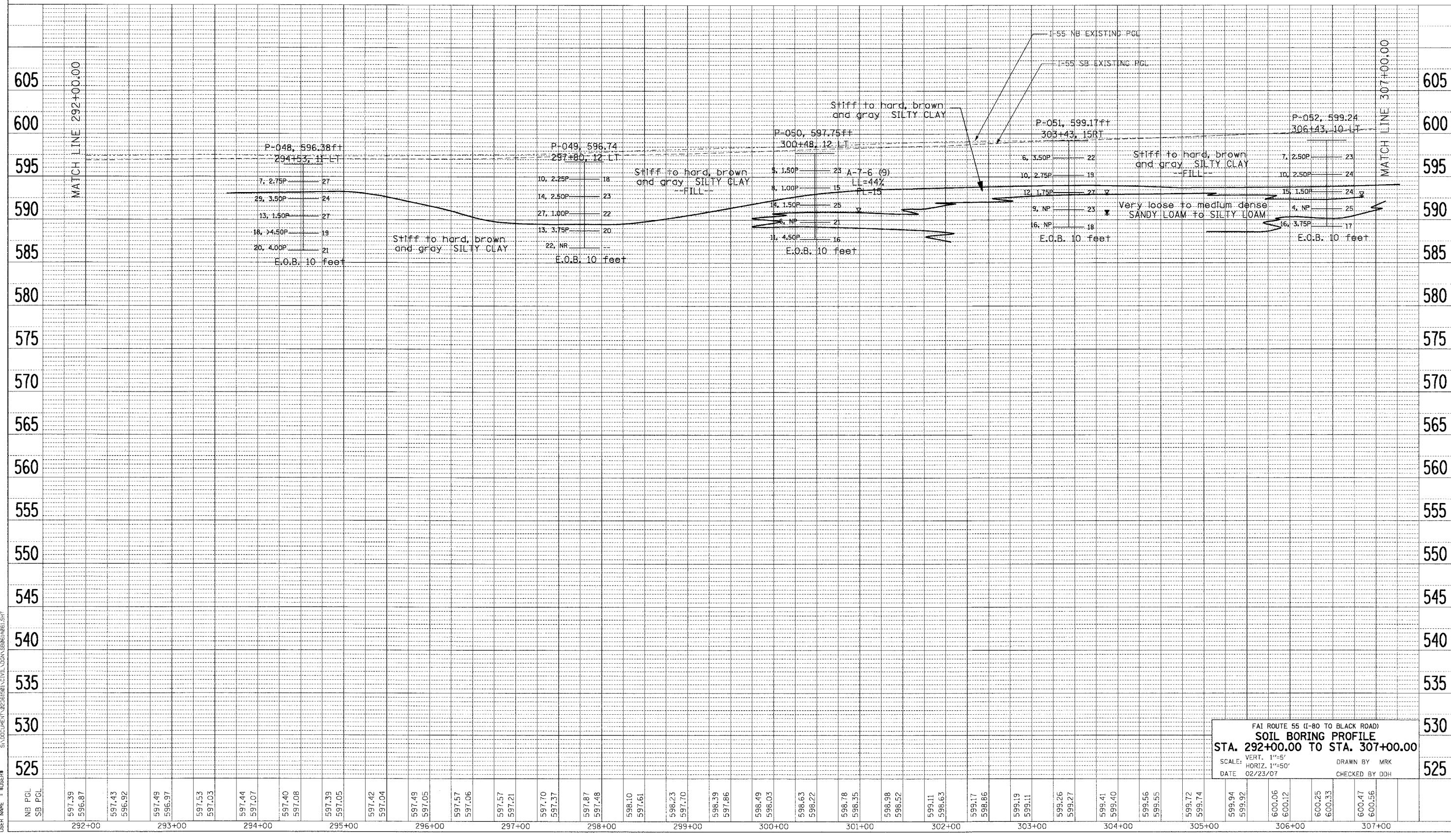


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	101
STA. TO STA.		ILLINOIS FED. AID PROJECT		

DATE	BY
DATE	BY

DATE	BY
DATE	BY

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 FILE NAME: 62895(99) I&2 WRS (I-80 TO BLACK ROAD) SOIL BORING SHEET  
 USER: MRK



FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**SOIL BORING PROFILE**  
 STA. 292+00.00 TO STA. 307+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 02/23/07  
 DRAWN BY MRK  
 CHECKED BY DDH

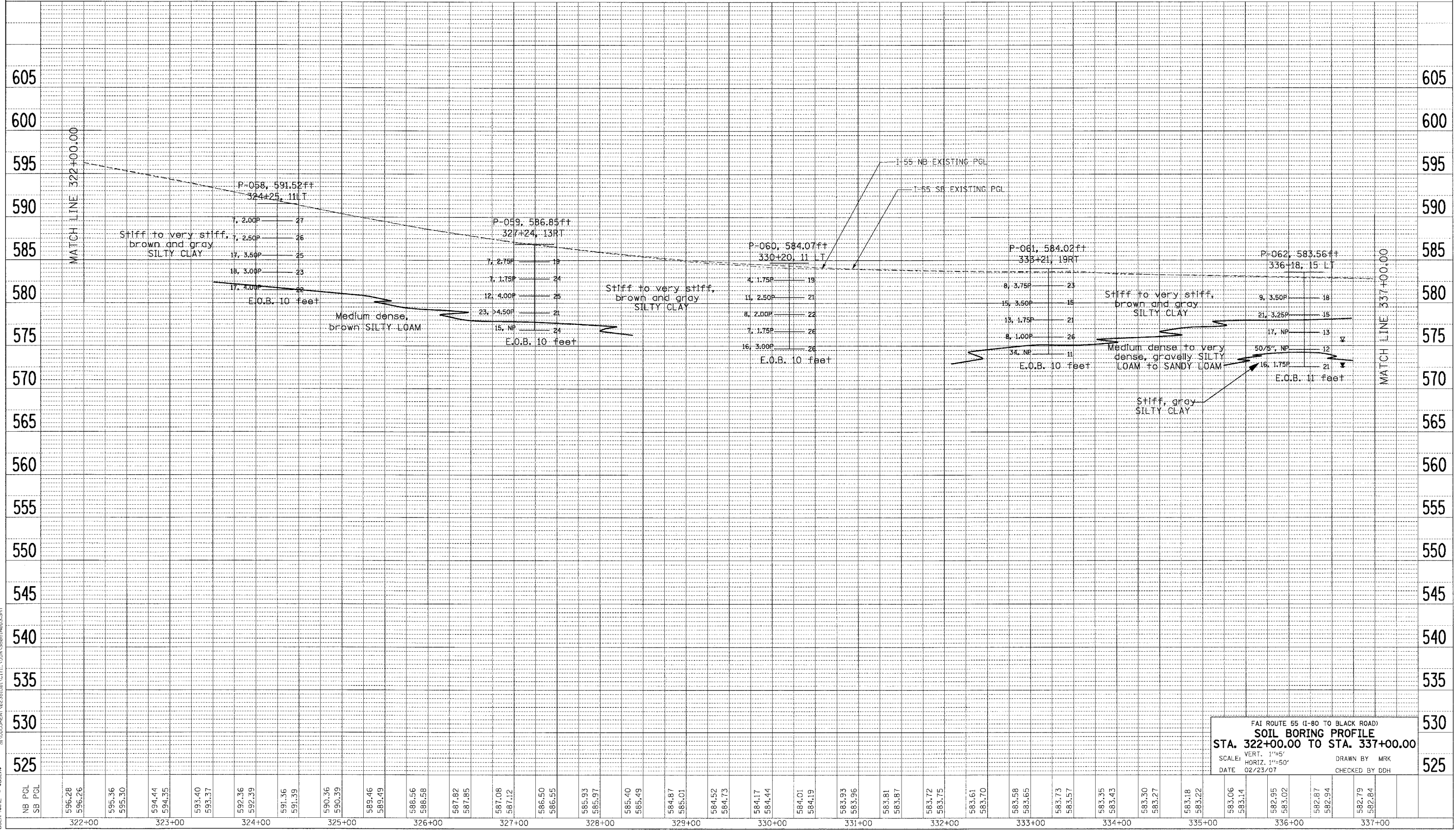


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	103
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PL	REVISIONS	BY	DATE
	1. PLOTTED		
	2. CHECKED		
	3. DATE		
	4. FILE NAME		

PROFILE	REVISIONS	BY	DATE
	1. PLOTTED		
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	4. FILE NAME		

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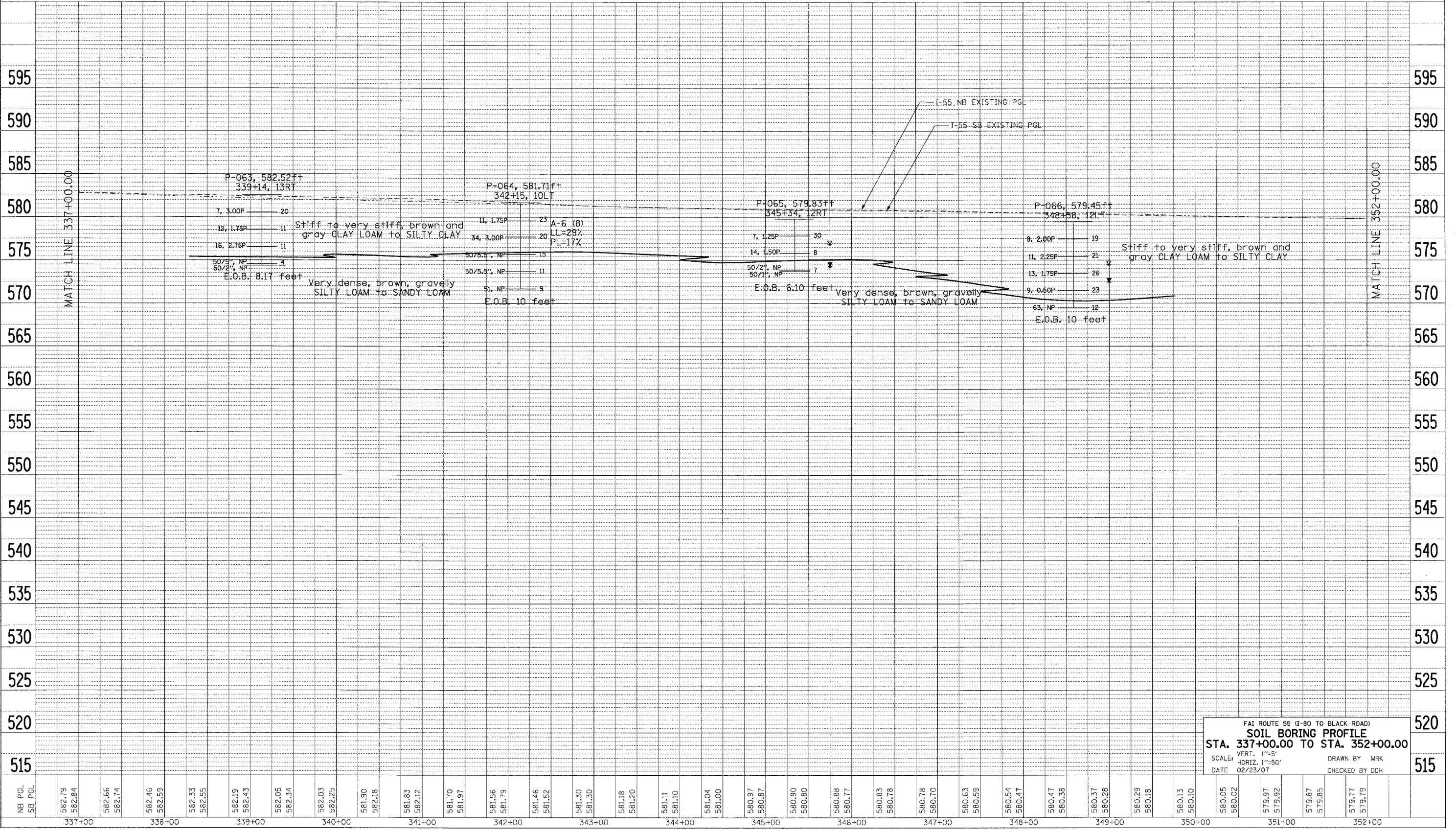
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**SOIL BORING PROFILE**  
 STA. 322+00.00 TO STA. 337+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 02/23/07  
 DRAWN BY MRK  
 CHECKED BY DDH

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	104
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PL	BY	DATE

PROFILE	BY	DATE

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FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**SOIL BORING PROFILE**  
 STA. 337+00.00 TO STA. 352+00.00  
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 HORIZ. 1"=50'  
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 DRAWN BY MRK  
 CHECKED BY DDH

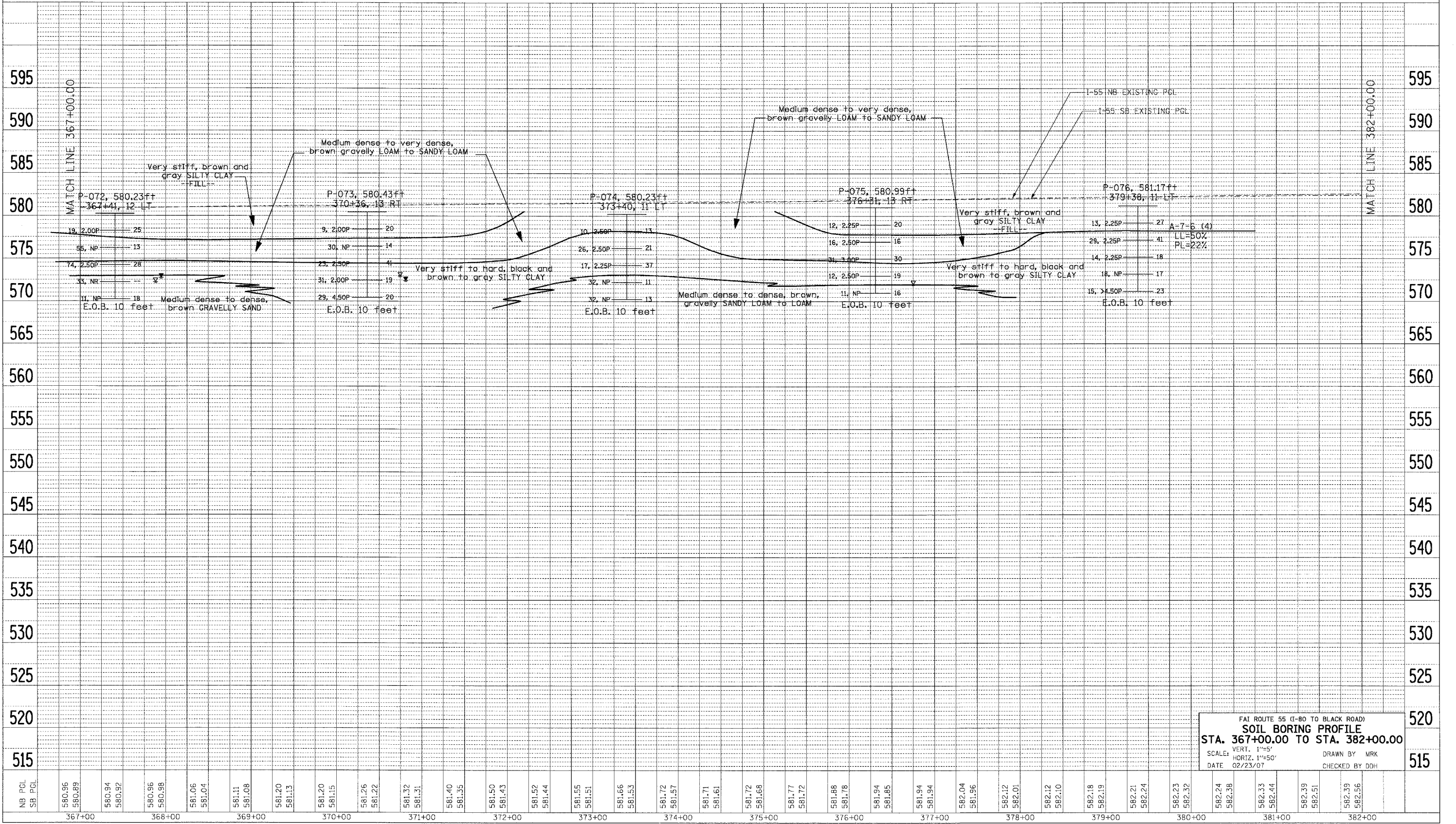


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	106
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

DATE	BY

DATE	BY

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FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**SOIL BORING PROFILE**  
 STA. 367+00.00 TO STA. 382+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 02/23/07  
 DRAWN BY MRK  
 CHECKED BY DDH







F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	109
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

## SUGGESTED STAGES OF CONSTRUCTION

### PRE-STAGE 2007

1. INSTALL "CHANGEABLE MESSAGE SIGNS" PRIOR TO THE START OF CONSTRUCTION ACTIVITY ON FAI-55.
2. CONSTRUCT DETENTION BASINS.
3. COMPLETE ALL PROPOSED GUARDRAIL WORK ON THE OUTSIDE SHOULDERS.
4. CONSTRUCT STABILIZED CONSTRUCTION ENTRANCES WITHIN THE STAGE I 2007 CONSTRUCTION SECTION(S).
5. CONSTRUCT PROPOSED STORM SEWER LATERAL OUTLETS BENEATH THE EXISTING I-55 NB AND SB LANES USING STANDARDS 701400 AND 701401 AND AS SHOWN ON THE TYPICAL SECTIONS FOR STORM SEWER LATERALS CONSTRUCTION. LANE CLOSURES WILL ONLY BE ALLOWED DURING THE OFF PEAK HOURS AS DETAILED IN THE SPECIAL PROVISION FOR "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC."
6. BEGIN CONSTRUCTION OF ACCIDENT INVESTIGATION SITES.

### STAGE I - 2007 CONSTRUCTION

1. COMPLETE CONSTRUCTION OF ACCIDENT INVESTIGATION SITES.  
COMPLETE THE FOLLOWING ITEMS OF WORK WITHIN THE SECTION(S) OF I-55 TO BE COMPLETED DURING THE 2007 CONSTRUCTION SEASON AS DESCRIBED IN THE SPECIAL PROVISIONS. THE STAGING TYPICAL SECTIONS AND THE SCHEDULE OF QUANTITIES SHOW THE SECTION FROM THE SOUTH PROJECT LIMIT TO THE CENTER PIER OF THE I-80 BRIDGE OVER I-55 AS THE SUGGESTED SECTION TO BE COMPLETED DURING THE 2007 CONSTRUCTION SEASON.
2. ESTABLISH TRAFFIC CONTROL, REMOVE EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT REFLECTORS THAT CONFLICT WITH THE STAGE I TEMPORARY TRAFFIC LANES, APPLY PAVEMENT MARKING TAPE, TYPE III, AND INSTALL TEMPORARY CONCRETE BARRIER AS SHOWN ON STAGE I TYPICAL SECTIONS AND STAGING AND TRAFFIC CONTROL PLAN SHEETS.
3. REMOVE GUARDRAIL FROM WITHIN THE MEDIAN.
4. REMOVE SHOULDER AND EXCAVATE AND CONSTRUCT EMBANKMENT WITHIN THE MEDIAN.
5. CONSTRUCT ALL DRAINAGE ITEMS WITHIN THE WORK AREA. DURING STAGE I, ALL FRAMES AND GRATES SHALL BE SET TO THE PROPOSED ELEVATION OF THE TOP OF THE BINDER COURSE AS DESCRIBED IN THE DRAINAGE GENERAL NOTES.
6. CONSTRUCT AGGREGATE SUBGRADE, BARRIER BASE, CONCRETE BARRIER, BARRIER WALL MARKERS, AND BRIDGE PIER CRASH WALL MODIFICATIONS.
7. CONSTRUCT THE BASE COURSE FOR THE NEW INSIDE LANES (LANE 1) AND THE INSIDE SHOULDERS.
8. CONSTRUCT TEMPORARY ASPHALT WEDGE ON THE NEW BASE COURSE AT THE EDGE OF THE EXISTING INSIDE LANE.
9. AFTER CONSTRUCTION OF THE CONCRETE BARRIER, THE BASE COURSE FOR THE NEW INSIDE LANES (LANE 1) AND SHOULDERS, AND THE TEMPORARY ASPHALT WEDGE IS COMPLETED, THE TEMPORARY CONCRETE BARRIER SHALL BE REMOVED.

### WINTER SHUTDOWN (SEE SPECIAL PROVISIONS FOR DATES)

REMOVE PAVEMENT MARKINGS THAT CONFLICT WITH THE WINTER SHUTDOWN TRAFFIC LANES. APPLY PAVEMENT MARKINGS AS SHOWN ON WINTER SHUTDOWN TYPICAL SECTIONS, INSTALL REPLACEMENT REFLECTORS IN THE CASTINGS OF THE EXISTING RAISED REFLECTIVE PAVEMENT MARKERS, AND MAINTAIN TRAFFIC IN EXISTING TRAFFIC LANES AS SHOWN ON WINTER SHUTDOWN TYPICAL SECTION.

### STAGE I - 2008 CONSTRUCTION

1. ESTABLISH TRAFFIC CONTROL, REMOVE PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS (REMOVE JUST THE REFLECTORS IN AREAS BEYOND THE MILLING AND RESURFACING LIMITS AS DESCRIBED IN THE GENERAL NOTES) THAT CONFLICT WITH THE STAGE I TEMPORARY TRAFFIC LANES, APPLY PAVEMENT MARKINGS, AND INSTALL TEMPORARY CONCRETE BARRIER AS SHOWN ON STAGE I TYPICAL SECTIONS AND STAGING AND TRAFFIC CONTROL PLAN SHEETS.
2. CONSTRUCT STABILIZED CONSTRUCTION ENTRANCES.
3. CONSTRUCT CENTER PIER FOR THE FUTURE IL RTE 59 BRIDGE OVER I-55 BY DATE SPECIFIED IN THE SPECIAL PROVISIONS.
4. REMOVE GUARDRAIL FROM WITHIN THE MEDIAN.
5. REMOVE SHOULDER AND EXCAVATE AND CONSTRUCT EMBANKMENT WITHIN THE MEDIAN.
6. CONSTRUCT ALL DRAINAGE ITEMS WITHIN THE WORK AREA. DURING STAGE I, ALL FRAMES AND GRATES SHALL BE SET TO THE PROPOSED ELEVATION OF THE TOP OF THE BINDER COURSE AS DESCRIBED IN THE DRAINAGE GENERAL NOTES.
7. CONSTRUCT AGGREGATE SUBGRADE, BARRIER BASE, CONCRETE BARRIER, BARRIER WALL MARKERS, AND BRIDGE PIER CRASH WALL MODIFICATIONS.
8. CONSTRUCT THE BASE COURSE FOR THE NEW INSIDE LANES (LANE 1) AND THE INSIDE SHOULDERS.
9. AFTER CONSTRUCTION OF THE CONCRETE BARRIER AND THE BASE COURSE FOR THE NEW INSIDE LANES (LANE 1) AND SHOULDERS IS COMPLETED, THE TEMPORARY CONCRETE BARRIER SHALL BE REMOVED AND REPLACED WITH DRUMS.
10. MILL THE TEMPORARY ASPHALT WEDGE AND THE INSIDE 7 FEET OF THE CENTER LANES (LANE 2).
11. PATCH THE INSIDE 7 FEET OF THE CENTER LANES (LANE 2).
12. CONSTRUCT THE BINDER COURSE ON THE INSIDE 7 FT OF THE CENTER LANES (LANE 2), THE NEW INSIDE LANES (LANE 1) AND INSIDE SHOULDERS.

### STAGE II

1. ESTABLISH TRAFFIC CONTROL AND APPLY PAVEMENT MARKINGS AS SHOWN ON STAGE II TYPICAL SECTIONS AND SUGGESTED STAGING AND TRAFFIC CONTROL PLAN SHEETS.
2. RELOCATE LIGHT POLES AND CONSTRUCT AUXILIARY LANE ON I-55 SB AT I-80 INTERCHANGE.
3. MILL THE OUTSIDE SHOULDERS, OUTSIDE LANES (LANE 3), AND THE OUTSIDE 5 FEET OF THE CENTER LANES (LANE 2).
4. PATCH AND THEN CONSTRUCT THE BINDER COURSE ON THE OUTSIDE LANES (LANE 3) AND THE OUTSIDE 5 FEET OF THE CENTER LANES (LANE 2).
5. CONSTRUCT THE SURFACE COURSE ON THE OUTSIDE LANES (LANE 3) AND SHOULDERS.
6. APPLY PERMANENT PAVEMENT MARKINGS AS SHOWN ON THE PAVEMENT MARKING PLANS FOR OUTSIDE EDGE LINES AND SHOULDERS. APPLY A FULL STANDARD 5" WHITE SKIP DASH LANE LINE TO THE BINDER COURSE BETWEEN THE PROPOSED CENTER LANE (LANE 2) AND THE OUTSIDE LANE (LANE 3) IN BOTH DIRECTIONS USING EPOXY PAVEMENT MARKING AS SHOWN ON THE STAGE III TYPICAL SECTIONS.

### STAGE III

1. ESTABLISH TRAFFIC CONTROL AND REMOVE THE EXISTING STAGE II RIGHT EDGE LINE FROM THE PROPOSED CENTER LANE (LANE 2) AND APPLY A 4" YELLOW LINE USING PAVEMENT MARKING TAPE, TYPE III AS THE INSIDE EDGE LINE BETWEEN THE PROPOSED INSIDE LANE (LANE 1) AND THE CENTER LANE (LANE 2) IN BOTH DIRECTIONS. OPEN THE PROPOSED CENTER AND OUTSIDE LANES TO TRAFFIC.
2. USING FREEWAY STANDARD 701400 AND 701401 FOR ONE LANE CLOSURES, DURING THE OFF PEAK HOURS AS DETAILED IN THE SPECIAL PROVISIONS FOR "KEEPING THE EXPRESSWAY OPEN TO TRAFFIC", PERFORM THE FOLLOWING OPERATIONS:
  - A. CONSTRUCT THE SURFACE COURSE ON THE INSIDE AND CENTER LANES (LANES 1 & 2) AND THE INSIDE SHOULDERS. CONSTRUCT THE SURFACE COURSE FIRST ON LANE 2 FOLLOWED BY LANE 1 AND THEN THE INSIDE SHOULDERS.
  - B. CONSTRUCT REMAINING PERMANENT PAVEMENT MARKINGS, RAISED REFLECTIVE PAVEMENT MARKERS, AND RUMBLE STRIPS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
SUGGESTED STAGES OF CONSTRUCTION**

SCALE: DATE 03/15/07 DRAWN BY OP CHECKED BY DDH

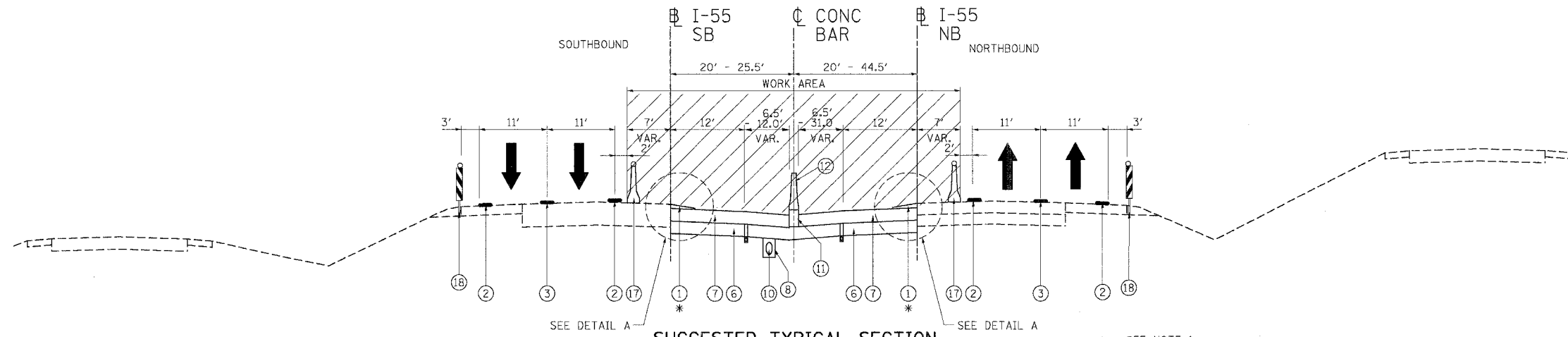
**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

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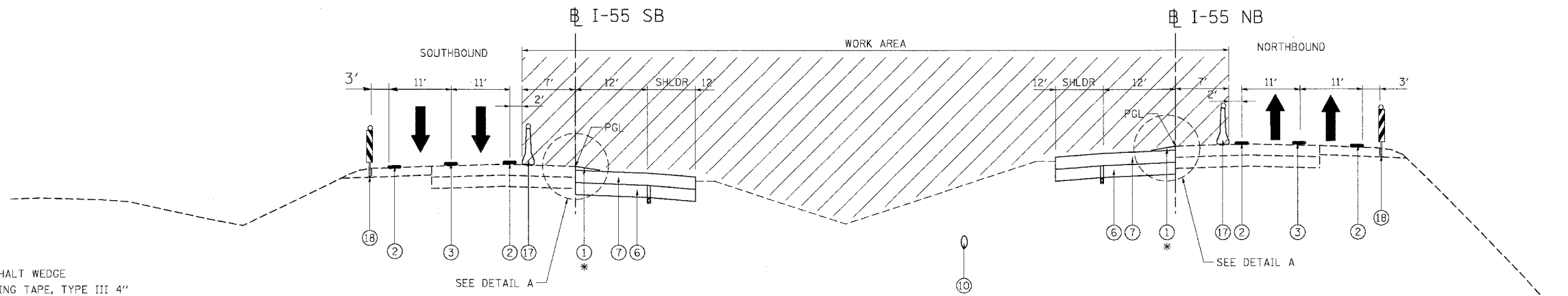
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	112
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**SUGGESTED TYPICAL SECTION**  
**STAGE I - 2007 CONSTRUCTION SEASON**

STA. 190+15 TO STA. 210+81

\* SEE NOTE 1



**SUGGESTED TYPICAL SECTION**  
**STAGE I - 2007 CONSTRUCTION SEASON**

STA. 186+03 TO STA. 190+15 SOUTHBOUND  
 STA. 187+28 TO STA. 190+15 NORTHBOUND

\* SEE NOTE 1

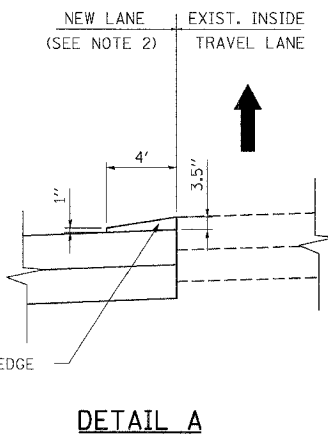
**LEGEND:**

- ① TEMPORARY ASPHALT WEDGE
- ② PAVEMENT MARKING TAPE, TYPE III 4"
- ③ PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH
- ④ BITUMINOUS SURFACE REMOVAL, 3 1/2"
- ⑤ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA 1 3/4"
- ⑥ AGGREGATE SUBGRADE, 12"
- ⑦ HOT-MIX ASPHALT BASE COURSE
- ⑧ TRENCH BACKFILL
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 1 3/4"
- ⑩ STORM SEWER
- ⑪ BARRIER BASE
- ⑫ CONCRETE BARRIER, DOUBLE FACE
- ⑬ DRUMS W/ STEADY BURN LIGHTS @ 100' C-C
- ⑭ CHECK DRUM W/ STEADY BURN LIGHTS @ 1000' C-C
- ⑮ BITUMINOUS SURFACE REMOVAL (VARIABLE DEPTH)
- ⑯ HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70, 2"
- ⑰ TEMPORARY CONCRETE BARRIER
- ⑱ VERTICAL PANEL W/ STEADY BURN LIGHTS @ 100' C-C IN CURVES AND 200' C-C ON TANGENTS
- ⑲ EPOXY PAVEMENT MARKING - LINE 4"
- ⑳ EPOXY PAVEMENT MARKING - LINE 5" SKIP-DASH
- ㉑ PERMANENT THERMOPLASTIC PAVEMENT MARKING - LINE 4"

- PAVEMENT PATCHING (AS DIRECTED BY THE ENGINEER)
- BITUMINOUS SURFACE REMOVAL AND RESURFACING
- WORK AREA
- TRAFFIC FLOW

**NOTES:**

1. THE COST OF TEMPORARY ASPHALT WEDGE REMOVAL WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR "TEMPORARY ASPHALT WEDGE."



**DETAIL A**

**MIXTURE REQUIREMENTS**

APPLICATION	MIXTURE TYPE	AC TYPE	AIR VOIDS
TEMPORARY ASPHALT WEDGE	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N 70 (IL 9.5 mm)	PG 64-22	4% @ N70

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

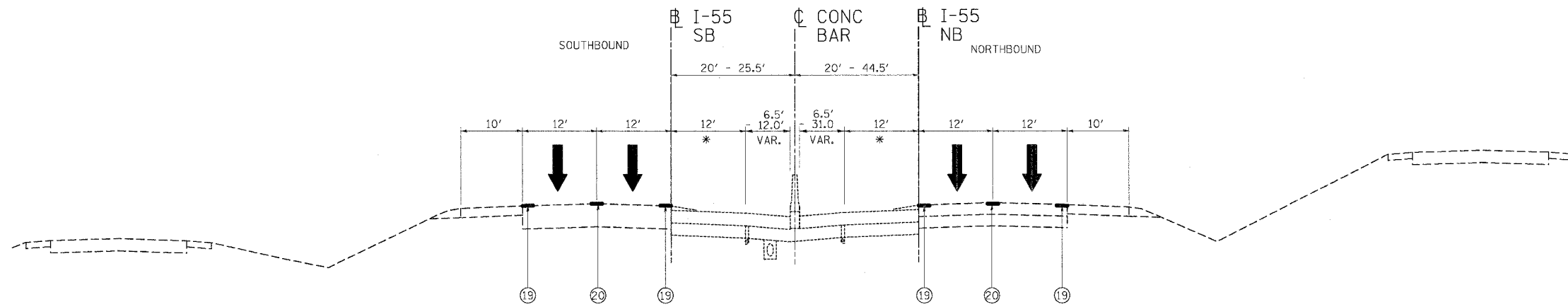
**STAGING AND TRAFFIC CONTROL**  
**FAI 55 - TYPICAL SECTIONS**  
**STAGE I - 2007**

SCALE: DRAWN BY AG  
 DATE 02/23/07 CHECKED BY DDH

**TENG** TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 02/23/07  
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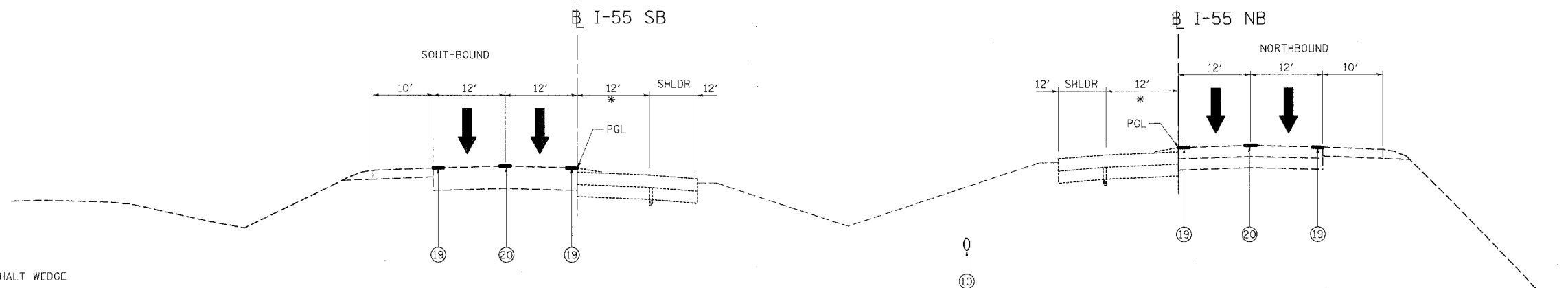
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	113
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



**SUGGESTED TYPICAL SECTION  
WINTER SHUTDOWN**

STA. 190+15 TO STA. 210+81

\* SEE NOTE 1



**SUGGESTED TYPICAL SECTION  
WINTER SHUTDOWN**

STA. 186+03 TO STA. 190+15 SOUTHBOUND  
STA. 187+28 TO STA. 190+15 NORTHBOUND

\* SEE NOTE 1

**NOTES:**

1. THE NEW INSIDE LANE SHALL BE STRIPED WITH 12" YELLOW DIAGONAL LINES SPACED AT 150' C-C USING EPOXY PAVEMENT MARKINGS. THE CONTRACTOR MAY ELECT TO USE EXTRUDED THERMOPLASTIC, PLACED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, IN LIEU OF EPOXY PAVEMENT MARKING AT NO ADDITIONAL COST TO THE CONTRACT.

**LEGEND:**

- ① TEMPORARY ASPHALT WEDGE
- ② PAVEMENT MARKING TAPE, TYPE III 4"
- ③ PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH
- ④ BITUMINOUS SURFACE REMOVAL, 3 1/2"
- ⑤ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA 1 3/4"
- ⑥ AGGREGATE SUBGRADE, 12"
- ⑦ HOT-MIX ASPHALT BASE COURSE
- ⑧ TRENCH BACKFILL
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 1 3/4"
- ⑩ STORM SEWER
- ⑪ BARRIER BASE
- ⑫ CONCRETE BARRIER, DOUBLE FACE
- ⑬ DRUMS W/ STEADY BURN LIGHTS @ 100' C-C
- ⑭ CHECK DRUM W/ STEADY BURN LIGHTS @ 1000' C-C
- ⑮ BITUMINOUS SURFACE REMOVAL (VARIABLE DEPTH)
- ⑯ HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70, 2"
- ⑰ TEMPORARY CONCRETE BARRIER
- ⑱ VERTICAL PANEL W/ STEADY BURN LIGHTS @ 100' C-C IN CURVES AND 200' C-C ON TANGENTS
- ⑲ EPOXY PAVEMENT MARKING - LINE 4"
- ⑳ EPOXY PAVEMENT MARKING - LINE 5" SKIP-DASH
- ㉑ PERMANENT THERMOPLASTIC PAVEMENT MARKING - LINE 4"

- PAVEMENT PATCHING (AS DIRECTED BY THE ENGINEER)
- BITUMINOUS SURFACE REMOVAL AND RESURFACING
- WORK AREA
- TRAFFIC FLOW

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

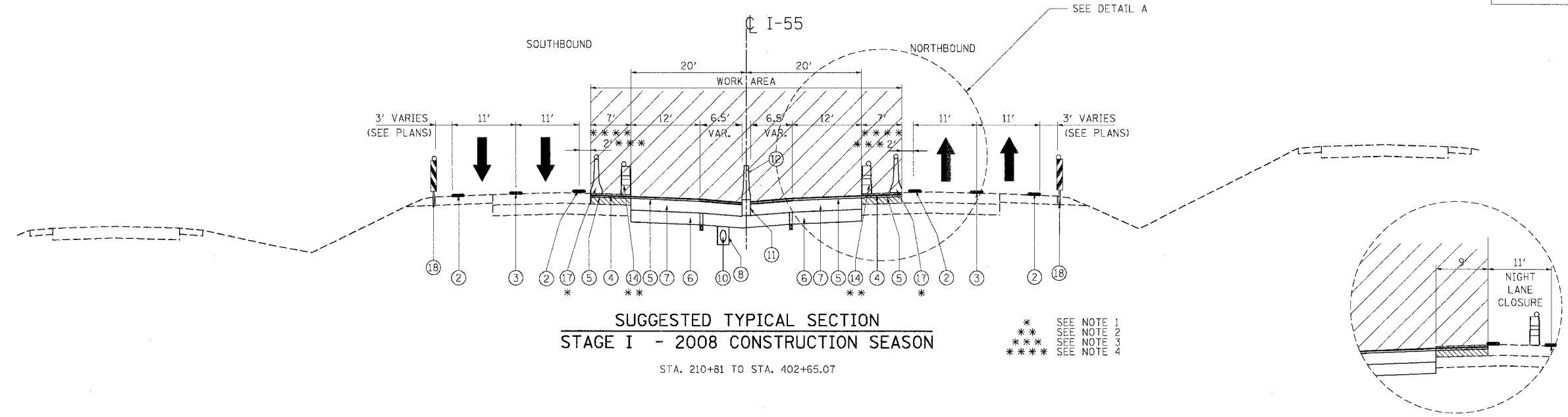
**STAGING AND TRAFFIC CONTROL  
FAI 55 - TYPICAL SECTIONS  
WINTER SHUTDOWN**

SCALE: \_\_\_\_\_ DRAWN BY AG  
DATE 02/23/07 CHECKED BY DDH

**TENG** TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE = 02/23/07  
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F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**SUGGESTED TYPICAL SECTION  
STAGE I - 2008 CONSTRUCTION SEASON**

STA. 210+81 TO STA. 402+65.07

SEE NOTE 1  
SEE NOTE 2  
SEE NOTE 3  
SEE NOTE 4

**DETAIL A  
MILLING, PATCHING, AND BINDER PLACEMENT  
DURING NIGHT TIME HOURS FOR LANE CLOSURES  
NEAR FUTURE IL RTE 59 PIER**

**LEGEND:**

- ① TEMPORARY ASPHALT WEDGE
  - ② PAVEMENT MARKING TAPE, TYPE III 4"
  - ③ PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH
  - ④ BITUMINOUS SURFACE REMOVAL, 3 1/2"
  - ⑤ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA 1 3/4"
  - ⑥ AGGREGATE SUBGRADE, 12"
  - ⑦ HOT-MIX ASPHALT BASE COURSE
  - ⑧ TRENCH BACKFILL
  - ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 1 3/4"
  - ⑩ STORM SEWER
  - ⑪ BARRIER BASE
  - ⑫ CONCRETE BARRIER, DOUBLE FACE
  - ⑬ DRUMS W/ STEADY BURN LIGHTS @ 100' C-C
  - ⑭ CHECK DRUM W/ STEADY BURN LIGHTS @ 1000' C-C
  - ⑮ BITUMINOUS SURFACE REMOVAL (VARIABLE DEPTH)
  - ⑯ HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70, 2"
  - ⑰ TEMPORARY CONCRETE BARRIER
  - ⑱ VERTICAL PANEL W/ STEADY BURN LIGHTS @ 100' C-C IN CURVES AND 200' C-C ON TANGENTS
  - ⑲ EPOXY PAVEMENT MARKING - LINE 4"
  - ⑳ EPOXY PAVEMENT MARKING - LINE 5" SKIP-DASH
  - ㉑ PERMANENT THERMOPLASTIC PAVEMENT MARKING - LINE 4"
- PAVEMENT PATCHING (AS DIRECTED BY THE ENGINEER)  
 BITUMINOUS SURFACE REMOVAL AND RESURFACING  
 WORK AREA  
 TRAFFIC FLOW

**NOTES:**

1. AFTER THE CONSTRUCTION OF THE CONCRETE BARRIER AND THE BASE COURSE OF NEW INSIDE LANES AND SHOULDERS IS COMPLETE, THE CONTRACTOR SHALL REMOVE THE TEMPORARY CONCRETE BARRIER AND REPLACE IT WITH DRUMS WITH STEADY BURN LIGHTS SPACED AS SHOWN ON THE PLANS. AFTER THE TEMPORARY CONCRETE BARRIER IS REPLACED WITH DRUMS, EMERGENCY PULLOUTS SHALL CONTINUE TO BE PROVIDED APPROXIMATELY EVERY MILE AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE DETAIL FOR EMERGENCY PULLOUT USING DRUMS IN LIEU OF THE TEMPORARY CONCRETE BARRIER.  
  
DRUMS SHALL NOT BE PLACED IN THE LANE WHICH IS OPEN TO TRAFFIC. DURING MILLING AND PAVING OPERATIONS, THE CONTRACTOR SHALL USE ONE FOOT WIDE VERTICAL BARRICADES WITH STEADY BURN LIGHTS ALONG THE MILLING AND PAVING OPERATIONS. DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS SHALL BE USED TO DELINEATE ALL OPEN RAMPS THROUGH THE WORK AREA.
2. CHECK DRUMS WILL NOT BE REQUIRED WHEN TEMPORARY CONCRETE BARRIER ON STAGE I CONSTRUCTION IS BEING USED.
3. A DISTANCE OF 2 FEET SHALL BE MAINTAINED BETWEEN THE LANE LINE AND THE TEMPORARY CONCRETE BARRIER THROUGHOUT THE PROJECT.  
  
THE DISTANCE BETWEEN THE LANE LINE AND THE EDGE OF THE MILLING, PATCHING, AND PLACING OF THE BINDER COURSE VARIES AT THE LOCATIONS WHERE LANE SHIFTS ARE SHOWN ON THE STAGING AND TRAFFIC CONTROL PLAN SHEETS. AT THESE LOCATIONS, THE MILLING, PATCHING, AND PLACING OF THE BINDER COURSE SHALL BE COMPLETED AT NIGHT USING NIGHT TIME LANE CLOSURES OF THE STAGE I INSIDE LANE.
4. AFTER THE TEMPORARY CONCRETE BARRIER IS NO LONGER REQUIRED (SEE NOTE 1), THE MILLING, PATCHING, AND PLACING OF THE BINDER COURSE SHALL EXTEND TO 27 FEET FROM THE CENTERLINE DURING THIS STAGE, EXCEPT AS SHOWN ON DETAIL A.

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REVISIONS	
NAME	DATE

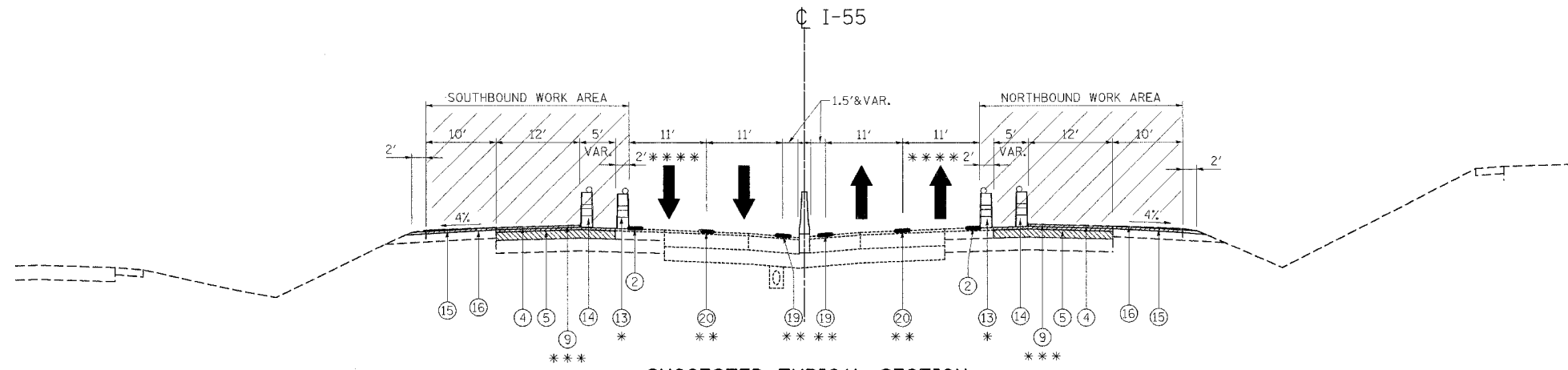
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - TYPICAL SECTIONS  
 STAGE I - 2008**

SCALE: DATE 03/15/07 DRAWN BY: AG CHECKED BY: DDH

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

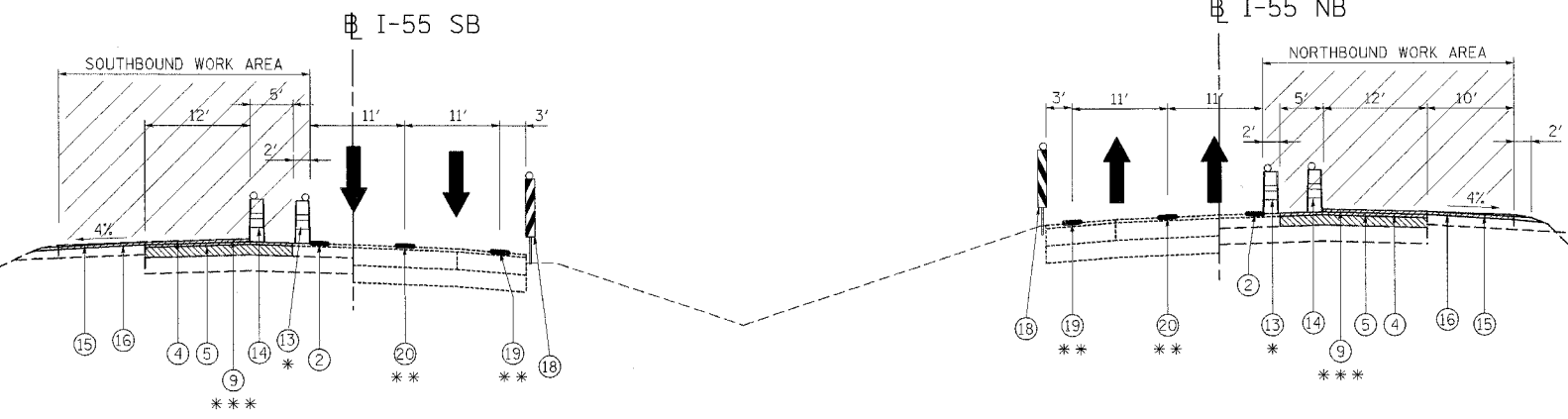
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	115
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**SUGGESTED TYPICAL SECTION  
STAGE II**

STA. 190+15 TO STA. 402+65.07

- \* SEE NOTE 1
- \*\* SEE NOTE 2 (APPLIED AT THE END OF STAGE I - 2008 CONSTRUCTION)
- \*\*\* SEE NOTE 3
- \*\*\*\* SEE NOTE 4



**SUGGESTED TYPICAL SECTION  
STAGE II**

STA. 186+03 TO STA. 190+15 SOUTHBOUND  
STA. 187+28 TO STA. 190+15 NORTHBOUND

- \* SEE NOTE 1
- \*\* SEE NOTE 2 (APPLIED AT THE END OF STAGE I - 2008 CONSTRUCTION)
- \*\*\* SEE NOTE 3

**LEGEND:**

- ① TEMPORARY ASPHALT WEDGE
- ② PAVEMENT MARKING TAPE, TYPE III 4"
- ③ PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH
- ④ BITUMINOUS SURFACE REMOVAL, 3 1/2"
- ⑤ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA 1 3/4"
- ⑥ AGGREGATE SUBGRADE, 12"
- ⑦ HOT-MIX ASPHALT BASE COURSE
- ⑧ TRENCH BACKFILL
- ⑨ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA 1 3/4"
- ⑩ STORM SEWER
- ⑪ BARRIER BASE
- ⑫ CONCRETE BARRIER, DOUBLE FACE
- ⑬ DRUMS W/ STEADY BURN LIGHTS @ 100' C-C
- ⑭ CHECK DRUM W/ STEADY BURN LIGHTS @ 1000' C-C
- ⑮ BITUMINOUS SURFACE REMOVAL (VARIABLE DEPTH)
- ⑯ HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70, 2"
- ⑰ TEMPORARY CONCRETE BARRIER
- ⑱ VERTICAL PANEL W/ STEADY BURN LIGHTS @ 100' C-C IN CURVES AND 200' C-C ON TANGENTS
- ⑲ EPOXY PAVEMENT MARKING - LINE 4"
- ⑳ EPOXY PAVEMENT MARKING - LINE 5" SKIP-DASH
- ㉑ PERMANENT THERMOPLASTIC PAVEMENT MARKING - LINE 4"

- PAVEMENT PATCHING (AS DIRECTED BY THE ENGINEER)
- BITUMINOUS SURFACE REMOVAL AND RESURFACING
- WORK AREA
- TRAFFIC FLOW

**NOTES:**

1. DRUMS SHALL NOT BE PLACED IN THE LANE WHICH IS OPEN TO TRAFFIC. DURING MILLING AND PAVING OPERATIONS, THE CONTRACTOR SHALL USE ONE FOOT WIDE VERTICAL BARRICADES WITH STEADY BURN LIGHTS ALONG THE MILLING AND PAVING OPERATIONS. DRUMS WITH STEADY BURN MONO-DIRECTIONAL LIGHTS SHALL BE USED TO DELINEATE ALL OPEN RAMPS THROUGH THE WORK AREA.
2. WITHIN THE PROJECT LIMITS OF THIS CONTRACT, FULL STANDARD PAVEMENT MARKINGS USING EPOXY PAVEMENT MARKING LINES SHALL BE APPLIED TO THE BINDER COURSE FOR THE LINES SHOWN. THE CONTRACTOR MAY ELECT TO USE EXTRUDED THERMOPLASTIC, PLACED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, IN LIEU OF EPOXY PAVEMENT MARKING AT NO ADDITIONAL COST TO THE CONTRACT. SOUTH OF THE PROJECT LIMITS, PAVEMENT MARKING TAPE, TYPE III SHALL BE APPLIED TO THE CRC PAVEMENT.
3. CONSTRUCT SURFACE COURSE ON THE OUTSIDE SHOULDER AND OUTSIDE LANE (LANE 3) ONLY DURING THIS STAGE.
4. THE DISTANCE BETWEEN THE LANE LINE AND THE EDGE OF THE MILLING, PATCHING, AND PLACING OF THE BINDER COURSE VARIES AT THE LOCATIONS WHERE LANE SHIFTS ARE SHOWN ON THE STAGING AND TRAFFIC CONTROL PLAN SHEETS. AT THESE LOCATIONS, THE MILLING, PATCHING, AND PLACING OF THE BINDER COURSE SHALL BE COMPLETED AT NIGHT USING NIGHT TIME LANE CLOSURES OF THE STAGE II INSIDE LANE. THE MILLING, PATCHING, AND PLACING OF THE BINDER COURSE SHALL EXTEND TO 27 FEET FROM THE CENTERLINE DURING THIS STAGE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
FAI 55 - TYPICAL SECTIONS  
STAGE II**

SCALE: DATE 04/20/07 DRAWN BY AG CHECKED BY DDH

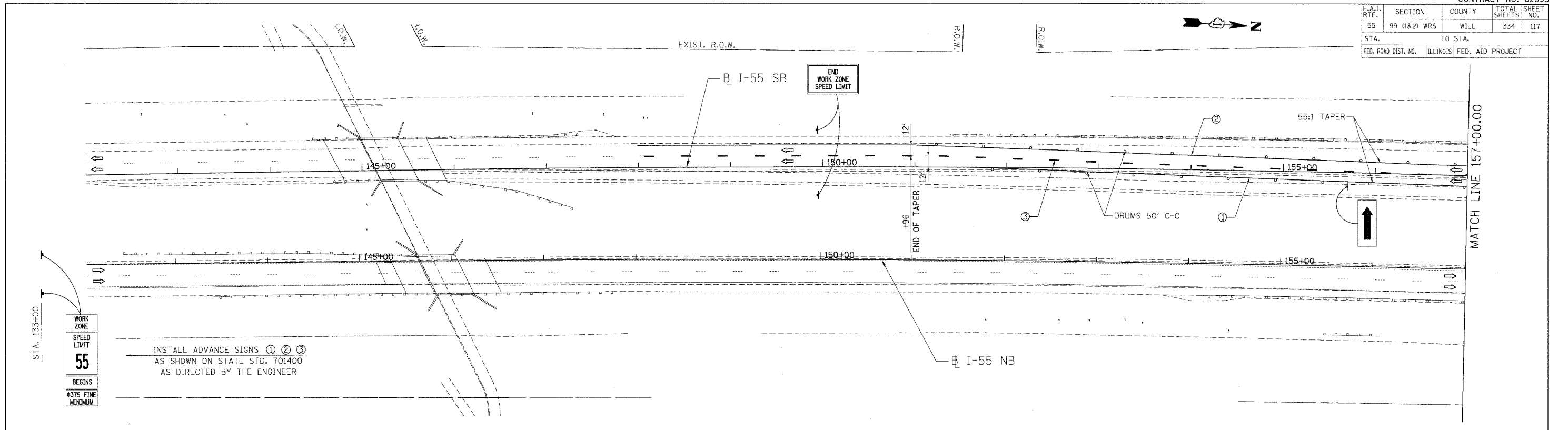
**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE = 04/20/07  
 FILE NAME = 042007\_143615  
 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = DDH

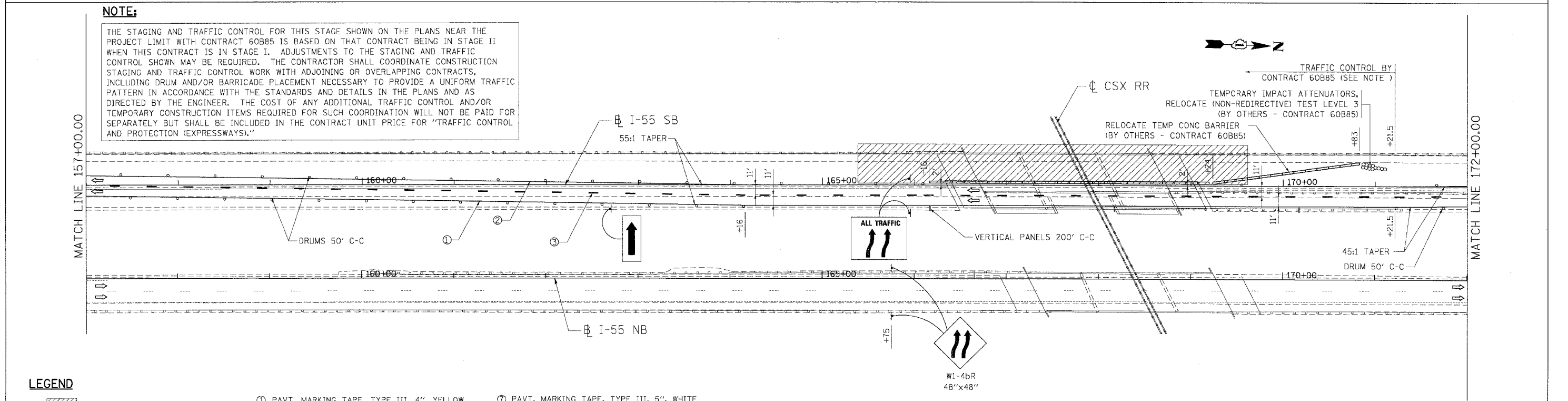




F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	117
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**NOTE:**  
 THE STAGING AND TRAFFIC CONTROL FOR THIS STAGE SHOWN ON THE PLANS NEAR THE PROJECT LIMIT WITH CONTRACT 60B85 IS BASED ON THAT CONTRACT BEING IN STAGE II WHEN THIS CONTRACT IS IN STAGE I. ADJUSTMENTS TO THE STAGING AND TRAFFIC CONTROL SHOWN MAY BE REQUIRED. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION STAGING AND TRAFFIC CONTROL WORK WITH ADJOINING OR OVERLAPPING CONTRACTS, INCLUDING DRUM AND/OR BARRICADE PLACEMENT NECESSARY TO PROVIDE A UNIFORM TRAFFIC PATTERN IN ACCORDANCE WITH THE STANDARDS AND DETAILS IN THE PLANS AND AS DIRECTED BY THE ENGINEER. THE COST OF ANY ADDITIONAL TRAFFIC CONTROL AND/OR TEMPORARY CONSTRUCTION ITEMS REQUIRED FOR SUCH COORDINATION WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)."



- LEGEND**
- WORK AREA
  - DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT
  - VERTICAL PANEL
  - TEMPORARY CONC. BARRIER
  - TRAFFIC FLOW
  - BARRICADE, TYPE III WITH TWO FLASHING LIGHTS
  - ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW
  - ② PAVT. MARKING TAPE, TYPE III, 4", WHITE
  - ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP)
  - ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)
  - ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE
  - ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE
  - ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE
  - ⑧ EPOXY PAVT. MARKING LINE 4", WHITE
  - ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW
  - ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10" DASH 30' SKIP)
  - ⑪ EPOXY PAVT. MARKING LINE 5", WHITE

**NOTE:**  
 AFTER THE CONSTRUCTION OF THE CONCRETE BARRIER AND THE BASE COURSE OF NEW INSIDE LANES AND SHOULDERS IS COMPLETE, THE CONTRACTOR SHALL REMOVE THE TEMPORARY CONCRETE BARRIER AND REPLACE IT WITH DRUMS WITH STEADY BURN LIGHTS SPACED AS SHOWN ON THE PLANS. AFTER THE TEMPORARY CONCRETE BARRIER IS REPLACED WITH DRUMS, EMERGENCY PULLOUTS SHALL CONTINUE TO BE PROVIDED APPROXIMATELY EVERY MILE AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE DETAIL FOR EMERGENCY PULLOUT USING DRUMS IN LIEU OF THE TEMPORARY CONCRETE BARRIER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - STAGE I  
 STA. 142+00.00 TO STA. 172+00.00**

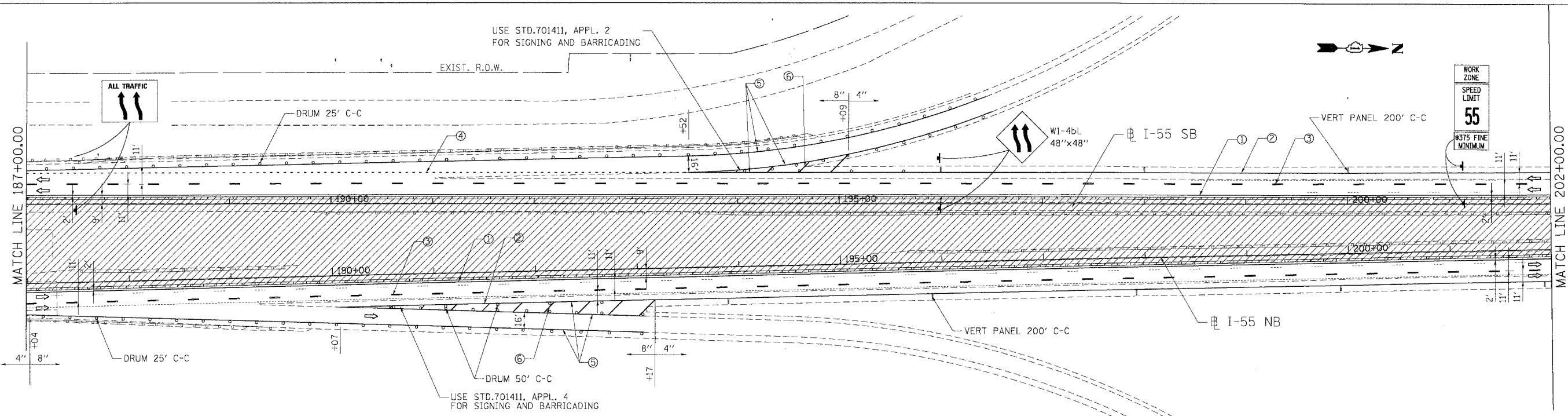
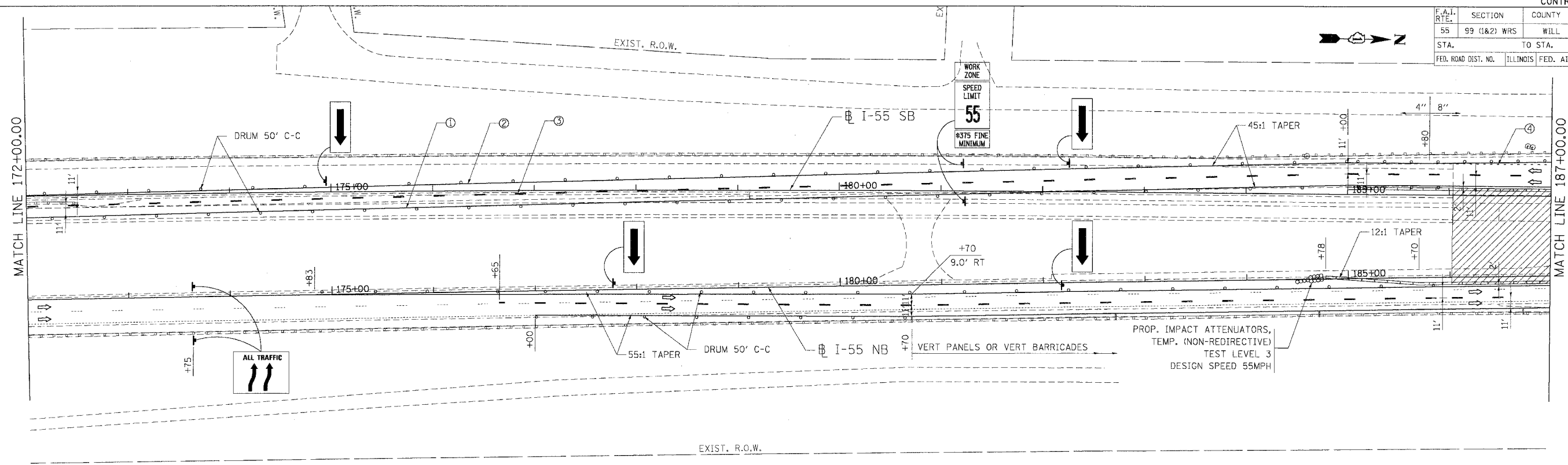
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 DATE: 03/15/07  
 DRAWN BY: AG  
 CHECKED BY: DDH

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 03/15/07  
 FILE NAME = I:\PROJECTS\62895\DRAWINGS\62895-99-117.DWG  
 USER NAME = JGARDNER  
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PLOT DATE: 04/07/07  
 FILE NAME: 047118.dwg  
 PLOT SCALE: 1/8"=1'-0"  
 USER NAME: JACOB

CONTRACT NO. 62895			
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS
55	99 (I&2) WRS	WILL	334
STA.	TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



**LEGEND**

- WORK AREA
- DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT
- VERTICAL PANEL
- TEMPORARY CONC. BARRIER
- TRAFFIC FLOW
- BARRICADE, TYPE III WITH TWO FLASHING LIGHTS
- ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW
- ② PAVT. MARKING TAPE, TYPE III, 4", WHITE
- ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP)
- ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)
- ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE
- ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE
- ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE
- ⑧ EPOXY PAVT. MARKING LINE 4", WHITE
- ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW
- ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10' DASH 30' SKIP)
- ⑪ EPOXY PAVT. MARKING LINE 5", WHITE

**NOTE:**

AFTER THE CONSTRUCTION OF THE CONCRETE BARRIER AND THE BASE COURSE OF NEW INSIDE LANES AND SHOULDERS IS COMPLETE, THE CONTRACTOR SHALL REMOVE THE TEMPORARY CONCRETE BARRIER AND REPLACE IT WITH DRUMS WITH STEADY BURN LIGHTS SPACED AS SHOWN ON THE PLANS. AFTER THE TEMPORARY CONCRETE BARRIER IS REPLACED WITH DRUMS, EMERGENCY PULLOUTS SHALL CONTINUE TO BE PROVIDED APPROXIMATELY EVERY MILE AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE DETAIL FOR EMERGENCY PULLOUT USING DRUMS IN LIEU OF THE TEMPORARY CONCRETE BARRIER.

REVISIONS	
NAME	DATE

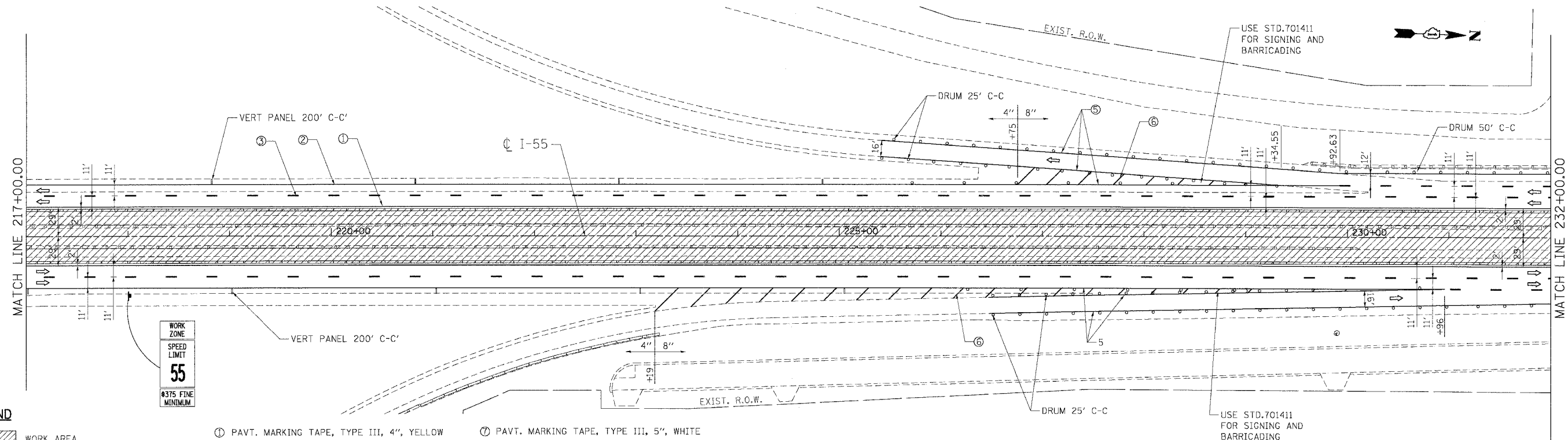
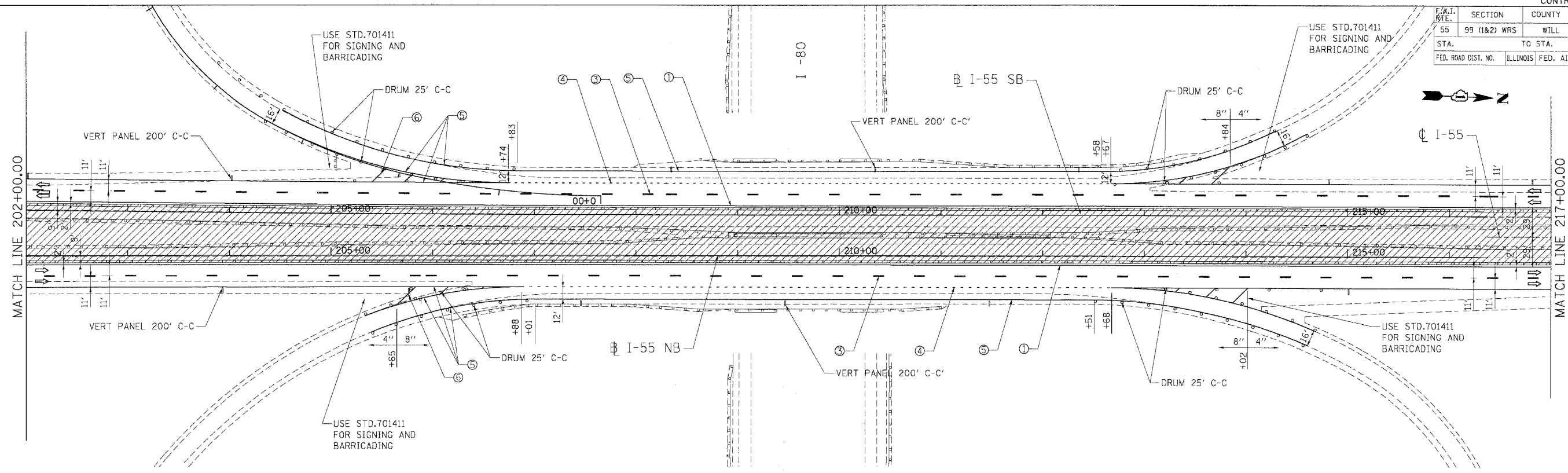
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - STAGE I  
 STA. 172+00.00 TO STA. 202+00.00**

SCALE: 1"=50'  
 DATE: 03/15/07  
 DRAWN BY: AG  
 CHECKED BY: DDH

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

F.A.I. SITE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
95	99 (I&2) WRS	WILL	334	119
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



WORK ZONE  
SPEED LIMIT  
**55**  
#375 FINE MINIMUM

**LEGEND**

- WORK AREA
- DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT
- VERTICAL PANEL
- TEMPORARY CONC. BARRIER
- TRAFFIC FLOW
- BARRICADE, TYPE III WITH TWO FLASHING LIGHTS
- ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW
- ② PAVT. MARKING TAPE, TYPE III, 4", WHITE
- ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10" DASH 30' SKIP)
- ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)
- ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE
- ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE
- ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE
- ⑧ EPOXY PAVT. MARKING LINE 4", WHITE
- ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW
- ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10" DASH 30' SKIP)
- ⑪ EPOXY PAVT. MARKING LINE 5", WHITE

**NOTE:**

AFTER THE CONSTRUCTION OF THE CONCRETE BARRIER AND THE BASE COURSE OF NEW INSIDE LANES AND SHOULDERS IS COMPLETE, THE CONTRACTOR SHALL REMOVE THE TEMPORARY CONCRETE BARRIER AND REPLACE IT WITH DRUMS WITH STEADY BURN LIGHTS SPACED AS SHOWN ON THE PLANS. AFTER THE TEMPORARY CONCRETE BARRIER IS REPLACED WITH DRUMS, EMERGENCY PULLOUTS SHALL CONTINUE TO BE PROVIDED APPROXIMATELY EVERY MILE AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE DETAIL FOR EMERGENCY PULLOUT USING DRUMS IN LIEU OF THE TEMPORARY CONCRETE BARRIER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

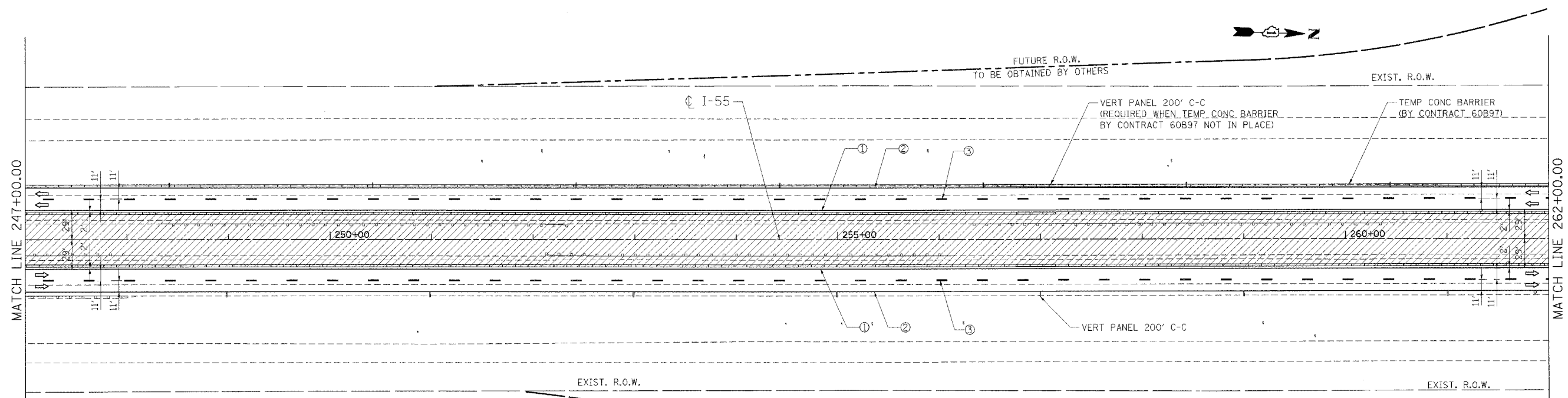
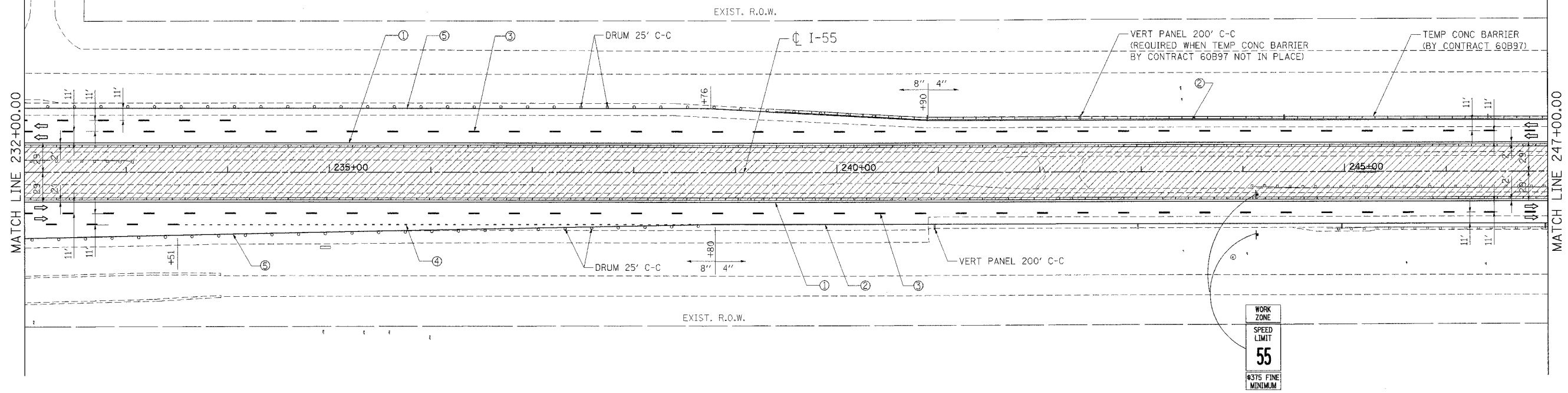
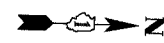
**STAGING AND TRAFFIC CONTROL  
FAI 55 - STAGE I  
STA. 202+00.00 TO STA. 232+00.00**

SCALE: 1"=50'  
DATE 03/15/07  
DRAWN BY AG  
CHECKED BY DDH

**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE: 03/15/07  
 FILE NAME: I:\PROJECTS\62895\DRAWINGS\PLAN\62895-119.DWG  
 PLOT SCALE: 1"=50'  
 USER NAME: DDH

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	120
STA. TO STA.		ILLINOIS FED. AID PROJECT		



**LEGEND**

- WORK AREA
- DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT
- VERTICAL PANEL
- TEMPORARY CONC. BARRIER
- TRAFFIC FLOW
- BARRICADE, TYPE III WITH TWO FLASHING LIGHTS
- ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW
- ② PAVT. MARKING TAPE, TYPE III, 4", WHITE
- ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP)
- ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)
- ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE
- ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE
- ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE
- ⑧ EPOXY PAVT. MARKING LINE 4", WHITE
- ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW
- ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10' DASH 30' SKIP)
- ⑪ EPOXY PAVT. MARKING LINE 5", WHITE

**NOTE:**

AFTER THE CONSTRUCTION OF THE CONCRETE BARRIER AND THE BASE COURSE OF NEW INSIDE LANES AND SHOULDERS IS COMPLETE, THE CONTRACTOR SHALL REMOVE THE TEMPORARY CONCRETE BARRIER AND REPLACE IT WITH DRUMS WITH STEADY BURN LIGHTS SPACED AS SHOWN ON THE PLANS. AFTER THE TEMPORARY CONCRETE BARRIER IS REPLACED WITH DRUMS, EMERGENCY PULLOUTS SHALL CONTINUE TO BE PROVIDED APPROXIMATELY EVERY MILE AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE DETAIL FOR EMERGENCY PULLOUT USING DRUMS IN LIEU OF THE TEMPORARY CONCRETE BARRIER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - STAGE I  
 STA. 232+00.00 TO STA. 262+00.00**

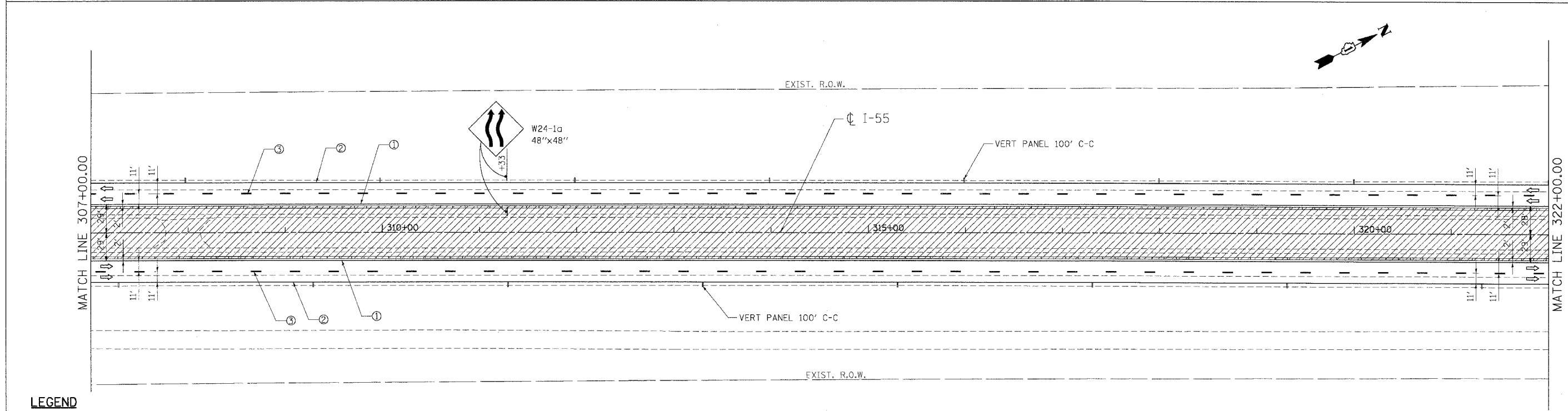
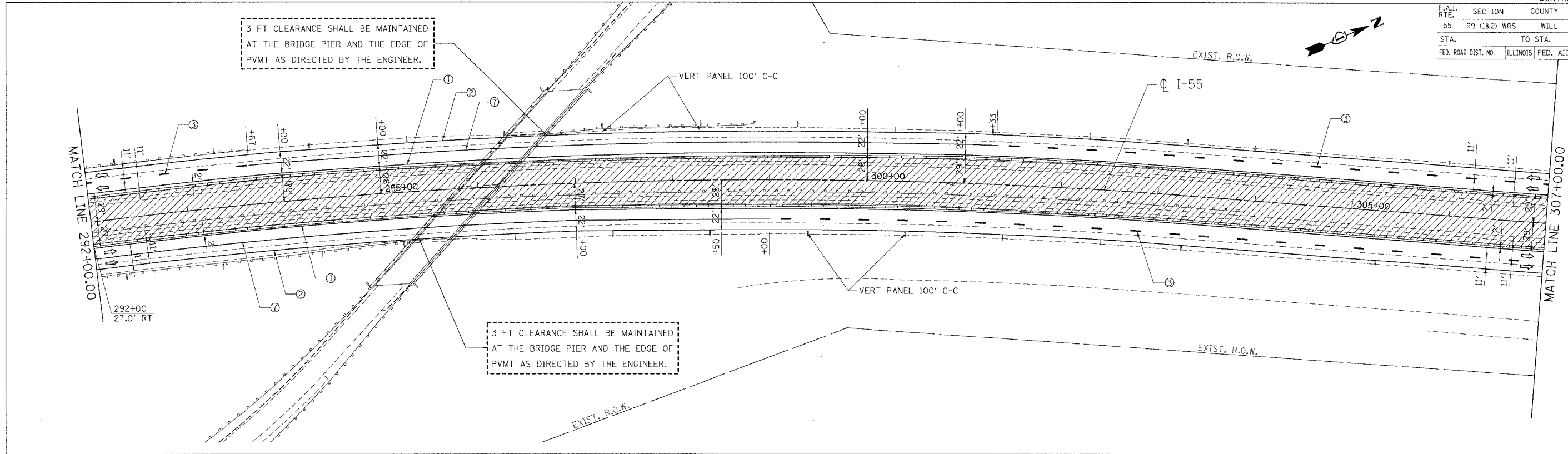
SCALE: 1"=50'      DRAWN BY AG  
 DATE 04/20/07      CHECKED BY DDH

**TENG**      TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 04/20/07    PLOT SCALE = 1"=50'    USER NAME = JUBERT  
 FILE NAME = STAGE I    PLOT SCALE = 1"=50'    USER NAME = JUBERT  
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 VAL91A001.DGN    YR05A011.DGN    YR05A012.DGN    YR05A013.DGN    YR05A014.DGN    YR05A015.DGN    YR05A016.DGN    YR05A017.DGN    YR05A018.DGN    YR05A019.DGN  
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	122
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**LEGEND**

- WORK AREA
- DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT
- VERTICAL PANEL
- TEMPORARY CONC. BARRIER
- TRAFFIC FLOW
- BARRICADE, TYPE III WITH TWO FLASHING LIGHTS
- PAVT. MARKING TAPE, TYPE III, 4", YELLOW
- PAVT. MARKING TAPE, TYPE III, 4", WHITE
- PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP)
- PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)
- PAVT. MARKING TAPE, TYPE III, 8", WHITE
- PAVT. MARKING TAPE, TYPE III, 12", WHITE
- PAVT. MARKING TAPE, TYPE III, 5", WHITE
- EPOXY PAVT. MARKING LINE 4", WHITE
- EPOXY PAVT. MARKING LINE 4", YELLOW
- EPOXY PAVT. MARKING LINE 5", WHITE (10" DASH 30' SKIP)
- EPOXY PAVT. MARKING LINE 5", WHITE

**NOTE:**

AFTER THE CONSTRUCTION OF THE CONCRETE BARRIER AND THE BASE COURSE OF NEW INSIDE LANES AND SHOULDERS IS COMPLETE, THE CONTRACTOR SHALL REMOVE THE TEMPORARY CONCRETE BARRIER AND REPLACE IT WITH DRUMS WITH STEADY BURN LIGHTS SPACED AS SHOWN ON THE PLANS. AFTER THE TEMPORARY CONCRETE BARRIER IS REPLACED WITH DRUMS, EMERGENCY PULLOUTS SHALL CONTINUE TO BE PROVIDED APPROXIMATELY EVERY MILE AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE DETAIL FOR EMERGENCY PULLOUT USING DRUMS IN LIEU OF THE TEMPORARY CONCRETE BARRIER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL**  
 FAI 55 - STAGE I  
 STA. 292+00.00 TO STA. 322+00.00

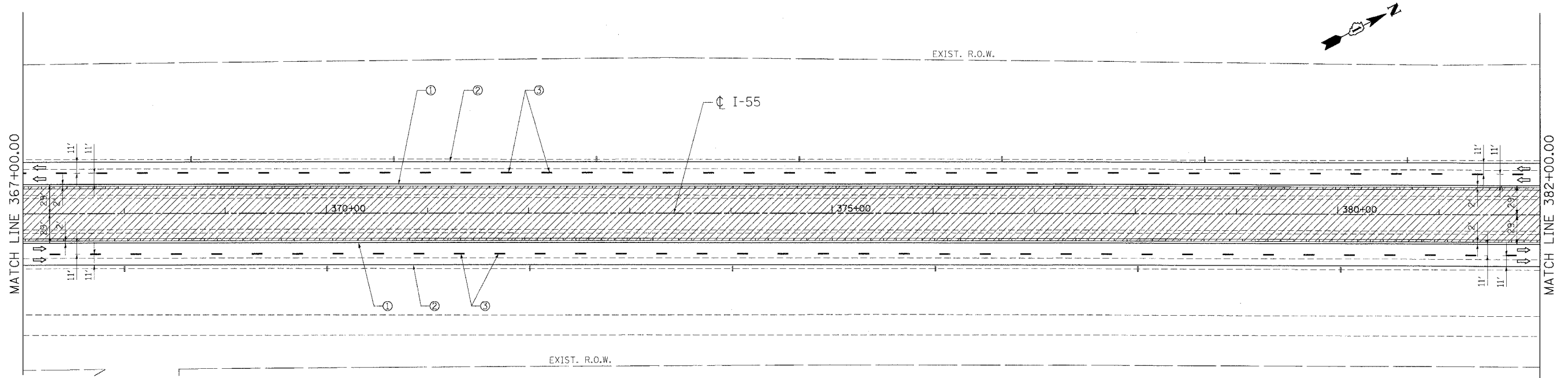
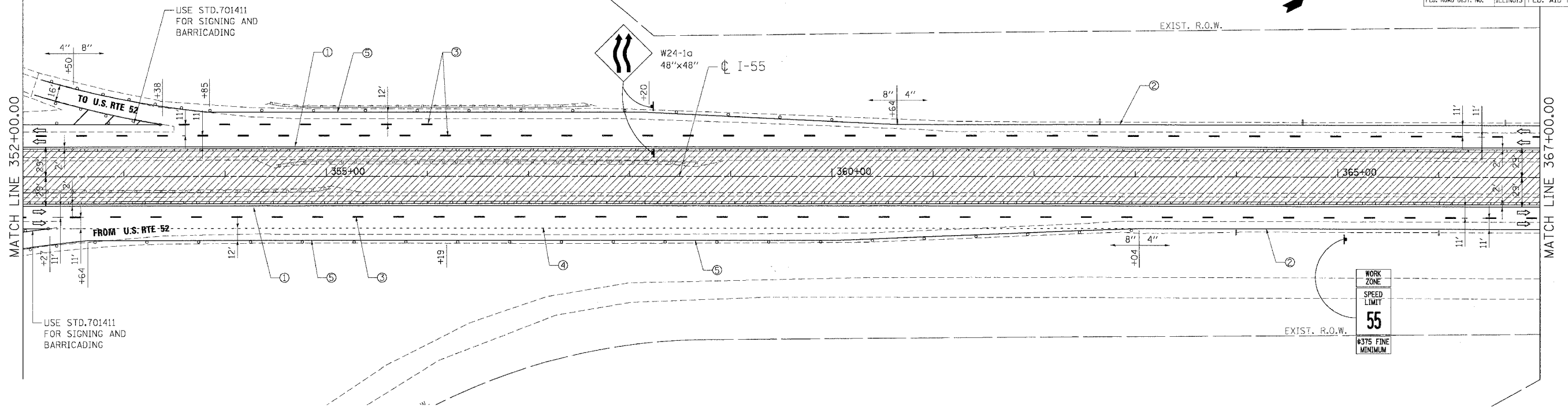
SCALE: 1"=50'  
 DATE: 03/15/07  
 DRAWN BY: AG  
 CHECKED BY: DDH

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 03/15/07  
 FILE NAME = 031507.DWG  
 USER NAME = MUSEBIA



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	124
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**LEGEND**

- |  |   |  |
|--|---|--|
| WORK AREA                                    | ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW                    | ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE                |
| DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT  | ② PAVT. MARKING TAPE, TYPE III, 4", WHITE                     | ⑧ EPOXY PAVT. MARKING LINE 4", WHITE                     |
| VERTICAL PANEL                               | ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP) | ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW                    |
| TEMPORARY CONC. BARRIER                      | ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)   | ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10' DASH 30' SKIP) |
| TRAFFIC FLOW                                 | ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE                     | ⑪ EPOXY PAVT. MARKING LINE 5", WHITE                     |
| BARRICADE, TYPE III WITH TWO FLASHING LIGHTS | ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE                    |  |

**NOTE:**

AFTER THE CONSTRUCTION OF THE CONCRETE BARRIER AND THE BASE COURSE OF NEW INSIDE LANES AND SHOULDERS IS COMPLETE, THE CONTRACTOR SHALL REMOVE THE TEMPORARY CONCRETE BARRIER AND REPLACE IT WITH DRUMS WITH STEADY BURN LIGHTS SPACED AS SHOWN ON THE PLANS. AFTER THE TEMPORARY CONCRETE BARRIER IS REPLACED WITH DRUMS, EMERGENCY PULLOUTS SHALL CONTINUE TO BE PROVIDED APPROXIMATELY EVERY MILE AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE DETAIL FOR EMERGENCY PULLOUT USING DRUMS IN LIEU OF THE TEMPORARY CONCRETE BARRIER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - STAGE I  
 STA. 352+00.00 TO STA. 382+00.00**

SCALE: 1"=50'  
 DATE: 03/15/07

DRAWN BY AG  
 CHECKED BY DDH

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

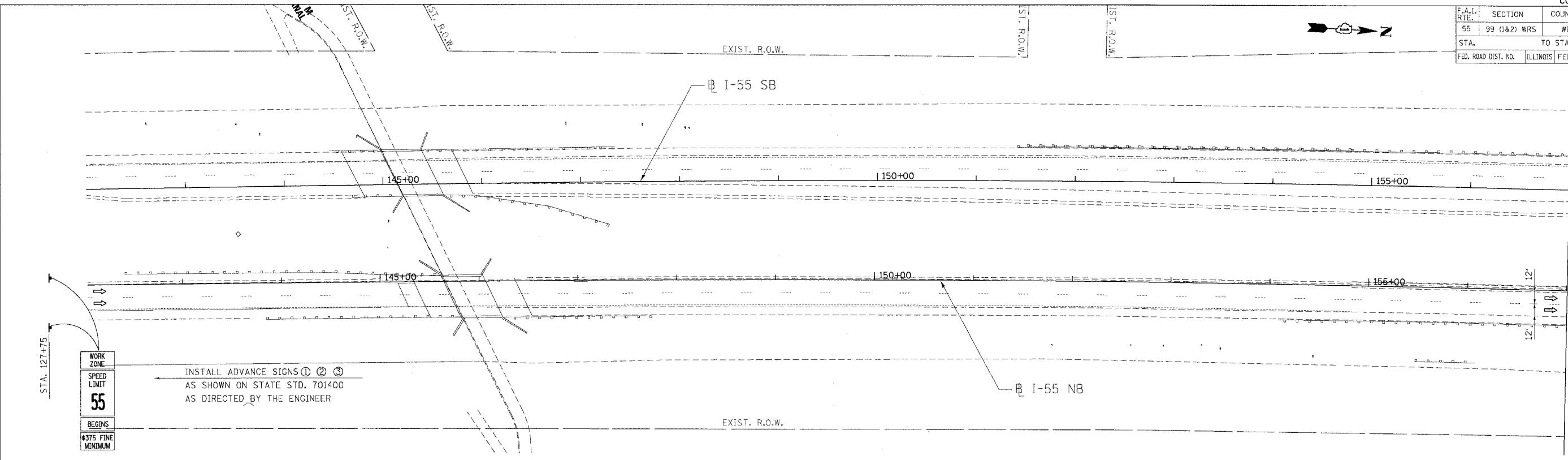
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 FILE NAME = 031507.DWG  
 USER NAME = 031507

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	126
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



WORK ZONE

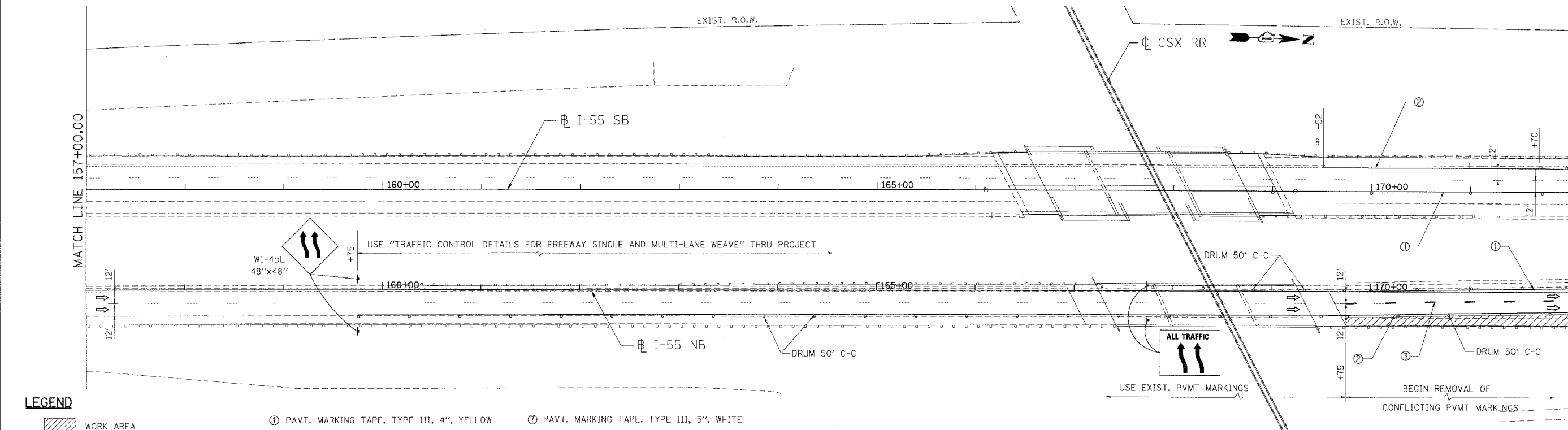
SPEED LIMIT

**55**

BEGINS

#375 FINE MINIMUM

INSTALL ADVANCE SIGNS ① ② ③ AS SHOWN ON STATE STD. 701400 AS DIRECTED BY THE ENGINEER



**LEGEND**

- |  |   |  |
|--|---|--|
| WORK AREA                                    | ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW                    | ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE                |
| DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT  | ② PAVT. MARKING TAPE, TYPE III, 4", WHITE                     | ⑧ EPOXY PAVT. MARKING LINE 4", WHITE                     |
| VERTICAL PANEL                               | ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP) | ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW                    |
| TEMPORARY CONC. BARRIER                      | ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)   | ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10" DASH 30' SKIP) |
| TRAFFIC FLOW                                 | ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE                     | ⑪ EPOXY PAVT. MARKING LINE 5", WHITE                     |
| BARRICADE, TYPE III WITH TWO FLASHING LIGHTS | ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE                    |  |

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
FAI 55 - STAGE II  
STA. 142+00.00 TO STA. 172+00.00**

SCALE: 1"=50' DRAWN BY AG  
DATE 03/15/07 CHECKED BY DDH

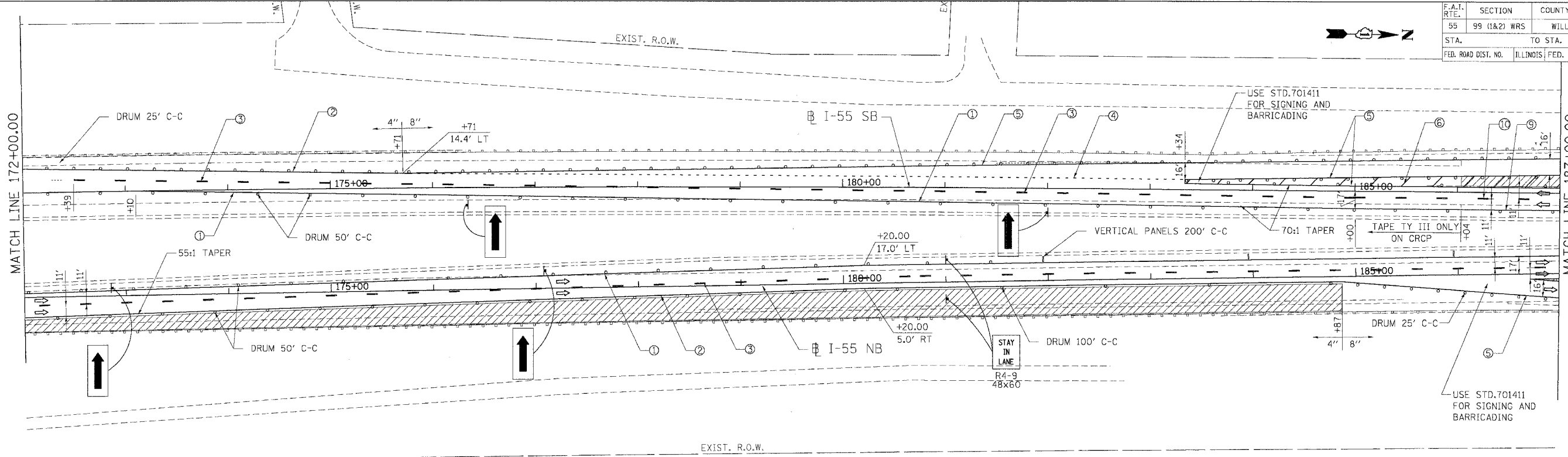
**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE = 03/15/07  
FILE NAME = 031507.DWG  
PLOT SCALE = 1"=50'  
USER NAME = JUBEN

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	127
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

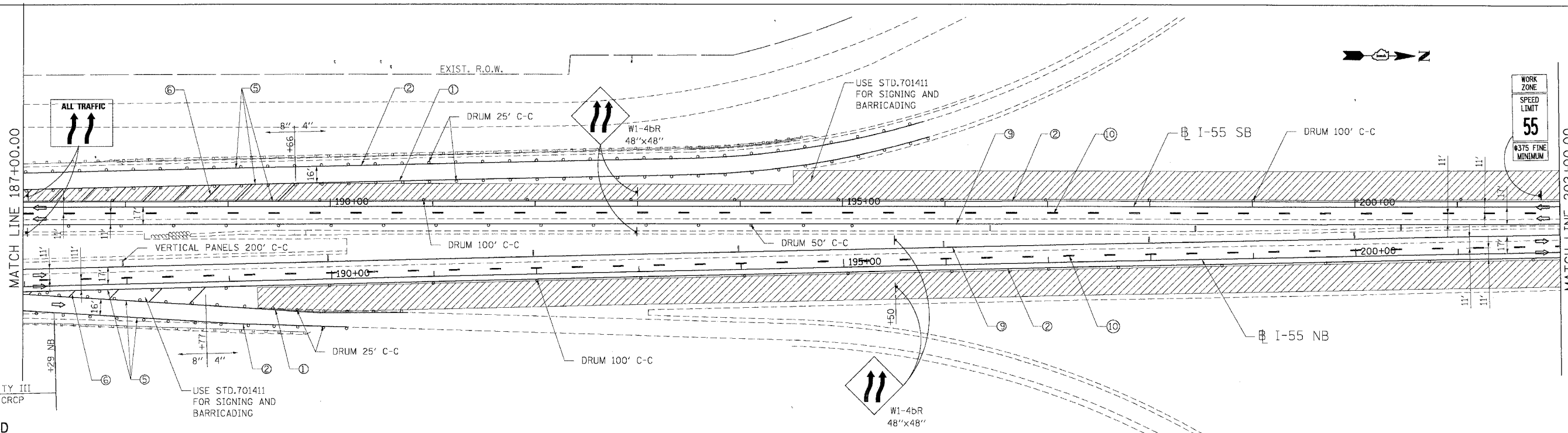
MATCH LINE 172+00.00

MATCH LINE 187+00.00



MATCH LINE 187+00.00

MATCH LINE 202+00.00



**LEGEND**

- WORK AREA
- DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT
- VERTICAL PANEL
- TEMPORARY CONC. BARRIER
- TRAFFIC FLOW
- BARRICADE, TYPE III WITH TWO FLASHING LIGHTS
- ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW
- ② PAVT. MARKING TAPE, TYPE III, 4", WHITE
- ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP)
- ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)
- ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE
- ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE
- ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE
- ⑧ EPOXY PAVT. MARKING LINE 4", WHITE
- ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW
- ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10" DASH 30' SKIP)
- ⑪ EPOXY PAVT. MARKING LINE 5", WHITE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - STAGE II  
 STA. 172+00.00 TO STA. 202+00.00**

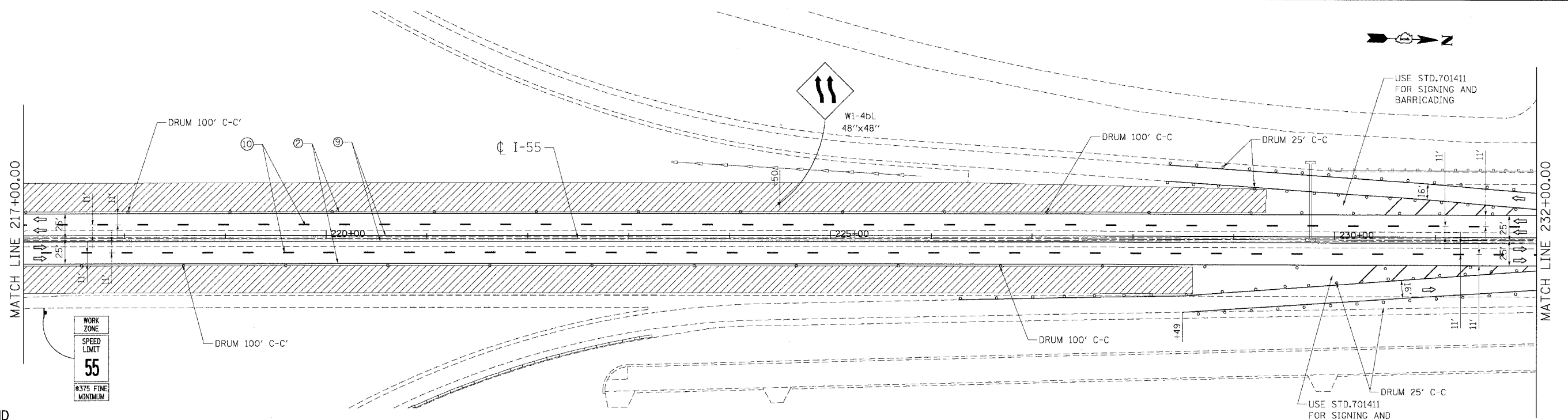
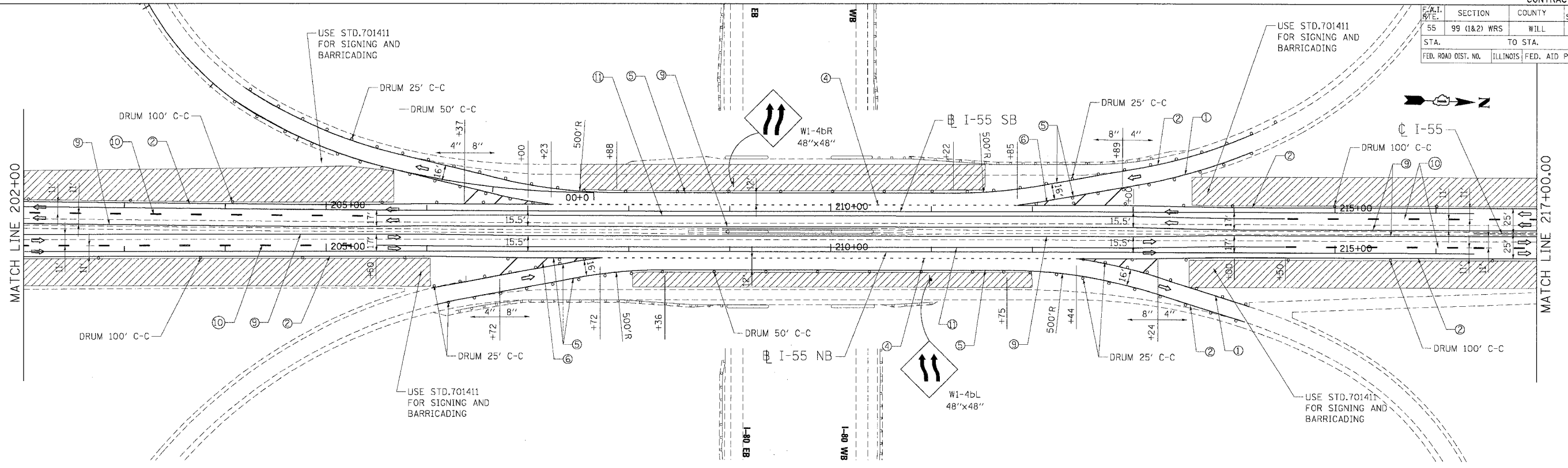
SCALE: 1"=50'  
 DATE: 03/15/07

DRAWN BY AG  
 CHECKED BY DDH

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = #DAYS  
 PLOT SCALE = #SCALE  
 USER NAME = #USER#

F.A.I. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	128
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**LEGEND**

- WORK AREA
- DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT
- VERTICAL PANEL
- TEMPORARY CONC. BARRIER
- TRAFFIC FLOW
- BARRICADE, TYPE III WITH TWO FLASHING LIGHTS
- ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW
- ② PAVT. MARKING TAPE, TYPE III, 4", WHITE
- ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP)
- ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)
- ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE
- ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE
- ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE
- ⑧ EPOXY PAVT. MARKING LINE 4", WHITE
- ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW
- ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10" DASH 30' SKIP)
- ⑪ EPOXY PAVT. MARKING LINE 5", WHITE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - STAGE II  
 STA. 202+00.00 TO STA. 232+00.00**

SCALE: 1"=50'  
 DATE 03/15/07  
 DRAWN BY AG  
 CHECKED BY DDH

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

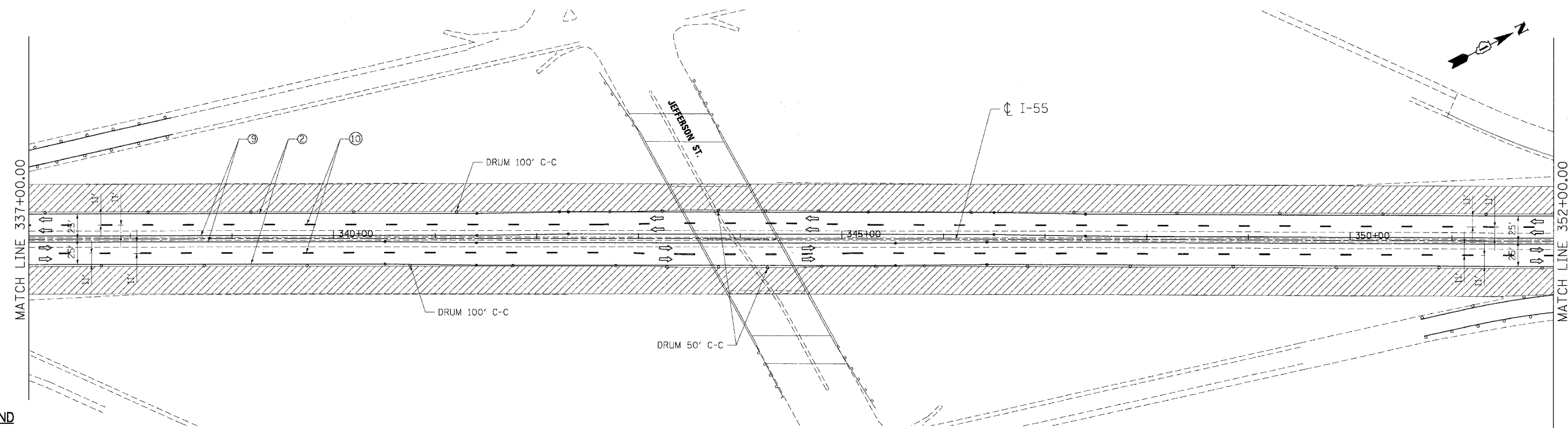
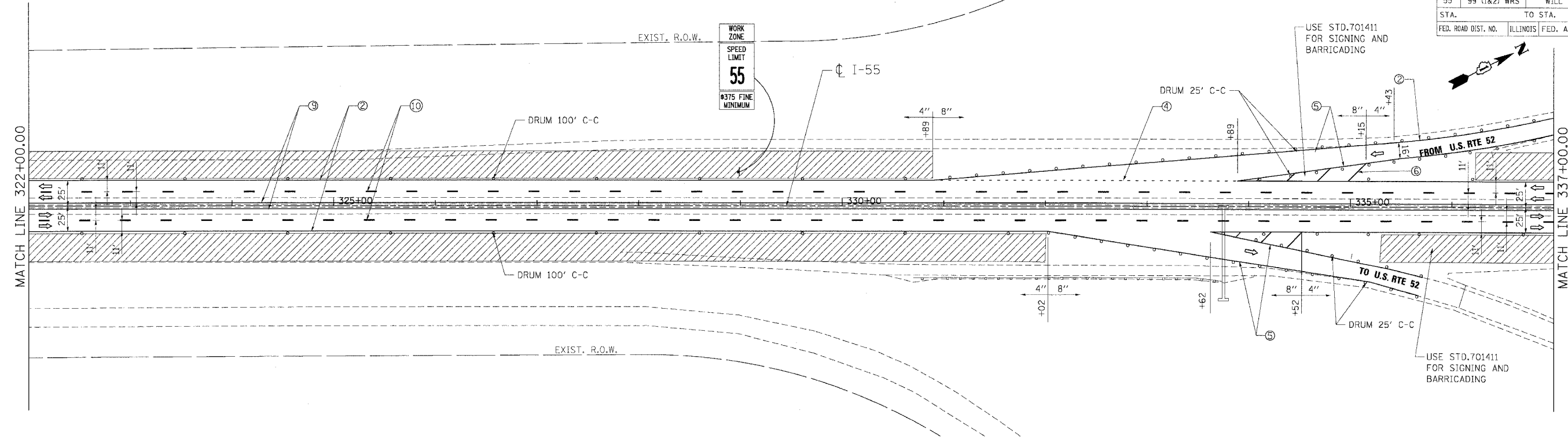
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 USER NAME = DDH







F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	132
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**LEGEND**

- WORK AREA
- DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT
- VERTICAL PANEL
- TEMPORARY CONC. BARRIER
- TRAFFIC FLOW
- BARRICADE, TYPE III WITH TWO FLASHING LIGHTS
- ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW
- ② PAVT. MARKING TAPE, TYPE III, 4", WHITE
- ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP)
- ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)
- ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE
- ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE
- ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE
- ⑧ EPOXY PAVT. MARKING LINE 4", WHITE
- ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW
- ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10" DASH 30' SKIP)
- ⑪ EPOXY PAVT. MARKING LINE 5", WHITE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - STAGE II  
 STA. 322+00.00 TO STA. 352+00.00**

SCALE: 1"=50'  
 DATE 03/15/07  
 DRAWN BY AG  
 CHECKED BY DDH

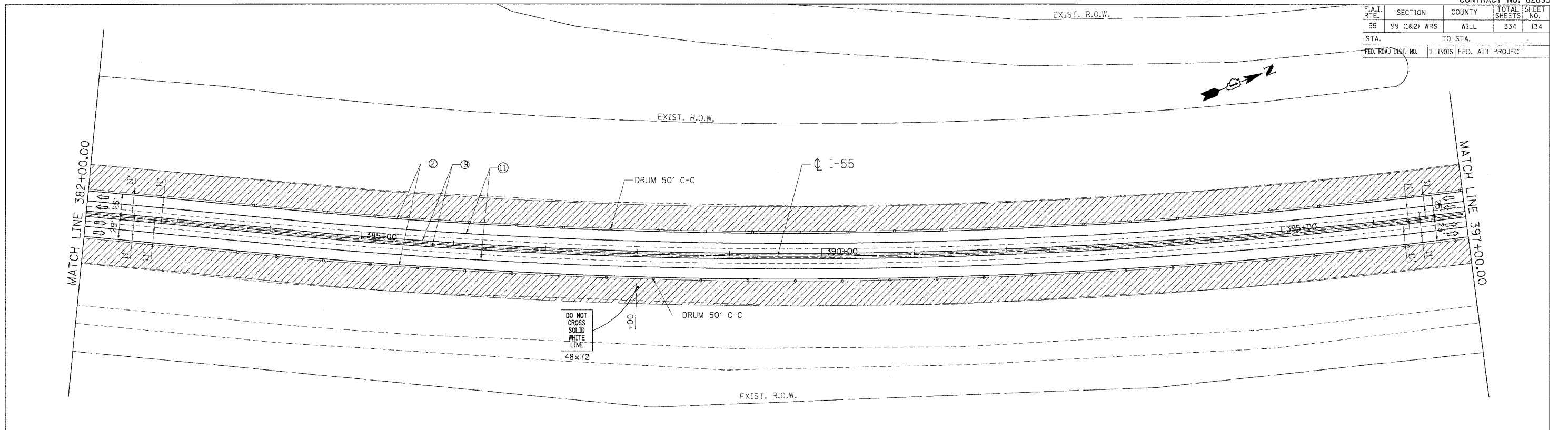
**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLT DATE = #DRTS  
 PLT SCALE = #SCALE  
 USER NAME = #USER



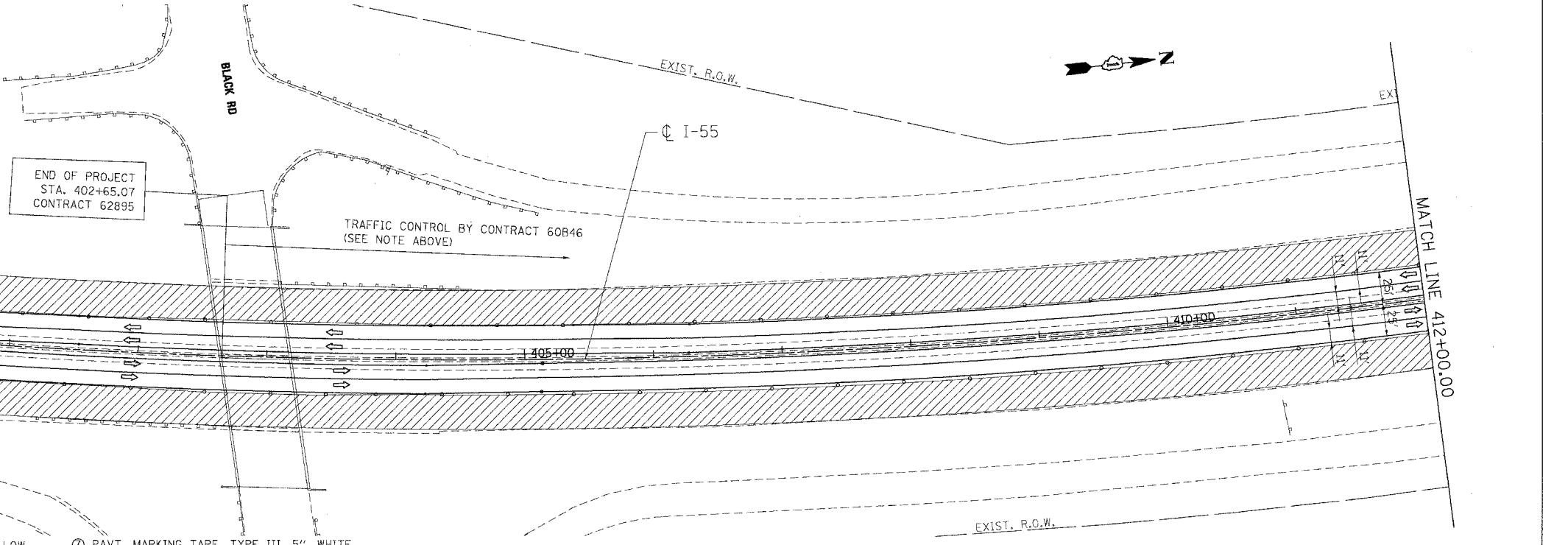


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	134
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**NOTE:**

THE STAGING AND TRAFFIC CONTROL FOR THIS STAGE SHOWN ON THE PLANS NEAR THE PROJECT LIMIT WITH CONTRACT 60B46 IS BASED ON THAT CONTRACT BEING IN STAGE II WHEN THIS CONTRACT IS IN STAGE II. ADJUSTMENTS TO THE STAGING AND TRAFFIC CONTROL SHOWN MAY BE REQUIRED. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION STAGING AND TRAFFIC CONTROL WORK WITH ADJOINING OR OVERLAPPING CONTRACTS, INCLUDING DRUM AND/OR BARRICADE PLACEMENT NECESSARY TO PROVIDE A UNIFORM TRAFFIC PATTERN IN ACCORDANCE WITH THE STANDARDS AND DETAILS IN THE PLANS AND AS DIRECTED BY THE ENGINEER. THE COST OF ANY ADDITIONAL TRAFFIC CONTROL AND/OR TEMPORARY CONSTRUCTION ITEMS REQUIRED FOR SUCH COORDINATION WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)."



**LEGEND**

- |  |   |  |
|--|---|--|
| WORK AREA                                    | ① PAVT. MARKING TAPE, TYPE III, 4", YELLOW                    | ⑦ PAVT. MARKING TAPE, TYPE III, 5", WHITE                |
| DRUMS W/ MONO-DIRECTIONAL STEADY BURN LIGHT  | ② PAVT. MARKING TAPE, TYPE III, 4", WHITE                     | ⑧ EPOXY PAVT. MARKING LINE 4", WHITE                     |
| VERTICAL PANEL                               | ③ PAVT. MARKING TAPE, TYPE III, 5", WHITE (10' DASH 30' SKIP) | ⑨ EPOXY PAVT. MARKING LINE 4", YELLOW                    |
| TEMPORARY CONC. BARRIER                      | ④ PAVT. MARKING TAPE, TYPE III, 4", WHITE (2' DASH 6' SKIP)   | ⑩ EPOXY PAVT. MARKING LINE 5", WHITE (10" DASH 30' SKIP) |
| TRAFFIC FLOW                                 | ⑤ PAVT. MARKING TAPE, TYPE III, 8", WHITE                     | ⑪ EPOXY PAVT. MARKING LINE 5", WHITE                     |
| BARRICADE, TYPE III WITH TWO FLASHING LIGHTS | ⑥ PAVT. MARKING TAPE, TYPE III, 12", WHITE                    |  |

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 FAI 55 - STAGE II  
 STA. 382+00.00 TO STA. 402+65.07**

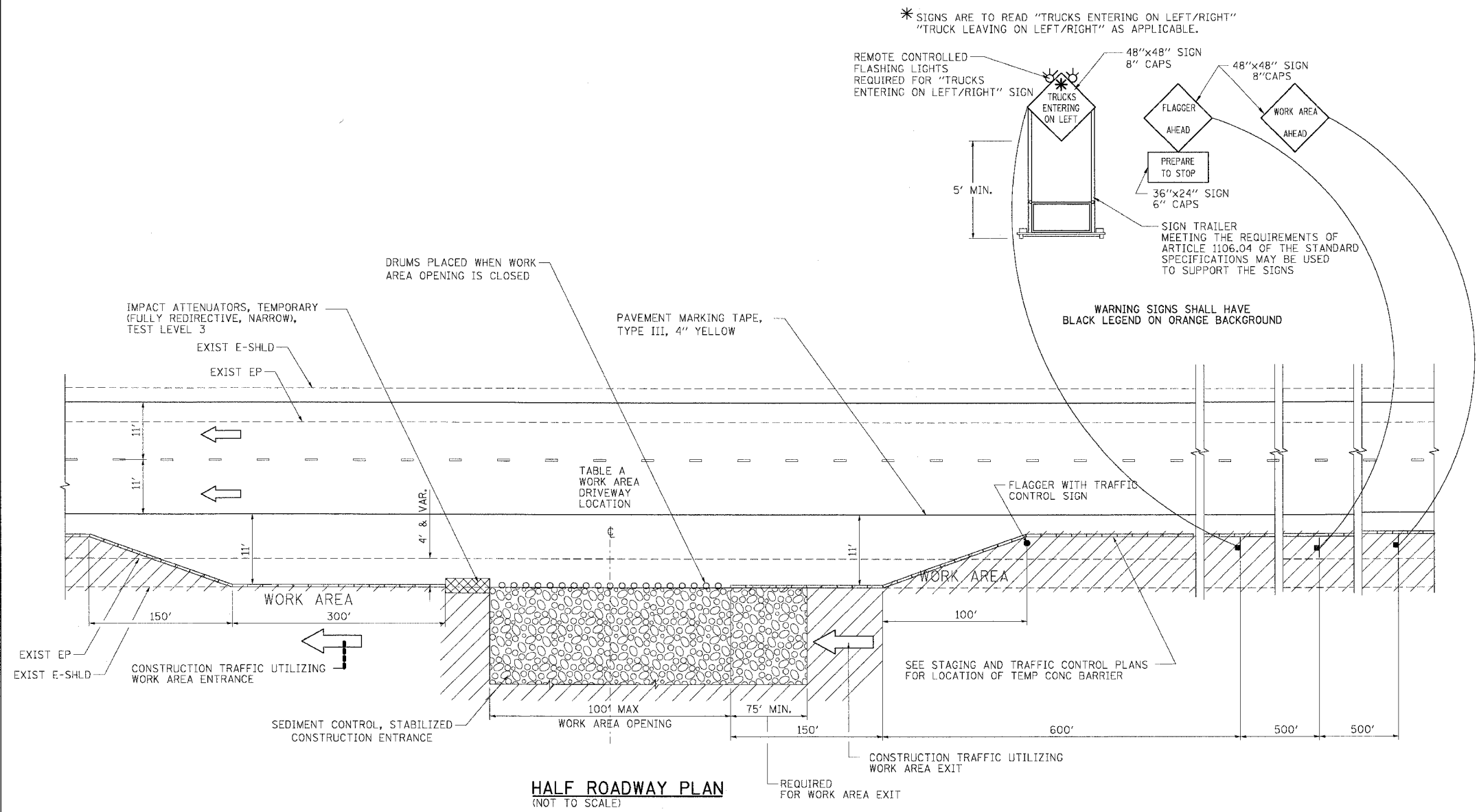
SCALE: 1"=50'  
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 DRAWN BY: AG  
 CHECKED BY: DDH

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 03/15/07  
 FILE NAME = I:\PROJECTS\62895\62895.DWG  
 USER = RUSSELL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	135
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

SUGGESTED SOUTH BOUND STATION	SUGGESTED NORTH BOUND STATION
STA 241+00	STA 237+00
STA 298+00	STA 286+00
STA 359+00	STA 348+00



\* SIGNS ARE TO READ "TRUCKS ENTERING ON LEFT/RIGHT"  
"TRUCK LEAVING ON LEFT/RIGHT" AS APPLICABLE.

WARNING SIGNS SHALL HAVE  
BLACK LEGEND ON ORANGE BACKGROUND

**NOTES**

1. THE "FLAGGER AHEAD" AND "TRUCKS ENTERING OR TRUCKS LEAVING ON LEFT/RIGHT" SIGNS SHALL BE COVERED OR TURNED AWAY FROM TRAFFIC WHEN THE FLAGGING OPERATION CEASES.
2. THE FLASHING LIGHTS SHALL MEET THE REQUIREMENTS OF ARTICLE 702.05(c) AND BE OPERATED BY THE FLAGGER REMOTELY. THE LIGHTS SHALL BE FLASHING ONLY WHEN A VEHICLE IS ENTERING THE EXPRESSWAY.
3. WORK AREA EXIT OPENINGS SHALL BE LOCATED A MINIMUM OF ONE HALF MILE APART. WORK AREA EXIT OPENINGS AND WORK AREA ENTRANCE OPENINGS SHALL BE LOCATED A MINIMUM OF 2,300' APART. WORK AREA OPENINGS MAY BE LOCATED A MINIMUM OF 3,200' BEFORE EMERGENCY PULLOUT LOCATIONS (BASED ON THE DIRECTION OF TRAFFIC) AND A MINIMUM OF 2,100' AFTER EMERGENCY PULLOUT LOCATIONS (BASED ON THE DIRECTION OF TRAFFIC). WORK AREA OPENINGS INTO THE MEDIAN FROM BOTH DIRECTIONS OF TRAFFIC SHALL NOT BE PLACED DIRECTLY ACROSS FROM EACH OTHER. WORK AREA OPENINGS INSTALLED DURING STAGE I CONSTRUCTION MAY BE LOCATED AS SHOWN IN TABLE A. THE WORK AREA OPENING LOCATIONS SHALL BE APPROVED BY THE ENGINEER.
4. ALL CONSTRUCTION VEHICLES SHALL EXIT THE WORK AREA AT A WORK AREA OPENING.
5. ALL CONSTRUCTION VEHICLES SHALL ENTER THE WORK AREA AT A WORK AREA ENTRANCE OPENING AND SHALL USE THEIR TURN SIGNALS TO WARN MOTORISTS.
6. THE CONTRACTOR SHALL PLACE DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHTS, SPACED AT 10' CENTERS, ACROSS THE WORK AREA OPENING WHEN THE WORK AREA OPENING IS CLOSED. THE COSTS OF INSTALLING, MAINTAINING AND REMOVING THESE DRUMS AND SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".
7. WORK AREA OPENINGS SHALL NOT BE SIMULTANEOUSLY USED AS A WORK AREA ENTRANCE AND A WORK AREA EXIT.
8. THE CONTRACTOR SHALL CLOSE THE WORK AREA OPENING WHEN REVISING THE SIGNING FROM AN ENTRANCE TO AN EXIT AND FROM AN EXIT TO AN ENTRANCE.
9. A FLAGGER IS REQUIRED AT ALL TIMES UNLESS THE WORK AREA OPENING IS CLOSED. THE COST OF THE FLAGGER WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".
10. SEE STAGING AND TRAFFIC CONTROL DETAIL FOR EMERGENCY PULLOUT AND EROSION AND SEDIMENT CONTROL DETAIL FOR STABILIZED CONSTRUCTION ENTRANCE.

**LEGEND:**

- FLAGGER WITH TRAFFIC CONTROL SIGN
- ← DIRECTION OF TRAFFIC
- ⊣ SIGN ON SUPPORT
- DRUM W/ MONODIRECTIONAL STEADY BURN LIGHT
- TBT = TRAFFIC BARRIER TERMINAL
- TCB = TEMPORARY CONCRETE BARRIER
- ▨ WORK AREA
- ▩ IMPACT ATTENUATOR

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
DETAIL FOR WORK AREA  
ENTRANCE/EXIT**

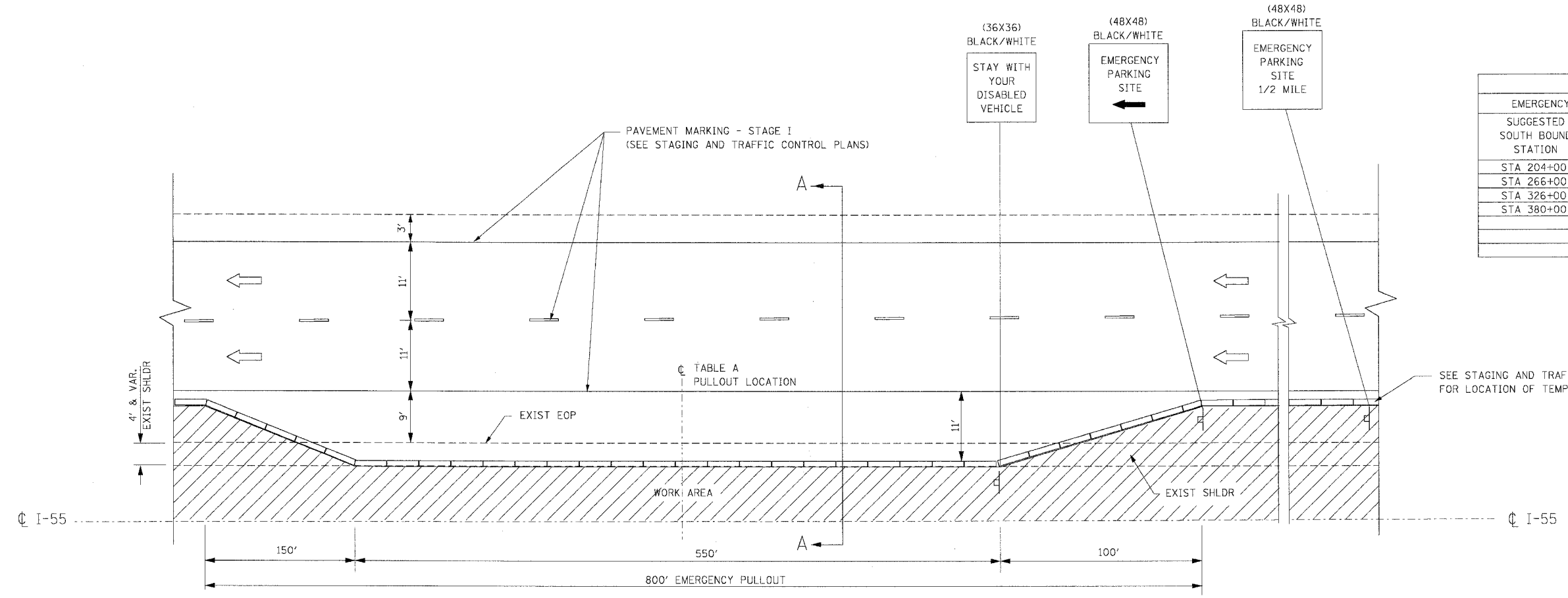
SCALE: N.T.S. DRAWN BY SEB  
DATE 02/23/07 CHECKED BY DDH

**TENG** TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE = 02/23/07  
 FILE NAME = 02/23/07  
 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = JUSERS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	136
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

SUGGESTED SOUTH BOUND STATION	SUGGESTED NORTH BOUND STATION
STA 204+00	STA 216+00
STA 266+00	STA 386+00
STA 326+00	
STA 380+00	



SEE STAGING AND TRAFFIC CONTROL PLANS FOR LOCATION OF TEMPORARY CONCRETE BARRIER

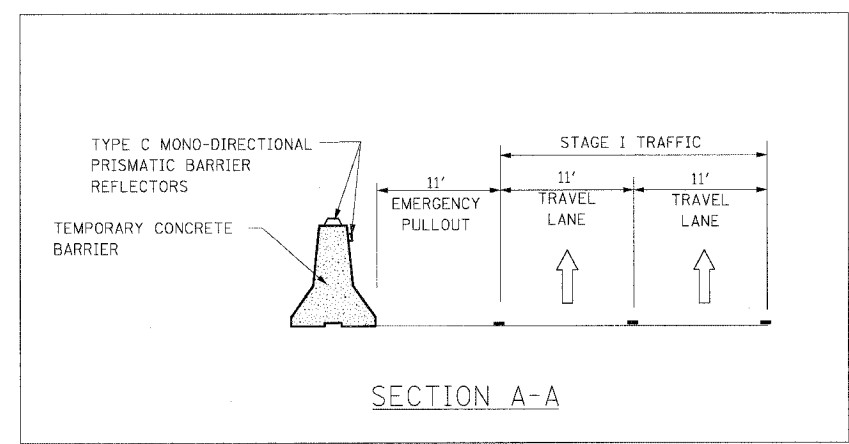
**EMERGENCY PULL OUT  
HALF ROADWAY PLAN  
STAGE I**

**NOTES:**

- EMERGENCY PULLOUTS INSTALLED DURING STAGE I CONSTRUCTION SHALL BE LOCATED AS SPECIFIED IN TABLE A. THE LOCATIONS SHALL BE APPROVED BY THE ENGINEER.
- EMERGENCY PULLOUTS SHALL BE RELOCATED TO AN ADJACENT LOCATION, APPROVED BY THE ENGINEER, TO COMPLETE STAGE I WORK. THE RELOCATION OF THE TEMPORARY CONCRETE BARRIER WILL BE PAID AT THE CONTRACT UNIT PRICE FOR "RELOCATE TEMPORARY BARRIER".
- ALL EMERGENCY PULLOUTS SIGNS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "TEMPORARY INFORMATION SIGNING".
- THE COST OF THE TRAFFIC CONTROL REQUIRED FOR THE INSTALLATION AND RELOCATION OF EMERGENCY PULLOUTS AND THE COST OF RELOCATION OF THE TEMPORARY INFORMATION SIGNING FOR EMERGENCY PULLOUTS WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".

**LEGEND:**

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON SUPPORT
- TEMPORARY CONCRETE BARRIER



PLOT DATE: \* \* \* \* \*  
 FILE NAME: \* \* \* \* \*  
 USER NAME: \* \* \* \* \*  
 USED: EMERGENCY PULLOUT.DWG, A:\9909\9909.DWG  
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NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 DETAIL FOR  
 EMERGENCY PULLOUT**

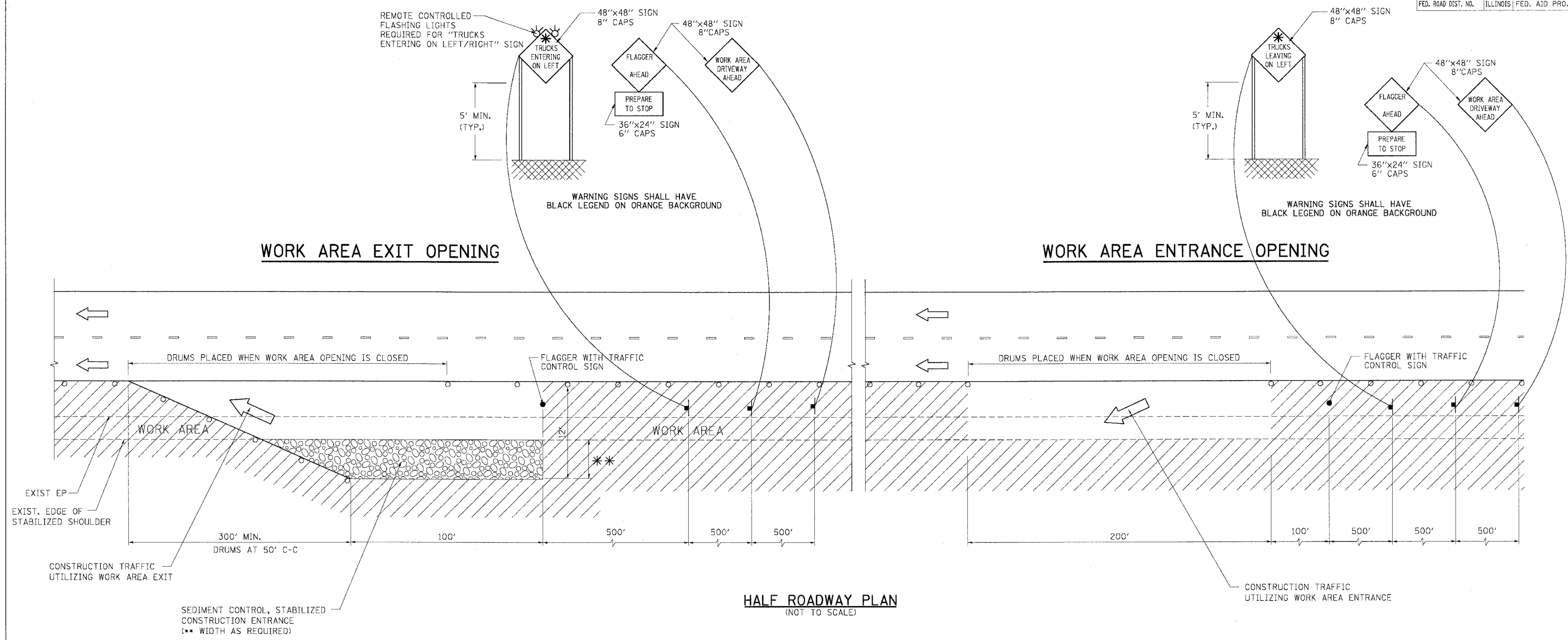
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 DATE: 02/23/07 CHECKED BY DDH

**TENG** TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	137
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

\* SIGNS ARE TO READ "TRUCKS ENTERING ON LEFT/RIGHT"

\* SIGNS ARE TO READ "TRUCKS LEAVING ON LEFT/RIGHT"



HALF ROADWAY PLAN (NOT TO SCALE)

**NOTES**

1. THE "FLAGGER AHEAD" AND "TRUCKS ENTERING OR TRUCKS LEAVING ON LEFT/RIGHT" SIGNS SHALL BE REMOVED OR COVERED WHEN THE FLAGGING OPERATION CEASES.
2. THE FLASHING LIGHTS SHALL MEET THE REQUIREMENTS OF ARTICLE 702.05(G) AND BE OPERATED BY THE FLAGGER REMOTELY. THE LIGHTS SHALL BE FLASHING ONLY WHEN A VEHICLE IS ENTERING THE EXPRESSWAY.
3. WORK AREA EXIT OPENINGS SHALL BE LOCATED A MINIMUM OF ONE HALF MILE APART. WORK AREA EXIT OPENINGS AND WORK AREA ENTRANCE OPENINGS SHALL BE LOCATED A MINIMUM OF 2,300' APART. WORK AREA OPENINGS INTO THE MEDIAN FROM BOTH DIRECTIONS OF TRAFFIC SHALL NOT BE PLACED DIRECTLY ACROSS FROM EACH OTHER. THE WORK AREA OPENING LOCATIONS SHALL BE APPROVED BY THE ENGINEER.
4. ALL CONSTRUCTION VEHICLES SHALL EXIT THE WORK AREA AT A WORK AREA EXIT OPENING.
5. ALL CONSTRUCTION VEHICLES SHALL ENTER THE WORK AREA AT A WORK AREA ENTRANCE OPENING AND SHALL USE THEIR TURN SIGNALS TO WARN MOTORISTS.
6. THE CONTRACTOR SHALL PLACE DRUMS WITH STEADY BURNING MONODIRECTIONAL LIGHTS, SPACED AT 50' CENTERS, ACROSS THE WORK AREA OPENING WHEN THE WORK AREA OPENING IS CLOSED. THE COSTS OF INSTALLING, MAINTAINING AND REMOVING THESE DRUMS AND SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".
7. WORK AREA OPENINGS SHALL NOT BE SIMULTANEOUSLY USED AS A WORK AREA ENTRANCE AND A WORK AREA EXIT.
8. THE CONTRACTOR SHALL CLOSE THE WORK AREA OPENING WHEN REVISING THE SIGNING FROM AN ENTRANCE TO AN EXIT AND FROM AN EXIT TO AN ENTRANCE.
9. A FLAGGER IS REQUIRED AT ALL TIMES UNLESS THE WORK AREA OPENING IS CLOSED. THE COST OF THE FLAGGER WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)".
10. SEE EROSION AND SEDIMENT CONTROL DETAIL FOR STABILIZED CONSTRUCTION ENTRANCE.

**LEGEND:**

- FLAGGER WITH TRAFFIC CONTROL SIGN
- ← DIRECTION OF TRAFFIC
- SIGN ON SUPPORT
- DRUM W/ MONODIRECTIONAL STEADY BURN LIGHT
- ▨ WORK AREA

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

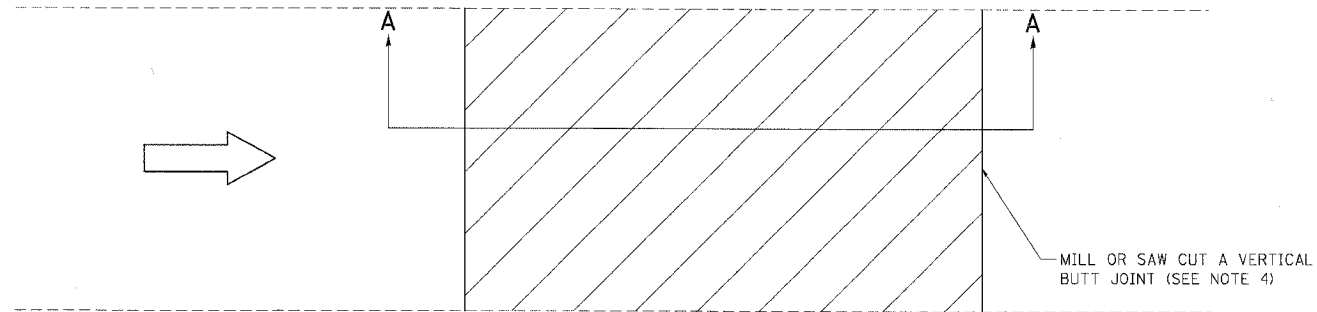
**STAGING AND TRAFFIC CONTROL  
 DETAIL FOR SIGNING FOR FLAGGING  
 OPERATIONS AT WORK AREA OPENINGS**

SCALE: N.T.S. DRAWN BY JP  
 DATE: 02/23/07 CHECKED BY DDH

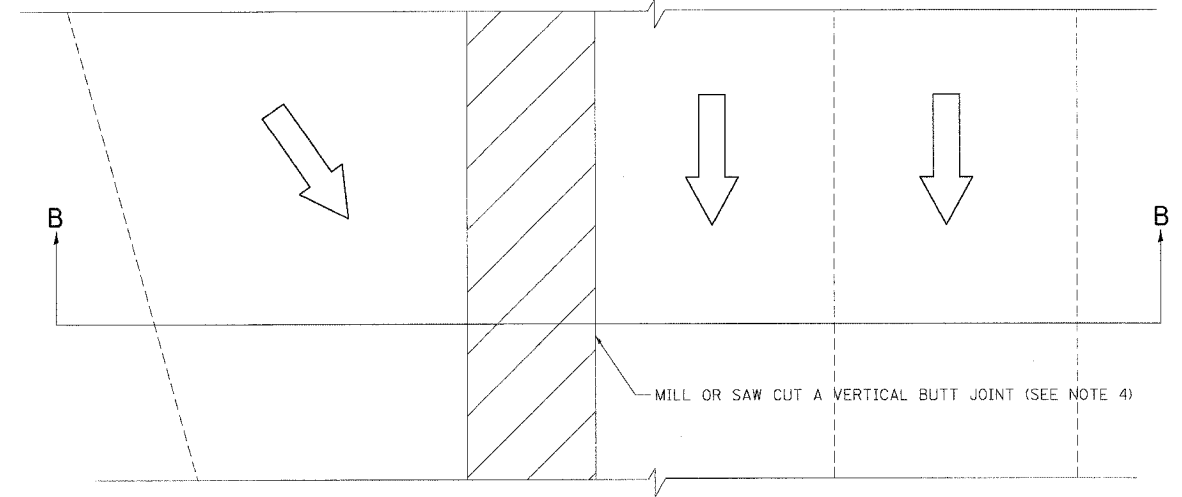
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 TENG & ASSOCIATES, INC.  
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 CHICAGO, ILLINOIS

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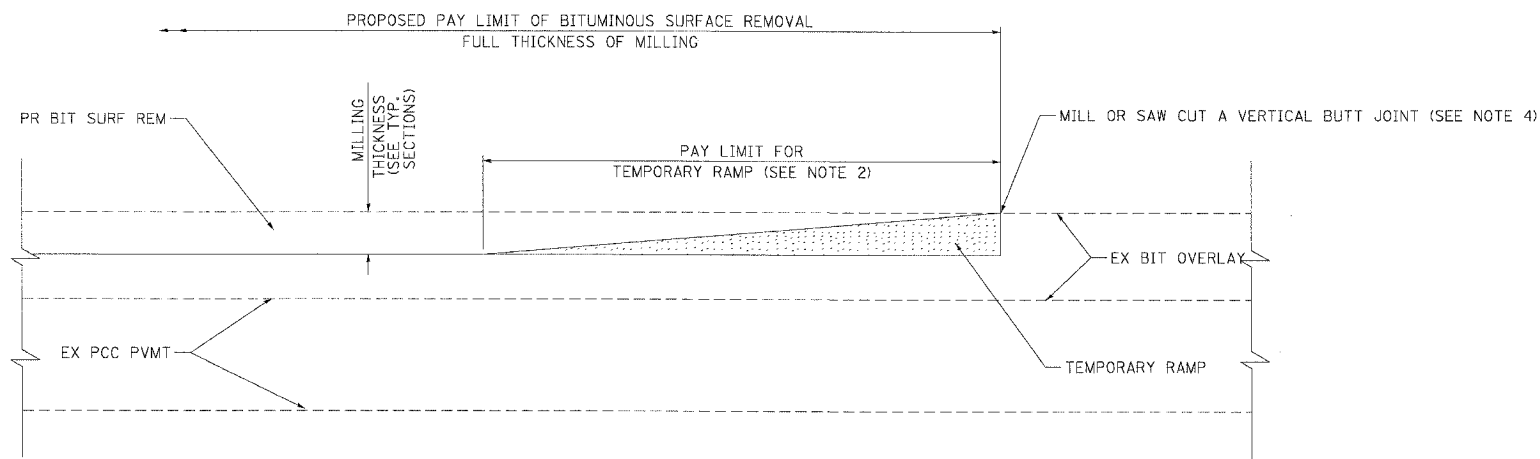
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



PLAN

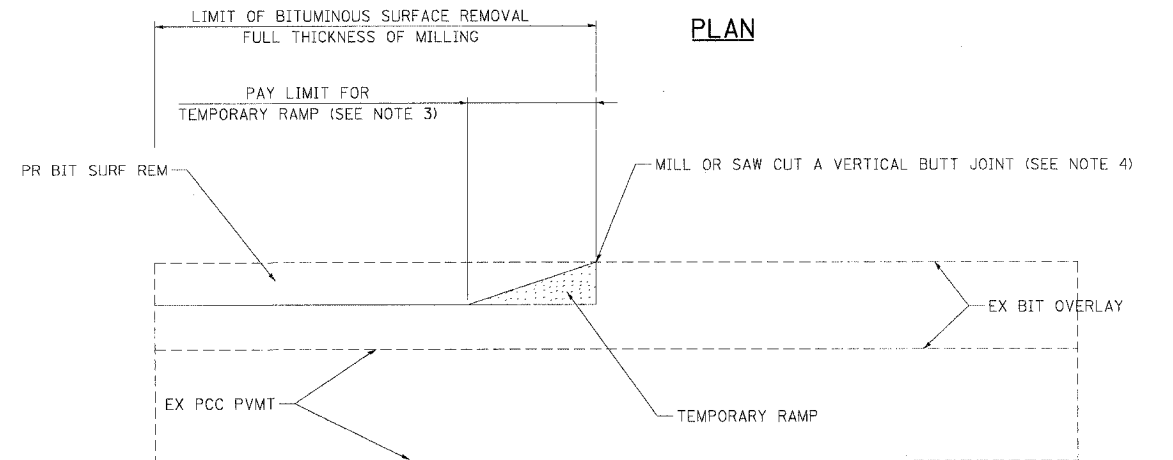


PLAN



SECTION A-A

**ROADWAY DETAILS FOR TEMPORARY RAMPS FOR MAINLINE TRAFFIC  
DOWNSTREAM RAMP SHOWN (UPSTREAM RAMP SIMILAR)**



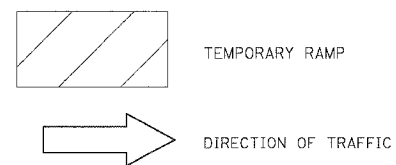
SECTION B-B

**ROADWAY DETAILS FOR MERGING AND DIVERGING TRAFFIC  
DOWNSTREAM MERGING RAMP SHOWN (UPSTREAM DIVERGING RAMP SIMILAR)**

**NOTES**

1. PRIOR TO OPENING BITUMINOUS SURFACE REMOVAL AREAS TO TRAFFIC, TEMPORARY RAMPS SHALL BE CONSTRUCTED AND MAINTAINED AT BOTH THE UPSTREAM AND DOWNSTREAM ENDS OF THE SURFACE REMOVAL AREAS OPEN TO TRAFFIC.
2. TAPER THE TEMPORARY RAMP AT A RATE OF 40 INCHES PER INCH OF MILLING THICKNESS.
3. WHERE THE TEMPORARY RAMP IS AT A SKEW TO THE MAINLINE ALIGNMENT, SUCH AS AT ENTRANCE AND EXIT RAMPS, TAPER THE TEMPORARY RAMP AT A RATE OF 12 INCHES PER INCH OF MILLING THICKNESS OR AS DIRECTED BY THE ENGINEER
4. INSTALLATION, MAINTENANCE AND REMOVAL OF THE TEMPORARY RAMP INCLUDING THE COST TO MILL OR SAW CUT TO PROVIDE A VERTICAL EDGE AS SHOWN, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "TEMPORARY RAMP".
5. CONSTRUCTION OF THE TEMPORARY RAMP SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 406 OF THE STANDARD SPECIFICATIONS.

**LEGEND**



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 I-55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 DETAIL FOR  
 TEMPORARY RAMP**

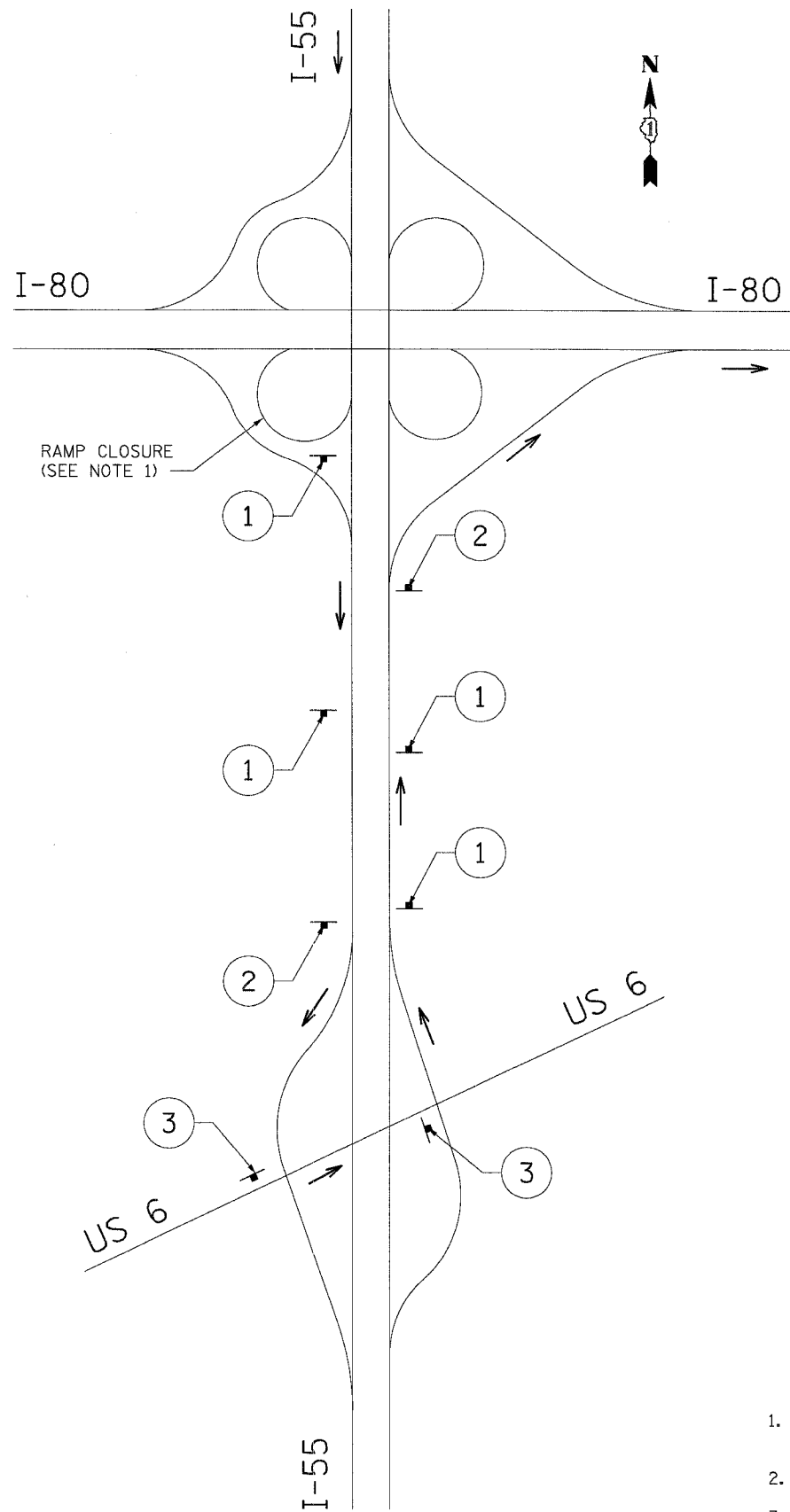
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 CHECKED BY KA

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	139
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



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BLACK ON ORANGE



REFERENCE: M6-3  
30" X 18"  
BLACK ON ORANGE

1

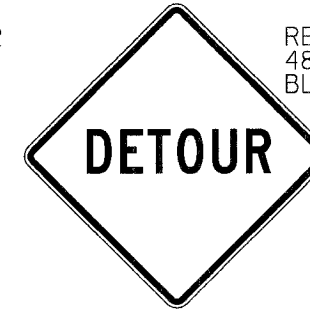


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48" X 48"  
BLACK ON ORANGE



REFERENCE: M6-2  
30" X 18"  
BLACK ON ORANGE

2



REFERENCE: W20-2  
48" X 48"  
BLACK ON ORANGE



REFERENCE: M6-1  
30" X 18"  
BLACK ON ORANGE

3

**NOTES:**

1. I-55 SB EXIT RAMP TO I-80 EB RAMP CLOSURE SHALL BE DONE IN ACCORDANCE WITH DISTRICT 1 DETAIL TC-8 (FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS).
2. PLACEMENT OF DETOUR SIGNS SHALL BE AS DIRECTED BY THE ENGINEER.
3. CHANGEABLE MESSAGE SIGNS SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAT ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**STAGING AND TRAFFIC CONTROL  
 I-55 SB EXIT RAMP TO I-55 EB  
 DETOUR PLAN**

SCALE: NO SCALE  
 DATE 02/23/07

DRAWN BY JP  
 CHECKED BY KAA

**TENG** & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	140
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

EROSION AND SEDIMENT CONTROL GENERAL NOTES

- THE WORK DESCRIBED ON THESE DRAWINGS ARE AN INTEGRAL PART OF THE STORM WATER POLLUTION PREVENTION PLAN USED TO OBTAIN A NPDES PERMIT FROM IEPA FOR THE CONSTRUCTION OF THIS PROJECT.
- THE PURPOSE OF THE EROSION AND SEDIMENT CONTROL MEASURES INCLUDED FOR THIS PROJECT IS TO LIMIT THE SEDIMENT POLLUTION IMPACT, OF ANY STORM WATER DISCHARGES THAT ORIGINATE ON THIS SITE OR OFF-SITE FLOWS THAT FLOW OVER THE DISTURBED AREAS, ON DOWNSTREAM AREAS.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSURE THAT SEDIMENT TRANSPORT OFF THE SITE IS REDUCED BY A COMBINATION OF MINIMIZATION OF EROSION AT THE SOURCE AND INSTALLATION OF SPECIFIC MEASURES TO CONTROL OR REDUCE THE TRANSPORT OF SEDIMENT. A COPY OF THE EROSION AND SEDIMENT CONTROL SCHEDULE BEING IMPLEMENTED BY THE CONTRACTOR WILL BE ON THE CONSTRUCTION SITE AT ALL TIMES.
- TO THE MAXIMUM EXTENT POSSIBLE, ALL FLOWS ORIGINATING OFF THE CONSTRUCTION SITE WILL BE DIVERTED AROUND DISTURBED AREAS OR WILL BE CONVEYED THROUGH THE SITE IN A MANNER THAT UNTREATED ON-SITE RUNOFF DOES NOT MIX WITH THE OFF-SITE RUNOFF.
- ALL RUNOFF ORIGINATING ON DISTURBED AREAS ASSOCIATED WITH THIS PROJECT WILL PASS THROUGH ONE OR MORE MEASURES THAT WILL MINIMIZE THE OFF-SITE SEDIMENT IMPACTS OF THE CONSTRUCTION ACTIVITY.
- ALL PERMANENT SEDIMENT BASINS, PERMANENT STORM WATER CONTROL MEASURES, AND RUNOFF CONTROL MEASURES REQUIRED TO KEEP OFF-SITE RUNOFF FROM FLOWING OVER THE CONSTRUCTION AREA WILL BE INSTALLED BEFORE CLEARING AND STRIPPING OF THE SITE PROCEEDS. PRIOR TO PROCEEDING WITH GENERAL EARTHWORK ON A PROJECT THE CONTRACTOR WILL OBTAIN APPROVAL OF HIS PROPOSED EARTHWORK AND STABILIZATION SCHEDULE.
- A MAXIMUM OF 10 ACRES MAY BE IN SOME STAGE OF GRADING AT A SINGLE TIME. ADDITIONAL AREAS (UP TO 10 ACRES) MAY BE CLEARED BUT WILL NOT BE STRIPPED OF VEGETATION UNTIL THE GRADED AREAS HAVE BEEN PROTECTED FROM EROSION THROUGH INSTALLATION OF EITHER TEMPORARY OR PERMANENT MEASURES. WHENEVER POSSIBLE, THE GRADING WILL BE COMPLETED TO THE DESIGN GRADE AND THE PERMANENT VEGETATION PLAN IMPLEMENTED PRIOR TO STARTING GRADING ACTIVITIES ON THE NEXT SITE.
  - WHEN BALANCING EARTHWORK (BORROW FROM A CUT USED AS FILL AT A LOCATION DISTANT FROM THE CUT) THE ENGINEER WILL CONSIDER ALLOWING MORE THAN 10 ACRES OF GRADING AT A TIME. THE 10 ACRES LIMITATION DOES NOT INCLUDE HAUL ROADS, BRIDGE CONSTRUCTION WORK AREAS AND STORAGE AREAS.
  - VARIATIONS TO THE ABOVE MAY BE CONSIDERED BY THE ENGINEER UNDER ALL THE FOLLOWING CONDITIONS:
    - IF THE CONTRACTOR FALLS BEHIND SCHEDULE THROUGH NO FAULT OF HIS OWN.
    - THE CONTRACTOR MUST PRESENT A SCHEDULE DEMONSTRATING THE NEED FOR SUCH VARIATION IN ORDER TO COMPLETE THE WORK ON TIME.
    - THE CONTRACTOR MUST COMPLY WITH ALL OTHER CONTRACT REQUIREMENTS.
- DISTURBED AREAS ARE TO BE PROTECTED FROM EROSION IN A TIMELY MANNER. UPON COMPLETION OF GRADING OR CONSTRUCTION, THE AREA WILL BE STABILIZED (USING PERMANENT MEASURES WHEN POSSIBLE) WITHIN 7 CALENDAR DAYS. TEMPORARY STABILIZATION THROUGH USE OF GROUND COVER, MULCHING, OR OTHER APPROVED MEASURES WILL BE INSTALLED WHENEVER SITE DEVELOPMENT WORK, GRADING OR OTHER EARTH DISTURBING ACTIVITIES CEASE TO BE CONTINUOUS FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. THE 7/14 DAY REQUIREMENT IS TAKEN TO MEAN THAT THE STABILIZATION OPERATION IS COMPLETE OR NEARING COMPLETION IN THE DEFINED TIME.
- STABILIZATION OF CUT OR FILL SLOPES WITH TEMPORARY OR PERMANENT EROSION CONTROL MEASURES IS REQUIRED WHENEVER THE CUT OR FILL ACTIVITY REACHES 10 FEET VERTICALLY OR THE FINISHED SLOPE EQUALS 30 FEET, WHICHEVER IS MORE RESTRICTIVE. ONCE THE STABILIZATION MEASURES ARE INSTALLED, THE PLACEMENT OF FILL EXCAVATION ACTIVITIES ARE ALLOWED TO PROCEED.
- THE CONTRACTOR SHALL DESIGNATE ONE OF HIS EMPLOYEES AS RESPONSIBLE FOR IMPLEMENTATION OF THE EROSION AND SEDIMENT CONTROL PLAN ON ALL DISTURBED AREAS. THIS PERSON IS TO BE KNOWLEDGEABLE ABOUT INSTALLATION AND MAINTENANCE OF THE REQUIRED MEASURES. THIS EMPLOYEE IS TO HAVE THE AUTHORITY TO CARRY OUT THE IMPLEMENTATION OF ANY INSTRUCTIONS CONCERNING THE EROSION AND SEDIMENT CONTROL PLAN GIVEN BY THE ENGINEER. ALL MEASURES WILL BE INSPECTED BY THIS INDIVIDUAL AND THE ENGINEER ON A REGULAR BASIS (AT LEAST ONCE EVERY 7 DAYS) AND AFTER RAINFALL EVENTS GREATER THAN 1/2 INCH.
- SEDIMENT TRAPS, SEDIMENT BASINS, DITCHES, SEDIMENT CONTROL, SILT FENCE, STONE OUTLET STRUCTURES, EARTH BERMS, ETC. SHALL BE MAINTAINED DURING THE CONSTRUCTION SEASON AS WELL AS THE WINTER MONTHS AND OTHER TIMES WHEN THE PROJECT IS CLOSED DOWN. TRAPS WILL BE CLEANED WHEN THEY ARE 50% FILLED, SILT FENCE STONE OUTLET STRUCTURES SHALL HAVE SEDIMENT REMOVED WHEN IT REACHES 50% THE HEIGHT OF THE CONTROL DEVICE. THESE SPOILS WILL BE REMOVED TO AN APPROVED SITE.
- SALVAGED TOPSOIL SHALL BE PLACED ON WELL DRAINED LAND AWAY FROM INTERMITTENT AND LIVE STREAMS OR WETLANDS WITH THE APPROPRIATE RUNOFF CONTROL AND SEDIMENT CONTROL MEASURES INSTALLED AROUND THE STORAGE SITE AND STABILIZED IMMEDIATELY AFTER FINAL SHAPING OF THE PILE IN ACCORDANCE WITH MULCH, METHOD 2. THE CONTRACTOR WILL PROVIDE AN ADEQUATE QUANTITY OF SILT FENCE TO CONTROL THE PERIMETER OF THE STOCKPILE.
- MATERIALS EXCAVATED FOR THE CONSTRUCTION OR CLEANOUT OF SEDIMENT TRAPS OR SEDIMENT BASINS SHALL NOT BE STOCKPILED IN THE (VICINITY) OF THE TRAP OR BASIN. IT WILL EITHER BE PLACED IN AN EMBANKMENT OR WASTED AS DIRECTED BY THE ENGINEER.
- EXCAVATION TO BE USED FOR EMBANKMENTS SHALL NOT BE STOCKPILED UNLESS PERIMETER CONTROLS ARE UTILIZED. WHEN THIS MATERIAL IS STOCKPILED FOR THE CONVENIENCE OF THE CONTRACTOR THE COST OF THE CONTROLS ARE BORNE BY THE CONTRACTOR. IF THE MATERIAL IS STOCKPILED AT THE DIRECTION OF THE ENGINEER THE DEPARTMENT WILL ASSUME THE COSTS OF THE CONTROLS.
- SEDIMENT LADEN DEWATERING DISCHARGE MUST BE DIRECTED TO AN APPROVED SEDIMENT TRAPPING MEASURE PRIOR TO RELEASE FROM THE SITE.
- WHEN THE CONTRACTOR REQUESTS A CHANGE TO POSTPONE COMPLETION OF THE EXCAVATION OF A SPECIFIC AREA AS A CONTINUOUS OPERATION AND PLACING THE TOPSOIL AS DEFINED IN THE STANDARD SPECIFICATIONS, THE ENGINEER MAY ALLOW THE CONTRACTOR TO STABILIZE THE AREA USING TEMPORARY STABILIZATION WITH STRAW MULCH PROVIDING THE FOLLOWING CONDITIONS ARE MET:
  - ALL AREAS BEING STABILIZED ARE 3:1 SLOPES OR FLATTER.
  - THE CONTRACTOR BEARS THE COST OF PREPARING THE SEED BED AND STABILIZING THE AREA WITH TEMPORARY STABILIZATION WITH STRAW MULCH.
  - ALL REQUIRED SEDIMENT CONTROL MEASURES FOR THE SECTION OF ROAD IN QUESTION HAVE BEEN INSTALLED ARE BEING MAINTAINED.
- SEEDING USAGE
  - TEMPORARY EROSION CONTROL SEEDING - USED ON SHORT TERM TEMPORARY SEEDING. CLASS 2A - SALT TOLERANT ROADSIDE MIX USED FOR NEW CONSTRUCTION OF LIMITED ACCESS ROUTES INTENDED TO BE MOWED BY IDOT.
  - CLASS 4 - USED IN PERMANENT 1:2 SLOPES AND 1:3 SLOPES HIGHER THAN 10 FEET.
- TOP SOIL PLACEMENT
 

TOPSOIL WILL BE PLACED ON FINAL SLOPES WHICH WILL NOT BE DISTURBED BY FUTURE CONSTRUCTION. TOPSOIL WILL NOT BE PLACED ON SURFACES WHICH WILL BE PAVED IN THE FUTURE, NOR ON TEMPORARILY STEEP SLOPES.
- INLET FILTERS ARE REQUIRED FOR THE STRUCTURES SHOWN ON THE PLANS. STRUCTURE OPENINGS VARY SUCH THAT FIELD MEASUREMENT AND/OR CONTRACTOR DESIGN WILL BE REQUIRED. COST OF DESIGN, LABOR AND MATERIALS WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR "INLET FILTER".
- THE CONSTRUCTION LIMITS WILL BE STAKED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
- THE RESIDENT ENGINEER SHALL HAVE FINAL DETERMINATION OF THE PLACEMENT AND LOCATION OF THE SEDIMENT CONTROL, SILT FENCE.
- SEE EROSION AND SEDIMENT CONTROL PLANS FOR PLACEMENT OF ALL EROSION AND SEDIMENT CONTROL PAY ITEMS.
- SEE PROPOSED DRAINAGE PLANS FOR FINAL DRAINAGE STRUCTURE, STORM SEWER AND PIPE CULVERT INFORMATION.
- SEE EXISTING DRAINAGE AND UTILITY PLANS FOR INFORMATION CONCERNING THE REMOVAL, ADJUSTMENT, RECONSTRUCTION, ETC. OF EXISTING STRUCTURE AND PIPES.
- THE ACTUAL NEED FOR TEMPORARY DRAINAGE FACILITIES, AS WELL AS THE STAGING OF THE PERMANENT DRAINAGE SYSTEM CONSTRUCTION, MAY BE MODIFIED BY THE RESIDENT ENGINEER, WHO SHALL BE CONSULTED BEFORE THE INSTALLATION. WHERE APPLICABLE, TEMPORARY STRUCTURE AND PIPE TABLES FOR THIS WORK ARE SHOWN ON THE EROSION CONTROL PLANS.
- AS DETERMINED BY THE CONTRACTOR, SOME OF THE PROPOSED DRAINAGE STRUCTURES WILL BE STAGE CONSTRUCTED VERTICALLY TO FACILITATE CONSTRUCTION OPERATIONS. IF THE TOP SLAB OF THE STRUCTURE MUST BE PLACED AT AN INTERIM ELEVATION, THE TOP SLAB WILL THEN BE REMOVED AND THE REMAINING PORTION OF THE STRUCTURE ADDED AND THE TOP SLAB RE-INSTALLED IN A LATER STAGE. FOR DRAINAGE STRUCTURES COMPLETED TO FULL HEIGHT IN THIS CONTRACT, THE COST OF STAGE CONSTRUCTING THEM VERTICALLY WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR THE DRAINAGE STRUCTURE OF THE TYPE SPECIFIED IN THE PLANS.

- EROSION CONTROL MEASURES SHALL BE REMOVED ONLY WHERE INDICATED ON THE PLANS. COST OF REMOVAL SHALL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE TYPE OF MEASURE INDICATED ON THE PLANS.
- TEES REQUIRED TO MAKE TEMPORARY CONNECTIONS TO EXISTING SEWER SHALL BE INSTALLED ACCORDING TO IDOT DISTRICT 1 STANDARD BD-07 AND PAID FOR AT CONTRACT UNIT PRICE PER EACH FOR "REINFORCED CONCRETE PIPE TEE" OF THE SIZES REQUIRED TO MAKE THE CONNECTIONS. CONTRACTOR MAY AT HIS OPTION USE PRE-FABRICATED WYES INSTEAD OF TEES, AT HIS COST. ELBOWS AND OTHER FITTINGS REQUIRED TO MAKE TEMPORARY CONNECTIONS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER METER FOR THE TYPE AND CLASS OF PROPOSED LATERAL PIPE SPECIFIED.

WILL/SOUTH COOK SOIL AND WATER CONSERVATION DISTRICT GENERAL NOTES

- THE SOIL AND WATER CONSERVATION DISTRICT IS RESPONSIBLE FOR CONDUCTING SITE VISITS AND VERIFYING THAT THE PRACTICES ARE WORKING PROPERLY AND DETERMINE IF ADDITIONAL PRACTICES ARE NEEDED FOR BETTER SOIL EROSION AND SEDIMENT CONTROL. IF ADDITIONAL PRACTICES ARE DEEMED NECESSARY BY THE SWCD THE CONTRACTOR WILL IMPLEMENT THE PRACTICES IN A TIMELY MANNER.
- THE WILL/SOUTH COOK SOIL AND WATER CONSERVATION DISTRICT MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITIES, AND ONE WEEK PRIOR TO FINAL INSPECTION.
- EROSION CONTROL BLANKET SHALL BE INSTALLED TO ALL DISTURBED AREAS WITH SLOPES EQUAL TO OR GREATER THAN 5H:1V AND IN CRITICAL AREAS (i.e. DETENTION BASIN PERIMETERS, STREAMBANKS, BERMS, etc.) IMMEDIATELY UPON FINAL GRADING.
- SILT FENCE SHALL BE INSTALLED FOLLOWING THE COMPLETION AND STABILIZATION OF THE STORMWATER FACILITIES WILL REMAIN IN PLACE UNTIL THE CONTRIBUTING AREA IS STABILIZED.
- STOCKPILES OF SOIL AND OTHER BUILDING MATERIALS TO REMAIN IN PLACE MORE THAN THREE (3) DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES (i.e. PERIMETER SILT FENCE). STOCKPILES TO REMAIN IN PLACE FOR 30 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.
- ALL ADJACENT STREETS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY AND CLEANED WHEN NECESSARY.
- ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL.
- THE USE OF SANDBAGS FOR CREATING A DIVERSION CHANNEL IS PROHIBITED. THE PROPOSED BRIDGE/CULVERT CROSSINGS MUST HAVE A COFFERDAM, OR TURBIDITY CURTAIN IN PLACE CONSTRUCTED OF A WATER FILLED BLADDER OR PORTADAM TYPE STRUCTURE AT THE TIME OF CONSTRUCTION. DURING DEWATERING OPERATIONS, WATER MUST BE PUMPED INTO THE FILTER BAGS OR SILT TRAPS OUTSIDE OF THE EXISTING CHANNEL. DEWATERING INTO DRAIN TILES IS STRICTLY PROHIBITED.
- A STAMPED AND SIGNED COPY OF THE APPROVED SOIL EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES AND BE PRESENTED WHEN REQUESTED BY WILL/SOUTH COOK SWCD, U.S. ARMY CORPS OF ENGINEERS OR ANY OTHER AUTHORIZED AGENCY.



DATE 2/15/2007  
EXP. 11/30/2007

REVISIONS	
NAME	DATE

**MORCOM, N.V., INC.**  
CONSULTING ENGINEERS  
CHICAGO, ILLINOIS

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**EROSION AND SEDIMENT CONTROL  
FAI 55  
GENERAL NOTES**

SCALE: \_\_\_\_\_ DRAWN BY: SN  
DATE: 02/23/07 CHECKED BY: TKL

**TENG** TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

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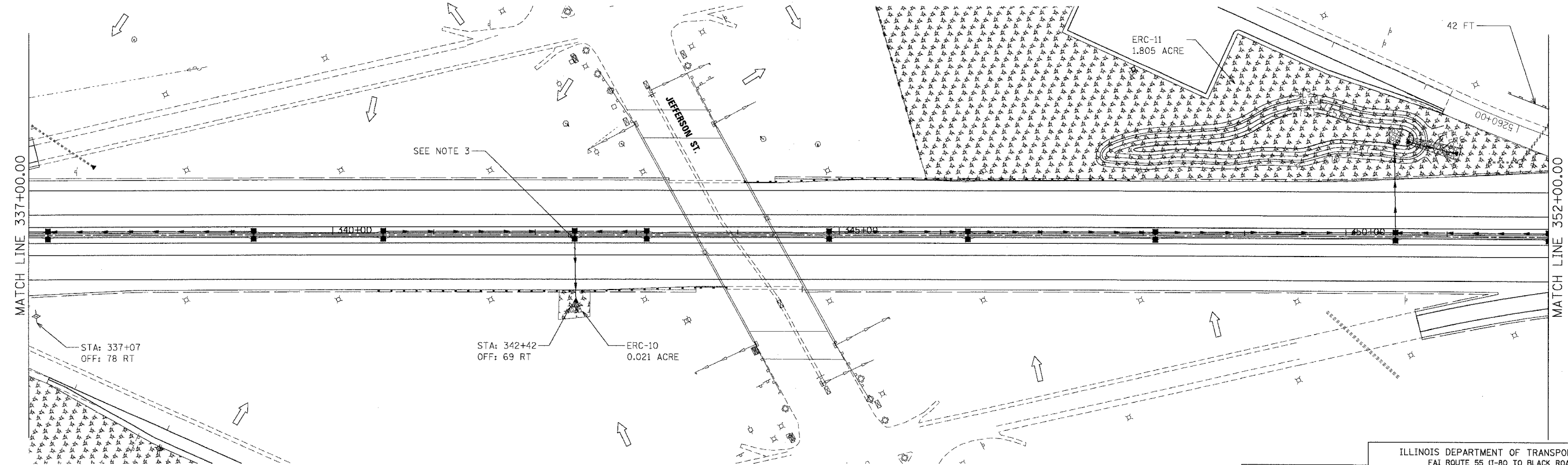
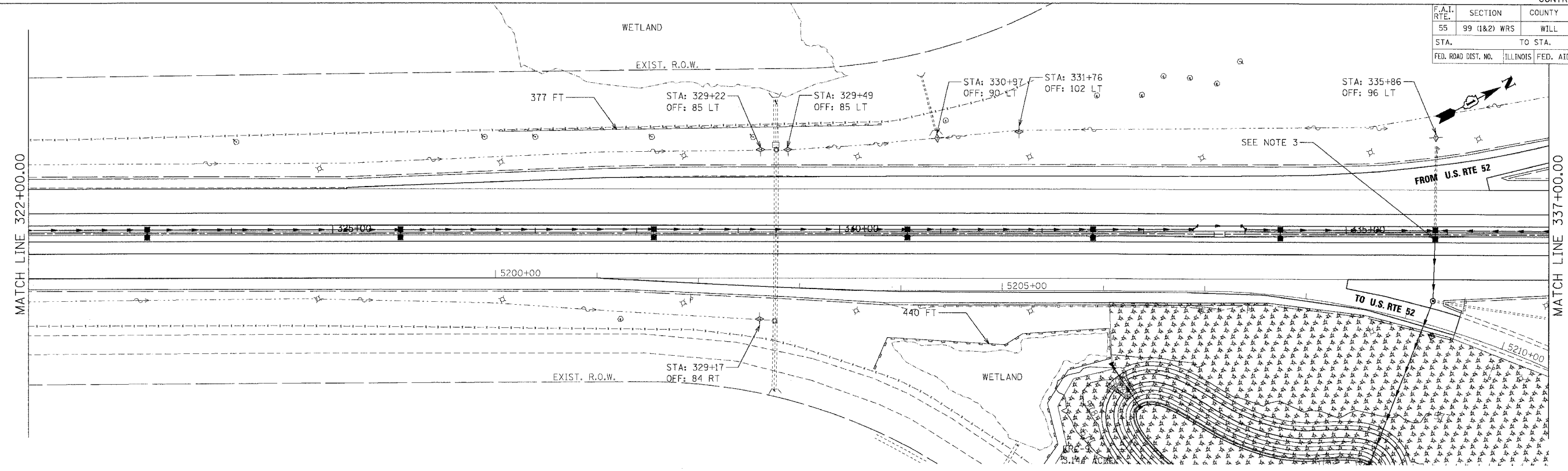


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CONTRACT NO. G2895

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	146

STA. TO STA.  
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



**LEGEND**

- INLET FILTERS
- TEMPORARY DITCH CHECK
- TEMPORARY EROSION CONTROL SEEDING AND MULCH METHOD 2
- RIPRAP CLASS A3 & A4
- TEMPORARY SILT FENCE
- OUTLET
- SHEET FLOW

**NOTES:**

1. FOR PERMANENT STRUCTURE, PERMANENT PIPE AND SEDIMENT CONTROL INLET FILTER, SEE DRAINAGE SCHEDULES, SHEETS 169 TO 175.

2. FOR ERC-9 AND ERC-11 DISTURBED AREAS SEE EROSION AND SEDIMENT CONTROL, DETENTION BASINS, SHEET 149. THE ACREAGE FOR ERC-9 AND ERC-11 (AS LISTED HERE) REPRESENTS THE WHOLE DISTURBED AREA, AS SHOWN IN SHEET 149.
3. TEMPORARY STRUCTURES (364) AND (383) SHALL BE CONSTRUCTED FOR INTERIM DRAINAGE IN PRE-STAGE ALONG WITH PIPES (363) AND (382) RESPECTIVELY. THE EXISTING DITCH SHALL BE GRADED AROUND STRUCTURE GRATES FOR POSITIVE DRAINAGE. THE GRADING SIDE SLOPES NO STEEPER THAN 3H : 1V AND LONGITUDINAL GRADES NO STEEPER THAN 6H : 1V. THE COST OF GRADING THE DITCH AT THE STRUCTURE SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR STORM SEWERS OF THE TYPE, SIZE AND CLASS SPECIFIED.

**REVISIONS**

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**EROSION AND SEDIMENT CONTROL**  
**FAI 55**  
**STA. 322+00.00 TO STA. 352+00.00**

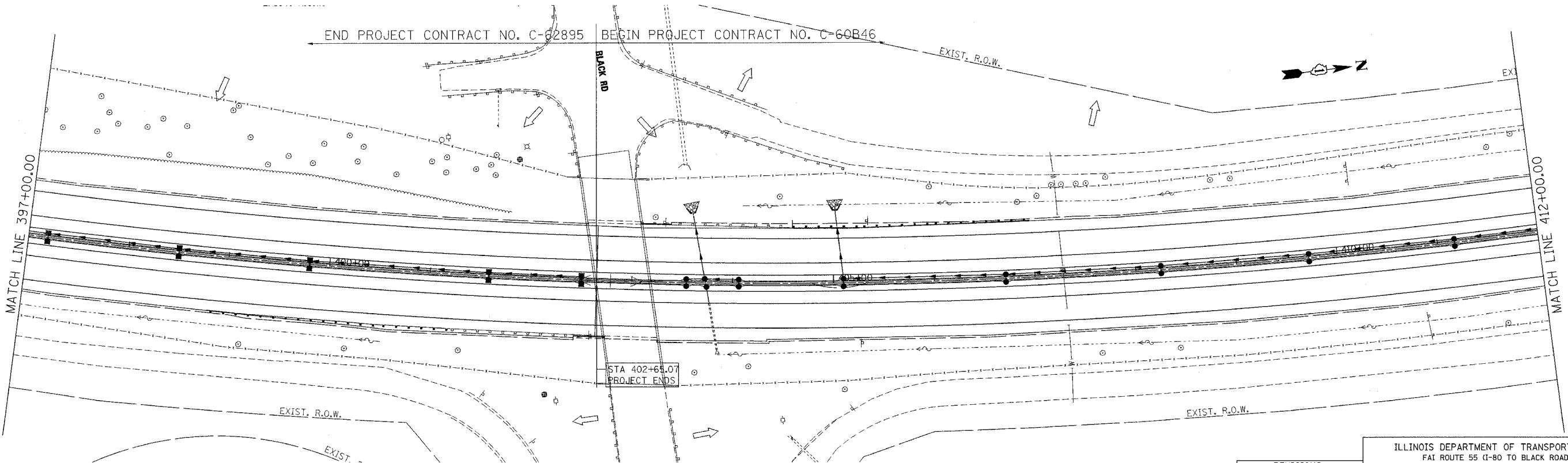
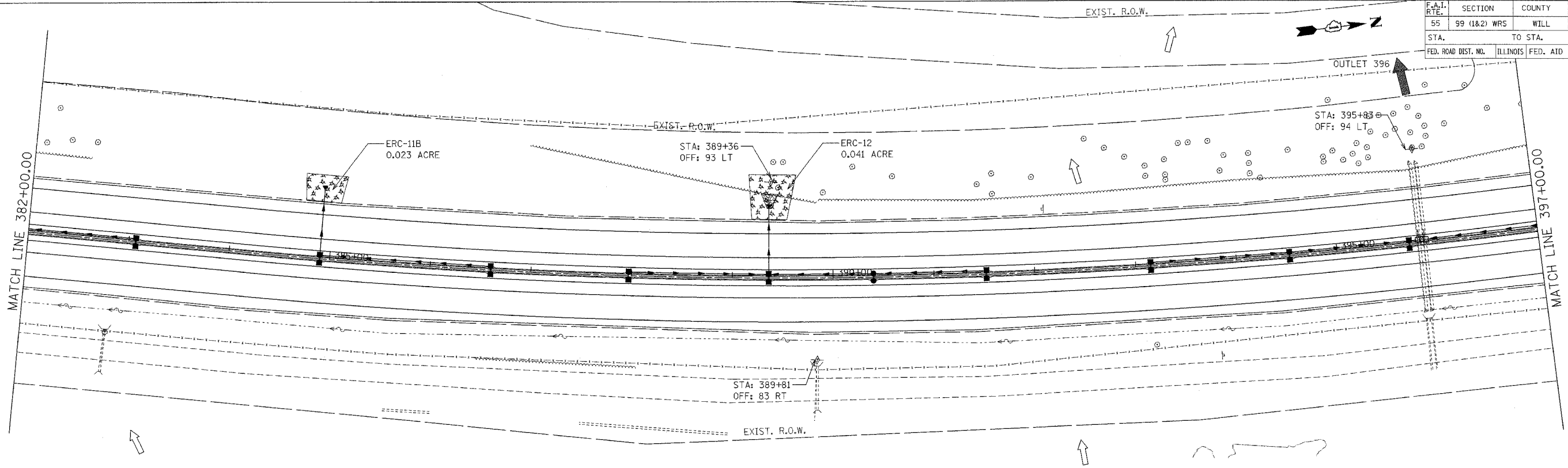
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 CHECKED BY: TKL

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	148
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



- LEGEND**
- ⊕ INLET FILTERS
  - ⊕ TEMPORARY DITCH CHECK
  - ⊕ TEMPORARY EROSION CONTROL SEEDING AND MULCH METHOD 2
  - ⊕ RIPRAP CLASS A3 & A4
  - ⊕ TEMPORARY SILT FENCE

- NOTES:**
1. FOR PERMANENT STRUCTURE, PERMANENT PIPE AND SEDIMENT CONTROL INLET FILTER, SEE DRAINAGE SCHEDULES, SHEETS 169 TO 175.
- ➔ OUTLET  
➔ SHEET FLOW

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**EROSION AND SEDIMENT CONTROL**  
**FAI 55**  
**STA. 382+00.00 TO STA. 402+65.07**

SCALE: 1"=50'  
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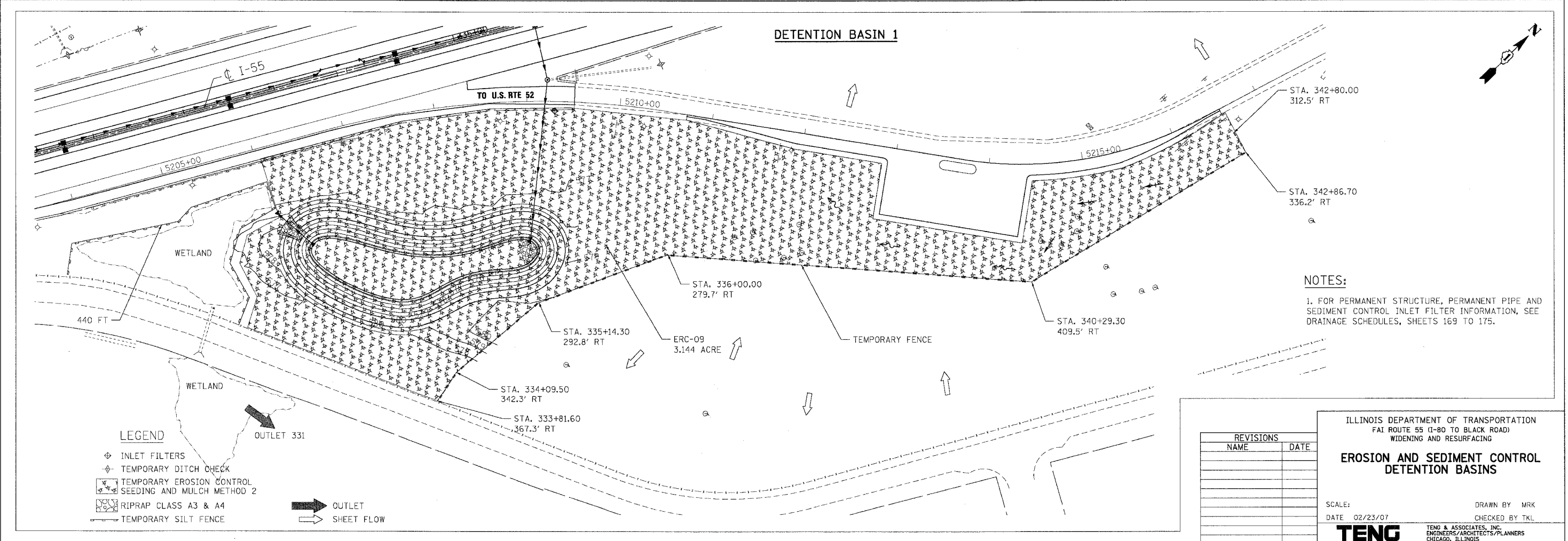
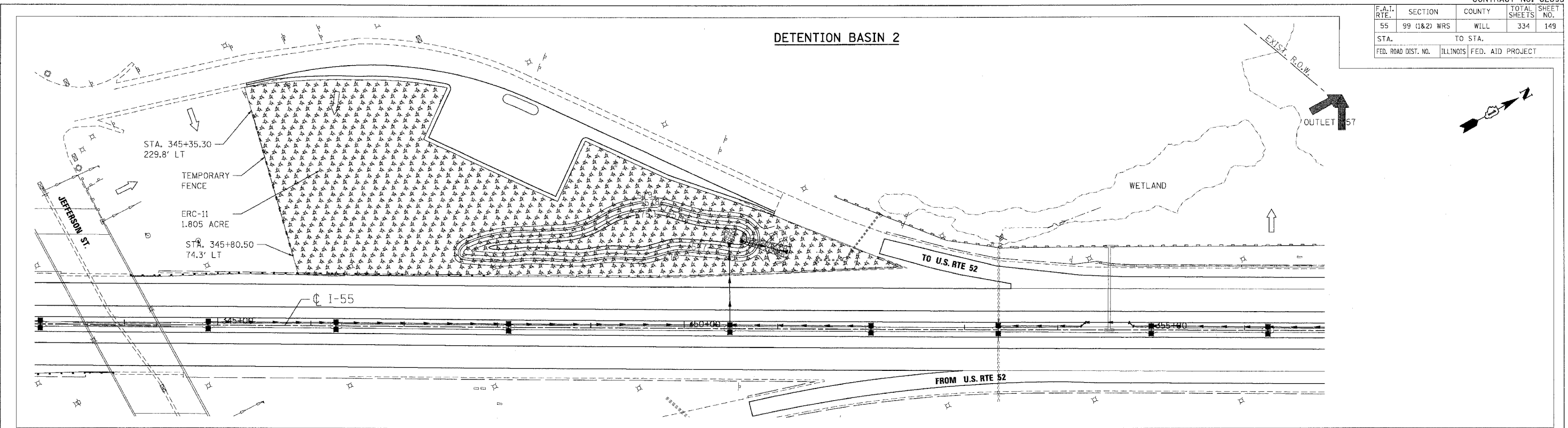
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 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	149
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**NOTES:**

1. FOR PERMANENT STRUCTURE, PERMANENT PIPE AND SEDIMENT CONTROL INLET FILTER INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169 TO 175.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**EROSION AND SEDIMENT CONTROL  
DETENTION BASINS**

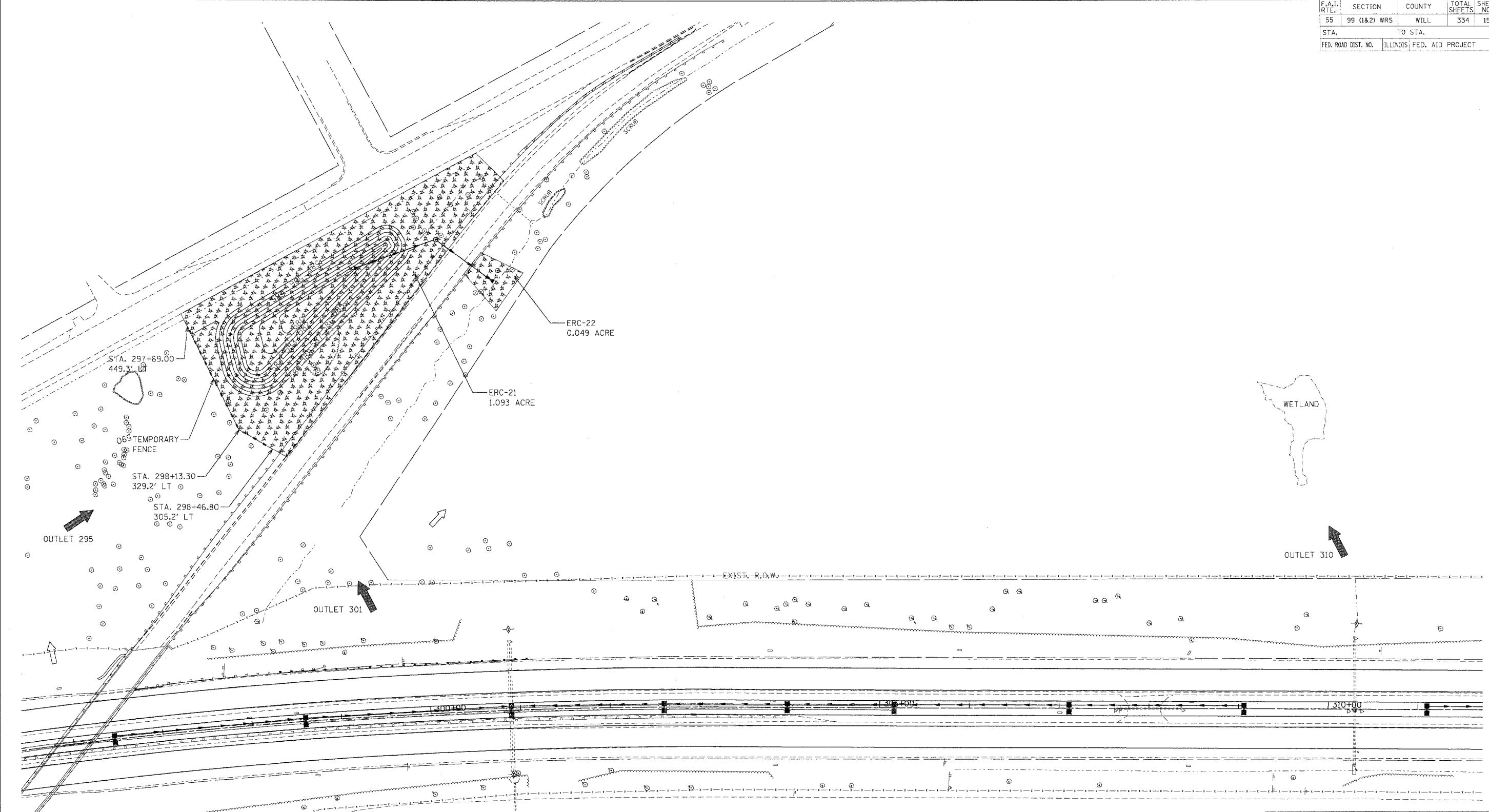
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**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE # 02/23/07  
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	150
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



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**LEGEND**

- ◆ INLET FILTERS
- ◇ TEMPORARY DITCH CHECK
- ▣ TEMPORARY EROSION CONTROL SEEDING AND MULCH METHOD 2
- ▣ RIPRAP CLASS A3 & A4
- TEMPORARY SILT FENCE

**NOTES:**

1. FOR PERMANENT STRUCTURE, PERMANENT PIPE, SEDIMENT CONTROL INLET FILTER AND RIPRAP INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169 TO 175.

- ➔ OUTLET
- ➔ SHEET FLOW

REVISIONS	
NAME	DATE

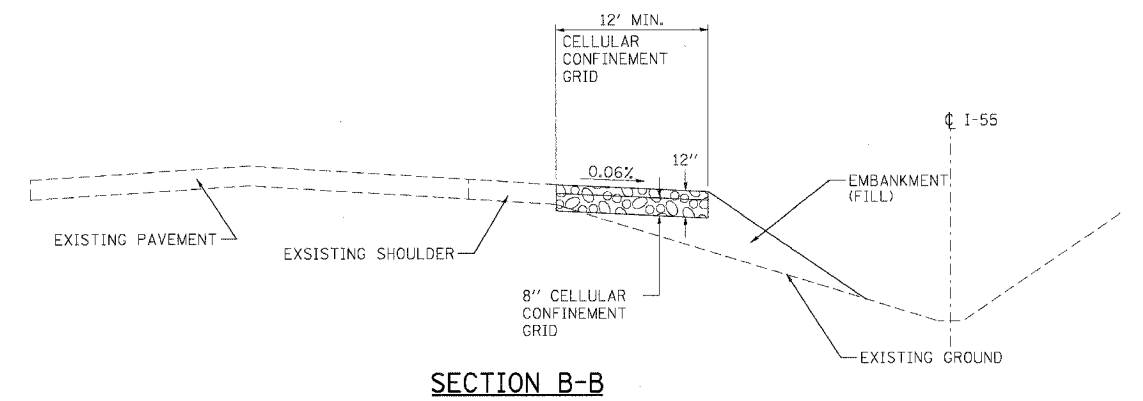
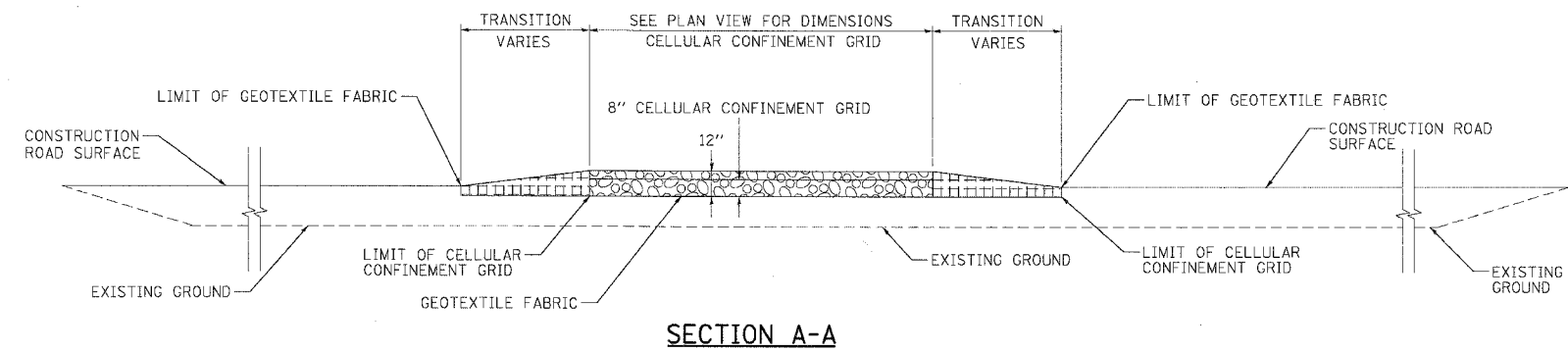
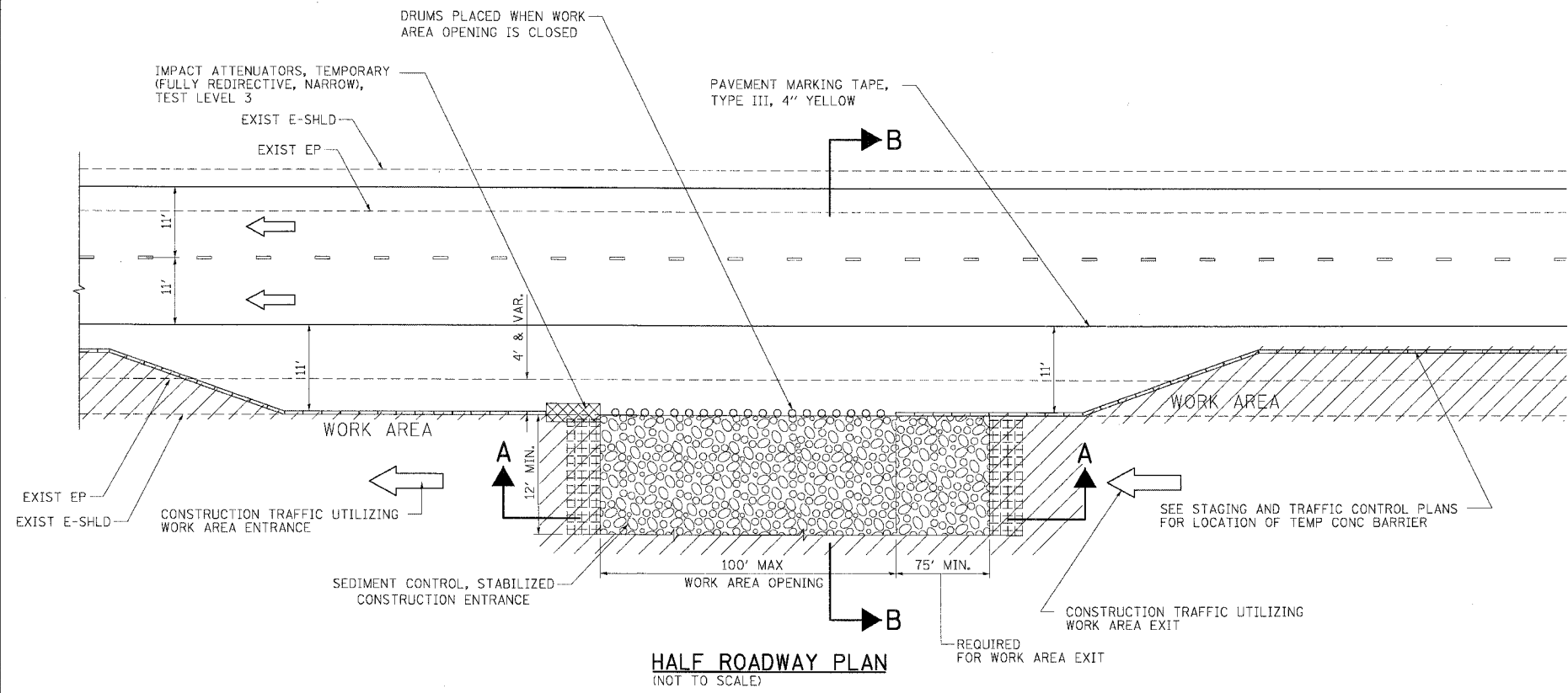
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**EROSION AND SEDIMENT CONTROL  
 DETENTION BASIN AT IL-59**






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 DATE 02/23/07 CHECKED BY TKL

**TENG** TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	151
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**LEGEND:**

-  COURSE AGGREGATE CA-3
-  SEDIMENT CONTROL, STABILIZED CONSTRUCTION ENTRANCE
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  IMPACT ATTENUATOR

**NOTES:**

1. THE EXISTING BITUMINOUS SHOULDER SHALL REMAIN IN THE PLACE UNTIL THE STABILIZED CONSTRUCTION ENTRANCE IS REMOVED.
2. EXISTING DITCH DRAINAGE TO BE MAINTAINED.
3. THE COST OF ANY COURSE AGGREGATE CA-3 USED FOR TRANSITION FROM THE CONSTRUCTION ROAD TO THE STABILIZED CONSTRUCTION ENTRANCE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "SEDIMENT CONTROL, STABILIZED CONSTRUCTION ENTRANCE".
4. SEE STAGING AND TRAFFIC CONTROL DETAIL FOR WORK AREA DRIVEWAY ENTRANCE/EXIT.
5. THE SEDIMENT CONTROL, STABILIZED CONSTRUCTION ENTRANCE DIMENSIONS SHALL BE APPROVED BY THE ENGINEER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

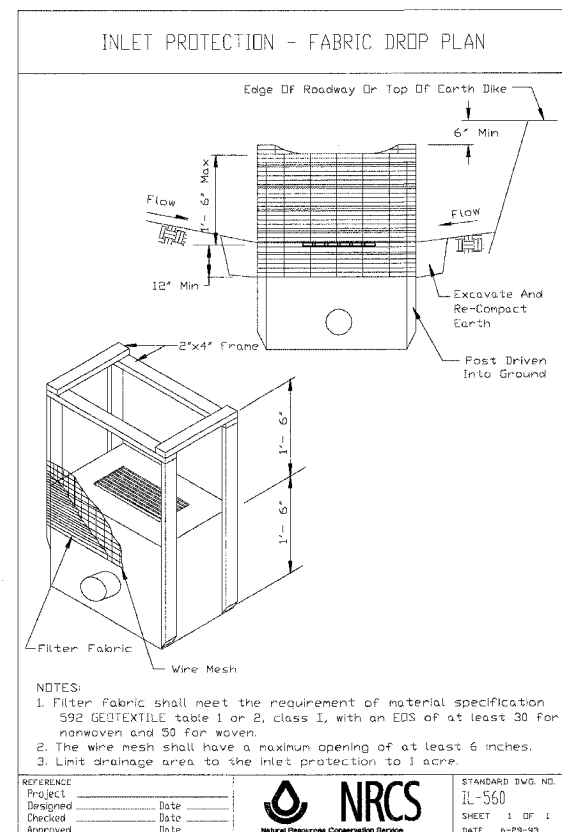
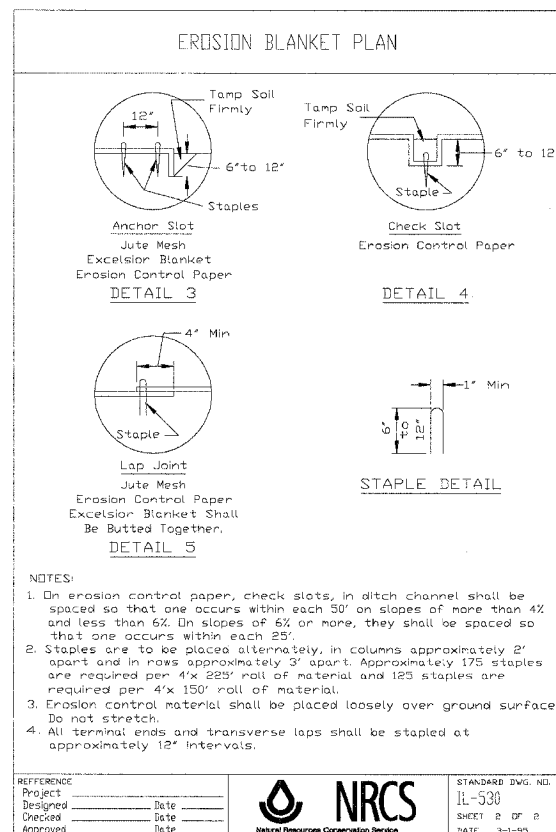
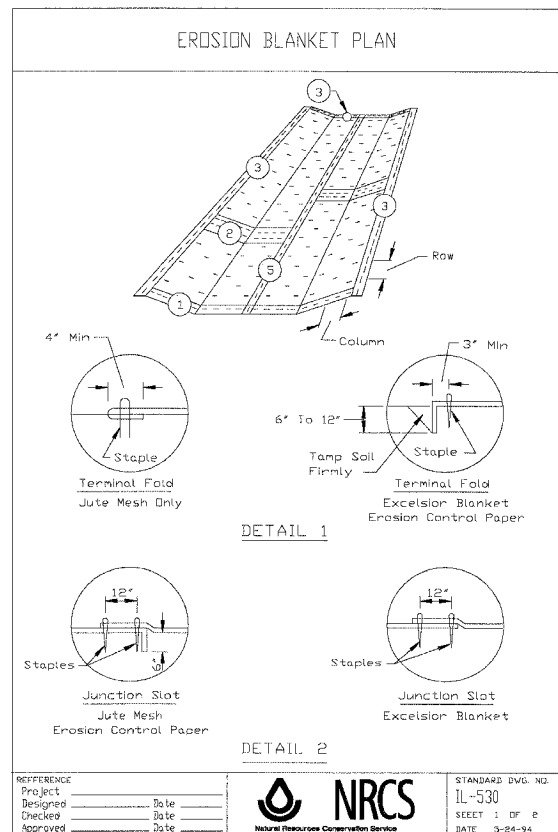
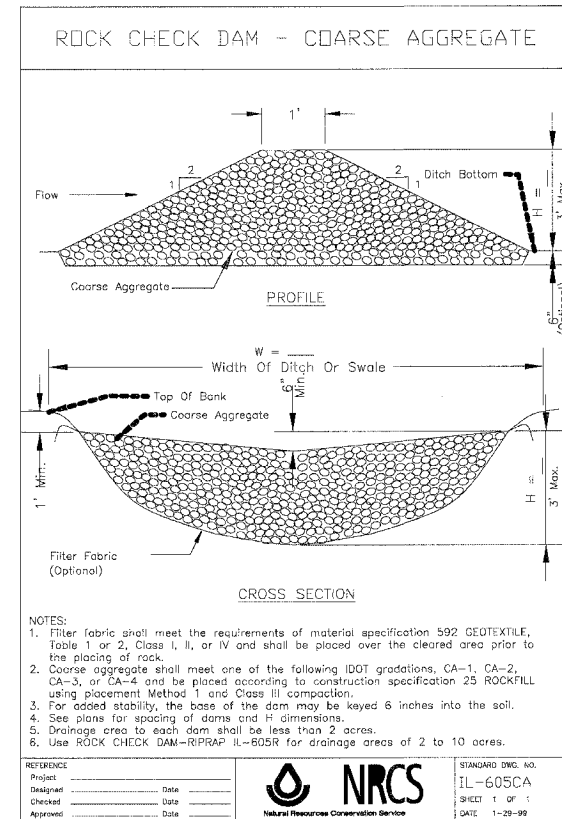
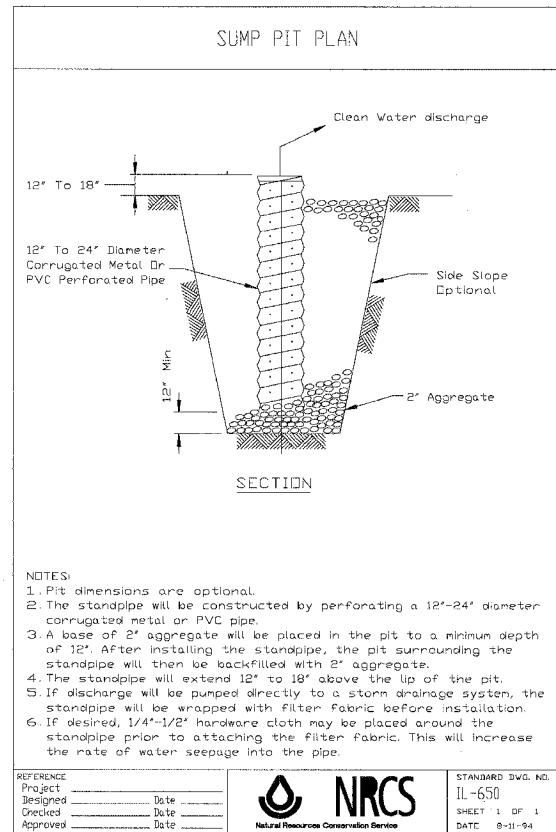
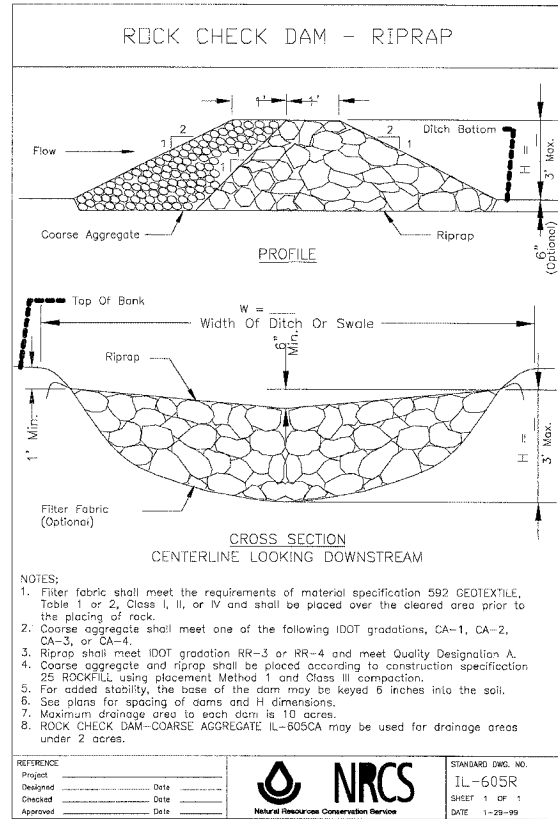
**EROSION AND SEDIMENT CONTROL  
 DETAIL FOR STABILIZED CONSTRUCTION  
 ENTRANCE**

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 DATE 02/23/07      CHECKED BY DDH

**TENG**      TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	152
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

### EROSION AND SEDIMENT CONTROL DETAILS

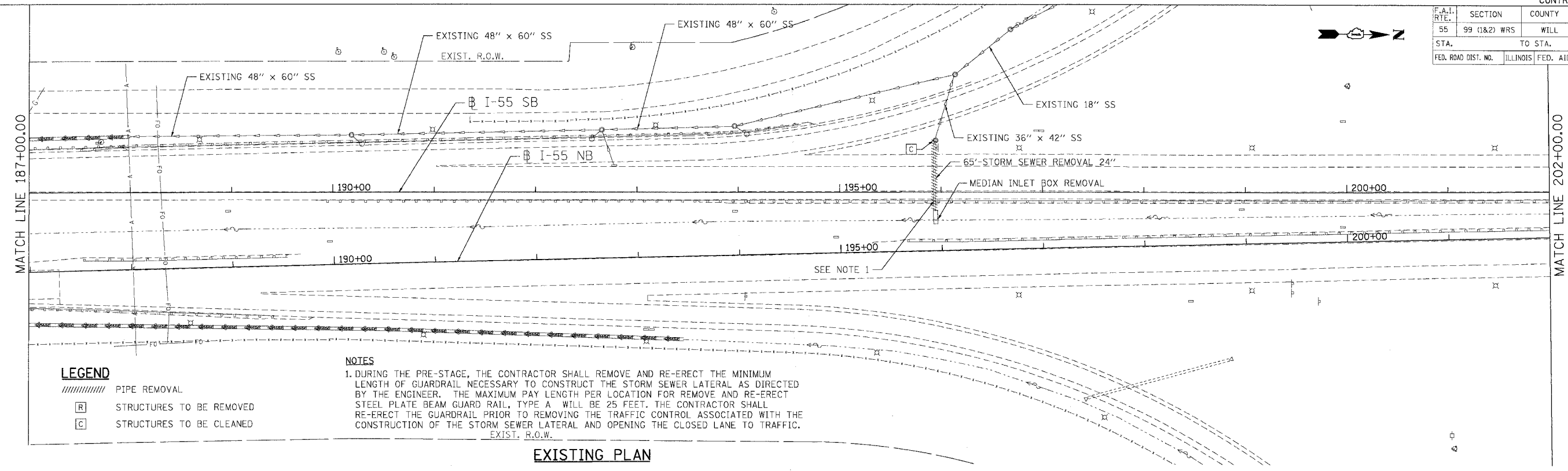
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**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	154
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



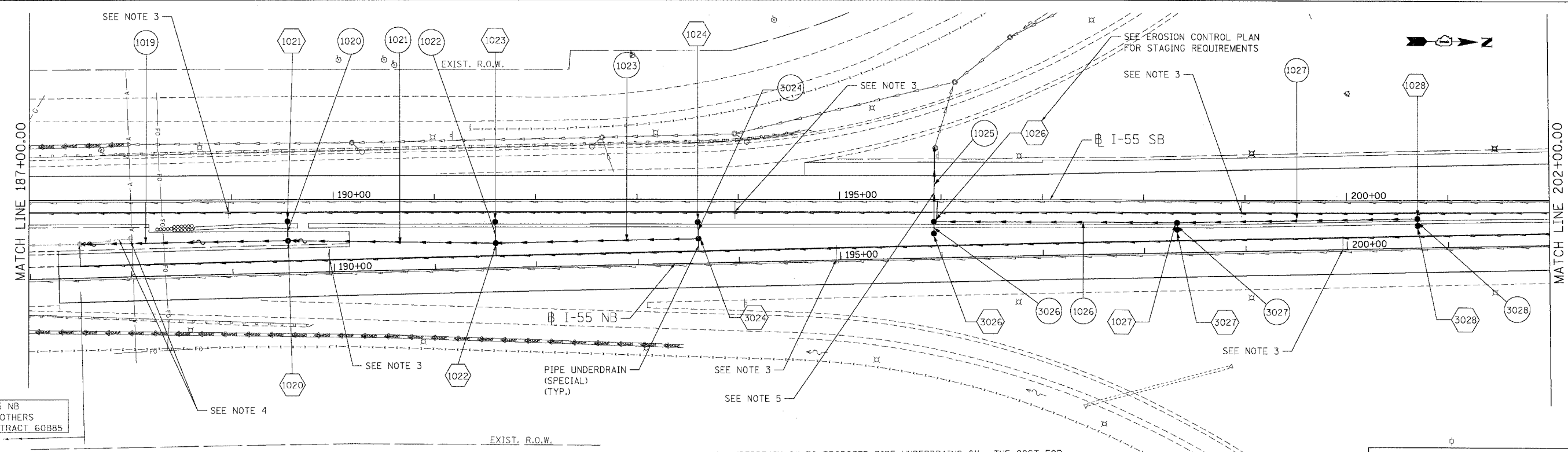
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//////	PIPE REMOVAL
[R]	STRUCTURES TO BE REMOVED
[C]	STRUCTURES TO BE CLEANED

**NOTES**

1. DURING THE PRE-STAGE, THE CONTRACTOR SHALL REMOVE AND RE-ERECT THE MINIMUM LENGTH OF GUARDRAIL NECESSARY TO CONSTRUCT THE STORM SEWER LATERAL AS DIRECTED BY THE ENGINEER. THE MAXIMUM PAY LENGTH PER LOCATION FOR REMOVE AND RE-ERECT STEEL PLATE BEAM GUARD RAIL, TYPE A, WILL BE 25 FEET. THE CONTRACTOR SHALL RE-ERECT THE GUARDRAIL PRIOR TO REMOVING THE TRAFFIC CONTROL ASSOCIATED WITH THE CONSTRUCTION OF THE STORM SEWER LATERAL AND OPENING THE CLOSED LANE TO TRAFFIC.

**EXISTING PLAN**



I-55 NB  
BY OTHERS  
CONTRACT 60B85

**LEGEND**

(100)	STRUCTURE NUMBER	●	CATCH BASIN
(100)GR	GRATING FOR STRUCTURE	○	MANHOLE
(100)	PIPE NUMBER	▲	FLARED END SECTION
—	STORM SEWER	—	HEADWALL
—	PROPOSED DITCH	—	PIPE UNDERDRAINS

**NOTES**

1. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

2. PROPOSED SEWER AND CULVERT CROSSING BENEATH THE EXISTING PAVEMENT SHALL BE CONSTRUCTED DURING PRE-STAGE PER THE STAGING AND TRAFFIC CONTROL PLANS. SEE ROADWAY PLANS FOR PATCHING DETAILS.

3. CONNECT EXISTING PIPE UNDERDRAIN 6" TO PROPOSED PIPE UNDERDRAINS 6". THE COST FOR MAKING THE CONNECTIONS USING APPROVED FITTINGS IS TO BE INCLUDED IN THE COST OF PIPE UNDERDRAINS 6" (SPECIAL), (TYPICAL).

4. PROPOSED 4'-DIA CATCH BASIN AND 18" STORM SEWER BUILT IN CONTRACT 60B85 TO BE REMOVED.

5. STORM SEWER LATERALS SHALL BE INSTALLED PER THE STAGING AND TRAFFIC CONTROL TYPICAL SECTIONS FOR PRE-STAGE (SEE SHEET 111). THE CONTRACTOR SHALL BEGIN CONSTRUCTION OF THIS STORM SEWER LATERAL WHEN PRECIPITATION IS NOT INCLUDED IN THE NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION'S NATIONAL WEATHER SERVICE FORECAST FOR THE 48 HOUR PERIOD FOLLOWING THE START OF CONSTRUCTION OF THIS STORM SEWER LATERAL ([HTTP://WWW.NWS.NOAA.GOV](http://www.nws.noaa.gov)). PRE-STAGE, PHASE II SHALL BE COMPLETED WITHIN 36 HOURS OF COMPLETING PRE-STAGE, PHASE I FOR THIS STORM SEWER LATERAL.

**PROPOSED PLAN**

**REVISIONS**

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
FAI 55  
STA. 187+00.00 TO STA. 202+00.00

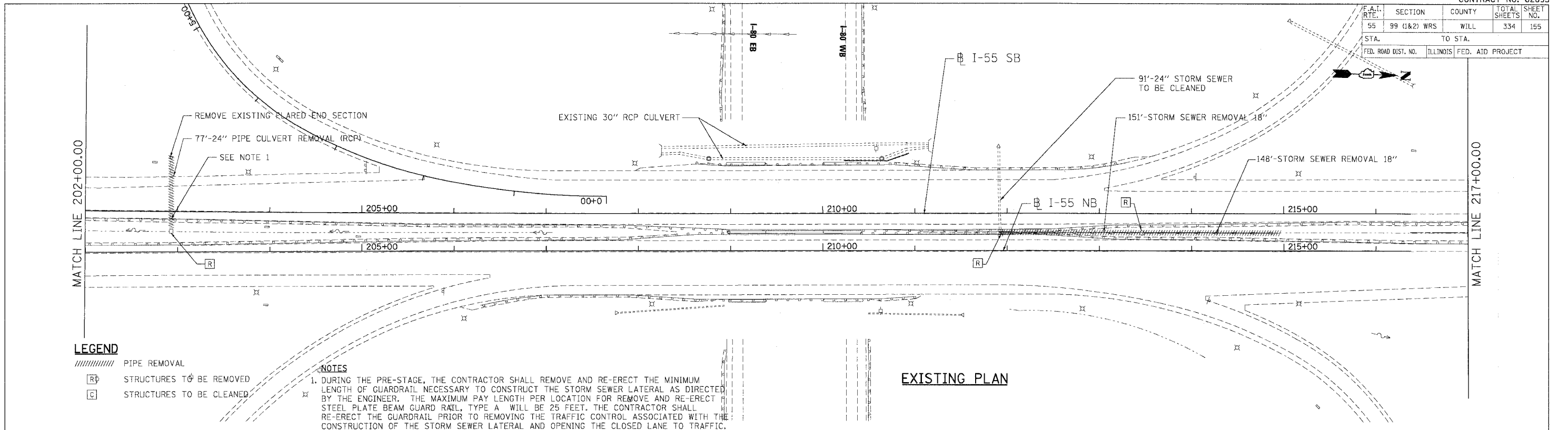
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**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	155
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



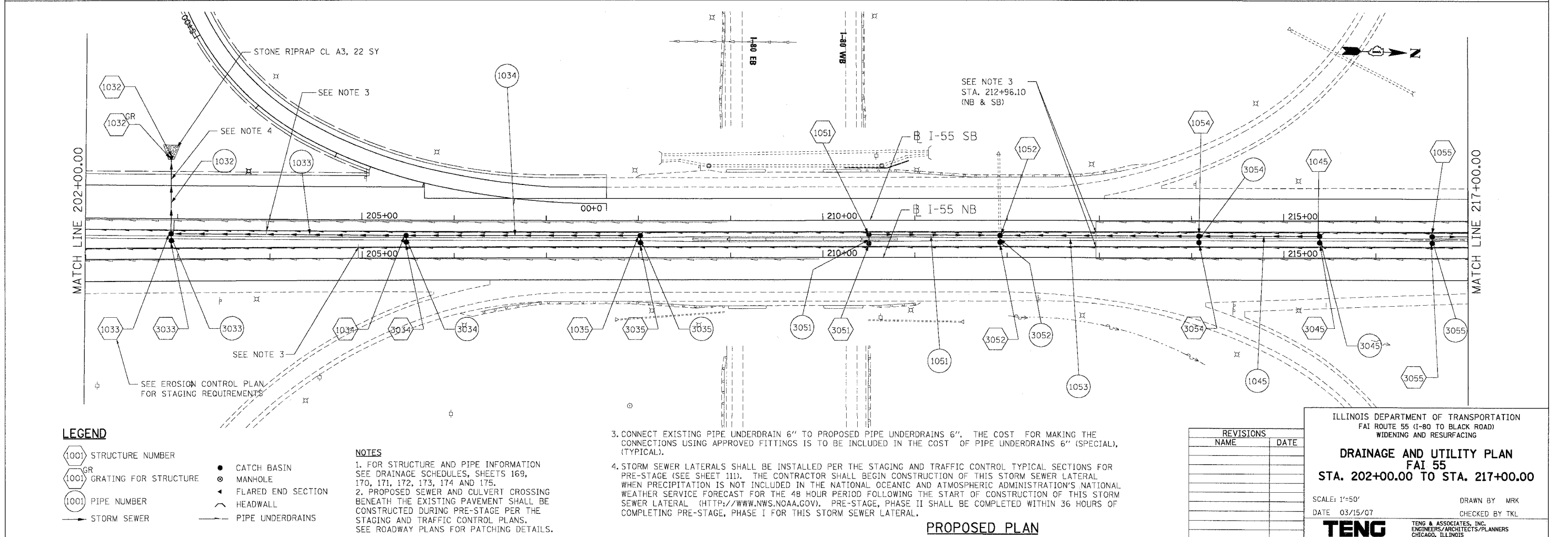
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- ▨ PIPE REMOVAL
- R STRUCTURES TO BE REMOVED
- C STRUCTURES TO BE CLEANED

**NOTES**

1. DURING THE PRE-STAGE, THE CONTRACTOR SHALL REMOVE AND RE-ERECT THE MINIMUM LENGTH OF GUARDRAIL NECESSARY TO CONSTRUCT THE STORM SEWER LATERAL AS DIRECTED BY THE ENGINEER. THE MAXIMUM PAY LENGTH PER LOCATION FOR REMOVE AND RE-ERECT STEEL PLATE BEAM GUARD RAIL, TYPE A WILL BE 25 FEET. THE CONTRACTOR SHALL RE-ERECT THE GUARDRAIL PRIOR TO REMOVING THE TRAFFIC CONTROL ASSOCIATED WITH THE CONSTRUCTION OF THE STORM SEWER LATERAL AND OPENING THE CLOSED LANE TO TRAFFIC.

**EXISTING PLAN**



**LEGEND**

- 1001 STRUCTURE NUMBER
- 1001GR GRATING FOR STRUCTURE
- 1001 PIPE NUMBER
- STORM SEWER
- CATCH BASIN
- MANHOLE
- ▲ FLARED END SECTION
- ~ HEADWALL
- PIPE UNDERDRAINS

**NOTES**

1. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.  
 2. PROPOSED SEWER AND CULVERT CROSSING BENEATH THE EXISTING PAVEMENT SHALL BE CONSTRUCTED DURING PRE-STAGE PER THE STAGING AND TRAFFIC CONTROL PLANS. SEE ROADWAY PLANS FOR PATCHING DETAILS.

- 3. CONNECT EXISTING PIPE UNDERDRAIN 6" TO PROPOSED PIPE UNDERDRAINS 6". THE COST FOR MAKING THE CONNECTIONS USING APPROVED FITTINGS IS TO BE INCLUDED IN THE COST OF PIPE UNDERDRAINS 6" (SPECIAL), (TYPICAL).
- 4. STORM SEWER LATERALS SHALL BE INSTALLED PER THE STAGING AND TRAFFIC CONTROL TYPICAL SECTIONS FOR PRE-STAGE (SEE SHEET 111). THE CONTRACTOR SHALL BEGIN CONSTRUCTION OF THIS STORM SEWER LATERAL WHEN PRECIPITATION IS NOT INCLUDED IN THE NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION'S NATIONAL WEATHER SERVICE FORECAST FOR THE 48 HOUR PERIOD FOLLOWING THE START OF CONSTRUCTION OF THIS STORM SEWER LATERAL (HTTP://WWW.NWS.NOAA.GOV). PRE-STAGE, PHASE II SHALL BE COMPLETED WITHIN 36 HOURS OF COMPLETING PRE-STAGE, PHASE I FOR THIS STORM SEWER LATERAL.

**PROPOSED PLAN**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
 FAI 55  
 STA. 202+00.00 TO STA. 217+00.00

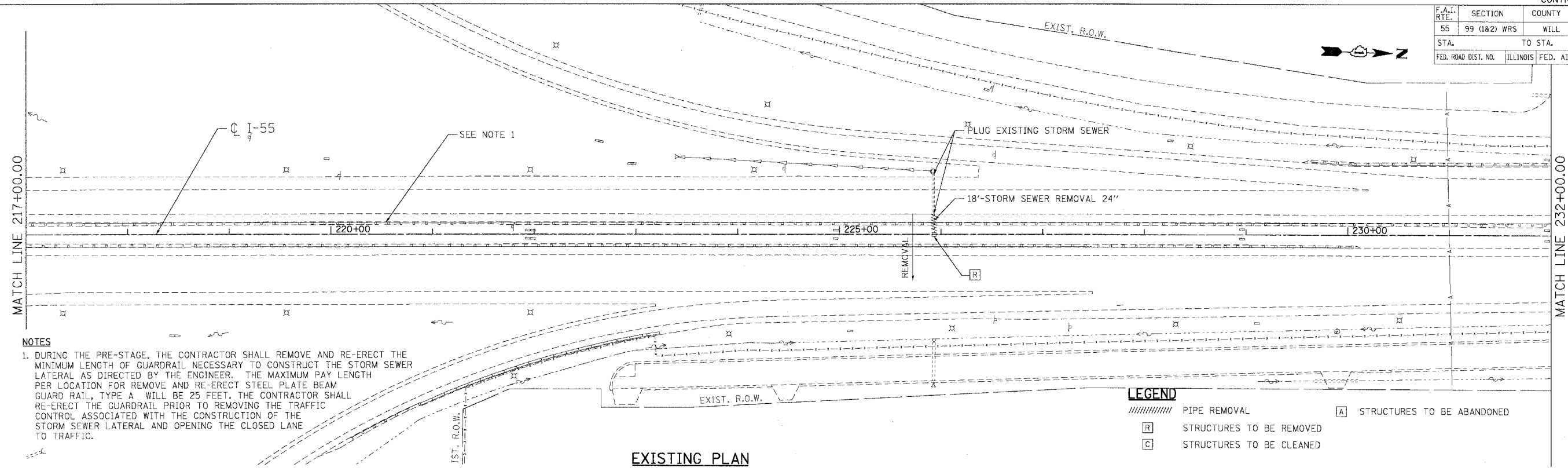
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**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS-PLANNERS  
 CHICAGO, ILLINOIS

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F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



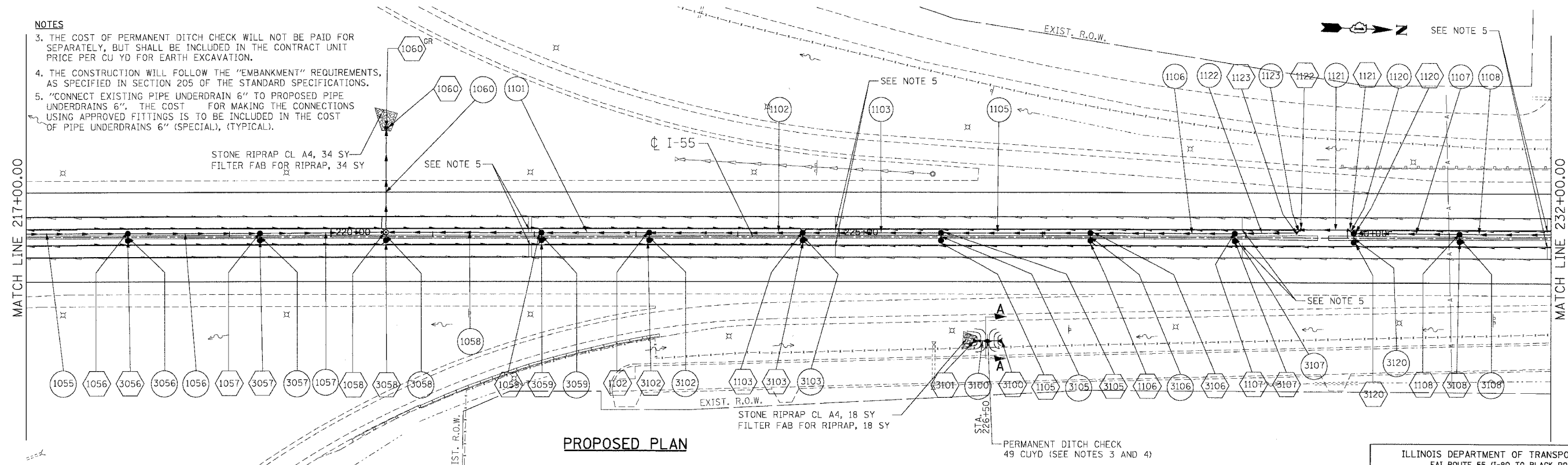
**NOTES**

1. DURING THE PRE-STAGE, THE CONTRACTOR SHALL REMOVE AND RE-ERECT THE MINIMUM LENGTH OF GUARDRAIL NECESSARY TO CONSTRUCT THE STORM SEWER LATERAL AS DIRECTED BY THE ENGINEER. THE MAXIMUM PAY LENGTH PER LOCATION FOR REMOVE AND RE-ERECT STEEL PLATE BEAM GUARD RAIL, TYPE A WILL BE 25 FEET. THE CONTRACTOR SHALL RE-ERECT THE GUARDRAIL PRIOR TO REMOVING THE TRAFFIC CONTROL ASSOCIATED WITH THE CONSTRUCTION OF THE STORM SEWER LATERAL AND OPENING THE CLOSED LANE TO TRAFFIC.

**LEGEND**

//////	PIPE REMOVAL	[A]	STRUCTURES TO BE ABANDONED
[R]	STRUCTURES TO BE REMOVED	[C]	STRUCTURES TO BE CLEANED

**EXISTING PLAN**



**NOTES**

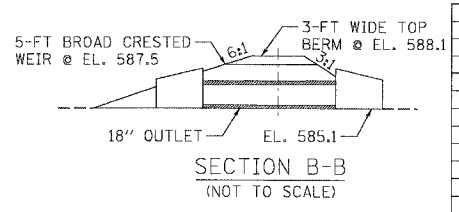
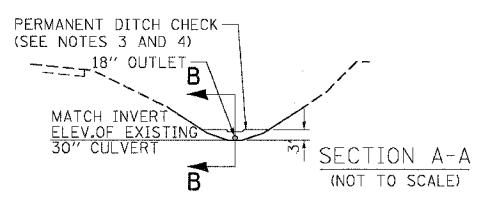
3. THE COST OF PERMANENT DITCH CHECK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER CU YD FOR EARTH EXCAVATION.
4. THE CONSTRUCTION WILL FOLLOW THE "EMBANKMENT" REQUIREMENTS, AS SPECIFIED IN SECTION 205 OF THE STANDARD SPECIFICATIONS.
5. "CONNECT EXISTING PIPE UNDERDRAIN 6" TO PROPOSED PIPE UNDERDRAINS 6". THE COST FOR MAKING THE CONNECTIONS USING APPROVED FITTINGS IS TO BE INCLUDED IN THE COST OF PIPE UNDERDRAINS 6" (SPECIAL), (TYPICAL).

**LEGEND**

(1001)	STRUCTURE NUMBER	●	CATCH BASIN
(1001)GR	GRATING FOR STRUCTURE	○	MANHOLE
(1001)	PIPE NUMBER	◄	FLARED END SECTION
→	STORM SEWER	—	HEADWALL
		—	PIPE UNDERDRAINS

**NOTES**

1. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.
2. PROPOSED SEWER AND CULVERT CROSSING BENEATH THE EXISTING PAVEMENT SHALL BE CONSTRUCTED DURING PRE-STAGE PER THE STAGING AND TRAFFIC CONTROL PLANS. SEE ROADWAY PLANS FOR PATCHING DETAILS.



**REVISIONS**

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
**FAI 55**  
**STA. 217+00.00 TO STA. 232+00.00**

SCALE: 1"=50'  
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 CHECKED BY: TKL

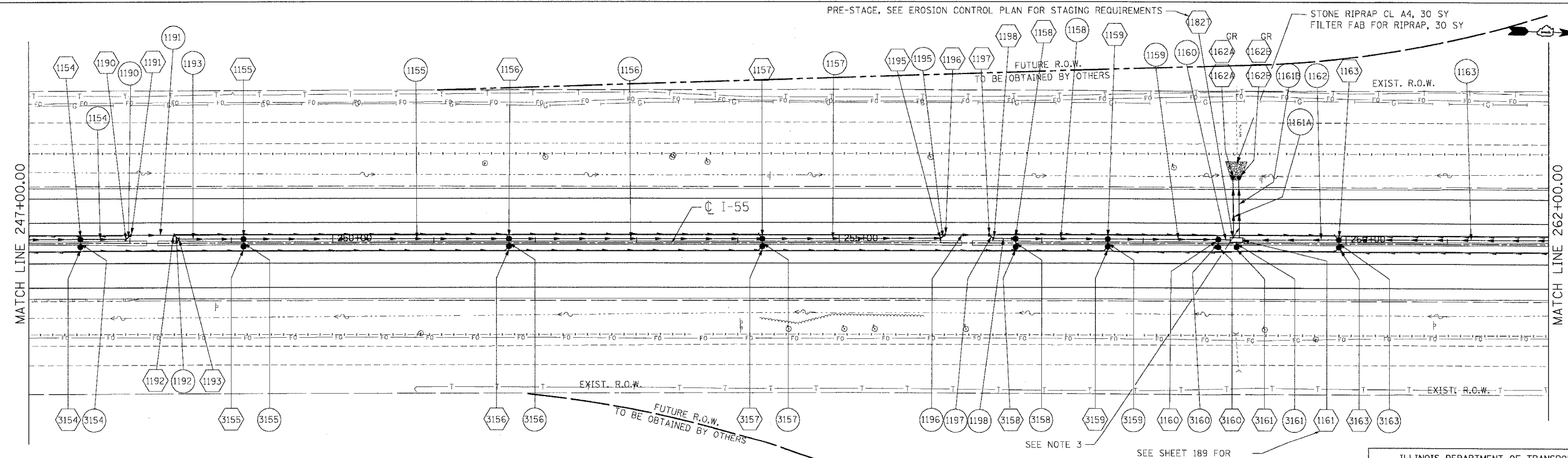
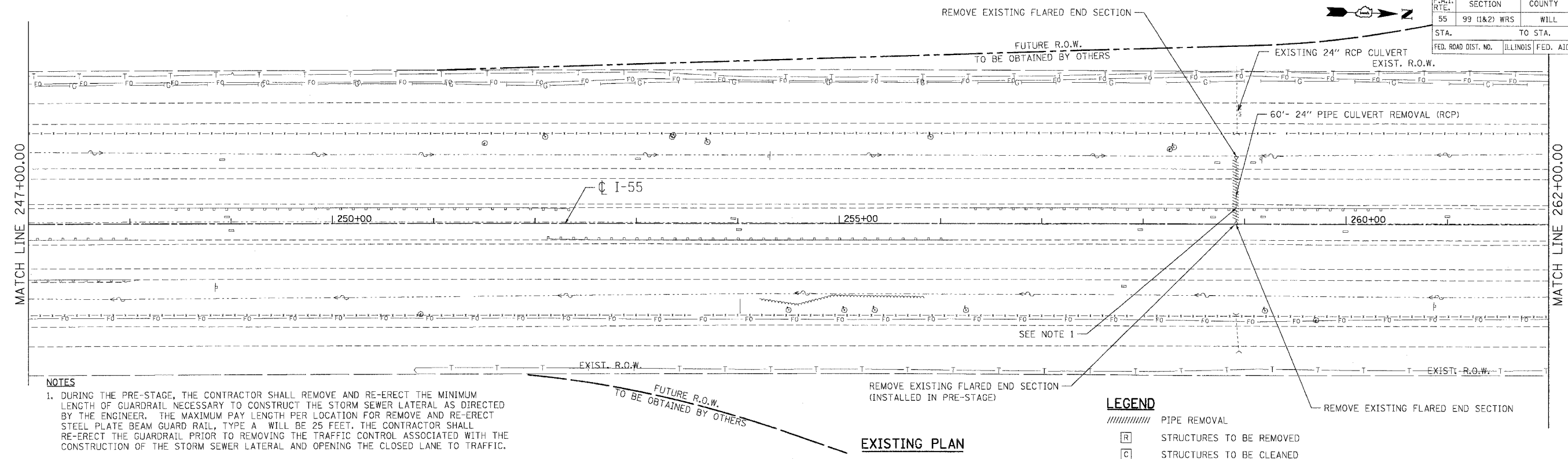
**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE: 03/15/07  
 FILE NAME: \\s:\projects\62895\62895.dwg  
 PLOT SCALE: 1"=50'  
 USER NAME: jk  
 3-15-2007, 10:30:41 AM





F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	158
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.				



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-90 TO BLACK ROAD)  
WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
FAI 55  
STA. 247+00.00 TO STA. 262+00.00

SCALE: 1"=50'  
DATE: 03/15/07

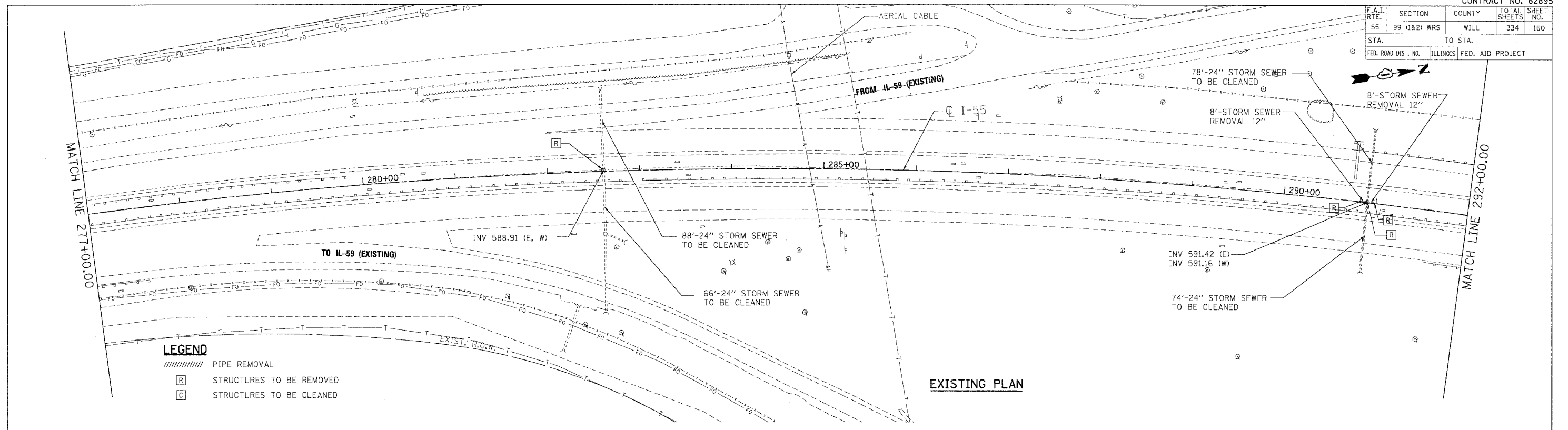
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CHECKED BY: TKL

**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE = 04/05/07  
FILE NAME = 031507.DWG  
SCALE = 1/8"=1'-0"  
USER NAME = 0155518



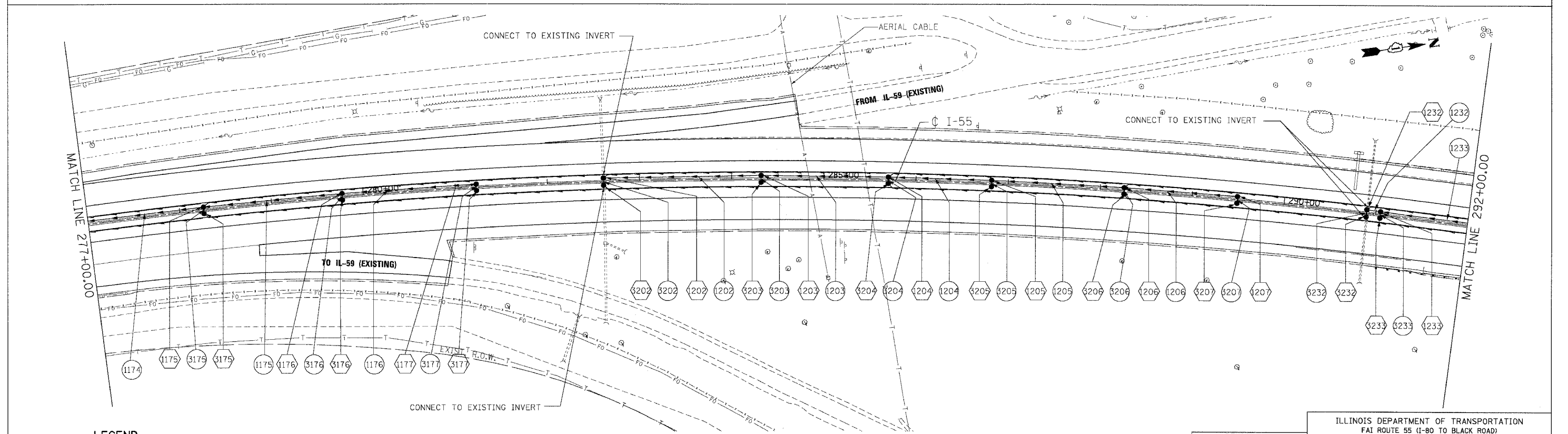
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	160
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.				



**LEGEND**

////	PIPE REMOVAL
[R]	STRUCTURES TO BE REMOVED
[C]	STRUCTURES TO BE CLEANED

**EXISTING PLAN**



**LEGEND**

(1001)	STRUCTURE NUMBER	●	CATCH BASIN
GR	GRATING FOR STRUCTURE	○	MANHOLE
(1001)	PIPE NUMBER	▲	FLARED END SECTION
→	STORM SEWER	⌋	HEADWALL
		—	PIPE UNDERDRAINS

**NOTES**

- FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.
- PROPOSED SEWER AND CULVERT CROSSING BENEATH THE EXISTING PAVEMENT SHALL BE CONSTRUCTED DURING PRE-STAGE PER THE STAGING AND TRAFFIC CONTROL PLANS. SEE ROADWAY PLANS FOR PATCHING DETAILS.

**PROPOSED PLAN**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN  
 FAI 55  
 STA. 277+00.00 TO STA. 292+00.00**

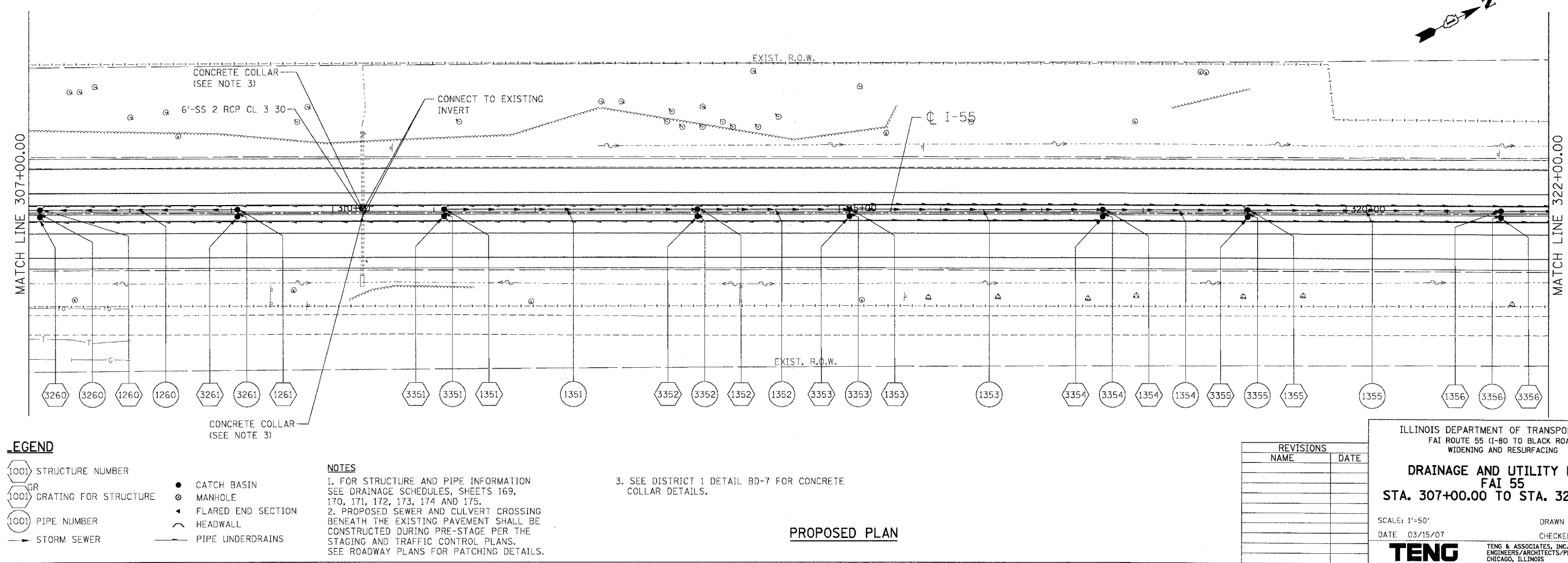
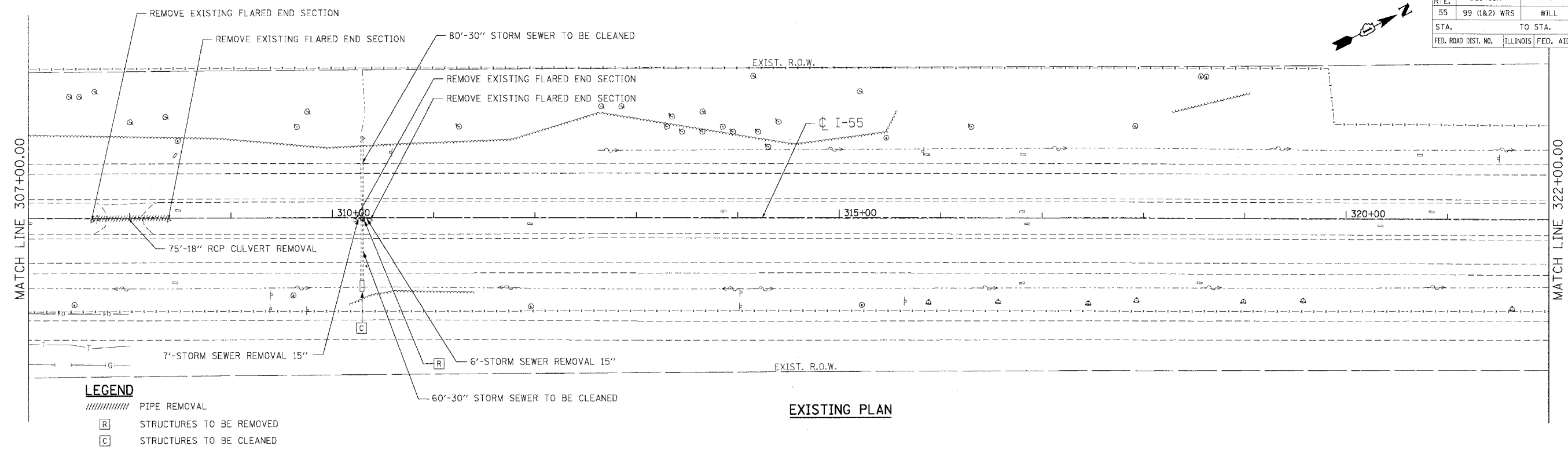
SCALE: 1"=50'  
 DATE 03/15/07  
 DRAWN BY WRK  
 CHECKED BY TKL

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 04/01/07  
 FILE NAME = 81515.DWG  
 PLOT SCALE = 1"=50'  
 USER NAME = 40598



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	162
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
**FAI 55**  
**STA. 307+00.00 TO STA. 322+00.00**

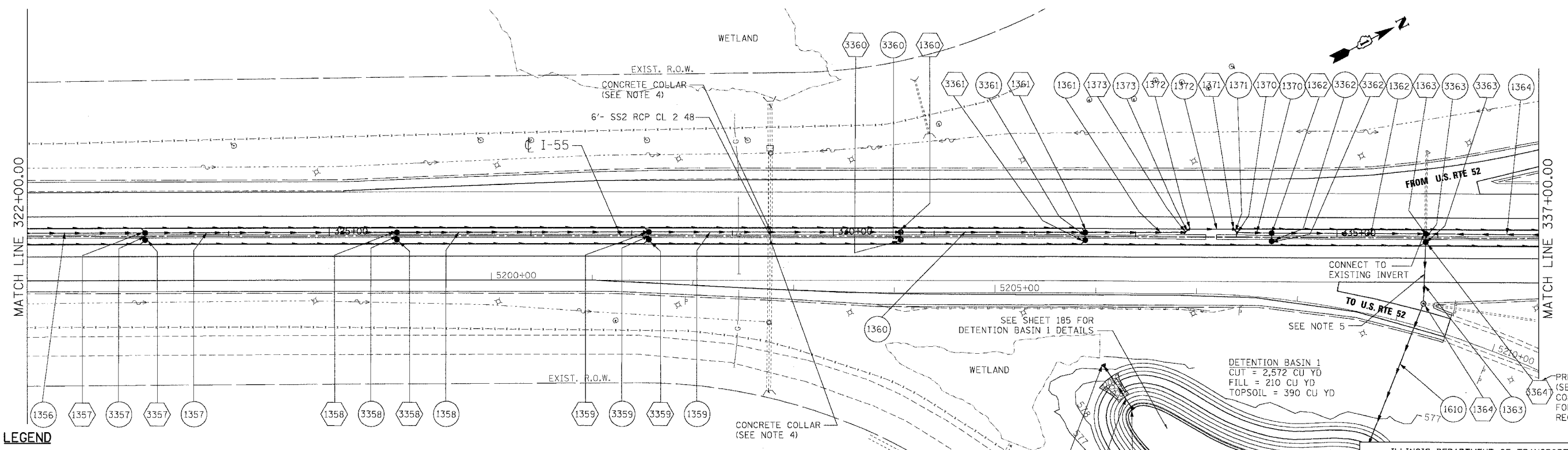
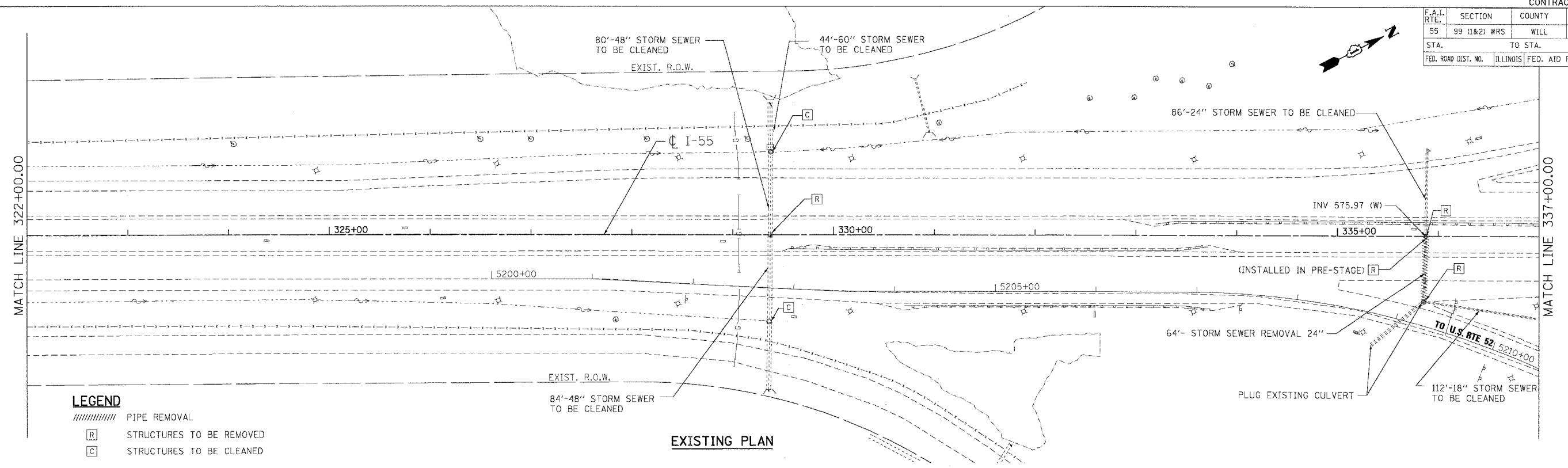
SCALE: 1"=50'  
 DATE: 03/15/07

DRAWN BY: MRK  
 CHECKED BY: TKL

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE: 03/15/07  
 PLOT SCALE: 1"=50'  
 USER NAME: KUSHER

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	163
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
FAI 55  
STA. 322+00.00 TO STA. 337+00.00

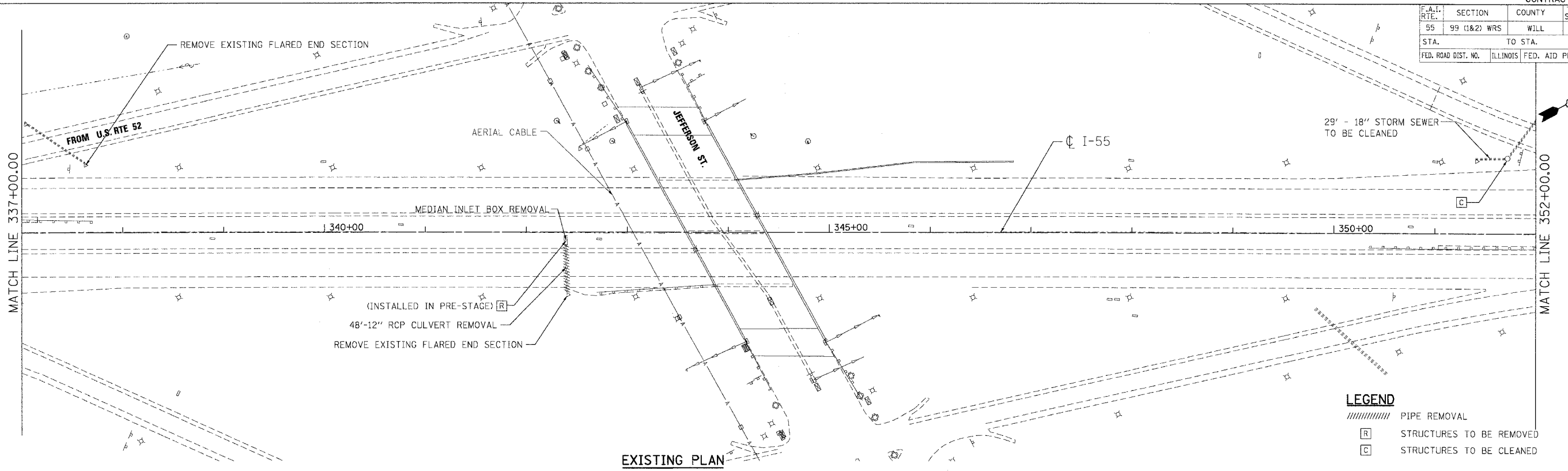
SCALE: 1"=50'  
DATE 03/15/07

DRAWN BY MRK  
CHECKED BY TKL

**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

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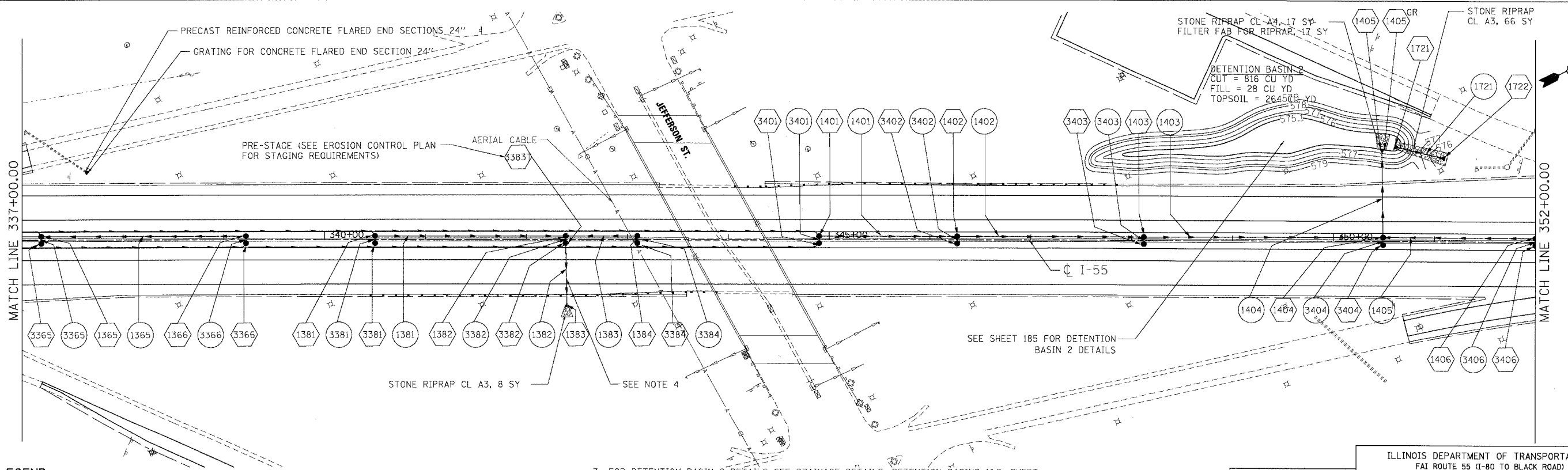
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	164
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



EXISTING PLAN

LEGEND

- ////// PIPE REMOVAL
- [R] STRUCTURES TO BE REMOVED
- [C] STRUCTURES TO BE CLEANED



PROPOSED PLAN

LEGEND

- (1001) STRUCTURE NUMBER
- (1001) GR GRATING FOR STRUCTURE
- (1001) PIPE NUMBER
- STORM SEWER
- CATCH BASIN
- MANHOLE
- ▲ FLARED END SECTION
- ⌋ HEADWALL
- PIPE UNDERDRAINS

NOTES

1. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.
2. PROPOSED SEWER AND CULVERT CROSSING BENEATH THE EXISTING PAVEMENT SHALL BE CONSTRUCTED DURING PRE-STAGE PER THE STAGING AND TRAFFIC CONTROL PLANS. SEE ROADWAY PLANS FOR PATCHING DETAILS.

3. FOR DETENTION BASIN 2 DETAILS SEE DRAINAGE DETAILS, DETENTION BASINS 1&2, SHEET 185.
4. STORM SEWER LATERALS SHALL BE INSTALLED PER THE STAGING AND TRAFFIC CONTROL TYPICAL SECTIONS FOR PRE-STAGE (SEE SHEET 111). THE CONTRACTOR SHALL BEGIN CONSTRUCTION OF THIS STORM SEWER LATERAL WHEN PRECIPITATION IS NOT INCLUDED IN THE NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION'S NATIONAL WEATHER SERVICE FORECAST FOR THE 48 HOUR PERIOD FOLLOWING THE START OF CONSTRUCTION OF THIS STORM SEWER LATERAL ([HTTP://WWW.NWS.NOAA.GOV](http://www.nws.noaa.gov)). PRE-STAGE, PHASE II SHALL BE COMPLETED WITHIN 36 HOURS OF COMPLETING PRE-STAGE, PHASE I FOR THIS STORM SEWER LATERAL.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
**FAI 55**  
**STA. 337+00.00 TO STA. 352+00.00**

SCALE: 1"=50'  
 DATE 03/15/07

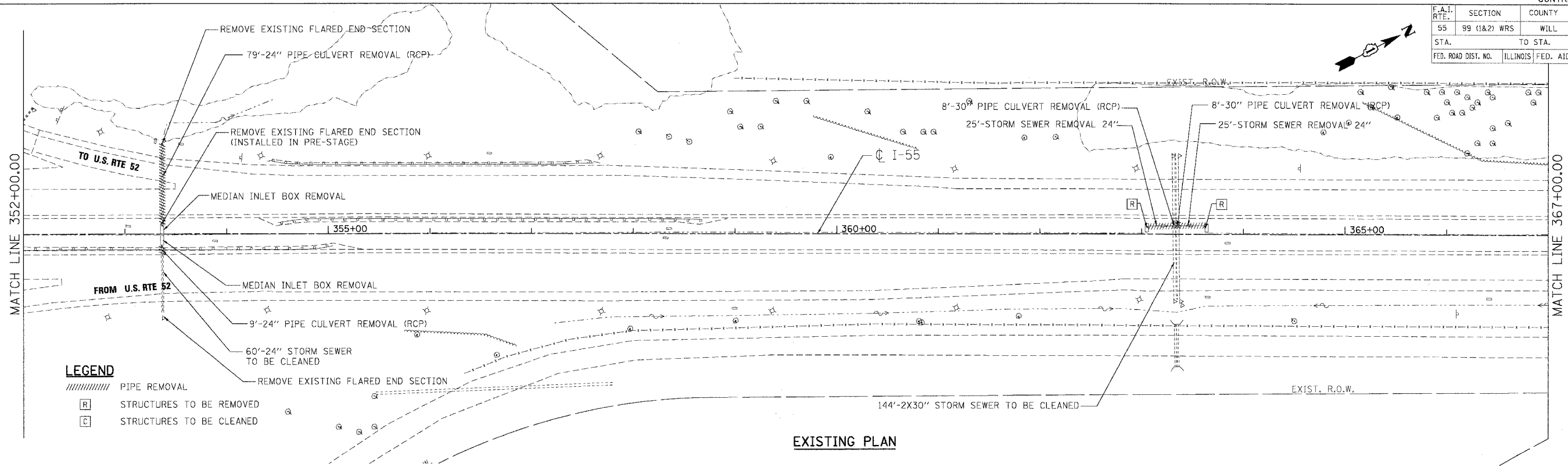
DRAWN BY MRK  
 CHECKED BY TKL

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 03/15/07  
 FILE NAME = 031507.DWG  
 USER = TENG



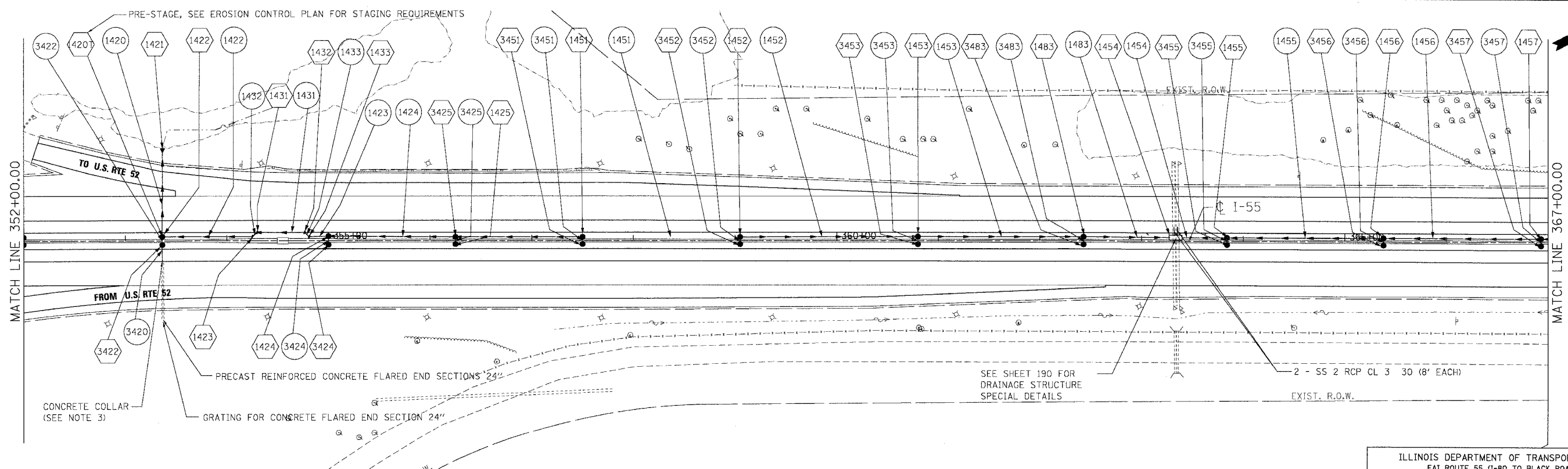
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	165
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.				



**LEGEND**

	PIPE REMOVAL
	STRUCTURES TO BE REMOVED
	STRUCTURES TO BE CLEANED

**EXISTING PLAN**



**LEGEND**

	STRUCTURE NUMBER		GRATING FOR STRUCTURE
	PIPE NUMBER		PIPE UNDERDRAINS
	STORM SEWER		CATCH BASIN
	MANHOLE		FLARED END SECTION
	HEADWALL		PIPE UNDERDRAINS

- NOTES**
- FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.
  - PROPOSED SEWER AND CULVERT CROSSING BENEATH THE EXISTING PAVEMENT SHALL BE CONSTRUCTED DURING PRE-STAGE PER THE STAGING AND TRAFFIC CONTROL PLANS. SEE ROADWAY PLANS FOR PATCHING DETAILS.
  - SEE DISTRICT 1 DETAIL BD-7 FOR CONCRETE COLLAR DETAILS.

**PROPOSED PLAN**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
 FAI 55  
 STA. 352+00.00 TO STA. 367+00.00

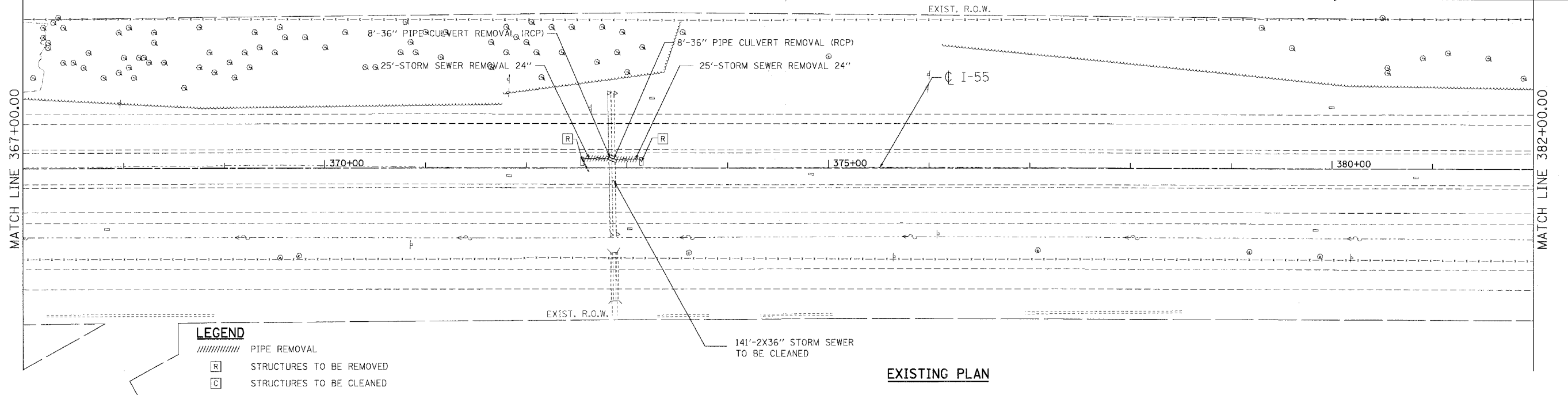
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 DATE: 03/15/07

DRAWN BY: MRK  
 CHECKED BY: TKL

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

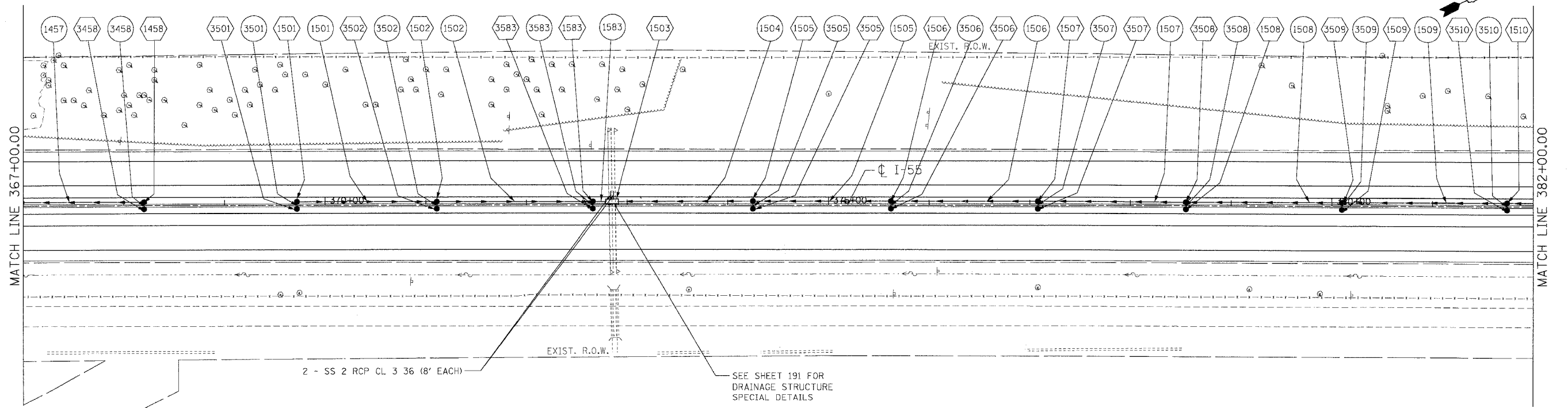
PLOT DATE = 03/15/07  
 FILE NAME = 031507.DWG  
 USER NAME = TENG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	166
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**LEGEND**  
 [Hatched] PIPE REMOVAL  
 [R] STRUCTURES TO BE REMOVED  
 [C] STRUCTURES TO BE CLEANED

**EXISTING PLAN**



**LEGEND**  
 (1001) STRUCTURE NUMBER  
 GR (1001) GRATING FOR STRUCTURE  
 (1001) PIPE NUMBER  
 → STORM SEWER  
 ● CATCH BASIN  
 ○ MANHOLE  
 ▲ FLARED END SECTION  
 ) HEADWALL  
 - PIPE UNDERDRAINS

**NOTES**  
 1. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.  
 2. PROPOSED SEWER AND CULVERT CROSSING BENEATH THE EXISTING PAVEMENT SHALL BE CONSTRUCTED DURING PRE-STAGE PER THE STAGING AND TRAFFIC CONTROL PLANS. SEE ROADWAY PLANS FOR PATCHING DETAILS.

**PROPOSED PLAN**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
 FAI 55  
 STA. 367+00.00 TO STA. 382+00.00

SCALE: 1"=50'  
 DATE: 03/15/07  
 DRAWN BY: MRK  
 CHECKED BY: TKL

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE: 03/15/07  
 FILE NAME: \\N:\projects\62895\DWG\DRN\DRN\_166.dwg  
 USER: MRK

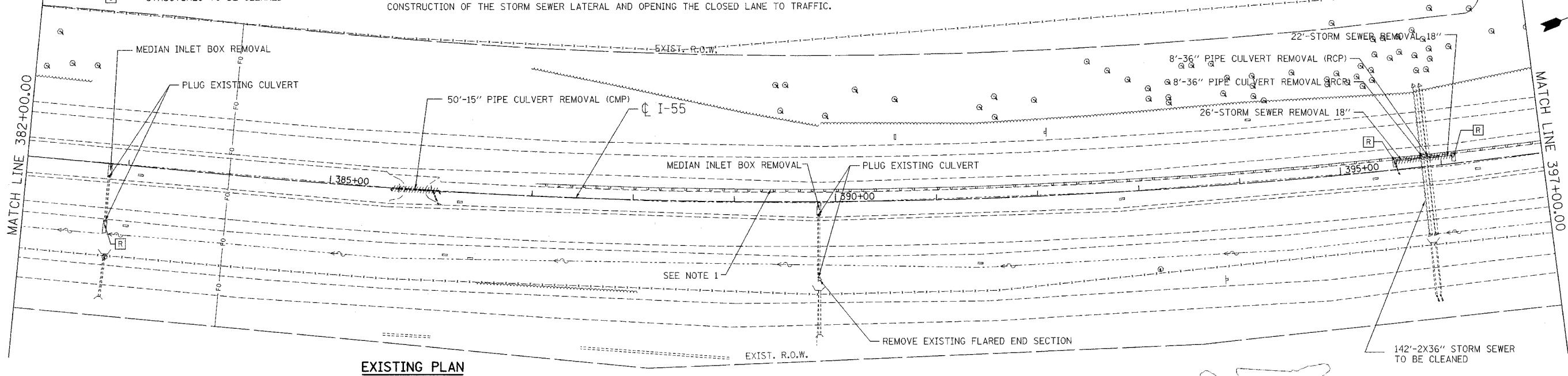
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	167
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

### LEGEND

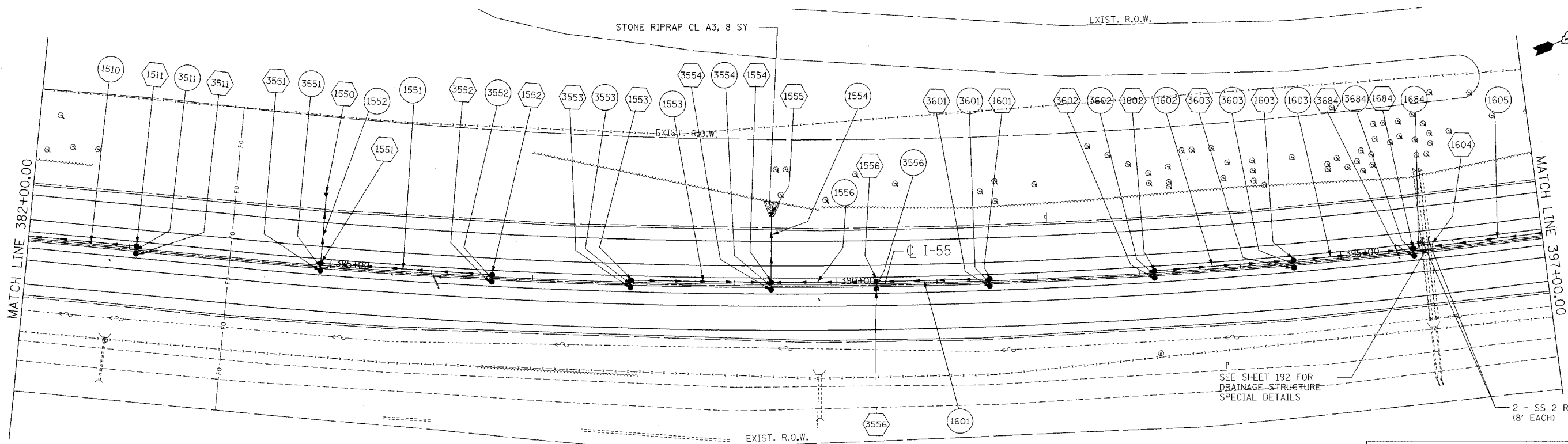
- ||||| PIPE REMOVAL
- [R] STRUCTURES TO BE REMOVED
- [C] STRUCTURES TO BE CLEANED

### NOTES

1. DURING THE PRE-STAGE, THE CONTRACTOR SHALL REMOVE AND RE-ERECT THE MINIMUM LENGTH OF GUARDRAIL NECESSARY TO CONSTRUCT THE STORM SEWER LATERAL AS DIRECTED BY THE ENGINEER. THE MAXIMUM PAY LENGTH PER LOCATION FOR REMOVE AND RE-ERECT STEEL PLATE BEAM GUARDRAIL, TYPE A WILL BE 25 FEET. THE CONTRACTOR SHALL RE-ERECT THE GUARDRAIL PRIOR TO REMOVING THE TRAFFIC CONTROL ASSOCIATED WITH THE CONSTRUCTION OF THE STORM SEWER LATERAL AND OPENING THE CLOSED LANE TO TRAFFIC.



### EXISTING PLAN



### PROPOSED PLAN

### LEGEND

- (1001) STRUCTURE NUMBER
- (GR) GRATING FOR STRUCTURE
- (1001) PIPE NUMBER
- STORM SEWER
- CATCH BASIN
- ⊙ MANHOLE
- ▲ FLARED END SECTION
- ⤷ HEADWALL
- PIPE UNDERDRAINS

### NOTES

1. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.  
 2. PROPOSED SEWER AND CULVERT CROSSING BENEATH THE EXISTING PAVEMENT SHALL BE CONSTRUCTED DURING PRE-STAGE PER THE STAGING AND TRAFFIC CONTROL PLANS. SEE ROADWAY PLANS FOR PATCHING DETAILS.

### REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN  
 FAI 55**

**STA. 382+00.00 TO STA. 397+00.00**

SCALE: 1"=50'  
 DATE: 03/22/07

DRAWN BY: MRK  
 CHECKED BY: TKL

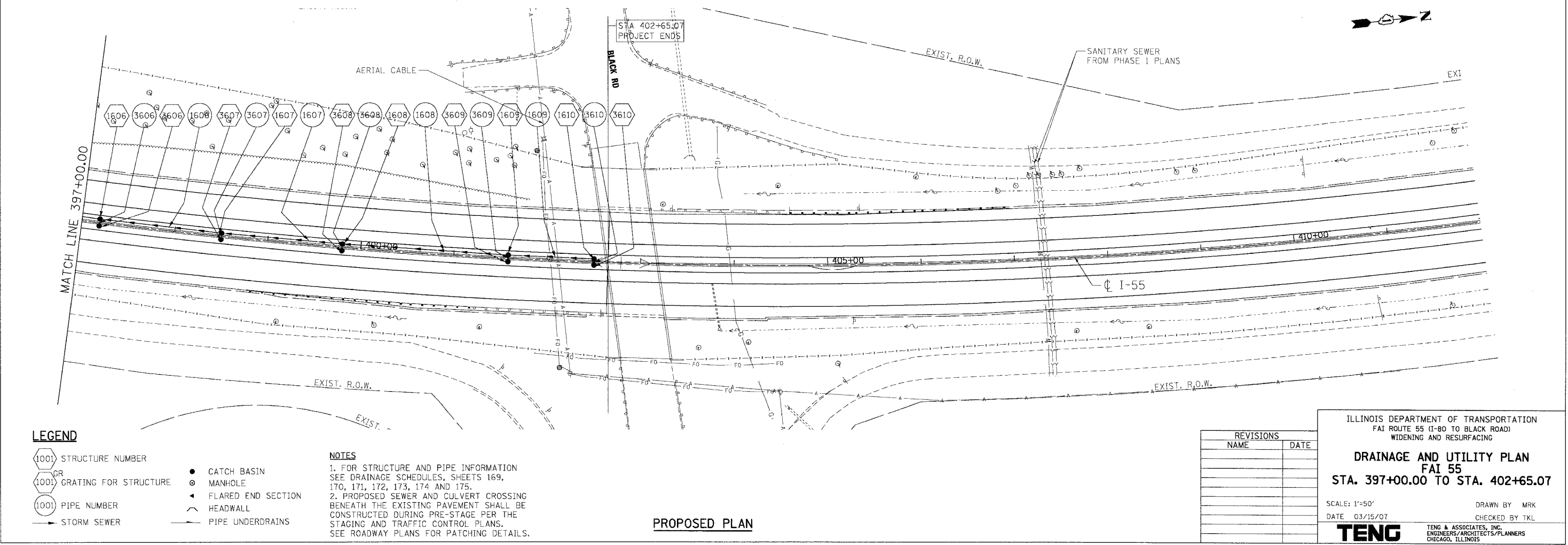
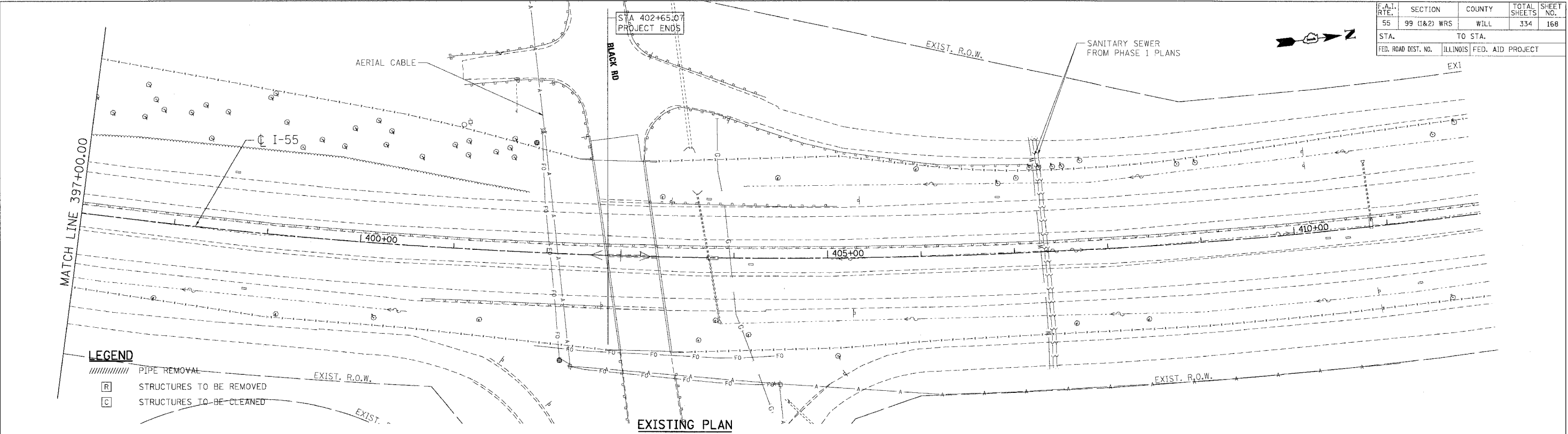
**TENG** TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 03/22/07  
 PLOT SCALE = 1"=50'  
 USER NAME = MURPHY  
 PLOT SCALE = 1"=50'  
 PLOT DATE = 03/22/07  
 USER NAME = MURPHY

PLOT DATE = 04/15/07  
 FILE NAME = I:\PROJECTS\031100\DRAWING\031100.DWG  
 USER = MURPHY

CONTRACT NO. 62895

F.A.I. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	168
STA. 397+00.00		TO STA. 402+65.07		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE AND UTILITY PLAN**  
 FAI 55  
 STA. 397+00.00 TO STA. 402+65.07

SCALE: 1"=50'  
 DATE: 03/15/07

DRAWN BY: MRK  
 CHECKED BY: TKL

**TENG** ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	169
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

STRUCTURE NO.	STRUCTURE DESCRIPTION	CENTERLINE			INVERTS				PIPE CONNECTIONS (Pipe No. - Pipe Size)				PROPOSED RIM ELEVATION	SHEET NO.	SC INLET FILTER	UNDERDRAIN CONNECTION	STRUCTURE TOP SLAB	TOP SLAB* CONFLICT					
		STATION	OFFSET	DIR.	NORTH	SOUTH	EAST	WEST	NORTH	SOUTH	EAST	WEST											
1020	CB TA 4 DIA T9F&G	189+55	NB 31.4	LT	562.50	562.02	--	564.00					1020 - DIA 18"	1019 - DIA 18"			1020 - DIA 15"	569.63	154	YES	--	TAPERED	--
1021	CB TA 4 DIA T20F&G	189+55	SB 23.2	RT	--	--	564.15	--					--	--			1020 - DIA 15"	572.84	154	YES	--	TAPERED	--
1022	CB TA 4 DIA T9F&G	191+60	NB 24.0	LT	566.27	565.52	--	568.00					1023 - DIA 15"	1021 - DIA 18"			1022 - DIA 15"	574.76	154	YES	YES	TAPERED	--
1023	CB TA 4 DIA T20F&G	191+60	SB 24.0	RT	--	--	568.52	--					--	--			1022 - DIA 15"	575.06	154	YES	YES	TAPERED	--
1024	CB TA 4 DIA T20F&G	193+60	SB 24.0	RT	--	--	569.88	--					--	--			1022 - DIA 15"	576.57	154	YES	YES	TAPERED	--
1026	CB TA 5 DIA T20F&G	195+93	SB 24.0	RT	574.04	--	574.79	574.04					1026 - DIA 24"	--			1025 - DIA 24"	578.37	154	YES	YES	FLAT	YES
1027	CB TA 5 DIA T20F&G	198+33	SB 24.0	RT	576.20	575.45	576.20	--					1027 - DIA 15"	1026 - DIA 24"			--	580.10	154	YES	--	FLAT	YES
1028	CB TA 4 DIA T20F&G	200+70	SB 19.9	RT	--	577.83	577.95	--					--	1027 - DIA 15"			--	582.04	154	YES	YES	FLAT	--
1032	PRC FLAR END SEC 24	202+92	SB 64.0	LT	--	--	579.25	--					--	--			--	583.70	155	--	--	FLAT	YES
1033	CB TA 5 DIA T21F&G	202+93	SB 18.5	RT	579.90	--	580.15	579.40					1033 - DIA 18"	--			1032 - DIA 24"	583.70	155	YES	YES	FLAT	YES
1034	CB TA 4 DIA T20F&G	205+48	SB 18.5	RT	581.55	581.30	581.55	--					1034 - DIA 15"	1033 - DIA 18"			1034 - DIA 15"	585.51	155	YES	--	FLAT	--
1035	CB TA 4 DIA T20F&G	208+02	SB 18.1	RT	--	582.80	582.80	--					--	1034 - DIA 15"			--	586.89	155	YES	--	FLAT	--
1045	CB TA 4 DIA T20F&G	215+39	SB 18.5	RT	--	585.59	585.59	--					1045 - DIA 15"	1045 - DIA 15"			--	588.93	155	YES	YES	FLAT	YES
1051	CB TA 4 DIA T20F&G	210+50	SB 17.1	RT	584.56	--	584.60	--					1051 - DIA 12"	--			1051 - DIA 12"	587.76	155	YES	--	FLAT	YES
1052	CB TA 5 DIA T20F&G	211+92	SB 18.5	RT	584.11	584.05	584.57	583.82					1051 - DIA 12"	1051 - DIA 12"			EP - DIA 24"	588.36	155	YES	YES	FLAT	YES
1054	CB TA 4 DIA T20F&G	214+08	SB 18.5	RT	585.21	584.96	585.42	--					1045 - DIA 15"	1053 - DIA 18"			--	588.81	155	YES	--	FLAT	YES
1055	CB TA 4 DIA T20F&G	216+61	1.5	LT	585.72	--	585.72	--					1055 - DIA 15"	--			--	589.06	155	YES	--	FLAT	YES
1056	CB TA 4 DIA T20F&G	218+00	1.5	LT	584.97	585.25	585.25	--					1056 - DIA 18"	1055 - DIA 15"			--	589.10	156	YES	--	FLAT	--
1057	CB TA 4 DIA T20F&G	219+30	1.5	LT	584.60	584.60	584.85	--					1057 - DIA 18"	1056 - DIA 18"			--	589.09	156	YES	--	FLAT	--
1058	MAN TA 6 DIA T20F&G	220+54	1.5	LT	582.97	584.00	584.75	582.97	1058 - SPAN 38", RISE 24"	1057 - DIA 18"	1058 - SPAN 38", RISE 24"		1057 - DIA 18"	1057 - DIA 18"			3058 - DIA 15"	589.14	156	YES	YES	FLAT	--
1059	CB TA 5 DIA T20F&G	222+07	1.5	LT	583.20	583.19	583.94	--	1101 - SPAN 38", RISE 24"	1058 - SPAN 38", RISE 24"	1058 - SPAN 38", RISE 24"		1057 - DIA 18"	1057 - DIA 18"			3059 - DIA 15"	589.11	156	YES	YES	FLAT	--
1060	PRC FLAR END SEC 36	220+54	105.0	LT	--	--	582.87	--					--	--			--	589.10	156	--	--	FLAT	--
1102	CB TA 5 DIA T20F&G	223+13	1.5	LT	583.34	583.34	584.09	--	1102 - SPAN 38", RISE 24"	1101 - SPAN 38", RISE 24"	1102 - SPAN 38", RISE 24"		1102 - SPAN 38", RISE 24"	1102 - SPAN 38", RISE 24"			3102 - DIA 15"	589.08	156	YES	YES	FLAT	--
1103	CB TA 5 DIA T20F&G	224+64	1.5	LT	583.53	583.53	584.24	--	1103 - SPAN 38", RISE 24"	1102 - SPAN 38", RISE 24"	1103 - SPAN 38", RISE 24"		1103 - SPAN 38", RISE 24"	1103 - SPAN 38", RISE 24"			3103 - DIA 15"	589.08	156	YES	YES	FLAT	--
1105	CB TA 5 DIA T20F&G	226+00	1.5	LT	583.70	583.70	584.45	--	1105 - SPAN 38", RISE 24"	1103 - SPAN 38", RISE 24"	1105 - SPAN 38", RISE 24"		1105 - SPAN 38", RISE 24"	1105 - SPAN 38", RISE 24"			3105 - DIA 15"	589.13	156	YES	--	FLAT	--
1106	CB TA 5 DIA T20F&G	227+47	1.5	LT	583.89	583.89	584.50	--	1106 - SPAN 38", RISE 24"	1105 - SPAN 38", RISE 24"	1106 - SPAN 38", RISE 24"		1106 - SPAN 38", RISE 24"	1106 - SPAN 38", RISE 24"			3106 - DIA 15"	589.18	156	YES	--	FLAT	--
1107	CB TA 5 DIA T20F&G	228+89	1.5	LT	584.06	584.06	584.75	--	1122 - DIA 24"	1106 - SPAN 38", RISE 24"	1107 - SPAN 38", RISE 24"		1107 - SPAN 38", RISE 24"	1107 - SPAN 38", RISE 24"			3107 - DIA 15"	589.17	156	YES	--	FLAT	--
1108	CB TA 5 DIA T20F&G	231+10	1.5	LT	584.74	584.74	585.08	--	1108 - SPAN 30", RISE 19"	1107 - SPAN 30", RISE 19"	1108 - SPAN 30", RISE 19"		1108 - SPAN 30", RISE 19"	1108 - SPAN 30", RISE 19"			3108 - DIA 15"	589.19	156	YES	--	FLAT	--
1109	CB TA 5 DIA T20F&G	232+56	1.5	LT	585.02	585.02	585.35	--	1109 - SPAN 30", RISE 19"	1108 - SPAN 30", RISE 19"	1109 - SPAN 30", RISE 19"		1109 - SPAN 30", RISE 19"	1109 - SPAN 30", RISE 19"			3109 - DIA 15"	589.32	157	YES	--	FLAT	--
1110	CB TA 5 DIA T20F&G	234+13	1.5	LT	585.33	585.33	585.67	--	1110 - SPAN 30", RISE 19"	1109 - SPAN 30", RISE 19"	1110 - SPAN 30", RISE 19"		1110 - SPAN 30", RISE 19"	1110 - SPAN 30", RISE 19"			3110 - DIA 15"	589.50	157	YES	YES	FLAT	YES
1111	CB TA 5 DIA T20F&G	235+31	1.5	LT	585.84	585.84	586.81	--	1111 - SPAN 23", RISE 14"	1110 - SPAN 30", RISE 19"	1111 - SPAN 30", RISE 19"		1111 - SPAN 30", RISE 19"	1111 - SPAN 30", RISE 19"			3111 - DIA 15"	589.64	157	YES	--	FLAT	YES
1112	CB TA 4 DIA T20F&G	237+00	1.5	LT	586.21	586.21	586.21	--	1112 - DIA 15"	1111 - SPAN 23", RISE 14"	1112 - DIA 15"		1112 - DIA 15"	1112 - DIA 15"			3112 - DIA 15"	589.82	157	YES	--	FLAT	YES
1113	CB TA 4 DIA T20F&G	238+59	1.5	LT	586.64	586.64	586.64	--	1113 - DIA 12"	1112 - DIA 15"	1113 - DIA 12"		1113 - DIA 12"	1113 - DIA 12"			3113 - DIA 15"	590.00	157	YES	YES	FLAT	YES
1114	CB TA 4 DIA T20F&G	240+16	1.5	LT	--	--	587.10	--	--	1113 - DIA 12"	1113 - DIA 12"		1113 - DIA 12"	1113 - DIA 12"			3114 - DIA 12"	590.12	157	YES	--	FLAT	YES
1120	CB TA 5 DIA T20F&G	230+06	2.1	LT	584.54	584.29	585.00	--	1107 - SPAN 30", RISE 19"	1120 - DIA 24"	1120 - DIA 24"		1120 - DIA 24"	1120 - DIA 24"			3120 - DIA 15"	589.25	156	YES	YES	FLAT	--
1121	R C PIPE ELBOW 24	230+00	8.0	LT	584.28	584.28	--	--	1120 - DIA 24"	1121 - DIA 24"	1121 - DIA 24"		1121 - DIA 24"	1121 - DIA 24"			--	590.12	157	--	--	FLAT	YES
1122	R C PIPE ELBOW 24	229+54	8.0	LT	584.19	584.19	--	--	1121 - DIA 24"	1122 - DIA 24"	1122 - DIA 24"		1122 - DIA 24"	1122 - DIA 24"			--	590.12	157	--	--	FLAT	YES
1123	R C PIPE ELBOW 24	229+50	5.0	LT	584.18	584.18	--	--	1123 - DIA 24"	1122 - DIA 24"	1122 - DIA 24"		1122 - DIA 24"	1122 - DIA 24"			--	590.12	157	--	--	FLAT	YES
1150	CB TA 4 DIA T20F&G	241+66	1.5	LT	586.61	--	586.75	--	--	1150 - DIA 15"	--		1150 - DIA 15"	1150 - DIA 15"			3150 - DIA 15"	590.27	157	YES	--	FLAT	YES
1151	CB TA 4 DIA T20F&G	243+16	1.5	LT	586.20	586.20	586.31	--	1151 - DIA 15"	1150 - DIA 15"	1151 - DIA 15"		1151 - DIA 15"	1151 - DIA 15"			3151 - DIA 15"	590.50	157	YES	YES	FLAT	--
1152	CB TA 4 DIA T20F&G	244+65	1.5	LT	585.79	585.79	585.90	--	1152 - DIA 15"	1151 - DIA 15"	1152 - DIA 15"		1152 - DIA 15"	1152 - DIA 15"			3152 - DIA 15"	590.61	157	YES	--	FLAT	--
1153	CB TA 4 DIA T20F&G	246+10	1.5	LT	585.22	585.40	585.47	--	1153 - DIA 18"	1152 - DIA 15"	1153 - DIA 18"		1153 - DIA 18"	1153 - DIA 18"			3153 - DIA 15"	590.77	157	YES	--	TAPERED	--
1154	CB TA 5 DIA T20F&G	247+51	1.6	LT	584.45	584.92	585.55	--	1154 - DIA 24"	1153 - DIA 18"	1154 - DIA 24"		1154 - DIA 24"	1154 - DIA 24"			3154 - DIA 15"	590.86	158	YES	--	FLAT	--
1155	CB TA 5 DIA T20F&G	249+12	1.5	LT	584.14	584.14	584.95	--	1155 - DIA 24"	1154 - DIA 24"	1155 - DIA 24"		1155 - DIA 24"	1155 - DIA 24"			3155 - DIA 15"	590.83	158	YES	--	FLAT	--
1156	CB TA 5 DIA T20F&G	251+74	1.5	LT	583.63	583.63	584.35	--	1156 - DIA 24"	1155 - DIA 24"	1156 - DIA 24"		1156 - DIA 24"	1156 - DIA 24"			3156 - DIA 15"	589.49	158	YES	YES	FLAT	--
1157	CB TA 5 DIA T20F&G	254+24	1.5	LT	583.14	583.14	583.85	--	1157 - SPAN 30", RISE 19"	1156 - DIA 24"	1157 - SPAN 30", RISE 19"		1157 - SPAN 30", RISE 19"	1157 - SPAN 30", RISE 19"			3157 - DIA 15"	587.75	158	YES	YES	FLAT	YES
1158	CB TA 5 DIA T20F&G	256+74	1.9	LT	582.64	582.64	583.05	--	1158 - SPAN 30", RISE 19"	1158 - SPAN 30", RISE 19"	1158 - SPAN 30", RISE 19"		1158 - SPAN 30", RISE 19"	1158 - SPAN 30", RISE 19"			3158 - DIA 15"	586.60	158	YES	--	FLAT	YES
1159	CB TA 5 DIA T20F&G	257+65	1.5	LT	582.47	582.47	582.85	--	1159 - SPAN 30", RISE 19"	1159 - SPAN 30", RISE 19"	1159 - SPAN 30", RISE 19"		1159 - SPAN 30", RISE 19"	1159 - SPAN 30", RISE 19"			3159 - DIA 15"						

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	170
STA.			TO STA.	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

STRUCTURE NO.	STRUCTURE DESCRIPTION	CENTERLINE			INVERTS				PIPE CONNECTIONS (Pipe No. - Pipe Size)				PROPOSED RIM ELEVATION	SHEET NO.	SC INLET FILTER	UNDERDRAIN CONNECTION	STRUCTURE TOP SLAB	TOP SLAB* CONFLICT	
		STATION	OFFSET	DIR.	NORTH	SOUTH	EAST	WEST	NORTH	SOUTH	EAST	WEST							
1190	R C PIPE ELBOW 24	247+96	4.0	LT	584.37	584.37	--	--	1190 - DIA 24"	1154 - DIA 24"	--	--	--	158	--	--	--	--	
1191	R C PIPE ELBOW 24	248+01	8.0	LT	584.36	584.36	--	--	1191 - DIA 24"	1190 - DIA 24"	--	--	--	158	--	--	--	--	
1192	R C PIPE ELBOW 24	248+43	8.0	LT	584.27	584.27	--	--	1192 - DIA 24"	1191 - DIA 24"	--	--	--	158	--	--	--	--	
1193	R C PIPE ELBOW 24	248+48	4.0	LT	584.26	584.26	--	--	1193 - DIA 24"	1192 - DIA 24"	--	--	--	158	--	--	--	--	
1195	R C PIPE ELBOW S30 R19	256+00	4.5	LT	582.79	582.79	--	--	1195 - SPAN 30", RISE 19"	1157 - SPAN 30", RISE 19"	--	--	--	158	--	--	--	--	
1196	R C PIPE ELBOW S30 R19	256+04	8.0	LT	582.78	582.78	--	--	1196 - SPAN 30", RISE 19"	1195 - SPAN 30", RISE 19"	--	--	--	158	--	--	--	--	
1197	R C PIPE ELBOW S30 R19	256+48	8.0	LT	582.69	582.69	--	--	1197 - SPAN 30", RISE 19"	1196 - SPAN 30", RISE 19"	--	--	--	158	--	--	--	--	
1198	R C PIPE ELBOW S30 R19	256+52	4.5	LT	582.68	582.68	--	--	1198 - SPAN 30", RISE 19"	1197 - SPAN 30", RISE 19"	--	--	--	158	--	--	--	--	
1202	CB TA 5 DIA T21F&G	282+61	1.5	LT	589.16	--	588.91	588.91	1202 - DIA 21"	--	--	3202 - DIA 24"	EP - DIA 24"	595.45	160	YES	YES	FLAT	--
1203	CB TA 5 DIA T20F&G	284+32	1.5	LT	590.25	590.00	590.50	--	1203 - DIA 18"	--	--	3203 - DIA 15"	--	595.65	160	YES	--	FLAT	--
1204	CB TA 4 DIA T20F&G	285+70	1.5	LT	590.89	590.89	591.05	--	1204 - DIA 18"	1203 - DIA 18"	--	3204 - DIA 15"	--	595.71	160	YES	YES	FLAT	--
1205	CB TA 4 DIA T20F&G	286+82	1.5	LT	591.27	591.27	591.52	--	1205 - DIA 18"	1204 - DIA 18"	--	3205 - DIA 15"	--	595.75	160	YES	--	FLAT	--
1206	CB TA 4 DIA T20F&G	288+26	1.5	LT	592.01	591.76	592.01	--	1206 - DIA 15"	1205 - DIA 18"	--	3206 - DIA 15"	--	595.90	160	YES	YES	FLAT	--
1207	CB TA 4 DIA T20F&G	289+49	1.5	LT	--	592.48	592.48	--	--	1206 - DIA 15"	--	3207 - DIA 15"	--	596.01	160	YES	--	FLAT	YES
1232	CB TA 5 DIA T21F&G	290+90	1.5	LT	591.65	--	591.20	591.16	1232 - DIA 18"	--	--	3232 - DIA 24"	EP - DIA 24"	596.09	160	YES	YES	FLAT	--
1233	CB TA 4 DIA T21F&G	291+05	1.5	LT	591.68	591.68	591.93	--	1233 - DIA 18"	1232 - DIA 18"	--	3233 - DIA 15"	--	596.10	160	YES	--	FLAT	--
1234	CB TA 4 DIA T20F&G	292+33	1.5	LT	592.30	592.05	592.30	--	1234 - DIA 15"	1233 - DIA 18"	--	3234 - DIA 15"	--	596.21	161	YES	--	FLAT	--
1235	CB TA 4 DIA T20F&G	293+80	1.5	LT	592.73	592.73	592.73	--	1235 - DIA 12"	1234 - DIA 15"	--	3235 - DIA 15"	--	596.36	161	YES	--	FLAT	YES
1253	CB TA 4 DIA T20F&G	295+09	1.5	LT	--	593.23	593.23	--	--	1235 - DIA 12"	--	3253 - DIA 12"	--	596.36	161	YES	--	FLAT	YES
1254	CB TA 4 DIA T20F&G	296+46	1.9	LT	592.92	--	592.92	--	1254 - DIA 15"	--	--	3254 - DIA 15"	--	596.38	161	YES	YES	FLAT	YES
1255	CB TA 4 DIA T20F&G	298+60	1.5	LT	592.18	592.25	592.45	--	1255 - DIA 18"	1254 - DIA 15"	--	3255 - DIA 15"	--	596.94	161	YES	YES	FLAT	--
1256	MAN TA 6 DIA T20F&G	300+90	1.5	LT	591.50	591.51	590.92	590.92	1256 - DIA 18"	1255 - DIA 18"	--	3256 - DIA 48"	EP - DIA 48"	597.63	161	YES	YES	FLAT	--
1257	CB TA 4 DIA T20F&G	302+60	1.5	LT	592.70	592.66	592.95	--	1257 - DIA 18"	1256 - DIA 18"	--	3257 - DIA 15"	--	598.36	161	YES	--	TAPERED	--
1258	CB TA 4 DIA T20F&G	303+97	1.5	LT	593.63	593.63	593.88	--	1258 - DIA 18"	1257 - DIA 18"	--	3258 - DIA 15"	--	598.89	161	YES	YES	FLAT	--
1259	CB TA 4 DIA T20F&G	305+16	1.5	LT	594.69	594.44	594.69	--	1259 - DIA 15"	1258 - DIA 18"	--	3259 - DIA 15"	--	599.30	161	YES	--	FLAT	--
1260	CB TA 4 DIA T20F&G	307+11	1.5	LT	596.03	596.03	596.03	--	1260 - DIA 15"	1259 - DIA 15"	--	3260 - DIA 15"	--	600.10	162	YES	YES	FLAT	--
1261	CB TA 4 DIA T20F&G	309+06	1.5	LT	--	596.98	597.00	--	--	1260 - DIA 15"	--	3261 - DIA 15"	--	600.86	162	YES	--	FLAT	--
1351	CB TA 4 DIA T20F&G	311+10	1.5	LT	597.36	--	597.36	--	1351 - DIA 15"	--	--	3351 - DIA 15"	--	601.57	162	YES	YES	FLAT	--
1352	CB TA 4 DIA T20F&G	313+60	1.5	LT	596.18	596.20	596.20	--	1352 - DIA 15"	1351 - DIA 15"	--	3352 - DIA 15"	--	602.38	162	YES	--	TAPERED	--
1353	CB TA 4 DIA T20F&G	315+10	1.5	LT	595.25	595.25	595.25	--	1353 - DIA 15"	1352 - DIA 15"	--	3353 - DIA 15"	--	602.49	162	YES	--	TAPERED	--
1354	CB TA 4 DIA T20F&G	317+60	1.5	LT	594.02	594.02	594.01	--	1354 - DIA 15"	1353 - DIA 15"	--	3354 - DIA 15"	--	601.31	162	YES	--	TAPERED	--
1355	CB TA 4 DIA T20F&G	319+03	1.5	LT	592.36	592.61	592.61	--	1355 - DIA 18"	1354 - DIA 15"	--	3355 - DIA 15"	--	600.03	162	YES	YES	TAPERED	--
1356	CB TA 4 DIA T20F&G	321+53	1.5	LT	589.90	589.90	590.15	--	1356 - DIA 18"	1355 - DIA 18"	--	3356 - DIA 15"	--	596.53	162	YES	--	TAPERED	--
1357	CB TA 4 DIA T20F&G	323+17	1.5	LT	587.76	587.75	588.00	--	1357 - DIA 18"	1356 - DIA 18"	--	3357 - DIA 15"	--	593.52	163	YES	YES	TAPERED	--
1358	CB TA 4 DIA T20F&G	325+67	1.5	LT	583.26	583.45	584.31	--	1358 - DIA 18"	1357 - DIA 18"	--	3358 - DIA 15"	--	588.68	163	YES	--	FLAT	--
1359	CB TA 5 DIA T20F&G	328+17	1.5	LT	580.43	580.80	581.36	--	1359 - SPAN 30", RISE 19"	1358 - DIA 18"	--	3359 - DIA 15"	--	585.31	163	YES	YES	FLAT	--
1360	CB TA 5 DIA T20F&G	330+67	1.5	LT	578.64	579.55	579.60	--	1360 - DIA 24"	1359 - SPAN 30", RISE 19"	--	3360 - DIA 15"	--	583.48	163	YES	--	FLAT	YES
1361	CB TA 5 DIA T20F&G	332+50	1.5	LT	578.11	578.11	579.06	--	1361 - DIA 24"	1360 - DIA 24"	--	3361 - DIA 15"	--	583.07	163	YES	YES	FLAT	--
1362	CB TA 5 DIA T20F&G	334+35	1.7	LT	577.09	577.34	577.98	--	1362 - DIA 27"	1370 - DIA 24"	--	3362 - DIA 15"	--	582.71	163	YES	--	FLAT	--
1363	CB TA 5 DIA T21F&G	335+88	1.5	LT	577.23	576.23	575.99	575.97	1364 - DIA 15"	1362 - DIA 27"	--	3363 - DIA 30"	EP - DIA 24"	582.42	163	YES	--	FLAT	--
1364	MAN TA 5 DIA T1F CL	335+86	65.0	RT	575.69	575.61	--	575.61	EP - DIA 18"	1610 - DIA 36"	--	--	1363 - DIA 30"	582.19	163	YES	--	FLAT	--
1365	CB TA 4 DIA T20F&G	337+19	1.5	LT	577.61	577.61	577.61	--	1365 - DIA 15"	1364 - DIA 15"	--	3365 - DIA 15"	--	582.17	164	YES	YES	FLAT	--
1366	CB TA 4 DIA T20F&G	339+22	1.5	LT	--	578.21	578.21	--	--	1365 - DIA 15"	--	3366 - DIA 15"	--	581.76	164	YES	--	FLAT	YES
1370	R C PIPE ELBOW 24	334+01	4.5	LT	577.55	--	--	577.55	1370 - DIA 24"	--	--	--	1371 - DIA 24"	163	--	--	--	--	
1371	R C PIPE ELBOW 24	333+97	8.0	LT	--	577.58	577.58	--	--	--	--	1371 - DIA 24"	--	163	--	--	--	--	
1372	R C PIPE ELBOW 24	333+53	8.0	LT	577.79	577.79	--	--	1372 - DIA 24"	--	--	1373 - DIA 24"	--	163	--	--	--	--	
1373	R C PIPE ELBOW 24	333+49	4.5	LT	577.82	577.82	--	--	1373 - DIA 24"	1361 - DIA 24"	--	--	--	163	--	--	--	--	
1381	CB TA 4 DIA T20F&G	340+50	1.5	LT	578.24	--	578.24	--	1381 - DIA 12"	--	--	3381 - DIA 12"	--	581.63	164	YES	YES	FLAT	YES
1382	CB TA 4 DIA T21F&G	342+39	1.5	LT	577.34	577.55	577.54	--	1383 - DIA 12"	1381 - DIA 12"	--	3382 - DIA 18"	--	580.95	164	YES	--	FLAT	YES
1383	PRC FLAR END SEC 18	342+40	63.0	RT	--	--	577.30	--	--	--	--	--	1382 - DIA 18"	164	--	--	--	--	
1384	CB TA 4 DIA T20F&G	343+10	1.5	LT	--	577.59	577.59	--	--	1383 - DIA 12"	--	3384 - DIA 12"	--	580.65	164	YES	--	FLAT	YES
1401	CB TA 4 DIA T20F&G	344+90	1.5	LT	576.94	--	576.94	--	1401 - DIA 15"	--	--	3401 - DIA 15"	--	580.27	164	YES	YES	FLAT	YES
1402	CB TA 4 DIA T20F&G	346+27	1.5	LT	576.53	576.55	576.75	--	1402 - DIA 15"	1401 - DIA 15"	--	3402 - DIA 15"	--	580.15	164	YES	--	FLAT	YES
1403	CB TA 4 DIA T20F&G	348+12	1.5	LT	575.86	576.00	576.25	--	1403 - DIA 18"	1402 - DIA 15"	--	3403 - DIA 15"	--	579.82	164	YES	--	FLAT	YES
1404	CB TA 5 DIA T21F&G	350+49	1.5	LT	575.75	575.55	576.01	575.26	1405 - DIA 12"	1403 - DIA 18"	--	3404 - DIA 15"	1404 - SPAN 30", RISE 19"	579.39	164	YES	--	FLAT	YES
1405	PRC FL END S EQ RS 24	350+48	87.9	LT	--	--	575.10	--	--	--	1404 - SPAN 30", RISE 19"	--	--	164	--	--	--	--	
1406	CB TA 4 DIA T20F&G	352+00	1.5	LT	--	576.09	576.09	--	--	1405 - DIA 12"	--	3406 - DIA 12"	--	579.16	164	YES	--	FLAT	YES
1420T	PRC FLAR END SEC 24	353+37	6.0	LT	--	--	574.42	--	--	--	--	--	1420 - DIA 24"	165	--	--	--	--	
1421	PRC FLAR END SEC 24	353+37	87.0	LT	--	--	574.02	--	--	--	--	1420 - DIA 24"	--	165	--	--	--	--	
1422	CB TA 5 DIA T21F&G	353+36	1.5	LT	574.50	--	574.45	574.42	1422 - DIA 24"	--	--	3422 - DIA 24"	1420 - DIA 24"	579.04	165	YES	--	FLAT	YES
1423	R C PIPE ELBOW 24	354+25	4.0	LT	574.67	574.67	--	--	1422 - DIA 24"	--	--	1422 - DIA 24"	--	165	--	--	--	--	
1424	CB TA 5 DIA T20F&G	355+00	1.9	LT	575.00	574.82	575.00	--	1424 - DIA 15"	1423 - DIA 24"	--	3424 - DIA 15"	--	579.13	165	YES	--	FLAT	YES
1425	CB TA 4 DIA T20F&G	356+25	1.5	LT	575.40	575.39	575.40	--	1425 - DIA 15"	1424 - DIA 15"	--	3425 - DIA 15"	--	579.10	165	YES	--	FLAT	YES
1431	R C PIPE ELBOW 24	354+30	8.0	LT	574.68	--	--	--	1431 - DIA 24"	--	--	1432 - DIA 24"	--	165	--	--	--	--	
1432	R C PIPE ELBOW 24	354+76	8.0	LT	--	574.68	574.78	--	--	1431 - DIA 24"	--	1433 - DIA 24"	--	165	--	--	--	--	
1433	R C PIPE ELBOW 24	354+81	4.0	LT	574.79	--	574.79	--	1423 - DIA 24"	--	--	--	1433 - DIA 24"	165	--	--	--	--	
1451	CB TA 4 DIA T20F&G	357+50	1.5	LT	--	575.79	575.85	--	--	1451 - DIA 15"	--	3451 - DIA 15"	--	579.22	165				

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL.	334	171
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

STRUCTURE NO.	STRUCTURE DESCRIPTION	CENTERLINE			INVERTS				PIPE CONNECTIONS (Pipe No. - Pipe Size)				PROPOSED RIM ELEVATION	SHEET NO.	SC INLET FILTER	UNDERDRAIN CONNECTION	STRUCTURE TOP SLAB	TOP SLAB* CONFLICT		
		STATION	OFFSET	DIR.	NORTH	SOUTH	EAST	WEST	NORTH		SOUTH								EAST	WEST
1455	CB TA 4 DIA T20F&G	363+84	1.5	LT	575.43	575.43	575.68	--	1455 - DIA 18"	1454 - DIA 18"	3455 - DIA 15"	--	579.95	165	YES	--	FLAT	--		
1456	CB TA 4 DIA T20F&G	365+38	1.5	LT	575.88	575.88	576.13	--	1456 - DIA 18"	1455 - DIA 18"	3456 - DIA 15"	--	580.13	165	YES	--	FLAT	--		
1457	CB TA 4 DIA T20F&G	366+93	1.5	LT	576.58	576.33	576.58	--	1457 - DIA 15"	1456 - DIA 18"	3457 - DIA 15"	--	580.25	165	YES	--	FLAT	YES		
1458	CB TA 4 DIA T20F&G	368+20	1.5	LT	--	577.08	577.08	--	--	1457 - DIA 15"	3458 - DIA 15"	--	580.37	166	YES	--	FLAT	YES		
1483	CB TA 5 DIA T20F&G	362+43	1.5	LT	575.45	575.47	575.97	--	1483 - DIA 21"	1453 - DIA 18"	3483 - DIA 15"	--	579.83	165	YES	--	FLAT	YES		
1501	CB TA 4 DIA T20F&G	369+72	1.5	LT	577.46	--	577.46	--	1501 - DIA 12"	--	3501 - DIA 12"	--	580.51	166	YES	--	FLAT	YES		
1502	CB TA 4 DIA T20F&G	371+11	1.5	LT	576.96	576.96	576.96	--	1502 - DIA 15"	1501 - DIA 12"	3502 - DIA 15"	--	580.69	166	YES	--	FLAT	--		
1503	DRAINAGE STRUCTURE SP	372+85	1.5	LT	576.40	576.50	576.15	576.05	1504 - DIA 24"	1583 - DIA 15"	EP - 2 X DIA 36"	EP - 2 X DIA 36"	580.86	166	--	--	--	YES		
1505	CB TA 5 DIA T20F&G	374+25	1.5	LT	576.66	576.66	577.41	--	1505 - SPAN 30', RISE 19"	1504 - DIA 24"	3505 - DIA 15"	--	580.96	166	YES	--	FLAT	YES		
1506	CB TA 5 DIA T20F&G	375+62	1.5	LT	577.01	577.01	577.34	--	1506 - SPAN 30', RISE 19"	1505 - SPAN 30', RISE 19"	3506 - DIA 15"	--	581.10	166	YES	--	FLAT	YES		
1507	CB TA 5 DIA T20F&G	377+08	1.5	LT	577.32	577.32	577.65	--	1507 - DIA 18"	1506 - SPAN 30', RISE 19"	3507 - DIA 15"	--	581.32	166	YES	--	FLAT	YES		
1508	CB TA 4 DIA T20F&G	378+55	1.5	LT	577.63	577.63	577.88	--	1508 - DIA 18"	1507 - DIA 18"	3508 - DIA 15"	--	581.48	166	YES	--	FLAT	YES		
1509	CB TA 4 DIA T20F&G	380+10	1.5	LT	577.97	577.97	578.22	--	1509 - DIA 15"	1508 - DIA 18"	3509 - DIA 15"	--	581.70	166	YES	--	FLAT	YES		
1510	CB TA 4 DIA T20F&G	381+74	1.5	LT	578.42	578.42	578.67	--	1510 - DIA 12"	1509 - DIA 15"	3510 - DIA 12"	--	581.95	166	YES	--	FLAT	YES		
1511	CB TA 4 DIA T20F&G	383+07	1.5	LT	--	578.83	578.83	--	--	1510 - DIA 12"	3511 - DIA 12"	--	582.31	167	YES	--	FLAT	YES		
1550	PRC FLAR END SEC 18	384+90	69.0	LT	--	--	578.50	--	--	--	1552 - DIA 18"	--	--	167	--	--	--	--		
1551	CB TA 4 DIA T20F&G	384+90	1.5	LT	578.66	--	579.10	578.66	1551 - DIA 12"	--	3551 - DIA 12"	1552 - DIA 18"	583.15	167	YES	--	FLAT	YES		
1552	CB TA 4 DIA T20F&G	386+60	1.5	LT	--	579.27	579.27	--	--	1551 - DIA 12"	3552 - DIA 12"	--	583.32	167	YES	--	FLAT	YES		
1553	CB TA 4 DIA T20F&G	387+97	1.5	LT	579.40	--	579.40	--	1553 - DIA 12"	--	3553 - DIA 12"	--	583.48	167	YES	--	FLAT	YES		
1554	CB TA 4 DIA T21F&G	389+36	1.5	LT	578.90	578.90	579.15	578.89	1556 - DIA 15"	1553 - DIA 12"	3554 - DIA 15"	1554 - DIA 18"	583.63	167	YES	--	FLAT	YES		
1555	PRC FLAR END SEC 18	389+36	69.0	LT	--	--	578.75	--	--	--	1554 - DIA 18"	--	--	167	--	--	--	--		
1556	CB TA 4 DIA T20F&G	390+40	1.5	LT	579.20	579.20	579.20	--	1601 - DIA 12"	1556 - DIA 15"	3556 - DIA 15"	--	583.69	167	YES	--	FLAT	YES		
1583	CB TA 4 DIA T20F&G	372+66	1.5	LT	576.54	576.54	576.55	--	1583 - DIA 15"	1502 - DIA 15"	3583 - DIA 15"	--	580.83	166	YES	--	FLAT	--		
1601	CB TA 4 DIA T20F&G	391+52	1.5	LT	--	579.60	579.60	--	--	1601 - DIA 12"	3601 - DIA 12"	--	583.84	167	YES	--	FLAT	YES		
1602	CB TA 4 DIA T20F&G	393+15	1.5	LT	579.40	579.41	579.41	--	1602 - DIA 15"	1601 - DIA 15"	3602 - DIA 15"	--	584.03	167	YES	--	FLAT	YES		
1603	CB TA 4 DIA T20F&G	394+53	1.5	LT	579.02	579.03	579.39	--	1603 - DIA 18"	1602 - DIA 15"	3603 - DIA 15"	--	584.10	167	YES	--	FLAT	--		
1604	DRAINAGE STRUCTURE SP	395+85	1.5	LT	578.75	578.75	578.57	578.56	1605 - DIA 21"	1684 - DIA 18"	EP - 2 X DIA 36"	EP - 2 X DIA 36"	584.21	167	--	--	--	YES		
1606	CB TA 5 DIA T20F&G	397+19	1.5	LT	578.98	578.98	579.48	--	1606 - DIA 18"	1605 - DIA 21"	3606 - DIA 15"	--	584.34	168	YES	--	FLAT	YES		
1607	CB TA 4 DIA T20F&G	398+50	1.5	LT	579.26	579.26	579.51	--	1607 - DIA 18"	1606 - DIA 18"	3607 - DIA 15"	--	584.43	168	YES	--	FLAT	--		
1608	CB TA 4 DIA T20F&G	399+80	1.5	LT	579.54	579.54	579.79	--	1608 - DIA 15"	1607 - DIA 18"	3608 - DIA 15"	--	584.53	168	YES	--	FLAT	--		
1609	CB TA 4 DIA T20F&G	401+58	1.5	LT	580.03	580.03	580.03	--	1609 - DIA 12"	1608 - DIA 15"	3609 - DIA 15"	--	584.38	168	YES	--	FLAT	YES		
1610	CB TA 4 DIA T20F&G	402+50	1.3	LT	--	580.36	580.36	--	--	1609 - DIA 12"	3610 - DIA 12"	--	584.46	168	YES	--	FLAT	YES		
1611	PRC FLAR END SEC 36	335+24	225.0	RT	575.40	--	--	--	1610 - DIA 36"	--	--	--	--	163	--	--	--	--		
1684	CB TA 4 DIA T20F&G	395+72	1.5	LT	578.76	578.76	579.01	--	1684 - DIA 18"	1603 - DIA 18"	3684 - DIA 15"	--	584.26	167	YES	--	TAPERED	--		
1711	PRC FLAR END SEC 12	332+95	168.3	RT	--	--	575.20	--	--	--	--	1711 - DIA 12"	--	163	--	--	--	--		
1712	PRC FLAR END SEC 12	332+70	130.3	RT	--	--	575.00	--	--	--	1711 - DIA 12"	--	--	163	--	--	--	--		
1721	PRC FLAR END SEC 12	350+64	92.9	LT	575.10	--	--	--	1721 - DIA 12"	--	--	--	--	164	--	--	--	--		
1722	PRC FLAR END SEC 12	351+06	83.0	LT	--	575.00	--	--	--	1721 - DIA 12"	--	--	--	164	--	--	--	--		
1900	PRC FLAR END SEC 24	300+84	480.5	LT	--	--	583.85	--	--	--	1900 - DIA 24"	--	--	161	--	--	--	--		
1901	PRC FLAR END SEC 24	299+91	522.6	LT	584.02	--	589.10	--	1901 - DIA 24"	--	EP - DIA 24"	--	--	161	--	--	--	--		
1902	MAN TA 5 DIA T1F CL	300+30	527.4	LT	--	583.96	583.96	592.44	--	1901 - DIA 24"	1900 - DIA 24"	--	597.34	161	YES	--	TAPERED	--		
3024	CB TA 4 DIA T9F&G	193+60	NB	24.0	LT	--	569.21	--	--	1023 - DIA 15"	--	3024 - DIA 15"	577.14	154	YES	YES	TAPERED	--		
3026	CB TA 4 DIA T9F&G	195+93	NB	24.0	LT	--	--	575.13	--	--	--	3026 - DIA 15"	578.81	154	YES	--	FLAT	--		
3027	CB TA 4 DIA T20F&G	198+33	NB	24.8	LT	--	--	576.58	--	--	--	3027 - DIA 15"	580.60	154	YES	YES	FLAT	YES		
3028	CB TA 4 DIA T20F&G	200+70	NB	24.0	LT	--	--	577.98	--	--	--	3028 - DIA 15"	582.47	154	YES	--	FLAT	--		
3033	CB TA 4 DIA T21F&G	202+93	NB	22.0	LT	--	--	580.18	--	--	--	3033 - DIA 15"	584.11	155	YES	YES	FLAT	YES		
3034	CB TA 4 DIA T20F&G	205+48	NB	19.4	LT	--	--	581.58	--	--	--	3034 - DIA 15"	586.02	155	YES	--	FLAT	--		
3035	CB TA 4 DIA T20F&G	208+02	NB	18.2	LT	--	--	582.84	--	--	--	3035 - DIA 15"	587.54	155	YES	YES	FLAT	--		
3045	CB TA 4 DIA T20F&G	215+39	NB	18.5	LT	--	--	585.60	--	--	--	3045 - DIA 15"	589.67	155	YES	YES	FLAT	YES		
3051	CB TA 4 DIA T20F&G	210+50	NB	17.2	LT	--	--	584.66	--	--	--	3051 - DIA 12"	588.44	155	YES	--	FLAT	YES		
3052	CB TA 4 DIA T20F&G	211+92	NB	18.5	LT	--	--	584.60	--	--	--	3052 - DIA 15"	589.23	155	YES	YES	FLAT	YES		
3054	CB TA 4 DIA T20F&G	214+08	NB	18.5	LT	--	--	585.45	--	--	--	3054 - DIA 15"	589.55	155	YES	--	FLAT	YES		
3055	CB TA 4 DIA T20F&G	216+61	1.5	RT	--	--	585.75	--	--	--	--	3055 - DIA 15"	589.61	155	YES	--	FLAT	YES		
3056	CB TA 4 DIA T20F&G	218+00	1.5	RT	--	--	585.28	--	--	--	--	3056 - DIA 15"	589.64	156	YES	--	FLAT	--		
3057	CB TA 4 DIA T20F&G	219+30	1.5	RT	--	--	584.88	--	--	--	--	3057 - DIA 15"	589.66	156	YES	--	FLAT	--		
3058	CB TA 4 DIA T21F&G	220+54	1.5	RT	--	--	584.78	--	--	--	--	3058 - DIA 15"	589.79	156	YES	YES	FLAT	--		
3059	CB TA 4 DIA T20F&G	222+07	1.5	RT	--	--	583.97	--	--	--	--	3059 - DIA 15"	589.68	156	YES	YES	TAPERED	--		
3100	PRC FLAR END SEC 18	226+56	102.0	RT	--	585.15	--	--	--	3100 - DIA 18"	--	--	--	156	--	--	--	--		
3101	PRC FLAR END SEC 18	226+36	102.0	RT	585.00	--	--	--	3100 - DIA 18"	--	--	--	--	156	--	--	--	--		
3102	CB TA 4 DIA T20F&G	223+13	1.5	RT	--	--	584.12	--	--	--	3102 - DIA 15"	589.69	156	YES	--	TAPERED	--			
3103	CB TA 4 DIA T20F&G	224+64	1.5	RT	--	--	584.27	--	--	--	3103 - DIA 15"	589.61	156	YES	YES	TAPERED	--			
3105	CB TA 4 DIA T21F&G	226+00	1.5	RT	--	--	584.48	--	--	--	3105 - DIA 15"	589.60	156	YES	--	TAPERED	--			
3106	CB TA 4 DIA T20F&G	227+47	1.5	RT	--	--	584.53	--	--	--	3106 - DIA 15"	589.74	156	YES	YES	TAPERED	--			
3107	CB TA 4 DIA T20F&G	228+89	1.5	RT	--	--	584.78	--	--	--	3107 - DIA 15"	589.72	156	YES	--	FLAT	--			
3108	CB TA 4 DIA T20F&G	231+10	1.5	RT	--	--	585.11	--	--	--	3108 - DIA 15"	589.95	156	YES	--	FLAT	--			
3109	CB TA 4 DIA T20F&G	232+56	1.5	RT	--	--	585.38	--	--	--	3109 - DIA 15"	590.09	157	YES	--	FLAT	--			
3110	CB TA 4 DIA T20F&G	234+13	1.5	RT	--	--	585.70	--	--	--	3110 - DIA 15"	590.30	157	YES	YES	FLAT	YES			
3111	CB TA 4 DIA T20F&G	235+31	1.5	RT	--	--	585.91	--	--	--	3111 - DIA 15"	590.37	157	YES	--	FLAT	YES			
3112	CB TA 4 DIA T20F&G	237+00	1.5	RT	--	--	586.22	--	--	--	3112 - DIA 15"	590.40	157	YES	--	FLAT	YES			
3113	CB TA 4 DIA T20F&G	238+59	1.5	RT	--	--	586.65	--	--	--	3113 - DIA 15"	590.69	157	YES	YES	FLAT	YES			

\* FOR THE STRUCTURES INDICATED, IT IS ANTICIPATED THAT THE TOP SLAB OF THE STRUCTURE WILL EXTEND UP INTO THE PROPOSED BARRIER BASE AND SHOULDER PAVEMENT AS SHOWN ON SHEET 188.

**LEGEND**

ES - EXISTING STRUCTURE  
EP - EXISTING PIPE

**NOTE:**

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	172
STA.			TO STA.	
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	

STRUCTURE NO.	STRUCTURE DESCRIPTION	CENTERLINE			INVERTS				PIPE CONNECTIONS (Pipe No. - Pipe Size)			PROPOSED RIM ELEVATION	SHEET NO.	SC INLET FILTER	UNDERDRAIN CONNECTION	STRUCTURE TOP SLAB	TOP SLAB* CONFLICT		
		STATION	OFFSET	DIR.	NORTH	SOUTH	EAST	WEST	NORTH	SOUTH	EAST							WEST	
3114	CB TA 4 DIA T20F&G	240+16	1.5	RT	--	--	--	587.13	--	--	--		3114 - DIA 12"	590.83	157	YES	--	FLAT	YES
3120	CB TA 4 DIA T20F&G	230+06	2.1	RT	--	--	--	585.02	--	--	--		3120 - DIA 15"	589.92	156	YES	YES	FLAT	--
3150	CB TA 4 DIA T20F&G	241+66	1.5	RT	--	--	--	586.78	--	--	--		3150 - DIA 15"	590.96	157	YES	--	FLAT	YES
3151	CB TA 4 DIA T20F&G	243+16	1.5	RT	--	--	--	586.34	--	--	--		3151 - DIA 15"	591.12	157	YES	YES	FLAT	--
3152	CB TA 4 DIA T20F&G	244+65	1.5	RT	--	--	--	585.33	--	--	--		3152 - DIA 15"	591.24	157	YES	--	TAPERED	--
3153	CB TA 4 DIA T20F&G	246+10	1.5	RT	--	--	--	585.50	--	--	--		3153 - DIA 15"	591.50	157	YES	--	TAPERED	--
3154	CB TA 4 DIA T20F&G	247+51	1.6	RT	--	--	--	585.58	--	--	--		3154 - DIA 15"	591.73	158	YES	--	TAPERED	--
3155	CB TA 4 DIA T20F&G	249+12	1.5	RT	--	--	--	584.98	--	--	--		3155 - DIA 15"	591.61	158	YES	--	TAPERED	--
3156	CB TA 4 DIA T20F&G	251+74	1.5	RT	--	--	--	584.38	--	--	--		3156 - DIA 15"	590.17	158	YES	YES	TAPERED	--
3157	CB TA 4 DIA T20F&G	254+24	1.5	RT	--	--	--	583.88	--	--	--		3157 - DIA 15"	588.35	158	YES	YES	FLAT	YES
3158	CB TA 4 DIA T20F&G	256+74	1.9	RT	--	--	--	583.09	--	--	--		3158 - DIA 15"	587.16	158	YES	--	FLAT	YES
3159	CB TA 4 DIA T20F&G	257+65	1.5	RT	--	--	--	582.88	--	--	--		3159 - DIA 15"	586.96	158	YES	YES	FLAT	YES
3160	CB TA 4 DIA T21F&G	258+80	1.5	RT	--	--	--	582.60	--	--	--		3160 - DIA 15"	587.03	158	YES	--	FLAT	YES
3161	CB TA 4 DIA T21F&G	258+92	1.5	RT	--	--	--	582.43	--	--	--		3161 - DIA 15"	587.04	158	YES	--	FLAT	YES
3163	CB TA 4 DIA T20F&G	259+93	1.5	RT	--	--	--	583.12	--	--	--		3163 - DIA 15"	587.04	158	YES	YES	FLAT	YES
3164	CB TA 4 DIA T20F&G	262+15	1.5	RT	--	--	--	583.55	--	--	--		3164 - DIA 15"	587.78	159	YES	YES	FLAT	YES
3165	CB TA 4 DIA T20F&G	264+65	1.5	RT	--	--	--	584.53	--	--	--		3165 - DIA 15"	589.53	159	YES	YES	FLAT	--
3166	CB TA 4 DIA T20F&G	267+15	1.5	RT	--	--	--	586.88	--	--	--		3166 - DIA 15"	591.79	159	YES	YES	FLAT	--
3167	CB TA 4 DIA T20F&G	269+73	4.0	RT	--	--	--	588.91	--	--	--		3167 - DIA 15"	594.11	159	YES	--	TAPERED	--
3171	CB TA 4 DIA T20F&G	270+83	2.3	RT	--	--	--	589.26	--	--	--		3171 - DIA 15"	594.64	159	YES	YES	TAPERED	--
3172	CB TA 4 DIA T20F&G	273+00	1.5	RT	--	--	--	589.86	--	--	--		3172 - DIA 15"	595.40	159	YES	YES	TAPERED	--
3173	CB TA 4 DIA T20F&G	275+85	1.5	RT	--	--	--	590.62	--	--	--		3173 - DIA 15"	596.02	159	YES	YES	TAPERED	--
3174	CB TA 4 DIA T20F&G	276+97	1.5	RT	--	--	--	590.96	--	--	--		3174 - DIA 15"	596.23	159	YES	--	TAPERED	--
3175	CB TA 4 DIA T20F&G	278+26	1.5	RT	--	--	--	591.38	--	--	--		3175 - DIA 15"	596.28	160	YES	YES	FLAT	--
3176	CB TA 4 DIA T20F&G	279+77	1.5	RT	--	--	--	591.83	--	--	--		3176 - DIA 15"	596.37	160	YES	--	FLAT	YES
3177	CB TA 4 DIA T20F&G	281+23	1.5	RT	--	--	--	592.36	--	--	--		3177 - DIA 12"	596.49	160	YES	--	FLAT	YES
3178	CB TA 4 DIA T20F&G	274+32	1.5	RT	--	--	--	590.24	--	--	--		3178 - DIA 15"	595.73	159	YES	--	TAPERED	--
3179	CB TA 4 DIA T20F&G	271+70	1.5	RT	--	--	--	589.49	--	--	--		3179 - DIA 15"	594.86	159	YES	--	TAPERED	--
3202	CB TA 5 DIA T21F&G	282+61	1.5	RT	--	--	588.95	588.92	--	--	EP - DIA 24"	3202 - DIA 24"	596.63	160	YES	YES	TAPERED	--	
3203	CB TA 4 DIA T20F&G	284+32	1.5	RT	--	--	--	590.53	--	--	--	3203 - DIA 15"	596.86	160	YES	--	TAPERED	--	
3204	CB TA 4 DIA T20F&G	285+70	1.5	RT	--	--	--	591.08	--	--	--	3204 - DIA 15"	596.84	160	YES	YES	TAPERED	--	
3205	CB TA 4 DIA T20F&G	286+82	1.5	RT	--	--	--	591.55	--	--	--	3205 - DIA 15"	596.91	160	YES	--	TAPERED	--	
3206	CB TA 4 DIA T20F&G	288+26	1.5	RT	--	--	--	592.04	--	--	--	3206 - DIA 15"	597.07	160	YES	YES	FLAT	--	
3207	CB TA 4 DIA T20F&G	289+49	1.5	RT	--	--	--	592.51	--	--	--	3207 - DIA 15"	597.16	160	YES	--	FLAT	YES	
3232	CB TA 5 DIA T21F&G	290+90	1.5	RT	--	--	591.15	591.29	--	--	EP - DIA 24"	3232 - DIA 24"	597.25	160	YES	YES	FLAT	--	
3233	CB TA 4 DIA T21F&G	291+05	1.5	RT	--	--	--	591.96	--	--	--	3233 - DIA 15"	597.24	160	YES	--	TAPERED	--	
3234	CB TA 4 DIA T20F&G	292+33	1.5	RT	--	--	--	592.33	--	--	--	3234 - DIA 15"	597.36	161	YES	--	FLAT	--	
3235	CB TA 4 DIA T20F&G	293+80	1.5	RT	--	--	--	592.76	--	--	--	3235 - DIA 15"	597.42	161	YES	--	FLAT	YES	
3253	CB TA 4 DIA T20F&G	295+09	1.3	RT	--	--	--	593.26	--	--	--	3253 - DIA 12"	597.34	161	YES	--	FLAT	YES	
3254	CB TA 4 DIA T20F&G	296+46	0.7	RT	--	--	--	592.95	--	--	--	3254 - DIA 15"	597.47	161	YES	YES	FLAT	YES	
3255	CB TA 4 DIA T20F&G	298+60	1.5	RT	--	--	--	592.48	--	--	--	3255 - DIA 15"	598.07	161	YES	YES	TAPERED	--	
3256	MAN TA 6 DIA T20F&G	300+90	1.5	RT	--	--	590.94	590.94	--	--	EP - DIA 48"	3256 - DIA 48"	598.61	161	YES	YES	FLAT	--	
3257	CB TA 4 DIA T20F&G	302+60	1.5	RT	--	--	--	592.98	--	--	--	3257 - DIA 15"	599.10	161	YES	--	TAPERED	--	
3258	CB TA 4 DIA T20F&G	303+97	1.5	RT	--	--	--	593.91	--	--	--	3258 - DIA 15"	598.98	161	YES	YES	FLAT	--	
3259	CB TA 4 DIA T20F&G	305+16	1.5	RT	--	--	--	594.72	--	--	--	3259 - DIA 15"	599.29	161	YES	--	FLAT	--	
3260	CB TA 4 DIA T20F&G	307+11	1.5	RT	--	--	--	596.06	--	--	--	3260 - DIA 15"	600.01	162	YES	YES	FLAT	--	
3261	CB TA 4 DIA T20F&G	309+06	1.5	RT	--	--	--	597.03	--	--	--	3261 - DIA 15"	600.77	162	YES	--	FLAT	--	
3351	CB TA 4 DIA T20F&G	311+10	1.5	RT	--	--	--	597.39	--	--	--	3351 - DIA 15"	601.51	162	YES	YES	FLAT	--	
3352	CB TA 4 DIA T20F&G	313+60	1.5	RT	--	--	--	596.23	--	--	--	3352 - DIA 15"	602.30	162	YES	--	TAPERED	--	
3353	CB TA 4 DIA T20F&G	315+10	1.5	RT	--	--	--	595.28	--	--	--	3353 - DIA 15"	602.38	162	YES	--	TAPERED	--	
3354	CB TA 4 DIA T20F&G	317+60	1.5	RT	--	--	--	594.04	--	--	--	3354 - DIA 15"	601.30	162	YES	--	TAPERED	--	
3355	CB TA 4 DIA T20F&G	319+03	1.5	RT	--	--	--	592.64	--	--	--	3355 - DIA 15"	600.01	162	YES	YES	TAPERED	--	
3356	CB TA 4 DIA T20F&G	321+53	1.5	RT	--	--	--	590.18	--	--	--	3356 - DIA 15"	596.57	162	YES	--	TAPERED	--	
3357	CB TA 4 DIA T20F&G	323+17	1.5	RT	--	--	--	588.03	--	--	--	3357 - DIA 15"	593.58	163	YES	YES	TAPERED	--	
3358	CB TA 4 DIA T20F&G	325+67	1.5	RT	--	--	--	584.34	--	--	--	3358 - DIA 15"	588.65	163	YES	--	FLAT	--	
3359	CB TA 4 DIA T20F&G	328+17	1.5	RT	--	--	--	581.39	--	--	--	3359 - DIA 15"	585.25	163	YES	YES	FLAT	--	
3360	CB TA 4 DIA T20F&G	330+67	1.5	RT	--	--	--	579.63	--	--	--	3360 - DIA 15"	583.35	163	YES	--	FLAT	YES	
3361	CB TA 4 DIA T20F&G	332+50	1.5	RT	--	--	--	579.09	--	--	--	3361 - DIA 15"	582.98	163	YES	YES	FLAT	--	
3362	CB TA 4 DIA T20F&G	334+35	1.7	RT	--	--	--	578.01	--	--	--	3362 - DIA 15"	582.70	163	YES	--	FLAT	--	
3363	CB TA 5 DIA T21F&G	335+88	1.5	RT	--	--	575.83	575.97	--	--	1363 - DIA 30"	3363 - DIA 30"	582.35	163	YES	YES	FLAT	--	
3364T	MAN TA 5 DIA T8G	335+88	4.0	RT	--	--	575.83	--	--	--	1363 - DIA 30"	--	580.43	163	YES	--	FLAT	--	
3365	CB TA 4 DIA T20F&G	337+19	1.5	RT	--	--	--	577.64	--	--	--	3365 - DIA 15"	582.11	164	YES	--	FLAT	--	
3366	CB TA 4 DIA T20F&G	339+22	1.5	RT	--	--	--	578.24	--	--	--	3366 - DIA 15"	581.50	164	YES	YES	FLAT	YES	
3381	CB TA 4 DIA T20F&G	340+50	1.5	RT	--	--	--	578.25	--	--	--	3381 - DIA 12"	581.27	164	YES	--	FLAT	YES	
3382	CB TA 4 DIA T21F&G	342+39	1.5	RT	--	--	577.53	577.53	--	--	1382 - DIA 18"	3382 - DIA 18"	580.94	164	YES	YES	FLAT	YES	
3383T	MAN TA 4 DIA T8G	342+39	4.0	RT	--	--	577.53	--	--	--	1382 - DIA 18"	--	579.60	164	YES	--	FLAT	--	
3384	CB TA 4 DIA T20F&G	343+10	1.5	RT	--	--	--	577.62	--	--	--	3384 - DIA 12"	580.65	164	YES	--	FLAT	YES	
3401	CB TA 4 DIA T20F&G	344+90	1.5	RT	--	--	--	576.97	--	--	--	3401 - DIA 15"	580.36	164	YES	YES	FLAT	YES	
3402	CB TA 4 DIA T20F&G	346+27	1.5	RT	--	--	--	576.78	--	--	--	3402 - DIA 15"	580.22	164	YES	--	FLAT	YES	
3403	CB TA 4 DIA T20F&G	348+12	1.5	RT	--	--	--	576.28	--	--	--	3403 - DIA 15"	579.89	164	YES	--	FLAT	YES	

\* FOR THE STRUCTURES INDICATED, IT IS ANTICIPATED THAT THE TOP SLAB OF THE STRUCTURE WILL EXTEND UP INTO THE PROPOSED BARRIER BASE AND SHOULDER PAVEMENT AS SHOWN ON SHEET 188.

**LEGEND**

ES - EXISTING STRUCTURE  
 EP - EXISTING PIPE

**NOTE:**

THE PROPOSED ELBOWS (REINFORCED CONCRETE PIPE ELBOWS) ARE DESIGNED AS A 45 DEGREE ELBOW.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 I-55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE SCHEDULE - IV  
 PROPOSED STRUCTURES**

SCALE: \_\_\_\_\_ DRAWN BY SN  
 DATE 03/22/07 CHECKED BY TKL

**TENG**

TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLT DATE = 03/22/07  
 FILE NAME = 032207.DWG  
 PLOT SCALE = 1:1  
 USER NAME = JUSHER



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	173
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

STRUCTURE NO.	STRUCTURE DESCRIPTION	CENTERLINE			INVERTS			PIPE CONNECTIONS (Pipe No. - Pipe Size)			PROPOSED RIM ELEVATION	SHEET NO.	SC INLET FILTER	UNDERDRAIN CONNECTION	STRUCTURE TOP SLAB	TOP SLAB* CONFLICT	
		STATION	OFFSET	DIR.	NORTH	SOUTH	EAST	WEST	NORTH	SOUTH							EAST
3404	CB TA 4 DIA T21F&G	350+49	1.5	RT	--	--	576.04	--	--	--	3404 - DIA 15"	579.42	164	YES	--	FLAT	YES
3406	CB TA 4 DIA T20F&G	352+00	1.5	RT	--	--	576.12	--	--	--	3406 - DIA 12"	579.14	164	YES	--	FLAT	YES
3422	CB TA 5 DIA T21F&G	353+36	1.5	RT	--	--	574.48	--	--	3422 - DIA 24"	579.01	165	YES	--	FLAT	YES	
3424	CB TA 4 DIA T20F&G	355+00	1.9	RT	--	--	575.04	--	--	3424 - DIA 15"	579.13	165	YES	--	FLAT	YES	
3425	CB TA 4 DIA T20F&G	356+25	1.5	RT	--	--	575.43	--	--	3425 - DIA 15"	579.23	165	YES	--	FLAT	YES	
3451	CB TA 4 DIA T20F&G	357+50	1.5	RT	--	--	575.88	--	--	3451 - DIA 15"	579.35	165	YES	--	FLAT	YES	
3452	CB TA 4 DIA T20F&G	359+05	1.5	RT	--	--	576.36	--	--	3452 - DIA 12"	579.43	165	YES	--	FLAT	YES	
3453	CB TA 4 DIA T20F&G	360+80	1.5	RT	--	--	576.28	--	--	3453 - DIA 15"	579.68	165	YES	--	FLAT	YES	
3455	CB TA 4 DIA T20F&G	363+84	1.5	RT	--	--	575.71	--	--	3455 - DIA 15"	580.01	165	YES	--	FLAT	--	
3456	CB TA 4 DIA T20F&G	365+38	1.5	RT	--	--	576.16	--	--	3456 - DIA 15"	580.20	165	YES	--	FLAT	--	
3457	CB TA 4 DIA T20F&G	366+93	1.5	RT	--	--	576.61	--	--	3457 - DIA 15"	580.32	165	YES	--	FLAT	YES	
3458	CB TA 4 DIA T20F&G	368+20	1.5	RT	--	--	577.11	--	--	3458 - DIA 15"	580.37	166	YES	--	FLAT	YES	
3483	CB TA 4 DIA T20F&G	362+43	1.5	RT	--	--	576.00	--	--	3483 - DIA 15"	579.85	165	YES	--	FLAT	YES	
3501	CB TA 4 DIA T20F&G	369+72	1.5	RT	--	--	577.49	--	--	3501 - DIA 12"	580.57	166	YES	--	FLAT	YES	
3502	CB TA 4 DIA T20F&G	371+11	1.5	RT	--	--	576.99	--	--	3502 - DIA 15"	580.71	166	YES	--	FLAT	--	
3505	CB TA 4 DIA T20F&G	374+25	1.5	RT	--	--	577.44	--	--	3505 - DIA 15"	581.09	166	YES	--	FLAT	YES	
3506	CB TA 4 DIA T20F&G	375+62	1.5	RT	--	--	577.37	--	--	3506 - DIA 15"	581.17	166	YES	--	FLAT	YES	
3507	CB TA 4 DIA T20F&G	377+08	1.5	RT	--	--	577.68	--	--	3507 - DIA 15"	581.33	166	YES	--	FLAT	YES	
3508	CB TA 4 DIA T20F&G	378+55	1.5	RT	--	--	577.91	--	--	3508 - DIA 15"	581.50	166	YES	--	FLAT	YES	
3509	CB TA 4 DIA T20F&G	380+10	1.5	RT	--	--	578.25	--	--	3509 - DIA 15"	581.60	166	YES	--	FLAT	YES	
3510	CB TA 4 DIA T20F&G	381+74	1.5	RT	--	--	578.70	--	--	3510 - DIA 12"	581.76	166	YES	--	FLAT	YES	
3511	CB TA 4 DIA T20F&G	383+07	1.5	RT	--	--	578.86	--	--	3511 - DIA 12"	581.90	167	YES	--	FLAT	YES	
3551	CB TA 4 DIA T20F&G	384+90	1.5	RT	--	--	579.13	--	--	3551 - DIA 12"	582.21	167	YES	--	FLAT	YES	
3552	CB TA 4 DIA T20F&G	386+60	1.5	RT	--	--	579.30	--	--	3552 - DIA 12"	582.33	167	YES	--	FLAT	YES	
3553	CB TA 4 DIA T20F&G	387+97	1.5	RT	--	--	579.43	--	--	3553 - DIA 12"	582.41	167	YES	--	FLAT	YES	
3554	CB TA 4 DIA T21F&G	389+36	1.5	RT	--	--	579.18	--	--	3554 - DIA 15"	582.48	167	YES	--	FLAT	YES	
3556	CB TA 4 DIA T20F&G	390+40	1.5	RT	--	--	579.23	--	--	3556 - DIA 15"	582.60	167	YES	--	FLAT	YES	
3583	CB TA 4 DIA T20F&G	372+66	1.5	RT	--	--	576.58	--	--	3583 - DIA 15"	580.90	166	YES	--	FLAT	--	
3601	CB TA 4 DIA T20F&G	391+52	1.5	RT	--	--	579.63	--	--	3601 - DIA 12"	582.67	167	YES	--	FLAT	YES	
3602	CB TA 4 DIA T20F&G	393+15	1.5	RT	--	--	579.44	--	--	3602 - DIA 15"	582.92	167	YES	--	FLAT	YES	
3603	CB TA 4 DIA T20F&G	394+53	1.5	RT	--	--	579.42	--	--	3603 - DIA 15"	583.11	167	YES	--	FLAT	--	
3606	CB TA 4 DIA T20F&G	397+19	1.5	RT	--	--	579.51	--	--	3606 - DIA 15"	583.30	168	YES	--	FLAT	YES	
3607	CB TA 4 DIA T20F&G	398+50	1.5	RT	--	--	579.54	--	--	3607 - DIA 15"	583.44	168	YES	--	FLAT	--	
3608	CB TA 4 DIA T20F&G	399+80	1.5	RT	--	--	579.82	--	--	3608 - DIA 15"	583.51	168	YES	--	FLAT	--	
3609	CB TA 4 DIA T20F&G	401+58	1.5	RT	--	--	580.06	--	--	3609 - DIA 15"	583.47	168	YES	--	FLAT	YES	
3610	CB TA 4 DIA T20F&G	402+50	1.3	RT	--	--	580.39	--	--	3610 - DIA 12"	583.53	168	YES	--	FLAT	YES	
3684	CB TA 4 DIA T20F&G	395+72	1.5	RT	--	--	579.04	--	--	3684 - DIA 15"	583.13	167	YES	--	FLAT	--	
1032GR	GRATING-C FL END S 24	202+92 NB	64.0	LT	--	--	--	--	--	--	--	155	--	--	--	--	--
1060GR	GRATING-C FL END S 36	220+54	105.0	LT	--	--	--	--	--	--	--	156	--	--	--	--	--
1405GR	GRATING-C FL END S 24	350+48	87.9	LT	--	--	--	--	--	--	--	164	--	--	--	--	--
1162AGR	GRT-C FL END S EGV 24	258+90	63.0	LT	--	--	--	--	--	--	--	158	--	--	--	--	--
1162BGR	GRT-C FL END S EGV 24	258+94	63.0	LT	--	--	--	--	--	--	--	158	--	--	--	--	--

\* FOR THE STRUCTURES INDICATED, IT IS ANTICIPATED THAT THE TOP SLAB OF THE STRUCTURE WILL EXTEND UP INTO THE PROPOSED BARRIER BASE AND SHOULDER PAVEMENT AS SHOWN ON SHEET 188.

**LEGEND**  
 ES - EXISTING STRUCTURE  
 EP - EXISTING PIPE

**NOTE:**  
 THE PROPOSED ELBOWS (REINFORCED CONCRETE PIPE ELBOWS) ARE DESIGNED AS A 45 DEGREE ELBOW.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 I-55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE SCHEDULE - V  
 PROPOSED STRUCTURES**

SCALE: DATE 03/22/07

DRAWN BY SN  
 CHECKED BY TKL

**TENG** TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

DATE = 03/22/07  
 FILE NAME = #P1162A  
 PLOT SCALE = #SCALE  
 USER NAME = #USER

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	174
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PIPE NO.	PIPE DESCRIPTION	PIPE SIZE	LENGTH (FT)	SLOPE (%)	TRENCH BACKFILL (CUYD)	SHEET NO.	FROM STRUCTURE	TO STRUCTURE
1019	SS 2 RCP CL 3 18	DIA 18"	201	2.00	-	154	1020	ES
1020	SS 2 RCP CL 3 15	DIA 15"	15	1.00	7.2	154	1021	1020
1021	SS 2 RCP CL 3 18	DIA 18"	201	1.50	182.3	154	1022	1020
1022	SS 2 RCP CL 3 15	DIA 15"	17	3.00	7.0	154	1023	1022
1023	SS 2 RCP CL 3 15	DIA 15"	196	1.50	174.4	154	3024	1022
1025	SS 2 RCP CL 3 24	DIA 24"	72	0.40	9.9	154	1026	ES
1026	SS 2 RCP CL 3 24	DIA 24"	235	0.60	26.6	154	1027	1026
1027	SS 2 RCP CL 3 15	DIA 15"	233	0.70	23.9	154	1028	1027
1032	SS 2 RCP CL 3 24	DIA 24"	78	0.19	6.9	155	1033	1032
1033	SS 2 RCP CL 3 18	DIA 18"	251	0.50	23.4	155	1034	1033
1034	SS 2 RCP CL 3 15	DIA 15"	250	0.50	25.9	155	1035	1034
1045	SS 2 RCP CL 3 15	DIA 15"	127	0.30	5.7	155	1045	1054
1051	SS 2 RCP CL 3 12	DIA 12"	138	0.37	12.3	155	1051	1052
1053	SS 2 RCP CL 3 18	DIA 18"	211	0.40	19.1	155	1054	1052
1055	SS 2 RCP CL 3 15	DIA 15"	135	0.35	8.0	155	1055	1056
1056	SS 2 RCP CL 3 18	DIA 18"	126	0.30	15.5	156	1056	1057
1057	SS 2 RCP CL 3 18	DIA 18"	119	0.50	22.2	156	1057	1058
1058	SS 2 RCEP S38 R24	SPAN 38", RISE 24"	147	0.15	61.9	156	1059	1058
1060	SS 2 RCP CL 3 36	DIA 36"	98	0.10	32.2	156	1058	1060
1101	SS 2 RCEP S38 R24	SPAN 38", RISE 24"	101	0.15	38.2	156	1102	1059
1102	SS 2 RCEP S38 R24	SPAN 38", RISE 24"	146	0.13	50.1	156	1103	1102
1103	SS 2 RCEP S38 R24	SPAN 38", RISE 24"	131	0.13	40.9	156	1105	1103
1105	SS 2 RCEP S38 R24	SPAN 38", RISE 24"	142	0.13	41.0	156	1106	1105
1106	SS 2 RCEP S38 R24	SPAN 38", RISE 24"	137	0.13	34.9	156	1107	1106
1107	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	99	0.20	17.8	156	1108	1120
1108	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	141	0.20	20.4	157	1109	1108
1109	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	152	0.20	18.7	157	1110	1109
1110	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	113	0.20	11.8	157	1111	1110
1111	SS 1 RCEP S23 R14	SPAN 23", RISE 14"	164	0.22	13.6	157	1112	1111
1112	SS 2 RCP CL 3 15	DIA 15"	155	0.28	7.0	157	1113	1112
1113	SS 2 RCP CL 3 12	DIA 12"	153	0.30	4.6	157	1114	1113
1120	SS 2 RCP CL 3 24	DIA 24"	4	0.20	0.7	156	1120	1121
1121	SS 2 RCP CL 3 24	DIA 24"	46	0.20	7.9	156	1121	1122
1122	SS 2 RCP CL 3 24	DIA 24"	58	0.20	10.9	156	1123	1122
1123	SS 2 RCP CL 3 24	DIA 24"	4	0.20	0.8	156	1122	1123
1150	SS 2 RCP CL 3 15	DIA 15"	146	0.28	14.5	157	1150	1151
1151	SS 2 RCP CL 3 15	DIA 15"	145	0.28	24.1	157	1151	1152
1152	SS 2 RCP CL 3 15	DIA 15"	141	0.28	32.1	157	1152	1153
1153	SS 2 RCP CL 3 18	DIA 18"	137	0.22	41.5	157	1153	1154
1154	SS 2 RCP CL 3 24	DIA 24"	42	0.20	17.5	158	1154	1190
1155	SS 2 RCP CL 3 24	DIA 24"	257	0.20	95.1	158	1155	1156
1156	SS 2 RCP CL 3 24	DIA 24"	245	0.20	53.0	158	1156	1157
1157	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	173	0.20	18.0	158	1157	1195
1158	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	86	0.20	6.4	158	1158	1159
1159	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	104	0.22	8.4	158	1159	1160
1160	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	10	0.36	0.4	158	1160	1161
1161A	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	58	0.18	6.1	158	1161	1162A
1161B	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	58	0.18	6.1	158	1161	1162B
1162	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	93	0.30	7.5	158	1163	1161
1163	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	217	0.30	14.2	159	1164	1163
1164	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	245	0.40	30.9	159	1165	1164
1165	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	245	0.80	49.4	159	1166	1165
1166	SS 2 RCP CL 3 24	DIA 24"	253	0.80	42.2	159	1167	1166
1167	SS 2 RCP CL 3 24	DIA 24"	42	0.40	8.0	159	1168	1167
1168	SS 2 RCP CL 3 24	DIA 24"	14	0.40	3.2	159	1169	1168
1169	SS 2 RCP CL 3 24	DIA 24"	44	0.40	12.0	159	1170	1169
1170	SS 2 RCP CL 3 24	DIA 24"	10	0.40	2.6	159	1171	1170
1171	SS2 RCP CL 3 21	DIA 21"	82	0.30	20.0	159	1179	1171
1172	SS2 RCP CL 3 21	DIA 21"	148	0.30	28.9	159	1173	1178
1173	SS 2 RCP CL 3 18	DIA 18"	108	0.32	15.5	159	1174	1173
1174	SS 2 RCP CL 3 18	DIA 18"	125	0.30	13.4	160	1175	1174
1175	SS 2 RCP CL 3 15	DIA 15"	147	0.30	8.1	160	1176	1175
1176	SS 2 RCP CL 3 12	DIA 12"	142	0.37	4.8	160	1177	1176
1178	SS2 RCP CL 3 21	DIA 21"	127	0.30	29.8	159	1178	1172
1179	SS2 RCP CL 3 21	DIA 21"	125	0.30	31.5	159	1172	1179
1190	SS 2 RCP CL 3 24	DIA 24"	6	0.20	2.5	158	1190	1191
1191	SS 2 RCP CL 3 24	DIA 24"	42	0.20	19.8	158	1191	1192
1192	SS 2 RCP CL 3 24	DIA 24"	6	0.20	2.6	158	1192	1193
1193	SS 2 RCP CL 3 24	DIA 24"	61	0.20	25.8	158	1193	1155
1195	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	5	0.20	0.3	158	1195	1196
1196	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	43	0.20	2.9	158	1196	1197
1197	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	5	0.20	0.3	158	1197	1198
1198	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	19	0.20	1.1	158	1198	1158
1202	SS2 RCP CL 3 21	DIA 21"	166	0.50	54.7	160	1203	1202
1203	SS 2 RCP CL 3 18	DIA 18"	134	0.48	29.9	160	1204	1203
1204	SS 2 RCP CL 3 18	DIA 18"	108	0.35	17.9	160	1205	1204
1205	SS 2 RCP CL 3 18	DIA 18"	140	0.35	17.3	160	1206	1205
1206	SS 2 RCP CL 3 15	DIA 15"	119	0.40	8.1	160	1207	1206
1232	SS 1 RCP CL 4 18	DIA 18"	11	0.30	1.5	160	1233	1232
1233	SS 1 RCP CL 4 18	DIA 18"	124	0.30	14.9	161	1234	1233

PIPE NO.	PIPE DESCRIPTION	PIPE SIZE	LENGTH (FT)	SLOPE (%)	TRENCH BACKFILL (CUYD)	SHEET NO.	FROM STRUCTURE	TO STRUCTURE
1234	SS 1 RCP CL 4 15	DIA 15"	143	0.30	10.7	161	1235	1234
1235	SS 2 RCP CL 3 12	DIA 12"	125	0.40	6.2	161	1253	1235
1254	SS 2 RCP CL 3 15	DIA 15"	210	0.32	23.1	161	1254	1255
1255	SS 2 RCP CL 3 18	DIA 18"	225	0.30	59.8	161	1255	1256
1256	SS 2 RCP CL 3 18	DIA 18"	165	0.70	53.9	161	1257	1256
1257	SS 2 RCP CL 3 18	DIA 18"	133	0.70	35.7	161	1258	1257
1258	SS 2 RCP CL 3 18	DIA 18"	115	0.70	25.1	161	1259	1258
1259	SS 2 RCP CL 3 15	DIA 15"	191	0.70	27.0	162	1260	1259
1260	SS 2 RCP CL 3 15	DIA 15"	191	0.50	18.8	162	1261	1260
1351	SS 2 RCP CL 3 15	DIA 15"	246	0.48	59.1	162	1351	1352
1352	SS 2 RCP CL 3 15	DIA 15"	146	0.64	60.7	162	1352	1353
1353	SS 2 RCP CL 3 15	DIA 15"	246	0.50	179.2	162	1353	1354
1354	SS 2 RCP CL 3 15	DIA 15"	139	1.01	103.5	162	1354	1355
1355	SS 2 RCP CL 3 18	DIA 18"	246	1.00	118.7	162	1355	1356
1356	SS 2 RCP CL 3 18	DIA 18"	160	1.34	57.9	162	1356	1357
1357	SS 2 RCP CL 3 18	DIA 18"	246	1.75	67.2	163	1357	1358
1358	SS 2 RCP CL 3 18	DIA 18"	246	1.00	50.6	163	1358	1359
1359	SS 1 RCEP S30 R19	SPAN 30", RISE 19"	245	0.36	37.1	163	1359	1360
1360	SS 2 RCP CL 3 24	DIA 24"	178	0.30	29.5	163	1360	1361
1361	SS 2 RCP CL 3 24	DIA 24"	96	0.30	18.0	163	1361	1373
1362	SS 2 RCP CL 3 27	DIA 27"	148	0.58	44.5	163	1362	1363
1363	SS 2 RCP CL 3 30	DIA 30"	56	0.40	22.0	163	1363	1364
1364	SS 2 RCP CL 3 15	DIA 15"	127	0.30	25.7	164	1365	1363
1365	SS 2 RCP CL 3 15	DIA 15"	199	0.30	21.5	164	1366	1365
1370	SS 2 RCP CL 3 24	DIA 24"	31	0.70	7.0	163	1370	1362
1371	SS 2 RCP CL 3 24	DIA 24"	5	0.50	1.0	163	1371	1370
1372	SS 2 RCP CL 3 24	DIA 24"	43	0.50	8.7	163	1372	1371
1373	SS 2 RCP CL 3 24	DIA 24"	5	0.50	1.0	163	1373	1372
1381	SS 2 RCP CL 3 12	DIA 12"	185	0.37	9.6	164	1381	1382
1382	SS 2 RCP CL 3 18	DIA 18"	58	0.40	7.8	164	1382	1383
1383	SS 2 RCP CL 3 12	DIA 12"	67	0.37	3.0	164	1384	1382
1401	SS 1 RCP CL 4 15	DIA 15"	133	0.29	5.3	164	1401	1402
1402	SS 1 RCP CL 4 15	DIA 15"	181	0.29	12.4	164	1402	1403
1403	SS 2 RCP CL 3 18	DIA 18"	233	0.22	19.7	164	1403	1404
1404	SS 2 RCEP S30 R19	SPAN 30", RISE 19"	81	0.20	9.5	164	1404	1405
1405	SS 2 RCP CL 3 12	DIA 12"	147	0.37	8.5	164	1406	1404
1420	SS 2 RCP CL 3 24	DIA 24"	81	0.50	12.0	165	1422	1421
1422	SS 2 RCP CL 3 24	DIA 24"	86	0.20	10.6	165	1423	1422
1423	SS 2 RCP CL 3 24	DIA 24"	16	0.20	1.7	165	1424	1433
1424	SS 2 RCP CL 3 15	DIA 15"	121	0.32	11.3	165	1425	1424
1431	SS 2 RCP CL 3 24	DIA 24"	46	0.20	6.9	165	1432	1431
1432	SS 2 RCP CL 3 24	DIA 24"	6	0.20	0.8	165	1431	1423
1433	SS 2 RCP CL 3 24	DIA 24"	6	0.20	0.8	165	1433	1432
1451	SS 2 RCP CL 3 15	DIA 15"	121	0.28	6.4	165	1451	1425
1452	SS 2 RCP CL 3 12	DIA 12"	171	0.30	9.2	165	1452	1453
1453	SS 2 RCP CL 3 18	DIA 18"	159	0.22	15.3	165	1453	1483
1454	SS 2 RCP CL 3 18	DIA 18"	43	0.30	7.3	165	1455	1454
1455	SS 2 RCP CL 3 18	DIA 18"	150	0.30	19.8	165	1456	1455
1456	SS 2 RCP CL 3 18	DIA 18"	151	0.30	14.2	165	1457	1456
1457	SS 2 RCP CL 3 15	DIA 15"	123	0.40	6.0	166	1458	1457
1483	SS2 RCP CL 3 21	DIA 21"	82	0.18	12.0	165	1483	1454
1501	SS 2 RCP CL 3 12	DIA 12"	1					

CONTRACT NO. 62895

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	175

STA. TO STA.  
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

PIPE NO.	PIPE DESCRIPTION	PIPE SIZE	LENGTH (FT)	SLOPE (%)	TRENCH BACKFILL (CUYD)	SHEET NO.	FROM STRUCTURE	TO STRUCTURE
1606	SS 2 RCP CL 3 18	DIA 18"	126	0.22	30.8	168	1607	1606
1607	SS 2 RCP CL 3 18	DIA 18"	126	0.22	27.8	168	1608	1607
1608	SS 2 RCP CL 3 15	DIA 15"	174	0.28	32.3	168	1609	1608
1609	SS 2 RCP CL 3 12	DIA 12"	88	0.37	12.6	168	1610	1609
1610	SS 2 RCP CL 3 36	DIA 36"	169	0.12	10.7	163	1364	1611
1684	SS 2 RCP CL 3 18	DIA 18"	5	0.25	1.3	167	1684	1604
1711	SS 2 RCP CL 3 12	DIA 12"	46	0.44	-	163	1711	1712
1721	SS 2 RCP CL 3 12	DIA 12"	43	0.23	-	164	1721	1722
1900	SS JKD CL A 24	DIA 24"	73	0.15	-	161	1902	1900
1901	SS 2 RCP CL 3 24	DIA 24"	90	0.15	-	161	1901	1902
3024	SS 2 RCP CL 3 15	DIA 15"	13	1.00	5.7	154	1024	3024
3026	SS 2 RCP CL 3 15	DIA 15"	7	1.00	0.4	154	3026	1026
3027	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	154	3027	1027
3028	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	154	3028	1028
3033	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	155	3033	1033
3034	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	155	3034	1034
3035	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	155	3035	1035
3045	SS 2 RCP CL 3 15	DIA 15"	3	0.50	0.2	155	3045	1045
3051	SS 2 RCP CL 3 12	DIA 12"	6	1.00	0.3	155	3051	1051
3052	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	155	3052	1052
3054	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	155	3054	1054
3055	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	155	3055	1055
3056	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	156	3056	1056
3057	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	156	3057	1057
3058	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	156	3058	1058
3059	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.8	156	3059	1059
3100	SS 2 RCP CL 3 18	DIA 18"	20	0.73	-	156	3100	3101
3102	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.8	156	3102	1102
3103	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.7	156	3103	1103
3105	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.6	156	3105	1105
3106	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.6	156	3106	1106
3107	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	156	3107	1107
3108	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	156	3108	1108
3109	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	157	3109	1109
3110	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	157	3110	1110
3111	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	157	3111	1111
3112	SS 2 RCP CL 3 15	DIA 15"	3	0.40	0.3	157	3112	1112
3113	SS 2 RCP CL 3 15	DIA 15"	3	0.40	0.2	157	3113	1113
3114	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.2	157	3114	1114
3120	SS 2 RCP CL 3 15	DIA 15"	3	0.50	0.7	156	3120	1120
3150	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	157	3150	1150
3151	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	157	3151	1151
3152	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.7	157	3152	1152
3153	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.9	157	3153	1153
3154	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.9	158	3154	1154
3155	SS 2 RCP CL 3 15	DIA 15"	3	1.00	1.1	158	3155	1155
3156	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.8	158	3156	1156
3157	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	158	3157	1157
3158	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	158	3158	1158
3159	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	158	3159	1159
3160	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	158	3160	1160
3161	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	158	3161	1161
3163	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	158	3163	1163
3164	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	159	3164	1164
3165	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	159	3165	1165
3166	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	159	3166	1166
3167	SS 2 RCP CL 3 15	DIA 15"	5	1.00	1.0	159	3167	1167
3171	SS 2 RCP CL 3 15	DIA 15"	6	1.00	1.3	159	3171	1171
3172	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.7	159	3172	1172
3173	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.6	159	3173	1173
3174	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	159	3174	1174
3175	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	160	3175	1175
3176	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	160	3176	1176
3177	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.2	160	3177	1177
3178	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.7	159	3178	1178
3179	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.9	159	3179	1179
3202	SS 2 RCP CL 3 24	DIA 24"	3	0.30	1.5	160	3202	1202
3203	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.9	160	3203	1203
3204	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.7	160	3204	1204
3205	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.6	160	3205	1205
3206	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	160	3206	1206
3207	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	160	3207	1207
3232	SS 2 RCP CL 3 24	DIA 24"	3	3.00	0.7	160	3232	1232
3233	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.6	160	3233	1233
3234	SS 1 RCP CL 4 15	DIA 15"	3	1.00	0.5	161	3234	1234
3235	SS 1 RCP CL 4 15	DIA 15"	3	1.00	0.4	161	3235	1235
3253	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.2	161	3253	1253
3254	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	161	3254	1254
3255	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.7	161	3255	1255
3256	SS 1 RCP CL 2 48	DIA 48"	3	0.80	1.6	161	3256	1256

PIPE NO.	PIPE DESCRIPTION	PIPE SIZE	LENGTH (FT)	SLOPE (%)	TRENCH BACKFILL (CUYD)	SHEET NO.	FROM STRUCTURE	TO STRUCTURE
3257	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.9	161	3257	1257
3258	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.7	161	3258	1258
3259	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	161	3259	1259
3260	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	162	3260	1260
3261	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	162	3261	1261
3351	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	162	3351	1351
3352	SS 2 RCP CL 3 15	DIA 15"	3	1.00	1.0	162	3352	1352
3353	SS 2 RCP CL 3 15	DIA 15"	3	1.00	2.1	162	3353	1353
3354	SS 2 RCP CL 3 15	DIA 15"	3	1.00	2.2	162	3354	1354
3355	SS 2 RCP CL 3 15	DIA 15"	3	1.00	2.3	162	3355	1355
3356	SS 2 RCP CL 3 15	DIA 15"	3	1.00	1.1	162	3356	1356
3357	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.8	163	3357	1357
3358	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	163	3358	1358
3359	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	163	3359	1359
3360	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	163	3360	1360
3361	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	163	3361	1361
3362	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.6	163	3362	1362
3363	SS 2 RCP CL 3 30	DIA 30"	3	0.60	1.1	163	3363	1363
3365	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	164	3365	1365
3366	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	164	3366	1366
3381	SS 2 RCP CL 3 12	DIA 12"	3	0.40	0.1	164	3381	1381
3382	SS 2 RCP CL 3 18	DIA 18"	3	0.40	0.1	164	3382	1382
3384	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.1	164	3384	1384
3401	SS 1 RCP CL 4 15	DIA 15"	3	1.00	0.1	164	3401	1401
3402	SS 1 RCP CL 4 15	DIA 15"	3	1.00	0.1	164	3402	1402
3403	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	164	3403	1403
3404	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	164	3404	1404
3406	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.1	164	3406	1406
3420	SS 2 RCP CL 3 24	DIA 24"	14	6.40	0.6	165	EP	3422
3422	SS 2 RCP CL 3 24	DIA 24"	3	1.00	0.2	165	3422	1422
3424	SS 2 RCP CL 3 15	DIA 15"	3	0.85	0.1	165	3424	1424
3425	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	165	3425	1425
3451	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	165	3451	1451
3452	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.1	165	3452	1452
3453	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	165	3453	1453
3455	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	165	3455	1455
3456	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	165	3456	1456
3457	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	165	3457	1457
3458	SS 1 RCP CL 4 15	DIA 15"	3	1.00	0.1	166	3458	1458
3483	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.3	165	3483	1483
3501	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.1	166	3501	1501
3502	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	166	3502	1502
3505	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.2	166	3505	1505
3506	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	166	3506	1506
3507	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	166	3507	1507
3508	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	166	3508	1508
3509	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.1	166	3509	1509
3510	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.1	166	3510	1510
3511	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.1	167	3511	1511
3551	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.2	167	3551	1551
3552	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.3	167	3552	1552
3553	SS 2 RCP CL 3 12	DIA 12"	3	1.00	0.3	167	3553	1553
3554	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.5	167	3554	1554
3556	SS 2 RCP CL 3 15	DIA 15"	3	1.00	0.4	167	3556	1556
3583	SS 2 RCP CL 3 15	DIA 15"	3	1.0				

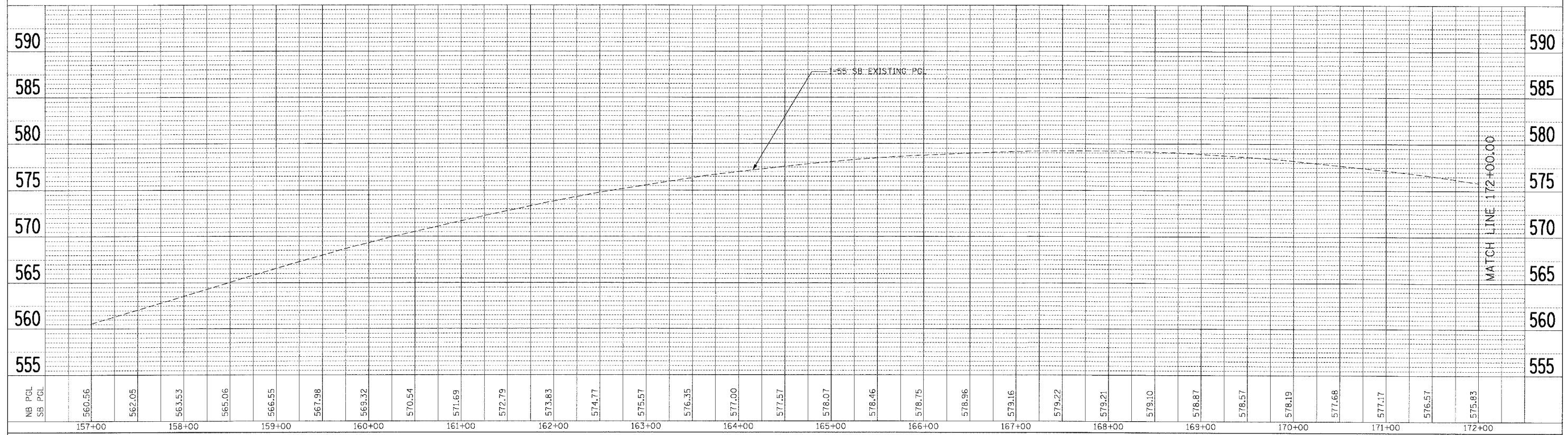
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	176
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

**LEGEND**  
 L - LENGTH OF PIPE  
 S - SLOPE OF PIPE  
 SS-1000 - PIPE NUMBER  
 DS-1000 - STRUCTURE NUMBER

PLAN

REVISION	DATE
BY	
NO.	
DATE	

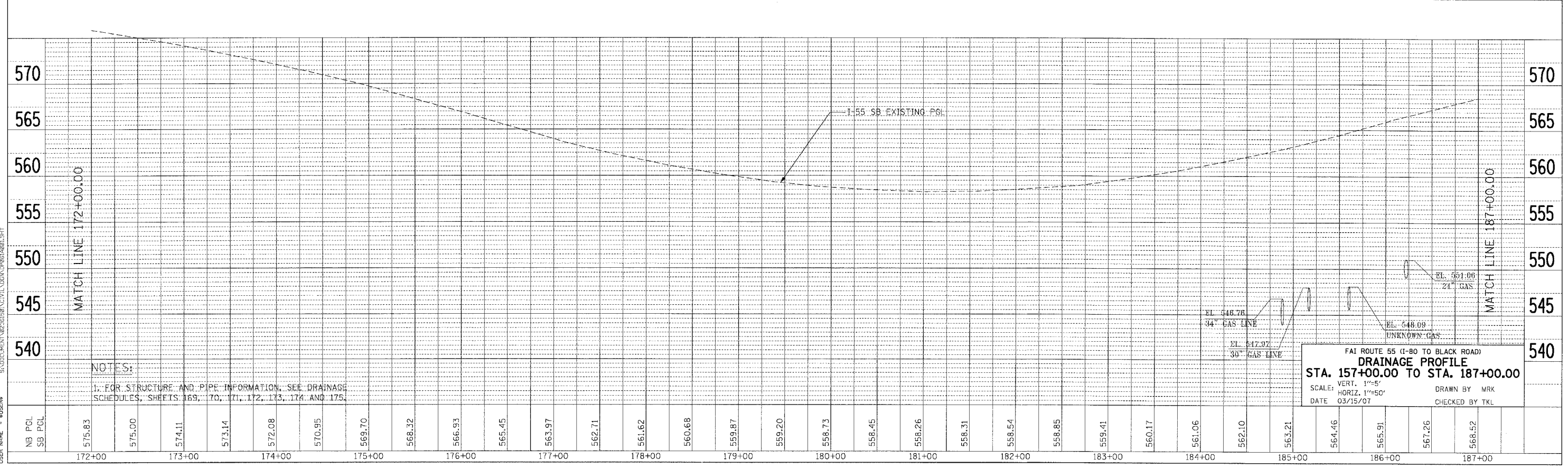
NOTE BOOK NO. \_\_\_\_\_  
 ALIGNED CHECKED \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 DATE FILED \_\_\_\_\_



PROFILE

REVISION	DATE
BY	
NO.	
DATE	

NOTE BOOK NO. \_\_\_\_\_  
 GRAVITY CHECKED \_\_\_\_\_  
 STRUCTURE NOTATION CHECKED \_\_\_\_\_



**NOTES:**  
 1. FOR STRUCTURE AND PIPE INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
 STA. 157+00.00 TO STA. 187+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 03/15/07  
 DRAWN BY MRK  
 CHECKED BY TKL

PLOT DATE = 03/15/07  
 PLOT SCALE = 1"=50'  
 USER NAME = 0105076

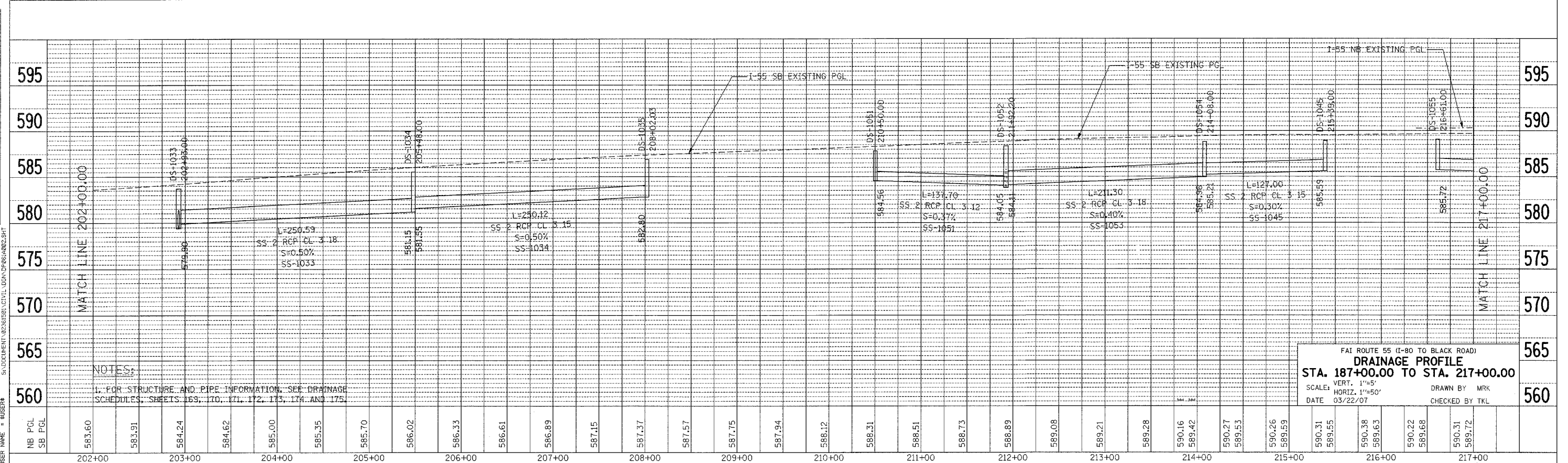
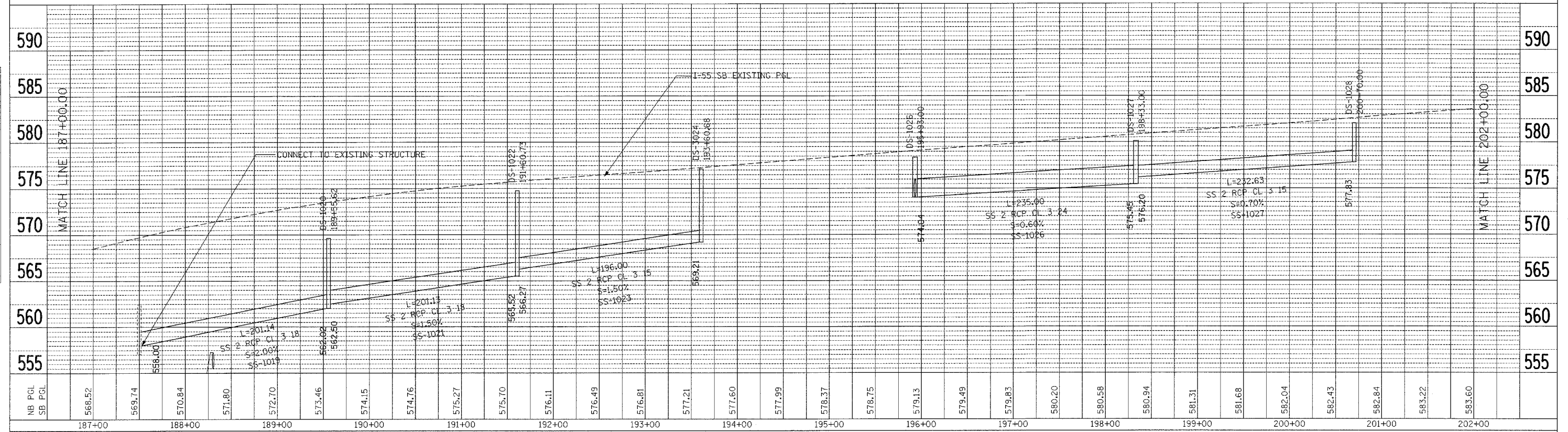
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	177
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

LEGEND

L - LENGTH OF PIPE  
 S - SLOPE OF PIPE  
 SS-1000 - PIPE NUMBER  
 DS-1000 - STRUCTURE NUMBER

PLAN	DATE
NO. _____	BY _____
NO. _____	DATE _____

PROFILE	DATE
NO. _____	BY _____
NO. _____	DATE _____



NOTES:

1. FOR STRUCTURE AND PIPE INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
 STA. 187+00.00 TO STA. 217+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 03/22/07  
 DRAWN BY MRK  
 CHECKED BY TKL

PLOT DATE = 03/22/07  
 FILE NAME = #FILEL4  
 PLOT SCALE = #SCALE4  
 USER NAME = #USER#

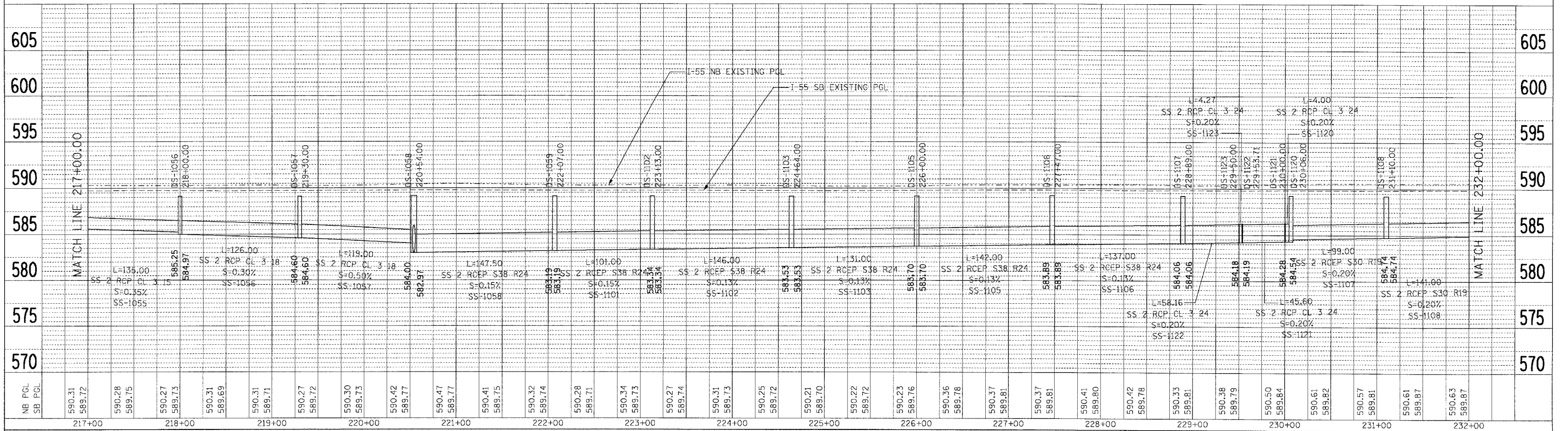
3-22-2007 13:23:44  
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	178
STA. TO STA.		FED. AID PROJECT		
FED. ROAD DIST. NO.		ILLINOIS		

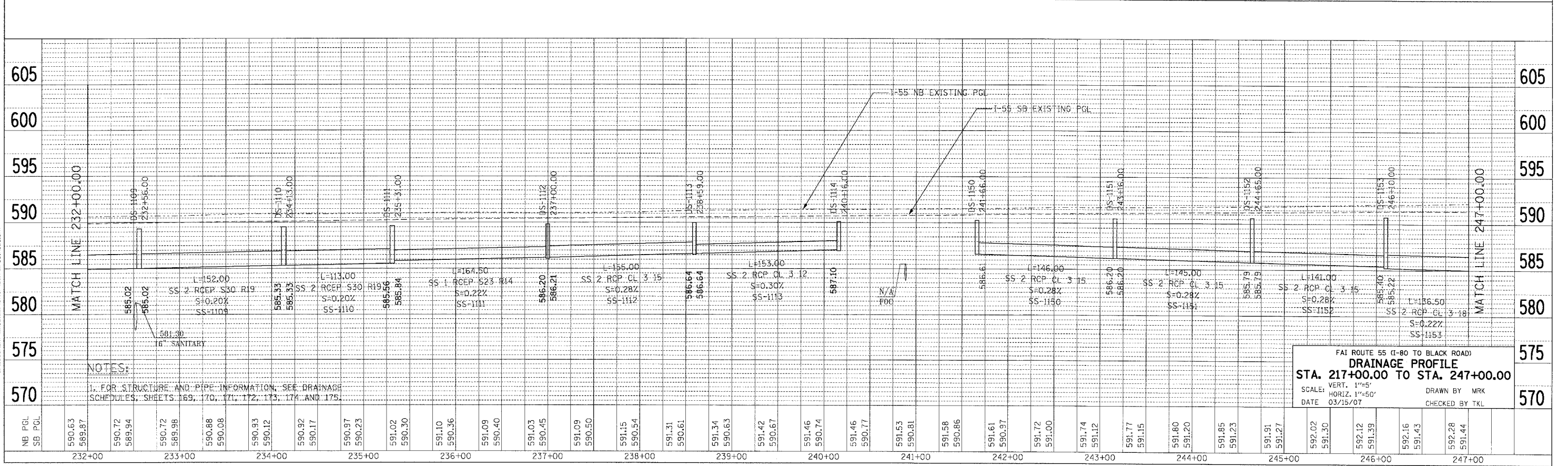
**LEGEND**

- L - LENGTH OF PIPE
- S - SLOPE OF PIPE
- SS-1000 - PIPE NUMBER
- DS-1000 - STRUCTURE NUMBER

PLAN	SURVEYED	DATE
NOTED	PLOTTED	BY
NO.	FILED	
	REVISIONS	
	DATE	
	BY	
	DESCRIPTION	



PROFILE	SURVEYED	DATE
NOTED	PLOTTED	BY
NO.	FILED	
	REVISIONS	
	DATE	
	BY	
	DESCRIPTION	



NOTES:  
1. FOR STRUCTURE AND PIPE INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

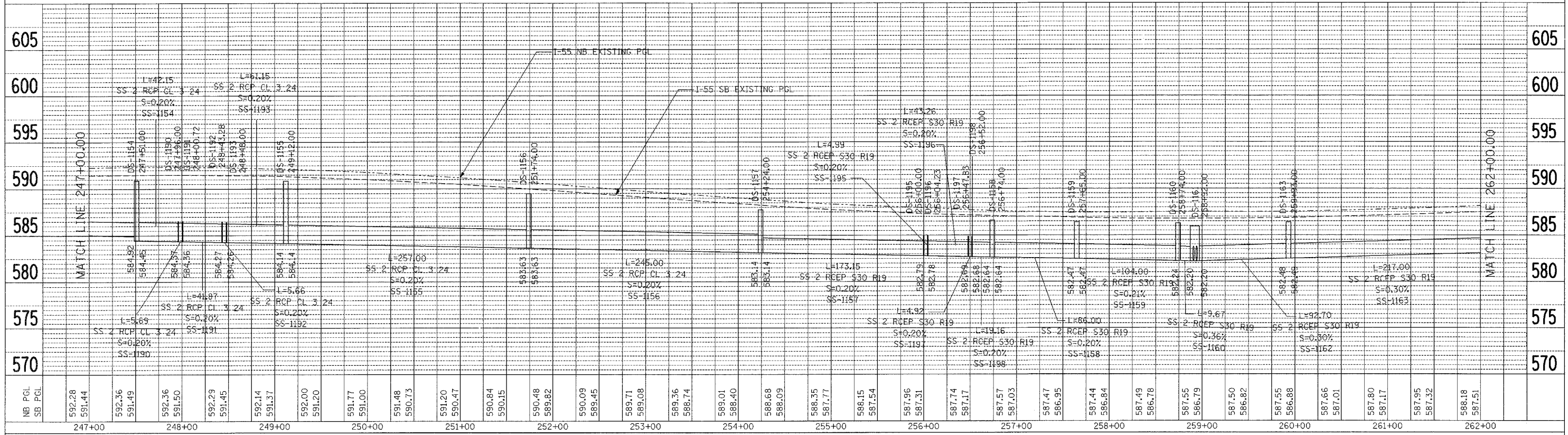
F&I ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
STA. 217+00.00 TO STA. 247+00.00  
SCALE: VERT. 1"=5'  
HORIZ. 1"=50'  
DATE 03/15/07  
DRAWN BY WRK  
CHECKED BY TKL

PLOT DATE = 03/15/07  
PLOT SCALE = 1"=5'  
PLOT FILE NAME = 62895.DWG  
PLOT USER = WRK

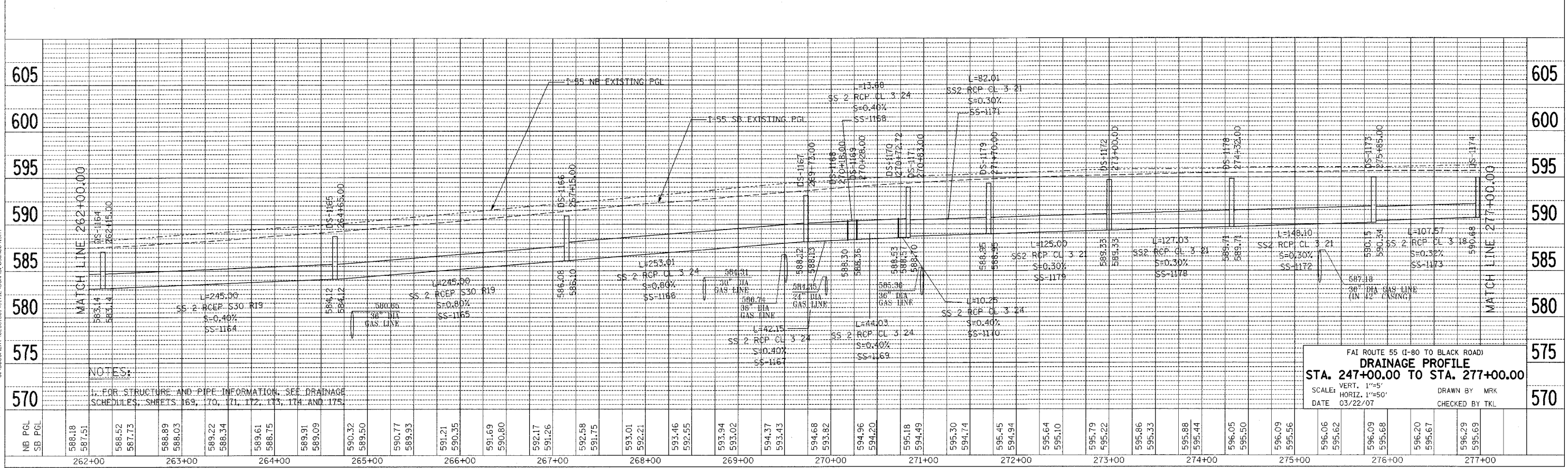
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	179
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

**LEGEND**  
 L - LENGTH OF PIPE  
 S - SLOPE OF PIPE  
 SS-1000 - PIPE NUMBER  
 DS-1000 - STRUCTURE NUMBER

PLAN	DATE
REVISED	
BY	
DATE	
NO.	



PROFILE	DATE
REVISED	
BY	
DATE	
NO.	



**NOTES:**  
 1. FOR STRUCTURE AND PIPE INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
 STA. 247+00.00 TO STA. 277+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 03/22/07  
 DRAWN BY MRK  
 CHECKED BY TKL

PLOT DATE = 03/22/07  
 FILE NAME = 03/22/07\_1302323  
 PLOT SCALE = 5/8"=1'  
 USER NAME = JUSEN

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	180
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

LEGEND

- L - LENGTH OF PIPE
- S - SLOPE OF PIPE
- SS-1000 - PIPE NUMBER
- DS-1000 - STRUCTURE NUMBER

PLAN

REVIEWED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

NOTED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

ALIGNED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PLANNED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

NO. \_\_\_\_\_

PROFILE

REVIEWED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

NOTED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

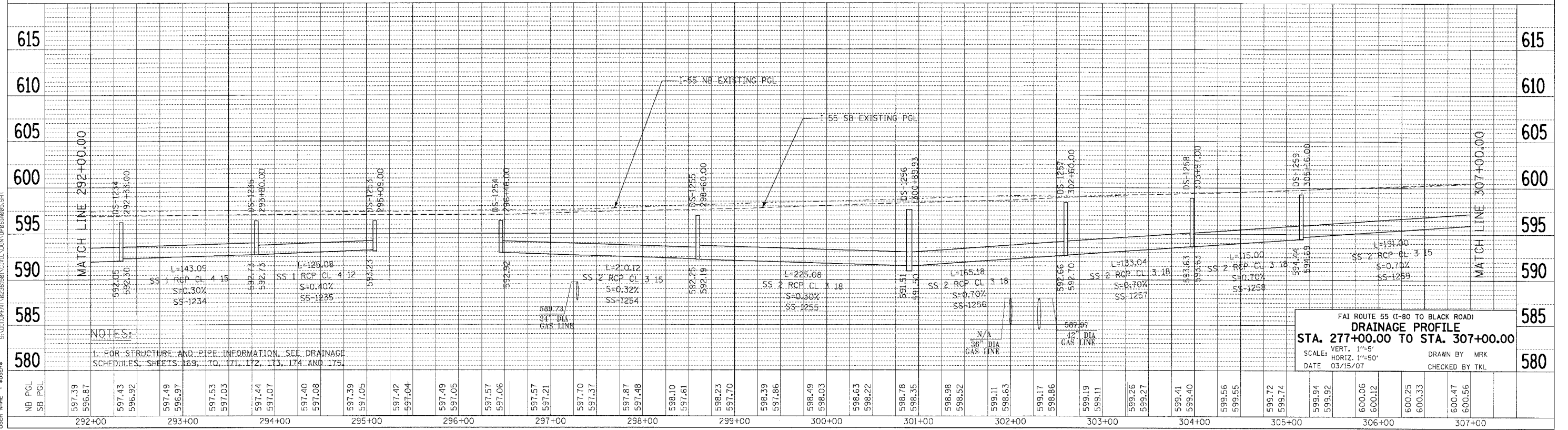
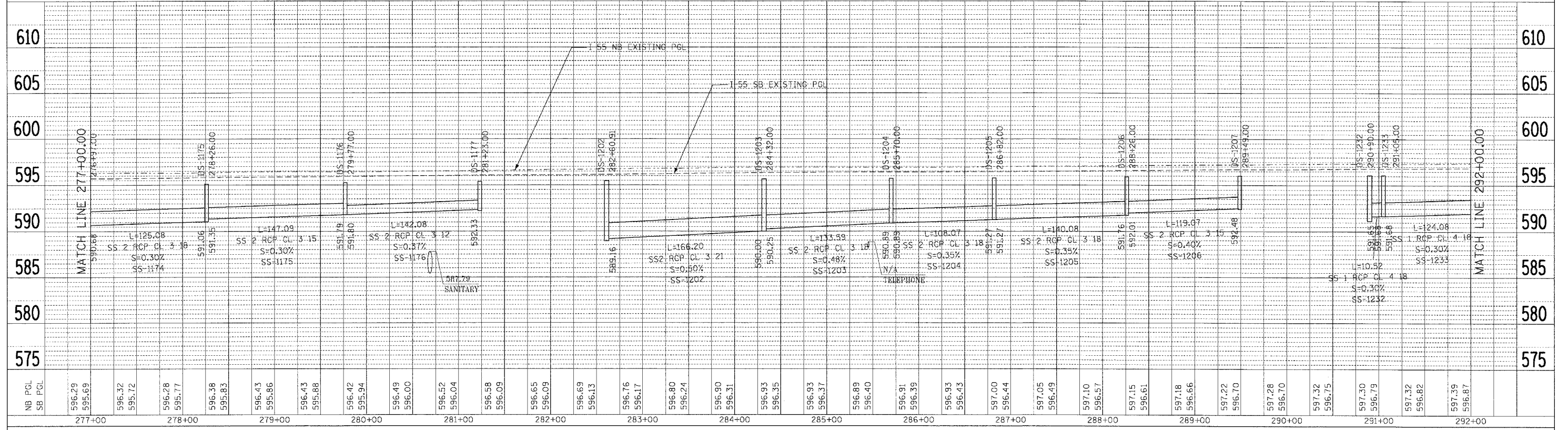
ALIGNED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

PLANNED BY: \_\_\_\_\_ DATE: \_\_\_\_\_

NO. \_\_\_\_\_

PLOT DATE = 03/15/07  
 PLOT SCALE = 1"=50'  
 USER NAME = RUSSELL



NOTES:

1. FOR STRUCTURE AND PIPE INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

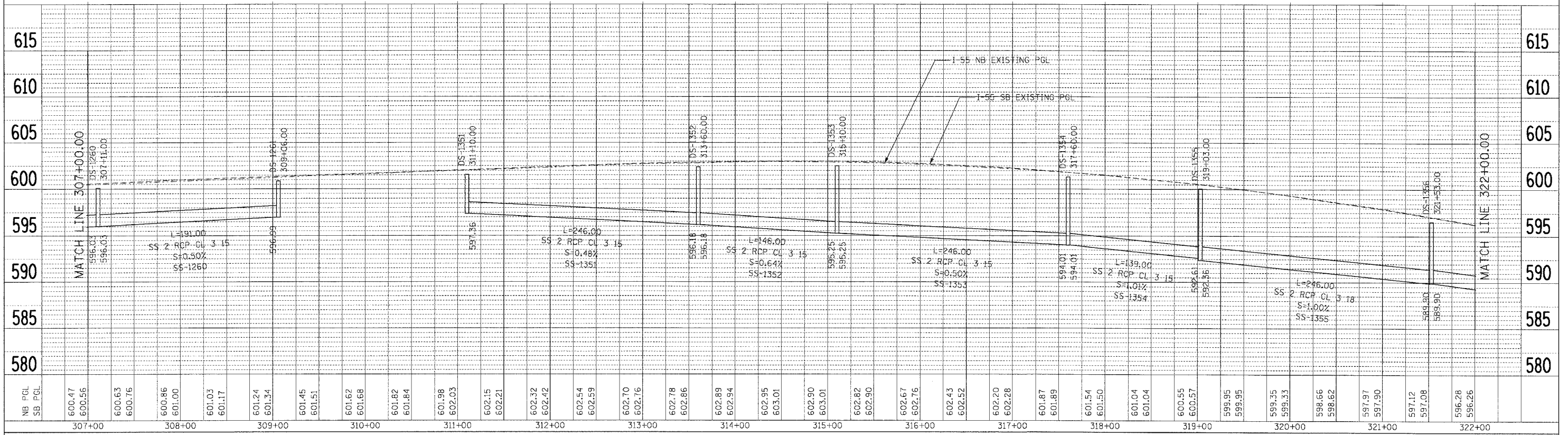
FBI ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
 STA. 277+00.00 TO STA. 307+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 03/15/07  
 DRAWN BY MKK  
 CHECKED BY TKL



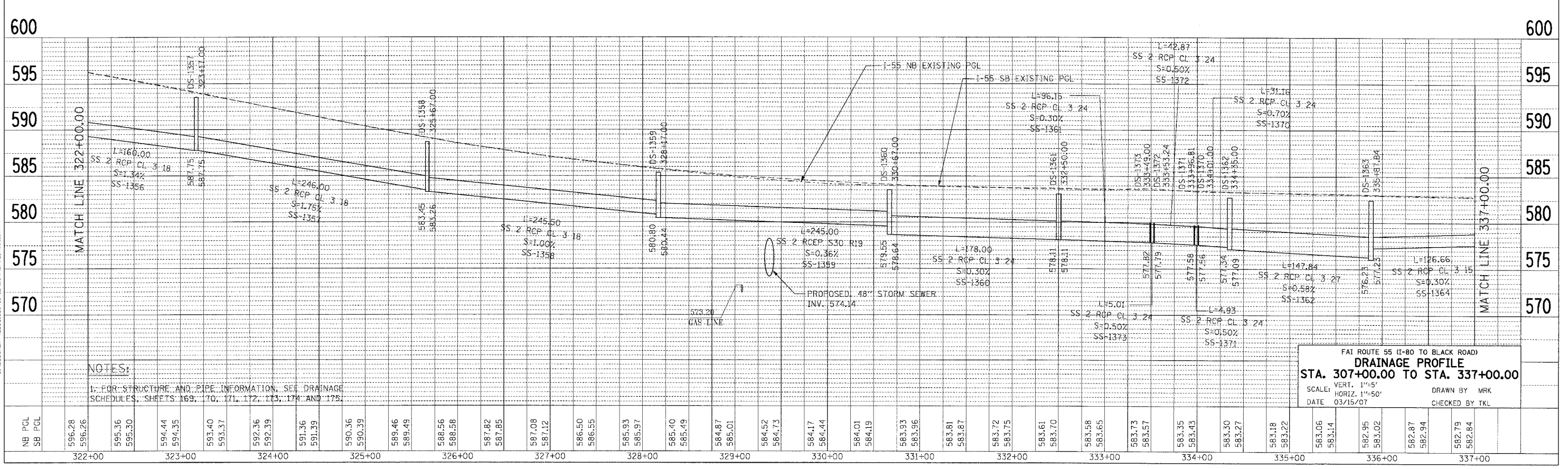
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	181
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

**LEGEND**  
 L - LENGTH OF PIPE  
 S - SLOPE OF PIPE  
 SS-1000 - PIPE NUMBER  
 DS-1000 - STRUCTURE NUMBER

PLAN SUBMITTED BY DATE  
 PLOTTED BY  
 CHECKED BY  
 NOTE BOOK NO.  
 STRUCTURE NOTATION CHD  
 NO.



PROFILE SUBMITTED BY DATE  
 PLOTTED BY  
 CHECKED BY  
 NOTE BOOK NO.  
 STRUCTURE NOTATION CHD  
 NO.



**NOTES:**  
 1. FOR STRUCTURE AND PIPE INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

FBI ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
**STA. 307+00.00 TO STA. 337+00.00**  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 03/15/07  
 DRAWN BY MRK  
 CHECKED BY TKL

PLOT DATE = 03/15/07  
 FILE NAME = 01FILE.DWG  
 PLOT SCALE = 1"=50'  
 USER NAME = RUSERS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	182
STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

**LEGEND**  
 L - LENGTH OF PIPE  
 S - SLOPE OF PIPE  
 SS-1000 - PIPE NUMBER  
 DS-1000 - STRUCTURE NUMBER

**PLAN**

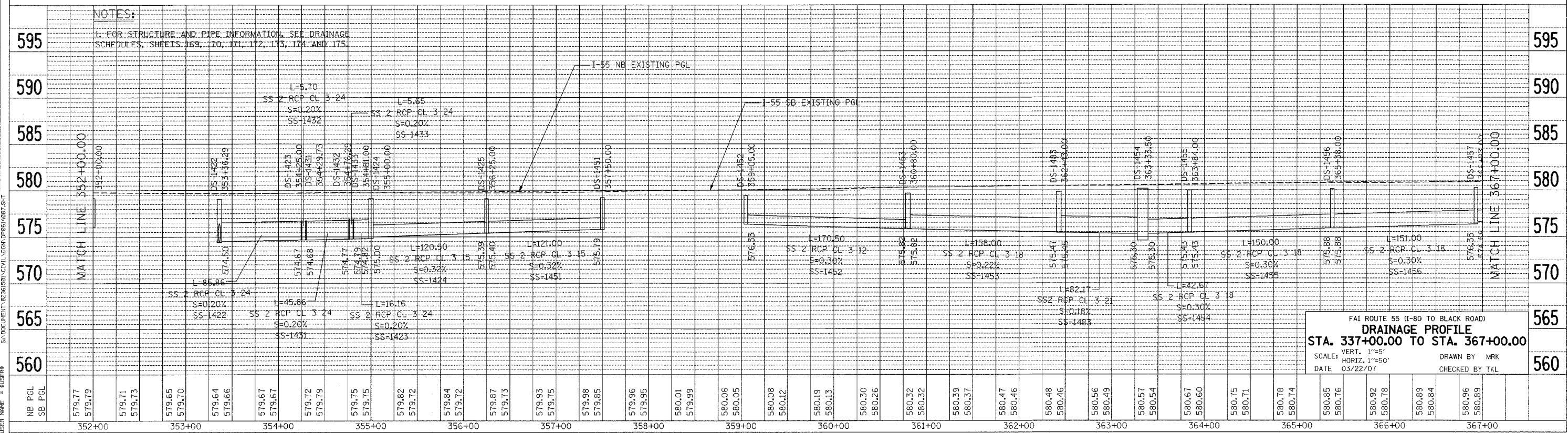
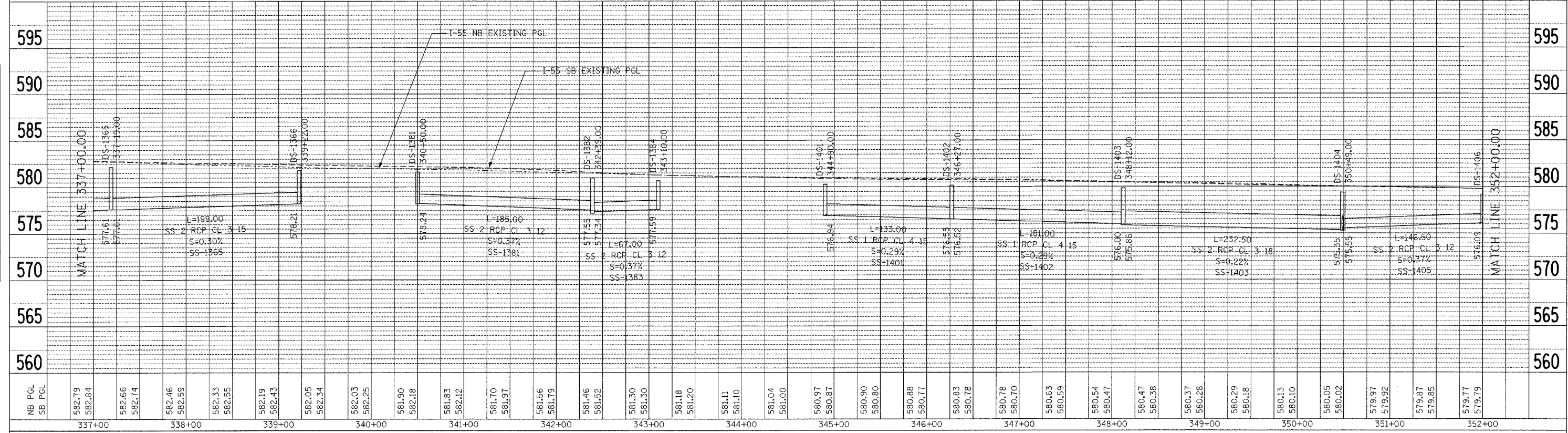
DATE	BY

NO. \_\_\_\_\_

**PROFILE**

DATE	BY

NO. \_\_\_\_\_



FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
 STA. 337+00.00 TO STA. 367+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 03/22/07  
 DRAWN BY MRK  
 CHECKED BY TKL

PLOT DATE = 03/28/07  
 FILE NAME = FILE14  
 USER NAME = MUSEER

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	183
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

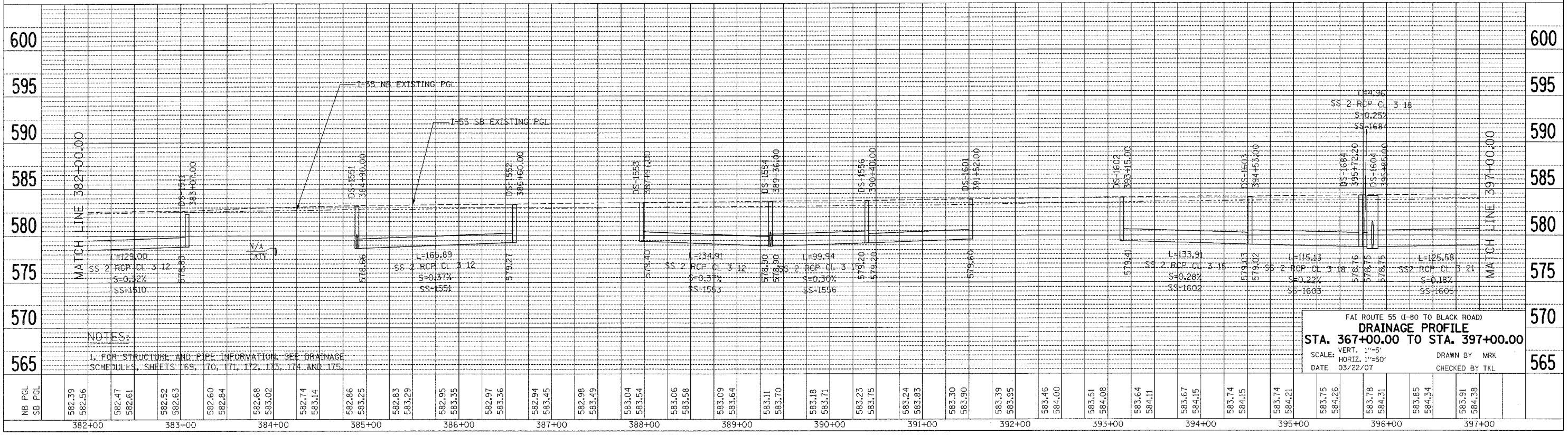
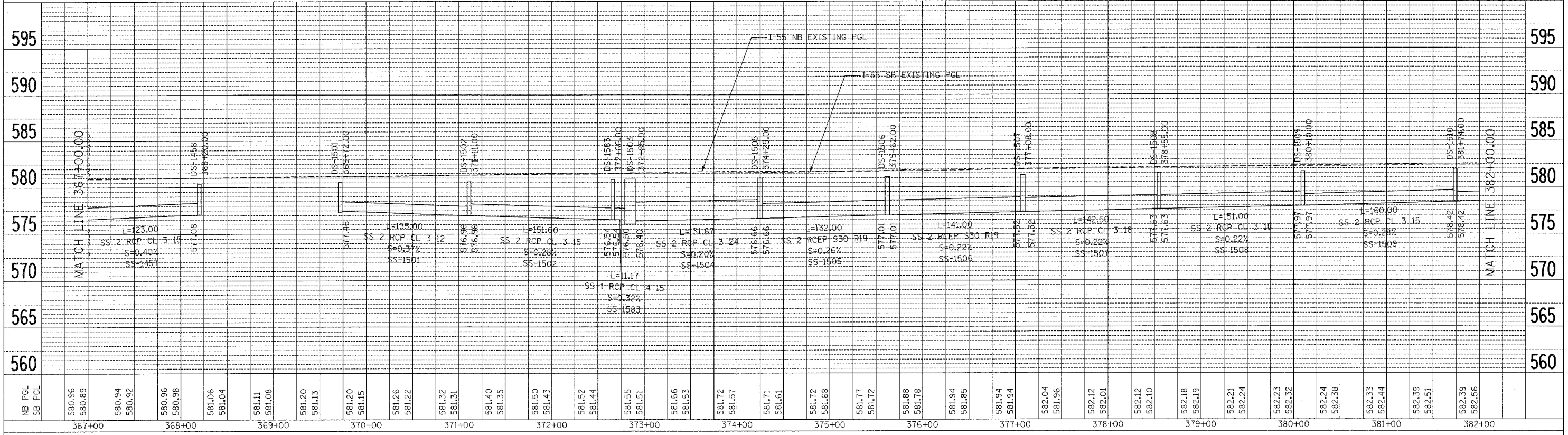
**LEGEND**

- L - LENGTH OF PIPE
- S - SLOPE OF PIPE
- SS-1000 - PIPE NUMBER
- DS-1000 - STRUCTURE NUMBER

PLAN	SURVEYED	BY	DATE
PLotted	CHECKED		
NOTE BOOK	NO. OF SHEETS		
	FILE NAME		
	ADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
Plotted	CHECKED		
NOTE BOOK	NO. OF SHEETS		
	FILE NAME		
	ADD FILE NAME		

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 PLOT DATE = 04/07/07  
 FILE NAME = 04R08.SHT  
 PLOT SCALE = 1"=50'  
 USER NAME = 04R08.SHT



NOTES:  
 1. FOR STRUCTURE AND PIPE INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
 STA. 367+00.00 TO STA. 397+00.00  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 03/22/07  
 DRAWN BY MRK  
 CHECKED BY TKL

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	184
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

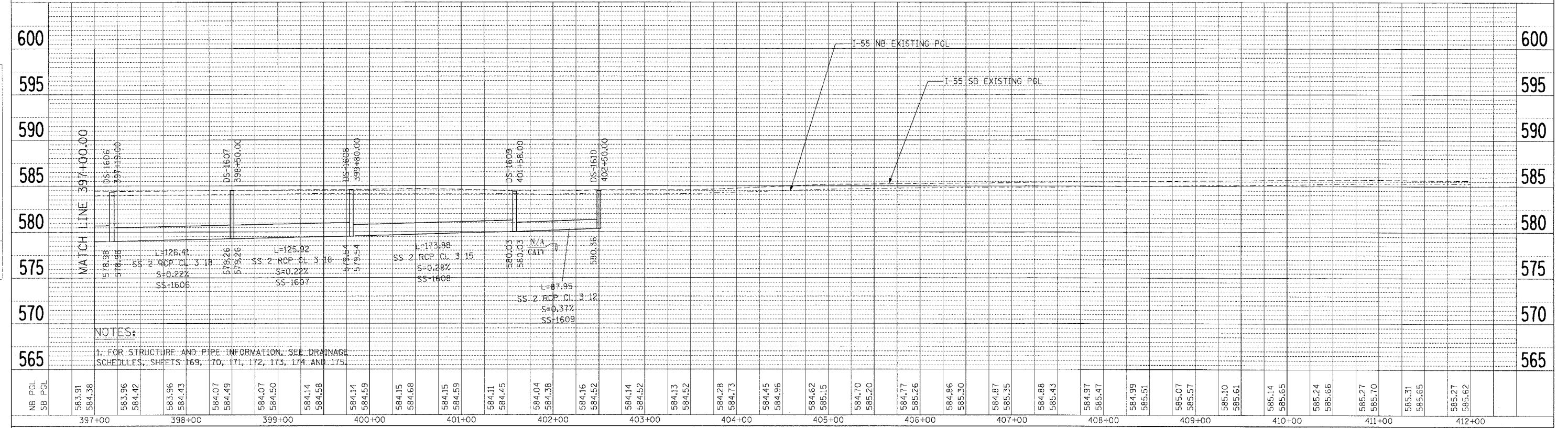
**LEGEND**

- L - LENGTH OF PIPE
- S - SLOPE OF PIPE
- SS-1000 - PIPE NUMBER
- DS-1000 - STRUCTURE NUMBER

PLAN	DATE
BY	
NO. _____	
NO. _____	
NO. _____	
NO. _____	
NO. _____	
NO. _____	

PROFILE	DATE
BY	
NO. _____	
NO. _____	
NO. _____	
NO. _____	
NO. _____	
NO. _____	

PLOT DATE = 03/15/07  
 FILE NAME = 03FILES  
 PLOT SCALE = 1"=50'  
 USER NAME = GUSPER



FAI ROUTE 55 (I-80 TO BLACK ROAD)  
**DRAINAGE PROFILE**  
**STA. 397+00.00 TO STA. 402+65.07**  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=50'  
 DATE 03/15/07  
 DRAWN BY MRK  
 CHECKED BY TKL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) R-6	WILL	185	185
STA. TO STA.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

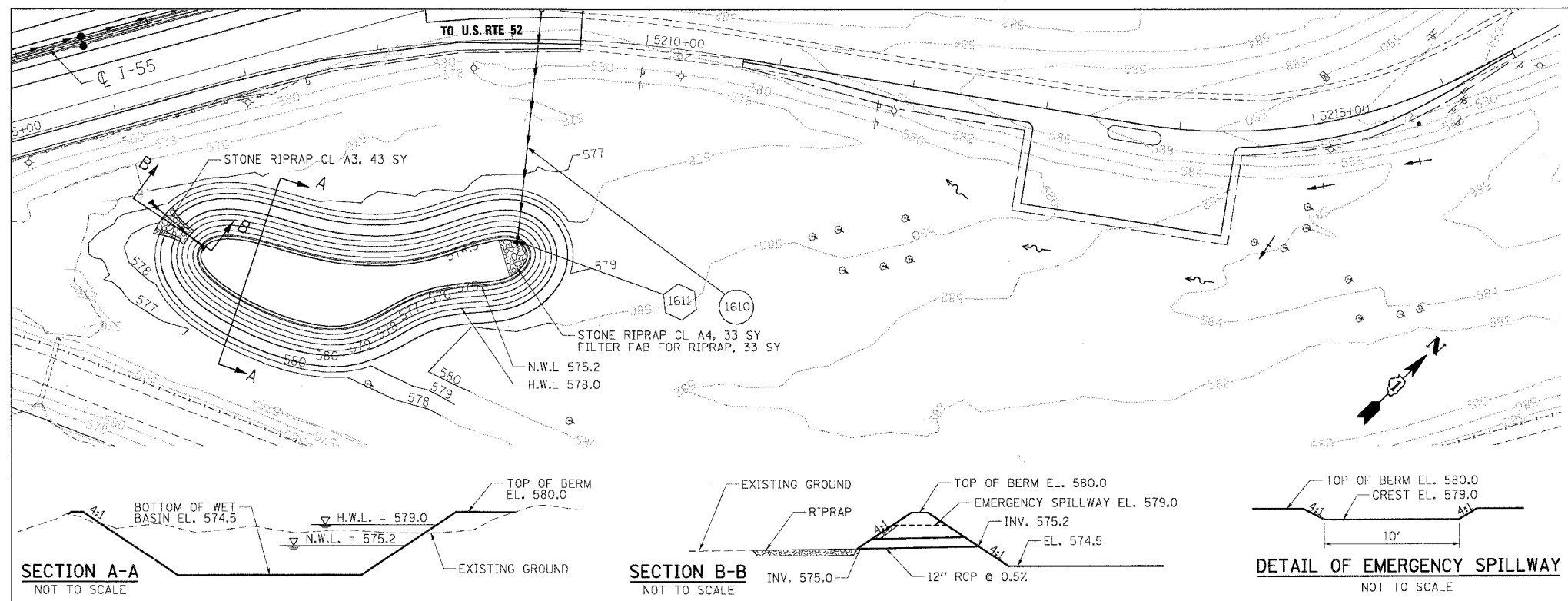
STATION	CONTOUR OFFSET AT:			
	580.0	574.5	574.5	580.0
332+71	178.0	--	--	178.0
332+80	156.7	--	--	205.6
332+90	149.3	--	--	218.8
333+00	146.1	168.9	195.8	229.6
333+10	145.5	167.6	208.3	238.8
333+20	147.0	170.0	218.3	247.0
333+30	150.2	174.3	226.9	254.3
333+40	154.7	179.8	234.5	260.7
333+50	160.1	185.7	241.0	266.1
333+60	166.0	191.6	246.5	270.4
333+70	172.0	197.4	250.7	273.5
333+80	177.8	202.6	253.3	275.4
333+90	183.0	207.2	253.9	275.9
334+00	187.6	211.2	253.0	275.2
334+10	191.5	214.7	251.0	273.5
334+20	194.7	217.5	248.5	271.2
334+30	197.3	219.8	246.2	268.7
334+40	199.2	221.5	244.7	266.8
334+50	200.4	222.5	244.2	266.2
334+60	200.9	222.9	244.6	266.7
334+70	200.7	222.7	245.8	268.0
334+80	200.1	222.2	247.5	269.8
334+90	199.6	221.6	249.4	271.7
335+00	199.5	221.4	250.8	272.9
335+10	200.0	222.1	250.5	272.7
335+20	201.6	225.0	246.6	270.7
335+30	205.4	--	--	266.3
335+40	212.8	--	--	258.6
335+50	236.2	--	--	236.2

**DETENTION BASIN 1 INFORMATION**

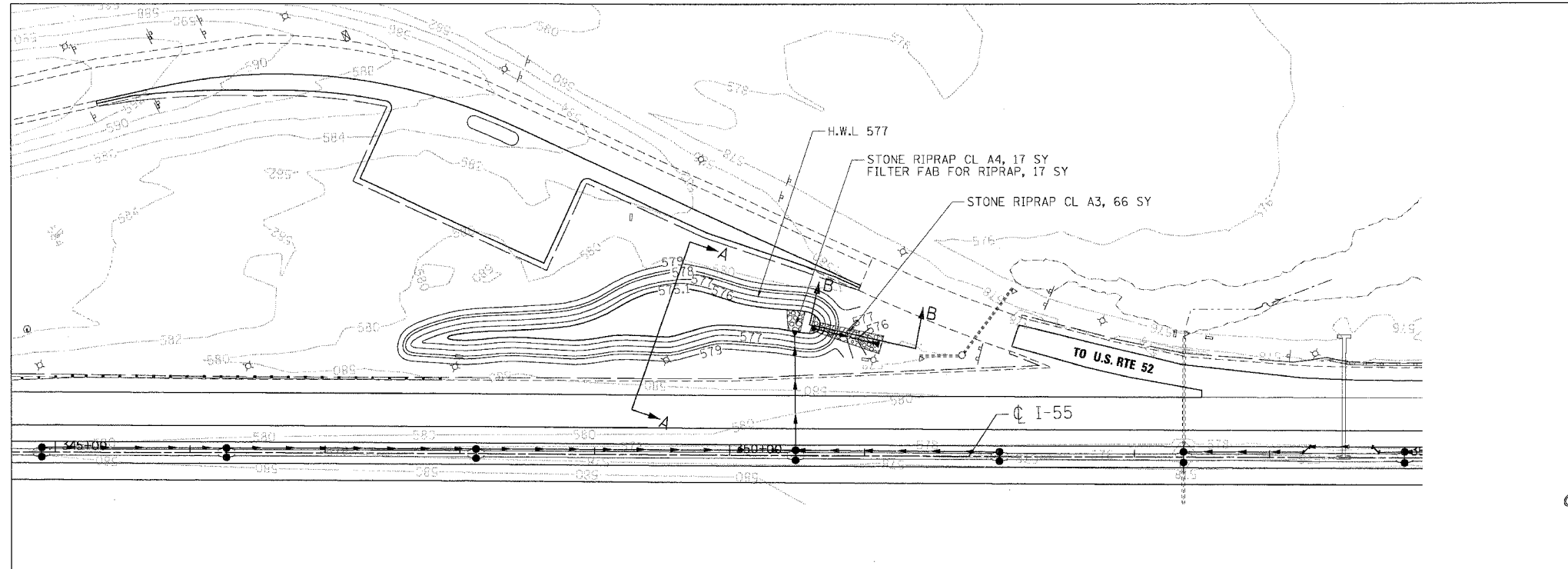
100-YEAR  
 DETENTION VOLUME REQUIRED = 1.082 AC-FT  
 DETENTION VOLUME PROVIDED = 1.265 AC-FT  
 HIGH WATER ELEVATION = 579.0  
 MAX. RELEASE RATE = 14.8 CFS  
 WATER QUALITY VOLUME REQUIRED = 0.160 AC-FT  
 WATER QUALITY VOLUME PROVIDED = 0.160 AC-FT

**NOTES**

1. IF THE BEDROCK GRADE WILL NOT ALLOW THE CONSTRUCTION OF THE PROPOSED WET BOTTOM DETENTION BASIN, THE BASIN WILL BE CONSTRUCTED AS A DRY BOTTOM DETENTION BASIN. THE BOTTOM ELEVATION MAY BE RAISED TO 575.2 AND STILL PROVIDE THE REQUIRED 100-YR DETENTION VOLUME.  
 2. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.



**DETENTION BASIN 1**



**DETENTION BASIN 2**

STATION	CONTOUR OFFSET AT:			
	578.0	575.1	575.1	578.0
347+58	-79.7	--	--	-79.7
347+60	-84.3	--	--	-75.2
347+70	-90.8	--	--	-73.3
347+80	-94.5	--	--	-72.8
347+90	-97.4	-85.9	-84.2	-72.0
348+00	-99.4	-88.0	-83.0	-71.6
348+10	-100.7	-89.0	-88.5	-71.9
348+20	-101.5	-89.8	-84.7	-73.0
348+30	-101.9	-90.3	-85.6	-74.0
348+40	-102.3	-90.7	-85.7	-74.1
348+50	-102.9	-91.2	-85.3	-73.7
348+60	-103.5	-91.9	-84.7	-73.1
348+70	-101.4	-92.7	-84.2	-72.6
348+80	-103.0	-94.2	-84.0	-72.2
348+90	-108.7	-96.6	-84.0	-72.4
349+00	-112.8	-100.1	-85.5	-73.8
349+10	-118.6	-105.1	-87.1	-75.4
349+20	-124.3	-111.0	-87.8	-76.4
349+30	-128.5	-116.1	-87.6	-76.0
349+40	-131.4	-119.5	-87.1	-75.5
349+50	-133.4	-121.7	-86.8	-75.1

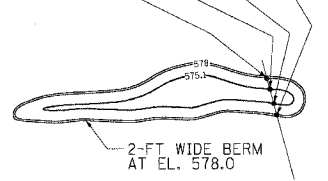
STATION	CONTOUR OFFSET AT:			
	578.0	575.1	575.1	578.0
349+60	-133.8	-122.1	-88.8	-76.8
349+70	-133.2	-121.4	-92.3	-80.0
349+80	-130.9	-118.8	-95.4	-83.4
349+90	-127.7	-115.5	-96.9	-85.3
350+00	-124.5	-112.4	-96.7	-85.1
350+10	-121.7	-109.8	-95.8	-84.1
350+20	-119.8	-108.1	-95.0	-83.4
350+30	-118.7	-107.0	-94.6	-83.0
350+40	-118.1	-106.4	-93.4	-81.6
350+50	-117.5	-105.8	-91.4	-79.6
350+60	-115.9	-103.5	-90.7	-79.1
350+70	-110.5	--	--	-82.7
350+76	-95.6	--	--	-95.6

**DETENTION BASIN 2 INFORMATION**

100-YEAR  
 DETENTION VOLUME REQUIRED = 0.271 AC-FT  
 DETENTION VOLUME PROVIDED = 0.297 AC-FT  
 HIGH WATER ELEVATION = 577.0  
 RELEASE RATE = 1.02 CFS

**NOTES**

1. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

**DRAINAGE DETAILS - I  
 DETENTION BASINS**

SCALE: DRAWN BY SN  
 DATE 02/23/07 CHECKED BY TKL

**TENG**  
 TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 02/23/07  
 FILE NAME = #FILES#  
 PLOT SCALE = #SCALE#  
 USER NAME = #USER#

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL.	334	186
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

**DETENTION BASIN AT IL-59**

**DETENTION BASIN INFORMATION**  
 100-YEAR  
 DETENTION VOLUME REQUIRED = 0.275 AC-FT  
 DETENTION VOLUME PROVIDED = 0.505 AC-FT  
 HIGH WATER ELEVATION = 584.0

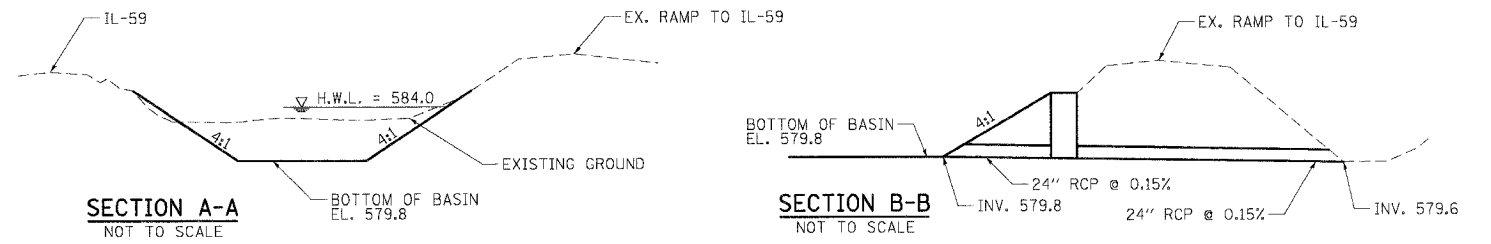
**LEGEND**  
 ✕ TREE REMOVAL

CUT = 2,200 CU YD  
 TOPSOIL REMOVAL = 328 CU YD

N=1764556.99  
 E=1021306.69

N=1764408.31  
 E=1021314.04

N=1764402.03  
 E=1021358.11



PLOT DATE = 02/23/07  
 FILE NAME = 021861.DWG  
 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = MURPHY

**NOTES:**  
 1. FOR STRUCTURE AND PIPE INFORMATION, SEE DRAINAGE SCHEDULES, SHEETS 169, 170, 171, 172, 173, 174 AND 175.

REVISIONS	
NAME	DATE

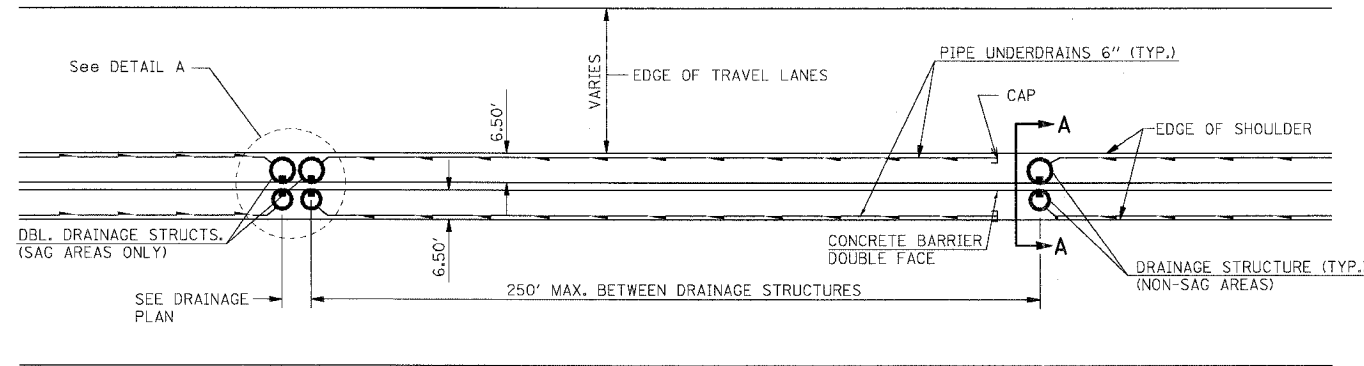
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 I-55 TO BLACK ROAD  
 WIDENING AND RESURFACING

**DRAINAGE DETAILS - II**  
**DETENTION BASIN AT IL-59**

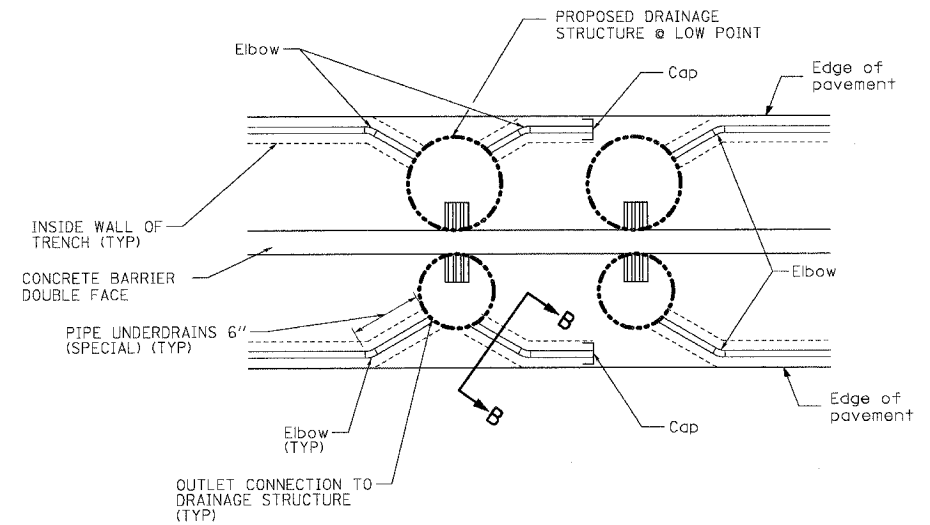
SCALE: DRAWN BY SN  
 DATE 02/23/07 CHECKED BY TKL

**TENG** TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

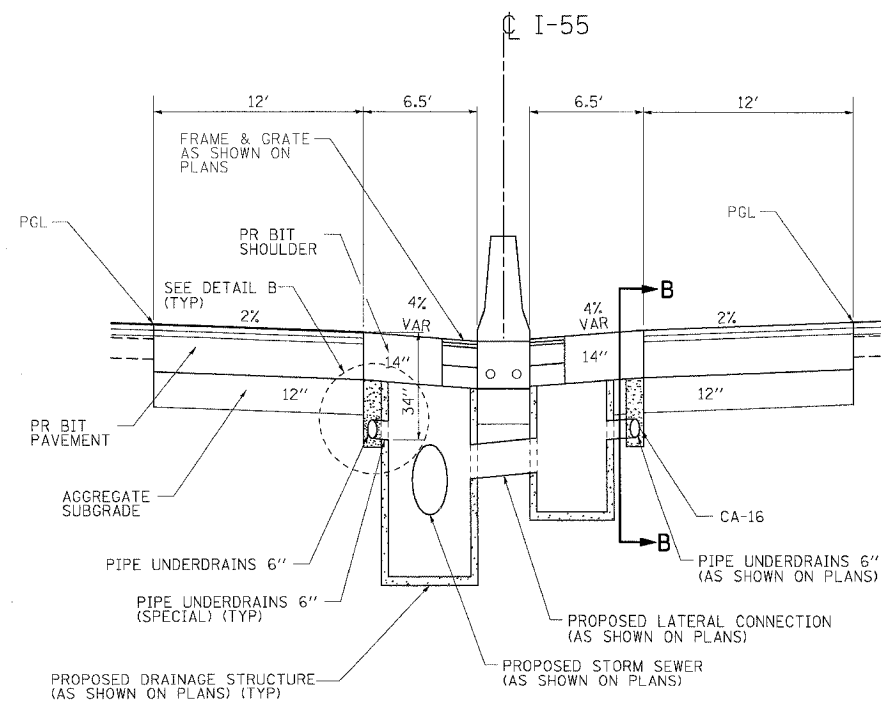
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	187
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



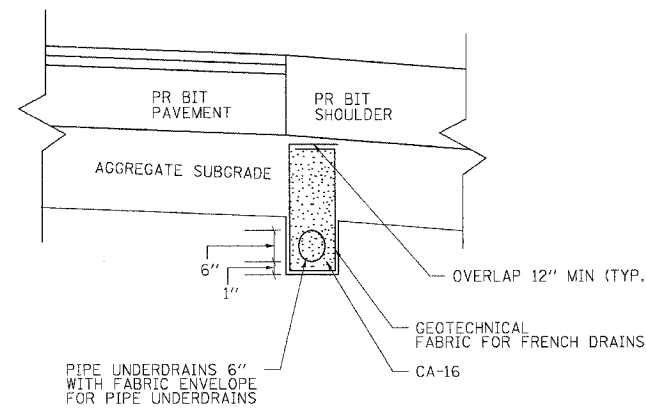
PROPOSED PLAN



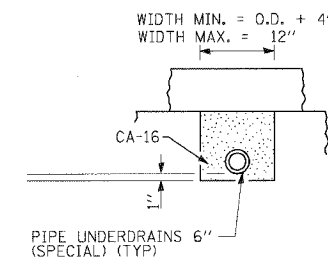
DETAIL A



SECTION A-A



DETAIL B



SECTION B-B

GENERAL NOTES

THE 24" RADIUS ON THE DRAINAGE FITTING IS ONLY A MINIMUM. LARGER RADII MEETING THE APPROVAL OF THE ENGINEER MAYBE SUBSTITUED.

NOTES

- SEE STANDARD 601001 FOR DETAILS NOT SHOWN.
- THE COST OF GEOTECHNICAL FABRIC FOR FRENCH DRAINS, FABRIC ENVELOPE FOR PIPE UNDERDRAINS, CA-16 AGGREGATE FOR BEDDING AND TRENCH BACKFILL AND OUTLET CONNECTION TO DRAINAGE STRUCTURES WILL NOT BE PAID SEPERATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PIPE UNDERDRAINS 6".

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FAI ROUTE 55 (I-80 TO BLACK ROAD)  
 WIDENING AND RESURFACING

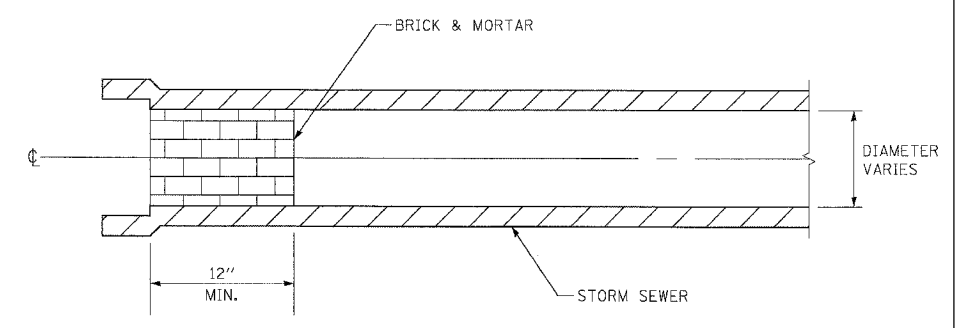
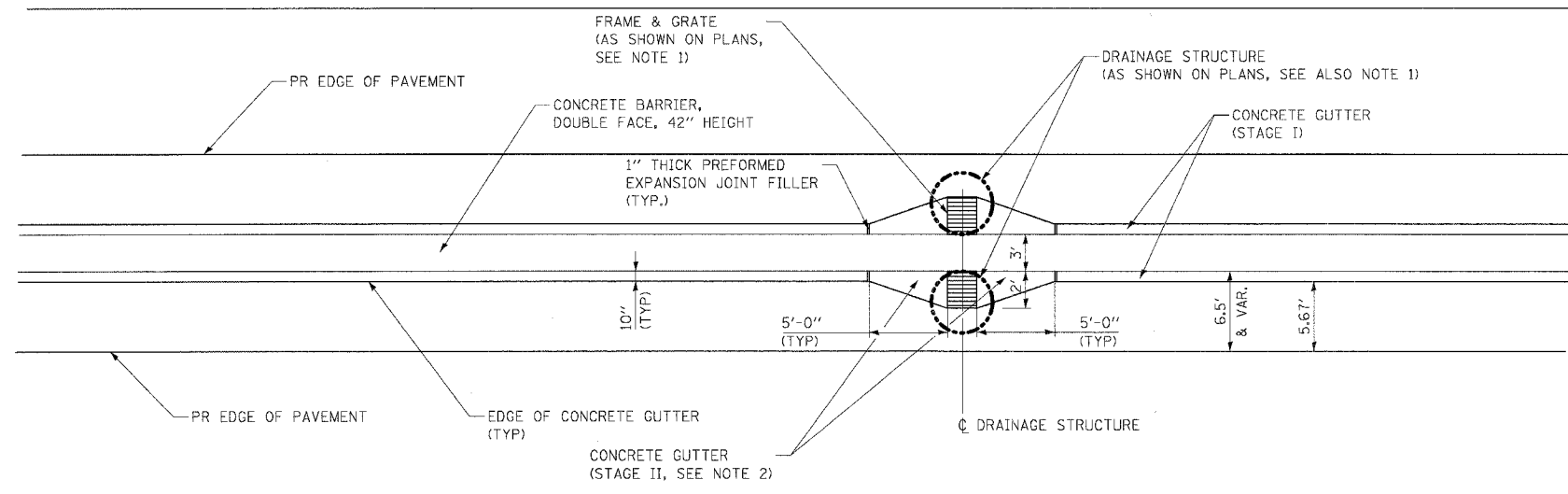
**DRAINAGE DETAILS - III**  
**PIPE UNDERDRAINS**

SCALE: N.T.S. DRAWN BY MRK  
 DATE 02/23/07 CHECKED BY DDH

**TENG** TENG & ASSOCIATES, INC.  
 ENGINEERS/ARCHITECTS/PLANNERS  
 CHICAGO, ILLINOIS

PLOT DATE = 02/23/07  
 FILE NAME = 022307.DWG  
 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = ddh

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	188
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

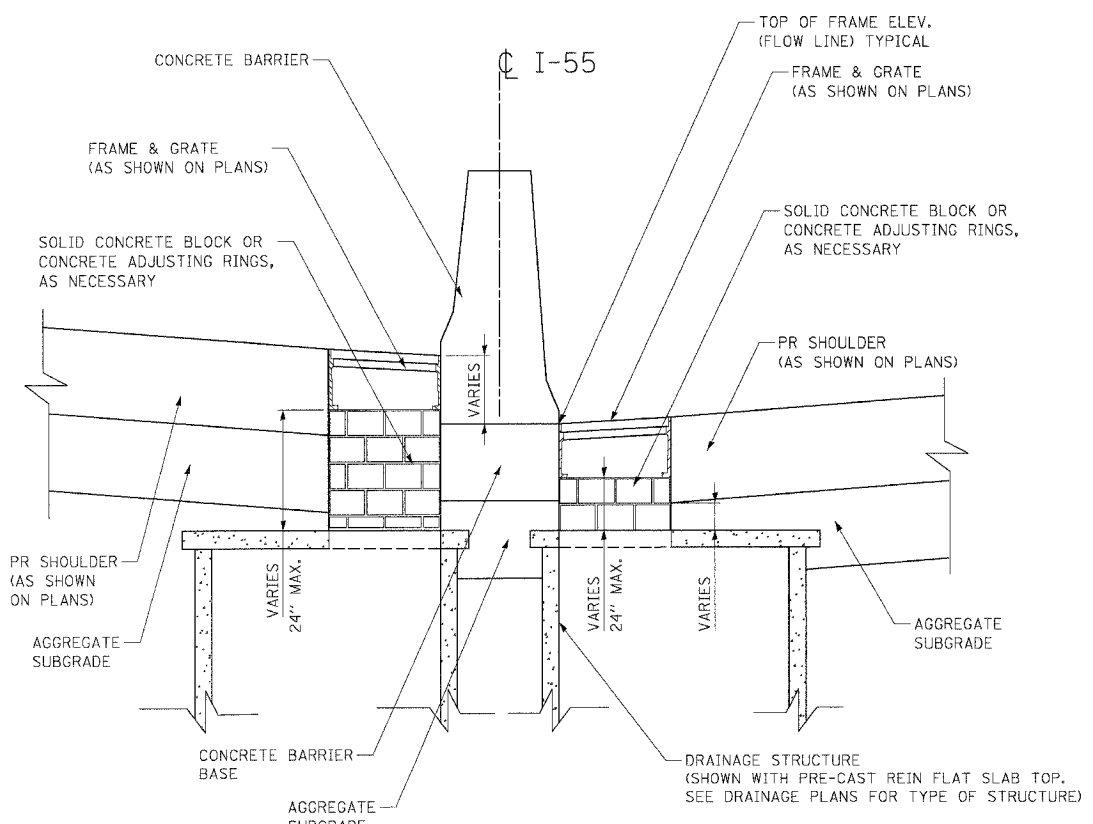


**CULVERT AND STORM SEWER PLUGS**

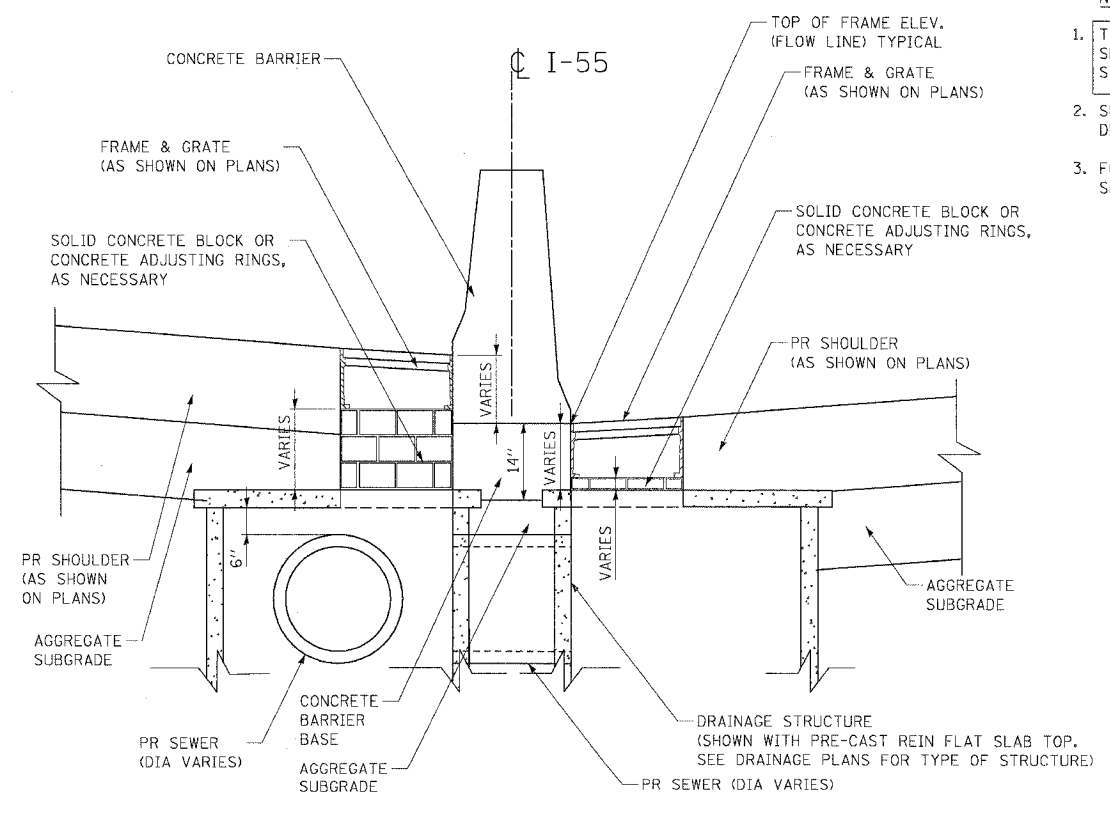
**NOTES**

1. SEE DRAINAGE GENERAL NOTES REGARDING ADJUSTMENT OF FRAMES AND GRATES.
2. SEE ROADWAY GENERAL NOTES FOR DETAILS REGARDING THE STAGING AND THE PAYMENT OF THE VARIABLE WIDTH CONCRETE GUTTER ADJACENT TO THE DRAINAGE STRUCTURES.

**CONCRETE GUTTER DETAIL AT DRAINAGE STRUCTURE**



**SIDE ELEVATION**  
**TOP OF STRUCTURE BENEATH OR AT BOTTOM OF BARRIER BASE AND SHOULDER**



**SIDE ELEVATION**  
**TOP OF STRUCTURE EXTENDS INTO BARRIER BASE AND SHOULDER**

**CONCRETE ADJUSTING RINGS OR CONCRETE BLOCKS FOR TYPE 20 AND TYPE 21 F & G**

**NOTES**

1. THE COST OF SOLID CONCRETE BLOCK OR CONCRETE ADJUSTING RINGS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF DRAINAGE STRUCTURES OF TYPE OR SIZE SPECIFIED.
2. SEE STATE AND DISTRICT 1 STANDARDS FOR DRAINAGE STRUCTURE DETAILS.
3. FOR STRUCTURE AND PIPE INFORMATION SEE DRAINAGE SCHEDULES, SHEETS 169 TO 175.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**DRAINAGE DETAILS - IV**  
**CONC GUTTER @ DRAINAGE STRUCTURE**  
**AND STRUCTURE ADJUSTMENT**

SCALE: NTS  
DATE: 03/15/07

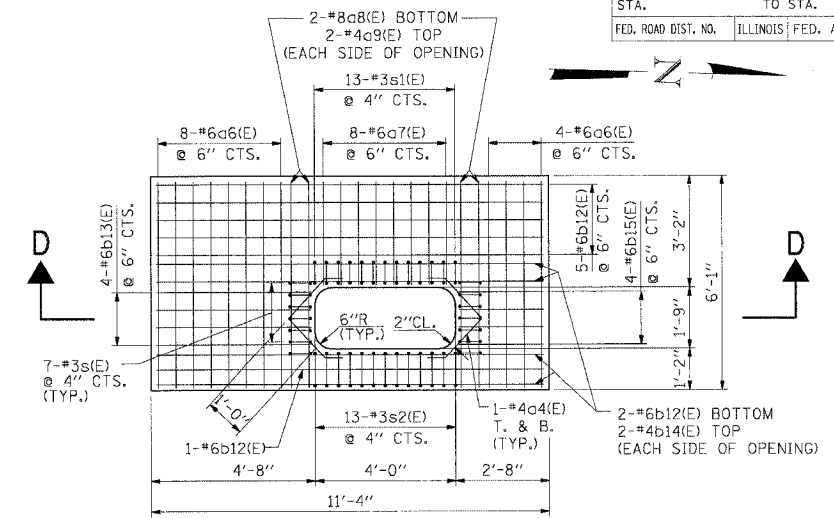
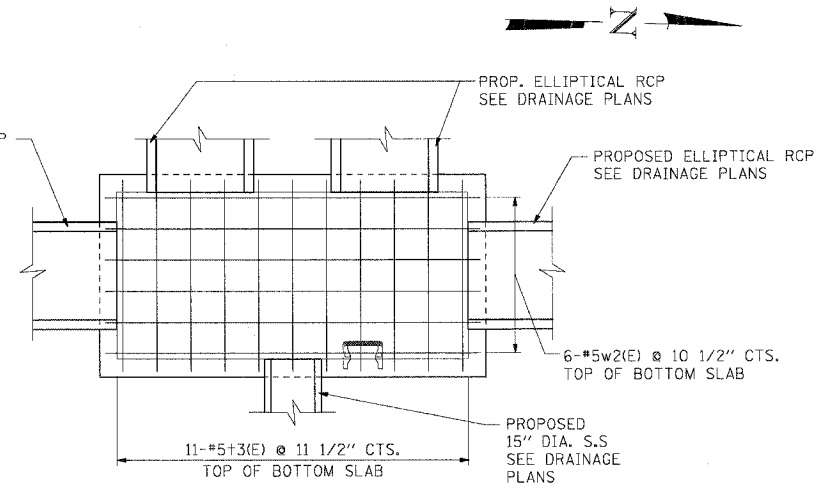
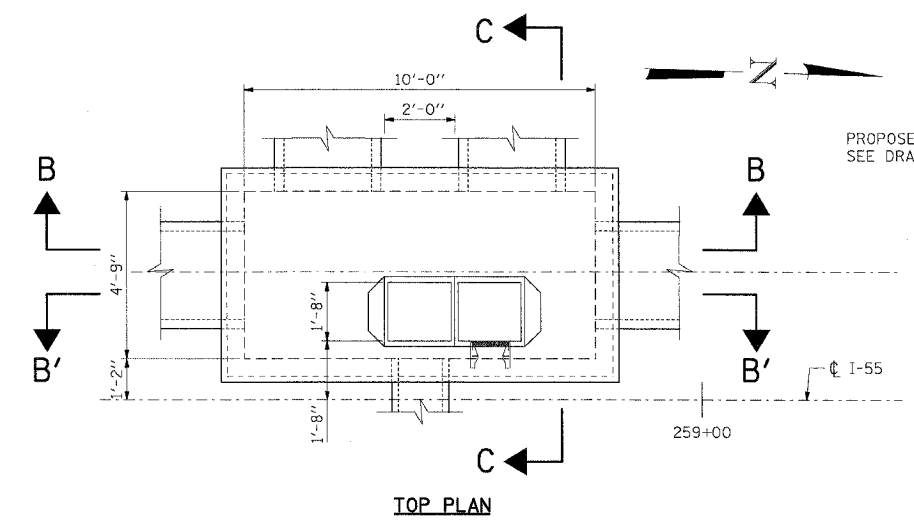
DRAWN BY: MRK  
CHECKED BY: TL

**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

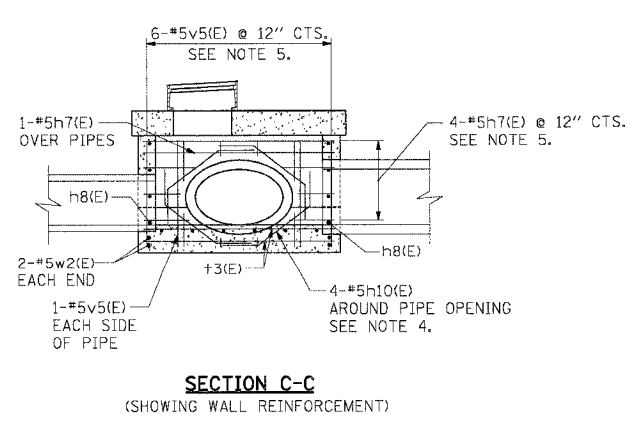
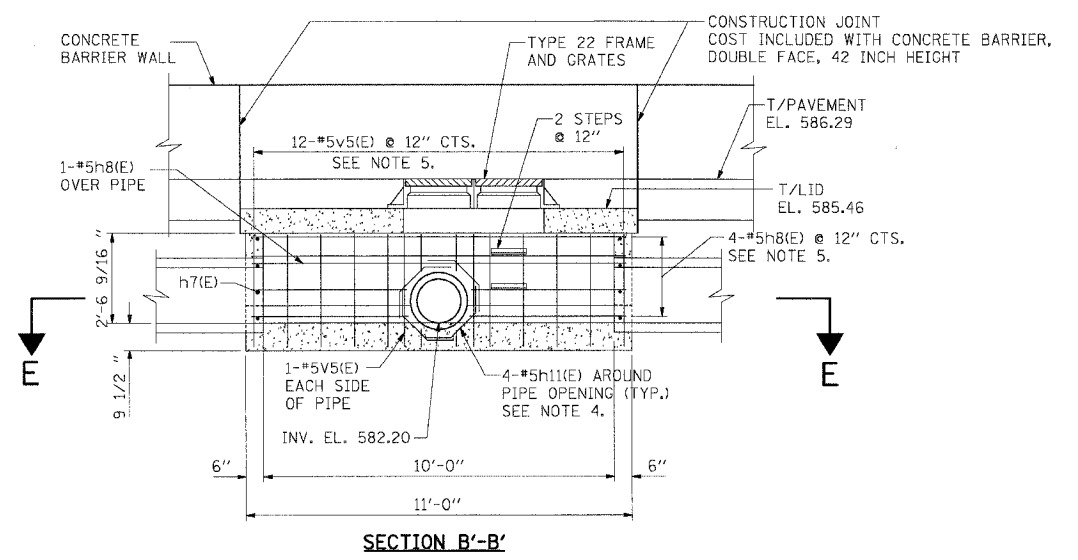
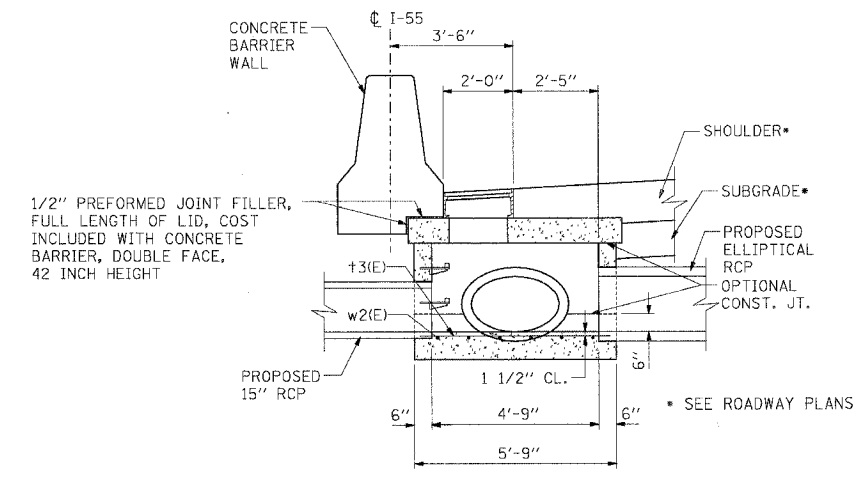
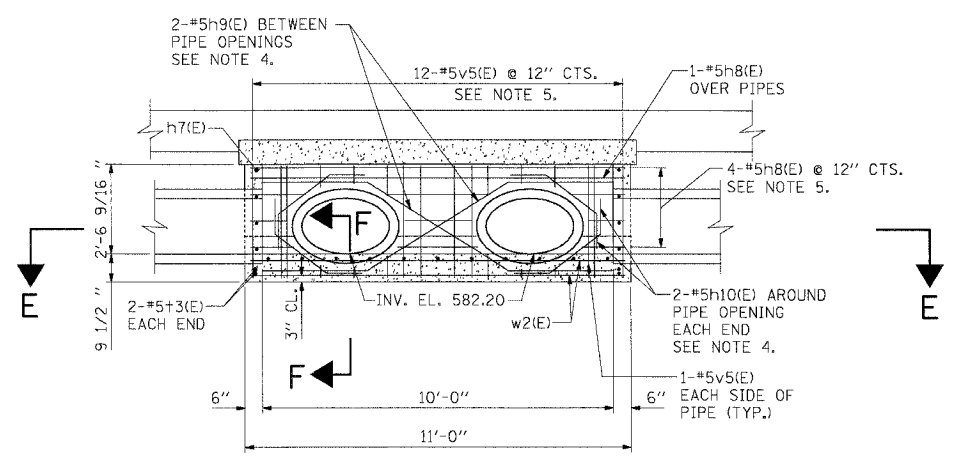
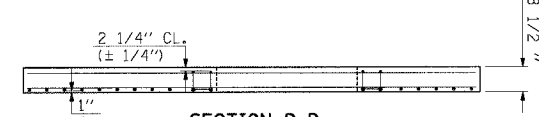
PLOT DATE = 03/15/07  
 FILE NAME = 031507.DWG  
 USER NAME = TENG  
 USER WORK = 031507



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&Z) WRS	WILL	334	189
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



ALL BARS IN LID SHALL BE EPOXY COATED.  
ALL BARS SHALL BE PLACED AT BOTTOM OF LID U.N.O.



**NOTES:**

1. WORK THIS SHEET WITH SHT. 193.
2. T. & B. DENOTES TOP AND BOTTOM.
3. ALL BARS DESIGNATED (E) SHALL BE EPOXY COATED.
4. BEND BARS IN FIELD AS NECESSARY TO MAINTAIN CLEAR COVER.
5. CUT BARS IN FIELD AS NECESSARY FOR PIPE OPENINGS.
6. THE LID MAY BE PRECAST, BUT THE REST OF THE STRUCTURE SHALL BE CAST IN PLACE.
7. PROVIDE 2" CLEAR COVER, EXCEPT AS NOTED FOR PRECAST LID AND AT BOTTOM OF BASE SLAB.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

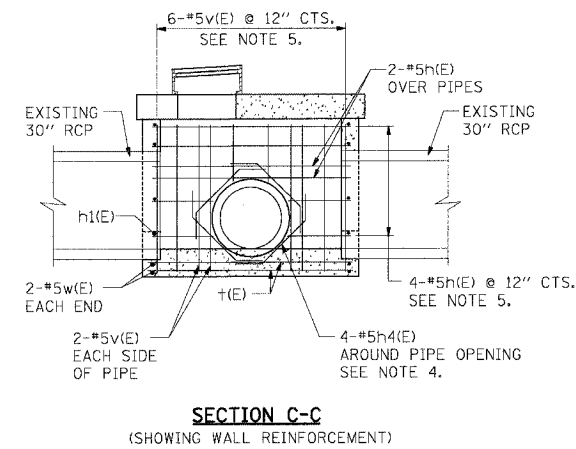
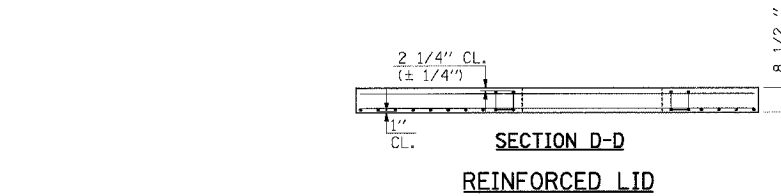
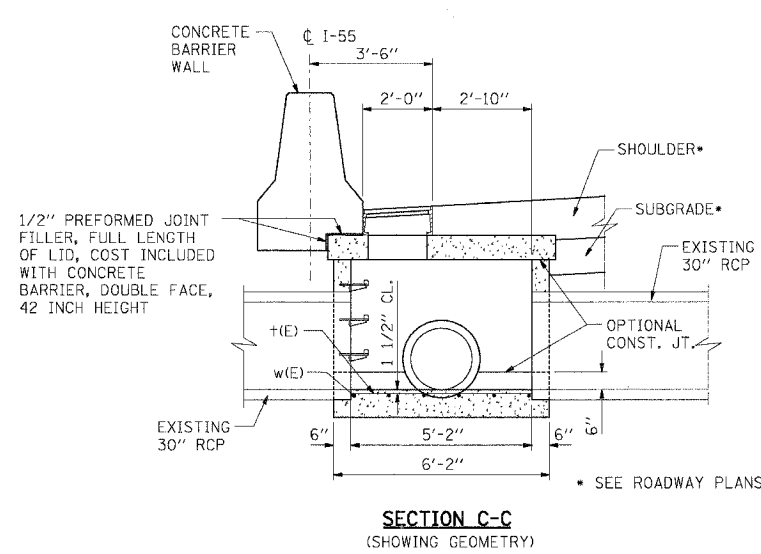
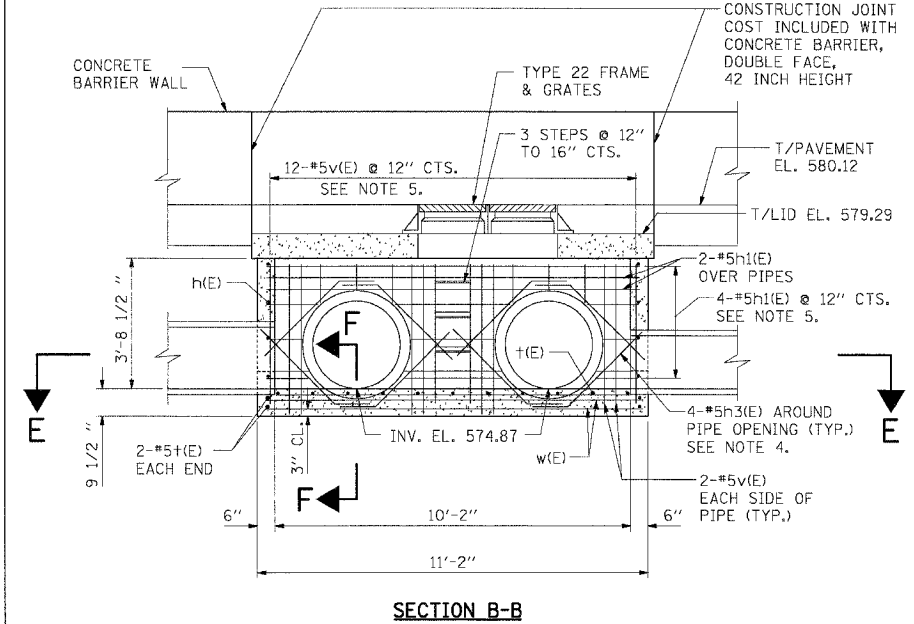
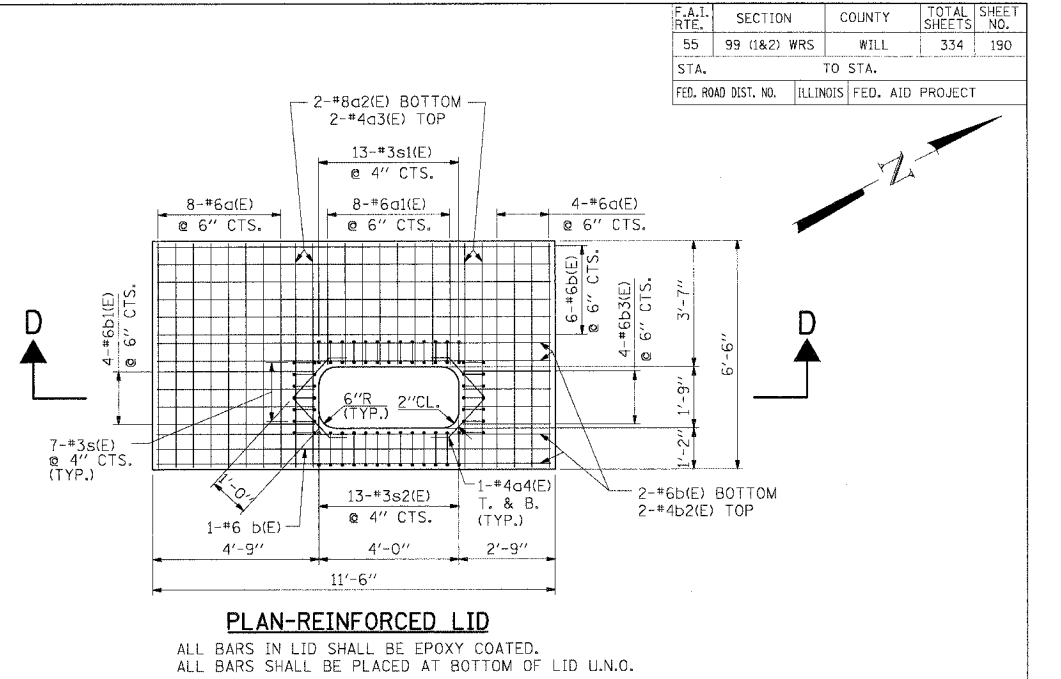
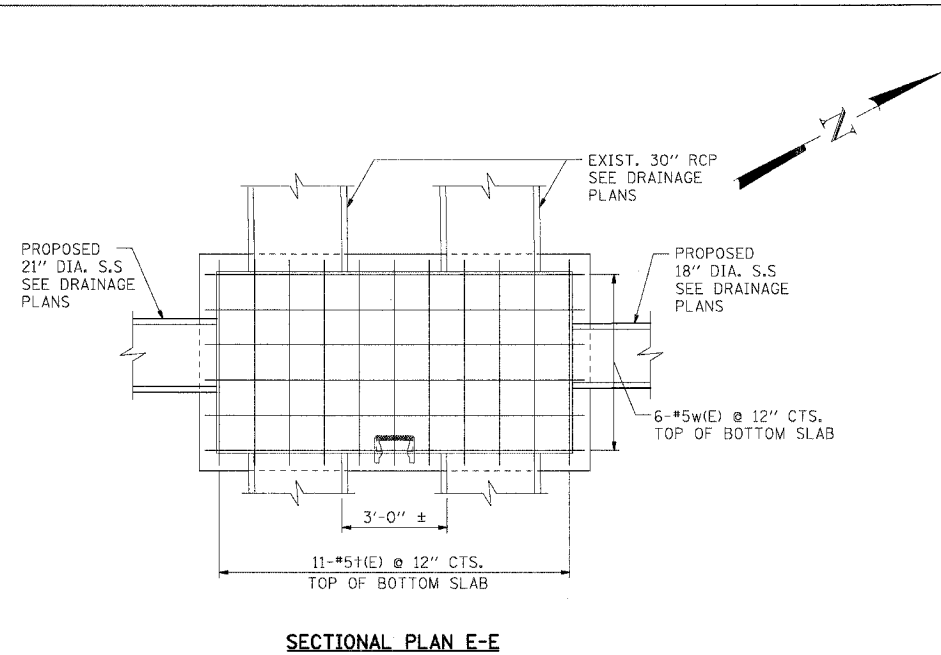
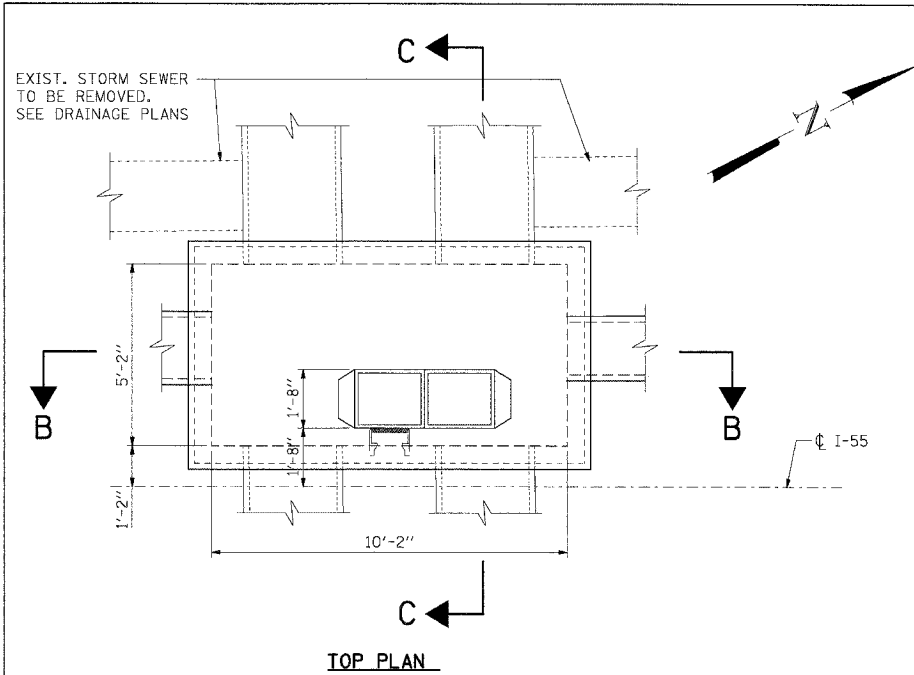
**DRAINAGE DETAILS - V**  
**DRAINAGE STRUCTURE SPECIAL**  
**STA. 258+92**

SCALE: N.T.S. DRAWN BY: MOB  
DATE: 02/23/07 CHECKED BY: MJK

**TENG** TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

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 PLOT SCALE = #SCALE\$  
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL	334	190
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



- NOTES:**
- 1 WORK THIS SHEET WITH SHIT. 193.
  2. T. & B. DENOTES TOP AND BOTTOM.
  3. ALL BARS DESIGNATED (E) SHALL BE EPOXY COATED.
  4. BEND BARS IN FIELD AS NECESSARY TO MAINTAIN CLEAR COVER.
  5. CUT BARS IN FIELD AS NECESSARY FOR PIPE OPENINGS.
  6. EXISTING PIPES SHALL BE CUT AND REMOVED AS SHOWN. COST OF CUTTING, REMOVAL AND DISPOSAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR DRAINAGE STRUCTURE, SPECIAL.
  7. THE LID MAY BE PRECAST, BUT THE REST OF THE STRUCTURE SHALL BE CAST IN PLACE.
  8. PROVIDE 2" CLEAR COVER, EXCEPT AS NOTED FOR PRECAST LID AND AT BOTTOM OF BASE SLAB.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

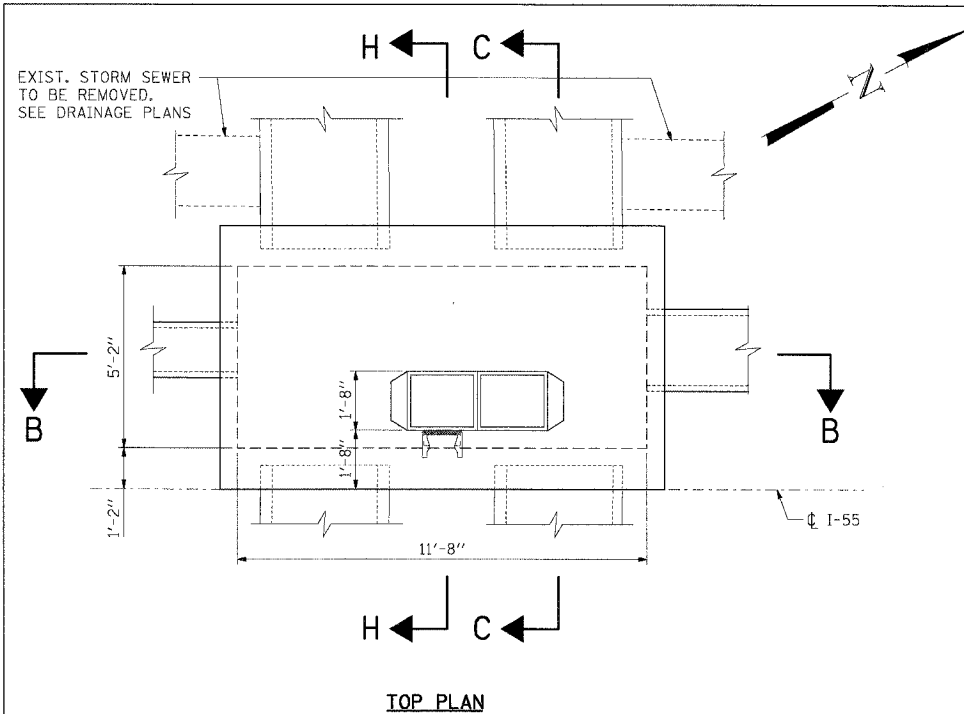
**DRAINAGE DETAILS - VI  
DRAINAGE STRUCTURE SPECIAL  
STA. 363+33**

SCALE: N.T.S. DRAWN BY: MDB  
DATE: 02/23/07 CHECKED BY: MJK

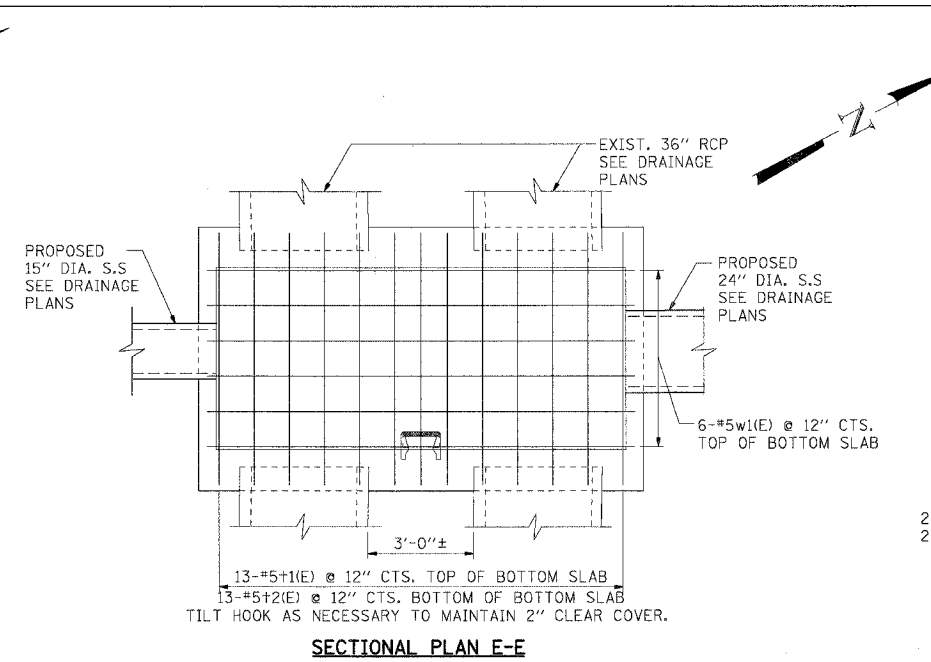
**TENG**  
TENGG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE = 02/23/07  
FILE NAME = 022307.DWG  
PLOT SCALE = 1/8"=1'-0"  
USER NAME = MJK

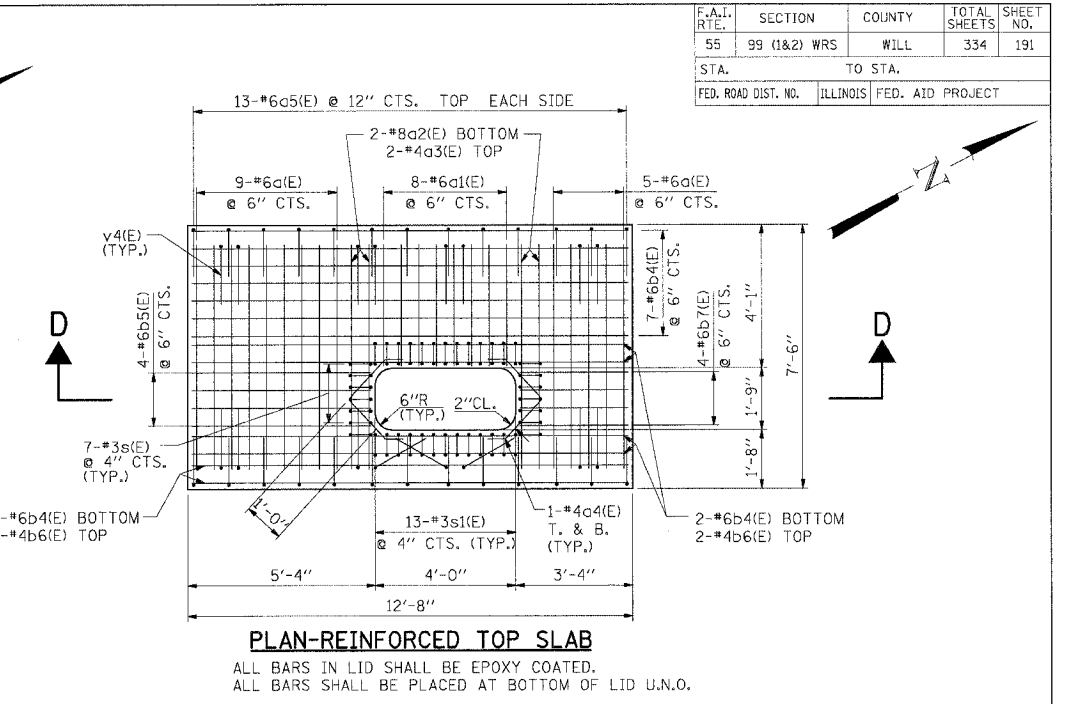
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55 99 (I&2) WRS WILL 334 191				
STA. TO STA.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



**TOP PLAN**

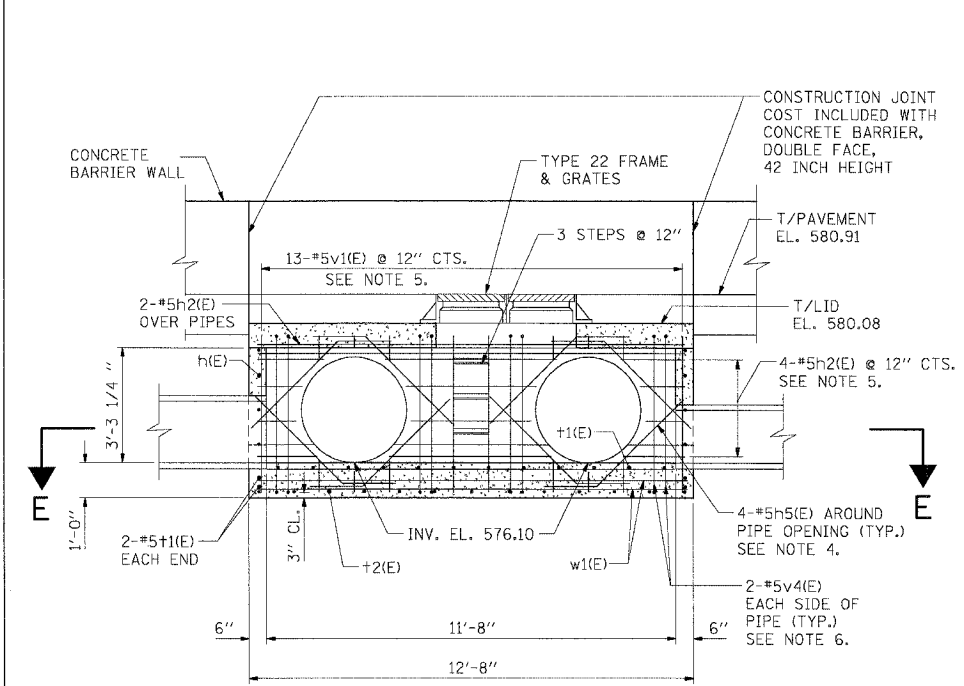


**SECTIONAL PLAN E-E**

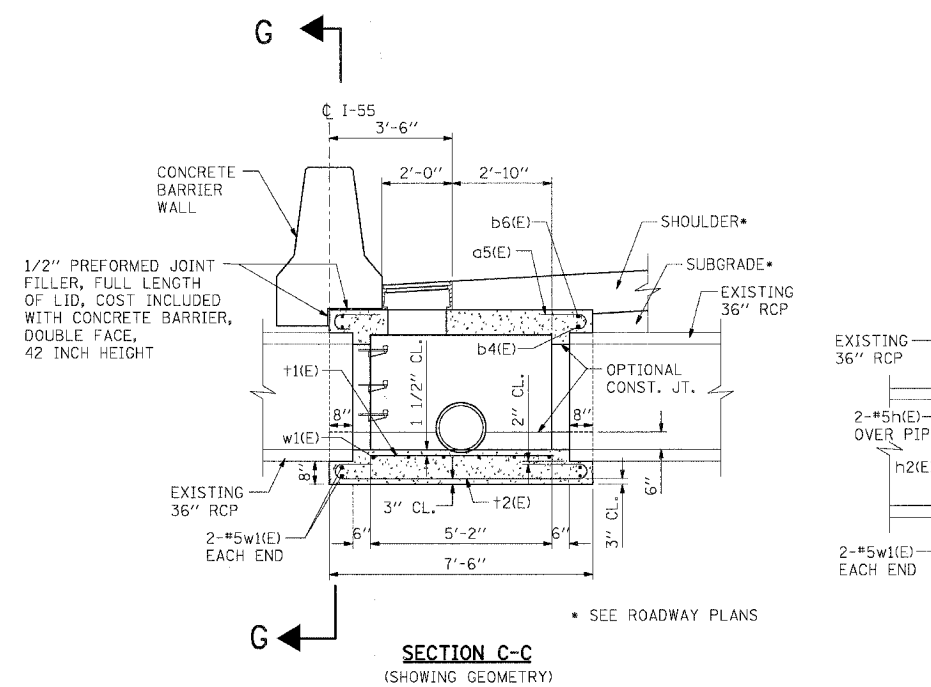


**PLAN-REINFORCED TOP SLAB**

ALL BARS IN LID SHALL BE EPOXY COATED.  
ALL BARS SHALL BE PLACED AT BOTTOM OF LID U.N.O.

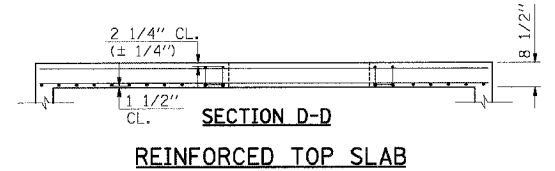


**SECTION B-B**

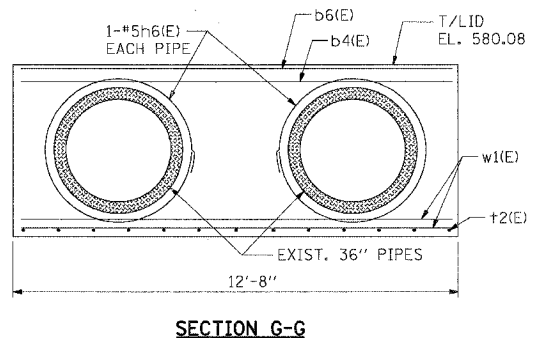


**SECTION C-C (SHOWING GEOMETRY)**

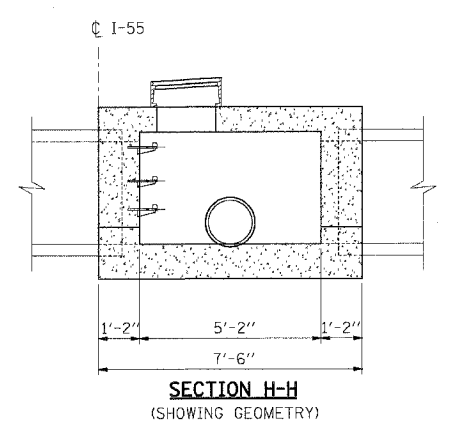
**SECTION C-C (SHOWING REINFORCEMENT)**



**SECTION D-D REINFORCED TOP SLAB**



**SECTION G-G**



**SECTION H-H (SHOWING GEOMETRY)**

**NOTES:**

- 1. WORK THIS SHEET WITH SHIT. 193.
- 2. T. & B. DENOTES TOP AND BOTTOM.
- 3. ALL BARS DESIGNATED (E) SHALL BE EPOXY COATED.
- 4. BEND BARS IN FIELD AS NECESSARY TO MAINTAIN CLEAR COVER.
- 5. CUT BARS IN FIELD AS NECESSARY FOR PIPE OPENINGS.
- 6. ROTATE BARS AS NECESSARY TO CLEAR OPENING IN LID.
- 7. EXISTING PIPES SHALL BE CUT AND REMOVED AS SHOWN. COST OF CUTTING, REMOVAL AND DISPOSAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR DRAINAGE STRUCTURE, SPECIAL.
- 8. THE ENTIRE STRUCTURE SHALL BE CAST IN PLACE.
- 9. PROVIDE 2" CLEAR COVER, EXCEPT AS NOTED FOR TOP SLAB AND AT BOTTOM OF BASE SLAB.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

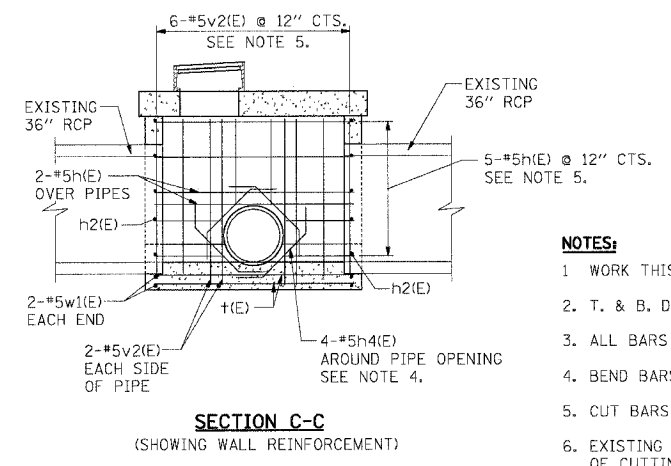
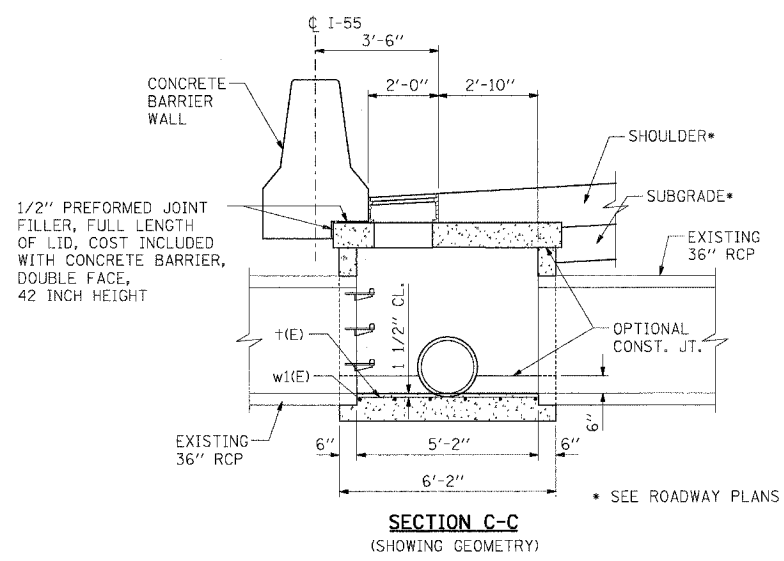
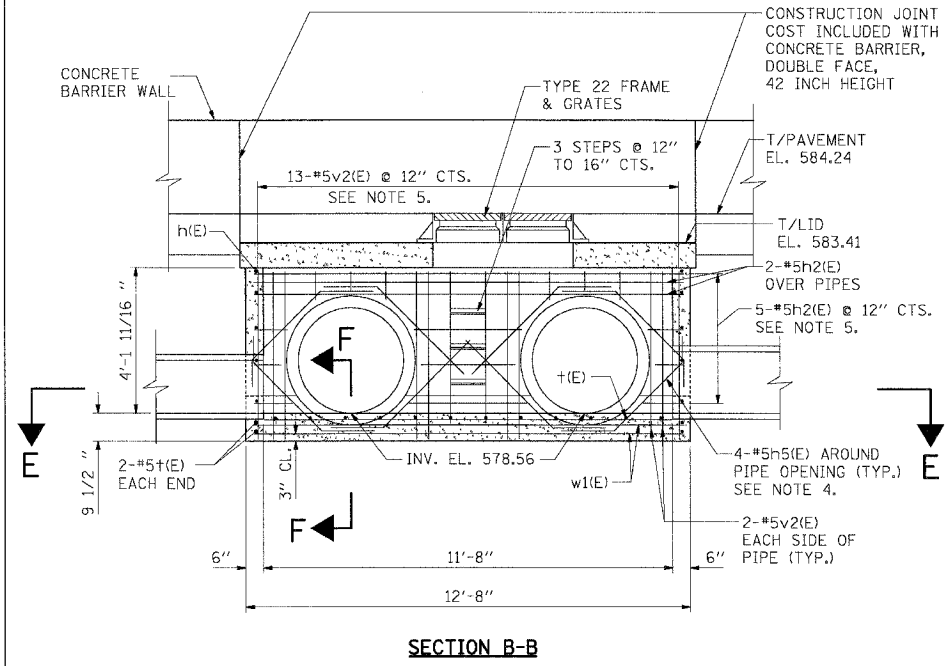
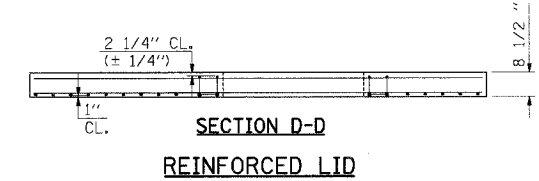
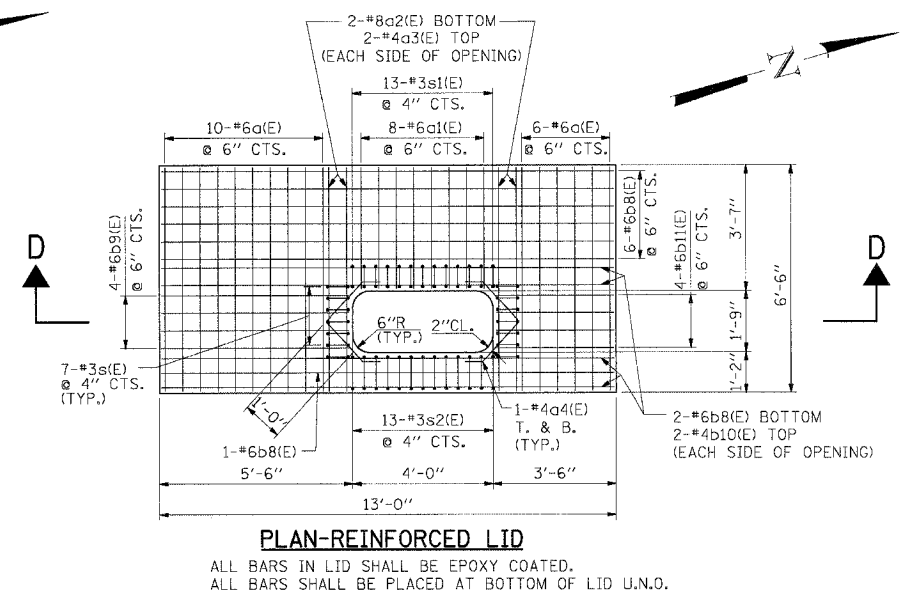
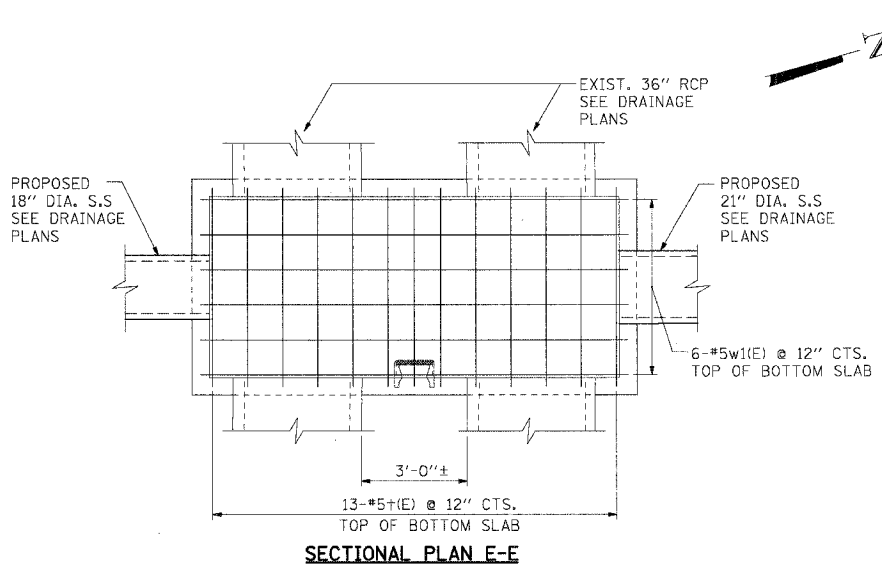
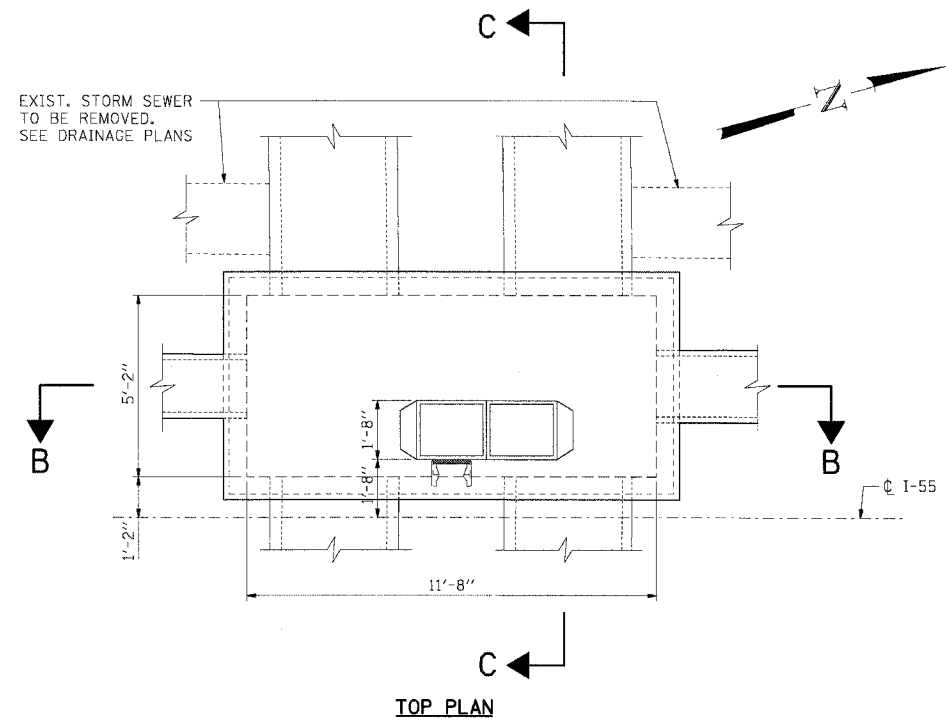
**DRAINAGE DETAILS - VII  
DRAINAGE STRUCTURE SPECIAL  
STA. 372+85**

SCALE: N.T.S. DATE: 02/23/07 DRAWN BY: MOB CHECKED BY: MJK

**TENG**  
TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

NOTES: 1. NOTES 2. FILE NAME 3. SCALE 4. USER NAME 5. DATE 6. 7. 8. 9. 10. 11. 12. 13. 14. 15. 16. 17. 18. 19. 20. 21. 22. 23. 24. 25. 26. 27. 28. 29. 30. 31. 32. 33. 34. 35. 36. 37. 38. 39. 40. 41. 42. 43. 44. 45. 46. 47. 48. 49. 50. 51. 52. 53. 54. 55. 56. 57. 58. 59. 60. 61. 62. 63. 64. 65. 66. 67. 68. 69. 70. 71. 72. 73. 74. 75. 76. 77. 78. 79. 80. 81. 82. 83. 84. 85. 86. 87. 88. 89. 90. 91. 92. 93. 94. 95. 96. 97. 98. 99. 100. 101. 102. 103. 104. 105. 106. 107. 108. 109. 110. 111. 112. 113. 114. 115. 116. 117. 118. 119. 120. 121. 122. 123. 124. 125. 126. 127. 128. 129. 130. 131. 132. 133. 134. 135. 136. 137. 138. 139. 140. 141. 142. 143. 144. 145. 146. 147. 148. 149. 150. 151. 152. 153. 154. 155. 156. 157. 158. 159. 160. 161. 162. 163. 164. 165. 166. 167. 168. 169. 170. 171. 172. 173. 174. 175. 176. 177. 178. 179. 180. 181. 182. 183. 184. 185. 186. 187. 188. 189. 190. 191. 192. 193. 194. 195. 196. 197. 198. 199. 200. 201. 202. 203. 204. 205. 206. 207. 208. 209. 210. 211. 212. 213. 214. 215. 216. 217. 218. 219. 220. 221. 222. 223. 224. 225. 226. 227. 228. 229. 230. 231. 232. 233. 234. 235. 236. 237. 238. 239. 240. 241. 242. 243. 244. 245. 246. 247. 248. 249. 250. 251. 252. 253. 254. 255. 256. 257. 258. 259. 260. 261. 262. 263. 264. 265. 266. 267. 268. 269. 270. 271. 272. 273. 274. 275. 276. 277. 278. 279. 280. 281. 282. 283. 284. 285. 286. 287. 288. 289. 290. 291. 292. 293. 294. 295. 296. 297. 298. 299. 300. 301. 302. 303. 304. 305. 306. 307. 308. 309. 310. 311. 312. 313. 314. 315. 316. 317. 318. 319. 320. 321. 322. 323. 324. 325. 326. 327. 328. 329. 330. 331. 332. 333. 334. 335. 336. 337. 338. 339. 340. 341. 342. 343. 344. 345. 346. 347. 348. 349. 350. 351. 352. 353. 354. 355. 356. 357. 358. 359. 360. 361. 362. 363. 364. 365. 366. 367. 368. 369. 370. 371. 372. 373. 374. 375. 376. 377. 378. 379. 380. 381. 382. 383. 384. 385. 386. 387. 388. 389. 390. 391. 392. 393. 394. 395. 396. 397. 398. 399. 400. 401. 402. 403. 404. 405. 406. 407. 408. 409. 410. 411. 412. 413. 414. 415. 416. 417. 418. 419. 420. 421. 422. 423. 424. 425. 426. 427. 428. 429. 430. 431. 432. 433. 434. 435. 436. 437. 438. 439. 440. 441. 442. 443. 444. 445. 446. 447. 448. 449. 450. 451. 452. 453. 454. 455. 456. 457. 458. 459. 460. 461. 462. 463. 464. 465. 466. 467. 468. 469. 470. 471. 472. 473. 474. 475. 476. 477. 478. 479. 480. 481. 482. 483. 484. 485. 486. 487. 488. 489. 490. 491. 492. 493. 494. 495. 496. 497. 498. 499. 500. 501. 502. 503. 504. 505. 506. 507. 508. 509. 510. 511. 512. 513. 514. 515. 516. 517. 518. 519. 520. 521. 522. 523. 524. 525. 526. 527. 528. 529. 530. 531. 532. 533. 534. 535. 536. 537. 538. 539. 540. 541. 542. 543. 544. 545. 546. 547. 548. 549. 550. 551. 552. 553. 554. 555. 556. 557. 558. 559. 560. 561. 562. 563. 564. 565. 566. 567. 568. 569. 570. 571. 572. 573. 574. 575. 576. 577. 578. 579. 580. 581. 582. 583. 584. 585. 586. 587. 588. 589. 590. 591. 592. 593. 594. 595. 596. 597. 598. 599. 600. 601. 602. 603. 604. 605. 606. 607. 608. 609. 610. 611. 612. 613. 614. 615. 616. 617. 618. 619. 620. 621. 622. 623. 624. 625. 626. 627. 628. 629. 630. 631. 632. 633. 634. 635. 636. 637. 638. 639. 640. 641. 642. 643. 644. 645. 646. 647. 648. 649. 650. 651. 652. 653. 654. 655. 656. 657. 658. 659. 660. 661. 662. 663. 664. 665. 666. 667. 668. 669. 670. 671. 672. 673. 674. 675. 676. 677. 678. 679. 680. 681. 682. 683. 684. 685. 686. 687. 688. 689. 690. 691. 692. 693. 694. 695. 696. 697. 698. 699. 700. 701. 702. 703. 704. 705. 706. 707. 708. 709. 710. 711. 712. 713. 714. 715. 716. 717. 718. 719. 720. 721. 722. 723. 724. 725. 726. 727. 728. 729. 730. 731. 732. 733. 734. 735. 736. 737. 738. 739. 740. 741. 742. 743. 744. 745. 746. 747. 748. 749. 750. 751. 752. 753. 754. 755. 756. 757. 758. 759. 760. 761. 762. 763. 764. 765. 766. 767. 768. 769. 770. 771. 772. 773. 774. 775. 776. 777. 778. 779. 780. 781. 782. 783. 784. 785. 786. 787. 788. 789. 790. 791. 792. 793. 794. 795. 796. 797. 798. 799. 800. 801. 802. 803. 804. 805. 806. 807. 808. 809. 810. 811. 812. 813. 814. 815. 816. 817. 818. 819. 820. 821. 822. 823. 824. 825. 826. 827. 828. 829. 830. 831. 832. 833. 834. 835. 836. 837. 838. 839. 840. 841. 842. 843. 844. 845. 846. 847. 848. 849. 850. 851. 852. 853. 854. 855. 856. 857. 858. 859. 860. 861. 862. 863. 864. 865. 866. 867. 868. 869. 870. 871. 872. 873. 874. 875. 876. 877. 878. 879. 880. 881. 882. 883. 884. 885. 886. 887. 888. 889. 890. 891. 892. 893. 894. 895. 896. 897. 898. 899. 900. 901. 902. 903. 904. 905. 906. 907. 908. 909. 910. 911. 912. 913. 914. 915. 916. 917. 918. 919. 920. 921. 922. 923. 924. 925. 926. 927. 928. 929. 930. 931. 932. 933. 934. 935. 936. 937. 938. 939. 940. 941. 942. 943. 944. 945. 946. 947. 948. 949. 950. 951. 952. 953. 954. 955. 956. 957. 958. 959. 960. 961. 962. 963. 964. 965. 966. 967. 968. 969. 970. 971. 972. 973. 974. 975. 976. 977. 978. 979. 980. 981. 982. 983. 984. 985. 986. 987. 988. 989. 990. 991. 992. 993. 994. 995. 996. 997. 998. 999. 1000.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	192
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



- NOTES:**
1. WORK THIS SHEET WITH SHT. 193.
  2. T. & B. DENOTES TOP AND BOTTOM.
  3. ALL BARS DESIGNATED (E) SHALL BE EPOXY COATED.
  4. BEND BARS IN FIELD AS NECESSARY TO MAINTAIN CLEAR COVER.
  5. CUT BARS IN FIELD AS NECESSARY FOR PIPE OPENINGS.
  6. EXISTING PIPES SHALL BE CUT AND REMOVED AS SHOWN. COST OF CUTTING, REMOVAL AND DISPOSAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR DRAINAGE STRUCTURE, SPECIAL.
  7. THE LID MAY BE PRECAST, BUT THE REST OF THE STRUCTURE SHALL BE CAST IN PLACE.
  8. PROVIDE 2" CLEAR COVER, EXCEPT AS NOTED FOR PRECAST LID AND AT BOTTOM OF BASE SLAB.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

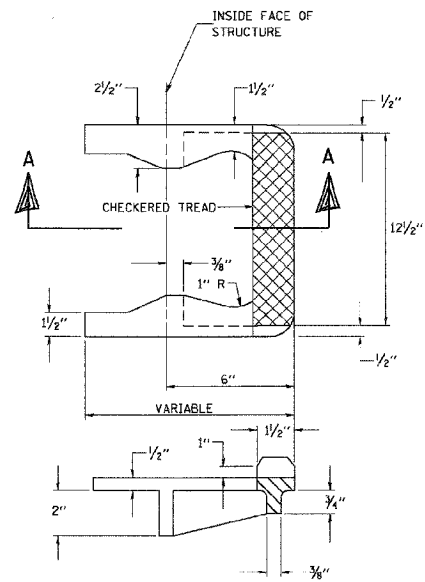
**DRAINAGE DETAILS - VIII  
DRAINAGE STRUCTURE SPECIAL  
STA. 395+85**

SCALE: N.T.S. DRAWN BY: MDB  
DATE: 02/23/07 CHECKED BY: MJK

**TENG** TENG & ASSOCIATES, INC.  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE = 02/23/07  
FILE NAME = 022307.DWG  
PLOT SCALE = 1/8"=1'-0"  
USER NAME = JWB

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (I&2) WRS	WILL.	334	193
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



SECTION A-A  
CAST IRON STEPS

**MATERIAL REQUIRED FOR  
DRAINAGE STRUCTURE SPECIAL  
AT STA. 258+92**

Bar	No.	Size	Length (ft)	Shape
a6(E)	12	#6	7'-1"	U
a7(E)	8	#6	4'-2"	U
a8(E)	4	#8	7'-7"	U
a9(E)	4	#4	5'-9"	U
a4(E)	8	#4	2'-0"	U
b12(E)	10	#6	11'-0"	U
b13(E)	4	#6	4'-4"	U
b14(E)	4	#4	11'-0"	U
b15(E)	4	#6	2'-4"	U
h7(E)	10	#5	5'-5"	U
h8(E)	10	#5	10'-8"	U
h9(E)	2	#5	10'-9"	U
h10(E)	12	#5	3'-10"	U
h11(E)	4	#5	2'-6"	U
s1(E)	14	#3	2'-1"	U
s1(E)	13	#3	1'-10"	U
s2(E)	13	#3	2'-0"	U
l3(E)	15	#5	5'-5"	U
v5(E)	46	#5	3'-0"	U
w2(E)	10	#5	10'-8"	U

Item	Unit	Quantities
Concrete Structures	Cu Yd	5.2
Reinforcement Bars, Epoxy Coated	lbs	1,140
Frame and Grates, Type 22	Each	1
Steps	Each	2

\*\* Item included in the cost for Drainage Structure Special.

**MATERIAL REQUIRED FOR  
DRAINAGE STRUCTURE SPECIAL  
AT STA. 363+33**

Bar	No.	Size	Length (ft)	Shape
a1(E)	12	#6	7'-6"	U
a1(E)	8	#6	4'-7"	U
a2(E)	4	#8	8'-0"	U
a3(E)	4	#4	6'-2"	U
a4(E)	8	#4	2'-0"	U
b1(E)	11	#6	11'-2"	U
b1(E)	4	#6	4'-5"	U
b2(E)	4	#4	11'-2"	U
b3(E)	4	#6	2'-5"	U
h1(E)	12	#5	5'-10"	U
h1(E)	12	#5	10'-10"	U
h3(E)	16	#5	4'-0"	U
h4(E)	8	#5	3'-3"	U
s1(E)	14	#3	2'-1"	U
s1(E)	13	#3	1'-10"	U
s2(E)	13	#3	2'-0"	U
l1(E)	15	#5	5'-10"	U
v1(E)	60	#5	4'-1"	U
w1(E)	10	#5	10'-10"	U

Item	Unit	Quantities
Concrete Structures	Cu Yd	5.4
Reinforcement Bars, Epoxy Coated	lbs	1,350
Frame and Grates, Type 22	Each	1
Steps	Each	3

\*\* Item included in the cost for Drainage Structure Special.

**MATERIAL REQUIRED FOR  
DRAINAGE STRUCTURE SPECIAL  
AT STA. 372+85**

Bar	No.	Size	Length (ft)	Shape
a1(E)	14	#6	7'-6"	U
a1(E)	8	#6	4'-7"	U
a2(E)	4	#8	8'-0"	U
a3(E)	4	#4	6'-2"	U
a4(E)	8	#4	2'-0"	U
a5(E)	26	#6	2'-0"	U
b4(E)	13	#6	12'-4"	U
b5(E)	4	#6	5'-0"	U
b6(E)	6	#4	12'-4"	U
b7(E)	4	#6	3'-0"	U
h1(E)	12	#5	5'-10"	U
h2(E)	12	#5	12'-4"	U
h4(E)	8	#5	3'-3"	U
h5(E)	16	#5	5'-0"	U
h6(E)	4	#5	13'-6"	U
s1(E)	14	#3	2'-1"	U
s1(E)	13	#3	1'-10"	U
s2(E)	13	#3	2'-0"	U
l1(E)	17	#5	7'-2"	U
l2(E)	13	#5	8'-4"	U
v1(E)	46	#5	4'-9"	U
v4(E)	16	#5	7'-6"	U
w1(E)	10	#5	12'-4"	U

Item	Unit	Quantities
Concrete Structures	Cu Yd	8.4
Reinforcement Bars, Epoxy Coated	lbs	1,890
Frame and Grates, Type 22	Each	1
Steps	Each	3

\*\* Item included in the cost for Drainage Structure Special.

**MATERIAL REQUIRED FOR  
DRAINAGE STRUCTURE SPECIAL  
AT STA. 395+85**

Bar	No.	Size	Length (ft)	Shape
a1(E)	16	#6	7'-6"	U
a1(E)	8	#6	4'-7"	U
a2(E)	4	#8	8'-0"	U
a3(E)	4	#4	6'-2"	U
a4(E)	8	#4	2'-0"	U
b8(E)	11	#6	12'-8"	U
b9(E)	4	#6	5'-2"	U
b10(E)	4	#4	12'-8"	U
b11(E)	4	#6	3'-2"	U
h1(E)	14	#5	5'-10"	U
h2(E)	14	#5	12'-4"	U
h4(E)	8	#5	3'-3"	U
h5(E)	16	#5	5'-0"	U
s1(E)	14	#3	2'-1"	U
s1(E)	13	#3	1'-10"	U
s2(E)	13	#3	2'-0"	U
l1(E)	17	#5	5'-10"	U
v2(E)	62	#5	4'-6"	U
w1(E)	10	#5	12'-4"	U

Item	Unit	Quantities
Concrete Structures	Cu Yd	6.2
Reinforcement Bars, Epoxy Coated	lbs	1,570
Frame and Grates, Type 22	Each	1
Steps	Each	3

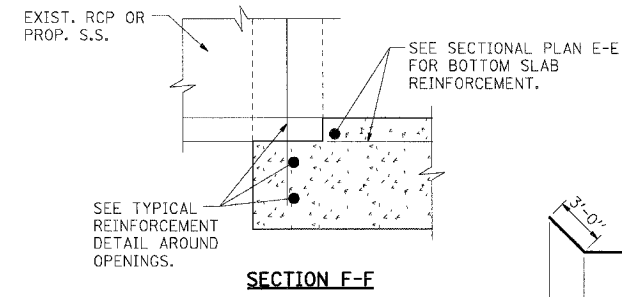
\*\* Item included in the cost for Drainage Structure Special.

**BILL OF MATERIAL**

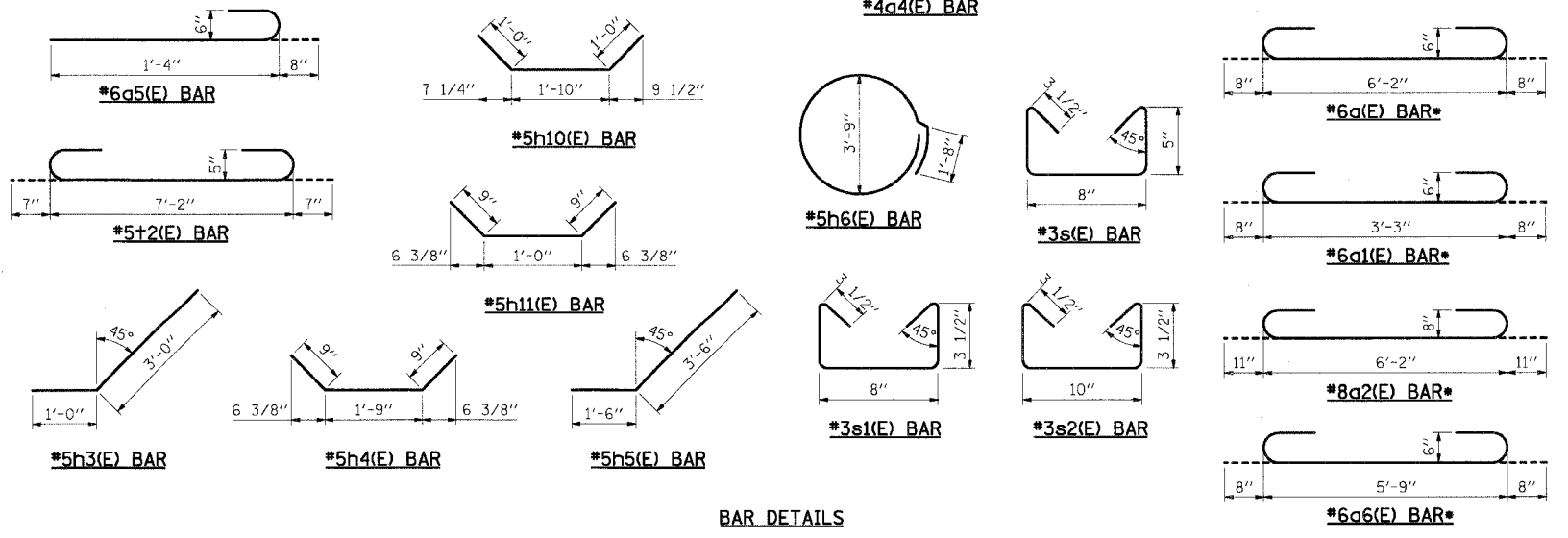
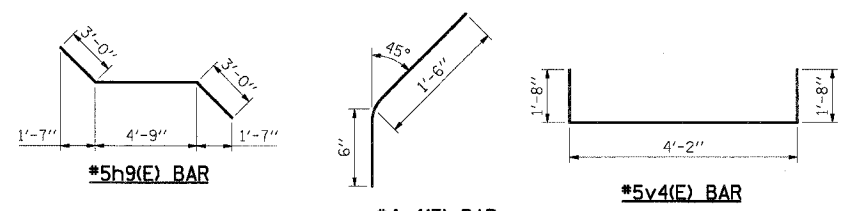
Item	Unit	Total
DRAINAGE STRUCTURE SPECIAL	EACH	4

**NOTES:**

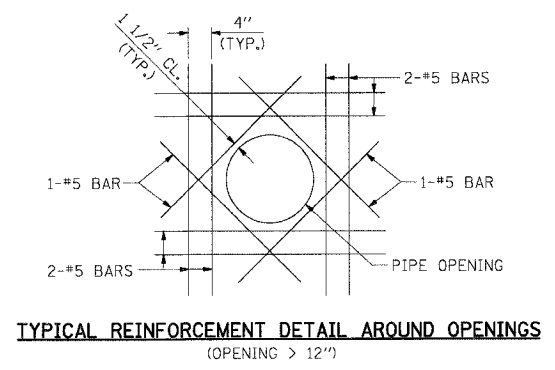
1. WORK THIS SHEET WITH SHTS. 189, 190, 191, & 192.
2. ALL BARS DESIGNATED (E) SHALL BE EPOXY COATED.



SECTION F-F



BAR DETAILS



TYPICAL REINFORCEMENT DETAIL AROUND OPENINGS  
(OPENING > 12")

REVISIONS	DATE
NAME	

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

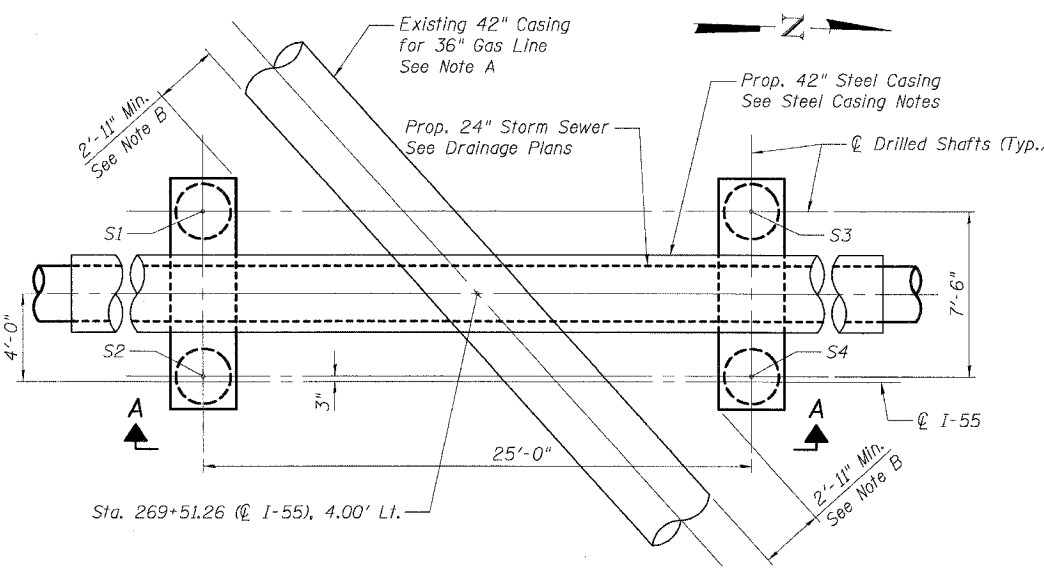
**DRAINAGE DETAILS - IX  
DRAINAGE STRUCTURE SPECIAL  
DETAILS & QUANTITIES**

SCALE: N.T.S. DRAWN BY: MJB  
DATE: 02/23/07 CHECKED BY: MJK

**TENG**  
ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

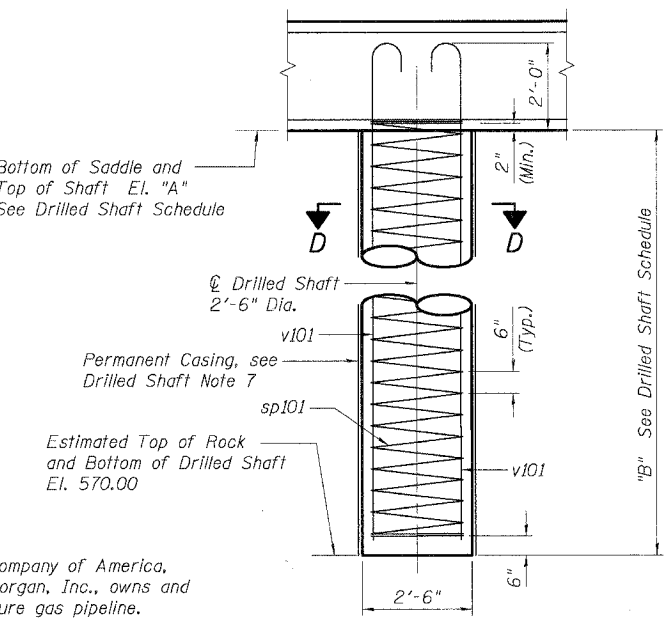
PLOT DATE = 02/23/07  
 FILE NAME = 022307.DWG  
 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = MJB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	194
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

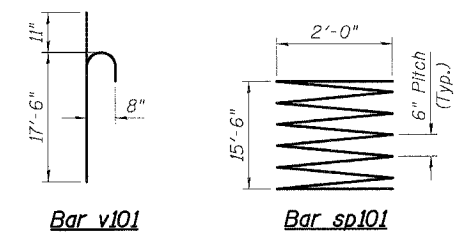


**DRILLED SHAFT AND SADDLE PLAN LAYOUT  
AT KINDER MORGAN GAS MAIN AM #4 \***

\* Natural Gas Pipeline Company of America, a subsidiary of Kinder Morgan, Inc., owns and operates this high pressure gas pipeline.



**DRILLED SHAFT ELEVATION**



**Spiral Notes:**

1. Provide 1/2 extra turns top and bottom of spiral. Dimensions are out-to-out. Min. lap length = 2'-0".
2. Extend spiral 2" min. into saddle. Provide 4- #5 spacers or equivalent for spirals. Cost of spacers included with Reinforcement Bars.

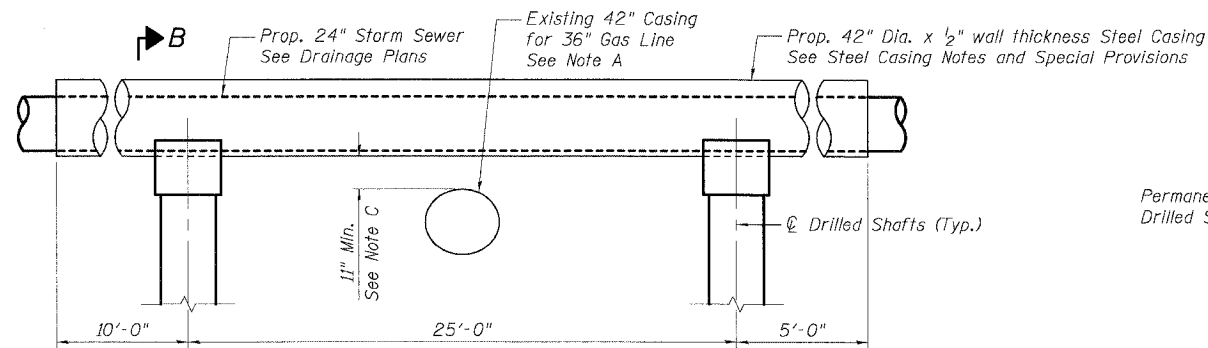
**DRILLED SHAFT SCHEDULE**

Location	Shaft No.	Station	Offset	El. "A"	"B"
AM #4	S1	269+38.76	7.75' Lt.	585.62	15.62 Ft.
	S2	269+38.76	0.25' Lt.	585.62	15.62 Ft.
	S3	269+63.76	7.75' Lt.	585.82	15.82 Ft.
	S4	269+63.76	0.25' Lt.	585.82	15.82 Ft.

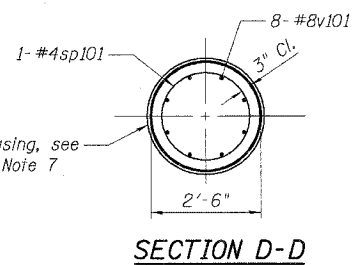
**BAR LIST**

Bar	No.	Size	Length	Shape
p101	12	#6	6'-6"	—
p102	6	#6	10'-2"	—
p103	16	#8	10'-2"	—
s101	32	#4	10'-5"	□
s102	4	#4	9'-11"	□
s103	4	#4	9'-5"	□
s104	14	#4	8'-11"	□
sp101	4	#4	15'-6"	
v101	32	#8	18'-5"	—

\*\*\* Length is height of spiral, not the actual bar length. The mass of spiral bars is based upon a 30 ft. maximum bar length with 2'-0" minimum lap splices.



**ELEVATION A-A**



**SECTION D-D**

**Drilled Shaft Notes:**

1. The maximum applied bearing pressure at the bottom of the drilled shaft is 12 tsf.
2. The allowable bearing pressure is 30 tsf.
3. The bottom of the drilled shaft shall be founded on top of rock.
4. The quantities and reinforcement detailing are based on the top of shaft and the estimated top of rock elevations shown and may change based on the actual top of rock encountered at each shaft and the final top of shaft elevation.
5. Provide a positive seal between the bottom of permanent casing and top of bedrock.
6. See Shls. 257, 261 and 262 for boring locations and boring logs.
7. Permanent Casing shall have 30" min. inside diameter.

**TOTAL BILL OF MATERIAL**

Item	Unit	Total
Structure Excavation **	Cu. Yd.	41
Concrete Structures	Cu. Yd.	5.5
Reinforcement Bars	Pound	3,190
Permanent Casing	Foot	63
Drilled Shaft in Soil	Cu. Yd.	11.5
Steel Casings 42"	Foot	40

\*\* See Note 2 below.

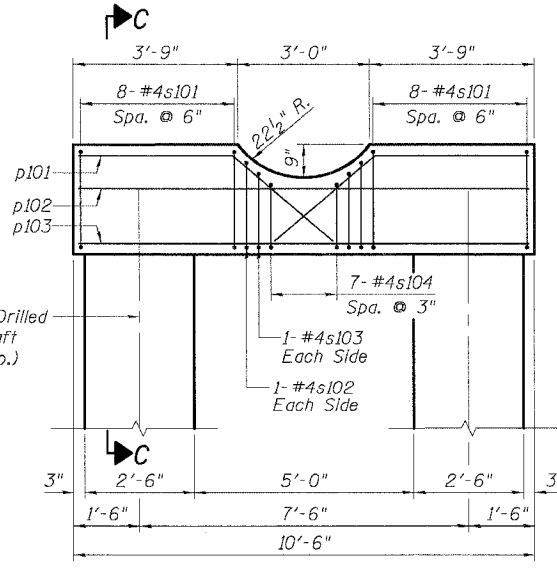
**Note A:** The drilled shaft and saddle design and details accommodate the existing utility lines as shown based on the available record documents. The Contractor shall field verify the exact location and elevation of the utility lines prior to proceeding with any construction activities that may affect the subject utility. If the Contractor finds conflict with the proposed construction, they should inform the Engineer of the conflict, provide relevant information/measurements, and ask for modifications to the proposed design and details.

**Note B:** A minimum of 35" horizontal clearance shall be maintained between drilled shaft/saddle and gas main.

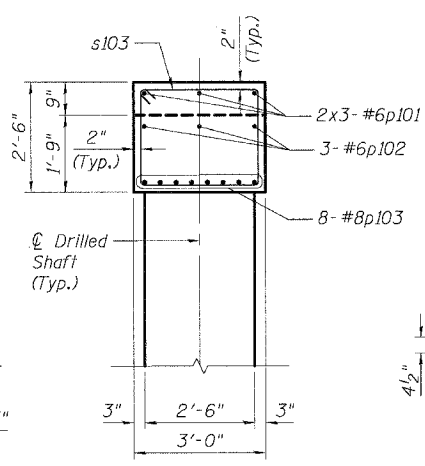
**Note C:** A minimum of 11" vertical clearance shall be maintained between proposed storm sewer and gas main.

**Steel Casing Notes:**

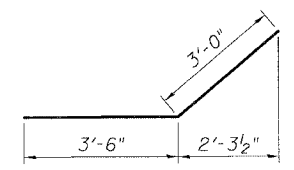
1. The Contractor shall provide cathodic protection as required by Kinder Morgan, Inc., including but not limited to the installation of cathodic protection test leads and interference testing. Cost shall be included in the contract unit price for STEEL CASINGS 42".
2. The steel casing pipe shall be coated with a suitable pipe coating as required by Kinder Morgan, Inc. for a distance of at least 10 feet on either side of the location where the sewer crosses the Kinder Morgan facility. Cost shall be included in the contract unit price for STEEL CASINGS 42".
3. The contractor shall plug the space between the outside of the sewer pipe and the inside of the steel casing pipe with bricks and mortar for a minimum of 18" along the length of the pipes. Cost shall be included in the contract unit price for STEEL CASINGS 42".



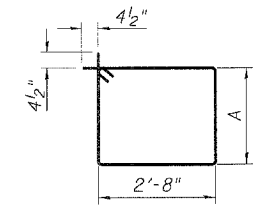
**SADDLE ELEVATION B-B**



**SECTION C-C**



**Bar p101**



**Bars s101 thru s104**

Bar	A
s101	2'-2"
s102	1'-11"
s103	1'-8"
s104	1'-5"

**Notes:**

1. All edges shall have a 3/4" chamfer unless noted otherwise.
2. Structure Excavation shall be measured and paid for according to Section 502 of Standard Specifications except the vertical dimension for Structure Excavation will be the average depth from the surface of the material to be excavated to the bottom of the saddle.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
FAI ROUTE 55 (I-80 TO BLACK ROAD)  
WIDENING AND RESURFACING

**DRAINAGE DETAILS - X  
STORM SEWER SADDLES  
AT KINDER MORGAN GAS MAIN**

SCALE: \_\_\_\_\_ DRAWN BY: MDB  
DATE: 02/23/07 CHECKED BY: MJK

**TENG** ENGINEERS/ARCHITECTS/PLANNERS  
CHICAGO, ILLINOIS

PLOT DATE: 02/23/07  
 PLOT SCALE: 1/8" = 1'-0"  
 USER NAME: MUSER8

VERIFIED UTILITY INFORMATION

TEST HOLE #	SIZE & TYPE	NORTHING	EASTING	OFFSET	TOP OF UTILITY ELEVATION	EXISTING CUT	REFERENCE GROUND ELEVATION	COMMENTS
1	36" G	1761977.91	1021140.02	30.97' LT	588.40'	5.78'	594.18'	UTILITY QUALITY LEVEL "A"
2	36" G	1761442.74	1021158.57	28.65' LT	583.76'	7.80'	591.56'	UTILITY QUALITY LEVEL "A"
3	24" G	1761342.59	1021165.10	25.29' LT	585.29'	5.77'	591.06'	UTILITY QUALITY LEVEL "A"
4	36" G	1761311.69	1021168.82	22.55' LT	584.45'	6.60'	591.05'	UTILITY QUALITY LEVEL "A"
5	30" G	1761217.69	1021170.05	24.24' LT	583.81'	6.03'	589.84'	UTILITY QUALITY LEVEL "A"
6	36" G	1760961.17	1021177.66	24.58' LT	581.10'	6.45'	587.56'	UTILITY QUALITY LEVEL "A"
7	36" G	1760960.02	1021451.05	28.96' RT	581.72'	5.45'	587.17'	UTILITY QUALITY LEVEL "A"
8	36" G	1761476.94	1021438.16	32.12' RT	588.50'	3.80'	592.30'	UTILITY QUALITY LEVEL "A"
9	30" G	1761502.81	1021498.60	93.34' RT	587.35'	6.32'	593.67'	UTILITY QUALITY LEVEL "A"
10	36" G	1761565.03	1021437.19	33.89' RT	586.18'	6.56'	592.74'	UTILITY QUALITY LEVEL "A"
11	24" G	1761620.42	1021437.02	35.44' RT	587.24'	6.48'	593.72'	UTILITY QUALITY LEVEL "A"
12	36" G	1761724.77	1021430.72	32.38' RT	585.79'	7.79'	593.57'	UTILITY QUALITY LEVEL "A"
13	36" G	1762033.48	1021427.22	35.74' RT	582.33'	11.82'	594.15'	UTILITY QUALITY LEVEL "A"
14	36" G	1764930.49	1021304.11	76.43' RT	576.62'	7.58'	584.20'	UTILITY QUALITY LEVEL "A"
15	36" G	1764892.83	1021316.93	88.20' RT	575.86'	9.18'	585.04'	UTILITY QUALITY LEVEL "A"
16	20" G	1764347.85	1021327.45	83.64' RT	583.13'	6.03'	589.16'	UTILITY QUALITY LEVEL "A"
17	42" G	1764997.02	1021191.01	34.70' LT	575.87'	7.42'	583.29'	UTILITY QUALITY LEVEL "A"
18	40" G	1764968.48	1021190.20	36.39' LT	578.66'	4.85'	583.51'	UTILITY QUALITY LEVEL "A"
19	24" G	1764412.46	1021208.28	33.70' LT	581.30'	5.83'	587.13'	UTILITY QUALITY LEVEL "A"
20	36" G	1761929.14	1020284.46	88.41' LT	587.94'	4.36'	592.30'	UTILITY QUALITY LEVEL "A"
21	36" G	1761113.92	1020837.27	358.93' LT	585.00'	3.99'	588.99'	UTILITY QUALITY LEVEL "A"
22	24" G	1761070.06	1020865.93	331.66' LT	584.19'	4.36'	588.55'	UTILITY QUALITY LEVEL "A"
23	36" G	1761033.53	1020896.84	301.92' LT	584.14'	4.01'	588.15'	UTILITY QUALITY LEVEL "A"
24	30" G	1760973.03	1020934.20	266.48' LT	583.97'	4.14'	588.11'	UTILITY QUALITY LEVEL "A"
25	36" G	1760931.81	1020970.94	231.05' LT	584.58'	3.48'	588.06'	UTILITY QUALITY LEVEL "A"
26	SEE NOTE G	1761558.07	1021554.22	152.28' RT	590.54'	3.36'	593.90'	UTILITY QUALITY LEVEL "A"
27	30" G	1761607.88	1021535.48	135.12' RT	590.48'	4.26'	594.74'	UTILITY QUALITY LEVEL "A"
28	SEE NOTE G	1761643.16	1021517.84	118.60' RT	586.00'	9.44'	595.44'	UTILITY QUALITY LEVEL "A"
29	24" G	1761708.92	1021492.06	94.90' RT	590.37'	4.86'	595.23'	UTILITY QUALITY LEVEL "A"
30	SEE NOTE G	1761763.53	1021469.32	73.89' RT	586.90'	8.79'	595.69'	UTILITY QUALITY LEVEL "A"

VERIFIED UTILITY INFORMATION

TEST HOLE #	SIZE & TYPE	NORTHING	EASTING	OFFSET	TOP OF UTILITY ELEVATION	EXISTING CUT	REFERENCE GROUND ELEVATION	COMMENTS
31	16" G	1789526.07	1027970.20	2.66' LT	608.82'	4.65'	613.47'	UTILITY QUALITY LEVEL "A"
32	24" W	1783790.30	1025967.71	0.00	587.07'	1.93'	589.00'	UTILITY QUALITY LEVEL "A"
33	UNK W	1774087.19	1026281.14	3.36' RT	574.91'	7.93'	582.84'	SEE NOTE
34	UNK G	1766986.22	1023114.78	1.67' LT	573.20'	7.89'	581.09'	SEE NOTE
35	42" G	1764617.74	1021889.31	0.28' RT	587.97'	6.96'	594.93'	SEE NOTE
36	UNK G	1764589.69	1021872.25	2.55' LT	SEE NOTE	7.00'	595.26'	SEE NOTE
37	24" G	1764157.65	1021682.33	1.48' LT	589.73'	5.45'	595.18'	SEE NOTE
38	UNK FM	1762546.66	1021297.11	6.66' LT	587.79'	6.21'	594.00'	SEE NOTE
39	*42" G	1762004.68	1021276.95	4.74' LT	587.18'	5.72'	592.90'	UTILITY QUALITY LEVEL "A"
40	36" G	1761581.40	1021293.39	0.37' RT	585.36'	5.66'	591.02'	UTILITY QUALITY LEVEL "A"
41	24" G	1761474.96	1021295.11	1.20' LT	584.35'	6.04'	590.39'	SEE NOTE
42	42" G	1761429.66	1021296.67	1.03' LT	586.74'	3.48'	590.22'	UTILITY QUALITY LEVEL "A"
43	30" G	1761375.95	1021297.64	1.72' LT	588.38'	1.52'	589.90'	UTILITY QUALITY LEVEL "A"
43A	30" G	1761370.06	1021291.56	7.98' LT	588.39'	2.73'	591.12'	UTILITY QUALITY LEVEL "A"
44	30" G	1761342.52	1021299.05	1.35' LT	584.31'	5.24'	589.55'	SEE NOTE
45	24" Other	1753098.46	1021540.98	31.58' LT	551.06'	6.96'	558.02'	UTILITY QUALITY LEVEL "A"
46	30" G	1752991.92	1021543.34	32.51' LT	547.97'	7.87'	555.84'	UTILITY QUALITY LEVEL "A"
47	34" G	1752963.80	1021545.85	30.86' LT	546.76'	8.56'	555.32'	UTILITY QUALITY LEVEL "A"
48	UNK G	1753032.83	1021546.00	28.59' LT	548.09'	9.26'	557.35'	SEE NOTE
49	NA G	1760959.49	1021311.18	1.05' LT	580.65'	5.02'	585.67'	UTILITY QUALITY LEVEL "A"

NOTES

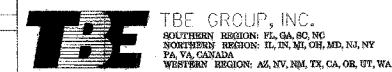
TH#17, 18 & 19 According to record drawings, the gas main under College St. is placed in a casing.  
 TH#26 Unable to expose utility due to water, but were able to probe to feel utility.  
 TH#28 Unable to expose utility due to mud and water, but were able to probe to feel utility.  
 TH#30 Unable to expose utility due to hard clay, but were able to probe to feel utility.  
 TH#33, 34, 35, 37, 38, 41, 41 & 48 Unable to expose utility due to excessive ground water, but were able to probe and feel top and sides of utility.  
 TH#36 Hole was opened to a depth of 7' before getting excessive ground water. Probed to a depth of 12' and could not find utility. Clearance depth only.  
 \*TH#39 42" casing for 36" gas main.

ALL INFORMATION SHOWN WAS OBTAINED FROM A LOCATION SURVEY. OFFSETS AND STATIONS ARE IN RELATION TO THE BASELINE OF SURVEY.

Illinois Department of Transportation

I-551-80 from US RT. 30 to Weber Road  
 Will County, Joliet, IL  
 Section No. 99 (1&2) WRS, 99 (1&2) WRS-2 & 2006-031  
 Contract No. 62895, 62896, 60B46 and 60B85  
 TBE Project No.: IL09500132, 142, 158, 229, 245, 263

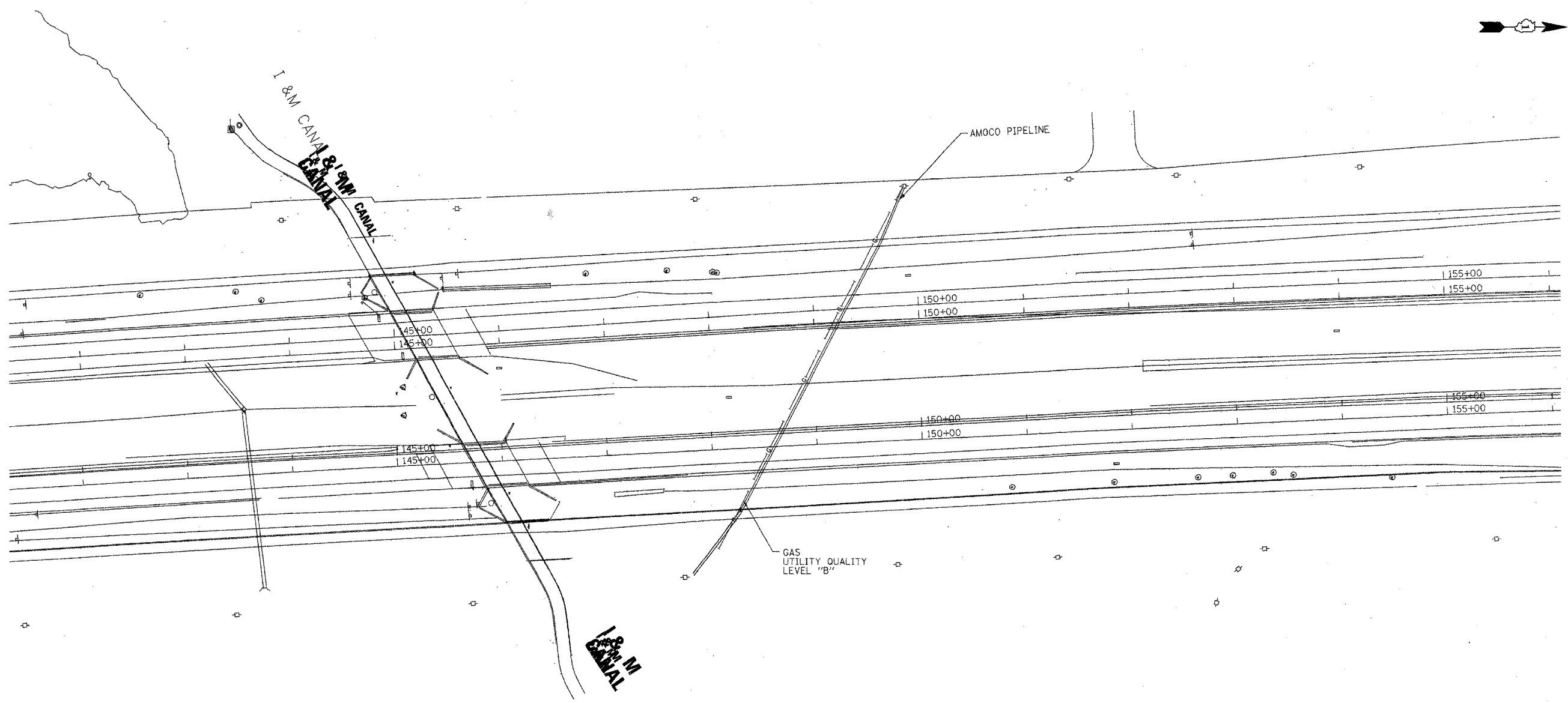
REVISIONS											
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION
12-4-06	ACC	GA/OC Test holes									



VERIFIED UTILITY LOCATIONS

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CONTRACT NO. 62895				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99 (1&2) WRS	WILL	334	196
STA. 142+00		TO STA. 156+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**TBE GROUP, INC.**  
 CIVIL ENGINEERING \* TRANSPORTATION \* ENVIRONMENTAL  
 \* PLANNING \* UTILITY ENGINEERING/LOCATING

IL09500132, 142, 158, 229, 245  
 TBE SUE PAGE NO: 3 of 29

Checked by: *S. DeLoe*

Utility Quality Level "A" : Test Holes  
 Utility Quality Level "B" : Designating

	FORCE MAIN
	TELEPHONE
	WATER
	GAS
	CABLE TELEVISION
	FIBER OPTIC
	ELECTRIC
	TEST HOLE

The SBC locations depicted have been obtained through the application of geophysical methods to determine the existence and approximate horizontal position of these facilities. However, SBC will not provide TBE Group, Inc. with utility records nor allow access to their field closures (pedestals/manholes etc.), to help verify the locations of their existing underground facilities. Therefore, TBE is unable to verify the completeness of the SBC locations depicted in accordance with the CI/ASCE Standard 38-02.

Utilities shown on these plans as depicted in the legend have been investigated by TBE Group, Inc in accordance with SUE Industry Standards. All other information shown has been provided to TBE Group, Inc by others. Changes to utilities after dates shown may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

205 W. WACKER DRIVE  
 SUITE 1020  
 CHICAGO, IL 60606  
 (312) 704-1970

REVISIONS	
NAME	DATE

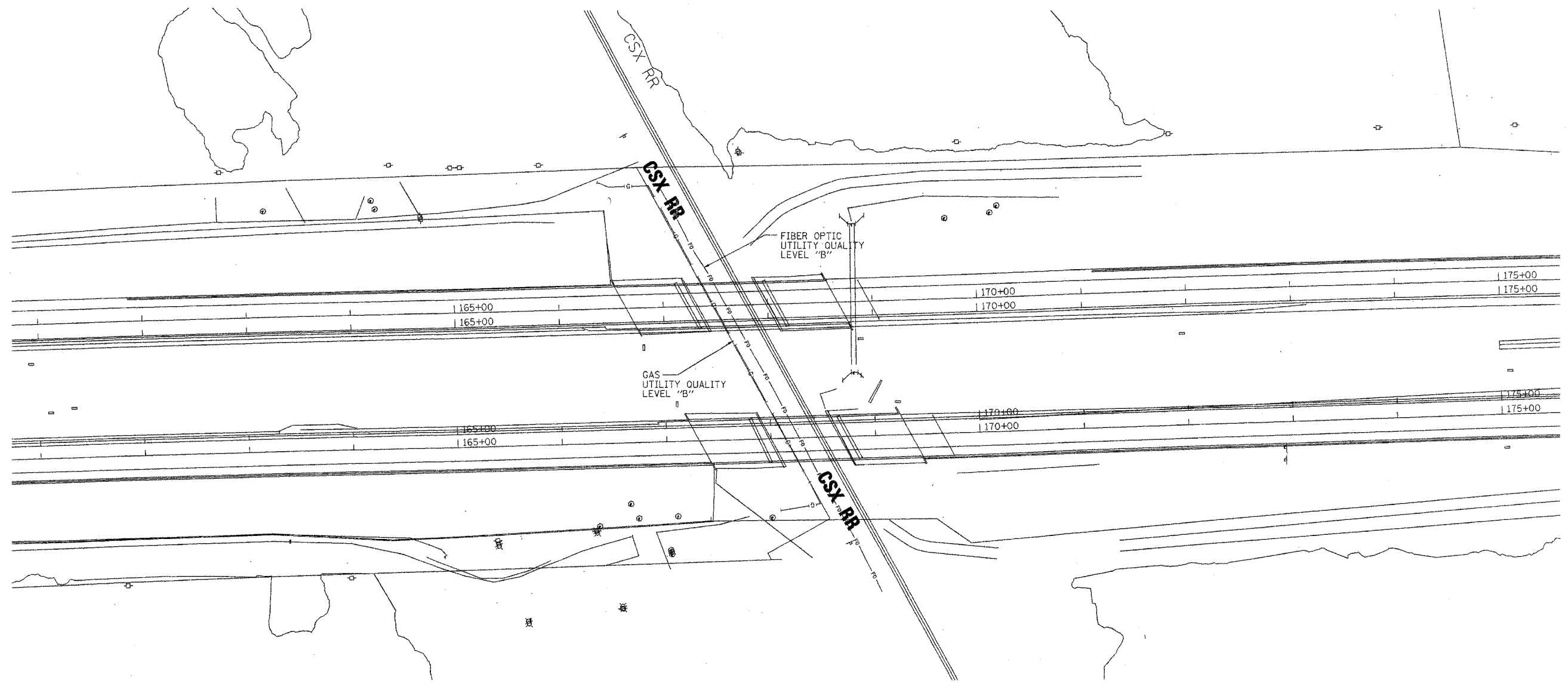
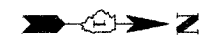
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SUE Investigation of Underground Utilities  
 I-55/I-80 from US RT. 30 to Weber Road  
 Section No. 99 (1&2) WRS-1  
 Contract No. 62895 and 62896  
 Will County  
 SQL "B" DATE : 1/17/06  
 DRAWN BY : KLC  
 SCALE : 1" = 50'



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CONTRACT NO. 62895

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	99(1&2)WRS	WILL	334	197
STA. 161+00		TO STA. 175+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



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 \* PLANNING \* UTILITY ENGINEERING/LOCATING

IL09500132, 142, 158, 229, 245  
 TBE SUE PAGE NO: 4 of 29  
 Checked by: *S. A. 06*

Utility Quality Level "A" : Test Holes  
 Utility Quality Level "B" : Designating

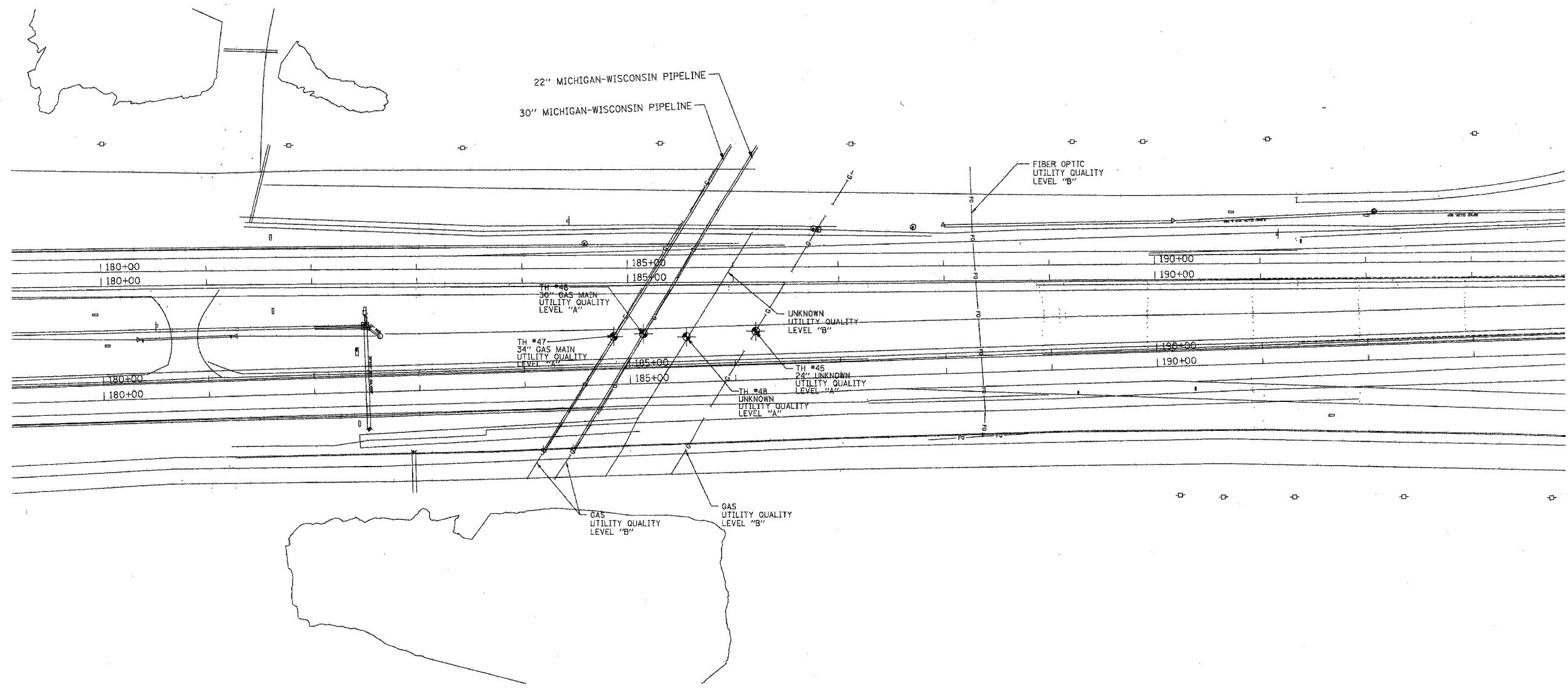
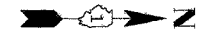
—●—●—●—●—●—	FORCE MAIN
—T—T—	TELEPHONE
—W—W—	WATER
—G—G—	GAS
—CTV—CTV—	CABLE TELEVISION
—FO—FO—	FIBER OPTIC
—E—E—	ELECTRIC
●	TEST HOLE

205 W. WACKER DRIVE  
 SUITE 1020  
 CHICAGO, IL 60606  
 (312) 704-1970

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SUE Investigation of Underground Utilities  
 I-55/I-80 from US RT. 30 to Weber Road  
 Section No. 99 (1&2) WRS-1  
 Contract No. 62895 and 62896  
 Will County  
 SOL "B" DATE : 1/17/06  
 DRAWN BY : KLC  
 SCALE : 1" = 50'

CONTRACT NO. 62895			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
99 (I&2) WRS, 99 (I&2) WRS-2 & 2006-031	W111	W111	334
SHEET NO. 198		TO STA. 193+00	
STA. 180+00		ILLINOIS FED. AID PROJECT	
FED. ROAD DIST. NO.			



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 \* PLANNING \* UTILITY ENGINEERING/LOCATING

ILO9500132, 142, 158, 229, 245  
 TBE SUE PAGE NO: 5 of 29

Checked by: *[Signature]*  
 Utility Quality Level "A" : Test Holes  
 Utility Quality Level "B" : Designating

—●—●—●—●—●—●—●—●—●—	FORCE MAIN
—T—T—T—T—T—T—T—T—T—	TELEPHONE
—W—W—W—W—W—W—W—W—W—	WATER
—G—G—G—G—G—G—G—G—G—	GAS
—CTV—CTV—CTV—CTV—CTV—CTV—CTV—CTV—CTV—	CABLE TELEVISION
—FO—FO—FO—FO—FO—FO—FO—FO—FO—	FIBER OPTIC
—E—E—E—E—E—E—E—E—E—	ELECTRIC
●	TEST HOLE

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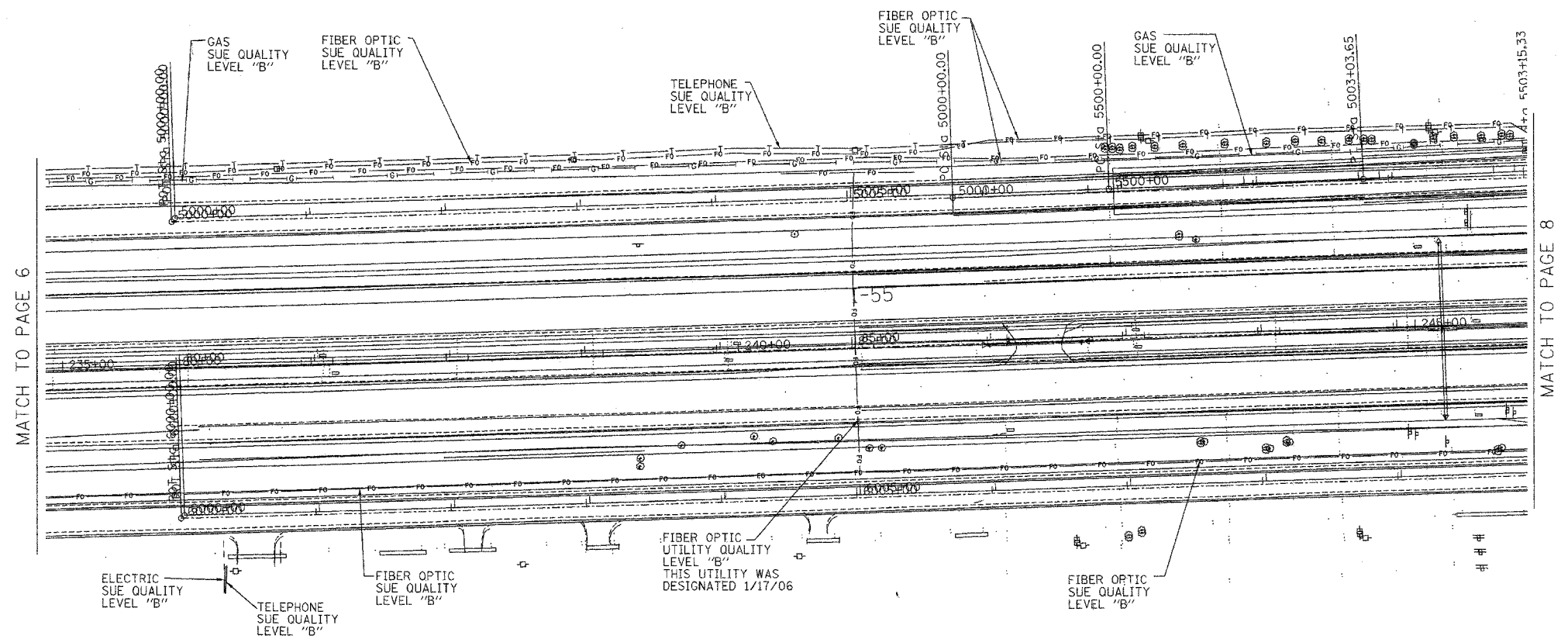
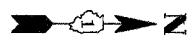
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SUE Investigation of Underground Utilities  
 I-55/I-80 from US RT. 30 to Weber Road  
 Will County  
 Section No. 99 (I&2) WRS, 99 (I&2) WRS-2 & 2006-031  
 Contract No. 62895, 62896, 60846 and 60885  
 SQL "A" DATE : 11/07/06      DRAWN BY : KLC  
 SQL "B" DATE : 1/17/06      SCALE : 1" = 50'



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CONTRACT NO. 62895			
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS NO.
55	99(1&2) WRS	WILL	334 200
STA. 285+00		TO STA. 245+00	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	

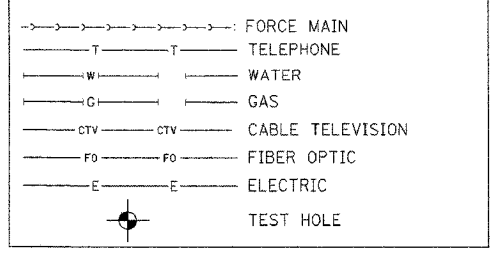


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 \* PLANNING \* UTILITY ENGINEERING/LOCATING

IL09500132, 142, 158, 229, 245  
 TBE SUE PAGE NO: 7 of 29

Checked by: *John A. Pe...*

Utility Quality Level "A" : Test Holes  
 Utility Quality Level "B" : Designating



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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SUE Investigation of Underground Utilities  
 I-55/I-80 from US RT. 30 to Weber Road  
 Section No. 99 (1&2) WRS-1  
 Contract No. 62895 and 62896  
 Will County  
 SOL "B" DATE : 1/13/03  
 DRAWN BY : KLC  
 SCALE : 1" = 50'