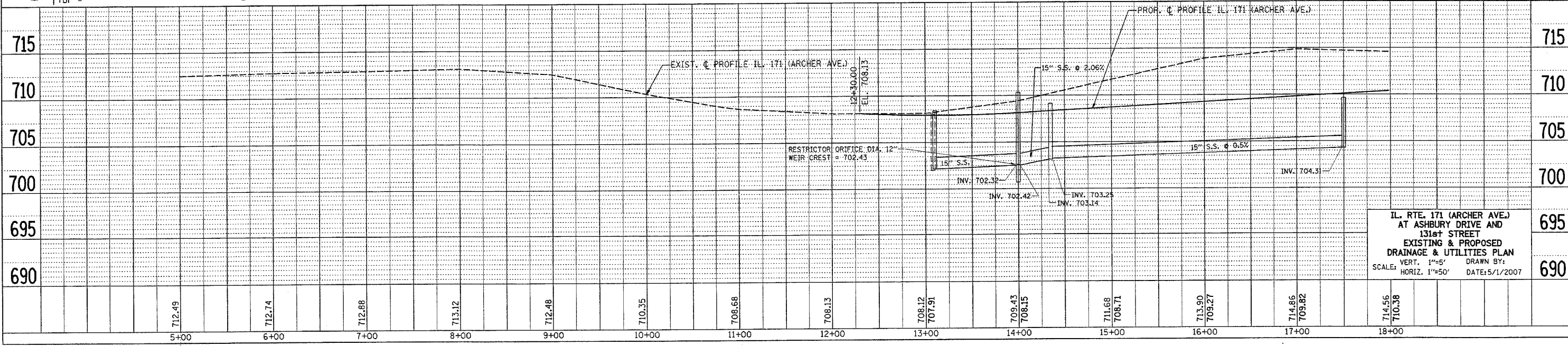


IL. RTE. 171 (EAST ARCHER AVE.)



- 1 MH, T-A, 5' DIA. W/T-1 FRAME CLOSED LID, RESTRICTOR PLATE STA. 14+00, 30' LT T.F. 710.40 INV. 702.42 SE INV. 702.32 NW
- 2 CB, T-A, 4' DIA. W/T-23 F&G STA. 14+35, 21.3' LT T.F. 708.56 INV. 703.25 E INV. 703.14 NW
- 3 CB, T-A, 4' DIA. W/T-23 F&G STA. 17+50, 25.93' LT T.F. 709.65 INV. 704.31 W

- 1 15" DIA. S.S. T-1, FT. MAKE CONN. TO EXIST. MANHOLE TBF =
- 2 15" DIA. S.S. T-1, 30 FT. TBF =
- 3 15" DIA. S.S. T-1, 315 FT. TBF = 58.37 CU. YD.
- 5 15" DIA. PIPE CULVERT CLASS A, T-1, 37.88 FT., 28.80' RT. W/2 PRCF END SECTIONS, 15" INV. = 704.72 E INV. = 704.62 W TBF = 4.02 CU. YD.
- 6 15" DIA. PIPE CULVERT CLASS A, T-1, 47.84 FT., 41.21' RT. W/2 PRCF END SECTIONS, 15" INV. = 705.87 W INV. = 706.35 E TBF = 10.24 CU. YD.
- 7 15" DIA. PIPE CULVERT CLASS A, T-1, 40.16 FT., 43.00' RT. W/2 PRCF END SECTIONS, 15" INV. = 707.32 E INV. = 706.76 W TBF = 2.71 CU. YD.



IL. RTE. 171 (ARCHER AVE.) AT ASHBURY DRIVE AND 131st STREET EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN SCALE: VERT. 1"=5' HORIZ. 1"=50' DRAWN BY: DATE: 5/1/2007

PLAN

| | |
|----------|--|
| DATE | |
| BY | |
| REVIEWED | |
| NOTED | |
| NO. | |

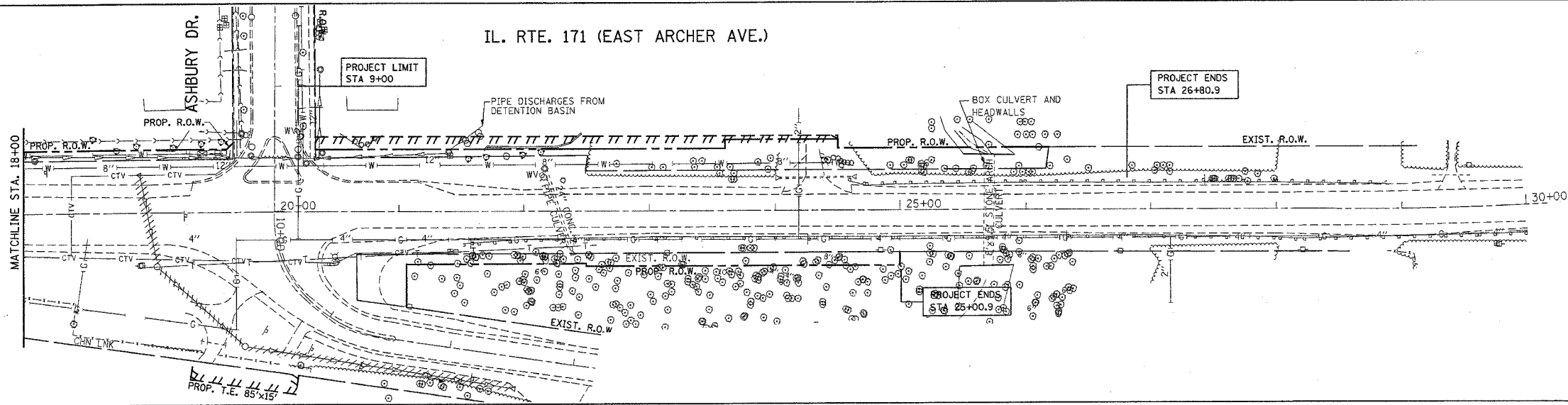
PROFILE

| | |
|----------|--|
| DATE | |
| BY | |
| REVIEWED | |
| NOTED | |
| NO. | |

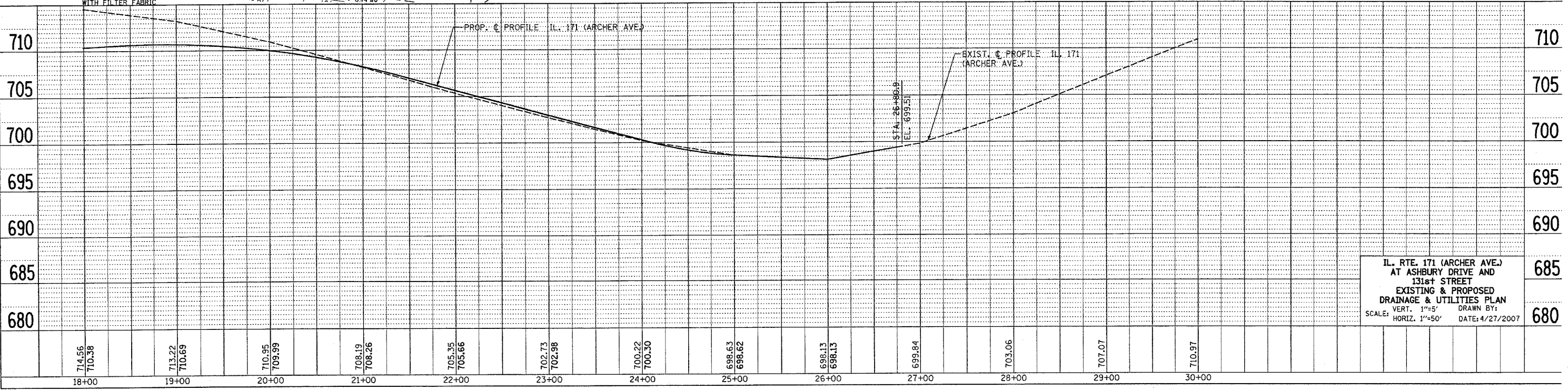
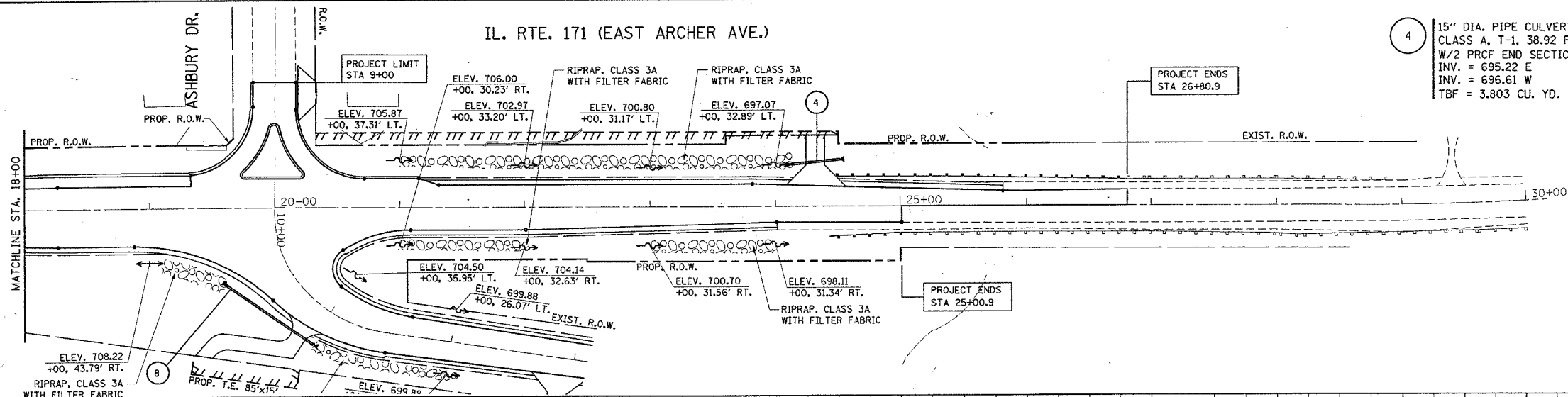
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 USER NAME = bowerd

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | | 26 |
| STA. 18+00 | | TO STA. 30+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

IL. RTE. 171 (EAST ARCHER AVE.)



IL. RTE. 171 (EAST ARCHER AVE.)



IL. RTE. 171 (ARCHER AVE.)
 AT ASHBURY DRIVE AND
 131st STREET
 EXISTING & PROPOSED
 DRAINAGE & UTILITIES PLAN
 SCALE: VERT. 1"=5' HORIZ. 1"=50' DATE: 4/27/2007

PLAN

| | |
|----------|------|
| REVISION | DATE |
| BY | |
| NOTED | |
| DATE | |
| BY | |
| NO. | |

PROFILE

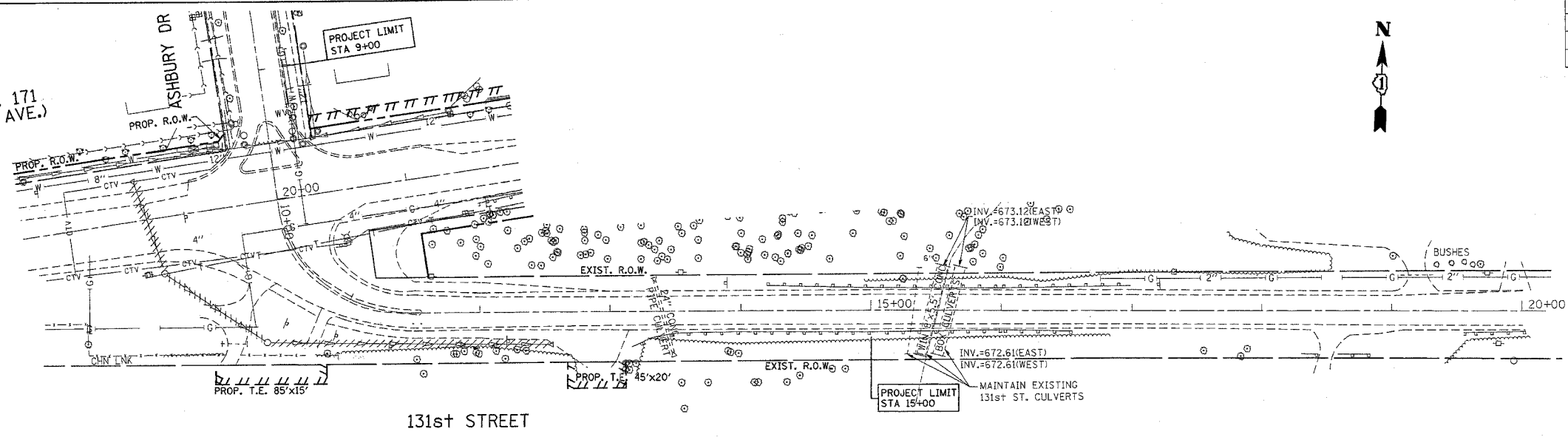
| | |
|----------|------|
| REVISION | DATE |
| BY | |
| NOTED | |
| DATE | |
| BY | |
| NO. | |

PLOT DATE = 4/27/2007
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | | 27 |
| STA. 9+00 | | TO STA. 20+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

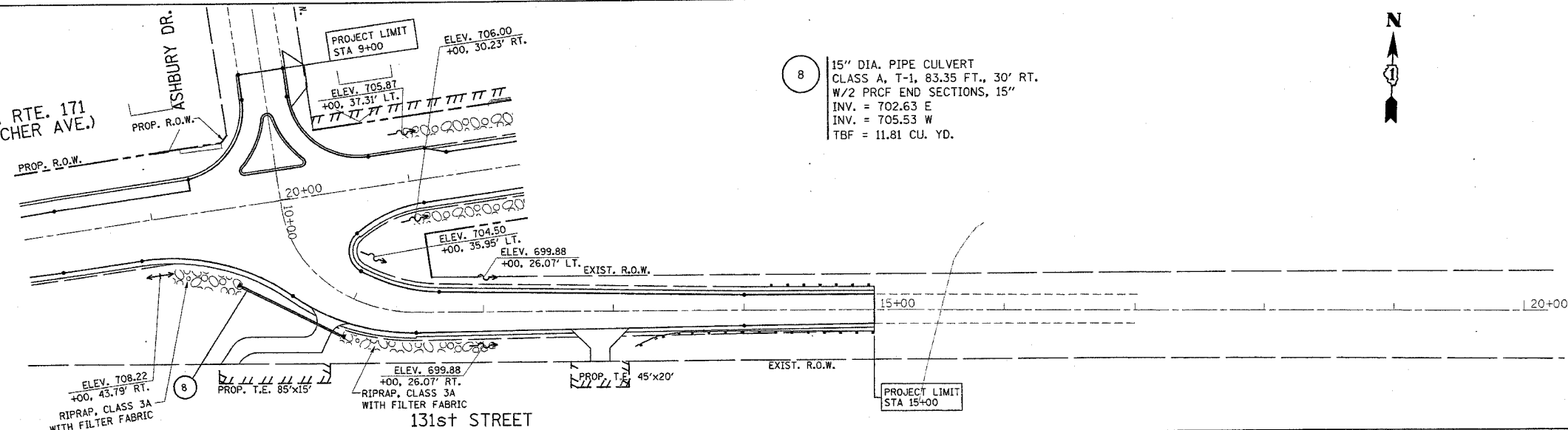


IL. RTE. 171
(ARCHER AVE.)



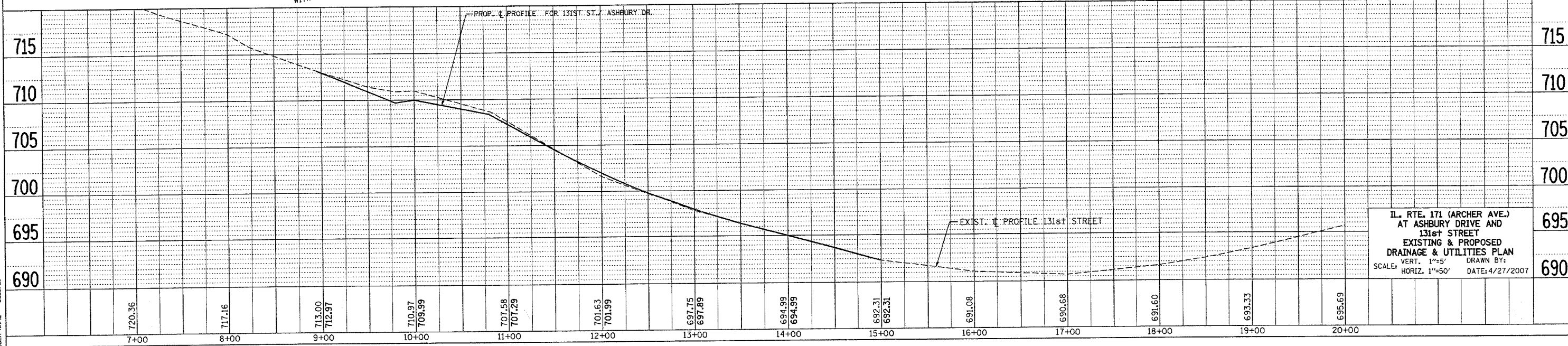
131st STREET

IL. RTE. 171
(ARCHER AVE.)



131st STREET

8 15" DIA. PIPE CULVERT
CLASS A, T-1, 83.35 FT., 30' RT.
W/2 PRCF END SECTIONS, 15"
INV. = 702.63 E
INV. = 705.53 W
TBF = 11.81 CU. YD.



IL. RTE. 171 (ARCHER AVE.)
AT ASHBURY DRIVE AND
131st STREET
EXISTING & PROPOSED
DRAINAGE & UTILITIES PLAN
SCALE: VERT. 1"=5' HORIZ. 1"=50' DATE: 4/27/2007

PLAN

| DATE | BY |
|------|----|
| | |

DATE: 4/27/2007
FILE NAME: C:\pcc\mca\p13682\ah-dr-utn\p13682.dgn
PLOT SCALE: 1"=50' / IN.
USER NAME: bboard1

PROFILE

| DATE | BY |
|------|----|
| | |

DATE: 4/27/2007
FILE NAME: C:\pcc\mca\p13682\ah-dr-utn\p13682.dgn
PLOT SCALE: 1"=50' / IN.
USER NAME: bboard1

PART OF THE NE 1/4 OF SEC. 32, AND PART OF THE NW 1/4 OF SEC. 33, ALL IN T37N, R11 EAST OF THE 3rd PM, COOK COUNTY, ILLINOIS.

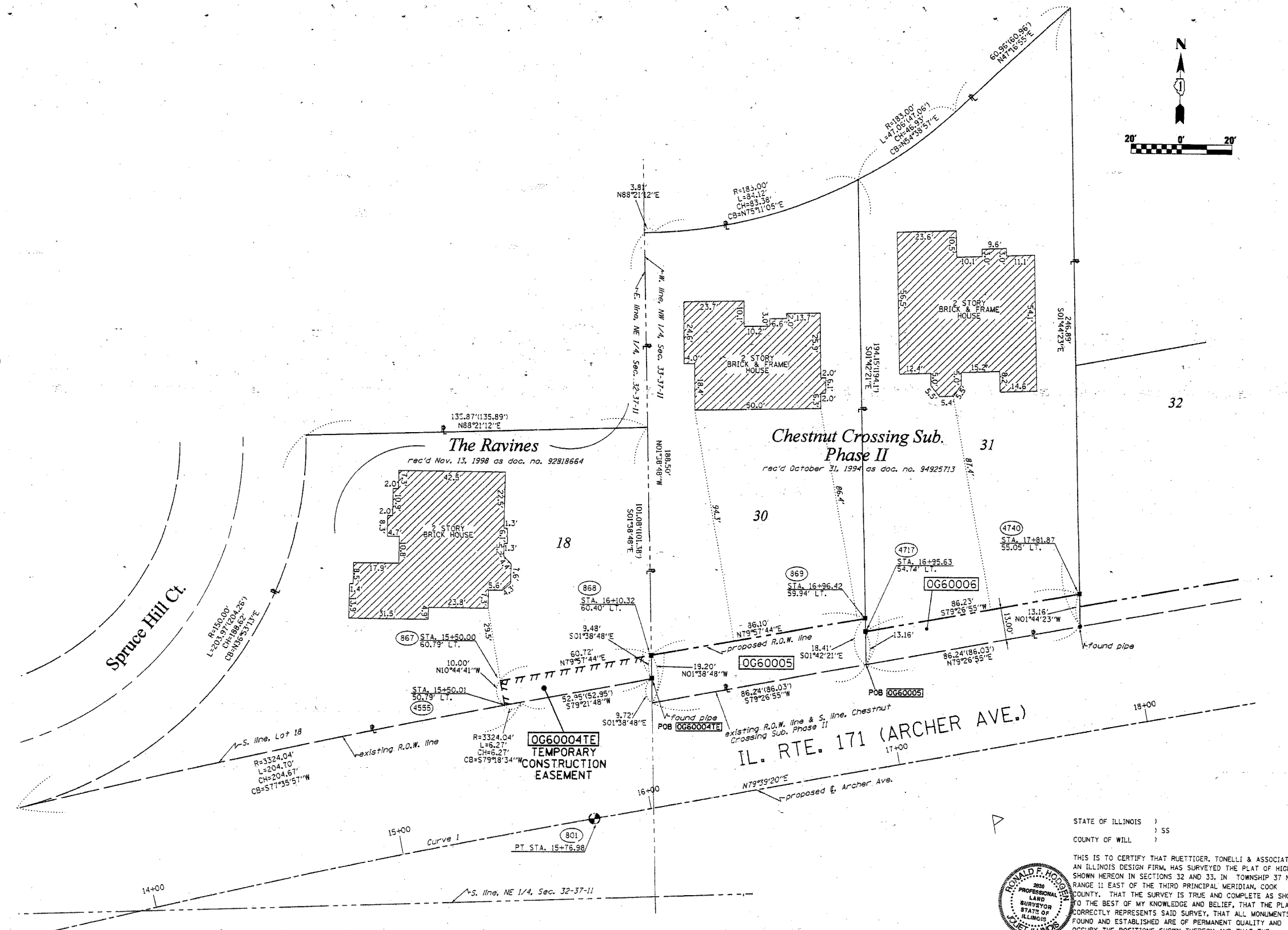
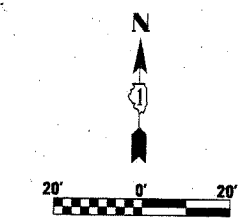
BEARINGS SHOWN HEREON ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------|---------------------------|--------------|-----------|
| IL 4977 | 314 N-1 | COOK | 72 | 29 |
| STATION 14+00 | | TO STATION 18+00 | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS FED. AID PROJECT | | |

LEGEND

- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT LINE
- SECTION LINE
- PROPERTY (DEED) LINE
- 121.45 MEASURED DIMENSION
- 123.45 (COMP) COMPUTED DIMENSION
- (123.45) RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD
- SET 3/8 INCH IRON ROD
- PERMANENT SURVEY MONUMENT
- I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
- + CUT CROSS FOUND OR SET
- SAME OWNERSHIP
- TEL EXISTING TELEPHONE SPLICE BOX
- LGT EXISTING STREET LIGHT
- MAIL EXISTING MAIL BOX
- WELL EXISTING WELL HEAD
- STAKING OF PROPOSED RIGHT OF WAY, SET 3/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, SET 3/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

SECTION CORNER QUARTER SECTION CORNER



CURVE DATA

CURVE 1

| |
|------------------|
| Δ=20°38'49" |
| D=1°45'11" |
| T=595.35' |
| L=1177.79' |
| PC=3268.40' |
| PT=3478' |
| PI STA. 3+99.19 |
| PT STA. 15+76.98 |
| PI STA. 9+94.54 |

RECEIVED
DEC 08 2006
PLATS & LEGALS

RUETTIGER, TONELLI & ASSOCIATES, INC.
Land Surveyors/Engineers/Planners/Landscape Architects/G.I.S. Consultants
2174 ONEIDA STREET
JOLIET, ILLINOIS 60435
PH. (815) 744-6600 FAX (815) 744-0101
2603 SOUTH WASHINGTON STREET SUITE 170
NAPERVILLE, ILLINOIS 60565
PH. (630) 420-1740 FAX (630) 420-1741



STATE OF ILLINOIS)
COUNTY OF WILL) SS
THIS IS TO CERTIFY THAT RUETTIGER, TONELLI & ASSOCIATES, INC., AN ILLINOIS DESIGN FIRM, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 32 AND 33, IN TOWNSHIP 37 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 15th DAY OF DECEMBER, 2006
RONALD F. HODGEN P.L.S. NO. 2630
MY LICENSE EXPIRES 11-30-2008

| PARCEL NO. | OWNER | TOTAL HOLDING | R.O.W. REQUIRED | PREVIOUSLY DEDICATED | REMAINDER | EASEMENTS | | EASEMENT PURPOSE | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|--------------------------------|---------------|-----------------|----------------------|-----------|-----------|------|------------------|----------------------|----------------------|
| | | ACRES | ACRES | ACRES | ACRES | TEMP | PERM | | | |
| 0G60004 | DAVID W. AND VIVIAN V. SMUKSTA | 0.417 | N/A | N/A | 0.417 | 0.013 | N/A | CONST. | 22-32-209-002 | |
| 0G60005 | JOHN J. & MARY BETH NOVAK | 0.367 | 0.037 | N/A | 0.330 | N/A | N/A | N/A | 22-33-111-001 | |
| 0G60006 | PHILLIP F. & JOAN T. HULTGEN | 0.426 | 0.026 | N/A | 0.400 | N/A | N/A | N/A | 22-33-111-002 | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
IL RTE. 171 (ARCHER AVE)
AT 131st STREET

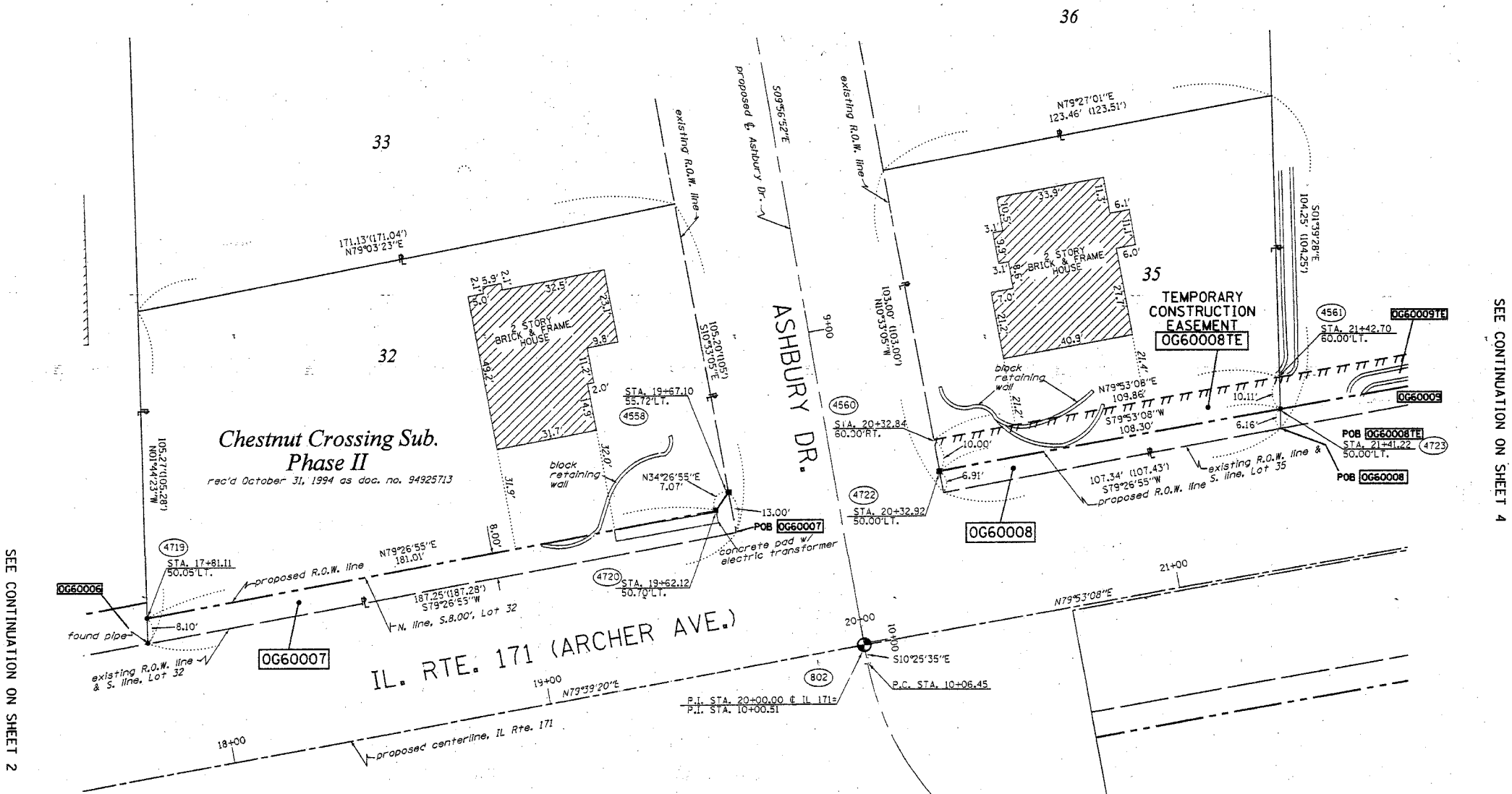
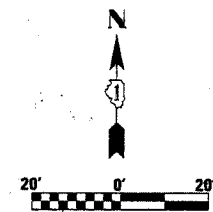
COOK COUNTY
JOB NO. R-90-010-04
STATION 14+00 TO STATION 18+00

SCALE: 1"=20' SHEET 2 OF 7

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT 1
201 WEST CENTER COURT
SCHAMBURG, ILLINOIS 60196

11-2-06 / REVISED PARCEL 4 PER IDOT
7-26-06 / REVISED PER IDOT
REVISED 7-18-06 CHANGED ROW LINE ON ALL PARCELS

| A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------------|---------|--------|---------------------------|-----------|
| 171 | 314 N | COOK | 72 | 29 |
| STATION 17+50 TO STATION 22+00 | | | | |
| FED. ROAD DIST. NO. 1 | | | ILLINOIS FED. AID PROJECT | |



- LEGEND**
- EXISTING CENTERLINE
 - PROPOSED CENTERLINE
 - EXISTING RIGHT OF WAY LINE
 - PROPOSED RIGHT OF WAY LINE
 - PROPOSED EASEMENT LINE
 - SECTION LINE
 - PROPERTY (DEED) LINE
 - 121.45 MEASURED DIMENSION
 - 123.45 (COMP) COMPUTED DIMENSION
 - (123.45) RECORDED DIMENSION
 - FOUND IRON PIPE OR IRON ROD
 - SET 3/8 INCH IRON ROD
 - PERMANENT SURVEY MONUMENT
 - I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
 - CUT CROSS FOUND OR SET
 - SAME OWNERSHIP
 - TEL --- EXISTING TELEPHONE SPLICE BOX
 - LG7 --- EXISTING STREET LIGHT
 - MAIL BOX --- EXISTING MAIL BOX
 - WELL --- EXISTING WELL HEAD
 - STAKING OF PROPOSED RIGHT OF WAY, SET 3/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
 - M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, SET 3/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

SEE CONTINUATION ON SHEET 4

SEE CONTINUATION ON SHEET 2

| PARCEL NO. | OWNER | TOTAL HOLDING ACRES | R.O.W. REQUIRED ACRES | PREVIOUSLY DEDICATED ACRES | REMAINDER ACRES | EASEMENTS | | EASEMENT PURPOSE | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|--|---------------------|-----------------------|----------------------------|-----------------|------------|------------|------------------|----------------------|----------------------|
| | | | | | | TEMP ACRES | PERM ACRES | | | |
| OG60007 | SANTOSH K. DEVULAPALLY AND PADMAVATHY DEVULLAPALLY | 0.430 | 0.035 | N/A | 0.395 | N/A | N/A | N/A | 22-33-111-003 | |
| OG60008 | DAVID PEREZ AND LUZ PEREZ | 0.273 | 0.016 | N/A | 0.257 | 0.025 | N/A | TEMP. | 22-33-112-002 | |

STATE OF ILLINOIS)
) SS
COUNTY OF WILL)

THIS IS TO CERTIFY THAT RUETTIGER, TONELLI & ASSOCIATES, INC., AN ILLINOIS DESIGN FIRM, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 32 AND 33, IN TOWNSHIP 37 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY. THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT CHICAGO, ILLINOIS THIS 14th DAY OF DECEMBER, 2006.
RONALD F. HODGEN P.L.S. NO. 2830
MY LICENSE EXPIRES 11-30-2008



RECEIVED
DEC 06 2006
PLATS & LEGALS

RUETTIGER, TONELLI & ASSOCIATES, INC.
Land Surveyors/Engineers/Planners/Landscape Architects/G.I.S. Consultants
2174 ONEIDA STREET 2603 SOUTH WASHINGTON STREET SUITE 170
CHICAGO, ILLINOIS 60645 MAPERVILLE, ILLINOIS 60963
PH. (815) 744-8600 FAX (815) 744-0101 PH. (630) 420-1740 FAX (630) 420-1741

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
IL RTE. 171 (ARCHER AVE)
AT 131st STREET

COOK COUNTY
JOB NO. R-90-010-04
STATION 17+50 TO STATION 22+00

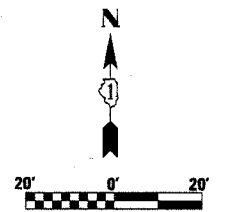
SCALE: 1"=20' SHEET 3 OF 7

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT 1
201 WEST CENTER COURT
SCHMUNBERG, ILLINOIS 60196

SHEET 1 OF -- IS A COVER SHEET AND IS NOT RECORDED
R7&A 20060656 (IL RTE. 171)

REVISED 7-26-06 / PER IDOT

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------|---------------------------|--------------|-----------|
| 577 | 314 N-1 | COOK | 72 | 30 |
| STATION 82+00 | | TO STATION 85+50 | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS FED. AID PROJECT | | |



LEGEND

- EXISTING CENTERLINE
- - - PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- - - PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT LINE
- SECTION LINE
- PROPERTY (DEED) LINE
- 121.45 MEASURED DIMENSION
- 123.45 (COMP) COMPUTED DIMENSION
- 123.45 RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD
- SET 1/2 INCH IRON ROD PERMANENT SURVEY MONUMENT, (TO BE SET BY OTHERS)
- I.O.T. STD. 2135 (TO BE SET BY OTHERS)
- + CUT CROSS FOUND OR SET
- SAME OWNERSHIP
- TEL EXISTING TELEPHONE SPLICE BOX
- LG EXISTING STREET LIGHT
- MAIL EXISTING MAIL BOX
- WELL EXISTING WELL HEAD
- STAKING OF PROPOSED RIGHT OF WAY, SET 1/2 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, SET 1/2 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- SECTION CORNER
- QUARTER SECTION CORNER

STATE OF ILLINOIS)
 COUNTY OF WILL) SS

THIS IS TO CERTIFY THAT RUETTIGER, TONELLI & ASSOCIATES, INC., AN ILLINOIS DESIGN FIRM, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 32 AND 33, IN TOWNSHIP 37 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY. THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RE-TRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATE OF SURVEY, ILLINOIS THIS 17th DAY OF December, 2006

RECEIVED
 DEC 6 2006
 PLATS & LEGALS

RUETTIGER, TONELLI & ASSOCIATES, INC.
 Land Surveyors/Engineers/Planners/Landscape Architects/G.I.S. Consultants
 2174 OREGON STREET SUITE 170
 JOLIET, ILLINOIS 60435
 PH. (815) 744-5600 FAX (815) 744-0001 PH. (630) 420-7740 FAX (630) 420-7741

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
 IL RTE. 171 (ARCHER AVE.)
 AT 131st STREET

COOK COUNTY
 JOB NO. R-90-010-04
 STATION 82+00 TO STATION 85+50

SCALE: 1"=20'
 SHEET 4 OF 7
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS/DISTRICT 1
 201 WEST CENTER COURT
 SCHUMBERG, ILLINOIS 60196

SHEET 1 OF -- IS A COVER SHEET AND IS NOT RECORDED
 RT&A 20060656 (IL RTE. 171)

Chestnut Crossing Sub. Phase II
 rec'd October 31, 1994 as doc. no. 94925713

Lot 14

detention pond

TEMPORARY CONSTRUCTION EASEMENT OG60009TE

TEMPORARY CONSTRUCTION EASEMENT OG60010TE

TEMPORARY CONSTRUCTION EASEMENT OG60011TE

IL. RTE. 171 (ARCHER AVE.)

| PARCEL NO. | OWNER. | TOTAL HOLDING ACRES | R.O.W. REQUIRED ACRES | PREVIOUSLY DEDICATED ACRES | REMAINDER ACRES | EASEMENTS TEMP ACRES | PERM ACRES | EASEMENTS PURPOSE | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|---|---------------------|-----------------------|----------------------------|-----------------|----------------------|------------|-------------------|----------------------|----------------------|
| OG60009 | VILLAGE OF LEMONT | 0.884 | 0.014 | N/A | 0.870 | 0.025 | N/A | TEMP. | 22-33-112-001 | |
| OG60010 | SOUTH HOLLAND TRUST & SAVINGS BANK, TRUST NO. 10958 | 0.785 | 0.035 | 0.023 | 0.750 | 0.007 | 298 s.f. | TEMP. | 22-33-102-006 | |
| OG60011 | PAUL MAKSIMOVIC & PAMELA MAKSIMOVIC, TRUSTEES OF MAKSIMOVIC FAMILY 2000 TRUST | 8.967 | 0.405 | 0.255 | 8.562 | 0.024 | N/A | TEMP. | 22-33-102-003 | |

SEE CONTINUATION ON SHEET 3

SEE CONTINUATION ON SHEET 6

7-26-06 / REVISED PER IDOT
 REVISED 7-18-06 / CHANGED OG60011

PART OF THE SW 1/4 OF SEC. 33, IN T37N, R11 EAST OF THE 3rd PM, COOK COUNTY, ILLINOIS.

LEGEND

- EXISTING CENTERLINE
- - - - PROPOSED CENTERLINE
- - - - EXISTING RIGHT OF WAY LINE
- - - - PROPOSED RIGHT OF WAY LINE
- - - - PROPOSED EASEMENT LINE
- - - - SECTION LINE
- - - - PROPERTY (DEED) LINE
- 121.45 MEASURED DIMENSION
- 123.45 (COMP) COMPUTED DIMENSION
- (123.45) RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD
- SET 1/4 INCH IRON ROD
- PERMANENT SURVEY MONUMENT
- I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
- CUT CROSS FOUND OR SET
- SAME OWNERSHIP
- TEL EXISTING TELEPHONE SPLICE BOX
- LGT EXISTING STREET LIGHT
- MAIL EXISTING MAIL BOX
- WELL EXISTING WELL HEAD

| PARCEL NO. | OWNER | TOTAL HOLDING ACRES | R.O.W. REQUIRED ACRES | PREVIOUSLY DEDICATED ACRES | REMAINDER ACRES | EASEMENTS | | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|-----------------|---------------------|-----------------------|----------------------------|-----------------|------------|------------|----------------------|--|
| | | | | | | TEMP ACRES | PERM ACRES | | |
| OG60001 | JACK GLINKOWSKI | 1.530 | N/A | N/A | 1.530 | 0.040 | N/A | TEMP. | 22-33-300-014(p1.) |
| OG60002 | STEFAN USTUPSKI | 5.708 | N/A | N/A | 5.708 | 0.029 | N/A | TEMP. | 22-33-300-008 |
| OG60014 | KASEY KOLBRECKI | 5.935 | N/A | N/A | 5.935 | 0.002 | 98s.f. | TEMP. | 22-33-300-014(p1.) 22-33-300-015 22-33-300-016 |

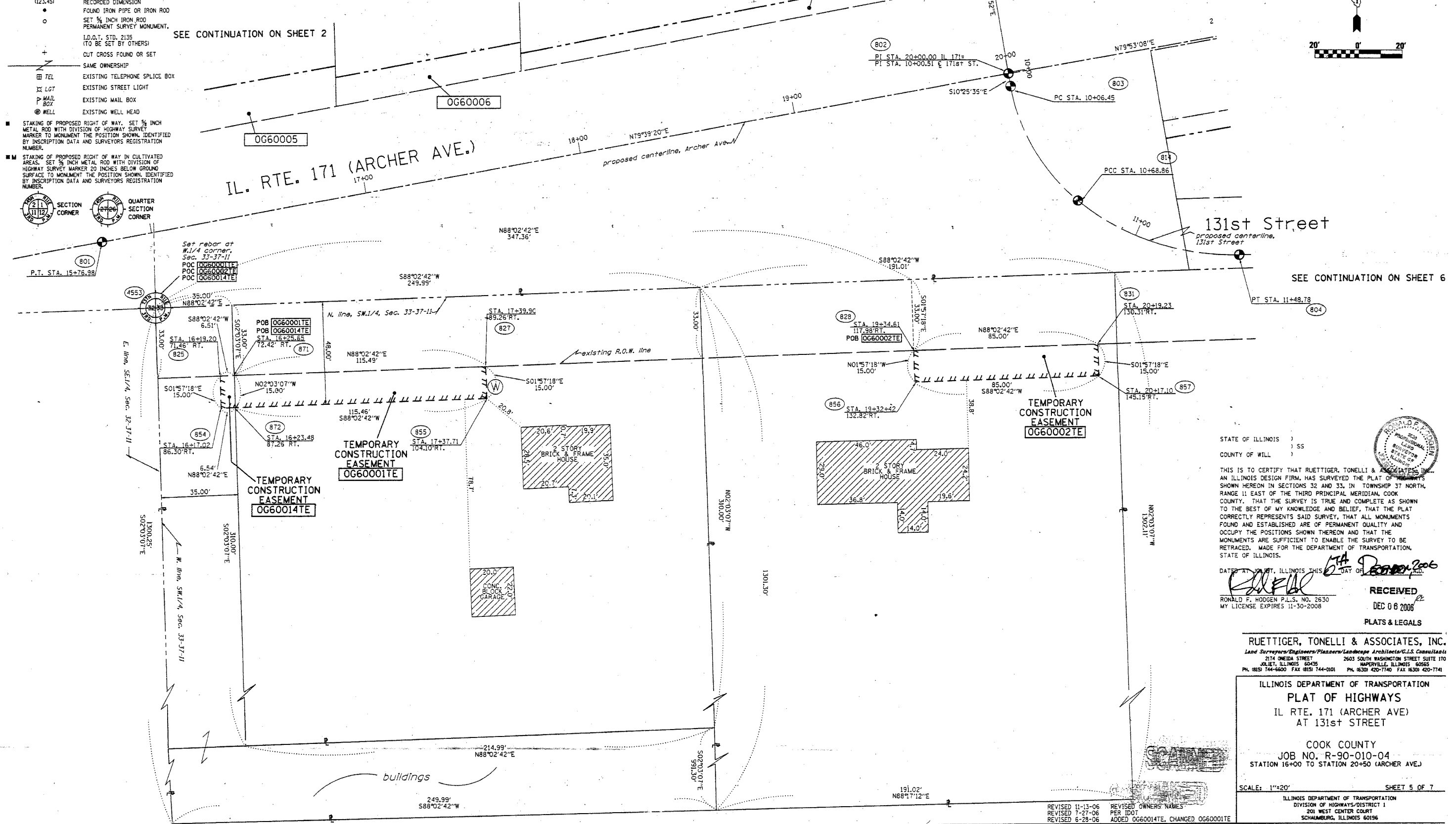
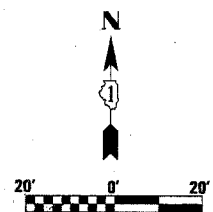
- STAKING OF PROPOSED RIGHT OF WAY. SET 1/4 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 1/4 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.



SEE CONTINUATION ON SHEET 2

SEE CONTINUATION ON SHEET 3

| | | | | |
|---|---------|------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 171 | 314 N-1 | COOK | 72 | 31 |
| STATION 16+00 | | TO STATION 20+50 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |



SEE CONTINUATION ON SHEET 6

STATE OF ILLINOIS)
 COUNTY OF WILL)

THIS IS TO CERTIFY THAT RLETTIGER, TONELLI & ASSOCIATES, INC. AN ILLINOIS DESIGN FIRM, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 32 AND 33, IN TOWNSHIP 37 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATE: 11-30-06 ILLINOIS THIS DAY OF DECEMBER 2006

RECEIVED
 DEC 0 6 2006

PLATS & LEGALS

RUETTIGER, TONELLI & ASSOCIATES, INC.
 Land Surveyors/Engineers/Planners/Landscape Architects/G.I.S. Consultants
 2174 ONEIDA STREET 2603 SOUTH WASHINGTON STREET SUITE 170
 DELET, ILLINOIS 60405 MAPERVILLE, ILLINOIS 60565
 PH. (630) 144-6600 FAX (630) 144-0101 PH. (630) 420-1740 FAX (630) 420-1741

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
 IL RTE. 171 (ARCHER AVE.)
 AT 131st STREET

COOK COUNTY
 JOB NO. R-90-010-04
 STATION 16+00 TO STATION 20+50 (ARCHER AVE.)

SCALE: 1"=20' SHEET 5 OF 7

REVISOR 11-13-06 REVISOR OWNERS NAMES
 REVISOR 7-27-06 PER IDOT
 REVISOR 6-28-06 ADDED OG60014TE, CHANGED OG60001TE

SHEET 1 OF -- IS A COVER SHEET AND IS NOT RECORDED
 RT&A 20060656 (IL RTE. 171)

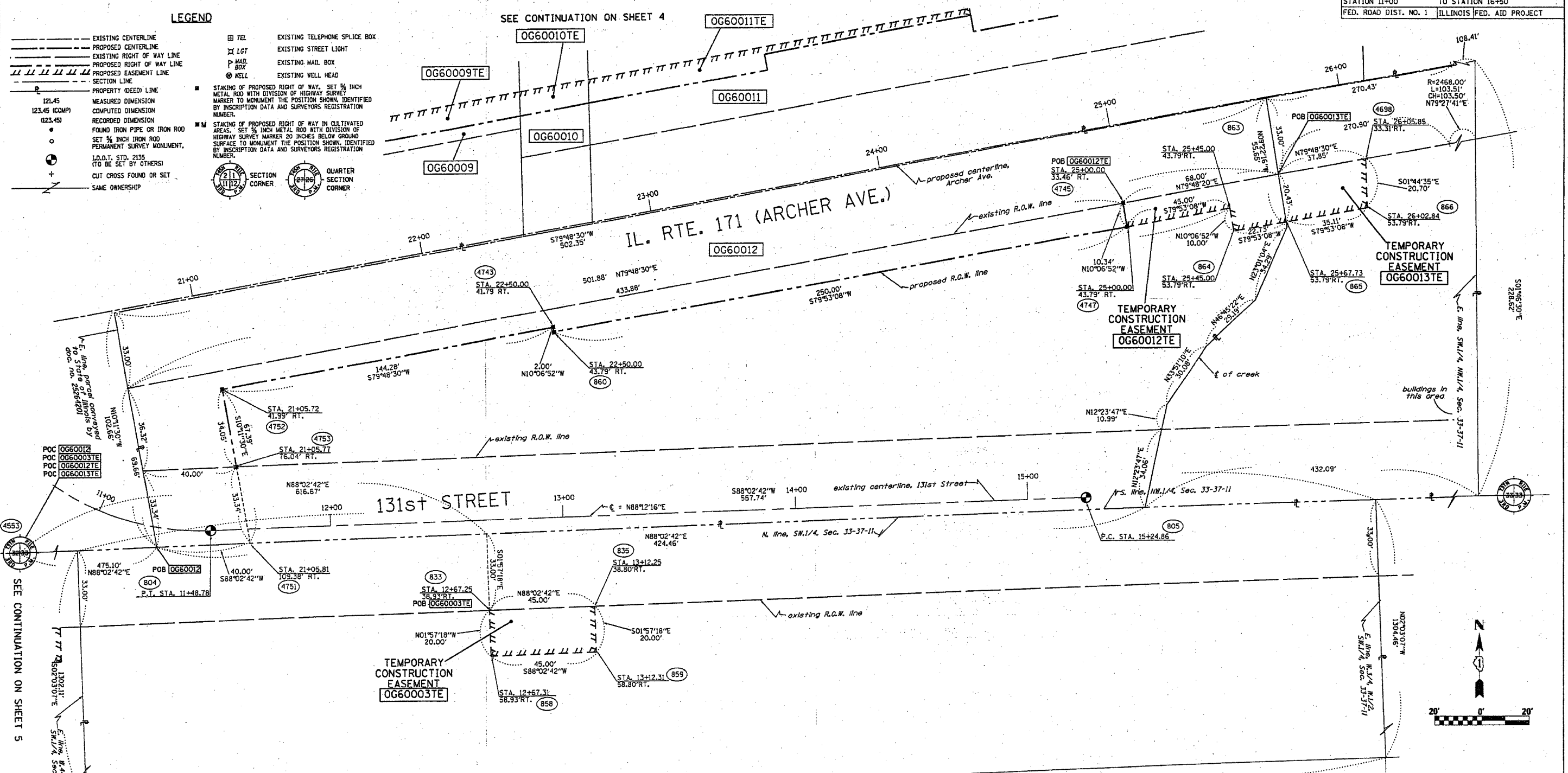
PART OF THE NW 1/4 AND PART OF THE SW 1/4 OF SEC. 33, IN T37N, R11 EAST OF THE 3rd PM, COOK COUNTY, ILLINOIS.

BEARINGS SHOWN HEREON ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|---------|---------------------------|--------------|-----------|
| 577 | 314 N-1 | COOK | 72 | 32 |
| STATION 11+00 | | TO STATION 16+50 | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS FED. AID PROJECT | | |

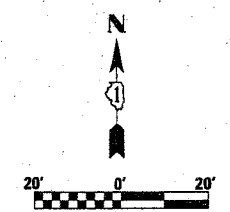
LEGEND

- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT LINE
- SECTION LINE
- PROPERTY (DEED) LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORDED DIMENSION
- FOUND IRON PIPE OR IRON ROD
- SET 3/4 INCH IRON ROD PERMANENT SURVEY MONUMENT.
- I.O.A.T. STD. 2135 (TO BE SET BY OTHERS)
- CUT CROSS FOUND OR SET
- SAME OWNERSHIP
- TEL EXISTING TELEPHONE SPLICE BOX
- LGT EXISTING STREET LIGHT
- MAIL BOX EXISTING MAIL BOX
- WELL EXISTING WELL HEAD
- STAKING OF PROPOSED RIGHT OF WAY, SET 3/4 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, SET 3/4 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- SECTION CORNER
- QUARTER SECTION CORNER



SEE CONTINUATION ON SHEET 5

SEE CONTINUATION ON SHEET 4



| PARCEL NO. | OWNER | TOTAL HOLDING ACRES | R.O.W. REQUIRED ACRES | PREVIOUSLY DEDICATED ACRES | REMAINDER ACRES | EASEMENTS | | EASEMENT PURPOSE | PERMANENT TAX NUMBER | PROPERTY ACQUIRED BY |
|------------|--|---------------------|-----------------------|----------------------------|-----------------|------------|------------|------------------|----------------------|----------------------|
| | | | | | | TEMP ACRES | PERM ACRES | | | |
| OG60003 | THE KATHRYN A. VAJDIK DECLARATION OF TRUST | 16.687 | N/A | N/A | 16.687 | 0.021 | N/A | TEMP. | 22-33-300-012 | |
| OG60012 | MARIUS & KRISTINA BUIVYDAS | 1.476 | 0.531 | 0.410 | 0.945 | 0.021 | N/A | TEMP. | 22-33-104-002 | |
| OG60013 | SAVO RADULOVIC | 1.802 | N/A | N/A | 1.802 | 0.017 | N/A | TEMP. | 22-33-104-003 | |

STATE OF ILLINOIS)
COUNTY OF WILL) SS

THIS IS TO CERTIFY THAT RUETTIGER, TONELLI & ASSOCIATES, INC., AN ILLINOIS DESIGN FIRM, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 32 AND 33, IN TOWNSHIP 37 NORTH RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 21 DAY OF FEBRUARY 2007.

RONALD F. HODGEN
PROFESSIONAL LAND SURVEYOR
STATE OF ILLINOIS
LICENSE NO. 0217

RONALD F. HODGEN P.L.S. NO. 2630
MY LICENSE EXPIRES 11-30-2008

RECEIVED
FEB 06 2007
PLATS & LEGALS

RUETTIGER, TONELLI & ASSOCIATES, INC.
Land Surveyors/Engineers/Planners/Landscape Architects/G.I.S. Consultants
2174 OREGON STREET JOLIET, ILLINOIS 60435
2603 SOUTH WASHINGTON STREET SUITE 170 NAPERVILLE, ILLINOIS 60563
PH: (630) 744-6600 FAX: (630) 744-0101

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
IL RTE. 171 (ARCHER AVE)
AT 131st STREET
COOK COUNTY
JOB NO. R-90-010-04
STATION 11+00 TO STATION 16+50 (131st STREET)

SCALE: 1"=20' SHEET 6 OF 7

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS/DISTRICT 1
201 WEST CENTER COURT
SCHAMBERG, ILLINOIS 60196

SHEET 1 OF -- IS A COVER SHEET AND IS NOT RECORDED
R7&A 20060656 (IL RTE. 171)

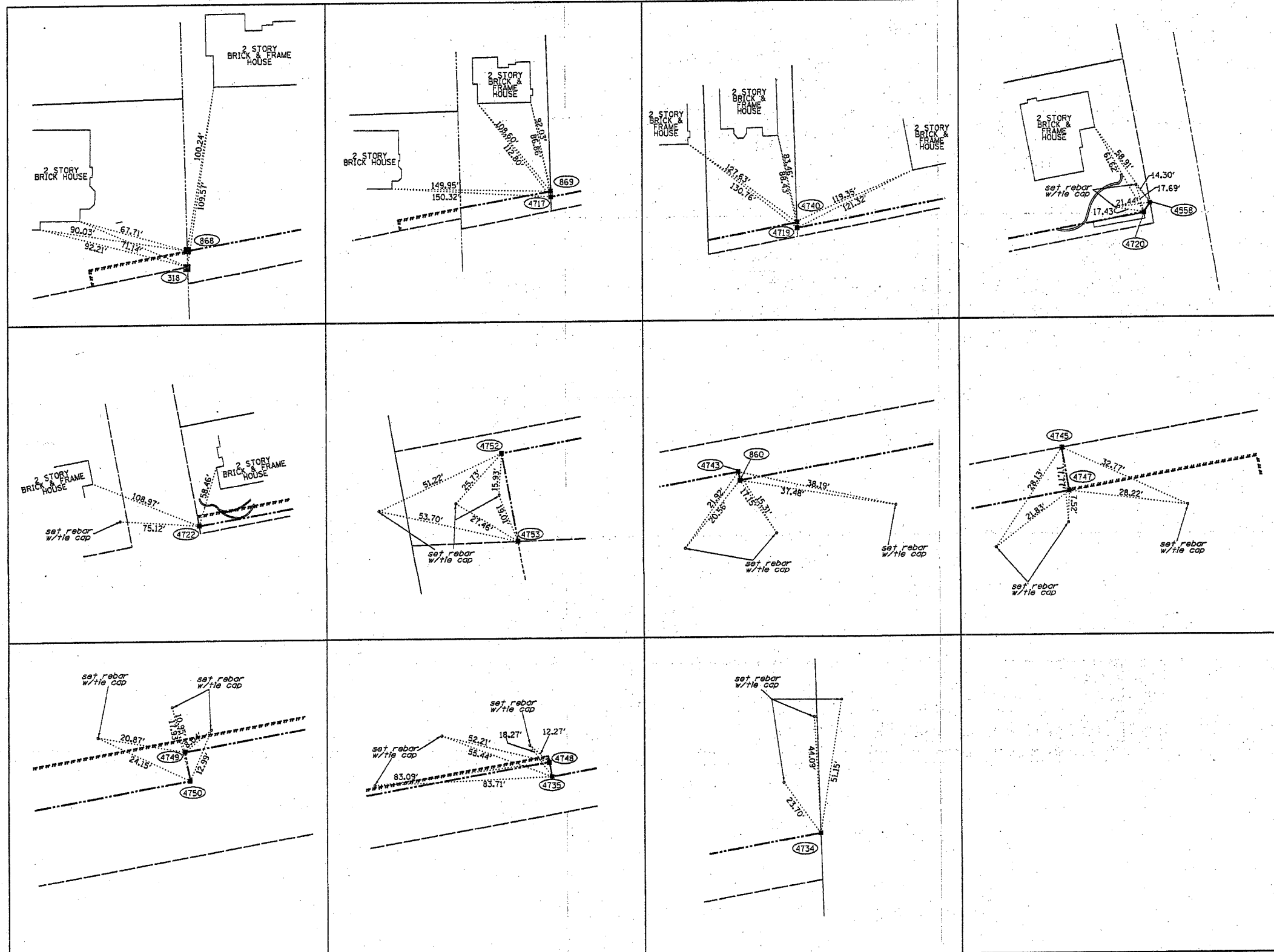
| | | | | |
|-----------------------|---------|---------------------------|--------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 314 H-I | COOK | 72 | 33 |
| STATION | | TO STATION | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS FED. AID PROJECT | | |

BEARINGS SHOWN HEREON ARE BASED ON THE ILLINOIS STATE PLANE COORDINATE SYSTEM EAST ZONE, NAD 83.

| STATION | OFFSET | PTH | NORTH | EAST |
|-----------|-----------|------|-------------|-------------|
| 10+08.990 | 81.03 LT. | 318 | 1818138.990 | 1079113.946 |
| 10+10.323 | 60.40 LT. | 888 | 1818148.488 | 1079113.374 |
| 10+96.836 | 84.74 LT. | 4717 | 1818158.222 | 1079188.315 |
| 10+96.424 | 88.94 LT. | 899 | 1818161.478 | 1079188.188 |
| 17+81.108 | 50.05 LT. | 4718 | 1818168.355 | 1078283.242 |
| 17+81.866 | 55.05 LT. | 4740 | 1818172.012 | 1078283.088 |
| 18+82.116 | 50.70 LT. | 4720 | 1818200.100 | 1078481.190 |
| 19+87.098 | 55.72 LT. | 4558 | 1818205.851 | 1078483.180 |
| 20+32.919 | 50.00 LT. | 4722 | 1818212.030 | 1078531.188 |
| 21+06.719 | 41.99 RT. | 4782 | 1818134.258 | 1078918.012 |
| 21+06.765 | 78.04 RT. | 4783 | 1818100.742 | 1078828.037 |
| 22+50.000 | 41.79 RT. | 4743 | 1818168.798 | 1078781.017 |
| 22+50.000 | 43.79 RT. | 860 | 1818197.817 | 1078781.388 |
| 23+50.000 | 57.00 LT. | 4748 | 1818228.381 | 1078859.588 |
| 23+50.000 | 50.00 LT. | 4780 | 1818268.470 | 1078863.188 |
| 24+50.000 | 57.00 LT. | 4748 | 1818282.168 | 1078840.580 |
| 24+50.000 | 50.00 LT. | 4735 | 1818288.278 | 1078841.788 |
| 25+00.000 | 32.48 RT. | 4745 | 1818211.896 | 1079005.888 |
| 25+00.000 | 43.79 RT. | 4747 | 1818201.720 | 1079007.483 |
| 26+18.111 | 50.00 LT. | 4734 | 1818314.787 | 1079107.287 |



- LEGEND**
- EXISTING CENTERLINE
 - PROPOSED CENTERLINE
 - EXISTING RIGHT OF WAY LINE
 - PROPOSED RIGHT OF WAY LINE
 - ===== PROPOSED PERMANENT EASEMENT LINE
 - ===== PROPOSED TEMPORARY EASEMENT LINE
 - SECTION LINE
 - PROPERTY (DEED) LINE
 - 121.45 MEASURED DIMENSION
 - (123.45) COMPUTED DIMENSION
 - (123.45) RECORDED DIMENSION
 - FOUND IRON PIPE OR IRON ROD
 - SET 3/8 INCH IRON ROD
 - PERMANENT SURVEY MONUMENT
 - I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)
 - ⊕ CUT CROSS FOUND OR SET
 - SAME OWNERSHIP
 - EXISTING TELEPHONE SPLICE BOX
 - EXISTING STREET LIGHT
 - EXISTING MAIL BOX
 - EXISTING WELL HEAD
 - STAKING OF PROPOSED RIGHT OF WAY. SET 3/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER TO MONUMENT THE POSITION SHOWN IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
 - STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. SET 3/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY MARKER 20 INCHES BELOW GROUND SURFACE TO MONUMENT THE POSITION SHOWN IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.



STATE OF ILLINOIS)
COUNTY OF WILL) 55

THIS IS TO CERTIFY THAT RUETTIGER, TONELLI & ASSOCIATES, INC., AN ILLINOIS DESIGN FIRM, HAS SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTIONS 32 AND 33, TOWNSHIP 37 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS 15TH DAY OF FEBRUARY 2007 A.D.

R. Hodgen
RONALD F. HODGEN P.L.S. NO. 2630
MY LICENSE EXPIRES 11-30-2008

RUETTIGER, TONELLI & ASSOCIATES, INC.
Lead Surveyors/Engineers/Planners/Landscape Architects/C.L.S. Consultants
2174 ONEIDA STREET JOLIET, ILLINOIS 60435
2603 SOUTH WASHINGTON STREET SUITE 170 NAPERVILLE, ILLINOIS 60563
PH. 815/344-6600 FAX 815/344-0101 PH. 815/344-1740 FAX 815/344-1741

ILLINOIS DEPARTMENT OF TRANSPORTATION
PLAT OF HIGHWAYS
IL RTE. 171 (ARCHER AVE)
AT 131ST STREET

COOK COUNTY
JOB NO. R-90-010-04

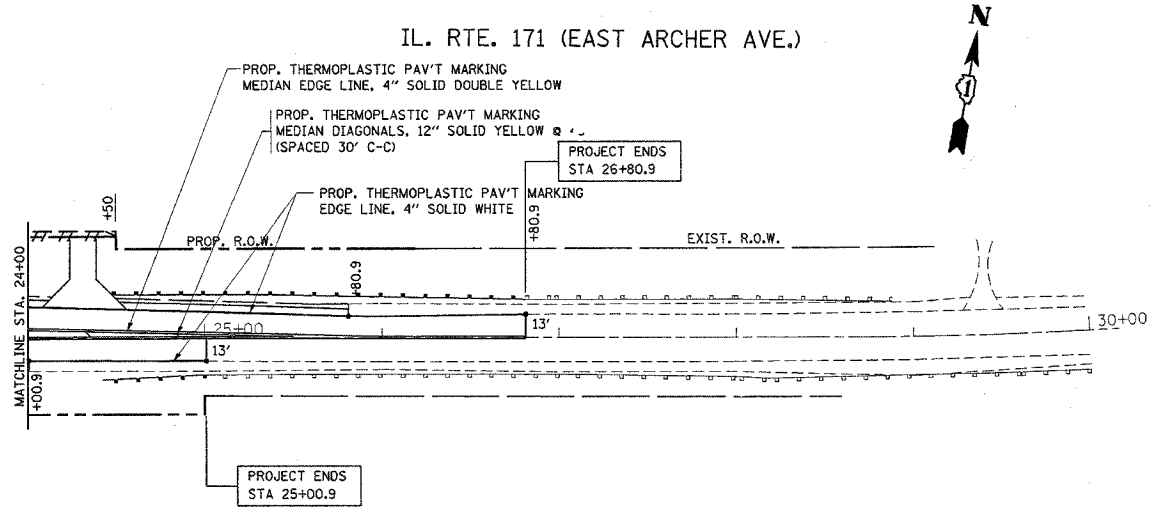
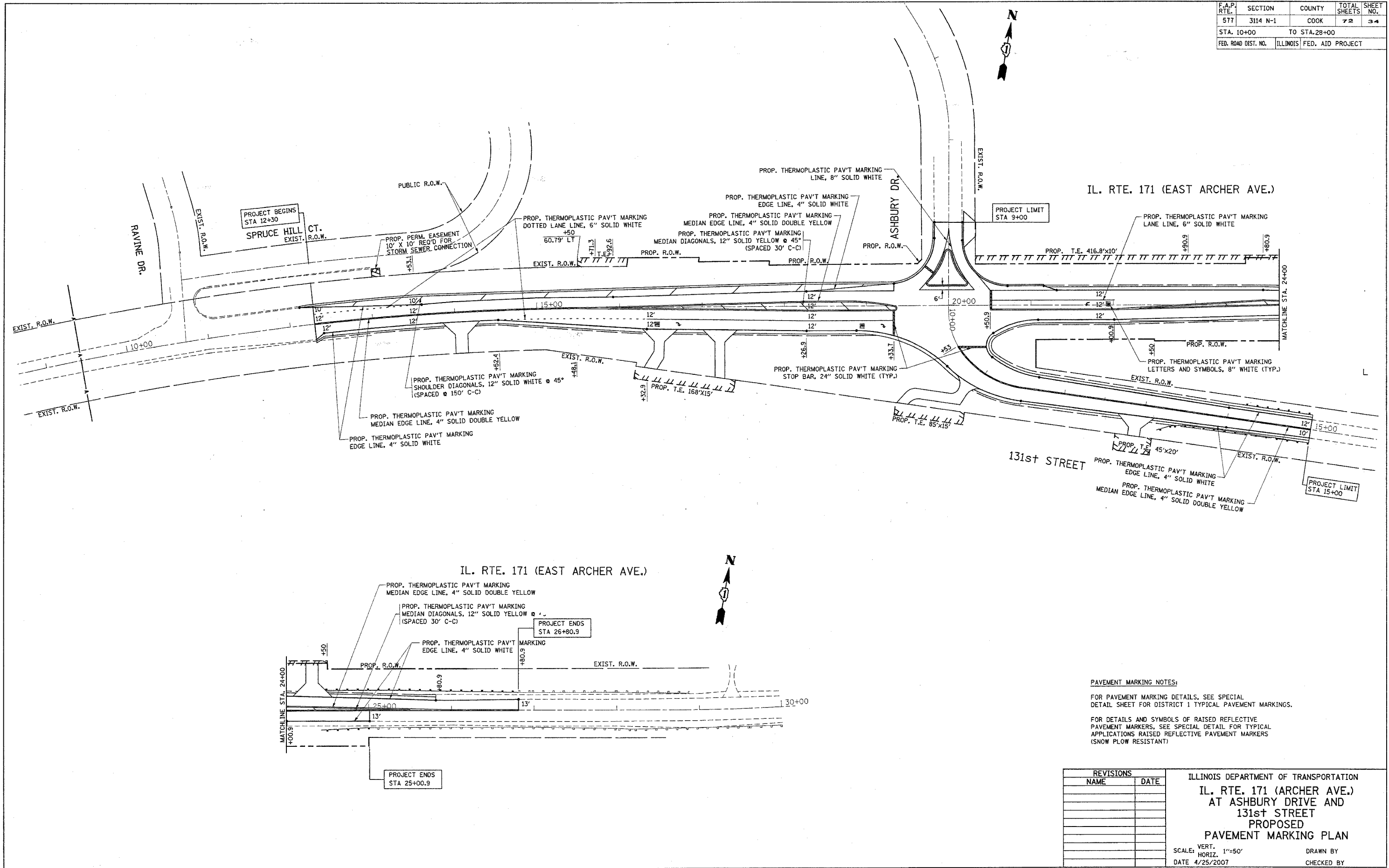
SCALE: NONE SHEET 7 OF 7

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS-DISTRICT 1
201 WEST CENTER COURT
SCHAMBERG, ILLINOIS 60156

RECEIVED
FEB 06 2007
PLATS & LEGALS

REVISED 2-6-07 / REVISED TIES

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 34 |
| STA. 10+00 | | TO STA. 28+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



PAVEMENT MARKING NOTES:
 FOR PAVEMENT MARKING DETAILS, SEE SPECIAL DETAIL SHEET FOR DISTRICT 1 TYPICAL PAVEMENT MARKINGS.
 FOR DETAILS AND SYMBOLS OF RAISED REFLECTIVE PAVEMENT MARKERS, SEE SPECIAL DETAIL FOR TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)

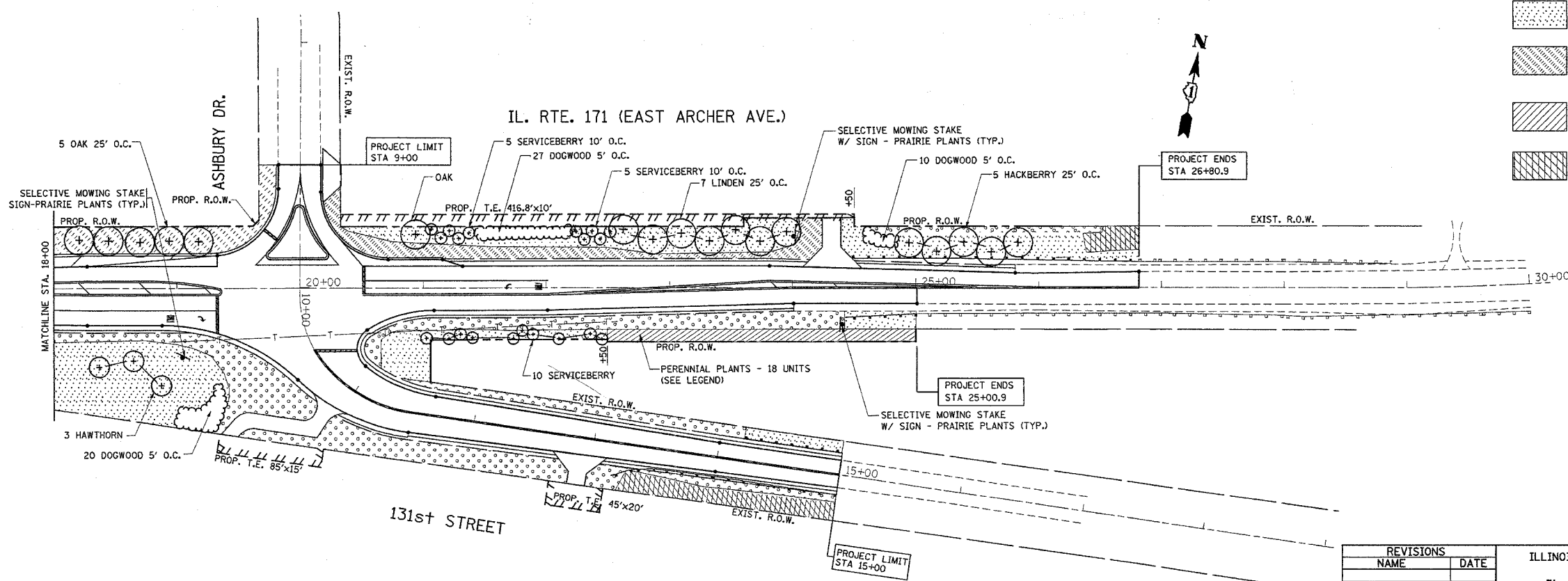
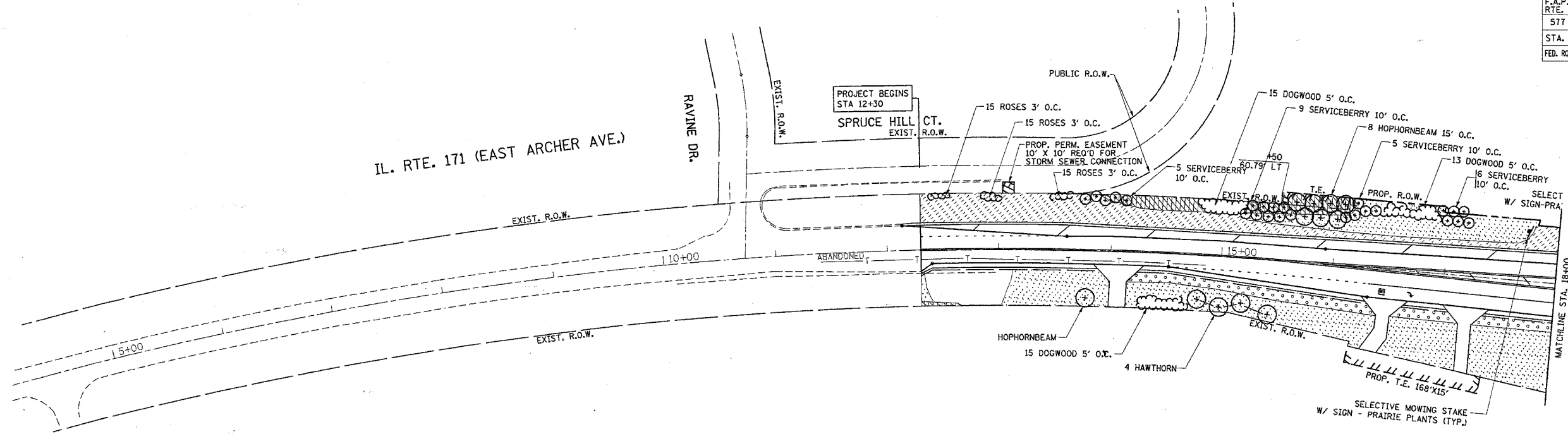
| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 IL. RTE. 171 (ARCHER AVE.)
 AT ASHBURY DRIVE AND
 131st STREET
 PROPOSED
 PAVEMENT MARKING PLAN

SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE 4/25/2007
 DRAWN BY
 CHECKED BY

PLOT DATE = 4/25/2007
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = ulrichhd

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 35 |
| STA. 10+00 | | TO STA. 28+00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



LEGEND

- SEEDING CLASS 2A
TOPSOIL
EROSION CONTROL BLANKET
- SEEDING CLASS 4A
COMPOST FURNISH & PLACE, 4"
EROSION CONTROL BLANKET
- SODDING, SALT TOLERANT
TOPSOIL
- PERENNIAL PLANTS, WOODLAND TYPE
PERENNIAL PLANTS, SEDGES
COARSE SAND PLACEMENT, 2"
COMPOST FURNISH & PLACE, 2"
- SELECTIVE CLEARING (1000 SQ. FT./UNIT)
MULCH PLACEMENT, 4"

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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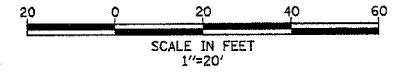
ILLINOIS DEPARTMENT OF TRANSPORTATION
**IL. RTE. 171 (ARCHER AVE.)
 AT ASHBURY DRIVE AND
 131st STREET**
 PROPOSED LANDSCAPING PLAN

SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE 4/25/2007

DRAWN BY
 CHECKED BY

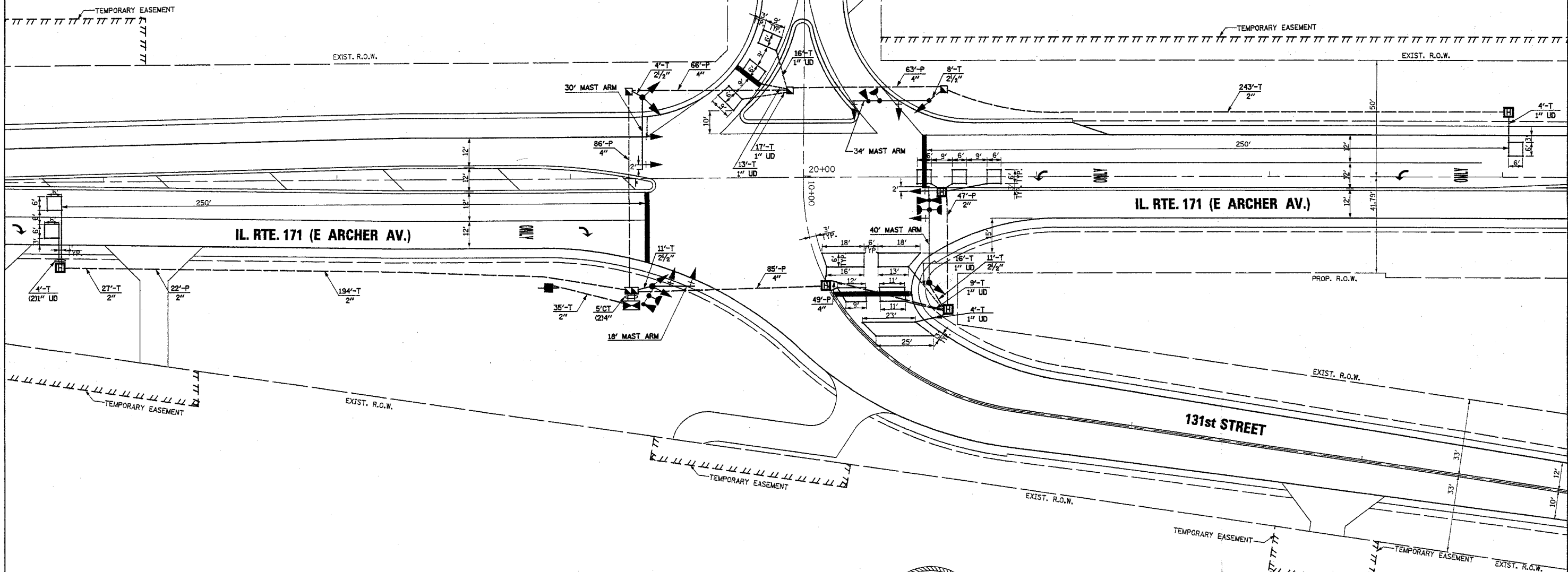
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 FILE NAME = c:\projects\60a25\60a25.dgn
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = jprichard

| | | | | |
|---------------------|---------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114N-1 | COOK | 72 | 36 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 60A25 | | | | |



TRAFFIC SIGNAL LEGEND

| | PROPOSED | EXISTING | | PROPOSED | EXISTING |
|--|----------|----------|--|----------|----------|
| CONTROLLER | | | EMERGENCY VEHICLE LIGHT DETECTOR | | |
| SERVICE INSTALLATION | | | CONFIRMATION BEACON | | |
| SIGNAL HEAD | | | SIGNAL HEAD OPTICALLY PROGRAMMED | | |
| SIGNAL HEAD WITH BACKPLATE | | | CONDUIT SPLICE | | |
| SIGNAL HEAD, PEDESTRIAN | | | WOOD POLE | | |
| SIGNAL POST | | | RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II | | |
| MAST ARM ASSEMBLY AND POLE, STEEL | | | VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE | | |
| MAST ARM ASSEMBLY AND POLE, ALUMINUM | | | RAILROAD CONTROL CABINET | | |
| COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE | | | TELEPHONE CONNECTION | | |
| UNIT DUCT | | | ILLUMINATED SIGN "NO LEFT TURN" | | |
| COMMON TRENCH | | | ILLUMINATED SIGN "NO RIGHT TURN" | | |
| HANDHOLE | | | MICROWAVE VEHICLE SENSOR | | |
| HEAVY DUTY HANDHOLE | | | | | |
| DOUBLE HANDHOLE | | | | | |
| G.S. CONDUIT IN TRENCH (T) OR PUSHED (P) | | | | | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | | | | |
| DETECTOR LOOP | | | | | |
| CAST IRON JUNCTION BOX | | | | | |



John A. Cabe
 JOHN A. CABE, JR.
 062-34158
 REGISTERED PROFESSIONAL ENGINEER
 OF ILLINOIS

SETON ENGINEERING
 A SERVICE CORPORATION
 CIVIL ENGINEERS
 19 S. BOTHWELL STREET
 PALATINE, ILLINOIS 60067
 VOICE: 847-776-7200 FAX: 847-776-7239

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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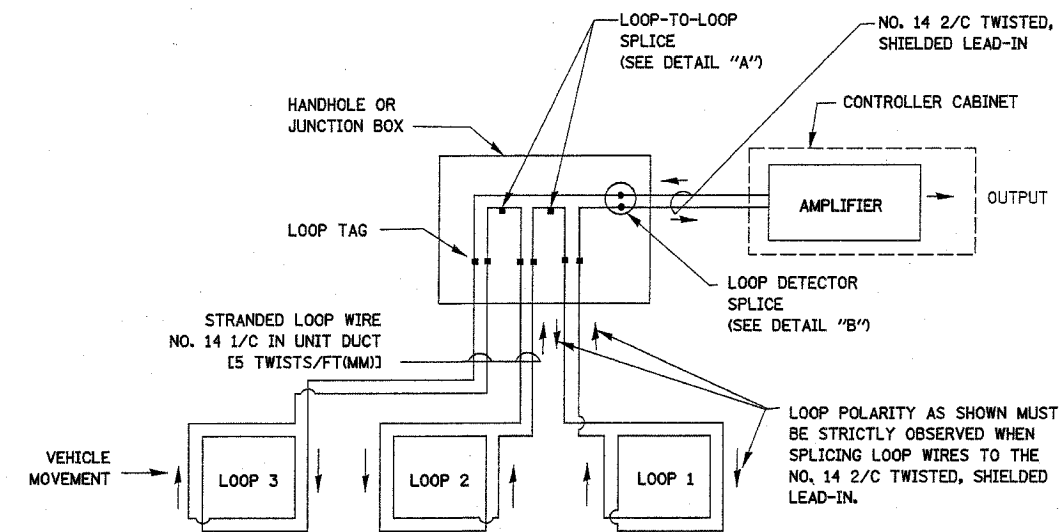
ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC SIGNAL
 INSTALLATION PLAN**
 IL RTE 171 (E ARCHER AV.)
 AT 131ST ST. / ASHBURY DR.
 LEMONT, ILLINOIS

SCALE: 1"=20'
 DATE: 01-22-07

DRAWN BY: BR
 DESIGNED BY: VO
 CHECKED BY: TJM

LOOP DETECTOR NOTES

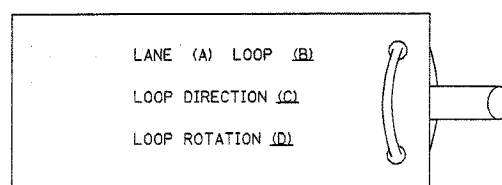
- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



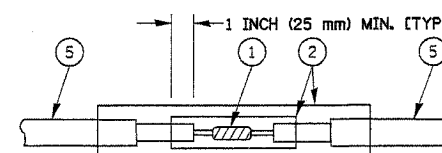
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

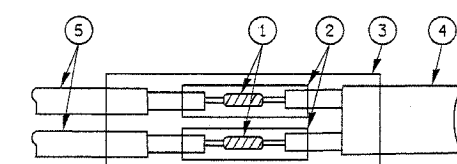
LOOP LEAD-IN CABLE TAG



- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

STON ENGINEERING
CONSULTING ENGINEERS
19 S. BOTHWELL STREET
PALATINE, ILLINOIS 60067
VOICE: 847-776-7200 FAX: 847-776-7239

| REVISIONS | |
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| NAME | DATE |
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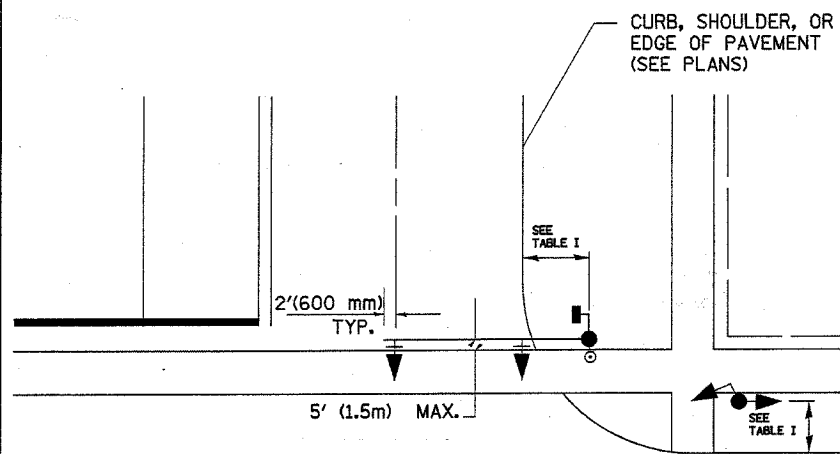
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: N.T.S.
DATE: 01-22-07
DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 1 OF 4

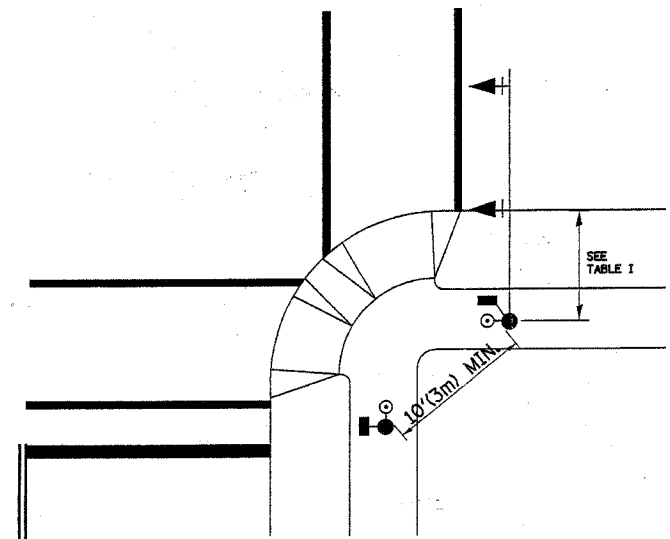
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114N-1 | COOK | 72 | 38 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 60A25 | | | | |

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.
 AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.
 PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK.
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

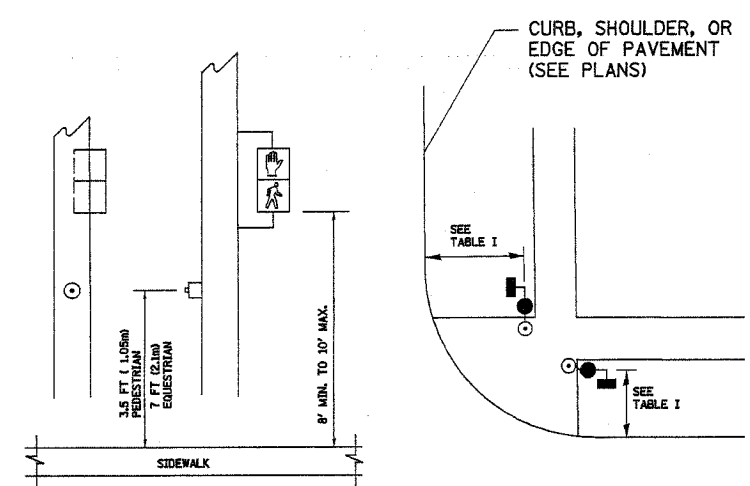


TABLE I

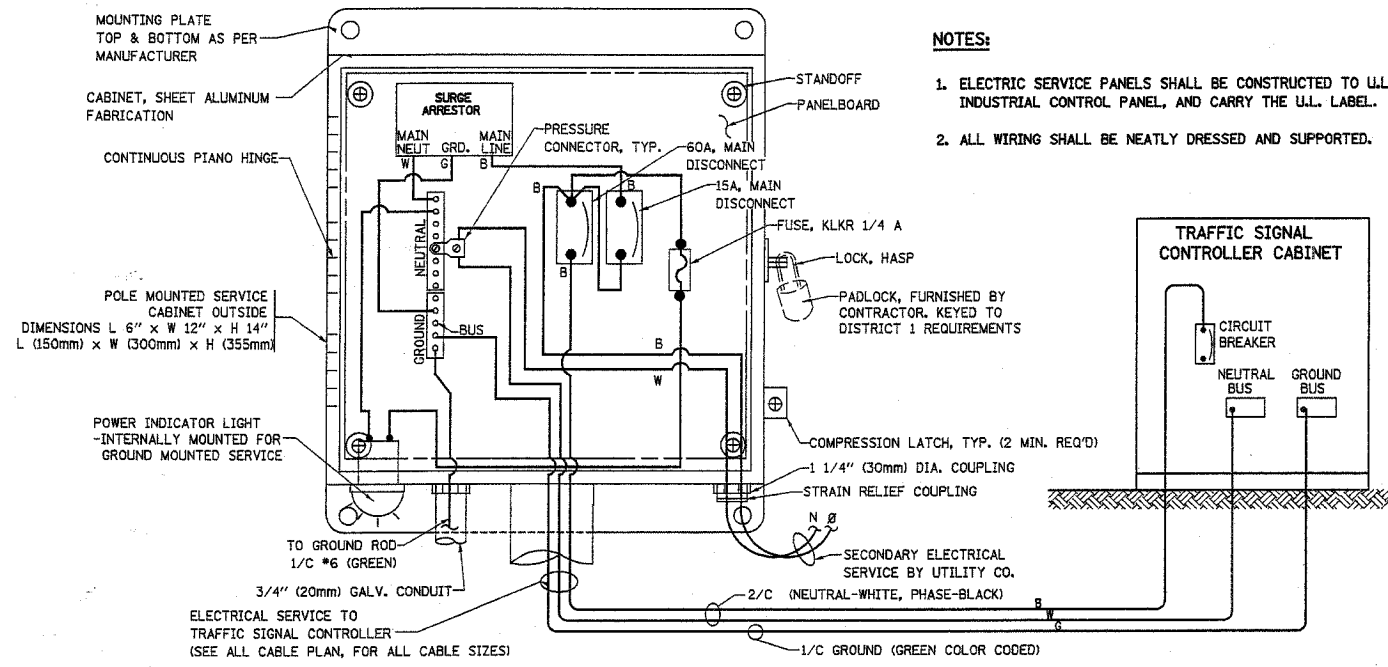
| TRAFFIC SIGNAL EQUIPMENT | COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB) | SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT) |
|------------------------------|---|---|
| TRAFFIC SIGNAL MAST ARM POLE | 6 FT (1.8m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| TRAFFIC SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| PEDESTRIAN SIGNAL POST | 4 FT (1.2m) | SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m) |
| PEDESTRIAN PUSHBUTTON | SEE NOTE 1 | SEE NOTE 1 |

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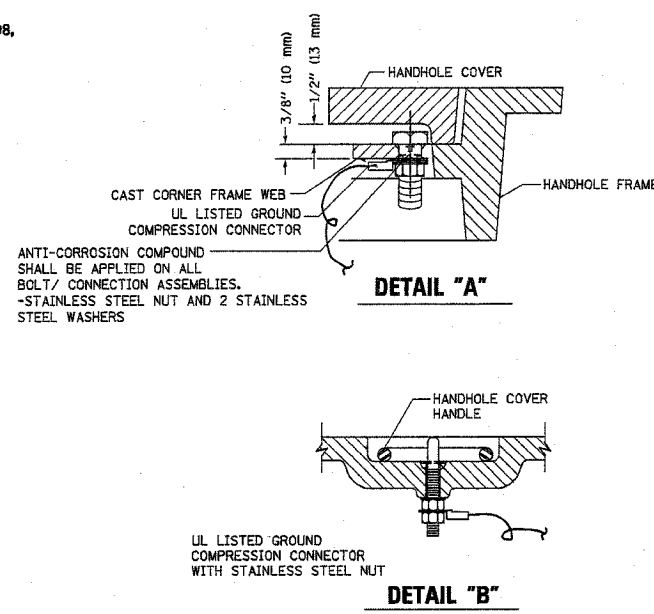
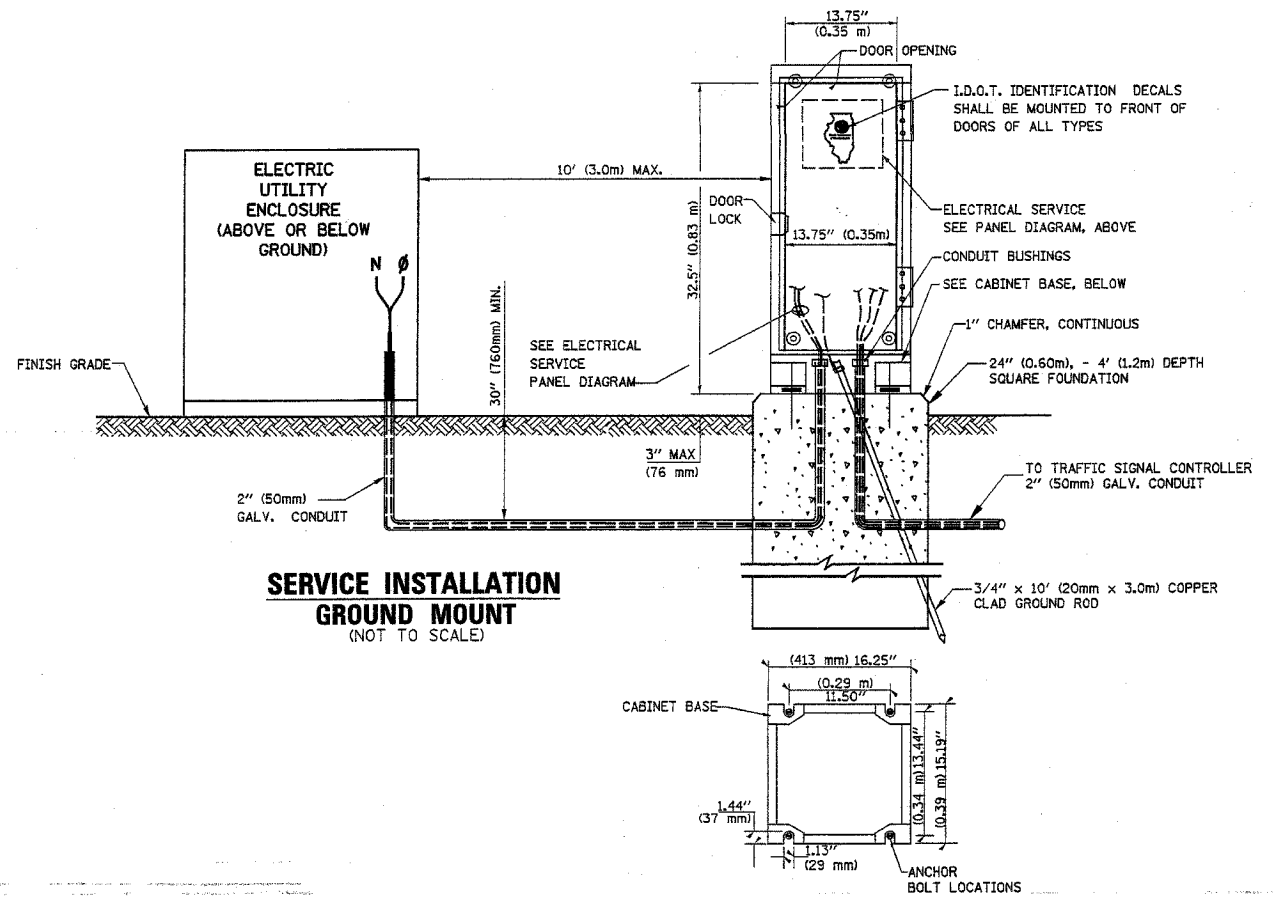
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS
 SCALE: N.T.S.
 DATE: 01-22-07
 DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 2 OF 4

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|--------------------------|---------|---|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114N-1 | COOK | 22 | 22 |
| STA. _____ TO STA. _____ | | FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 60A25 | | | | |



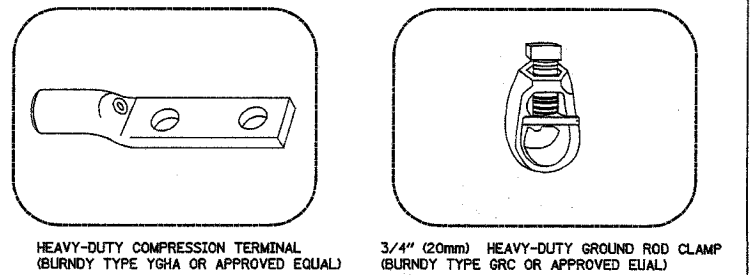
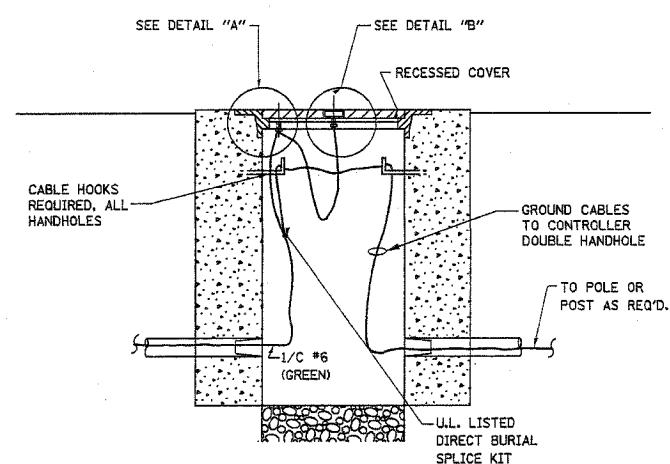
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



NOTES:

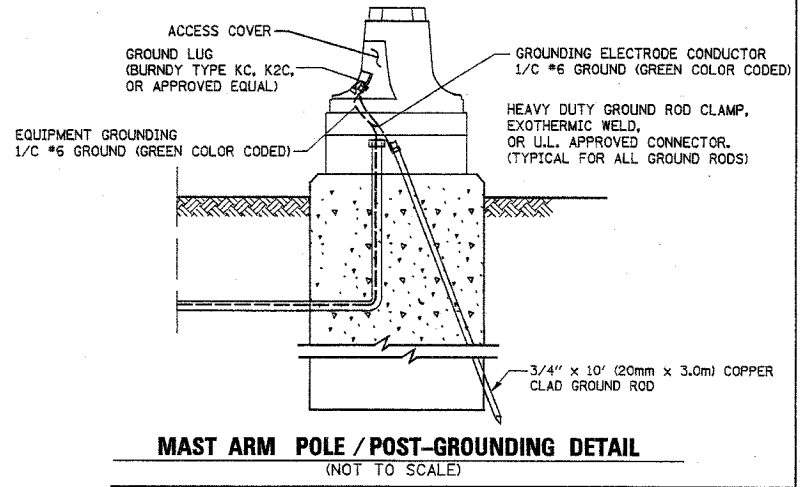
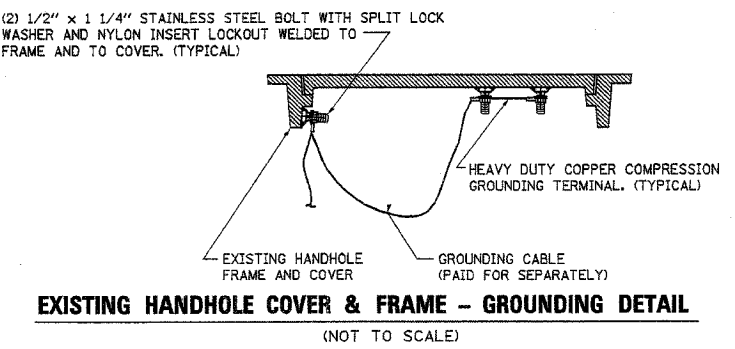
GROUNDING SYSTEM

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



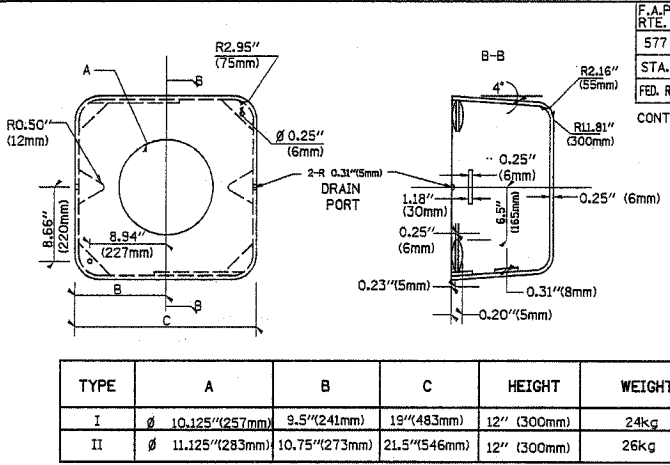
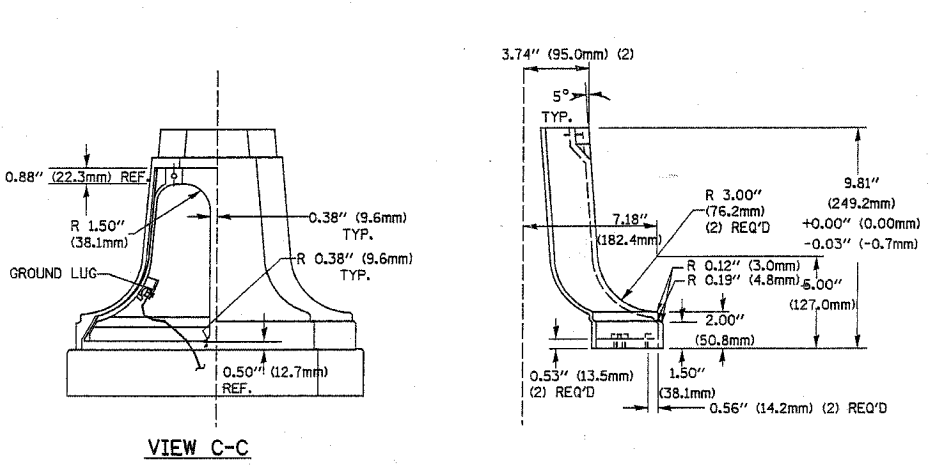
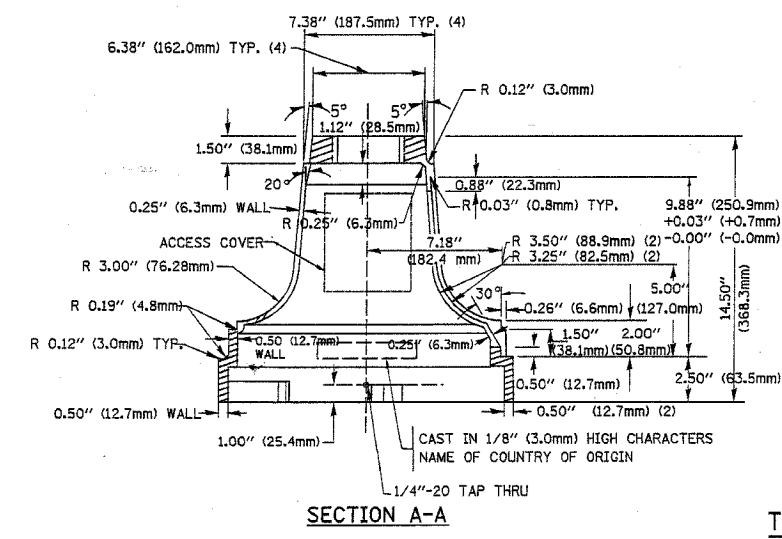
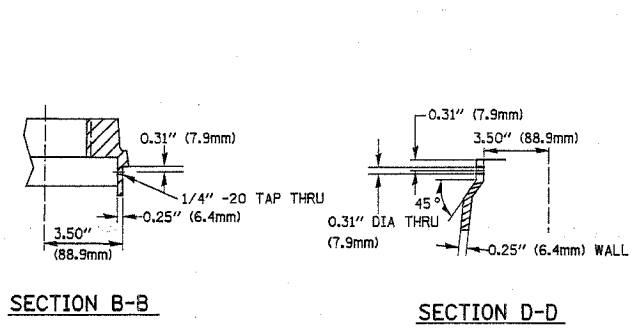
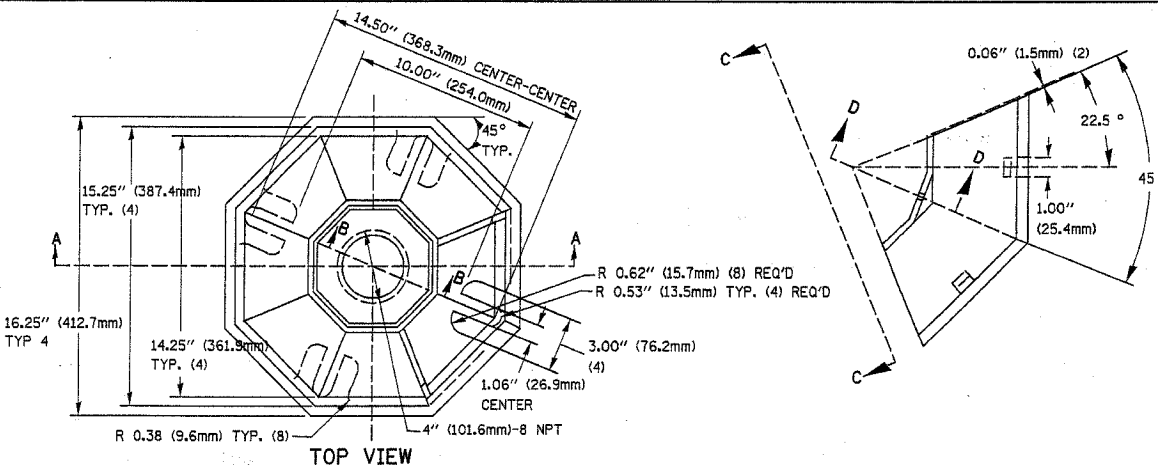
STON ENGINEERING
 SERVICE CORPORATION
 CIVIL ENGINEERS
 19 S. BOTHWELL STREET
 PALATINE, ILLINOIS 60067
 VOICES 847-776-7200 FAX 847-776-7239

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS

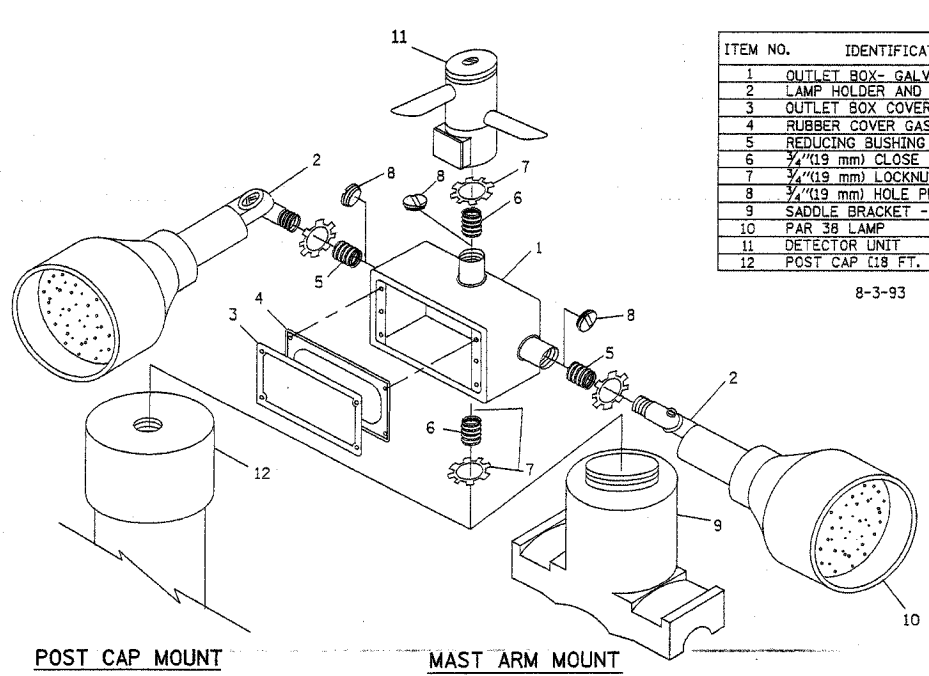
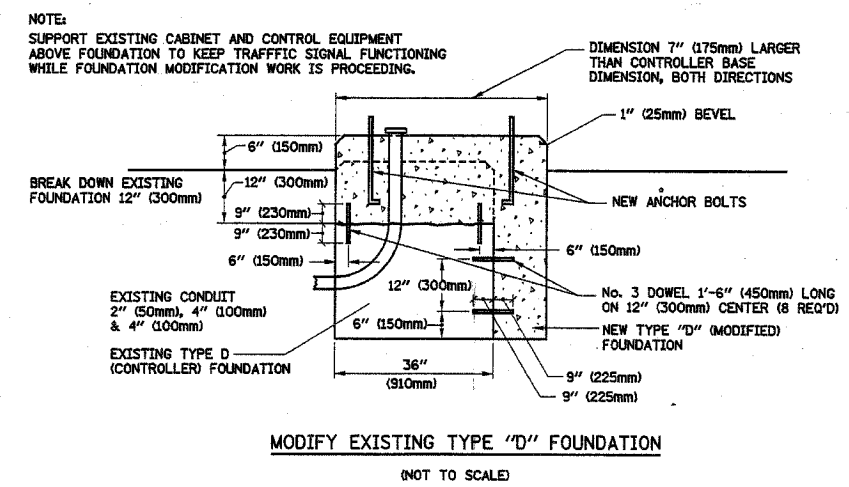
SCALE: N.T.S.
 DATE: 01-22-07

DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 3 OF 4



| TYPE | A | B | C | HEIGHT | WEIGHT |
|------|-------------------|----------------|---------------|-------------|--------|
| I | Ø 10.125\"(257mm) | 9.5\"(241mm) | 19\"(483mm) | 12\"(300mm) | 24kg |
| II | Ø 11.125\"(283mm) | 10.75\"(273mm) | 21.5\"(546mm) | 12\"(300mm) | 26kg |

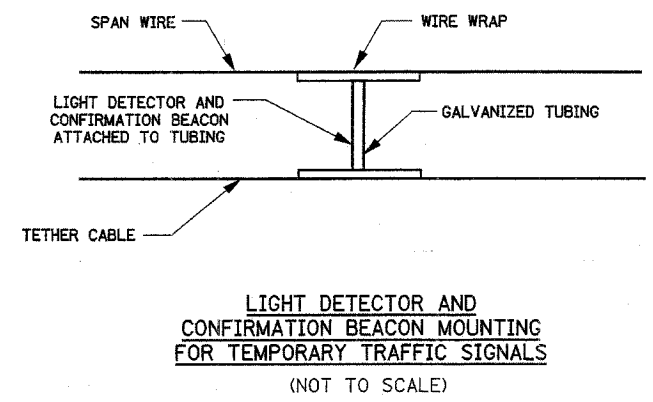
MATERIAL:
 - ASTM A48 CLASS 30 GREY IRON
 - ASTM A123 HOT DIPPED GALVANIZED



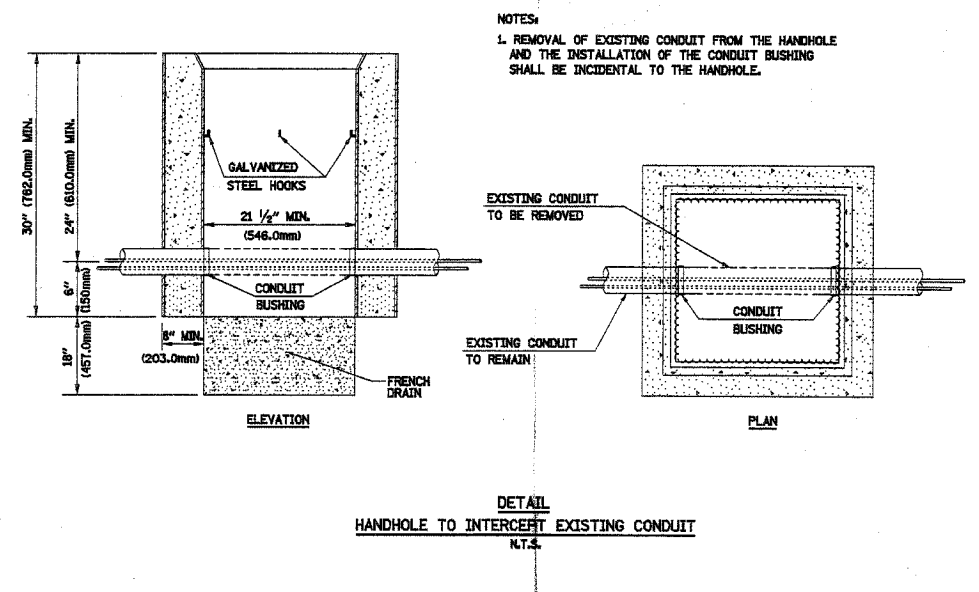
| ITEM NO. | IDENTIFICATION |
|----------|---|
| 1 | OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M) |
| 2 | LAMP HOLDER AND COVER |
| 3 | OUTLET BOX COVER |
| 4 | RUBBER COVER GASKET |
| 5 | REDUCING BUSHING |
| 6 | 3/4\"(19 mm) CLOSE NIPPLE |
| 7 | 3/4\"(19 mm) LOCKNUT |
| 8 | 3/4\"(19 mm) HOLE PLUG |
| 9 | SADDLE BRACKET - GALV. |
| 10 | PAR 38 LAMP |
| 11 | DETECTOR UNIT |
| 12 | POST CAP [18 FT. (5.4 m) POST MIN.] |

- NOTES:
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

POST CAP MOUNT MAST ARM MOUNT
 EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS (NOT TO SCALE)



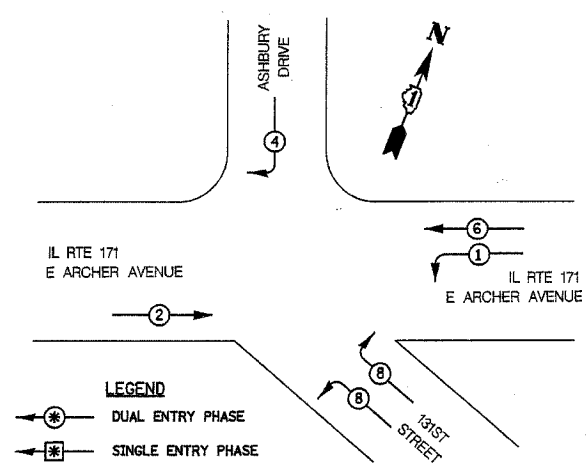
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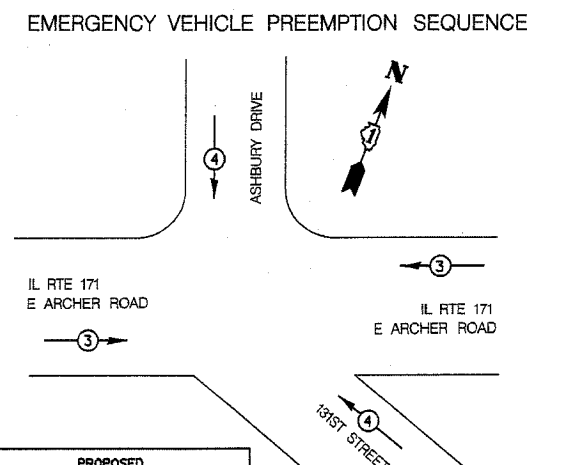
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS
 SCALE: N.T.S.
 DATE 01-22-07
 DRAWN BY RWP
 DESIGNED BY DAD
 CHECKED BY DAZ
 SHEET 4 OF 4

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------|---------------------------|--------------|-----------|
| 577 | 3114N-1 | COOK | 72 | 41 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. _____ | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 60A25 | | | | |

CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

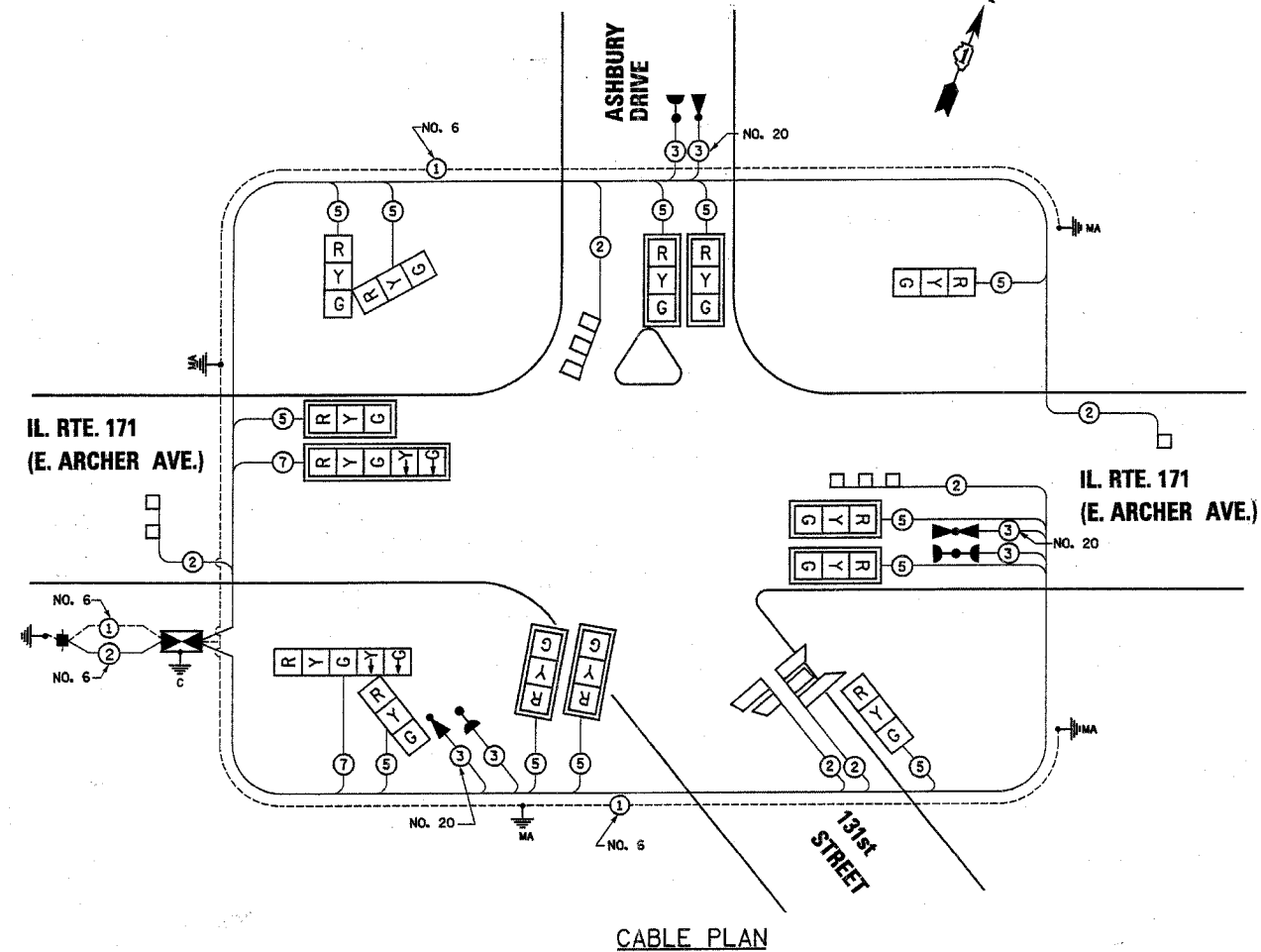


| PROPOSED EMERGENCY VEHICLE PREEMPTORS | |
|---------------------------------------|---------|
| EMERGENCY VEHICLE PREEMPTOR | 3 4 |
| MOVEMENT | ← → ↑ ↓ |

| I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS | | | | | TOTAL WATTAGE |
|--|-----------|-----------------|-----|--------------|---------------|
| TYPE | NO. LAMPS | WATTAGE INCAND. | LED | X% OPERATION | |
| SIGNAL (RED) | 14 | 135 | 17 | 0.50 | 119.0 |
| (YELLOW) | 14 | 135 | 25 | 0.25 | 87.5 |
| (GREEN) | 14 | 135 | 15 | 0.25 | 52.5 |
| ARROW | 4 | 135 | 12 | 0.10 | 4.8 |
| CONTROLLER | 1 | 100 | 100 | 1.00 | 100.0 |
| TOTAL = | | | | | 363.8 |

ENERGY COSTS - BILLED TO: VILLAGE OF LEMONT
 (ADDRESS) 418 MAIN STREET
 LEMONT, ILLINOIS 60439

ENERGY SUPPLY - CONTACT: DOLORES KREMNITZER
 PHONE: (815) 724-5241
 COMPANY: COMED



CABLE PLAN

CABLE PLAN LEGEND

| EXISTING | PROPOSED | EXISTING | PROPOSED |
|----------|--|----------|--|
| [Symbol] | [Symbol] 8" (200mm) TRAFFIC SIGNAL SECTION | [Symbol] | [Symbol] GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) |
| [Symbol] | [Symbol] 12" (300mm) TRAFFIC SIGNAL SECTION | [Symbol] | [Symbol] FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F SMI2F |
| [Symbol] | [Symbol] 12" (300mm) PEDESTRIAN SIGNAL SECTION | [Symbol] | [Symbol] SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD |
| [Symbol] | [Symbol] 12" (300mm) PEDESTRIAN SIGNAL SECTION | [Symbol] | [Symbol] RAILROAD CONTROL CABINET |
| [Symbol] | [Symbol] CONTROLLER CABINET | [Symbol] | [Symbol] ILLUMINATED SIGN "NO LEFT TURN" |
| [Symbol] | [Symbol] SERVICE INSTALLATION | [Symbol] | [Symbol] ILLUMINATED SIGN "NO RIGHT TURN" |
| [Symbol] | [Symbol] TELEPHONE CONNECTION | [Symbol] | [Symbol] GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (HH), OR CONTROLLER (C) |
| [Symbol] | [Symbol] VEHICLE DETECTOR, INDUCTION LOOP | [Symbol] | [Symbol] GROUND ROD AT POST (P), OR MAST ARM POLE (MA) |
| [Symbol] | [Symbol] MAGNETIC DETECTOR | [Symbol] | [Symbol] GROUND ROD AT ELECTRIC SERVICE INSTALLATION |
| [Symbol] | [Symbol] EMERGENCY VEHICLE LIGHT DETECTOR | [Symbol] | [Symbol] MICROWAVE VEHICLE SENSOR |
| [Symbol] | [Symbol] CONFIRMATION BEACON | | |
| [Symbol] | [Symbol] PUSHBUTTON DETECTOR | | |
| [Symbol] | [Symbol] DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. | | |

| FOUNDATION (DEPTH) | (FT.) (m) | CABLE SLACK | (FT.) (m) | VERTICAL | (FT.) (m) |
|--------------------|-----------|------------------|-----------|-------------------|----------------------------------|
| TYPE A - POST | 4 (1.2) | HANDHOLE | 6.5 (2.0) | ALL FOUNDATIONS | 3.5 (1.0) |
| D - FOUNDATION | 4 (1.2) | DOUBLE HANDHOLE | 13 (4.0) | MAST ARM (L) POLE | 20' + L - 2' = (6m + L - 0.6m) = |
| C - M. ARM POLE | | SIGNAL POST | 2 (0.6) | BRACKET MOUNTED | 13 (4.0) |
| 24" (600mm) | 10 (3.0) | CONTROLLER CAB. | 1 (0.5) | PED. PUSHBUTTON | 4 (1.2) |
| 30" (750mm) | 15 (4.6) | FIBER OPTIC | 13 (4.0) | ELECTRIC SERVICE | 13.5 (4.1) |
| | | ELECTRIC SERVICE | 1 (0.5) | SERVICE TO GROUND | 13.5 (4.1) |
| | | GROUND CABLE | 1 (0.5) | POST MOUNTED | 6 (1.8) |

SCHEDULE OF QUANTITIES

| ITEM | UNIT | QUANTITY |
|--|--------|----------|
| SIGN PANEL - TYPE 1 | SQ FT | 28.5 |
| SIGN PANEL - TYPE 2 | SQ FT | 25 |
| CHANGEABLE MESSAGE SIGN | CAL MO | 1 |
| SERVICE INSTALLATION, POLE MOUNT | EACH | 1 |
| CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL | FOOT | 499 |
| CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL | FOOT | 34 |
| CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL | FOOT | 10 |
| CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL | FOOT | 69 |
| CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL | FOOT | 349 |
| HANDHOLE | EACH | 3 |
| HEAVY DUTY HANDHOLE | EACH | 5 |
| DOUBLE HANDHOLE | EACH | 1 |
| TRENCH AND BACKFILL FOR ELECTRICAL WORK | FOOT | 538 |
| FULL-ACTUATED CONTROLLER AND TYPE IV CABINET | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C | FOOT | 446 |
| * ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED | FOOT | 614 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14, 3C | FOOT | 612 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14, 5C | FOOT | 2175 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14, 7C | FOOT | 218 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14, 1 PAIR | FOOT | 1359 |
| ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6, 2C | FOOT | 54 |
| STEEL MAST ARM ASSEMBLY AND POLE, 18 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 30 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 34 FT. | EACH | 1 |
| STEEL MAST ARM ASSEMBLY AND POLE, 40 FT. | EACH | 1 |
| CONCRETE FOUNDATION, TYPE C | FOOT | 4 |
| CONCRETE FOUNDATION, TYPE E, 30-INCH DIAMETER | FOOT | 60 |
| SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED | EACH | 7 |
| SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 2 |
| SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED | EACH | 1 |
| SIGNAL HEAD, L.E.D., 2-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 1 |
| SIGNAL HEAD, L.E.D., 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED | EACH | 1 |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM | EACH | 8 |
| INDUCTIVE LOOP DETECTOR | EACH | 5 |
| DETECTOR LOOP, TYPE 1 | FOOT | 512 |
| * LIGHT DETECTOR | EACH | 4 |
| * LIGHT DETECTOR AMPLIFIER | EACH | 1 |

* - 100% COST OF VILLAGE OF LEMONT

NOTE: THE PORTABLE CHANGEABLE MESSAGE SIGN (CMS) SHALL BE OPERATIONAL ONE WEEK IN ADVANCE OF THE NEW TRAFFIC SIGNAL TURN-ON AND WILL INCLUDE A MESSAGE IDENTIFYING THE NEW TRAFFIC SIGNAL LOCATION AND THE DATE IT WILL BE OPERATIONAL.

STON ENGINEERING
 CONSULTING SERVICE CORPORATION
 CIVIL ENGINEERS

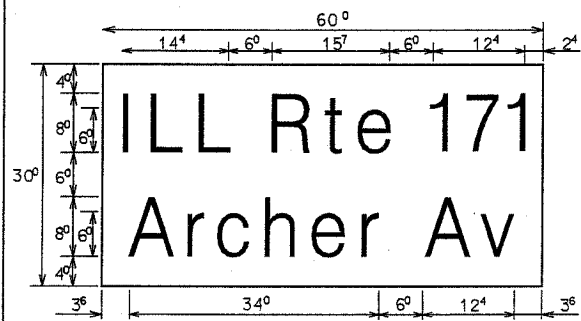
19 S. BOTHWELL STREET
 PALATINE, ILLINOIS 60067
 VOICE: 847-776-7200 FAX: 847-776-7239

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
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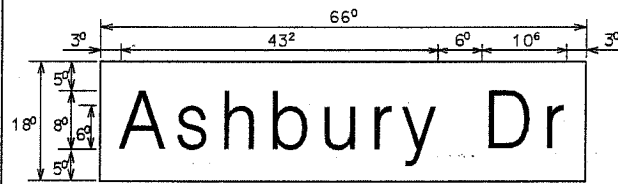
ILLINOIS DEPARTMENT OF TRANSPORTATION
 CABLE PLAN
 PHASE DESIGNATION DIAGRAM
 EMERGENCY VEHICLE PREEMPTION SEQUENCE
 AND SCHEDULE OF QUANTITIES
 IL RTE 171 (E. ARCHER AV.) AT 131ST ST.
 AND ASHBURY DR. LEMONT, ILLINOIS

SCALE: N.T.S. DRAWN BY BR
 DATE 01-22-07 DESIGNED BY VO
 CHECKED BY TJM

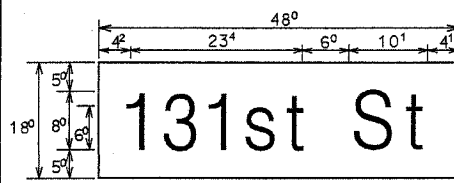
PANEL SIGN DESIGN TYPE 2



12.50 Sq. Ft. each
1 Required
Design Series D



8.25 Sq. Ft. each
2 Required
Design Series D



6.00 Sq. Ft. each
2 Required
Design Series D

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS.

GENERAL NOTES

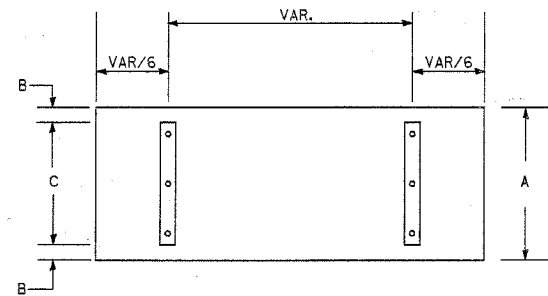
- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" X 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:
 - *A.K.T. CORPORATION *AMERICAN FABRICATION CO.
 - SCHAUMBURG, IL CHICAGO HEIGHTS, IL
 - *TUCKER COMPANY, INC. *WESTERN TRAFFIC CONTROL, INC.
 - WAUWATOSA, WI CICERO, IL

PART LISTING:

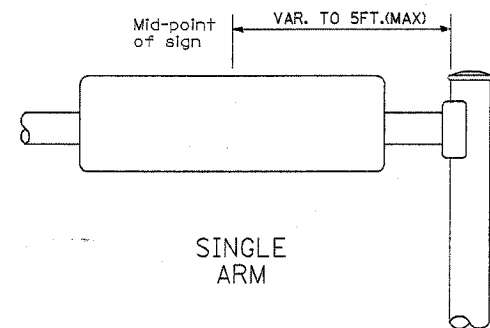
| | |
|--------------|---|
| SIGN CHANNEL | PART #HPN053(MED. CHANNEL) |
| SIGN SCREWS | 1/4" X 14 X 1" H.W.H. #3 |
| BRACKETS | SELF TAPPING WITH NEOPRENE WASHER PART # HPN034 (UNIVERSAL) CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING |

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

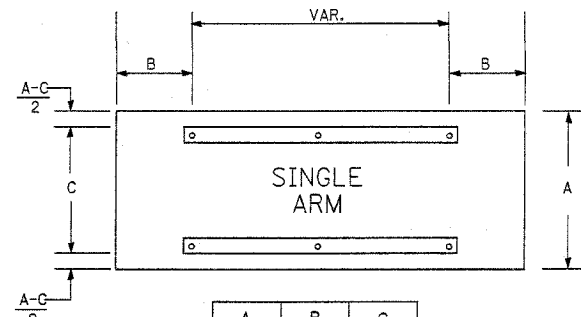
SUPPORTING CHANNELS



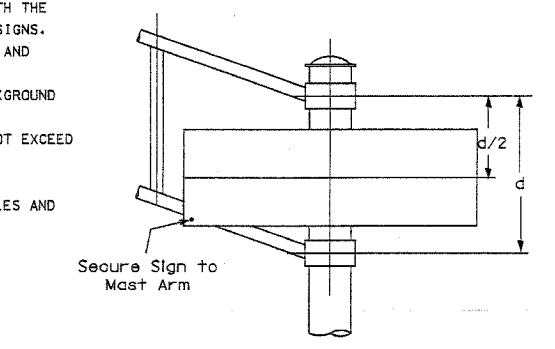
| A | B | C |
|-----|----|-----|
| 18" | 2" | 14" |



SUPPORTING CHANNELS



| A | B | C |
|-----|----|-----|
| 18" | 2" | 14" |
| 30" | 2" | 22" |



DUAL ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5

EXAMPLE, 2 DENOTES 3/8"

Upper Case To Lower Case Spacing Chart 8-6 Inch Series "C & D"

| SERIES | acde goq | | bhikl mnp ru | | fw | | j | | st | | vy | | x | | z | |
|---------|----------|----|--------------|----|----|----|----|----|----|----|----|----|----|----|----|----|
| | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D |
| A W X | 12 | 14 | 14 | 15 | 12 | 14 | 06 | 10 | 11 | 14 | 06 | 10 | 11 | 12 | 12 | 14 |
| B | 14 | 15 | 20 | 21 | 14 | 15 | 11 | 12 | 14 | 15 | 12 | 14 | 12 | 14 | 16 | 17 |
| C E G | 14 | 15 | 20 | 21 | 12 | 14 | 06 | 10 | 12 | 14 | 12 | 14 | 14 | 15 | 14 | 15 |
| D O Q R | 14 | 15 | 20 | 21 | 14 | 15 | 06 | 10 | 12 | 14 | 12 | 14 | 14 | 15 | 14 | 15 |
| F | 05 | 06 | 14 | 15 | 06 | 10 | 05 | 06 | 06 | 10 | 06 | 10 | 06 | 10 | 11 | 12 |
| H I M N | 20 | 21 | 22 | 24 | 20 | 21 | 14 | 15 | 16 | 17 | 16 | 17 | 20 | 21 | 20 | 21 |
| J U | 20 | 21 | 20 | 21 | 16 | 17 | 14 | 15 | 16 | 17 | 16 | 17 | 16 | 17 | 20 | 21 |
| K L | 11 | 12 | 16 | 17 | 11 | 12 | 05 | 06 | 11 | 12 | 11 | 12 | 11 | 12 | 12 | 14 |
| P | 12 | 14 | 14 | 15 | 12 | 14 | 05 | 06 | 11 | 12 | 11 | 12 | 12 | 14 | 12 | 14 |
| S | 12 | 14 | 16 | 17 | 12 | 14 | 06 | 10 | 12 | 14 | 12 | 14 | 12 | 14 | 12 | 14 |
| T | 11 | 12 | 16 | 17 | 06 | 10 | 06 | 10 | 11 | 12 | 11 | 12 | 11 | 12 | 12 | 14 |
| V | 06 | 10 | 14 | 15 | 11 | 12 | 06 | 10 | 12 | 14 | 12 | 14 | 12 | 14 | 12 | 14 |
| Y | 05 | 06 | 14 | 15 | 06 | 10 | 05 | 06 | 05 | 07 | 05 | 06 | 06 | 10 | 11 | 12 |
| Z | 16 | 17 | 22 | 24 | 16 | 17 | 12 | 14 | 16 | 17 | 16 | 17 | 16 | 17 | 20 | 21 |

Lower Case To Lower Case Spacing Chart 6 Inch Series "C & D"

| SERIES | acde go p | | bhikl mnp ru | | fw | | j | | st | | vy | | x | | z | |
|--------|-----------|----|--------------|----|----|----|----|----|----|----|----|----|----|----|----|----|
| | C | D | C | D | C | D | C | D | C | D | C | D | C | D | C | D |
| adhgij | 16 | 17 | 22 | 24 | 16 | 17 | 12 | 14 | 14 | 15 | 14 | 15 | 16 | 17 | 16 | 17 |
| lmnqu | 16 | 17 | 22 | 24 | 16 | 17 | 12 | 14 | 14 | 15 | 14 | 15 | 16 | 17 | 16 | 17 |
| bfkops | 12 | 14 | 16 | 17 | 11 | 12 | 05 | 06 | 11 | 12 | 11 | 12 | 12 | 14 | 12 | 14 |
| ce | 12 | 14 | 16 | 17 | 12 | 14 | 06 | 10 | 12 | 14 | 12 | 14 | 12 | 14 | 12 | 14 |
| r | 06 | 10 | 12 | 14 | 06 | 10 | 03 | 03 | 05 | 06 | 05 | 06 | 06 | 10 | 06 | 10 |
| tz | 12 | 14 | 16 | 17 | 12 | 14 | 06 | 10 | 11 | 12 | 11 | 12 | 12 | 14 | 12 | 14 |
| vy | 11 | 12 | 14 | 15 | 11 | 12 | 05 | 06 | 06 | 10 | 06 | 10 | 11 | 12 | 11 | 12 |
| w | 11 | 12 | 14 | 15 | 11 | 12 | 05 | 06 | 11 | 12 | 11 | 12 | 11 | 12 | 12 | 14 |
| x | 12 | 14 | 16 | 17 | 11 | 12 | 05 | 06 | 11 | 12 | 11 | 12 | 11 | 12 | 12 | 14 |

Number To Number Spacing Chart 8 Inch Series "C & D"

| SERIES | SECOND NUMBER | | | | | | | | | | | | | | | | | | | |
|--------|---------------|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|----|
| | 0 | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | | | | | | | | | | |
| 0 9 | 16 | 17 | 16 | 17 | 14 | 15 | 12 | 14 | 14 | 15 | 14 | 15 | 16 | 17 | 12 | 14 | 16 | 17 | 16 | 17 |
| 1 | 20 | 21 | 20 | 21 | 20 | 21 | 16 | 17 | 14 | 15 | 20 | 21 | 20 | 21 | 14 | 15 | 20 | 21 | 20 | 21 |
| 2 3 4 | 14 | 15 | 14 | 15 | 14 | 15 | 12 | 14 | 14 | 15 | 14 | 15 | 11 | 12 | 16 | 17 | 14 | 15 | | |
| 5 | 14 | 15 | 14 | 15 | 14 | 15 | 11 | 12 | 11 | 12 | 14 | 15 | 14 | 15 | 11 | 12 | 14 | 15 | 14 | 15 |
| 6 | 16 | 17 | 14 | 15 | 14 | 15 | 12 | 15 | 12 | 14 | 14 | 15 | 14 | 15 | 11 | 12 | 14 | 15 | 14 | 15 |
| 7 | 12 | 14 | 12 | 14 | 14 | 15 | 12 | 15 | 05 | 06 | 12 | 14 | 14 | 15 | 11 | 12 | 14 | 15 | 12 | 14 |
| 8 | 16 | 17 | 16 | 17 | 14 | 15 | 12 | 15 | 12 | 14 | 14 | 15 | 16 | 17 | 12 | 14 | 16 | 17 | 14 | 15 |

UPPER AND LOWER CASE LETTER WIDTHS

| LETTERS | 6 INCH UPPER CASE LETTERS | | 8 INCH UPPER CASE LETTERS | | LETTERS | 8 INCH LOWER CASE LETTERS | |
|---------|---------------------------|----------------|---------------------------|----------------|---------|---------------------------|----------------|
| | C | D | C | D | | C | D |
| A | 3 ⁶ | 5 ⁰ | 5 ⁰ | 6 ⁵ | a | 3 ⁵ | 4 ² |
| B | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | b | 3 ⁵ | 4 ² |
| C | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | c | 3 ⁵ | 4 ¹ |
| D | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | d | 3 ⁵ | 4 ² |
| E | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | e | 3 ⁵ | 4 ² |
| F | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | f | 2 ³ | 2 ⁶ |
| G | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | g | 3 ⁵ | 4 ² |
| H | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | h | 3 ⁵ | 4 ² |
| I | 0 ⁷ | 0 ⁷ | 1 ¹ | 1 ² | i | 1 ¹ | 1 ¹ |
| J | 3 ⁰ | 3 ⁶ | 4 ⁰ | 5 ⁰ | j | 2 ⁰ | 2 ² |
| K | 3 ² | 4 ¹ | 4 ³ | 5 ⁴ | k | 3 ⁵ | 4 ² |
| L | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | l | 1 ¹ | 1 ¹ |
| M | 3 ⁷ | 4 ⁵ | 5 ¹ | 6 ¹ | m | 6 ⁰ | 7 ⁰ |
| N | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | n | 3 ⁵ | 4 ² |
| O | 3 ⁴ | 4 ² | 4 ⁵ | 5 ⁵ | o | 3 ⁶ | 4 ³ |
| P | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | p | 3 ⁵ | 4 ² |
| Q | 3 ⁴ | 4 ² | 4 ⁵ | 5 ⁵ | q | 3 ⁵ | 4 ² |
| R | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | r | 2 ⁶ | 3 ² |
| S | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | s | 3 ⁶ | 4 ² |
| T | 3 ⁰ | 3 ⁵ | 4 ⁰ | 4 ⁷ | t | 2 ⁷ | 3 ² |
| U | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | u | 3 ⁵ | 4 ² |
| V | 3 ⁵ | 4 ⁴ | 4 ⁷ | 6 ⁰ | v | 4 ² | 4 ⁷ |
| W | 4 ⁴ | 5 ² | 6 ⁰ | 7 ⁰ | w | 5 ⁵ | 6 ⁴ |
| X | 3 ⁴ | 4 ⁰ | 4 ⁵ | 5 ³ | x | 4 ⁴ | 5 ¹ |
| Y | 3 ⁵ | 5 ⁰ | 5 ⁰ | 6 ⁶ | y | 4 ⁶ | 5 ³ |
| Z | 3 ² | 4 ⁰ | 4 ³ | 5 ³ | z | 3 ⁶ | 4 ³ |

| NUMBER | 6 INCH SERIES | | 8 INCH SERIES | |
|--------|----------------|----------------|----------------|----------------|
| | C | D | C | D |
| 1 | 1 ² | 1 ⁴ | 1 ⁵ | 2 ⁰ |
| 2 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 3 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 4 | 3 ⁵ | 4 ³ | 4 ⁷ | 5 ⁷ |
| 5 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 6 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 7 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 8 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 9 | 3 ² | 4 ⁰ | 4 ³ | 5 ³ |
| 0 | 3 ⁴ | 4 ² | 4 ⁵ | 5 ⁵ |

| REVISIONS | |
|-----------|------|
| NAME | DATE |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAST ARM MOUNTED STREET NAME SIGNS
 IL RTE 171 (E. ARCHER AV.)
 AT 131ST ST. / ASHBURY DR.
 LEMONT, ILLINOIS

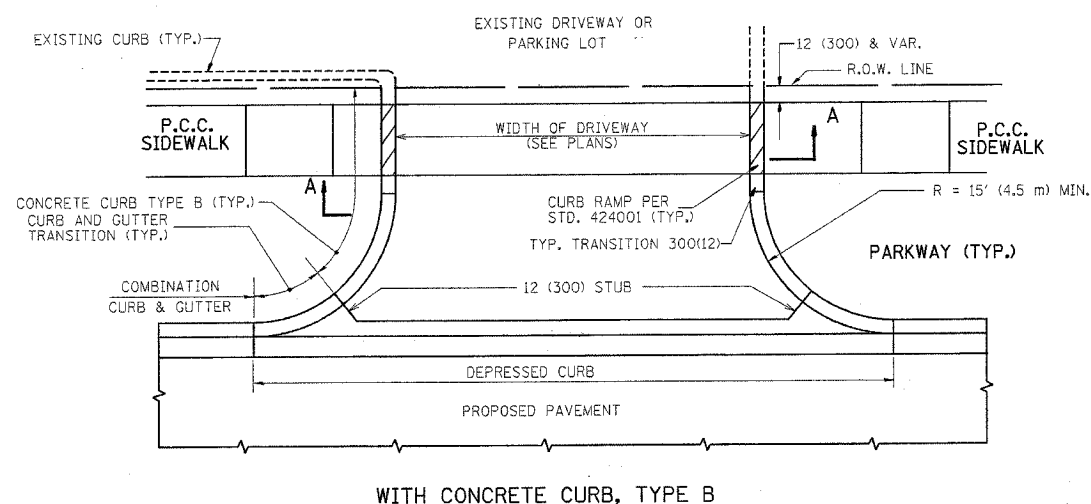
SCALE: N.T.S.
 DATE 01-22-07

DRAWN BY BR
 DESIGNED BY VO
 CHECKED BY TJM

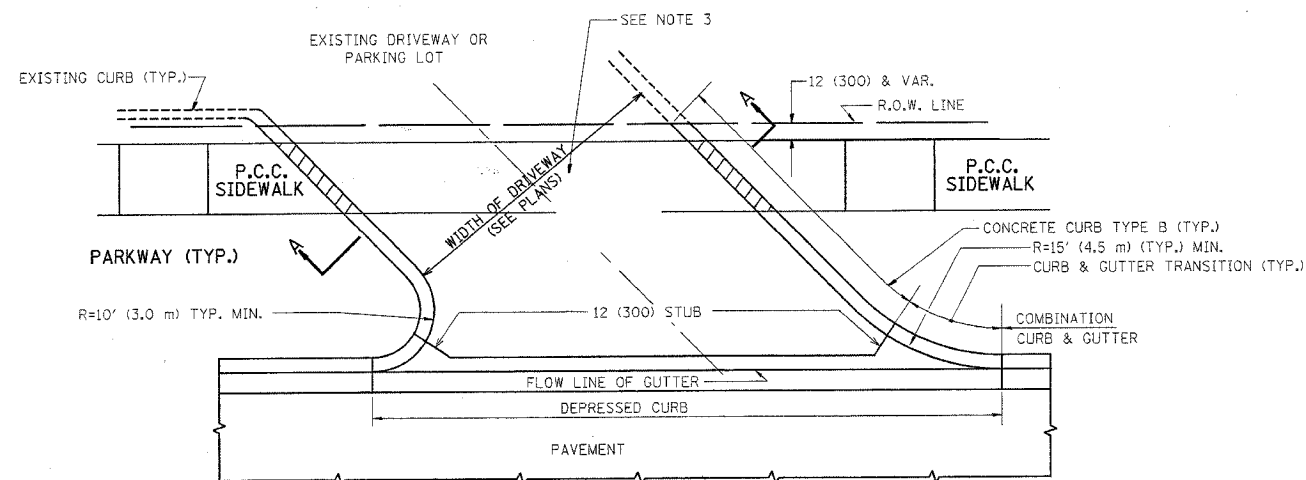
19 S. BOWWELL STREET
 PALATINE, ILLINOIS 60067
 VOICE: 847-776-7200 FAX: 847-776-7239

| | | | | |
|---|----------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 43 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

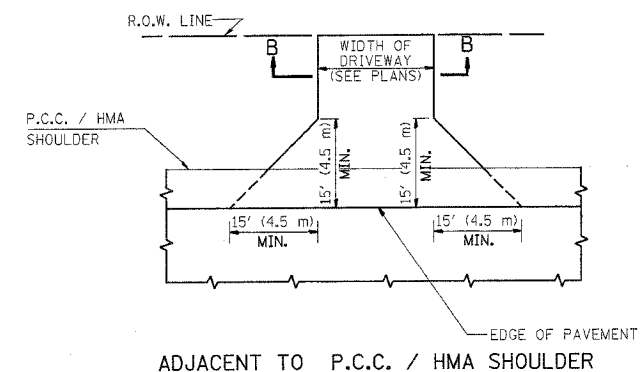
CONTRACT NO. 60A25



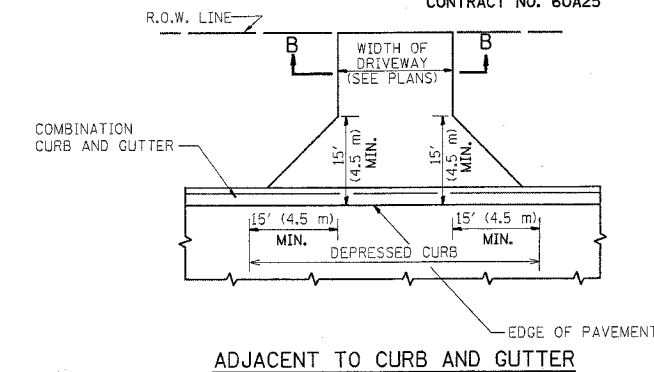
WITH CONCRETE CURB, TYPE B



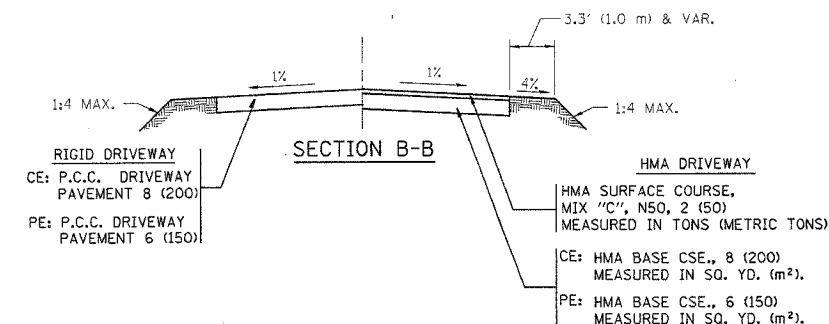
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (IFE)
HMA SURFACE COURSE,
MIX "C", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)
AGGREGATE BASE CSE., TYPE A 8 (200)
MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

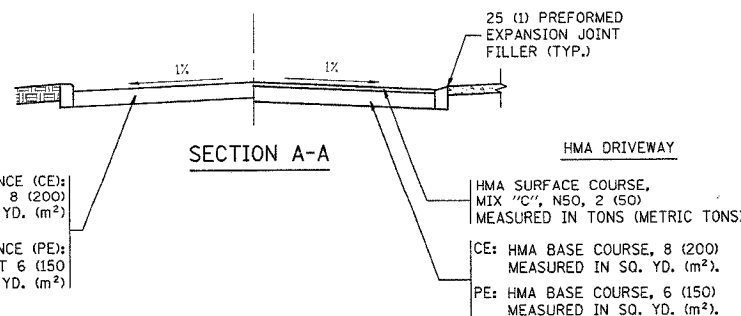
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

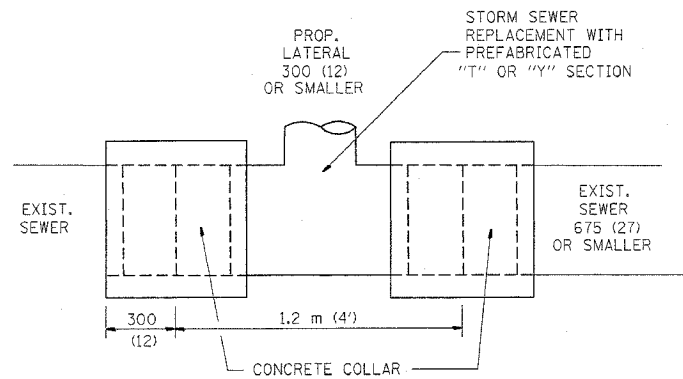
| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION | |
|----------------|----------|--|--|
| NAME | DATE | DRIVEWAY DETAILS DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m) | |
| R. SHAH | 11-04-95 | | |
| J. POLLASTRINI | 08-12-96 | | |
| J. POLLASTRINI | 12-14-96 | | |
| A. ABBAS | 03-21-97 | | |
| T. HOLTZ | 04-08-97 | | |
| M. GOMEZ | 04-06-01 | | |
| P. LOFLEUR | 04-15-03 | | |
| R. BORO | 01-01-07 | SCALE: VERT. NONE HORIZ. PLLOT DATE: 5/2/2007 | |

DRAWN BY
CHECKED BY

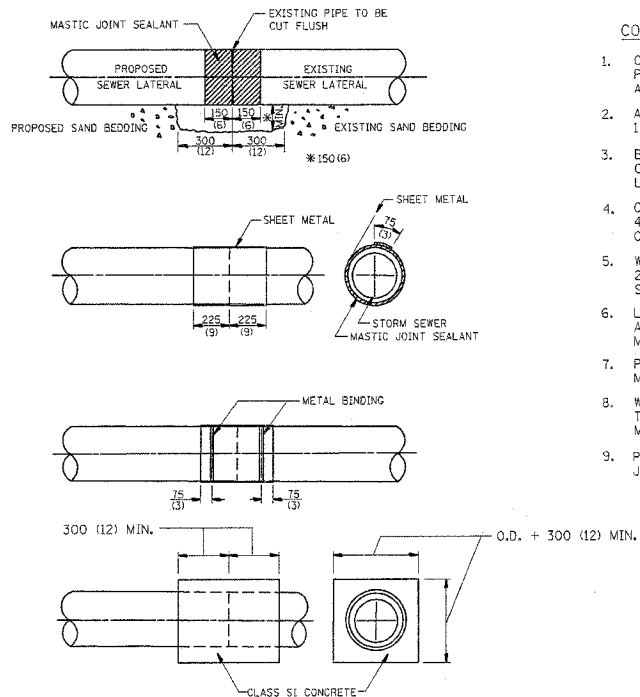
BD0156-07 (BD-01)
REVISION DATE: 01/01/07

PLOT DATE = 5/2/2007
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PLOT SCALE = 30.00000 / IN.
USER NAME = gab\gab

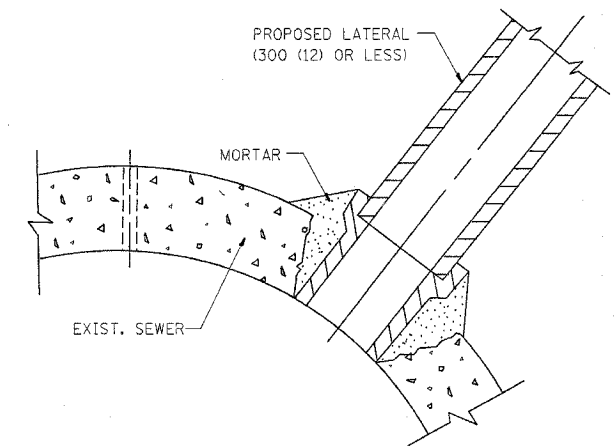
| | | | | |
|-----------------------|----------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 44 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 60A25 | | | | |



DETAIL "A"
LATERAL CONNECTION TO EXISTING SEWER
OF 675 (27) OR SMALLER



DETAIL "B"
CLASS SI CONCRETE COLLAR



DETAIL "C"
PROPOSED LATERAL
CONNECTION TO EXISTING SEWER
OF 750 (30) OR LARGER

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 150 (6) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 300 X 150 (12 X 6) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 450 (18) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 75 (3) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 225 (9) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 75 (3) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

I THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.

II CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:

- PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 675 (27) OR SMALLER SEE DETAIL "A" AND "B".
- PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 750 (30) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|------------|----------|
| NAME | DATE |
| M. DE YONG | 07/25/90 |
| M. DE YONG | 02/05/92 |
| M. DE YONG | 05/08/93 |
| R. SHAH | 09/09/94 |
| R. SHAH | 10/25/94 |
| R. SHAH | 06/12/96 |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: VERT.
HORIZ.
DATE: 5/2/2007

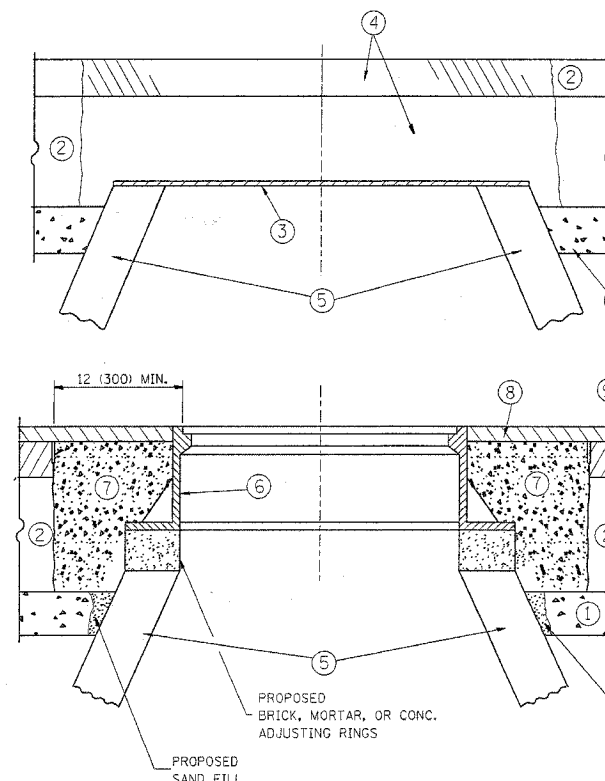
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CHECKED BY

BD500-01 (BD-7)

REVISION DATE: 06/12/96

| | | | | |
|-----------------------|----------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 45 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 60A25



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| REVISIONS | |
|-------------|----------|
| NAME | DATE |
| R. SHAH | 10/25/94 |
| R. SHAH | 01/30/95 |
| R. SHAH | 03/10/95 |
| A. ABBAS | 03/21/97 |
| R. WIEDEMAN | 05/14/04 |
| R. BORO | 01/01/07 |
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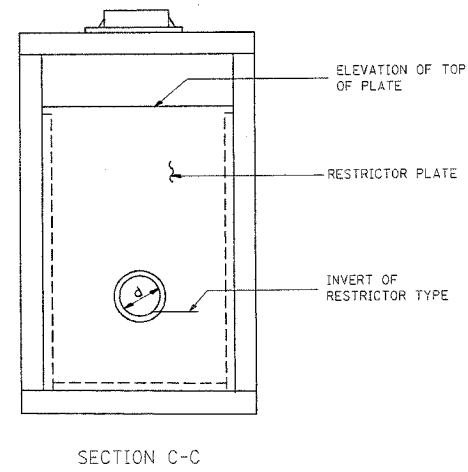
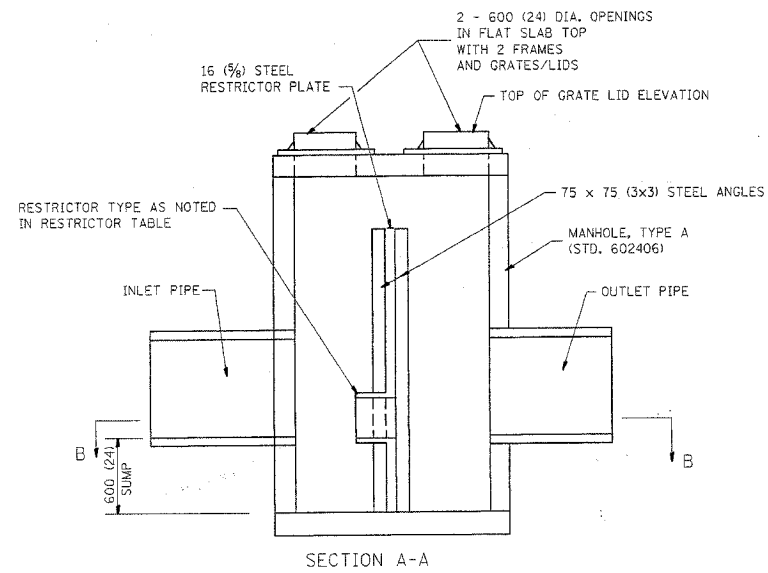
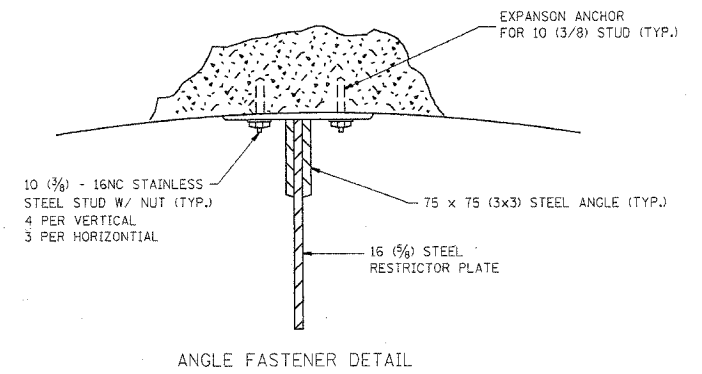
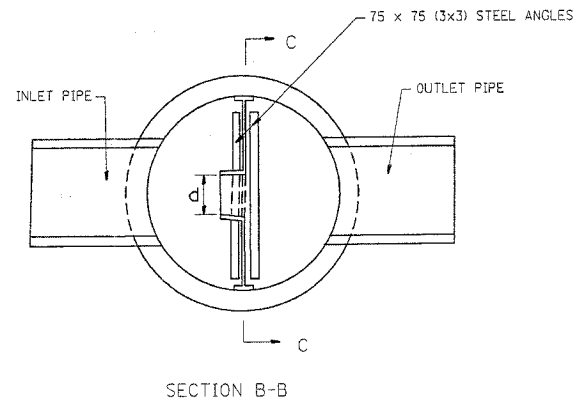
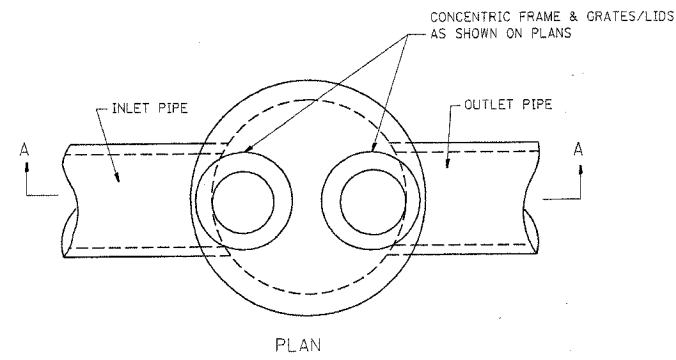
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: VERT. NONE
HORIZ. NONE
PLOT DATE: 5/2/2007

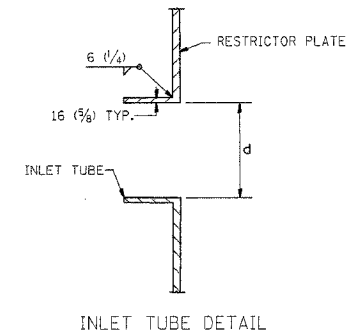
DRAWN BY
CHECKED BY

BD600-03 (BD-8)
REVISION DATE: 01/01/07

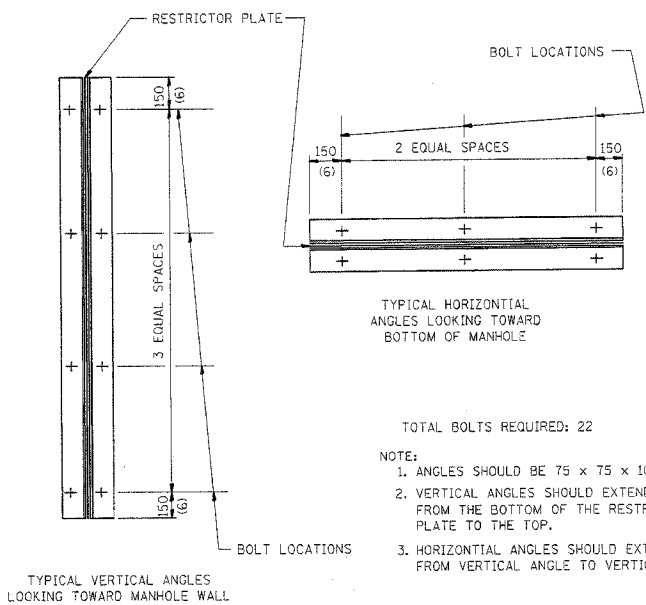
PLOT DATE = 5/2/2007
FILE NAME = C:\Users\galbarnd\Desktop\ludf8b.dgn
PLOT SCALE = 3/8"=1'-0"
USER NAME = galbarnd



- NOTES:
1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
 2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
 3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 1.8m (6FT)-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



| STATION | MANHOLE DIAMETER | FRAME AND GRATE | RESTRICTOR TYPE | INSIDE RESTRICTOR TYPE DIAMETER mm (in.) (d) | INVERT OF RESTRICTOR TYPE | ELEVATION OF TOP OF PLATE OVERFLOW |
|---------|------------------|-----------------|-----------------|--|---------------------------|------------------------------------|
| 14+00 | 5' DIA. | T1 CL | PLATE | 12 INCH. | 702.42 | 704.25 |
| | | | | | | |
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- TOTAL BOLTS REQUIRED: 22
- NOTE:
1. ANGLES SHOULD BE 75 x 75 x 10 (3x3x3/8)
 2. VERTICAL ANGLES SHOULD EXTEND FROM THE BOTTOM OF THE RESTRICTOR PLATE TO THE TOP.
 3. HORIZONTAL ANGLES SHOULD EXTEND FROM VERTICAL ANGLE TO VERTICAL ANGLE.

| RESTRICTOR TYPE | | | | | |
|-----------------------|------------|---------------------|--------------------|--------------------|---------|
| 1 | 2 | 3 | 4 | 5 | 6 |
| RE-ENTRANT TUBE | SHARP EGED | SQUARE EGED | RE-ENTRANT TUBE | SQUARE EGED | ROUNDED |
| | | | | | |
| LENGTH: 1/2 TO 1 DIA. | | STREAM CLEARS SIDES | LENGTH: 2-1/2 DIA. | LENGTH: 2-1/2 DIA. | |
| C=.52 | C=.61 | C=.61 | C=.73 | C=.82 | C=.98 |

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|-----------|----------|
| NAME | DATE |
| R. SHAH | 09/09/94 |
| R. SHAH | 10/25/94 |
| E. GOMEZ | 08/28/00 |
| M. GOMEZ | 01/08/01 |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

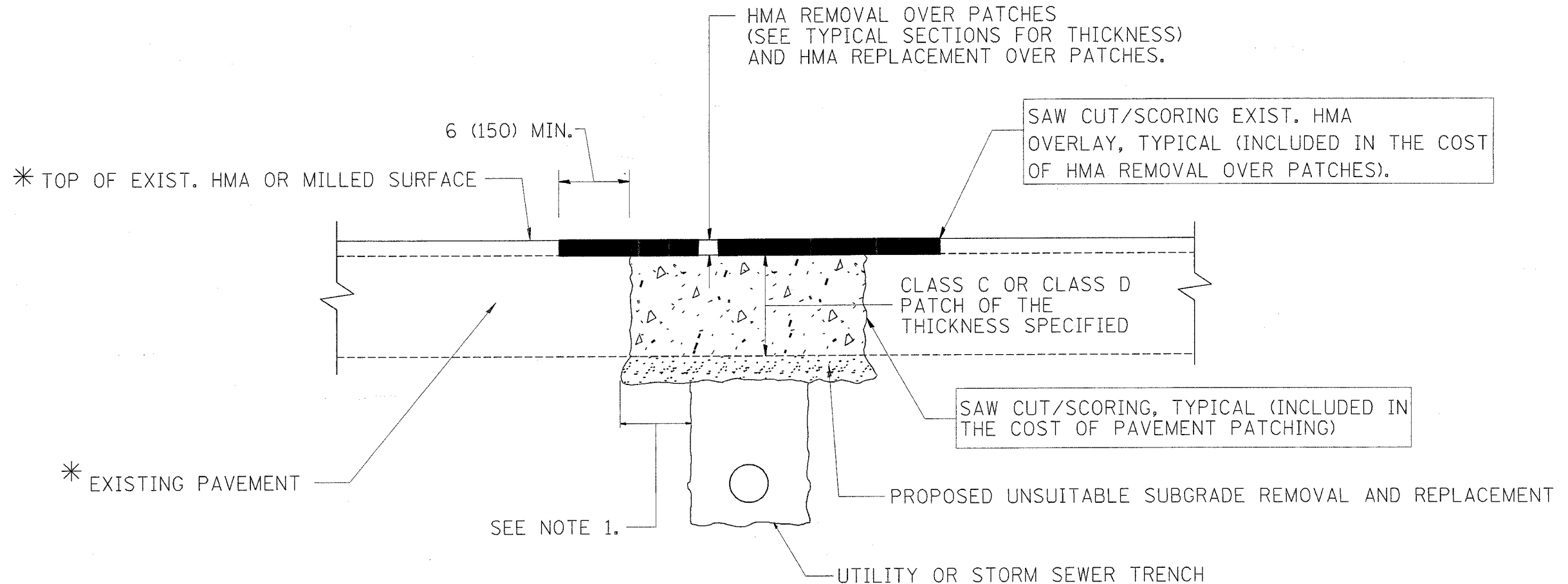
MANHOLE WITH RESTRICTOR PLATE

SCALE: VERT. DATE: 5/2/2007

DRAWN BY CHECKED BY

BD600-04 (BD-12)

REVISION DATE: 01/08/01



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|-----------|----------|
| NAME | DATE |
| R. SHAH | 10/25/94 |
| R. SHAH | 01/14/95 |
| R. SHAH | 03/23/95 |
| R. SHAH | 04/24/95 |
| A. HOUSEH | 03/15/96 |
| A. ABBAS | 03/21/97 |
| A. ABBAS | 01/20/98 |
| ART ABBAS | 04/27/98 |
| R. BORO | 01/01/01 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

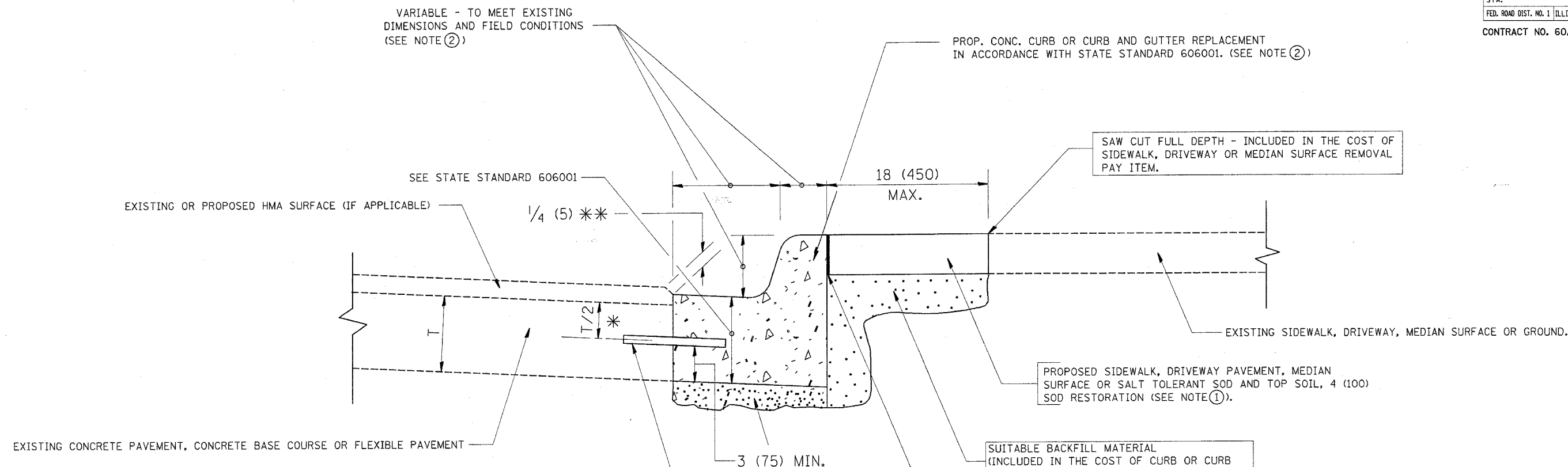
PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: VERT. NONE
HORIZ. NONE

DRAWN BY
CHECKED BY

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------|---------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 48 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

CONTRACT NO. 60A25



- * 3 (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4 (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4 (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4 (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24 (600) LONG AT 24 (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|-----------|----------|
| NAME | DATE |
| A. HOUSEH | 03/11/94 |
| R. SHAH | 02/24/95 |
| R. SHAH | 03/02/95 |
| R. SHAH | 08/19/96 |
| R. SHAH | 09/12/96 |
| R. SHAH | 09/19/96 |
| R. SHAH | 10/03/96 |
| A. ABBAS | 03/21/97 |
| M. GOMEZ | 01/22/01 |
| R. BORO | 01/01/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE
HORIZ. NONE
PLOT DATE: 5/2/2007

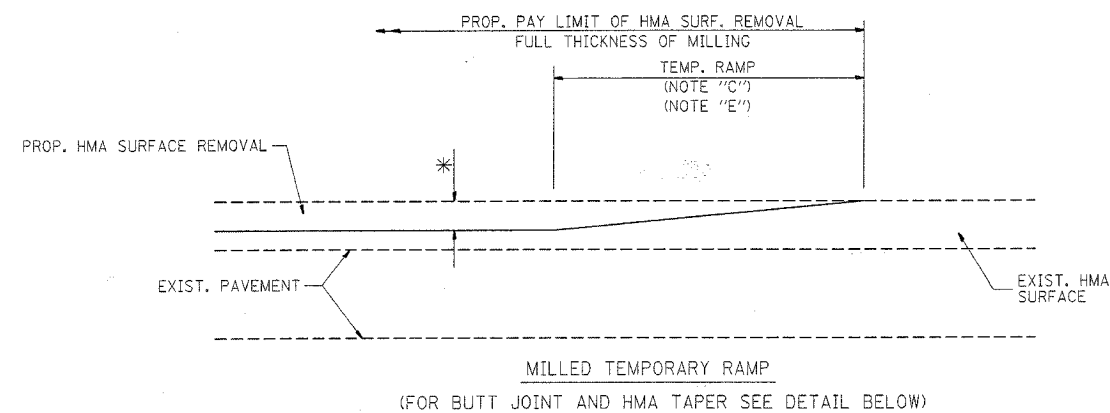
DRAWN BY
CHECKED BY
BD600-06 (BD-24)

REVISION DATE: 01/01/07

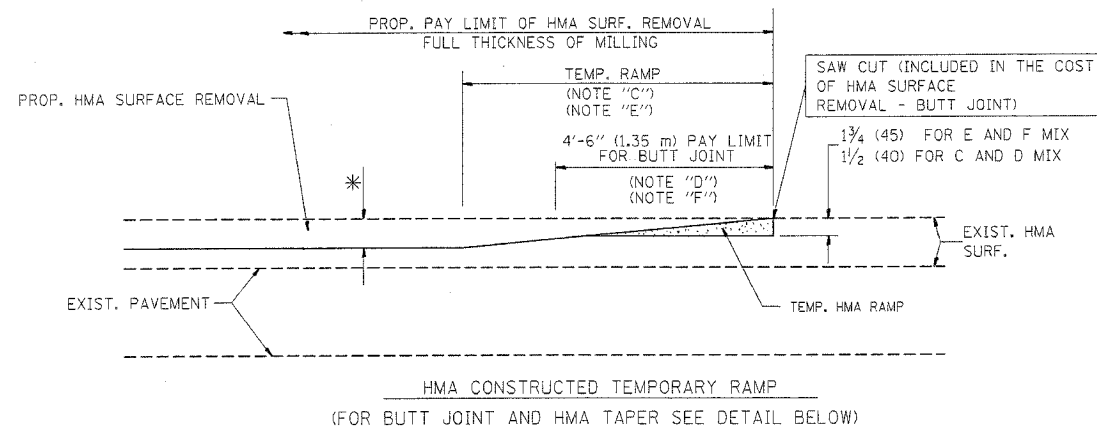
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

PLOT DATE = 5/2/2007
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USER = gabbarb

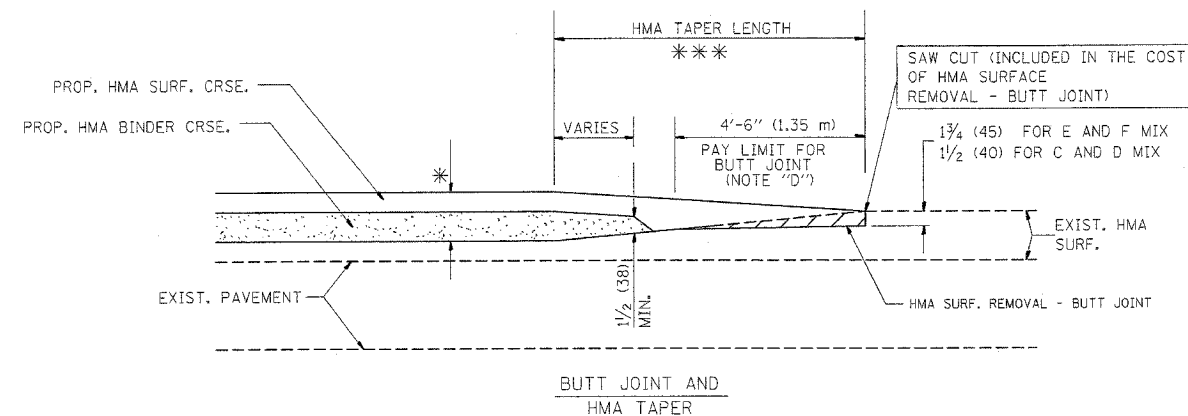
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|---|----------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 49 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| CONTRACT NO. 60A25 | | | | |



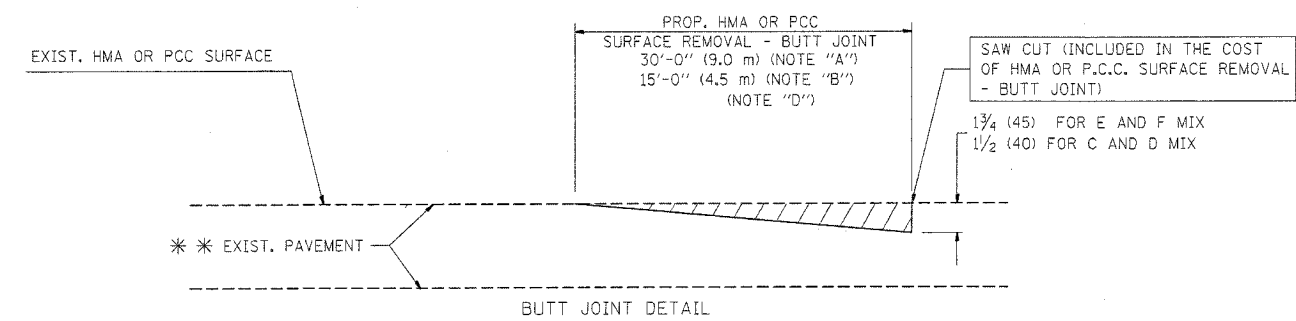
OPTION 1



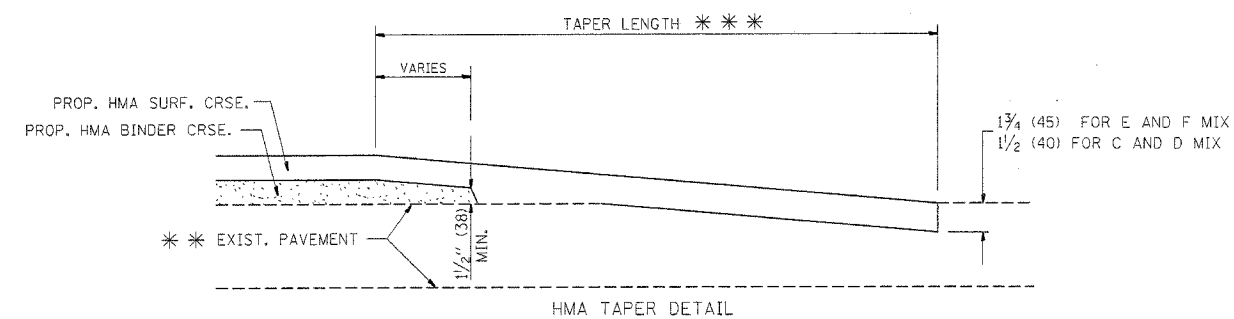
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

| REVISIONS | |
|------------|----------|
| NAME | DATE |
| M. DE YONG | 6-13-90 |
| M. DE YONG | 7-3-90 |
| M. DE YONG | 3-27-92 |
| R. SHAH | 09/09/94 |
| R. SHAH | 10/25/94 |
| A. ABBAS | 03/21/97 |
| M. GOMEZ | 04/06/01 |
| R. BORO | 01/01/07 |

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER
DETAILS

SCALE: VERT. NONE
HORIZ. PLOT DATE: 5/2/2007

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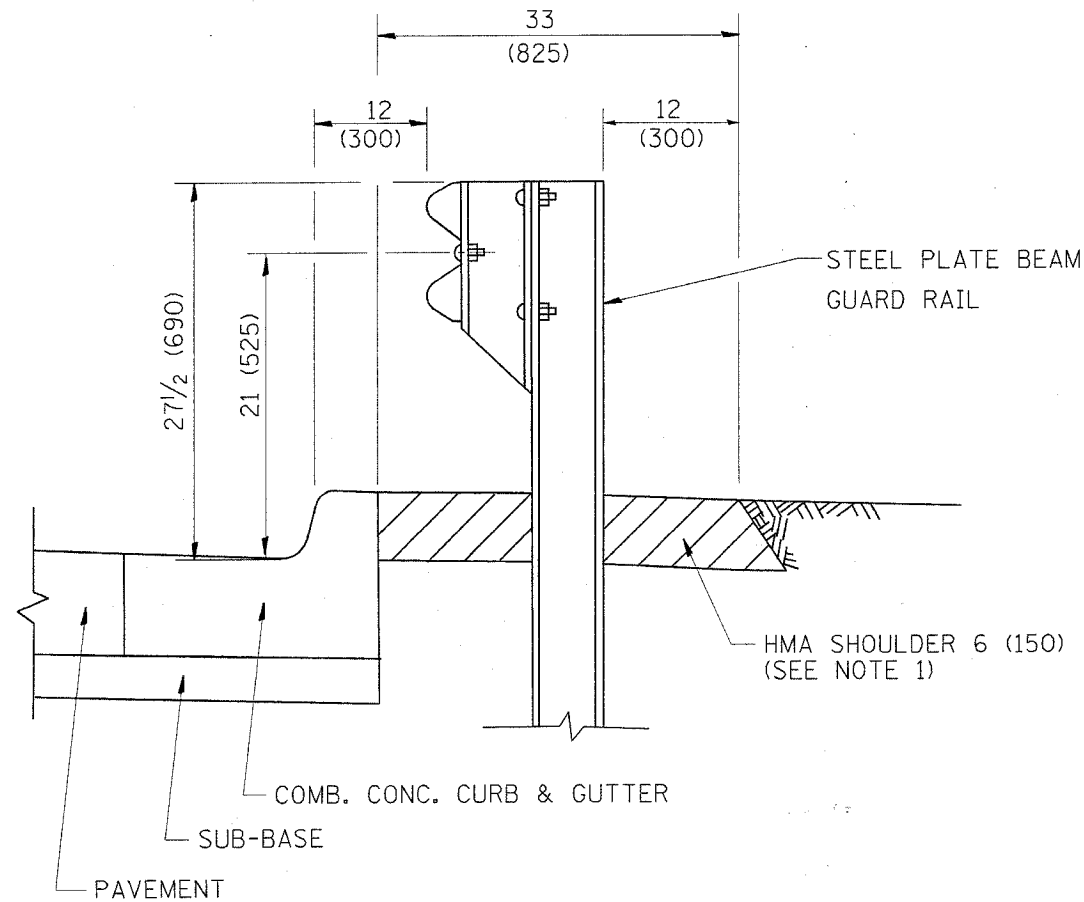
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BD400-05 (VI-BD32)

REVISION DATE: 01/01/07

PLOT DATE = 5/2/2007
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USER NAME = gabbar

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|---|----------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 50 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| CONTRACT NO. 60A25 | | | | |

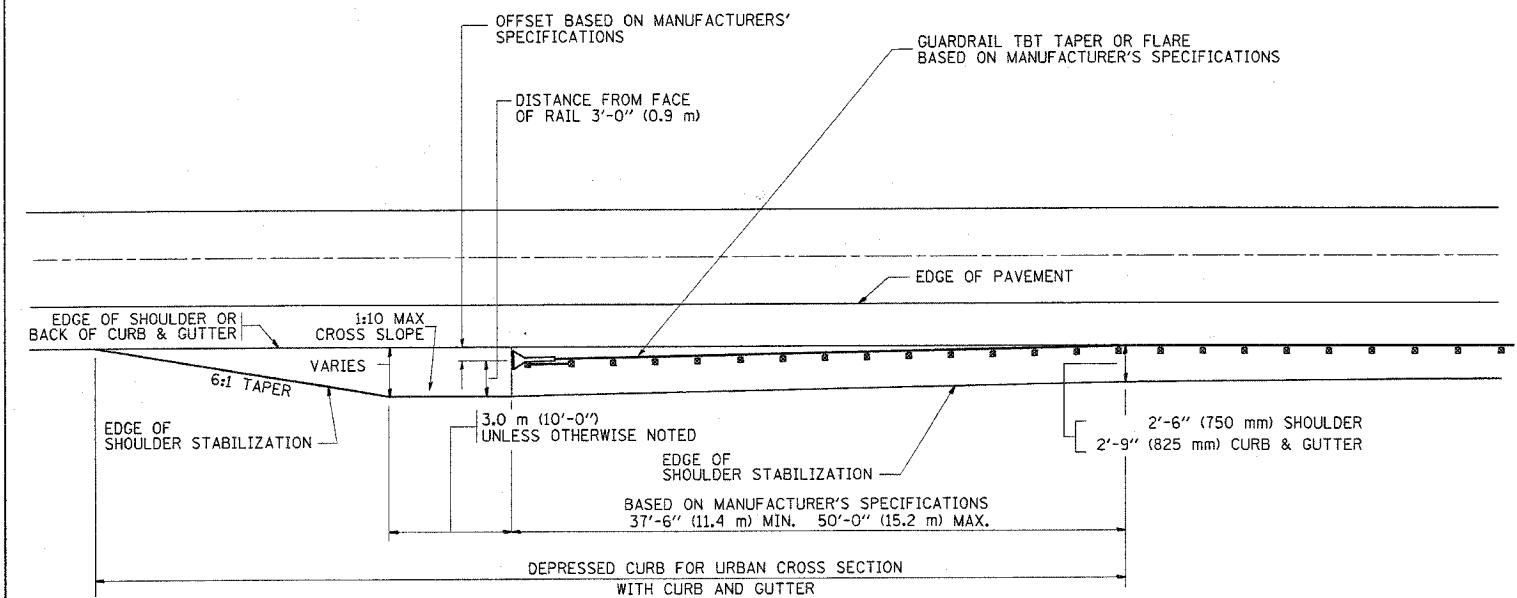


- NOTES: 1. THE HMA SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: HMA SHOULDER 6 (150) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDER 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



STABILIZATION AT TBT TY. 1 SPL.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|------------|----------|
| NAME | DATE |
| M. DE YONG | 09-22-90 |
| M. DE YONG | 07-14-92 |
| R. SHAH | 09/09/94 |
| R. SHAH | 10/25/94 |
| R. SHAH | 02/23/95 |
| A. ABBAS | 03/21/97 |
| E. GOMEZ | 08/28/00 |
| R. BORO | 01/01/07 |

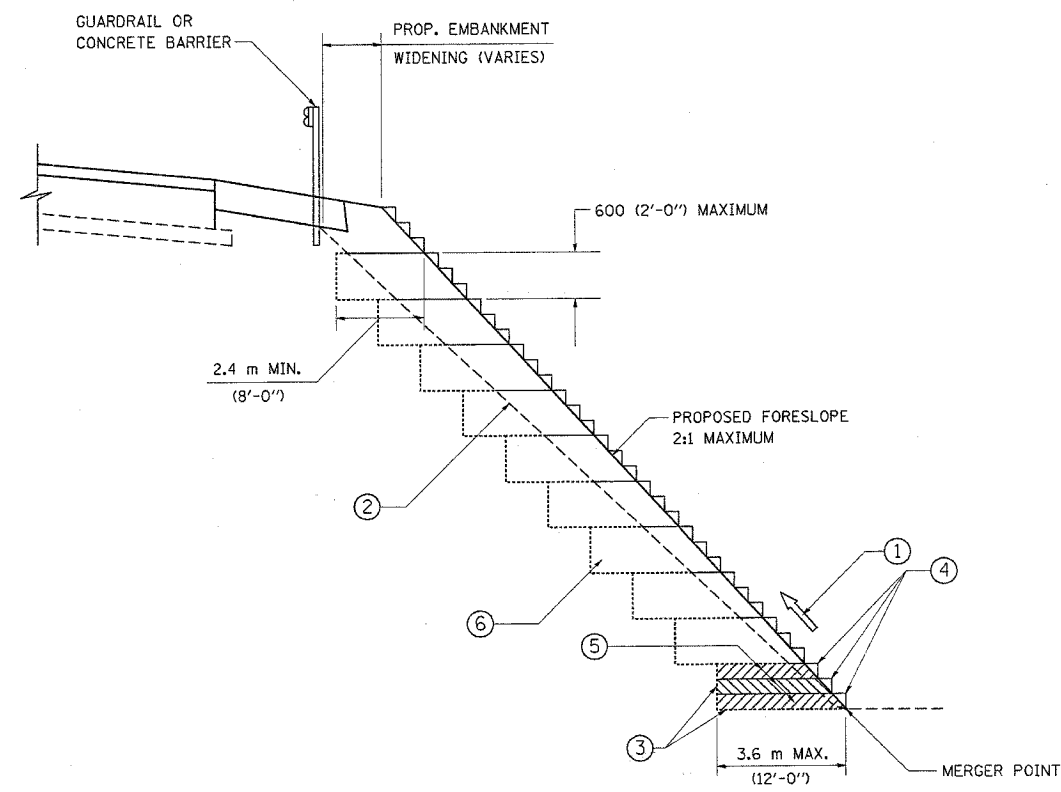
ILLINOIS DEPARTMENT OF TRANSPORTATION
**DETAILS FOR
STEEL PLATE BEAM GUARD RAIL
ADJACENT TO CURB AND GUTTER
STABILIZATION AT TBT TY 1 SPL.**

SCALE: VERT. NONE HORIZ. NONE
DRAWN BY Jls
PLOT DATE: 5/2/2007 CHECKED BY

PLOT DATE = 5/2/2007
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USER NAME = galbamba

BD600-10 (BD 34)
REVISION DATE: 01/01/07

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|-----------------------|----------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 51 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 60A25 | | | | |



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 200 (8-INCH) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
**BENCHING DETAIL
FOR EMBANKMENT
WIDENING**

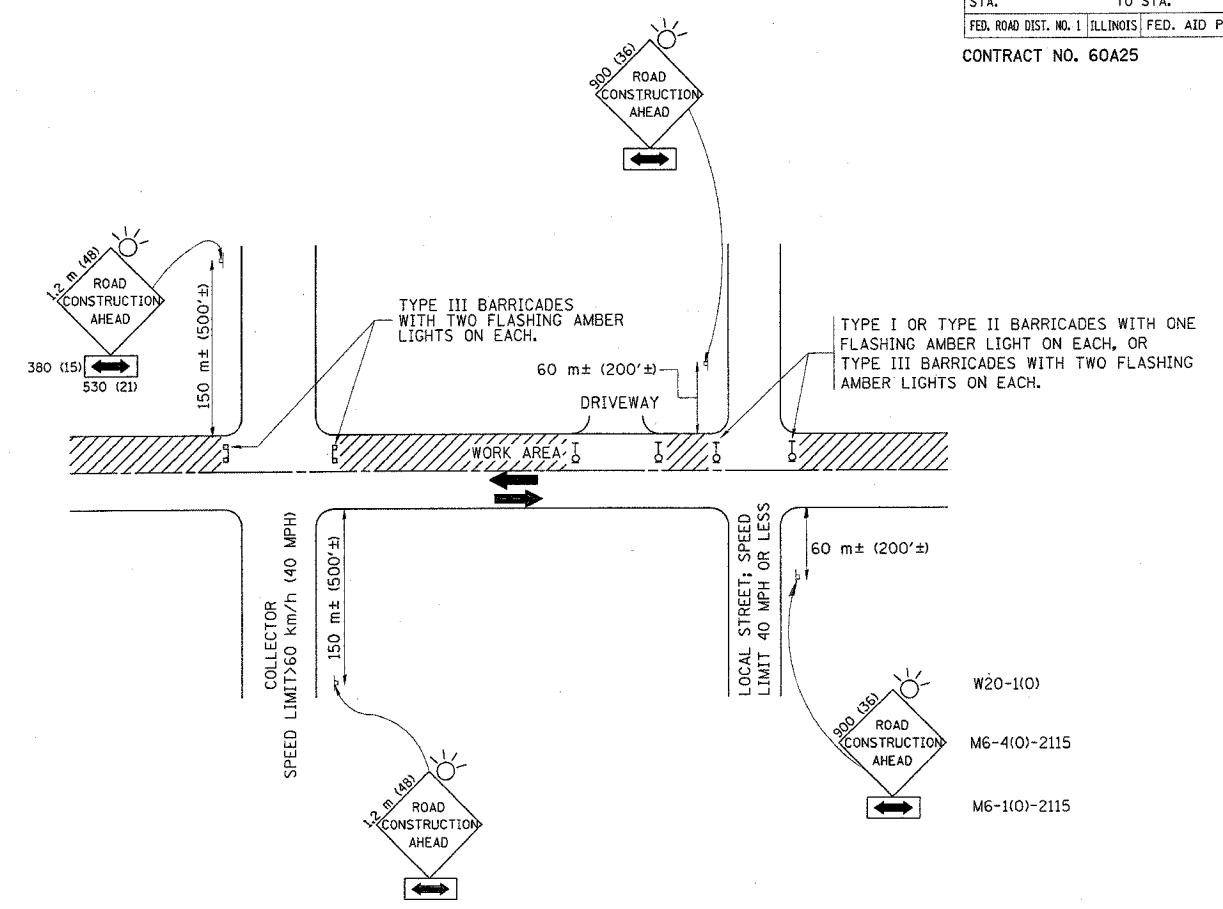
VERT. SCALE:
HORIZ. DATE: 5/2/2007

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CHECKED BY: S.E.B.
BD-51
REVISION DATE: 6-16-2004

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USER NAME = galbarr

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|-----------------------|----------|----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 52 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 60A25



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

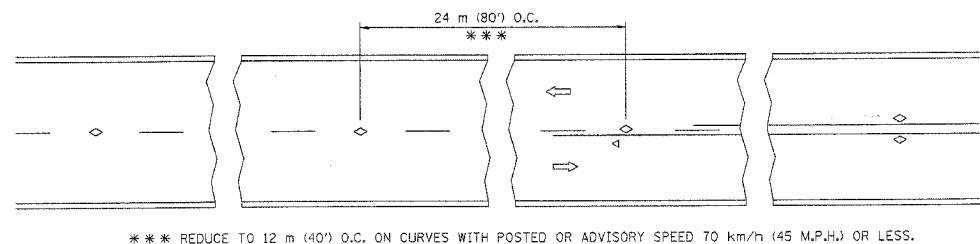
| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| LHA | 6/89 |
| T. RAMMACHER | 09/08/94 |
| J. OBERLE | 10/18/95 |
| A. HOUSEH | 03/06/96 |
| A. HOUSEH | 10/15/96 |
| T. RAMMACHER | 01/06/00 |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

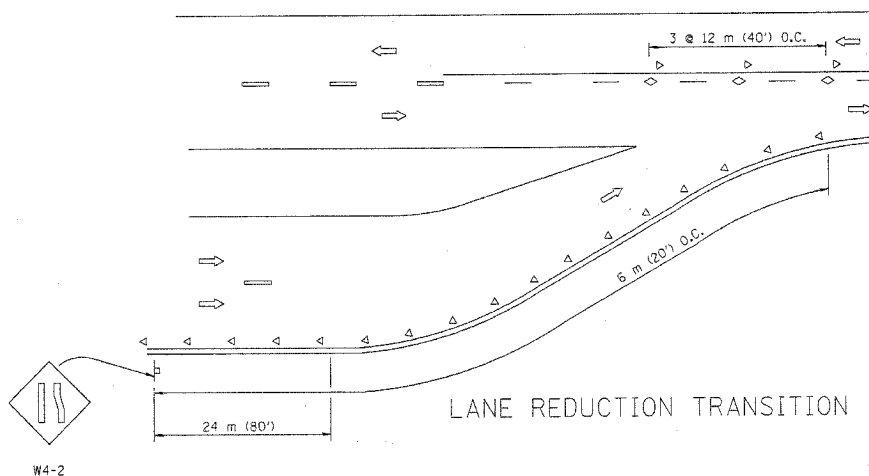
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TC-10
 REVISION DATE: 01/06/00

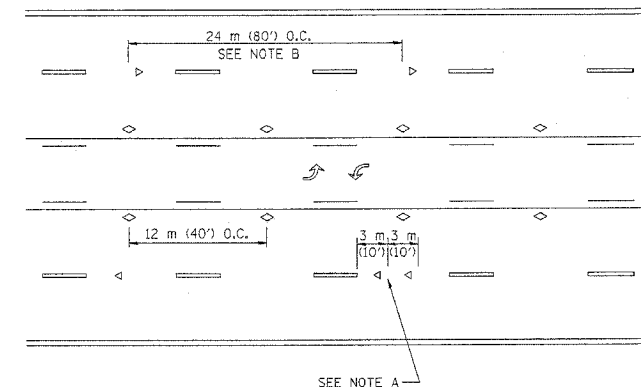
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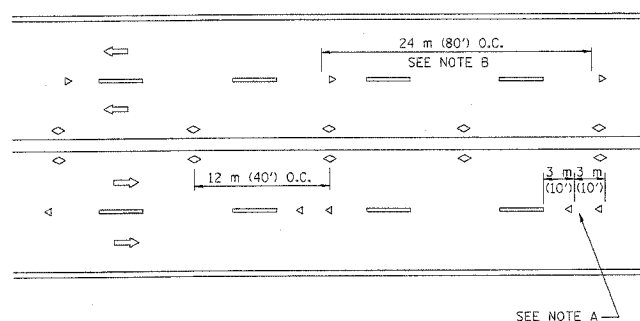
TWO-LANE/TWO-WAY



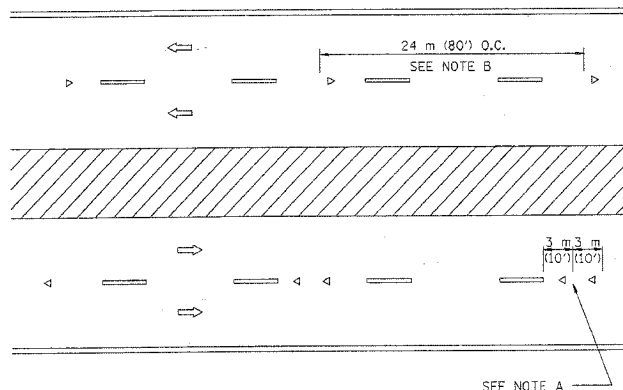
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◊ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

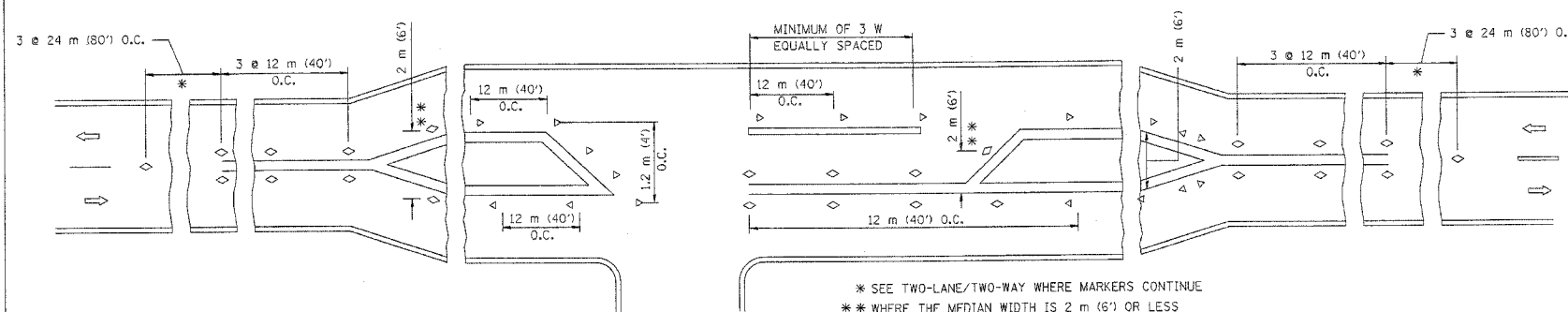
All dimensions are in millimeters (Inches) unless otherwise shown.

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09-19-94 |
| T. RAMMACHER | 03-12-99 |
| T. RAMMACHER | 01-06-00 |
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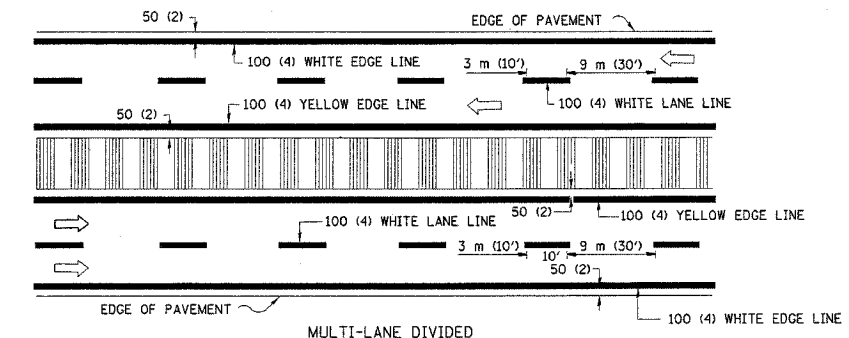
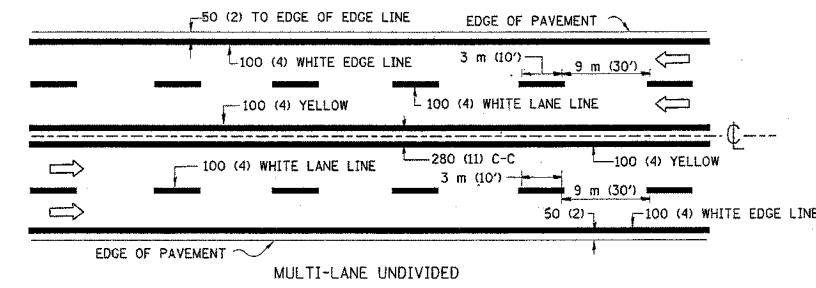
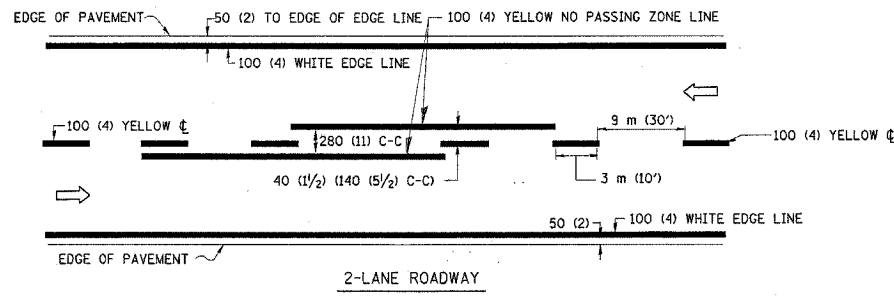
ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT
 MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE
 DATE: 5/2/2007

DRAWN BY CADD
 CHECKED BY
 TC-11
 REVISION DATE: 01/06/00

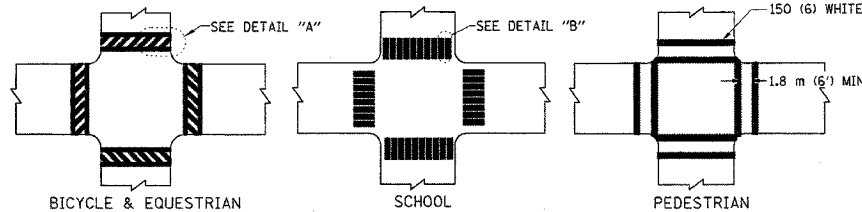


LEFT TURN

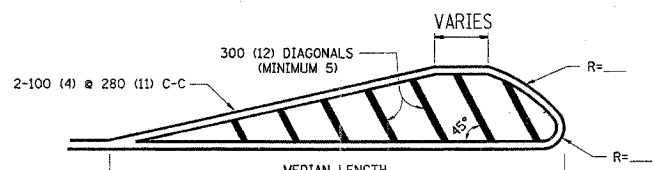
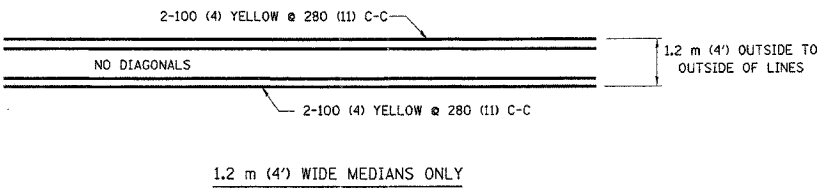


TYPICAL LANE AND EDGE LINE MARKING

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

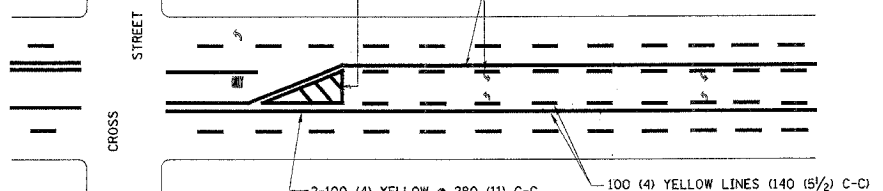


TYPICAL CROSSWALK MARKING

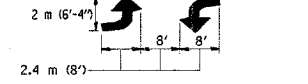


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
DIAGONAL LINE SPACING: 15 m (50') C-C (LESS THAN 50 km/h (30 MPH))
25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH))
45 m (150') C-C (MORE THAN 70 km/h (45 MPH))

MEDIANS OVER 1.2 m (4') WIDE

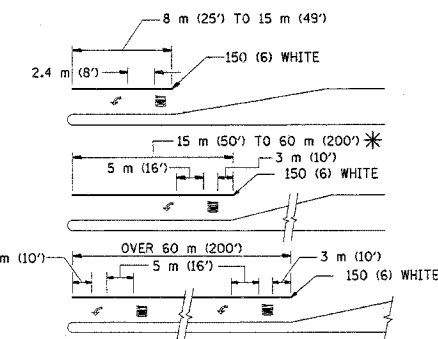


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

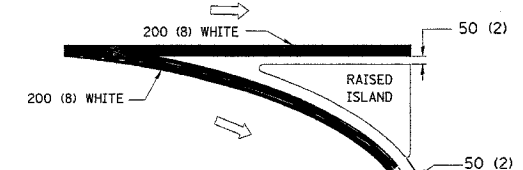
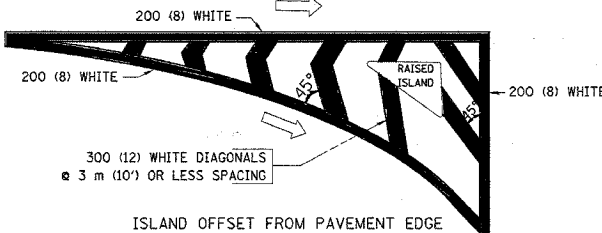


FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED. AREA = 1.5 m² (15.6 SQ. FT.) ONLY AREA = 1.9 m² (20.8 SQ. FT.)

* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|-------------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 100 (4) | SKIP-DASH | YELLOW | 3 m (10') LINE WITH 9 m (30') SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 100 (4) | SOLID | YELLOW | 280 (11) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 100 (4) 2 @ 100 (4) | SOLID SOLID | YELLOW YELLOW | 140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 100 (4) 125 (5) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 3 m (10') LINE WITH 9 m (30') SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 600 (2') LINE WITH 1.8 m (6') SPACE |
| EDGE LINES | 100 (4) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8')) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 1.8 m (6') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 600 (24) | SOLID | WHITE | PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 200 (8) WITH 300 (12) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH)) |
| RAILROAD CROSSING | 600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.) |
| SHOULDER DIAGONALS | 300 (12) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| EVERS | 03-19-90 |
| T. RAMMACHER | 10-27-94 |
| ALEX HOUSE | 10-09-96 |
| ALEX HOUSE | 10-17-96 |
| T. RAMMACHER | 01-06-00 |

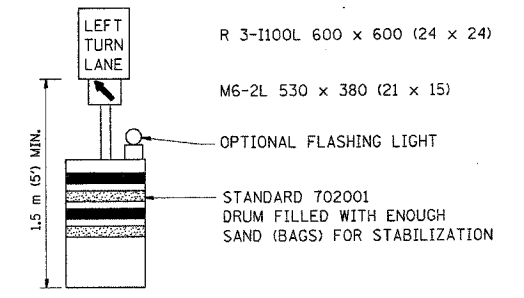
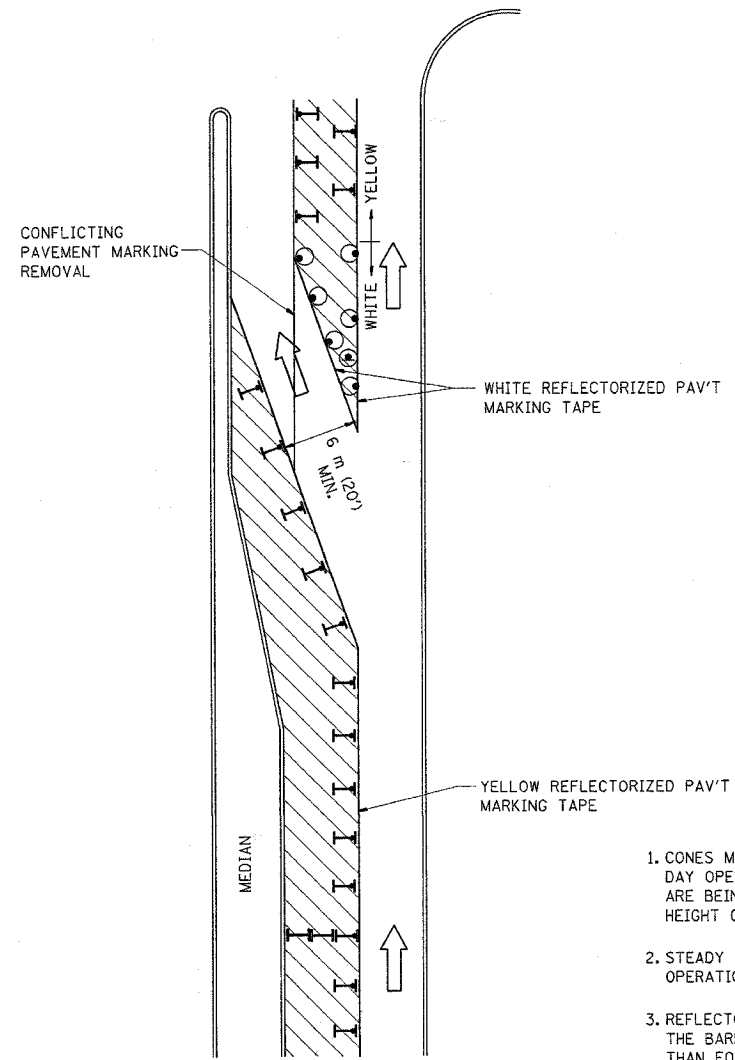
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE
DATE: 5/2/2007
DRAWN BY CADD
CHECKED BY

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USER NAME = galbamb

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|---|----------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 55 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

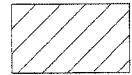





CONTRACT NO. 60A25



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in millimeters (inches) unless otherwise shown.

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09/08/94 |
| A. HOUSEH | 11/07/95 |
| A. HOUSEH | 10/12/96 |
| T. RAMMACHER | 01/06/00 |
| | |
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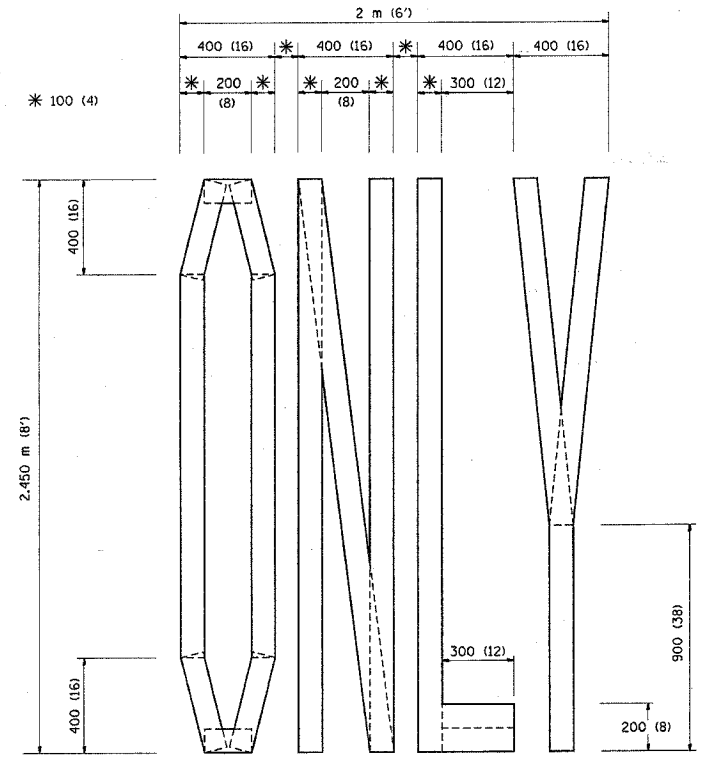
ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE
 DATE: 5/2/2007
 DRAWN BY
 CHECKED BY LHA

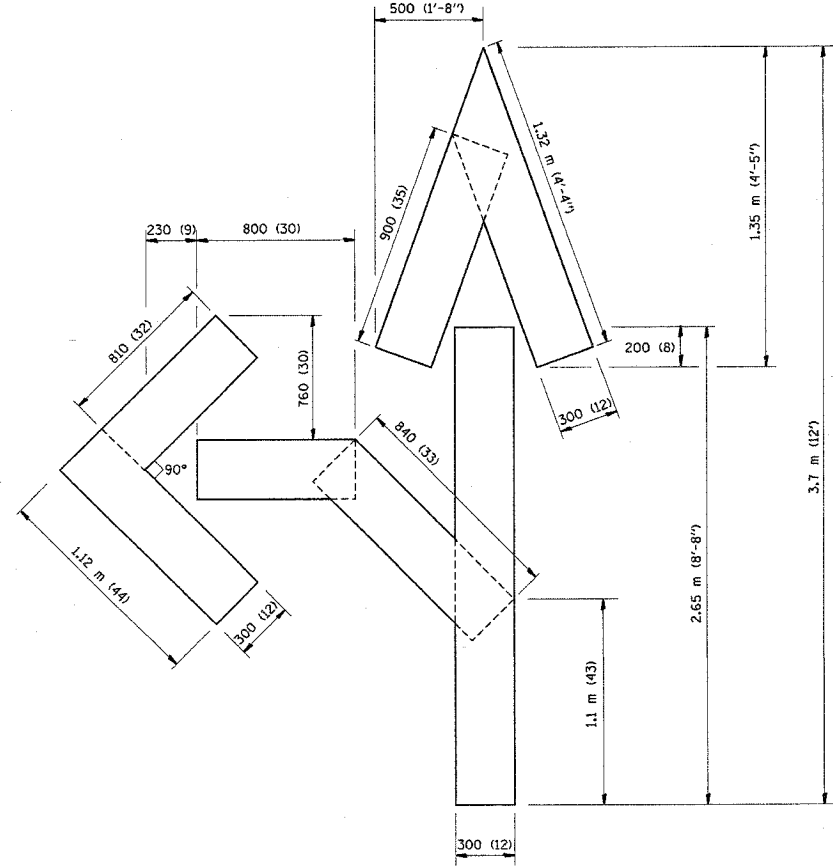
TC-14
 REVISION DATE: 01/06/00

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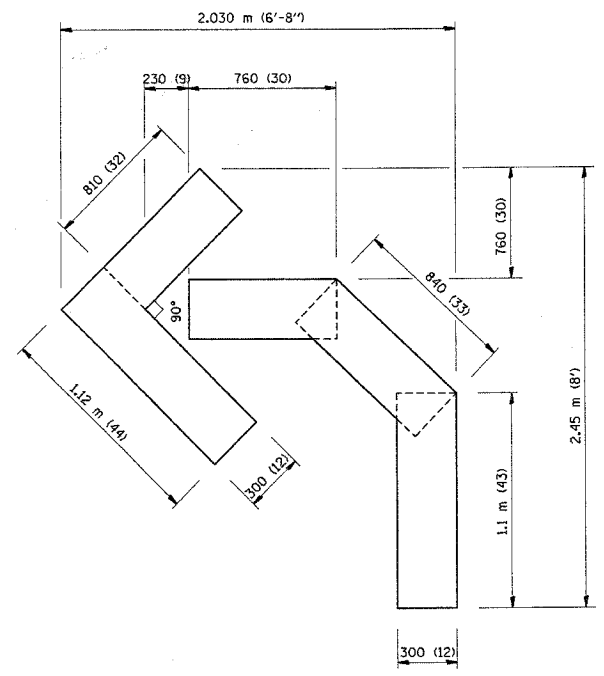
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|---|----------|---------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 56 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |
| CONTRACT NO. 60A25 | | | | |



QUANTITY
 100 (4) LINE = 19.7 m (64.1 ft.)
 1.97 sq. m (21.1 sq. ft.)



QUANTITY
 100 (4) LINE = 25.3 m (82.5 ft.)
 2.53 sq. m (27.5 sq. ft.)



QUANTITY
 100 (4) LINE = 13.9 m (45.5 ft.)
 1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (Inches) unless otherwise shown.

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| T. RAMMACHER | 09/18/94 |
| J. OBERLE | 06/01/96 |
| T. RAMMACHER | 06/05/96 |
| T. RAMMACHER | 11/04/97 |
| T. RAMMACHER | 03/02/98 |
| E. GOMEZ | 08/28/00 |
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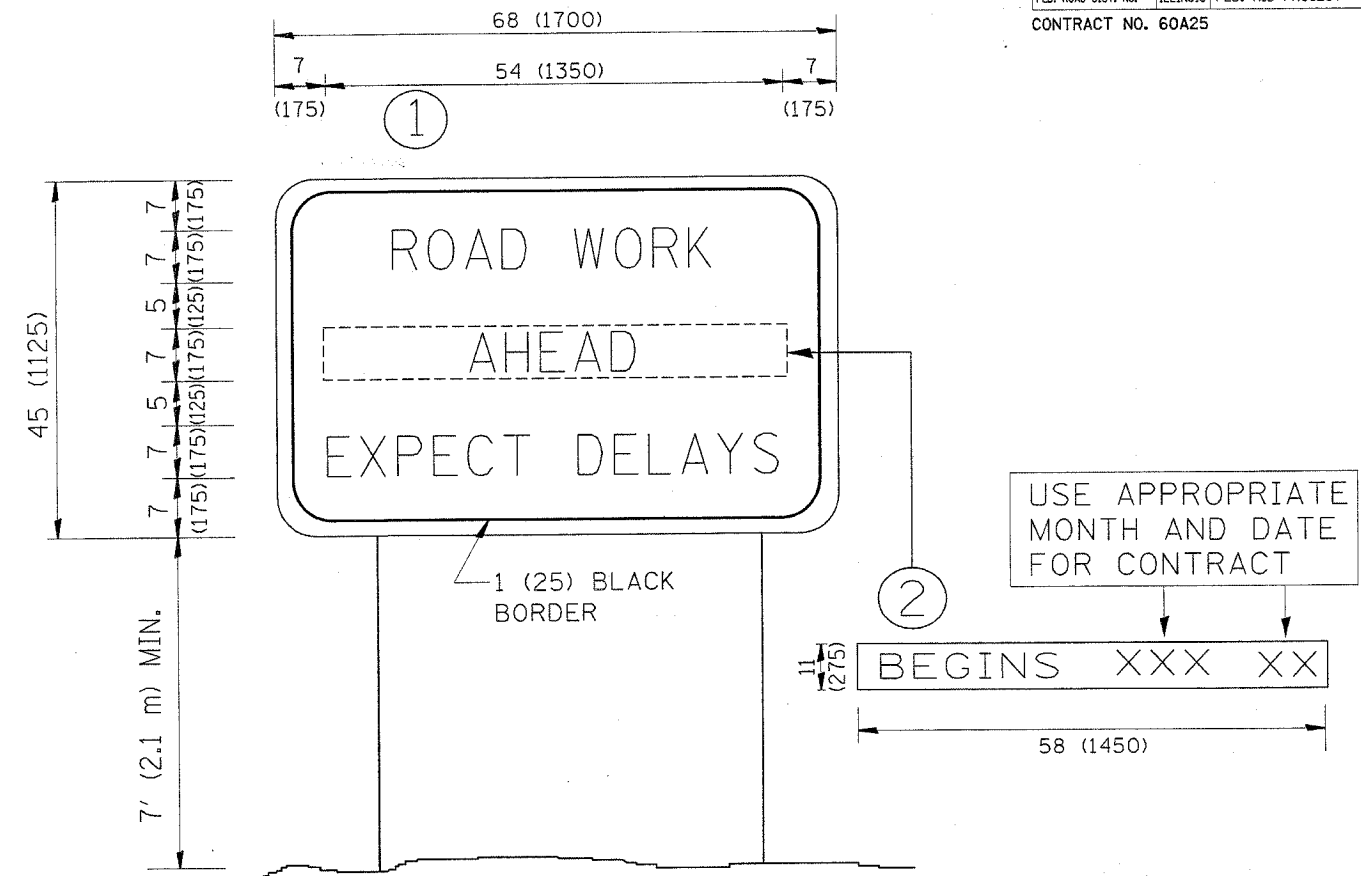
ILLINOIS DEPARTMENT OF TRANSPORTATION
**PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE
 DATE: 5/2/2007

DRAWN BY CADD
 CHECKED BY
 TC-16

REVISION DATE: 08/28/00

PLOT DATE = 5/2/2007
 FILE NAME = \\c:\winnt\p2\users\galbamb\Desktop\1061.dgn
 PLOT SCALE = 50.00000 / IN.
 USER NAME = galbamb



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| R. MIRS | 9-15-97 |
| R. MIRS | 12-11-97 |
| T. RAMMACHER | 2-2-99 |
| C. JUCIUS | 1-31-07 |
| | |
| | |
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| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE

DRAWN BY DESIGN

CHECKED BY

TC22

PROPOSED SEQUENCE OF OPERATION

| MOVEMENT | 1 + 5 | | | | 1 + 6 | | | | 2 + 5 | | | | 2 + 6 | | | | 3 + 7 | | | | 3 + 8 | | | | 4 + 7 | | | | 4 + 8 | | | |
|--|-------|-----|-----|-----|-------|------|-----|------|-------|-----|----|------|--------------------------|-----|---------------------------------|----|-------|----|----|----|--------------------------|-----|-----|----|-------|--------------------------|-----|-----|-------|----|--------------------------|-----|
| PHASE | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13A | 13B | 14 | 15 | 16 | 17 | 18 | 19 | 20A | 20B | 21 | 22 | 23 | 24A | 24B | 25 | 26 | 27 | 28A | 28B |
| CHANGE TO | | 1+6 | 2+5 | 2+6 | φ | φ | 2+6 | φ | φ | 2+6 | | | 3+7 3+8 4+7 4+8 | | 1+5 1+6 2+5 2+6 4+8 | | | | φ | φ | 1+5 1+6 2+5 2+6 | | 4+8 | φ | φ | 1+5 1+6 2+5 2+6 | | 4+8 | | | 1+5 1+6 2+5 2+6 | |
| MAIN STREET END MAST ARM AND FAR LEFT SIGNALS | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| MAIN STREET FAR RIGHT SIGNAL | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| MAIN STREET END MAST ARM AND FAR LEFT SIGNALS | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| MAIN STREET FAR RIGHT SIGNAL | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| CROSS STREET END MAST ARM AND FAR LEFT SIGNALS | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| CROSS STREET FAR RIGHT SIGNAL | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| CROSS STREET END MAST ARM AND FAR LEFT SIGNALS | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| CROSS STREET FAR RIGHT SIGNAL | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R |
| PEDESTRIAN SIGNALS CROSSING CROSS STREET ON NORTHSIDE OF MAIN STREET | H | H | H | H | *P | **FH | H | H | H | H | *P | **FH | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | |
| PEDESTRIAN SIGNALS CROSSING CROSS STREET ON SOUTHSIDE OF MAIN STREET | H | H | H | H | H | H | *P | **FH | H | H | *P | **FH | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | |
| PEDESTRIAN SIGNALS CROSSING MAIN STREET ON EASTSIDE OF CROSS STREET | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H |
| PEDESTRIAN SIGNALS CROSSING MAIN STREET ON WESTSIDE OF CROSS STREET | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H |

- TO APPEAR ONLY UPON PUSHBUTTON ACTIVATION
 - FLASHING [Symbol] IS TO TERMINATE AT THE COMPLETION OF THE PEDESTRIAN INTERVAL CLEARANCE.
 - φ THIS [Symbol] OR FLASHING [Symbol] INTERVAL MAY FINISH TIMING IN THE BIDIRECTIONAL STRAIGHT THROUGH MOVEMENT IF THE LEFT ARROW TIME IS NOT SUFFICIENT TO COMPLETE [Symbol] OR FLASHING [Symbol] INTERVALS. [Symbol] AND FLASHING [Symbol] TIMINGS TO BE SET ONLY ON PHASES WHERE [Symbol] AND FLASHING [Symbol] ARE INDICATED IN THE SEQUENCE OF OPERATION.
- P = ILLUMINATED PERSON = WALK
 FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
 H = ILLUMINATED SOLID HAND = DON'T WALK

PHASE 2+6 SHALL BE PLACED ON RECALL.

NLT = "NO LEFT TURN" OR [Symbol]
 NRT = "NO RIGHT TURN" OR [Symbol]

PROPOSED RAILROAD PREEMPTION SEQUENCE OF OPERATION

| | 1 | 5 | 8 | 11 | 14 | 18 | 22 | 26 | PREEMPTOR NUMBER 3 | PREEMPTOR NUMBER 4 | PREEMPTOR NUMBER 2 | | | | | | | | | | | |
|--|-----|-----|-----|-----|-----|-----|-----|-----|--------------------|--------------------|--------------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|--------------------------|
| CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER | | | | | | | | | | | | | | | | | | | | | | |
| CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER | | | | | | | | | 2 | 3 | | | | | | | | | | | | |
| RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER | 1A | 1B | 1C | 1D | 1E | 1F | 1G | 1H | 1J | 1K | 1L | 1M | 1N | 1P | 1Q | 1R | 1S | 2 | 3 | 4 | 5 | CLEAR TO NORMAL SEQUENCE |
| CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER | 2 | 1C | 2 | 1E | 2 | 1G | 2 | 2 | 1K | 2 | 2 | 1N | 2 | 1Q | 2 | 1S | 2 | 3 | 4 | 5 | | |
| MAIN STREET END MAST ARM AND FAR LEFT SIGNALS | R | Y | R | R | R | Y | R | R | R | R | R | R | R | Y | R | R | R | R | R | R | R | Δ |
| MAIN STREET FAR RIGHT SIGNAL | R | Y | R | R | R | Y | R | R | R | R | R | R | R | Y | R | R | R | R | R | R | R | Δ |
| MAIN STREET END MAST ARM AND FAR LEFT SIGNALS | R | Y | R | Y | R | Y | R | R | R | R | R | R | R | Y | R | R | R | R | R | R | R | Δ |
| MAIN STREET FAR RIGHT SIGNAL | R | Y | R | Y | R | Y | R | R | R | R | R | R | R | Y | R | R | R | R | R | R | R | Δ |
| CROSS STREET END MAST ARM AND FAR LEFT SIGNALS | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | Δ |
| CROSS STREET FAR RIGHT SIGNAL | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | Δ |
| CROSS STREET END MAST ARM AND FAR LEFT SIGNALS | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | Δ |
| CROSS STREET FAR RIGHT SIGNAL | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | R | Δ |
| PEDESTRIAN SIGNALS CROSSING CROSS STREET ON NORTHSIDE OF MAIN STREET | H | FH | H | H | H | FH | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | Δ |
| PEDESTRIAN SIGNALS CROSSING CROSS STREET ON SOUTHSIDE OF MAIN STREET | H | H | H | FH | H | FH | H | H | H | H | H | H | H | H | H | H | H | H | H | H | H | Δ |
| PEDESTRIAN SIGNALS CROSSING MAIN STREET ON EASTSIDE OF CROSS STREET | H | H | H | H | H | H | H | H | FH | H | H | FH | H | H | H | H | H | H | H | H | H | Δ |
| PEDESTRIAN SIGNALS CROSSING MAIN STREET ON WESTSIDE OF CROSS STREET | H | H | H | H | H | H | H | H | FH | FH | H | H | H | H | H | H | H | H | H | H | H | Δ |
| INTERNALLY ILLUMINATED NRT SIGNS | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | NRT | Δ |
| INTERNALLY ILLUMINATED NLT SIGNS | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | NLT | Δ |

Δ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

PLOT DATE = 5/2/2007
 FILE NAME = \\daxintv\p2\usera\gibbamb\Desktop\1080.dgn
 PLOT SCALE = 0.00000 / IN.
 USER NAME = gibbamb

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SEQUENCE OF OPERATION AND RAILROAD PREEMPTION SEQUENCE OF OPERATION
MAIN STREET AND CROSS STREET

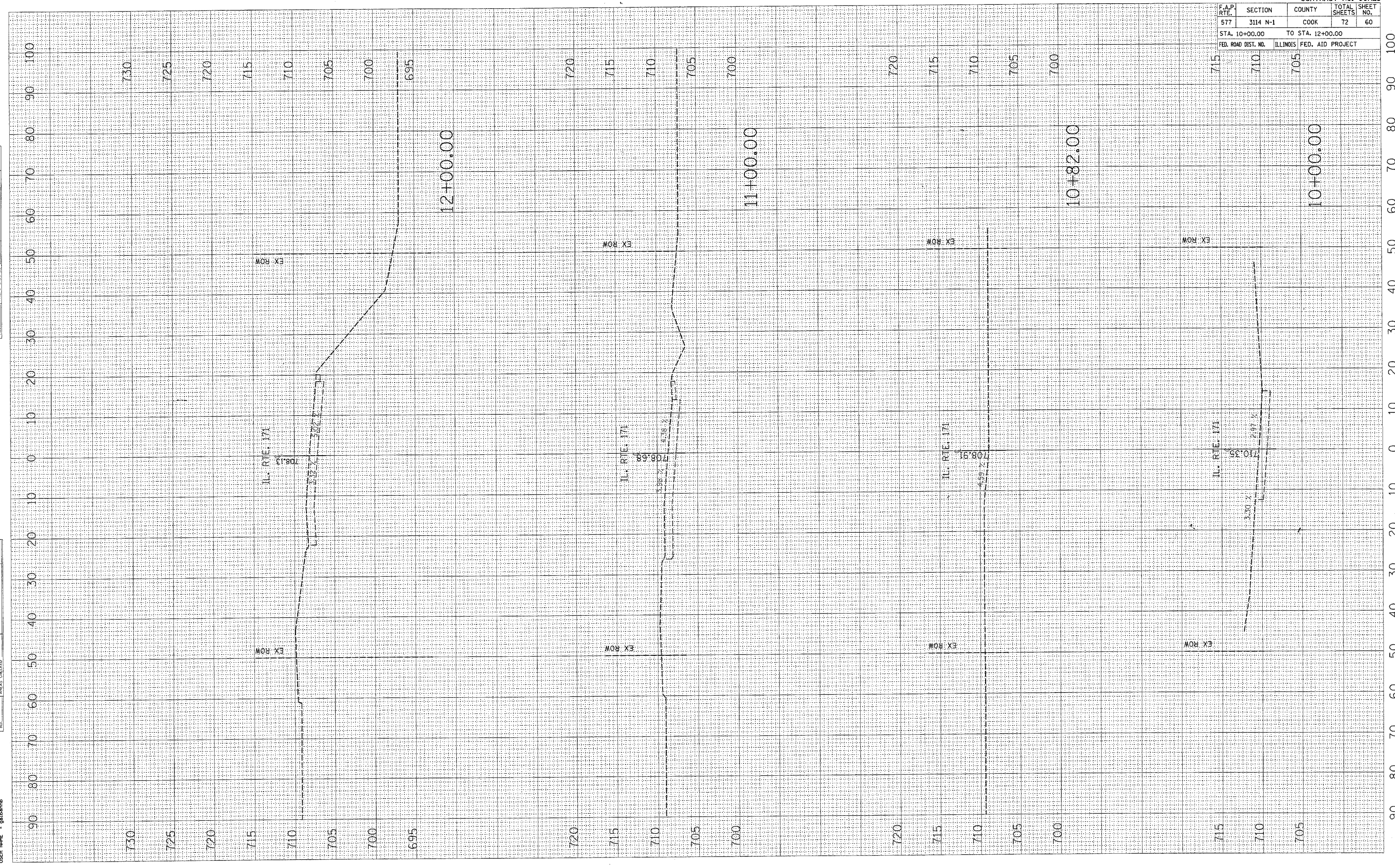
SCALE: NONE
 DATE: 5/2/2007

DRAWN BY CADD
 CHECKED BY
 TS08 (2 OF 2)
 REVISION DATE:

PLOT DATE = 5/2/2007
 PLOT SCALE = 1/4" = 100'
 USER NAME = galbamb

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED
 NOTE BOOK NO.
 BY DATE

FILM SURVEY PLOTTED
 SURVEY PLOTTED
 NOTE BOOK NO.
 BY DATE

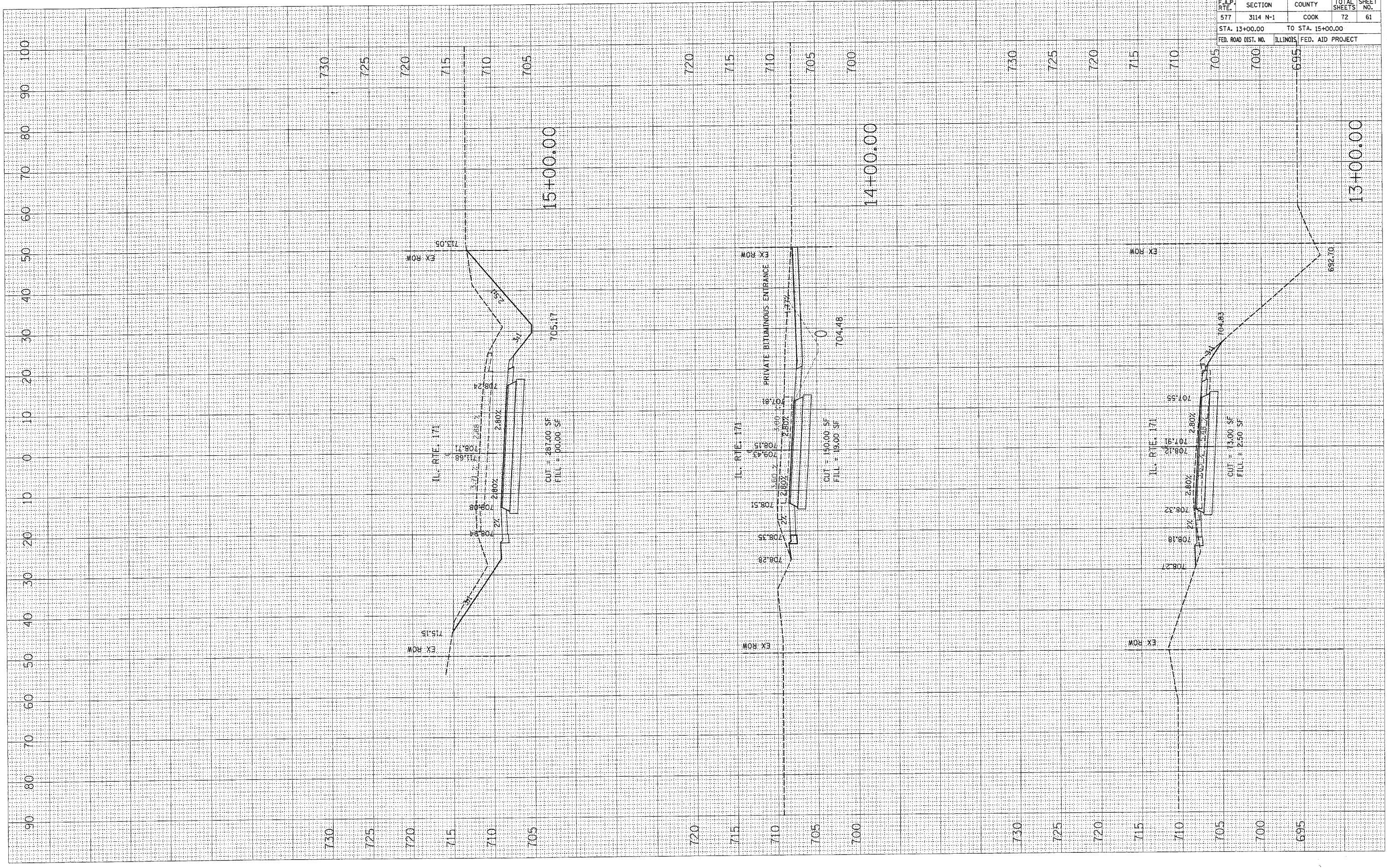


| CONTRACT NO. 60A25 | | | | |
|---------------------|----------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 60 |
| STA. 10+00.00 | | TO STA. 12+00.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

PLOT DATE = 5/2/2007
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 PLOT SCALE = 1/8" = 1'-0"
 USER NAME = gutenberg

| | | | |
|-----------------|---------------|----|------|
| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| SURVEY | PLOTTED | | |
| NOTE BOOK | TEMPLATE | | |
| NO. | AREAS CHECKED | | |

| | | | |
|--------------|---------------|----|------|
| FINAL SURVEY | SURVEYED | BY | DATE |
| SURVEY | PLOTTED | | |
| NOTE BOOK | TEMPLATE | | |
| NO. | AREAS CHECKED | | |

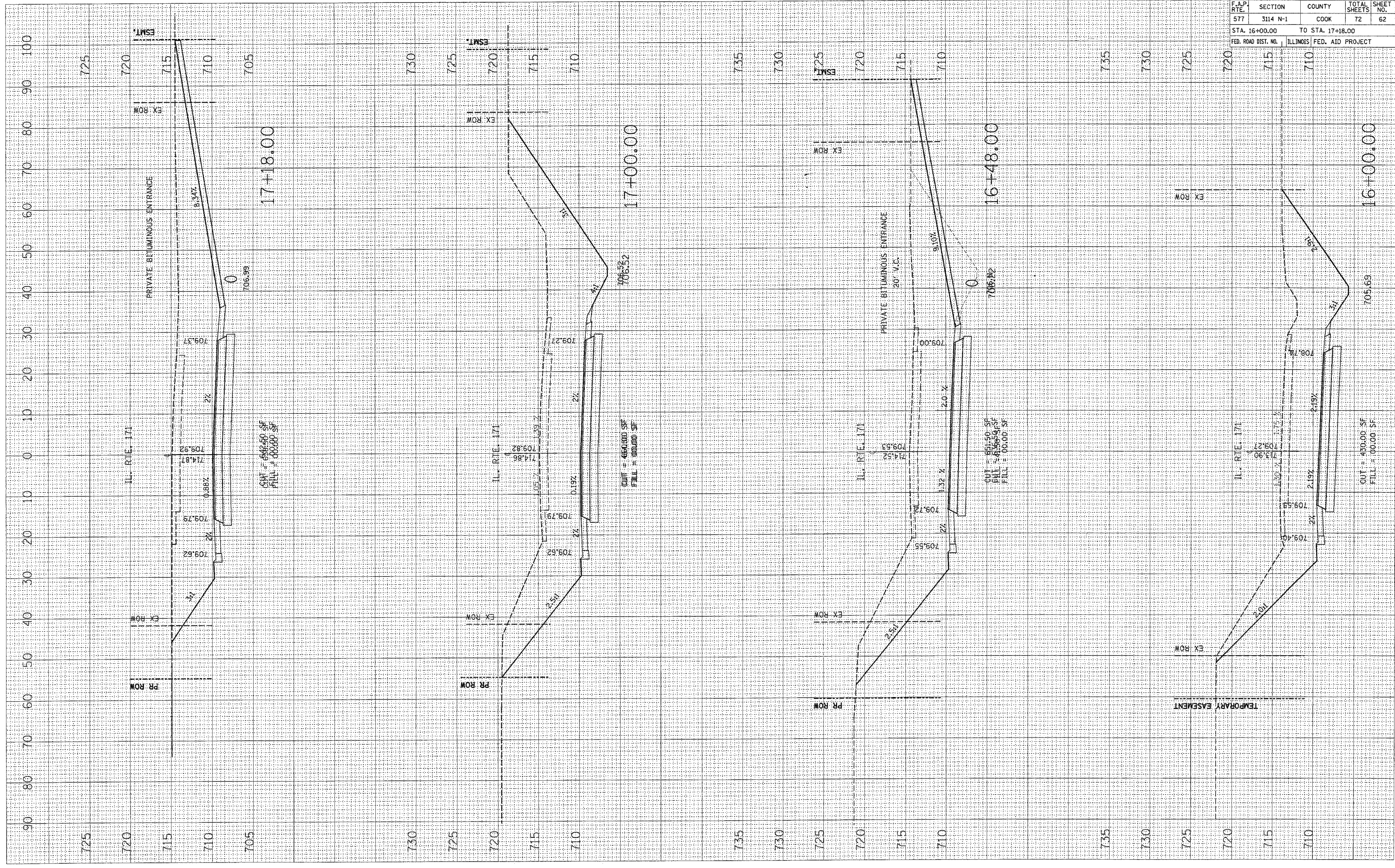


| | | | | |
|---------------------|----------|------------------|--------------|-----------|
| CONTRACT NO. 60A25 | | | | |
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 61 |
| STA. 13+00.00 | | TO STA. 15+00.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

PLOT DATE = 5/2/2007
 FILE NAME = c:\pro-jobs\113802 vs. 171b-sh\113802.dwg
 PLOT SCALE = 1/8" = 100'-0"
 USER NAME = galderb

| | |
|---------------------|------|
| ORIGINAL SURVEY NO. | DATE |
| SURVEY NO. | DATE |

| | |
|------------------|------|
| FINAL SURVEY NO. | DATE |
| SURVEY NO. | DATE |



| | | | | |
|---------------------|----------|---------------------------|--------------|-----------|
| CONTRACT NO. 60A25 | | | | |
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 62 |
| STA. 16+00.00 | | TO STA. 17+18.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

17+18.00

17+00.00

16+48.00

16+00.00

CUT = 666.50 SF
 FILL = 000.00 SF

CUT = 464.00 SF
 FILL = 000.00 SF

CUT = 851.50 SF
 FILL = 456.94 SF
 FILL = 000.00 SF

CUT = 400.00 SF
 FILL = 000.00 SF

PLOT DATE = 5/2/2007
 FILE NAME = c:\projects\60a25\pl13602\vs-17\ba-sh-6\p13602.plt
 USER NAME = gsbarnes

ORIGINAL SURVEY SHOWN
 PLOTTED TEMPLATE AREAS CHECKED

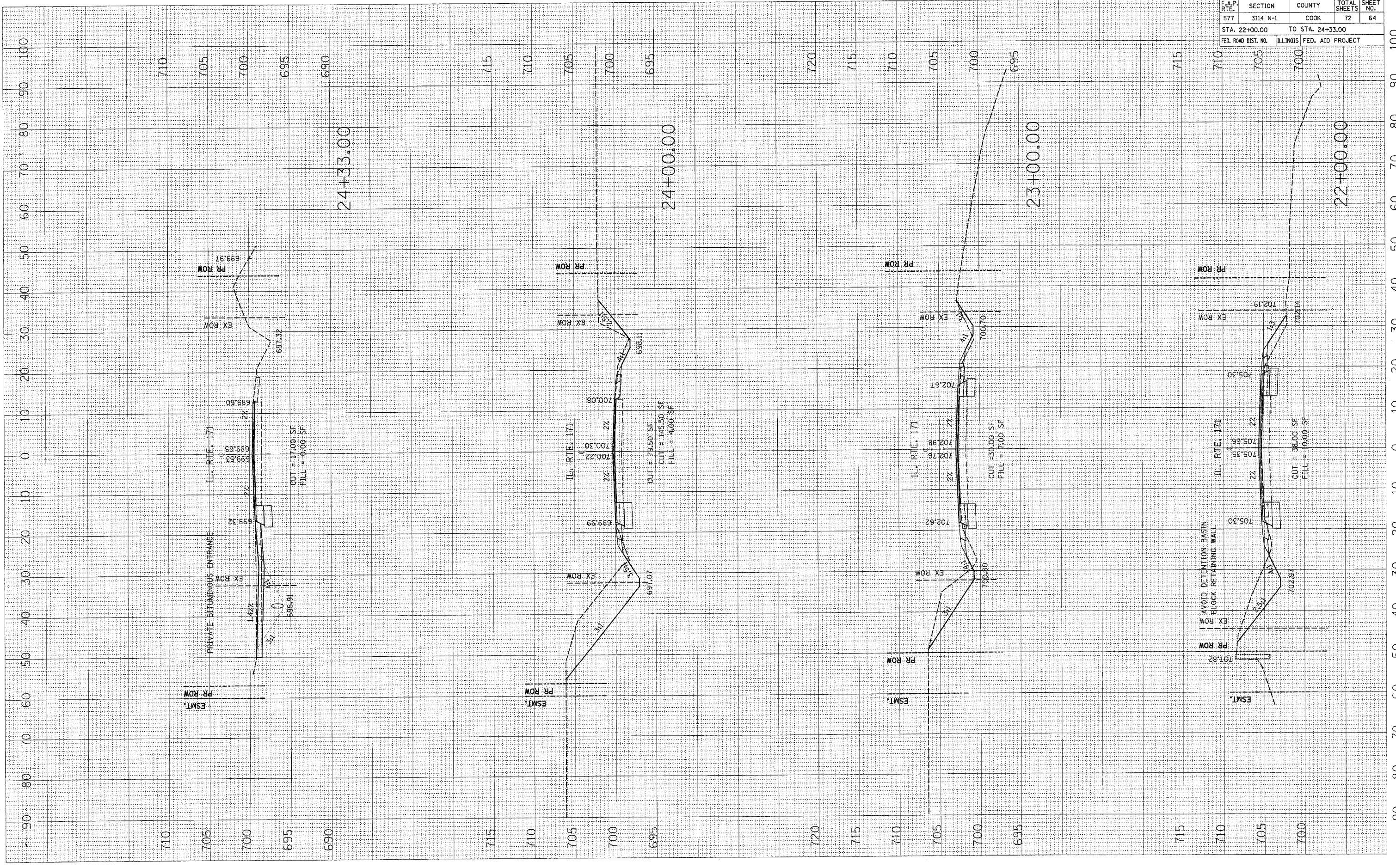
FINAL SURVEY SHOWN
 PLOTTED TEMPLATE AREAS CHECKED

BY _____ DATE _____

BY _____ DATE _____

CONTRACT NO. 60A25

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------------------|----------|--------|---------------------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 64 |
| STA. 22+00.00 TO STA. 24+33.00 | | | | |
| FED. ROAD DIST. NO. | | | ILLINOIS FED. AID PROJECT | |

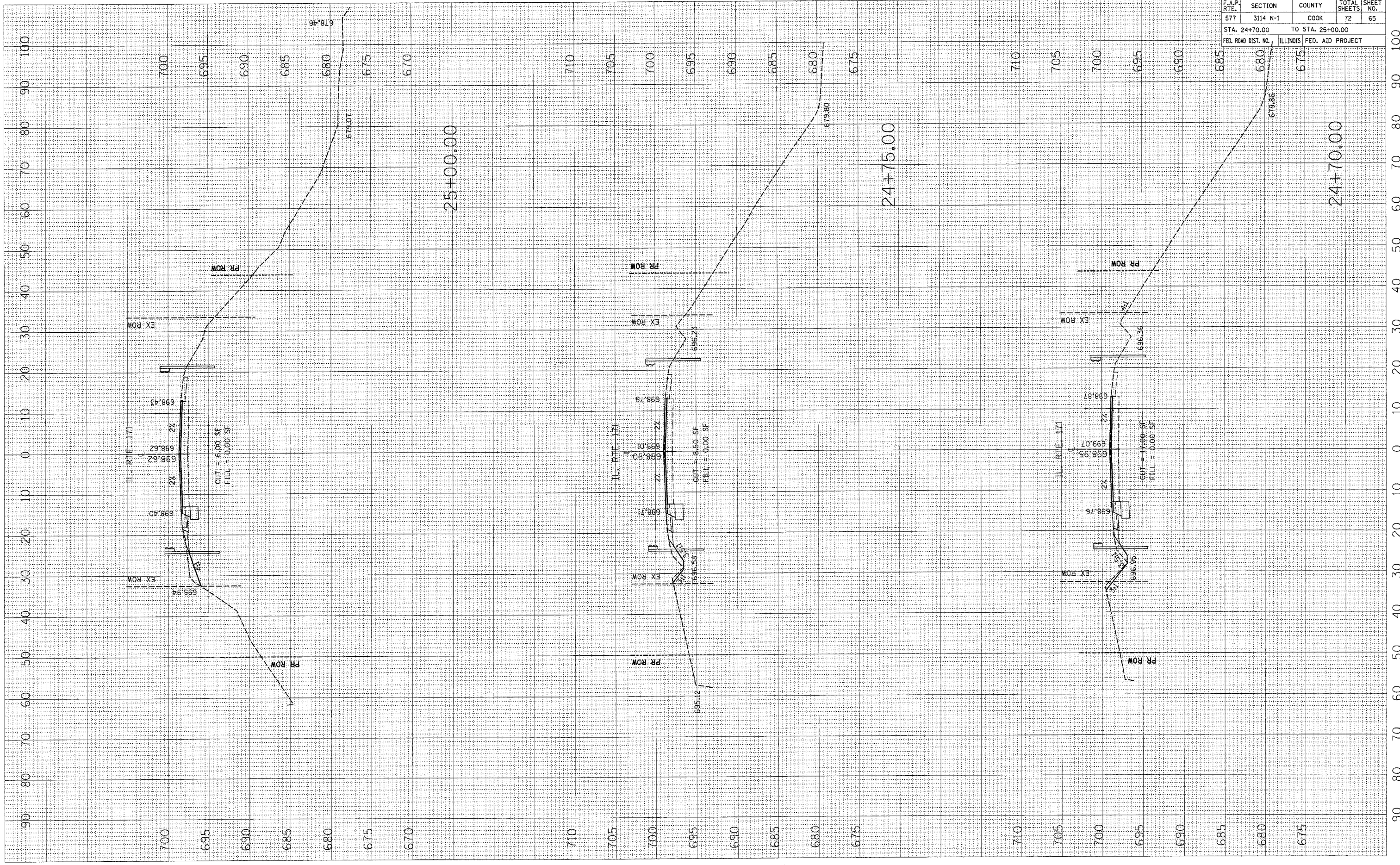


PLOT DATE = 5/2/2007
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 USER NAME = galbenb

ORIGINAL SURVEY PLOT DATE
 SURVEY PLOT DATE
 NO. AREAS CHECKED

FINAL SURVEY PLOT DATE
 SURVEY PLOT DATE
 NO. AREAS CHECKED

BY DATE

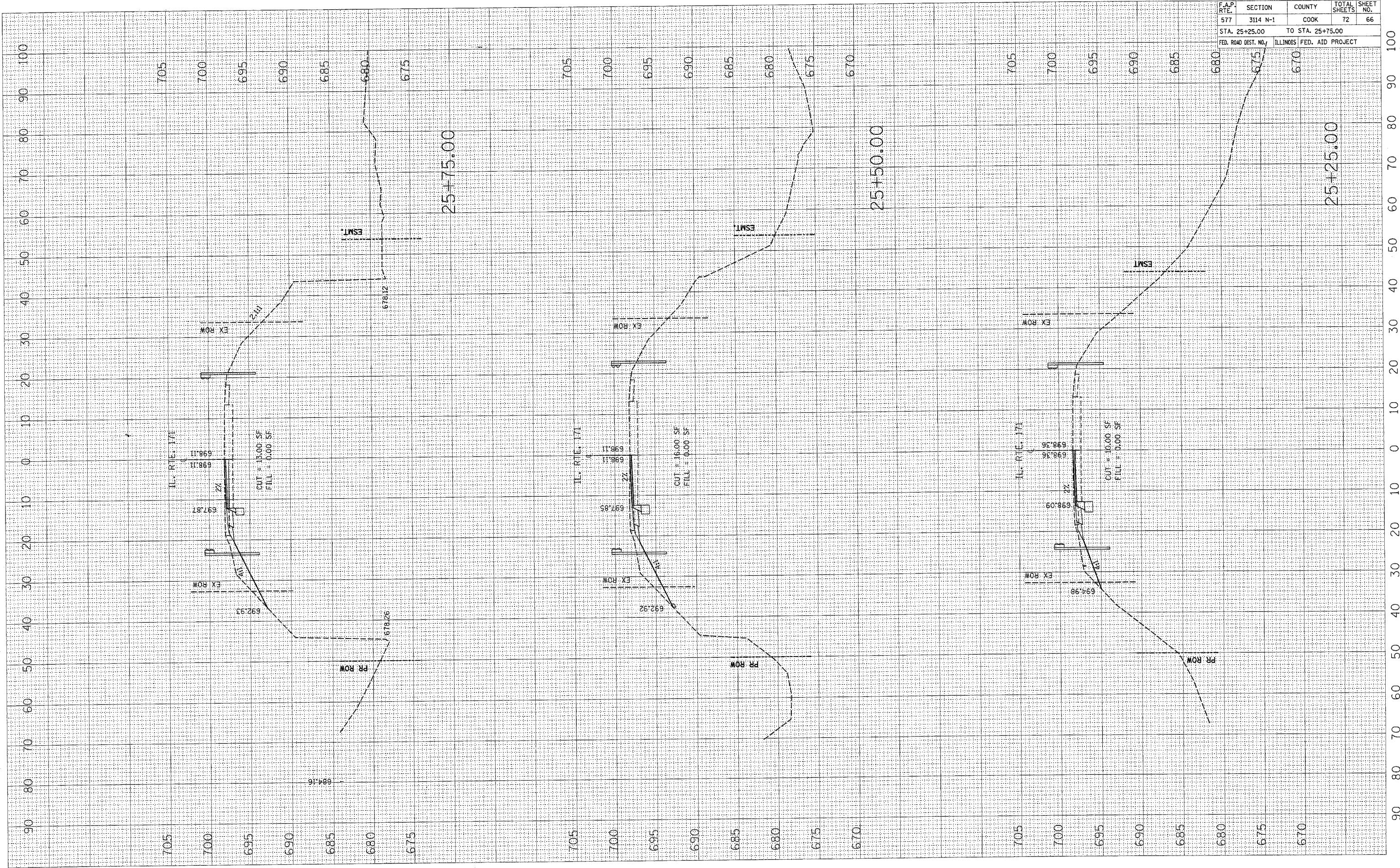


| CONTRACT NO. 60A25 | | | | |
|---------------------|----------|---------------------------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 65 |
| STA. 24+70.00 | | TO STA. 25+00.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

PLOT DATE = 5/2/2007
 FILE NAME = c:\proje\60a\118802\w-17\w-17b-17b.dwg
 PLOT SCALE = 18.00000 / 1 IN.
 USER NAME = galbarab

ORIGINAL SURVEY
 SURVEY PLOTTED
 NOTE BOOK NO.
 AREAS CHECKED

FINAL SURVEY
 SURVEY PLOTTED
 NOTE BOOK NO.
 AREAS CHECKED



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------|---------------------------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 66 |
| STA. 25+25.00 | | TO STA. 25+75.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 60A25

25+25.00

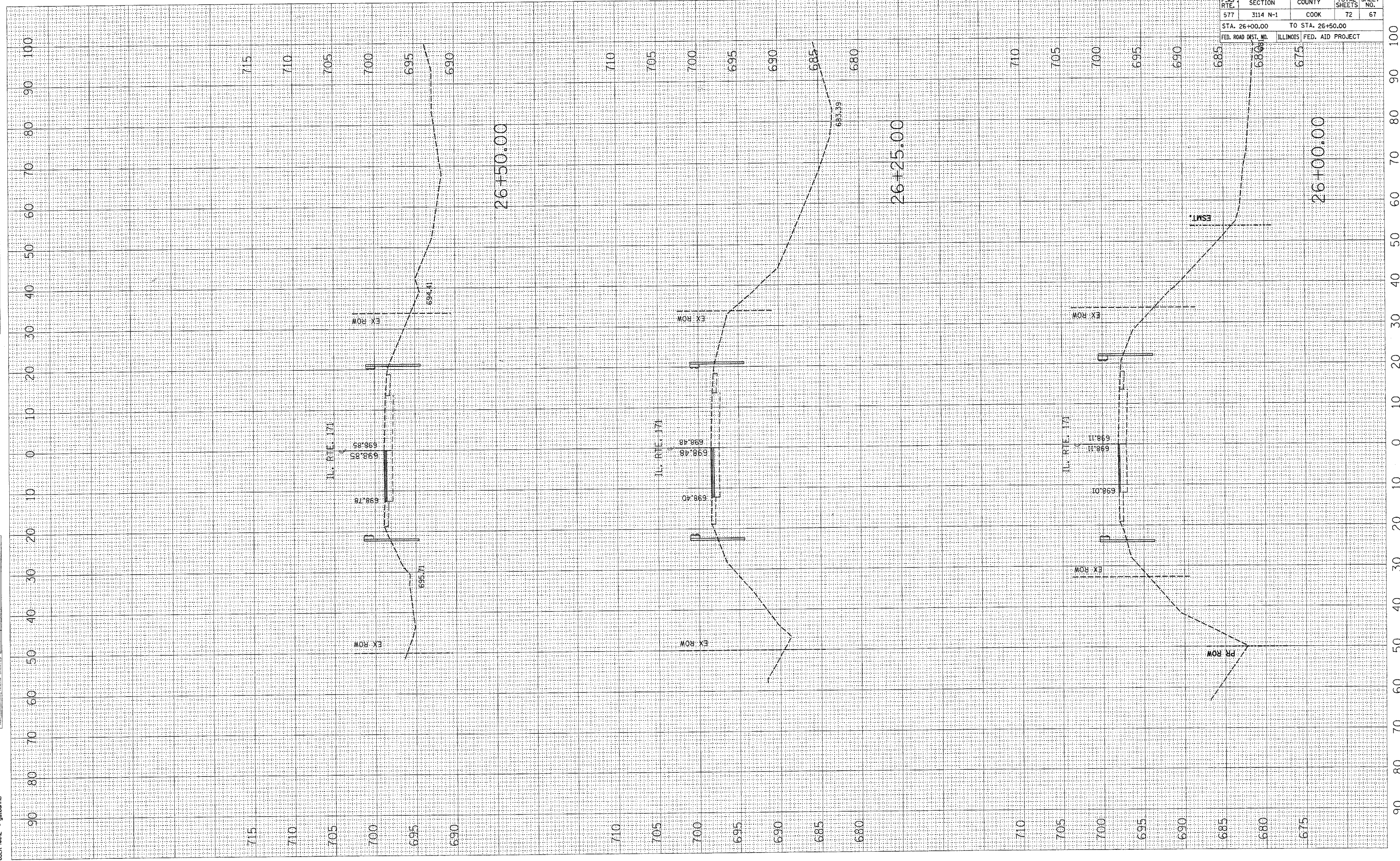
25+50.00

25+75.00

PLOT DATE = 5/2/2007
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = galbanbb

ORIGINAL SURVEY
 SURVEY PLOTTED
 PLOT DATE
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = galbanbb

FINAL SURVEY
 SURVEY PLOTTED
 PLOT DATE
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = galbanbb



| | |
|---------------------|---------------------------|
| CONTRACT NO. 60A25 | |
| F.A.P. RTE. | SECTION |
| 577 | 3114 N-1 |
| COUNTY | TOTAL SHEETS |
| COOK | 72 |
| STA. 26+00.00 | TO STA. 26+50.00 |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT |
| | SHEET NO. 67 |

CONTRACT NO. 60A25

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------|--------|--------------|-----------|
| 577 | 3114 N-1 | COOK | 72 | 68 |

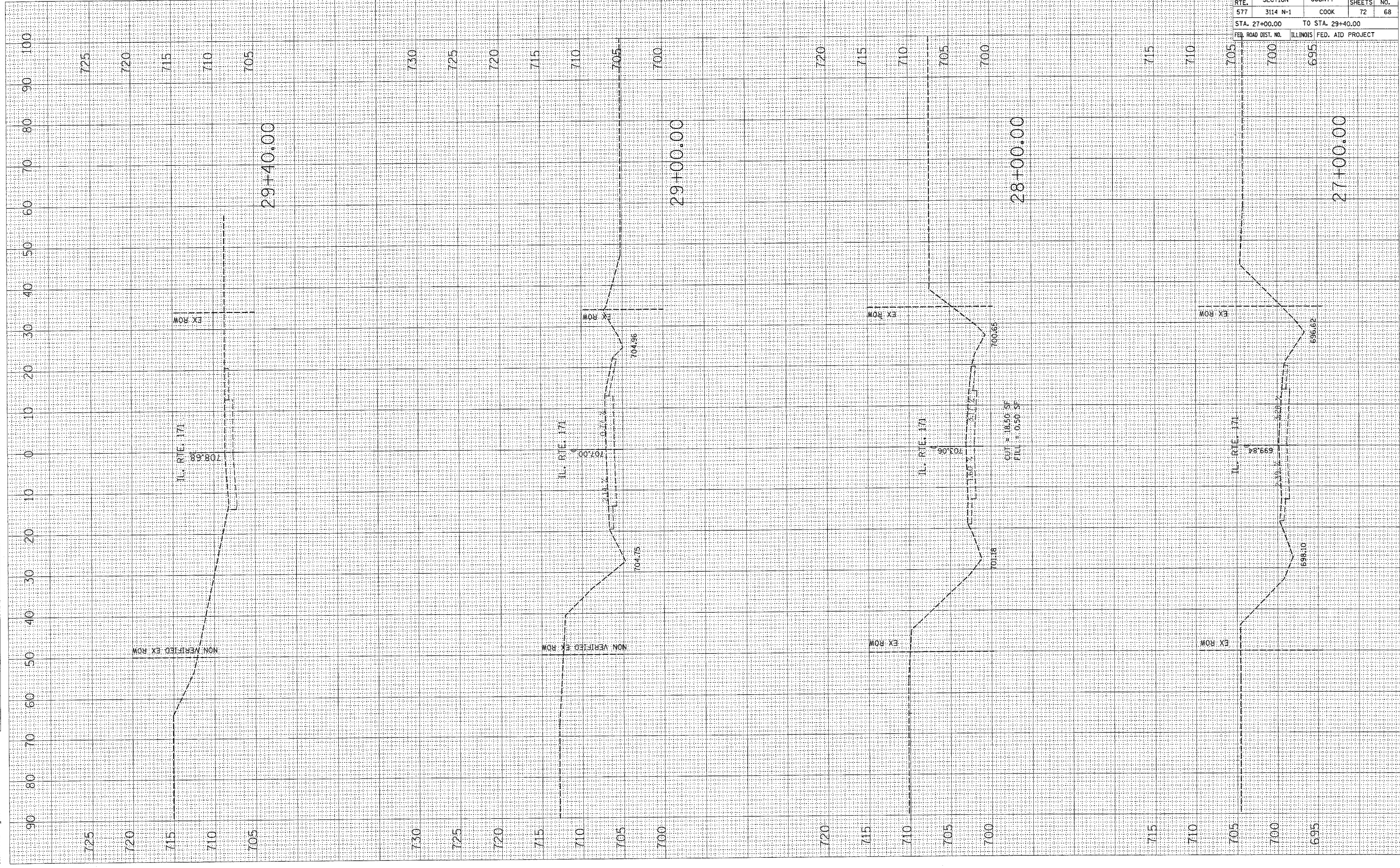
STA. 27+00.00 TO STA. 29+40.00

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

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|---------------|----|------|
| FINAL SURVEY | BY | DATE |
| NOTE BOOK | | |
| AREAS | | |
| AREAS CHECKED | | |

| | | |
|-----------------|----|------|
| ORIGINAL SURVEY | BY | DATE |
| PLOTTED | | |
| AREAS | | |
| AREAS CHECKED | | |

PLOT DATE = 5/2/2007
 PLOT SCALE = 1/8" = 30.00'
 USER NAME = gellamb

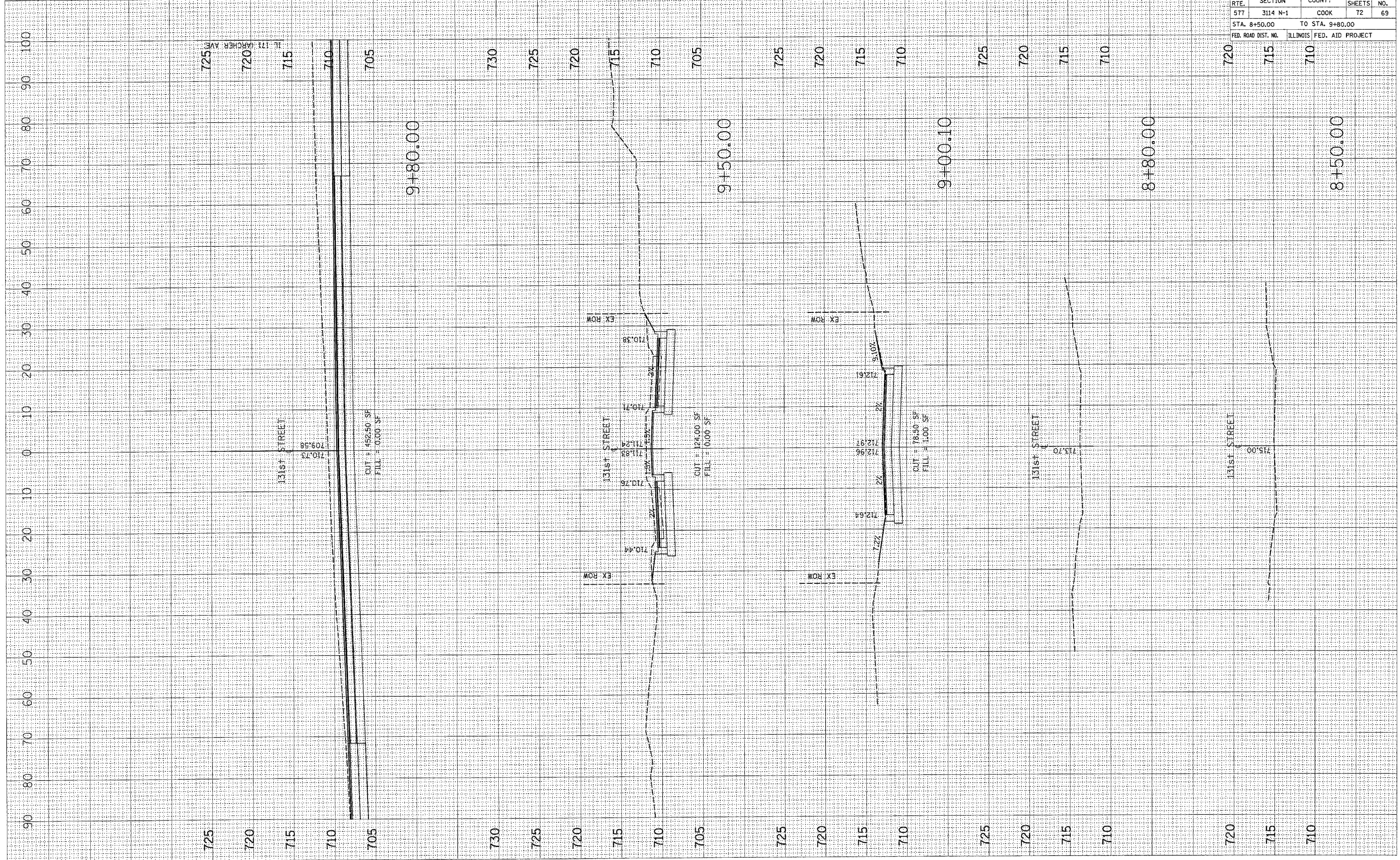


PLOT DATE = 5/2/2007
 FILE NAME = c:\projects\62391\131st\131st.dgn
 USER NAME = galbano

ORIGINAL SURVEYED
 SURVEY PLOTTED
 NOTE BOOK NO. _____
 TEMPLATE AREAS CHECKED

FINAL SURVEYED
 SURVEY PLOTTED
 NOTE BOOK NO. _____
 TEMPLATE AREAS CHECKED

BY _____
 DATE _____



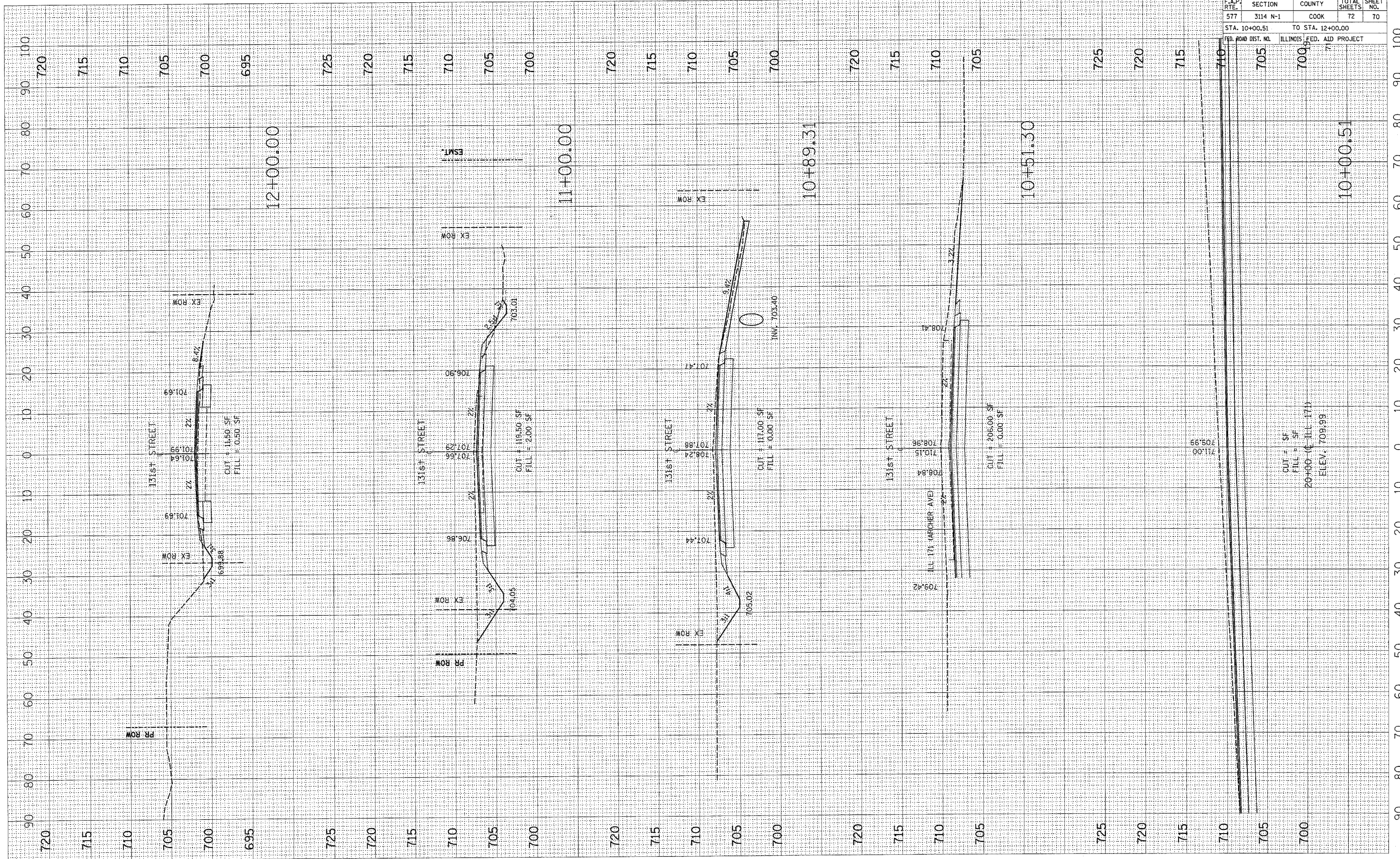
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|---------------------|----------|---------------------------|--------------|-----------|
| F.A.P. DIST. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 69 |
| STA. 8+50.00 | | TO STA. 9+80.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

PLOT DATE = 5/2/2007
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = galbamb

ORIGINAL SURVEY PLOTTED AREAS CHECKED
 SURVEY PLOTTED AREAS CHECKED
 NOTE BOOK NO. _____

FINAL SURVEY PLOTTED AREAS CHECKED
 SURVEY PLOTTED AREAS CHECKED
 NOTE BOOK NO. _____

BY _____ DATE _____



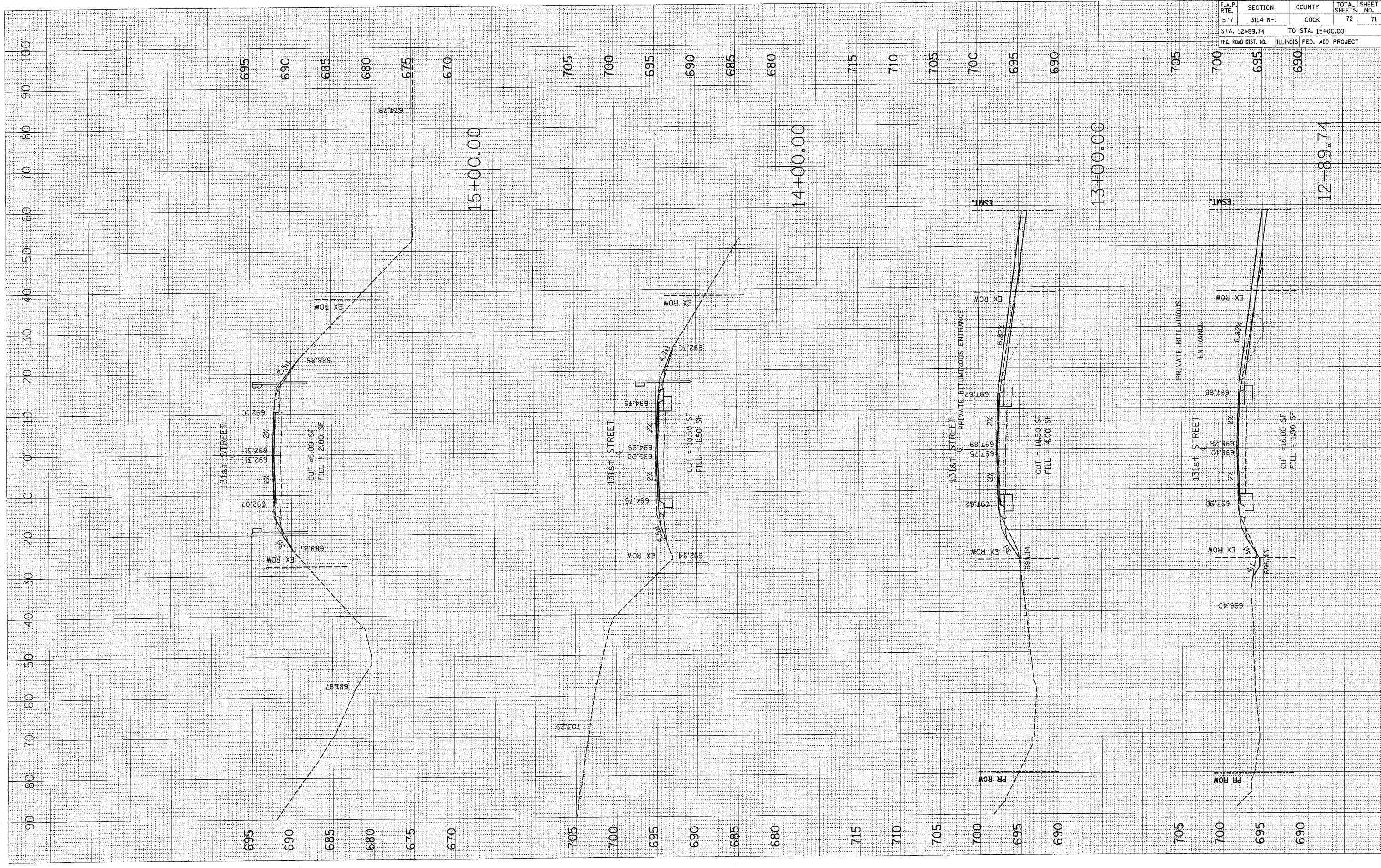
| CONTRACT NO. 60A25 | | | | |
|---------------------|----------|------------------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 70 |
| STA. 10+00.51 | | TO STA. 12+00.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CUT = SF
 FILL = SF
 20+00 (ILL 171)
 Elev. 709.99

PLOT DATE = 5/2/2007
 PLOT SCALE = 10.0000' / IN.
 USER NAME = gplbamb

ORIGINAL SURVEY PLOTTED
 SURVEY PLOTTED
 NOTE BOOK AREAS CHECKED
 NO.

FINAL SURVEY PLOTTED
 SURVEY PLOTTED
 NOTE BOOK AREAS CHECKED
 NO.

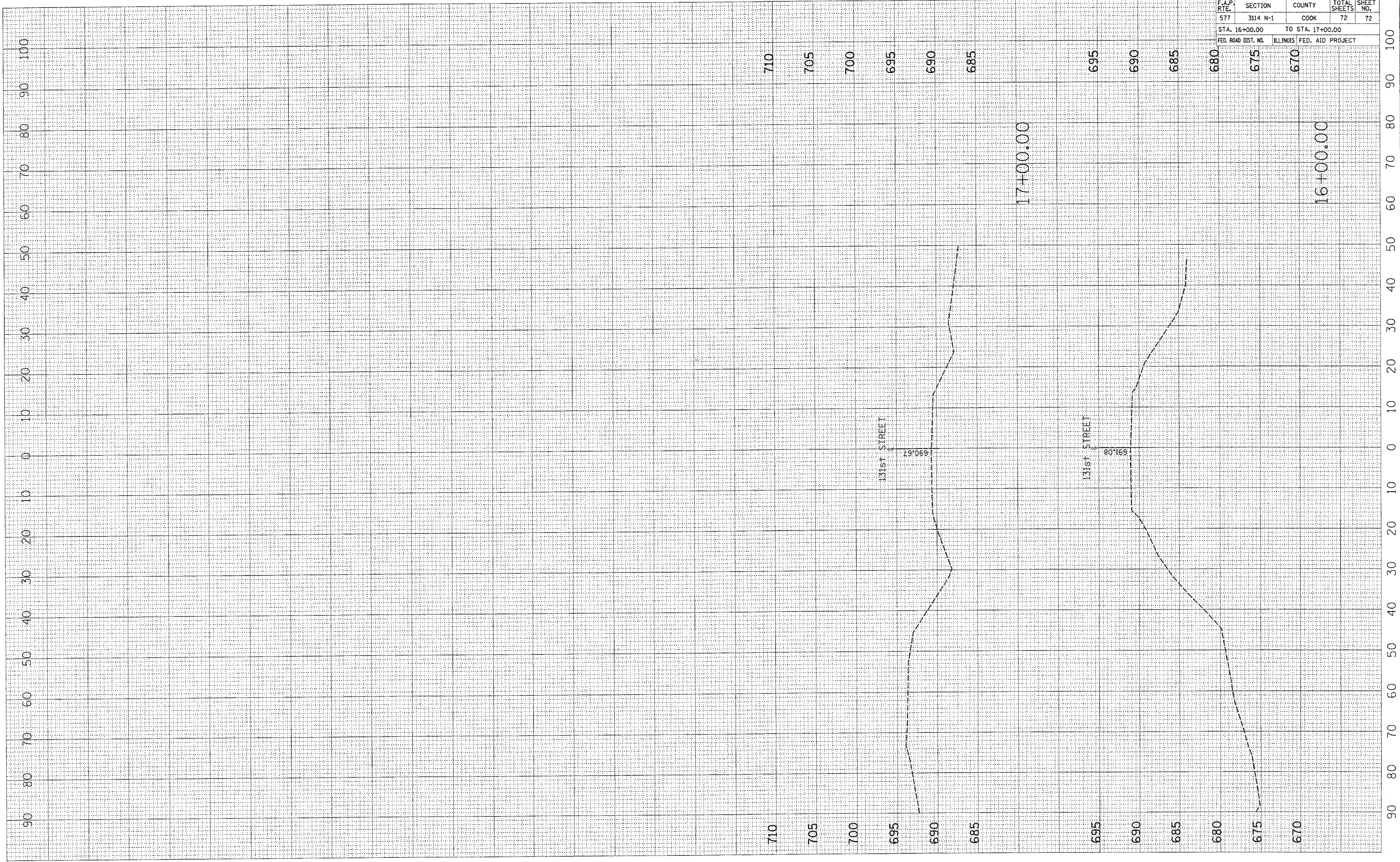


| CONTRACT NO. 60A25 | | | |
|---------------------|----------|------------------|------------------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 |
| STA. 12+89.74 | | TO STA. 15+00.00 | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT |

PLOT DATE = 5/2/2007
 FILE NAME = c:\pcc\mcs\p13682\as131_10.dgn
 PLOT SCALE = 10.0000 / IN.
 USER NAME = galbano

| | | | |
|-----------------|---------------|----|------|
| ORIGINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| AREAS CHECKED | TEMPLATE | | |
| | AREAS CHECKED | | |

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| FINAL SURVEY | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| AREAS CHECKED | TEMPLATE | | |
| | AREAS CHECKED | | |



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|---------------------|----------|---------------------------|--------------|-----------|
| CONTRACT NO. 60A25 | | | | |
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 577 | 3114 N-1 | COOK | 72 | 72 |
| STA. 16+00.00 | | TO STA. 17+00.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |