

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	1

40

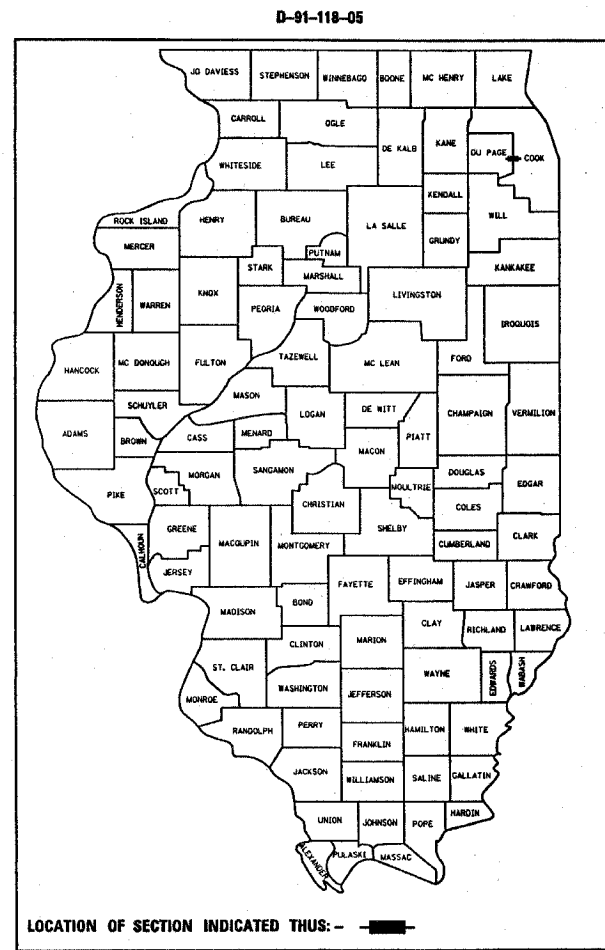
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

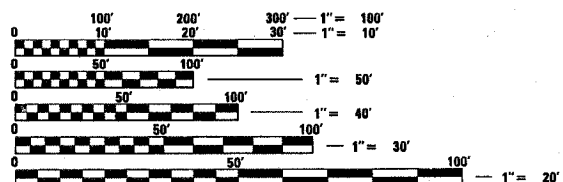
F.A.P. 311: U.S. 34  
AT COUNTY LINE ROAD  
SECTION: 9 Y-N-2  
INTERSECTION IMPROVEMENT  
PROJECT: HSIP-0311 (032)  
DUPAGE AND COOK COUNTIES  
C-91-118-05

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF HINSDALE



DISTRICT ONE - PLAN PREPARATION ENGINEER: KEN ENG / J.P. CHANG (847) 705-4432



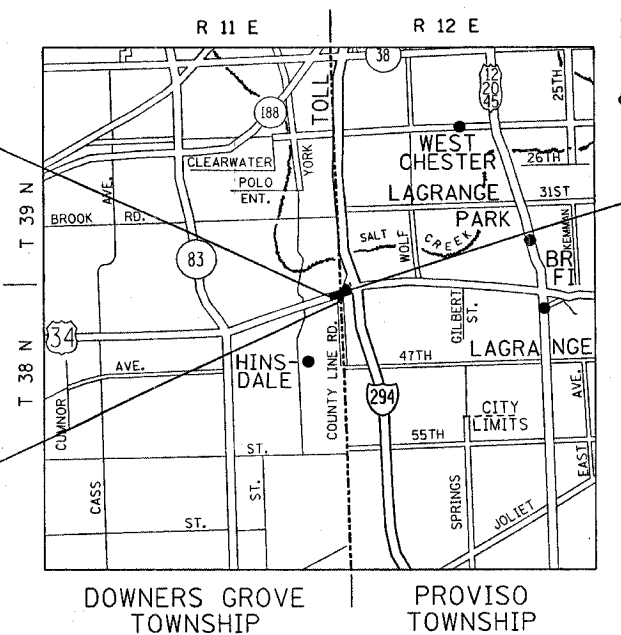
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

CONTRACT NO. 62911

U.S. 34:  
PROJECT LIMIT  
STATION 93+69

COUNTY LINE RD.:  
PROJECT LIMIT  
STATION 19+00



U.S. 34:  
PROJECT LIMIT  
STATION 106+00

TRAFFIC DATA  
US 34  
2004 ADT = 51,173  
SPEED LIMIT = 35 MPH  
COUNTY LINE ROAD  
2004 ADT = 6,625  
SPEED LIMIT = 25 MPH

SCALE: NONE

**LOCATION MAP**

US 34 GROSS AND NET LENGTH OF PROJECT = 1231 FEET (0.23 MILE)  
COUNTY LINE ROAD GROSS AND NET LENGTH OF PROJECT = 200 FEET (0.04 MILE)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED Feb 7 2007  
Dave Okie  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 23 2007  
Eric E. Haran  
ENGINEER OF DESIGN AND ENVIRONMENT

March 23 2007  
Milton R. See, P.E.  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	2
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

### INDEX OF SHEETS

SHEET NO.	DESCRIPTION
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2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-11	TYPICAL SECTIONS
12	SCHEDULE OF QUANTITIES
13	ALIGNMENT, TIES AND BENCHMARKS
14	EXISTING & PROPOSED ROADWAY PLAN
15	SUE INVESTIGATION OF UNDERGROUND UTILITIES
16	EXISTING AND PROPOSED DRAINAGE PLANS
17	RIGHT OF WAY PLAN (FOR INFORMATION ONLY)
18-19	RIGHT OF WAY PLATS (FOR INFORMATION ONLY)
20	PAVEMENT MARKING AND LANDSCAPING PLANS
21	EROSION CONTROL PLAN
22	DETECTOR LOOP REPLACEMENT PLAN
23	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
25	BUTT JOINT AND HMA TAPER DETAILS
25A	STEEL PLATE BEAM GUARDRAIL ADJACENT TO CURB AND GUTTER AND STABILIZATION AT TBT TY. 1 SPL
26	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
27	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
28	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
29	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
30	PAVEMENT MARKINGS, LETTERS AND SYMBOLS FOR TRAFFIC STAGING
31	TEMPORARY INFORMATION SIGNING
32	MEDIAN NOSE DETAIL
33	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
34	DRIVEWAY DETAILS DISTANCE BETWEEN R.O.W. AND FACE OF CURB/EDGE OF SHOULDER >= 4.5m (15')
35	DRIVEWAY DETAILS DISTANCE BETWEEN R.O.W. AND FACE OF CURB <= 4.5m (15')
36-39	CROSS SECTIONS

### STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
482001-01	BITUMINOUS SHOULDER DETAILS - ADJACENT TO FLEXIBLE PAVEMENT
542606	REINFORCED CONCRETE PIPE TEE
602001	CATCH BASIN, TYPE A
602006-01	CATCH BASIN, TYPE C
604086-01	FRAME AND GRATE, TYPE 24
606001-03	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
606301-03	PC CONCRETE ISLANDS AND MEDIANS
606306-02	
630001-07	STEEL PLATE BEAM GUARDRAIL
630301-04	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
635006-02	
701301-02	LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
701306-01	LANE CLOSURE, 2L, 2W SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH
701326-02	LANE CLOSURE 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS > 45 MPH
701336-04	LANE CLOSURE 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS > 45 MPH
701601-04	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-04	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-03	LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
702001-06	TRAFFIC CONTROL DEVICES
720001	SIGN PANEL MOUNTING DETAILS
780001-01	TYPICAL PAVEMENT MARKINGS
814001-01	CONCRETE HANDHOLES
886001	DETECTOR LOOP INSTALLATION

### GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF HINSDALE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

10' (3 METER) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB & GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER AT (847) 741-5302 A MINIMUM OF 72 HOURS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO START OF CONSTRUCTION.

THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATED PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FUFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF THE APPROVED MIX DESIGN, PLAN DIMENSIONS AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURES.

ENGINEER SHALL FIELD VERIFY INVERT ELEVATIONS PRIOR TO ORDERING MATERIALS.

SAW CUTTING SHALL BE INCLUDED IN THE COST OF PAVEMENT REMOVAL, SIDEWALK REMOVAL, DRIVEWAY REMOVAL AND MEDIAN REMOVAL PAY ITEMS.

THE RESIDENT ENGINEER SHALL CONTACT MR. CORY JUCIUS, BUREAU OF TRAFFIC, AT (847) 705-4470 BEFORE FABRICATION OF "LEFT TURN AT COUNTY LINE ROAD PROHIBITED" SIGN.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO PLACING ANY TEMPORARY TRAFFIC CONTROL DEVICES.

PLOT DATE = 3/27/2007  
 PLOT SCALE = 501/100' / IN.  
 REFERENCE = #REF#

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES

SCALE: VERT. HORIZ. DATE DRAWN BY CHECKED BY

R1

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	3
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	COOK COUNTY 1000-1A 90% FED 10% STATE	DUPAGE COUNTY 1000-1A 90% FED 10% STATE		
20200100	EARTH EXCAVATION	CU YD	467	87	380		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	282	111	171		
20800150	TRENCH BACKFILL	CU YD	66	66			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	920	260	660		
25200110	SODDING, SALT TOLERANT	SQ YD	920	260	660		
25200200	SUPPLEMENTAL WATERING	UNIT	8	3	5		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	20	6	14		
28000400	PERIMETER EROSION BARRIER	FOOT	469	469			
28000510	INLET FILTERS	EACH	10	3	7		
31101200	SUB-BASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	292		292		
31101400	SUB-BASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	1515	593	922		
35300410	PORTLAND CEMENT CONCRETE BASE COURSE 9 1/2"	SQ YD	999	442	557		
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	413		413		
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	6	3	3		
40600300	AGGREGATE (PRIME COAT)	TON	22	11	11		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	4	2	2		
40600895	CONSTRUCTING TEST STRIP	EACH	2	1	1		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	106	59	47		
40600990	TEMPORARY RAMP	SQ YD	106	59	47		
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	47		47		
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	1065	534	531		
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	44		44		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2230		2230		
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	10134	5275	4859		
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	481	20	461		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1850	518	1332		
44000600	SIDEWALK REMOVAL	SQ FT	2547		2547		

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	COOK COUNTY 1000-1A 90% FED 10% STATE	DUPAGE COUNTY 1000-1A 90% FED 10% STATE		
44003100	MEDIAN REMOVAL	SQ FT	2647	2447	200		
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	82	82			
50300300	PROTECTIVE COAT	SQ YD	1917	618	1299		
NP 55039700	STORM SEWERS TO BE CLEANED	FOOT	107		107		
550A0030	STORM SEWERS, CLASS A, TYPE 1 8"	FOOT	14	14			
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	425	425			
55100300	STORM SEWER REMOVAL 8"	FOOT	14		14		
55100500	STORM SEWER REMOVAL 12"	FOOT	70		70		
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	7	1	6		
60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	2	2			
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	3	1	2		
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	3	1	2		
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	13	1	12		
60500050	REMOVING CATCH BASINS	EACH	5		5		
60624600	CORRUGATED MEDIAN	SQ FT	60	60			
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	193		193		
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1330	176	1154		
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	245		245		
* 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	12.5	12.5			
63200310	GUARDRAIL REMOVAL	FOOT	75	75			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	3	3		
67100100	MOBILIZATION	L SUM	1	0.5	0.5		
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	0.5	0.5		
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	0.5	0.5		
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	0.5	0.5		
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1		1		
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1			

NP - NON-PARTICIPATING

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
SUMMARY OF QUANTITIES

Rev. R1

PLOT DATE: 3/2/2007

3/2/2007  
\* SPECIALTY ITEM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	4
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		COOK COUNTY 1000-1A 90% FED 10% STATE	DUPAGE COUNTY 1000-1A 90% FED 10% STATE			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.5	0.5			
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	0.5	0.5			
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1557	395	1162			
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	318	146	172			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4461	2518	1943			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	559	330	229			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	980	906	74			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	374	227	147			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	59	22	37			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	520	132	388			
* 72000100	SIGN PANEL - TYPE 1	SQ FT	6	6				
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	318	146	172			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4461	2518	1943			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	559	330	229			
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	980	906	74			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	374	227	147			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	59	22	37			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	89	48	41			
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1				
78300100	PAVEMENT MARKING REMOVAL	SQ FT	350	350				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	54	29	25			
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	216		216			
X0301245	SHOULDER REMOVAL	SQ YD	52	52				
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	128.5	77.1	51.4			

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		COOK COUNTY 1000-1A 90% FED 10% STATE	DUPAGE COUNTY 1000-1A 90% FED 10% STATE			
X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	10	3	7			
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	4		4			
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	457	229	228			
X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FOOT	348	348				
* XX006128	TRAFFIC BARRIER TERMINAL TYPE 1 (SPECIAL)	EACH	1	1				
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1				
NP Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	6	2	4			

3/2/2007

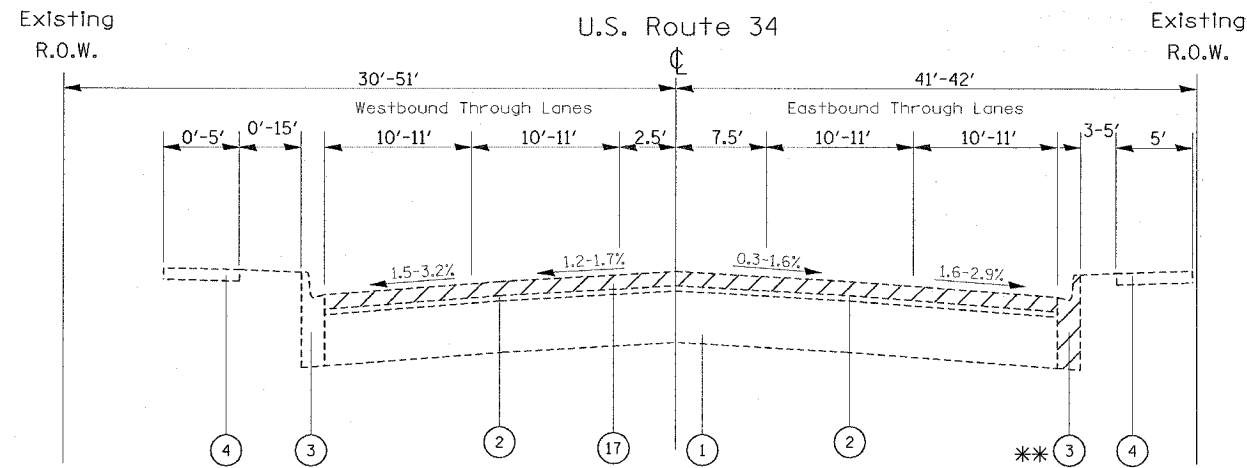
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ILLINOIS DEPARTMENT OF TRANSPORTATION  
SUMMARY OF QUANTITIES

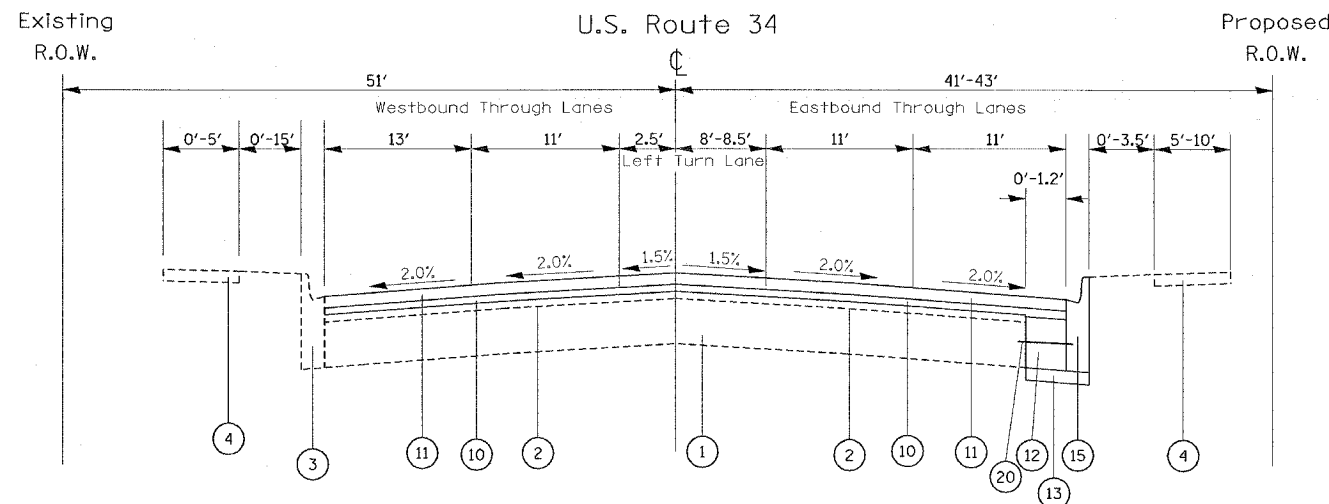
PLOT DATE: 3/2/2007

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 N-Y-2	COOK/DUPAGE	39	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



Typical Existing Cross Section

U.S. Route 34  
Sta. 93+69 - 94+88



Typical Proposed Cross Section

U.S. Route 34  
Sta. 93+69 - 94+88

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ± 9"
- ② EXISTING HMA OVERLAY, ± 3"
- ③ EXISTING CURB AND GUTTER
- ④ EXISTING P.C.C. SIDEWALK
- ⑤ EXISTING P.C.C. MEDIAN
- ⑥ EXISTING CRUSHED STONE
- ⑦ EXISTING PC CONCRETE CURB
- ⑧ EXISTING CONCRETE MEDIAN SURFACE
- ⑨ EXISTING HMA SHOULDER
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
- ⑫ PROPOSED P.C.C. BASE COURSE, 9 1/2"
- ⑬ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 6"
- ⑭ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- ⑮ PROPOSED CURB AND GUTTER, TYPE B-6.24
- ⑮A PROPOSED CURB AND GUTTER, TYPE M-4.24
- ⑯ PROPOSED P.C.C. SIDEWALK
- ⑰ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑱ PROPOSED HMA SURFACE REMOVAL
- ⑲ PROPOSED 8" HMA SHOULDER
- ⑳ PROP. DRILL AND GROUT #4 TIE BAR, 24" LONG, SHAPE (24" C-C), COST INCLUDED IN CONCRETE CURB AND GUTTER, TYPE B-6.12 PAY ITEM
- ㉑ PROPOSED MEDIAN REMOVAL

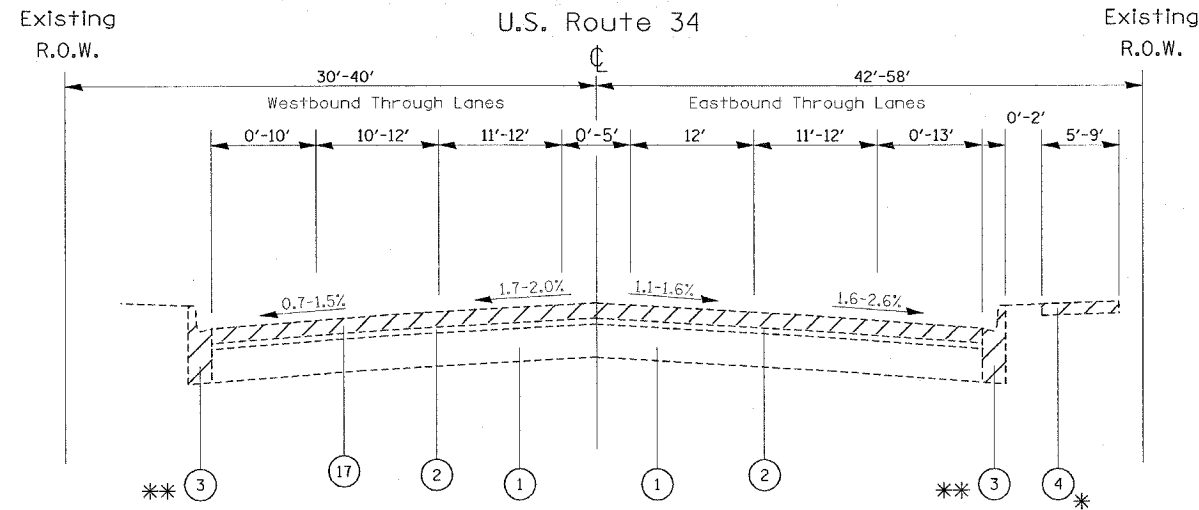
NOTES

- \* STA. 95+11 TO STA. 96+67  
STA. 97+17 TO STA. 98+22  
STA. 98+38 TO STA. 99+56
  - \*\* CURB AND GUTTER REMOVAL AND REPLACEMENT  
LOCATIONS SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN
  - \*\*\* HMA SHOULDER REMOVAL AND REPLACEMENT  
SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN
- SAW CUTTING LOCATIONS SHOWN ON PROPOSED ROADWAY PLAN

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		US 34 TYPICAL SECTIONS
SCALE:	VERT. DATE	HORIZ. CHECKED BY

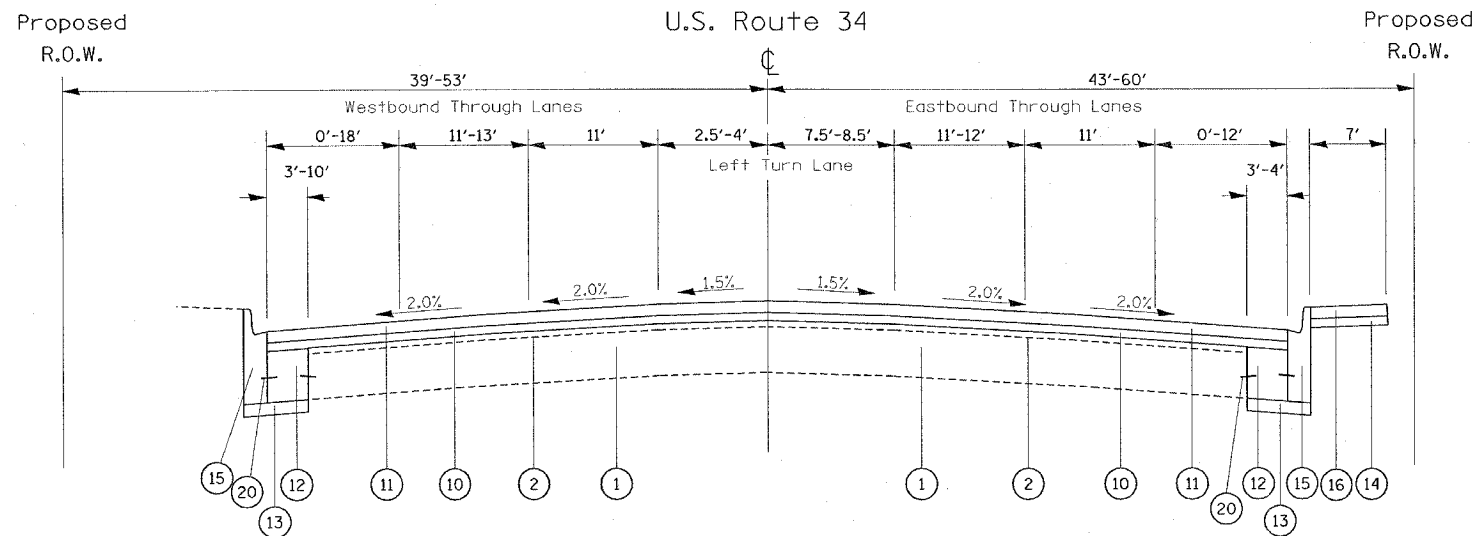
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USER = jmc

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	6
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



Typical Existing Cross Section

U.S. Route 34  
Sta. 94+88 - 100+40



Typical Proposed Cross Section

U.S. Route 34  
Sta. 94+88 - 100+40

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ± 9"
- ② EXISTING HMA OVERLAY, ± 3"
- ③ EXISTING CURB AND GUTTER
- ④ EXISTING P.C.C. SIDEWALK
- ⑤ EXISTING P.C.C. MEDIAN
- ⑥ EXISTING CRUSHED STONE
- ⑦ EXISTING PC CONCRETE CURB
- ⑧ EXISTING CONCRETE MEDIAN SURFACE
- ⑨ EXISTING HMA SHOULDER
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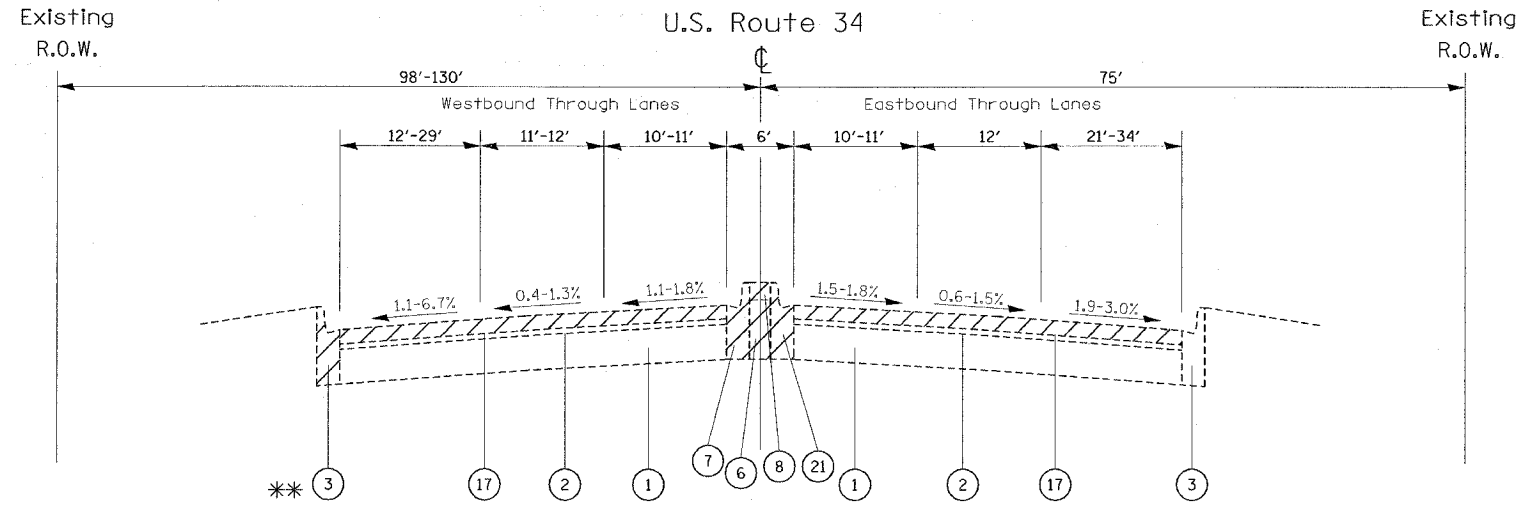
NOTES

- \* STA. 95+11 TO STA. 96+67  
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- \*\* CURB AND GUTTER REMOVAL AND REPLACEMENT  
LOCATIONS SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN
- \*\*\* HMA SHOULDER REMOVAL AND REPLACEMENT  
SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN

SAW CUTTING LOCATIONS SHOWN ON PROPOSED ROADWAY PLAN

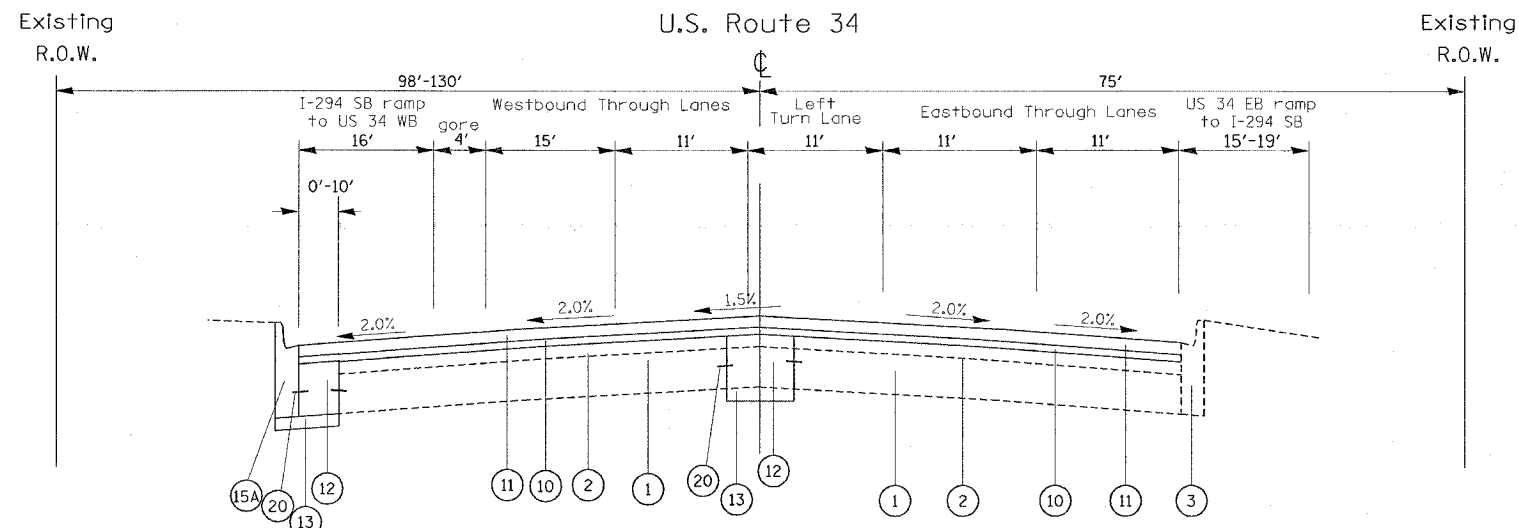
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		US 34 TYPICAL SECTIONS
SCALE:	VERT. HORIZ.	DRAWN BY
DATE		CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	7
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



Typical Existing Cross Section

U.S. Route 34  
Sta. 100+40 - 102+63



Typical Proposed Cross Section

U.S. Route 34  
Sta. 100+40 - 102+63

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ± 9"
- ② EXISTING HMA OVERLAY, ± 3"
- ③ EXISTING CURB AND GUTTER
- ④ EXISTING P.C.C. SIDEWALK
- ⑤ EXISTING P.C.C. MEDIAN
- ⑥ EXISTING CRUSHED STONE
- ⑦ EXISTING PC CONCRETE CURB
- ⑧ EXISTING CONCRETE MEDIAN SURFACE
- ⑨ EXISTING HMA SHOULDER
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
- ⑫ PROPOSED P.C.C. BASE COURSE, 9 1/2"
- ⑬ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 6"
- ⑭ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- ⑮ PROPOSED CURB AND GUTTER, TYPE B-6.24
- ⑮A PROPOSED CURB AND GUTTER, TYPE M-4.24
- ⑯ PROPOSED P.C.C. SIDEWALK
- ⑰ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑱ PROPOSED HMA SURFACE REMOVAL
- ⑲ PROPOSED 8" HMA SHOULDER
- ⑳ PROP. DRILL AND GROUT #4 TIE BAR, 24" LONG, SHAPE (24" C-C), COST INCLUDED IN CONCRETE CURB AND GUTTER, TYPE B-6.12 PAY ITEM
- ㉑ PROPOSED MEDIAN REMOVAL

NOTES

- \* STA. 95+11 TO STA. 96+67  
STA. 97+17 TO STA. 98+22  
STA. 98+38 TO STA. 99+56
- \*\* CURB AND GUTTER REMOVAL AND REPLACEMENT  
LOCATIONS SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN
- \*\*\* HMA SHOULDER REMOVAL AND REPLACEMENT  
SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN

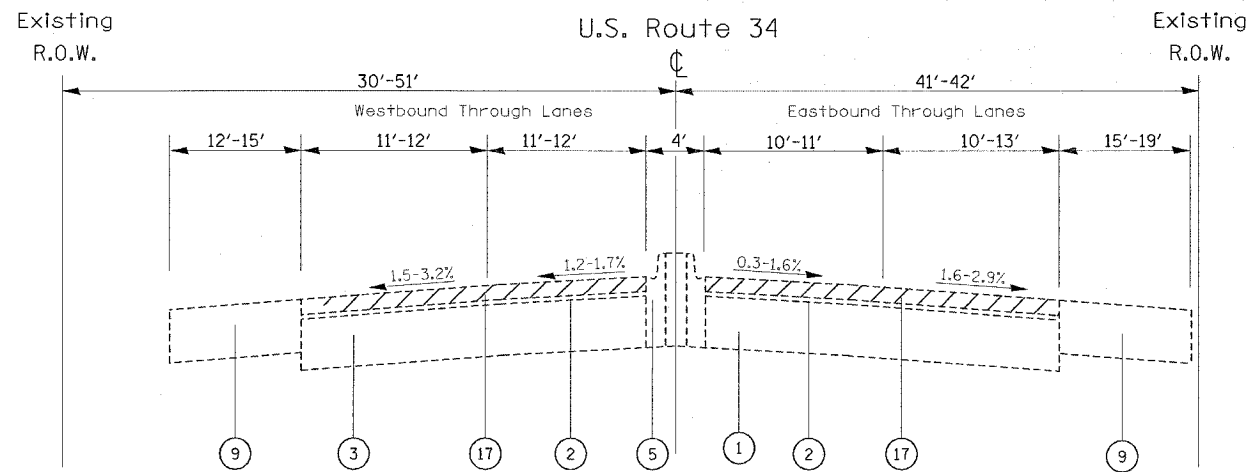
SAW CUTTING LOCATIONS SHOWN ON PROPOSED ROADWAY PLAN

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		US 34 TYPICAL SECTIONS
SCALE:	VERT. HORIZ.	DRAWN BY
DATE		CHECKED BY



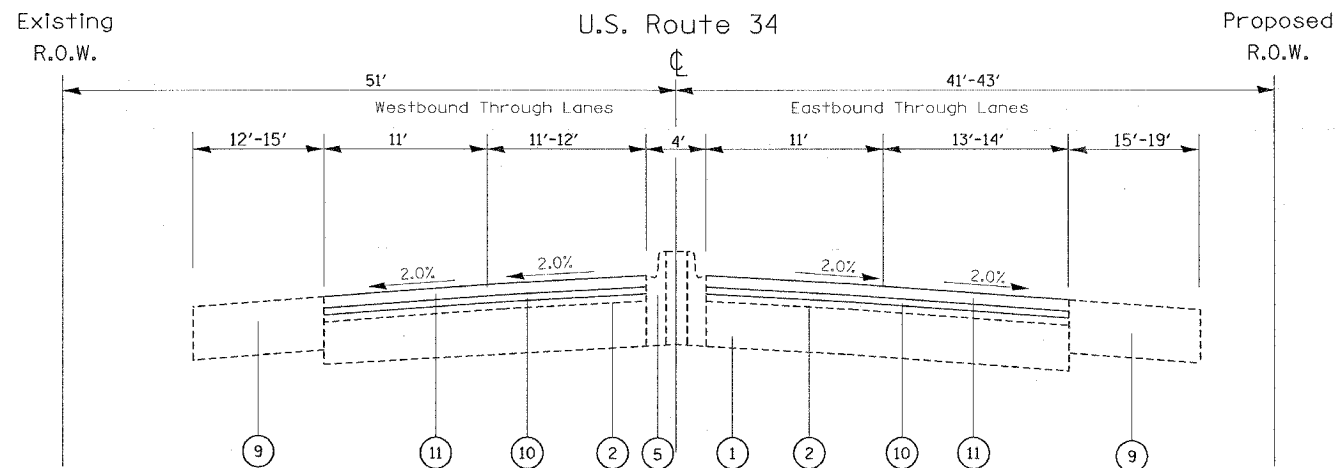


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	9
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



Typical Existing Cross Section

U.S. Route 34  
Sta. 104+19 - 106+00



Typical Proposed Cross Section

U.S. Route 34  
Sta. 104+19 - 106+00

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ± 9"
- ② EXISTING HMA OVERLAY, ± 3"
- ③ EXISTING CURB AND GUTTER
- ④ EXISTING P.C.C. SIDEWALK
- ⑤ EXISTING P.C.C. MEDIAN
- ⑥ EXISTING CRUSHED STONE
- ⑦ EXISTING PC CONCRETE CURB
- ⑧ EXISTING CONCRETE MEDIAN SURFACE
- ⑨ EXISTING HMA SHOULDER
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
- ⑫ PROPOSED P.C.C. BASE COURSE, 9 1/2"
- ⑬ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 6"
- ⑭ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- ⑮ PROPOSED CURB AND GUTTER, TYPE B-6.24
- ⑮A PROPOSED CURB AND GUTTER, TYPE M-4.24
- ⑯ PROPOSED P.C.C. SIDEWALK
- ⑰ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑱ PROPOSED HMA SURFACE REMOVAL
- ⑲ PROPOSED 8" HMA SHOULDER
- ⑳ PROP. DRILL AND GROUT #4 TIE BAR, 24" LONG, SHAPE (24" C-C), COST INCLUDED IN CONCRETE CURB AND GUTTER, TYPE B-6.12 PAY ITEM
- ㉑ PROPOSED MEDIAN REMOVAL

NOTES

- \* STA. 95+11 TO STA. 96+67  
STA. 97+17 TO STA. 98+22  
STA. 98+38 TO STA. 99+56
- \*\* CURB AND GUTTER REMOVAL AND REPLACEMENT  
LOCATIONS SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN
- \*\*\* HMA SHOULDER REMOVAL AND REPLACEMENT  
SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN

SAW CUTTING LOCATIONS SHOWN ON PROPOSED ROADWAY PLAN

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		US 34 TYPICAL SECTIONS

SCALE: VERT. DATE      HORIZ. DATE      DRAWN BY      CHECKED BY

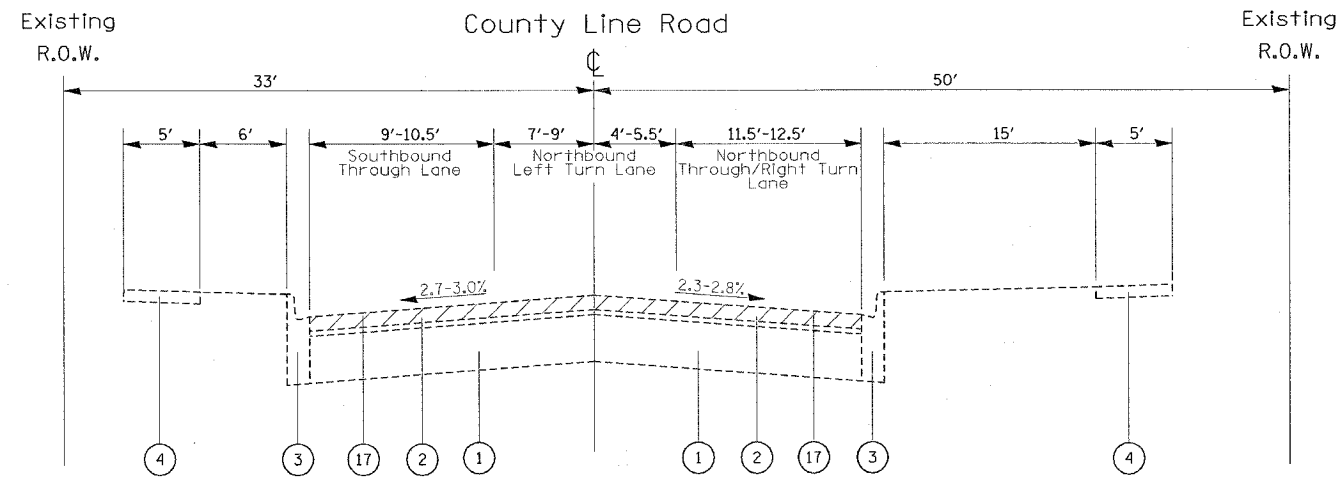
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	10
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

**LEGEND**

- ① EXISTING P.C.C. PAVEMENT, ± 9"
- ② EXISTING HMA OVERLAY, ± 3"
- ③ EXISTING CURB AND GUTTER
- ④ EXISTING P.C.C. SIDEWALK
- ⑤ EXISTING P.C.C. MEDIAN
- ⑥ EXISTING CRUSHED STONE
- ⑦ EXISTING PC CONCRETE CURB
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- ㉑ PROPOSED MEDIAN REMOVAL

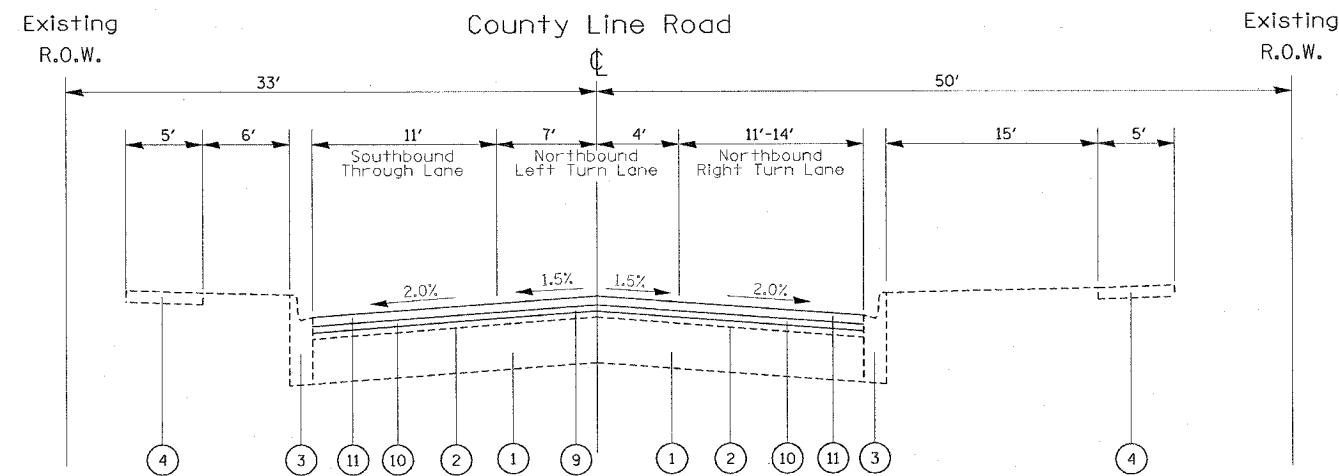
**NOTES**

- \* STA. 95+11 TO STA. 96+67  
STA. 97+17 TO STA. 98+22  
STA. 98+38 TO STA. 99+56
  - \*\* CURB AND GUTTER REMOVAL AND REPLACEMENT  
LOCATIONS SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN
  - \*\*\* HMA SHOULDER REMOVAL AND REPLACEMENT  
SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN
- SAW CUTTING LOCATIONS SHOWN ON PROPOSED ROADWAY PLAN



**Typical Existing Cross Section**

**County Line Road  
Sta. 18+00 - 19+00**



**Typical Proposed Cross Section**

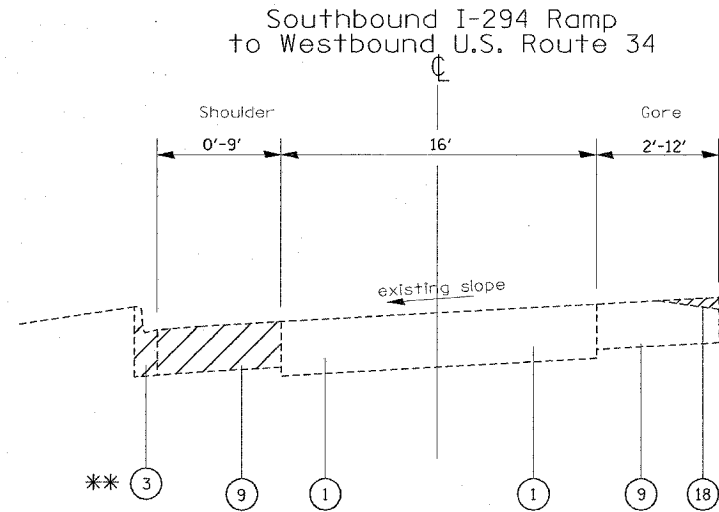
**County Line Road  
Sta. 18+00 - 19+00**

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		COUNTY LINE ROAD TYPICAL SECTIONS

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_

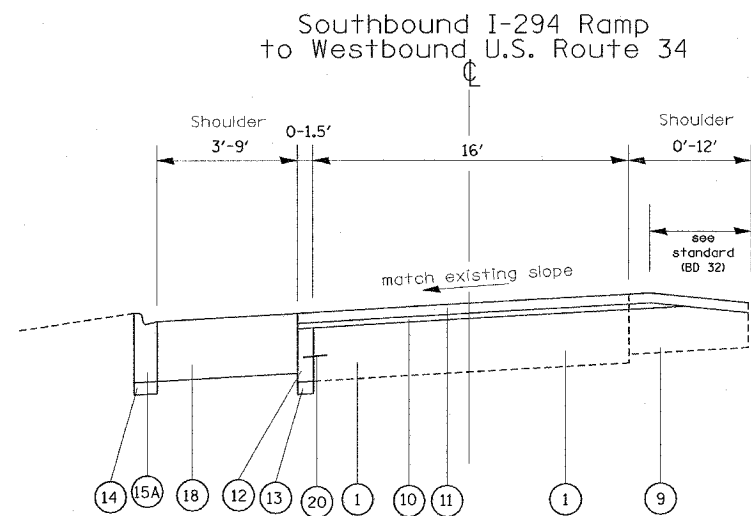
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	11
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



Typical Existing Cross Section

I-294 Ramp  
Sta. 102+63 - 103+75



Typical Proposed Cross Section

I-294 Ramp  
Sta. 102+63 - 103+75

LEGEND

- ① EXISTING P.C.C. PAVEMENT, ± 9"
- ② EXISTING HMA OVERLAY, ± 3"
- ③ EXISTING CURB AND GUTTER
- ④ EXISTING P.C.C. SIDEWALK
- ⑤ EXISTING P.C.C. MEDIAN
- ⑥ EXISTING CRUSHED STONE
- ⑦ EXISTING PC CONCRETE CURB
- ⑧ EXISTING CONCRETE MEDIAN SURFACE
- ⑨ EXISTING HMA SHOULDER
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- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
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- ⑬ PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 6"
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- ⑰ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
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- ㉑ PROPOSED MEDIAN REMOVAL

NOTES

- \* STA. 95+11 TO STA. 96+67  
STA. 97+17 TO STA. 98+22  
STA. 98+38 TO STA. 99+56
- \*\* CURB AND GUTTER REMOVAL AND REPLACEMENT  
LOCATIONS SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN
- \*\*\* HMA SHOULDER REMOVAL AND REPLACEMENT  
SHOWN ON EXISTING AND PROPOSED ROADWAY PLAN

SAW CUTTING LOCATIONS SHOWN ON PROPOSED ROADWAY PLAN

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		US 34 TYPICAL SECTIONS
SCALE:	VERT. DATE	DRAWN BY CHECKED BY

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	12
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

SCHEDULE OF QUANTITIES (EARTHWORK)						
1	2	3	4	5	6	7
US 34 AT COUNTY LINE ROAD	EARTH EXCAVATION (CU YD)	UNSUITABLE MATERIAL (CU YD)	EMBANKMENT (CU YD)	ADJUSTMENT FOR SHRINKAGE (CU YD)	FURNISHED EXCAVATION (CU YD)	TOP SOIL FURNISH AND PLACE (SQ YD)
US 34 - STA. 94+00 TO 105+00	464	273	9	394	385	751
US 34 AT SPINNING WHEEL ROAD	2	2	0	2	2	50
US 34 @ COUNTY LINE ROAD	1	7	0	1	1	119
TOTAL	467	282	9	397	388	920
COLUMN 1: LOCATION FROM PLANS		COLUMN 2: CUT QUANTITIES AFTER UNSUITABLE MATERIAL IS REMOVED		COLUMN 5: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR WAS DETERMINED TO BE 15%		
COLUMN 3: MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT (TOP SOIL EXCAVATED AT 6" (150 MM) AVERAGE DEPTH)		COLUMN 4: FILL QUANTITIES AFTER UNSUITABLE MATERIAL IS REMOVED			COLUMN 6: COLUMN 5 - COLUMN 4, POSITIVE QUANTITY= EXTRA EXCAVATION, NEGATIVE QUANTITY= FURNISHED EXCAVATION NEEDED	
				COLUMN 7: TOPSOIL FURNISH AND PLACE= AREA OF SODDING		

NOTE: THE TOP 6" OF TOPSOIL IS TO BE REMOVED AND PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.

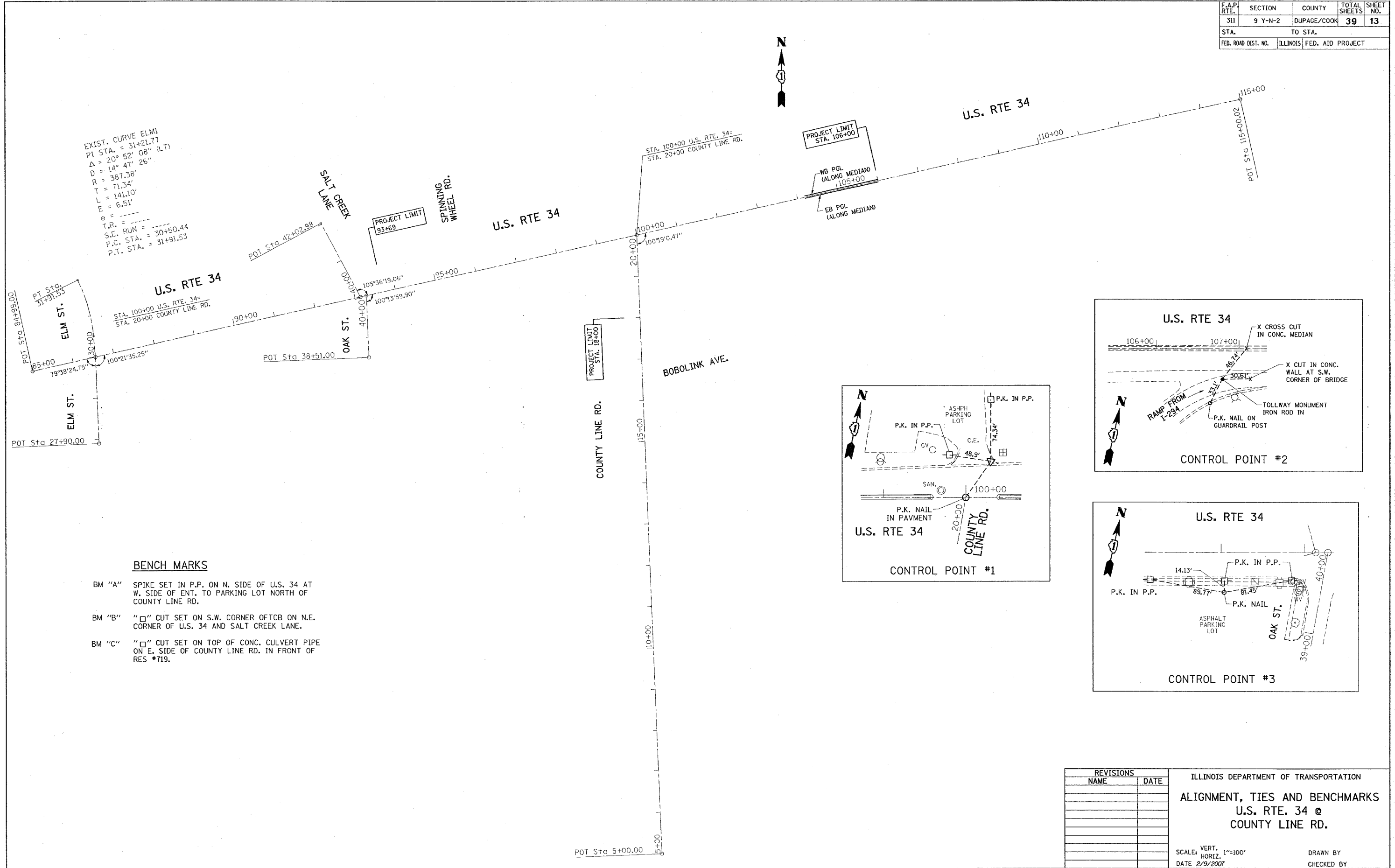
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

	MIXTURE USE	AC TYPE	AIR VOIDS (%)
ROADWAY	POLYMERIZED LEVELING BINDER, (MACHINE METHOD), IL-4.75, N50, 3/4"	SBS/SBR PG 76-28/-22	4% @ 50 GYR
	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 MM), 1 3/4"	SBS/SBR PG 70-22	4% @ 90 GYR
DRIVEWAY	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5 MM), 2"	PG 64-22	4% @ 50 GYR
	HOT-MIX ASPHALT BASE COURSE, (HMA BINDER IL-19 MM), 8"	PG 64-22/58-22	4% @ 50 GYR
SHOULDER	HOT-MIX ASPHALT SHOULDERS, 8"	PG 64-22/58-22	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.  
WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

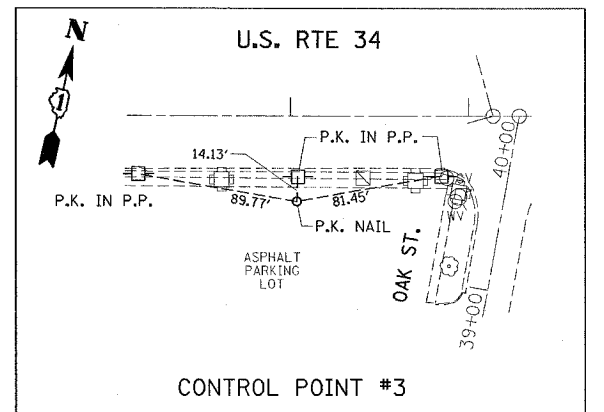
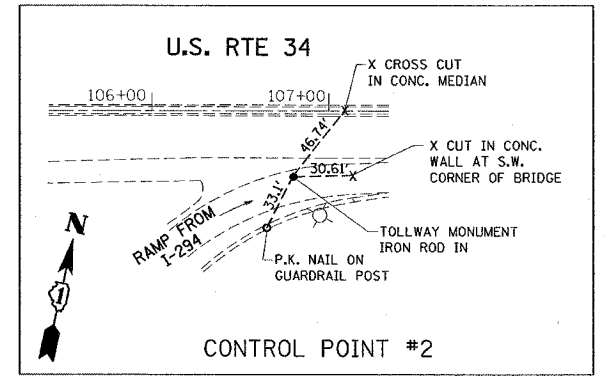
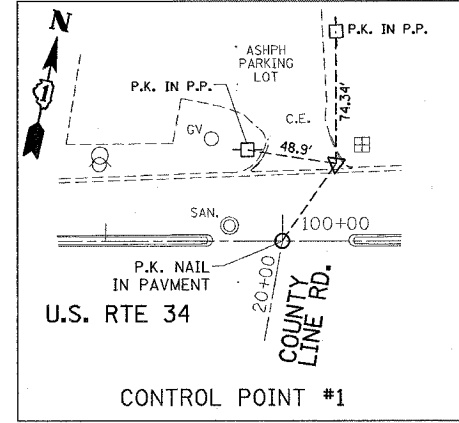
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION  <b>US 34 AT COUNTY LINE ROAD SCHEDULE OF QUANTITIES</b>  SCALE: VERT. _____ HORIZ. _____ DATE _____	DRAWN BY _____ CHECKED BY _____
NAME	DATE		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	DUPAGE/COOK	39	13
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**BENCH MARKS**

- BM "A" SPIKE SET IN P.P. ON N. SIDE OF U.S. 34 AT W. SIDE OF ENT. TO PARKING LOT NORTH OF COUNTY LINE RD.
- BM "B" "□" CUT SET ON S.W. CORNER OF TCB ON N.E. CORNER OF U.S. 34 AND SALT CREEK LANE.
- BM "C" "□" CUT SET ON TOP OF CONC. CULVERT PIPE ON E. SIDE OF COUNTY LINE RD. IN FRONT OF RES #719.



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 USER NAME = beaver-d1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ALIGNMENT, TIES AND BENCHMARKS**  
**U.S. RTE. 34 @**  
**COUNTY LINE RD.**

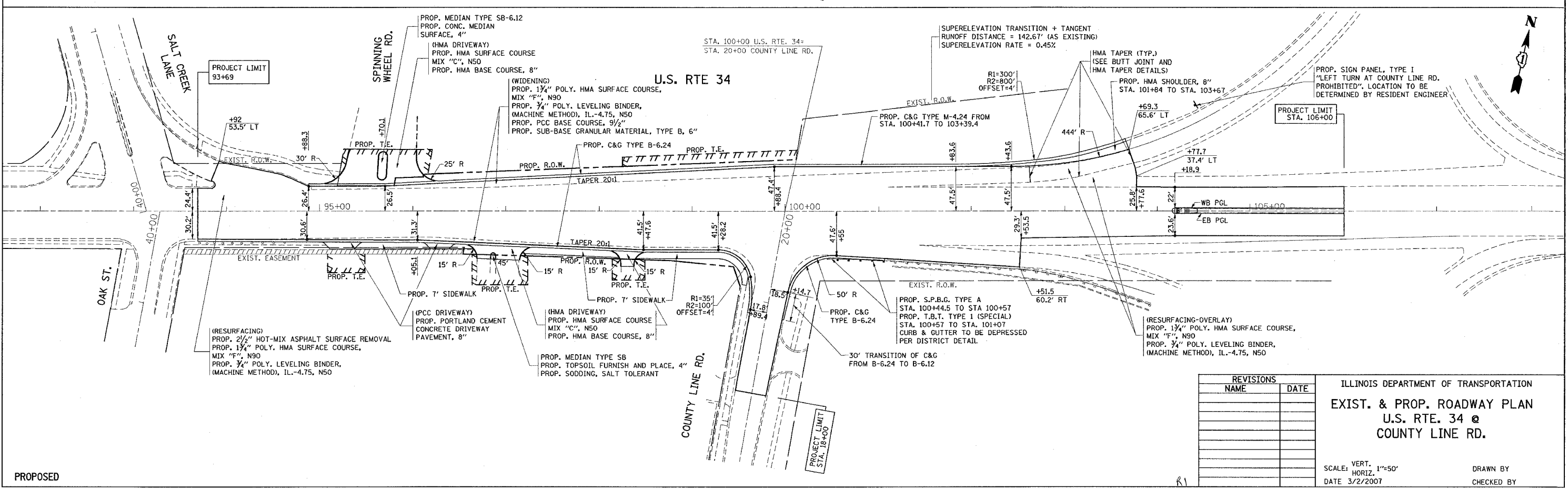
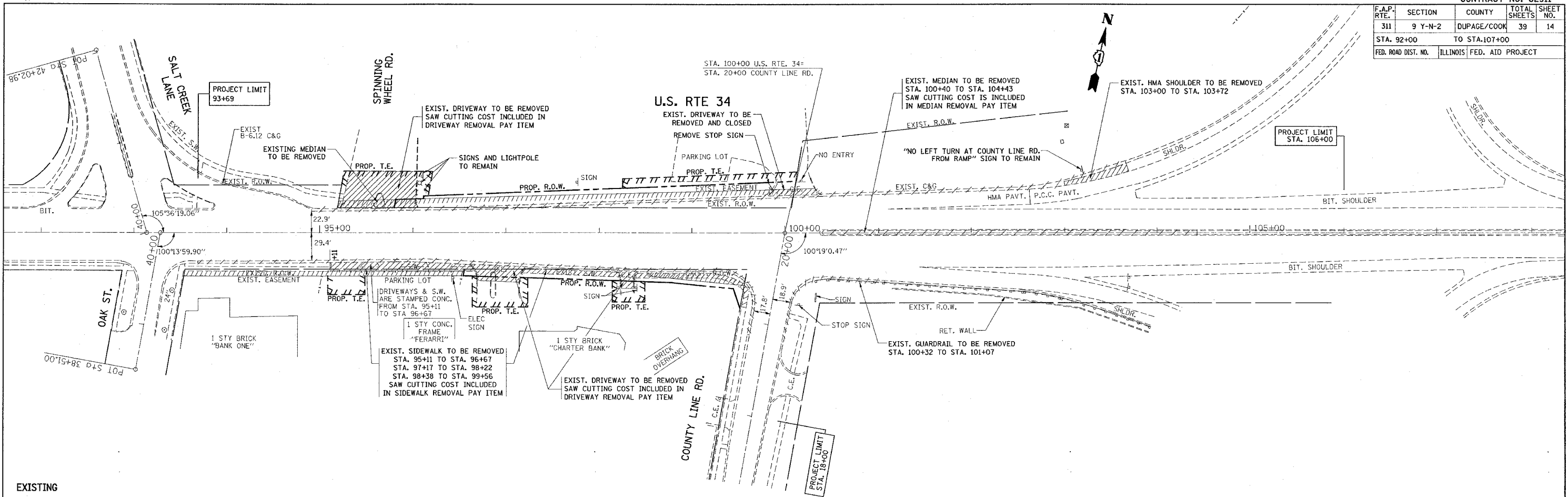
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 HORIZ. 1"=100'  
 DATE 2/9/2007

DRAWN BY  
 CHECKED BY

\*REF-topo01  
\*REF-pp01

CONTRACT NO. 62911

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	DUPAGE/COOK	39	14
STA. 92+00		TO STA. 107+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



PLOT DATE = 3/2/2007  
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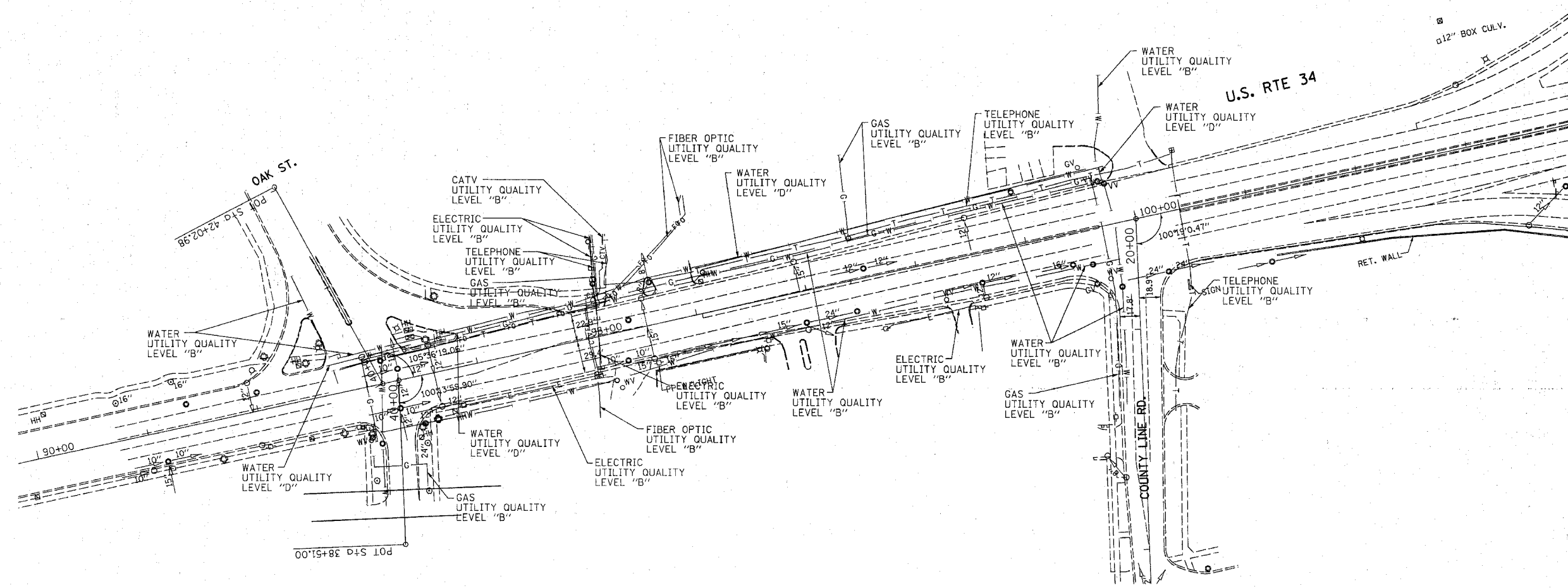
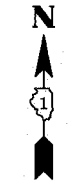
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXIST. & PROP. ROADWAY PLAN**  
**U.S. RTE. 34 @**  
**COUNTY LINE RD.**

SCALE: VERT. 1"=50'  
 DATE 3/2/2007

DRAWN BY  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	15
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



Water lines on North side of US. RTE 34 cross each other at approx. Station 94+90 approximately 33.4' North of centerline. TBE was unable to determine the horizontal location of the Quality Level "D" water main as indicated on plan sheet due to the utility being made out of a non-conductive material.

The SBC locations depicted have been obtained through the application of geophysical methods to determine the existence and approximate horizontal position of these facilities. However, SBC will not provide TBE Group, Inc. with utility records nor allow access to their field closures (pedestals/manholes etc.), to help verify the locations of their existing underground facilities. Therefore, TBE is unable to verify the completeness of the SBC locations depicted in accordance with the CI/ASCE Standard 38-02.

Utilities shown on these plans as depicted in the legend have been investigated by TBE Group, Inc in accordance with SUE Industry Standards. All other information shown has been provided to TBE Group, Inc by others. TBE's SUE field investigation was performed 11/22/06 through 1/26/07. Changes to utilities after these dates may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.




**TBE GROUP, INC.**  
 CIVIL ENGINEERING \* TRANSPORTATION \* ENVIRONMENTAL  
 \* PLANNING \* UTILITY ENGINEERING/LOCATING

IL09500266  
 TBE SUE PAGE NO: 1 of 1  
 Checked by: *[Signature]*

Utility Quality Level "B" : Designating

— T — T —	TELEPHONE
— W — W —	WATER
— G — G —	GAS
— CTV — CTV —	CABLE TELEVISION
— FO — FO —	FIBER OPTIC
— E — E —	ELECTRIC

  
 205 W. WACKER DRIVE  
 SUITE 1020  
 CHICAGO, IL 60606  
 (312) 704-1970

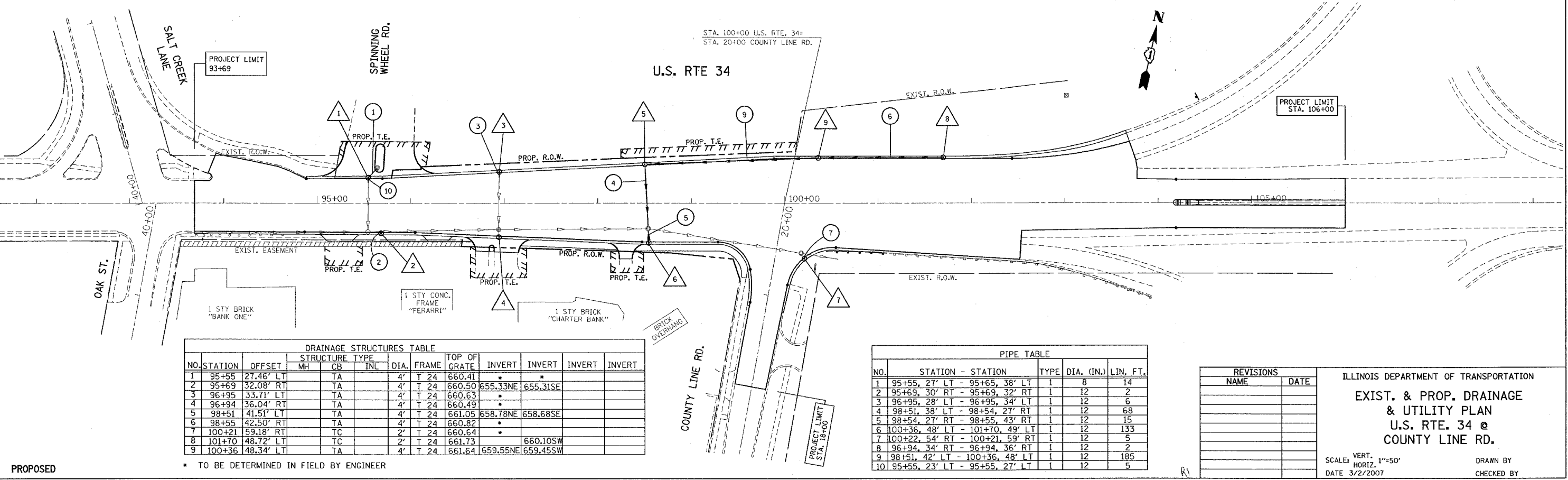
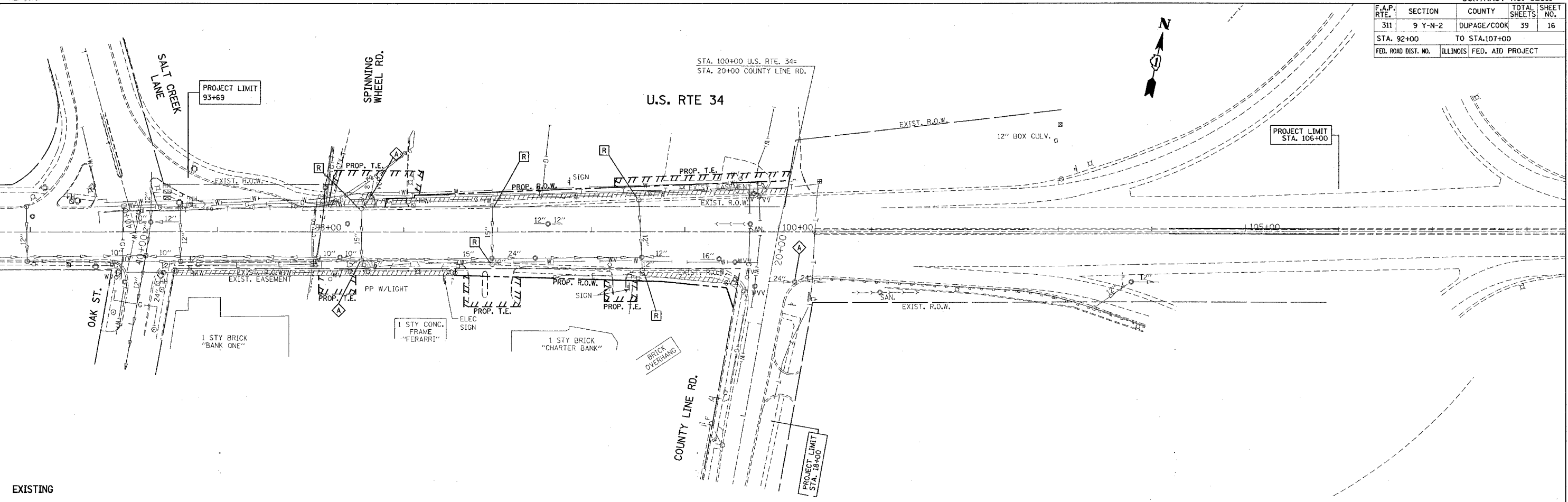
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 SUE Investigation of Underground Utilities  
 US RT. 34 at County Line Road  
 Section No. 9-YN2  
 Contract No. 62911  
 DuPage County  
 DRAWN BY : KLC  
 SCALE : 1" = 50'

\*REF-edr01  
\*REF-pdr01

CONTRACT NO. 62911

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	DUPAGE/COOK	39	16
STA. 92+00		TO STA. 107+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



NO.	STATION	OFFSET	STRUCTURE TYPE			DIA.	FRAME	TOP OF GRATE	INVERT	INVERT	INVERT	INVERT
			MH	CB	INL							
1	95+55	27.46' LT		TA		4'	T 24	660.41				
2	95+69	32.08' RT		TA		4'	T 24	660.50	655.33NE	655.31SE		
3	96+95	33.71' LT		TA		4'	T 24	660.63				
4	96+94	36.04' RT		TA		4'	T 24	660.49				
5	98+51	41.51' LT		TA		4'	T 24	661.05	658.78NE	658.68SE		
6	98+55	42.50' RT		TA		4'	T 24	660.82				
7	100+21	59.18' RT		TC		2'	T 24	660.64				
8	101+70	48.72' LT		TC		2'	T 24	661.73		660.10SW		
9	100+36	148.34' LT		TA		4'	T 24	661.64	659.55NE	659.45SW		

NO.	STATION - STATION	TYPE	DIA. (IN.)	LIN. FT.
2	95+69, 30' RT - 95+69, 32' RT	1	12	2
3	96+95, 28' LT - 96+95, 34' LT	1	12	6
4	98+51, 38' LT - 98+54, 27' RT	1	12	68
5	98+54, 27' RT - 98+55, 43' RT	1	12	15
6	100+36, 48' LT - 101+70, 49' LT	1	12	133
7	100+22, 54' RT - 100+21, 59' RT	1	12	5
8	96+94, 34' RT - 96+94, 36' RT	1	12	2
9	98+51, 42' LT - 100+36, 48' LT	1	12	185
10	95+55, 23' LT - 95+55, 27' LT	1	12	5

NAME	DATE

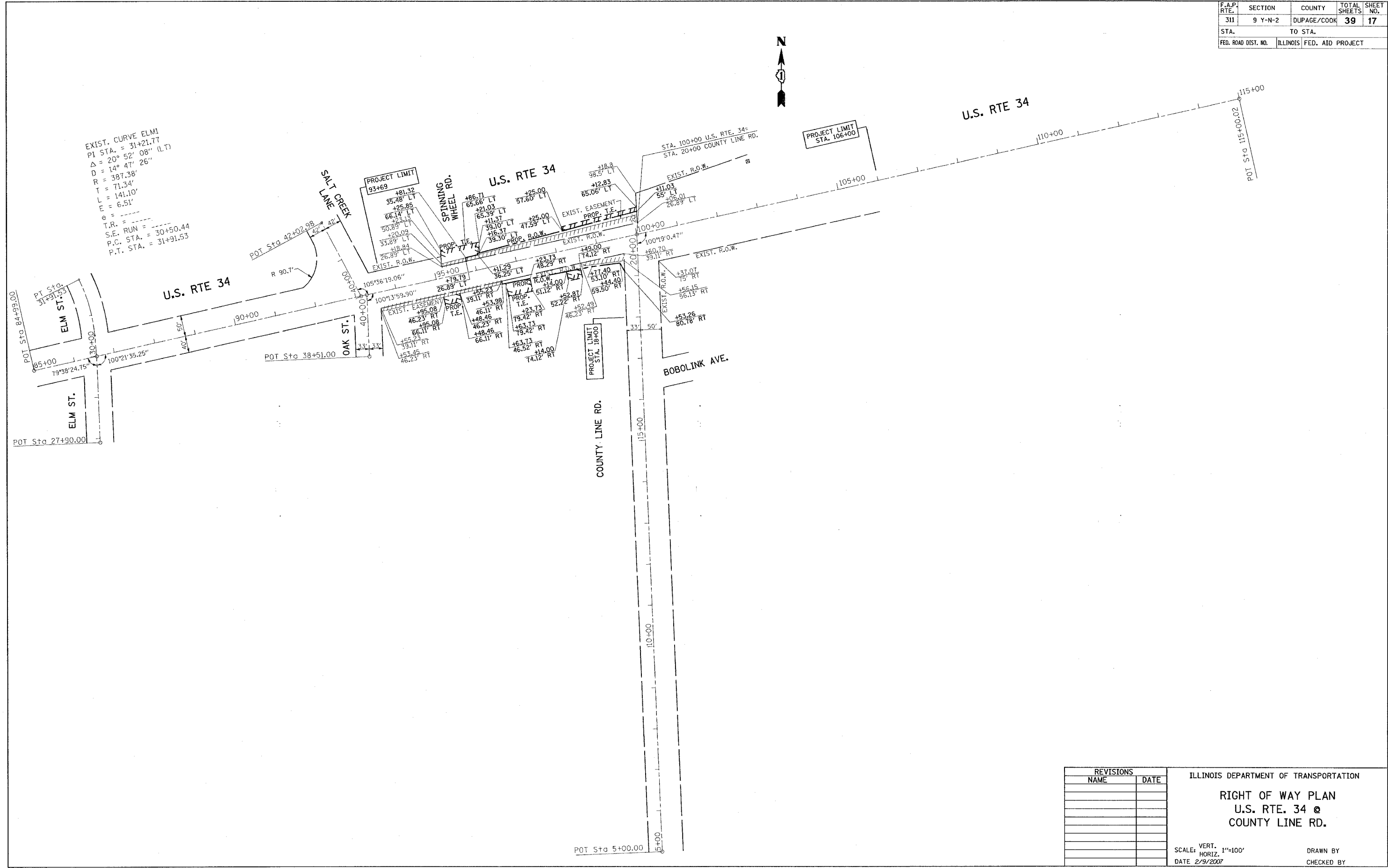
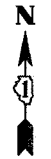
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXIST. & PROP. DRAINAGE & UTILITY PLAN**  
**U.S. RTE. 34 @**  
**COUNTY LINE RD.**  
 SCALE: VERT. 1"=50'  
 DATE 3/2/2007  
 DRAWN BY  
 CHECKED BY

PLOT DATE = 3/2/2007  
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 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = drvwkng

• TO BE DETERMINED IN FIELD BY ENGINEER



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	DUPAGE/COOK	39	17
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



EXIST. CURVE ELMI  
 PI STA. = 31+21.77  
 $\Delta = 20^\circ 52' 08''$  (LT)  
 $D = 14^\circ 47' 26''$   
 $R = 387.38'$   
 $T = 71.34'$   
 $L = 141.10'$   
 $E = 6.51'$   
 $e =$   
 $T.R. =$   
 $S.E. RUN = 30+50.44$   
 $P.C. STA. = 31+91.53$   
 $P.T. STA. = 31+91.53$

PLOT DATE = 2/9/2007  
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 PLOT SCALE = 1/8"=100'  
 USER NAME = bevard

REVISIONS	
NAME	DATE

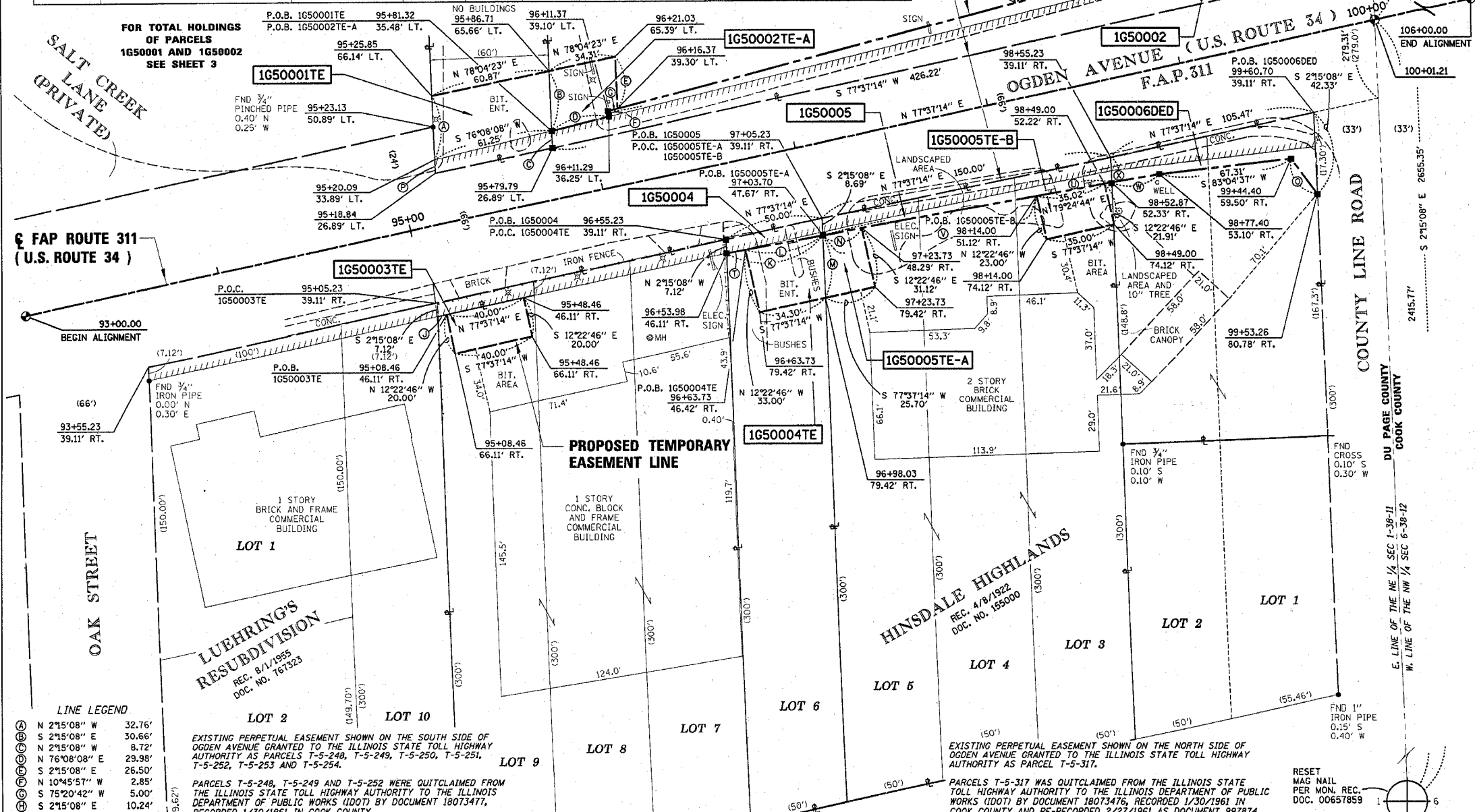
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**RIGHT OF WAY PLAN**  
**U.S. RTE. 34 @**  
**COUNTY LINE RD.**  
 SCALE: VERT. 1"=100'  
 HORIZ. 1"=100'  
 DATE 2/9/2007  
 DRAWN BY  
 CHECKED BY

PART OF NE 1/4 OF SECTION 1, T38N, R11E, OF THE 3RD PM, DU PAGE, ILLINOIS

CONTRACT NO. 62911

F.A.P. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	18
STA. 93+00 TO STA. 100+13				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PARCEL NO.	OWNER	PERMANENT INDEX NUMBER	TOTAL HOLDING	AREA TAKING	PREVIOUSLY DEDICATED	REMAINDER	EASEMENTS TEMP	PERM	EASEMENT PURPOSE	PROPERTY ACQUIRED BY
1G50001TE	21 SPINNING WHEEL LLC	09-01-208-001	0.854 AC 37,190 SF			0.854 AC 37,190 SF	0.044 AC 1,903 SF		CONSTRUCTION	
1G50002				0.190 AC 8,260 SF	0.138 AC 6,026 SF	5.683 AC 247,577 SF	0.023 AC 1,003 SF		CONSTRUCTION	
1G50002TE-A	HINSDALE HOSPITAL	09-01-208-002	5.873 AC 255,837 SF				0.043 AC 1,871 SF		CONSTRUCTION	
1G50002TE-B							0.018 AC 800 SF		CONSTRUCTION	
1G50003TE	HINSDALE PROPERTIES, LLC	09-10-212-004 (PT)	1.009 AC 43,939 SF			1.009 AC 43,939 SF			CONSTRUCTION	



**LEGEND**

- EXISTING CENTERLINE
- EXISTING RIGHT OF WAY LINE
- EXISTING PERPETUAL EASEMENT LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED TEMPORARY EASEMENT LINE
- PROPOSED PERMANENT EASEMENT LINE
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- ACCESS CONTROL LINE
- MEASURED OR COMPUTED DIMENSION
- RECORD DATA
- FOUND IRON PIPE OR IRON ROD
- SET 3/8 INCH IRON ROD
- PERMANENT SURVEY MONUMENT, I.O.T. STD. 2135 (TO BE SET BY OTHERS)
- CUT CROSS FOUND OR SET
- SAME OWNERSHIP
- FENCE LINE
- SIGN

BEARINGS SHOWN HEREON REFERENCE THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983 (1986 ADJUSTMENT). ALL MEASURED AND CALCULATED VALUES ARE "GROUND."

COMBINED SCALE FACTOR:  
(GROUND TO GRID) = 0.99996669755625  
(GRID TO GROUND) = 1.00003330244375

- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 3/8 INCH IRON ROD FLUSH WITH GROUND. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT2 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 3/8 INCH IRON ROD 20 INCHES BELOW GROUND SURFACE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 3/8 INCH IRON ROD 20 INCHES BELOW GROUND SURFACE TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP AND SURVEYORS REGISTRATION NUMBER.
- U STAKING OF PROPOSED RIGHT OF WAY IN CLOSE PROXIMITY TO UNDERGROUND UTILITIES. SET DIVISION OF HIGHWAYS SURVEY MARKER ON 3/8 INCH IRON ROD (BETWEEN 6 INCHES AND 12 INCHES IN LENGTH) TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STATE OF ILLINOIS )  
COUNTY OF COOK ) S.S.

THIS IS TO CERTIFY THAT I, COVENTINE FIDIS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 1, TOWNSHIP 38 NORTH, RANGE 11 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN DU PAGE COUNTY, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT CHICAGO, ILLINOIS THIS 28TH DAY OF SEPTEMBER, 2006.

COVENTINE FIDIS  
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2159  
LICENSE EXPIRATION DATE 11/30/2006

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM SURVEY STANDARDS.

RECEIVED  
OCT 3 2006  
PLATS & LEGALS

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311  
OGDEN AVENUE (U.S. ROUTE 34)

SECTION: AT COUNTY LINE ROAD DU PAGE COUNTY  
STATION 93+00 JOB NO. R-91-009-05  
SCALE 1" = 30' TO STATION 100+13  
SHEET 2

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

PARCEL NO.	OWNER	PERMANENT INDEX NUMBER	TOTAL HOLDING	AREA TAKING	PREVIOUSLY DEDICATED	REMAINDER	EASEMENTS TEMP	PERM	EASEMENT PURPOSE	PROPERTY ACQUIRED BY
1G50004	LYONS SAVINGS AND LOAN ASSOCIATION	09-01-212-006	0.339 AC 14,763 SF	0.009 AC 389 SF	0.008 AC 356 SF	0.330 AC 14,374 SF	0.028 AC 1,204 SF		CONSTRUCTION	
1G50005				0.037 AC 1,614 SF	0.024 AC 1,045 SF	0.980 AC 42,675 SF	0.017 AC 720 SF		CONSTRUCTION	
1G50005TE-A	SUPERIOR BANK, F.S.B.	09-01-213-001 09-01-213-002 09-01-213-003	1.017 AC 44,289 SF				0.018 AC 786 SF		CONSTRUCTION	
1G50005TE-B									CONSTRUCTION	
1G50006DED	LYONS SAVINGS AND LOAN ASSOCIATION	09-01-213-005	0.377 AC 16,411 SF	0.044 AC 1,919 SF	0.030 AC 1,307 SF	0.377 AC 16,411 SF				

**LINE LEGEND**

- Ⓐ N 2°15'08" W 32.76'
- Ⓑ S 2°15'08" E 30.66'
- Ⓒ N 2°15'08" W 8.72'
- Ⓓ S 76°08'08" E 29.98'
- Ⓔ S 2°15'08" E 26.50'
- Ⓕ N 10°45'57" W 2.85'
- Ⓖ S 75°20'42" W 5.00'
- Ⓗ S 2°15'08" E 10.24'
- Ⓘ S 2°15'08" E 28.54'
- Ⓚ N 77°37'14" E 4.48'
- Ⓛ S 79°24'44" W 49.75'
- Ⓜ N 79°24'44" E 39.99'
- Ⓝ S 2°15'08" E 32.25'
- Ⓟ N 79°24'44" E 20.04'
- Ⓠ N 34°58'38" W 23.05'
- Ⓡ N 2°15'08" W 7.12'
- Ⓢ S 2°15'08" E 173.49'
- Ⓣ S 2°15'08" E 183.73'
- Ⓤ S 2°15'08" E 212.27'
- Ⓡ N 79°24'44" E 9.75'
- Ⓢ S 79°24'44" W 149.24'
- Ⓣ N 79°24'44" E 110.35'
- Ⓤ S 79°24'44" W 24.54'
- Ⓡ N 2°15'08" W 13.43'

CALCULATIONS BY: J. DYBAS DRAFTING BY: M. BARAN AND D. DEMKOVICH CHECKED BY: D. DEMKOVICH

ASC American Surveying Consultants  
841 N. Galena Avenue / Dixon, IL 61021 / 815-288-6231  
8604 W. Catolpa Avenue / Chicago, IL 60656 / 773-444-0800  
Illinois Professional Design Firm No. 184-003192

REVISION DATE: 09/28/06

ASC JOB NO. 205053-5

SCANNED

**PART OF NE 1/4 OF SECTION 1, T38N, R11E, OF THE 3RD PM, DU PAGE, ILLINOIS**

CONTRACT NO. 62911

374+17.62 / 105.05' RT. 16095	374+36.62 / 98.05' RT. 16096	375+32.41 / 101.66' RT. 16098	375+42.62 / 102.05' RT. 16097	376+90.31 / 98.95' LT. 23326	377+15.00 / 99.95' LT. 23327	377+47.38 / 96.86' RT. 16108
TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION
T1 51.32' 964 (DRIVEWAY)	T1 67.89' 964 (DRIVEWAY)	T1 38.26' 959 (SHED)	T1 10.60' 959 (SHED)	T1 33.49' 951 (S. FACE)	T1 31.70' 950	T1 23.45' 951
T2 44.88' 956	T2 50.08' 956	T2 29.90' 956 (SHED)	T2 23.72' 956 (SHED)	T2 15.79' 950	T2 12.84' 951	T2 109.85' 951
T3 28.77' 951	T3 8.63' 951	T3 51.23' 951 (W. FACE 24')	T3 41.01' 951 (W. FACE 24')	T3 15.45' 951 (S. FACE)	T3 31.43' 962	T3 14.48' 951
378+80.00 / 99.95' LT. 23323	379+07.77 / 114.95' LT. 16120	379+67.78 / 109.95' LT. 16122	379+90.00 / 95.95' LT. 23328	380+16.03 / 90.05' RT. 15166	381+85.33 / 89.95' LT. 15201	383+33.40 / 83.05' RT. 16117
TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION	TIE DISTANCE DESCRIPTION
T1 27.23' 972	T1 48.91' 972	T1 4.83' 951	T1 26.08' 951	T1 50.30' 951 (N. FACE 8')	T1 19.88' FND IP	T1 19.23' 951
T2 38.62' 958	T2 49.20' 958	T2 7.50' 962	T2 24.75' 962	T2 26.85' 951 (N. FACE 8')	T2 24.72' 962	T2 2.92' 951
T3 15.73' 951	T3 28.37' 951	T3 18.92' 951	T3 11.81' 951	T3 28.30' 951 (W. FACE 12')	T3 97.03' 959	T3 7.30' 951

F.A.P. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	19
STA. 93+00 TO STA. 100+13				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**LEGEND**

EXISTING CENTERLINE  
EXISTING RIGHT OF WAY LINE  
EXISTING PERPETUAL EASEMENT LINE  
PROPOSED CENTERLINE  
PROPOSED RIGHT OF WAY LINE  
PROPOSED TEMPORARY EASEMENT LINE  
PROPOSED PERMANENT EASEMENT LINE  
SECTION LINE  
QUARTER SECTION LINE  
QUARTER QUARTER SECTION LINE  
PLATTED LOT LINE  
PROPERTY (DEED) LINE  
APPARENT PROPERTY LINE  
ACCESS CONTROL LINE  
MEASURED OR COMPUTED DIMENSION RECORD DATA  
FOUND IRON PIPE OR IRON ROD SET 3/8 INCH IRON ROD PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 2135 (TO BE SET BY OTHERS)  
CUT CROSS FOUND OR SET  
SAME OWNERSHIP  
FENCE LINE  
SIGN

BEARINGS SHOWN HEREON REFERENCE THE ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NORTH AMERICAN DATUM OF 1983 (1986 ADJUSTMENT). ALL MEASURED AND CALCULATED VALUES ARE "GROUND."  
COMBINED SCALE FACTOR: (GROUND TO GRID) = 0.99996669755657 (GRID TO GROUND) = 1.000033331356285

**DESCRIPTION LEGEND**

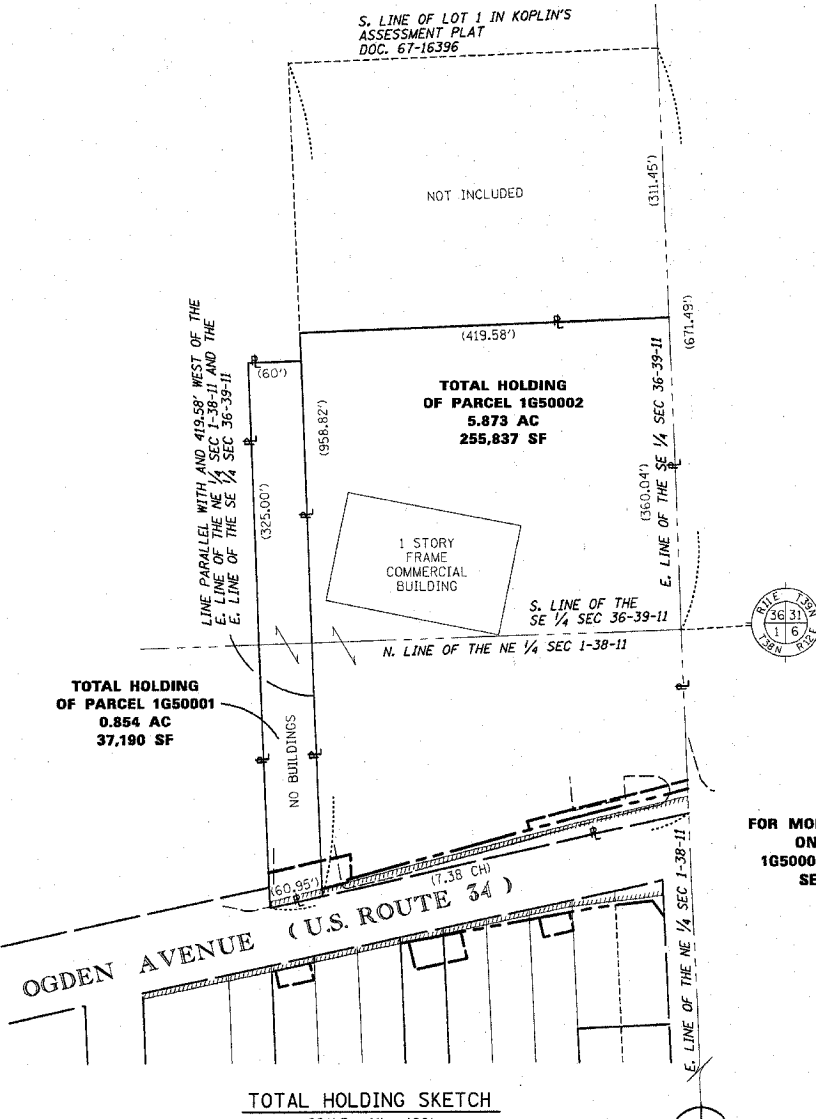
950 - MAG NAIL IN UTILITY POLE	960 - IRON ROD WITH YELLOW PLASTIC CAP	970 - "4" ON LEG OF ELECTRIC TRANSMISSION TOWER
951 - MAG NAIL IN TREE	961 - "4" ON METAL FENCE POST	971 - CORNER OF LARGE SIGN
952 - "4" IN POLE FOUNDATION, BASE, OR BOLT	962 - MAG NAIL IN FENCE POST	972 - FENCE CORNER
953 - CENTER TOP SPINDLE BOLT OF FIRE HYDRANT	963 - "4" ON MANHOLE, CATCH BASIN, OR VAULT RIM	973 - MAG NAIL ON WOOD GUARDRAIL POST
954 - "4" ON CURB	964 - MAG NAIL IN PAVEMENT	974 - "4" ON CONC. SIGN FOUNDATION
955 - "4" ON WALK / CONCRETE PAD	965 - "4" ON CONCRETE RETAINING WALL	975 - MAG NAIL ON STUMP
956 - NW CORNER OF BUILDING	966 - "4" ON CONCRETE HEADWALL	976 - "4" ON BUILDING FACE
957 - NE CORNER OF BUILDING	967 - MAG NAIL IN LIGHT POLE	977 - "4" ON RAILROAD SPIKE
958 - SE CORNER OF BUILDING	968 - MAG NAIL IN WOOD SIGN POST	978 - CORNER OF PLANTER
959 - SW CORNER OF BUILDING	969 - MAG NAIL IN RAILROAD TIE	979 - CORNER OF COLUMN

- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 3/8 INCH IRON ROD FLUSH WITH GROUND. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2
- T3
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 3/8 INCH IRON ROD 20 INCHES BELOW GROUND SURFACE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT2
- BT3
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 3/8 INCH IRON ROD 20 INCHES BELOW GROUND SURFACE TO MARK FUTURE SURVEY MARKER POSITION. IDENTIFIED BY COLORED PLASTIC CAP AND SURVEYORS REGISTRATION NUMBER.
- U STAKING OF PROPOSED RIGHT OF WAY IN CLOSE PROXIMITY TO UNDERGROUND UTILITIES. SET DIVISION OF HIGHWAYS SURVEY MARKER ON 3/8 INCH IRON ROD (BETWEEN 6 INCHES AND 12 INCHES IN LENGTH) TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

**COORDINATE TABLE**

GROUND COORDINATES  
TO CONVERT TO STATE PLANE (GRID),  
MULTIPLY BY COMBINED SCALE FACTOR: 0.99996669755657

OGDEN AVENUE				SECTION CORNERS	
STATION	OFFSET	NORTH	EAST	POINT	NORTH EAST
93+00.00	0.00	438	1876406.687	1096749.359	
95+08.46	66.11	509	1876386.804	1096967.146	
95+08.46	46.11	508	1876406.339	1096962.859	
95+20.09	-33.89	497	1876486.972	1096957.065	
95+25.85	-66.14	498	1876519.706	1096955.778	
95+48.46	66.11	510	1876395.380	1097006.216	
95+48.46	46.11	507	1876414.915	1097001.929	
95+79.79	-26.89	431	1876492.933	1097016.877	
95+81.32	-35.48	502	1876501.650	1097016.534	
95+86.71	-65.66	501	1876532.284	1097015.329	
96+11.29	-36.25	503	1876508.833	1097045.639	
96+11.37	-39.10	443	1876511.631	1097045.107	
96+16.37	-39.30	499	1876512.896	1097049.944	
96+21.03	-65.39	500	1876539.376	1097048.902	
96+53.98	46.11	12024	1876437.536	1097104.994	
96+55.23	39.11	411	1876444.641	1097104.715	
96+63.73	79.42	513	1876407.095	1097121.655	
96+63.73	46.42	511	1876439.328	1097114.581	
96+98.03	79.42	521	1876414.450	1097155.161	
97+03.70	47.67	520	1876446.677	1097153.894	
97+05.23	39.11	410	1876455.361	1097153.552	
97+23.73	79.42	514	1876419.959	1097180.260	
97+23.73	48.29	512	1876450.358	1097173.588	
98+14.13	51.12	516	1876466.976	1097262.489	
98+14.13	74.12	517	1876444.511	1097267.420	
98+25.00	-57.60	505	1876575.497	1097249.804	
98+25.00	-47.59	506	1876565.722	1097251.950	
98+49.13	52.22	515	1876473.410	1097296.910	
98+49.13	74.12	518	1876452.014	1097301.607	
98+77.40	53.10	453	1876478.608	1097324.717	
99+44.40	59.50	454	1876486.721	1097391.532	
99+53.26	80.78	456	1876467.837	1097404.743	
99+60.70	39.11	401	1876510.129	1097403.080	
100+06.01	-26.89	427	1876584.309	1097433.188	
100+11.03	-54.98	447	1876612.824	1097432.066	
100+12.83	-65.06	504	1876623.055	1097431.664	
106+00.00	-0.00	437	1876685.388	1098019.133	



FOR MORE INFORMATION ON PARCELS 1650001 AND 1650002 SEE SHEET 2

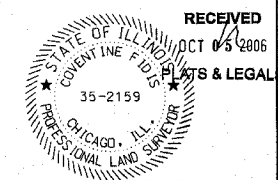
SCANNED

ASC  
American Surveying Consultants  
841 N. Galena Avenue / Dixon, IL 61021 / 815-288-6231  
8604 W. Catalpa Avenue / Chicago, IL 60656 / 773-444-0800  
Illinois Professional Design Firm No. 184-003192

**PLAT OF HIGHWAYS**  
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
F.A.P. 311  
OGDEN AVENUE (U.S. ROUTE 34)

SECTION: AT COUNTY LINE ROAD DU PAGE COUNTY  
STATION 93+00 JOB NO. R-91-009-05  
SCALE: NONE TO STATION 100+13  
SHEET 3

BUREAU OF LAND ACQUISITION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196



DATE-TIME  
DATE-SPEC  
REF  
REF  
REF  
REF

CALCULATIONS BY: J. DYBAS

DRAFTING BY: M. BARAN AND D. DEMKOVICH

CHECKED BY: D. DEMKOVICH

REVISION DATE:

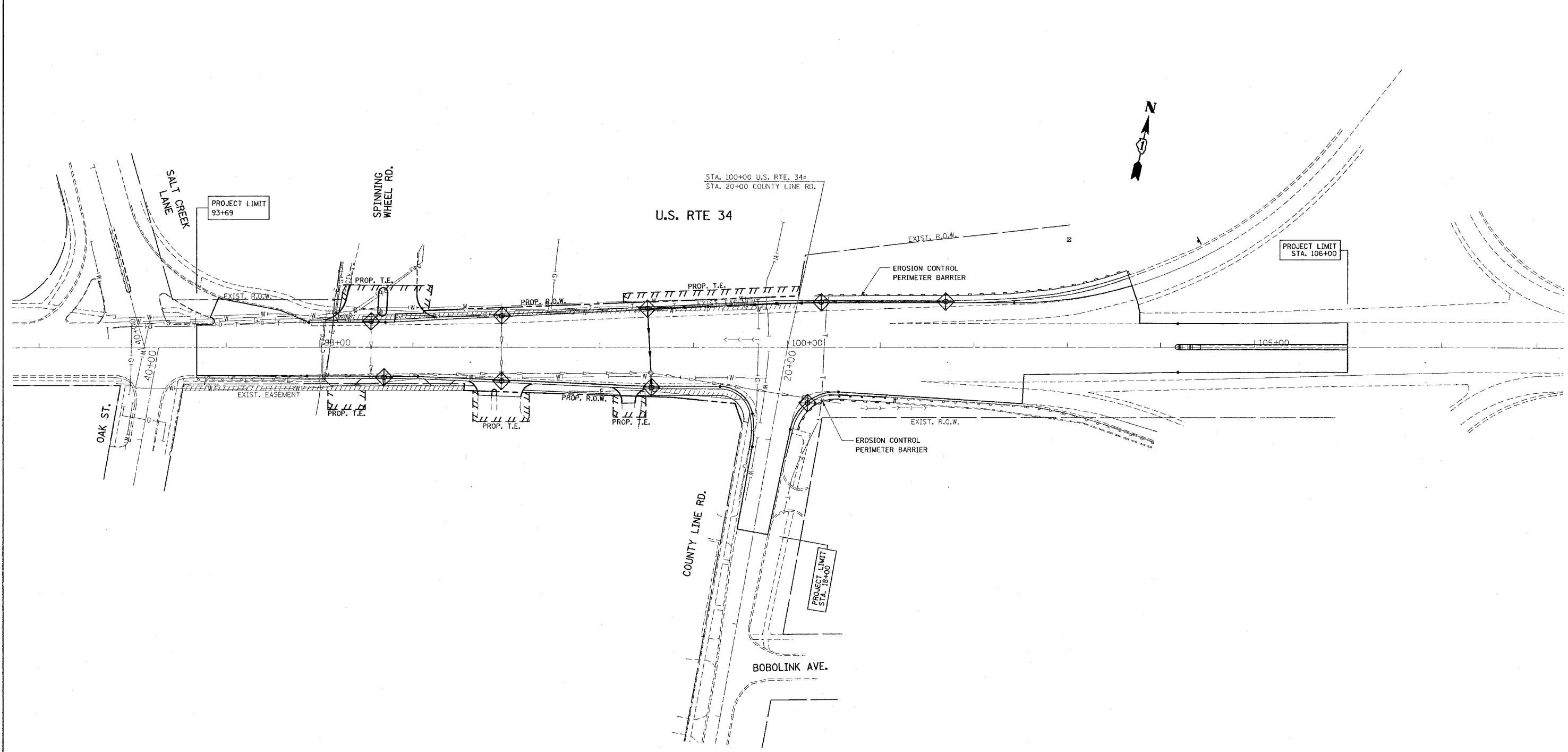
ASC JOB NO. 205053-5





REF-ero01

CONTRACT NO. 62911

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	DUPAGE/COOK	39	21
STA. 92+00		TO STA. 107+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



LEGEND

-  PERIMETER EROSION CONTROL BARRIER
  -  SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER \*\*
- \*\* ITEMS TO BE INSTALLED AFTER THE AREA IS DISTURBED

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EROSION CONTROL PLAN**  
**U.S. RTE. 34 @**  
**COUNTY LINE RD.**

SCALE: VERT. 1"=50'  
 HORIZ. 1"=50'  
 DATE 3/2/2007

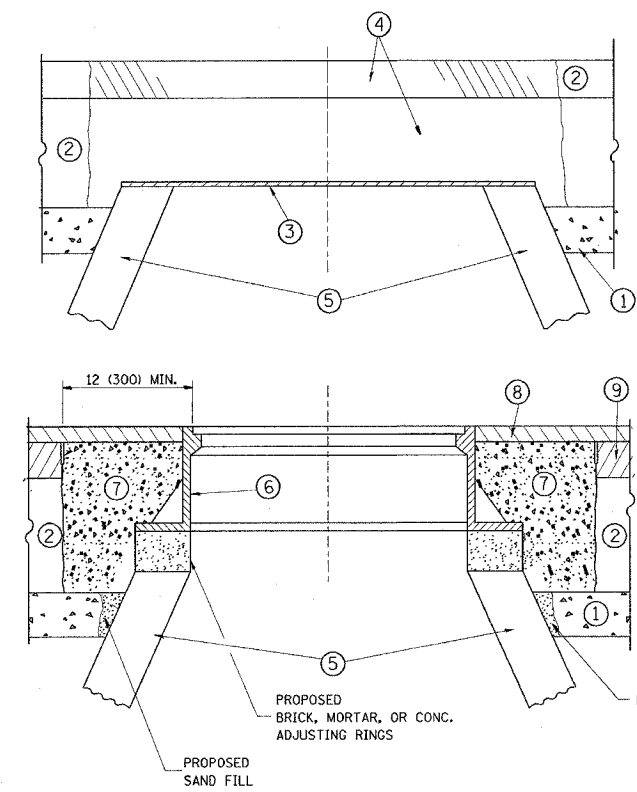
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 CHECKED BY

PLOT DATE = 2/2/2007  
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 USER NAME = driveksgn

R1



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	23
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

**ILLINOIS DEPARTMENT OF TRANSPORTATION**  
**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

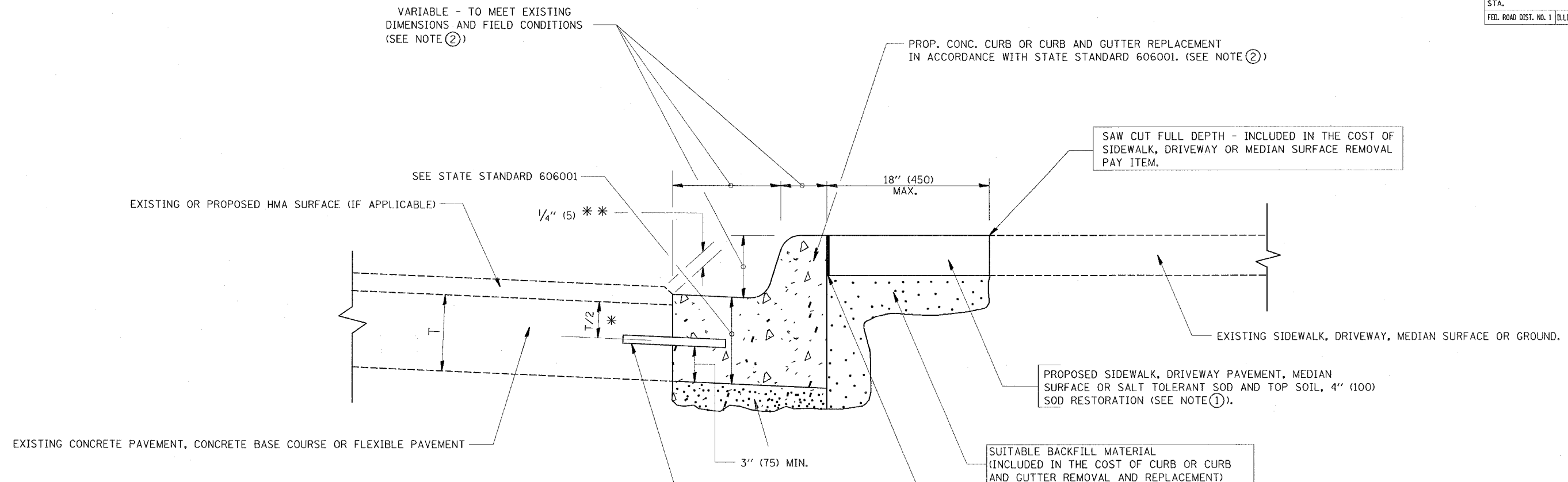
SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 2/16/2007

DRAWN BY  
CHECKED BY

BD600-03 (BD-8)  
REVISION DATE: 01/01/07

PLOT DATE = 2/16/2007  
 FILE NAME = \\casin\c\p2\usera\lan\lsh\l\cra\c\top\l\dd\8\8.dgn  
 USER NAME = lsh  
 USER NAME = lsh

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	24
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

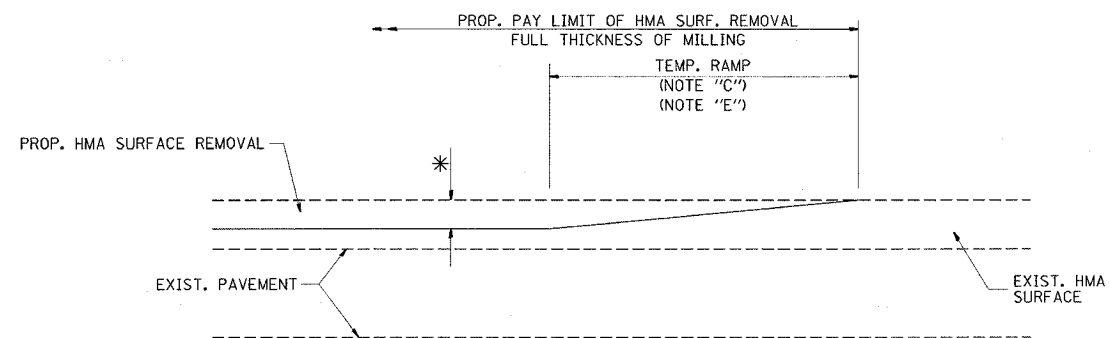
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT**

SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 2/16/2007  
DRAWN BY  
CHECKED BY  
BD600-06 (BD-24)

PLOT DATE = 2/16/2007  
FILE NAME = \\saw\p162\user-a\mshah\desk\top\bd24.dgn  
PLOT SCALE = 1/8"=1'-0"  
USER NAME = mshah

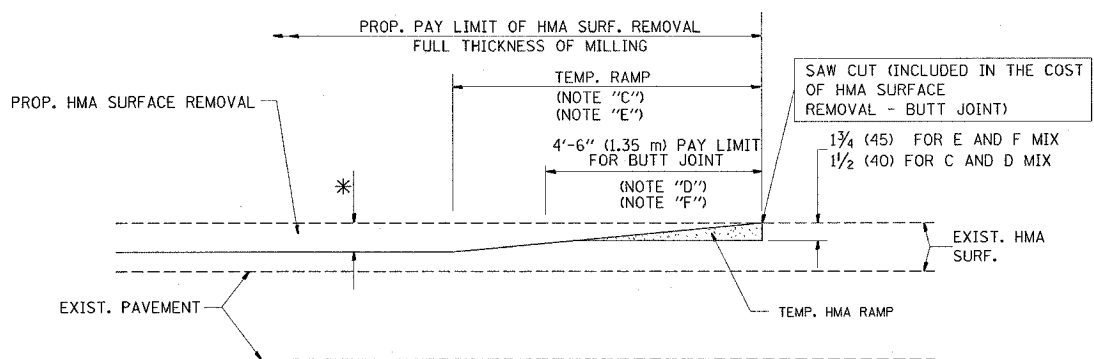


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	25
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

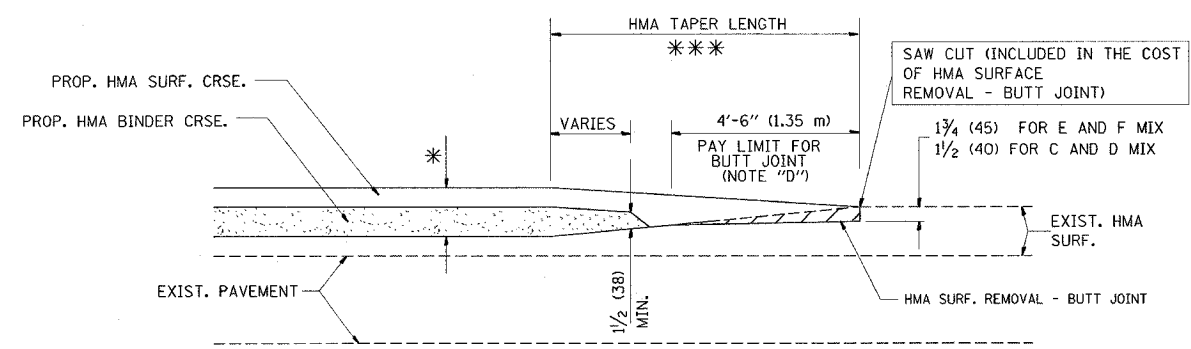
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

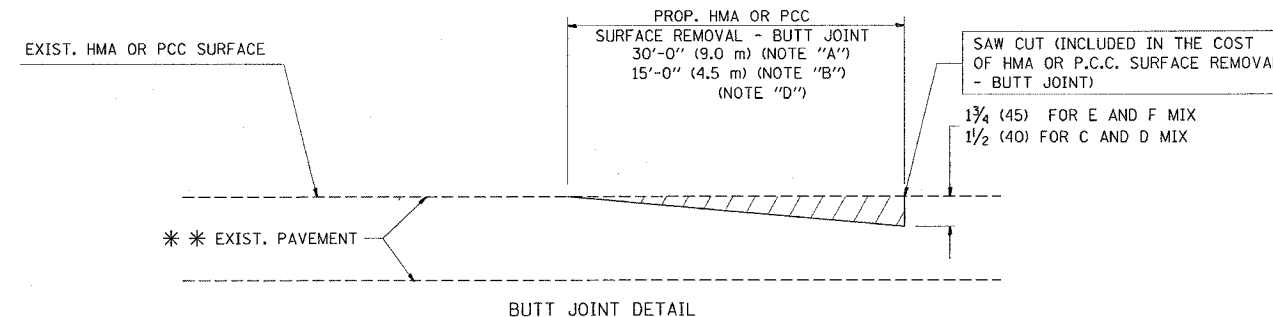
OPTION 2

TYPICAL TEMPORARY RAMP

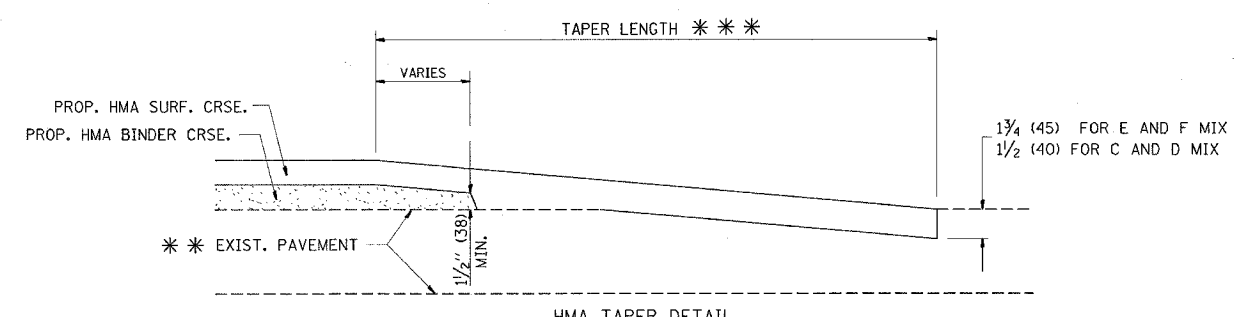


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

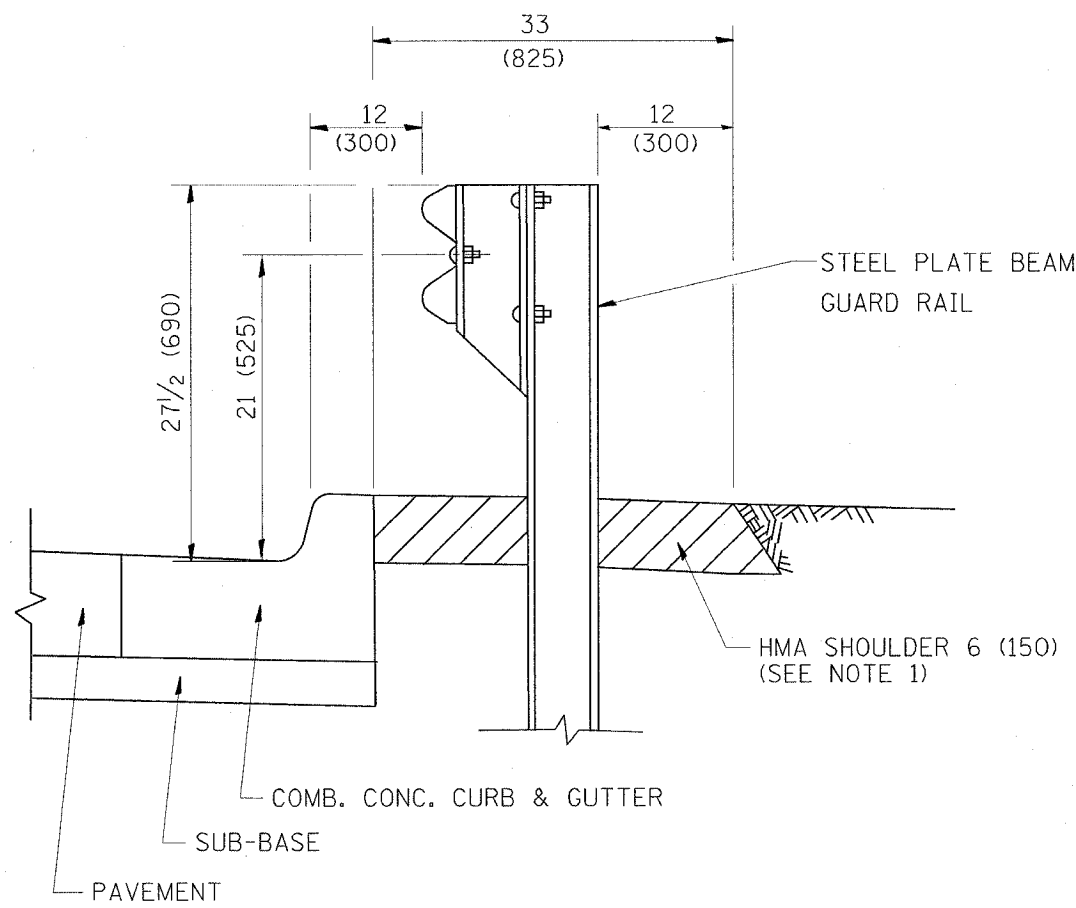
SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 2/16/2007

DRAWN BY  
CHECKED BY

BD400-05 (VI=BD32)  
REVISION DATE: 01/01/07

PLOT DATE = 2/16/2007  
 PLOT TIME = 11:58:51 AM  
 USER NAME = amshk1

CONTRACT NO. 62911				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	25A
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

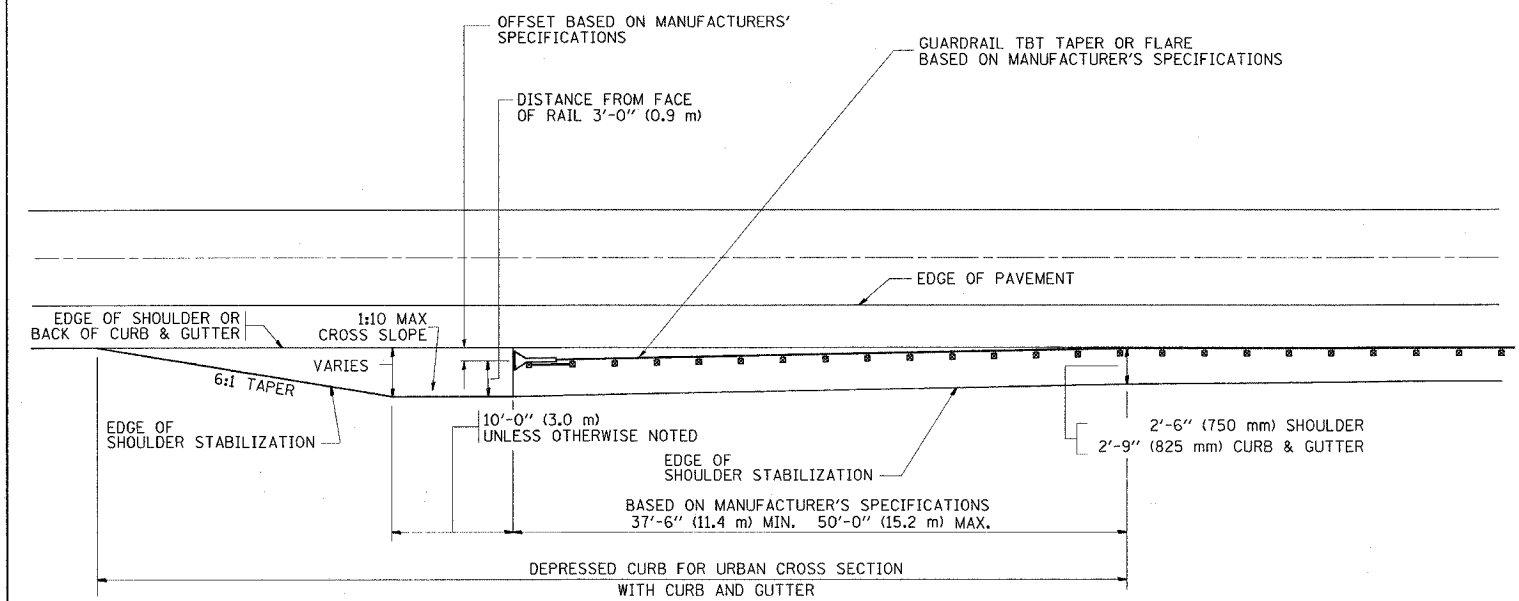


- NOTES: 1. THE HMA SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: HMA SHOULDER 6 (150) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDER 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

**DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER**  
 [FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



**STABILIZATION AT TBT TY. 1 SPL.**

TBT = TRAFFIC BARRIER TERMINAL  
 ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	09-22-90
M. DE YONG	07-14-92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	02/23/95
A. ABBAS	03/21/97
E. GOMEZ	08/28/03
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

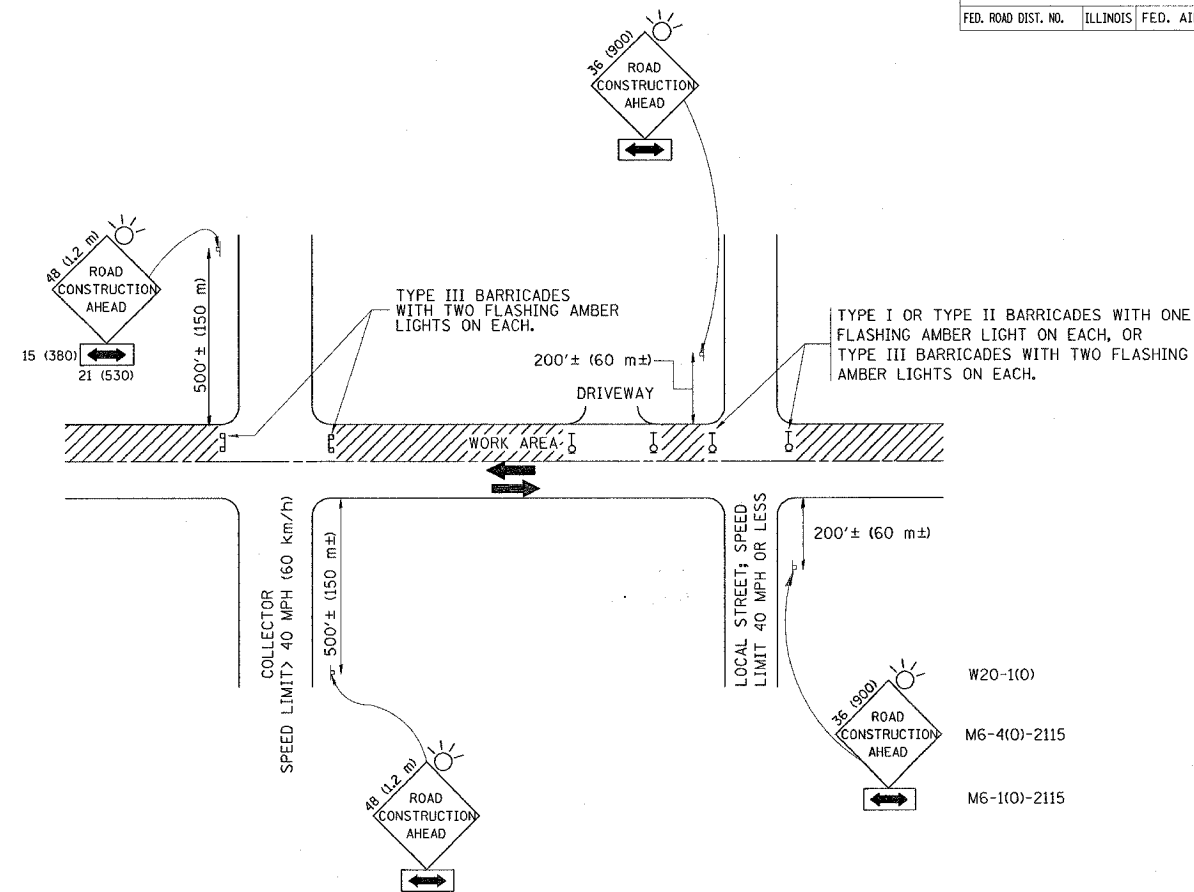
**DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT TO CURB AND GUTTER STABILIZATION AT TBT TY 1 SPL.**

SCALE: VERT. NONE  
 HORIZ. NONE  
 PLOT DATE: 3/2/2007

DRAWN BY Jls  
 CHECKED BY

PLOT DATE = 3/2/2007  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = amthk1

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	26
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

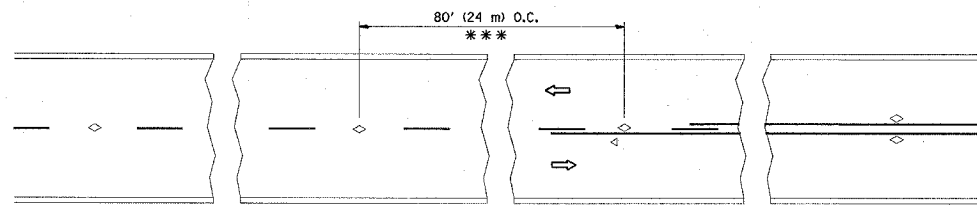
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL AND PROTECTION  
 FOR  
 SIDE ROADS, INTERSECTIONS, AND  
 DRIVEWAYS

SCALE: DRAWN BY  
 DATE: 2/16/2007 CHECKED BY

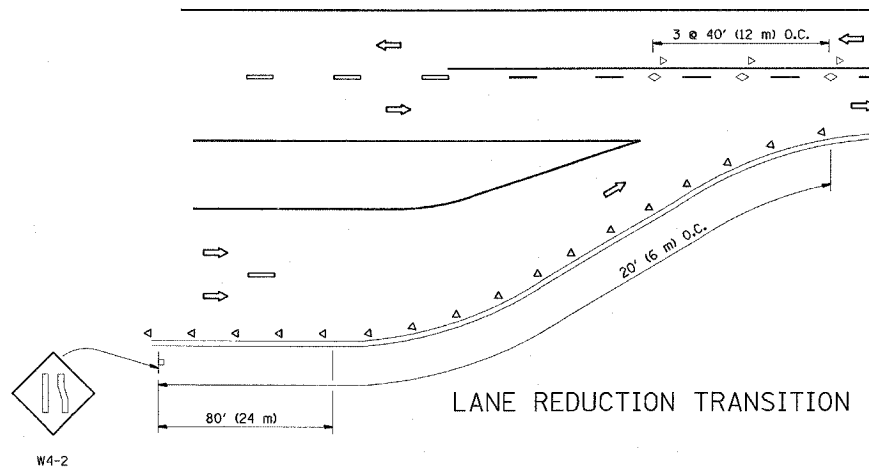
TC-10  
 REVISION DATE: 01/06/00

F.A.P. RTE.	SECTION	COUNTY	SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	27
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

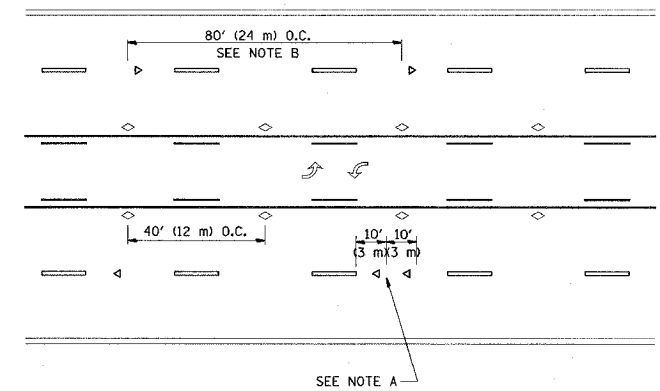


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

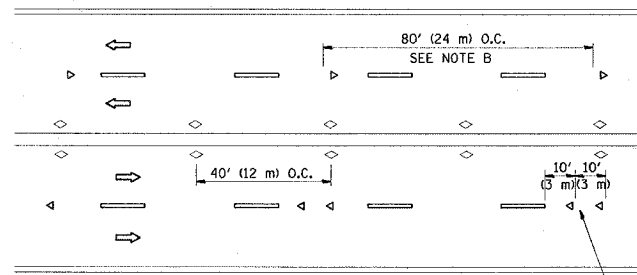
TWO-LANE/TWO-WAY



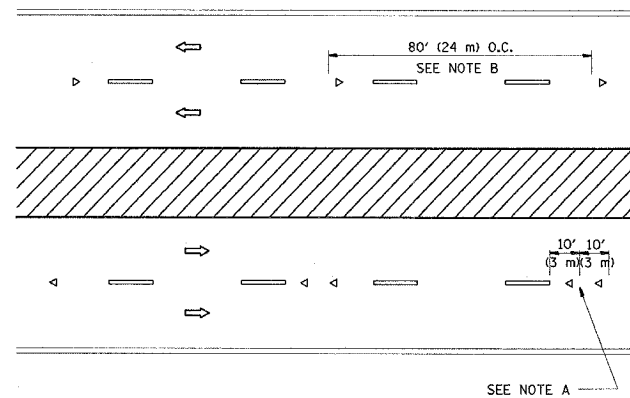
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

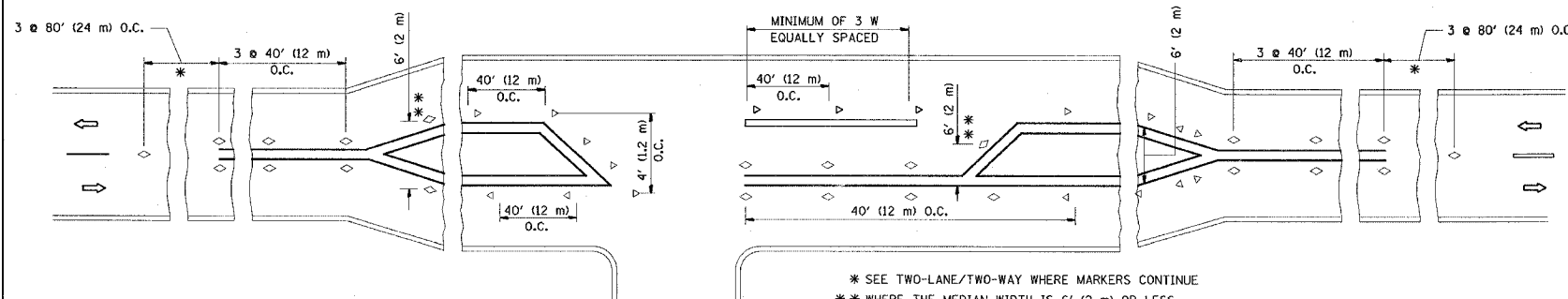
SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◊ TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in millimeters (inches) unless otherwise shown.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

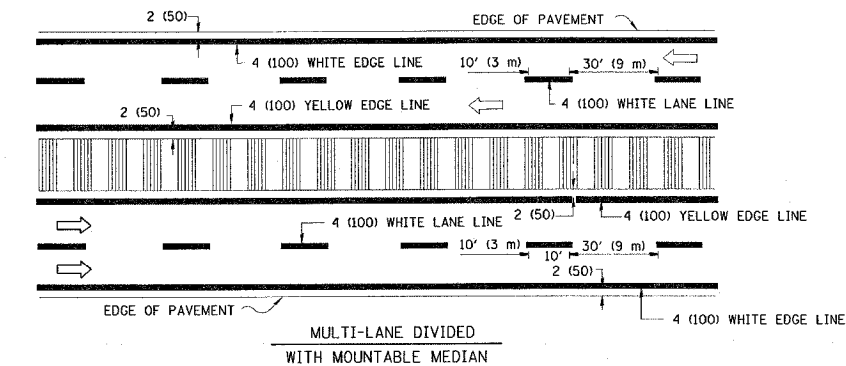
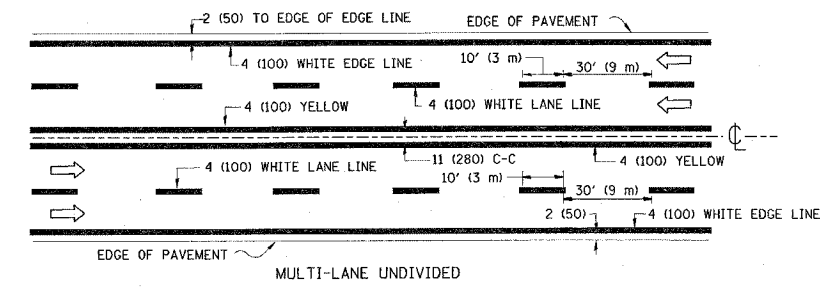
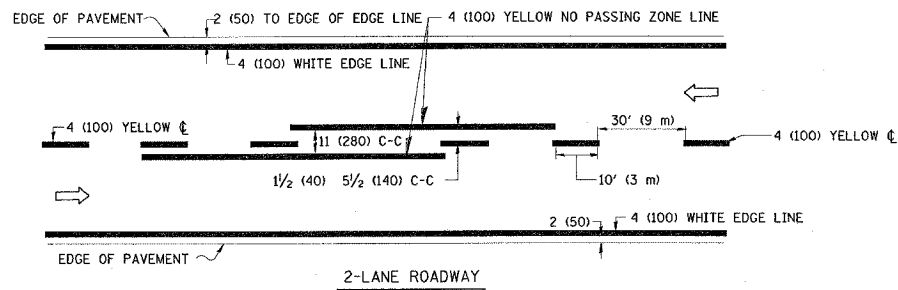
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT  
 MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE  
 DATE: 2/16/2007

DRAWN BY CADD  
 CHECKED BY TC-11

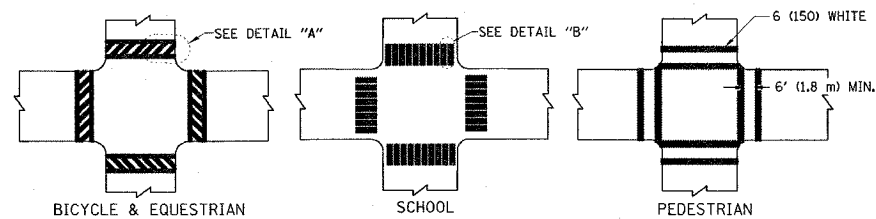
REVISION DATE: 01/06/00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	28
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

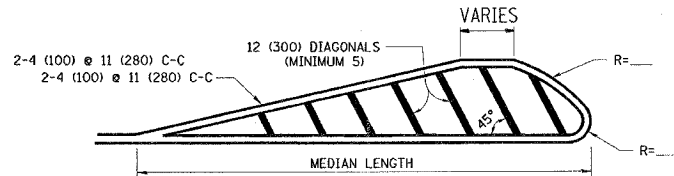
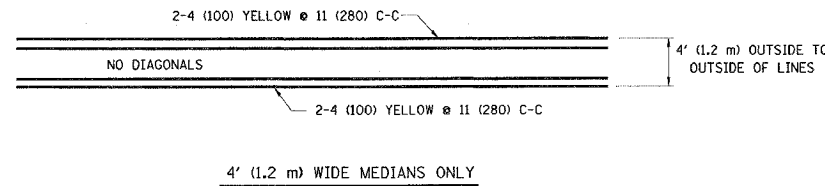


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**



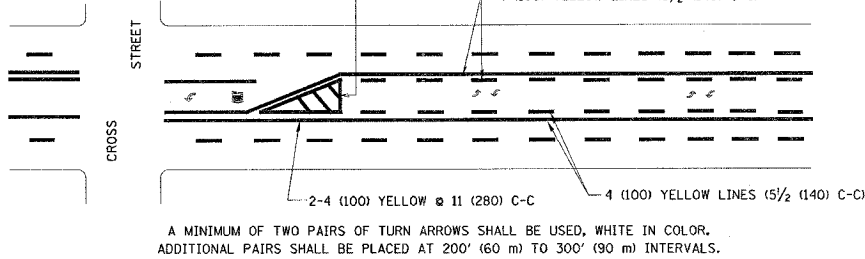
**TYPICAL CROSSWALK MARKING**



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

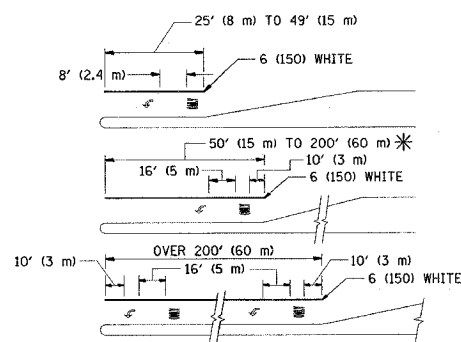
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**



**TYPICAL PAINTED MEDIAN MARKING**

**TYPICAL TURN LANE MARKING**

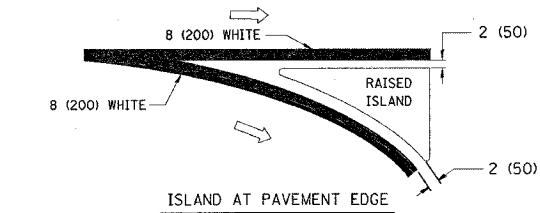
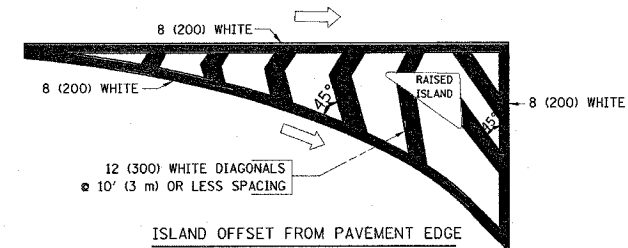


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	5 (125) ON FREEWAYS	SKIP-DASH	WHITE	
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	2' (600) LINE WITH 6' (1.8 m) SPACE
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16' (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

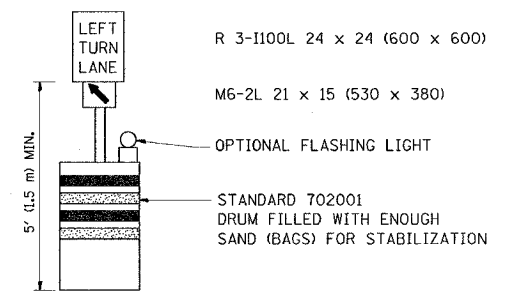
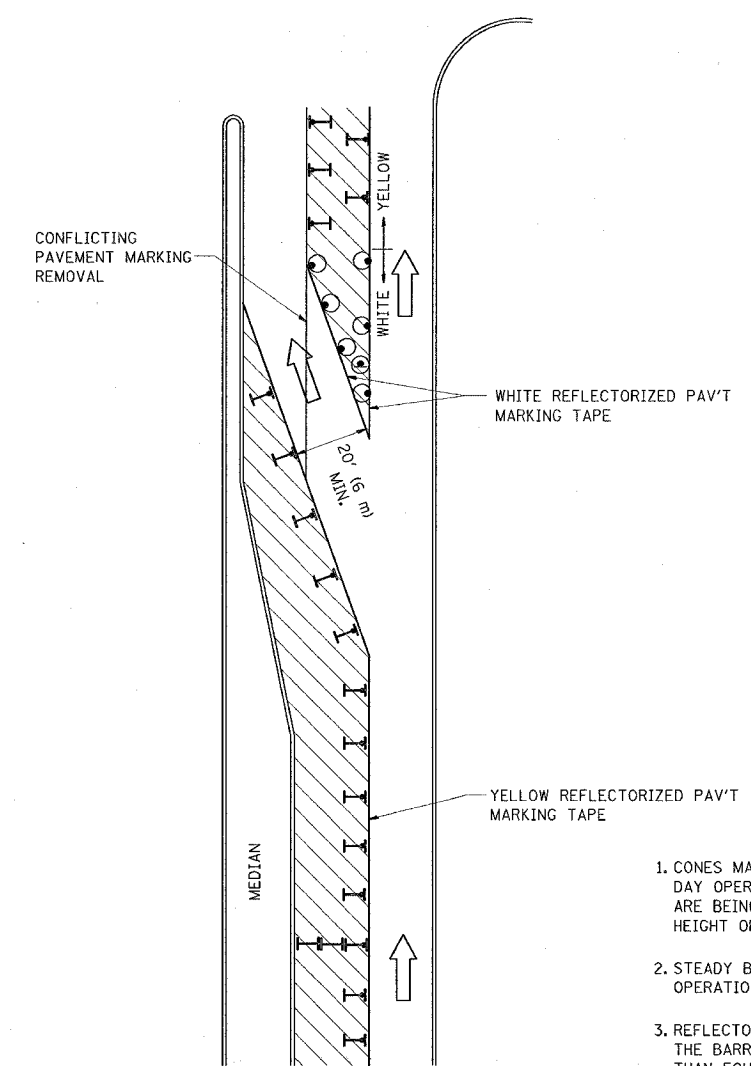
All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DISTRICT ONE  
 TYPICAL PAVEMENT MARKINGS

SCALE: NONE  
 DATE: 2/16/2007  
 DRAWN BY CADD  
 CHECKED BY

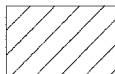





CONTRACT NO. 62911				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	29
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**GENERAL NOTES**

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

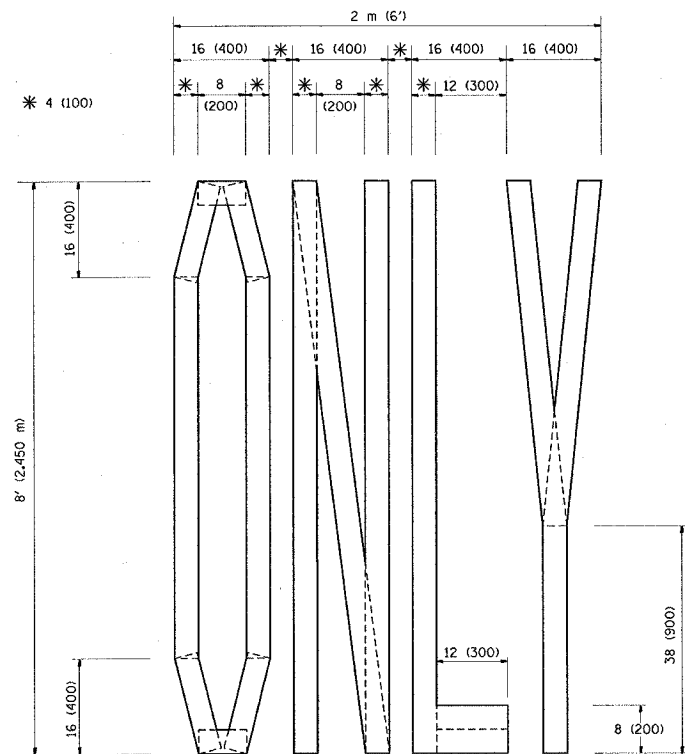
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TRAFFIC CONTROL AND PROTECTION  
 AT TURN BAYS  
 (TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE  
 DATE: 2/16/2007  
 DRAWN BY  
 CHECKED BY LHA  
 TC-14

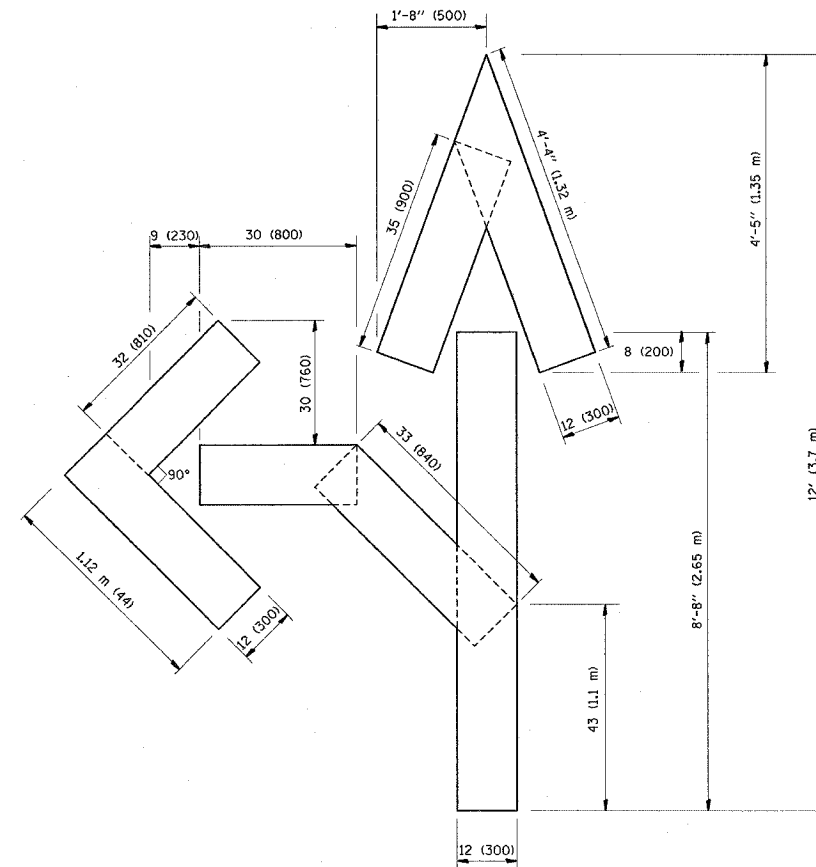
REVISION DATE: 01/06/00

PLOT DATE = 2/16/2007  
 PLOT TIME = 11:27:58  
 PLOT SCALE = 49.99999 / IN.  
 USER NAME = amthh1

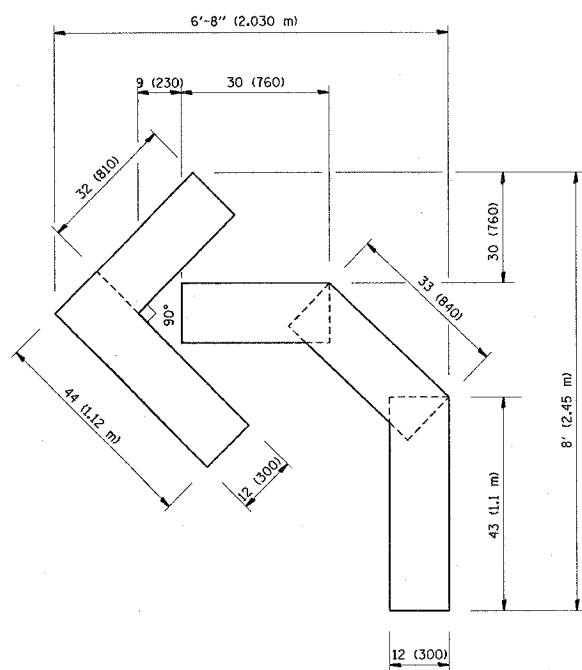
CONTRACT NO. 62911				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	30
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



QUANTITY  
4 (100) LINE = 64.1 ft. (19.7 m)  
21.1 sq. ft. (1.97 sq. m)



QUANTITY  
4 (100) LINE = 82.5 ft. (25.3 m)  
27.5 sq. ft. (2.53 sq. m)



QUANTITY  
4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.39 sq. m)

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

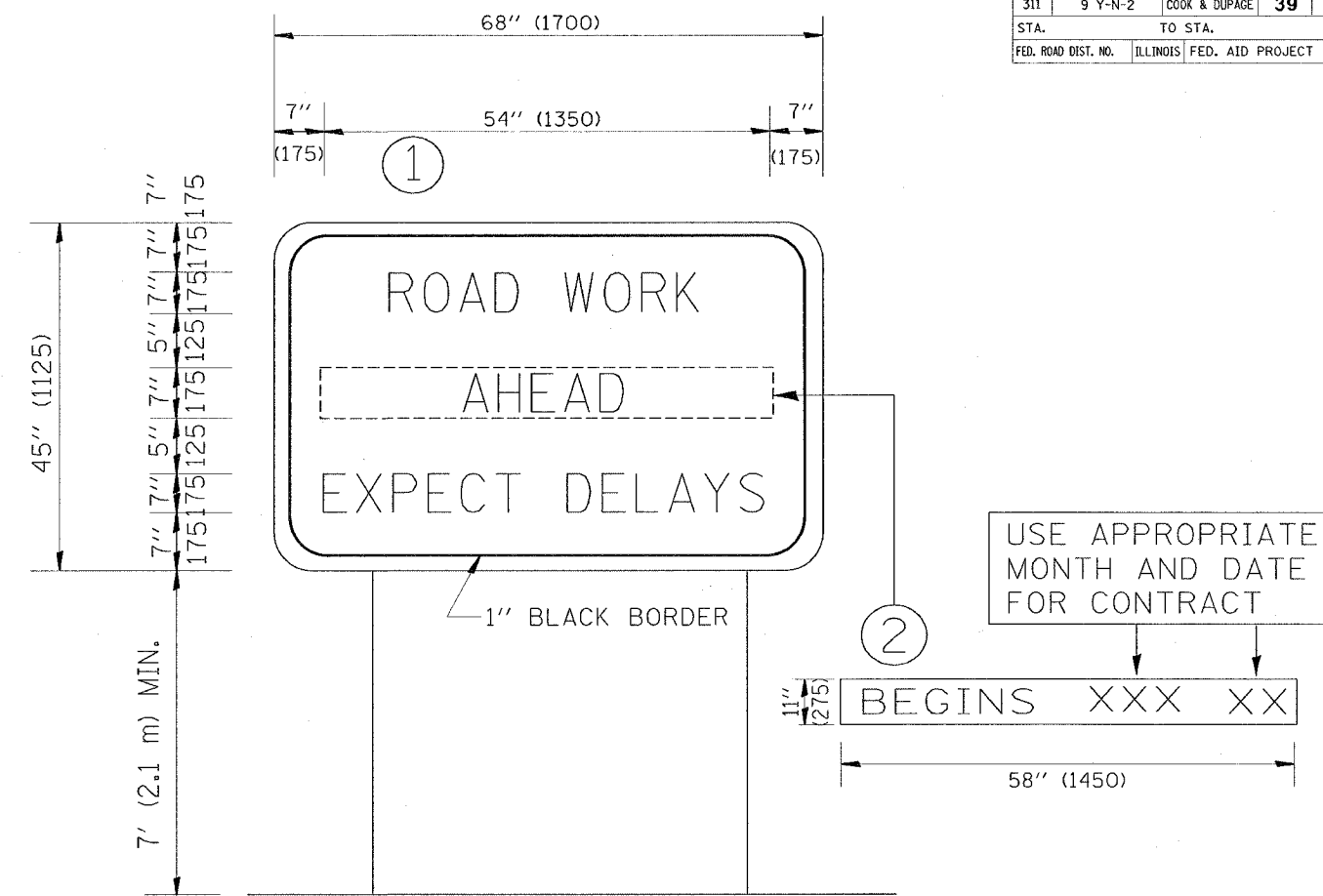
PAVEMENT MARKING  
LETTERS AND SYMBOLS  
FOR TRAFFIC STAGING

SCALE: NONE  
DATE: 2/16/2007

DRAWN BY CADD  
CHECKED BY  
TC-16

REVISION DATE: 08/28/00

CONTRACT NO. 62911				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	31
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

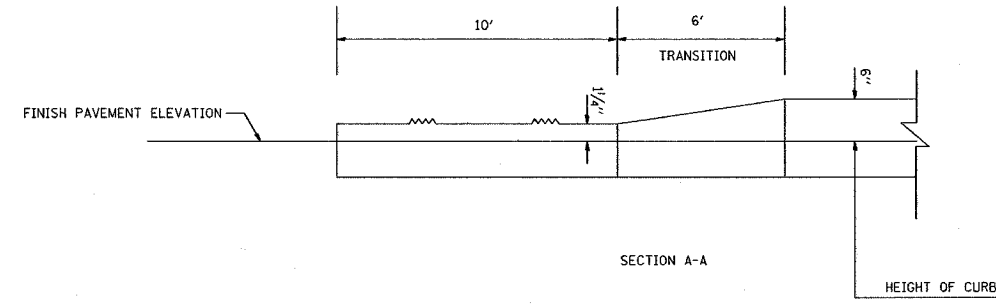
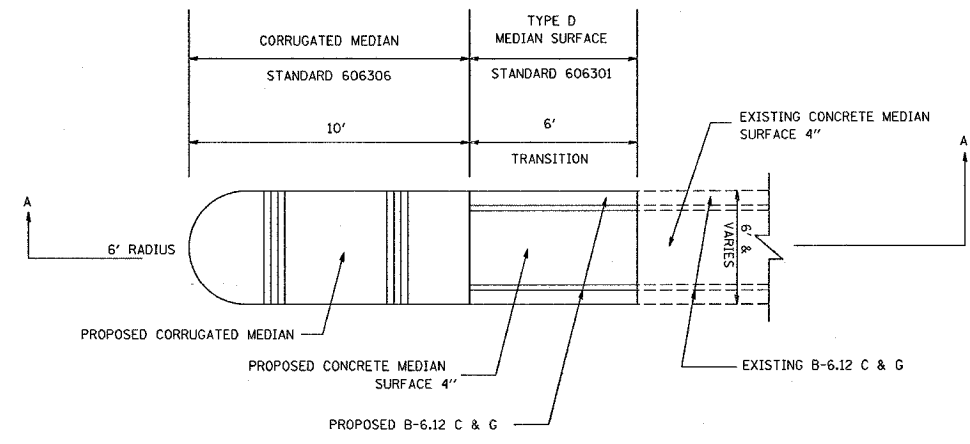
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
R. MIRS	9-15-97	TEMPORARY INFORMATION SIGNING
R. MIRS	12-11-97	
T. RAMMACHER	2-2-99	

SCALE: DATE: 2/16/2007  
 DRAWN BY DESIGN  
 CHECKED BY  
 TC22  
 REVISION DATE: 02/02/99

PLOT DATE = 2/16/2007  
 PLOT SCALE = 3/8" = 1'-0"  
 USER NAME = smt/hkl



F.A.P. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK/DUPAGE	39	32
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**MEDIAN NOSE DETAIL**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

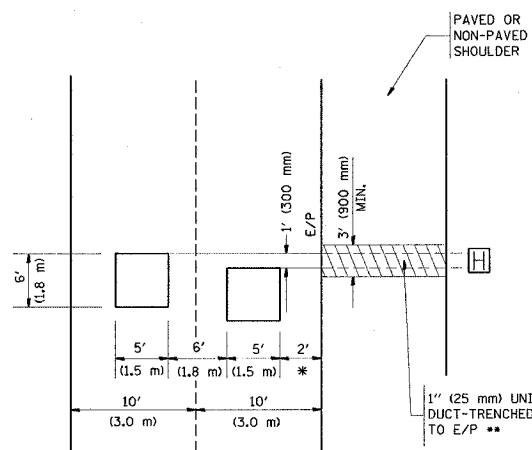
DATE 12/11/2006

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	33
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

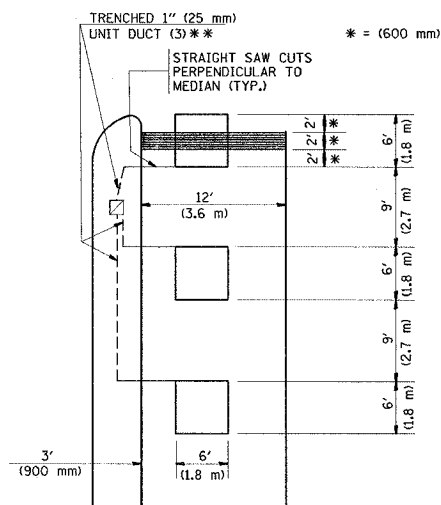


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)

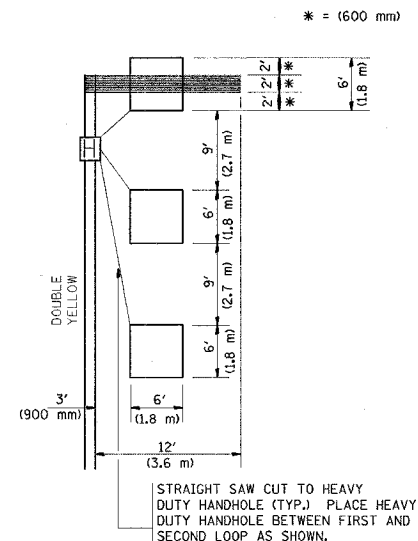
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD B14001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

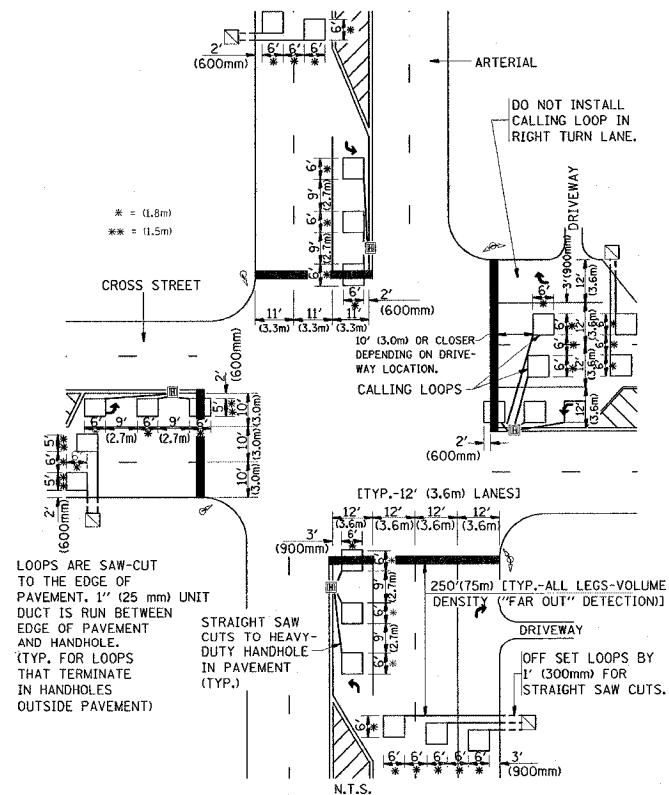
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
DISTRICT 1  
DETECTOR LOOP  
INSTALLATION DETAILS  
FOR ROADWAY RESURFACING

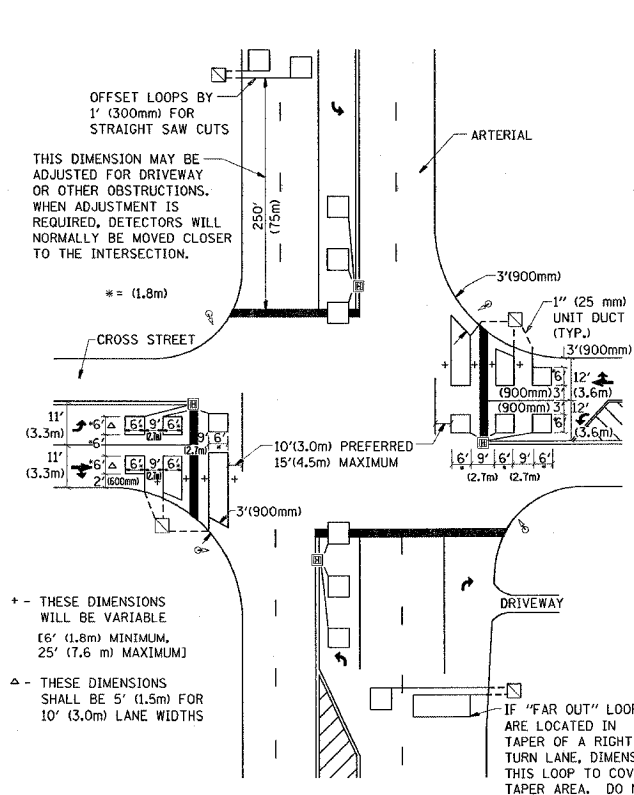
DESIGNED BY  
DRAWN BY CADD  
CHECKED BY R.K.F.  
TS07  
REVISION DATE:

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



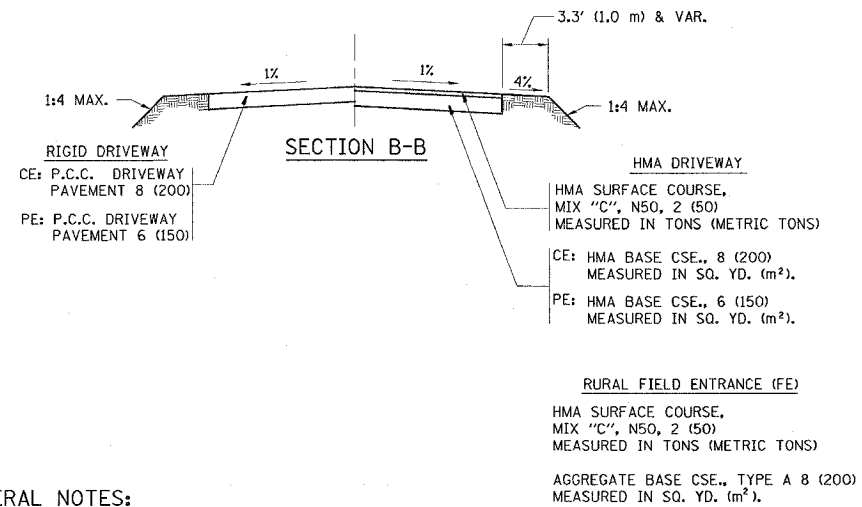
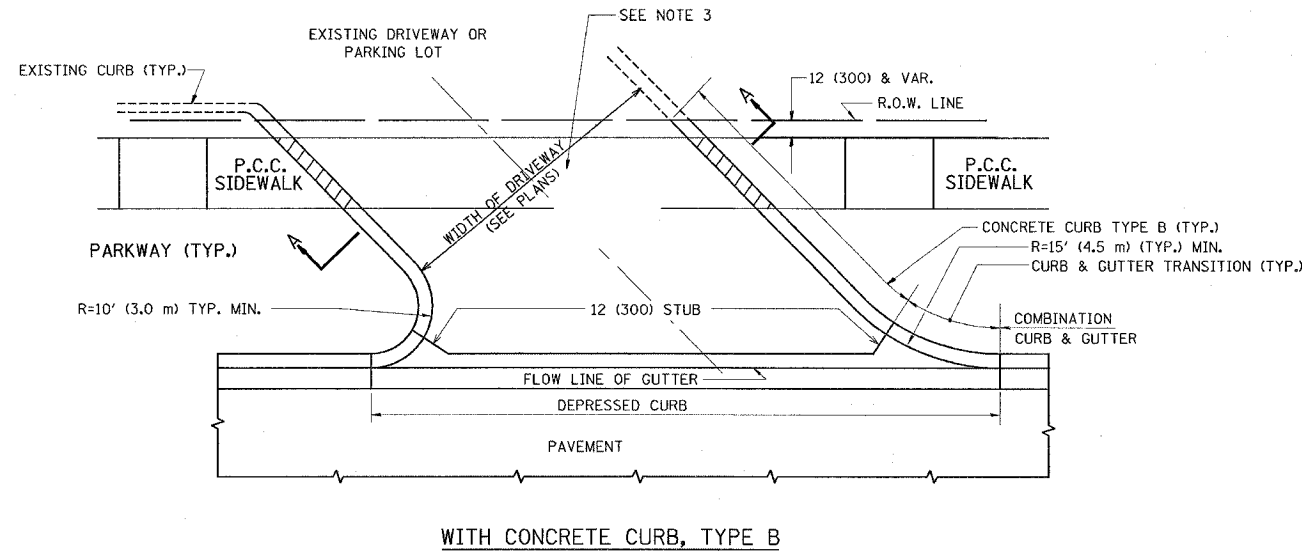
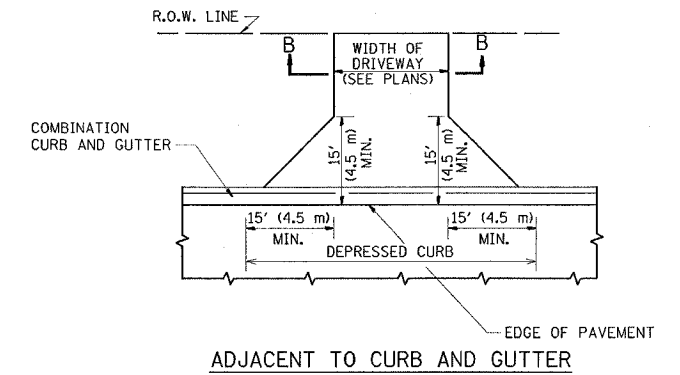
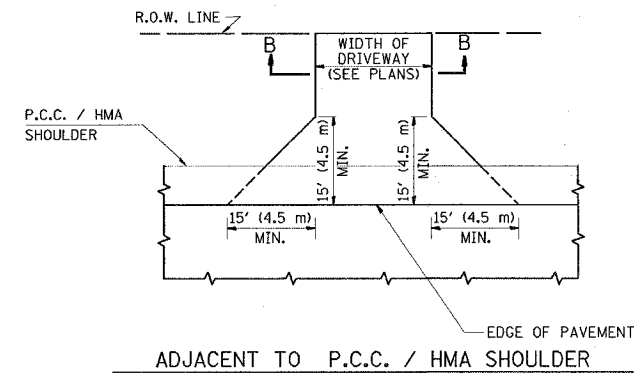
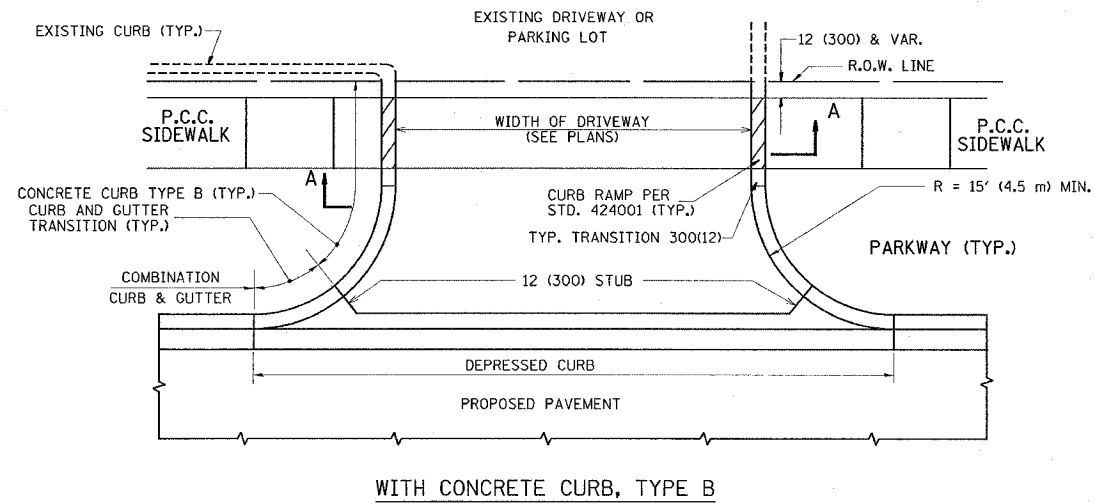
DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	34
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

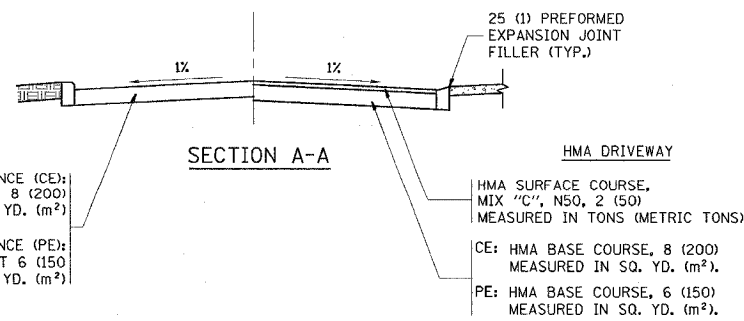
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DRIVEWAY DETAILS**  
DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

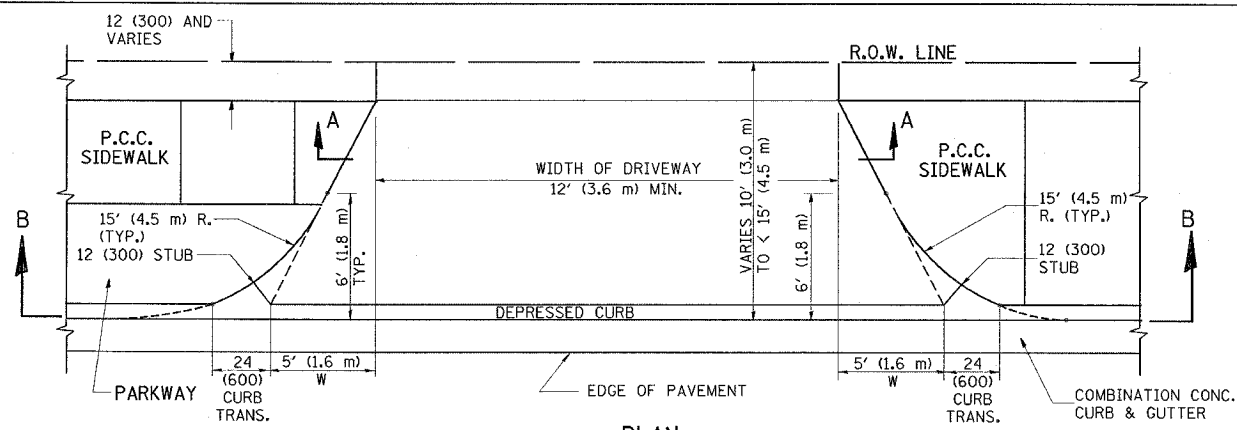
REVISIONS	
NAME	DATE
R. SHAH	11-04-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01
P. LAFLEUR	04-15-03
R. BORO	01-01-07

SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 2/16/2007

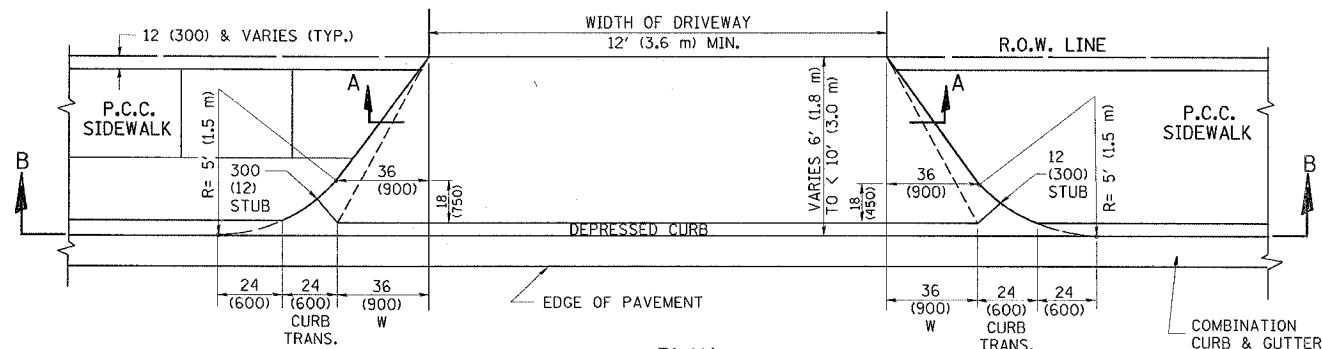
DRAWN BY  
CHECKED BY

BD0156-07 (BD-01)  
REVISION DATE: 01/01/07

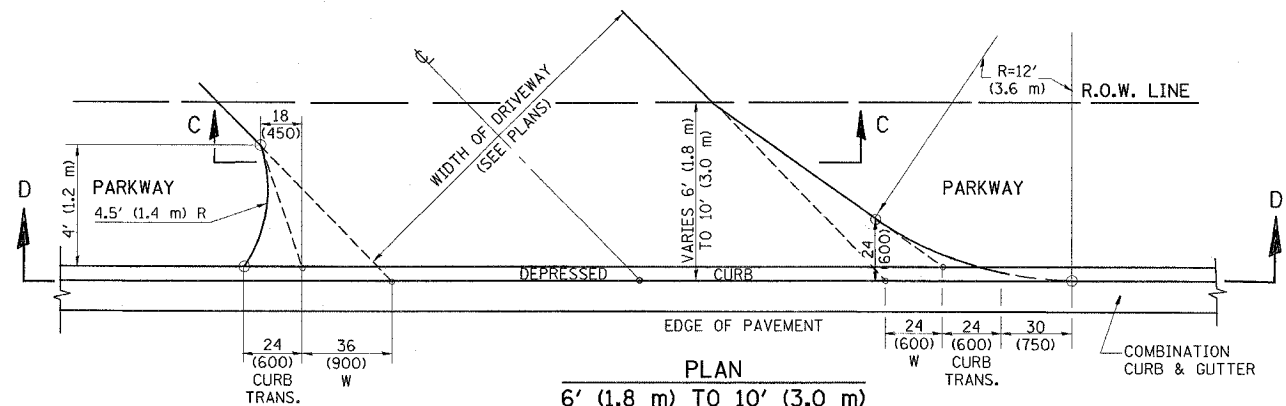
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9 Y-N-2	COOK & DUPAGE	39	35
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



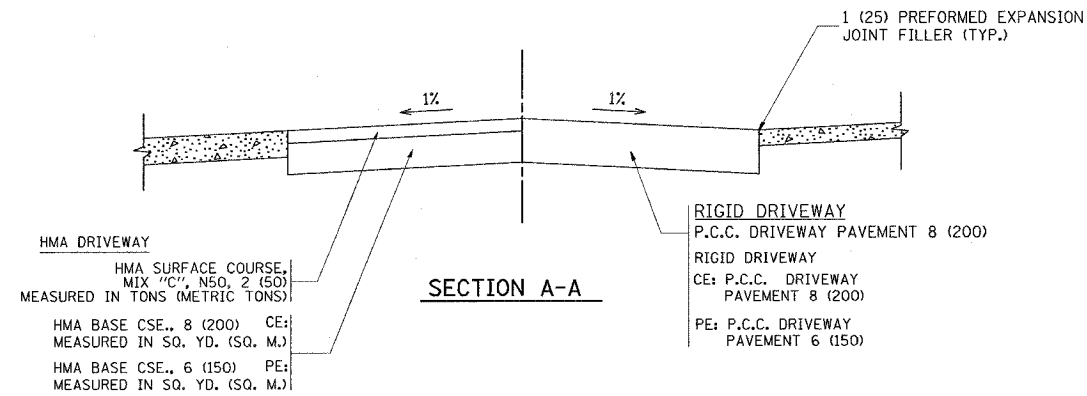
**PLAN**  
10' (3.0 m) TO < 15' (4.5 m)



**PLAN**  
6' (1.8 m) TO < 10' (3.0 m)

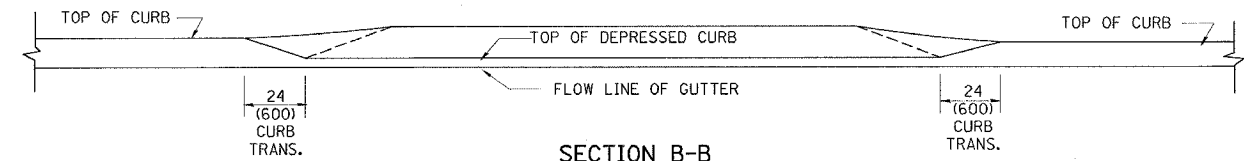


**PLAN**  
6' (1.8 m) TO 10' (3.0 m)

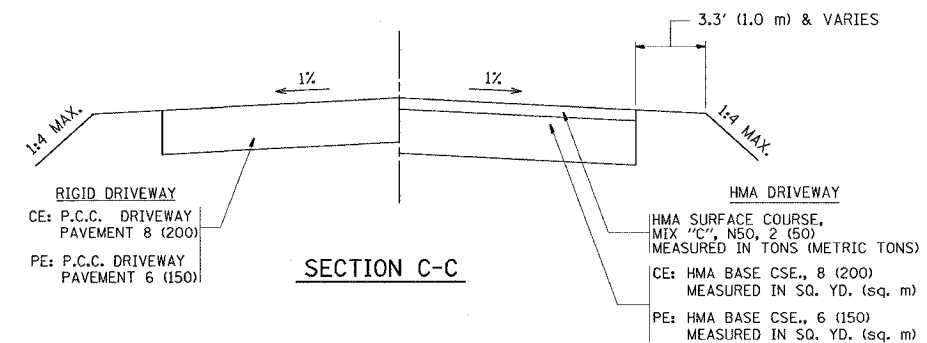


**HMA DRIVEWAY**  
HMA SURFACE COURSE, MIX "C", N50, 2 (50) MEASURED IN TONS (METRIC TONS)  
HMA BASE CSE., 8 (200) MEASURED IN SQ. YD. (SQ. M.)  
HMA BASE CSE., 6 (150) MEASURED IN SQ. YD. (SQ. M.)

**RIGID DRIVEWAY**  
P.C.C. DRIVEWAY PAVEMENT 8 (200)  
RIGID DRIVEWAY CE: P.C.C. DRIVEWAY PAVEMENT 8 (200)  
PE: P.C.C. DRIVEWAY PAVEMENT 6 (150)

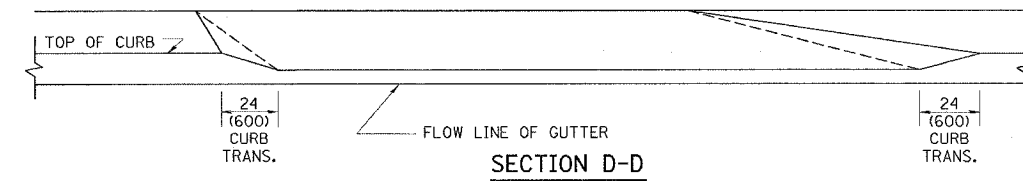


**SECTION B-B**



**RIGID DRIVEWAY**  
CE: P.C.C. DRIVEWAY PAVEMENT 8 (200)  
PE: P.C.C. DRIVEWAY PAVEMENT 6 (150)

**HMA DRIVEWAY**  
HMA SURFACE COURSE, MIX "C", N50, 2 (50) MEASURED IN TONS (METRIC TONS)  
CE: HMA BASE CSE., 8 (200) MEASURED IN SQ. YD. (SQ. M.)  
PE: HMA BASE CSE., 6 (150) MEASURED IN SQ. YD. (SQ. M.)



**SECTION D-D**

**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

ILLINOIS DEPARTMENT OF TRANSPORTATION

**DRIVEWAY DETAILS**  
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)

REVISIONS	
NAME	DATE
R. SHAH	11/06/95
J. POLLASTRINI	08/12/96
J. POLLASTRINI	12/14/96
A. ABBAS	03/21/97
T. HOLTZ	04/08/97
M. GOMEZ	04/06/01
P. LAFLEUR	04/15/03
R. BORO	01/01/07

SCALE: VERT. HORIZ. DATE PLOTTED: 2/16/2007

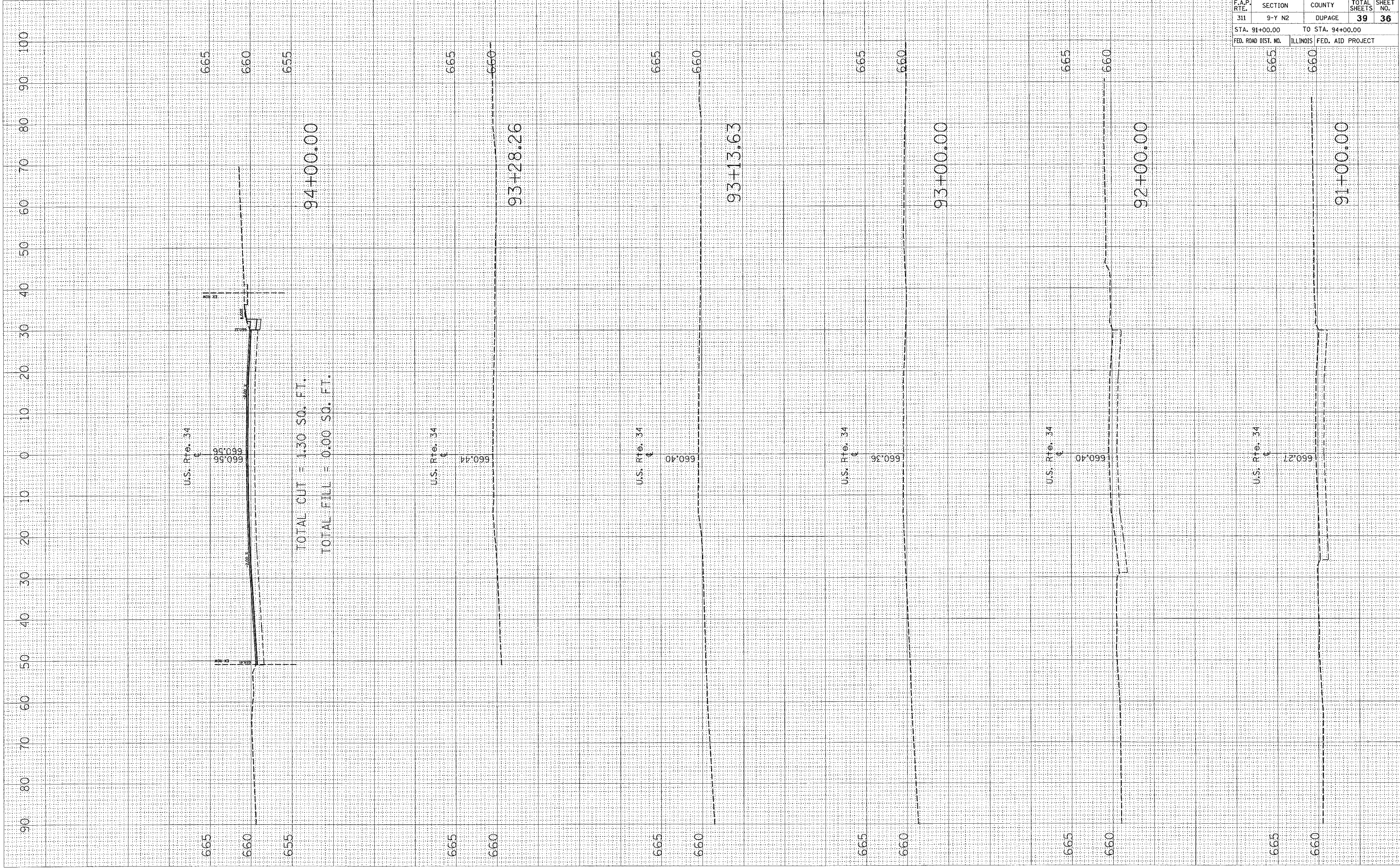
DRAWN BY CHECKED BY

BD400-02 (80-02) REVISION DATE: 01/01/07

PLOT DATE = 2/16/2007  
 FILE NAME = c:\pco\pco\p1009303\p1009303.dwg  
 PLOT SCALE = 1/8"=1'-0"  
 USER NAME = sm19k1

ORIGINAL SURVEY  
 SURVEYED BY \_\_\_\_\_ DATE \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_  
 TYPED BY \_\_\_\_\_  
 NO. \_\_\_\_\_ AREAS CHECKED \_\_\_\_\_

FINAL SURVEY  
 SURVEYED BY \_\_\_\_\_ DATE \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_  
 TYPED BY \_\_\_\_\_  
 NO. \_\_\_\_\_ AREAS CHECKED \_\_\_\_\_



CONTRACT NO. 62911			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
311	9-Y N2	DUPAGE	39
STA. 91+00.00		TO STA. 94+00.00	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	
665	660	665	660

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100



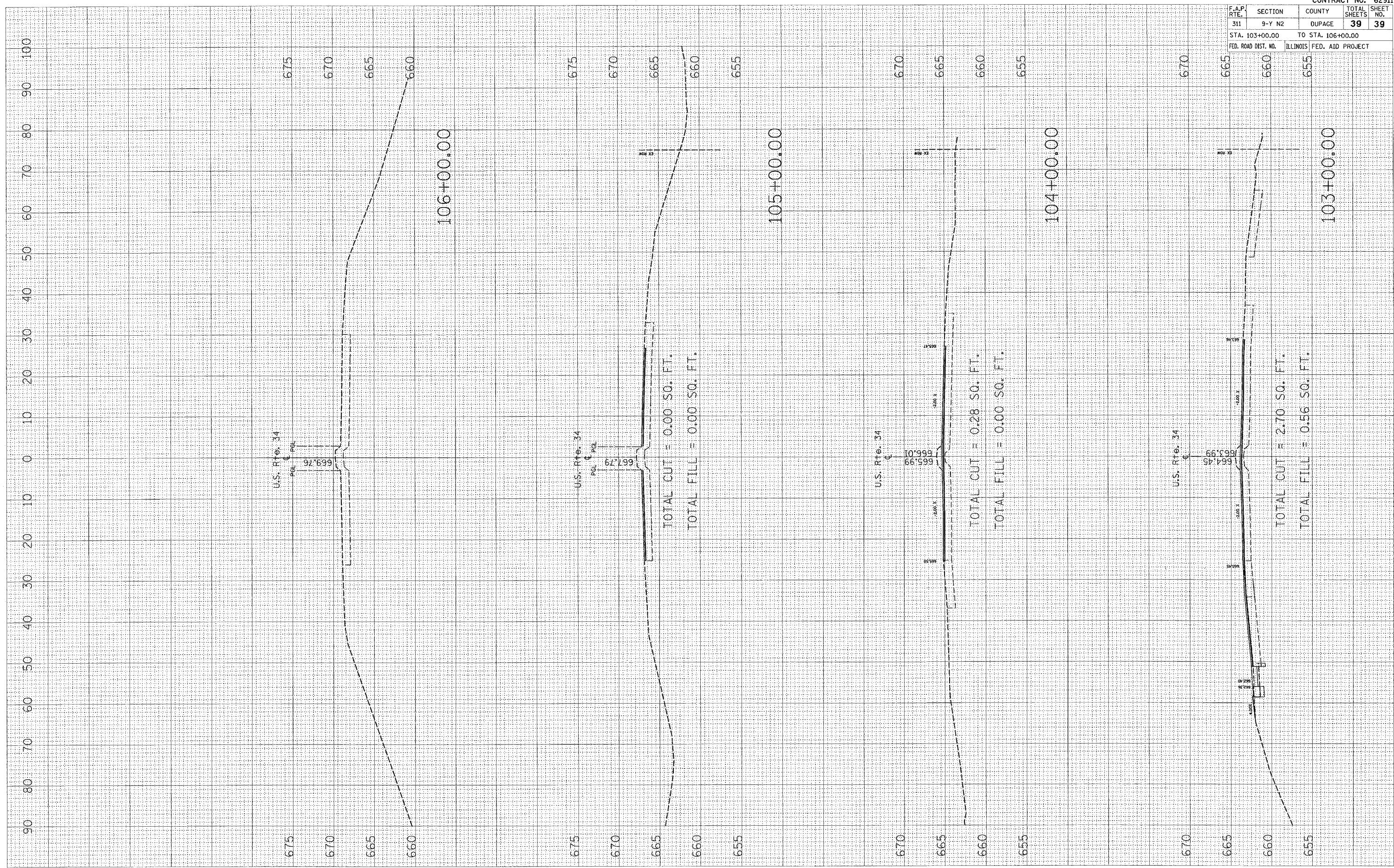


PLOT DATE = 2/16/2007  
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 PLOT SCALE = 18.0000' / 1"

ORIGINAL SURVEY PLOTTED  
 SURVEY PLOTTED  
 NOTE BOOK AREAS CHECKED  
 NO. AREAS CHECKED

FINAL SURVEY PLOTTED  
 SURVEY PLOTTED  
 NOTE BOOK AREAS CHECKED  
 NO. AREAS CHECKED

BY \_\_\_\_\_  
 DATE \_\_\_\_\_



TOTAL CUT = 0.00 SQ. FT.  
 TOTAL FILL = 0.00 SQ. FT.

TOTAL CUT = 0.28 SQ. FT.  
 TOTAL FILL = 0.00 SQ. FT.

TOTAL CUT = 2.70 SQ. FT.  
 TOTAL FILL = 0.56 SQ. FT.

F.A.P. RTE.		SECTION		COUNTY		TOTAL SHEETS		SHEET NO.	
311		9-Y N2		DUPAGE		39		39	
STA. 103+00.00				TO STA. 106+00.00					
FED. ROAD DIST. NO.				ILLINOIS FED. AID PROJECT					

CONTRACT NO. 62911

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100