

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**		37	2
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

•-255-55/70
 ••(60-7, 60-9)RS AND 60-(6-1,7-1)RS

INDEX OF SHEETS

- 1 COVER SHEET
- 2 INDEX OF SHEETS, GENERAL NOTES, AND STANDARDS
- 3 SUMMARY OF QUANTITIES
- 4 DETAILED LOCATION MAPS
- 5 SCHEDULE OF QUANTITIES-LOCATIONS 1&2
- 6-10 TYPICAL SECTIONS-LOCATIONS 1&2
- 11-18 SCHEDULES-LOCATIONS 1&2
- 19 RAMP CLOSURE DETAIL-LOCATIONS 1&2
- 20 DETOUR SIGNING-LOCATIONS 1&2
- 21-28 DETOUR SHEETS-LOCATIONS 1&2
- 29 DETAILS-LOCATIONS 1&2
- 30 SCHEDULE OF QUANTITIES-LOCATION 3
- 31 TYPICALS-LOCATION 3
- 32-33 SCHEDULES-LOCATION 3
- 34-35 TRAFFIC CONTROL-LOCATION 3
- 36-37 DETAILS-LOCATION 3

GENERAL NOTES

1. THE STANDARDS AND REVISION NUMBERS SHALL APPLY TO THIS PROJECT.
2. THE 155/70 AT I255 INTERSECTION SHALL BE DENOTED AS LOCATION #1, THE I270 AT I255 INTERSECTION SHALL BE DENOTED AS LOCATION #2, AND I-55/70 SHALL BE DENOTED AS LOCATION 3.
3. IF ANY SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED OR RESURFACED OVER. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.
4. EXCESS BITUMEN REMOVAL SHALL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
5. MIXTURES FOR JOINTS, CRACKS, AND FLANGWAYS SHALL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
6. THE MATERIAL TRANSFER DEVICE (MTD) MAY BE ALLOWED TO TRAVEL OVER STRUCTURES IF GIVEN APPROVAL FROM THE ENGINEER, THE MTD IS EMPTIED OF BITUMINOUS MATERIAL PRIOR TO CROSSING THE STRUCTURE, AND THE TIRES OF THE MTD TRAVEL ON OR IN CLOSE PROXIMITY AND PARALLEL TO THE BEAM AND/OR GIRDER LINES OF THE STRUCTURE. THE RESIDENT ENGINEER SHOULD MARK THE DECK SURFACE FOR THE LOCATION OF THE BEAMS AND GIRDERS. THE COST TO EMPTY THE MTD OF BITUMINOUS MATERIAL WILL BE AT THE EXPENSE OF THE CONTRACTOR, IF THE NBIS RATING OF THE STRUCTURE IS LESS THAN OR EQUAL TO 4.0, THE DEPARTMENT WILL PAY TO HAVE THE MTD MOVED IN ACCORDANCE TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
7. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
8. ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:
 - AMERENCIPS
 - MOUND PUBLIC WATER & SEWER DISTRIC
 - AMEREN IP
 - CENTERPOINT ENERGY
 - CHARTER COMMUNICATIONS, INC.
 - CITY OF COLLINSVILLE
 - VILLAGE OF GLEN CARBON
 - ILLINOIS AMERICAN WATER COMPANY
 - MCLEOD USA TELECOMMUNICATIONS, INC.
 - MADISON COUNTY SPECIAL SERVICE AREA, #1
 - MITCHELL PUBLIC WATER DISTRICT
 - AT&T ILLINOIS
 - LEVEL 3 COMMUNICATIONS
 - SOUTHWESTERN ELECTRIC COOP
 - CITY OF TROY

MEMBERS OF J.U.L.I.E. (800)-892-0123 ARE INDICATED BY *. NON J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.
9. ALL TEMPORARY PAVEMENT MARKINGS WILL BE PLACED IN SUCH A MANNER SO AS NOT TO INTERFERE WITH THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
10. BITUMINOUS CONCRETE USED FOR PAVEMENT PATCHING AND/OR BASE COURSE WIDENING SHALL BE PRODUCED UNDER THE SUPERPAVE SPECIAL PROVISION.
11. HIGH EARLY STRENGTH CONCRETE MIX SHALL BE USED FOR ALL PCC PATCHING.
12. ALL PATCHING ON THIS PROJECT SHALL BE COMPLETED PRIOR TO SURFACE REMOVAL.
13. THE CONTRACTOR AND THE ENGINEER SHALL BE AWARE THAT NO SURVEY WAS PERFORMED FOR THIS PROJECT. THE STATIONING AND TOPO SHOWN IN THE PLANS WAS CREATED USING MICROFILM AND FIELD MEASUREMENTS MADE BY DESIGN AND OPERATIONS PERSONNEL. BOTH SHALL BE CONSIDERED APPROXIMATE.
14. ADVANCE WARNING SIGNS SHALL BE PLACED ON ALL ROUTES WHERE RAMPS WILL BE CLOSED ACCORDING TO STANDARD 701400 AND AS DIRECTED BY THE RESIDENT ENGINEER TO NOTIFY MOTORISTS OF RAMP AND LANE CLOSURES ON THE VARIOUS RAMPS AT LOCATION #1 AND LOCATION #2.
15. OVER NIGHT LANE CLOSURES WILL NOT BE ALLOWED.
16. ANY REFERENCE TO BITUMINOUS CONCRETE SHALL BE CONSTRUED TO MEAN HMA.
17. ALL LOCATIONS WHERE MAINLINE 155/70 (WB) TIE IN TO THE I255 RAMPS SHALL BE CONSTRUCTED ACCORDING TO THE SPECIAL DETAIL SHOWN IN THE PLANS.
18. THE PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE PLACED AS DIRECTED BY THE ENGINEER. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THE RELOCATION OF THESE SIGNS.
19. EXCEPT WHERE DESIGNATED OTHERWISE, THE LOCATIONS AND/OR DEPTHS OF UNDERGROUND UTILITIES SHOWN HAVE BEEN TAKEN FROM OFFICE RECORD INFORMATION FURNISHED BY THE UTILITY OWNERS AND MUST BE CONSIDERED APPROXIMATE.
20. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
21. ALL MILLINGS FROM LOCATIONS 1 AND 2, NOT USED BY THE CONTRACTOR ON THIS PROJECT, WILL BE DELIVERED TO THE MITCHELL OPERATIONS YARD. PLEASE CONTACT STEVE WHEELER AT 259-1440 PRIOR TO DELIVERING.
22. DOWEL BARS SHALL BE MEASURED AND PAID FOR AS SPECIFIED IN SECTION 442 OF THE STANDARD SPECIFICATIONS.
23. THE INTENT OF BITUMINOUS SURFACE REMOVAL 2" IS TO REMOVE THE EXISTING OVERLAY PORTION TO ENABLE MAINLINE 1-55/70 WEST BOUND PAVEMENT RESURFACING PORTION TO MATCH EXISTING SHOULDERS AND RAMP. IT IS SUGGESTED THAT THE CONTRACTOR SCHEDULE FULL DEPTH SHOULDER REPAIR FROM STATION 478+50 TO 475+80 PRIOR TO OTHER WORK IN ORDER TO AID IN TRAFFIC CONTROL.
24. IT HAS BEEN ESTIMATED THAT 6120 TONS OF BITUMINOUS MATERIAL WILL BE REMOVED AT LOCATION 3. THIS MATERIAL SHALL BE DELIVERED TO AN AREA DESIGNATED BY THE IODT FIELD TECH.
25. LISTED BELOW ARE ENGLISH STATIONS FOR STRUCTURES ON THE SUBJECT PROJECT. EACH STATION IS LOCATED ON THE CENTERLINE OF THE BRIDGE OR BRIDGES AND THE CENTERLINE OF FAI-70 ENGLISH STATIONS ARE STAMPED IN THE PAVEMENT OF FAI-70.

LOCATION	ENGLISH
CANTEEN CREEK	377+52.70
BLACK LANE	416+30.00
I - 255	453+84.33
IL RTE. 157	514+14.34

26. TRAFFIC CONTROL COUNTERS EMBEDDED IN THE SUFRACE TO BE MILLED ARE AT 4 KNOWN LOCATIONS. SINCE THE EXACT DEPTH IS UNKNOWN, CONTRACTOR IS ADVISED THAT THE ONLY DETECTOR LOOPS TO BE RESTORED ARE CURRENTLY IN USE AT STATION 395+58 NEAR BLACK LANE. ALL OTHER LOOPS ARE TO BE ABANDONED IN PLACE. NO ADDITIONAL COMPENSATION WILL BE ALLOWED EXCEPT FOR THE DETECTOR LOOP REPLACEMENT QUANTITY FOR STATION 395+58.
27. ANY AREAS DISTURBED DUE TO CONSTRUCTION ACTIVITIES SHALL BE SEEDED (TEMPORARY & PERMANENT) AT THE CONTRACTOR'S EXPENSE AND NO OTHER COMPENSATION WILL BE ALLOWED.
28. SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE MILLED, PRIMED, AND FINAL SURFACE. AN AMOUNT OF TEMPORARY PAVEMENT MARKING EQUAL TO TWICE THE AMOUNT OF PERMANENT PAVEMENT MARKING HAS ALSO BEEN ADDED TO THE PLANS.
29. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSURE THAT ADJACENT PAVEMENT MARKING IS NOT DAMAGED DURING ANY OPERATIONS.
30. THE MILLING FROM LOCATION 3 SHALL BE DELIVERED TO TROY IODT YARD STORAGE SITE BEHIND FULL GOSPEL FELLOWSHIP CHURCH NEAR THE INTERSECTION OF IL 111 AND HORSESHOE LAKE ROAD. THE CONTRACTOR WILL NOT DRIVE OR TURN AROUND ON CHURCH PARKING LOT. CONTACT STAN RUDY AT (618) 667-0720 FOR MOR INFORMATION.
31. THE PURPOSE OF PARTIAL DEPTH REMOVAL 2" IS TO ACHIEVE A UNIFORM SURFACE. IF REBAR IS EXPOSED THEN MILLING SHOULD BE STOPPED.

MIXTURE USE	SMA*	TOP LIFT SHOULDERS	FULL DEPTH PATCH	BINDER
AC/PG	SBS PG 76-22	PG 58-22	PG 64-22	SBS PG 76-22
RA/FZ (MAX)		30%	10%	10%
DESIGN AIR VOIDS	4.0% @ Ndes=80 IL 12.5	**2.0% @ Ndes=30	4.0% @ Ndes= 105	4.0% @ Ndes=105
MIX COMPOSITION				IL 19.0
(GRADATION MIXTURE)	SMA			MIXTURE "B"
FRICITION AGG		BAM	MIXTURE "B"	

* AND PARTIAL DEPTH PATCHING

TRAFFIC CONTROL NOTES FOR LOCATION 1 & 2:

1. ALL TRAFFIC CONTROL DEVICES SHALL BE SKID MOUNTED.
2. "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE PLACED AT THE BEGINNING AND END OF THE PROJECT PLUS THE INTERSECTING SIDE ROADS, AND WILL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS. ALL CONSTRUCTION SIGNS SHALL BE FLUORESCENT ORANGE.
3. THE STAGING AND RAMP CLOSURES DETAILS AS SHOWN IN THE PLANS IS SUGGESTED. THE CONTRACTOR MAY SUBMIT IN WRITING A PROPOSED STAGE/RAMP CLOSURE CONSTRUCTION TRAFFIC CONTROL SEQUENCE MEETING THE TRAFFIC CONTROL STANDARDS, PLAN NOTES AND SPECIAL PROVISIONS IN THIS CONTRACT FOR APPROVAL BY THE DEPARTMENT.
4. FLAGMEN SHALL BE PRESENT DURING ALL CLOSURE HOURS, INCLUDING LUNCH HOUR, AND NO ADDITIONAL COMPENSATION SHALL BE APPLIED.
5. THE CONTRACTOR SHALL, AT THE ENGINEERS' DISCRESSION, RELOCATE THE CHANGEABLE MESSAGE SIGN AT THE VARIOUS RAMPS AS NEEDED, AMONG THE TWO LOCATIONS.

COMMITMENTS
 NO COMMITMENTS.

STANDARDS

000001	701301-02	781001-02
406001-04	701311-02	814001-01
406101-03	701400-02	886001
442001-03	701401-03	
442101-06	701406-04	
442201-02	701411-03	
482001-01	701426-02	
635001	701446	
701011-01	702001-06	
701101-01	720001	
701106-02	780001-01	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
INDEX OF SHEETS
 GENERAL NOTES
 FAI 255, FAI 55/70
 SECTION (60-7, 60-9)RS &
 60-(6-1,7-1)RS
 MADISON COUNTY

SCALE: VERT. DRAWN BY
 DATE HORIZ. CHECKED BY

Rev. Sheet 6-5-07

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	1000 LOCATION #1	1000 LOCATION #2	1000 LOCATION #3
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	74.7	29.2	24.5	21
40600215	POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT)	TON	74.5	19.2	34.3	21
40600300	AGGREGATE (PRIME COAT)	TON	326.6	116.5	118.7	91.4
40600895	CONSTRUCTING TEST STRIP	EACH	2			2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	371	188		183
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	2332	246	1873	213
40600990	TEMPORARY RAMP	SO YD	643	128	360	155
44000151	HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"	SO YD	4626		4626	
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	56878			56878
44004250	PAVED SHOULDER REMOVAL	SO YD	491			491
44200120	PAVEMENT PATCHING, TYPE II, 10 INCH	SO YD	131			131
44200565	CLASS A PATCHES, TYPE II, 11 INCH	SO YD	31		31	
44200569	CLASS A PATCHES, TYPE III, 11 INCH	SO YD	78		78	
44200571	CLASS A PATCHES, TYPE IV, 11 INCH	SO YD	659		659	
44200620	CLASS A PATCHES, TYPE II, 14 INCH	SO YD	39		39	
44200624	CLASS A PATCHES, TYPE III, 14 INCH	SO YD	54		54	
44200628	CLASS A PATCHES, TYPE IV, 14 INCH	SO YD	184		184	
44200966	CLASS B PATCHES, TYPE I, 10 INCH	SO YD	39	39		
44200970	CLASS B PATCHES, TYPE II, 10 INCH	SO YD	551	551		
44200974	CLASS B PATCHES, TYPE III, 10 INCH	SO YD	74	74		
44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SO YD	143	143		
44213000	PATCHING REINFORCEMENT	SO YD	1045		1045	
44213100	PAVEMENT FABRIC	SO YD	160	160		
44213200	SAW CUTS	FOOT	7580	4201	2873	506
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	3774	3774		
45100100	CRACK ROUTING (PAVEMENT)	FOOT	193580	95912	97668	
45100200	CRACK FILLING	POUND	71412	31471	39941	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	4476	858	3418	
48203005	HOT-MIX ASPHALT SHOULDERS, 2"	SO YD	2817	2817		
48203012	HOT-MIX ASPHALT SHOULDERS, 3 3/4"	SO YD	59168	16378	42790	
48203100	HOT-MIX ASPHALT SHOULDERS	TON	9166	2757	6228	181
60100080	FRENCH DRAINS	CU YD	20		20	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	3	3	6
67100100	MOBILIZATION	L SUM	1	0.3	0.3	0.4

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	1000 LOCATION #1	1000 LOCATION #2	1000 LOCATION #3
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	21	8	8	5
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	0.3	0.3	0.4
70100815	TRAFFIC CONTROL AND PROTECTION, STANDARD 701446	L SUM	1	0.3	0.3	0.4
70101805	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	EACH	5			5
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	26	1	1	24
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	14547	5904	4324	4319
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	140988	37287	61264	42437
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	2706	1266		1440
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	32077			32077
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2770			2770
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	94910	35987	58923	
* 78001140	PAINT PAVEMENT MARKING - LINE 8"	FOOT	3641	1300	2341	
78004210	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4"	FOOT	7590			7590
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2530	492	1266	772
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2530	492	1266	772
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	168			168
X0322729	MATERIAL TRANSFER DEVICE	TON	7983	369	1254	6360
X0324952	DETOUR SIGNING	L SUM	1	0.5	0.5	
X4066580	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	19,529	6527	6642	6360
△ 40603245	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N105	TON	11,863	2554	9153	155
△ X0325807	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) 1/2"	SO YD	92,914	20,273	72,641	
△ X0325808	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) 2"	SO YD	63,853	57,424	6,429	
X4421000	PARTIAL DEPTH PATCHING	TON	1214	194	80	940
X4422025	PARTIAL DEPTH REMOVAL 2"	SO YD	10827	1728	711	8388
Z0017202	DOWEL BARS 1 1/2"	EACH	1528	1528		
Z0075310	TIE BARS 3/4"	EACH	193		193	

PLOT DATE = 5/11/2007
 PLOT SCALE = 1/8" = 100'
 REFERENCE = SHEET #

*SPECIALTY ITEMS

△ Rev. 6-5-07

F.A.I. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SCHEDULE OF QUANTITIES - LOCATIONS #1 AND #2

CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
				I000 LOCATION #1	I000 LOCATION #2
	BITUMINOUS MATERIALS (PRIME COAT)	TON	6645	29.2	16.2
	AGGREGATE (PRIME COAT)	TON	195	116.5	78.5
	CONSTRUCTING TEST STRIP	EACH	2		
	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	188	188	
	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	2119	246	1873
	TEMPORARY RAMP	SQ YD	488	128	360
	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 19.0 N105	TON	8333	2554	5779
	CLASS A PATCHES, TYPE II, 11 INCH	SQ YD	31		31
	CLASS A PATCHES, TYPE III, 11 INCH	SQ YD	78		78
	CLASS A PATCHES, TYPE IV, 11 INCH	SQ YD	659		659
	CLASS A PATCHES, TYPE II, 14 INCH	SQ YD	39		39
	CLASS A PATCHES, TYPE III, 14 INCH	SQ YD	54		54
	CLASS A PATCHES, TYPE IV, 14 INCH	SQ YD	184		184
	CLASS B PATCHES, TYPE I, 10 INCH	SQ YD	39	39	
	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD	551	551	
	CLASS B PATCHES, TYPE III, 10 INCH	SQ YD	74	74	
	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD	143	143	
	PATCHING REINFORCEMENT	SQ YD	1045		1045
	PAVEMENT FABRIC	SQ YD	160	160	
	SAW CUTS	FOOT	7580	4201	2873
	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	3774	3774	
	CRACK ROUTING (PAVEMENT)	FOOT	193580	95912	97668
	CRACK FILLING	POUND	71412	31471	39941
	AGGREGATE SHOULDERS, TYPE B	TON	4476	858	3618
	HOT-MIX ASPHALT SHOULDERS	TON	8986	2757	6229
	FRENCH DRAINS	CU YD	20		20
	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	3	3
	MOBILIZATION	L SUM	.6	0.3	0.3

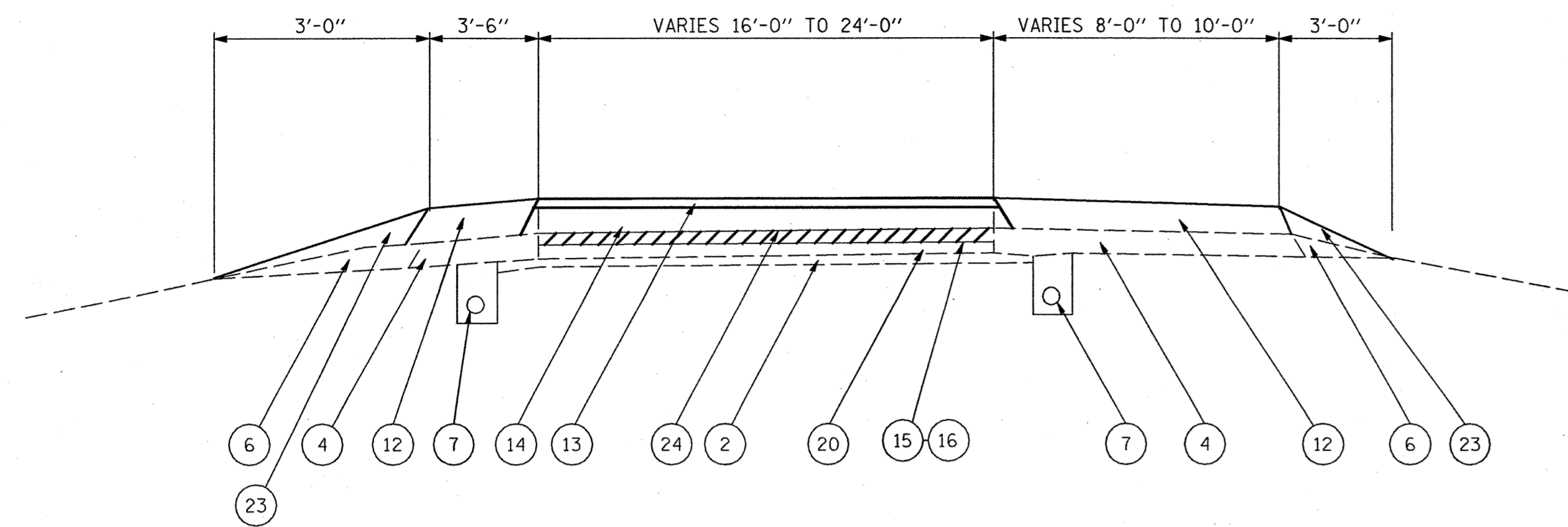
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
				I000 LOCATION #1	I000 LOCATION #2	I000 LOCATION #3
	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	16	8	8	
	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	0.3	0.3	
	TRAFFIC CONTROL AND PROTECTION, STANDARD 701446	L SUM	.6	0.3	0.3	
	CHANGEABLE MESSAGE SIGN	CAL MO	2	1	1	
	SHORT-TERM PAVEMENT MARKING	FOOT	10228	5904	4324	
	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	98551	37287	61264	
	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1266	1266		
	PAINT PAVEMENT MARKING - LINE 4"	FOOT	94910	35987	58923	
	PAINT PAVEMENT MARKING - LINE 8"	FOOT	3641	1300	2341	
	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1758	492	1266	
	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1758	492	1266	
	MATERIAL TRANSFER DEVICE	TON	1623	369	1254	
	DETOUR SIGNING	L SUM	1	0.5	0.5	
	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	10919	6527	4392	
	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 1/2"	SQ YD	66135	20273	45862	
	PORTLAND CEMENT CONCRETE SURFACE REMOVAL 2"	SQ YD	63853	57424	6429	
	PARTIAL DEPTH PATCHING	TON	274	194	80	
	PARTIAL DEPTH REMOVAL 2"	SQ YD	2439	1728	711	
	DOWEL BARS 1 1/2"	EACH	1528	1528		
	TIE BARS 3/4"	EACH	193		193	

PLOT DATE = 5/25/2007
 FILE NAME = c:\p\projects\4868307\oodd\qoh08307.dgn
 SCALE = 1/16"
 REFERENCE = #REF#

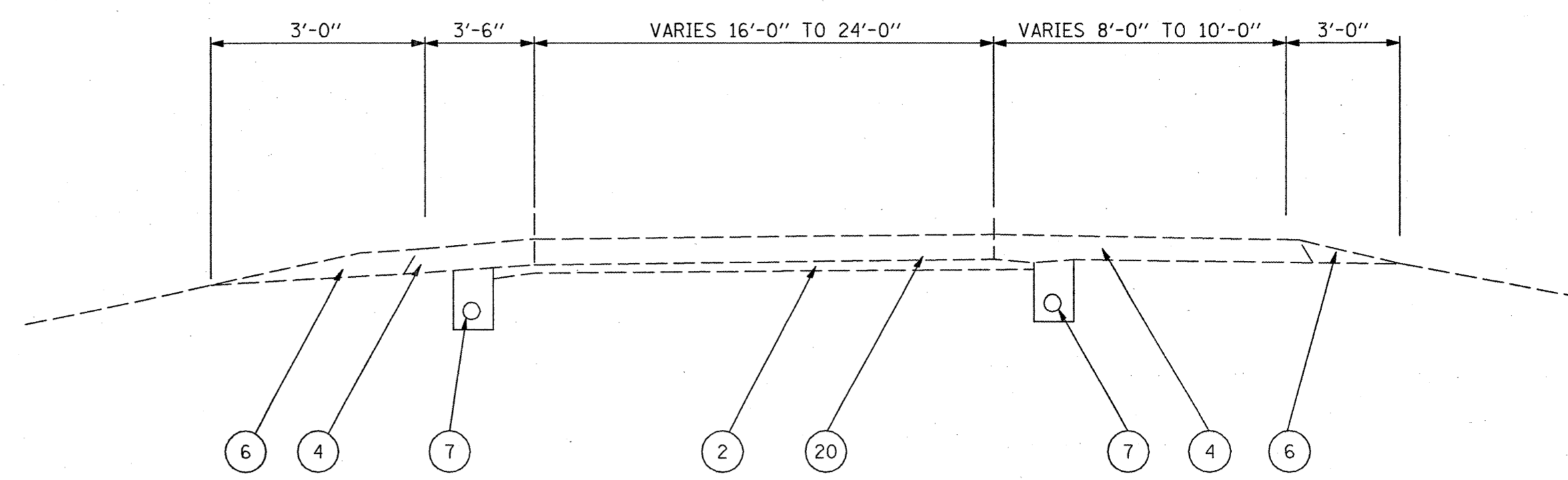
Rev. Sheet 6-5-07

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	7
STA. TO STA.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

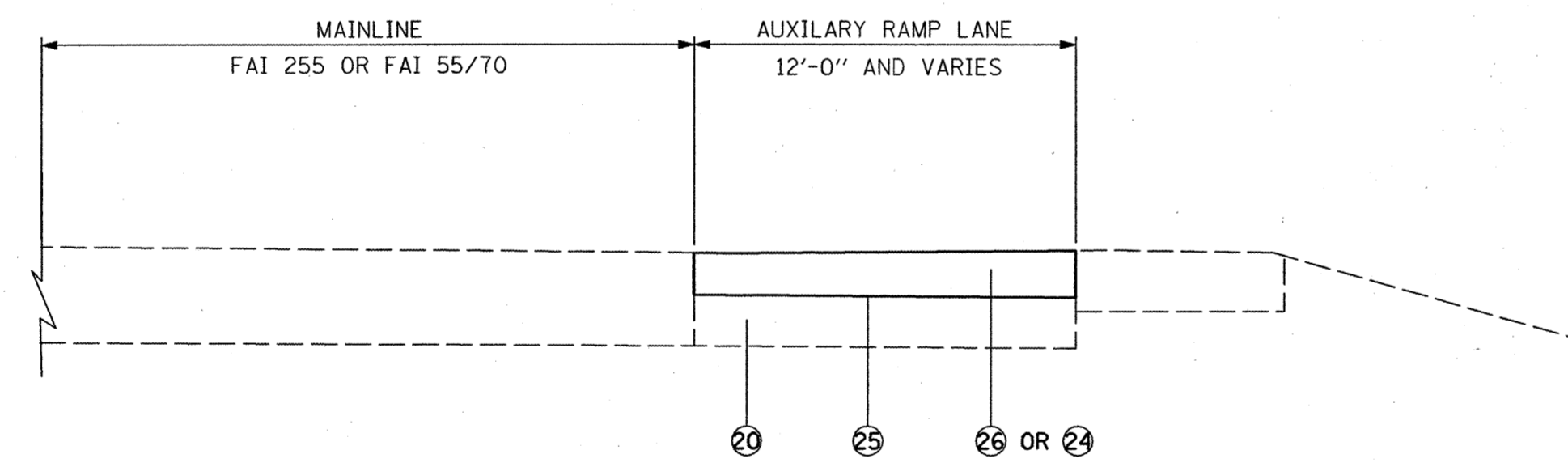
#-255-55/70
 ##60-7, 60-9IRS AND 60-16-1,7-1IRS



LOCATION #1
 TYPICAL SECTION - RAMPS 1 & 2
 STA. 101+70 TO STA. 162+00 (RAMP 1)
 STA. 200+00 TO STA. 224+00 (RAMP 2)
 STA. 310+00 TO STA. 315+55 (RAMP 1)
 STA. 400+00 TO STA. 403+00 (RAMP 4)



LOCATION #1
 TYPICAL SECTION - RAMPS 3,4,5,6, ROADWAY "A" & ROADWAY "B"
 STA. 301+00 TO STA. 310+00 (RAMP 3)
 STA. 403+00 TO STA. 414+16 (RAMP 4)
 STA. 500+00 TO STA. 519+40 (RAMP 5)
 STA. 600+40 TO STA. 619+420 (RAMP 6)
 STA. 699+90 TO STA. 738+77 (ROADWAY "A")
 STA. 800+00 TO STA. 807+04 (ROADWAY "B")



LOCATION #1
 TYPICAL SECTION - RAMPS 1 & 2
 STA. 161+40 TO STA. 1256+00 (RAMP 1)
 STA. 1274+00 TO STA. 200+00 (RAMP 2)
 STA. 224+00 TO STA. 492+00 (RAMP 2)

LEGEND

- 1 EXISTING CRPCC PAVEMENT - 10"
- 2 EXISTING STABILIZED SUB-BASE - 4"
- 3 EXISTING HOT- MIX ASPHALT SHOULDERS - 9"
- 4 EXISTING HOT- MIX ASPHALT SHOULDERS - 10"
- 5 EXISTING HOT- MIX ASPHALT SHOULDERS - 11"
- 6 EXISTING AGGREGATE SHOULDERS, TYPE B
- 7 EXISTING PIPE UNDERDRAINS
- 8 DELETED
- 9 EXISTING CRPCC PAVEMENT - 11"
- 10 DELETED
- 11 EXISTING HOT- MIX ASPHALT RESURFACING - 3"
- 12 PROPOSED HOT- MIX ASPHALT SHOULDERS - 3 3/4"
- 13 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, N80 - 1 1/2"
- 14 PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA, N80 - 2 1/4"
- 15 PROPOSED BITUMINOUS MATERIALS - PRIME COAT
- 16 PROPOSED AGGREGATE PRIME COAT
- 17 DELETED
- 18 DELETED
- 19 STABILIZED SUB-BASE 4"
- 20 EXISTING PCC PAVEMENT 10"
- 21 DELETED
- 22 DELETED
- 23 PROPOSED AGGREGATE SHOULDERS TYPE B
- 24 PROPOSED PCC SURFACE REM - 1/2"
- 25 PROPOSED PARTIAL DEPTH REMOVAL 2"
- 26 PROPOSED PARTIAL DEPTH PATCHING
- 27 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 1/2"
- 28 DELETED

LOCATION #1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
 FAI 255, FAI 55/70
 SECTION (60-7, 60-9)RS &
 60-(6-1,7-1)RS
 MADISON COUNTY
 SCALE: VERT. HORIZ.
 DATE: DRAWN BY: CHECKED BY:

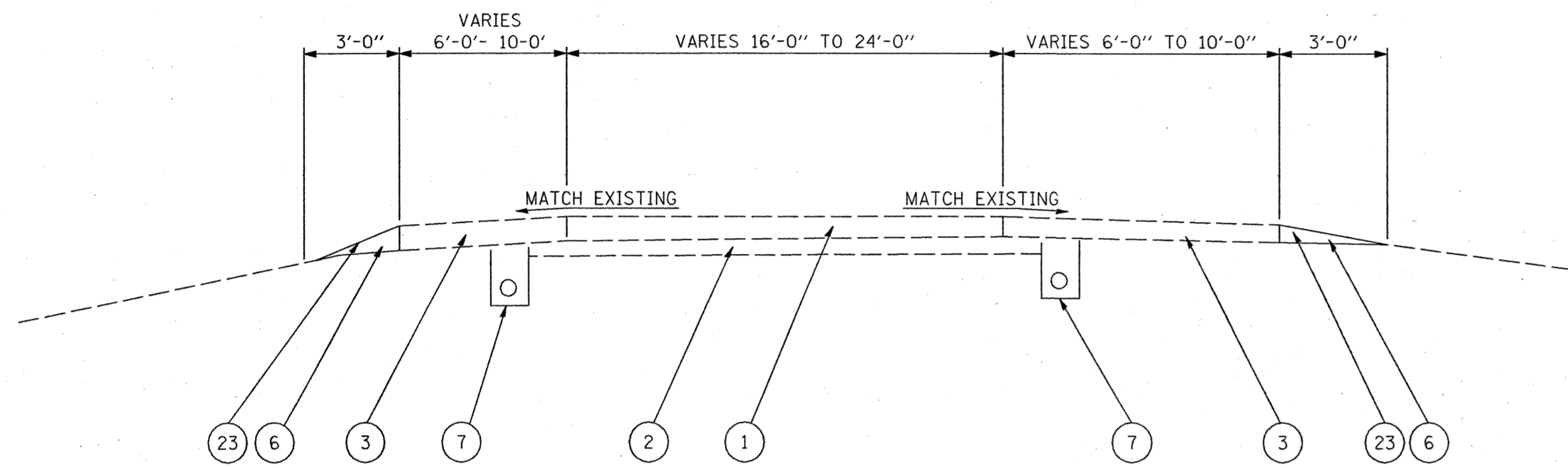
Rev. Sheet 6-5-07

PLOT DATE = 5/25/2007
 PLOT SCALE = 1/4" = 1'-0"
 REFERENCE = #REF#

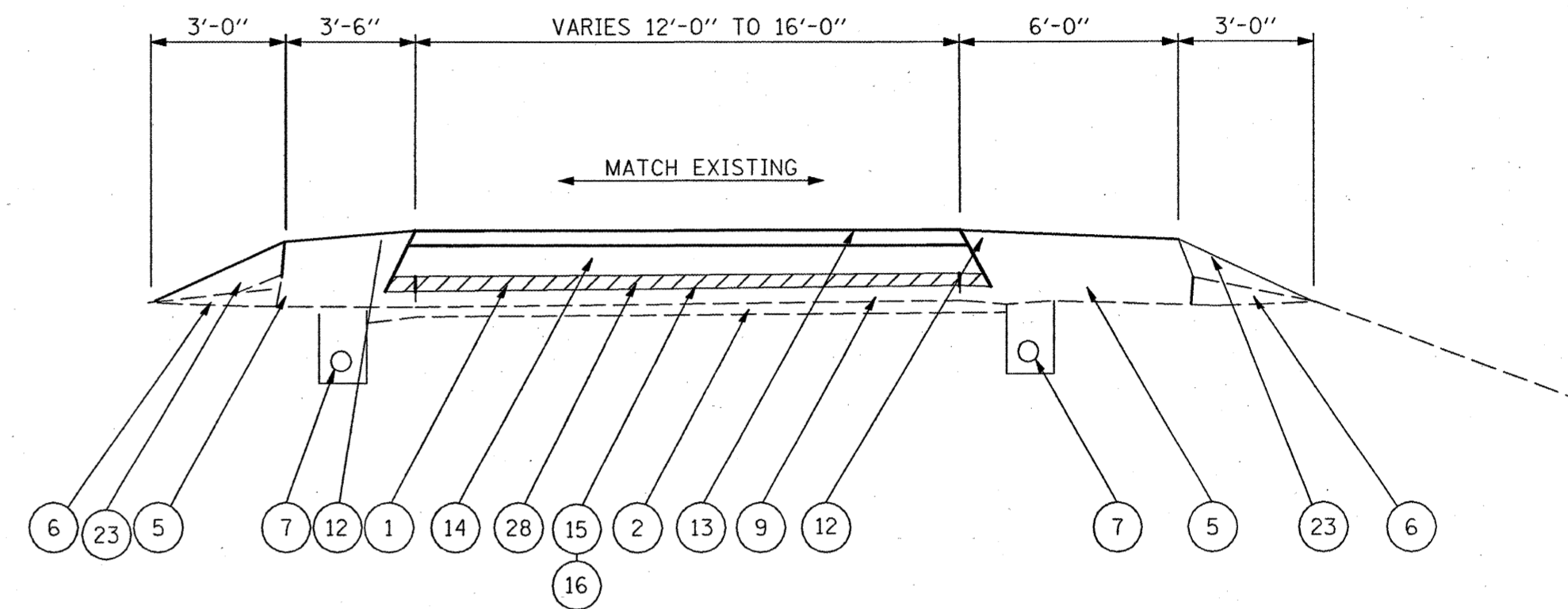
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	**	MADISON	37	8

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

•-255-55/70
 ••(60-7, 60-9)RS AND 60-(6-1,7-1)RS



LOCATION #2
 TYPICAL SECTION - RAMPS 1 & 7
 STA. 148+04 TO STA. 179+64 (RAMP 1)
 STA. 703+19 TO 722+55 (RAMP 7)



LOCATION #2
 TYPICAL SECTION - RAMP 3 & 7
 STA. 340+48 TO STA. 366+35 (RAMPS)
 STA. 703+16 TO STA. 712+50 (RAMPS)

LEGEND

- ① EXISTING CRPCC PAVEMENT - 10"
- ② EXISTING STABILIZED SUB-BASE - 4"
- ③ EXISTING HOT- MIX ASPHALT SHOULDERS - 9"
- ④ EXISTING HOT- MIX ASPHALT SHOULDERS - 10"
- ⑤ EXISTING HOT- MIX ASPHALT SHOULDERS - 11"
- ⑥ EXISTING AGGREGATE SHOULDERS, TYPE B
- ⑦ EXISTING PIPE UNDERDRAINS
- ⑧ DELETED
- ⑨ EXISTING CRPCC PAVEMENT - 11"
- ⑩ DELETED
- ⑪ EXISTING HOT- MIX ASPHALT RESURFACING - 3"
- ⑫ PROPOSED HOT- MIX ASPHALT SHOULDERS - 3 3/4"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, N80 - 1/2"
- ⑭ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA, N80 - 2/4"
- ⑮ PROPOSED BITUMINOUS MATERIALS - PRIME COAT
- ⑯ PROPOSED AGGREGATE PRIME COAT
- ⑰ DELETED
- ⑱ DELETED
- ⑲ STABILIZED SUB-BASE 4"
- ⑳ EXISTING PCC PAVEMENT 10"
- ㉑ DELETED
- ㉒ DELETED
- ㉓ PROPOSED AGGREGATE SHOULDERS TYPE B
- ㉔ PROPOSED PCC SURFACE REM - 1/2"
- ㉕ PROPOSED PARTIAL DEPTH REMOVAL 2"
- ㉖ PROPOSED PARTIAL DEPTH PATCHING
- ㉗ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 1/2"
- ㉘ DELETED

PLOT DATE = 5/25/2007
 FILE NAME = c:\25\2007\add\p\h08307.dgn
 PLOT SCALE = 50.0000 1/ IN.
 REFERENCE = *REF*

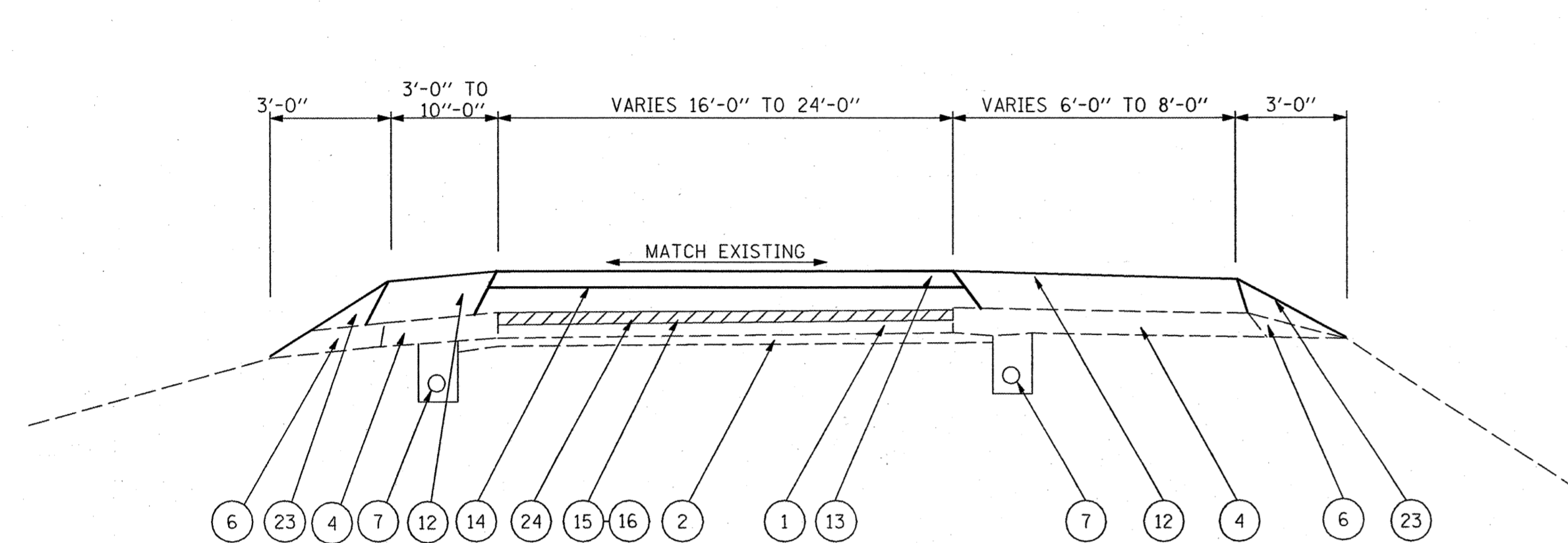
Rev. Sheet 6-5-07

REVISIONS	
NAME	DATE

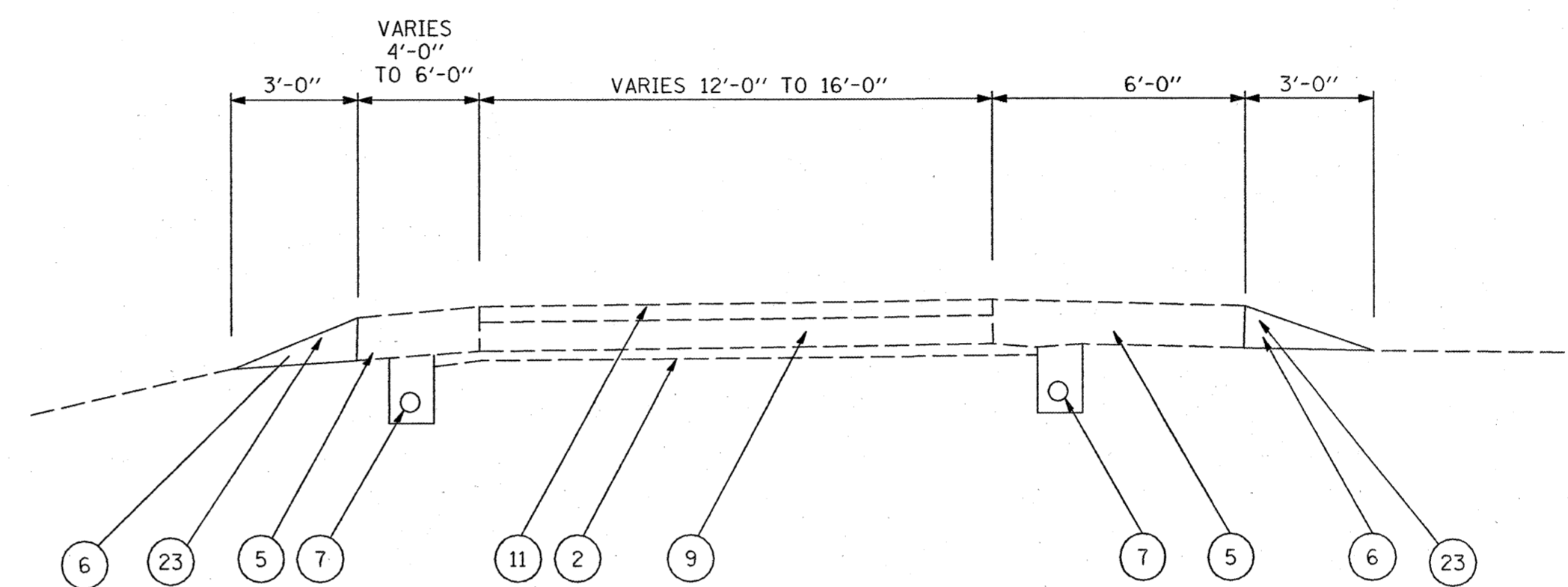
LOCATION #2
 ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
LOCATION #2
 FAI 255, FAI 55/70
 SECTION (60-7, 60-9)RS &
 60-(6-1,7-1)RS
 MADISON COUNTY
 SCALE: VERT. DRAWN BY
 HORIZ. CHECKED BY
 DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	9
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

*-255-55/70
 **60-7, 60-9)RS AND 60-(6-1,7-1)RS



LOCATION #2
 TYPICAL SECTION - RAMP 6 & 1
 STA. 693+63 TO STA. 760+00 (RAMP 6)
 STA. 1558+00 TO STA. 1593+65 (RAMP 6)
 STA. 1564+00 TO STA. 1589+00 (RAMP 1)



LOCATION #2
 TYPICAL SECTION - RAMPS 2, 4, 5, & 8
 STA. 212+58 TO STA. 231+00 (RAMP 2)
 STA. 462+62 TO STA. 485+46 (RAMP 4)
 STA. 580+00 TO STA. 613+93 (RAMP 5)
 STA. 872+40 TO STA. 885+00 (RAMP 8)

LEGEND

- 1 EXISTING CRPCC PAVEMENT - 10"
- 2 EXISTING STABILIZED SUB-BASE - 4"
- 3 EXISTING HOT- MIX ASPHALT SHOULDERS - 9"
- 4 EXISTING HOT- MIX ASPHALT SHOULDERS - 10"
- 5 EXISTING HOT- MIX ASPHALT SHOULDERS - 11"
- 6 EXISTING AGGREGATE SHOULDERS, TYPE B
- 7 EXISTING PIPE UNDERDRAINS
- 8 DELETED
- 9 EXISTING CRPCC PAVEMENT - 11"
- 10 DELETED
- 11 EXISTING HOT- MIX ASPHALT RESURFACING - 3"
- 12 PROPOSED HOT- MIX ASPHALT SHOULDERS - 3 3/4"
- 13 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, N80 - 1 1/2"
- 14 PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA, N80 - 2 1/4"
- 15 PROPOSED BITUMINOUS MATERIALS - PRIME COAT
- 16 PROPOSED AGGREGATE PRIME COAT
- 17 DELETED
- 18 DELETED
- 19 STABILIZED SUB-BASE 4"
- 20 EXISTING PCC PAVEMENT 10"
- 21 DELETED
- 22 DELETED
- 23 PROPOSED AGGREGATE SHOULDERS TYPE B
- 24 PROPOSED PCC SURFACE REM - 1/2"
- 25 PROPOSED PARTIAL DEPTH REMOVAL 2"
- 26 PROPOSED PARTIAL DEPTH PATCHING
- 27 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 1/2"
- 28 DELETED

LOCATION #2

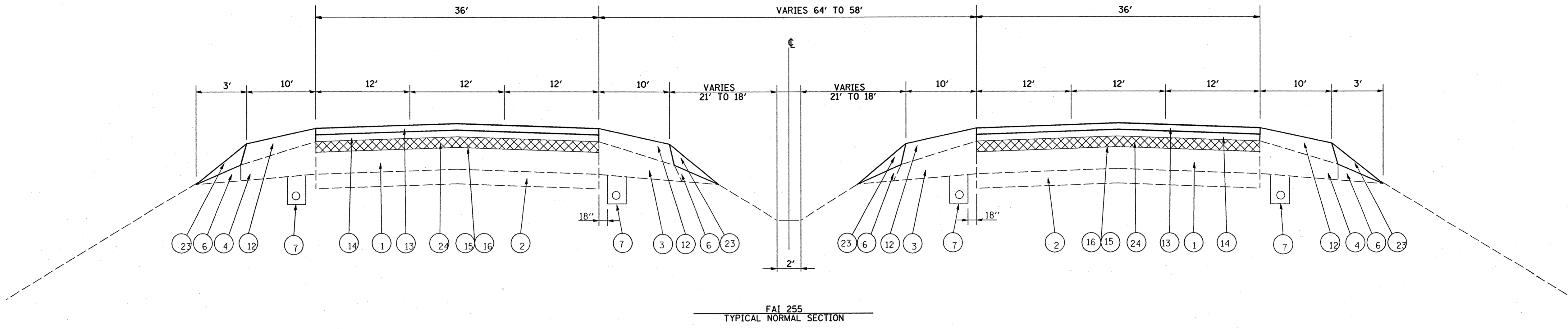
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 LOCATION #2
 FAI 255, FAI 55/70
 SECTION 60-7, 60-9)RS &
 60-(6-1,7-1)RS
 MADISON COUNTY

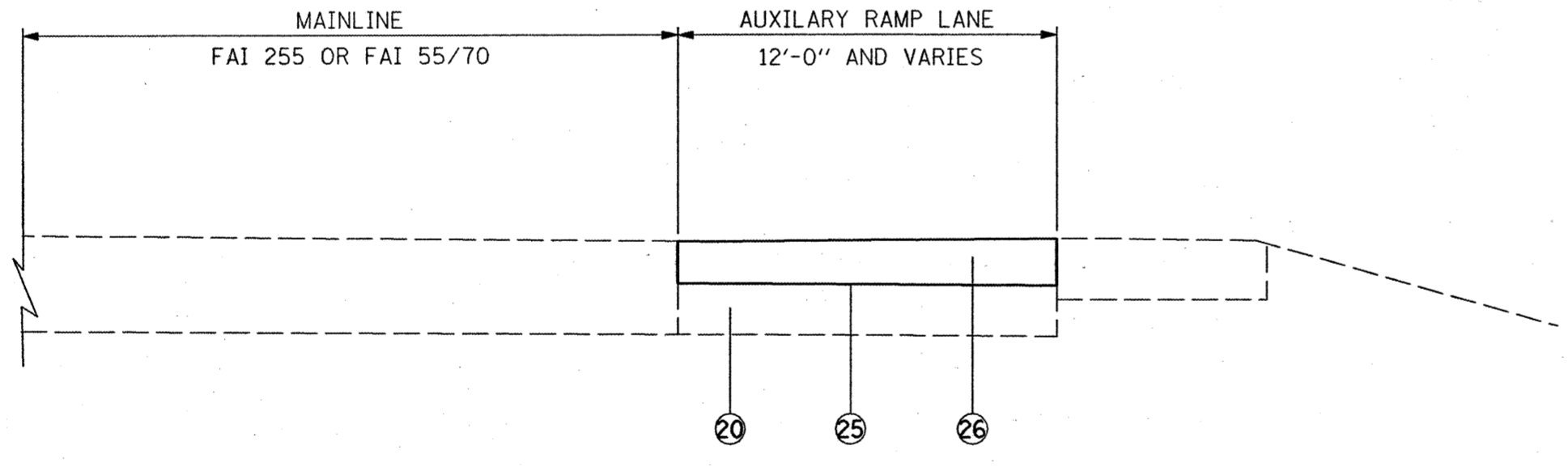
SCALE: VERT. _____
 HORIZ. _____
 DATE _____ DRAWN BY _____
 CHECKED BY _____

Rev. Sheet 6-5-07

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	MADISON	37	10
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*-255-55/70				
***(60-7, 60-9)RS AND 60-(6-1,7-1)RS				



F.A.I. 255
TYPICAL NORMAL SECTION
I-255 (NB) STA. 1556+00 TO STA. 1598+00
I-255 (SB) STA. 1556+00 TO STA. 1614+52



LOCATION #2
TYPICAL SECTION - RAMP 2
STA. 760+00 TO STA. 792+00 (RAMP 6)

LEGEND

- ① EXISTING CRPCC PAVEMENT - 10"
- ② EXISTING STABILIZED SUB-BASE - 4"
- ③ EXISTING HOT- MIX ASPHALT SHOULDERS - 9"
- ④ EXISTING HOT- MIX ASPHALT SHOULDERS - 10"
- ⑤ EXISTING HOT- MIX ASPHALT SHOULDERS - 11"
- ⑥ EXISTING AGGREGATE SHOULDERS, TYPE B
- ⑦ EXISTING PIPE UNDERDRAINS
- ⑧ DELETED
- ⑨ EXISTING CRPCC PAVEMENT - 11"
- ⑩ DELETED
- ⑪ EXISTING HOT- MIX ASPHALT RESURFACING - 3"
- ⑫ PROPOSED HOT- MIX ASPHALT SHOULDERS - 3 3/4"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, N80 - 1 1/2"
- ⑭ PROPOSED POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA, N80 - 2 1/4"
- ⑮ PROPOSED BITUMINOUS MATERIALS - PRIME COAT
- ⑯ PROPOSED AGGREGATE PRIME COAT
- ⑰ DELETED
- ⑱ DELETED
- ⑲ STABILIZED SUB-BASE 4"
- ⑳ EXISTING PCC PAVEMENT 10"
- ㉑ DELETED
- ㉒ DELETED
- ㉓ PROPOSED AGGREGATE SHOULDERS TYPE B
- ㉔ PROPOSED PCC SURFACE REM - 1/2"
- ㉕ PROPOSED PARTIAL DEPTH REMOVAL 2"
- ㉖ PROPOSED PARTIAL DEPTH PATCHING
- ㉗ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL - 1/2"
- ㉘ DELETED

LOCATION #2

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
LOCATION #2
FAI 255, FAI 55/70
SECTION (60-7, 60-9)RS &
60-(6-1,7-1)RS
MADISON COUNTY

REVISIONS	
NAME	DATE

SCALE: VERT. HORIZ.
DATE
DRAWN BY
CHECKED BY

Rev. Sheet 6-5-07

PLOT DATE= 5/25/2007
FILE NAME = c:\pcc\loc\tsf\loc\08307\loc\08307a.dgn
PLOT SCALE= 50.0000 / 1" = 100'
USER NAME= challandeske

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	11
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
*255-55/70				
**60-7, 60-9)RS AND 60-(6-1,7-1)RS				

PARTIAL DEPTH PATCHING - LOCATION 1 (RAMP 2)							
STATION	LENGTH	WIDTH	AREA	PARTIAL DEPTH REMOVAL, 2"	PARTIAL DEPTH PATCHING	BITUMINOUS MATERIAL (PRIME COAT)	
	FOOT	FOOT	SQ YD	SQ YD	TON	TON	
200+34	2	2	0.4	0.4	0.0	0.0001	
200+69	4	5	2.2	2.2	0.2	0.0007	
200+83	16	2	3.6	3.6	0.4	0.0011	
201+00	16	4	7.1	7.1	0.8	0.0022	
20+18	5	2	1.1	1.1	0.1	0.0003	
201+61	14	2	3.1	3.1	0.3	0.0010	
201+61	5	2	1.1	1.1	0.1	0.0003	
201+61	10	2	2.2	2.2	0.2	0.0007	
202+00	14	2	3.1	3.1	0.3	0.0010	
202+06	4	4	1.8	1.8	0.2	0.0006	
202+19	4	2	0.9	0.9	0.1	0.0003	
202+38	3	2	0.7	0.7	0.1	0.0002	
202+47	5	2	1.1	1.1	0.1	0.0003	
202+75	6	4	2.7	2.7	0.3	0.0008	
203+16	6	2	1.3	1.3	0.1	0.0004	
203+39	8	2	1.8	1.8	0.2	0.0006	
203+59	10	2	2.2	2.2	0.2	0.0007	
203+78	4	4	1.8	1.8	0.2	0.0006	
204+18	4	4	1.8	1.8	0.2	0.0006	
205+36	7	3	2.3	2.3	0.3	0.0007	
207+13	3	3	1.0	1.0	0.1	0.0003	
208+52	7	6	4.7	4.7	0.5	0.0015	
208+94	7	5	3.9	3.9	0.4	0.0012	
209+87	9	4	4.0	4.0	0.4	0.0013	
210+23	4	4	1.8	1.8	0.2	0.0006	
210+50	6	2	1.3	1.3	0.1	0.0004	
210+72	8	2	1.8	1.8	0.2	0.0006	
210+91	8	6	5.3	5.3	0.6	0.0017	
211+72	4	2	0.9	0.9	0.1	0.0003	
214+84	12	2	2.7	2.7	0.3	0.0008	
216+15	8	2	1.8	1.8	0.2	0.0006	
218+82	5	6	3.3	3.3	0.4	0.0010	
225+19	10	3	3.3	3.3	0.4	0.0010	
230+96	7	2	1.6	1.6	0.2	0.0005	
RAMP 2 TOTAL				79.7	8.9	0.02	

PARTIAL DEPTH PATCHING - LOCATION 1 (RAMP 3)							
STATION	LENGTH	WIDTH	AREA	PARTIAL DEPTH REMOVAL, 2"	PARTIAL DEPTH PATCHING	BITUMINOUS MATERIAL (PRIME COAT)	
	FOOT	FOOT	SQ YD	SQ YD	TON	TON	
293+97	4	2	0.9	0.9	0.1	0.0003	
294+46	100	2	22.2	22.2	2.5	0.0070	
294+58	8	2	1.8	1.8	0.2	0.0006	
294+80	6	2	1.3	1.3	0.1	0.0004	
295+36	2	2	0.4	0.4	0.0	0.0001	
296+40	10	2	2.2	2.2	0.2	0.0007	
296+80	10	2	2.2	2.2	0.2	0.0007	
297+22	4	2	0.9	0.9	0.1	0.0003	
297+39	10	2	2.2	2.2	0.2	0.0007	
297+62	6	2	1.3	1.3	0.1	0.0004	
297+85	16	2	3.6	3.6	0.4	0.0011	
298+43	10	2	2.2	2.2	0.2	0.0007	
298+43	8	2	1.8	1.8	0.2	0.0006	
299+23	16	2	3.6	3.6	0.4	0.0011	
299+36	16	2	3.6	3.6	0.4	0.0011	
300+03	6	2	1.3	1.3	0.1	0.0004	
300+69	6	2	1.3	1.3	0.1	0.0004	
307+36	6	2	1.3	1.3	0.1	0.0004	
308+55	6	2	1.3	1.3	0.1	0.0004	
309+74	8	2	1.8	1.8	0.2	0.0006	
313+77	8	2	1.8	1.8	0.2	0.0006	
314+40	16	2	3.6	3.6	0.4	0.0011	
315+24	16	2	3.6	3.6	0.4	0.0011	
315+46	16	2	3.6	3.6	0.4	0.0011	
315+60	6	2	1.3	1.3	0.1	0.0004	
316+04	6	2	1.3	1.3	0.1	0.0004	
316+44	16	2	3.6	3.6	0.4	0.0011	
316+67	2	2	0.4	0.4	0.0	0.0001	
316+84	8	2	1.8	1.8	0.2	0.0006	
317+20	6	2	1.3	1.3	0.1	0.0004	
317+66	14	2	3.1	3.1	0.3	0.0010	
317+82	6	2	1.3	1.3	0.1	0.0004	
317+95	2	2	0.4	0.4	0.0	0.0001	
318+03	12	2	2.7	2.7	0.3	0.0008	
318+41	16	2	3.6	3.6	0.4	0.0011	
318+62	8	2	1.8	1.8	0.2	0.0006	
318+79	12	2	2.7	2.7	0.3	0.0008	
318+82	2	2	0.4	0.4	0.0	0.0001	
319+17	16	2	3.6	3.6	0.4	0.0011	
319+17	8	2	1.8	1.8	0.2	0.0006	
319+56	16	2	3.6	3.6	0.4	0.0011	
319+77	2	2	0.4	0.4	0.0	0.0001	
319+94	16	2	3.6	3.6	0.4	0.0011	
320+32	16	2	3.6	3.6	0.4	0.0011	
320+70	12	2	2.7	2.7	0.3	0.0008	
321+09	2	2	0.4	0.4	0.0	0.0001	
321+25	12	2	2.7	2.7	0.3	0.0008	
321+35	16	2	3.6	3.6	0.4	0.0011	
321+47	6	2	1.3	1.3	0.1	0.0004	
321+67	20	2	4.4	4.4	0.5	0.0014	
321+75	10	2	2.2	2.2	0.2	0.0007	
321+85	10	2	2.2	2.2	0.2	0.0007	
321+85	30	2	6.7	6.7	0.7	0.0021	
322+24	10	2	2.2	2.2	0.2	0.0007	
322+42	20	2	4.4	4.4	0.5	0.0014	
322+62	8	2	1.8	1.8	0.2	0.0006	
322+80	40	2	8.9	8.9	1.0	0.0028	
323+00	8	2	1.8	1.8	0.2	0.0006	
323+31	6	2	1.3	1.3	0.1	0.0004	
323+31	20	2	4.4	4.4	0.5	0.0014	
323+62	10	2	2.2	2.2	0.2	0.0007	
323+78	10	2	2.2	2.2	0.2	0.0007	
324+17	4	2	0.9	0.9	0.1	0.0003	
324+57	4	2	0.9	0.9	0.1	0.0003	
324+97	2	2	0.4	0.4	0.0	0.0001	
RAMP 3 TOTAL				138.7	15.5	0.04	

PARTIAL DEPTH PATCHING - LOCATION 1 CONTINUED (RAMP 5)							
STATION	LENGTH	WIDTH	AREA	PARTIAL DEPTH REMOVAL, 2"	PARTIAL DEPTH PATCHING	BITUMINOUS MATERIAL (PRIME COAT)	
	FOOT	FOOT	SQ YD	SQ YD	TON	TON	
500+00	16	2	3.6	3.6	0.4	0.0011	
500+55	16	2	3.6	3.6	0.4	0.0011	
500+95	6	2	1.3	1.3	0.1	0.0004	
501+10	8	2	1.8	1.8	0.2	0.0006	
501+21	4	2	0.9	0.9	0.1	0.0003	
501+36	16	2	3.6	3.6	0.4	0.0011	
501+79	16	2	3.6	3.6	0.4	0.0011	
502+15	16	2	3.6	3.6	0.4	0.0011	
502+38	8	2	1.8	1.8	0.2	0.0006	
502+53	16	2	3.6	3.6	0.4	0.0011	
502+66	16	2	3.6	3.6	0.4	0.0011	
503+18	8	2	1.8	1.8	0.2	0.0006	
503+36	8	2	1.8	1.8	0.2	0.0006	
503+56	8	2	1.8	1.8	0.2	0.0006	
503+77	16	2	3.6	3.6	0.4	0.0011	
503+98	8	2	1.8	1.8	0.2	0.0006	
504+15	16	2	3.6	3.6	0.4	0.0011	
504+33	6	2	1.3	1.3	0.1	0.0004	
504+50	16	2	3.6	3.6	0.4	0.0011	
504+73	8	2	1.8	1.8	0.2	0.0006	
504+90	6	2	1.3	1.3	0.1	0.0004	
505+16	14	2	3.1	3.1	0.3	0.0010	
505+31	16	2	3.6	3.6	0.4	0.0011	
505+48	16	2	3.6	3.6	0.4	0.0011	
505+67	16	2	3.6	3.6	0.4	0.0011	
505+85	16	2	3.6	3.6	0.4	0.0011	
506+06	16	2	3.6	3.6	0.4	0.0011	
506+22	16	2	3.6	3.6	0.4	0.0011	
506+59	16	2	3.6	3.6	0.4	0.0011	
506+79	16	2	3.6	3.6	0.4	0.0011	
506+97	6	2	1.3	1.3	0.1	0.0004	
507+15	16	2	3.6	3.6	0.4	0.0011	
507+35	10	2	2.2	2.2	0.2	0.0007	
507+35	20	2	4.4	4.4	0.5	0.0014	
507+73	16	2	3.6	3.6	0.4	0.0011	
508+12	16	2	3.6	3.6	0.4	0.0011	
508+50	16	2	3.6	3.6	0.4	0.0011	
508+87	16	2	3.6	3.6	0.4	0.0011	
509+27	16	2	3.6	3.6	0.4	0.0011	
509+65	16	2	3.6	3.6	0.4	0.0011	
510+03	10	2	2.2	2.2	0.2	0.0007	
510+41	16	2	3.6	3.6	0.4	0.0011	
510+79	16	2	3.6	3.6	0.4	0.0011	
510+96	6	2	1.3	1.3	0.1	0.0004	
511+17	16	2	3.6	3.6	0.4	0.0011	
511+27	20	2	4.4	4.4	0.5	0.0014	
511+55	16	2	3.6	3.6	0.4	0.0011	
511+72	2	2	0.4	0.4	0.0	0.0001	
511+94	16	2	3.6	3.6	0.4	0.0011	
512+31	12	2	2.7	2.7	0.3	0.0008	
512+55	16	2	3.6	3.6	0.4	0.0011	
512+70	12	2	2.7	2.7	0.3	0.0008	
512+91	16	2	3.6	3.6	0.4	0.0011	
513+09	16	2	3.6	3.6	0.4	0.0011	
513+31	6	2	1.3	1.3	0.1	0.0004	
513+47	16	2	3.6	3.6	0.4	0.0011	
513+64	16	2	3.6	3.6	0.4	0.0011	
513+85	12	2	2.7	2.7	0.3	0.0008	
514+23	16	2	3.6	3.6	0.4	0.0011	
514+45	12	2	2.7	2.7	0.3	0.0008	
514+61	14	2	3.1	3.1	0.3	0.0010	
514+96	10	2	2.2	2.2	0.2	0.0007	
515+17	16	2	3.6	3.6	0.4	0.0011	
515+31	16	2	3.6	3.6	0.4	0.0011	
515+64	4	2	0.9	0.9	0.1	0.0003	
516+00	6	2	1.3	1.3	0.1	0.0004	
516+35	12	2	2.7	2.7	0.3	0.0008	
516+56	2	2	0.4	0.4	0		

E.A.I. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	**	MADISON	37	12

TO STA. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

255-55/70 60-7, 60-9)RS AND 60-16-1,7-1)RS

PARTIAL DEPTH PATCHING - LOCATION 1 CONTINUED (RAMP 6)

STATION	LENGTH	WIDTH	AREA		PARTIAL DEPTH		BITUMINOUS MATERIAL (PRIME COAT)
			FOOT	SO YD	REMOVAL, 2"	PATCHING	
600+12	16	2	3.6	3.6	0.2	0.0011	
600+33	4	2	0.9	0.9	0.0	0.0003	
600+45	2	2	0.4	0.4	0.0	0.0001	
600+52	16	2	3.6	3.6	0.2	0.0011	
600+82	16	2	3.6	3.6	0.2	0.0011	
601+11	16	2	3.6	3.6	0.2	0.0011	
601+42	12	2	2.7	2.7	0.1	0.0008	
601+82	14	2	3.1	3.1	0.2	0.0010	
601+97	2	2	0.4	0.4	0.0	0.0001	
602+20	16	2	3.6	3.6	0.2	0.0011	
602+80	16	2	3.6	3.6	0.2	0.0011	
603+85	8	2	1.8	1.8	0.1	0.0006	
603+96	16	2	3.6	3.6	0.2	0.0011	
604+26	8	2	1.8	1.8	0.1	0.0006	
604+36	16	2	3.6	3.6	0.2	0.0011	
604+45	2	2	0.4	0.4	0.0	0.0001	
604+52	2	2	0.4	0.4	0.0	0.0001	
604+72	16	2	3.6	3.6	0.2	0.0011	
605+09	16	2	3.6	3.6	0.2	0.0011	
605+42	16	2	3.6	3.6	0.2	0.0011	
605+75	10	2	2.2	2.2	0.1	0.0007	
605+83	16	2	3.6	3.6	0.2	0.0011	
606+17	16	2	3.6	3.6	0.2	0.0011	
606+52	16	2	3.6	3.6	0.2	0.0011	
606+91	6	2	1.3	1.3	0.1	0.0004	
607+00	16	2	3.6	3.6	0.2	0.0011	
607+32	16	2	3.6	3.6	0.2	0.0011	
607+66	16	2	3.6	3.6	0.2	0.0011	
607+72	2	2	0.4	0.4	0.0	0.0001	
608+12	16	2	3.6	3.6	0.2	0.0011	
608+49	16	2	3.6	3.6	0.2	0.0011	
608+55	2	2	0.4	0.4	0.0	0.0001	
608+87	16	2	3.6	3.6	0.2	0.0011	
609+21	16	2	3.6	3.6	0.2	0.0011	
609+56	32	2	7.1	7.1	0.4	0.0022	
609+59	16	2	3.6	3.6	0.2	0.0011	
609+97	8	2	1.8	1.8	0.1	0.0006	
610+00	16	2	3.6	3.6	0.2	0.0011	
610+58	8	2	1.8	1.8	0.1	0.0006	
611+08	24	2	5.3	5.3	0.3	0.0017	
611+17	10	2	2.2	2.2	0.1	0.0007	
611+29	10	2	2.2	2.2	0.1	0.0007	
612+34	16	2	3.6	3.6	0.2	0.0011	
612+41	2	2	0.4	0.4	0.0	0.0001	
613+25	8	2	1.8	1.8	0.1	0.0006	
613+64	8	2	1.8	1.8	0.1	0.0006	
614+18	6	2	1.3	1.3	0.1	0.0004	
614+26	16	2	3.6	3.6	0.2	0.0011	
615+55	4	2	0.9	0.9	0.0	0.0003	
615+93	3	2	0.7	0.7	0.0	0.0002	
616+12	6	2	1.3	1.3	0.1	0.0004	
616+32	9	2	2.0	2.0	0.1	0.0006	
617+09	2	2	0.4	0.4	0.0	0.0001	
617+49	2	2	0.4	0.4	0.0	0.0001	
618+06	16	2	3.6	3.6	0.2	0.0011	
618+32	12	2	2.7	2.7	0.1	0.0008	
618+40	6	2	1.3	1.3	0.1	0.0004	
619+19	2	2	0.4	0.4	0.0	0.0001	
619+29	6	2	1.3	1.3	0.1	0.0004	
619+35	16	2	3.6	3.6	0.2	0.0011	
619+42	16	2	3.6	3.6	0.2	0.0011	
RAMP 6 TOTAL				154.7	8.7	0.05	

PARTIAL DEPTH PATCHING - LOCATION 1 CONTINUED (RDWY "A")

STATION	T/RT	LENGTH	WIDTH	AREA		PARTIAL DEPTH		BITUMINOUS MATERIAL (PRIME COAT)	STATION	T/RT	LENGTH	WIDTH	AREA		PARTIAL DEPTH		BITUMINOUS MATERIAL (PRIME COAT)	STATION	T/RT	LENGTH	WIDTH	AREA		PARTIAL DEPTH		BITUMINOUS MATERIAL (PRIME COAT)
				FOOT	SO YD	REMOVAL, 2"	PATCHING						FOOT	SO YD	REMOVAL, 2"	PATCHING						FOOT	SO YD	REMOVAL, 2"	PATCHING	
699+76	RT	30	2	6.7	6.7	0.4	0.0021	722+89	RT	12	2	2.7	2.7	0.1	0.0008	734+56	LT	2	2	0.4	0.4	0.0	0.0001			
699+92	RT	12	2	2.7	2.7	0.1	0.0008	723+09	LT	2	2	0.4	0.4	0.0	0.0001	734+56	RT	12	2	2.7	2.7	0.1	0.0008			
699+99	RT	6	2	1.3	1.3	0.1	0.0004	723+27	RT	6	2	1.3	1.3	0.1	0.0004	734+70	RT	8	2	1.8	1.8	0.1	0.0006			
700+10	LT	8	2	1.8	1.8	0.1	0.0006	724+50	RT	12	2	2.7	2.7	0.1	0.0008	734+80	RT	6	2	1.3	1.3	0.1	0.0004			
700+24	RT	15	2	3.3	3.3	0.2	0.0010	724+67	RT	4	2	0.9	0.9	0.0	0.0003	734+95	LT	8	2	1.8	1.8	0.1	0.0006			
700+31	RT	12	2	2.7	2.7	0.1	0.0008	725+07	LT	8	2	1.8	1.8	0.1	0.0006	734+95	RT	6	2	1.3	1.3	0.1	0.0004			
700+42	RT	3	2	0.7	0.7	0.0	0.0002	725+25	RT	6	2	1.3	1.3	0.1	0.0004	735+34	LT	6	2	1.3	1.3	0.1	0.0004			
700+55	RT	4	2	0.9	0.9	0.0	0.0003	725+47	LT	12	2	2.7	2.7	0.1	0.0008	735+34	RT	2	2	0.4	0.4	0.0	0.0001			
700+70	RT	12	2	2.7	2.7	0.1	0.0008	725+47	RT	4	2	0.9	0.9	0.0	0.0003	735+70	LT	8	2	1.8	1.8	0.1	0.0006			
702+00	RT	12	2	2.7	2.7	0.1	0.0008	726+10	RT	15	2	3.3	3.3	0.2	0.0010	735+70	RT	3	2	0.7	0.7	0.0	0.0002			
702+16	RT	12	2	2.7	2.7	0.1	0.0008	726+26	LT	2	2	0.4	0.4	0.0	0.0001	736+00	LT	12	2	2.7	2.7	0.1	0.0008			
702+16	LT	12	2	2.7	2.7	0.1	0.0008	726+26	RT	2	2	0.4	0.4	0.0	0.0001	736+00	RT	13	2	2.9	2.9	0.2	0.0009			
702+26	LT	12	2	2.7	2.7	0.1	0.0008	726+49	RT	60	2	13.3	13.3	0.7	0.0042	736+20	LT	2	2	0.4	0.4	0.0	0.0001			
702+31	LT	2	2	0.4	0.4	0.0	0.0001	726+65	LT	12	2	2.7	2.7	0.1	0.0008	736+20	RT	10	2	2.2	2.2	0.1	0.0007			
702+39	RT	12	2	2.7	2.7	0.1	0.0008	726+65	RT	6	2	1.3	1.3	0.1	0.0004	736+20	RT	10	2	2.2	2.2	0.1	0.0007			
702+62	RT	16	2	3.6	3.6	0.2	0.0011	726+81	RT	40	2	8.9	8.9	0.5	0.0028	736+33	RT	6	2	1.3	1.3	0.1	0.0004			
702+79	RT	12	2	2.7	2.7	0.1	0.0008	727+06	LT	10	2	2.2	2.2	0.1	0.0007	736+45	RT	6	2	1.3	1.3	0.1	0.0004			
702+79	LT	4	2	0.9	0.9	0.0	0.0003	727+06	RT	6	2	1.3	1.3	0.1	0.0004	736+59	LT	4	2	0.9	0.9	0.0	0.0003			
702+99	RT	12	2	2.7	2.7	0.1	0.0008	727+25	RT	30	2	6.7	6.7	0.4	0.0021	736+59	RT	2	2	0.4	0.4	0.0	0.0001			
703+19	RT	12	2	2.7	2.7	0.1	0.0008	727+45	LT	2	2	0.4	0.4	0.0	0.0001	736+72	RT	8	2	1.8	1.8	0.1	0.0006			
703+35	RT	8	2	1.8	1.8	0.1	0.0006	727+45	RT	4	2	0.9	0.9	0.0	0.0003	736+85	RT	2	2	0.4	0.4	0.0	0.0001			
703+42	RT	8	2	1.8	1.8	0.1	0.0006	727+85	LT	6	2	1.3	1.3	0.1	0.0004	737+50	LT	2	2	0.4	0.4	0.0	0.0001			
703+58	RT	12	2	2.7	2.7	0.1	0.0008	727+85	RT	8	2	1.8	1.8	0.1	0.0006	737+50	RT	2	2	0.4	0.4	0.0	0.0001			
704+00	RT	12	2	2.7	2.7	0.1	0.0008	728+00	RT	2	2	0.4	0.4	0.0	0.0001	737+81	RT	2	2	0.4	0.4	0.0	0.0001			
705+19	RT	12	2	2.7	2.7	0.1	0.0008	728+44	RT	2	2	0.4	0.4	0.0	0.0001	737+84	RT	2	2	0.4	0.4	0.0	0.0001			
715+52	LT	2	2	0.4	0.4	0.0	0.0001	728+65	RT	2	2	0.4	0.4	0.0	0.0001	738+00	LT	6	2	1.3	1.3	0.1	0.0004			
716+71	LT	8	2	1.8	1.8	0.1	0.0006	728+84	RT	12	2	2.7	2.7	0.1	0.0008	738+06	RT	8	2	1.8	1.8	0.1	0.0006			
716+71	RT	12	2	2.7	2.7	0.1	0.0008	729+05	RT	2	2	0.4	0.4	0.0	0.0001	738+06	LT	6	2	1.3	1.3	0.1	0.0004			
716+86	RT	2	2	0.4	0.4	0.0	0.0001	729+30	RT	2	2	0.4	0.4	0.0	0.0001	738+10	RT	12	2	2.7	2.7	0.1	0.0008			
717+11	RT	2	2	0.4	0.4	0.0	0.0001	729+45	LT	8	2	1.8	1.8	0.1	0.0006	738+20	LT	10	2	2.2	2.2	0.1	0.0007			
717+33	RT	12	2	2.7	2.7	0.1	0.0008	729+70	RT	20	2	4.4	4.4	0.2	0.0014	738+20	RT	8	2	1.8	1.8	0.1	0.0006			
717+40	RT	15	2	3.3	3.3	0.2	0.0010	729+85	LT	4	2	0.9	0.9	0.0	0.0003	738+40	LT	2	2	0.4	0.4	0.0	0.0001			
717+50	LT	6	2	1.3	1.3	0.1	0.0004	729+85	RT	2	2	0.4	0.4	0.0	0.0001	SUBTOTAL					43.6	2.4	0.01			
717+50	RT	12	2	2.7	2.7	0.1	0.0008	730+24	LT	6	2	1.3	1.3	0.1	0.0004	RDWY "A" TOTAL					332.0	18.6	0.10			
717+60	RT	12	2	2.7	2.7	0.1	0.0008	730+24	RT	4	2	0.9	0.9	0.0	0.0003											
717+73	RT	20	2	4.4	4.4	0.2	0.0014	730+47	RT	2	2	0.4	0.4	0.0	0.0001											
717+91	LT	6	2	1.3	1.3	0.1	0.0004	730+64	LT	2	2	0.4	0.4	0.0	0.0001											

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	13
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

•-255-55/70
••(60-7, 60-9)RS AND 60-(6-1,7-1)RS

PARTIAL DEPTH PATCHING - LOCATION 1 CONTINUED (RDWY "B")

STATION	LT/RT	LENGTH	WIDTH	AREA	PARTIAL DEPTH REMOVAL, 2"	PARTIAL DEPTH PATCHING	BITUMINOUS MATERIAL (PRIME COAT)	STATION	LT/RT	LENGTH	WIDTH	AREA	PARTIAL DEPTH REMOVAL, 2"	PARTIAL DEPTH PATCHING	BITUMINOUS MATERIAL (PRIME COAT)	STATION	LT/RT	LENGTH	WIDTH	AREA	PARTIAL DEPTH REMOVAL, 2"	PARTIAL DEPTH PATCHING	BITUMINOUS MATERIAL (PRIME COAT)	STATION	LT/RT	LENGTH	WIDTH	AREA	PARTIAL DEPTH REMOVAL, 2"	PARTIAL DEPTH PATCHING	BITUMINOUS MATERIAL (PRIME COAT)		
		FOOT	FOOT	SQ YD	SQ YD	TON	TON			FOOT	FOOT	SQ YD	SQ YD	TON	TON			FOOT	FOOT	SQ YD	SQ YD	TON	TON			FOOT	FOOT	SQ YD	SQ YD	TON	TON		
800+16	RT	2	2	0.4	0.4	0.0	0.0001	812+79	LT	6	2	1.3	1.3	0.1	0.0004	832+40	LT	10	2	2.2	2.2	0.1	0.0007	853+84	RT	8	2	1.8	1.8	0.1	0.0006		
800+32	RT	2	2	0.4	0.4	0.0	0.0001	813+19	RT	4	2	0.9	0.9	0.0	0.0003	832+40	RT	20	2	4.4	4.4	0.2	0.0014	854+00	RT	40	2	8.9	8.9	0.5	0.0028		
800+60	RT	20	2	4.4	4.4	0.2	0.0014	813+19	LT	12	2	2.7	2.7	0.1	0.0008	832+80	RT	8	2	1.8	1.8	0.1	0.0006	854+00	RT	4	2	0.9	0.9	0.0	0.0003		
800+80	RT	2	2	0.4	0.4	0.0	0.0001	813+41	LT	3	2	0.7	0.7	0.0	0.0002	833+20	LT	12	2	2.7	2.7	0.1	0.0008	854+00	LT	6	2	1.3	1.3	0.1	0.0004		
800+90	RT	20	2	4.4	4.4	0.2	0.0014	813+46	LT	6	2	1.3	1.3	0.1	0.0004	833+20	RT	10	2	2.2	2.2	0.1	0.0007	854+44	RT	2	2	0.4	0.4	0.0	0.0001		
801+18	LT	20	2	4.4	4.4	0.2	0.0014	813+56	LT	6	2	1.3	1.3	0.1	0.0004	833+60	LT	12	2	2.7	2.7	0.1	0.0008	854+79	RT	4	2	0.9	0.9	0.0	0.0003		
801+18	RT	2	2	0.4	0.4	0.0	0.0001	813+81	LT	2	2	0.4	0.4	0.0	0.0001	833+60	RT	6	2	1.3	1.3	0.1	0.0004	854+90	RT	2	2	0.4	0.4	0.0	0.0001		
801+48	RT	50	2	11.1	11.1	0.6	0.0035	814+31	LT	2	2	0.4	0.4	0.0	0.0001	833+83	RT	2	2	0.4	0.4	0.0	0.0001	855+00	LT	3	2	0.7	0.7	0.0	0.0002		
801+83	RT	12	2	2.7	2.7	0.1	0.0008	814+36	LT	2	2	0.4	0.4	0.0	0.0001	834+00	LT	12	2	2.7	2.7	0.1	0.0008	855+29	RT	2	2	0.4	0.4	0.0	0.0001		
802+01	RT	4	2	0.9	0.9	0.0	0.0003	814+79	RT	6	2	1.3	1.3	0.1	0.0004	834+00	RT	4	2	0.9	0.9	0.0	0.0003	855+47	RT	2	2	0.4	0.4	0.0	0.0001		
802+16	LT	6	2	1.3	1.3	0.1	0.0004	814+87	LT	3	2	0.7	0.7	0.0	0.0002	834+41	RT	3	2	0.7	0.7	0.0	0.0002	855+63	RT	6	2	1.3	1.3	0.1	0.0004		
802+32	LT	6	2	1.3	1.3	0.1	0.0004	815+00	LT	2	2	0.4	0.4	0.0	0.0001	835+20	LT	3	2	0.7	0.7	0.0	0.0002	855+73	RT	4	2	0.9	0.9	0.0	0.0003		
802+32	RT	4	2	0.9	0.9	0.0	0.0003	823+46	RT	4	2	0.9	0.9	0.0	0.0003	835+20	RT	2	2	0.4	0.4	0.0	0.0001	855+87	RT	2	2	0.4	0.4	0.0	0.0001		
802+45	RT	15	2	3.3	3.3	0.2	0.0010	823+80	LT	4	2	0.9	0.9	0.0	0.0003	835+60	LT	6	2	1.3	1.3	0.1	0.0004	856+10	RT	4	2	0.9	0.9	0.0	0.0003		
802+56	LT	2	2	0.4	0.4	0.0	0.0001	823+80	RT	2	2	0.4	0.4	0.0	0.0001	835+79	RT	12	2	2.7	2.7	0.1	0.0008	856+10	LT	2	2	0.4	0.4	0.0	0.0001		
802+72	RT	12	2	2.7	2.7	0.1	0.0008	824+05	RT	2	2	0.4	0.4	0.0	0.0001	836+41	LT	4	2	0.9	0.9	0.0	0.0003	856+41	RT	2	2	0.4	0.4	0.0	0.0001		
802+95	LT	4	2	0.9	0.9	0.0	0.0003	824+86	RT	8	2	1.8	1.8	0.1	0.0006	836+62	LT	3	2	0.7	0.7	0.0	0.0002	856+51	RT	3	2	0.7	0.7	0.0	0.0002		
804+55	LT	2	2	0.4	0.4	0.0	0.0001	824+86	LT	4	2	0.9	0.9	0.0	0.0003	836+80	LT	8	2	1.8	1.8	0.1	0.0006	856+81	RT	2	2	0.4	0.4	0.0	0.0001		
804+55	RT	10	2	2.2	2.2	0.1	0.0007	825+00	RT	2	2	0.4	0.4	0.0	0.0001	837+21	LT	8	2	1.8	1.8	0.1	0.0006	856+92	RT	6	2	1.3	1.3	0.1	0.0004		
804+72	LT	4	2	0.9	0.9	0.0	0.0003	825+19	LT	8	2	1.8	1.8	0.1	0.0006	837+21	RT	12	2	2.7	2.7	0.1	0.0008	857+08	RT	8	2	1.8	1.8	0.1	0.0006		
804+74	LT	2	2	0.4	0.4	0.0	0.0001	825+19	RT	8	2	1.8	1.8	0.1	0.0006	837+43	LT	4	2	0.9	0.9	0.0	0.0003	857+08	LT	2	2	0.4	0.4	0.0	0.0001		
804+74	RT	6	2	1.3	1.3	0.1	0.0004	825+59	LT	8	2	1.8	1.8	0.1	0.0006	837+43	RT	6	2	1.3	1.3	0.1	0.0004	857+12	RT	2	2	0.4	0.4	0.0	0.0001		
804+95	LT	6	2	1.3	1.3	0.1	0.0004	825+59	RT	4	2	0.9	0.9	0.0	0.0003	838+00	RT	4	2	0.9	0.9	0.0	0.0003	857+29	RT	6	2	1.3	1.3	0.1	0.0004		
805+20	RT	2	2	0.4	0.4	0.0	0.0001	825+75	RT	2	2	0.4	0.4	0.0	0.0001	838+27	RT	2	2	0.4	0.4	0.0	0.0001	857+29	LT	3	2	0.7	0.7	0.0	0.0002		
805+20	LT	2	2	0.4	0.4	0.0	0.0001	825+80	RT	2	2	0.4	0.4	0.0	0.0001	841+58	LT	12	2	2.7	2.7	0.1	0.0008	857+73	RT	10	2	2.2	2.2	0.1	0.0007		
805+35	LT	2	2	0.4	0.4	0.0	0.0001	826+00	LT	6	2	1.3	1.3	0.1	0.0004	841+80	RT	6	2	1.3	1.3	0.1	0.0004	858+00	RT	6	2	1.3	1.3	0.1	0.0004		
805+35	RT	12	2	2.7	2.7	0.1	0.0008	826+26	RT	2	2	0.4	0.4	0.0	0.0001	841+98	LT	10	2	2.2	2.2	0.1	0.0007	858+40	RT	10	2	2.2	2.2	0.1	0.0007		
805+47	LT	5	2	1.1	1.1	0.1	0.0003	826+40	LT	8	2	1.8	1.8	0.1	0.0006	841+98	RT	2	2	0.4	0.4	0.0	0.0001	858+40	RT	6	2	1.3	1.3	0.1	0.0004		
805+47	RT	12	2	2.7	2.7	0.1	0.0008	826+40	RT	4	2	0.9	0.9	0.0	0.0003	842+28	RT	4	2	0.9	0.9	0.0	0.0003	859+68	RT	8	2	1.8	1.8	0.1	0.0006		
805+74	LT	2	2	0.4	0.4	0.0	0.0001	826+57	RT	2	2	0.4	0.4	0.0	0.0001	842+49	RT	2	2	0.4	0.4	0.0	0.0001	860+07	RT	8	2	1.8	1.8	0.1	0.0006		
805+74	RT	4	2	0.9	0.9	0.0	0.0003	826+68	RT	2	2	0.4	0.4	0.0	0.0001	842+60	RT	2	2	0.4	0.4	0.0	0.0001	865+67	LT	8	2	1.8	1.8	0.1	0.0006		
805+97	KT	3	2	0.7	0.7	0.0	0.0002	826+80	LT	6	2	1.3	1.3	0.1	0.0004	842+68	RT	2	2	0.4	0.4	0.0	0.0001	865+95	LT	2	2	0.4	0.4	0.0	0.0001		
805+97	RT	6	2	1.3	1.3	0.1	0.0004	826+80	RT	8	2	1.8	1.8	0.1	0.0006	842+79	LT	8	2	1.8	1.8	0.1	0.0006	865+95	RT	5	2	1.1	1.1	0.1	0.0003		
806+22	LT	3	2	0.7	0.7	0.0	0.0002	826+98	RT	2	2	0.4	0.4	0.0	0.0001	842+90	RT	16	2	3.6	3.6	0.2	0.0011	866+03	RT	2	2	0.4	0.4	0.0	0.0001		
806+37	LT	2	2	0.4	0.4	0.0	0.0001	827+08	RT	2	2	0.4	0.4	0.0	0.0001	843+08	RT	16	2	3.6	3.6	0.2	0.0011	866+03	LT	2	2	0.4	0.4	0.0	0.0001		
806+37	RT	8	2	1.8	1.8	0.1	0.0006	827+21	LT	4	2	0.9	0.9	0.0	0.0003	843+24	RT	10	2	2.2	2.2	0.1	0.0007	866+28	RT	2	2	0.4	0.4	0.0	0.0001		
806+77	RT	8	2	1.8	1.8	0.1	0.0006	827+21	RT	6	2	1.3	1.3	0.1	0.0004	843+45	LT	15	2	3.3	3.3	0.2	0.0010	866+28	LT	2	2	0.4	0.4	0.0	0.0001		
806+93	RT	12	2	2.7	2.7	0.1	0.0008	827+39	RT	2	2	0.4	0.4	0.0	0.0001	843+45	RT	15	2	3.3	3.3	0.2	0.0010	866+35	RT	4	2	0.9	0.9	0.0	0.0003		
807+18	RT	12	2	2.7	2.7	0.1	0.0008	827+49	RT	2	2	0.4	0.4	0.0	0.0001	843+55	LT	7	2	1.6	1.6	0.1	0.0005	866+50	RT	4	2	0.9	0.9	0.0	0.0003		
807+31	RT	2	2	0.4	0.4	0.0	0.0001	827+60	LT	6	2	1.3	1.3	0.1	0.0004	843+77	LT	9	2	2.0	2.0	0.1	0.0006	866+55	RT	3	2	0.7	0.7	0.0	0.0002		
807+39	LT	2	2	0.4	0.4	0.0	0.0001	827+60	RT	2	2	0.4	0.4	0.0	0.0001	844+15	LT	6	2	1.3	1.3	0.1	0.0004	866+68	RT	3	2	0.7	0.7	0.0	0.0002		
807+39	RT	6	2	1.3	1.3	0.1	0.0004	828+00	LT	12	2	2.7	2.7	0.1	0.0008	844+57	RT	30	2	6.7	6.7	0.4	0.0021	866+74	LT	3	2	0.7	0.7	0.0	0.0002		
807+50	RT	10	2	2.2	2.2	0.1	0.0007	828+18	RT	3	2	0.7	0.7	0.0	0.0002	844+95	RT	40	2	8.9	8.9	0.5	0.0028	SUBTOTAL							47.3	2.7	0.01
807+58	LT	6	2	1.3	1.3	0.1	0.0004	828+40	LT	10	2	2.2	2.2	0.1																			

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	14

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

■-255-55/70
 ■(60-7, 60-9)RS AND 60-6-1,7-1)RS

STATIONING	BITUMINOUS MATERIAL PRIME COAT TON	AGGREGATE PRIME COAT TON	POLYMERIZED HMA BINDER COURSE, IL-19.0, N105 TON	POLYMERIZED HMA SURFACE COURSE, SMA, N80 TON	PCC SURFACE REMOVAL - 1/2" BUTT JOINT SQ YD	PCC SURFACE REMOVAL - 1/2" SURFACE SQ YD	HOT MIX ASPHALT SHOULDERS TON	COMMENTS
310+00 TO 315+55	0.3	1.7	140	93		1110	181	RAMP 3
101+00 TO 107+40	1.1	5.1	427	285		3392	303	RAMP 1
110+00 TO 114+00	0.3	1.6	134	90		1067	75	RAMP 1
114+00 TO 152+25.50	3.2	15.3	1285	857		10201	1250	RAMP 1
152+25.5 TO 162+00	1.2	5.6	470	314		3733	318	RAMP 1
400+00 TO 403+00	0.2	0.8	67	45		533	70	RAMP 4
200+00 TO 224+00	0.1	0.4	30	20	246	237	560	RAMP 2
	6.3	30.4	2554	1703	246	20273	2757	

STATIONING	BITUMINOUS MATERIAL PRIME COAT TON	AGGREGATE PRIME COAT TON	POLYMERIZED HMA SURFACE COURSE, SMA, N80 TON	PCC SURFACE REMOVAL - 2" SQ YD	COMMENTS
760+00 TO 764+64	0.4	1.9	104	1237	LOC. 2 - RAMP 6
764+64 TO 788+28	1.3	6.3	353	4200	LOC. 2 - RAMP 6
788+28 TO 792+00	0.3	1.5	83	992	LOC. 2 - RAMP 6
1256+00 TO 1272+00	3.3	15.8	887	10561	LOC. 1 - MAINLINE (I-270)
1274+00 TO 200+00	0.4	2.0	114	1353	LOC. 1 - RAMP 2
224+00 TO 492+00	14.9	71.5	4002	47644	LOC. 1 - RAMP 2
	20.6	99.0	5543	65987	

STATIONING	BITUMINOUS MATERIAL PRIME COAT TON	AGGREGATE PRIME COAT TON	POLYMERIZED HMA BINDER COURSE, IL-19.0, N105 TON	POLYMERIZED HMA SURFACE COURSE, SMA, N80 TON	PCC SURFACE REMOVAL - 1/2" BUTT JOINT SQ YD	BITUMINOUS SURF. REMOV. - 1/2" BUTT JOINT SQ YD	PCC SURFACE REMOVAL - 1/2" SURFACE SQ YD	HOT MIX ASPHALT SHOULDERS TON	COMMENTS
340+48 TO 366+50	1.4	6.9	583	389		188	4626	486	RAMP 3
693+63 TO 703+16	0.8	3.8	320	213			2541	311	RAMP 6
703+16 TO 706+00	0.2	1.1	95	64			757	53	RAMP 6
706+00 TO 756+02	4.2	20.0	1681	1120			13339	1634	RAMP 6
756+02 TO 760+00	0.3	1.6	134	89			1061	130	RAMP 6
1558+00 TO 1593+65	1.8	8.8	740	493			5872	0	RAMP 6
703+16 TO 706+00	0.1	0.4	30	20			237	53	RAMP 7
706+00 TO 712+50	0.4	1.7	146	97	188		1156	212	RAMP 7
1564+00 TO 1584+31	0.5	2.5	206	137	246		1634	0	RAMP 1
1584+31 TO 1589+00	0.4	1.9	158	105			1251	69	RAMP 1
1556+00 TO 1598+00	5.3	25.2	2117	1411	722		16800	1372	MAINLINE - I-255(NB)
1556+00 TO 1614+42	7.3	35.1	2944	1963	722		23368	1908	MAINLINE - I-255(SB)
TOTAL	22.7	109.0	9153	6102	1878	188	72641	6229	

PLOT DATE = 5/25/2007
 FILE NAME = c:\projects\76b15\loc1\loc1.dgn
 PLOT SCALE = 5/8"=1'-0"
 USER NAME = challandak

Rev. Sheet 0-5-04

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULES
 FAI 255, FAI 55/70
 SECTION (60-7, 60-9)RS &
 60-6-1,7-1)RS
 MADISON COUNTY

SCALE: VERT. DRAWN BY
 HORIZ. CHECKED BY
 DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	15

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

•-255-55/70
 ••(60-7, 60-9)RS AND 60-(6-1,7-1)RS

LOCATION	CRACK ROUTING (PAVEMENT)	CRACK FILLING
	FOOT	POUND
RAMP 1	16160	6608
RAMP 2	5172	2115
RAMP 4	11700	4784
RAMP 5	20028	8190
RAMP 7	36648	14987
RAMP 8	7960	3256
TOTAL	97668	39940

LOCATION	CRACK ROUTING (PAVEMENT)	CRACK FILLING
	FOOT	POUND
RAMP 1	8522	2797
RAMP 2	7554	2478
RAMP 3	8335	2735
RAMP 4	7758	2545
RAMP 5	23727	7785
RAMP 6	40016	13131
TOTAL	95912	31471

STATIONING	AGGREGATE SHOULDERS, TY B TON	COMMENTS
101+70 TO 162+00	557	RAMP 1 - LOC. 1
310+00 TO 315+55	51	RAMP 1 - LOC. 1
200+00 TO 224+00	222	RAMP 2 - LOC. 1
400+00 TO 403+00	28	RAMP 4 - LOC. 1
1556+00 TO 1598+00	388	I-255 (SB) - LOC. 2
1556+00 TO 1614+42	539	I-255 (NB) - LOC. 2
148+04 TO 179+64	292	RAMP 1 - LOC. 2
1564+00 TO 1589+00	231	RAMP 1 - LOC. 2
212+58 TO 231+00	170	RAMP 2 - LOC. 2
340+48 TO 366+35	239	LOC. 2 - RAMP 3
462+62 TO 485+46	211	LOC. 2 - RAMP 4
580+00 TO 613+93	313	LOC. 2 - RAMP 5
693+63 TO 760+00	613	LOC. 2 - RAMP 6
1558+00 TO 1593+65	329	LOC. 2 - RAMP 6
703+16 TO 722+55	179	LOC. 2 - RAMP 7
872+40 TO 885+00	116	LOC. 2 - RAMP 8
TOTAL	4476	

PLOT DATE = 5/25/2007
 FILE NAME = c:\projects\76b15\76b15.dgn
 USER NAME = shallandak

Rev. Sheet 6-5-07

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULES
 FAI 255, FAI 55/70
 SECTION (60-7, 60-9)RS &
 60-(6-1,7-1)RS
 MADISON COUNTY
 DRAWN BY
 CHECKED BY

SCALE: VERT.
 HORIZ.
 DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	17
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
*-255-55/70				
***(60-7, 60-9)RS AND 60-(6-1,7-1)RS				

11" TYPE B PATCHING SCHEDULE

STATION	DRIV. LANE		TYPE I SQ YDS	TYPE II SQ YDS	TYPE III SQ YDS	TYPE IV SQ YDS	SAW CUT FT	DOWEL BARS EACH	PAVT. FABRIC SQ YDS
	LTH.	WTH.							
	ROADWAY "B"								
RT 800+00	6	12		8			49	20	
LT 800+00	6	12		8			49	20	
RT 800+40	6	12		8			49	20	
RT 800+58	6	12		8			49	20	
RT 801+00	6	12		8			49	20	
LT 801+00	6	12		8			49	20	
RT 801+34	6	6	4				31	10	
LT 801+34	6	6	4				31	10	
RT 801+55	6	12		8			49	20	
LT 801+55	6	6	4				31	10	
LT 801+74	6	6	4				31	10	
RT 801+74	6	12		8			49	20	
RT 801+87	6	12		8			49	20	
LT 801+87	6	12		8			49	20	
RT 802+16	6	12		8			49	20	
RT 802+56	6	12		8			49	20	
RT 802+95	6	12		8			49	20	
RT 803+36	6	12		8			49	20	
LT 803+54	6	12		8			49	20	
RT 803+54	6	6	4				31	10	
LT 803+76	6	6	4				31	10	
RT 803+76	6	12		8			49	20	
RT 804+15	6	12		8			49	20	
LT 804+32	6	12		8			49	20	
RT 804+32	6	15		10			58	26	
RT 804+55	6	15		10			58	26	
RT 804+95	6	16		11			61	28	
LT 804+62	6	12		8			49	20	
RT 810+38	6	12		8			49	20	
RT 814+44	6	12		8			49	20	
LT 814+44	6	12		8			49	20	
LT 814+79	6	12		8			49	20	
RT 824+62	6	12		8			49	20	
LT 824+62	6	12		8			49	20	
LT 835+79	6	12		8			49	20	
RT 840+28	6	6	4				31	10	
LT 840+28	6	6	4				31	10	
LT 843+35	6	12		8			49	20	
RT 843+35	6	12		8			49	20	
RT 843+77	6	12		8			49	20	
RT 859+45	6	12		8			49	20	
RT 864+00	6	12		8			49	20	
RT 864+59	6	12		8			49	20	
LT 866+14	6	12		8			49	20	
RT 866+14	6	12		8			49	20	
RT 866+97	6	12		8			49	20	
RDWY "B" TOTAL			32	311	0	0	2140	860	0
ROADWAY "A"									
RT 699+07	12	12			16		61	20	
LT 699+07	6	12		8			49	20	
RT 700+05	20	12				27	77	20	27
RT 700+31	6	12		8			49	20	
RT 735+15	6	12		8			49	20	
RT 735+45	6	12		8			49	20	
LT 735+45	6	12		8			49	20	
LT 737+27	6	12		8			49	20	
RT 737+27	6	12		8			49	20	
RT 737+70	6	12		8			49	20	
LT 737+70	6	12		8			49	20	
RT 738+00	6	12		8			49	20	
LT 738+30	6	12		8			49	20	
RT 738+30	6	12		8			49	20	
RDWY "A" TOTAL			0	96	16	27	726	280	27

STATION	LENGTH FOOT	WIDTH FOOT	AREA SQ YD	PARTIAL DEPTH REMOVAL, 2"	PARTIAL DEPTH PATCHING	BITUMINOUS MATERIAL		REMARKS
				SQ YD	TON	(PRIME COAT)	TON	
148+04	100	2	22.2	22.2	2.5	0.0070	RAMP 1 - I-270(EB) TO I-255 (SB)	
158+60	100	2	22.2	22.2	2.5	0.0070	RAMP 1 - I-270(EB) TO I-255 (SB)	
169+16	1584	2	352.0	352.0	39.4	0.1101	RAMP 1 - I-270(EB) TO I-255 (SB)	
SUB-TOTAL				396.4	44.4	0.12	RAMP 1 TOTAL	
467+25	8	2	1.8	1.8	0.2	0.0006	RAMP 4 - I-270(WB) TO I-255(SB)	
467+33	10	2	2.2	2.2	0.2	0.0007	RAMP 4 - I-270(WB) TO I-255(SB)	
467+45	12	2	2.7	2.7	0.3	0.0008	RAMP 4 - I-270(WB) TO I-255(SB)	
466+30	1060	2	235.6	235.6	26.4	0.0737	RAMP 4 - I-270(WB) TO I-255(SB)	
477+10	125	2	27.8	27.8	3.1	0.0087	RAMP 4 - I-270(WB) TO I-255(SB)	
202+00	14	2	3.1	3.1	0.3	0.0010	RAMP 4 - I-270(WB) TO I-255(SB)	
SUB-TOTAL				273.1	30.6	0.09	RAMP 4 TOTAL	
709+00	100	2	22.2	22.2	2.5	0.0070	RAMP 7 - I-255(NB) TO I-270(EB)	
722+85	100	2	22.2	22.2	2.5	0.0070	RAMP 7 - I-255(NB) TO I-270(EB)	
SUB-TOTAL				44.4	5.0	0.01	RAMP 7 TOTAL	
LOCATION 2 TOTAL				714.0	80.0	0.2		

PLOT DATE = 5/25/2007
 FILE NAME = c:\pva\pva\080307\road\p1\080307.dgn
 USER NAME = chaitan@arc

Rev. Sheet 6-5-07

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULES
 FAI 255, FAI 55/70
 SECTION (60-7, 60-9)RS &
 60-(6-1,7-1)RS
 MADISON COUNTY
 SCALE: VERT. DRAWN BY
 DATE HORIZ. CHECKED BY

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	MADISON	37	18
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
*-255-55/70				
**(60-7, 60-9)RS AND 60-(6-1,7-1)RS				

11" TYPE B PATCHING SCHEDULE CONTINUED										
STATION	DRIV. LANE		TYPE I	TYPE II	TYPE III	TYPE IV	SAW CUT FT	DOWEL BARS EACH	PAVT. FABRIC SQ YDS	TIE BARS EACH
	LTH.	WTH.								
	SQ YDS	SQ YDS	SQ YDS	SQ YDS	FT	EACH	SQ YDS	EACH		
RAMP 1										
111+15	6	16		11			61	28		
11+36	6	16		11			61	28		
RAMP 1 TOTAL			0	22	0	0	122	56		0
RAMP 2										
216+74	8	25			22		92	42	0	
216+85	8	6		5			35	10	0	
217+48	10	16			18		69	28	0	
217+85	6	16		11			61	28	0	
221+00	6	16		11			61	28	0	
223+00	6	16		11			61	28	0	
224+21	6	16		11			61	28	0	
224+63	6	16		11			61	28	0	
226+32	6	16		11			61	28	0	
226+79	6	8		5			37	14	0	
226+90	6	8		5			37	14	0	
227+38	6	8		5			37	14	0	
227+64	40	16				71	129	28	71	38
228+23	10	16			18		69	28	18	
228+58	8	16		14			65	28		
229+14	6	16		11			61	28		
229+68	25	16				44	99	28	44	23
RAMP 2 TOTAL			0	111	58	115	1096	430	133	61
RAMP 3										
293+78	6	4	3				25	6		
294+18	6	6	4				31	10		
296+99	6	16		11			61	28		
RAMP 3 TOTAL			7	11	0	0	117	44	0	0
TOTAL PATCHING			39	551	74	142	4201	1670	160	61

PAVEMENT MARKING SCHEDULE					
LOCATION	PAINT PAVEMENT MARKING		SHORT TERM PAVEMENT MARKING LINE - 4"	RAISED REFLECTIVE PAVEMENT MKRS	RAISED REFLECTIVE PAVEMENT MKRS REM
	FOOT	SKIP-DASH	FOOT	EACH	EACH
LOCATION #1					
RAMP 1					
STA. 1272+55 TO STA.	2,910.0	364.0	133	36	36
STA. 101+70 TO STA. 114+75	2,610.0	327.0	119	32	32
STA. 114+75 TO 161+40	9,330.0	1,167.0	425	116	116
RAMP 2					
STA. 479+20 TO STA. 490+00	2,160.0	270.0	98	80	80
STA. 200+00 TO 232+26	6,452.0	807.0	293	228	228
RAMP 3					
STA. 800+00 TO 811+82					
STA. 301+00 TO STA. 315+54					
STA. 152+20 TO STA. 161+75					
RAMP 4					
STA. 400+00 TO STA. 414+16					
RAMP 5					
STA. 500+00 TO STA. 519+40					
RAMP 6					
STA. 600+00 TO STA. 619+42					
ROADWAY "A"					
STA. 699+90 TO STA. 738+77					
ROADWAY "B"					
STA. 800+00 TO STA. 867+04					
LOCATION #2					
RAMP 1					
STA. 148+04 TO STA. 179+64					
RAMP 2					
STA. 212+58 TO STA. 231+00					
RAMP 3					
STA. 1638+00 TO STA.	1,600.0		73		
STA. 340+48 TO STA. 366+35	5,174.0		235		
RAMP 4					
STA. 462+62 TO STA. 485+46					
RAMP 5					
STA. 580+00 TO STA. 613+93					
RAMP 6					
STA. 1558+00 TO 1593+70	7,140.0	893.0	328		
STA. 693+63 TO STA. 792+00	19,000.0	2,375.0	864	326	326
OMISSION: STA. 717+30 TO STA. 720+67	-674.0	-85.0	-31		
RAMP 7					
STA. 703+19 TO STA. 722+55					
RAMP 8					
STA. 872+40 TO STA. 885+00					
MAINLINE					
I255 (NB)					
STA.1556+00 TO STA. 1598+00	8,400.0	2,100.0	382	420	420
I255 (SB)					
STA. 1557+00 TO STA. 1609+00	10,400.0	2,600.0	473	520	520
• ADDITIONAL QUANTITY			3836		
• ADDITIONAL QUANTITY			3000		
• ADDITIONAL QUANTITY	9,590				
TOTAL	94,910	48,386.0	10,228	1758	1758

- ADDITIONAL SHORT TERM PAVEMENT MARKING DUE TO CRACK ROUTING/CRACK FILLING 3836 FEET.
- ADDITIONAL SHORT TERM PAVEMENT MARKING FOR EDGE LINES 3,000 FEET
- ADDITIONAL PAINT PAVEMENT MARKING DUE TO CRACK ROUTING 9,590 FEET

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SCHEDULES
 FAI 255, FAI 55/70
 SECTION (60-7, 60-9)RS &
 60-(6-1,7-1)RS
 MADISON COUNTY
 SCALE: VERT. DRAWN BY
 HORIZ. CHECKED BY
 DATE

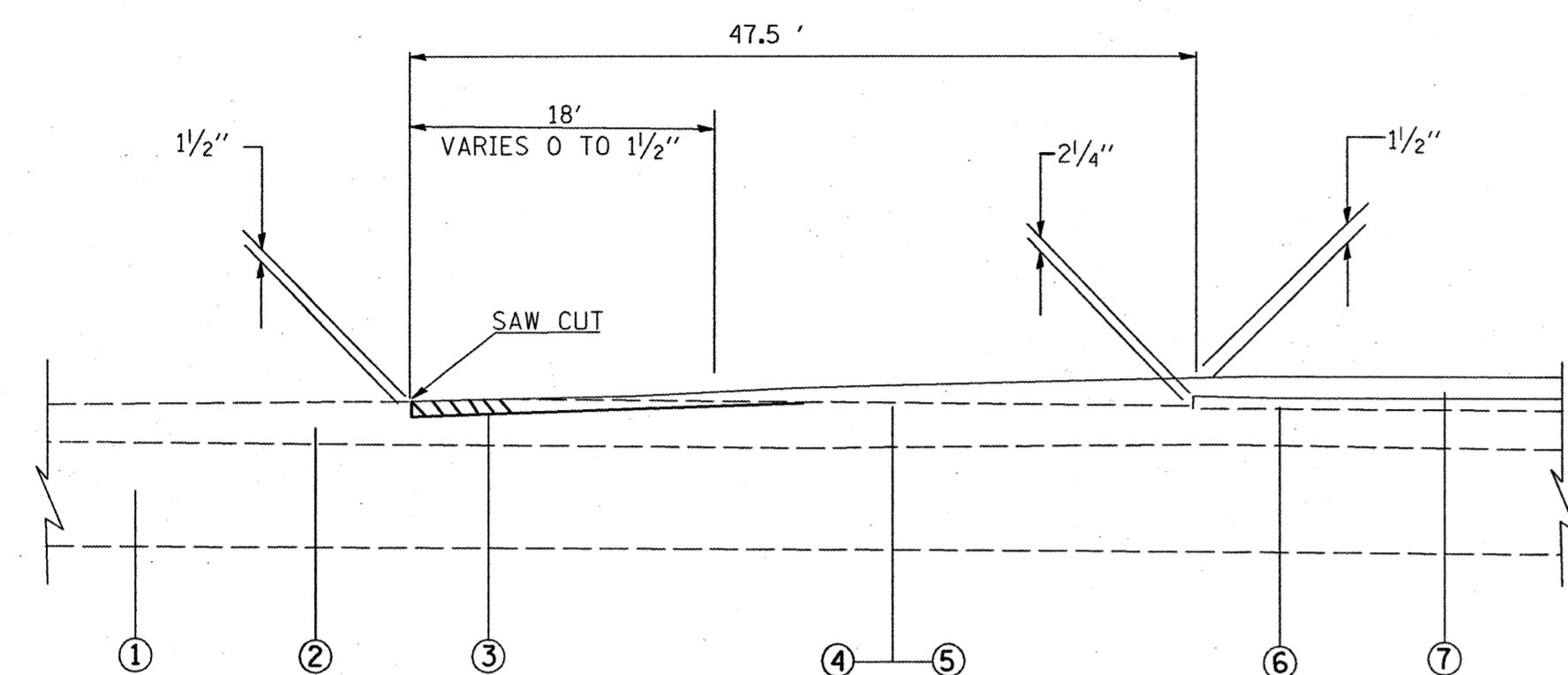
Rev. Sheet 6-5-07

PLOT DATE = 5/25/2007
 PLOT SCALE = 50%
 USER NAME = challandak

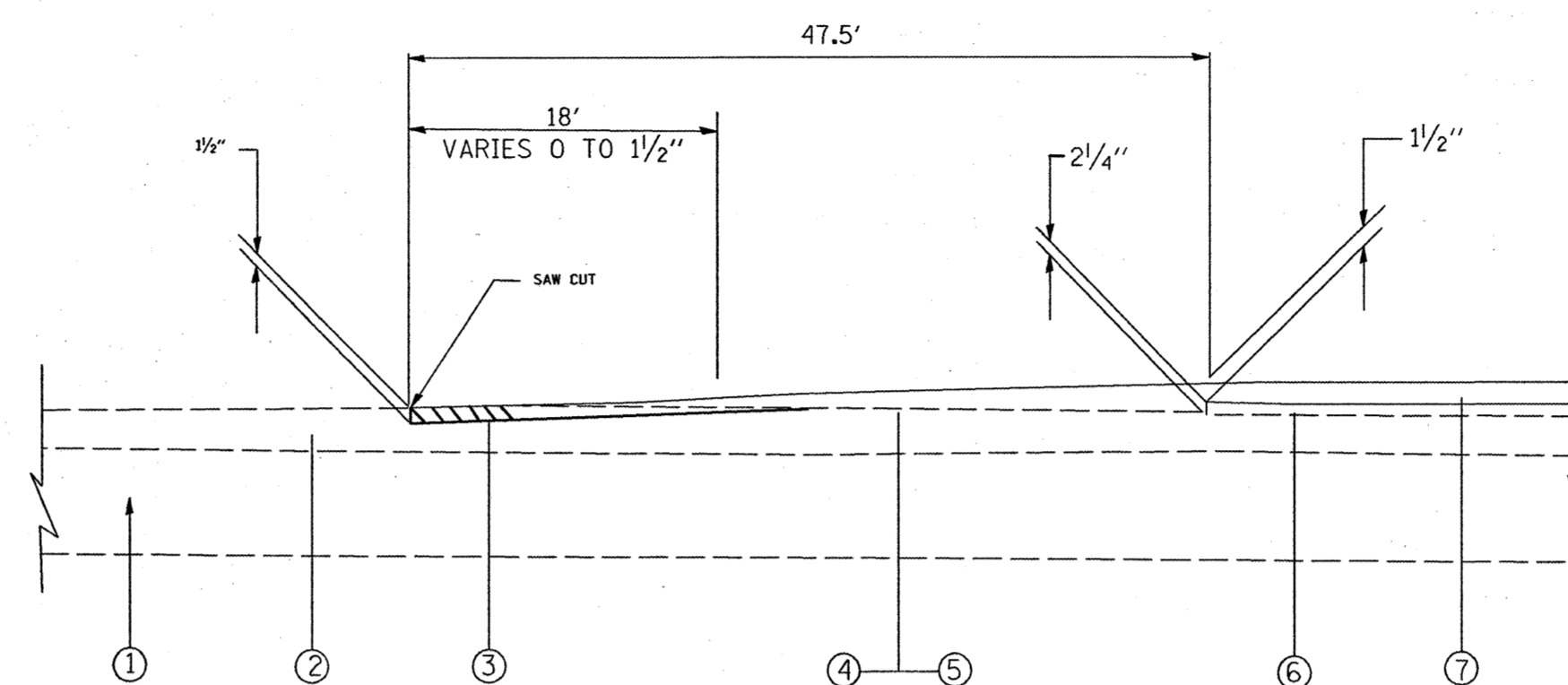
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	••	MADISON	37	29
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.		•-255-55/70		
		••(60-7, 60-9)RS AND 60-(6-1,7-1)RS		

LEGEND

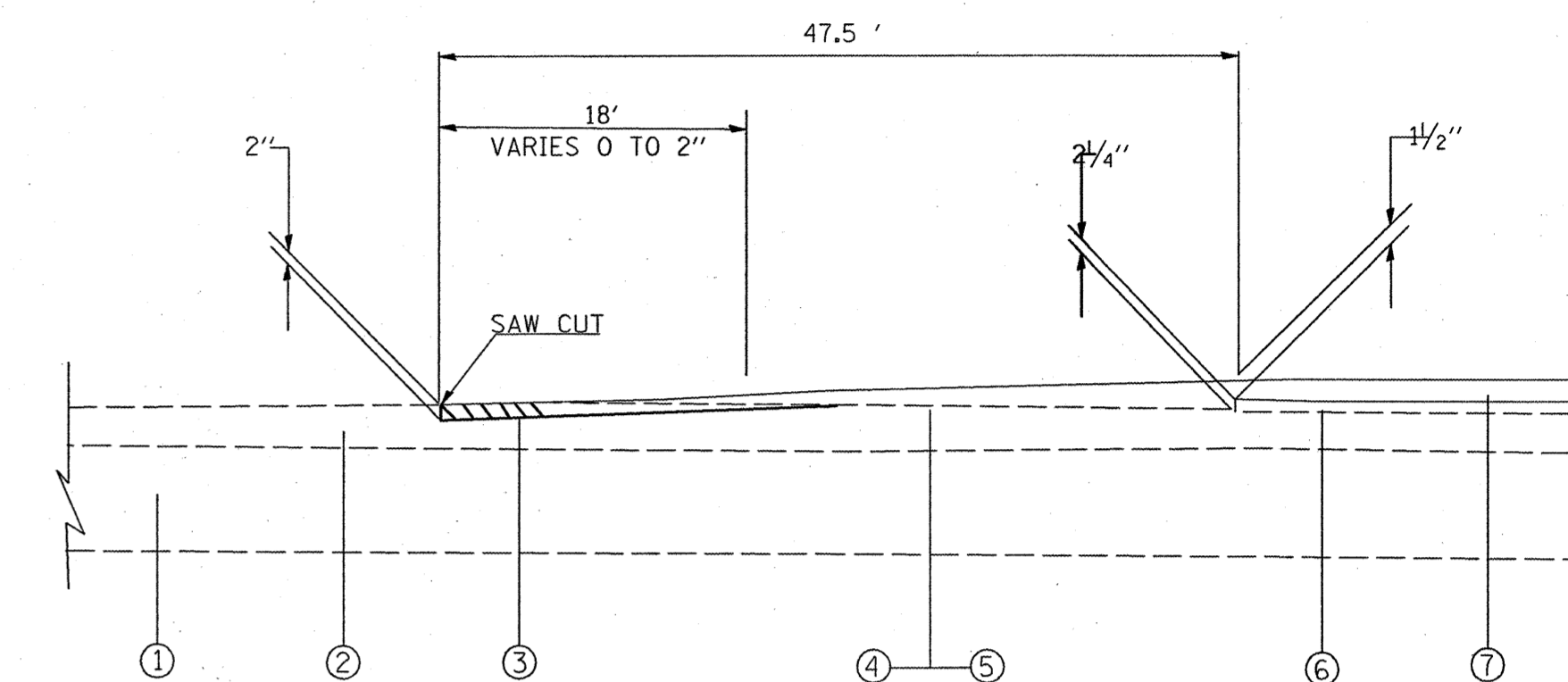
1. EXISTING PAVEMENT
2. EXISTING PCC PAVEMENT
3. PROPOSED PCC SURFACE REMOVAL - BUTT JOINT
4. PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
5. PROPOSED AGGREGATE (PRIME COAT)
6. PROPOSED POLYMERIZED BCSC SUPER IL 19.0 N70 - 2 1/4"
7. PROPOSED POLYMERIZED BCSC SUPER "E" N70 - 1 1/2"



**BUTT JOINT DETAIL
(CONCRETE)**
STA. 101+70 TO STA. 102+17

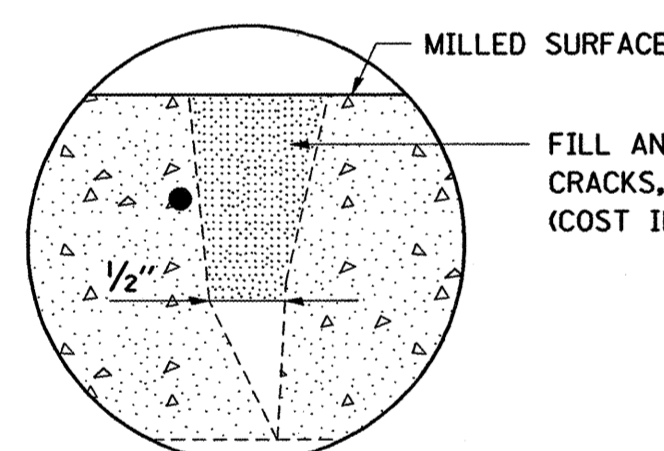


**BUTT JOINT DETAIL
(HOT MIX ASPHALT)**
STA. 161+40 TO STA. 161+87



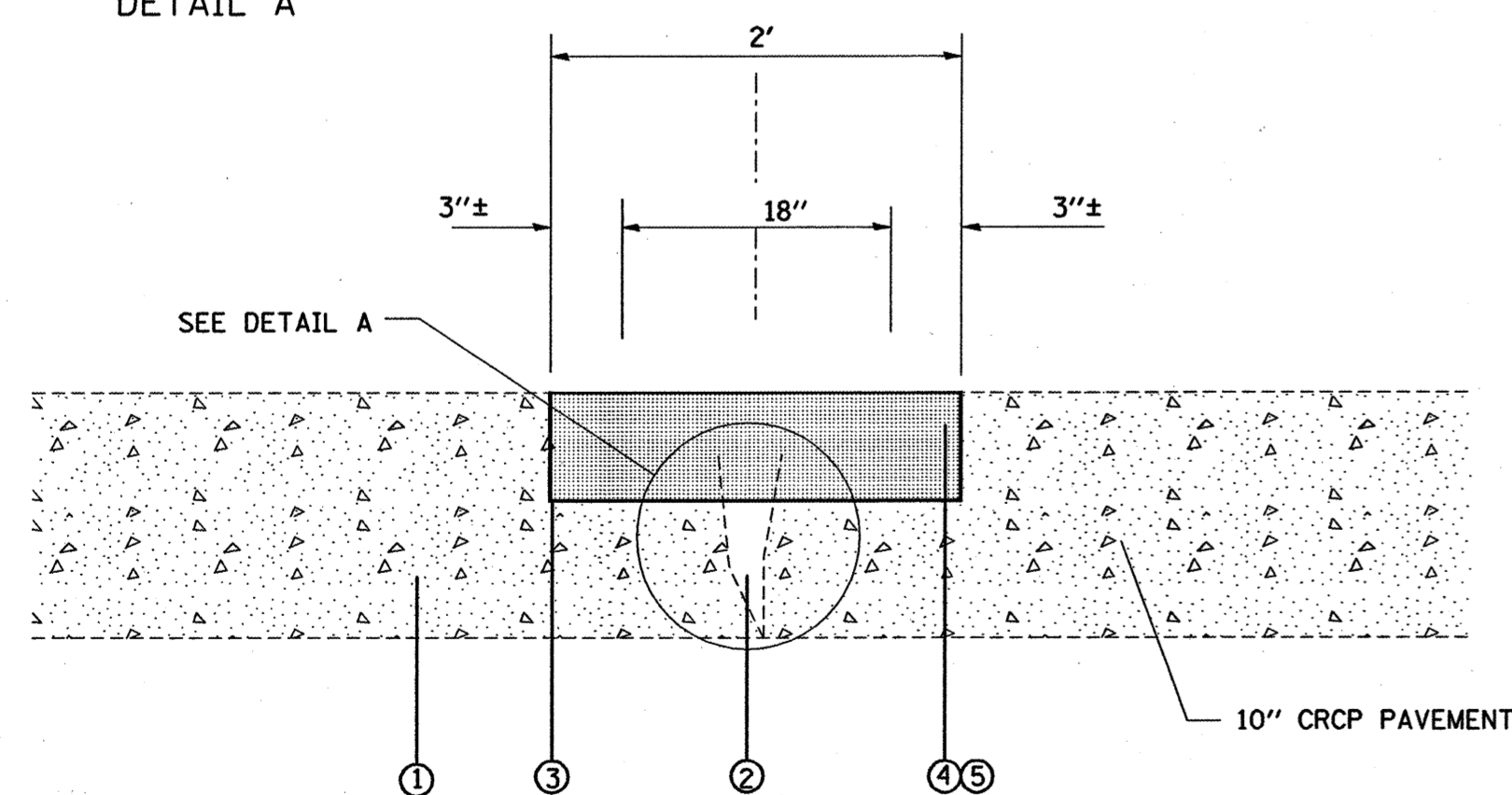
**BUTT JOINT DETAIL
PAVEMENT TRANSITION DETAIL**
STA. 232+26.55 TO STA. 232+74.05

PARTIAL DEPTH PATCHING DETAIL



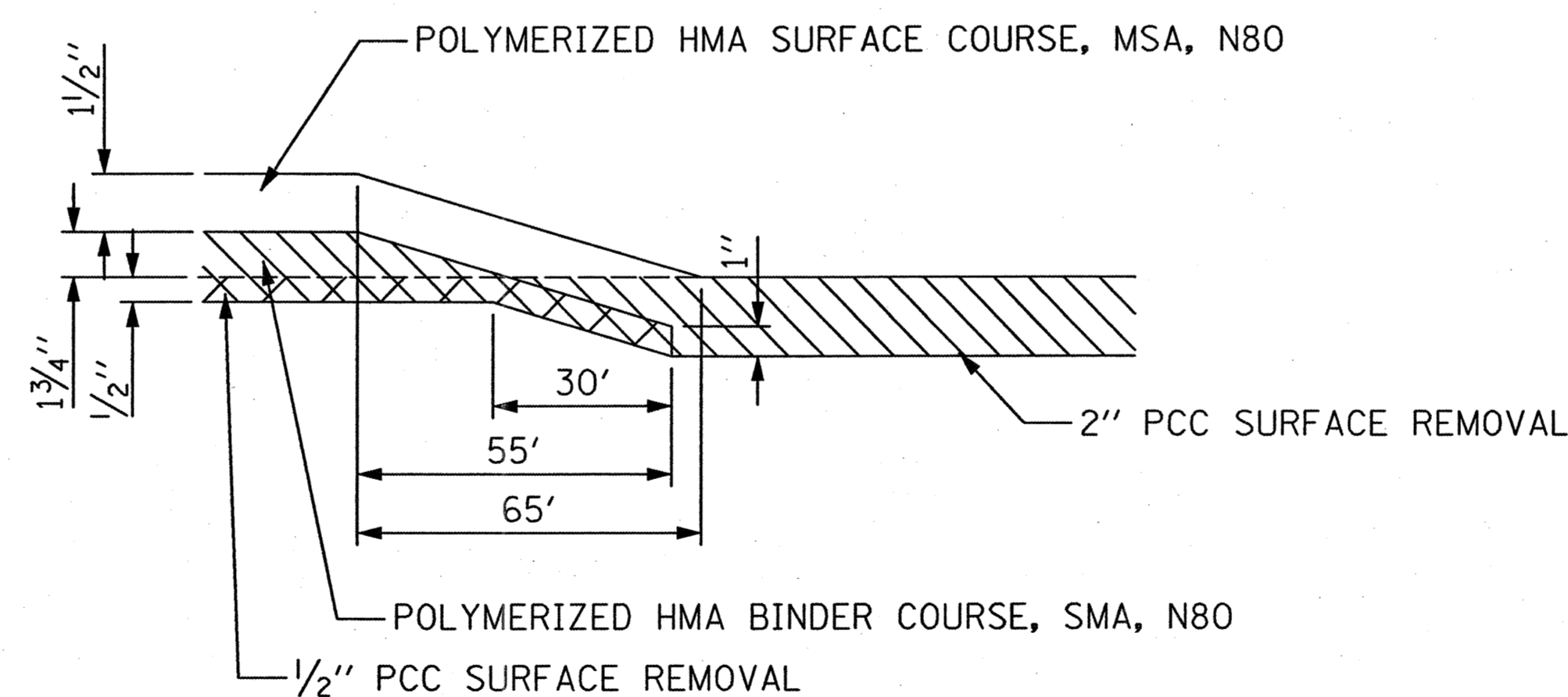
FILL AND COMPACT WITH BITUMINOUS MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS (CRACKS > 1/2") (COST INCLUDED WITH PARTIAL DEPTH PATCHING)

DETAIL A



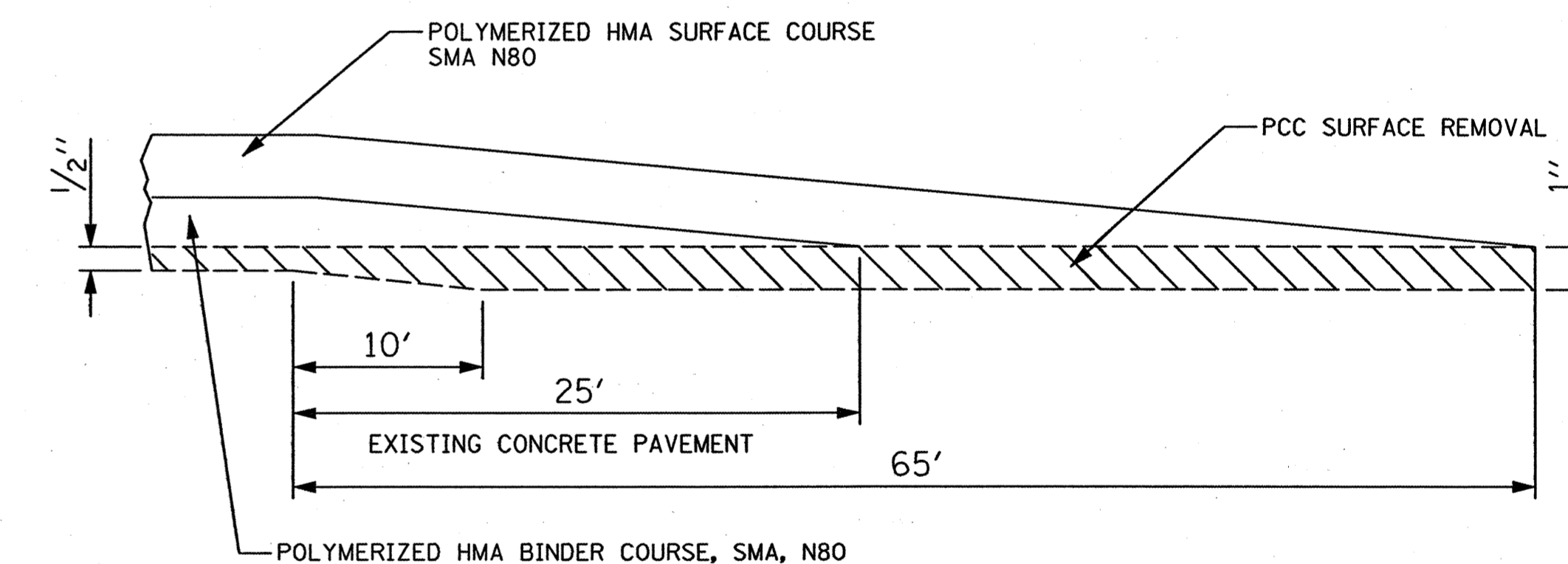
LEGEND

- ① EXISTING CRCP PAVEMENT, 10"
- ② EXISTING LONGITUDINAL PAVEMENT CRACK
- ③ EXISTING PAVEMENT REINFORCEMENT
- ④ PROPOSED PARTIAL DEPTH REMOVAL, 2"
- ⑤ PROPOSED PARTIAL DEPTH PATCHING



RESURFACING TRANSITIONS

- STA. 200+00 TO STA. 200+65 RAMP 1 (LOC 1)
- STA. 161+35 TO STA. 162+00 RAMP 1 (LOC 1)
- STA. 224+35 TO STA. 225+00 RAMP 2 (LOC 1)
- STA. 759+35 TO STA. 760+00 RAMP 3 (LOC 2)
- STA. 1273+35 TO STA. 1274+00 MAINLINE (LOC 1)
- STA. 1525+35 TO STA. 1526+00 MAINLINE (LOC 1)



BUTT JOINT DETAIL

- STA. 491+35 TO STA. 492+00 RAMP 2 (LOC 1)
- STA. 340+48 TO STA. 341+13 RAMP 3 (LOC 2)
- STA. 712+50 TO STA. 713+15 RAMP 7 (LOC 2)
- STA. 1614+52 TO STA. 1615+17 MAINLINE - SB (LOC 2)
- STA. 1598+00 TO STA. 1598+65 MAINLINE - NB (LOC 2)
- STA. 1556+00 TO STA. 1556+65 MAINLINE - NB&SB (LOC 2)
- STA. 1589+00 TO STA. 1589+65 RAMP 1 (LOC 2)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**BUTT JOINT DETAILS AND
PARTIAL DEPTH PATCHING DETAIL**
FAI 255, FAI 55/70
SECTION (60-7, 60-9)RS &
60-(6-1,7-1)RS
MADISON COUNTY

SCALE: VERT. HORIZ.
DATE: DRAWN BY: CHECKED BY:

Rev. Sheet 6-5-07