

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	1
COVER SHEET				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(641)				

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

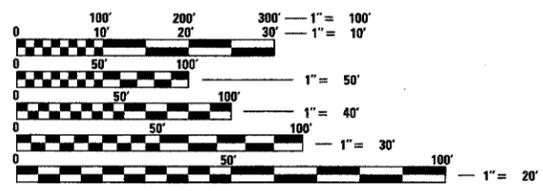
**PLANS FOR PROPOSED FEDERAL AID PROJECT  
F.A.U. ROUTE 3869 (HALIGUS ROAD)  
F.A.P. ROUTE 0339 (ALGONQUIN ROAD) TO REED ROAD  
SECTION 06-00030-00-FP  
PROJECT NO. M-8003(641)  
ROADWAY CONSTRUCTION  
AND TRAFFIC SIGNAL INSTALLATION  
MCHENRY COUNTY  
C-91-350-06**

**INDEX OF SHEETS**

SHEET NO.	DESCRIPTION
1	COVER SHEET
2-4	GENERAL NOTES AND STATE STANDARDS
5-6	SUMMARY OF QUANTITIES
7	TYPICAL SECTIONS
8	SCHEDULE OF QUANTITIES
9	ALIGNMENT, TIES AND BENCHMARKS
10-19	ROADWAY IMPROVEMENT PLANS
20	INTERSECTION GRADING PLANS
21-22	PAVEMENT MARKING AND SIGNING
23-24	LANDSCAPING AND EROSION CONTROL
25	TEMPORARY TRAFFIC SIGNAL PLAN
26	TEMPORARY CABLE PLAN, SEQUENCES, AND SCHEDULE OF QUANTITIES
27	MAST ARM MOUNTED STREET NAME SIGNS
28-31	TRAFFIC SIGNAL DETAILS
32-33	ROADWAY DETAILS
34	BD-03 OUTLET FOR CONCRETE CURB AND GUTTER
35	BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
36	BD-32 BUTT JOINT AND BITUMINOUS TAPER DETAILS
37	TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
38	TC-11 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
39	TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
40	TC-22 TEMPORARY INFORMATION SIGNING
41-56	CROSS SECTIONS



FEDERAL AID ENGINEER: ABIGAIL WILGREEN 847-705-4233 SCHAUMBURG, IL  
CONSULTANT ENGINEER: JONATHAN VANA, P.E. CIVILTECH ENGINEERING, INC.

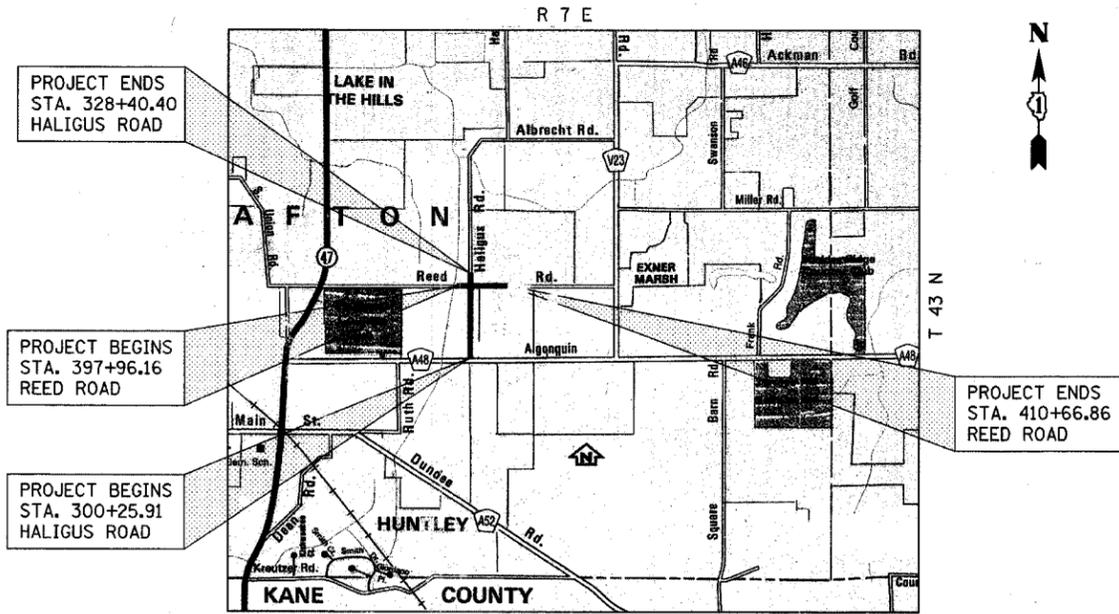


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

**CONTRACT NO. 83919**

THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007 BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, SHALL GOVERN THIS WORK.



LOCATION MAP  
NOT TO SCALE

**PROJECT LENGTH:**  
HALIGUS ROAD - PROJECT LENGTH (NET & GROSS) - 2,814.5 (0.533 MILE)  
REED ROAD - PROJECT LENGTH (NET & GROSS) - 1,271 FT. (0.241 MILE)  
TOTAL PROJECT LENGTH (NET & GROSS) - 4,085.5 FT. (0.774 MILE)

**DESIGN DESIGNATIONS:**  
HALIGUS ROAD - 5000(2030) • COLLECTOR TWS-2 • 0.41 (FD-20)  
REED ROAD - 11000(2030) • COLLECTOR TWS-2 • 0.39 (FD-20)



3-7-07  
REGISTERED P.E., STATE OF ILLINOIS

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	<i>Carl Johnson</i> 2-28-2007 VILLAGE MANAGER, VILLAGE OF HUNTLEY
PASSED	<i>April A. Holt</i> 2007 DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	April 4, 2007 20 <i>Diane O'Keefe</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PLANS PREPARED BY:



450 E. Devon Ave, Suite 300 - Itasca, Illinois 60143  
Tel: 630.773.3900 - Fax: 630.773.3975  
www.civiltechinc.com

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	2

DETAILS		
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT M-8003(64D)

**SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS**

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007 AND THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2007.
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2007; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD); THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", (SSTCI). "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS AND IDOT STANDARD DRAWINGS INCLUDED IN THE CONTRACT DOCUMENTS.
- ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND SHALL BE INCLUDED IN THE PAY ITEM "TRAFFIC CONTROL AND PROTECTION (SPECIAL)"
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED AS THE RESIDENT ENGINEER.
- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET AND APPROPRIATE PERMITS HAVE BEEN OBTAINED FROM THE VILLAGE OF HUNTLEY, THE VILLAGE OF LAKE-IN-THE-HILLS AND THE MCHENRY COUNTY DIVISION OF TRANSPORTATION.
- ALL UTILITY COMPANIES, SCHOOL DISTRICTS, AND LOCAL POLICE AND FIRE DEPARTMENTS SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

**STAKING**

- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- THE STATION/OFFSET/ELEVATIONS NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR EACH STRUCTURE TO SET THE FRAME AND GRATE IN THE PROPER LOCATION. ALL OTHER STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE.
- PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE INDICATED.
- ESTIMATED LOCATIONS OF SIDEWALK AND BIKE PATH REMOVAL HAVE BEEN SHOWN ON THE PLANS. THE ENGINEER WILL DETERMINE THE EXACT LIMITS IN THE FIELD DURING CONSTRUCTION.
- ALL ELEVATIONS SHOWN ON THESE PLANS ARE ON U.S.G.S. DATUM.
- THE CONSTRUCTION BASELINE HAS BEEN ESTABLISHED FOR STAKING PURPOSES ONLY AND IS NOT INTENDED TO BE A CENTERLINE OF RIGHT-OF-WAY.

**PAVING, SHOULDERS AND CURB & GUTTER**

- THE CONTRACTOR SHALL SAW CUT PAVEMENT, CURB & GUTTER, SHOULDER, AND SIDEWALK AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING MATERIAL TO BE REMOVED BY MEANS OF AN APPROVED CONCRETE SAW TO A DEPTH AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE ITEM BEING REMOVED.  
  
THE CONTRACTOR SHALL BE REQUIRED TO SAW VERTICAL CUTS SO AS TO FORM CLEAN VERTICAL JOINTS. SHOULD THE CONTRACTOR DEFACE ANY EDGE, A NEW SAWED JOINT SHALL BE PROVIDED AND ANY ADDITIONAL WORK, INCLUDING REMOVAL AND REPLACEMENT, SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.  
  
IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE THICKNESS OF THE EXISTING PAVEMENT AND WHETHER OR NOT IT CONTAINS REINFORCEMENT.
- HOT-MIX ASPHALT BASE COURSE AND BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER OR CONCRETE SHOULDER UNTIL THE CURB AND GUTTER OR CONCRETE SHOULDER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.
- HOT-MIX ASPHALT SURFACE COURSE SHALL NOT BE PLACED UNTIL ALL EARTH EXCAVATION, TOPSOIL PLACEMENT, AND HOT-MIX ASPHALT BINDER COURSE HAVE BEEN COMPLETED TO THE SATISFACTION OF THE ENGINEER.
- THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED.
- A QUANTITY OF CLASS D PATCHES HAS BEEN INCLUDED FOR THE USE IN MAKING SPOT REPAIRS TO REED ROAD ONCE THE EXISTING SURFACE HAS BEEN REMOVED. THIS WORK SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER.

**TREE REMOVAL, CLEARING AND HEDGE REMOVAL**

- ALL CLEARING AND REMOVAL OF TREES UNDER 6" IN DIAMETER SHALL BE INCIDENTAL TO THE COST OF EARTH EXCAVATION.
- ALL CLEARING AND THE REMOVAL AND/OR RELOCATION OF BUSHES, AS DIRECTED BY THE ENGINEER, SHALL BE INCIDENTAL TO THE COST OF "EARTH EXCAVATION".
- ALL LIMBS, BRANCHES AND OTHER DEBRIS RESULTING FROM THIS WORK SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN EXPENSE OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY.

**UTILITIES**

- THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE LOCATION OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATION OF SUCH UTILITIES AND EXERCISE CARE DURING HIS CONSTRUCTION OPERATIONS SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.31 OF THE "STANDARD SPECIFICATIONS." THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING UTILITIES SO THAT THEIR FACILITIES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF THE CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS OF HIS CONSTRUCTION SCHEDULE AND SHALL COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY OWNERS SO THAT RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER. NOTIFICATION SHALL BE IN WRITING, WITH COPIES TRANSMITTED TO THE ENGINEER.
- COORDINATION OF ANY UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)
- WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT.
- ANY EXISTING OR PROPOSED SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AT NO COST TO THE VILLAGE.
- THE CONTRACTOR SHALL RECEIVE NO ADDITIONAL COMPENSATION FOR CONSTRUCTION STAGING NECESSARY TO ACCOMMODATE UTILITY RELOCATION OR ADJUSTMENT AND/OR FOR DELAYS CAUSED BY UTILITY RELOCATION OR ADJUSTMENT.
- THE CONTRACTOR SHALL FURNISH ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR DEWATERING TRENCH EXCAVATIONS AS WELL AS SHORING TRENCH WALLS DURING UTILITY OPERATIONS. COMPLIANCE WITH THE ABOVE WILL BE INCIDENTAL TO THE UTILITY INSTALLATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING LOCAL AGENCIES MAINTAINING SANITARY SEWERS AND WATER MAINS TO VERIFY THE MATERIALS AND METHODS ALLOWED FOR THE ADJUSTMENT OR RELOCATION OF THEIR FACILITIES, IF NECESSARY.
- WHERE TRENCH BACKFILL IS REQUIRED, THE MATERIAL USED SHALL BE COMPACTED AS SPECIFIED IN ARTICLE 550.07 OF THE "STANDARD SPECIFICATIONS" USING METHOD ONE.

**ROADWAY EXCAVATION**

- ALL EMBANKMENTS AND SUB-GRADE SHALL BE COMPACTED TO THE SATISFACTION OF THE ENGINEER PRIOR TO THE PLACEMENT OF GRANULAR SUB-BASE OR EMBANKMENT.
- ALL EXCESS MATERIAL (BROKEN CONCRETE, SEWER PIPE, WASTE ROADWAY EXCAVATION AND SURPLUS MATERIAL FROM SEWER TRENCHES) SHALL BE LEGALLY DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SELECT DUMP SITES AND OBTAIN PERMISSION AND ALL NECESSARY PERMITS TO USE SUCH DUMP SITES. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN "EARTH EXCAVATION."
- POROUS GRANULAR EMBANKMENT, SUBGRADE AND GEOTECHNICAL FABRIC FOR GROUND STABILIZATION HAS BEEN PROVIDED TO REPLACE SOILS WHICH TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF UNSUITABLE SOILS ARE ENCOUNTERED THE SOILS SHALL BE REMOVED AND REPLACED WITH PGES. THE REMOVAL AND REPLACEMENT AREA SHALL EXTEND TO 12 INCHES BEYOND THE CURB AND GUTTER AND CONCRETE SHOULDER COME UP AT A 1:1 SLOPE TO EXISTING GROUND SURFACE. THESE LIMITS MAY BE ALTERED BY THE ENGINEER IF FIELD CONDITIONS SO WARRANT. REMOVAL OF THESE UNSUITABLE SOILS SHALL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL." EXTRA QUANTITY HAS BEEN PROVIDED FOR REPLACING UNSTABLE SOIL IN AREAS WHERE DRAIN TILES HAVE BEEN REMOVED.

**STORM & SANITARY SEWER**

- THE COST OF MAKING SEWER CONNECTIONS TO EXISTING OR PROPOSED SEWER OR DRAINAGE STRUCTURES SHALL BE INCIDENTAL TO THE COST OF THE SEWER OR STRUCTURE BEING CONSTRUCTED.
- UNLESS OTHERWISE NOTED ON THE PLANS, THE EXISTING DRAINAGE FACILITIES SHALL REMAIN IN USE DURING THE PERIOD OF CONSTRUCTION. LOCATIONS OF EXISTING DRAINAGE STRUCTURES AND SEWERS AS SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO COMMENCING WORK THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL DETERMINE THE EXACT LOCATIONS OF EXISTING STRUCTURES WHICH ARE WITHIN THE PROPOSED CONSTRUCTION LIMITS.  
  
DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWERS, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE SHALL SO INFORM THE ENGINEER, WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF THE NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE. SHOULD THE ENGINEER HAVE DIRECTED THE REPLACEMENT OF A FACILITY, THE NECESSARY WORK AND PAYMENT SHALL BE IN ACCORDANCE WITH SECTIONS 550 AND 601, AND ARTICLE 104.02 OF THE STANDARD SPECIFICATIONS.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWER ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT.
- ALL ABANDONED PIPE AND STRUCTURE INVERTS SHALL BE PLUGGED WITH BRICK AND MORTAR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE STORM OR SANITARY SEWER ITEMS BEING REMOVED.
- THE CONTRACTOR SHALL DETERMINE WHEN FLAT SLAB TOPS ARE REQUIRED ON MANHOLES. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THE USE OF FLAT SLAB TOPS.
- TOP OF FRAME ("RIM") ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF EACH STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATIONS OF THE AREAS IN WHICH THEY ARE LOCATED, AS PART OF THE STRUCTURE COST.
- DRAINAGE STRUCTURE FLAT-TOPS AND CONES SHALL BE TURNED SO THAT THE FRAMES ARE CLOSEST TO THE CENTERLINE OF THE ROAD. ALL FLAT-TOPS AND CONES ARE ASSUMED TO BE ECCENTRIC.
- ALL SEWER AND WATER SERVICES CROSSED BY NEW STORM SEWERS SHALL BE PROPERLY LOCATED AND PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO SAID SERVICES NOT CONSIDERED TO BE IN CONFLICT WITH THE PROPOSED STORM SEWER SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
- ANY TEMPORARY DAMMING OR PUMPING REQUIRED FOR THE EXCAVATIONS FOR THE STORM SEWER CONNECTIONS TO THE ON-SITE PONDS SHALL BE INCLUDED IN THE COST OF THE STORM SEWER BEING CONSTRUCTED.

ILLINOIS DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES**

DATE: 3/7/07  
DESIGNED BY: KRK  
CHECKED BY: JRV

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	3

DETAILS				
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	M-8003641	

**SIGNING, STRIPING & LANDSCAPING**

- WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SEEDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER.
- THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR AT HIS EXPENSE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL EXISTING TRAFFIC SIGNS WHICH INTERFERE WITH THE CONTRACTOR'S WORK SHALL BE REMOVED, A RECORD MADE OF THEIR CONDITION, AND SAFELY STORED AND SAFEGUARDED BY THE CONTRACTOR UNTIL THE ENGINEER DETERMINES THAT THEY BE REINSTALLED IN THE PERMANENT LOCATIONS.
- IMMEDIATELY AFTER EACH SIGN IS REMOVED, A TEMPORARY SIGN OF THE SAME TYPE SHALL BE INSTALLED ON A SIGN SUPPORT APPROVED BY, AND AT A LOCATION DETERMINED BY, THE ENGINEER. THESE SIGNS SHALL BE MAINTAINED STRAIGHT AND CLEAN UNTIL THE PERMANENT SIGNS ARE REINSTALLED.
- ANY SIGN WHICH IS DAMAGED DURING THE TIME IT IS STORED SHALL BE REPAIRED OR REPLACED IN KIND BY THE CONTRACTOR AT HIS OWN EXPENSE PRIOR TO PERMANENT REINSTALLATION.
- ALL UNUSED SIGNS AND POSTS SHALL BE RETURNED TO THE JURISDICTION FROM WHICH IT WAS REMOVED; VILLAGE OF HUNTLEY, VILLAGE OF LAKE IN THE HILLS, OR MCHENRY COUNTY DIVISION OF TRANSPORTATION.
- THE COST OF STORING AND SAFEGUARDING THE PERMANENT SIGNS AND POSTS, AND REINSTALLING THE PERMANENT SIGNS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR "RELOCATE SIGN PANEL ASSEMBLY" OF THE TYPE SPECIFIED. NEW SIGN SUPPORTS SHALL BE USED FOR REINSTALLED SIGNS UNLESS OTHERWISE NOTED. FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE TEMPORARY SIGNS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL AND PROTECTION ITEMS. THE NEW SUPPORTS SHALL BE PAID FOR AS "TELESCOPING STEEL SIGN SUPPORT."
- ALL PROPOSED SIGNS SHALL HAVE TYPE AA REFLECTIVE SHEETING. THIS SHALL BE INCLUDED IN THE COST OF THE PROPOSED SIGN.

**EROSION CONTROL**

- ALL VEGETATIVE AND STRUCTURAL EROSION CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE "ILLINOIS PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL" AND THE "STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL" OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RUNOFF-PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY.
- THE MAINTENANCE AND REPAIR OR REPLACEMENT OF EROSION CONTROL ITEMS, WHEN DIRECTED BY THE ENGINEER, WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED PAY ITEMS.
- ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT. MUD AND SEDIMENT DEPOSITS SHALL BE REMOVED FROM THE ROADWAY AT THE END OF EACH WORK DAY BY SHOVELING AND/OR SWEEPING.
- ALL SLOPES SHALL BE COVERED WITH SEED AS SOON AS GRADING AND PLACEMENT OF TOPSOIL HAS BEEN COMPLETED. THE LIMITS OF THE SEEDING SHALL BE THE LIMITS OF GRADING.
- INLET FILTERS SHALL BE PLACED ON ALL CATCH BASINS, INLETS, AND MANHOLES WITH OPEN GRATES.
- THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER.
- SEE STANDARD 280001-03 FOR ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL DETAILS AND REQUIREMENTS.

**MISCELLANEOUS**

- DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- SITE OBJECTS: REMOVAL OF MISCELLANEOUS PARKWAY IMPROVEMENTS INCLUDING, BUT NOT LIMITED TO, BLOCK RETAINING WALLS, CONCRETE RETAINING WALLS, LANDSCAPE TIMBERS, LANDSCAPE ROCKS, FENCES, FENCE POSTS, PLANTERS, VEGETATION, BRICK OR BRICK PAVER WALKWAYS WITHIN R.O.W. LIMITS SHALL BE INCIDENTAL TO THE PAY ITEM FOR "EARTH EXCAVATION."
- THE CONTRACTOR SHALL ADHERE TO IDOT STANDARD DRAWING NO. 701801-03 WHEN CLOSING ANY SIDEWALK TO PERMIT CONSTRUCTION OF THE IMPROVEMENTS.
- UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOT CROSS COMPLETED BINDER COURSE, OR EXISTING PAVEMENT NOT SCHEDULED TO BE REMOVED, WITH CONSTRUCTION EQUIPMENT WHICH MAY DAMAGE THE PAVEMENT.

**MAINTENANCE OF TRAFFIC GENERAL NOTES**

- TRAFFIC CONTROL DEPICTED IN THESE PLANS AND THE APPLICABLE IDOT DETAILS AND STANDARDS ARE THE MINIMUM REQUIREMENTS. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, DIVISION 700; APPLICABLE GUIDELINES IN THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; AND APPLICABLE HIGHWAY STANDARDS FOR TRAFFIC CONTROL, UNLESS HEREIN REVISED.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES SHALL FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ALL CONSTRUCTION SIGNS SHALL HAVE FLUORESCENT ORANGE BACKGROUNDS.
- ALL SIGNS SHALL BE MOUNTED ON METAL POSTS, 7 FEET ABOVE THE EXISTING GROUND AND DRIVEN A MINIMUM OF 3 FEET INTO THE GROUND. A J.U.L.I.E. LOCATE SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF THE POSTS.
- BARRICADES WILL BE REQUIRED ADJACENT TO PAVEMENT EDGES WHERE WIDENING, CURB AND GUTTER OR OVERLAYING WORK IS BEING DONE, AS SPECIFIED IN SECTION 701 OF THE STANDARD SPECIFICATIONS, EXCEPT THAT ALL BARRICADES SHALL HAVE MONO-DIRECTIONAL STEADY-BURN LIGHTS. SPACING SHALL BE AS SHOWN ON THE STANDARDS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. BARRICADES THAT MUST BE PLACED IN EXCAVATED AREAS SHALL HAVE LEG EXTENSIONS INSTALLED SUCH THAT THE TOPS OF THE BARRICADES ARE IN COMPLIANCE WITH THE HEIGHT REQUIREMENTS OF STANDARD 702001.
- ALL BARRICADES AT LANE DIVERSIONS WITHIN TAPER SECTIONS SHALL HAVE DIRECTION INDICATOR PANELS.
- BARRICADES EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER STRUCTURES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER OR LAW ENFORCEMENT AGENCIES. BARRICADES SHALL BE PLACED AT 50' CENTERS ALONG TANGENTS, 25' ALONG TAPERS AND 10' AROUND RADII.
- DRUMS SHALL HAVE ALTERNATING REFLECTORIZED TYPE AA OR TYPE AP FLUORESCENT ORANGE AND REFLECTORIZED WHITE HORIZONTAL, CIRCUMFERENTIAL STRIPES.
- DRUMS AND BARRICADES SHALL MEET THE REQUIREMENTS OF THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND THE SPECIAL PROVISION "WORK ZONE TRAFFIC CONTROL DEVICES".
- TYPE III BARRICADES ARE TO BE PLACED IN ACCORDANCE WITH STANDARD 702001-06 UNLESS AUTHORIZED BY THE ENGINEER TO USE AN ALTERNATE ARRANGEMENT. A MINIMUM OF TWO TYPE III BARRICADES WITH ROAD CLOSED SIGNS, R11-2 SHALL BE PLACED AT EACH OF THE FOLLOWING LOCATIONS: THE SOUTH END OF HALIGUS ROAD, THE NORTH END OF HALIGUS ROAD (SOUTH OF REED ROAD), AND AT THE EXISTING END OF FAITHS WAY. BARRICADES SHALL BE REMOVED WHEN HALIGUS ROAD IS OPENED TO TRAFFIC. THIS WORK SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".
- THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WEEKS IN ADVANCE OF THE CHANGE.
- EXISTING TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE REMOVED OR RELOCATED BY THE CONTRACTOR AFTER THE TRAFFIC CONTROL REQUIREMENTS ARE MET OR AS AUTHORIZED BY THE ENGINEER; ANY SIGNS OR DEVICES LEFT IN PLACE ARE TO BE PROTECTED FROM DAMAGE AND MAINTAINED.
- THE FIRST WARNING SIGNS IN EACH DIRECTION OF TRAVEL SHALL BE EQUIPPED WITH MONO-DIRECTIONAL AMBER FLASHING LIGHTS DURING HOURS OF DARKNESS. FLAGS ARE OPTIONAL.
- EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- TEMPORARY LANE CLOSURES WILL BE ALLOWED ONLY BETWEEN THE HOURS OF 9:00 A.M. AND 3:00 P.M., WITH TRAFFIC MAINTAINED IN ACCORDANCE WITH STANDARD 701501-03 OR 701606-04 UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL NOTIFY THE MCHENRY COUNTY DIVISION OF TRANSPORTATION PRIOR TO CLOSING ANY LANES OR TEMPORARILY SHIFTING TRAFFIC ON ALGONQUIN ROAD.
- "WORKERS" SIGNS SHALL ONLY BE ERECTED WHEN WORKERS ARE PRESENT. SIGN MUST BE COVERED OR REMOVED WHEN NO WORKERS ARE PRESENT.
- "FRESH OIL" SIGNS (#21-2-4848) WITH DATE SIGNS SHALL BE ERECTED 48 HOURS PRIOR TO PRIMING ALONG HALIGUS ROAD AND REED ROAD. THE COST OF THESE SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".
- ARROW BOARDS WILL BE REQUIRED WHEN IMPLEMENTING ALL LANE CLOSURES, AND SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)".
- THE COST OF SUPPLYING, ERECTING, AND MAINTAINING BARRICADES, DRUMS, WARNING LIGHTS, AND SIGNS SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)". QUANTITIES FOR SHORT-TERM PAVEMENT MARKINGS AND TEMPORARY PAVEMENT MARKINGS ARE NOT INCLUDED IN "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND SHALL BE MEASURED SEPARATELY FOR PAYMENT.

**IDOT STANDARDS**

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-03	TEMPORARY EROSION CONTROL SYSTEMS
424001-04	SIDEWALK RAMPS ACCESSIBLE TO THE DISABLED
442201-02	CLASS C AND D PATCHES
542301-01	PRECAST REINFORCED CONCRETE FLARED END SECTION
542311	GRATING FOR CONCRETE FLARED END SECTION (FOR 24" THRU 54" PIPE)
602001	CATCH BASIN TYPE A
602301-01	INLET, TYPE A
602401-01	MANHOLE TYPE A
602601-01	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-01	CAST IRON STEPS
604001-02	FRAME AND LIDS TYPE 1
604036-01	GRATE, TYPE 8
606001-03	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-02	OFF-ROAD OPERATIONS, 2L 2W, 4.5 m (15') TO 600 mm (24") AWAY, FOR SPEEDS > OR = 45 MPH
701201-02	LANE CLOSURE, 2L 2W, DAY ONLY, ON-ROAD TO 600 mm (24") OFF-ROAD, FOR SPEEDS > OR = 45 MPH
701301-02	LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS, FOR SPEEDS > OR = 45 MPH
701326-02	LANE CLOSURE, 2L 2W, PAVEMENT WIDENING, FOR SPEEDS > OR = 45 MPH
701501-03	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-04	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701801-03	LANE CLOSURE, MULTILANE, 1W OR 2W, CROSSWALK OR SIDEWALK CLOSURE, FOR SPEEDS < 45 MPH
702001-06	TRAFFIC CONTROL DEVICES
720001	SIGN PANEL MOUNTING DETAILS
720006-01	SIGN PANEL ERECTION DETAILS
720016-01	MAST ARM MOUNTED STREET NAME SIGNS
728001	TELESCOPING STEEL SIGN SUPPORT
780001-01	TYPICAL PAVEMENT MARKINGS
805001	ELECTRICAL SERVICE INSTALLATION DETAILS
857001	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
880001	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION

ILLINOIS DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES**

DATE: 3/7/07  
DESIGNED BY: KRK  
CHECKED BY: JRV



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869 06-00030-00-FF	MCHENRY		56	5
SUMMARY OF QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(641)				

CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				1000-2A	Y031-1F	100% VILLAGE
20101100	TREE TRUNK PROTECTION	EACH	3	3		
20101200	TREE ROOT PRUNING	EACH	3	3		
20200100	EARTH EXCAVATION	CU YD	3056	3056		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	11161	11161		
20400800	FURNISHED EXCAVATION	CU YD	228	228		
20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	785	785		
20800150	TRENCH BACKFILL	CU YD	98	98		
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	2500	2500		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	14423	14423		
21300010	EXPLORATION TRENCH, SPECIAL	FOOT	400	400		
25000110	SEEDING, CLASS 1A	ACRE	2.7	2.7		
25000300	SEEDING, CLASS 3	ACRE	0.3	0.3		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	270	270		
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	270	270		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	270	270		
25100630	EROSION CONTROL BLANKET	SQ YD	14423	14423		
25200200	SUPPLEMENTAL WATERING	UNIT	220	220		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	300	300		
28000300	TEMPORARY DITCH CHECKS	EACH	69	69		
28000400	PERIMETER EROSION BARRIER	FOOT	2458	2458		
28000510	INLET FILTERS	EACH	14	14		
28100105	STONE RIPRAP, CLASS A3	SQ YD	28	28		
31101400	SUB-BASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	3093	3093		
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	1577	1577		
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	11739	11739		
40600300	AGGREGATE (PRIME COAT)	TON	38	38		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	83	83		
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	520	520		
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	788	788		
40701841	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 8"	SQ YD	12259	12259		
42001300	PROTECTIVE COAT	SQ YD	1201	1201		
42400430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	1045	1045		
42400800	DETECTABLE WARNING	SQ FT	213	213		
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	694	694		
44000196	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	4959	4959		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	201	201		
44000600	SIDEWALK REMOVAL	SQ FT	302	302		
44004510	PORTLAND CEMENT CONCRETE SHOULDER REMOVAL	SQ YD	57	57		
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	250	250		
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	6504	6504		
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	604	604		
48300300	PORTLAND CEMENT CONCRETE SHOULDERS 8"	SQ YD	802	802		
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 15"	EACH	2	2		
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 18"	EACH	1	1		
54213672	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, 27"	EACH	3	3		
54247140	GRATING FOR CONCRETE FLARED END SECTION 27"	EACH	1	1		
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	477	477		
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	72	72		

CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				1000-2A	Y031-1F	100% VILLAGE
550A0130	STORM SEWERS, CLASS A, TYPE 1 27"	FOOT	183	183		
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	13	13		
550A0420	STORM SEWERS, CLASS A, TYPE 2 27"	FOOT	28	28		
55034400	STORM SEWERS, TYPE 1, REINFORCED CONCRETE ELLIPTICAL PIPE, SPAN 34, RISE 22	FOOT	72	72		
* 56103300	DUCTILE IRON WATER MAIN 12"	FOOT	90			90
* 56105200	WATER VALVES 12"	EACH	1			1
* 56106600	ADJUSTING WATER MAIN 12"	FOOT	50	50		
* 56400100	FIRE HYDRANTS TO BE MOVED	EACH	2			2
* 56400300	FIRE HYDRANTS TO BE ADJUSTED	EACH	1	1		
60100905	PIPE DRAINS 4"	FOOT	100	100		
60100935	PIPE DRAINS 10"	FOOT	100	100		
60109510	PIPE UNDERDRAINS, FABRIC LINED TRENCH 4"	FOOT	349	349		
60203905	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2		
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3		
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3		
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	8	8		
60248900	VALVE VAULTS, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1			1
60255600	MANHOLES TO BE ADJUSTED (SPECIAL)	EACH	2			2
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	7	7		
60258000	MANHOLES TO BE RECONSTRUCTED (SPECIAL)	EACH	1			1
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1		
60266100	VALVE VAULTS TO BE RECONSTRUCTED	EACH	3	3		
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	1			1
60500405	FILLING VALVE VAULTS	EACH	2	2		
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	2.3	2.3		
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	827	827		
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	132	132		
66700095	PERMANENT SURVEY MARKERS	EACH	1	1		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6		
67100100	MOBILIZATION	L SUM	1	1		
70101800	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1		
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	913	913		
70300610	TEMPORARY PAINT PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	381	381		
70300625	TEMPORARY PAINT PAVEMENT MARKING LINE 4"	FOOT	24345	24345		
70300635	TEMPORARY PAINT PAVEMENT MARKING LINE 6"	FOOT	2589	2589		
70300645	TEMPORARY PAINT PAVEMENT MARKING LINE 12"	FOOT	1242	1242		
70300660	TEMPORARY PAINT PAVEMENT MARKING LINE 24"	FOOT	178	178		
* 72000100	SIGN PANEL - TYPE 1	SQ FT	84	84		
* 72000105	SIGN PANEL - TYPE 1 (SPECIAL)	SQ FT	32			32
* 72000200	SIGN PANEL - TYPE 2	SQ FT	10	10		
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	10	10		
* 72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	5	5		

\* SPECIALTY ITEMS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

DATE 3/7/07  
DRAWN BY DJK  
CHECKED BY JRV

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FF	MCHENRY	56	6
SUMMARY OF QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-80036411				

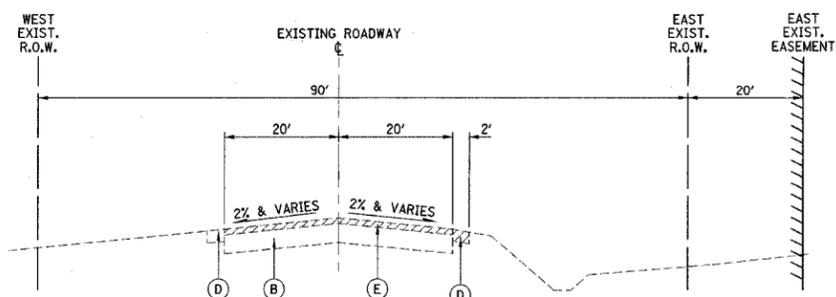
CODED PAY ITEM NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				1000-2A	Y031-1F	Y06100% VILLAGE
• 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	225	225		
• 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	381	381		
• 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	24345	24345		
• 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2589	2589		
• 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1242	1242		
• 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	178	178		
• 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	27	27		
78300100	PAVEMENT MARKING REMOVAL	SQ FT	1586	1586		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	26	26		
• 81018500	CONDUIT PUSHED, 2" DIAMETER, GALVANIZED STEEL	FOOT	15		15	
• 84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	1	1		
• 85700205	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1			1
• 87200400	SPAN WIRE	FOOT	618		618	
• 87200500	TETHER WIRE	FOOT	589		589	
• 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	34		34	
• 87302225	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 3C	FOOT	346		346	
• 87302245	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 5C	FOOT	985		985	
• 87302255	ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 7C	FOOT	1970		1970	
• 88000180	SIGNAL HEAD, 1-FACE, 3-SECTION, SPAN WIRE MOUNTED	EACH	4		4	
• 88000300	SIGNAL HEAD, 1-FACE, 5-SECTION, SPAN WIRE MOUNTED	EACH	8		8	
• 88700200	LIGHT DETECTOR	EACH	2			2
• 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1			1
Z0001050	AGGREGATE SUBGRADE 12"	SQ YD	15578	15578		
△ Z0076600	TRAINEES	HOURS	500	500		
• X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	78	78		
X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	42	42		
• X8050010	SERVICE INSTALLATION, GROUND MOUNTED	EACH	1		1	
• X8730350	ELECTRIC CABLE AERIAL SUSPENDED NO. 20 3/C, TWISTED, SHIELDED	FOOT	346			346
XX004801	BITUMINOUS BIKE PATH REMOVAL	SQ YD	231	231		
* XX005107	CONNECTION TO EXISTING WATER MAIN (NON-PRESSURE) - 12"	EACH	1			1
• XX005723	VIDEO DETECTION SYSTEM COMPLETE INTERSECTION	EACH	1			1
XX005770	STRUCTURE TO BE ABANDONED	EACH	1	1		
• XX006963	TRAFFIC SIGNAL, WOOD POLE, 45 FT, CLASS 5	EACH	4		4	
• XX006964	ELECTRIC CABLE AERIAL SUSPENDED, COMMUNICATION, NO. 16 5 1/2 PAIR	FOOT	763		763	

• INDICATES SPECIALTY ITEMS  
 △ Y080

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SUMMARY OF QUANTITIES

DRAWN BY DJK  
 CHECKED BY JRV  
 DATE 3/7/07

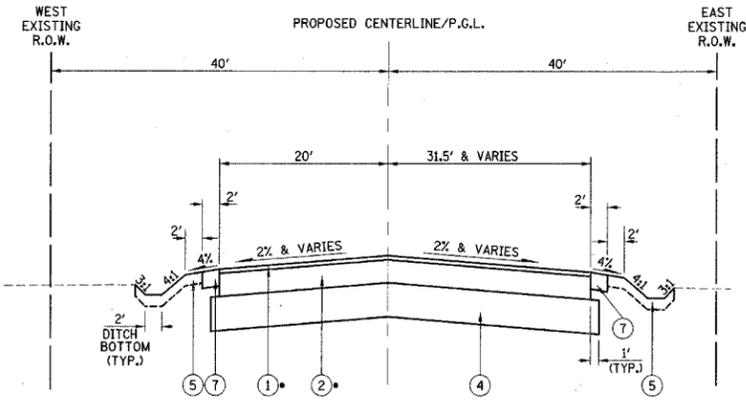
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FF	MCHENRY	56	7
DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-800364(I)				



**EXISTING TYPICAL SECTION**

HALIGUS ROAD  
STA. 326+64 TO STA. 328+40.40

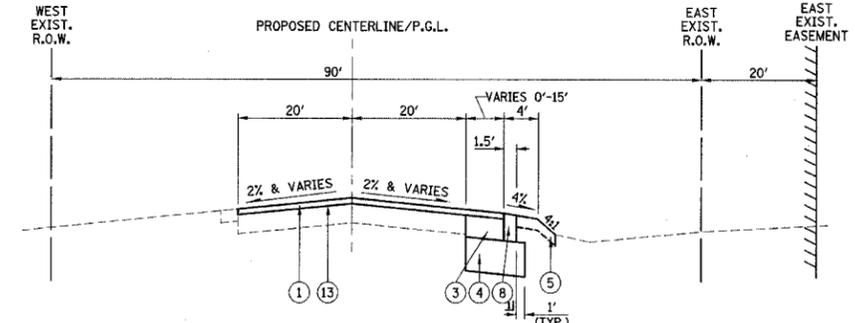
REMOVAL ITEMS



**PROPOSED TYPICAL SECTION**

HALIGUS ROAD  
STA. 300+25.91 TO STA. 301+05.04

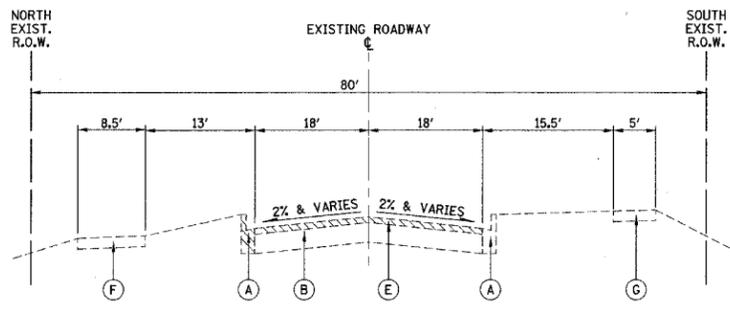
• PAID FOR AS "HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 8"



• STA. 326+64 TO STA. 327+13:  
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

**PROPOSED TYPICAL SECTION**

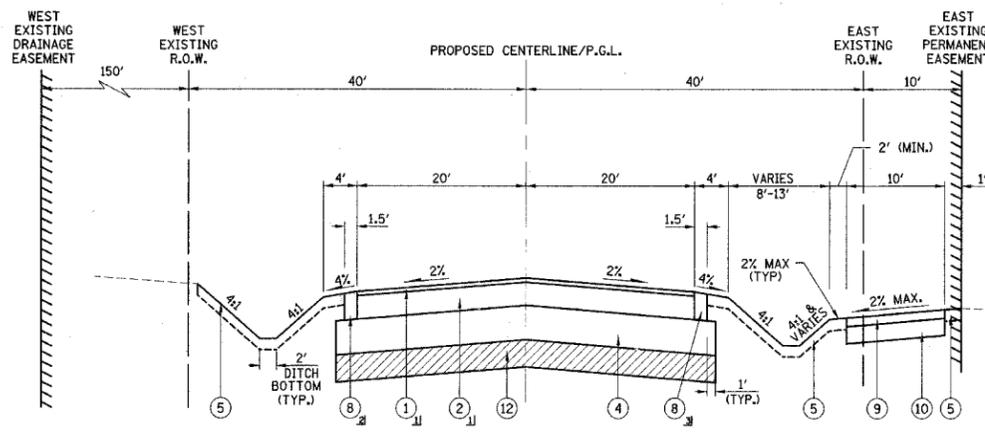
HALIGUS ROAD  
STA. 326+64 TO STA. 328+40.40



**EXISTING TYPICAL SECTION**

REED ROAD  
STA. 397+96.16 TO STA. 400+00

REMOVAL ITEMS

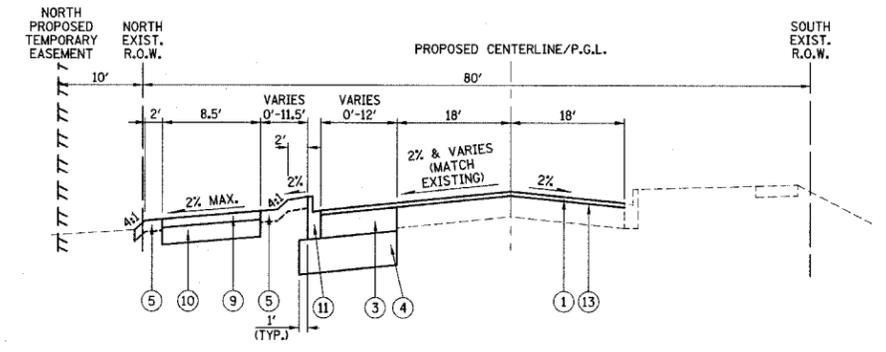


**PROPOSED TYPICAL SECTION**

HALIGUS ROAD  
STA. 301+05.04 TO STA. 326+00  
SEE PLAN AND PROFILE SHEETS FOR STATION LIMITS OF EXISTING DRAINAGE EASEMENTS

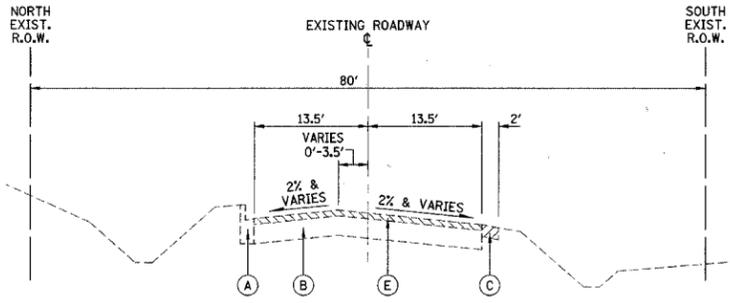
- 1 PAID FOR AS "HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 8"
- 2 STA. 325+68 TO STA. 326+00:  
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- 3 STA. 301+05.04 TO STA. 302+20:  
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- 4 STA. 325+50 TO STA. 326+00:  
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

REMOVAL ITEMS



**PROPOSED TYPICAL SECTION**

REED ROAD  
STA. 397+96.16 TO STA. 400+00



**EXISTING TYPICAL SECTION**

REED ROAD  
STA. 400+00 TO STA. 410+66.86

REMOVAL ITEMS

- PROPOSED LEGEND**
- 1 HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50 (2")
  - 2 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (6")
  - 3 HOT-MIX ASPHALT BASE COURSE, 6"
  - 4 AGGREGATE SUBGRADE, 12"
  - 5 TOPSOIL FURNISH & PLACE, 4"  
SEEDING, CLASS 1A
  - 6 STRIP REFLECTIVE CRACK CONTROL TREATMENT
  - 7 AGGREGATE SHOULDER, TYPE B, 6"
  - 8 PORTLAND CEMENT CONCRETE SHOULDERS 8"
  - 9 HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50 (3 IN.)
  - 10 SUB-BASE GRANULAR MATERIAL, TYPE B 6"
  - 11 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
  - 12 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
POROUS GRANULAR EMBANKMENT, SUBGRADE  
SEE ROADWAY SOILS INVESTIGATION BY MIDLAND STANDARD  
ENGINEERING, INC. DATED NOVEMBER 30, 2006 AND THE CROSS  
SECTIONS FOR ADDITIONAL INFORMATION.
  - 13 AREA REFLECTIVE CRACK CONTROL TREATMENT

**EXISTING LEGEND**

- A COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
- B BITUMINOUS CONCRETE PAVEMENT
- C AGGREGATE SHOULDER
- D CONCRETE SHOULDER
- E HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL (2" MIN. & VARIES)
- F BITUMINOUS PATH
- G CONCRETE SIDEWALK

**MIXTURE REQUIREMENT**

PAY ITEM	AC TYPE	VOIDS
HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 MM); 2"	PG 64-22	4% @ 50 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 6"	PG 64-22 / 58-22	4% @ 50 GYR.
PAVEMENT WIDENING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 MM); 2"	PG 64-22	4% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 MM); 6"	PG 64-22 / 58-22	4% @ 50 GYR.
RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 MM); 2"	PG 64-22	4% @ 50 GYR.
SHARED USE PATH		
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5 MM); 3"	PG 64-22	4% @ 50 GYR.
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 MM); 8"	PG 64-22 / 58-22	4% @ 70 GYR.

NOTES: THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LB/SY-IN.  
WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

**PROPOSED TYPICAL SECTION**

REED ROAD  
STA. 400+00 TO STA. 410+66.86

• STA. 400+52 TO STA. 402+04:  
COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
HALIGUS ROAD & REED ROAD

DATE: 3/7/07  
DESIGNED BY: KRK  
CHECKED BY: JRV

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FF	MCHENRY	56	8
SCHEDULE OF QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003641				

SCHEDULE OF RELOCATE SIGN PANEL ASSEMBLY - TYPE A	
PROPOSED LOCATION	TELESCOPING STEEL SIGN SUPPORT (FT)
398+60, LT	14.5
326+97, LT	14.5
327+11, LT	16.0
401+43, RT	14.5
400+61, LT	0

SCHEDULE OF SIGN PANEL - TYPE 1				
PROPOSED LOCATION	SIGN NUMBER	DIMENSIONS	AREA (SQ FT)	TELESCOPING STEEL SIGN SUPPORT (FT)
301+50, RT	R2-1	24"X30"	5.0	14.5
302+53, LT	R3-8	30"X30"	6.25	14.5
308+21, LT	W11-1	30"X30"	6.25	14.5
	W16-7	24"X18"	3.0	16.0
308+30, LT	R1-1	36"X36"	9.0	16.5
	R1-1100	24"X18"	3.0	16.5
308+99, LT	W11-1	30"X30"	6.25	14.5
325+00, LT	R2-1	24"X30"	5.0	14.5
325+78, RT	R1-1	36"X36"	9.0	16.5
	R1-1100	24"X18"	3.0	16.5
327+11, LT	R1-1100	24"X18"	3.0	0
392+70, RT	W2-1	30"X30"	6.25	0
399+15.5, RT	W11-1	30"X30"	6.25	14.5
400+70, LT	W11-1	30"X30"	6.25	14.5
404+71, LT	W2-1	30"X30"	6.25	0
SCHEDULE OF SIGN PANEL - TYPE 2				
299+71, LT	R3-8B	48"X30"	10.0	29.0

EARTHWORK SCHEDULE			
ITEM	UNIT	HALIGUS ROAD	REED ROAD
EARTH EXCAVATION	C.Y.	2566	490
EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	C.Y.	2182	417
EMBANKMENT REQUIRED	C.Y.	2296	531
EARTHWORK BALANCE			
WASTE (+) OR SHORTAGE (-)	C.Y.	-114	-114
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL			
TOPSOIL STRIPPING	C.Y.	9883	493
UNDERCUT	C.Y.	785	0
TOTAL	C.Y.	11161	

NOTE: SHRINKAGE CALCULATED USING 15% SHRINKAGE FACTOR

UNDERCUT DEPTH SCHEDULE		
FROM	TO	DEPTH
STA. 303+50	STA. 306+00	10"

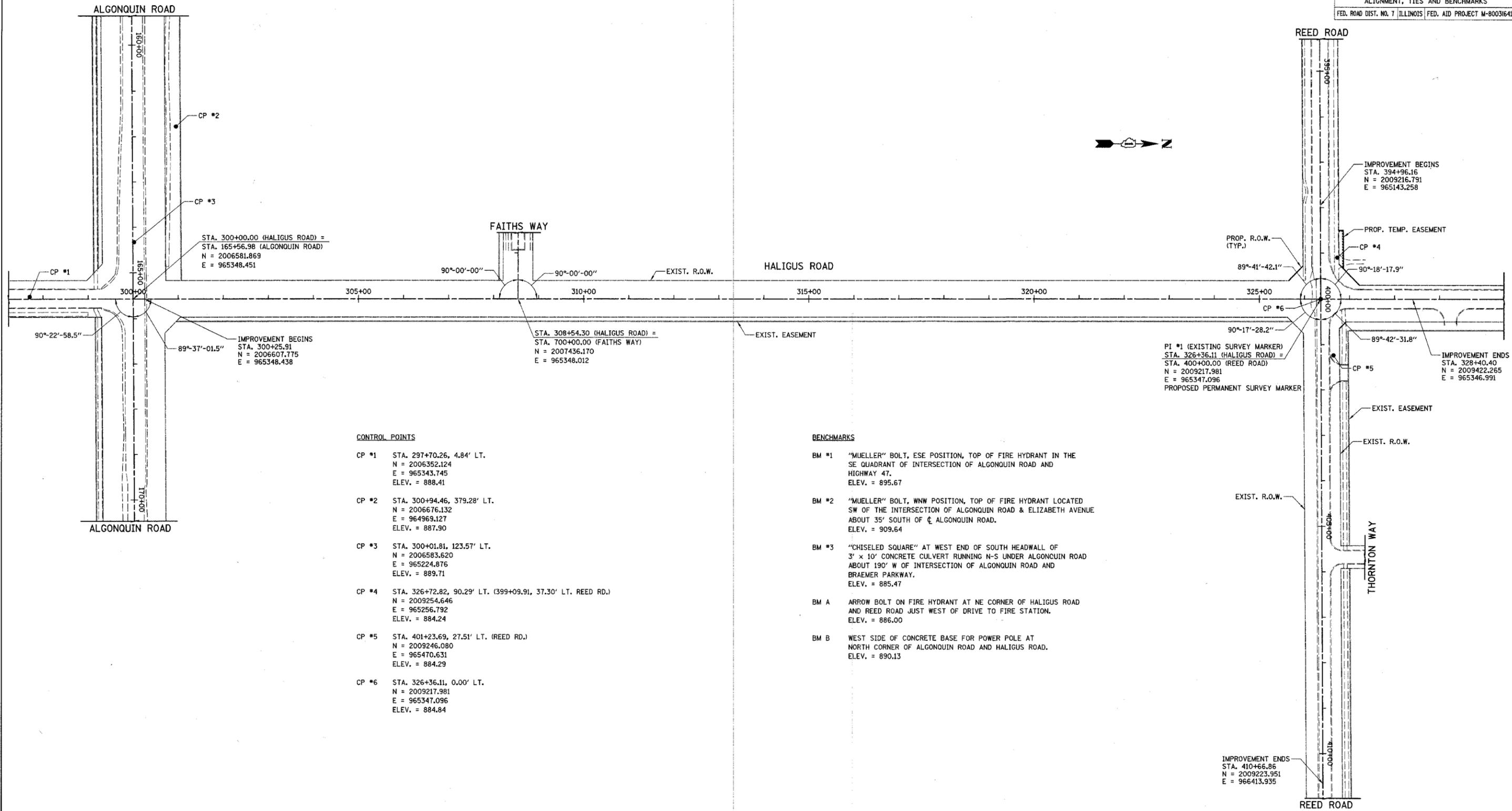
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

DATE 3/7/07  
 DRAWN BY DJK  
 CHECKED BY JRV

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	9
ALIGNMENT, TIES AND BENCHMARKS				
FED. ROAD DIST. NO. 7   ILLINOIS   FED. AID PROJECT M-800364D				



STA. 300+00.00 (HALIGUS ROAD) =  
 STA. 165+56.98 (ALGONQUIN ROAD)  
 N = 2006581.869  
 E = 965348.451

STA. 308+54.30 (HALIGUS ROAD) =  
 STA. 700+00.00 (FAITHS WAY)  
 N = 2007436.170  
 E = 965348.012

PI #1 (EXISTING SURVEY MARKER)  
 STA. 326+36.11 (HALIGUS ROAD) =  
 STA. 400+00.00 (REED ROAD)  
 N = 2009217.981  
 E = 965347.096  
 PROPOSED PERMANENT SURVEY MARKER

IMPROVEMENT BEGINS  
 STA. 394+96.16  
 N = 2009216.791  
 E = 965143.258

IMPROVEMENT ENDS  
 STA. 328+40.40  
 N = 2009422.265  
 E = 965346.991

**CONTROL POINTS**

- CP #1 STA. 297+70.26, 4.84' LT.  
 N = 2006352.124  
 E = 965343.745  
 ELEV. = 888.41
- CP #2 STA. 300+94.46, 379.28' LT.  
 N = 2006676.132  
 E = 964969.127  
 ELEV. = 887.90
- CP #3 STA. 300+01.81, 123.57' LT.  
 N = 2006583.620  
 E = 965224.876  
 ELEV. = 889.71
- CP #4 STA. 326+72.82, 90.29' LT. (399+09.91, 37.30' LT. REED RD.)  
 N = 2009254.646  
 E = 965256.792  
 ELEV. = 884.24
- CP #5 STA. 401+23.69, 27.51' LT. (REED RD.)  
 N = 2009246.080  
 E = 965470.631  
 ELEV. = 884.29
- CP #6 STA. 326+36.11, 0.00' LT.  
 N = 2009217.981  
 E = 965347.096  
 ELEV. = 884.84

**BENCHMARKS**

- BM #1 "MUELLER" BOLT, ESE POSITION, TOP OF FIRE HYDRANT IN THE SE QUADRANT OF INTERSECTION OF ALGONQUIN ROAD AND HIGHWAY 47.  
 ELEV. = 895.67
- BM #2 "MUELLER" BOLT, WNW POSITION, TOP OF FIRE HYDRANT LOCATED SW OF THE INTERSECTION OF ALGONQUIN ROAD & ELIZABETH AVENUE ABOUT 35' SOUTH OF CL ALGONQUIN ROAD.  
 ELEV. = 909.64
- BM #3 "CHISELED SQUARE" AT WEST END OF SOUTH HEADWALL OF 3' x 10' CONCRETE CULVERT RUNNING N-S UNDER ALGONQUIN ROAD ABOUT 190' W OF INTERSECTION OF ALGONQUIN ROAD AND BRAEMER PARKWAY.  
 ELEV. = 885.47
- BM A ARROW BOLT ON FIRE HYDRANT AT NE CORNER OF HALIGUS ROAD AND REED ROAD JUST WEST OF DRIVE TO FIRE STATION.  
 ELEV. = 886.00
- BM B WEST SIDE OF CONCRETE BASE FOR POWER POLE AT NORTH CORNER OF ALGONQUIN ROAD AND HALIGUS ROAD.  
 ELEV. = 890.13

ILLINOIS DEPARTMENT OF TRANSPORTATION

**ALIGNMENT, TIES AND BENCHMARKS**

**HALIGUS ROAD**

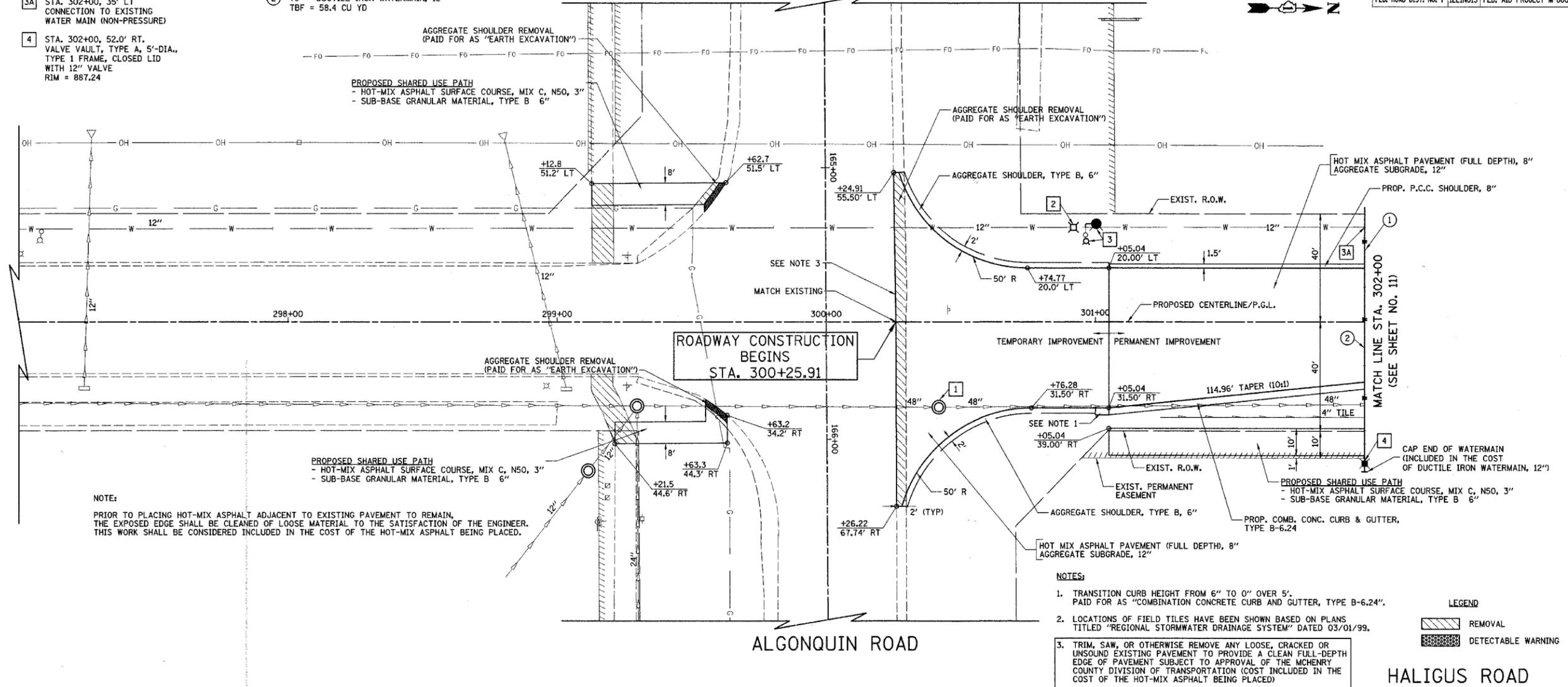
SCALE IN FEET

DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRJ

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FF	MCHENRY	56	10
STA. 300+25.91		TO STA. 302+00		
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT M-8003(641)

SEE SHEET NO. 21 FOR RESURFACING LIMITS ON ALGONQUIN ROAD

- 1 STA. 300+41.87, 31.24' RT. STRUCTURE TO BE ABANDONED
- 2 STA. 300+92, 35.00' LT. V.V. TO BE RECONSTRUCTED EXIST. RIM = 889.15 PROP. RIM = 888.58
- 3 STA. 300+97, 30.00' LT. FH TO BE MOVED TO STA. 301+00.33, 36.38' LT.
- 3A STA. 302+00, 35' LT CONNECTION TO EXISTING WATER MAIN (NON-PRESSURE)
- 4 STA. 302+00, 52.0' RT. VALVE VAULT, TYPE A, 5'-DIA., TYPE 1 FRAME, CLOSED LID WITH 12" VALVE RIM = 887.24
- ① 12" - DUCTILE IRON WATERMAIN, 12" TBF = 0 CU YD
- ② 78" - DUCTILE IRON WATERMAIN, 12" TBF = 58.4 CU YD

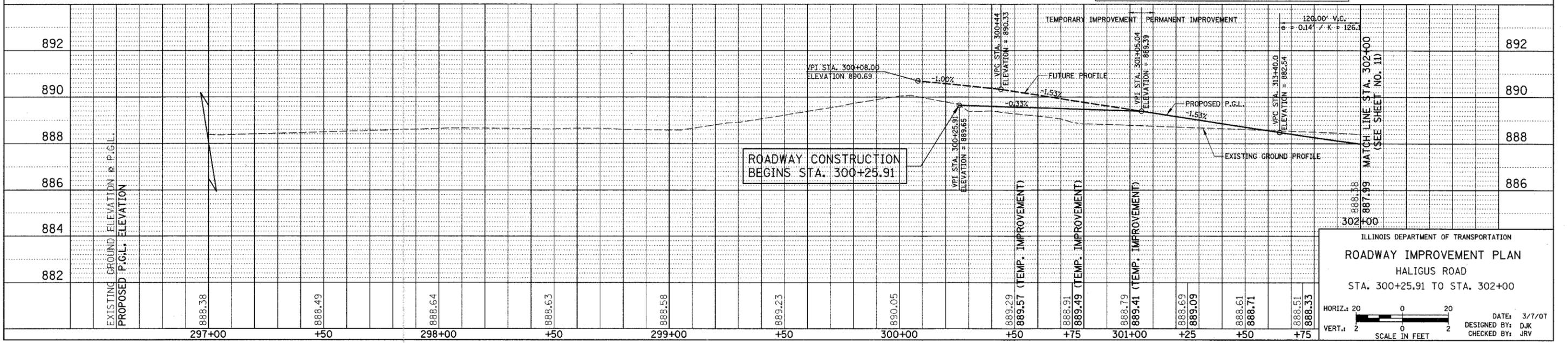


NOTE:  
PRIOR TO PLACING HOT-MIX ASPHALT ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE HOT-MIX ASPHALT BEING PLACED.

- NOTES:
- TRANSITION CURB HEIGHT FROM 6" TO 0" OVER 5'. PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24".
  - LOCATIONS OF FIELD TILES HAVE BEEN SHOWN BASED ON PLANS TITLED "REGIONAL STORMWATER DRAINAGE SYSTEM" DATED 03/01/99.
  - TRIM, SAW, OR OTHERWISE REMOVE ANY LOOSE, CRACKED OR UNSOUND EXISTING PAVEMENT TO PROVIDE A CLEAN FULL-DEPTH EDGE OF PAVEMENT SUBJECT TO APPROVAL OF THE MCHENRY COUNTY DIVISION OF TRANSPORTATION (COST INCLUDED IN THE COST OF THE HOT-MIX ASPHALT BEING PLACED)

LEGEND

	REMOVAL
	DETECTABLE WARNING



ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ROADWAY IMPROVEMENT PLAN**  
 HALIGUS ROAD  
 STA. 300+25.91 TO STA. 302+00

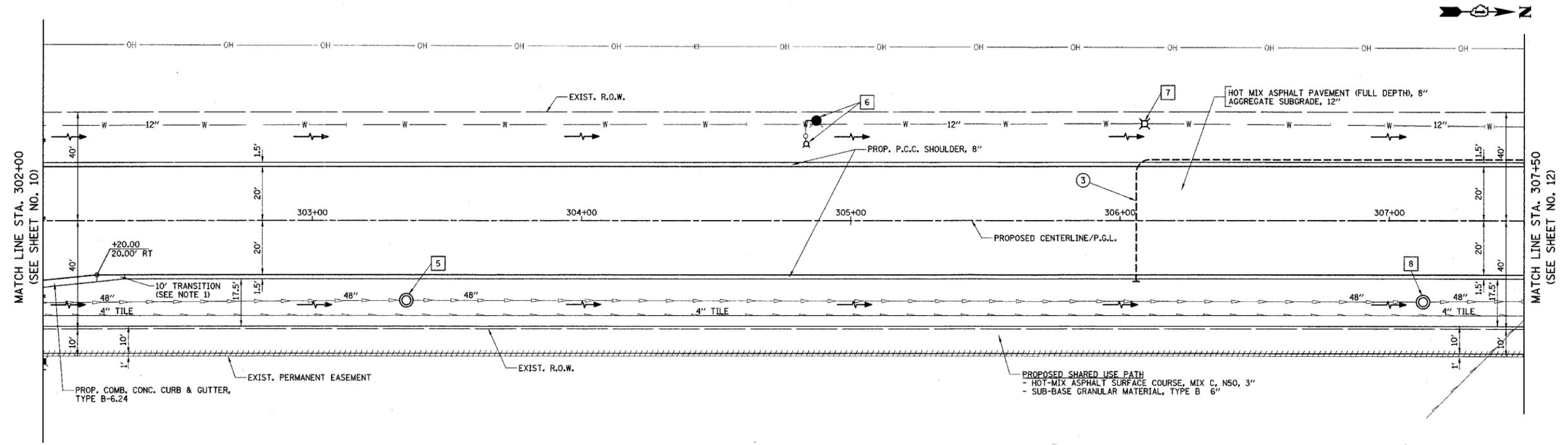
HORIZ. 20  
 VERT. 2

SCALE IN FEET

DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRJ

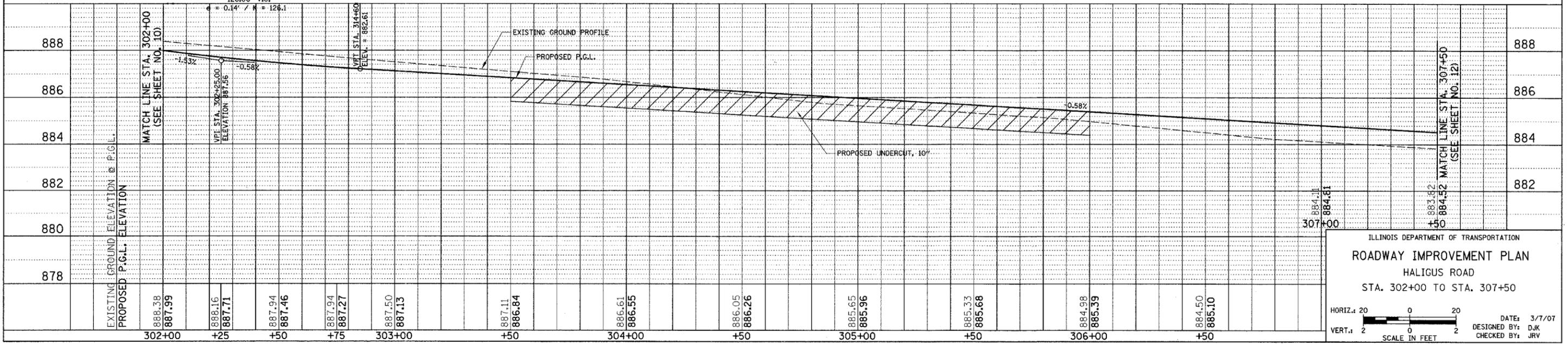
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	11
STA. 302+00		TO STA. 307+50		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT M-8003(641)	

- 5 STA. 303+34.90, 29.40' RT. MH TO BE RECONSTRUCTED PROP. RIM = 884.83
- 6 STA. 304+83.44, 28.35 LT. FH TO BE MOVED TO STA. 304+87.17, 36.88' LT.
- 7 STA. 306+09, 36.00 LT. V.V. TO BE RECONSTRUCTED EXIST. RIM = 885.40 PROP. RIM = 884.73
- 8 STA. 307+12.50, 29.97' RT. MH TO BE RECONSTRUCTED PROP. RIM = 882.10
- 3 24" - 4" PIPE UNDERDRAIN, FABRIC LINED TRENCH



- NOTES:**
- TRANSITION CURB HEIGHT FROM 6" TO 0" AND TRANSITION CURB AND GUTTER WIDTH FROM 2.5' TO 1.5' OVER 10'. PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24".
  - LOCATIONS OF FIELD TILES HAVE BEEN SHOWN BASED ON PLANS TITLED "REGIONAL STORMWATER DRAINAGE SYSTEM" DATED 03/01/99.
  - THE EXISTING MANHOLES ON THE 48" SEWER WERE INSTALLED AS PART OF THE "REGIONAL STORMWATER DRAINAGE SYSTEM". ACCORDING TO THE PLANS FOR THE STORM SEWER INSTALLATION, THE MANHOLE LIDS WERE INSTALLED APPROXIMATELY 2' BELOW EXISTING GRADE. ALL WORK REQUIRED TO LOCATE AND EXPOSE THE EXISTING STRUCTURES SHALL BE INCLUDED IN THE COST OF "MANHOLES TO BE RECONSTRUCTED."

HALIGUS ROAD



ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ROADWAY IMPROVEMENT PLAN**  
 HALIGUS ROAD  
 STA. 302+00 TO STA. 307+50

HORIZ. 20 0 20  
 VERT. 2 0 2  
 SCALE IN FEET

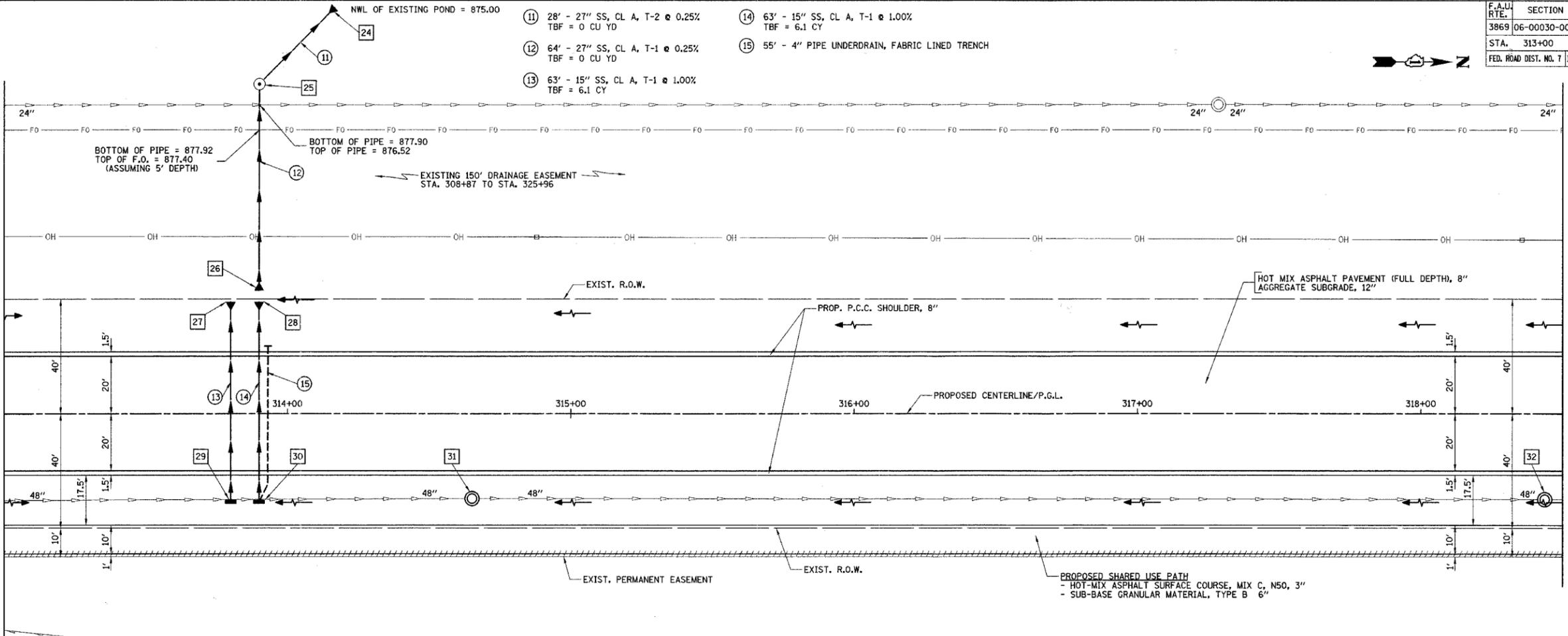
DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRV



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	13
STA. 313+00		TO STA. 318+50		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT M-8003(641)	

- 24 STA. 314+6.5, 141.5' LT  
PRC FLARED END SECTION, 27"  
INV = 875.00
- 25 STA. 313+90, 115' LT  
MH, T-A, 5'-DIA.,  
T-1 FRAME AND CLOSED LTD  
RIM = 882.40  
INV = 878.13 (27" E)  
INV = 875.08 (27" NW)
- 26 STA. 313+90, 43' LT  
PRC FLARED END SECTION, 27"  
WITH GRATE  
INV = 878.30
- 27 STA. 313+80, 39' LT  
PRC FLARED END SECTION, 15"  
INV = 878.35
- 28 STA. 313+90, 39' LT  
PRC FLARED END SECTION, 15"  
INV = 878.30
- 29 STA. 313+80, 30.80' RT  
INLET, T-A, T-8 GRATE  
RIM = 880.55  
INV = 879.05 (15' W)
- 30 STA. 313+90, 30.80' RT  
INLET, T-A, T-8 GRATE  
RIM = 880.50  
INV = 879.00 (15' W)  
INV = 879.33 (4' N)
- 31 STA. 314+65.14, 29.63' RT  
MH TO BE RECONSTRUCTED  
PROP. RIM = 879.06
- 32 STA. 318+43.69, 29.92' RT  
MH TO BE RECONSTRUCTED  
PROP. RIM = 881.59

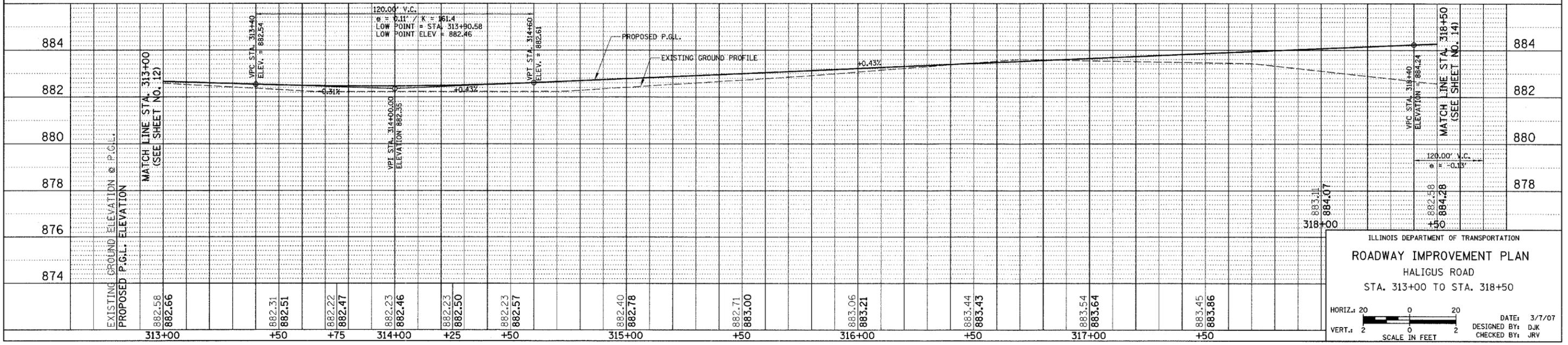
- 11 28' - 27" SS, CL A, T-2 @ 0.25%  
TBF = 0 CU YD
- 12 64' - 27" SS, CL A, T-1 @ 0.25%  
TBF = 0 CU YD
- 13 63' - 15" SS, CL A, T-1 @ 1.00%  
TBF = 6.1 CY
- 14 63' - 15" SS, CL A, T-1 @ 1.00%  
TBF = 6.1 CY
- 15 55' - 4" PIPE UNDERDRAIN, FABRIC LINED TRENCH



**NOTES:**

- LOCATIONS OF FIELD TILES HAVE BEEN SHOWN BASED ON PLANS TITLED "REGIONAL STORMWATER DRAINAGE SYSTEM" DATED 03/01/99.
- THE EXISTING MANHOLES ON THE 48" SEWER WERE INSTALLED AS PART OF THE "REGIONAL STORMWATER DRAINAGE SYSTEM". ACCORDING TO THE PLANS FOR THE STORM SEWER INSTALLATION, THE MANHOLE LIDS WERE INSTALLED APPROXIMATELY 2' BELOW EXISTING GRADE. ALL WORK REQUIRED TO LOCATE AND EXPOSE THE EXISTING STRUCTURES SHALL BE INCLUDED IN THE COST OF "MANHOLES TO BE RECONSTRUCTED."

HALIGUS ROAD



ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ROADWAY IMPROVEMENT PLAN**  
 HALIGUS ROAD  
 STA. 313+00 TO STA. 318+50

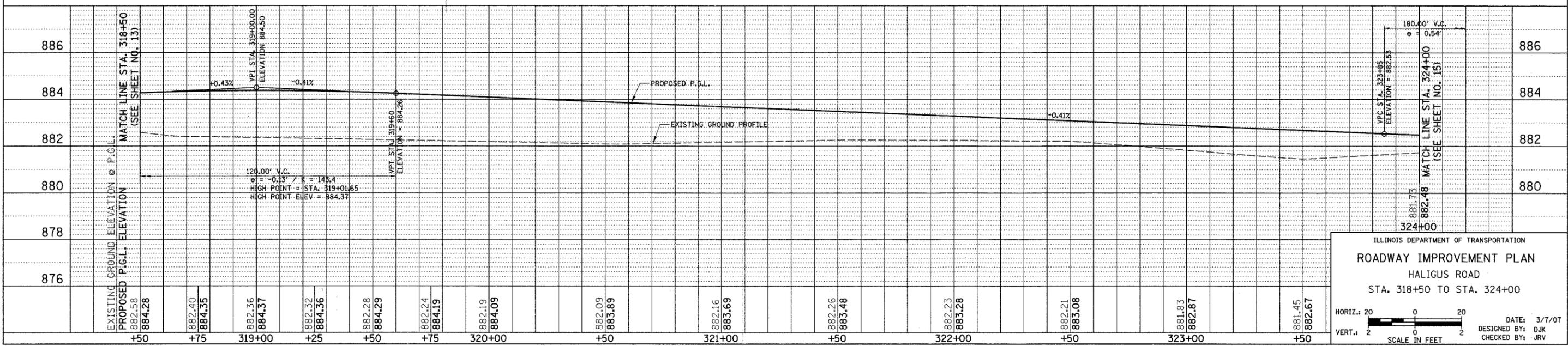
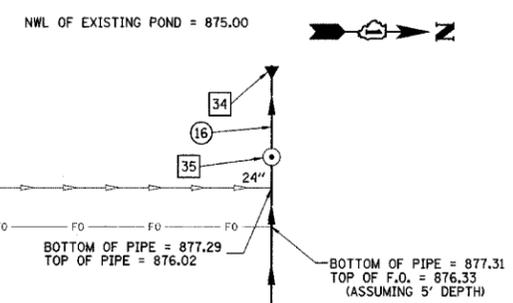
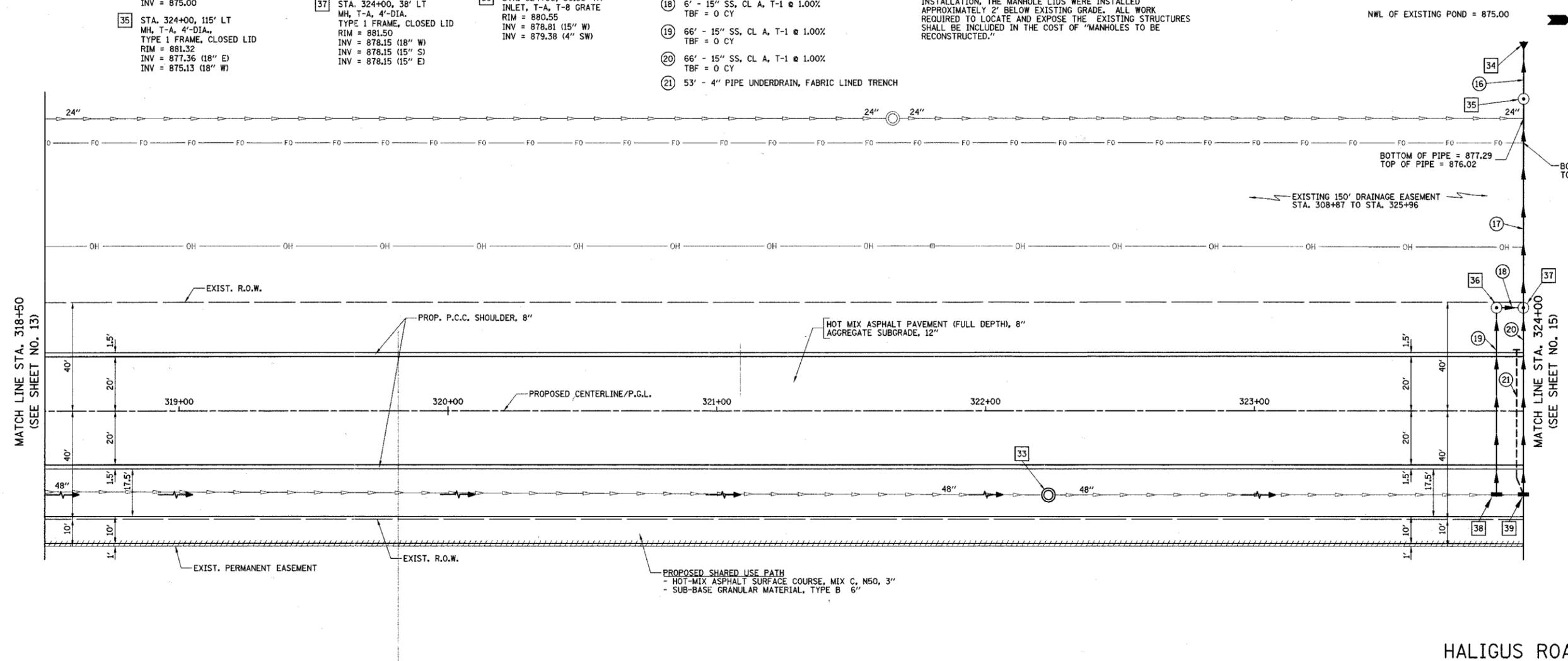
HORIZ. 20 0 20  
 VERT. 2 0 2  
 SCALE IN FEET

DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRJ

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	NO.
3869	06-00030-00-FP	MCHENRY	56	14
STA. 318+50		TO STA. 324+00		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT M-8003(64D)	

- 33 STA. 322+23.41, 31.11' RT MH TO BE RECONSTRUCTED PROP. RIM = 880.40
- 34 STA. 324+00, 134.33' LT PRC FLARED END SECTION, 18" INV = 875.00
- 35 STA. 324+00, 115' LT MH, T-A, 4'-DIA., TYPE 1 FRAME, CLOSED LID RIM = 881.32 INV = 877.36 (18" E) INV = 875.13 (18" W)
- 36 STA. 323+90, 38' LT MH, T-A, 4'-DIA., TYPE 1 FRAME, CLOSED LID RIM = 881.50 INV = 878.21 (15" E) INV = 878.21 (15" N)
- 37 STA. 324+00, 38' LT MH, T-A, 4'-DIA., TYPE 1 FRAME, CLOSED LID RIM = 881.50 INV = 878.15 (18" W) INV = 878.15 (15" S) INV = 878.15 (15" E)
- 38 STA. 33+90, 30.80' RT INLET, T-A, T-8 GRATE RIM = 880.60 INV = 878.87 (15" W)
- 39 STA. 324+00, 30.80' RT INLET, T-A, T-8 GRATE RIM = 880.55 INV = 878.81 (15" W) INV = 879.38 (4" SW)
- 16 13' - 18" SS, CL A, T-2 @ 1.10% TBF = 0 CY
- 17 72' - 18" SS, CL A, T-1 @ 1.10% TBF = 5.7 CY
- 18 6' - 15" SS, CL A, T-1 @ 1.00% TBF = 0 CY
- 19 66' - 15" SS, CL A, T-1 @ 1.00% TBF = 0 CY
- 20 66' - 15" SS, CL A, T-1 @ 1.00% TBF = 0 CY
- 21 53' - 4" PIPE UNDERDRAIN, FABRIC LINED TRENCH

**NOTES:**  
 1. THE EXISTING MANHOLES ON THE 48" SEWER WERE INSTALLED AS PART OF THE "REGIONAL STORMWATER DRAINAGE SYSTEM". ACCORDING TO THE PLANS FOR THE STORM SEWER INSTALLATION, THE MANHOLE LIDS WERE INSTALLED APPROXIMATELY 2" BELOW EXISTING GRADE. ALL WORK REQUIRED TO LOCATE AND EXPOSE THE EXISTING STRUCTURES SHALL BE INCLUDED IN THE COST OF "MANHOLES TO BE RECONSTRUCTED."



ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ROADWAY IMPROVEMENT PLAN**  
 HALIGUS ROAD  
 STA. 318+50 TO STA. 324+00

HORIZ.: 20  
 VERT.: 2

DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRV

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	15
STA. 324+00		TO STA. 328+40.40		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(64)				

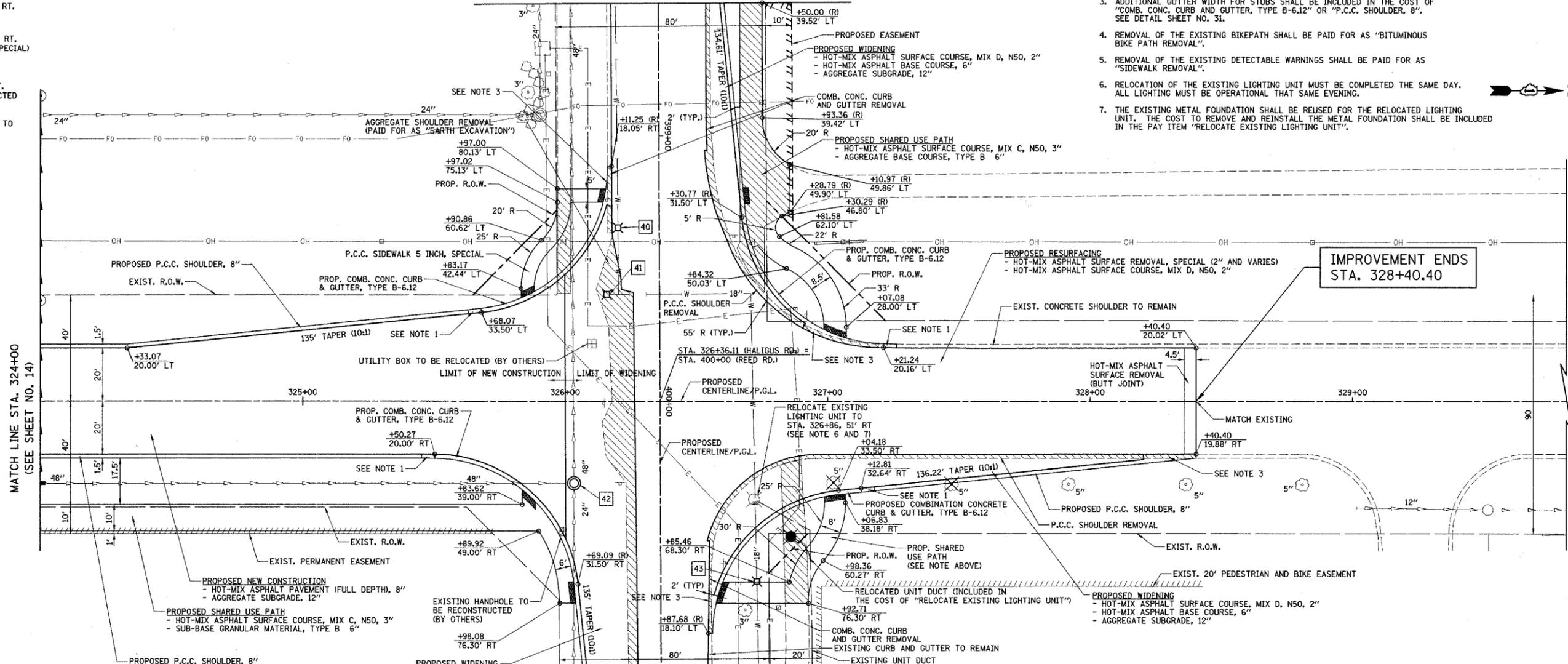
- 40 STA. 399+34.42, 15.75' RT. FRAME AND LID TO BE ADJUSTED (SPECIAL) EXIST. RIM = 884.89 PROP. RIM = 884.80
- 41 STA. 399+59.66, 20.10' RT. V.V. TO BE FILLED
- 42 STA. 400+30.66, 32.90' RT. MH TO BE ADJUSTED (SPECIAL) EXIST. RIM = 884.21 PROP. RIM = 884.56
- 43 STA. 400+68, 36.56' LT. V.V. TO BE RECONSTRUCTED EXIST. RIM = 884.77 PROP. RIM = 884.68 ROTATE CONE SECTION TO PLACE LID OUTSIDE OF PROPOSED BIKE PATH PAVEMENT

NOTE:

PRIOR TO PLACING HOT-MIX ASPHALT ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE HOT-MIX ASPHALT BEING PLACED.

REED ROAD

MATCH LINE STA. 398+50 (SEE SHEET NO. 16)



NOTES:

1. TRANSITION CURB HEIGHT FROM 6" TO 0" OVER 5'. PAID FOR AS "COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12".
2. STATION OFFSET CALL-OFFS NOTED WITH (R) ARE DEFINED BY THE REED ROAD ALIGNMENT.
3. ADDITIONAL GUTTER WIDTH FOR STUBS SHALL BE INCLUDED IN THE COST OF "COMB. CONC. CURB AND GUTTER, TYPE B-6.12" OR "P.C.C. SHOULDER, 8". SEE DETAIL SHEET NO. 31.
4. REMOVAL OF THE EXISTING BIKEPATH SHALL BE PAID FOR AS "BITUMINOUS BIKE PATH REMOVAL".
5. REMOVAL OF THE EXISTING DETECTABLE WARNINGS SHALL BE PAID FOR AS "SIDEWALK REMOVAL".
6. RELOCATION OF THE EXISTING LIGHTING UNIT MUST BE COMPLETED THE SAME DAY. ALL LIGHTING MUST BE OPERATIONAL THAT SAME EVENING.
7. THE EXISTING METAL FOUNDATION SHALL BE REUSED FOR THE RELOCATED LIGHTING UNIT. THE COST TO REMOVE AND REINSTALL THE METAL FOUNDATION SHALL BE INCLUDED IN THE PAY ITEM "RELOCATE EXISTING LIGHTING UNIT".

IMPROVEMENT ENDS STA. 328+40.40

REED ROAD

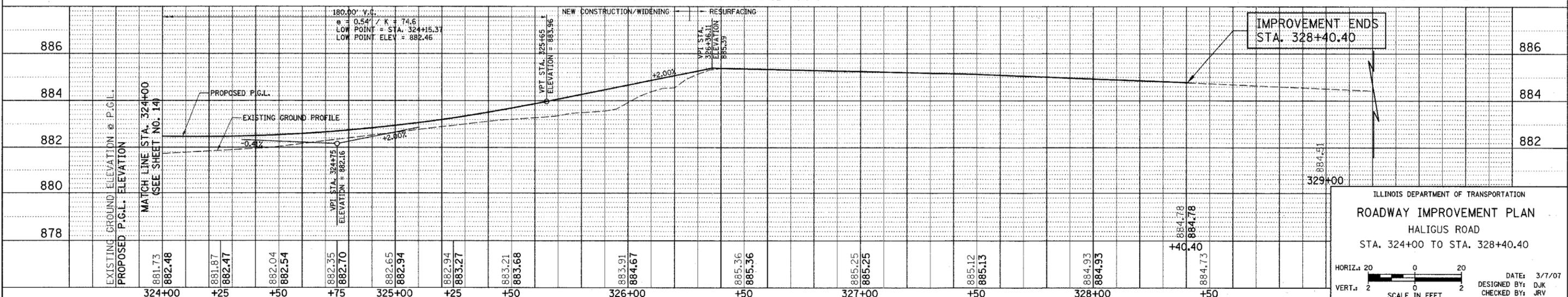
MATCH LINE STA. 401+00 (SEE SHEET NO. 17)

LEGEND



HALIGUS ROAD

IMPROVEMENT ENDS STA. 328+40.40

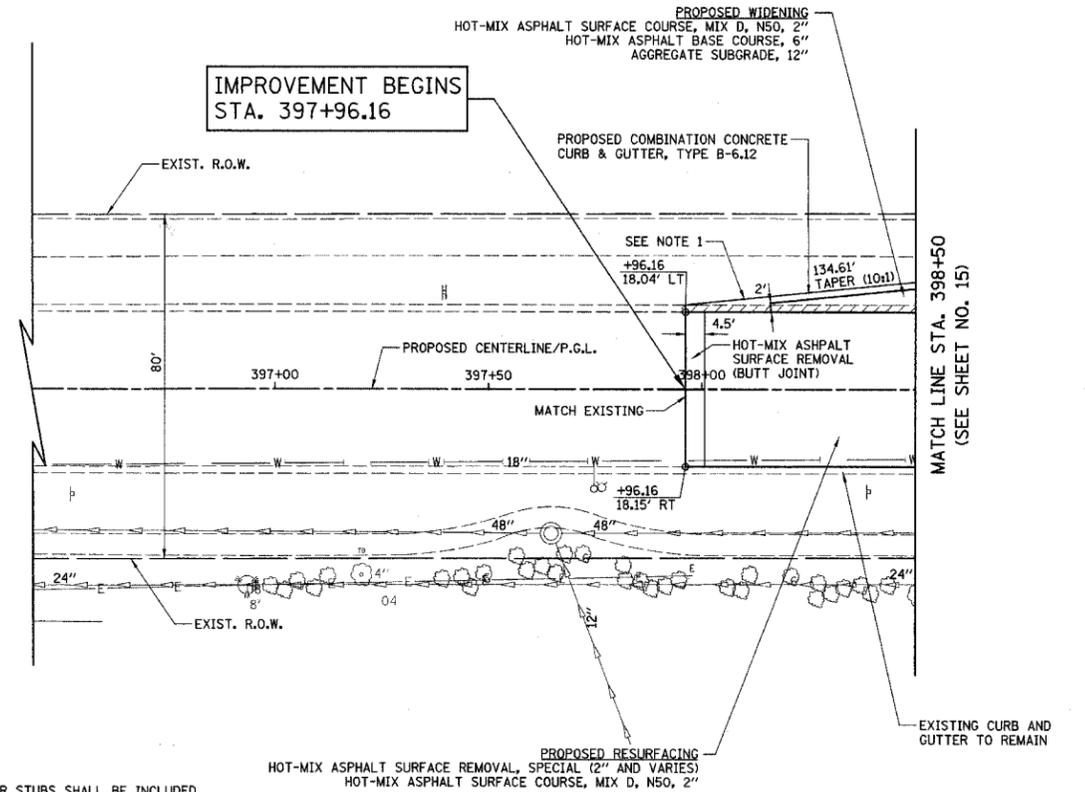
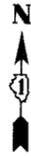


ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ROADWAY IMPROVEMENT PLAN**  
 HALIGUS ROAD  
 STA. 324+00 TO STA. 328+40.40

HORIZ. 20 0 20  
 VERT. 2 0 2  
 SCALE IN FEET

DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRV

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	16
STA. 397+96.16		TO STA. 398+50		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT W-8003641	



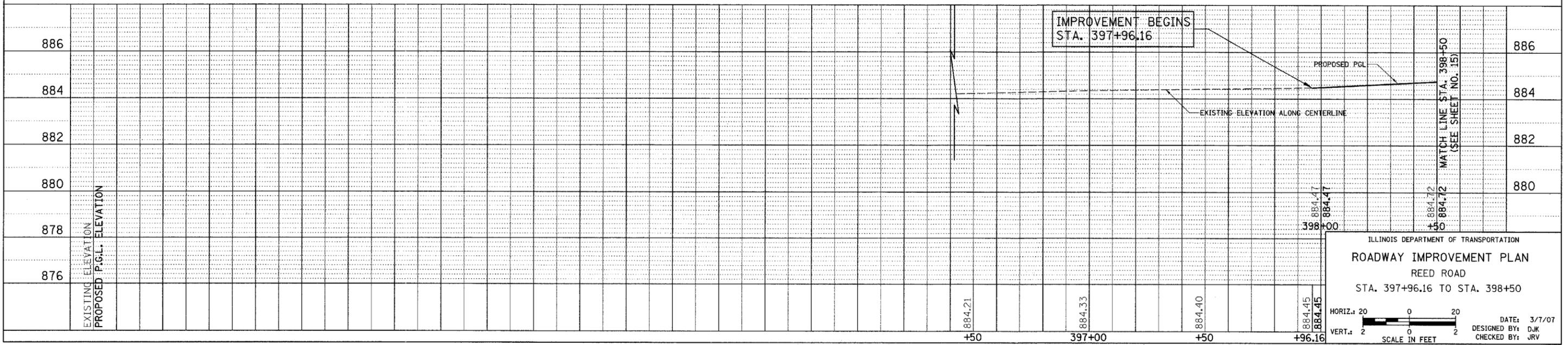
NOTE:

- 1) ADDITIONAL GUTTER WIDTH FOR STUBS SHALL BE INCLUDED IN THE COST OF "COMB. CONC. CURB AND GUTTER, TYPE B-6.12". SEE DETAIL SHEET NO. 32.
- 2) PRIOR TO PLACING HOT-MIX ASPHALT ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE HOT-MIX ASPHALT BEING PLACED.

LEGEND

REMOVAL

REED ROAD



ILLINOIS DEPARTMENT OF TRANSPORTATION

**ROADWAY IMPROVEMENT PLAN**

REED ROAD

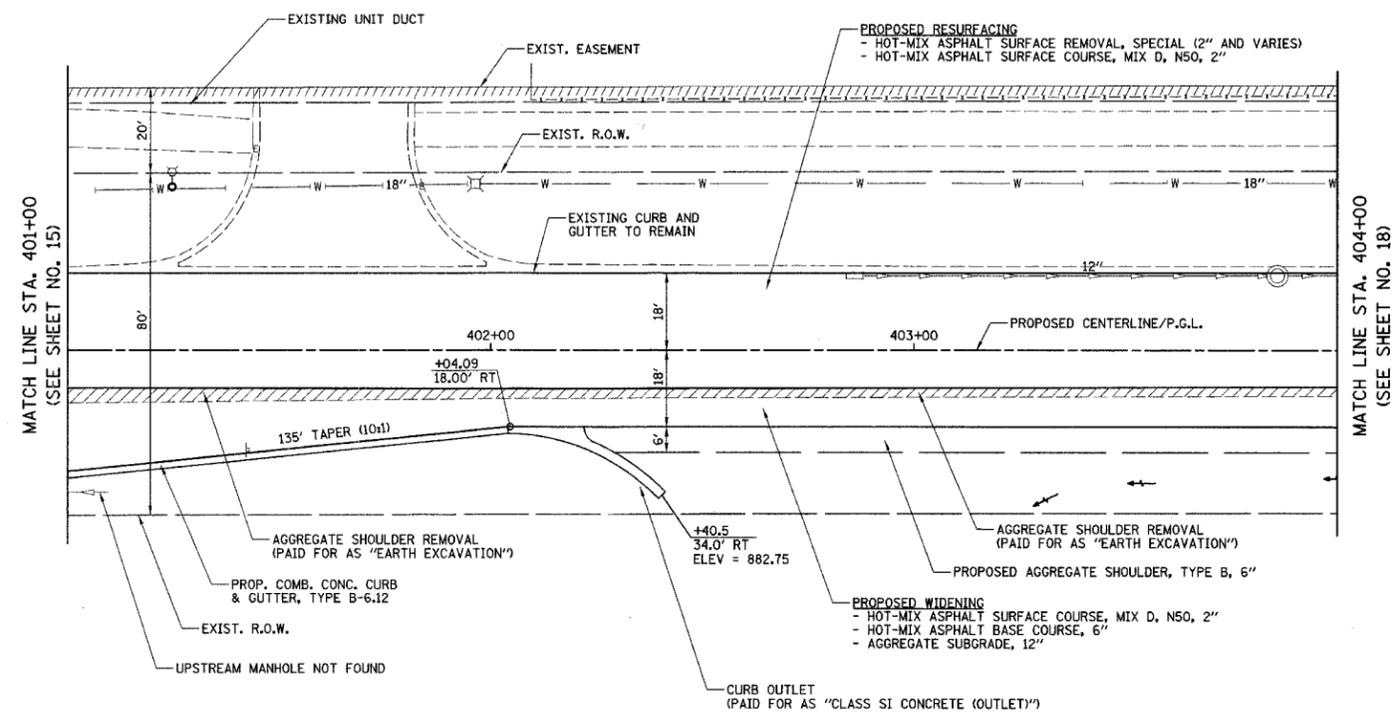
STA. 397+96.16 TO STA. 398+50

HORIZ.: 20  
VERT.: 2

SCALE IN FEET

DATE: 3/7/07  
DESIGNED BY: DJK  
CHECKED BY: JRV

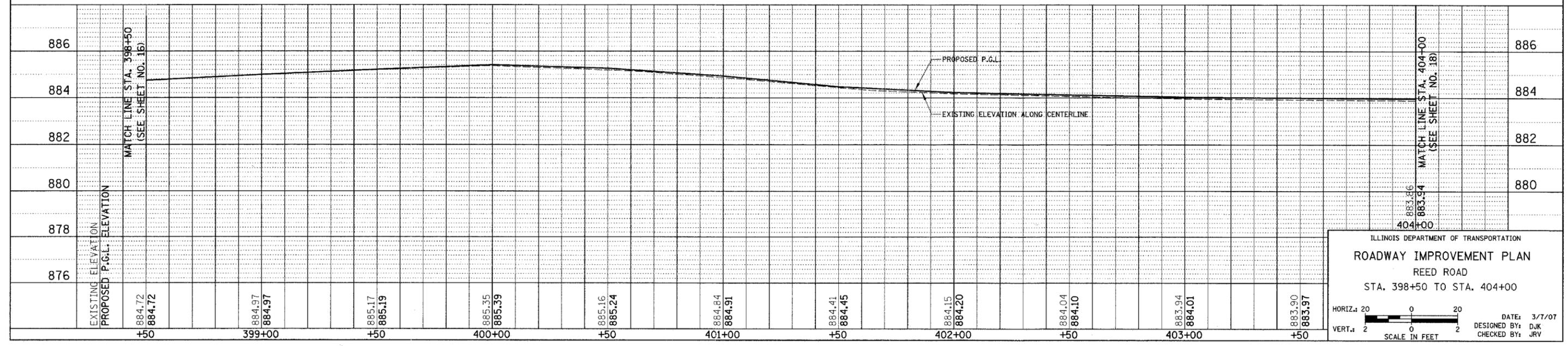
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	17
STA. 398+50		TO STA. 404+00		
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT W-8003641	



**LEGEND**  
 REMOVAL

**NOTE:**  
 PRIOR TO PLACING HOT-MIX ASPHALT ADJACENT TO EXISTING PAVEMENT TO REMAIN, THE EXPOSED EDGE SHALL BE CLEANED OF LOOSE MATERIAL TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE HOT-MIX ASPHALT BEING PLACED.

**REED ROAD**



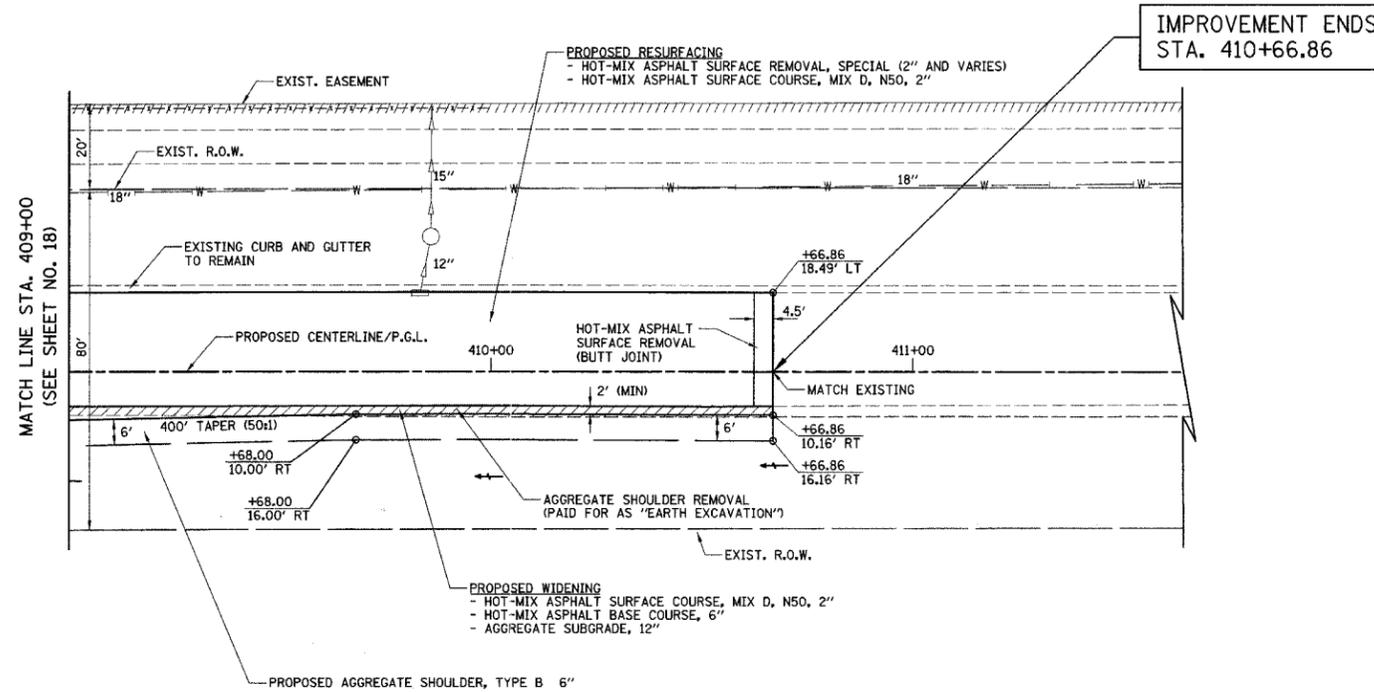
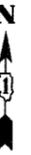
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ROADWAY IMPROVEMENT PLAN**  
 REED ROAD  
 STA. 398+50 TO STA. 404+00

HORIZ.: 20 0 20  
 VERT.: 2 0 2  
 SCALE IN FEET

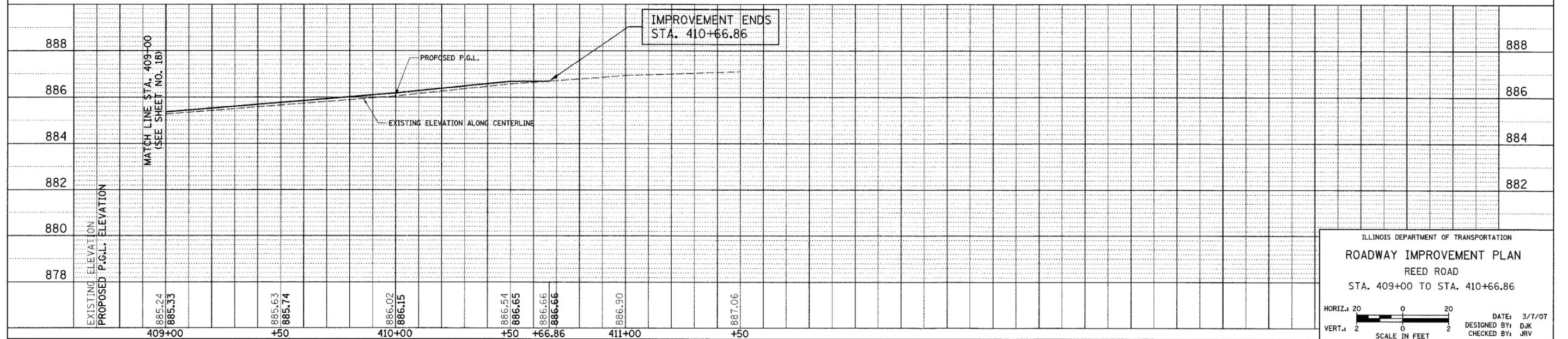
DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRV



F.A.M. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FF	MCHENRY	56	19
STA. 409+00		TO STA. 410+66.86		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(64D)				



REED ROAD



ILLINOIS DEPARTMENT OF TRANSPORTATION  
**ROADWAY IMPROVEMENT PLAN**  
 REED ROAD  
 STA. 409+00 TO STA. 410+66.86

HORIZ. 20 0 20  
 VERT. 2 0 2  
 SCALE IN FEET

DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRV



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	21
PAVEMENT MARKING AND SIGNING PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003641				

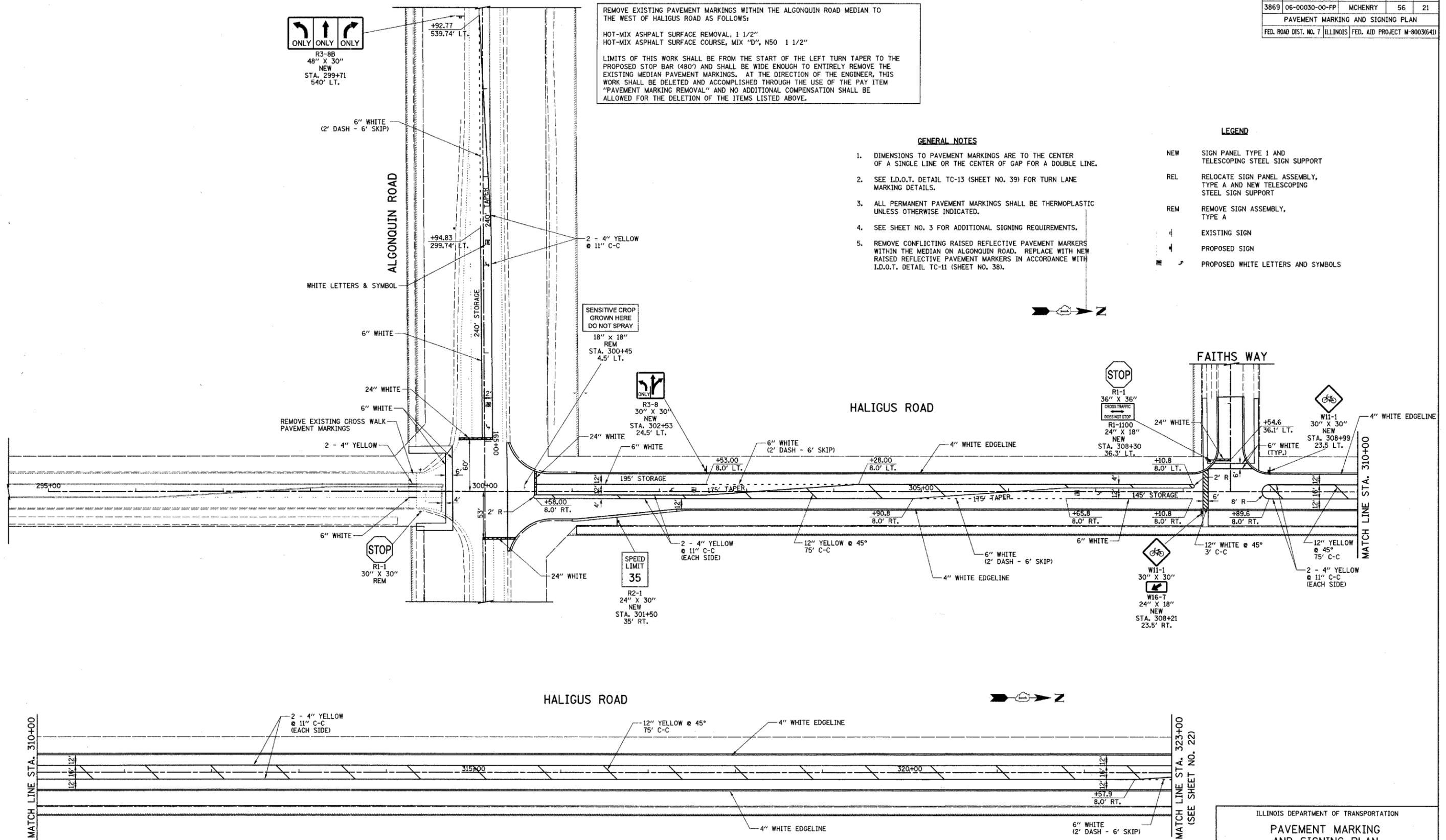


REMOVE EXISTING PAVEMENT MARKINGS WITHIN THE ALGONQUIN ROAD MEDIAN TO THE WEST OF HALIGUS ROAD AS FOLLOWS:  
 HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"  
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 1 1/2"  
 LIMITS OF THIS WORK SHALL BE FROM THE START OF THE LEFT TURN TAPER TO THE PROPOSED STOP BAR (480') AND SHALL BE WIDE ENOUGH TO ENTIRELY REMOVE THE EXISTING MEDIAN PAVEMENT MARKINGS. AT THE DIRECTION OF THE ENGINEER, THIS WORK SHALL BE DELETED AND ACCOMPLISHED THROUGH THE USE OF THE PAY ITEM "PAVEMENT MARKING REMOVAL" AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THE DELETION OF THE ITEMS LISTED ABOVE.

- GENERAL NOTES**
- DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.
  - SEE I.D.O.T. DETAIL TC-13 (SHEET NO. 39) FOR TURN LANE MARKING DETAILS.
  - ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE INDICATED.
  - SEE SHEET NO. 3 FOR ADDITIONAL SIGNING REQUIREMENTS.
  - REMOVE CONFLICTING RAISED REFLECTIVE PAVEMENT MARKERS WITHIN THE MEDIAN ON ALGONQUIN ROAD. REPLACE WITH NEW RAISED REFLECTIVE PAVEMENT MARKERS IN ACCORDANCE WITH I.D.O.T. DETAIL TC-11 (SHEET NO. 38).

**LEGEND**

NEW	SIGN PANEL TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
REL	RELOCATE SIGN PANEL ASSEMBLY, TYPE A AND NEW TELESCOPING STEEL SIGN SUPPORT
REM	REMOVE SIGN ASSEMBLY, TYPE A
EXISTING SIGN	EXISTING SIGN
PROPOSED SIGN	PROPOSED SIGN
PROPOSED WHITE LETTERS AND SYMBOLS	PROPOSED WHITE LETTERS AND SYMBOLS

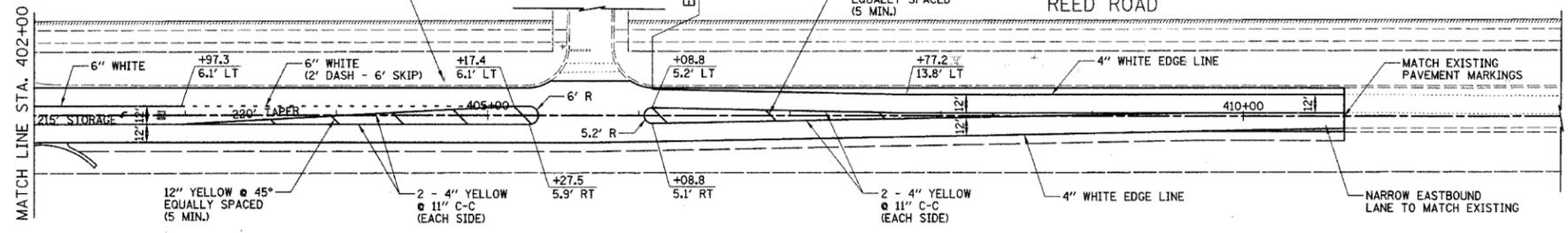
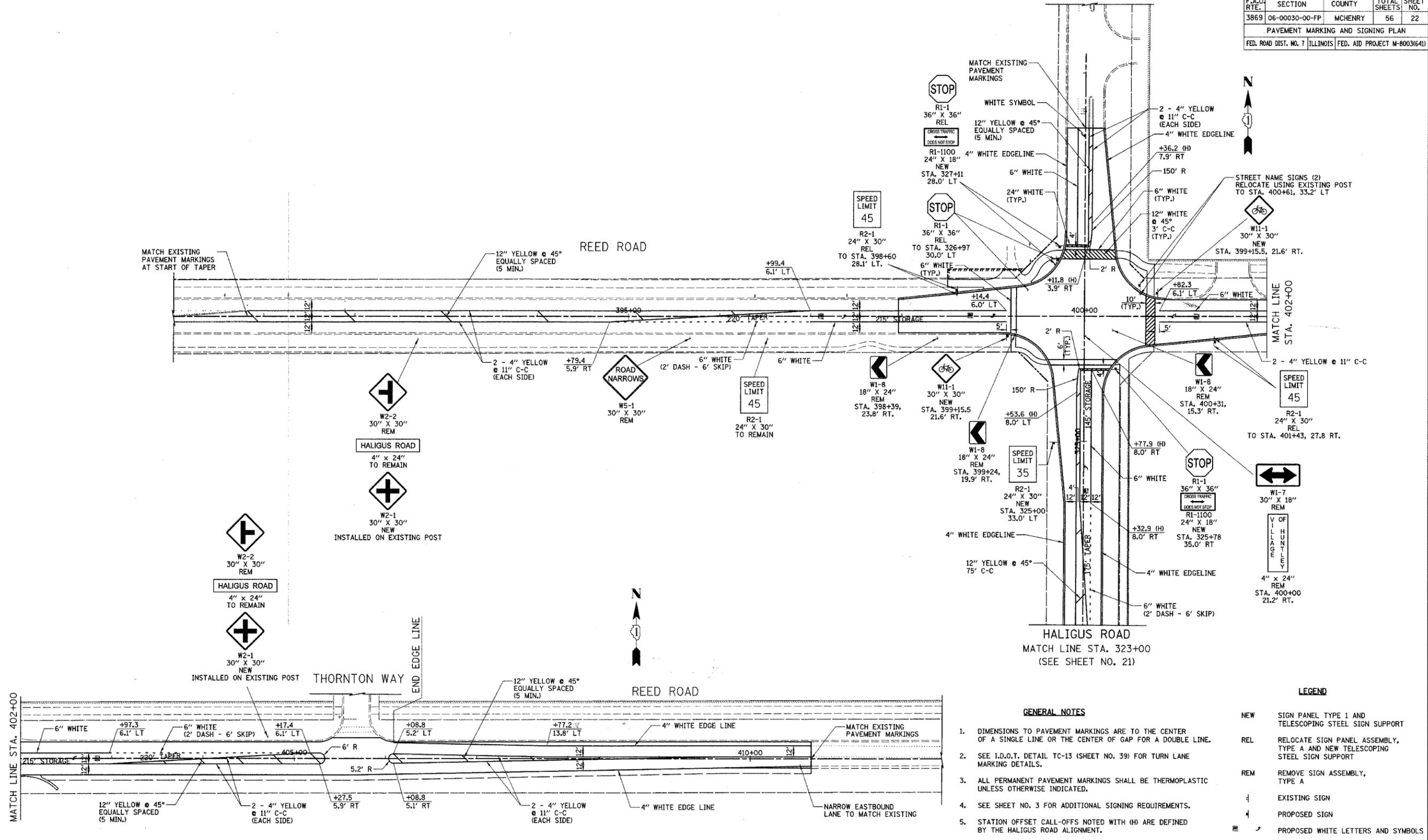


ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING AND SIGNING PLAN**  
 HALIGUS ROAD  
 STA. 300+25.91 TO STA. 323+00

50 0 50  
 SCALE IN FEET

DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRV

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	22
PAVEMENT MARKING AND SIGNING PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-800364U				



- GENERAL NOTES**
- DIMENSIONS TO PAVEMENT MARKINGS ARE TO THE CENTER OF A SINGLE LINE OR THE CENTER OF GAP FOR A DOUBLE LINE.
  - SEE I.D.O.T. DETAIL TC-13 (SHEET NO. 39) FOR TURN LANE MARKING DETAILS.
  - ALL PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC UNLESS OTHERWISE INDICATED.
  - SEE SHEET NO. 3 FOR ADDITIONAL SIGNING REQUIREMENTS.
  - STATION OFFSET CALL-OFFS NOTED WITH (H) ARE DEFINED BY THE HALIGUS ROAD ALIGNMENT.

**LEGEND**

NEW	SIGN PANEL TYPE 1 AND TELESCOPING STEEL SIGN SUPPORT
REL	RELOCATE SIGN PANEL ASSEMBLY, TYPE A AND NEW TELESCOPING STEEL SIGN SUPPORT
REM	REMOVE SIGN ASSEMBLY, TYPE A
+	EXISTING SIGN
+	PROPOSED SIGN
+	PROPOSED WHITE LETTERS AND SYMBOLS

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PAVEMENT MARKING AND SIGNING PLAN**

REED ROAD

STA. 390+82.0 TO STA. 410+70.9

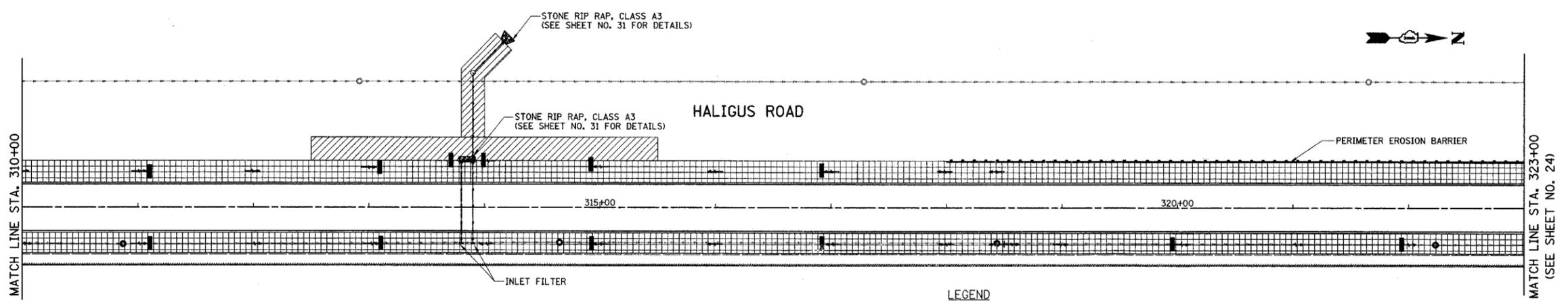
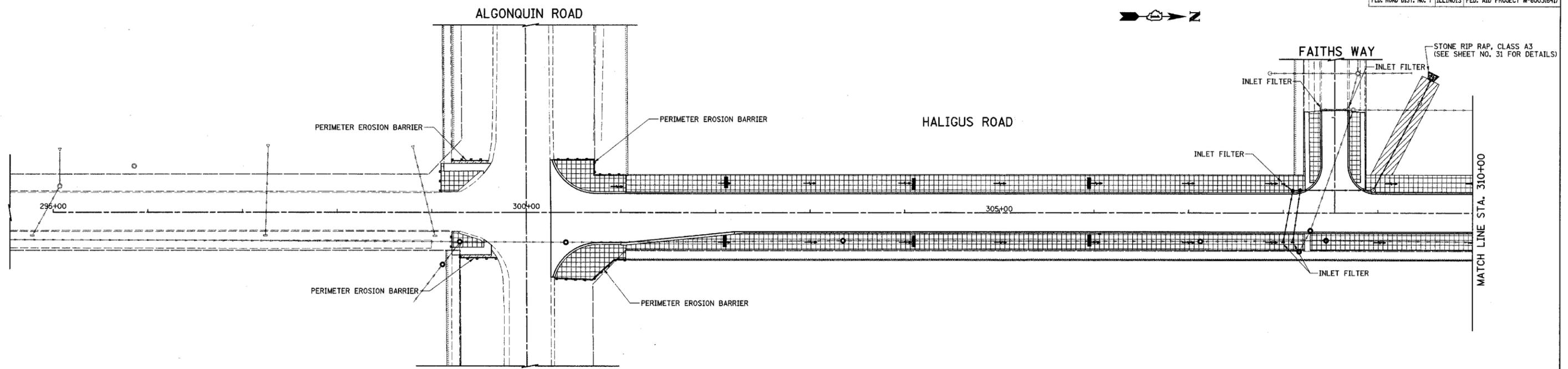
SCALE IN FEET

DATE: 3/7/07

DESIGNED BY: DJK

CHECKED BY: JRV

CONTRACT NO. 83919			
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS
3869	06-00030-00-FP	MCHENRY	56
LANDSCAPING & EROSION CONTROL PLAN			
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(64)			



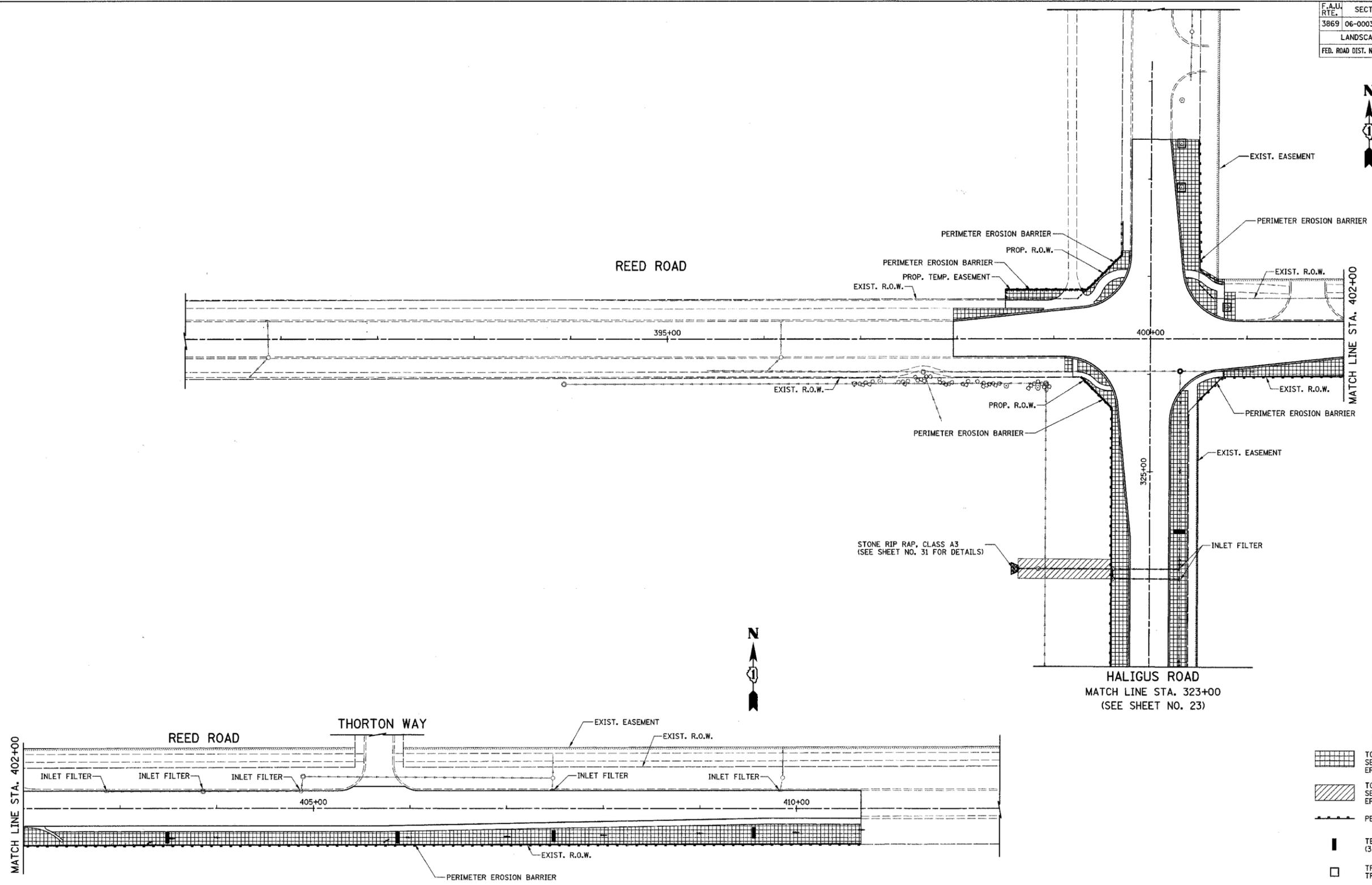
- LEGEND**
-  TOPSOIL FURNISH AND PLACE, 4" SEEDING, CLASS 3 EROSION CONTROL BLANKET
  -  TOPSOIL FURNISH AND PLACE, 4" SEEDING, CLASS 3 EROSION CONTROL BLANKET
  -  PERIMETER EROSION BARRIER
  -  TEMPORARY DITCH CHECKS (3 EACH PER LOCATION)

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**LANDSCAPING & EROSION CONTROL PLAN**  
**HALIGUS ROAD**  
 STA. 300+25.91 TO STA. 323+00

50 0 50  
 SCALE IN FEET

DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRV

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	24
LANDSCAPING & EROSION CONTROL PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(641)				



LEGEND

-  TOPSOIL FURNISH AND PLACE, 4" SEEDING, CLASS 1A EROSION CONTROL BLANKET
-  TOPSOIL FURNISH AND PLACE, 4" SEEDING, CLASS 3 EROSION CONTROL BLANKET
-  PERIMETER EROSION BARRIER
-  TEMPORARY DITCH CHECKS (3 EACH PER LOCATION)
-  TREE TRUNK PROTECTION TREE ROOT PRUNING

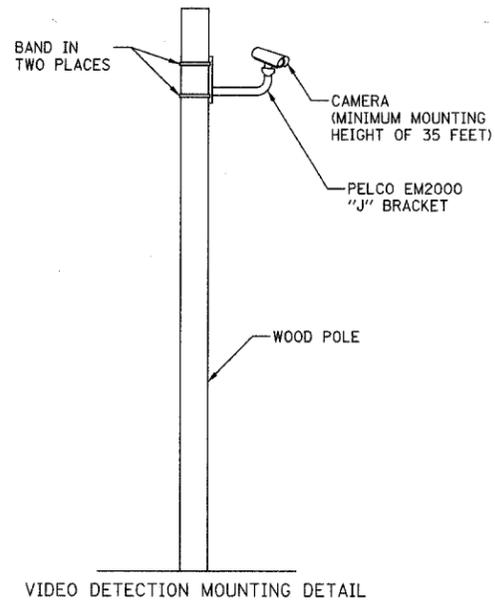
ILLINOIS DEPARTMENT OF TRANSPORTATION  
 LANDSCAPING &  
 EROSION CONTROL PLAN  
 REED ROAD  
 STA. 397+96.16 TO STA. 410+66.86

50 0 50  
 SCALE IN FEET  
 DATE: 3/7/07  
 DESIGNED BY: DJK  
 CHECKED BY: JRV

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	25
TEMPORARY TRAFFIC SIGNAL PLAN				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-800364D				

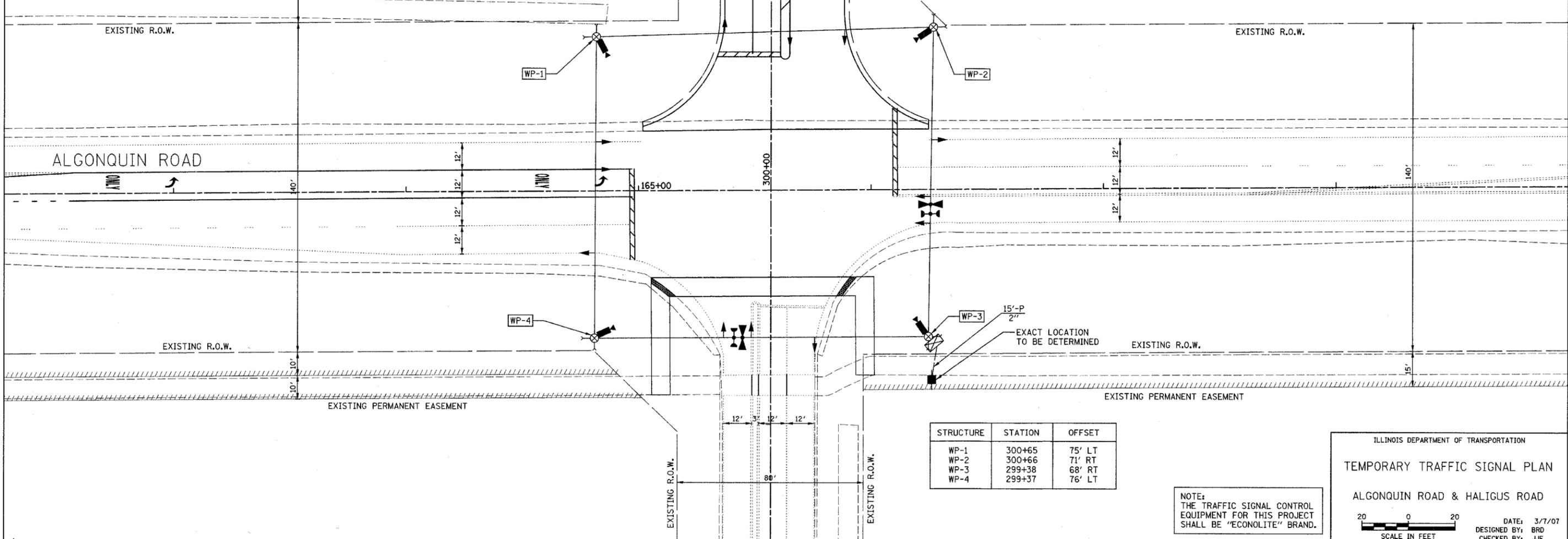
**TEMPORARY TRAFFIC SIGNAL LEGEND**

- TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- CONTROLLER AND CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- TEMPORARY SERVICE INSTALLATION
- TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- TEMPORARY PEDESTRIAN PUSHBUTTON DETECTOR
- VIDEO VEHICLE DETECTOR
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- COMMON TRENCH
- UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- HANDHOLE
- HEAVY-DUTY HANDHOLE



**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

1. THE TEMPORARY TRAFFIC SIGNAL SHALL NOT BE TURNED ON UNTIL THE PROPOSED NORTH APPROACH OF THE INTERSECTION IS OPEN TO TRAFFIC. ALL SIGNAL HEADS SHALL BE BAGGED UNTIL THE TEMPORARY TRAFFIC SIGNAL IS TURNED ON.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY MCDOT OR IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL PROPOSED STREET NAME SIGNS SHALL BE SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. ALL STATIONS AND OFFSETS ARE DIMENSIONED FROM THE CENTERLINE OF HALIGUS ROAD.
7. THE TYPE IV TRAFFIC SIGNAL CABINET SHALL HAVE A CLOSED BOTTOM MADE OF ALUMINUM ALLOY. THE BOTTOM SHALL BE SEALED ALONG THE ENTIRE PERIMETER OF THE CABINET BASE TO ENSURE A WATER, DUST AND INSECT-PROOF SEAL. THE BOTTOM SHALL PROVIDE A MINIMUM OF TWO (2) 4 INCH DIAMETER HOLES TO RUN THE ELECTRIC CABLES THROUGH. THE 4 INCH DIAMETER HOLES SHALL HAVE A BUSHING INSTALLED TO PROTECT THE ELECTRIC CABLES AND SHALL BE SEALED AFTER THE ELECTRIC CABLES ARE INSTALLED. THIS SHALL BE INCLUDED IN THE UNIT PRICE EACH FOR "FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL."
8. ALL SIGNAL HEADS SHALL BE 12" L.E.D., EXPANDED VIEW TYPE.



STRUCTURE	STATION	OFFSET
WP-1	300+65	75' LT
WP-2	300+66	71' RT
WP-3	299+38	68' RT
WP-4	299+37	76' LT

NOTE:  
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" BRAND.

ILLINOIS DEPARTMENT OF TRANSPORTATION

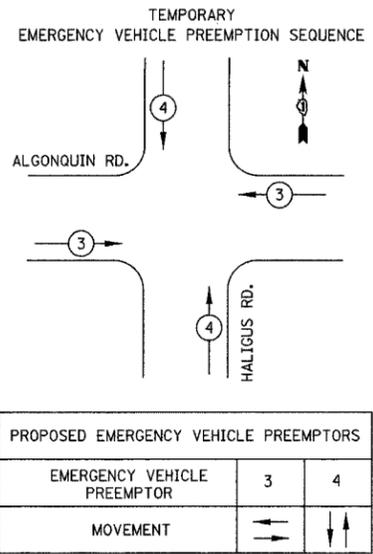
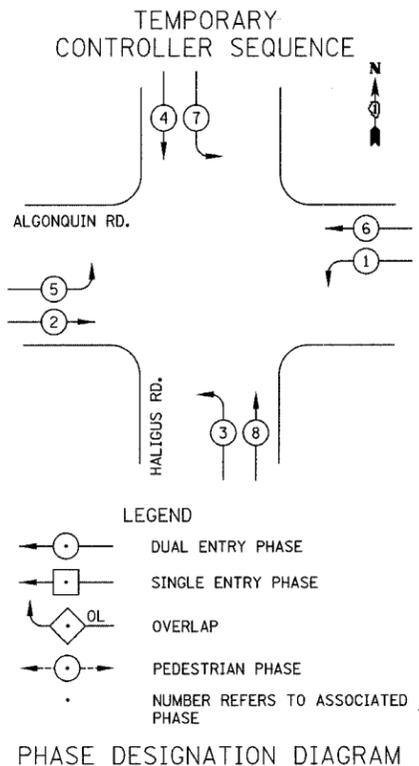
**TEMPORARY TRAFFIC SIGNAL PLAN**

ALGONQUIN ROAD & HALIGUS ROAD

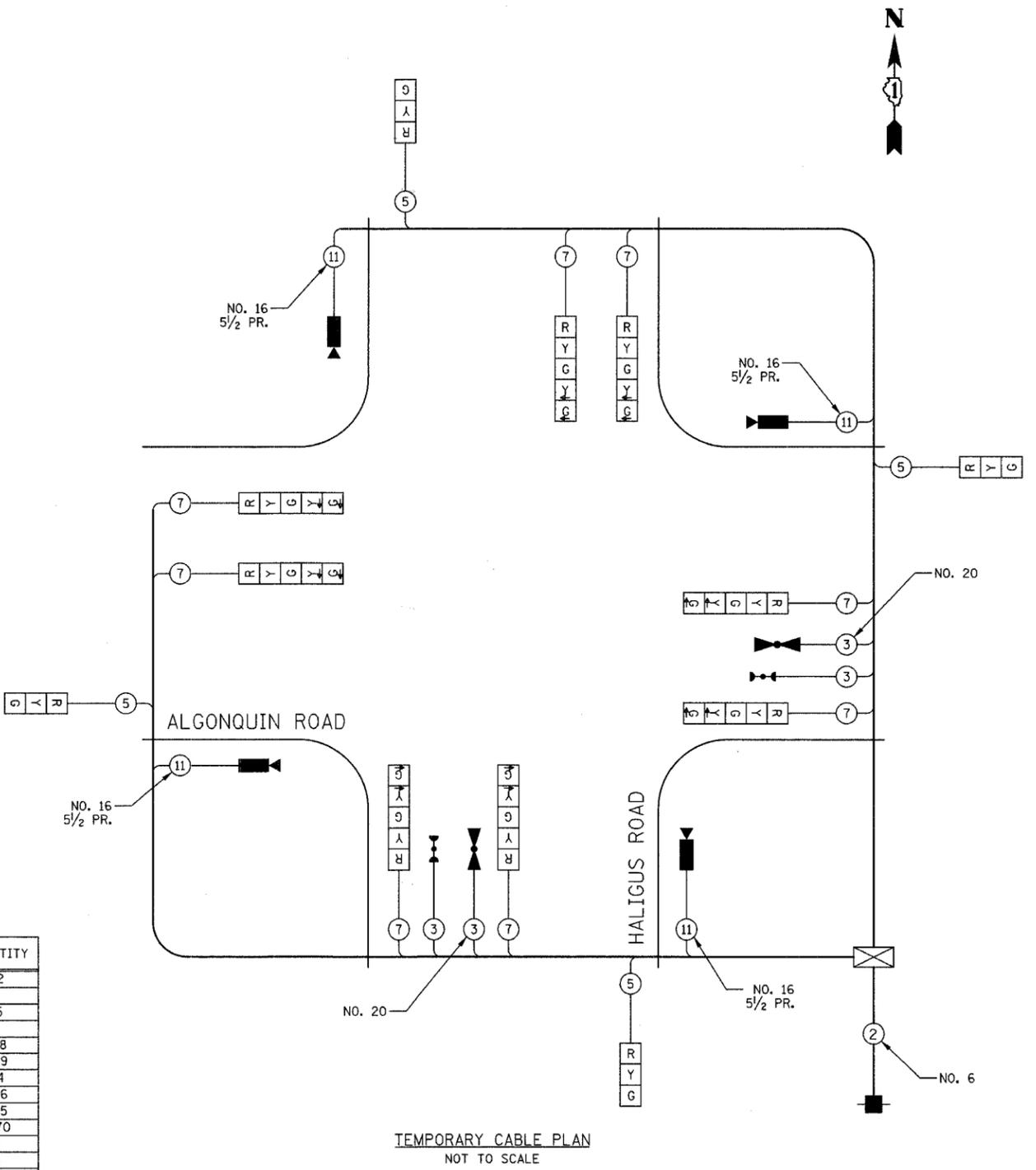
SCALE IN FEET

DATE: 3/7/07  
DESIGNED BY: BRD  
CHECKED BY: JJE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	26
TEMPORARY CABLE PLAN, SEQUENCES, & QUANTITIES				
FED. ROAD DIST. NO. 7   ILLINOIS   FED. AID PROJECT M-8003(64D)				



- ### TEMPORARY CABLE DIAGRAM LEGEND
- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300 mm)
  - CONTROLLER CABINET
  - SERVICE INSTALLATION
  - INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
  - EMERGENCY VEHICLE LIGHT DETECTOR
  - CONFIRMATION BEACON
  - PEDESTRIAN PUSHBUTTON DETECTOR
  - VEHICLE DETECTOR, INDUCTION LOOP
  - 12" (300mm) PEDESTRIAN SIGNAL SECTION
  - VIDEO VEHICLE DETECTOR



### SCHEDULE OF QUANTITIES

PAY ITEM	UNIT	QUANTITY
SIGN PANEL - TYPE 1 (SPECIAL)	50 FT	32
* TRAFFIC SIGNAL WOOD POLE, 45 FT, CLASS 5	EACH	4
CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	15
* FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
SPAN WIRE	FOOT	618
TETHER WIRE	FOOT	589
ELECTRIC CABLE IN CONDUIT, SERVICE NO. 6 2C	FOOT	34
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 3C	FOOT	346
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 5C	FOOT	985
ELECTRIC CABLE AERIAL SUSPENDED, SIGNAL, NO. 14 7C	FOOT	1970
SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, SPAN WIRE MOUNTED	EACH	4
SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, SPAN WIRE MOUNTED	EACH	8
* LIGHT DETECTOR	EACH	2
* LIGHT DETECTOR AMPLIFIER	EACH	1
SERVICE INSTALLATION, GROUND MOUNTED	EACH	1
* ELECTRIC CABLE AERIAL SUSPENDED NO. 20 3/C, TWISTED, SHIELDED	FOOT	346
* VIDEO DETECTION SYSTEM (COMPLETE INTERSECTION)	EACH	1
ELECTRIC CABLE AERIAL SUSPENDED, COMMUNICATION, NO. 16 5/2 PAIR	FOOT	763
* 100% COST TO VILLAGE OF HUNTLEY		

### I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS

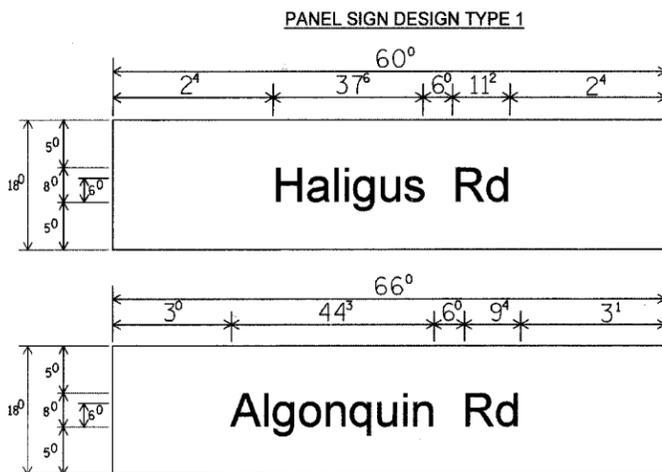
TYPE	NO. LAMPS	WATTAGE		% OPERATION	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	12		17	0.50	102
(YELLOW)	12		25	0.25	75
(GREEN)	12		15	0.25	45
ARROW	16		12	0.10	19
CONTROLLER	1		100	1.00	100
VIDEO SYSTEMS	1		150	1.00	150
FLASHER				0.50	
TOTAL =					491

ENERGY COSTS TO: VILLAGE OF HUNTLEY  
 10987 MAIN STREET  
 HUNTLEY, IL 60142  
 ENERGY SUPPLY: CONTACT: DAVE HUBERTY  
 PHONE: (847) 608-2342  
 COMPANY: COM ED

NOTE:  
 THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" BRAND.

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TEMPORARY CABLE PLAN, SEQUENCES, AND SCHEDULE OF QUANTITIES**  
 ALGONQUIN ROAD & HALIGUS ROAD  
 DATE: 3/7/07  
 DESIGNED BY: BRD  
 CHECKED BY: JJE  
 NOT TO SCALE

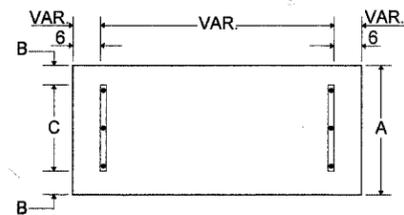
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	27
TEMPORARY CABLE PLAN, SEQUENCES, & QUANTITIES				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003641				



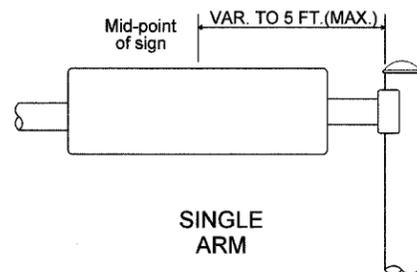
Sq. M Each  
7.50 Sq. Ft. Each  
2 Required  
Design Series\_D

Sq. M Each  
8.25 Sq. Ft. Each  
2 Required  
Design Series\_C

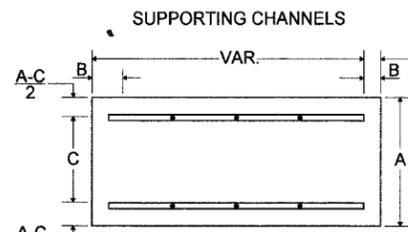
SUPPORTING CHANNELS



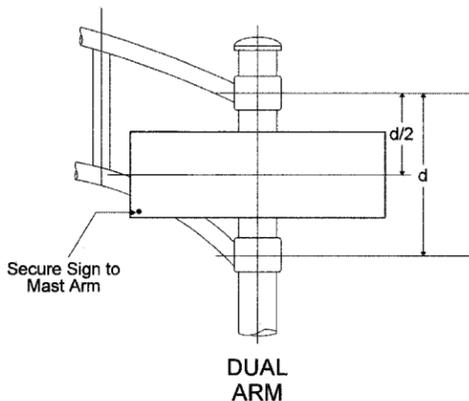
A	B	C
18"	2"	14"



SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.



A	B	C
18"	2"	12"
30"	2"	22"



EXAMPLE, 2<sup>3</sup> DENOTES 3/8"

UPPER TO LOWER CASE SPACING CHART 8-6 INCH SERIES "C & D"

SERIES	SECOND LETTER															
	a c d e		b h i k				f w		j		s t		v y		x z	
	g o q	l m n p r u														
A W X	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
B	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>
C E G	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
D O Q R	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
F	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>
H I M N	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>2</sup>	2 <sup>4</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>
J U	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>
K L	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
P	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
S	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>						
T	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
V	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>						
Y	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>4</sup>	1 <sup>5</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>7</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>
Z	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>0</sup>	2 <sup>1</sup>

LOWER CASE TO LOWER CASE SPACING CHART 6 INCH SERIES "C" & "D"

SERIES	SECOND LETTER															
	a c d e		b h i k				f w		j		s t		v y		x z	
	g o q	l m n p r u														
F	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
I	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
R	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
S	1 <sup>6</sup>	1 <sup>7</sup>	2 <sup>2</sup>	2 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
T	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
L	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>						
E	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>3</sup>	0 <sup>3</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>
T	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>
T	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	0 <sup>6</sup>	1 <sup>0</sup>	1 <sup>1</sup>	1 <sup>2</sup>
E	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>
R	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	0 <sup>5</sup>	0 <sup>6</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>2</sup>	1 <sup>4</sup>

NUMBER TO NUMBER SPACING CHART 8 INCH SERIES "C" & "D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
F	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>6</sup>	1 <sup>7</sup>
I	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>	2 <sup>1</sup>	2 <sup>0</sup>	2 <sup>1</sup>
R	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>
S	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>								
T	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
L	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>
E	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
N	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>
U	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>
M	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>
B	1 <sup>6</sup>	1 <sup>7</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>1</sup>	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	1 <sup>4</sup>	1 <sup>5</sup>

UPPER AND LOWER CASE LETTER WIDTHS

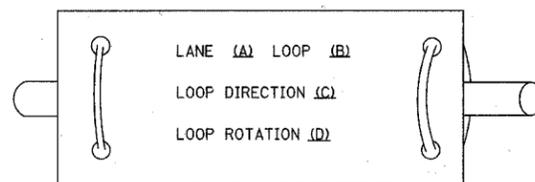
LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>5</sup>	a	3 <sup>5</sup>	4 <sup>2</sup>
B	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	b	3 <sup>5</sup>	4 <sup>2</sup>
C	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	c	3 <sup>5</sup>	4 <sup>1</sup>
D	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	d	3 <sup>5</sup>	4 <sup>2</sup>
E	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	e	3 <sup>5</sup>	4 <sup>2</sup>
F	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	f	2 <sup>3</sup>	2 <sup>6</sup>
G	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	g	3 <sup>5</sup>	4 <sup>2</sup>
H	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	h	3 <sup>5</sup>	4 <sup>2</sup>
I	0 <sup>7</sup>	0 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	i	1 <sup>1</sup>	1 <sup>1</sup>
J	3 <sup>0</sup>	3 <sup>6</sup>	4 <sup>0</sup>	5 <sup>0</sup>	j	2 <sup>0</sup>	2 <sup>2</sup>
K	3 <sup>2</sup>	4 <sup>1</sup>	4 <sup>3</sup>	5 <sup>4</sup>	k	3 <sup>5</sup>	4 <sup>2</sup>
L	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	l	1 <sup>1</sup>	1 <sup>1</sup>
M	3 <sup>7</sup>	4 <sup>5</sup>	5 <sup>1</sup>	6 <sup>1</sup>	m	6 <sup>0</sup>	7 <sup>0</sup>
N	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	n	3 <sup>5</sup>	4 <sup>2</sup>
O	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	o	3 <sup>6</sup>	4 <sup>3</sup>
P	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	p	3 <sup>5</sup>	4 <sup>2</sup>
Q	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	q	3 <sup>5</sup>	4 <sup>2</sup>
R	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	r	2 <sup>6</sup>	3 <sup>2</sup>
S	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	s	3 <sup>6</sup>	4 <sup>2</sup>
T	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	t	2 <sup>7</sup>	3 <sup>2</sup>
U	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	u	3 <sup>5</sup>	4 <sup>2</sup>
V	3 <sup>5</sup>	4 <sup>4</sup>	4 <sup>7&lt;/</sup>				

F.A.I.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	28
DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS				
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT M-8003(641)		

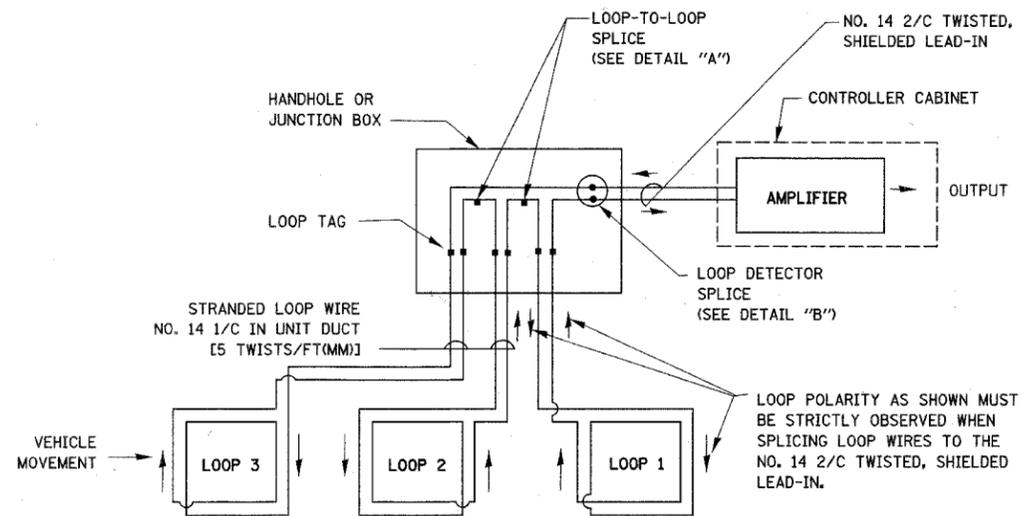
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

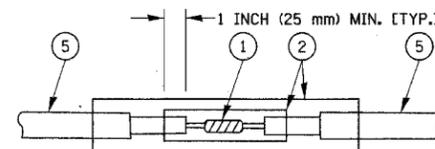


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

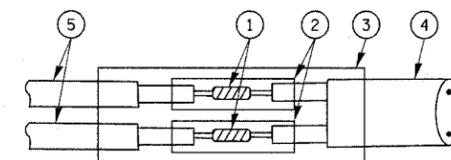


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



**DETAIL "A"  
LOOP-TO-LOOP SPLICE**



**DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE

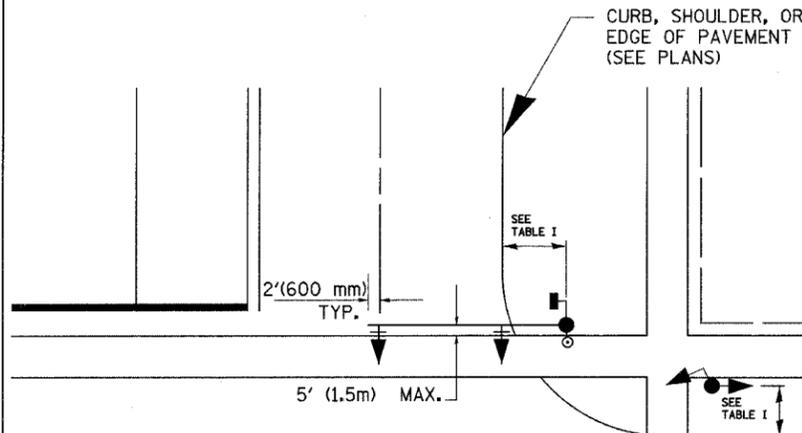
ILLINOIS DEPARTMENT OF TRANSPORTATION  
DISTRICT ONE  
STANDARD TRAFFIC SIGNAL  
DESIGN DETAILS

SCALE: VERT. NONE  
HORIZ. DATE 1-01-02  
DRAWN BY: RWP  
DESIGNED BY: DAD  
CHECKED BY: DAZ  
SHEET 1 OF 4

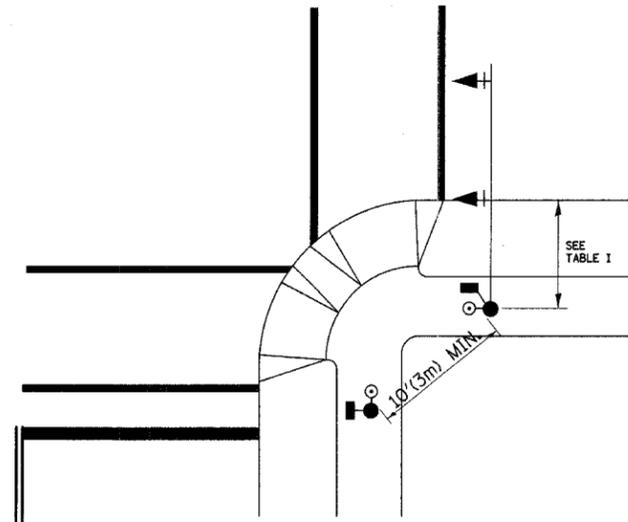
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	29
DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(641)				

**TRAFFIC SIGNAL MAST ARM AND POST**

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



**PEDESTRIAN SIGNAL PUSHBUTTON**



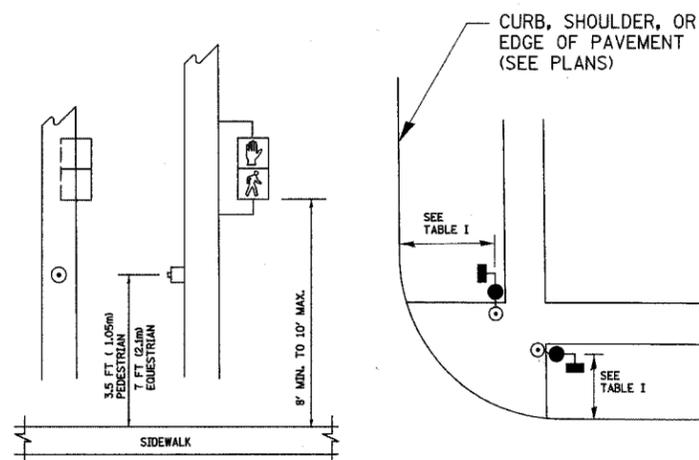
RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

**NOTES:**

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.  
  
AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.  
  
PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:  
 A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.  
 B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.  
 C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.  
 D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).  
 E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

**PEDESTRIAN SIGNAL POST**

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION



**TABLE I**

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

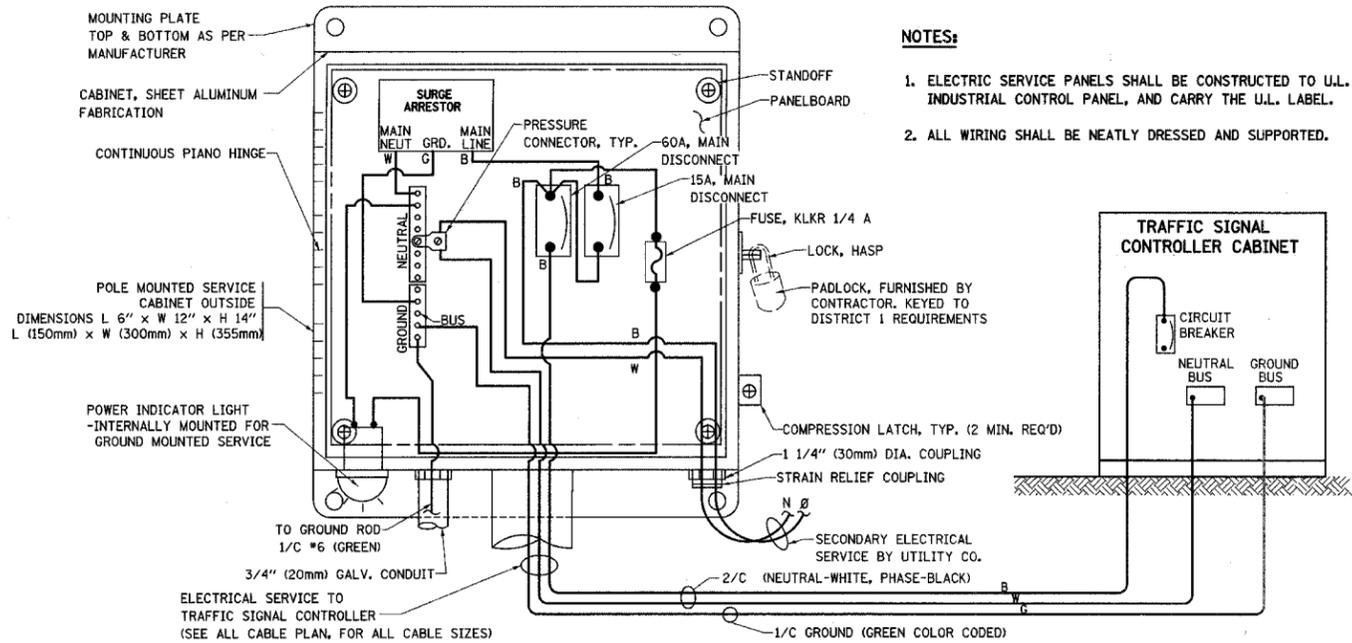
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
  
DISTRICT 1  
STANDARD TRAFFIC SIGNAL  
DESIGN DETAILS

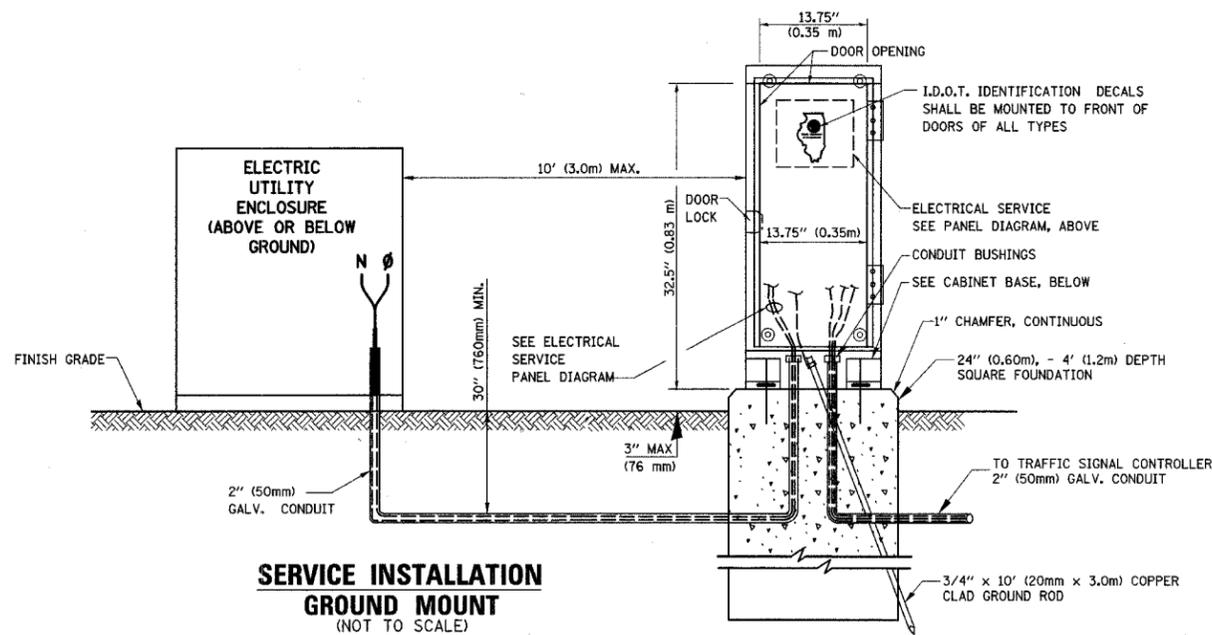
SCALE: VERT. NONE  
HORIZ. NONE  
DATE 1-01-02

DRAWN BY: RWP  
DESIGNED BY: DAD  
CHECKED BY: DAZ  
SHEET 2 OF 4

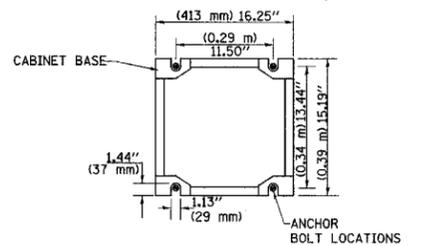
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	30
DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003641				



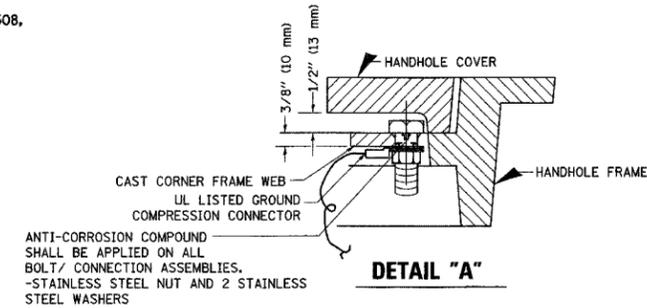
**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
(NOT TO SCALE)



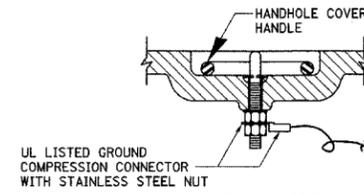
**SERVICE INSTALLATION GROUND MOUNT**  
(NOT TO SCALE)



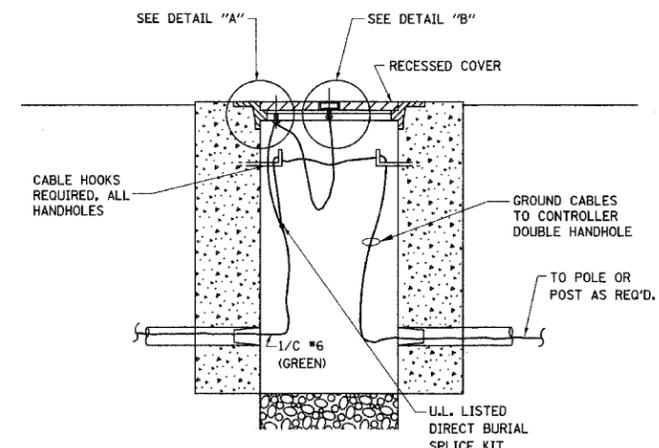
**CABINET - BASE BOLT PATTERN**  
(NOT TO SCALE)



**DETAIL "A"**

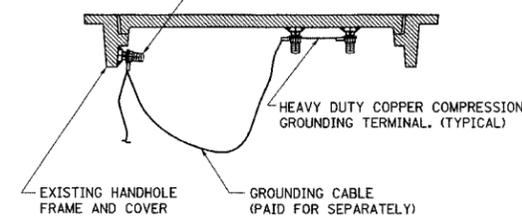


**DETAIL "B"**



**HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
(NOT TO SCALE)

(2) 1/2" x 1 1/4" STAINLESS STEEL BOLT WITH SPLIT LOCK WASHER AND NYLON INSERT LOCKOUT WELDED TO FRAME AND TO COVER. (TYPICAL)

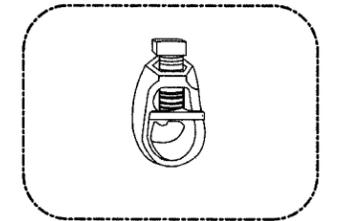
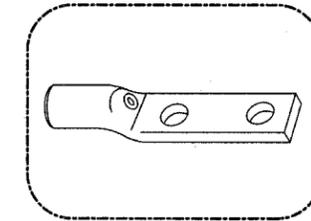


**EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
(NOT TO SCALE)

**NOTES:**

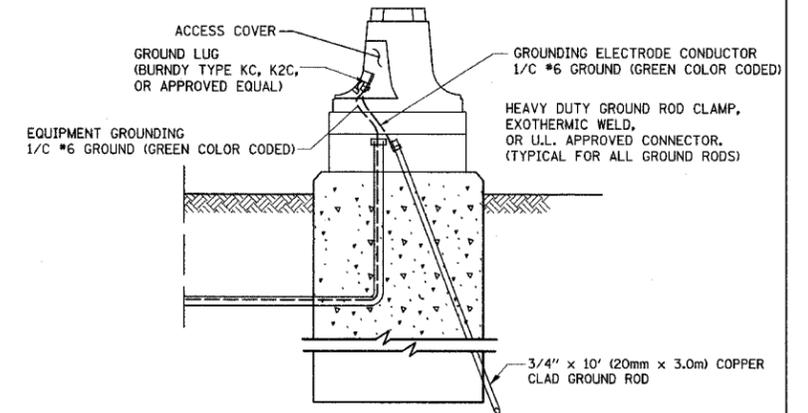
**GROUNDING SYSTEM**

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



**NOTES:**

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**MAST ARM POLE / POST-GROUNDING DETAIL**  
(NOT TO SCALE)

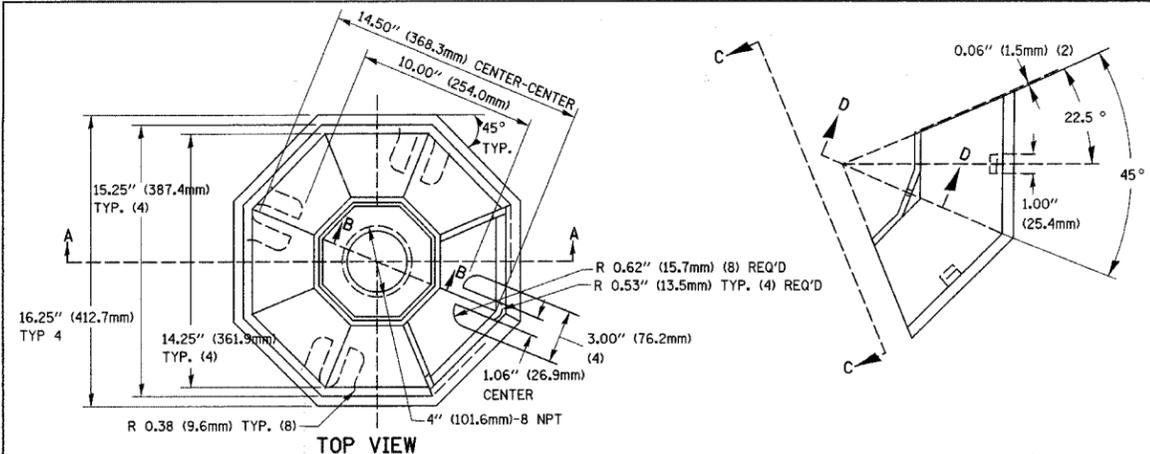
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**DISTRICT 1**  
STANDARD TRAFFIC SIGNAL  
DESIGN DETAILS

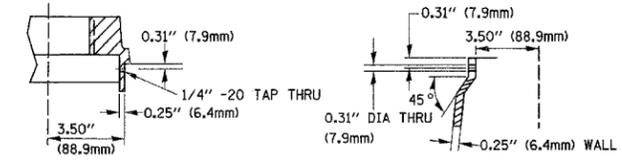
SCALE: VERT. NONE  
HORIZ. NONE  
DATE 1-01-02

DRAWN BY: RWP  
DESIGNED BY: DAD  
CHECKED BY: DAZ  
SHEET 3 OF 4

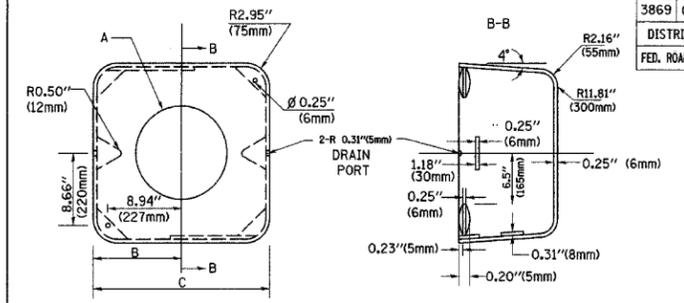
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	31
DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-80031641				



SECTION B-B



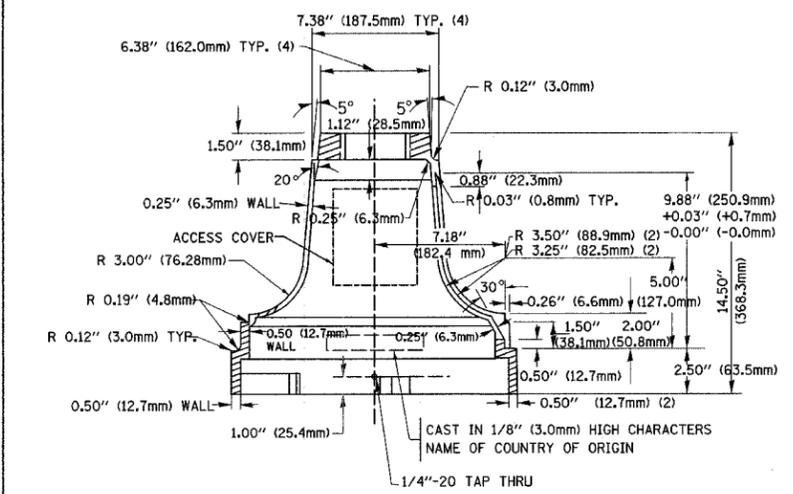
SECTION D-D



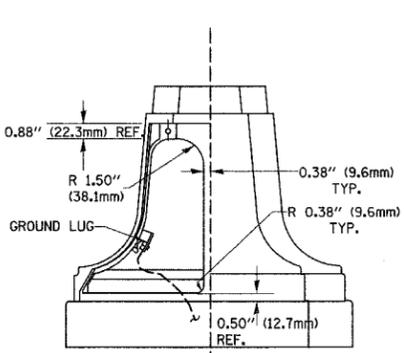
TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125\"(257mm)	9.5\"(241mm)	19\"(483mm)	12\"(300mm)	24kg
II	Ø 11.125\"(283mm)	10.75\"(273mm)	21.5\"(546mm)	12\"(300mm)	26kg

SHROUD DETAIL

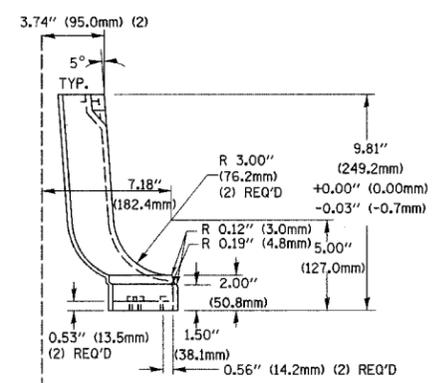
MATERIAL:  
 - ASTM A48 CLASS 30 GREY IRON  
 - ASTM A123 HOT DIPPED GALVANIZED



SECTION A-A



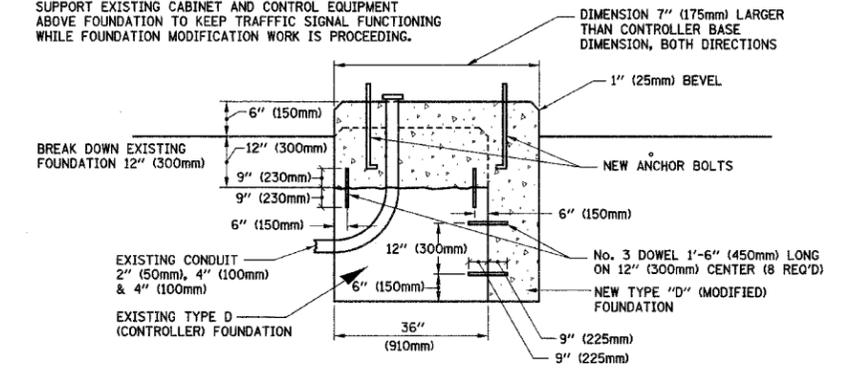
VIEW C-C



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

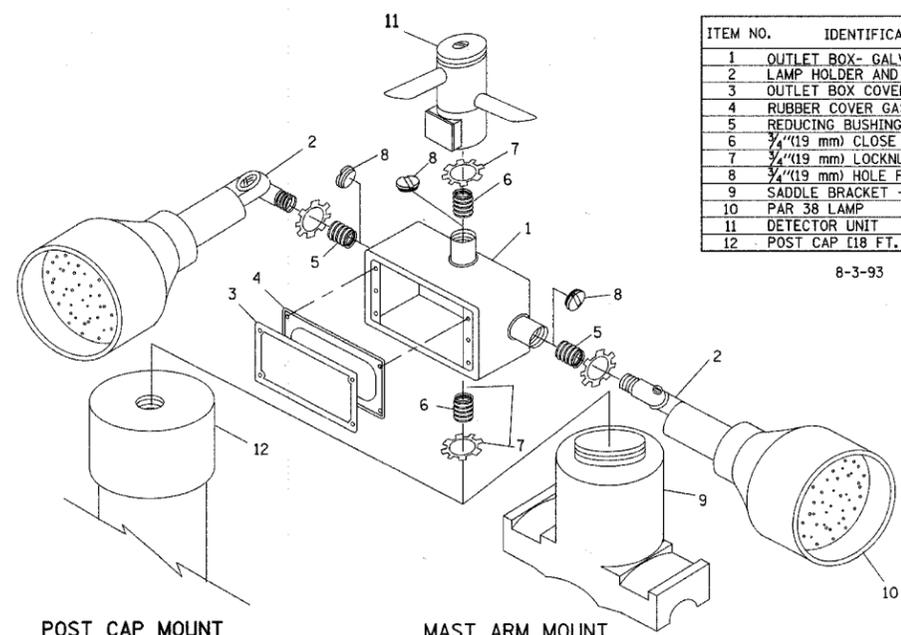
NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION

(NOT TO SCALE)



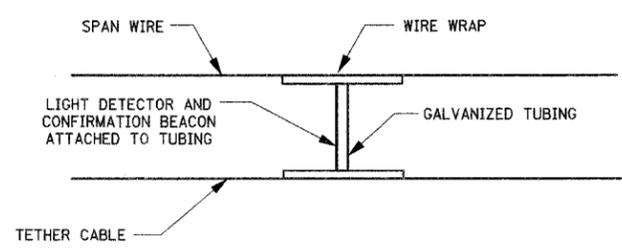
ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU. IN. (0.000344 CU.-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

8-3-93

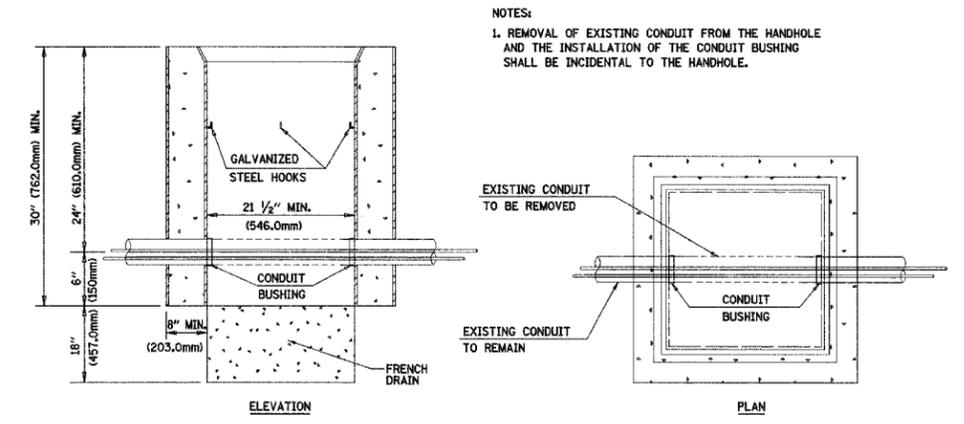
NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

POST CAP MOUNT  
 MAST ARM MOUNT  
 EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS  
 (NOT TO SCALE)



DETAIL  
 HANDHOLE TO INTERCEPT EXISTING CONDUIT  
 N.T.S.

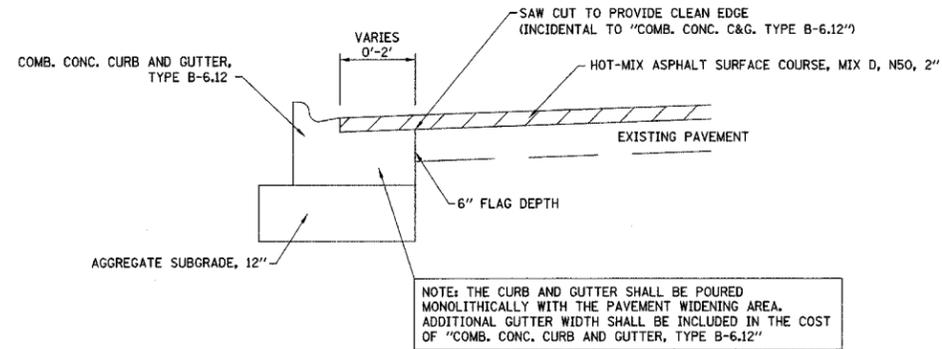
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DISTRICT 1  
 STANDARD TRAFFIC SIGNAL  
 DESIGN DETAILS

SCALE: VERT. NONE  
 HORIZ. NONE  
 DATE 1-01-02

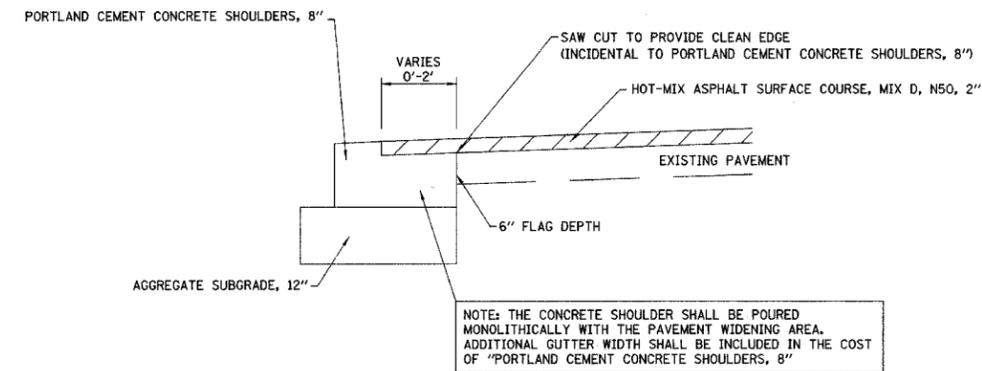
DRAWN BY: RWP  
 DESIGNED BY: DAD  
 CHECKED BY: DAZ  
 SHEET 4 OF 4

F.A.U. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	32
DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(641)				



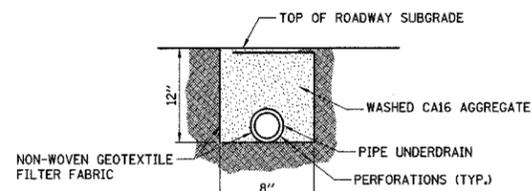
NOTE: THE CURB AND GUTTER SHALL BE POURED MONOLITHICALLY WITH THE PAVEMENT WIDENING AREA. ADDITIONAL GUTTER WIDTH SHALL BE INCLUDED IN THE COST OF "COMB. CONC. CURB AND GUTTER, TYPE B-6.12"

**COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12**  
AT NARROW WIDENING LOCATIONS



NOTE: THE CONCRETE SHOULDER SHALL BE POURED MONOLITHICALLY WITH THE PAVEMENT WIDENING AREA. ADDITIONAL GUTTER WIDTH SHALL BE INCLUDED IN THE COST OF "PORTLAND CEMENT CONCRETE SHOULDERS, 8"

**PORTLAND CEMENT CONCRETE SHOULDERS, 8"**  
AT NARROW WIDENING LOCATIONS



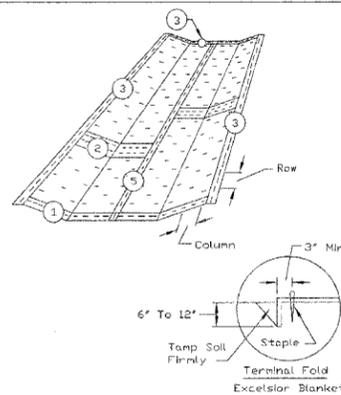
**SECTION B-B**

**GENERAL NOTES:**

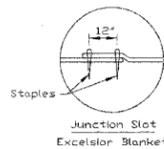
1. BOTH THE TRENCH AND DRAIN TILE SHALL BE WRAPPED WITH NON-WOVEN GEOTEXTILE FILTER FABRIC.
2. WASHED AGGREGATE SHALL BE PLACED AROUND THE DRAIN TILE.
3. HOLE SHALL BE DRILLED INTO STRUCTURE.
4. HYDRAULIC CEMENT SHALL BE PLACED AROUND THE PIPE TO SEAL THE OPENING, BOTH INSIDE AND OUTSIDE THE STRUCTURE.

**PIPE UNDERDRAIN, FABRIC LINED TRENCH, 4"**

**EROSION CONTROL BLANKET**

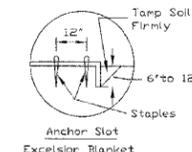


**DETAIL 1**

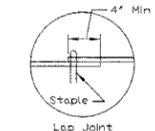


**DETAIL 2**

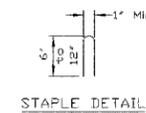
**EROSION CONTROL BLANKET**



**DETAIL 3**



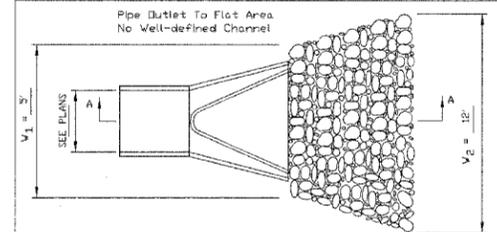
**DETAIL 5**



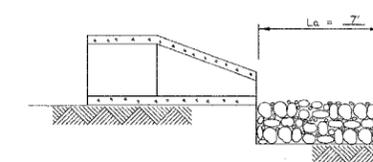
**NOTES:**

1. Staples are to be placed alternately, in columns approximately 2' apart and in rows approximately 3' apart. Approximately 175 staples are required per 4' x 225' roll of material and 125 staples are required per 4' x 150' roll of material.
2. Erosion control material shall be placed loosely over ground surface. Do not stretch.
3. All terminal ends and transverse laps shall be stapled at approximately 12' intervals.

**PIPE OUTLET TO FLAT AREA**



**PLAN**

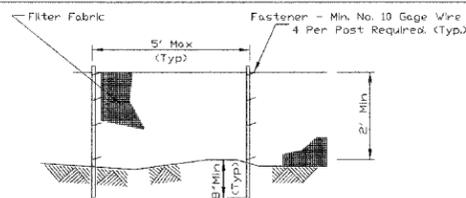


**SECTION A-A**

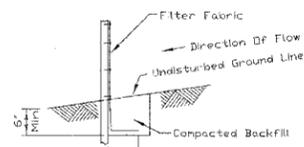
**NOTES:**

1. The rock riprap shall meet the IDOT requirements for the following gradation RR-3, Quality A.

**SILT FENCE PLAN**



**ELEVATION**

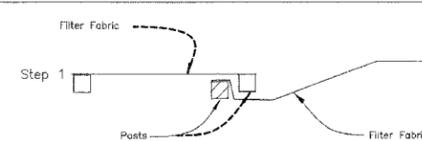


**FABRIC ANCHOR DETAIL**

**NOTES:**

1. Temporary sediment fence shall be installed prior to any grading work in the area to be protected. They shall be maintained throughout the construction period and removed in conjunction with the final grading and site stabilization.
2. Fence posts shall be either standard steel post or wood post with a minimum cross-sectional area of 3.0 sq. in.

**SILT FENCE**

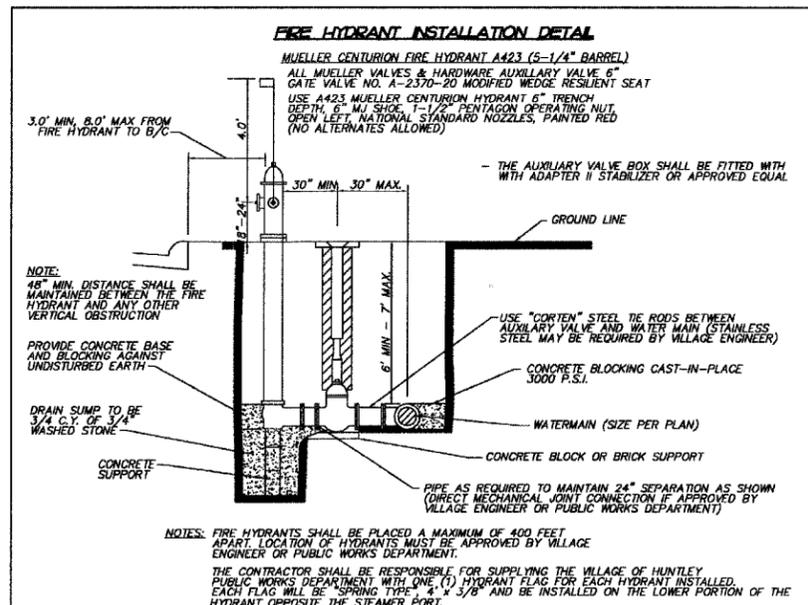
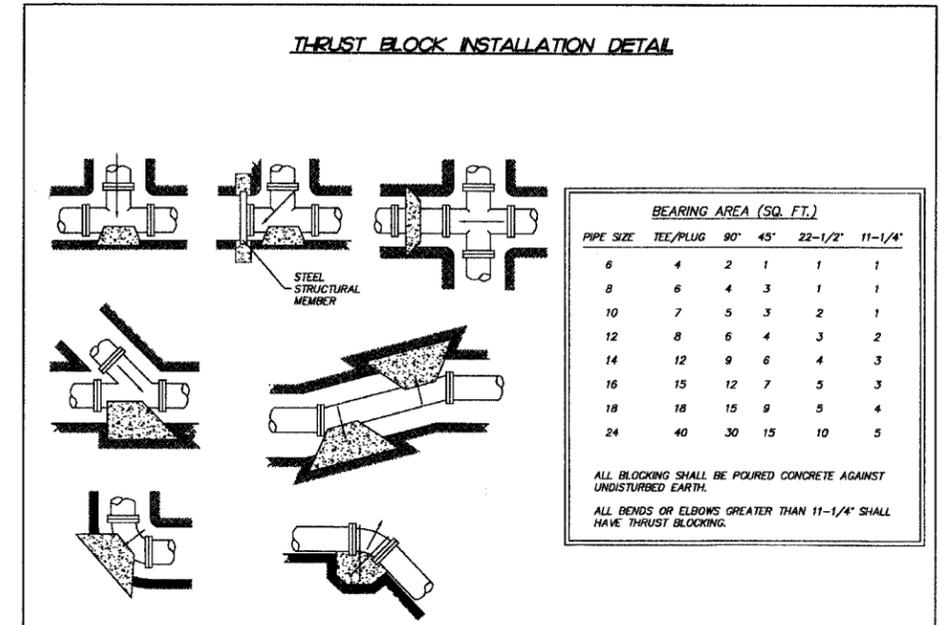
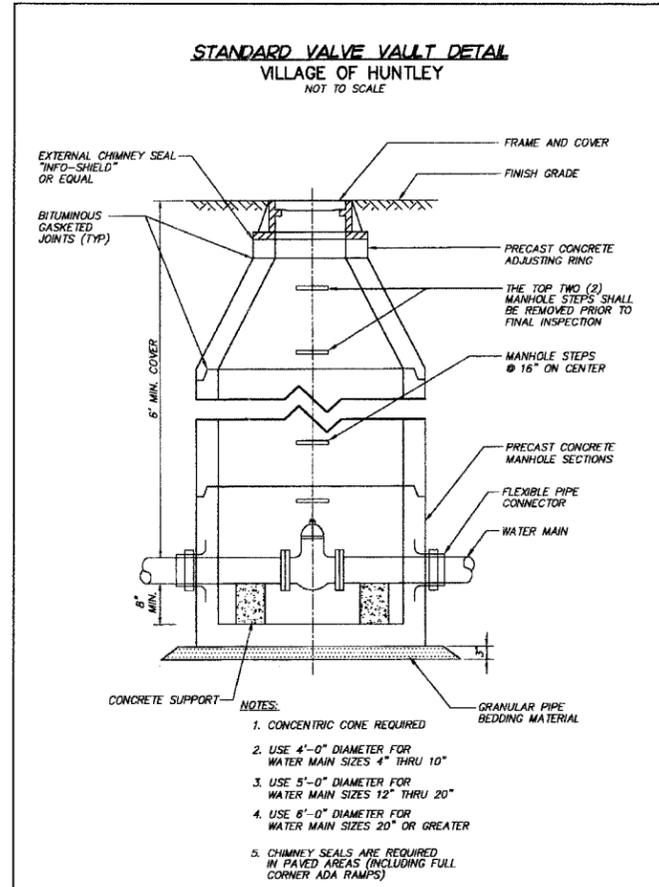
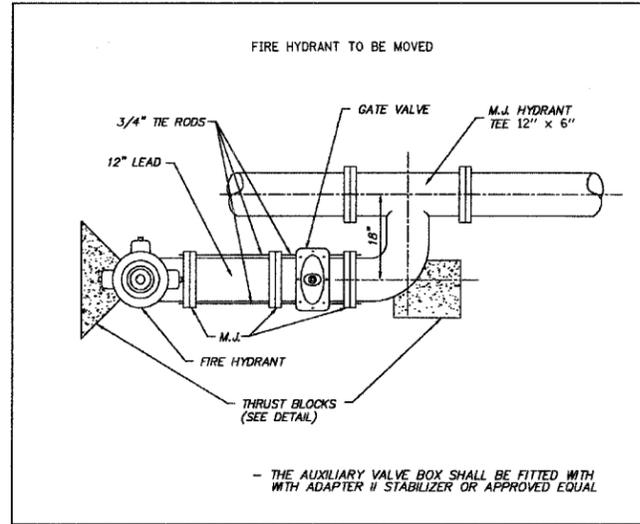


**ATTACHING TWO SILT FENCES**

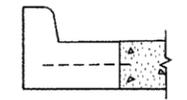
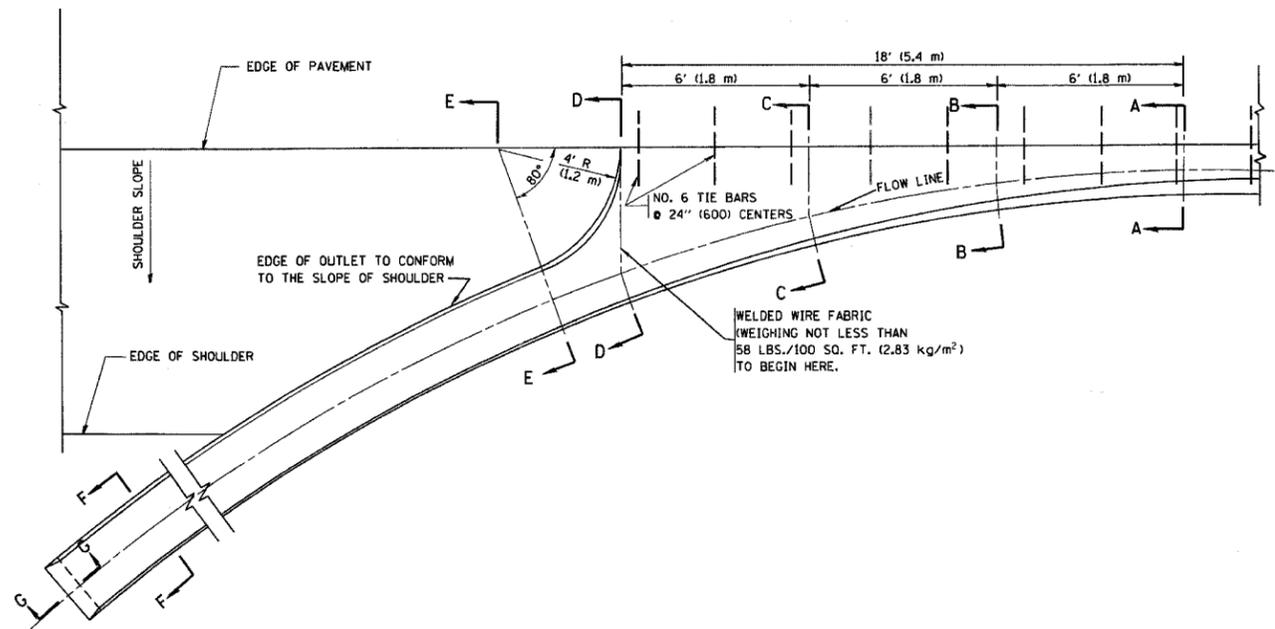
**NOTES:**

1. Place the end post of the second fence inside the end post of the first fence.
2. Rotate both posts at least 180 degrees in a clockwise direction to create a tight seal with the fabric material.
3. Drive both posts a minimum of 18 inches into the ground and bury the flap.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FF	MCHENRY	56	33
DETAILS				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-80031641				

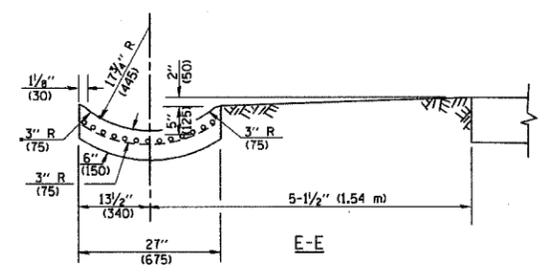


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3889	08-00030-00-FP	MCHENRY	56	34
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

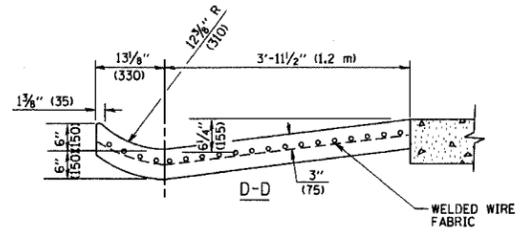


A-A \*

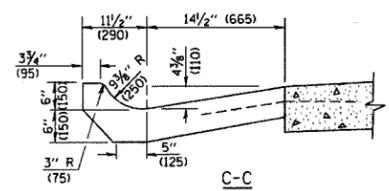
\* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



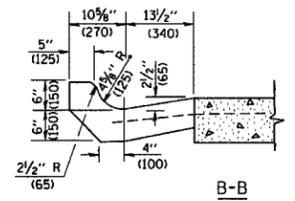
E-E



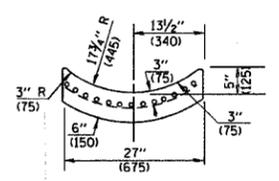
D-D



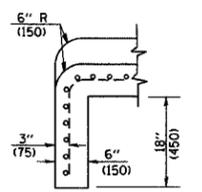
C-C



B-B



F-F



G-G

**GENERAL NOTES**

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO.6) AT 24\"/>

IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%. THIS DISTANCE SHALL BE INCREASED 6' (1.8 m) FOR EACH 1% INCREASE IN GRADE.

**QUANTITIES**

FOR SECTION A-A TO E-E AND CURTAIN WALL =  
 1.25 CU. YDS. (0.96 m<sup>3</sup>) CLASS SI CONCRETE (OUTLET) FOR 9\"/>

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	8/4/86
R. SHAH	09/09/94
R. SHAH	10/25/94
E. GOMEZ	12/21/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

**OUTLET FOR CONCRETE CURB AND GUTTER**

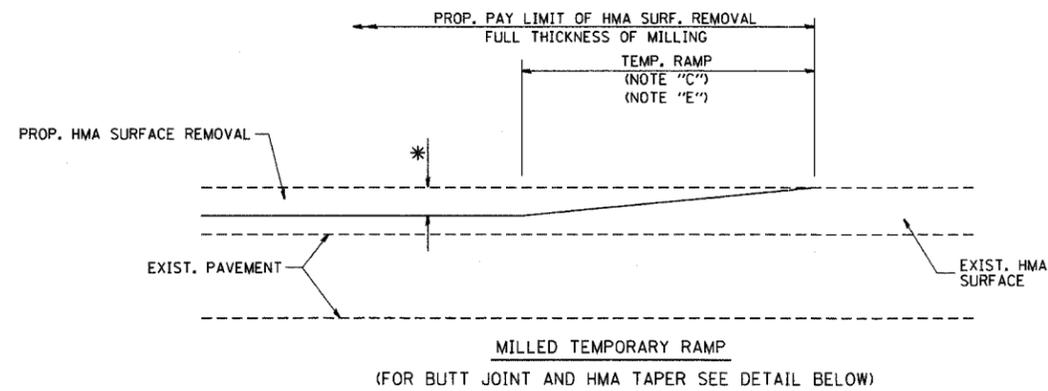
SCALE: VERT. HORIZ. DATE: 1/18/2007

DRAWN BY  
 CHECKED BY  
 B0600-01 (BD-03)  
 REVISION DATE: 01/01/07

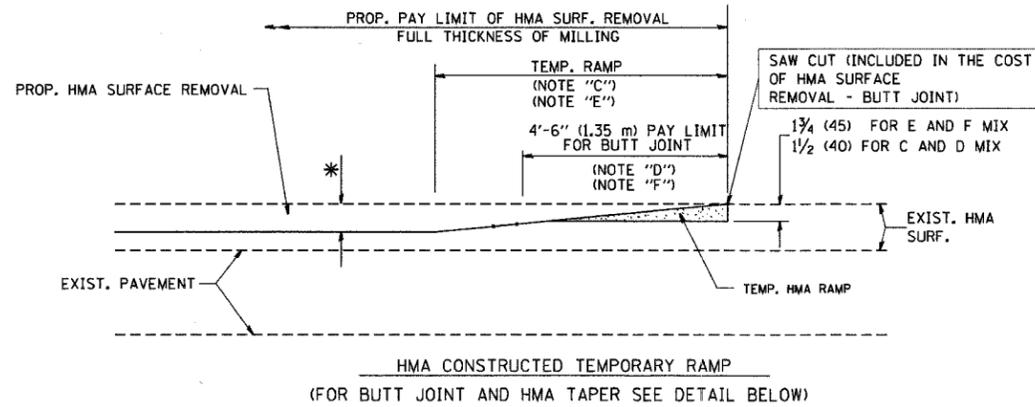
PLOT DATE = 1/18/2007  
 PLOT SCALE = 80000% / IN.  
 USER NAME = drivak03gn



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3889	06-00030-00-FP	MCHENRY	56	36
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		
		M-8003(641)		

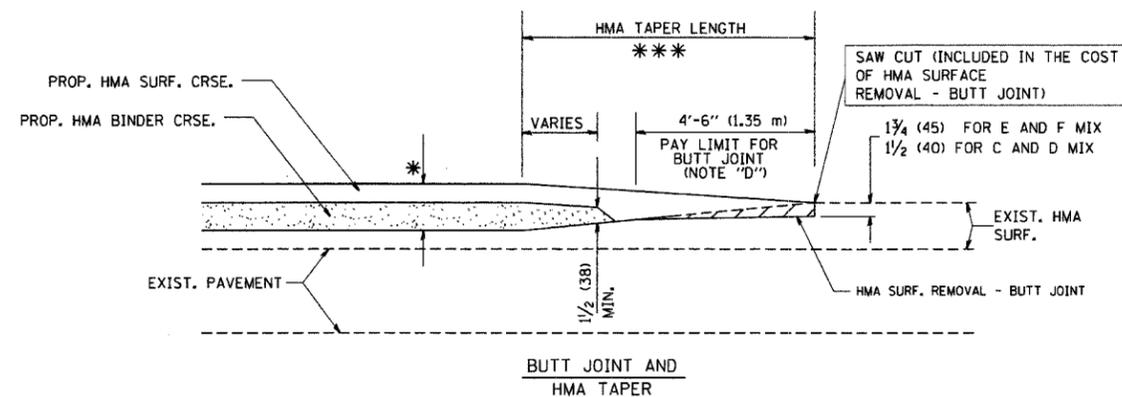


OPTION 1

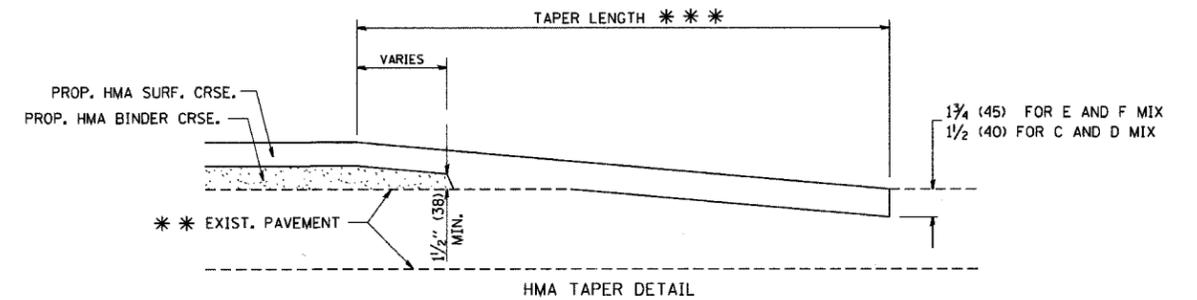
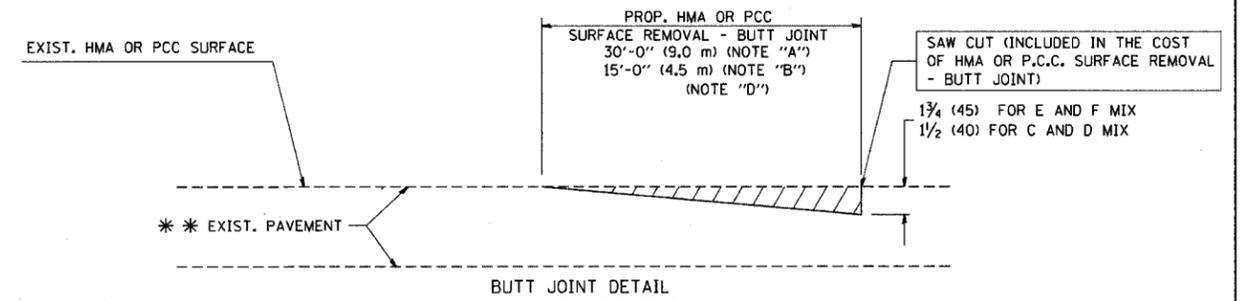


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

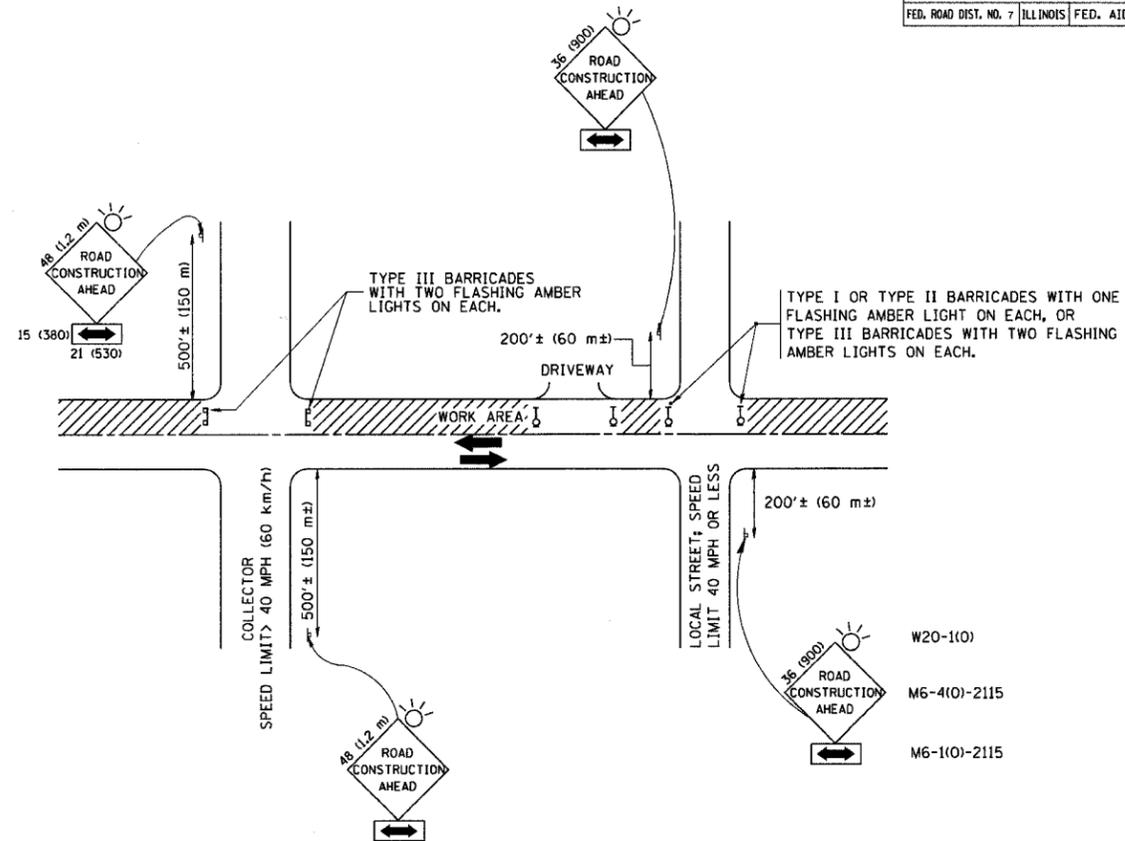
BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE  
HORIZ. NONE  
PLOT DATE: 1/18/2007

DRAWN BY  
CHECKED BY

BD400-05 (VI-BD32)  
REVISION DATE: 01/01/07

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	37
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		
M-8003(641)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

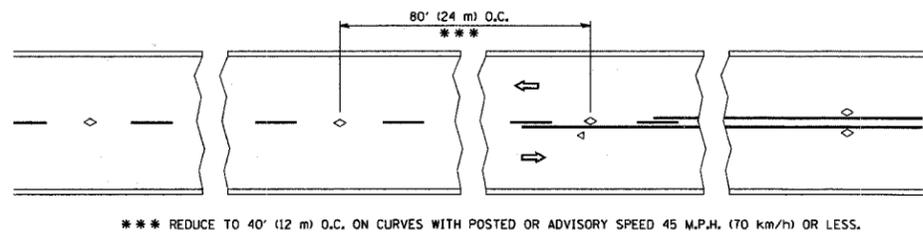
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TRAFFIC CONTROL AND PROTECTION  
 FOR  
 SIDE ROADS, INTERSECTIONS, AND  
 DRIVEWAYS

SCALE:  
 DATE: 1/17/2007

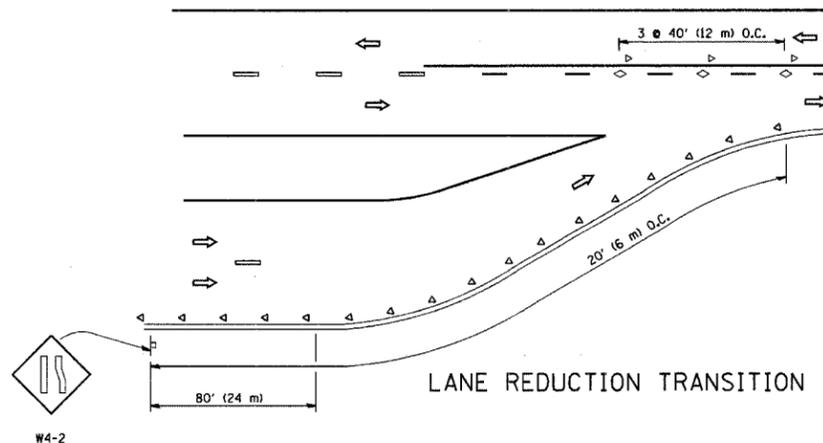
DRAWN BY  
 CHECKED BY  
 TC-10  
 REVISION DATE: 01/06/00

F.A.J.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	08-00030-00-FP	MCHENRY	56	38
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

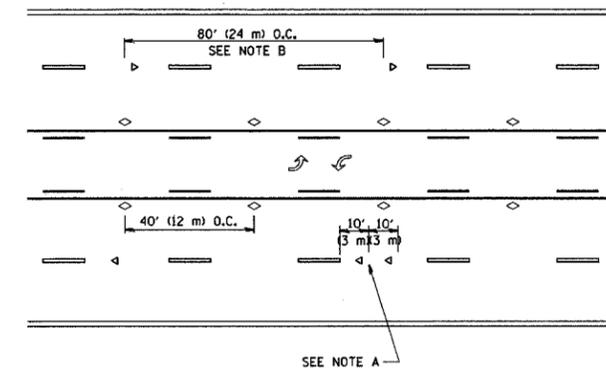


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

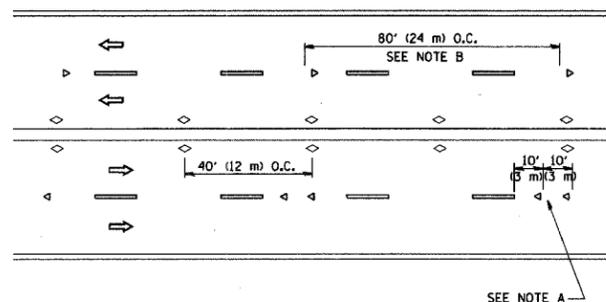
TWO-LANE/TWO-WAY



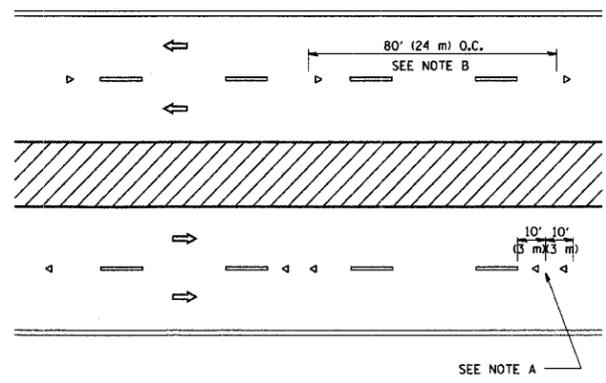
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◁ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◇ TWO-WAY AMBER MARKER

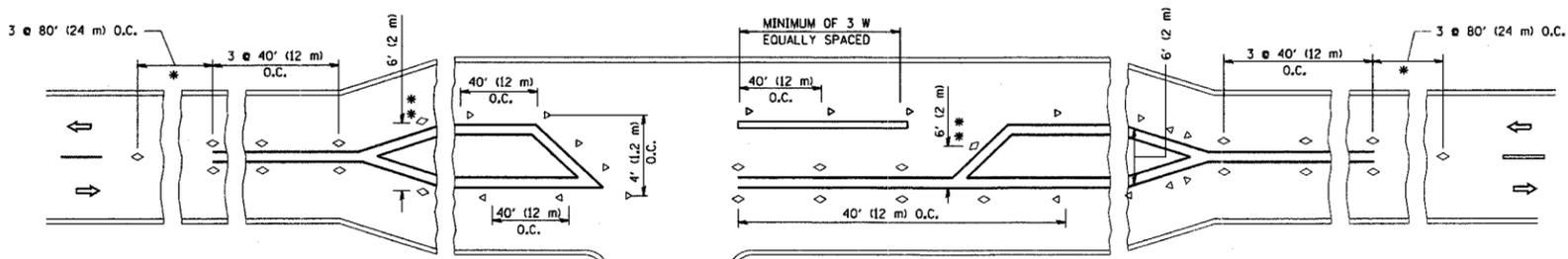
LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in millimeters (inches) unless otherwise shown.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT  
 MARKERS (SNOW-PLOW RESISTANT)

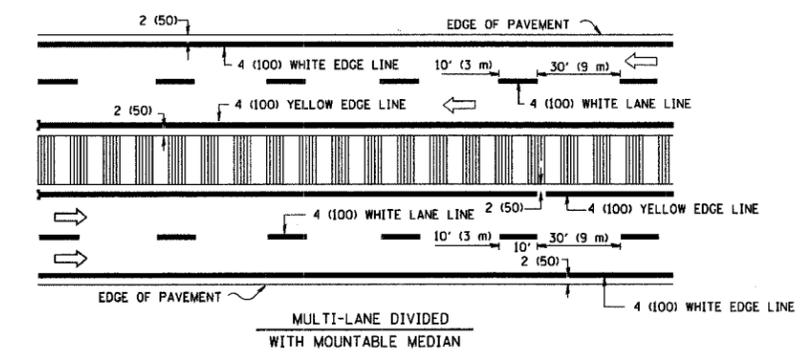
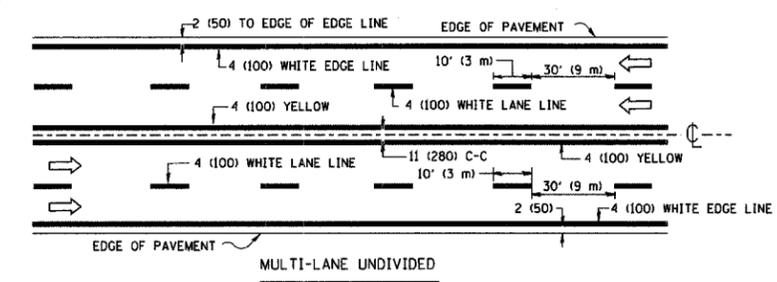
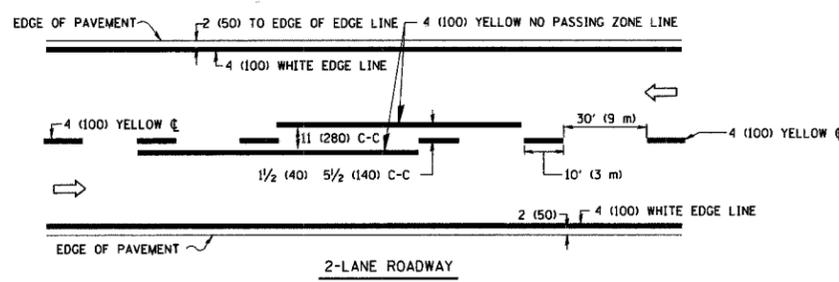
SCALE: NONE  
 DATE: 1/17/2007

DRAWN BY CADD  
 CHECKED BY

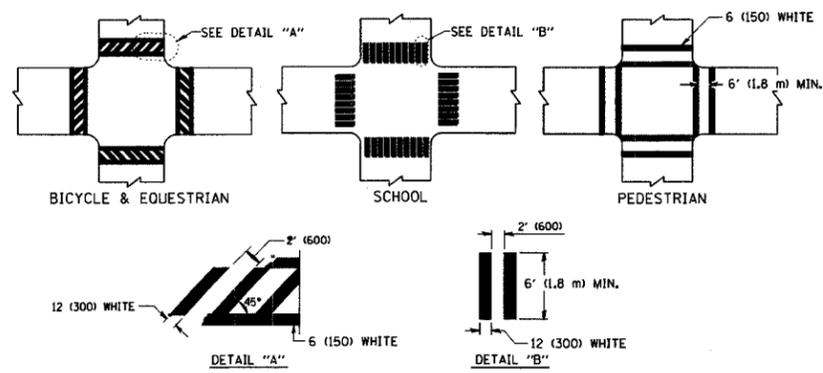
TC-11

REVISION DATE: 01/06/00

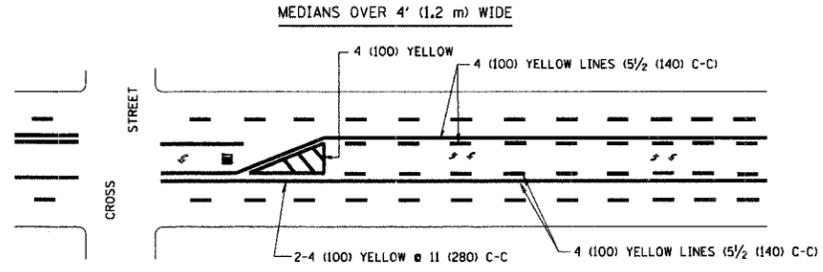
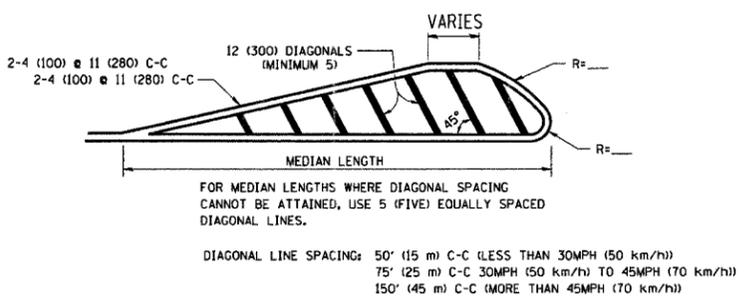
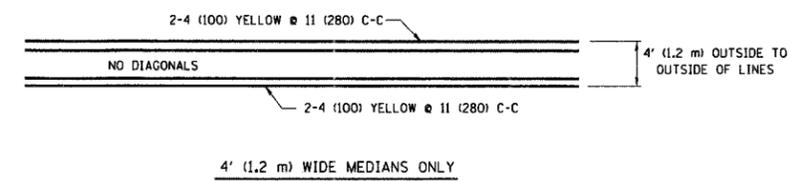
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	39
STA.		TO STA.		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



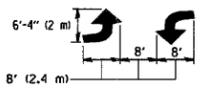
TYPICAL LANE AND EDGE LINE MARKING



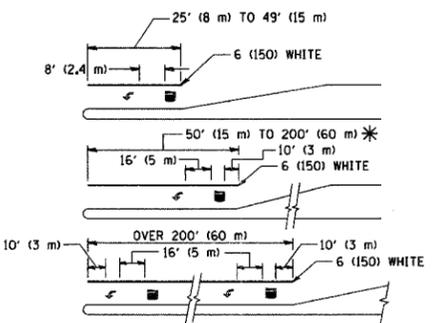
TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

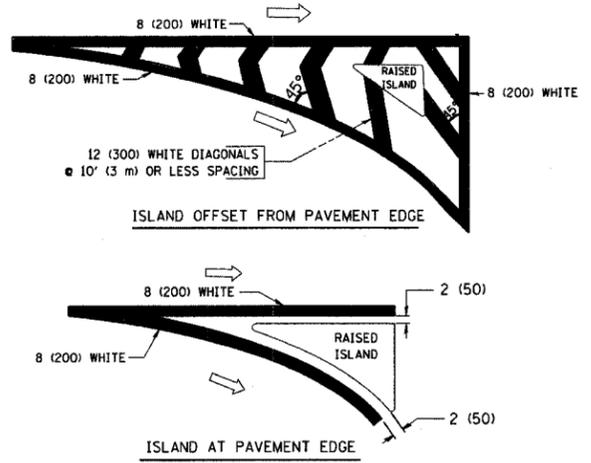


TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>) \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	5 (125) ON FREEWAYS	SKIP-DASH	WHITE	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN)	2 @ 6 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
A. DIAGONALS (BIKE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
B. LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

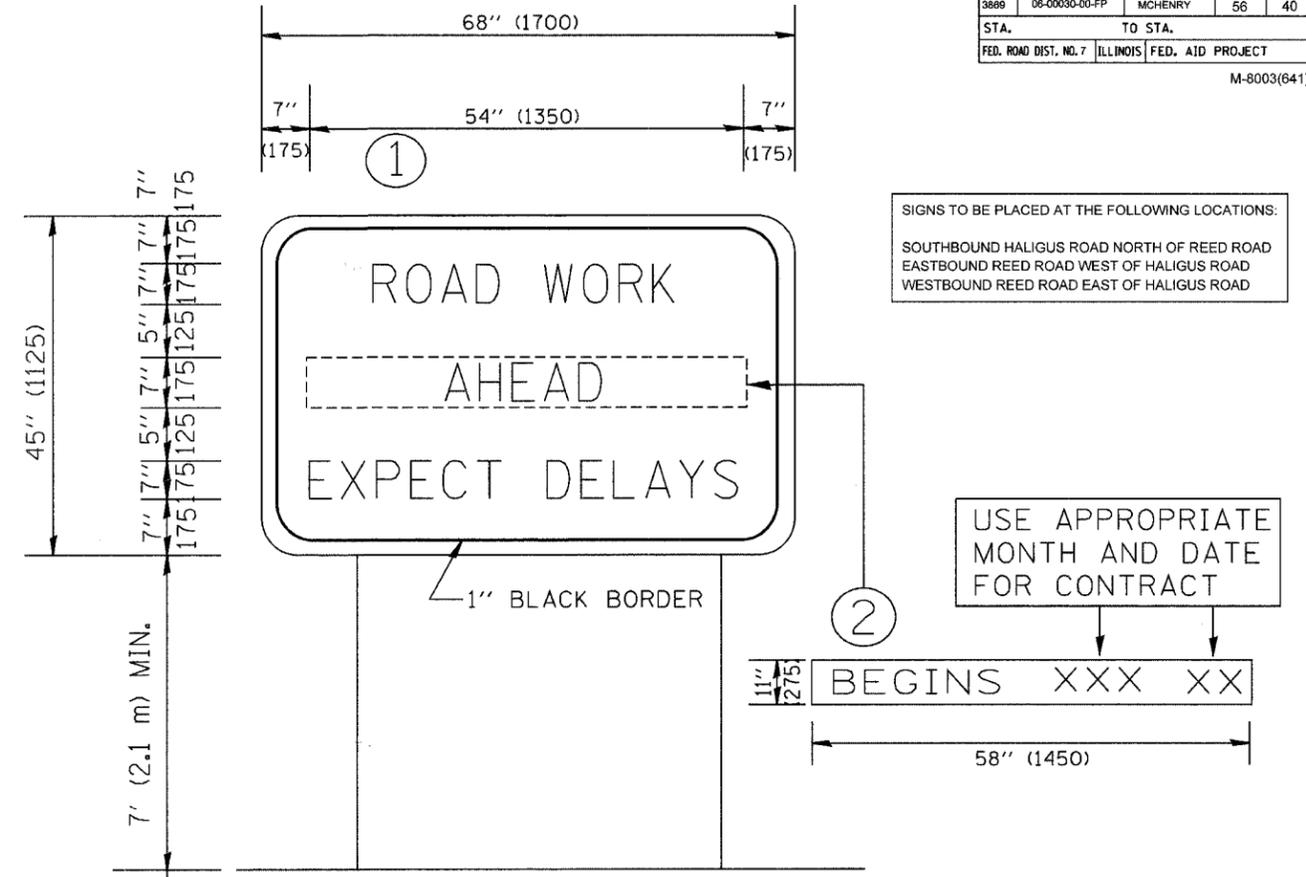
All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE  
DATE: 1/17/2007  
DRAWN BY CADD  
CHECKED BY  
TC-13  
REVISION DATE: 01/06/00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3889	06-00030-00-FP	MCHENRY	56	40
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		
M-8003(641)				



SIGNS TO BE PLACED AT THE FOLLOWING LOCATIONS:  
 SOUTHBOUND HALIGUS ROAD NORTH OF REED ROAD  
 EASTBOUND REED ROAD WEST OF HALIGUS ROAD  
 WESTBOUND REED ROAD EAST OF HALIGUS ROAD

USE APPROPRIATE  
 MONTH AND DATE  
 FOR CONTRACT

NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES)  
 UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. MIRS	9-15-97
R. MIRS	12-11-97
T. RAMMACHER	2-2-99

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TEMPORARY INFORMATION SIGNING

SCALE:  
 DATE: 1/17/2007

DRAWN BY DESIGN  
 CHECKED BY

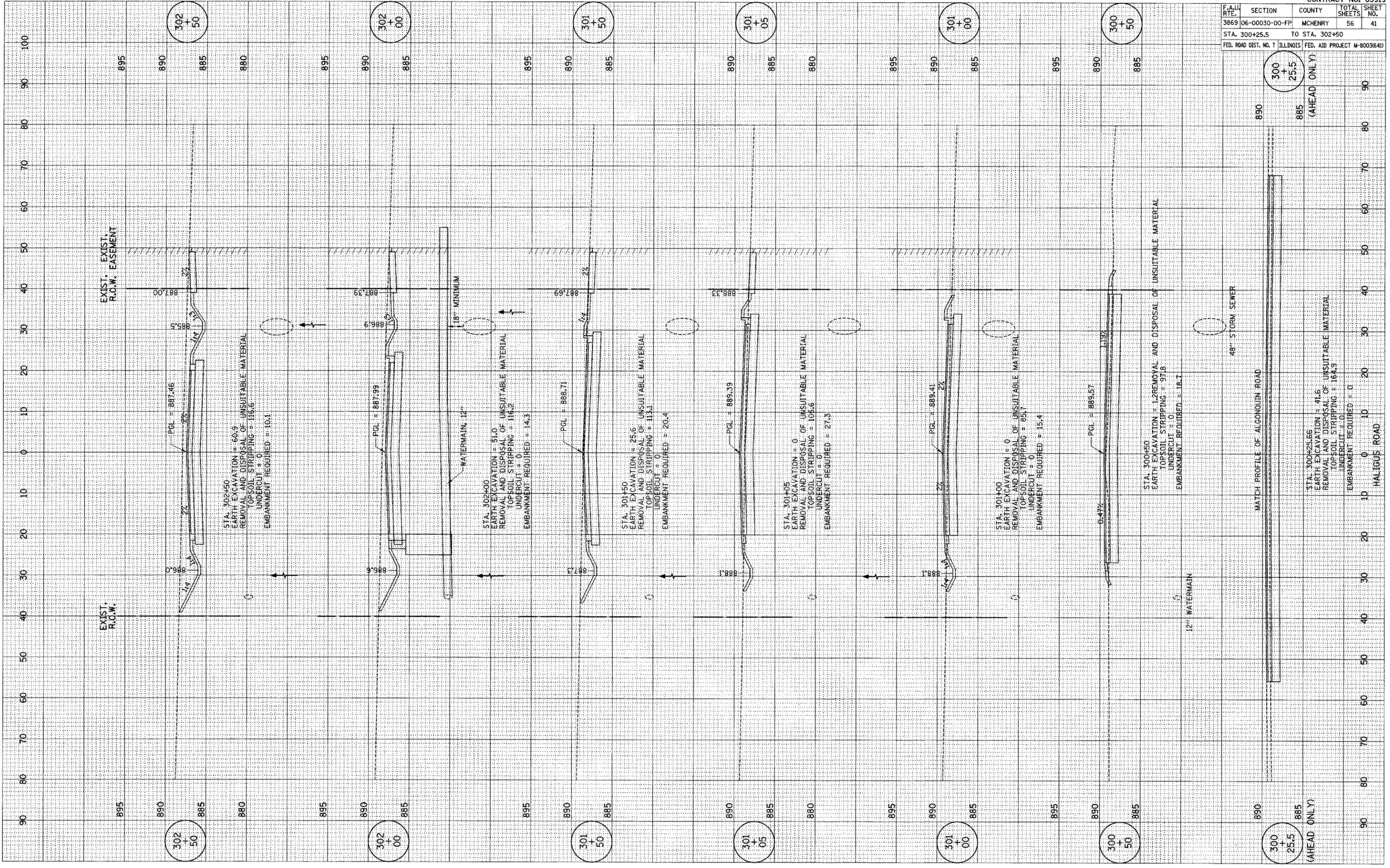
TC22  
 REVISION DATE: 02/02/99

PLOT DATE = 1/17/2007  
 PLOT SCALE = 1/16" = 1' (1:96)  
 PLOT SIZE = 11x17 IN.  
 USER NAME = lrpgo

PLOT DATE = DATE  
 PLOT SCALE = SCALE  
 REFERENCE = REF

ORIGINAL SURVEY PLOTTED  
 NOTE BOOK NO.  
 BY DATE

FINAL SURVEY PLOTTED  
 NOTE BOOK NO.  
 BY DATE



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	41
STA. 300+25.5		TO STA. 302+50		
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT W-800364D

300 + 25.5  
 (AHEAD ONLY)

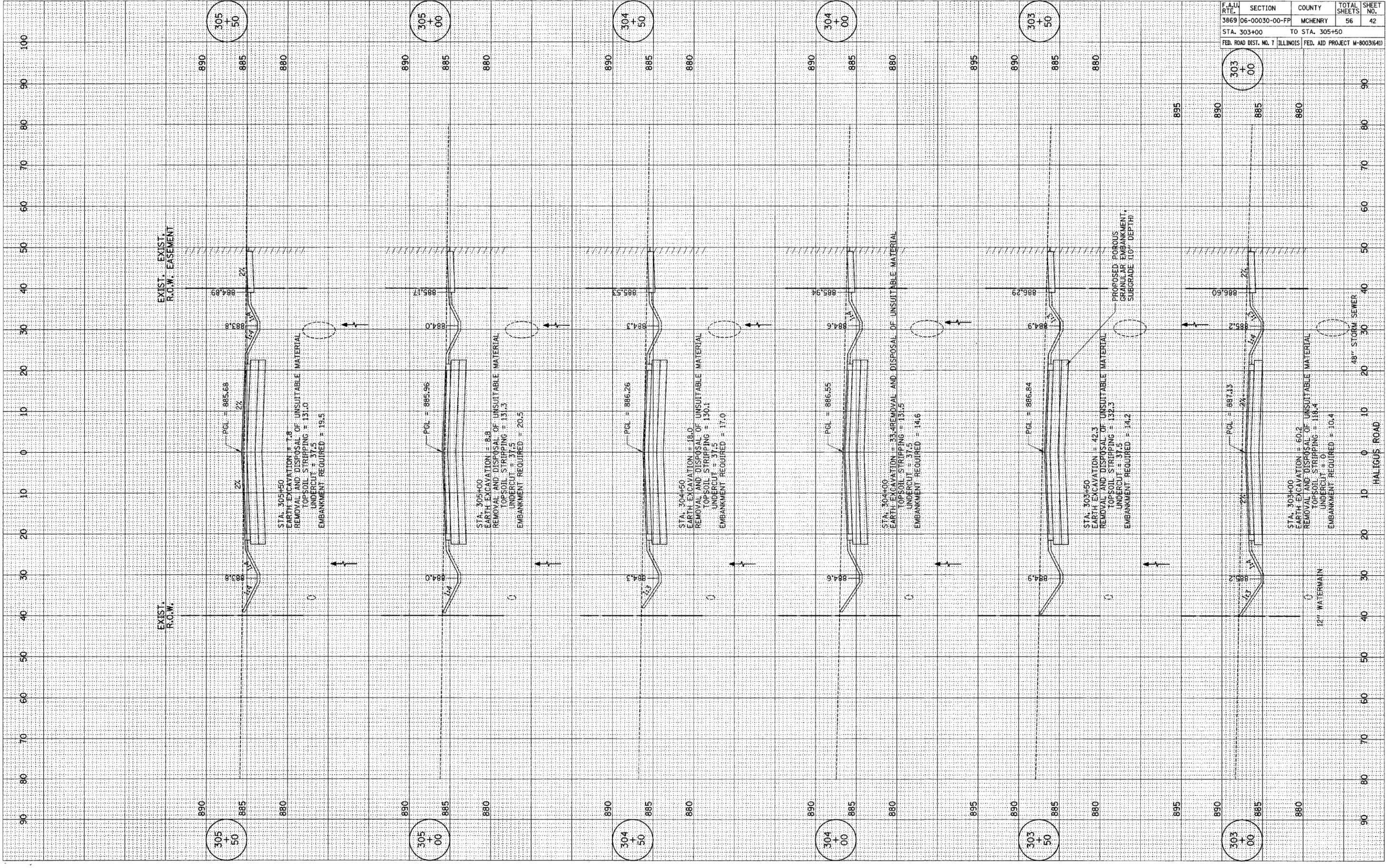
300 + 25.5  
 (AHEAD ONLY)

HALIGUIS ROAD

PLOT DATE = #DATE#  
 PLOT SCALE = #SCALE#  
 REFERENCE = #REF#

ORIGINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED  
 BY DATE

FINAL SURVEY PLOTTED TEMPLATE AREAS CHECKED  
 BY DATE



F.A.U. RT. 3869		SECTION 06-00030-00-FP	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 42
STA. 303+00			TO STA. 305+50		
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT M-8003(641)	

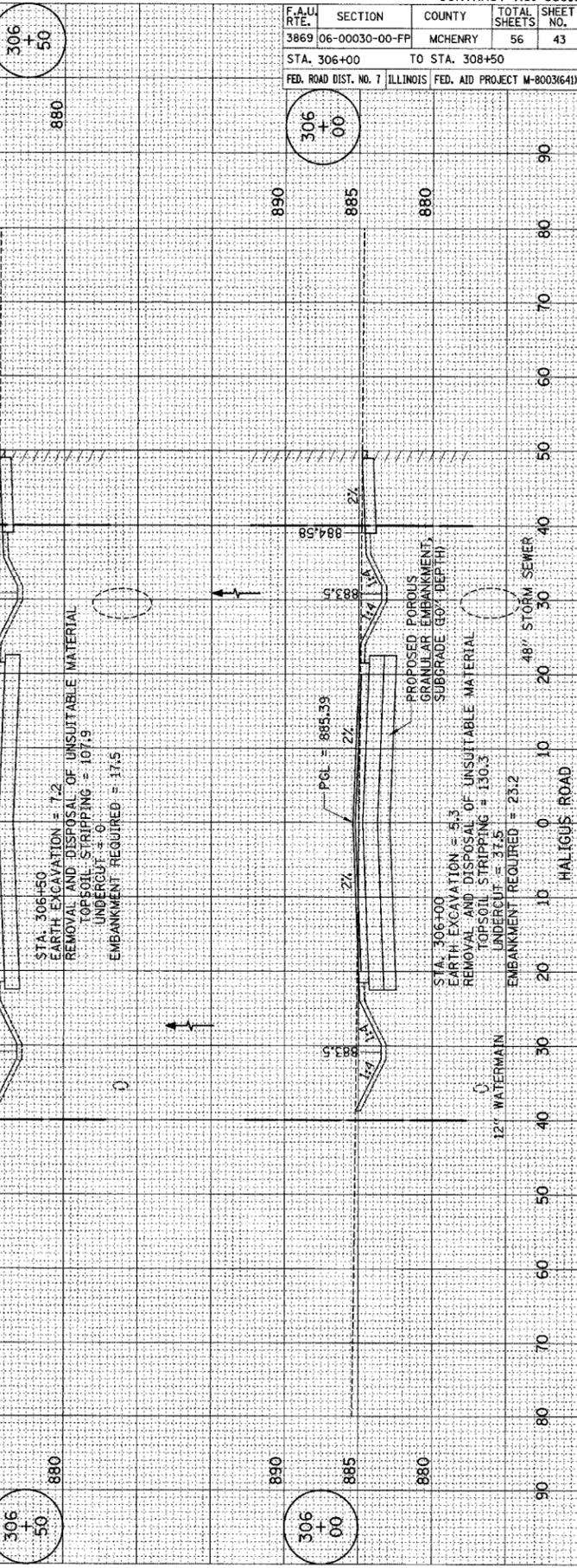
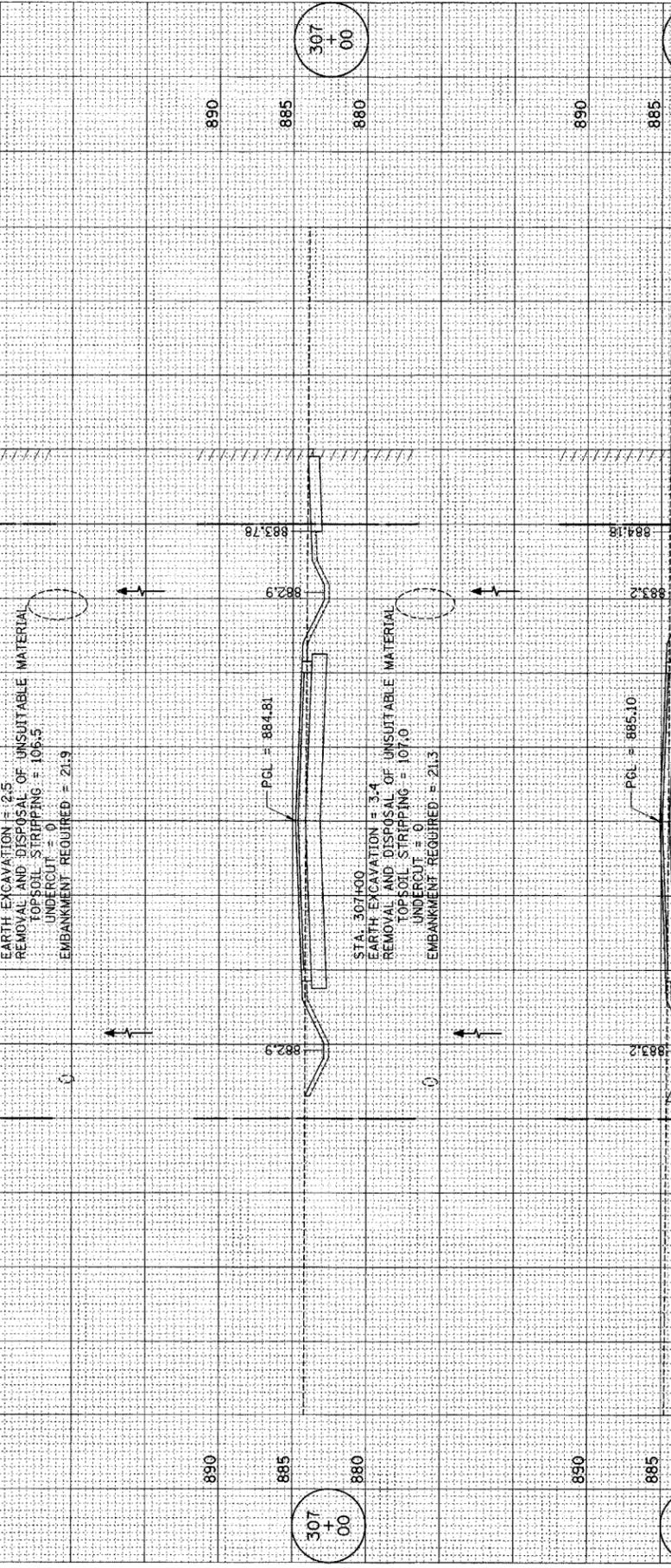
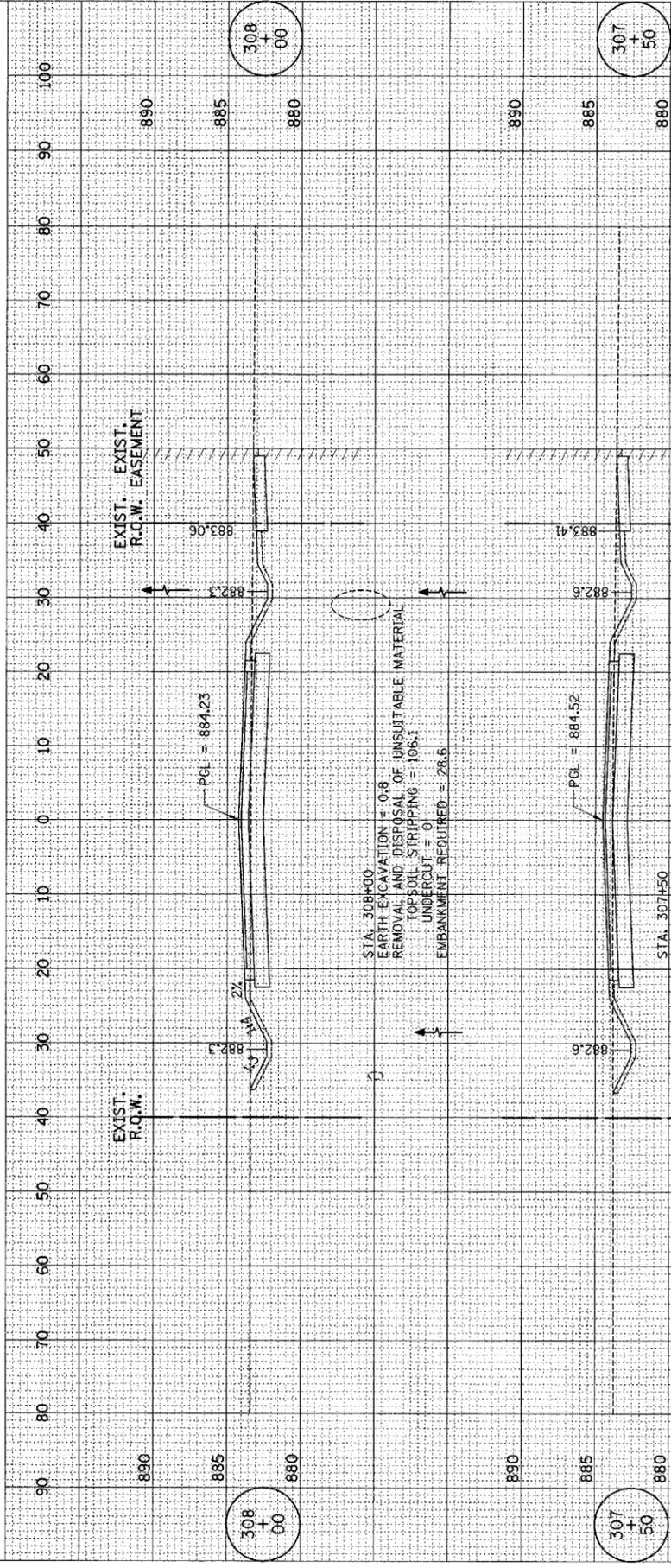
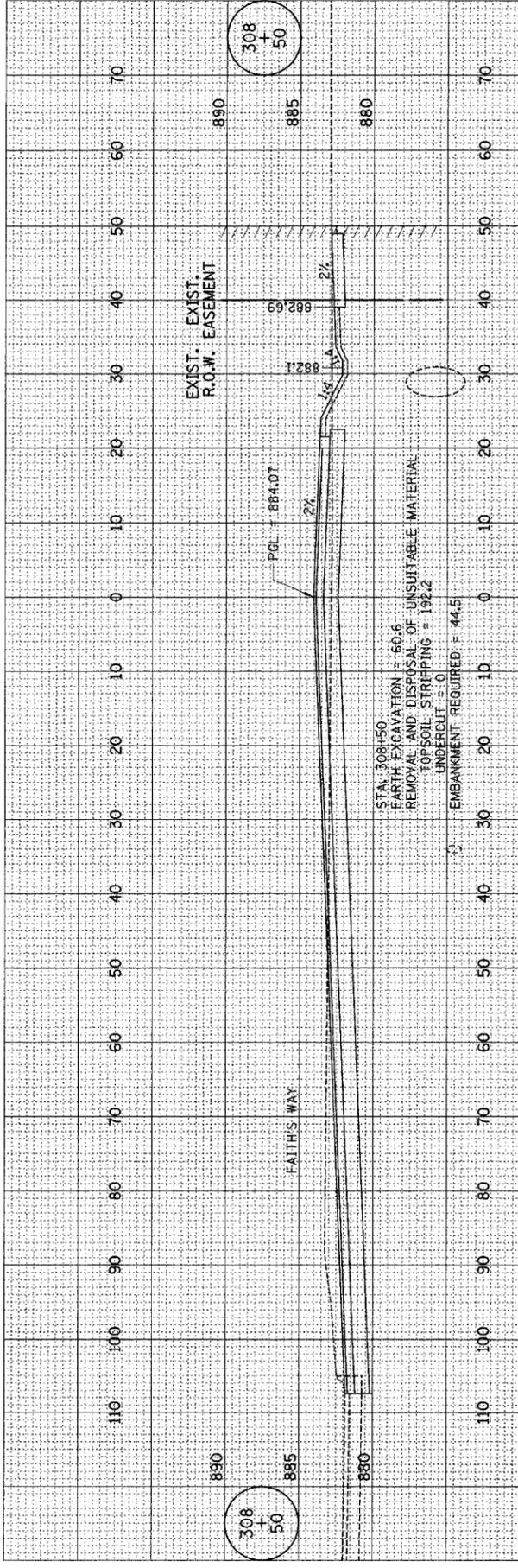
CONTRACT NO. 83919

PLOT DATE = #DATE\*  
 PLOT SCALE = #SCALE\*  
 REFERENCE = #REF#

ORIGINAL SURVEY PLOTTED  
 NOTE BOOK NO. AREAS CHECKED

FINAL SURVEY PLOTTED  
 NOTE BOOK NO. AREAS CHECKED

BY: \_\_\_\_\_ DATE: \_\_\_\_\_



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FF	MCHENRY	56	43
STA. 306+00		TO STA. 308+50		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT M-8003641		

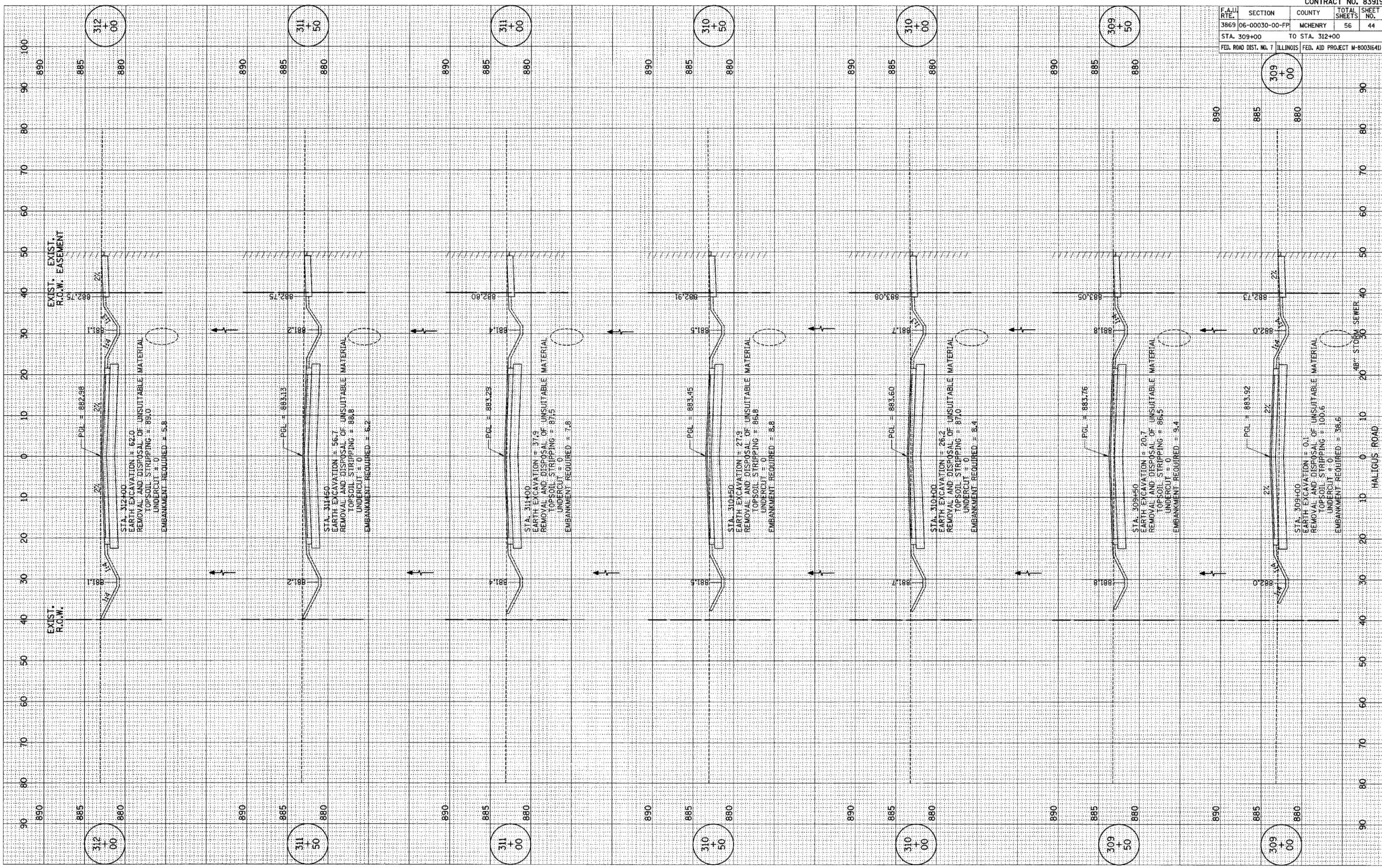
CONTRACT NO. 83919

PLOT DATE = 04/25/08  
 PLOT SCALE = AS SHOWN  
 REFERENCE = #REF#

ORIGINAL SURVEY PLOTTED TEMPLATE NO. AREAS CHECKED

FINAL SURVEY PLOTTED TEMPLATE NO. AREAS CHECKED

BY DATE



F.A.U. RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869		06-00030-00-FP	MCHENRY	56	44
STA. 309+00			TO STA. 312+00		
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT M-80036(41)	

CONTRACT NO. 83919



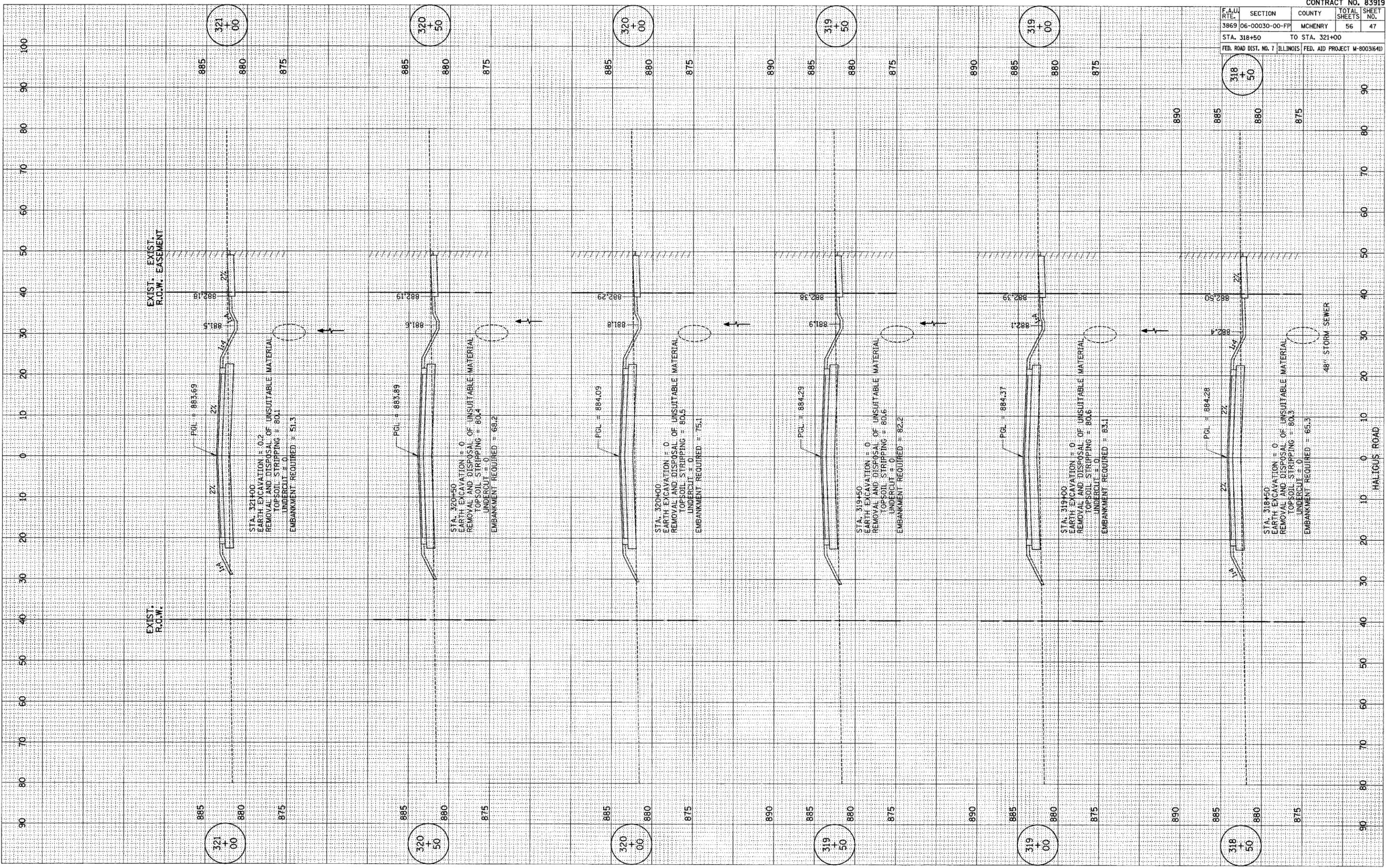


PLOT DATE = \*DATE\*  
 FILE NAME = \*FILE\*  
 PLOT SCALE = \*SCALE\*  
 REFERENCE = \*REF\*

ORIGINAL SURVEY  
 SURVEY PLOTTED  
 NOTE BOOK  
 AREAS CHECKED

FINN SURVEY  
 SURVEY PLOTTED  
 NOTE BOOK  
 AREAS CHECKED

BY DATE



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	47

STA. 318+50 TO STA. 321+00  
 FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003641D

48" STORM SEWER

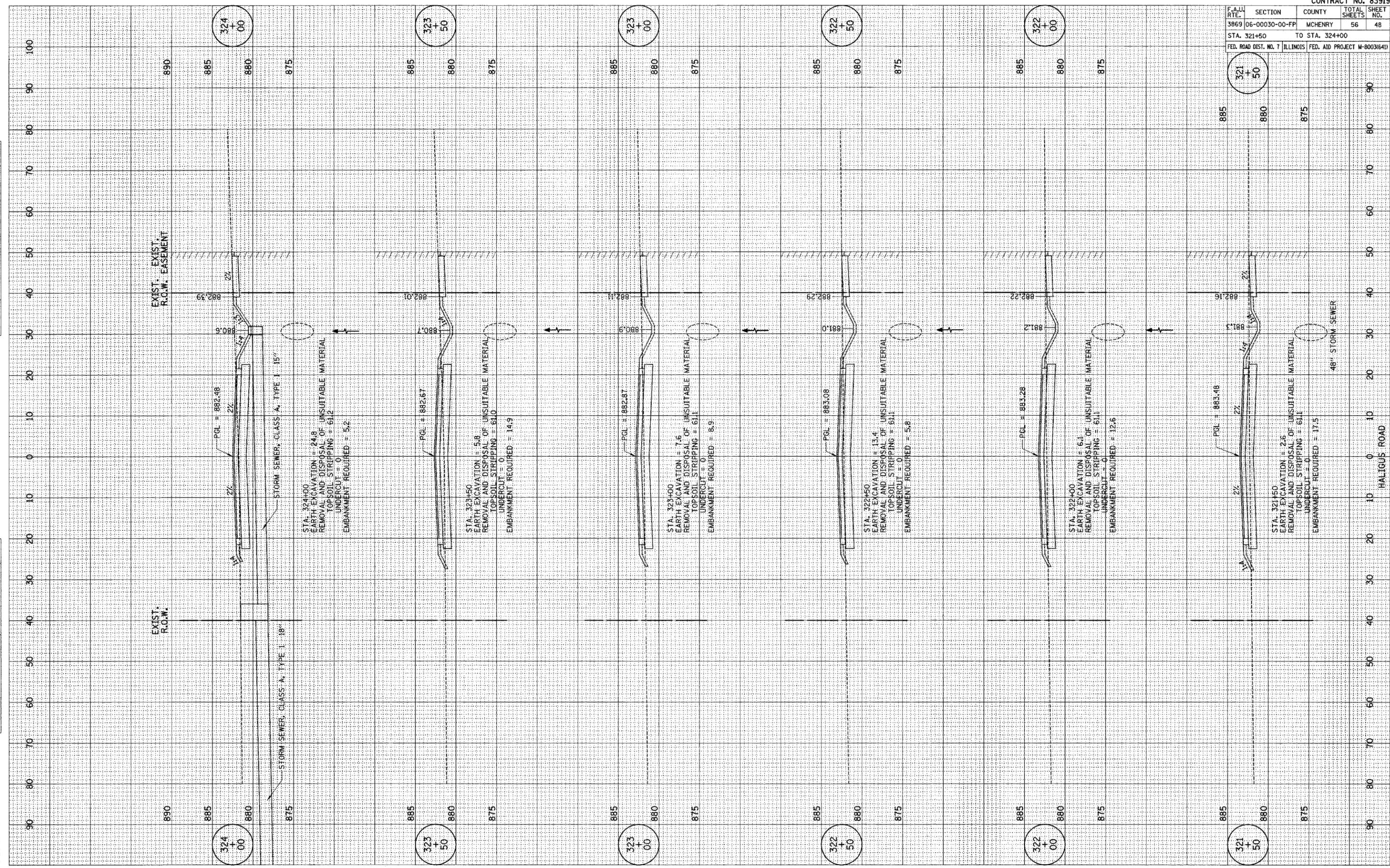
HALIGUS ROAD

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-PP	MCHENRY	56	48
STA. 321+50		TO STA. 324+00		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT W-6003641				

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK NO.		
AREAS CHECKED		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK NO.		
AREAS CHECKED		

PLOT DATE = 8/1/84  
 FILE NAME = 83919  
 PLOT SCALE = 1"=40'  
 REFERENCE = 83919



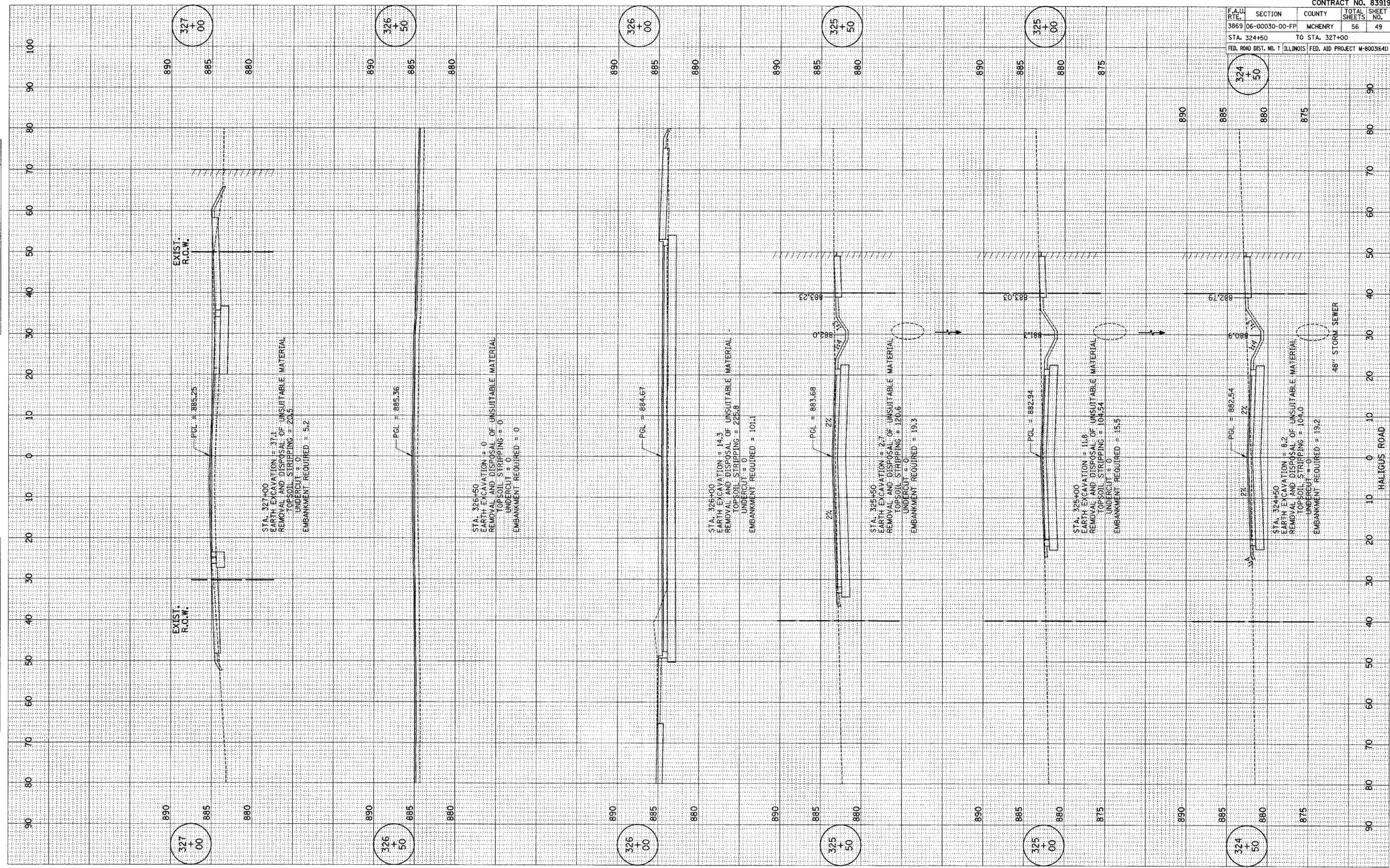
HALLIGUS ROAD

48" STORM SEWER

PLOT DATE = 04/27/06  
 FILE NAME = 06FILE14  
 PLOT SCALE = 1/4"=1'-0"  
 REFERENCE = 06REF

ORIGINAL SURVEY BY DATE  
 SURVEYED BY DATE  
 PLOTTED BY DATE  
 NOTE BOOK NO.  
 TYPED BY DATE  
 AREAS CHECKED

FINAL SURVEY BY DATE  
 SURVEYED BY DATE  
 PLOTTED BY DATE  
 NOTE BOOK NO.  
 TYPED BY DATE  
 AREAS CHECKED



327  
+  
00

890  
885  
880

EXIST.  
R.O.W.

PCL = 885.25

STA. 327+00  
 EARTH EXCAVATION = 37.1  
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
 TOPSOIL STRIPPING = 20.5  
 UNDERCUT = 0  
 EMBANKMENT REQUIRED = 5.2

326  
+  
50

890  
885  
880

EXIST.  
R.O.W.

PCL = 885.36

STA. 326+50  
 EARTH EXCAVATION = 0  
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
 TOPSOIL STRIPPING = 0  
 UNDERCUT = 0  
 EMBANKMENT REQUIRED = 0

326  
+  
00

890  
885  
880

EXIST.  
R.O.W.

PCL = 884.57

STA. 326+00  
 EARTH EXCAVATION = 14.3  
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
 TOPSOIL STRIPPING = 225.8  
 UNDERCUT = 0  
 EMBANKMENT REQUIRED = 101.1

325  
+  
50

890  
885  
880

EXIST.  
R.O.W.

PCL = 883.68

STA. 325+50  
 EARTH EXCAVATION = 2.7  
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
 TOPSOIL STRIPPING = 120.6  
 UNDERCUT = 0  
 EMBANKMENT REQUIRED = 19.3

325  
+  
00

890  
885  
880

EXIST.  
R.O.W.

PCL = 882.94

STA. 325+00  
 EARTH EXCAVATION = 11.8  
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
 TOPSOIL STRIPPING = 104.54  
 UNDERCUT = 0  
 EMBANKMENT REQUIRED = 15.5

324  
+  
50

890  
885  
880  
875

EXIST.  
R.O.W.

PCL = 882.54

STA. 324+50  
 EARTH EXCAVATION = 9.2  
 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
 TOPSOIL STRIPPING = 104.0  
 UNDERCUT = 0  
 EMBANKMENT REQUIRED = 19.2

48" STORM SEWER

MALIGUS ROAD

F.A.U. SECTION		CONTRACT NO. 83919	
3869	06-00030-00-PP	MCHENRY	TOTAL SHEETS 56
STA. 324+50		TO STA. 327+00	
FED. ROAD DIST. NO. 7 ILLINOIS		FED. AID PROJECT M-8003(641)	

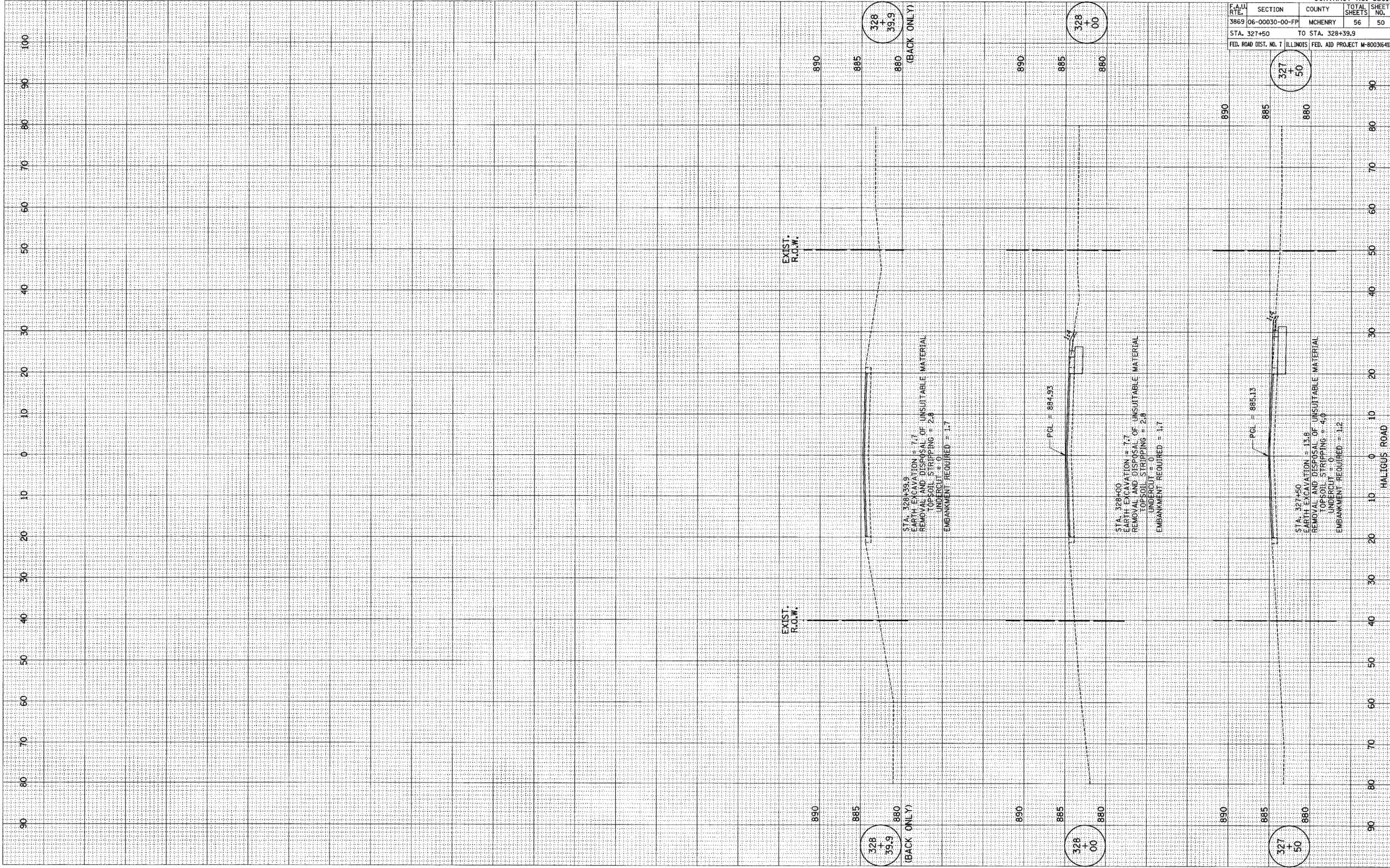
324 + 50	890	885	880	875
----------------	-----	-----	-----	-----

PLOT DATE = #DATE\*  
 PLOT SCALE = #SCALE\*  
 REFERENCE = #REF\*

ORIGINAL SURVEY  
 SURVEYED  
 PLOTTED  
 TEMPLATE  
 NOTE BOOK  
 NO. AREAS CHECKED

FINAL SURVEY  
 SURVEYED  
 PLOTTED  
 TEMPLATE  
 NOTE BOOK  
 NO. AREAS CHECKED

BY \_\_\_\_\_  
 DATE \_\_\_\_\_



CONTRACT NO. 83919			
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS
3869	06-00030-00-FP	MCHENRY	56
STA. 327+50		TO STA. 328+39.9	
FED. ROAD DIST. NO. 7 ILLINOIS		FED. AID PROJECT M-8003641	

327  
 +  
 50

328  
 +  
 00

328  
 +  
 39.9

328  
 +  
 39.9  
 (BACK ONLY)

328  
 +  
 00

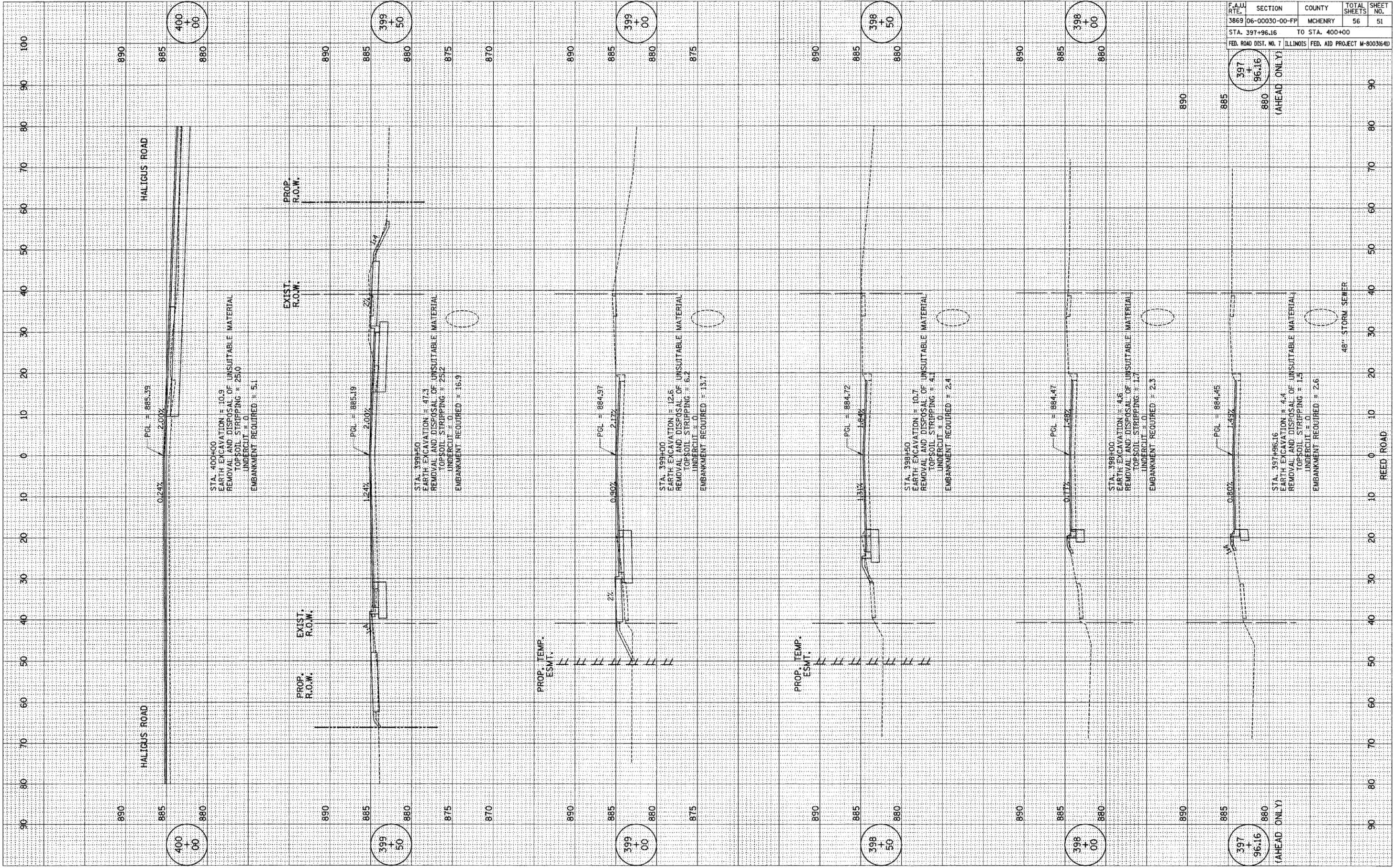
327  
 +  
 50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	106-00030-00-FP	MCHENRY	56	51
STA. 397+96.16		TO STA. 400+00		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT W-800316(4)				

FINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
TEMPLATE		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
NOTE BOOK		
TEMPLATE		
AREAS CHECKED		
NO.		

PLOT DATE = DATE  
 FILE NAME = FILE#  
 PLOT SCALE = SCALE  
 REFERENCE = REF#

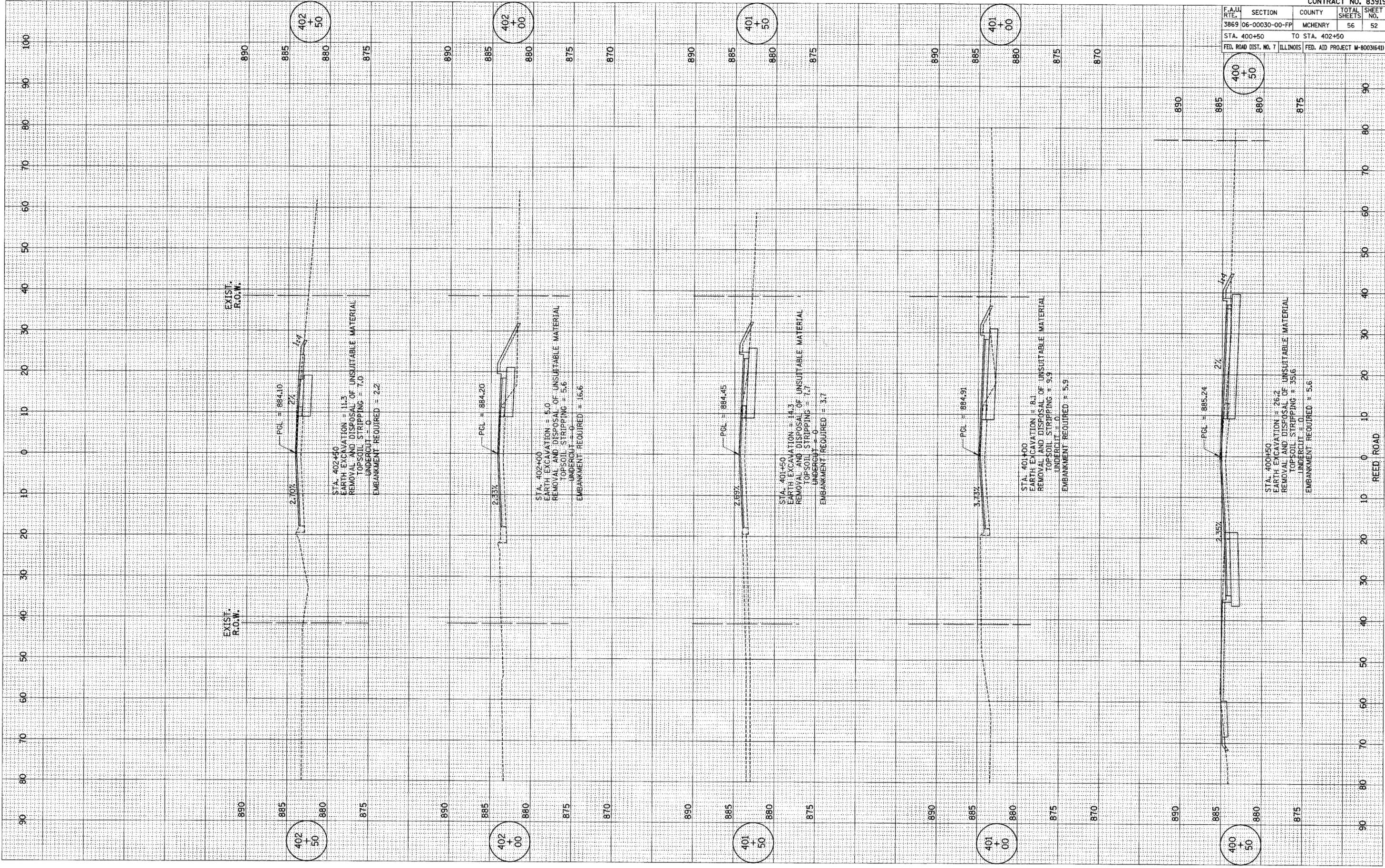


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	52
STA. 400+50 TO STA. 402+50		FED. ROAD DIST. NO. 7 ILLINOIS		
		FED. AID PROJECT M-8003(641)		

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
AREA CHECKED		
NOTE BOOK NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
AREA CHECKED		
NOTE BOOK NO.		

PLOT DATE = #DAYS  
 FILE NAME = #FILE#  
 PLOT SCALE = #SCALE#  
 REFERENCE = #REF#

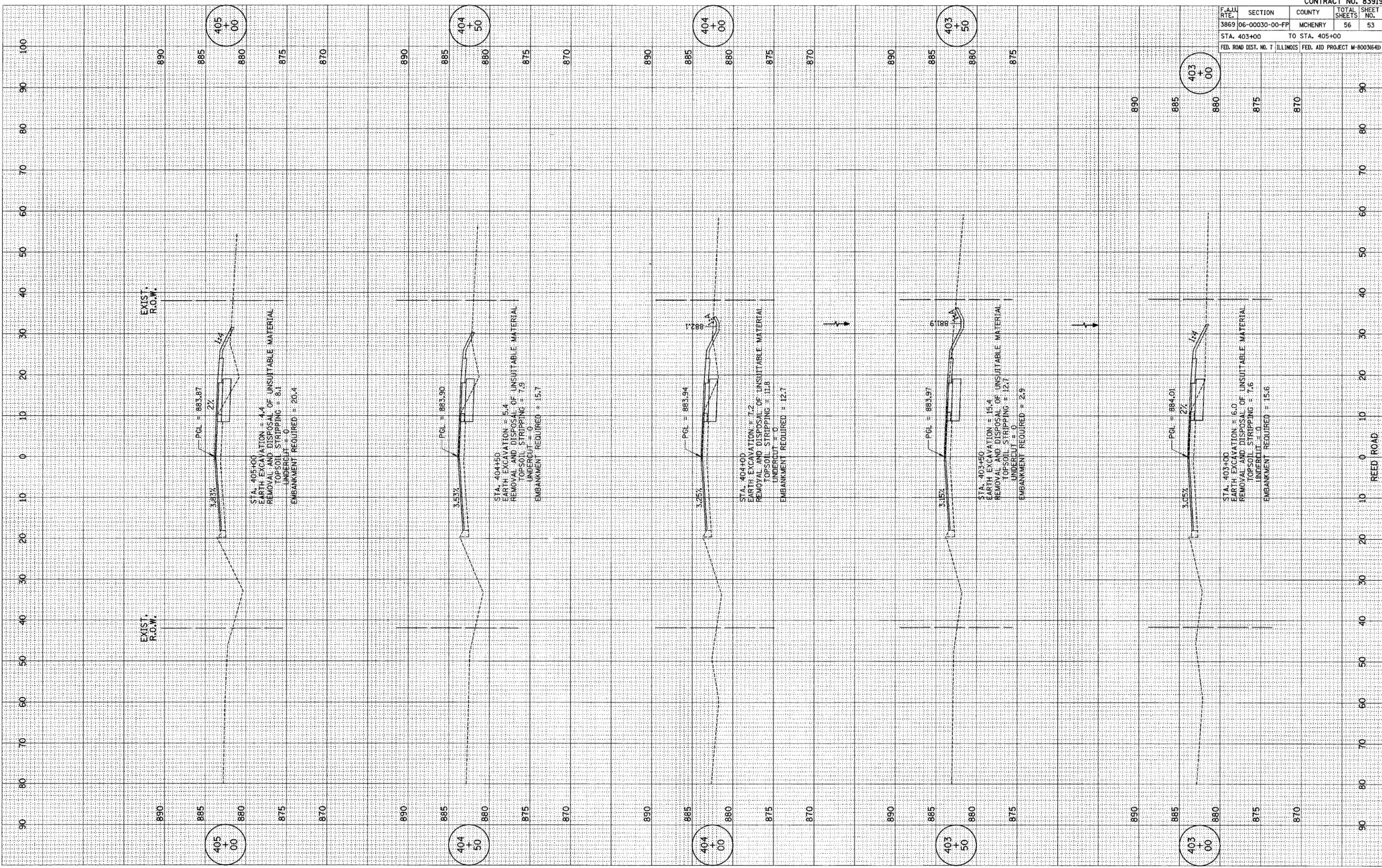


PLOT DATE = \*DATE\*  
 FILE NAME = \*FILE\*  
 PLOT SCALE = \*SCALE\*  
 REFERENCE = \*REF\*

ORIGINAL SURVEY  
 SURVEYED PLOTTED  
 NOTE BOOK  
 AREAS CHECKED

FINAL SURVEY  
 SURVEYED PLOTTED  
 NOTE BOOK  
 AREAS CHECKED

BY \_\_\_\_\_ DATE \_\_\_\_\_  
 BY \_\_\_\_\_ DATE \_\_\_\_\_



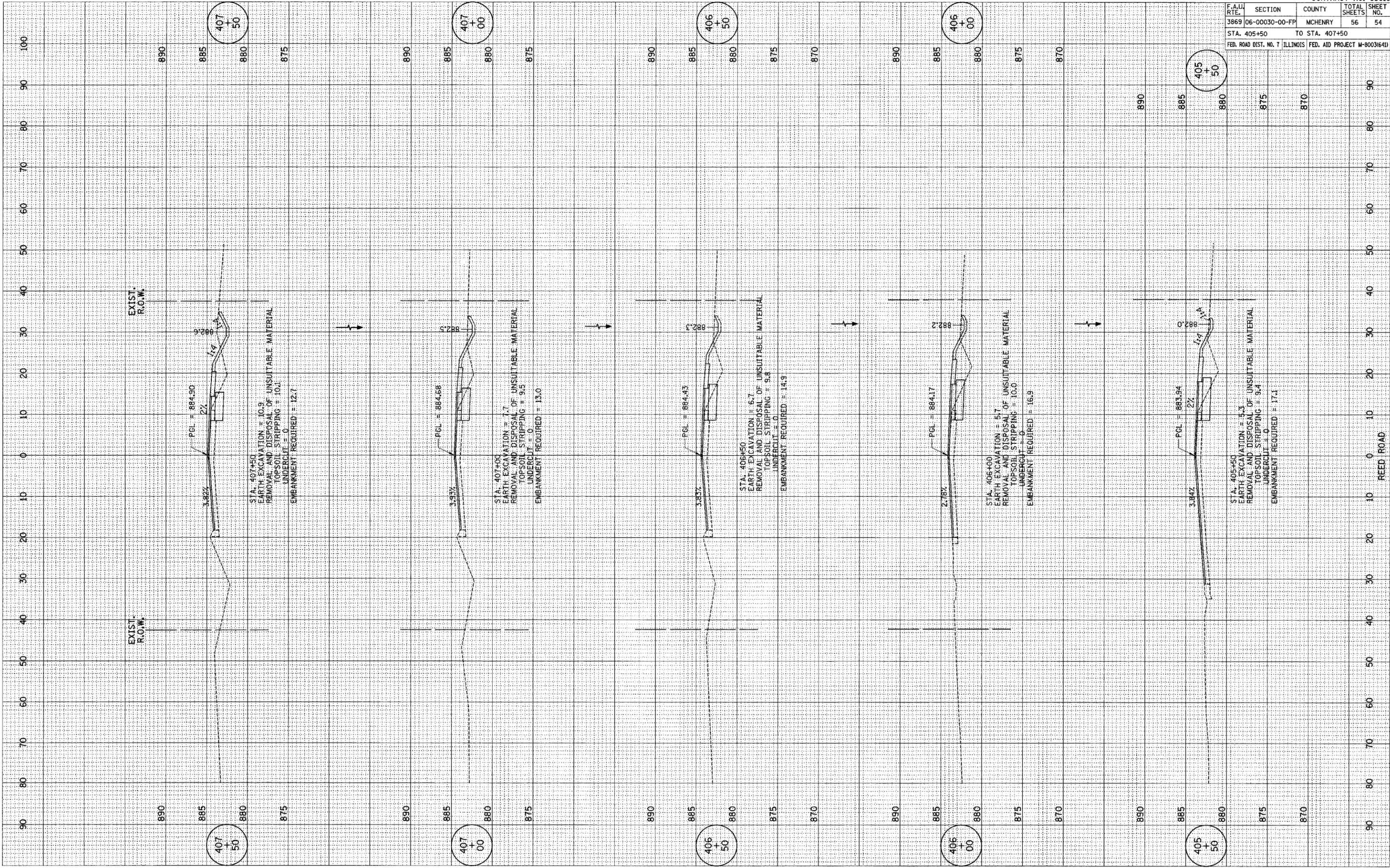
CONTRACT NO. 83919				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	53
STA. 403+00		TO STA. 405+00		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(641)				

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	54
STA. 405+50		TO STA. 407+50		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(641)				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

PLOT DATE = #DATE\*  
 PLOT SCALE = #SCALE\*  
 REFERENCE = #REF\*



EXIST. R.O.W.

EXIST. R.O.W.

PGL = 884.90  
2%  
3.82%

407 + 50

STA. 407+50  
EARTH EXCAVATION = 10.9  
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
TOPSOIL STRIPPING = 10.1  
UNDERCUT = 0  
EMBANKMENT REQUIRED = 12.7

PGL = 884.68  
3.93%

407 + 00

STA. 407+00  
EARTH EXCAVATION = 7.7  
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
TOPSOIL STRIPPING = 9.5  
UNDERCUT = 0  
EMBANKMENT REQUIRED = 13.0

PGL = 884.43  
3.83%

406 + 50

STA. 406+50  
EARTH EXCAVATION = 6.7  
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
TOPSOIL STRIPPING = 9.8  
UNDERCUT = 0  
EMBANKMENT REQUIRED = 14.9

PGL = 884.17  
2.78%

406 + 00

STA. 406+00  
EARTH EXCAVATION = 5.7  
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
TOPSOIL STRIPPING = 10.0  
UNDERCUT = 0  
EMBANKMENT REQUIRED = 16.9

PGL = 883.94  
2%  
3.84%

405 + 50

STA. 405+50  
EARTH EXCAVATION = 5.3  
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL  
TOPSOIL STRIPPING = 9.4  
UNDERCUT = 0  
EMBANKMENT REQUIRED = 17.1

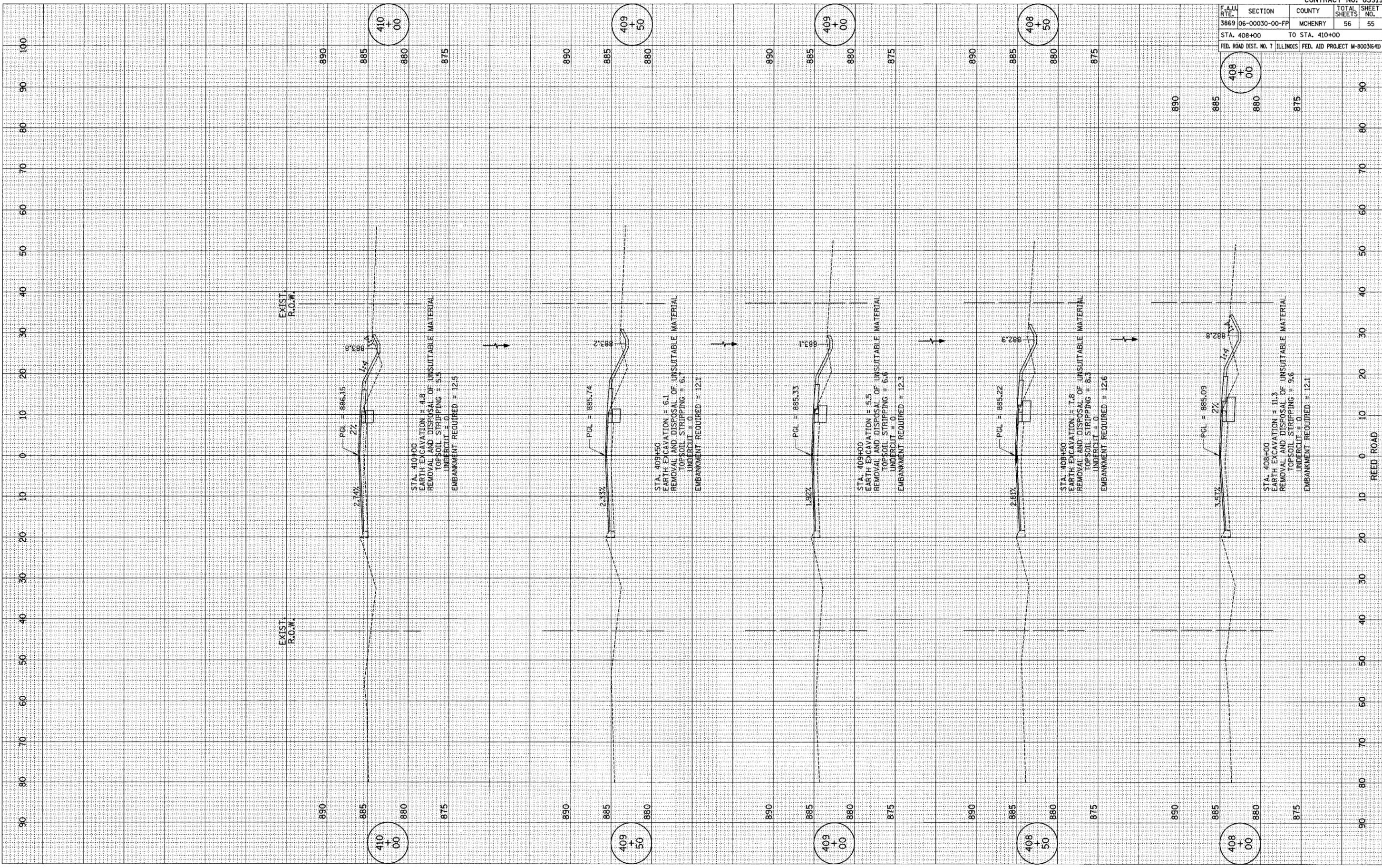
REED ROAD

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3869	06-00030-00-FP	MCHENRY	56	55
STA. 408+00		TO STA. 410+00		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT M-8003(641)				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS	CHECKED		
NO.			

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS	CHECKED		
NO.			

PLOT DATE = #/##/##  
 PLOT NAME = #SCALE#  
 PLOT SCALE = #SCALE#  
 REFERENCE = #REF#



PLOT DATE = #DATE  
 FILE NAME = #FILE  
 PLOT SCALE = #SCALE  
 REFERENCE = #REF

ORIGINAL SURVEY  
 SURVEY PLOTTED  
 NOTE BOOK NO. \_\_\_\_\_  
 AREAS CHECKED \_\_\_\_\_

FINAL SURVEY  
 SURVEY PLOTTED  
 NOTE BOOK NO. \_\_\_\_\_  
 AREAS CHECKED \_\_\_\_\_

BY \_\_\_\_\_ DATE \_\_\_\_\_  
 BY \_\_\_\_\_ DATE \_\_\_\_\_

