

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL HIGHWAY PROJECT

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|-----------|--------------|-----------|
| 95 | • | EFFINGHAM | 409 | 1 |

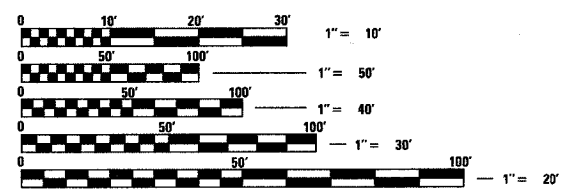
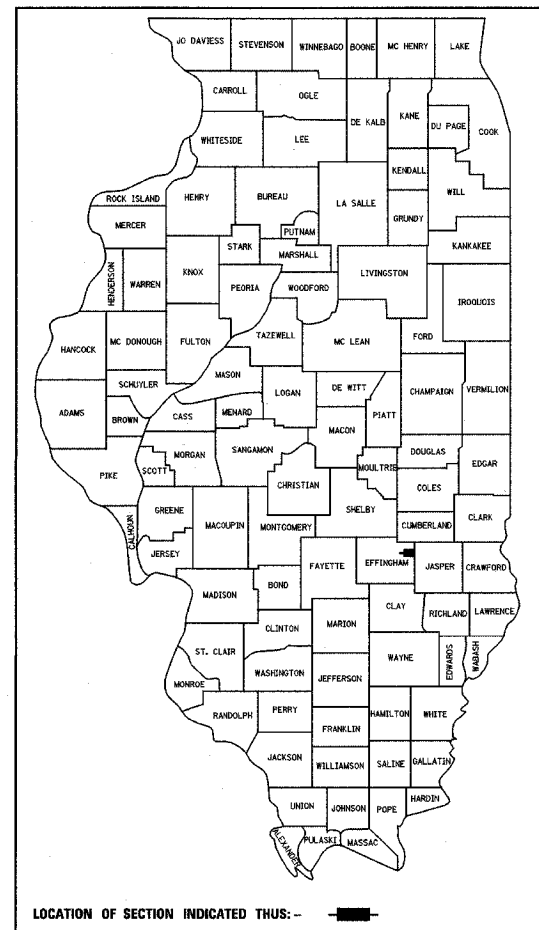
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT
 (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 409 + 21 = 430
 CONTRACT NO. 94356

FOR INDEX OF SHEETS, SEE SHEET NO. 2
 FOR SUMMARY OF QUANTITIES, SEE SHEET NOS. 4-7

WHERE REFERENCE IS MADE TO BITUMINOUS CONCRETE MATERIALS, HOT-MIX-ASPHALT PAY ITEMS SHALL APPLY PER CURRENT STANDARD SPECIFICATIONS.

FAP ROUTE 95 (ILLINOIS ROUTE 33)
 SECTION (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2
 PROJECT: F-BRF-0095 (052)
 EFFINGHAM COUNTY
 C-97-089-01

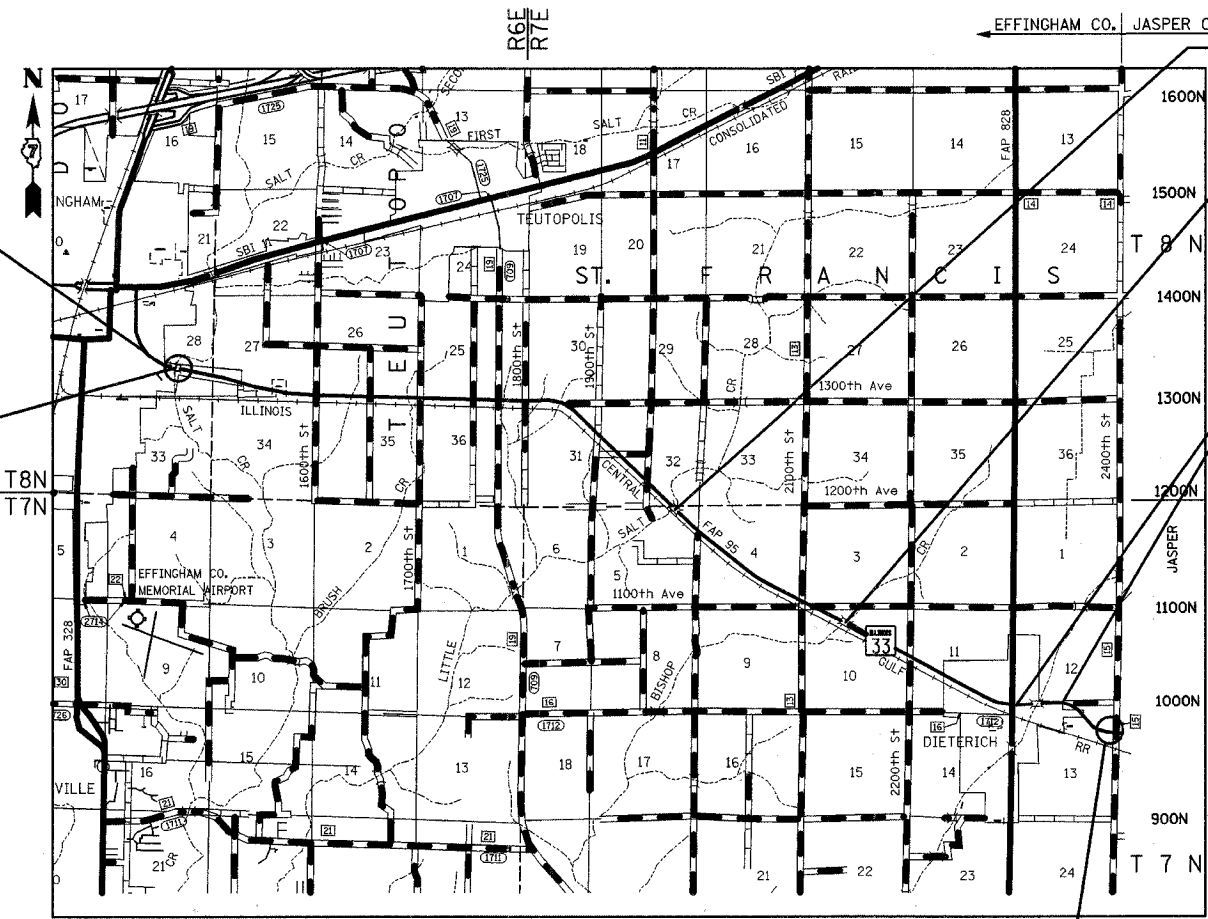
D-97-021-00



FOR UNDERGROUND UTILITY LOCATIONS CALL
 J.U.L.I.E. TOLL FREE
 1-800-892-0123

PROPOSED IMPROVEMENT BEGINS
 STA. 142 + 00.00

BRIDGE SECTION 7B-1
 NEW 3-SPAN STEEL
 STRUCTURAL
 BEGINS STA. 145 + 74.00
 ENDS STA. 147 + 51.00



BRIDGE SECTION 7BR-2
 RESURFACING OF 3-SPAN
 PPC I-BEAM STRUCTURAL
 BEGINS STA. 422 + 94.33
 ENDS STA. 424 + 45.67

BRIDGE SECTION 6B-1
 NEW 1-SPAN PPC I-BEAM
 STRUCTURAL
 BEGINS STA. 531 + 53.00
 ENDS STA. 532 + 07.00

OMISSION
 BEGINS STA. 621 + 24.00
 ENDS STA. 643 + 15.00

- STATION EQUATIONS**
- STA. 209 + 78.77 BK = STA. 209 + 67.66 AH
 - STA. 314 + 45.17 BK = STA. 314 + 13.07 AH
 - STA. 355 + 20.31 BK = STA. 357 + 15.38 AH
 - STA. 412 + 78.37 BK = STA. 412 + 74.63 AH
 - STA. 531 + 41.06 BK = STA. 531 + 33.78 AH
 - STA. 642 + 94.54 BK = STA. 643 + 00.00 AH
 - STA. 687 + 28.06 BK = STA. 687 + 73.01 AH

PROPOSED IMPROVEMENT ENDS
 STA. 689 + 81.63

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

DESIGN DESIGNATION: N.A. - RESURFACING

CONTRACT NO. 94356

TOWNSHIP: DOUGLAS, TEUTOPOLIS, ST. FRANCIS & BISHOP

LOCATION MAP

GROSS LENGTH OF PROJECT = 54,590 FT. = 10.34 MILES
 NET LENGTH OF PROJECT = 52,405 FT. = 9.93 MILES ADT = 6100 (2001)

STATE OF ILLINOIS
 CHARLES E. OUTHRIE, JR.
 062-050644
 PROFESSIONAL ENGINEER
 DATE: 3/12/07
 EXPIRES: 11/30/2007

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED March 26, 2007
Chas. H. Red
 DEPUTY DIRECTOR OF HIGHWAYS, REGIONAL ENGINEER

ENGINEER OF PROJECT DEVELOPMENT AND IMPLEMENTATION
May 11, 2007
Eric E. Harrel
 INTERIM ENGINEER OF DESIGN AND ENVIRONMENT

May 11, 2007
Milton R. Seer, P.E.
 DIRECTOR, DIVISION OF HIGHWAYS

FHWA _____ 20 _____
 DIVISION ADMINISTRATOR

**PRINTED BY AUTHORITY
 OF THE STATE OF ILLINOIS**

PROJECT ENGINEER: BILL STANLEY
 SQUAD LEADER: JENNIFER WENTHE
 TELEPHONE: 217-342-3951 EXT. 361

| | | | | |
|------------------------------------|---------|-----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 2 |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED AFTER SHEET NUMBER: 382

| STANDARD NO | DESCRIPTION |
|-------------|---|
| 000001-04 | STANDARD SYMBOLS, ABBREVIATION, AND PATTERNS |
| 280001-03 | TEMPORARY EROSION CONTROL SYSTEMS |
| 406201 | MAILBOX TURNOUT |
| 420401-05 | BRIDGE APPROACH PAVEMENT |
| 442201-02 | CLASS C AND D PATCHES |
| 482011-02 | BITUMINOUS SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS |
| 515001-02 | NAME PLATE FOR BRIDGES |
| 542301-01 | PRECAST REINFORCED CONCRETE FLARED END SECTION |
| 542306-01 | PRECAST REINFORCED CONCRETE ELLIPTICAL FLARED END SECTION |
| 542401 | METAL END SECTION FOR PIPE CULVERT |
| 542406 | METAL END SECTION FOR PIPE ARCHES |
| 602301-01 | INLET - TYPE A |
| 602401-01 | MANHOLE TYPE A |
| 602601-01 | PRECAST REINFORCED CONCRETE FLAT SLAB TOP |
| 602701-01 | CAST IRON STEPS |
| 604001-02 | FRAME AND LIDS TYPE 1 |
| 604006-02 | FRAME AND GRATE TYPE 3 |
| 606001-03 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 606301-03 | PC CONCRETE ISLANDS AND MEDIANS |
| 606306-02 | CORRUGATED PC CONCRETE MEDIANS |
| 609001-03 | BRIDGE APPROACH SHOULDER PAVEMENT AND DRAIN |
| 609006-03 | BRIDGE APPROACH PAVEMENT (DRAIN DETAIL) |
| 630001-07 | STEEL PLATE BEAM GUARDRAIL |
| 630201-04 | PCC/BITUMINOUS STABILIZATION AT STEEL PLATE BEAM GUARDRAIL |
| 630301-04 | SHOULDER WIDENING FOR TYPE 1, (SPECIAL) GUARDRAIL TERMINALS |
| 631026-03 | TRAFFIC BARRIER TERMINAL, TYPE 5 & 5A |
| 631031-06 | TRAFFIC BARRIER TERMINAL, TYPE 6 |
| 635006-02 | REFLECTOR AND TERMINAL MARKER PLACEMENT |
| 635011-01 | REFLECTOR MARKER AND MOUNTING DETAILS |
| 666001 | RIGHT OF WAY MARKERS |
| 667101 | PERMANENT SURVEY MARKERS |
| 701001-01 | OFF-ROAD OPERATIONS, 2L 2W, 4.5 M (15') MIN. AWAY |
| 701006-02 | OFF-ROAD OPERATIONS, 2L, 2W, 4.5 M (15') TO 600 MM (24") FROM PAVEMENT EDGE |
| 701011-01 | OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY |
| 701201-02 | LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS \geq 45 MPH |
| 701301-02 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701306-01 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS \geq 45 MPH |
| 701311-02 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS DAY ONLY |
| 701316-03 | LANE CLOSURE, 2L, 2W, BRIDGE REPAIR, FOR SPEEDS \geq 45 MPH |
| 701321-08 | LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER |
| 701326-02 | LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS \geq 45 MPH |
| 701501-03 | URBAN LANE CLOSURE, 2L, 2W UNDIVIDED |
| 702001-06 | TRAFFIC CONTROL DEVICES |
| 704001-03 | TEMPORARY CONCRETE BARRIER |
| 780001-01 | TYPICAL PAVEMENT MARKINGS |
| 781001-02 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS |
| BLR-21-6 | TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL LOCAL HIGHWAYS |

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| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|------------|------|---------------------------------------|
| NAME | DATE | |
| | | INDEX OF SHEETS AND HIGHWAY STANDARDS |
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| | | |
| DATE 11/01 | | DRAWN BY MLO CHECKED BY BKB |

GENERAL NOTES

- THIS SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS, THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" INDICATED ON THE CHECK SHEET, AND "THE SPECIAL PROVISIONS" INCLUDED IN THE PROPOSAL.
- THE WORK INCLUDED IN THIS SECTION CONSISTS OF REHABILITATING APPROXIMATELY 9.93 MILES OF FAP ROUTE 95 (IL RTE 33) BY MILLING, WIDENING OF SHOULDERS, REALIGNING DITCHES, RESURFACING, EXTENDING, REMOVING AND REPLACING CULVERTS, AND REMOVAL OF EXISTING GUTTERS.
- THIS PROJECT ALSO INCLUDES TOTAL REPLACEMENT OF THE BRIDGES OVER BIG SALT CREEK AND OVER BISHOP CREEK AND REHABILITATION OF THE BRIDGE OVER LITTLE SALT CREEK.
- THE PROPOSED PROJECT BEGINS WEST OF THE BIG SALT CREEK STRUCTURE AT STA. 142+00.00 AND EXTENDS IN AN EASTERLY DIRECTION A DISTANCE OF 10.34 MILES TO STA. 689+81.63, WITH AN OMISSION FROM STA. 621+24.00 TO STA. 643+15.00.
- PAVEMENT MARKING SHALL BE APPLIED IN ACCORDANCE WITH SECTION 780 OF THE STANDARD SPECIFICATIONS, AS SHOWN ON THE PLANS, AND AS DETERMINED BY THE ENGINEER. SEE SHEET 150A FOR SCHEDULES.

SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE MILLED SURFACE, BITUMINOUS MATERIALS (PRIME COAT), AND HOT-MIX ASPHALT SURFACE COURSE AS SPECIFIED IN SECTION 703 OF THE STANDARD SPECIFICATIONS. TEMPORARY TAPE SHALL BE USED ON THE SURFACE COURSE AND PAINT SHALL BE USED ON MILLED SURFACES.

TOTAL QUANTITY 18,864 FEET

OTHER APPLICATIONS SHALL HAVE THE OPTION OF PAINT OR TAPE.

TEMPORARY PAVEMENT MARKING APPLIED ON THE FINAL SURFACE SHALL BE TAPE. EXCEPT, MARKING PLACED PRIOR TO THE WINTER SHUTDOWN SHALL BE PAINT. QUANTITIES CONSIST OF:

LETTERS & SYMBOLS 296 SQUARE FEET OF WHITE.
 LINE 4" 24,679 FEET OF YELLOW AND 105,278 FEET OF WHITE.
 LINE 6" 117 FEET OF WHITE.
 LINE 12" 738 FEET OF YELLOW AND 40 FEET OF WHITE.
 LINE 24" 44 FEET OF WHITE.

FINAL PAVEMENT MARKINGS SHALL BE PAINT, EXCEPT AT THE BRIDGE DECKS, 1775TH STREET INTERSECTION AND 2100TH STREET INTERSECTION TURN LANES FINAL MARKING SHALL BE EPOXY, AS SHOWN ON THE PAVEMENT MARKING PLANS. QUANTITIES CONSIST OF:

PAINT:

LINE 4" 12,013 FEET OF YELLOW AND 96,092 FEET OF WHITE.

EPOXY:

LETTERS & SYMBOLS 296 SQUARE FEET OF WHITE.
 LINE 4" 12,666 FEET OF YELLOW AND 9,186 FEET OF WHITE.
 LINE 6" 117 FEET OF WHITE.
 LINE 12" 738 FEET OF YELLOW AND 40 FEET OF WHITE.
 LINE 24" 44 FEET OF WHITE.

- RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, THE PLANS, AND AS DETERMINED BY THE ENGINEER. THE FINAL PAVEMENT MARKINGS SHALL BE PLACED PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS. SEE SHEET 150A FOR SCHEDULES. ESTIMATED QUANTITIES:

CRYSTAL (MONODIRECTIONAL) 30
 CRYSTAL (BIDIRECTIONAL) 607
 AMBER (MONODIRECTIONAL) 141
 AMBER (BIDIRECTIONAL) 44
 TOTAL 822

- FOR THE PAY ITEM BITUMINOUS MATERIALS (PRIME COAT), THE CONTRACTOR SHALL USE EITHER RC-70 OR AN EMULSIFIED POLYMER PRIME SS-1HP.
- THE ACTUAL LOCATION AND QUANTITY OF PAVEMENT PATCHING SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- THE FOLLOWING LEVELING BINDER (MACHINE METHOD), N70 MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

MIXTURE USE(S): LEVELING BINDER (3/4")
 AC/PG: 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 70
 MIXTURE COMPOSITION: (GRADATION MIXTURE)
 IL-9.5
 FRICTION AGGREGATE: N/A

- THE FOLLOWING HOT-MIX ASPHALT ASPHALT BINDER COURSE, SUPERPAVE, IL-19.0, N70 MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

MIXTURE USE(S): BINDER COURSE (VAR. DEPTH)
 AC/PG: 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 70
 MIXTURE COMPOSITION: (GRADATION MIXTURE)
 IL-19.0
 FRICTION AGGREGATE: N/A

- THE FOLLOWING HOT-MIX ASPHALT ASPHALT SURFACE COURSE, MIX D, N70 MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

MIXTURE USE(S): MAINLINE SURFACE COURSE (1 1/2")
 AC/PG: 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 70
 MIXTURE COMPOSITION: (GRADATION MIXTURE)
 IL-9.5
 FRICTION AGGREGATE: MIXTURE D

- THE FOLLOWING HOT-MIX ASPHALT ASPHALT SHOULDERS, 8" MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

MIXTURE USE(S): BITUMINOUS SHOULDER
 AC/PG: 58-22
 DESIGN AIR VOIDS: 2.0% @ NDESIGN = 30
 MIXTURE COMPOSITION: (GRADATION MIXTURE)
 N/A
 FRICTION AGGREGATE: N/A

- THE FOLLOWING HOT-MIX ASPHALT ASPHALT BASE COURSE, 8" MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

MIXTURE USE(S): HOT-MIX ASPHALT BASE COURSE, (SIDEROAD RETURN)
 AC/PG: 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 30
 MIXTURE COMPOSITION: (GRADATION MIXTURE)
 IL-19.0L
 FRICTION AGGREGATE: N/A

- THE FOLLOWING HOT-MIX ASPHALT ASPHALT SURFACE COURSE, MIX "C", N50 MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

MIXTURE USE(S): INCIDENTAL HOT-MIX ASPHALT SURFACING (DRIVEWAYS) AND SIDEROAD SURFACE COURSE (2")
 AC/PG: 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 50
 MIXTURE COMPOSITION: (GRADATION MIXTURE)
 IL-9.5
 FRICTION AGGREGATE: MIXTURE C

- THE FOLLOWING HOT-MIX ASPHALT ASPHALT SURFACE COURSE, MIX "C", N70 MIXTURE REQUIREMENTS ARE APPLICABLE TO THIS PROJECT:

MIXTURE USE(S): HOT-MIX ASPHALT ASPHALT SURFACE COURSE, MIX "C", N70
 AC/PG: 64-22
 DESIGN AIR VOIDS: 4.0% @ NDESIGN = 70
 MIXTURE COMPOSITION: (GRADATION MIXTURE)
 IL-9.5
 FRICTION AGGREGATE: MIXTURE C

- BASE COURSE WIDENING SHALL INCLUDE ALL EXCAVATION FOR CONSTRUCTION OF THE WIDENING. BASE COURSE WIDENING SHALL, AT THE CONTRACTOR'S OPTION, BE CONSTRUCTED OF EITHER PORTLAND CEMENT CONCRETE 8" THICK, OR BITUMINOUS CONCRETE 10" THICK.

- POINTS OF GRADE FOR STORM SEWER CASTINGS ARE AS FOLLOWS:

TYPE 1 - TOP OF CASTING
 TYPE 3 - EDGE OF PAVEMENT
 TYPE 8 - TOP OF HORIZONTAL RIM OF CASTINGS

- ANY SECTION CORNERS DISTURBED DURING CONSTRUCTION SHALL BE REESTABLISHED BY A REGISTERED LAND SURVEYOR AT THE CONTRACTOR'S EXPENSE. A LIST OF SECTION CORNERS WITHIN THE PROJECT LIMITS IS CONTAINED IN THE SCHEDULE OF QUANTITIES.

- PORTIONS OF EXISTING CULVERTS MAY BE REMOVED TO FACILITATE THEIR EXTENSION. THE LOCATIONS AND REMOVAL AREAS SHALL BE DETERMINED BY THE ENGINEER. THE COST OF THE REMOVAL SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE OF THE PROPOSED CULVERTS INVOLVED AND NO ADDITIONAL COMPENSATION SHALL BE PROVIDED.

- ALL ELEVATIONS SHOWN IN PLANS ARE BASED ON U.S.G.S. DATUM.

- ALL DISTURBED AREAS WITHIN THE CONSTRUCTION LIMITS SHALL BE FERTILIZED AND SEED. SEEDING SHALL BE CLASS 2A IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND THE APPLICABLE ARTICLES OF SECTION 250 OF THE STANDARD SPECIFICATIONS. THE FOLLOWING APPLICATION RATE SHALL BE USED FOR THE VARIOUS ITEMS NECESSARY FOR SEEDING.

NITROGEN FERTILIZER NUTRIENTS - 90 LB/ACRE
 PHOSPHORUS FERTILIZER NUTRIENTS - 90 LB/ACRE
 POTASSIUM FERTILIZER NUTRIENTS - 90 LB/ACRE
 AGRICULTURAL GROUND LIMESTONE - 2 TONS/ACRE

- THE LOCATIONS AND/OR DEPTHS OF UNDERGROUND UTILITIES SHOWN HAVE BEEN TAKEN FROM INFORMATION FURNISHED BY THE UTILITY OWNERS AND MUST BE CONSIDERED APPROXIMATE. FIELD MARKINGS OF FACILITIES IN CRITICAL AREAS MAY BE OBTAINED BY PROVIDING A MINIMUM OF 96 HOURS ADVANCE NOTICE THROUGH THE J.U.L.I.E. SYSTEM BY CALLING 800-892-0123

- RIGHT-OF-WAY MARKERS SHALL BE ERECTED WITH THE BACK FACE OF THE MARKER ON THE RIGHT-OF-WAY LINE UNLESS THE NEW RIGHT-OF-WAY LINE HAS BEEN SURVEYED AND PINNED, IN WHICH INSTANCE THE RIGHT-OF-WAY MARKER WILL BE ERECTED 12 INCHES INSIDE THE NEW RIGHT-OF-WAY LINE.

- THE CONTRACTOR SHALL HAVE THE RESPONSIBILITY TO MAINTAIN DRAINAGE DURING STAGE CONSTRUCTION OF THE BOX AND PIPE CULVERTS.

- THE TEMPORARY CONCRETE BARRIERS SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 704 OF THE STANDARD SPECIFICATIONS AND AS FOLLOWS:

TEMPORARY CONCRETE BARRIERS SHALL BE FURNISHED BY THE CONTRACTOR AND SHALL BE THE F-SHAPED CONCRETE BARRIER PER STANDARD 704001. THE TEMPORARY CONCRETE BARRIER SHALL REMAIN THE PROPERTY OF THE CONTRACTOR AT THE CONCLUSION OF THE JOB.

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR RELOCATING SIGNS OR DELIVERING EXISTING SIGNS TO THE IDOT DISTRICT 7 SIGN SHOP AS DIRECTED BY THE ENGINEER. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL ITEMS. ALL NEW SIGNAGE WILL BE FURNISHED AND INSTALLED BY DISTRICT 7.

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|-----------------------|---------|-----------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 3 |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT | |

(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2
 CONTRACT NO. 94356

- ALL MAINLINE AND PUBLIC-ACCESS ROAD CULVERTS SHALL BE BACKFILLED WITH TRENCH BACKFILL.
- ALL MAILBOXES SHALL BE RELOCATED IN ACCORDANCE WITH IDOT STANDARD 406201. COST OF RELOCATION SHALL BE INCLUDED IN THE COST OF THE PROJECT.
- ALL DRIVEWAYS AND PUBLIC ROADS BEING RECONSTRUCTED SHALL BE COMPLETED IN STAGES TO ALLOW ACCESS AT ALL TIMES, UNLESS OTHERWISE NOTED.
- THE MATERIAL USED FOR SUB-BASE GRANULAR MATERIAL, TYPE B; SUB-BASE GRANULAR MATERIAL, TYPE C; AGGREGATE BASE COURSE, TYPE B; AGGREGATE SURFACE COURSE, TYPE B; AND AGGREGATE SHOULDERS, TYPE B SHALL HAVE A MINIMUM ILLINOIS BEARING RATIO OF 80.
- ANY AREAS OF NEW WIDENING OVER EXISTING DITCHES WHERE SHOWN ON THE PLANS TO BE UNDERCUT BEYOND TOP SOIL REMOVAL SHALL BE PAID FOR AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL. THE MATERIAL OBTAINED FROM THE UNDERCUT AND FROM CHANNEL EXCAVATION OPERATIONS SHALL BE REMOVED FROM THE PROJECT SITE.
- EXISTING REMOVED RIPRAP MAY BE USED IN EMBANKMENT IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. THIS WORK IS NOT PAID FOR SEPARATELY, BUT INCLUDED IN THE COST OF EARTHWORK.
- THE TREES LISTED IN THE TREE SCHEDULE SHALL BE APPROVED AND HAND PLANTED AT LOCATIONS AS DIRECTED BY THE ROADSIDE MAINTENANCE TECHNICIAN, TOM WILSON, (217)-342-8270. THE CONTRACTOR SHALL BE REQUIRED TO GIVE TWO WEEKS NOTICE TO SCHEDULE A TIME FOR THE LOCATIONS TO BE STAKED AND ON THE SAME DAY THE TREES SHALL BE DELIVERED TO THE JOBSITE FOR ACCEPTANCE OF THE PLANTING MATERIAL BY THE ROADSIDE MAINTENANCE TECHNICIAN.
- IN ORDER TO AVOID ADVERSE EFFECTS BY THE PROJECT ON INDIANA BATS, TREE FELLING WILL BE RESTRICTED TO THE DATES BETWEEN SEPTEMBER 1 AND APRIL 15 OF ANY YEAR, WHEN THE BATS ARE NOT BREEDING (I.E. NOT OCCUPYING NURSERY TREES).
- THE CONTRACTOR SHALL PROVIDE INTERNET ACCESSIBILITY TO THE BITUMINOUS PLANT'S QUALITY CONTROL LAB SO THAT BITUMINOUS REPORTS CAN BE E-MAILED TO THE DISTRICT HEADQUARTERS. THIS WORK SHALL BE INCLUDED IN THE COST OF ALL BITUMINOUS ITEMS.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---------------------------------------|
| NAME | DATE | |
| | | GENERAL NOTES AND COMMITMENTS |
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| | | |

DATE 11/01
 REVISED DATE 03/26/07

DRAWN BY MLO
 CHECKED BY BKB

Revised 5/2/07

F BRF

| PAY CODE NUMBER | ITEM DESCRIPTION | UNIT | TOTAL | CONSTRUCTION TYPE CODE | | | | | |
|-----------------|---|--------|-------|------------------------|--------|---------|-------|---------|-------|
| | | | | 1000-2A | | X071-2A | | X081-2A | |
| | | | | URBAN | RURAL | URBAN | RURAL | URBAN | RURAL |
| 2010010 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 1933 | 578 | 1355 | | | | |
| 2010020 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 2446 | III | 1335 | | | | |
| 2020010 | EARTH EXCAVATION | CU YD | 8160 | 13095 | 68515 | | | | |
| 2020120 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 2185 | | 2185 | | | | |
| 2030010 | CHANNEL EXCAVATION | CU YD | 1425 | | 1425 | | | | |
| 20700220 | POROUS GRANULAR EMBANKMENT | CU YD | 566 | | | 154 | 412 | | |
| 20800150 | TRENCH BACKFILL | CU YD | 345 | 18 | 327 | | | | |
| * 2101505 | TOPSOIL EXCAVATION AND PLACEMENT | CU YD | 31000 | 3115 | 27885 | | | | |
| 21400100 | GRADING AND SHAPING DITCHES | FOOT | 266 | | 266 | | | | |
| * 25000210 | SEEDING, CLASS 2A | ACRE | 47.25 | 5.0 | 42.25 | | | | |
| * 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 4253 | 450 | 3803 | | | | |
| * 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 4253 | 450 | 3803 | | | | |
| * 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 4253 | 450 | 3803 | | | | |
| * 25000700 | AGRICULTURAL GROUND LIMESTONE | TON | 94.5 | 10.0 | 84.5 | | | | |
| * 25100115 | MULCH, METHOD 2 | ACRE | 47.25 | 5.0 | 42.25 | | | | |
| * 25100630 | EROSION CONTROL BLANKET | SQ YD | 1335 | 470 | 865 | | | | |
| * 28000200 | EARTH EXCAVATION FOR EROSION CONTROL | CU YD | 120 | | 120 | | | | |
| * 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 4725 | 500 | 4225 | | | | |
| * 28000300 | TEMPORARY DITCH CHECKS | EACH | 100 | 46 | 54 | | | | |
| * 28000400 | PERIMETER EROSION BARRIER | FOOT | 110 | 710 | 400 | | | | |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 58 | 11 | 47 | | | | |
| 28100107 | STONE RIPRAP, CLASS A4 | SQ YD | 2937 | 896 | 580 | 1032 | 429 | | |
| 28200200 | FILTER FABRIC | SQ YD | 3581 | 1299 | 821 | 1032 | 429 | | |
| 28400100 | GABIONS | CU YD | 325 | 202 | 123 | | | | |
| 3101200 | SUB-BASE GRANULAR MATERIAL, TYPE B 4" | SQ YD | 8257 | | 8257 | | | | |
| 3101900 | SUB-BASE GRANULAR MATERIAL, TYPE C | TON | 316 | 114 | 202 | | | | |
| 35101400 | AGGREGATE BASE COURSE, TYPE B | TON | 882 | 292 | 590 | | | | |
| 35102000 | AGGREGATE BASE COURSE, TYPE B 8" | SO YD | 2838 | | 2838 | | | | |
| 35501316 | HOT-MIX ASPHALT BASE COURSE, 8" | SQ YD | 10772 | 754 | 10,018 | | | | |
| 35650700 | BASE COURSE WIDENING | SO YD | 1615 | 519 | 1096 | | | | |
| 40200800 | AGGREGATE SURFACE COURSE, TYPE B | TON | 939 | 262 | 677 | | | | |
| 40300100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 1430 | | 1430 | | | | |
| 40300300 | BITUMINOUS MATERIALS (COVER AND SEAL COATS) | GALLON | 3889 | | 3889 | | | | |
| 40300500 | COVER COAT AGGREGATE | TON | 71 | | 71 | | | | |
| 40300600 | SEAL COAT AGGREGATE | TON | 34 | | 34 | | | | |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 37106 | 2627 | 34479 | | | | |

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|---|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 4 |
| STA. TO STA. | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7B-2 | | | | |
| CONTRACT NO. 94356 | | | | |

| PAY CODE NUMBER | ITEM DESCRIPTION | UNIT | TOTAL | CONSTRUCTION TYPE CODE | | | | | |
|---------------------|--|------------------|----------------|------------------------|----------------|---------|-------|---------|-------|
| | | | | 1000-2A | | X071-2A | | X081-2A | |
| | | | | URBAN | RURAL | URBAN | RURAL | URBAN | RURAL |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 687 | 51 | 636 | | | | |
| 40600635 | LEVELING BINDER (MACHINE METHOD), N70 | TON | 6556 | 431 | 6125 | | | | |
| 40600895 | CONSTRUCTING TEST STRIP | EACH | 1 | | 1 | | | | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SO YD | 513 | 87 | 426 | | | | |
| 40600990 | TEMPORARY RAMP | SO YD | 585 | 91 | 494 | | | | |
| 40603085 | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | TON | 8019 | 1213 | 6806 | | | | |
| 40603310 | HOT-MIX ASPHALT SURFACE COURSE, MIX 'C', N50 | TON | 1113 | 74 | 1039 | | | | |
| 40603315 | HOT-MIX ASPHALT SURFACE COURSE, MIX 'C', N70 | TON | 80 | | 80 | | | | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N70 | TON | 14396 | 923 | 13473 | | | | |
| 40800050 | INCIDENTAL HOT-MIX ASPHALT SURFACING | TON | 505 | 156 | 349 | | | | |
| 42000300 | PORTLAND CEMENT CONCRETE PAVEMENT 8" | SO YD | 999 | 178 | 821 | | | | |
| 4200165 | BRIDGE APPROACH PAVEMENT | SO YD | 548 | 274 | 274 | | | | |
| 42001200 | PAVEMENT FABRIC | SO YD | 999 | 178 | 821 | | | | |
| 42001430 | BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) | SO YD | 108 | 54 | 54 | | | | |
| 42001500 | P.C. CONCRETE BRIDGE APPROACH SHOULDER PAVEMENT | SO YD | 160 | | 160 | | | | |
| 42300200 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH | SO YD | 408 | 204 | 204 | | | | |
| 42300400 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH | SO YD | 855 | | 855 | | | | |
| 44000100 | PAVEMENT REMOVAL | SO YD | 2841 | 599 | 2242 | | | | |
| 44000157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2" | SO YD | 1136 | | 1136 | | | | |
| 44000198 | HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | SO YD | 151041 | 9084 | 141957 | | | | |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SO YD | 1692 | 104 | 1588 | | | | |
| 44000400 | GUTTER REMOVAL | FOOT | 4071 | 2070 | 2001 | | | | |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 758 | 758 | | | | | |
| 44000910 | BITUMINOUS CONCRETE REMOVAL (DECK) | SO YD | 654 | | 654 | | | | |
| 44000915 | HOT-MIX ASPHALT SURFACE REMOVAL (DECK) | SO YD | 654 | | 654 | | | | |
| 44001700 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 135 | 108 | 27 | | | | |
| 44002600 | GUTTER OUTLET REMOVAL | FOOT | 165 | 93 | 72 | | | | |
| 44002805 | ISLAND REMOVAL | SO FT | 115 | | 115 | | | | |
| 44004250 | PAVED SHOULDER REMOVAL | SO YD | 1615 | | 1615 | | | | |
| 44201803 | CLASS D PATCHES, TYPE II, 13 INCH | SO YD | 5426 | | 5426 | | | | |

URBAN SECTION STA. 142+00.00 TO STA. 180+60.00
 RURAL SECTION STA. 180+60.00 TO STA. 689+81.63
 * DENOTES SPECIALTY ITEM

| REVISIONS | NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUMMARY OF QUANTITIES
 SHEET 1 OF 4
 DATE 6/02
 DRAWN BY ASB
 CHECKED BY SJK

| PAY CODE NUMBER | ITEM DESCRIPTION | UNIT | TOTAL | F | | BRF | | F | |
|-----------------|--|-------|--------|---|-------|---------|---------|---------|---------|
| | | | | 80% FEDERAL 20% STATE CONSTRUCTION TYPE | | CODE | | CODE | |
| | | | | URBAN | RURAL | X071-2A | X081-2A | X081-2A | X081-2A |
| 44201807 | CLASS D PATCHES, TYPE III, 13 INCH | SQ YD | 784 | | 784 | | | | |
| 44201809 | CLASS D PATCHES, TYPE IV, 13 INCH | SQ YD | 1366 | | 1366 | | | | |
| 48101200 | AGGREGATE SHOULDERS, TYPE B | TON | 313 | | 313 | | | | |
| 48101500 | AGGREGATE SHOULDERS, TYPE B 6" | SQ YD | 43157 | 3179 | 39978 | | | | |
| 48203023 | HOT-MIX ASPHALT SHOULDERS, 6 1/2" | SQ YD | 368 | 368 | | | | | |
| 48203029 | HOT-MIX ASPHALT SHOULDERS, 8" | SQ YD | 15067 | 1423 | 13644 | | | | |
| 50100300 | REMOVAL OF EXISTING STRUCTURES NO.1 | EACH | 1 | | 1 | | | | |
| 50100400 | REMOVAL OF EXISTING STRUCTURES NO. 2 | EACH | 1 | | 1 | | | | |
| 50102400 | CONCRETE REMOVAL | CU YD | 31.3 | | | | | | 31.3 |
| 50104400 | CONCRETE HEADWALL REMOVAL | EACH | 1 | | 1 | | | | |
| 50105200 | REMOVE EXISTING CULVERTS | EACH | 61 | 4 | 47 | | | | |
| 50200100 | STRUCTURE EXCAVATION | CU YD | 428 | | 348 | 80 | | | |
| 50300100 | FLOOR DRAINS | EACH | 6 | | 6 | | | | |
| 50300225 | CONCRETE STRUCTURES | CU YD | 309.9 | 147.4 | 125.6 | 36.9 | | | |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 376.7 | | 249.9 | 95.6 | | | 31.2 |
| 50300260 | BRIDGE DECK GROOVING | SQ YD | 1599 | | 748 | 228 | | | 623 |
| 50400805 | FURNISHING AND ERECTING PRECAST PRESTRESSED CONCRETE I-BEAMS, 36 IN. | FOOT | 318 | | 318 | | | | |
| 50500105 | FURNISHING AND ERECTING STRUCTURAL STEEL | L SUM | 1 | | 1 | | | | |
| 50500505 | STUD SHEAR CONNECTORS | EACH | 3384 | | 3384 | | | | |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 145638 | 207 | 46481 | 72400 | 22,230 | | 4320 |
| 50800515 | BAR SPLICERS | EACH | 1078 | | 760 | 294 | | | 24 |
| 51201400 | FURNISHING STEEL PILES HPI0X42 | FOOT | 540 | | 540 | | | | |
| 51201500 | FURNISHING STEEL PILES HPI0X57 | FOOT | 416 | | 416 | | | | |
| 51201700 | FURNISHING STEEL PILES HPI2X74 | FOOT | 248 | | 248 | | | | |
| 51202305 | DRIVING PILES | FOOT | 956 | | 416 | 540 | | | |
| 51203400 | TEST PILE STEEL HPI0X42 | EACH | 1 | | 1 | | | | |
| 51203500 | TEST PILE STEEL HPI0X57 | EACH | 2 | | 2 | | | | |
| 51205200 | TEMPORARY SHEET PILING | SQ FT | 954 | | 954 | | | | |
| 51500100 | NAME PLATES | EACH | 2 | | 1 | 1 | | | |
| 52100520 | ANCHOR BOLTS, 1" | EACH | 48 | | 48 | | | | |
| 54001002 | BOX CULVERT END SECTION, CULVERT NO.2 | EACH | 1 | 1 | | | | | |
| 54001003 | BOX CULVERT END SECTION, CULVERT NO.3 | EACH | 1 | 1 | | | | | |
| 54001004 | BOX CULVERT END SECTION, CULVERT NO.4 | EACH | 2 | 2 | | | | | |
| 54001005 | BOX CULVERT END SECTION, CULVERT NO.5 | EACH | 2 | 2 | | | | | |
| 54001006 | BOX CULVERT END SECTION, CULVERT NO.6 | EACH | 2 | 2 | | | | | |
| 54001007 | BOX CULVERT END SECTION, CULVERT NO.7 | EACH | 2 | 2 | | | | | |

F.A.P. RTE. 95 SECTION * COUNTY EFFINGHAM TOTAL SHEETS 409 SHEET NO. 5
 STA. TO STA.
 FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT
 * (6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2
 CONTRACT NO. 94356
 F BRF

| PAY CODE NUMBER | ITEM DESCRIPTION | UNIT | TOTAL | 80% FEDERAL 20% STATE CONSTRUCTION TYPE | | CODE | |
|-----------------|--|------|-------|---|-------|---------|---------|
| | | | | CODE | | CODE | |
| | | | | URBAN | RURAL | X071-2A | X081-2A |
| 54001008 | BOX CULVERT END SECTION, CULVERT NO.8 | EACH | 2 | | 2 | | |
| 54001009 | BOX CULVERT END SECTION, CULVERT NO.9 | EACH | 1 | | 1 | | |
| 54001010 | BOX CULVERT END SECTION, CULVERT NO.10 | EACH | 1 | | 1 | | |
| 54001011 | BOX CULVERT END SECTION, CULVERT NO.11 | EACH | 2 | | 2 | | |
| 54001012 | BOX CULVERT END SECTION, CULVERT NO.12 | EACH | 4 | | 4 | | |
| 54001013 | BOX CULVERT END SECTION, CULVERT NO.13 | EACH | 2 | | 2 | | |
| 54001014 | BOX CULVERT END SECTION, CULVERT NO.14 | EACH | 2 | | 2 | | |
| 54001015 | BOX CULVERT END SECTION, CULVERT NO.15 | EACH | 1 | | 1 | | |
| 54001016 | BOX CULVERT END SECTION, CULVERT NO.16 | EACH | 1 | | 1 | | |
| 54002020 | EXPANSION BOLTS 3/4 INCH | EACH | 408 | 48 | 360 | | |
| 54010202 | PRECAST CONCRETE BOX CULVERT 2' X 2' | FOOT | 111 | 62 | 49 | | |
| 54010302 | PRECAST CONCRETE BOX CULVERT 3' X 2' | FOOT | 42 | 42 | | | |
| 54010303 | PRECAST CONCRETE BOX CULVERT 3' X 3' | FOOT | 33 | 33 | | | |
| 54010402 | PRECAST CONCRETE BOX CULVERT 4' X 2' | FOOT | 23 | 23 | | | |
| 54010403 | PRECAST CONCRETE BOX CULVERT 4' X 3' | FOOT | 43 | 43 | | | |
| 54010603 | PRECAST CONCRETE BOX CULVERT 6' X 3' | FOOT | 69 | 69 | | | |
| 54010703 | PRECAST CONCRETE BOX CULVERT 7' X 3' | FOOT | 140 | 140 | | | |
| 54010802 | PRECAST CONCRETE BOX CULVERT 8' X 2' | FOOT | 10 | 10 | | | |
| 54010806 | PRECAST CONCRETE BOX CULVERT 8' X 6' | FOOT | 26 | 26 | | | |
| 542A0220 | PIPE CULVERTS, CLASS A, TYPE 115" | FOOT | 98 | 98 | | | |
| 542A0228 | PIPE CULVERTS, CLASS A, TYPE 118" | FOOT | 5 | 5 | | | |
| 542A0229 | PIPE CULVERTS, CLASS A, TYPE 124" | FOOT | 79 | 2 | 77 | | |
| 542A0235 | PIPE CULVERTS, CLASS A, TYPE 130" | FOOT | 45 | 31 | 14 | | |
| 542A0247 | PIPE CULVERTS, CLASS A, TYPE 142" | FOOT | 13 | 13 | | | |
| 542D0217 | PIPE CULVERTS, CLASS D, TYPE 112" | FOOT | 94 | 94 | | | |
| 542D0220 | PIPE CULVERTS, CLASS D, TYPE 115" | FOOT | 1446 | 403 | 1043 | | |
| 542D0223 | PIPE CULVERTS, CLASS D, TYPE 118" | FOOT | 309 | 309 | | | |
| 542D0226 | PIPE CULVERTS, CLASS D, TYPE 121" | FOOT | 194 | 194 | | | |
| 542D0229 | PIPE CULVERTS, CLASS D, TYPE 124" | FOOT | 432 | 45 | 387 | | |

URBAN SECTION STA. 142+00.00 TO STA. 180+60.00
 RURAL SECTION STA. 180+60.00 TO STA. 689+81.63
 * DENOTES SPECIALTY ITEM

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUMMARY OF QUANTITIES SHEET 2 OF 4
 DATE 6/02
 DRAWN BY ASB
 CHECKED BY SJK

| PAY CODE NUMBER | ITEM DESCRIPTION | UNIT | TOTAL | F | | BRF | |
|-----------------|---|-------|-------|--|---------|---------|----|
| | | | | 80% FEDERAL 20% STATE CONSTRUCTION TYPE CODE | | | |
| | | | | 1000-2A | X071-2A | X081-2A | |
| URBAN | RURAL | URBAN | RURAL | | | | |
| 542D0241 | PIPE CULVERTS, CLASS D, TYPE 136" | FOOT | 24 | | 24 | | |
| 542D0247 | PIPE CULVERTS, CLASS D, TYPE 142" | FOOT | 20 | 20 | | | |
| 542D1060 | PIPE CULVERTS, CLASS D, TYPE 2 15' | FOOT | 326 | 110 | 216 | | |
| 542D5470 | PIPE CULVERTS, CLASS D, TYPE EQUIVALENT ROUND-SIZE 15" | FOOT | 486 | 237 | 249 | | |
| 542D5473 | PIPE CULVERTS, CLASS D, TYPE EQUIVALENT ROUND-SIZE 18" | FOOT | 191 | | 191 | | |
| 542D5479 | PIPE CULVERTS, CLASS D, TYPE EQUIVALENT ROUND-SIZE 24" | FOOT | 348 | | 348 | | |
| 542D5482 | PIPE CULVERTS, CLASS D, TYPE EQUIVALENT ROUND-SIZE 27" | FOOT | 47 | | 47 | | |
| 542D5491 | PIPE CULVERTS, CLASS D, TYPE EQUIVALENT ROUND-SIZE 36" | FOOT | 74 | | 74 | | |
| 542JA018 | PIPE CULVERTS, CLASS A 18" (JACKED) | FOOT | 29 | | 29 | | |
| 542JA024 | PIPE CULVERTS, CLASS A 24" (JACKED) | FOOT | 110 | | 110 | | |
| 542JA036 | PIPE CULVERTS, CLASS A 36" (JACKED) | FOOT | 45 | | 45 | | |
| 54207159 | PIPE CULVERTS, TYPE I, REINFORCED CONCRETE - ELLIPTICAL, EQUIVALENT ROUND-SIZE 24" | FOOT | 189 | | 189 | | |
| 54207165 | PIPE CULVERTS, TYPE I, REINFORCED CONCRETE - ELLIPTICAL, EQUIVALENT ROUND-SIZE 30" | FOOT | 46 | | 46 | | |
| 54213447 | END SECTIONS 12" | EACH | 6 | 2 | 4 | | |
| 54213450 | END SECTIONS 15" | EACH | 68 | 18 | 50 | | |
| 54213453 | END SECTIONS 18" | EACH | 8 | | 8 | | |
| 54213456 | END SECTIONS 21" | EACH | 4 | | 4 | | |
| 54213459 | END SECTIONS 24" | EACH | 18 | 2 | 16 | | |
| 54213471 | END SECTIONS 36" | EACH | 2 | | 2 | | |
| 54213660 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15" | EACH | 8 | 1 | 7 | | |
| 54213663 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18" | EACH | 3 | | 3 | | |
| 54213669 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24" | EACH | 10 | 1 | 9 | | |
| 54213675 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30" | EACH | 3 | 1 | 2 | | |
| 54213681 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36" | EACH | 2 | | 2 | | |
| 54213687 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 42" | EACH | 1 | 1 | | | |
| 54214290 | END SECTIONS, EQUIVALENT ROUND-SIZE 15" | EACH | 24 | 10 | 14 | | |
| 54214293 | END SECTIONS, EQUIVALENT ROUND-SIZE 18" | EACH | 10 | | 10 | | |
| 54214299 | END SECTIONS, EQUIVALENT ROUND-SIZE 24" | EACH | 12 | | 12 | | |
| 54214302 | END SECTIONS, EQUIVALENT ROUND-SIZE 27" | EACH | 2 | | 2 | | |
| 54214311 | END SECTIONS, EQUIVALENT ROUND-SIZE 36" | EACH | 4 | | 4 | | |
| 54214719 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS - ELLIPTICAL, EQUIVALENT ROUND-SIZE 24" | EACH | 8 | | 8 | | |
| 54214725 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS - ELLIPTICAL, EQUIVALENT ROUND-SIZE 30" | EACH | 2 | | 2 | | |
| 54216555 | REINFORCED CONCRETE PIPE TEE, 15" PIPE WITH 15" RISER | EACH | 1 | | 1 | | |
| 550A0360 | STORM SEWERS, CLASS A, TYPE 2 15" | FOOT | 873 | | 873 | | |
| 55100500 | STORM SEWER REMOVAL 12" | FOOT | 80 | | 80 | | |
| 59100100 | GEOCOMPOSITE WALL DRAIN | SQ YD | 164 | | | 84 | 80 |
| 60100945 | PIPE DRAINS 12" | FOOT | 204 | 91 | 113 | | |

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|---|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 6 |
| STA. TO STA. | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |
| F | | BRF | | |

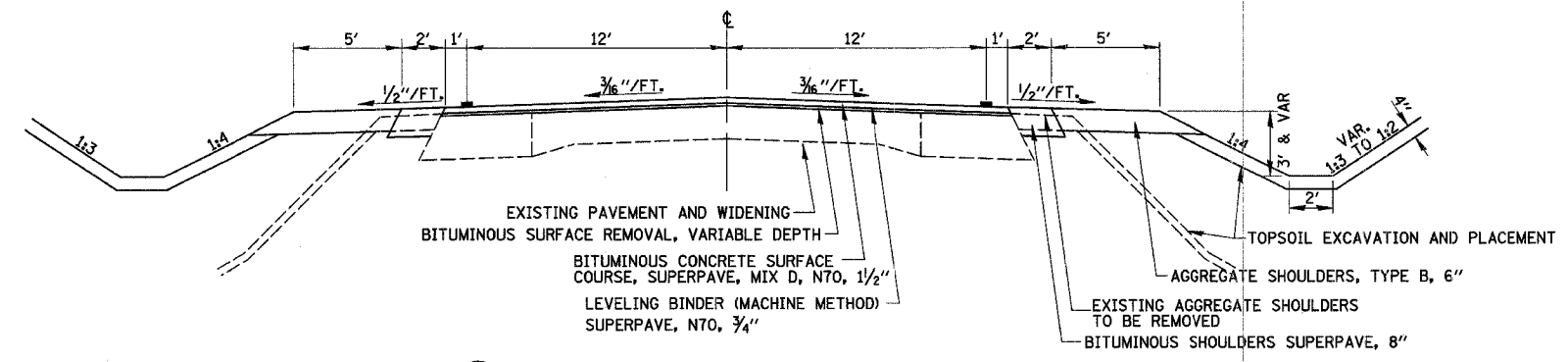
| PAY CODE NUMBER | ITEM DESCRIPTION | UNIT | TOTAL | 80% FEDERAL 20% STATE CONSTRUCTION TYPE CODE | | | |
|-----------------|--|--------|-------|--|---------|---------|-------|
| | | | | 1000-2A | X071-2A | X081-2A | |
| | | | | URBAN | RURAL | URBAN | RURAL |
| 60109580 | PIPE UNDERDRAINS FOR STRUCTURES 4" | FOOT | 190 | | 190 | | |
| 60218300 | MANHOLES, TYPE A, 4'-DIAMETER, TYPE IFRAME, OPEN LID | EACH | 4 | | 4 | | |
| 60223800 | MANHOLES, TYPE A, 6'-DIAMETER, TYPE IFRAME, CLOSED LID | EACH | 2 | 2 | | | |
| 60235700 | INLETS, TYPE A, TYPE 3 FRAME AND GRATE | EACH | 9 | | 9 | | |
| 60235735 | INLETS, TYPE A, TYPE 3 FRAME AND GRATE (42" DEPTH) | EACH | 1 | | 1 | | |
| 60236510 | INLETS, TYPE A, TYPE 8 GRATE (36" DEPTH) | EACH | 1 | 1 | | | |
| 60266600 | VALVE BOXES TO BE ADJUSTED | EACH | 2 | | 2 | | |
| 60267000 | METER VAULTS TO BE ADJUSTED | EACH | 2 | | 2 | | |
| *60500205 | FILLING CATCH BASINS | EACH | 1 | 1 | | | |
| *60601805 | CONCRETE CURB TRANSITION | FOOT | 20 | 20 | | | |
| *60605100 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (ABUTTING EXISTING PAVEMENT) | FOOT | 3231 | 668 | 2563 | | |
| *60623200 | CONCRETE MEDIAN, TYPE SM-6.24 | SQ FT | 315 | | 315 | | |
| 60624600 | CORRUGATED MEDIAN | SQ FT | 425 | | 425 | | |
| 60900315 | TYPE D INLET BOX, STANDARD 609006 | EACH | 2 | 2 | | | |
| 60900330 | TYPE D INLET BOX, STANDARD 609001 | EACH | 4 | | 4 | | |
| 60900515 | CONCRETE THRUST BLOCKS | EACH | 6 | 2 | 4 | | |
| *63000000 | STEEL PLATE BEAM GUARD RAIL, TYPE A | FOOT | 2075 | 587.5 | 1487.5 | | |
| *63100070 | TRAFFIC BARRIER TERMINAL, TYPE 5 | EACH | 4 | | 4 | | |
| *63100085 | TRAFFIC BARRIER TERMINAL, TYPE 6 | EACH | 8 | 4 | 4 | | |
| *63100167 | TRAFFIC BARRIER TERMINAL TYPE I, SPECIAL (TANGENT) | EACH | 12 | 4 | 8 | | |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 3533 | 505 | 3028 | | |
| 63400205 | GUARD POSTS REMOVAL | EACH | 4 | 4 | | | |
| 66600105 | FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS | EACH | 209 | 41 | 168 | | |
| 66700205 | PERMANENT SURVEY MARKERS, TYPE I | EACH | 51 | 5 | 46 | | |
| 66700305 | PERMANENT SURVEY MARKERS, TYPE II | EACH | 7 | | 7 | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 21 | 2 | 19 | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 0.1 | 0.9 | | |
| 67201000 | SEALING ABANDONED WATER WELLS | EACH | 3 | 1 | 2 | | |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 70101 | L SUM | 1 | 0.1 | 0.9 | | |

URBAN SECTION STA. 142+00.00 TO STA. 180+60.00
RURAL SECTION STA. 180+60.00 TO STA. 689+81.63
* DENOTES SPECIALTY ITEM

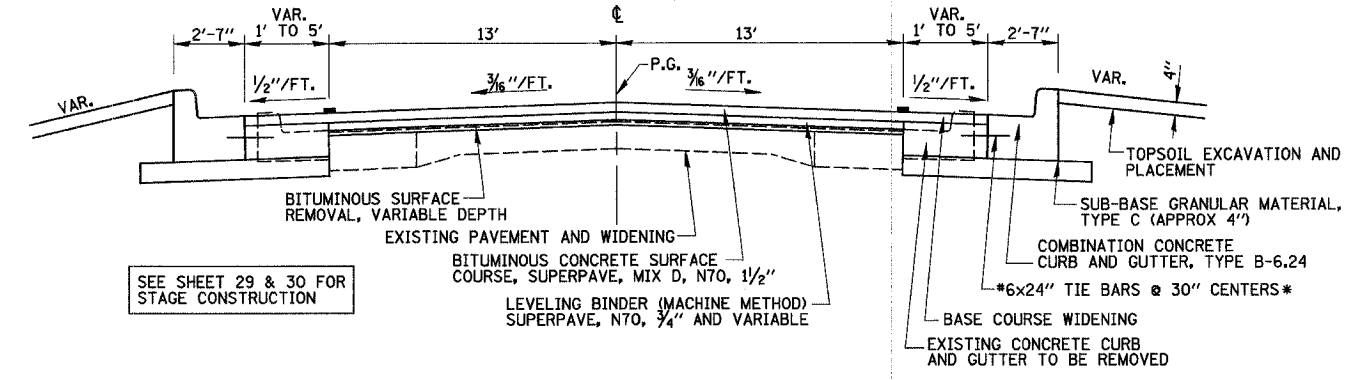
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
SHEET 3 OF 4
DRAWN BY ASB
CHECKED BY SJK
DATE 6/02

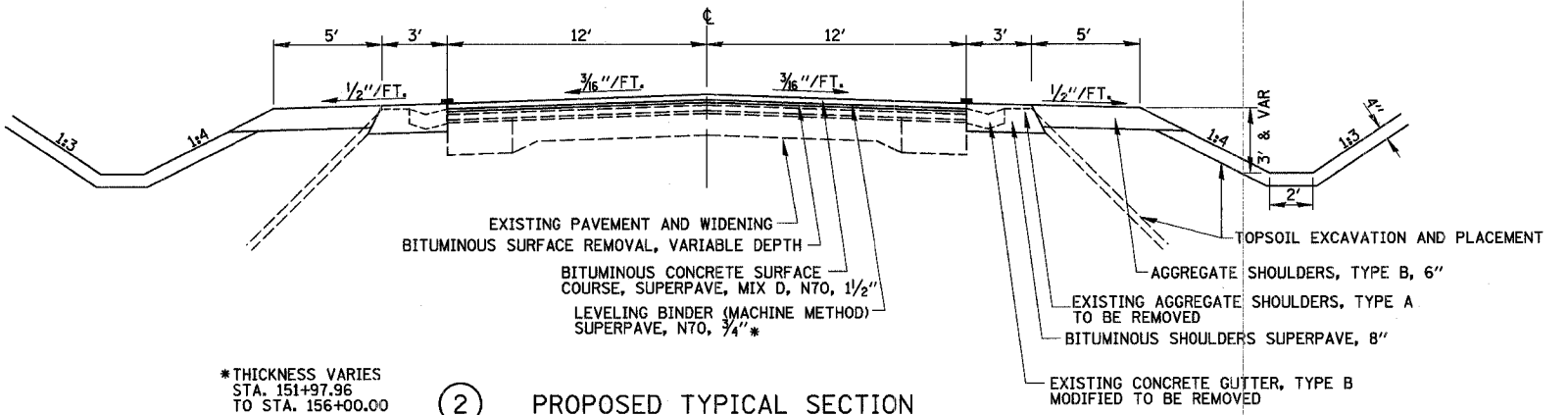
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | EFFINGHAM | 409 | 8 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| CONTRACT NO. 94356 | | | | |



① PROPOSED TYPICAL SECTION
 STA. 160+00.00 TO STA. 187+00.00 LT
 STA. 160+00.00 TO STA. 187+50.00 RT
 STA. 197+20.00 TO STA. 218+50.00
 STA. 223+00.00 TO STA. 244+50.00
 STA. 249+50.00 TO STA. 271+00.00
 STA. 276+50.00 TO STA. 305+16.00
 STA. 323+82.08 TO STA. 325+50.00
 STA. 329+00.00 TO STA. 338+30.37
 STA. 352+32.45 TO STA. 408+30.00

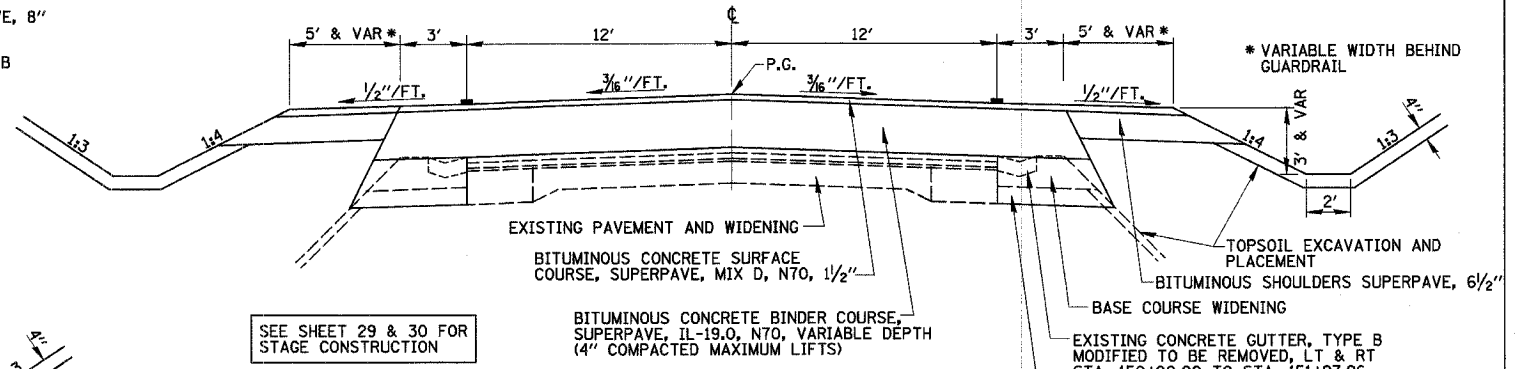


①A PROPOSED TYPICAL SECTION
 STA. 142+00.00 TO STA. 145+44.00

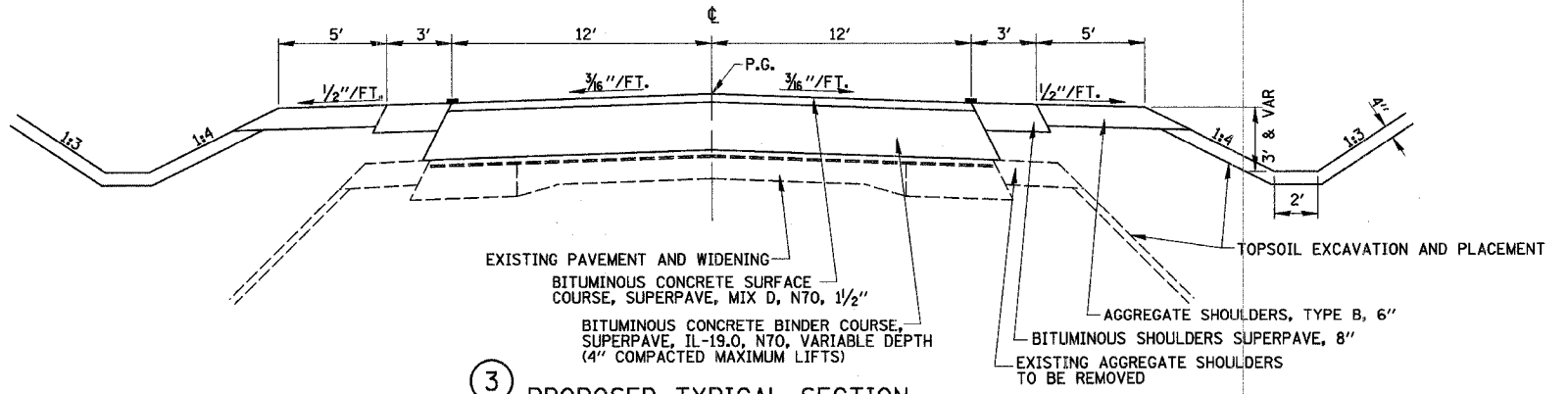


② PROPOSED TYPICAL SECTION
 STA. 151+97.96 TO STA. 160+00.00
 STA. 187+00.00 TO STA. 197+20.00 LT
 STA. 187+50.00 TO STA. 197+20.00 RT

*THICKNESS VARIES
 STA. 151+97.96
 TO STA. 156+00.00



②A PROPOSED TYPICAL SECTION
 STA. 147+81.00 TO STA. 151+97.96



③ PROPOSED TYPICAL SECTION
 STA. 218+50.00 TO STA. 223+00.00
 STA. 244+50.00 TO STA. 249+50.00
 STA. 271+00.00 TO STA. 276+50.00
 STA. 325+50.00 TO STA. 329+00.00

NOTE: MATCH EXISTING SUPERELEVATION TRANSITION FROM STA. 142+00.00 TO APPROXIMATELY STA. 143+00.00.

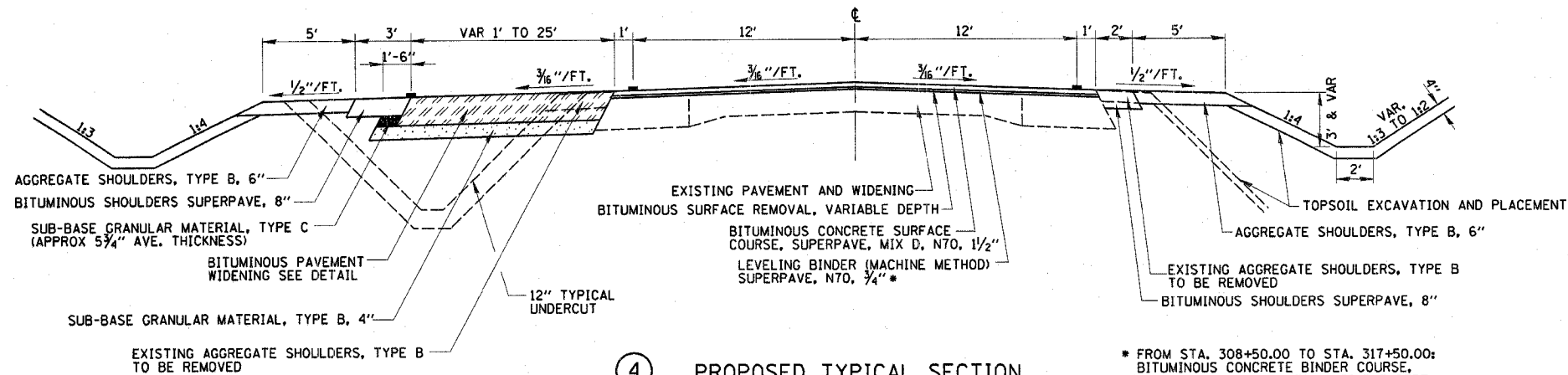
* TIE BARS TO BE PROVIDED AT PCC BASE COURSE WIDENING OPTION ONLY.

SEE SHEET 29 & 30 FOR STAGE CONSTRUCTION

SEE SHEET 29 & 30 FOR STAGE CONSTRUCTION

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ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 SHEET 1 OF 4
 DATE 6/01
 DRAWN BY MLO/TL5
 CHECKED BY RDW

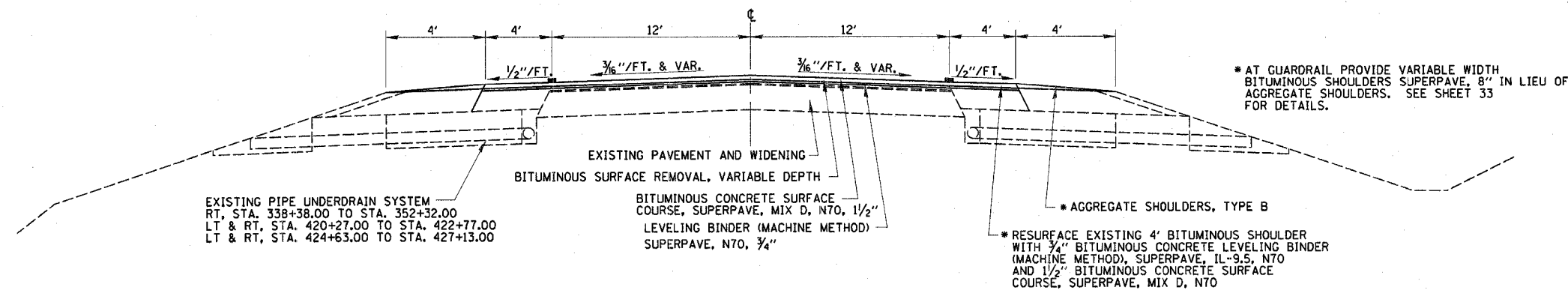


④ PROPOSED TYPICAL SECTION
STA. 305+16.00 TO STA. 323+82.08

* FROM STA. 308+50.00 TO STA. 317+50.00:
BITUMINOUS CONCRETE BINDER COURSE,
SUPERPAVE, IL-19.0, N70, VARIABLE DEPTH
(4" COMPACTED MAXIMUM LIFTS)

DETAIL OF PAVEMENT WIDENING

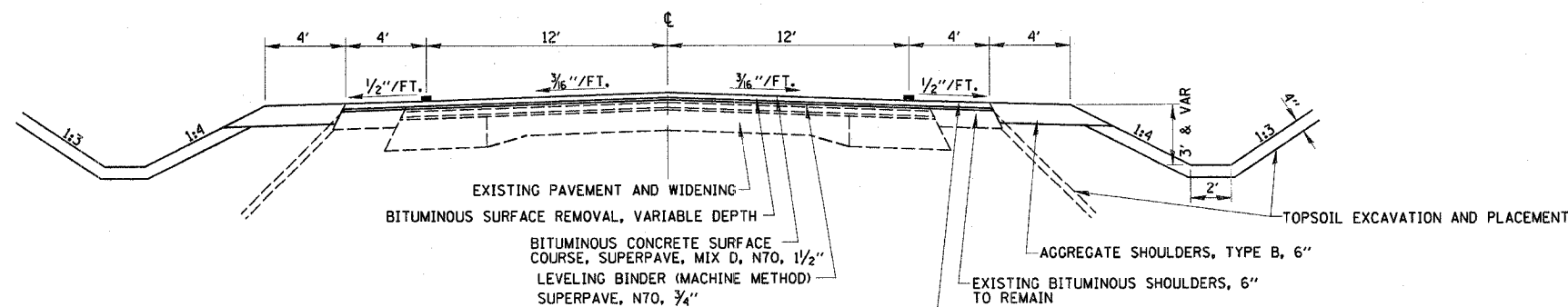
** MINIMUM COMPACTED LIFT OF 2 1/4"
MAXIMUM COMPACTED LIFT OF 4"



⑤ PROPOSED TYPICAL SECTION
STA. 338+30.37 TO STA. 352+32.45
STA. 408+30.00 TO STA. 422+94.33
STA. 424+45.67 TO STA. 464+90.00

* AT GUARDRAIL PROVIDE VARIABLE WIDTH
BITUMINOUS SHOULDERS SUPERPAVE, 8" IN LIEU OF
AGGREGATE SHOULDERS. SEE SHEET 33
FOR DETAILS.

* RESURFACE EXISTING 4' BITUMINOUS SHOULDER
WITH 3/4" BITUMINOUS CONCRETE LEVELING BINDER
(MACHINE METHOD), SUPERPAVE, IL-9.5, N70
AND 1 1/2" BITUMINOUS CONCRETE SURFACE
COURSE, SUPERPAVE, MIX D, N70



⑥ PROPOSED TYPICAL SECTION
STA. 464+90.00 TO STA. 502+08.00
STA. 520+87.00 TO STA. 529+00.00
STA. 534+40.00 TO STA. 604+90.52

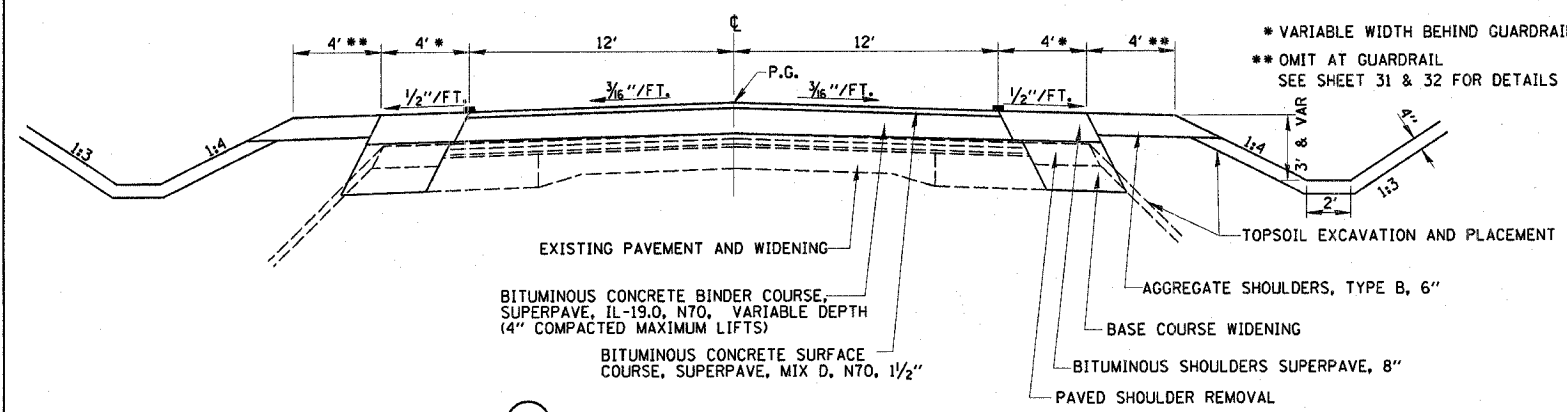
RESURFACE EXISTING 4' BITUMINOUS SHOULDER
WITH 3/4" BITUMINOUS CONCRETE LEVELING BINDER
(MACHINE METHOD), SUPERPAVE, IL-9.5, N70
AND 1 1/2" BITUMINOUS CONCRETE SURFACE
COURSE, SUPERPAVE, MIX D, N70

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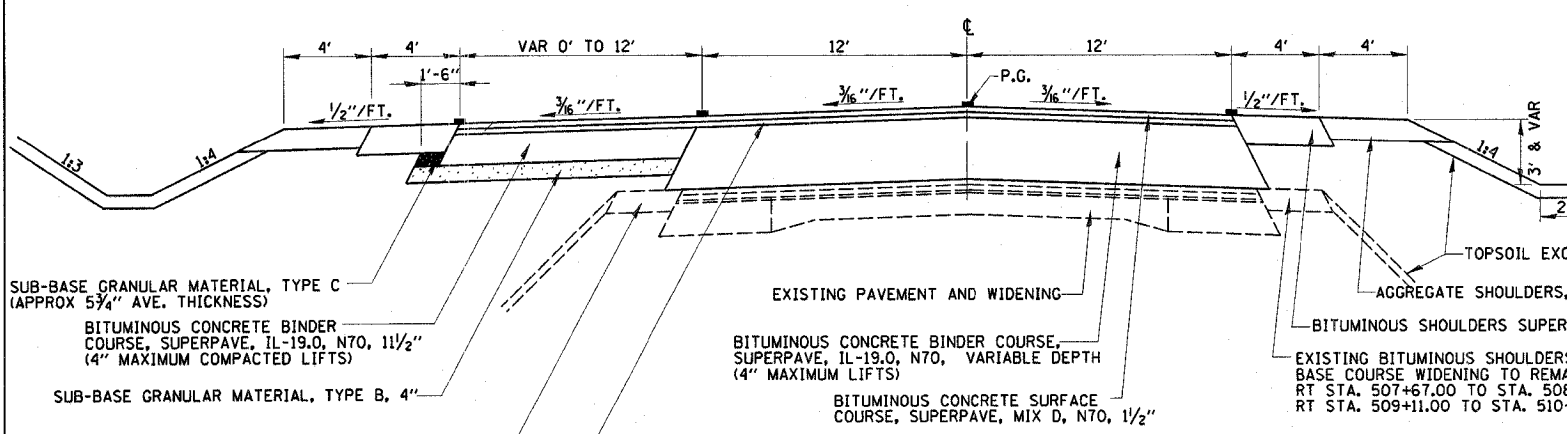
ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
SHEET 2 OF 4

DATE 6/01 DRAWN BY MLO
CHECKED BY RDW



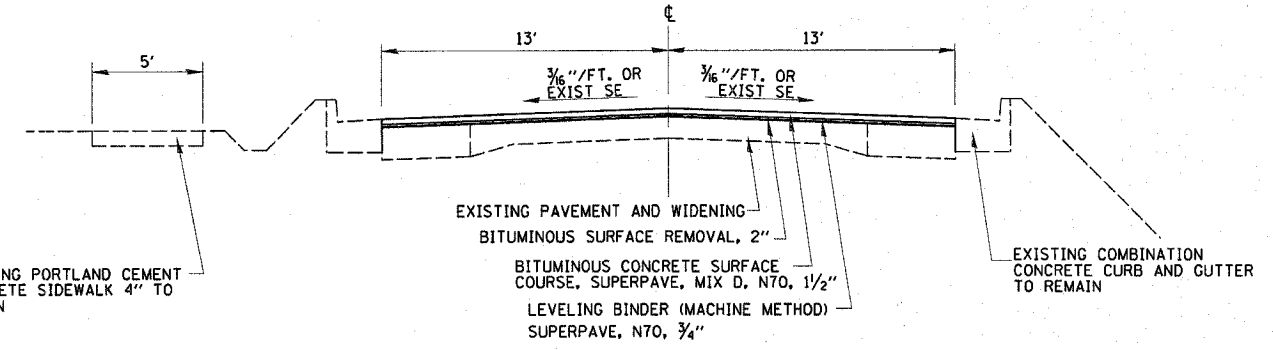
7 PROPOSED TYPICAL SECTION
 STA. 529+00.00 TO STA. 531+30.28
 STA. 532+37.00 TO STA. 534+40.00



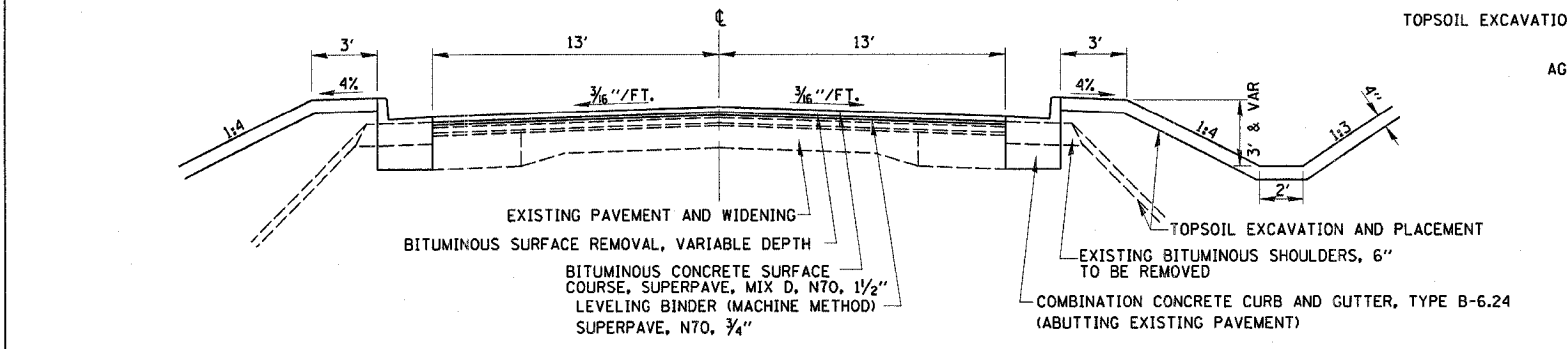
7A PROPOSED TYPICAL SECTION
 STA. 502+08.00 TO STA. 520+87.00

EXISTING BITUMINOUS SHOULDER 6" TO BE REMOVED-LT
 BASE COURSE WIDENING TO BE REMOVED
 LT STA. 508+08.00 TO STA. 508+37.00
 LT STA. 508+95.00 TO STA. 510+06.00
 (SEE SHEET 38 FOR STAGE CONSTRUCTION)

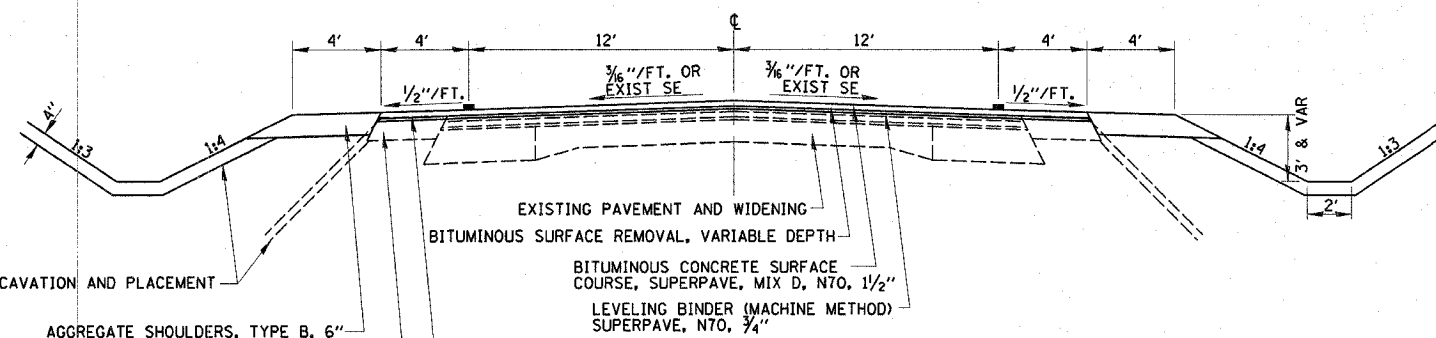
LEVELING BINDER (MACHINE METHOD)
 SUPERPAVE, N70, 3/4". (OMIT FROM
 STA. 504+50.00 TO STA. 513+50.00)



9 PROPOSED TYPICAL SECTION
 STA. 617+30.82 TO STA. 621+24.00



8 PROPOSED TYPICAL SECTION
 STA. 604+90.52 TO STA. 617+30.82



10 PROPOSED TYPICAL SECTION
 STA. 643+15.00 TO STA. 689+81.63

RESURFACE EXISTING 4' BITUMINOUS SHOULDER WITH 3/4" LEVELING BINDER (MACHINE METHOD) SUPERPAVE, IL-9.5, N70 AND 1 1/2" BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX D, N70

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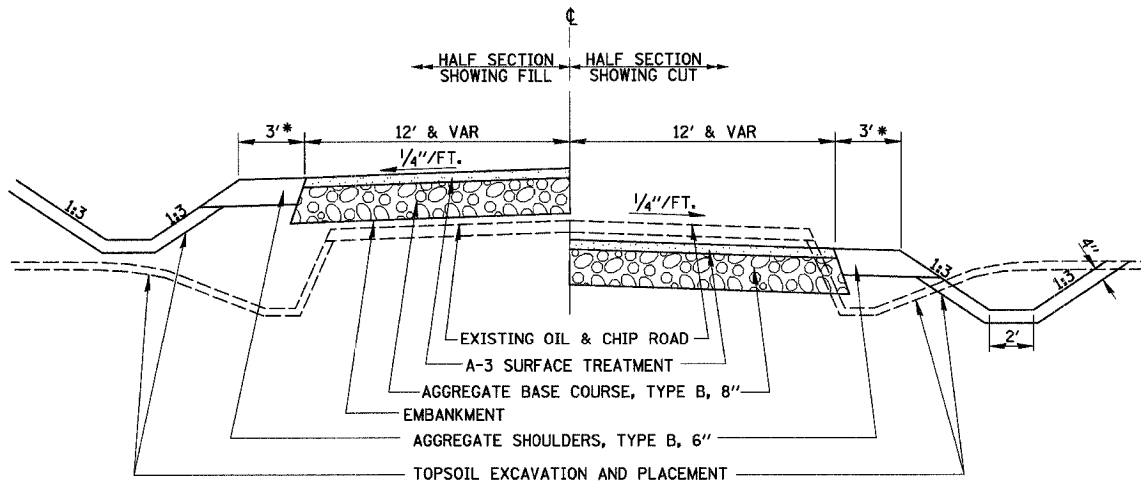
ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
 SHEET 3 OF 4

DATE 6/01

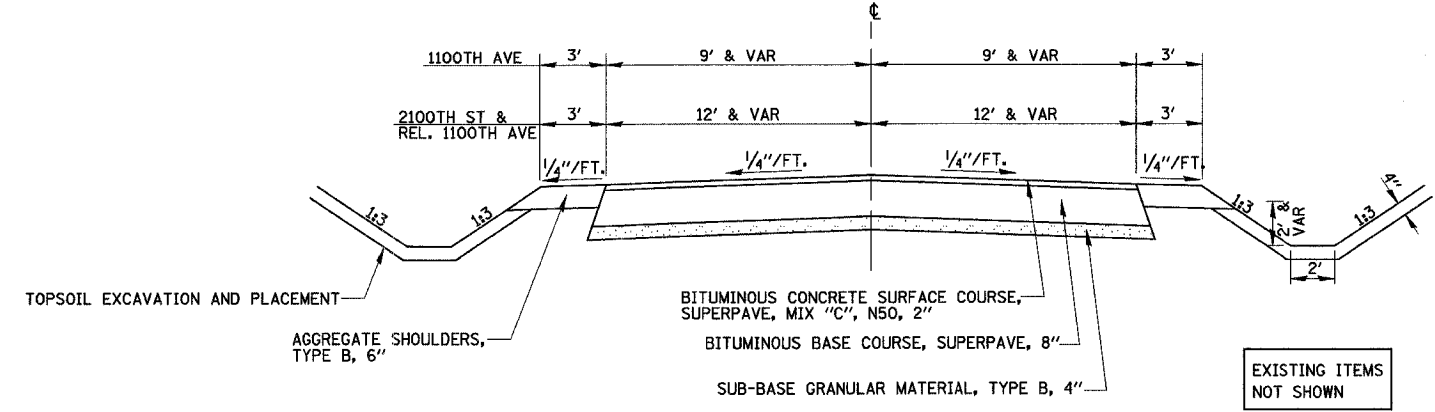
DRAWN BY MLO
 CHECKED BY RDW

Revised 5/2/07

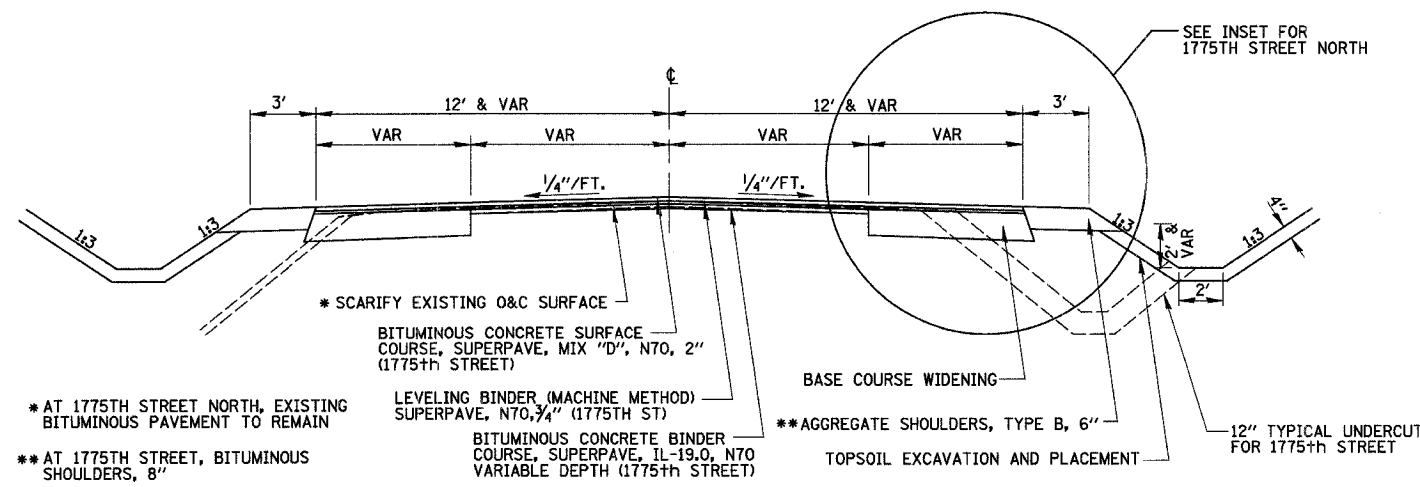


* AT T.R. 109 AND 1560TH STREET, OMIT AGGREGATE SHOULDER.

- 11** PROPOSED TYPICAL SECTION
 STA. 7+35.71 TO STA. 8+88.00 (1560TH STREET)
 STA. 108+46.95 TO STA. 112+65.07 (T.R. 109)
 STA. 11+71.00 TO STA. 12+49.09 (1600TH STREET)
 STA. 15+50.00 TO STA. 16+50.83 (1650TH STREET)
 STA. 18+45.00 TO STA. 19+45.44 (1700TH STREET)
 STA. 21+30.00 TO STA. 22+37.70 (1750TH STREET)
 STA. 126+20.00 TO STA. 127+24.46 (1800TH STREET)
 STA. 56+16.00 TO STA. 57+15.67 (2200TH STREET)
 STA. 73+20.00 TO STA. 74+23.08 (2400TH STREET)

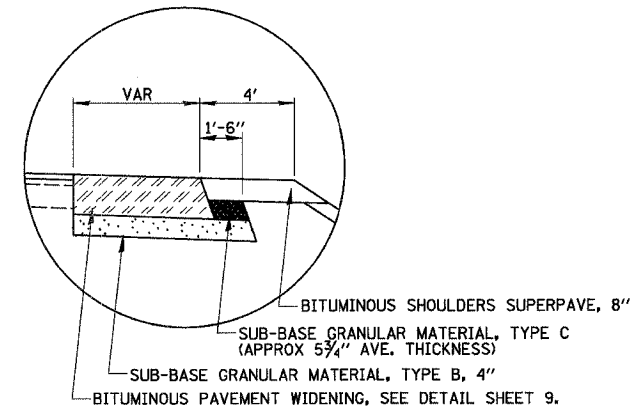


- 13** PROPOSED TYPICAL SECTION
 STA. 85+17.68 TO STA. 89+22.48 (2100TH STREET)
 STA. 89+32.84 TO STA. 89+87.58 (2100TH STREET)
 STA. 90+24.84 TO STA. 91+80.57 (2100TH STREET)
 STA. 50+20.00 TO STA. 51+50.33 (1100TH AVENUE)
 STA. 80+24.00 TO STA. 82+83.81 (RELOCATED 1100TH AVENUE)



* AT 1775TH STREET NORTH, EXISTING BITUMINOUS PAVEMENT TO REMAIN
 ** AT 1775TH STREET, BITUMINOUS SHOULDERS, 8"

- 12** PROPOSED TYPICAL SECTION
 STA. 84+28.01 TO STA. 85+17.68 (2100TH STREET)
 STA. 91+80.57 TO STA. 92+61.55 (2100TH STREET)
 STA. 25+64.66 TO STA. 26+30.75 LT (1775TH STREET SOUTH)
 STA. 27+38.00 TO STA. 28+69.64 RT (1775TH STREET NORTH)
 STA. 27+26.00 TO STA. 28+15.86 LT (1775TH STREET NORTH)



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ILLINOIS DEPARTMENT OF TRANSPORTATION
 TYPICAL SECTIONS
 SHEET 4 OF 4
 DRAWN BY MLO
 CHECKED BY RDW
 DATE 6/01

| SUB-BASE GRANULAR MATERIAL, TYPE B, 4" | | | |
|--|--------------------|-----------|-------|
| LOCATION | STATION TO STATION | | SQ YD |
| MAINLINE | 305+16.00 | 310+76.00 | 529 |
| MAINLINE | 310+76.00 | 312+82.64 | 333 |
| MAINLINE | 314+90.23 | 317+55.00 | 780 |
| MAINLINE | 317+55.00 | 320+20.00 | 533 |
| MAINLINE | 320+20.00 | 323+07.00 | 195 |
| MAINLINE | 502+08.00 | 506+88.00 | 427 |
| MAINLINE | 506+88.00 | 508+43.00 | 233 |
| MAINLINE | 508+81.00 | 516+07.00 | 1,089 |
| MAINLINE | 516+07.00 | 520+87.00 | 427 |
| 1775TH ST | 27+13.00 | 28+70.00 | 326 |
| 1775TH ST | 27+13.00 | 28+16.00 | 461 |
| 2100TH ST | 85+17.68 | 89+22.48 | 1,079 |
| 2100TH ST | 89+32.84 | 89+87.58 | 252 |
| 2100TH ST | 90+12.42 | 90+77.81 | 247 |
| 2100TH ST | 90+77.81 | 91+80.57 | 274 |
| 1100TH ST | 50+20.00 | 51+30.14 | 220 |
| 1100TH ST | 51+30.14 | 51+50.33 | 57 |
| REL 1100TH ST | 80+24.00 | 80+84.03 | 262 |
| REL 1100TH ST | 80+84.03 | 82+83.81 | 533 |
| TOTAL | | | 8,257 |

| SUB-BASE GRANULAR MATERIAL, TYPE C | | | | |
|------------------------------------|--------------------|-----------|--------|------|
| LOCATION | STATION TO STATION | | OFFSET | TONS |
| MAINLINE | 142+00.00 | 145+44.00 | LT | 57 |
| MAINLINE | 142+00.00 | 145+44.00 | RT | 57 |
| MAINLINE | 305+96.00 | 310+76.00 | LT | 26 |
| MAINLINE | 310+76.00 | 312+83.00 | LT | 11 |
| MAINLINE | 314+90.00 | 317+55.00 | LT | 14 |
| MAINLINE | 317+55.00 | 320+20.00 | LT | 14 |
| MAINLINE | 320+20.00 | 323+07.00 | LT | 16 |
| MAINLINE | 502+08.00 | 508+43.00 | LT | 35 |
| MAINLINE | 508+81.00 | 520+87.00 | LT | 66 |
| 1775TH ST | 27+38.00 | 38+69.64 | RT | 9 |
| 1775TH ST | 27+26.00 | 28+15.86 | LT | 11 |
| TOTAL | | | | 316 |

| BITUMINOUS MATERIALS (PRIME COAT) | | | |
|-----------------------------------|--------------------|-----------|---------|
| LOCATION | STATION TO STATION | | GALLONS |
| 1560TH ST | 7+78.15 | 8+58.39 | 61 |
| T.R. 109 | 108+46.95 | 112+65.07 | 465 |
| 1600TH ST | 11+71.00 | 12+49.09 | 97 |
| 1650TH ST | 15+50.00 | 16+50.83 | 131 |
| 1700TH ST | 18+45.00 | 19+45.44 | 135 |
| 1750TH ST | 21+30.00 | 22+37.70 | 132 |
| 1800TH ST | 126+20.00 | 127+24.46 | 137 |
| 2200TH ST | 56+16.00 | 57+15.67 | 128 |
| 2400TH ST | 73+20.00 | 74+23.08 | 144 |
| TOTAL | | | 1,430 |

QUANTITY FOR A-3 SURFACE TREATMENT

| AGGREGATE BASE COURSE, TYPE B, 8" | | | |
|-----------------------------------|--------------------|-----------|-------|
| LOCATION | STATION TO STATION | | SQ YD |
| 1560TH ST | 7+35.71 | 8+88.00 | 350 |
| T.R. 109 | 108+46.95 | 112+65.07 | 836 |
| 1600TH ST | 11+71.00 | 12+49.09 | 177 |
| 1650TH ST | 15+50.00 | 16+50.83 | 240 |
| 1700TH ST | 18+45.00 | 19+45.44 | 247 |
| 1750TH ST | 21+30.00 | 22+37.70 | 240 |
| 1800TH ST | 126+20.00 | 127+24.46 | 250 |
| 2200TH ST | 56+16.00 | 57+15.67 | 233 |
| 2400TH ST | 73+20.00 | 74+23.08 | 265 |
| TOTAL | | | 2,838 |

| BITUMINOUS MATERIALS (COVER & SEAL COATS) | | | |
|---|--------------------|-----------|---------|
| LOCATION | STATION TO STATION | | GALLONS |
| 1560TH ST | 7+78.15 | 8+58.39 | 155 |
| T.R. 109 | 108+46.95 | 112+65.07 | 1,254 |
| 1600TH ST | 11+71.00 | 12+49.09 | 266 |
| 1650TH ST | 15+50.00 | 16+50.83 | 360 |
| 1700TH ST | 18+45.00 | 19+45.44 | 371 |
| 1750TH ST | 21+30.00 | 22+37.70 | 360 |
| 1800TH ST | 126+20.00 | 127+24.46 | 375 |
| 2200TH ST | 56+16.00 | 57+15.67 | 350 |
| 2400TH ST | 73+20.00 | 74+23.08 | 398 |
| TOTAL | | | 3,889 |

| TRENCH BACKFILL | | | |
|-----------------|-----------|--------|-------|
| LOCATION | STATION | OFFSET | CU YD |
| MAINLINE | 221+74.03 | - | 7 |
| MAINLINE | 274+72.57 | - | 4 |
| MAINLINE | 300+60.00 | - | 8 |
| MAINLINE | 492+75.11 | - | 4 |
| MAINLINE | 509+65.00 | - | 48 |
| MAINLINE | 688+48.00 | - | 5 |
| 2100TH ST | 87+00.00 | - | 21 |
| GOBBLER | 160+04.70 | RT | 9 |
| 1525TH | 180+32.40 | LT | 9 |
| T.R. 109 | 200+95.86 | RT | 5 |
| 1600TH | 221+24.83 | LT | 6 |
| 1650TH | 247+78.42 | LT | 4 |
| 1650TH | 247+78.42 | RT | 5 |
| 1700TH | 274+32.00 | LT | 5 |
| 1750TH | 301+09.14 | LT | 7 |
| 1775TH | 313+50.00 | LT | 29 |
| 1775TH | 313+75.00 | RT | 14 |
| 1800TH | 327+49.59 | LT | 5 |
| 1800TH | 327+44.85 | RT | 4 |
| 1900TH | 372+50.06 | LT | 13 |
| 1900TH | 373+17.89 | RT | 14 |
| 1250TH | 385+20.12 | LT | 5 |
| 1950TH | 408+77.11 | LT | 10 |
| 1950TH | 409+52.36 | RT | 24 |
| 1950TH | 409+56.91 | RT | 4 |
| 2100TH | 508+29.93 | LT | 5 |
| 2100TH | 508+51.96 | RT | 13 |
| 1100TH | 88+37.52 | LT | 6 |
| REL 1100TH | 514+52.80 | LT | 5 |
| 2200TH | 569+82.15 | LT | 6 |
| 2200TH | 570+21.82 | RT | 10 |
| NORB | 591+95.31 | LT | 14 |
| VIOLA | 613+51.62 | LT | 17 |
| TOTAL | | | 350 |

| BASE COURSE WIDENING | | | |
|----------------------|--------------------|-----------|-------|
| LOCATION | STATION TO STATION | | SQ YD |
| BIG SALT CREEK | 143+48.00 | 145+39.00 | 64 |
| BIG SALT CREEK | 145+39.00 | 145+80.00 | 14 |
| BIG SALT CREEK | 143+48.00 | 145+39.00 | 64 |
| BIG SALT CREEK | 147+51.00 | 147+87.00 | 12 |
| BIG SALT CREEK | 147+87.00 | 152+25.00 | 219 |
| BIG SALT CREEK | 147+81.00 | 152+25.00 | 146 |
| CULV 221+74.03 | 220+73.00 | 221+07.00 | 8 |
| CULV 221+74.03 | 221+45.00 | 222+01.00 | 12 |
| CULV 221+74.03 | 220+64.00 | 221+04.00 | 9 |
| CULV 221+74.03 | 221+41.00 | 222+11.00 | 16 |
| CULV 274+72.57 | 273+71.00 | 274+13.00 | 9 |
| CULV 274+72.57 | 274+54.00 | 275+03.00 | 11 |
| CULV 274+72.57 | 273+79.00 | 274+12.00 | 7 |
| CULV 274+72.57 | 274+51.00 | 274+89.00 | 8 |
| CULV 300+60.00 | 300+30.00 | 300+95.00 | 14 |
| CULV 300+60.00 | 301+23.00 | 301+75.00 | 12 |
| CULV 300+60.00 | 300+30.00 | 300+80.00 | 11 |
| CULV 300+60.00 | 301+27.00 | 301+75.00 | 11 |
| LITTLE SALT CREEK | 422+00.00 | 422+94.00 | 84 |
| LITTLE SALT CREEK | 424+46.00 | 425+15.00 | 61 |
| CULV 492+75.11 | 492+43.00 | 493+06.00 | 21 |
| CULV 492+75.11 | 492+45.00 | 493+05.00 | 20 |
| CULV 509+65.00 | 508+08.00 | 508+37.00 | 10 |
| CULV 509+65.00 | 508+95.00 | 510+06.00 | 37 |
| CULV 509+65.00 | 507+67.00 | 508+65.00 | 33 |
| CULV 509+65.00 | 509+11.00 | 510+06.00 | 32 |
| BISHOP CREEK | 529+45.00 | 531+41.06 | 90 |
| BISHOP CREEK | 531+33.78 | 531+64.00 | 13 |
| BISHOP CREEK | 532+12.00 | 534+80.00 | 119 |
| BISHOP CREEK | 528+55.00 | 531+21.00 | 118 |
| BISHOP CREEK | 532+39.00 | 534+40.00 | 89 |
| CULV 688+92.98 | 688+61.00 | 689+02.00 | 18 |
| CULV 688+92.98 | 689+47.00 | 689+81.63 | 12 |
| CULV 688+92.98 | 688+59.00 | 689+14.00 | 24 |
| CULV 688+92.98 | 689+53.00 | 689+81.63 | 10 |
| 1775TH ST S | 25+52.80 | 26+30.75 | 31 |
| 2100TH ST S | 84+28.01 | 85+17.68 | 31 |
| 2100TH ST S | 84+28.01 | 85+17.68 | 31 |
| 2100TH ST N | 91+80.57 | 92+61.55 | 42 |
| 2100TH ST N | 91+80.57 | 92+61.55 | 42 |
| TOTAL | | | 1,615 |

| COVER COAT AGGREGATE | | | |
|----------------------|--------------------|-----------|------|
| LOCATION | STATION TO STATION | | TONS |
| 1560TH ST | 7+35.71 | 8+88.00 | 9 |
| T.R. 109 | 108+46.95 | 112+65.07 | 21 |
| 1600TH ST | 11+71.00 | 12+49.09 | 4 |
| 1650TH ST | 15+50.00 | 16+50.83 | 6 |
| 1700TH ST | 18+45.00 | 19+45.44 | 6 |
| 1750TH ST | 21+30.00 | 22+37.70 | 6 |
| 1800TH ST | 126+20.00 | 127+24.46 | 6 |
| 2200TH ST | 56+16.00 | 57+15.67 | 6 |
| 2400TH ST | 73+20.00 | 74+23.08 | 7 |
| TOTAL | | | 71 |

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ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
SHEET 2 OF 4

DRAWN BY MLO
CHECKED BY SJK

DATE 8/03

| SEAL COAT AGGREGATE | | | |
|---------------------|---------------------|------|--|
| LOCATION | STATION TO STATION | TONS | |
| 1560TH ST | 7+35.71 8+88.00 | 4 | |
| T.R. 109 | 108+46.95 112+65.07 | 10 | |
| 1600TH ST | 11+71.00 12+49.09 | 2 | |
| 1650TH ST | 15+50.00 16+50.83 | 3 | |
| 1700TH ST | 18+45.00 19+45.44 | 3 | |
| 1750TH ST | 21+30.00 22+37.70 | 3 | |
| 1800TH ST | 126+20.00 127+24.46 | 3 | |
| 2200TH ST | 56+16.00 57+15.67 | 3 | |
| 2400TH ST | 73+20.00 74+23.08 | 3 | |
| TOTAL | | 34 | |

| AGGREGATE (PRIME COAT) | | | |
|------------------------|---------------------|---------|--|
| LOCATION | STATION TO STATION | GALLONS | |
| MAINLINE | 142+00.00 145+44.00 | 4 | |
| MAINLINE | 147+81.00 151+97.96 | 14 | |
| MAINLINE | 151+97.96 160+00.00 | 9 | |
| MAINLINE | 160+00.00 180+60.00 | 24 | |
| MAINLINE | 180+60.00 187+00.00 | 4 | |
| MAINLINE | 180+60.00 187+50.00 | 4 | |
| MAINLINE | 187+00.00 197+20.00 | 5 | |
| MAINLINE | 187+50.00 197+20.00 | 5 | |
| MAINLINE | 197+20.00 218+50.00 | 25 | |
| MAINLINE | 218+50.00 223+00.00 | 5 | |
| MAINLINE | 223+00.00 244+50.00 | 25 | |
| MAINLINE | 244+50.00 249+50.00 | 5 | |
| MAINLINE | 249+50.00 271+00.00 | 25 | |
| MAINLINE | 271+00.00 276+50.00 | 6 | |
| MAINLINE | 276+50.00 305+16.00 | 33 | |
| MAINLINE | 305+16.00 323+82.00 | 22 | |
| MAINLINE | 323+82.00 325+50.00 | 2 | |
| MAINLINE | 325+50.00 329+00.00 | 4 | |
| MAINLINE | 329+00.00 338+30.37 | 11 | |
| MAINLINE | 338+30.37 352+32.45 | 20 | |
| MAINLINE | 352+32.45 408+30.00 | 62 | |
| MAINLINE | 408+30.00 464+90.00 | 79 | |
| MAINLINE | 464+90.00 502+08.00 | 53 | |
| MAINLINE | 502+08.00 520+87.00 | 40 | |
| MAINLINE | 520+87.00 529+00.00 | 12 | |
| MAINLINE | 529+00.00 534+40.00 | 4 | |
| MAINLINE | 534+40.00 604+90.52 | 100 | |
| MAINLINE | 604+90.52 617+30.82 | 14 | |
| MAINLINE | 617+30.82 621+24.00 | 5 | |
| MAINLINE | 643+15.00 689+81.63 | 66 | |
| TOTAL | | 687 | |

| DRIVEWAY PAVEMENT REMOVAL | | | |
|---------------------------|--------|-------|--|
| STATION | OFFSET | SQ YD | |
| 177+48.07 | LT | 104 | |
| 194+25.72 | RT | 164 | |
| 201+34.68 | LT | 230 | |
| 207+54.79 | LT | 58 | |
| 605+50.93 | LT | 280 | |
| 611+33.26 | LT | 143 | |
| 612+69.59 | LT | 133 | |
| 643+47.09 | RT | 71 | |
| 645+03.33 | LT | 70 | |
| 645+82.14 | RT | 41 | |
| 646+85.75 | RT | 40 | |
| 648+30.07 | RT | 56 | |
| 649+02.51 | RT | 44 | |
| 650+86.38 | RT | 75 | |
| 652+25.97 | RT | 70 | |
| 653+95.48 | RT | 88 | |
| 55+58.09 | LT | 25 | |
| TOTAL | | 1,692 | |

| AGGREGATE SHOULDER, TYPE B 6" | | | | |
|-------------------------------|---------------------|--------|-------|--|
| LOCATION | STATION TO STATION | OFFSET | SQ YD | |
| MAINLINE | 149+38.00 151+97.96 | RT | 128 | |
| MAINLINE | 150+24.00 151+97.96 | LT | 97 | |
| MAINLINE | 151+97.96 160+00.00 | LT | 435 | |
| MAINLINE | 151+97.96 159+78.00 | RT | 433 | |
| MAINLINE | 160+25.00 179+90.00 | LT | 945 | |
| MAINLINE | 160+43.00 180+15.00 | RT | 1,010 | |
| MAINLINE | 180+60.00 187+00.00 | LT | 356 | |
| MAINLINE | 180+85.00 187+50.00 | RT | 369 | |
| MAINLINE | 187+00.00 197+20.00 | LT | 567 | |
| MAINLINE | 187+50.00 197+20.00 | RT | 511 | |
| MAINLINE | 197+20.00 218+50.00 | LT | 1,094 | |
| MAINLINE | 197+20.00 218+50.00 | RT | 1,146 | |
| MAINLINE | 218+50.00 223+00.00 | LT | 219 | |
| MAINLINE | 218+50.00 223+00.00 | RT | 219 | |
| MAINLINE | 223+00.00 244+50.00 | LT | 1,178 | |
| MAINLINE | 223+00.00 244+50.00 | RT | 1,194 | |
| MAINLINE | 244+50.00 249+50.00 | LT | 209 | |
| MAINLINE | 244+50.00 249+50.00 | RT | 248 | |
| MAINLINE | 249+50.00 271+00.00 | LT | 1,175 | |
| MAINLINE | 249+50.00 271+00.00 | RT | 1,194 | |
| MAINLINE | 271+00.00 276+50.00 | LT | 275 | |
| MAINLINE | 271+00.00 276+50.00 | RT | 275 | |
| MAINLINE | 276+50.00 305+16.00 | LT | 1,562 | |
| MAINLINE | 276+50.00 305+16.00 | RT | 1,547 | |
| MAINLINE | 305+16.00 323+82.00 | LT | 661 | |
| MAINLINE | 305+16.00 323+82.00 | RT | 942 | |
| MAINLINE | 323+82.00 325+50.00 | LT | 93 | |
| MAINLINE | 323+82.00 325+50.00 | RT | 93 | |
| MAINLINE | 325+50.00 329+00.00 | LT | 162 | |
| MAINLINE | 325+50.00 329+00.00 | RT | 163 | |
| MAINLINE | 329+00.00 338+30.37 | LT | 466 | |
| MAINLINE | 329+00.00 338+30.37 | RT | 517 | |
| MAINLINE | 352+32.45 408+30.00 | LT | 2,910 | |
| MAINLINE | 352+32.45 408+30.00 | RT | 2,935 | |
| MAINLINE | 464+90.00 504+50.00 | LT | 1,652 | |
| MAINLINE | 464+90.00 502+08.00 | RT | 1,643 | |
| MAINLINE | 502+80.00 520+87.00 | LT | 756 | |
| MAINLINE | 502+08.00 520+87.00 | RT | 798 | |
| MAINLINE | 520+87.00 528+67.38 | LT | 347 | |
| MAINLINE | 520+87.00 529+00.00 | RT | 361 | |
| MAINLINE | 529+00.00 529+79.00 | LT | 35 | |
| MAINLINE | 533+91.00 534+40.00 | RT | 22 | |
| MAINLINE | 535+00.00 604+90.52 | LT | 2,908 | |
| MAINLINE | 533+90.00 604+90.52 | RT | 3,103 | |
| MAINLINE | 643+15.00 689+81.63 | LT | 1,969 | |
| MAINLINE | 643+15.00 689+81.63 | RT | 1,819 | |
| 1525TH ST | 3+33.00 6+15.00 | LT | 69 | |
| 1525TH ST | 3+96.00 6+15.00 | RT | 62 | |
| 1600TH ST | 11+71.00 13+73.85 | LT | 54 | |
| 1600TH ST | 11+71.00 13+73.85 | RT | 54 | |
| 1650TH ST | 15+50.00 17+92.47 | LT | 67 | |
| 1650TH ST | 15+50.00 17+92.47 | RT | 67 | |
| 1700TH | 18+45.00 20+74.40 | LT | 62 | |
| 1700TH | 18+45.00 20+74.40 | RT | 62 | |
| 1750TH | 21+30.00 23+94.90 | LT | 74 | |
| 1750TH | 21+30.00 23+94.90 | RT | 75 | |
| 1800TH | 126+20.00 128+88.17 | LT | 76 | |
| 1800TH | 126+20.00 128+88.17 | RT | 76 | |
| 1300TH | 30+20.00 30+75.00 | LT | 21 | |
| 1300TH | 30+20.00 30+75.00 | RT | 18 | |
| 1900TH S | 32+24.80 32+90.00 | LT | 22 | |
| 1900TH S | 32+14.06 32+60.00 | RT | 17 | |
| 1900TH N | 33+62.00 34+31.45 | LT | 26 | |
| 1900TH N | 33+05.00 34+31.45 | RT | 43 | |
| 1250TH | 35+01.97 37+16.83 | LT | 55 | |
| 1250TH | 35+15.24 37+16.83 | RT | 48 | |
| 1950TH S | 38+20.26 38+86.00 | LT | 24 | |
| 1950TH S | 38+13.56 38+68.00 | RT | 21 | |
| 1950TH N | 39+55.00 40+34.17 | LT | 31 | |
| 1950TH N | 39+00.00 40+34.17 | RT | 46 | |
| 1175TH | 42+20.00 42+77.45 | LT | 19 | |
| 1175TH | 42+20.00 42+77.45 | RT | 19 | |
| 2000TH | 44+25.36 44+80.00 | LT | 20 | |
| 2000TH | 44+22.29 44+80.00 | RT | 20 | |
| 2100TH N | 90+45.00 92+61.55 | LT | 78 | |
| 2100TH N | 90+24.00 92+61.55 | RT | 81 | |
| SUB-TOTAL | | 42,148 | | |

| BITUMINOUS MATERIALS (PRIME COAT) | | | |
|-----------------------------------|---------------------|---------|--|
| LOCATION | STATION TO STATION | GALLONS | |
| MAINLINE | 142+00.00 145+44.00 | 221 | |
| MAINLINE | 147+81.00 151+97.96 | 723 | |
| MAINLINE | 151+97.96 160+00.00 | 428 | |
| MAINLINE | 160+00.00 180+60.00 | 1,190 | |
| MAINLINE | 180+60.00 187+00.00 | 185 | |
| MAINLINE | 180+60.00 187+50.00 | 199 | |
| MAINLINE | 187+00.00 197+20.00 | 272 | |
| MAINLINE | 187+50.00 197+20.00 | 259 | |
| MAINLINE | 197+20.00 218+50.00 | 1,237 | |
| MAINLINE | 218+50.00 223+00.00 | 240 | |
| MAINLINE | 223+00.00 244+50.00 | 1,242 | |
| MAINLINE | 244+50.00 249+50.00 | 267 | |
| MAINLINE | 249+50.00 271+00.00 | 1,242 | |
| MAINLINE | 271+00.00 276+50.00 | 293 | |
| MAINLINE | 276+50.00 305+16.00 | 1,656 | |
| MAINLINE | 305+16.00 314+45.17 | 991 | |
| MAINLINE | 314+13.07 323+82.00 | 1,352 | |
| MAINLINE | 323+82.00 325+50.00 | 97 | |
| MAINLINE | 325+50.00 329+00.00 | 187 | |
| MAINLINE | 329+00.00 338+30.37 | 538 | |
| MAINLINE | 338+30.37 352+32.45 | 997 | |
| MAINLINE | 352+32.45 408+30.00 | 3,121 | |
| MAINLINE | 408+30.00 464+90.00 | 3,920 | |
| MAINLINE | 464+90.00 502+08.00 | 2,644 | |
| MAINLINE | 502+08.00 520+87.00 | 2,180 | |
| MAINLINE | 520+87.00 529+00.00 | 578 | |
| MAINLINE | 529+00.00 534+40.00 | 231 | |
| MAINLINE | 534+40.00 604+90.52 | 5,014 | |
| MAINLINE | 604+90.52 617+30.82 | 717 | |
| MAINLINE | 617+30.82 621+24.00 | 227 | |
| MAINLINE | 643+15.00 689+81.63 | 3,286 | |
| 1775TH ST | 24+84.90 26+30.75 | 83 | |
| 2100TH ST | 84+28.01 89+22.48 | 132 | |
| 2100TH ST | 90+77.81 92+61.55 | 49 | |
| 1100TH ST | 50+20.00 51+30.14 | 22 | |
| REL 1100TH ST | 80+84.03 82+83.81 | 53 | |
| TOTAL | | 36,073 | |

(SEE PUBLIC ROAD SCHEDULE FOR REMAINING QUANTITIES)

| BITUMINOUS SURFACE REMOVAL (VARIABLE DEPTH) | | | |
|---|--------|---------|--|
| STATION TO STATION | OFFSET | TONS | |
| 142+00.00 145+44.00 | LT&RT | 994 | |
| 151+97.96 156+00.00 | LT&RT | 1,072 | |
| 156+00.00 160+00.00 | LT&RT | 1,067 | |
| 160+00.00 180+60.00 | LT&RT | 5,951 | |
| 180+60.00 187+00.00 | LT | 924 | |
| 180+60.00 187+50.00 | RT | 997 | |
| 187+00.00 197+20.00 | LT | 1,360 | |
| 187+50.00 197+20.00 | RT | 1,293 | |
| 197+20.00 244+50.00 | LT&RT | 12,396 | |
| 244+50.00 245+00.00 | LT&RT | 133 | |
| 249+50.00 271+00.00 | LT&RT | 6,211 | |
| 271+00.00 271+50.00 | LT&RT | 133 | |
| 276+50.00 305+16.00 | LT&RT | 8,280 | |
| 305+16.00 323+82.00 | LT&RT | 2,791 | |
| 323+82.00 338+30.37 | LT&RT | 3,173 | |
| 338+30.37 352+32.45 | LT&RT | 4,985 | |
| 352+32.45 408+30.00 | LT&RT | 15,608 | |
| 408+30.00 464+90.00 | LT&RT | 19,600 | |
| 464+90.00 502+08.00 | LT&RT | 13,220 | |
| 502+08.00 520+87.00 | LT&RT | 2,744 | |
| 520+87.00 529+00.00 | LT&RT | 2,891 | |
| 529+00.00 529+50.00 | LT&RT | 133 | |
| 534+40.00 604+90.52 | LT&RT | 25,069 | |
| 604+90.52 617+30.82 | LT&RT | 3,583 | |
| 643+15.00 689+81.63 | LT&RT | 16,433 | |
| TOTAL | | 141,957 | |

| BITUMINOUS SURFACE REMOVAL, 2" | | | |
|--------------------------------|--------|-------|--|
| STATION TO STATION | OFFSET | TONS | |
| 617+30.82 621+24.00 | LT&RT | 1,136 | |

| PAVED SHOULDER REMOVAL | | | |
|------------------------|--------|-------|--|
| STATION TO STATION | OFFSET | SQ YD | |
| 220+73.00 221+07.00 | RT | 8 | |
| 221+45.00 222+01.00 | RT | 12 | |
| 220+64.00 221+04.00 | LT | 9 | |
| 221+41.00 222+11.00 | LT | 16 | |
| 273+71.00 274+13.00 | RT | 9 | |
| 274+54.00 275+03.00 | RT | 11 | |
| 273+79.00 274+12.00 | LT | 7 | |
| 274+51.00 274+89.00 | LT | 8 | |
| 300+30.00 3 | | | |

BITUMINOUS SHOULDERS SUPERPAVE, 6.5"

| LOCATION | STATION TO STATION | OFFSET | SQ YD |
|----------|---------------------|--------|-------|
| MAINLINE | 147+81.00 149+38.00 | RT | 144 |
| MAINLINE | 147+81.00 150+25.00 | LT | 244 |
| TOTAL | | | 368 |

BITUMINOUS SHOULDERS SUPERPAVE, 8"

| LOCATION | STATION TO STATION | OFFSET | SQ YD |
|-----------|---------------------|--------|--------|
| MAINLINE | 150+25.00 159+96.00 | LT | 313 |
| MAINLINE | 149+38.00 159+59.00 | RT | 340 |
| MAINLINE | 160+27.00 179+90.00 | LT | 373 |
| MAINLINE | 160+64.00 180+15.00 | RT | 397 |
| MAINLINE | 180+60.00 187+00.00 | LT | 142 |
| MAINLINE | 180+85.00 187+50.00 | RT | 148 |
| MAINLINE | 187+00.00 197+20.00 | LT | 340 |
| MAINLINE | 187+50.00 197+20.00 | RT | 307 |
| MAINLINE | 197+20.00 218+50.00 | LT | 434 |
| MAINLINE | 197+20.00 218+50.00 | RT | 455 |
| MAINLINE | 218+50.00 223+00.00 | LT | 130 |
| MAINLINE | 218+50.00 223+00.00 | RT | 130 |
| MAINLINE | 223+00.00 244+50.00 | LT | 469 |
| MAINLINE | 223+00.00 244+50.00 | RT | 478 |
| MAINLINE | 244+50.00 249+50.00 | LT | 102 |
| MAINLINE | 244+50.00 249+50.00 | RT | 146 |
| MAINLINE | 249+50.00 271+00.00 | LT | 469 |
| MAINLINE | 249+50.00 271+00.00 | RT | 478 |
| MAINLINE | 271+00.00 276+50.00 | LT | 164 |
| MAINLINE | 271+00.00 276+50.00 | RT | 164 |
| MAINLINE | 276+50.00 305+16.00 | LT | 624 |
| MAINLINE | 276+50.00 305+16.00 | RT | 617 |
| MAINLINE | 305+16.00 323+82.00 | LT | 553 |
| MAINLINE | 305+16.00 323+82.00 | RT | 371 |
| MAINLINE | 323+82.00 325+50.00 | LT | 37 |
| MAINLINE | 323+82.00 325+50.00 | RT | 37 |
| MAINLINE | 325+50.00 329+00.00 | LT | 97 |
| MAINLINE | 325+50.00 329+00.00 | RT | 96 |
| MAINLINE | 329+00.00 338+00.00 | LT | 183 |
| MAINLINE | 329+00.00 338+00.00 | RT | 207 |
| MAINLINE | 352+32.45 408+30.00 | LT | 1,160 |
| MAINLINE | 352+32.45 408+30.00 | RT | 1,172 |
| MAINLINE | 420+96.08 427+23.00 | LT | 588 |
| MAINLINE | 419+83.60 426+44.01 | RT | 636 |
| MAINLINE | 502+08.00 520+87.00 | LT | 762 |
| MAINLINE | 502+08.00 520+87.00 | RT | 798 |
| MAINLINE | 529+00.00 529+45.00 | LT | 35 |
| MAINLINE | 529+79.00 531+30.00 | LT | 138 |
| MAINLINE | 532+49.00 535+00.00 | LT | 230 |
| MAINLINE | 528+67.38 531+18.00 | RT | 230 |
| MAINLINE | 532+07.00 533+90.00 | RT | 168 |
| 1775TH ST | 25+52.80 26+88.00 | LT | 68 |
| 1775TH ST | 27+30.00 28+73.27 | LT | 110 |
| 1775TH ST | 25+52.80 26+88.00 | RT | 99 |
| 1775TH ST | 27+30.00 28+73.72 | RT | 72 |
| TOTAL | | | 15,067 |

REMOVE EXISTING CULVERTS

| STATION | OFFSET | EACH | |
|---------------|--------|------|----|
| 149+43.00 | RT | 1 | |
| 162+18.00 | RT | 1 | |
| 166+41.48 | RT | 1 | |
| 167+14.84 | RT | 1 | |
| 167+55.12 | LT | 1 | |
| 169+10.07 | LT | 1 | |
| 169+48.15 | RT | 1 | |
| 170+85.52 | LT | 1 | |
| 171+36.03 | LT | 1 | |
| 173+30.61 | LT | 1 | |
| 173+43.41 | RT | 1 | |
| 174+12.05 | RT | 1 | |
| 175+00.00 | RT | 1 | |
| 177+48.07 | LT | 1 | |
| 207+54.79 | LT | 1 | |
| 232+24.82 | LT | 1 | |
| 241+38.05 | RT | 1 | |
| 245+66.63 | LT | 1 | |
| 255+05.66 | RT | 1 | |
| 255+18.21 | LT | 1 | |
| 260+98.61 | LT | 1 | |
| 271+04.65 | LT | 1 | |
| 287+77.81 | RT | 1 | |
| 290+92.71 | LT | 1 | |
| 291+08.92 | RT | 1 | |
| 334+26.06 | LT | 1 | |
| 338+16.38 | LT | 1 | |
| 339+31.87 | LT | 1 | |
| 364+87.40 | RT | 1 | |
| 364+88.74 | LT | 1 | |
| 1250TH AVE | RT | 1 | |
| 395+64.47 | LT | 1 | |
| 395+68.60 | RT | 1 | |
| 1950TH STREET | RT | 1 | |
| 1950TH STREET | RT | 1 | |
| 470+39.00 | LT | 1 | |
| 470+73.85 | RT | 1 | |
| 490+69.66 | LT | 1 | |
| 491+01.27 | RT | 1 | |
| 512+37.00 | RT | 1 | |
| 2100TH STREET | RT | 1 | |
| 578+96.90 | LT | 1 | |
| 578+97.21 | RT | 1 | |
| NORB AVE | LT | 1 | |
| 594+26.38 | LT | 1 | |
| 594+48.72 | RT | 1 | |
| 605+50.93 | LT | 1 | |
| 608+92.53 | LT | 1 | |
| 611+33.26 | LT | 1 | |
| 612+69.59 | LT | 1 | |
| VIOLA STREET | LT | 1 | |
| 645+03.33 | LT | 1 | |
| 645+88.26 | LT | 1 | |
| 675+22.53 | RT | 1 | |
| 680+90.28 | RT | 1 | |
| 683+21.00 | RT | 1 | |
| 683+88.00 | RT | 1 | |
| 51+76.00 | - | 1 | |
| 53+22.00 | RT | 1 | |
| 55+58.09 | LT | 1 | |
| 101+99.22 | LT | 1 | |
| TOTAL | | | 61 |

BITUMINOUS BASE COURSE, SUPERPAVE, 8"

| LOCATION | STATION TO STATION | SQ YD |
|---------------|--------------------|-------|
| 2100TH ST | 85+17.68 89+22.48 | 1,079 |
| 2100TH ST | 90+77.81 91+80.57 | 274 |
| 1100TH ST | 50+20.00 51+30.14 | 220 |
| REL 1100TH ST | 80+84.03 82+83.81 | 533 |
| TOTAL | | 2,106 |

(SEE ENTRANCE SCHEDULE FOR REMAINING QUANTITIES)

BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50

| LOCATION | STATION TO STATION | OFFSET | TON |
|---------------|--------------------|--------|-----|
| 2100TH ST | 84+28.01 89+22.48 | LT&RT | 148 |
| 2100TH ST | 90+77.81 92+61.55 | LT&RT | 73 |
| 1100TH ST | 50+20.00 51+30.14 | LT&RT | 25 |
| REL 1100TH ST | 80+84.03 82+83.81 | LT&RT | 60 |
| TOTAL | | | 306 |

(SEE ENTRANCE SCHEDULE FOR REMAINING QUANTITIES)

BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70

| LOCATION | STATION TO STATION | OFFSET | TONS |
|-----------|---------------------|--------|--------|
| MAINLINE | 142+00.00 145+44.00 | LT&RT | 103 |
| MAINLINE | 147+81.00 151+97.96 | LT&RT | 140 |
| MAINLINE | 151+97.96 160+00.00 | LT&RT | 180 |
| MAINLINE | 160+00.00 180+60.00 | LT&RT | 500 |
| MAINLINE | 180+60.00 187+00.00 | LT | 78 |
| MAINLINE | 180+60.00 187+50.00 | RT | 84 |
| MAINLINE | 187+00.00 197+20.00 | LT | 114 |
| MAINLINE | 187+50.00 197+20.00 | RT | 109 |
| MAINLINE | 197+20.00 218+50.00 | LT&RT | 519 |
| MAINLINE | 218+50.00 223+00.00 | LT&RT | 101 |
| MAINLINE | 223+00.00 244+50.00 | LT&RT | 522 |
| MAINLINE | 244+50.00 249+50.00 | LT&RT | 112 |
| MAINLINE | 249+50.00 271+00.00 | LT&RT | 522 |
| MAINLINE | 271+00.00 276+50.00 | LT&RT | 123 |
| MAINLINE | 276+50.00 305+16.00 | LT&RT | 695 |
| MAINLINE | 305+16.00 323+82.00 | LT&RT | 669 |
| MAINLINE | 323+82.00 325+50.00 | LT&RT | 41 |
| MAINLINE | 325+50.00 329+00.00 | LT&RT | 78 |
| MAINLINE | 329+00.00 338+30.37 | LT&RT | 226 |
| MAINLINE | 338+30.37 352+32.45 | LT&RT | 419 |
| MAINLINE | 352+32.45 355+20.31 | LT&RT | 70 |
| MAINLINE | 357+15.38 408+30.00 | LT&RT | 1,241 |
| MAINLINE | 408+30.00 464+90.00 | LT&RT | 1,647 |
| MAINLINE | 464+90.00 502+08.00 | LT&RT | 1,110 |
| MAINLINE | 502+08.00 520+87.00 | LT&RT | 582 |
| MAINLINE | 520+87.00 529+00.00 | LT&RT | 243 |
| MAINLINE | 529+00.00 534+40.00 | LT&RT | 97 |
| MAINLINE | 534+40.00 604+90.52 | LT&RT | 2,106 |
| MAINLINE | 604+90.52 617+30.82 | LT&RT | 301 |
| MAINLINE | 617+30.82 621+24.00 | LT&RT | 95 |
| MAINLINE | 643+15.00 689+81.63 | LT&RT | 1,380 |
| 1775TH ST | 24+84.90 26+30.75 | LT&RT | 47 |
| TOTAL | | | 14,254 |

(SEE ENTRANCE SCHEDULE FOR REMAINING QUANTITIES)

BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19.0, N70

| LOCATION | STATION TO STATION | TONS |
|----------|---------------------|-------|
| MAINLINE | 147+81.00 151+97.96 | 1,213 |
| MAINLINE | 218+50.00 223+00.00 | 242 |
| MAINLINE | 244+50.00 249+50.00 | 278 |
| MAINLINE | 271+00.00 276+50.00 | 304 |
| MAINLINE | 305+96.00 314+45.17 | 907 |
| MAINLINE | 314+13.07 317+50.00 | 1,142 |
| MAINLINE | 325+50.00 329+00.00 | 73 |
| MAINLINE | 502+08.00 520+87.00 | 3,010 |
| MAINLINE | 529+00.00 531+30.28 | 70 |
| MAINLINE | 532+37.00 534+40.00 | 84 |
| 1775TH N | 27+13.00 28+73.27 | 617 |
| 1775TH S | 24+84.80 26+30.75 | 79 |
| TOTAL | | 8,019 |

LEVELING BINDER (MACHINE METHOD), N70

| LOCATION | STATION TO STATION | TONS |
|----------|---------------------|-------|
| MAINLINE | 142+00.00 145+44.00 | 42 |
| MAINLINE | 151+97.96 156+00.00 | 94 |
| MAINLINE | 156+00.00 160+00.00 | 45 |
| MAINLINE | 160+00.00 180+60.00 | 250 |
| MAINLINE | 180+60.00 187+00.00 | 39 |
| MAINLINE | 180+60.00 187+50.00 | 42 |
| MAINLINE | 187+00.00 197+20.00 | 57 |
| MAINLINE | 187+50.00 197+20.00 | 54 |
| MAINLINE | 197+20.00 209+78.77 | 153 |
| MAINLINE | 209+67.66 218+50.00 | 107 |
| MAINLINE | 223+00.00 244+50.00 | 261 |
| MAINLINE | 249+50.00 271+00.00 | 261 |
| MAINLINE | 276+50.00 305+16.00 | 348 |
| MAINLINE | 305+16.00 308+50.00 | 49 |
| MAINLINE | 317+50.00 323+82.00 | 104 |
| MAINLINE | 323+82.00 325+50.00 | 20 |
| MAINLINE | 329+00.00 338+30.37 | 113 |
| MAINLINE | 338+30.37 352+32.45 | 209 |
| MAINLINE | 352+32.45 355+20.31 | 35 |
| MAINLINE | 357+15.38 408+30.00 | 621 |
| MAINLINE | 408+30.00 412+78.37 | 67 |
| MAINLINE | 412+74.63 422+94.33 | 152 |
| MAINLINE | 424+45.67 464+90.00 | 604 |
| MAINLINE | 464+90.00 504+50.00 | 586 |
| MAINLINE | 513+50.00 520+87.00 | 112 |
| MAINLINE | 520+87.00 529+00.00 | 121 |
| MAINLINE | 534+40.00 604+90.52 | 1,053 |
| MAINLINE | 604+90.52 617+30.82 | 150 |
| MAINLINE | 617+30.82 621+24.00 | 48 |
| MAINLINE | 643+15.00 687+28.06 | 659 |
| MAINLINE | 687+23.01 689+81.63 | 31 |
| 1775TH S | 27+13.00 28+73.27 | 52 |
| 1775TH N | 24+84.80 26+30.75 | 17 |
| TOTAL | | 6,556 |

SECTION CORNERS

| DESCRIPTION | STATION | OFFSET |
|---|-----------|------------|
| IRON PIN (S. 1/4 CORNER OF SECT. 27, T. 8 N., R. 6 E.) | 195+65.34 | 238.83' RT |
| IRON PIN (S.E. CORNER OF SECT. 27, T. 8 N., R. 6 E.) | 221+25.69 | 31.00' RT |
| IRON PIN (S. 1/4 CORNER OF SECT. 26, T. 8 N., R. 6 E.) | 247+76.42 | 14.29' RT |
| P.K. NAIL (S.E. CORNER OF SECT. 26, T. 8 N., R. 6 E.) | 274+33.05 | 0.00' |
| MAG NAIL (S. 1/4 CORNER OF SECT. 25, T. 8 N., R. 6 E.) | 301+08.17 | 16.67' RT |
| P.K. NAIL (S.W. CORNER OF SECT. 30, T. 8 N., R. 7 E.) | 327+50.43 | 0.00' |
| IRON PIN (S.E. CORNER OF SECT. 25, T. 8 N., R. 6 E.) | 327+50.49 | 37.75' RT |
| P.K. NAIL (S. 1/4 CORNER OF SECT. 30, T. 8 N., R. 7 E.) | 338+53.62 | 0.00' |
| MAG NAIL (S.E. CORNER OF SECT. 4, T. 7 N., R. 7 E.) | 509+41.83 | 128.18' RT |
| IRON PIN (E. 1/4 CORNER OF SECT. 10, T. 7 N., R. 7 E.) | 570+35.39 | 66.29' RT |
| TOTAL | | 10 EACH |

TREE SCHEDULE

| DESCRIPTION | UNIT (EACH) |
|--|-------------|
| TREE, ACER PLATANOIDES COLUMNARE, (COLUMNAR NORWAY MAPLE), 2" CALIPER BALLED AND BURLAPPED | 22 |
| TREE, AMELANCHIER LAEVIS (ALLEGHENY SERVICEBERRY), 5' HEIGHT, CLUMP FORM, BALLED AND BURLAPPED | 23 |
| TREE, CERCIIS CANADENSIS (EASTERN REDBUD) 2" CALIPER, TREE FORM, BALLED AND BURLAPPED | 22 |
| SHRUB, FORSYTHIA NORTHERN SUN (NORTHERN SUN BORDER FORSYTHIA), 3' HEIGHT, CONTAINER | 75 |
| EVERGREEN, PICEA ABIES (NORWAY SPRUCE), 6' HEIGHT, BALLED AND BURLAPPED | 22 |

(SEE GENERAL NOTE #34 FOR ADDITIONAL INFORMATION)

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
|-----------|------|---------------------------------------|
| NAME | DATE | |
| | | SCHEDULE OF QUANTITIES SHEET 4 OF 4 |
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| | | |

DATE 8/03

DRAWN BY MLO
CHECKED BY SJK

* (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2
 CONTRACT NO. 94356

SEEDING

| STATION TO | STATION | SEEDING CLASS 2A | NITROGEN FERTILIZER | PHOSPHOROUS FERTILIZER | POTASSIUM FERTILIZER | AGRICULTURAL GROUND LIMESTONE | MULCH METHOD 2 | TEMPORARY EROSION CONTROL SEEDING |
|------------|-----------|------------------|---------------------|------------------------|----------------------|-------------------------------|----------------|-----------------------------------|
| | | ACRE | POUND | POUND | POUND | TON | ACRE | POUND |
| 142+00.00 | 156+00.00 | 1.82 | 163.8 | 163.8 | 163.8 | 3.6 | 3.64 | 182 |
| 156+00.00 | 170+00.00 | 1.55 | 139.5 | 139.5 | 139.5 | 3.1 | 3.10 | 155 |
| 170+00.00 | 180+60.00 | 1.63 | 146.7 | 146.7 | 146.7 | 3.3 | 3.26 | 163 |
| 180+60.00 | 182+00.00 | 0.25 | 22.5 | 22.5 | 22.5 | 0.50 | 0.50 | 25 |
| 182+00.00 | 194+00.00 | 2.09 | 188.1 | 188.1 | 188.1 | 4.18 | 4.18 | 209 |
| 194+00.00 | 206+00.00 | 1.35 | 121.5 | 121.5 | 121.5 | 2.70 | 2.70 | 135 |
| 206+00.00 | 209+78.77 | 0.23 | 20.7 | 20.7 | 20.7 | 0.46 | 0.46 | 23 |
| 209+78.77 | 217+00.00 | 0.54 | 48.6 | 48.6 | 48.6 | 1.08 | 1.08 | 54 |
| 217+00.00 | 229+00.00 | 0.97 | 87.3 | 87.3 | 87.3 | 1.94 | 1.94 | 97 |
| 229+00.00 | 241+00.00 | 1.03 | 92.7 | 92.7 | 92.7 | 2.06 | 2.06 | 103 |
| 241+00.00 | 253+00.00 | 1.63 | 146.7 | 146.7 | 146.7 | 3.26 | 3.26 | 163 |
| 253+00.00 | 265+00.00 | 0.93 | 83.7 | 83.7 | 83.7 | 1.86 | 1.86 | 93 |
| 265+00.00 | 277+00.00 | 1.03 | 92.7 | 92.7 | 92.7 | 2.06 | 2.06 | 103 |
| 277+00.00 | 289+00.00 | 0.97 | 87.3 | 87.3 | 87.3 | 1.94 | 1.94 | 97 |
| 289+00.00 | 301+00.00 | 1.20 | 108.0 | 108.0 | 108.0 | 2.40 | 2.40 | 120 |
| 301+00.00 | 313+00.00 | 1.87 | 168.3 | 168.3 | 168.3 | 3.74 | 3.74 | 187 |
| 313+00.00 | 314+45.17 | 0.33 | 29.7 | 29.7 | 29.7 | 0.66 | 0.66 | 33 |
| 314+45.17 | 324+00.00 | 1.08 | 97.2 | 97.2 | 97.2 | 2.16 | 2.16 | 108 |
| 324+00.00 | 336+00.00 | 1.42 | 127.8 | 127.8 | 127.8 | 2.84 | 2.84 | 142 |
| 336+00.00 | 338+30.37 | 0.16 | 14.4 | 14.4 | 14.4 | 0.32 | 0.32 | 16 |
| 338+30.37 | 352+32.45 | 0.07 | 6.3 | 6.3 | 6.3 | 0.14 | 0.14 | 7 |
| 352+32.45 | 357+15.38 | 0.28 | 25.2 | 25.2 | 25.2 | 0.56 | 0.56 | 28 |
| 357+15.38 | 361+00.00 | 0.98 | 88.2 | 88.2 | 88.2 | 1.96 | 1.96 | 98 |
| 361+00.00 | 373+00.00 | 1.08 | 97.2 | 97.2 | 97.2 | 2.16 | 2.16 | 108 |
| 373+00.00 | 385+00.00 | 0.98 | 88.2 | 88.2 | 88.2 | 1.96 | 1.96 | 98 |
| 385+00.00 | 397+00.00 | 1.29 | 116.1 | 116.1 | 116.1 | 2.58 | 2.58 | 129 |
| 397+00.00 | 408+30.00 | 0.96 | 86.4 | 86.4 | 86.4 | 1.92 | 1.92 | 96 |
| 408+30.00 | 464+90.00 | 0.31 | 27.9 | 27.9 | 27.9 | 0.62 | 0.62 | 31 |
| 464+90.00 | 468+00.00 | 1.40 | 126.0 | 126.0 | 126.0 | 2.80 | 2.80 | 140 |
| 468+00.00 | 480+00.00 | 0.89 | 80.1 | 80.1 | 80.1 | 1.78 | 1.78 | 89 |
| 480+00.00 | 492+00.00 | 0.93 | 83.7 | 83.7 | 83.7 | 1.86 | 1.86 | 93 |
| 492+00.00 | 504+00.00 | 2.48 | 223.2 | 223.2 | 223.2 | 4.96 | 4.96 | 248 |
| 504+00.00 | 516+00.00 | 0.99 | 89.1 | 89.1 | 89.1 | 1.98 | 1.98 | 99 |
| 516+00.00 | 528+00.00 | 0.38 | 34.2 | 34.2 | 34.2 | 0.76 | 0.76 | 38 |
| 528+00.00 | 531+41.06 | 0.76 | 68.4 | 68.4 | 68.4 | 1.52 | 1.52 | 76 |
| 531+41.06 | 539+00.00 | 1.60 | 144.0 | 144.0 | 144.0 | 3.20 | 3.20 | 160 |
| 539+00.00 | 551+00.00 | 1.57 | 141.3 | 141.3 | 141.3 | 3.14 | 3.14 | 157 |
| 551+00.00 | 563+00.00 | 1.54 | 138.6 | 138.6 | 138.6 | 3.08 | 3.08 | 154 |
| 563+00.00 | 575+00.00 | 0.94 | 84.6 | 84.6 | 84.6 | 1.88 | 1.88 | 94 |
| 575+00.00 | 587+00.00 | 0.97 | 87.3 | 87.3 | 87.3 | 1.94 | 1.94 | 97 |
| 587+00.00 | 599+00.00 | 1.52 | 136.8 | 136.8 | 136.8 | 3.04 | 3.04 | 152 |
| 599+00.00 | 611+00.00 | 0.66 | 59.4 | 59.4 | 59.4 | 1.32 | 1.32 | 66 |
| 611+00.00 | 623+00.00 | 0.00 | 0.0 | 0.0 | 0.0 | 0.00 | 0.00 | 0 |
| 623+00.00 | 635+00.00 | 0.03 | 2.7 | 2.7 | 2.7 | 0.06 | 0.06 | 3 |
| 635+00.00 | 642+94.54 | 0.21 | 18.9 | 18.9 | 18.9 | 0.42 | 0.42 | 21 |
| 642+94.54 | 647+00.00 | 0.57 | 51.3 | 51.3 | 51.3 | 1.14 | 1.14 | 57 |
| 647+00.00 | 659+00.00 | 0.95 | 85.5 | 85.5 | 85.5 | 1.90 | 1.90 | 95 |
| 659+00.00 | 671+00.00 | 1.15 | 103.5 | 103.5 | 103.5 | 2.30 | 2.30 | 115 |
| 671+00.00 | 683+00.00 | 0.36 | 32.4 | 32.4 | 32.4 | 0.72 | 0.72 | 36 |
| 683+00.00 | 687+73.01 | 0.30 | 27.0 | 27.0 | 27.0 | 0.60 | 0.60 | 30 |
| TOTAL | | 47.25 | 4,253 | 4,253 | 4,253 | 94.5 | 47.25 | 4,725 |

REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

| LOCATION | STATION TO | STATION | CU YD |
|-----------|------------|-----------|-------|
| MAINLINE | 305+00.00 | 324+00.00 | 1717 |
| 1775TH ST | 24+84.90 | 29+50.00 | 468 |
| TOTAL | | | 2,185 |

NOTE: UNSUITABLE MATERIALS SHALL BE DISPOSED OF OFF THE PROJECT SITE.

EARTHWORK

| LOCATION | STATION TO | STATION | EARTH EXCAVATION (CUT) | CHANNEL EXCAVATION (CUT) | *EXCAVATION ADJUSTED FOR SHRINKAGE | EMBANKMENT (FILL) | EARTHWORK BALANCE WASTE (+) SHORTAGE (-) FURNISHED EXCAVATION |
|----------------|------------|-----------|------------------------|--------------------------|------------------------------------|-------------------|---|
| | | | CU YD | CU YD | CU YD | CU YD | CU YD |
| MAINLINE | 142+00.00 | 180+60.00 | 12,555 | - | 9,416 | 12,189 | -2,773 |
| MAINLINE | 180+60.00 | 338+30.37 | 27,642 | - | 20,732 | 16,816 | 3,916 |
| MAINLINE | 352+32.45 | 413+00.00 | 7,090 | - | 5,318 | 3,307 | 2,011 |
| MAINLINE | 420+00.00 | 428+00.00 | 7 | - | 5 | 300 | -295 |
| MAINLINE | 464+90.00 | 502+00.00 | 4,749 | - | 3,562 | 2,428 | 1,134 |
| MAINLINE | 502+00.00 | 604+90.52 | 20,536 | - | 15,402 | 8,908 | 6,494 |
| MAINLINE | 604+90.52 | 617+00.00 | 2,001 | - | 1,501 | 1,132 | 369 |
| MAINLINE | 644+00.00 | 689+81.63 | 2,802 | - | 2,102 | 2,707 | -605 |
| GOBBLER | 1+53.29 | 1+68.95 | 6 | - | 5 | - | 5 |
| 1525TH ST | 3+33.00 | 6+15.00 | 534 | - | 400 | 193 | 207 |
| 1560TH ST | 8+00.00 | 8+50.00 | 170 | - | 128 | - | 128 |
| T.R. 109 | 108+46.95 | 112+68.19 | 174 | - | 131 | - | 131 |
| 1600TH ST | 11+71.00 | 13+73.85 | 15 | - | 11 | 70 | -59 |
| 1650TH ST | 15+50.00 | 17+92.47 | 70 | - | 53 | 86 | -33 |
| 1700TH ST | 18+45.00 | 20+74.40 | 63 | - | 47 | 110 | -63 |
| 1750TH ST | 21+30.00 | 23+94.90 | 92 | - | 69 | 238 | -169 |
| 1775TH ST | 24+84.90 | 29+50.00 | 516 | - | 387 | 680 | -293 |
| 1800TH ST | 126+20.00 | 128+88.17 | 91 | - | 68 | 79 | -11 |
| 1300TH AVE | 30+50.00 | 30+75.00 | 19 | - | 14 | - | 14 |
| 1900TH ST | 32+20.32 | 34+31.45 | 59 | - | 44 | 37 | 7 |
| 1250TH AVE | 35+07.88 | 37+16.83 | 41 | - | 31 | 19 | 12 |
| 1950TH ST | 38+17.36 | 40+34.17 | 113 | - | 85 | 40 | 45 |
| 1175TH AVE | 42+50.00 | 42+77.45 | 19 | - | 14 | 1 | 13 |
| 2000TH ST | 44+21.55 | 44+60.99 | 5 | - | 4 | 21 | -17 |
| 1100TH AVE | 50+20.00 | 56+36.83 | 349 | - | 262 | 290 | -28 |
| REL 1100TH AVE | 80+32.00 | 83+00.00 | 329 | - | 247 | 59 | 188 |
| 2100TH ST | 84+28.01 | 92+61.55 | 1,079 | - | 809 | 1,346 | -537 |
| 2200TH ST | 56+16.00 | 59+10.76 | 74 | - | 56 | 244 | -188 |
| NORB | 80+33.46 | 80+37.00 | - | - | - | - | - |
| FAYETTE | 60+50.00 | 60+85.00 | 14 | - | 11 | - | 11 |
| VIOLA | 63+50.00 | 64+04.15 | 31 | - | 23 | 6 | 17 |
| OAK | 67+50.00 | 68+01.87 | 31 | - | 23 | - | 23 |
| 1000TH AVE | 71+00.00 | 71+04.37 | 3 | - | 2 | - | 2 |
| 2400TH ST | 73+20.00 | 76+00.00 | 66 | - | 50 | 215 | -165 |
| EXISTING LANE | 116+00.00 | 120+50.00 | 180 | - | 135 | 27 | 108 |
| PROPOSED LANE | 107+32.14 | 113+85.80 | 85 | - | 64 | 239 | -175 |
| CHANNEL WORK | 800+12.00 | 802+87.14 | - | 1,425 | 1,069 | 829 | 240 |
| TOTAL | | | 81,610 | 1,425 | 62,280 | 52,616 | 9,664 |

* EARTH EXCAVATION MULTIPLIED BY (1-0.25=0.75) SHRINKAGE FACTOR

TOPSOIL

| LOCATION | STATION TO | STATION | 4" TOPSOIL EXCAVATION (CUT) | *EXCAVATION ADJUSTED FOR SHRINKAGE | 4" TOPSOIL PLACEMENT (FILL) | EARTHWORK BALANCE WASTE (+) SHORTAGE (-) FURNISHED EXCAVATION |
|----------------|------------|-----------|-----------------------------|------------------------------------|-----------------------------|---|
| | | | CU YD | CU YD | CU YD | CU YD |
| MAINLINE | 142+00.00 | 180+60.00 | 3,028 | 2,725 | 2,208 | 517 |
| MAINLINE | 180+60.00 | 338+30.37 | 10,635 | 9,572 | 7,314 | 2,258 |
| MAINLINE | 352+32.45 | 413+00.00 | 3,685 | 3,317 | 2,368 | 949 |
| MAINLINE | 420+00.00 | 428+00.00 | 166 | 149 | 90 | 59 |
| MAINLINE | 464+90.00 | 502+00.00 | 2,122 | 1,910 | 1,536 | 374 |
| MAINLINE | 502+00.00 | 604+90.52 | 6,973 | 6,276 | 5,194 | 1,082 |
| MAINLINE | 604+90.52 | 617+00.00 | 685 | 617 | 723 | -106 |
| MAINLINE | 644+00.00 | 689+81.63 | 1,825 | 1,643 | 1,213 | 430 |
| GOBBLER | 1+53.29 | 1+68.95 | 3 | 3 | 2 | 1 |
| 1525TH ST | 3+33.00 | 6+15.00 | 84 | 76 | 65 | 11 |
| 1560TH ST | 8+00.00 | 8+50.00 | 45 | 41 | 19 | 22 |
| T.R. 109 | 108+46.95 | 112+68.19 | 10 | 9 | 17 | -8 |
| 1600TH ST | 11+71.00 | 13+73.85 | 25 | 23 | 17 | 6 |
| 1650TH ST | 15+50.00 | 17+92.47 | 54 | 49 | 26 | 23 |
| 1700TH ST | 18+45.00 | 20+74.40 | 51 | 46 | 26 | 20 |
| 1750TH ST | 21+30.00 | 23+94.90 | 91 | 82 | 55 | 27 |
| 1775TH ST | 24+84.90 | 29+50.00 | 74 | 67 | 156 | -89 |
| 1800TH ST | 126+20.00 | 128+88.17 | 46 | 41 | 19 | 22 |
| 1300TH AVE | 30+50.00 | 30+75.00 | 3 | 3 | - | 3 |
| 1900TH ST | 32+20.32 | 34+31.45 | 32 | 29 | 18 | 11 |
| 1250TH AVE | 35+07.88 | 37+16.83 | 26 | 23 | 11 | 12 |
| 1950TH ST | 38+17.36 | 40+34.17 | 36 | 32 | 22 | 10 |
| 1175TH AVE | 42+50.00 | 42+77.45 | 3 | 3 | 1 | 2 |
| 2000TH ST | 44+21.55 | 44+60.99 | 6 | 5 | 3 | - |
| 1100TH AVE | 50+20.00 | 56+36.83 | 103 | 93 | 119 | -26 |
| REL 1100TH AVE | 80+32.00 | 83+00.00 | 147 | 132 | 92 | 40 |
| 2100TH ST | 84+28.01 | 92+61.55 | 528 | 475 | 401 | 74 |
| 2200TH ST | 56+16.00 | 59+10.76 | 75 | 68 | 31 | 37 |
| NORB | 80+33.46 | 80+37.00 | - | - | - | - |
| FAYETTE | 60+50.00 | 60+85.00 | 1 | 1 | 1 | - |
| VIOLA | 63+50.00 | 64+04.15 | 13 | 12 | 6 | 6 |
| OAK | 67+50.00 | 68+01.87 | 3 | 3 | - | 3 |
| 1000TH AVE | 71+00.00 | 71+04.37 | - | - | - | - |
| 2400TH ST | 73+20.00 | 76+00.00 | 67 | 60 | 35 | 25 |
| EXISTING LANE | 116+00.00 | 120+50.00 | 122 | 110 | 171 | -61 |
| PROPOSED LANE | 107+32.14 | 113+85.80 | 233 | 210 | 138 | 72 |
| TOTAL | | | 31,000 | 27,905 | 22,097 | 5,808 |

* SHRINKAGE FACTOR = 0.10

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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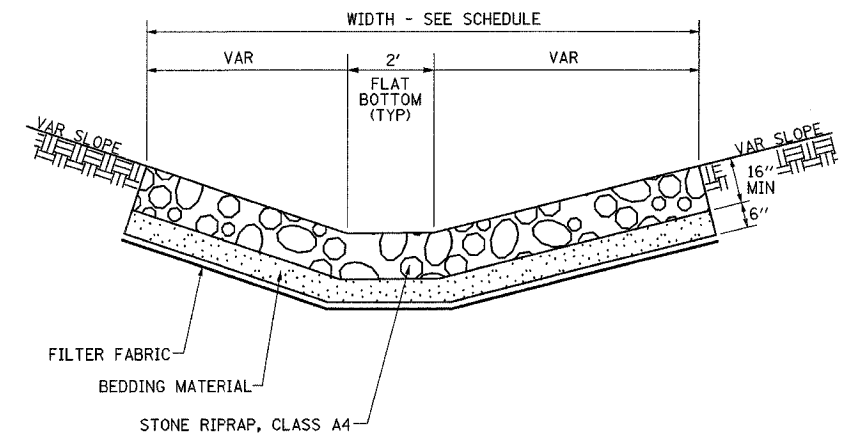
ILLINOIS DEPARTMENT OF TRANSPORTATION

SEEDING AND EARTHWORK SCHEDULE

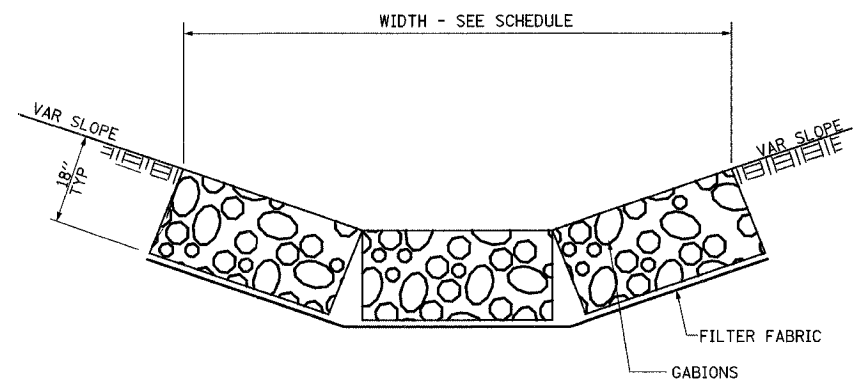
DRAWN BY _____
 CHECKED BY SJK

DATE 8/03

| DITCH SCHEDULE | | | | | | | | | |
|----------------|-----------------------|--------|--------|-------|-------------------------|-----------------------|---------|---------------|------|
| LOCATION | STATION TO STATION | OFFSET | LENGTH | WIDTH | EROSION CONTROL BLANKET | STONE RIPRAP CLASS A4 | GABIONS | FILTER FABRIC | |
| | | | FEET | FEET | SQ YD | SQ YD | CU YD | SQ YD | |
| MAINLINE | 150+46.00 - 151+00.00 | LT | 54 | 9 | - | - | 27 | 54 | |
| MAINLINE | 149+50.00 - 150+00.00 | RT | 50 | 13 | - | - | 36 | 72 | |
| MAINLINE | 150+00.00 - 151+42.44 | RT | 166 | 9 | - | 166 | - | 166 | |
| MAINLINE | 151+00.00 - 152+00.00 | LT | 100 | 9 | 100 | - | - | - | |
| MAINLINE | 151+87.00 - 157+00.00 | RT | 513 | 9 | - | 513 | - | 513 | |
| MAINLINE | 153+00.00 - 155+00.00 | LT | 200 | 9 | - | 200 | - | 200 | |
| MAINLINE | 155+00.00 - 156+00.00 | LT | 100 | 9 | - | - | 50 | 100 | |
| MAINLINE | 156+00.00 - 156+49.00 | LT | 49 | 9 | 49 | - | - | - | |
| MAINLINE | 156+98.00 - 158+00.00 | LT | 102 | 9 | 102 | - | - | - | |
| MAINLINE | 162+73.00 - 162+70.00 | LT | 15 | 10 | - | 17 | - | 17 | |
| MAINLINE | 172+50.00 - 173+15.00 | LT | 65 | 9 | 65 | - | - | - | |
| MAINLINE | 173+44.00 - 174+31.00 | LT | 87 | 9 | - | - | 44 | 87 | |
| MAINLINE | 174+39.00 - 175+29.00 | LT | 90 | 9 | - | - | 45 | 90 | |
| MAINLINE | 175+40.00 - 176+94.00 | LT | 154 | 9 | 154 | - | - | - | |
| MAINLINE | 180+65.00 - 182+05.00 | LT | 140 | 9 | 140 | - | - | - | |
| MAINLINE | 198+42.00 - 198+53.00 | RT | 11 | 9 | - | 11 | - | 11 | |
| MAINLINE | 198+53.00 - 199+00.00 | RT | 47 | 9 | 47 | - | - | - | |
| MAINLINE | 203+98.00 - 204+04.00 | RT | 6 | 9 | - | - | 3 | 6 | |
| MAINLINE | 215+78.00 - 216+00.00 | RT | 22 | 9 | 22 | - | - | - | |
| MAINLINE | 238+00.00 - 239+00.00 | RT | 101 | 9 | - | - | 51 | 101 | |
| MAINLINE | 238+40.00 - 239+00.00 | LT | 60 | 9 | - | 60 | - | 60 | |
| MAINLINE | 268+48.00 - 268+75.00 | LT | 27 | 9 | - | - | 14 | 27 | |
| MAINLINE | 268+48.00 - 268+75.00 | RT | 27 | 9 | - | - | 14 | 27 | |
| MAINLINE | 268+93.00 - 269+02.00 | RT | 9 | 9 | - | - | 5 | 9 | |
| MAINLINE | 268+93.00 - 269+25.00 | LT | 32 | 9 | - | 32 | - | 32 | |
| MAINLINE | 315+00.00 - 316+00.00 | LT | 100 | 9 | - | 100 | - | 100 | |
| MAINLINE | 315+00.00 - 316+00.00 | RT | 100 | 9 | 100 | - | - | - | |
| MAINLINE | 329+00.00 - 329+28.00 | RT | 28 | 9 | - | 28 | - | 28 | |
| MAINLINE | 329+17.00 - 330+00.00 | LT | 83 | 9 | - | 83 | - | 83 | |
| MAINLINE | 334+52.00 - 334+89.00 | LT | 37 | 9 | 37 | - | - | - | |
| MAINLINE | 334+80.00 - 335+00.00 | RT | 20 | 9 | - | - | 10 | 20 | |
| MAINLINE | 334+89.00 - 335+25.00 | LT | 36 | 9 | - | - | 18 | 36 | |
| MAINLINE | 357+65.00 - 358+00.00 | LT | 35 | 9 | - | 35 | - | 35 | |
| MAINLINE | 357+80.00 - 357+95.00 | RT | 15 | 9 | - | - | 8 | 15 | |
| MAINLINE | 372+00.00 - 372+65.00 | RT | 65 | 9 | - | 65 | - | 65 | |
| MAINLINE | 372+96.00 - 374+00.00 | LT | 104 | 9 | - | 104 | - | 104 | |
| MAINLINE | 509+75.00 - 510+00.00 | RT | 25 | 9 | 25 | - | - | - | |
| 1525TH ST | 5+27.00 - 6+15.31 | RT | 88 | 29 | 284 | - | - | - | |
| 1525TH ST | 5+54.00 - 6+15.31 | LT | 61 | 31 | 210 | - | - | - | |
| 2100TH ST | 87+82.00 - 88+14.00 | RT | 32 | 10 | - | 36 | - | 36 | |
| 2100TH ST | 88+62.00 - 88+92.00 | RT | 23 | 10 | - | 26 | - | 26 | |
| TOTALS: | | | | | | 1335 | 1476 | 325 | 2120 |



DETAIL OF STONE RIPRAP, CLASS A4



DETAIL OF GABIONS
(ASSUMED GABION BASKET SIZE OF 3'Wx6'Lx18\"/>

| REVISIONS | |
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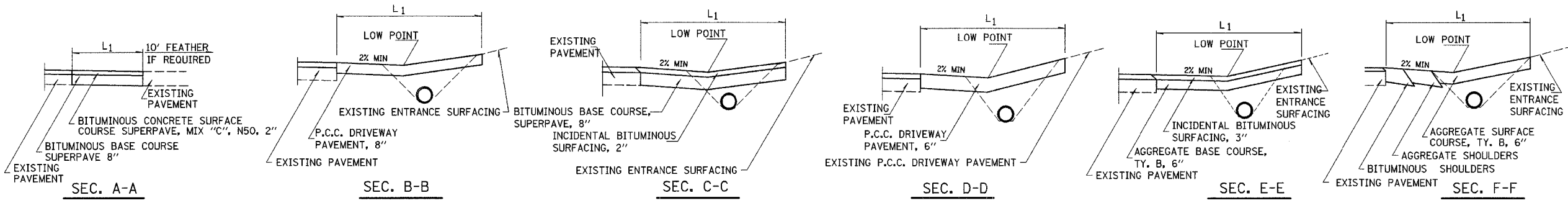
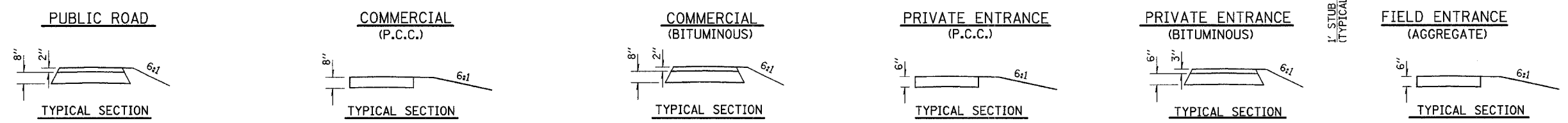
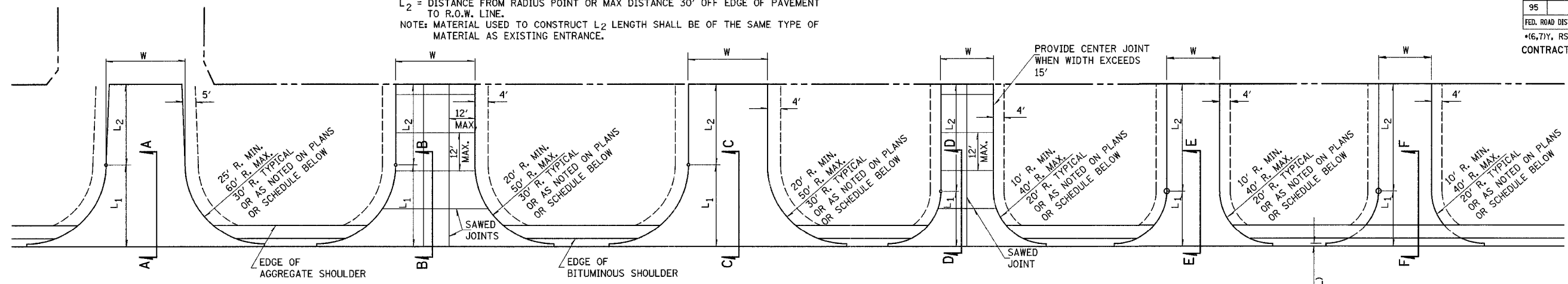
ILLINOIS DEPARTMENT OF TRANSPORTATION

DITCH RIPRAP SCHEDULE AND DETAILS

DATE 9/01

DRAWN BY MLO
CHECKED BY TGC

L₁ = DISTANCE FROM EDGE OF PAVEMENT TO RADIUS POINT OR MAX. DISTANCE 30'
L₂ = DISTANCE FROM RADIUS POINT OR MAX. DISTANCE 30' OFF EDGE OF PAVEMENT TO R.O.W. LINE.
NOTE: MATERIAL USED TO CONSTRUCT L₂ LENGTH SHALL BE OF THE SAME TYPE OF MATERIAL AS EXISTING ENTRANCE.



ENTRANCE SCHEDULE

| TYPE | SIDE/STATION | WIDTH | LENGTH | | RADI | AGGREGATE BASE COURSE, TYPE B | HOT MIX ASPHALT BASE COURSE, 8" | AGGREGATE SURFACE COURSE TYPE B | INCIDENTAL BITUMINOUS SURFACING | P.C.C. DRIVEWAY PAVEMENT | | TEMPORARY ACCESS (PE) | TEMPORARY ACCESS (CE) |
|---------|--------------|-------|----------------|----------------|------|-------------------------------|---------------------------------|---------------------------------|---------------------------------|--------------------------|----|-----------------------|-----------------------|
| | | | L ₁ | L ₂ | | | | | | 6" | 8" | | |
| AGG FE | RT/150+29.75 | 12' | 20' | 95' | 20 | | | 65 | | | | 1 | |
| AGG FE | LT/150+34.63 | 12' | 20' | 94' | 20 | | | 49 | | | | 1 | |
| AGG PE | RT/151+65.27 | 12' | 20' | 10' | 20 | 16 | | 5 | 8 | | | 1 | |
| AGG PE | LT/156+73.10 | 12' | 20' | 21' | 20 | 16 | | 10 | 8 | | | 1 | |
| BIT PE | LT/160+11.89 | 12' | 20' | 5' | 20 | 18 | | | 9 | | | 1 | |
| AGG PE | LT/165+73.05 | 13' | 20' | 3' | 20 | 16 | | 1 | 8 | | | 1 | |
| AGG PE | RT/165+45.57 | 12' | 20' | 58' | 20 | 16 | | 26 | 8 | | | 1 | |
| CONC PE | RT/167+14.84 | 12' | 22' | 16' | 20 | | | | 70 | | | 1 | |
| AGG PE | LT/167+55.12 | 15' | 20' | 2' | 20 | 18 | | 1 | 9 | | | 1 | |
| AGG PE | LT/169+10.07 | 12' | 20' | 21' | 20 | 16 | | 10 | 8 | | | 1 | |
| AGG PE | RT/169+48.15 | 16' | 20' | 10' | 20 | 19 | | 6 | 9 | | | 1 | |
| AGG PE | LT/170+85.52 | 18' | 20' | 14' | 20 | 20 | | 10 | 10 | | | 1 | |
| AGG PE | LT/171+36.03 | 17' | 20' | 19' | 20 | 19 | | 12 | 10 | | | 1 | |
| AGG PE | LT/173+30.61 | 12' | 20' | 18' | 20 | 16 | | 8 | 8 | | | 1 | |
| AGG CE | RT/173+43.41 | 35' | 20' | 8' | 20 | | 97 | 11 | 11 | | | 1 | 1 |
| AGG PE | RT/174+12.05 | 12' | 20' | 8' | 20 | 16 | | 4 | 8 | | | 1 | |

ENTRANCE SCHEDULE

| TYPE | SIDE/STATION | WIDTH | LENGTH | | RADI | AGGREGATE BASE COURSE, TYPE B | HOT MIX ASPHALT BASE COURSE, 8" | AGGREGATE SURFACE COURSE TYPE B | INCIDENTAL BITUMINOUS SURFACING | P.C.C. DRIVEWAY PAVEMENT | | TEMPORARY ACCESS (PE) | TEMPORARY ACCESS (CE) |
|----------|--------------|---------|----------------|----------------|------|-------------------------------|---------------------------------|---------------------------------|---------------------------------|--------------------------|----|-----------------------|-----------------------|
| | | | L ₁ | L ₂ | | | | | | 6" | 8" | | |
| AGG PE | LT/174+35.96 | 12' | 20' | 12' | 20 | 16 | | 5 | 8 | | | 1 | |
| AGG PE | LT/175+35.51 | 12' | 20' | 20' | 20 | 16 | | 9 | 8 | | | 1 | |
| CONC PE | LT/177+48.07 | 12' | 20' | 30' | 20 | | | | 46 | | | 1 | |
| AGG PE | RT/179+49.90 | 12' | 25' | 23' | 20 | 18 | | 10 | 16 | | | 1 | |
| CONC CE | RT/194+25.72 | 35' | 20' | 11' | 20 | | | | | 140 | | 1 | 1 |
| AGG FE | LT/194+26.00 | 12' | 20' | 25' | 20 | | | 18 | | | | 1 | |
| BIT CE | LT/201+34.68 | 2 @ 24' | 24' | - | 100 | | 264 | | 30 | | | 1 | 1 |
| BIT CE | LT/207+58.00 | 30' | 20' | - | 20 | | 86 | | 70 | | | 1 | 1 |
| AGG PE | LT/232+24.82 | 18' | 20' | 4' | 20 | 20 | | 3 | 10 | | | 1 | |
| CONC CE | LT/245+66.63 | 35' | 42' | - | 50 | | | | | 276 | | 1 | 1 |
| AGG PE | LT/255+18.21 | 24' | 20' | 4' | 20 | 25 | | 4 | 12 | | | 1 | |
| AGG FE | LT/261+04.61 | 24' | 20' | 7' | 20 | | | 18 | | | | 1 | |
| AGG FE | LT/271+04.65 | 12' | 20' | 7' | 20 | | | 10 | | | | 1 | |
| AGG PE | RT/287+77.81 | 12' | 20' | 17' | 20 | 16 | | 8 | 8 | | | 1 | |
| AGG FE | LT/290+92.71 | 12' | 20' | 5' | 20 | | | 9 | | | | 1 | |
| EARTH FE | RT/291+08.92 | 12' | 20' | 10' | 20 | | | 7 | | | | 1 | |
| EARTH FE | LT/305+71.85 | 12' | 20' | 15' | 20 | | | 7 | | | | 1 | |

- ① SEE PLAN AND PROFILE SHEETS FOR RADI DETAILS.
- ② SEE STANDARD 406201 FOR DETAILS.

| REVISIONS | |
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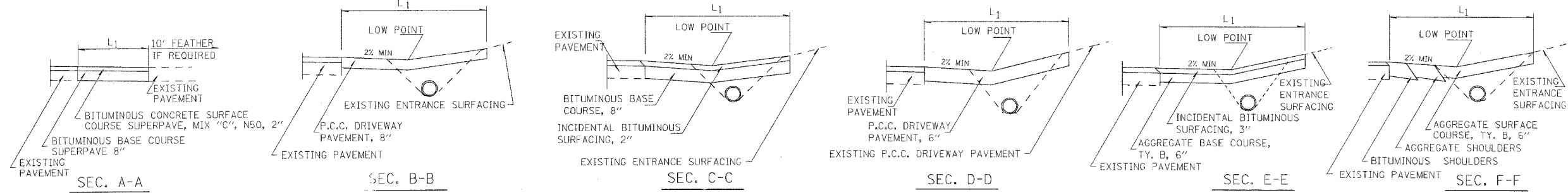
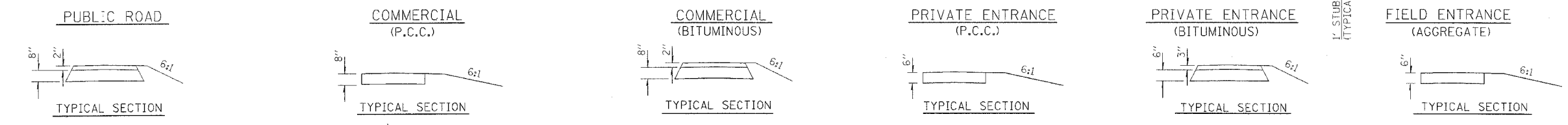
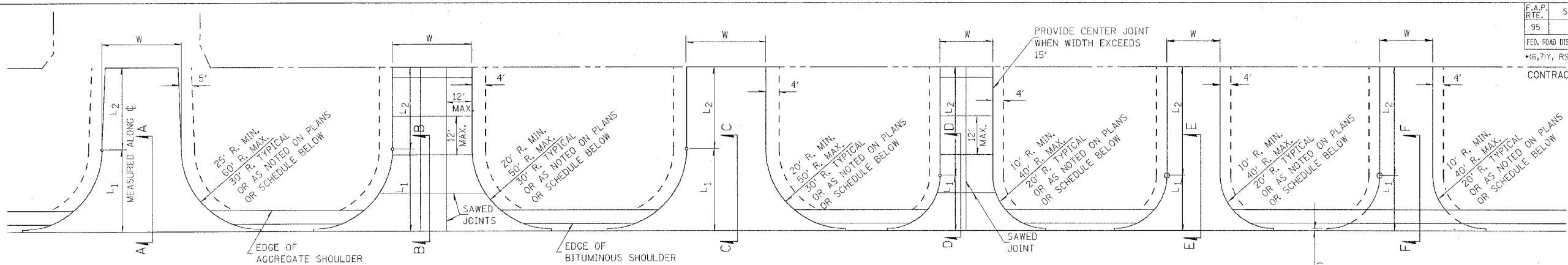
ILLINOIS DEPARTMENT OF TRANSPORTATION

**RURAL ENTRANCE
DETAILS & SCHEDULES
SHEET 1 OF 3**

SCALE: _____ DRAWN BY ASB
DATE 06/02 CHECKED BY SJK

DGN-SPEC
DATE

| | | | | |
|---------------------------------|---------------------------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 20 |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| *6,7Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

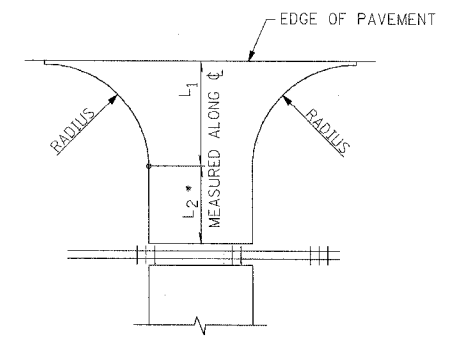


PUBLIC ROAD SCHEDULE

| O&C | PR | SIDE/STATION | WIDTH | LENGTH | | RADI | HOT MIX ASPHALT BASE COURSE, 8" | BITUMINOUS MATERIALS (PRIME COAT) | HOT MIX ASPHALT SURFACE COURSE MIX "C", N50 | HOT MIX ASPHALT SURFACE COURSE SUPERPAVE MIX "D", N70 | TEMPORARY ACCESS (ROAD) |
|-----|----|--------------|-------|--------|------|------|---------------------------------|-----------------------------------|---|---|-------------------------|
| | | | | L1 | L2 | | | | | | |
| O&C | PR | 1525TH ST N | 24' | 48' | 54' | ① | 304 | 30 | 34 | | 1 |
| O&C | PR | 1525TH ST S | 24' | 48' | 106' | ① | 353 | 35 | 40 | | 1 |
| AGG | PR | 1560TH ST | 18' | 30' | - | 30 | 103 | 10 | 12 | | 1 |
| AGG | PR | T.R. 109 | 18' | 20' | - | ① | 71 | 7 | 8 | | 1 |
| O&C | PR | 1600TH ST N | 24' | 30' | 32' | 30 | 198 | 20 | 22 | | 1 |
| BIT | PR | 1600TH ST S | 24' | 30' | - | 30 | 124 | 12 | 14 | | 1 |
| O&C | PR | 1650TH ST N | 24' | 30' | 50' | 30 | 224 | 22 | 25 | | 1 |
| BIT | PR | 1650TH ST S | 24' | 30' | - | 30 | 119 | 12 | 13 | | 1 |
| O&C | PR | 1700TH ST N | 24' | 30' | 32' | 30 | 197 | 20 | 22 | | 1 |
| BIT | PR | 1700TH ST S | 24' | 30' | 4' | 30 | 134 | 13 | 15 | | 1 |
| O&C | PR | 1750TH ST N | 24' | 30' | 53' | 30 | 230 | 23 | 26 | | 1 |
| O&C | PR | 1750TH ST S | 24' | 30' | 11' | 30 | 154 | 15 | 17 | | 1 |
| BIT | PR | 1775TH ST S | 24' | 49' | - | ① | 440 | 249 | 49 | | 1 |
| BIT | PR | 1775TH ST N | 24' | 144' | - | ① | ③ | 44 | - | 93 | |
| BIT | PR | 1800TH ST N | 24' | 30' | 46' | 30 | 217 | 22 | 24 | | 1 |
| BIT | PR | 1800TH ST S | 24' | 30' | 24' | 30 | 190 | 19 | 21 | | 1 |
| BIT | PR | 1300TH AVE | 24' | 43' | 20' | 30 | 214 | 21 | 24 | | 1 |
| O&C | PR | 1900TH ST N | 24' | 62' | 52' | ① | 351 | 35 | 39 | | 1 |

PUBLIC ROAD SCHEDULE

| O&C | PR | SIDE/STATION | WIDTH | LENGTH | | RADI | HOT MIX ASPHALT BASE COURSE, 8" | BITUMINOUS MATERIALS (PRIME COAT) | HOT MIX ASPHALT SURFACE COURSE MIX "C", N50 | HOT MIX ASPHALT SURFACE COURSE SUPERPAVE MIX "D", N70 | TEMPORARY ACCESS (ROAD) |
|-----|----|-----------------|-------|--------|-----|------|---------------------------------|-----------------------------------|---|---|-------------------------|
| | | | | L1 | L2 | | | | | | |
| O&C | PR | 1900TH ST S | 24' | 45' | 21' | ① | 275 | 28 | 31 | | 1 |
| O&C | PR | 1250TH AVE E | 24' | 44' | 56' | ① | 265 | 26 | 30 | | 1 |
| O&C | PR | 1950TH ST N | 24' | 70' | 49' | ① | 415 | 41 | 46 | | 1 |
| O&C | PR | 1950TH ST S | 24' | 46' | 24' | ① | 247 | 25 | 28 | | 1 |
| AGG | PR | 1175TH AVE | 24' | 39' | 26' | ① | 224 | 22 | 25 | | 1 |
| O&C | PR | 2000TH ST | 24' | 38' | 26' | 30 | 203 | 20 | 23 | | 1 |
| BIT | PR | 2100TH ST N | 24' | 42' | - | ① | 247 | 25 | 28 | | 1 |
| O&C | PR | 2100TH ST S | 24' | 42' | 12' | ① | 252 | 25 | 28 | | 1 |
| O&C | PR | 1100TH AVE W | 18' | 20' | - | ① | 57 | 6 | 6 | | 1 |
| BIT | PR | REL. 1100TH AVE | 24' | 60' | - | ① | 262 | 26 | 29 | | 1 |
| AGG | PR | 2200TH ST N | 24' | 55' | 45' | ① | 314 | 31 | 35 | | 1 |
| O&C | PR | 2200TH ST S | 24' | 61' | - | ① | 242 | 24 | 27 | | 1 |
| O&C | PR | 1000TH AVE N | 24' | 54' | 35' | ① | 277 | 28 | 31 | | 1 |
| O&C | PR | 1000TH AVE S | 24' | 54' | 18' | ① | 244 | 24 | 27 | | 1 |
| O&C | PR | 2400TH ST N | 24' | 43' | 44' | ① | 291 | 29 | 33 | | 1 |
| BIT | PR | 2400TH ST S | 24' | 43' | 13' | ① | 218 | 22 | 24 | | 1 |



PUBLIC ROAD AT RR TRACKS
• WHERE APPLICABLE

L1 = DISTANCE FROM EDGE OF PAVEMENT TO RADIUS POINT.
L2 = DISTANCE FROM RADIUS POINT TO END OF SIDEROAD IMPROVEMENT.

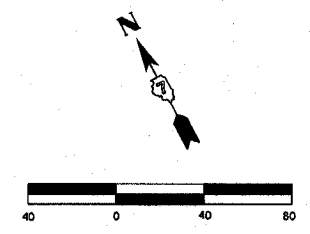
① SEE PLAN AND PROFILE SHEETS FOR RADI DETAILS.
② SEE PLAN AND PROFILE SHEETS FOR L2.
③ SEE TYPICAL SECTION 12 FOR BINDER.

| REVISIONS | | ILLINOIS DEPARTMENT OF TRANSPORTATION |
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| NAME | DATE | |
| | | RURAL ENTRANCE DETAILS & SCHEDULES SHEET 3 OF 3 |
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SCALE: _____ DATE 06/02 DRAWN BY ASB CHECKED BY SJK

DCN-SPEC
DATE

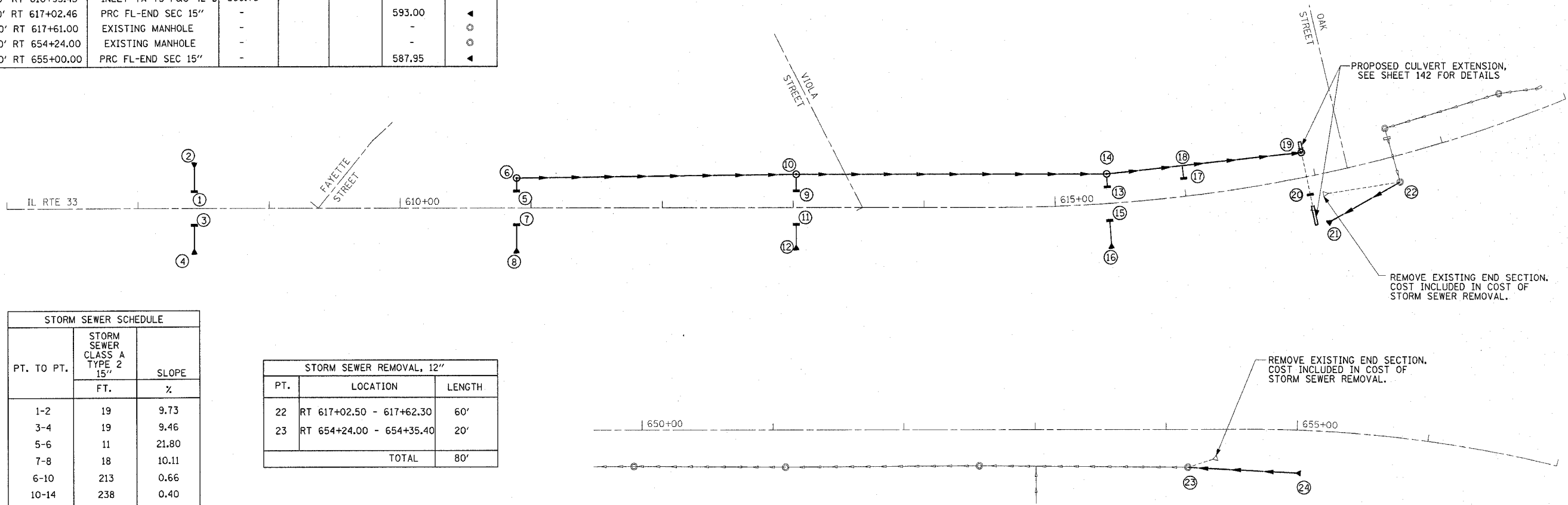
Revised 5/11/07



| DRAINAGE STRUCTURE SCHEDULE | | | | | | | |
|-----------------------------|---------------------|----------------------|-------------------|----------|----------|----------|--------|
| PT | LOCATION | DESCRIPTION | EOP OR TOP OF LID | INLET | INLET | OUTLET | SYMBOL |
| 1 | 14.00' LT 608+42.00 | INLETS TA T3 F&G | 604.55 | | | 602.05 N | ■ |
| 2 | 31.50' LT 608+42.00 | PRC FL-END SEC 15" | - | | | 600.25 | ◀ |
| 3 | 14.00' RT 608+42.00 | INLETS TA T3 F&G | 604.50 | | | 602.00 S | ■ |
| 4 | 31.50' RT 608+42.00 | PRC FL-END SEC 15" | - | | | 600.25 | ◀ |
| 5 | 14.00' LT 610+89.00 | INLETS TA T3 F&G | 602.77 | | | 600.27 N | ■ |
| 6 | 23.00' LT 610+89.00 | MAN TA 4'Ø TIF OL | 601.50 | 597.00 S | | 596.80 E | ⊙ |
| 7 | 14.00' RT 610+89.00 | INLETS TA T3 F&G | 602.72 | | | 600.22 | ■ |
| 8 | 31.00' RT 610+89.00 | PRC FL-END SEC 15" | - | | | 598.40 | ◀ |
| 9 | 14.00' LT 613+02.00 | INLETS TA T3 F&G | 600.90 | | | 597.50 N | ■ |
| 10 | 23.00' LT 613+02.00 | MAN TA 4'Ø TIF OL | 598.50 | 595.40 W | 595.60 S | 595.30 E | ⊙ |
| 11 | 14.00' RT 613+02.00 | INLETS TA T3 F&G | 600.90 | | | 597.50 S | ■ |
| 12 | 29.00' RT 613+02.00 | PRC FL-END SEC 15" | - | | | 597.10 | ◀ |
| 13 | 14.00' LT 615+40.00 | INLETS TA T3 F&G | 598.54 | | | 595.00 N | ■ |
| 14 | 23.00' LT 615+40.00 | MAN TA 4'Ø TIF OL | 597.50 | 594.35 W | 594.50 S | 594.25 E | ⊙ |
| 15 | 14.00' RT 615+40.00 | INLETS TA T3 F&G | 599.95 | | | 596.50 S | ■ |
| 16 | 30.00' RT 615+40.00 | PRC FL-END SEC 15" | - | | | 595.80 | ◀ |
| 17 | 14.00' LT 616+00.00 | INLET TA T3 F&G | 597.93 | | | 595.43 | ■ |
| 18 | 24.58' LT 616+00.00 | RC PIPE TEE 15P 15R | | | 594.06 | 594.06 | ■ |
| 19 | 21.00' LT 616+93.32 | MAN TA 4'Ø TIF OL | 598.20 | 593.61 W | | 593.65 N | ⊙ |
| 20 | 14.00' RT 616+93.43 | INLET TA T3 F&G 42 D | 599.76 | | | - | ■ |
| 21 | 37.00' RT 617+02.46 | PRC FL-END SEC 15" | - | | | 593.00 | ◀ |
| 22 | 20.00' RT 617+61.00 | EXISTING MANHOLE | - | | | - | ⊙ |
| 23 | 38.00' RT 654+24.00 | EXISTING MANHOLE | - | | | - | ⊙ |
| 24 | 30.60' RT 655+00.00 | PRC FL-END SEC 15" | - | | | 587.95 | ◀ |

NOTES:

- AT 19 CONNECT PROPOSED MANHOLE TO EXISTING CULVERT. CONNECT PROPOSED CULVERT EXTENSION TO PROPOSED MANHOLE. COST OF CONNECTIONS INCLUDED IN COST OF PROPOSED MANHOLE.
- AT 20 CONNECT PROPOSED INLET TO EXISTING CULVERT. COST OF CONNECTION INCLUDED IN COST OF PROPOSED INLET. (SEE SHEET 82)
- AT 22 CONNECT PROPOSED STORM SEWER TO EXISTING MANHOLE. COST INCLUDED IN COST OF STORM SEWER.
- AT 23 CONNECT PROPOSED STORM SEWER TO EXISTING MANHOLE. COST INCLUDED IN COST OF STORM SEWER.



| STORM SEWER SCHEDULE | | |
|----------------------|--------------------------------|-------|
| PT. TO PT. | STORM SEWER CLASS A TYPE 2 15" | SLOPE |
| | FT. | |
| 1-2 | 19 | 9.73 |
| 3-4 | 19 | 9.46 |
| 5-6 | 11 | 21.80 |
| 7-8 | 18 | 10.11 |
| 6-10 | 213 | 0.66 |
| 10-14 | 238 | 0.40 |
| 14-19 | 153 | 0.41 |
| 9-10 | 15 | 12.67 |
| 11-12 | 18 | 2.22 |
| 13-14 | 10 | 5.00 |
| 15-16 | 18 | 3.88 |
| 17-18 | 9 | 15.22 |
| 21-22 | 60 | 1.85 |
| 23-24 | 72 | 2.74 |
| TOTAL | 873 | |

| STORM SEWER REMOVAL, 12" | | |
|--------------------------|--------------------------|--------|
| PT. | LOCATION | LENGTH |
| 22 | RT 617+02.50 - 617+62.30 | 60' |
| 23 | RT 654+24.00 - 654+35.40 | 20' |
| | TOTAL | 80' |

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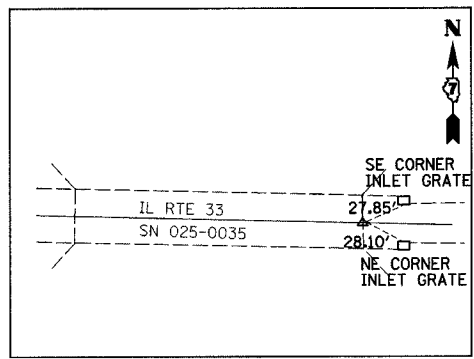
ILLINOIS DEPARTMENT OF TRANSPORTATION

STORM SEWER SCHEDULE

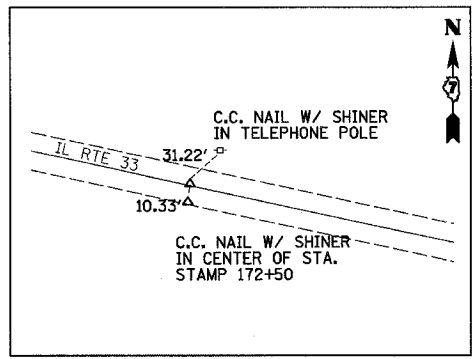
DATE 9/01

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CHECKED BY RMD

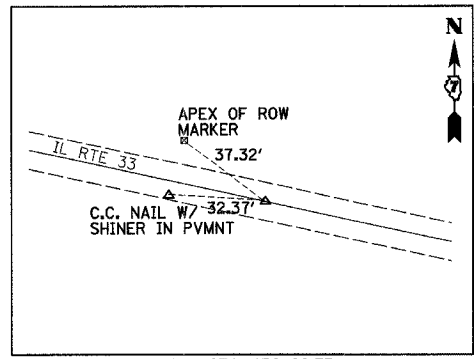
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | • | EFFINGHAM | 409 | 23 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| •(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



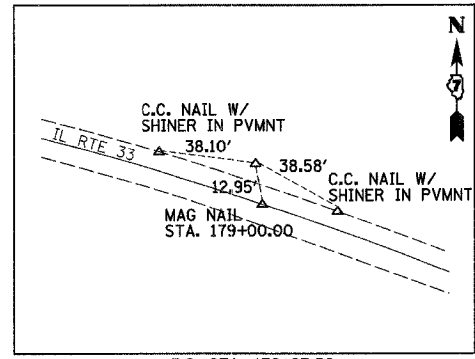
P.O.T. STA. 147+28
(C.C. NAIL W/ SHINER)



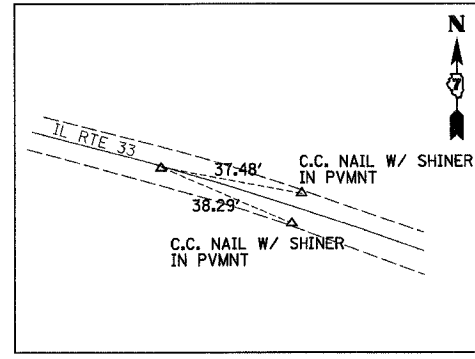
P.I. STA. 172+63.44
(C.C. NAIL W/ SHINER)



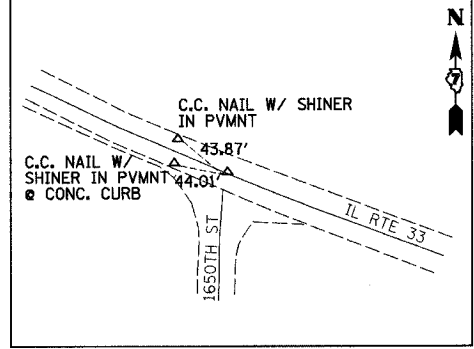
P.C. STA. 176+29.35
(MAG NAIL)



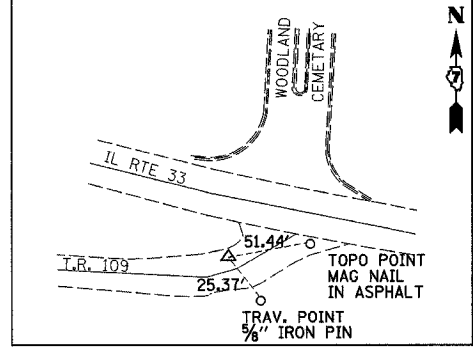
P.I. STA. 178+97.56
(SPIKE NAIL)



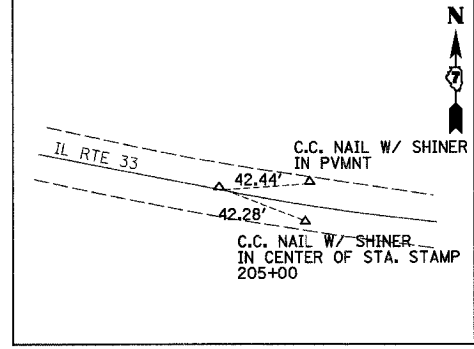
P.T. STA. 181+64.21
(MAG NAIL)



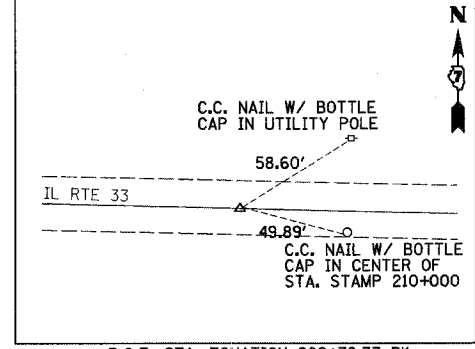
P.C. STA. 197+48.45
(MAG NAIL)



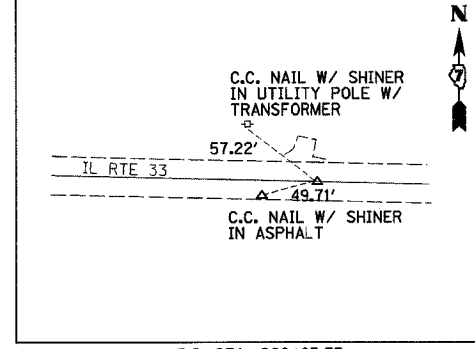
P.I. STA. 201+14.06
(5/8" IRON PIN)



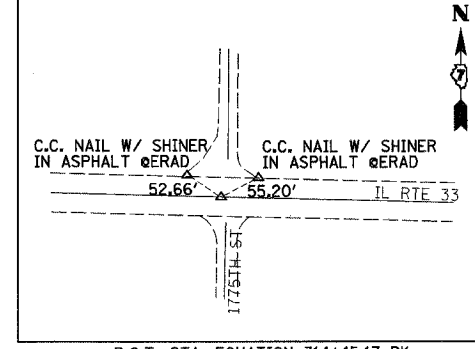
P.T. STA. 204+70.94
(MAG NAIL)



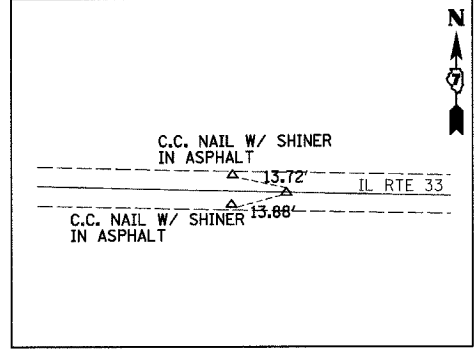
P.O.T. STA. EQUATION 209+78.77 BK
209+67.66 AH
(MAG NAIL)



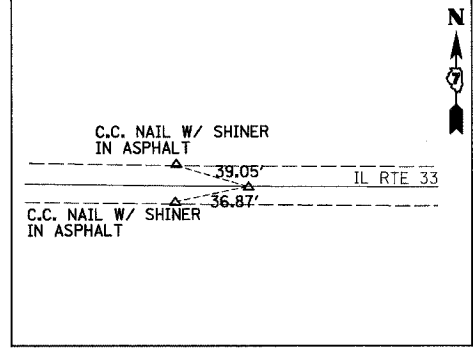
P.I. STA. 260+95.33
(MAG NAIL)



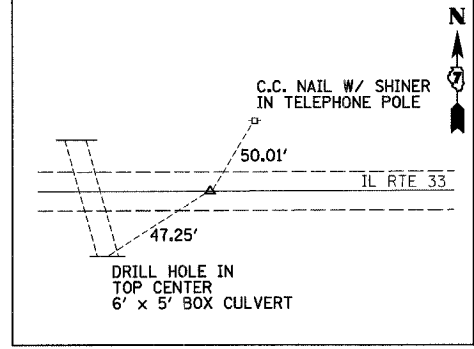
P.O.T. STA. EQUATION 314+45.17 BK
314+13.07 AH
(MAG NAIL)



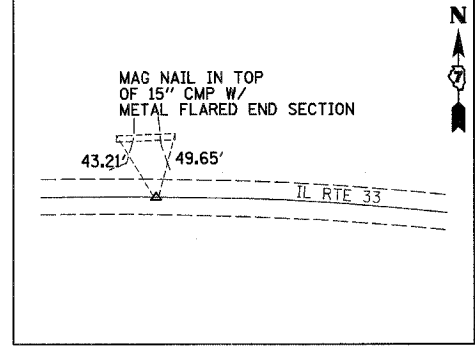
P.C. STA. 323+04.51
(MAG NAIL)



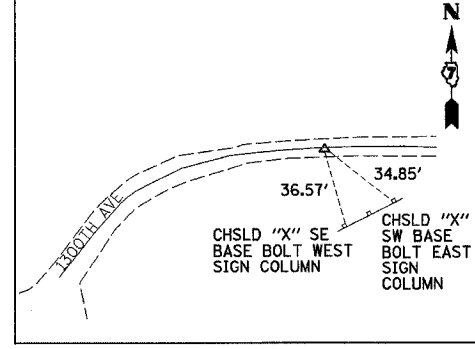
P.I. STA. 326+34.68
(MAG NAIL)



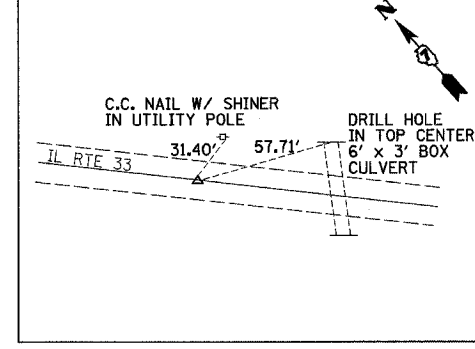
P.T. STA. 329+64.80
(MAG NAIL)



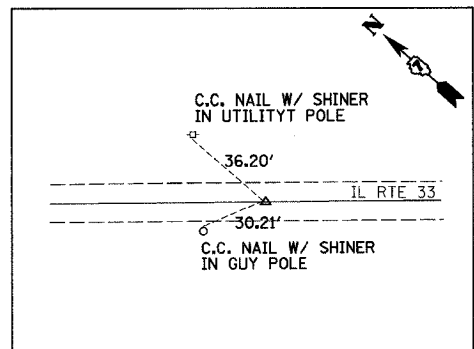
P.C. STA. 339+25.07
(MAG NAIL)



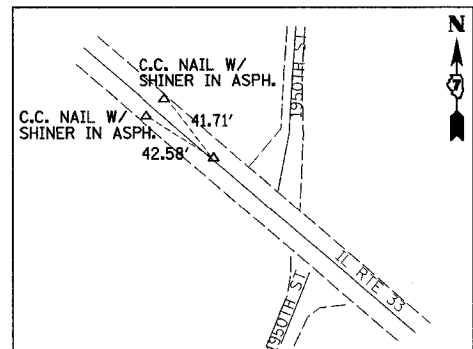
P.I. STA. 347+72.53
(MAG NAIL)



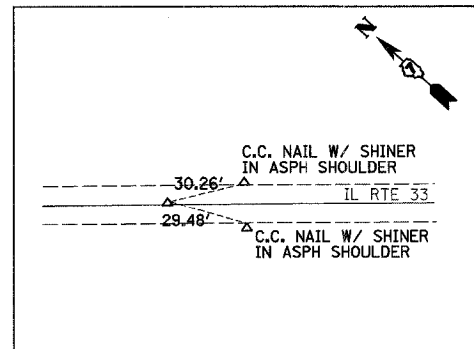
P.T. STA. EQUATION 355+20.31 BK
357+15.38 AH
(MAG NAIL)



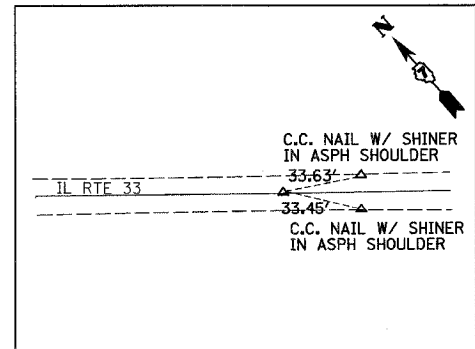
P.C. STA. 390+16.07
(MAG NAIL)



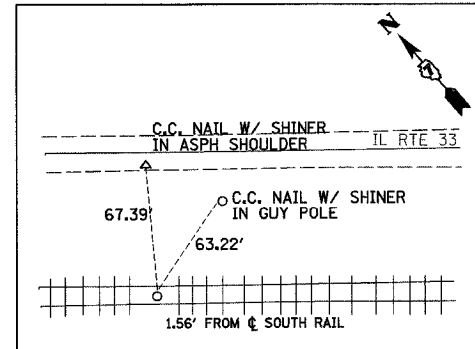
P.T. STA. 408+39.99
(MAG NAIL)



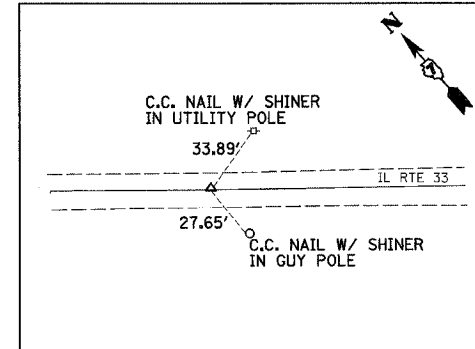
P.O.T. STA. EQUATION 412+74.63 AH
(MAG NAIL)



P.C. STA. 463+69.48
(MAG NAIL)



P.I. STA. 477+99.84
(5/8" IRON PIN)



P.T. STA. 492+17.50
(MAG NAIL)

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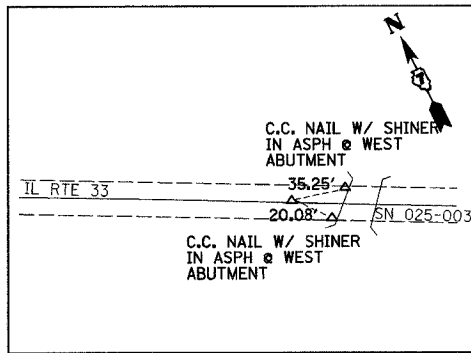
ILLINOIS DEPARTMENT OF TRANSPORTATION

CONTROL TIES
SHEET 1 OF 2

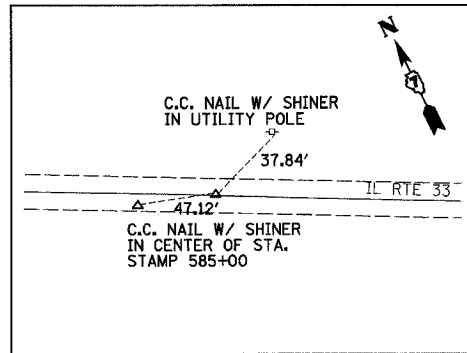
DATE 5/01

DRAWN BY MLO
CHECKED BY RW

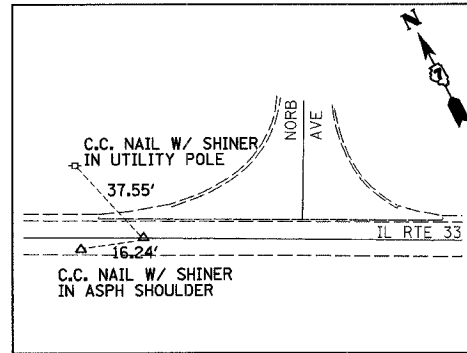
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | • | EFFINGHAM | 409 | 24 |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT | |
| *(6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



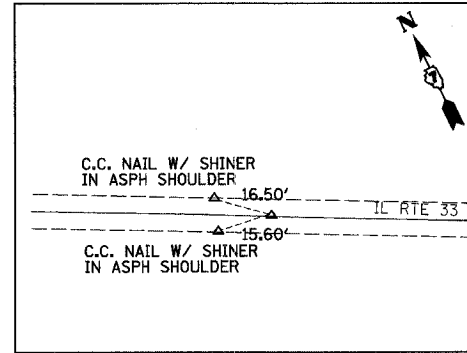
P.I. STA. EQUATION 531+41.06 BK
531+33.78 AH
(MAG NAIL)



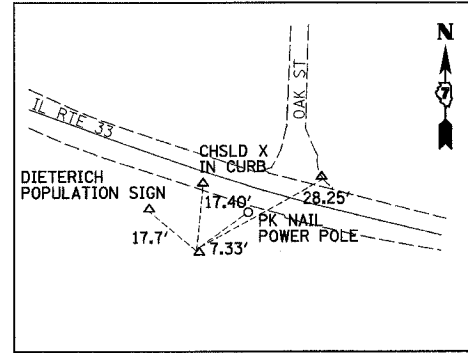
P.C. STA. 585+45.20
(MAG NAIL)



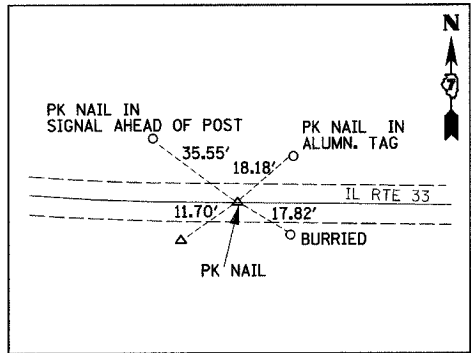
P.I. STA. 590+74.46
(MAG NAIL)



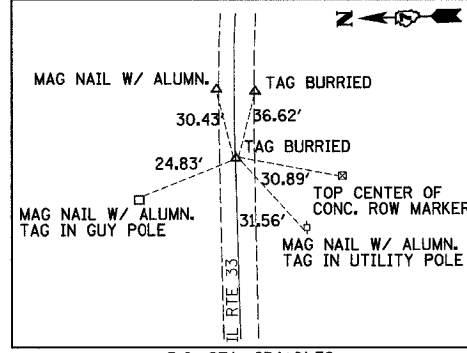
P.T. STA. 596+03.72
(MAG NAIL)



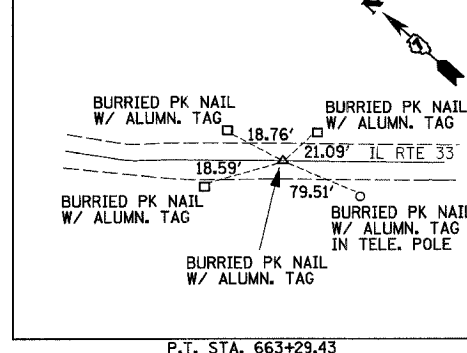
P.I. STA. 617+37.31
(IP W/ ALUMN. CAP)



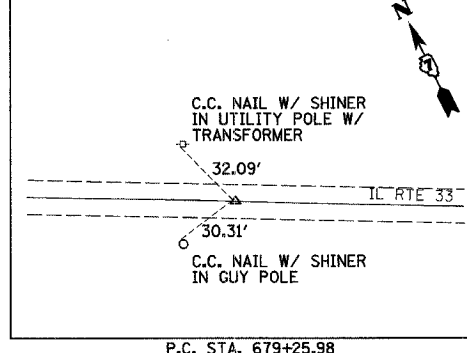
P.T. STA. 620+02.52
(MAG NAIL)



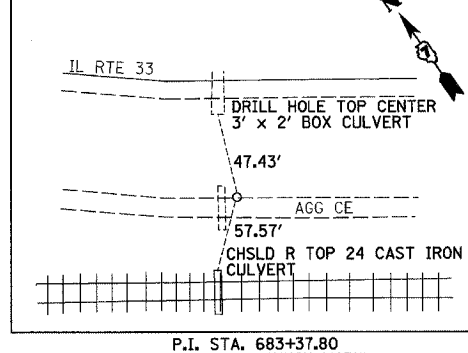
P.C. STA. 654+61.78
(MAG NAIL)



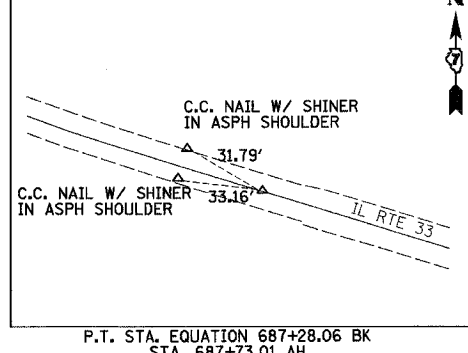
P.T. STA. 663+29.43
(PK NAIL)



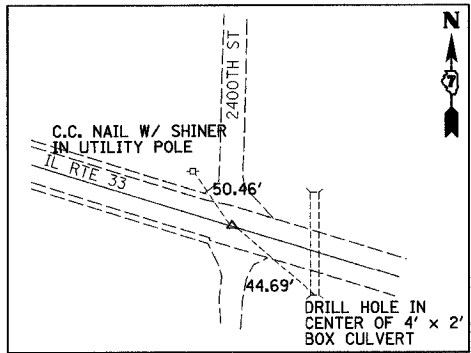
P.C. STA. 679+25.98
(MAG NAIL)



P.I. STA. 683+37.80
(IDOT DISK W/ PUNCH MARK)



P.T. STA. EQUATION 687+28.06 BK
STA. 687+73.01 AH
(MAG NAIL)



COUNTY LINE STA. 689+25.90
IL RTE 33/2400TH ST
(MAG NAIL)

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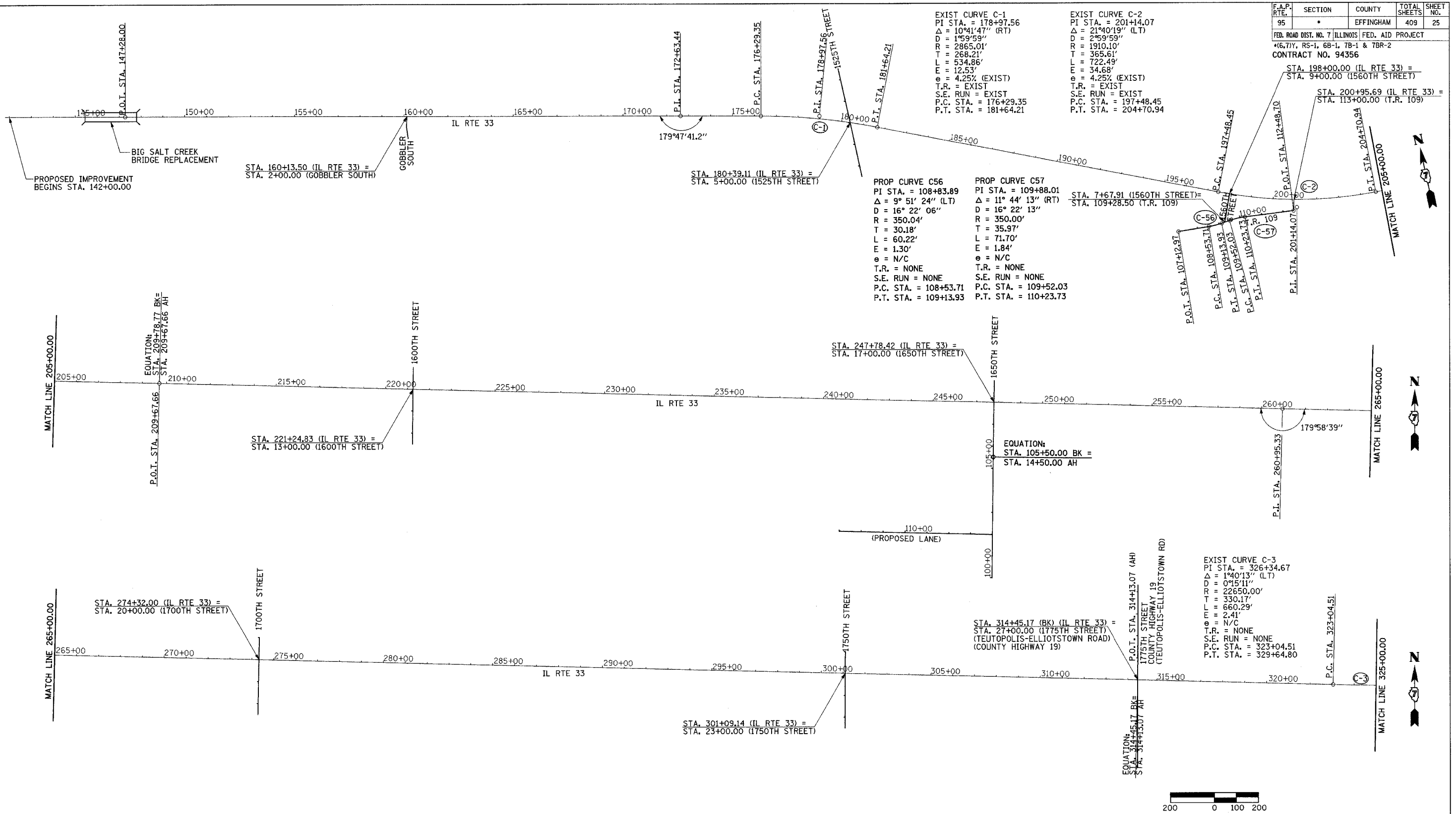
ILLINOIS DEPARTMENT OF TRANSPORTATION

CONTROL TIES
SHEET 2 OF 2

DATE 5/01
DRAWN BY MLO
CHECKED BY RDW

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 25 |

FED. ROAD DIST. NO. 7 [ILLINOIS] FED. AID PROJECT
 (6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2
 CONTRACT NO. 94356



EXIST CURVE C-1
 PI STA. = 178+97.56
 $\Delta = 10^{\circ}41'47''$ (RT)
 $D = 1^{\circ}59'59''$
 $R = 2865.01'$
 $T = 268.21'$
 $E = 534.86'$
 $L = 12.53'$
 $\theta = 4.25\%$ (EXIST)
 T.R. = EXIST
 S.E. RUN = EXIST
 P.C. STA. = 176+29.35
 P.T. STA. = 181+64.21

EXIST CURVE C-2
 PI STA. = 201+14.07
 $\Delta = 21^{\circ}40'19''$ (LT)
 $D = 2^{\circ}59'59''$
 $R = 1910.10'$
 $T = 365.61'$
 $E = 722.49'$
 $L = 34.68'$
 $\theta = 4.25\%$ (EXIST)
 T.R. = EXIST
 S.E. RUN = EXIST
 P.C. STA. = 197+48.45
 P.T. STA. = 204+70.94

PROP CURVE C56
 PI STA. = 108+83.89
 $\Delta = 9^{\circ}51'24''$ (LT)
 $D = 16^{\circ}22'06''$
 $R = 350.04'$
 $T = 30.18'$
 $L = 60.22'$
 $E = 1.30'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 108+53.71
 P.T. STA. = 109+13.93

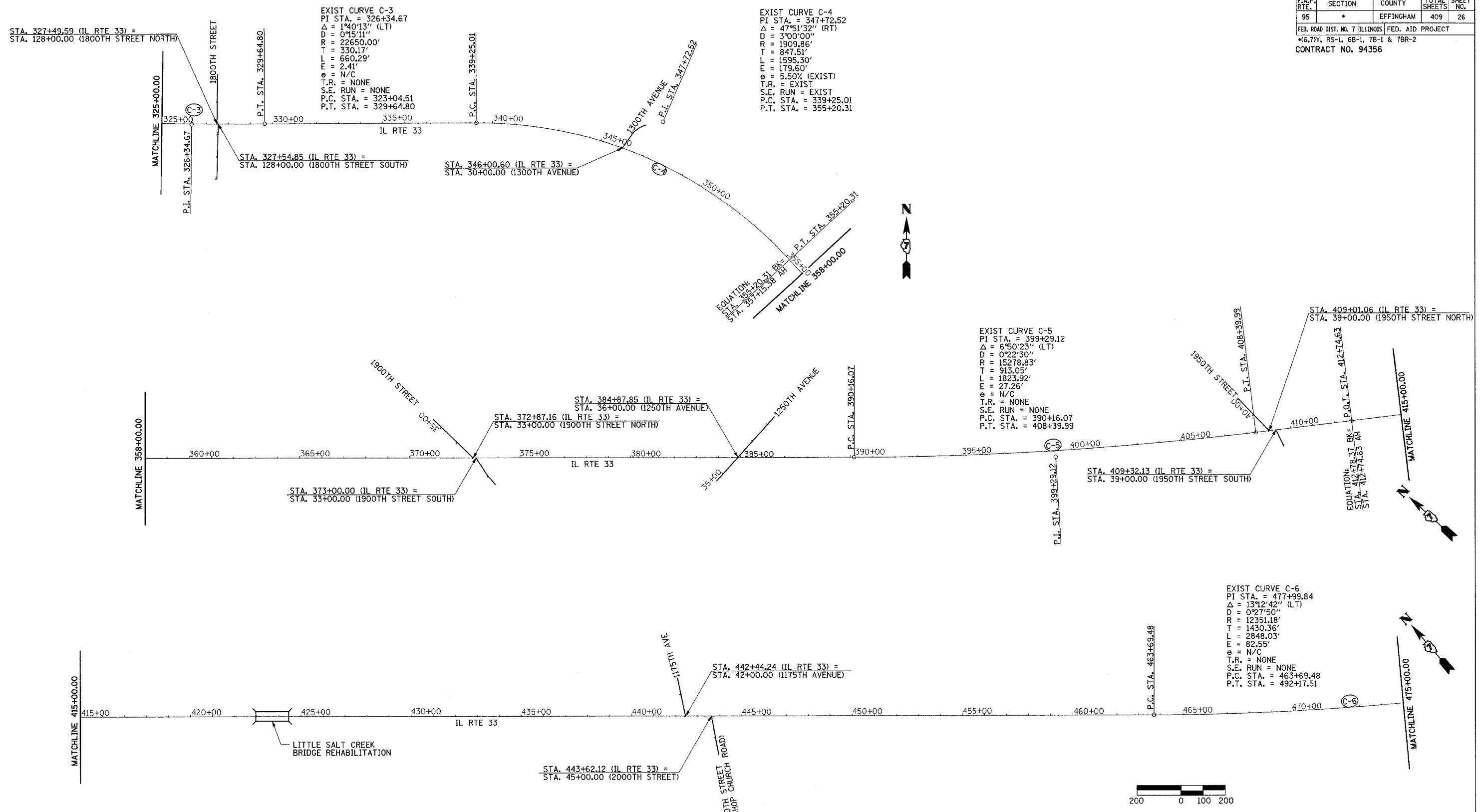
PROP CURVE C57
 PI STA. = 109+88.01
 $\Delta = 11^{\circ}44'13''$ (RT)
 $D = 16^{\circ}22'13''$
 $R = 350.00'$
 $T = 35.97'$
 $L = 71.70'$
 $E = 1.84'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 109+52.03
 P.T. STA. = 110+23.73

EXIST CURVE C-3
 PI STA. = 326+34.67
 $\Delta = 1^{\circ}40'13''$ (LT)
 $D = 0^{\circ}15'11''$
 $R = 22650.00'$
 $T = 330.17'$
 $E = 660.29'$
 $L = 2.41'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 323+04.51
 P.T. STA. = 329+64.80

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ILLINOIS DEPARTMENT OF TRANSPORTATION
 HORIZONTAL ALIGNMENT PLAN
 AND BENCHMARKS
 SHEET 1 OF 4
 SCALE: 1"=200'
 DATE 11/14/01
 DRAWN BY TLS
 CHECKED BY RMD

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 26 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



EXIST CURVE C-3
 PI STA. = 326+34.67
 $\Delta = 1^\circ 40' 13''$ (LT)
 $D = 0^\circ 15' 11''$
 $R = 22650.00'$
 $T = 330.17'$
 $L = 660.29'$
 $E = 2.41'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 323+04.51
 P.T. STA. = 329+64.80

EXIST CURVE C-4
 PI STA. = 347+72.52
 $\Delta = 47^\circ 51' 32''$ (RT)
 $D = 3^\circ 00' 00''$
 $R = 1909.86'$
 $T = 847.51'$
 $L = 1595.30'$
 $E = 179.60'$
 $\theta = 5.50\%$ (EXIST)
 T.R. = EXIST
 S.E. RUN = EXIST
 P.C. STA. = 339+25.01
 P.T. STA. = 355+20.31

EXIST CURVE C-5
 PI STA. = 399+29.12
 $\Delta = 6^\circ 50' 23''$ (LT)
 $D = 0^\circ 22' 30''$
 $R = 15278.83'$
 $T = 913.05'$
 $L = 1823.92'$
 $E = 27.26'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 390+16.07
 P.T. STA. = 408+39.99

EXIST CURVE C-6
 PI STA. = 477+99.84
 $\Delta = 13^\circ 12' 42''$ (LT)
 $D = 0^\circ 27' 50''$
 $R = 12351.18'$
 $T = 1430.36'$
 $L = 2848.03'$
 $E = 82.55'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 463+69.48
 P.T. STA. = 492+17.51

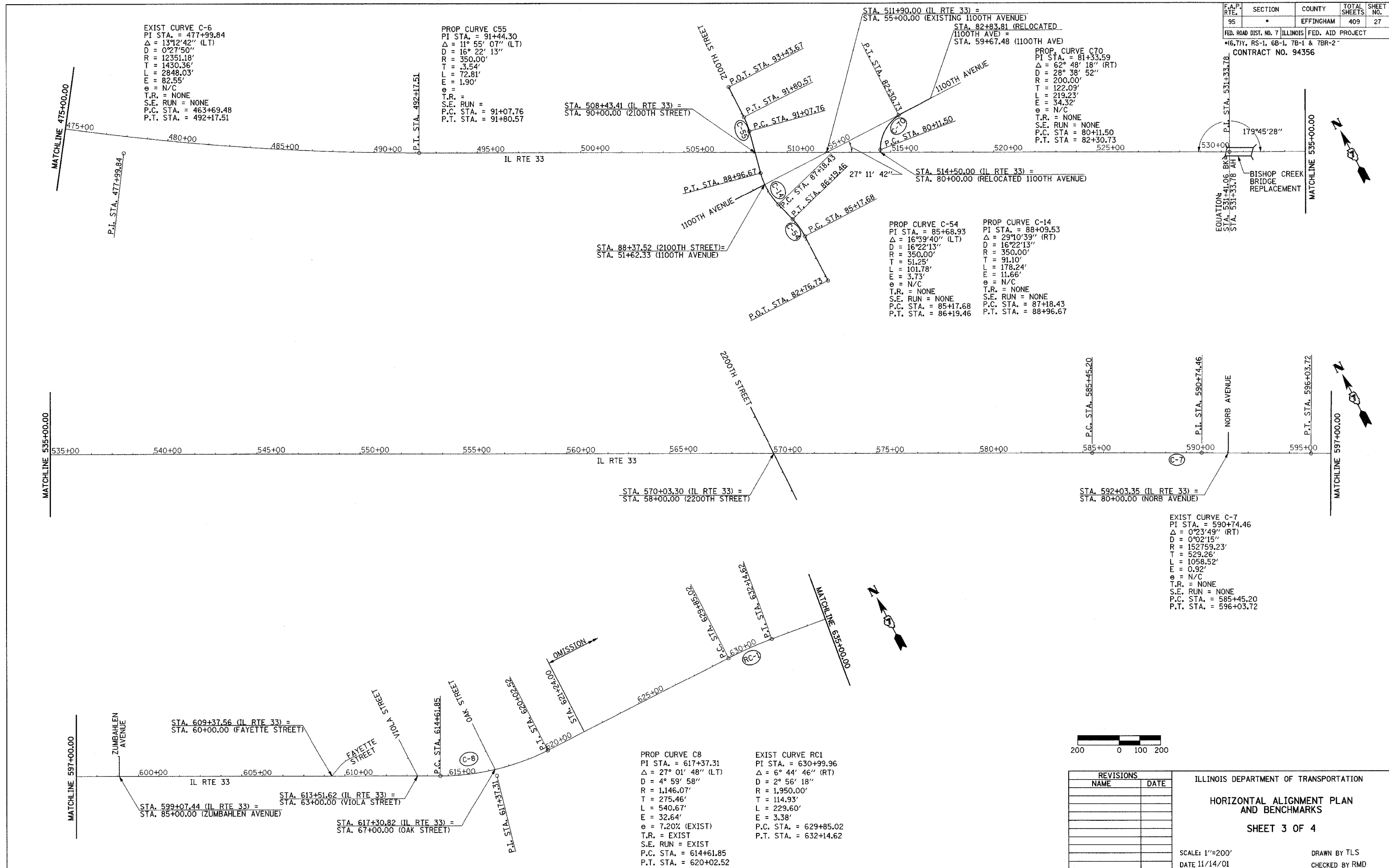


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ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL ALIGNMENT PLAN AND BENCHMARKS
 SHEET 2 OF 4
 SCALE: 1"=200'
 DATE 11/14/01
 DRAWN BY TLS
 CHECKED BY RMD

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------------------------|-----------|--------------|-----------|
| 95 | (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | EFFINGHAM | 409 | 27 |

FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT
 CONTRACT NO. 94356



EXIST CURVE C-6
 PI STA. = 477+99.84
 $\Delta = 131'24''$ (LT)
 $D = 0'27'50''$
 $R = 12351.18'$
 $T = 1430.36'$
 $L = 2848.03'$
 $E = 82.55'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 463+69.48
 P.T. STA. = 492+17.51

PROP CURVE C55
 PI STA. = 91+44.30
 $\Delta = 11'55'07''$ (LT)
 $D = 16'22'13''$
 $R = 350.00'$
 $T = 3.54'$
 $L = 72.81'$
 $E = 1.90'$
 $\theta = N/C$
 T.R. =
 S.E. RUN =
 P.C. STA. = 91+07.76
 P.T. STA. = 91+80.57

PROP CURVE C70
 PI STA. = 81+33.59
 $\Delta = 62'48'18''$ (RT)
 $D = 28'38'52''$
 $R = 200.00'$
 $T = 122.09'$
 $L = 219.23'$
 $E = 34.52'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 80+11.50
 P.T. STA. = 82+30.73

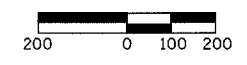
PROP CURVE C-54
 PI STA. = 85+68.93
 $\Delta = 16'39'40''$ (LT)
 $D = 16'22'13''$
 $R = 350.00'$
 $T = 51.25'$
 $L = 101.78'$
 $E = 3.73'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 85+17.68
 P.T. STA. = 86+19.46

PROP CURVE C-14
 PI STA. = 88+09.53
 $\Delta = 29'10'39''$ (RT)
 $D = 16'22'13''$
 $R = 350.00'$
 $T = 91.10'$
 $L = 178.24'$
 $E = 11.66'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 87+18.43
 P.T. STA. = 88+96.67

EXIST CURVE C-7
 PI STA. = 590+74.46
 $\Delta = 0'23'49''$ (RT)
 $D = 0'02'15''$
 $R = 152759.23'$
 $T = 529.26'$
 $L = 1058.52'$
 $E = 0.92'$
 $\theta = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 585+45.20
 P.T. STA. = 596+03.72

PROP CURVE C8
 PI STA. = 617+37.31
 $\Delta = 27'01'48''$ (LT)
 $D = 4'59'58''$
 $R = 1,146.07'$
 $T = 275.46'$
 $L = 540.67'$
 $E = 32.64'$
 $\theta = 7.20\%$ (EXIST)
 T.R. = EXIST
 S.E. RUN = EXIST
 P.C. STA. = 614+61.85
 P.T. STA. = 620+02.52

EXIST CURVE RC1
 PI STA. = 630+99.96
 $\Delta = 6'44'46''$ (RT)
 $D = 2'56'18''$
 $R = 1,950.00'$
 $T = 114.93'$
 $L = 229.60'$
 $E = 3.38'$
 P.C. STA. = 629+85.02
 P.T. STA. = 632+14.62

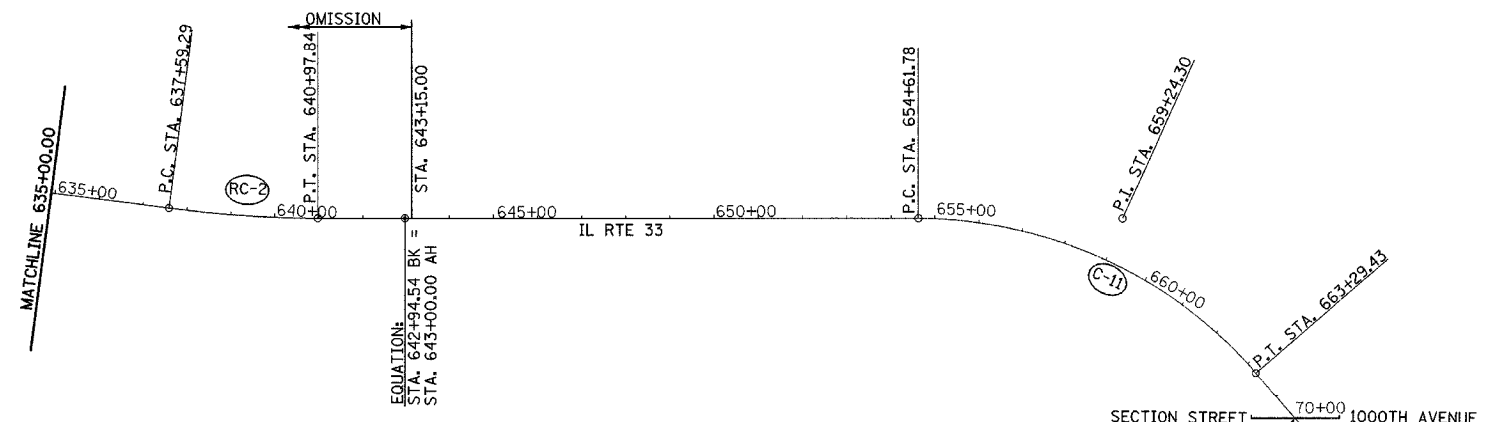


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ILLINOIS DEPARTMENT OF TRANSPORTATION
 HORIZONTAL ALIGNMENT PLAN
 AND BENCHMARKS
 SHEET 3 OF 4
 SCALE: 1"=200'
 DATE 11/14/01
 DRAWN BY TLS
 CHECKED BY RMD

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | | EFFINGHAM | 409 | 28 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

EXIST CURVE RC2
 PI STA. = 639+28.81
 $\Delta = 7^{\circ} 27' 38''$ (LT)
 $D = 2^{\circ} 12' 13''$
 $R = 2,600.00'$
 $T = 169.51'$
 $L = 338.55'$
 $E = 5.52'$
 P.C. STA. = 637+59.29
 P.T. STA. = 640+97.84



EXIST CURVE C-11
 PI STA. = 659+24.30
 $\Delta = 49^{\circ} 08' 10''$ (RT)
 $D = 5^{\circ} 39' 47''$
 $R = 1011.73'$
 $T = 462.52'$
 $L = 867.65'$
 $E = 100.71'$
 $e = 7.40\%$ (EXIST)
 T.R. = EXIST
 S.E. RUN = EXIST
 P.C. STA. = 654+61.78
 P.T. STA. = 663+29.43

EXIST CURVE C-12
 PI STA. = 683+37.79
 $\Delta = 32^{\circ} 01' 10''$ (LT)
 $D = 3^{\circ} 59' 31''$
 $R = 1435.26'$
 $T = 411.82'$
 $L = 802.08'$
 $E = 57.91'$
 $e = 7.00\%$ (EXIST)
 T.R. = EXIST
 S.E. RUN = EXIST
 P.C. STA. = 679+25.98
 P.T. STA. = 687+28.06

- TBM 1
16' LT STA. 148+09 EL 540.23
CHISELED "□" ON 6" BARRIER CURB EAST OF BIG SALT CREEK BRIDGE. APPROXIMATELY 41' WEST OF THE EAST END OF THE LEFT BRIDGE GUARDRAIL.
- IDOT BM 95033002
20.7' RT STA. 159+26.8 EL 588.14
SOUTHWEST QUAD RTE 33 & LANDFILL GOBBLER, 7'± SOUTH FROM THE BACK OF THE CURB & GUTTER ON RTE 33
- IDOT BM 95033003
711.9' RT STA. 183+39.5 EL 589.04
NO DESCRIPTION
- TBM 30
75' RT STA. 192+40 EL 588.53
"MAG" NAIL SET IN NW CORNER OF BITUMINOUS PARKING LOT
- TBM 4
30' LT STA. 205+74 EL 592.86
RR SPIKE IN POWER POLE
- TBM 31
31' LT STA. 212+42 EL 594.13
RR SPIKE IN POWER POLE
- IDOT BM 95033004
80.7' RT STA. 221+53.0 EL 592.65
SOUTHWEST QUAD RR CROSSING WITH TOWNSHIP RD 1300 NORTH 1600 EAST, 25'± EAST OF RD, 33'± SOUTH OF RR TRACKS
- TBM 5
36' LT STA. 234+28 EL 590.76
RR SPIKE IN POWER POLE
- IDOT BM 95033005
73.1' RT STA. 248+04.9 EL 593.87
SOUTHWEST QUAD RR CROSSING WITH TOWNSHIP RD 1300 NORTH 1700 EAST, SOUTH SIDE OF RR TRACKS JUST NORTH OF POWER POLE
- TBM 32
31' LT STA. 263+07 EL 590.26
RR SPIKE IN POWER POLE
- IDOT BM 95033006
90.4' RT STA. 274+07 EL 586.98
SOUTHWEST QUAD RR CROSSING WITH TOWNSHIP RD 1300 NORTH 1700 EAST, SOUTH SIDE OF RR TRACKS JUST NORTH OF POWER POLE
- TBM 33
31' LT STA. 280+10 EL 592.88
RR SPIKE IN POWER POLE
- IDOT BM 95033007
96.6' RT STA. 300+88.9 EL 588.96
SOUTHWEST QUAD RR CROSSING WITH TOWNSHIP RD 1300 NORTH 1750 EAST, SOUTH SIDE OF RR TRACKS JUST NORTH OF POWER POLE
- TBM 34
41.9' LT STA. 306+85.9 EL 591.20
RR SPIKE IN POWER POLE
- TBM 7
40' RT STA. 315+00 EL 586.73
CHISELED "□" ON BOX CULVERT HEADWALL

- IDOT BM 95033008
109.6' RT STA. 327+28.1 EL 586.81
SOUTHWEST QUAD RR CROSSING WITH TOWNSHIP RD 1300 NORTH 1800 EAST, SOUTH SIDE OF RR TRACKS NORTHWEST OF OLD FENCE CORNER POST
- IDOT BM 95033009
78.9' RT STA. 346+32.4 EL 587.55
NORTHEAST QUAD RR CROSSING WITH WERNING CURVE, 22'± EAST OF ROCK PE & 33'± NORTH OF RR TRACKS
- TBM 35
31.0' LT STA. 355+20 EL 588.63
RR SPIKE IN POWER POLE
- TBM 36
31.0' LT STA. 366+87 EL 589.67
RR SPIKE IN POWER POLE
- TBM 10
25' LT STA. 376+00 EL 588.38
RR SPIKE IN POWER POLE
- IDOT BM 95033010
104.2' RT STA. 383+57.3 EL 585.69
SOUTHWEST QUAD RR CROSSING WITH TOWNSHIP RD 1250 NORTH 1925 EAST, SOUTH SIDE OF RR TRACKS NORTHEAST OF FENCE CORNER
- TBM 11
30' LT STA. 389+98 EL 584.30
RR SPIKE IN POWER POLE
- TBM 12
31' LT STA. 399+21 EL 586.36
RR SPIKE IN POWER POLE
- IDOT BM 95033011
60.5' RT STA. 409+81.9 EL 577.16
NORTHEAST QUAD RR CROSSING WITH TOWNSHIP RD 1250 NORTH 1950 EAST, 20'± NORTH OF TRACKS & 29'± EAST OF RD
- TBM 13
20' RT STA. 415+00 EL 570.60
5/8" Ø IRON PIN
- TBM 14
105' LT STA. 427+00 EL 563.18
TOP OF R.O.W. MARKER
- TBM 37
110.0' LT STA. 434+86 EL 565.85
RR SPIKE IN POWER POLE
- IDOT BM 95033012
61.1' RT STA. 444+01.5 EL 572.08
NORTHEAST QUAD RR CROSSING WITH TOWNSHIP RD 2000 EAST, 20'± NORTH OF RR TRACKS & 24'± EAST OF RD RR CROSSING JUST EAST OF MILE POST 170
- TBM 41
48' LT STA. 451+83 EL 574.20
CHSLD "□" ON TOP OF 36" RCP END SECTION
- TBM 42
88.0' LT STA. 459+95 EL 577.48
5/8" Ø IRON PIN - 6' S OF PETROLEUM WARNING SIGN

- STA. 664+64.86 (IL RTE 33) = STA. 70+00.00 (SECTION STREET)
- IDOT BM 95033013
96.7' RT STA. 471+11.3 EL 585.84
SOUTHWEST QUAD 2ND RR FIELD CROSSING, SOUTH OF RR TRACKS 1/2 MILE EAST OF TOWNSHIP RD 2000 EAST
- TBM 16
30' LT STA. 475+52 EL 587.49
WP 422 TOP OF IRON PIN 584.31
RR SPIKE IN POWER POLE
- TBM 17
30' LT STA. 489+95 EL 589.49
WP 423 TOP OF IRON PIN 586.44
RR SPIKE IN POWER POLE
- IDOT BM 95033014
83.6' RT STA. 491+10.6 EL 585.92
SOUTHWEST QUAD RR CROSSING, 0.34 MILE WEST OF TOWNSHIP RD 1100 NORTH 2100 EAST ON SOUTH SIDE OF RR TRACKS ON EAST SIDE OF PE
- TBM 38
31.0' LT STA. 496+90 EL 589.44
RR SPIKE IN POWER POLE
- IDOT BM 95033015
101.7' RT STA. 509+44.5 EL 586.15
SOUTHWEST QUAD RR CROSSING WITH TOWNSHIP RD 1100 NORTH 2100 EAST, 14'± EAST OF RD 32'± SOUTH OF RR TRACKS
- TBM 19
30' LT STA. 519+72 EL 589.34
RR SPIKE IN POWER POLE
- TBM 20
30' LT STA. 535+20 EL 581.50
RR SPIKE IN POWER POLE
- IDOT BM 95033016
86.8' RT STA. 539+85.4 EL 584.24
SOUTHWEST QUAD RR CROSSING 0.57 MILE EAST OF TOWNSHIP RD 2100 EAST ON EAST SIDE OF FE AND ON SOUTH SIDE OF RR TRACKS
- TBM 39
30.0' RT STA. 554+89 EL 589.44
RR SPIKE IN POWER POLE
- TBM 22
30' LT STA. 564+71 EL 594.19
RR SPIKE IN POWER POLE
- IDOT BM 95033017
90.0' RT STA. 570+29.6 EL 594.45
SOUTHWEST QUAD RR CROSSING WITH TOWNSHIP RD 1050 NORTH 2200 EAST, 15'± SOUTH OF RR TRACKS 15'± WEST OF RD

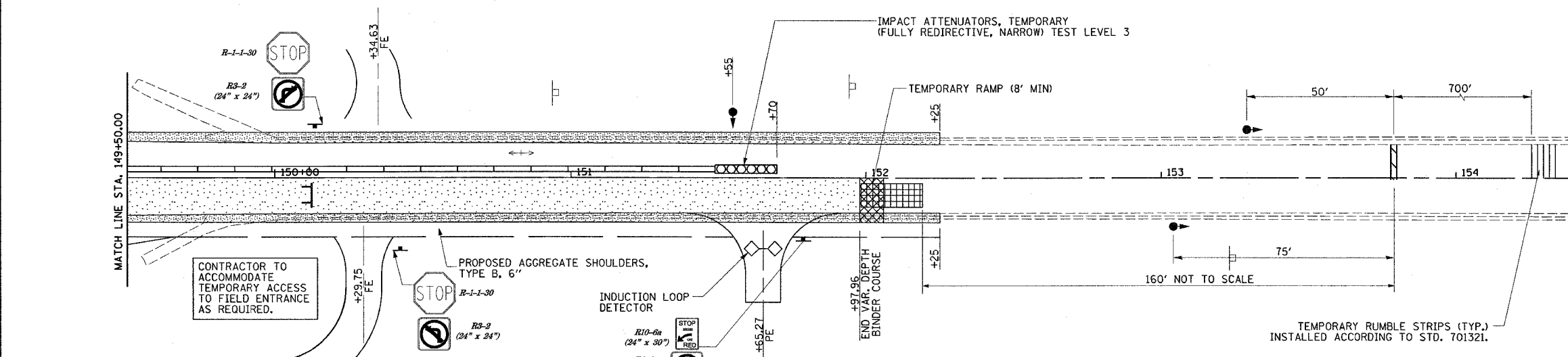
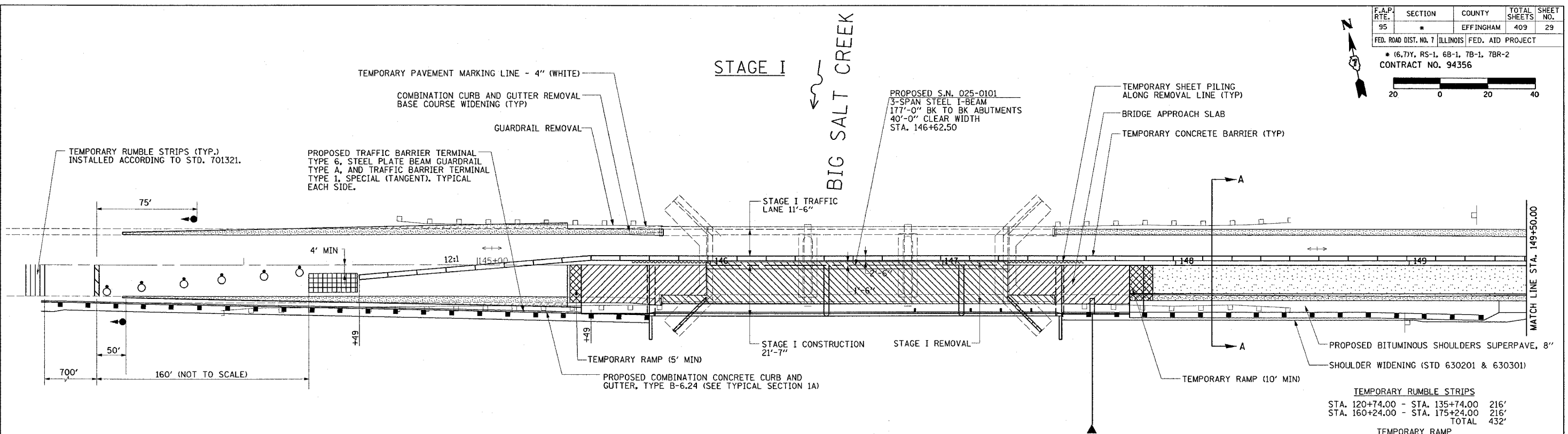
- TBM 23
32' LT STA. 578+77 EL 598.99
RR SPIKE IN POWER POLE
- IDOT BM 95033018
80.9' RT STA. 594+54.5 EL 606.45
2ND FE ON SOUTH SIDE OF RTE 33 1/2 MILE EAST 95033017 ON SOUTH SIDE OF RR TRACKS 6'±
- TBM 24
31' LT STA. 594+79 EL 608.08
RR SPIKE IN POWER POLE
- TBM 25
31' LT STA. 610+21 EL 605.18
RR SPIKE IN POWER POLE
- IDOT BM 95033019
129.0' RT STA. 617+74.1 EL 597.61
WEST SIDE OF DIETERICH 35'± EAST OF OAK ST 12'± SOUTH OF MAIN RR TRACKS & 15'± WEST OF NORTH WEST CORNER OLD CONC FOUNDATION ON SOUTH SIDE OF TRACKS
- TBM 27
25' RT STA. 644+31 EL 590.83
RR SPIKE IN POWER POLE
- IDOT BM 95033021
34.7' RT STA. 655+62.3 EL 589.78
158'± EAST OF E EAST ENTRANCE TO WINTERROWD AF & AM #664 27'± SOUTH OF RTE 33
- TBM 40
21.0' LT STA. 664+06 EL 599.21
RR SPIKE IN POWER POLE
- IDOT BM 95033023
83.0' RT STA. 689+62.55 EL 592.42
SOUTHWEST QUAD RR CROSSING AT COUNTY LINE RD EAST OF DIETERICH 12'± EAST OF E ROAD 12'± SOUTH OF E TRACKS

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
HORIZONTAL ALIGNMENT PLAN AND BENCHMARKS
 SHEET 4 OF 4
 SCALE: 1"=200'
 DATE 11/14/01
 DRAWN BY TLS
 CHECKED BY RMD

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | | EFFINGHAM | 409 | 29 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |

* (6.7)Y, RS-1, 6B-1, 7B-1, 7BR-2
CONTRACT NO. 94356



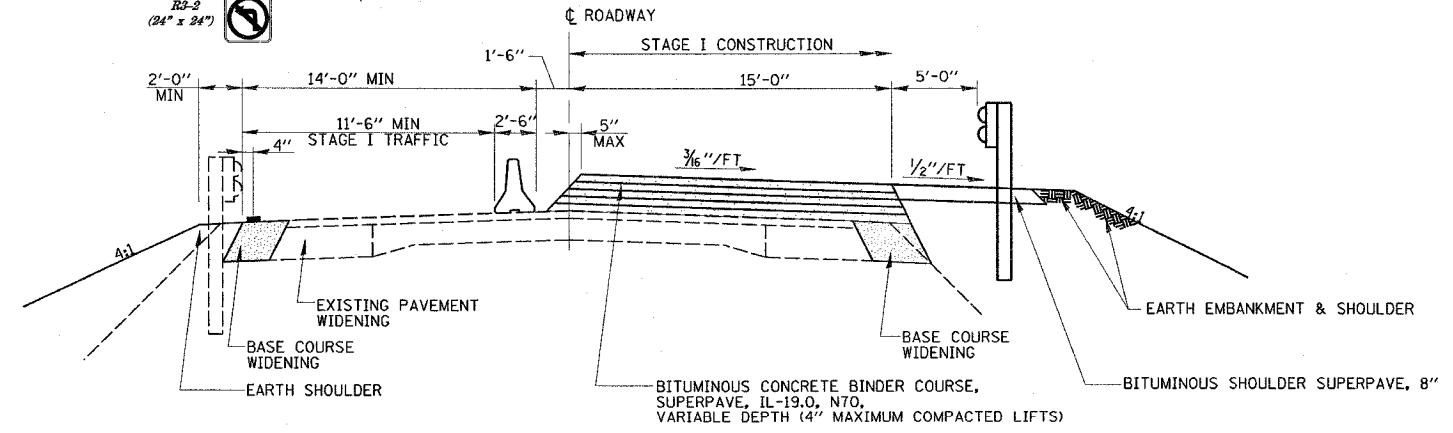
| TEMPORARY RUMBLE STRIPS | | |
|---|-----------------|--|
| STA. 120+74.00 - STA. 135+74.00 | 216' | |
| STA. 160+24.00 - STA. 175+24.00 | 216' | |
| TOTAL | 432' | |
| TEMPORARY RAMP | | |
| STA. 145+39.00 - STA. 145+44.00 | 8 SQ YD | |
| STA. 147+81.00 - STA. 147+91.00 | 16 SQ YD | |
| STA. 151+97.96 - STA. 152+05.96 | 13 SQ YD | |
| TOTAL | 37 SQ YD | |
| IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3 BIG SALT CREEK | | |
| | 1 EACH | |
| TEMPORARY CONCRETE BARRIER | | |
| STA. 144+49.00 - STA. 151+49.00 | 700' | |

- SUGGESTED SEQUENCE OF OPERATIONS**
1. CONSTRUCT BASE COURSE WIDENING, LEFT SIDE
 2. ERECT SIGNS, TRAFFIC SIGNALS, TEMPORARY BARRIERS, ETC. ACCORDING TO THE TRAFFIC CONTROL STANDARD 701321. INSTALL TRAFFIC CONTROL FOR PRIVATE ENTRANCE AS SHOWN.
 3. INSTALL TEMPORARY SHEET PILING. REMOVE THE STAGE I PORTION OF THE EXISTING STRUCTURE, PAVEMENT AND GUARDRAIL.
 4. CONSTRUCT EMBANKMENT AND RAISE ROAD PER CROSS SECTIONS.
 5. CONSTRUCT THE STAGE I PORTION OF THE NEW BRIDGE, APPROACH PAVEMENT, TEMPORARY RAMPS AND GUARDRAIL.

- NOTES:**
1. FOR ADDITIONAL DETAILS ON LANE CLOSURES WITH BARRIERS, SEE STANDARD 701321 AND SPECIAL PROVISION FOR TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL).
 2. SEE BRIDGE PLANS FOR ADDITIONAL INFORMATION ON STAGE CONSTRUCTION.
 3. UNLESS NOTED, BASE COURSE WIDENING TO REMAIN IN PLACE AFTER BRIDGE CONSTRUCTION IS COMPLETE.
 4. PROVIDE CONTINUOUS ACCESS TO PRIVATE ENTRANCE AT STA. 151+65.27.
 5. FINAL SURFACE COURSE TO BE APPLIED UNDER TRAFFIC.

LEGEND

| | |
|--|---|
| | PAVEMENT REMOVAL |
| | BITUMINOUS BINDER COURSE |
| | BASE COURSE WIDENING |
| | TEMPORARY RAMP |
| | TEMPORARY CONCRETE BARRIER |
| | STRUCTURE REMOVAL |
| | IMPACT ATTENUATOR |
| | TYPE III BARRICADE WITH ROAD CLOSED SIGN (R11-2) |
| | TRAFFIC SIGNALS |
| | BARRICADE OR DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT |
| | STOP BAR |



SECTION A-A

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
STAGE I CONSTRUCTION
IL RTE 33 OVER BIG SALT CREEK
STRUCTURE NO. 025-0101
STA. 146+62.50

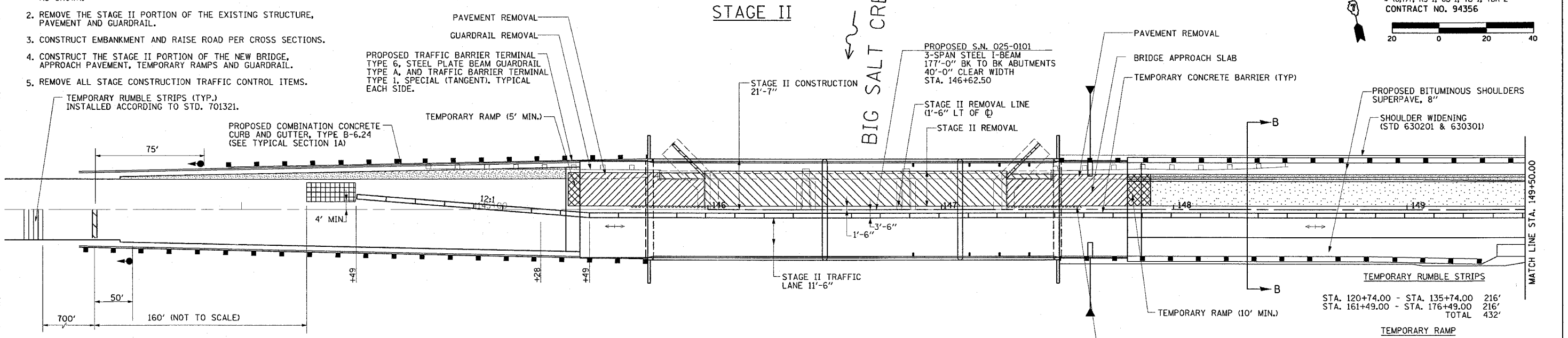
DATE 8/03
REVISED 03/22/07

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CHECKED BY PBB

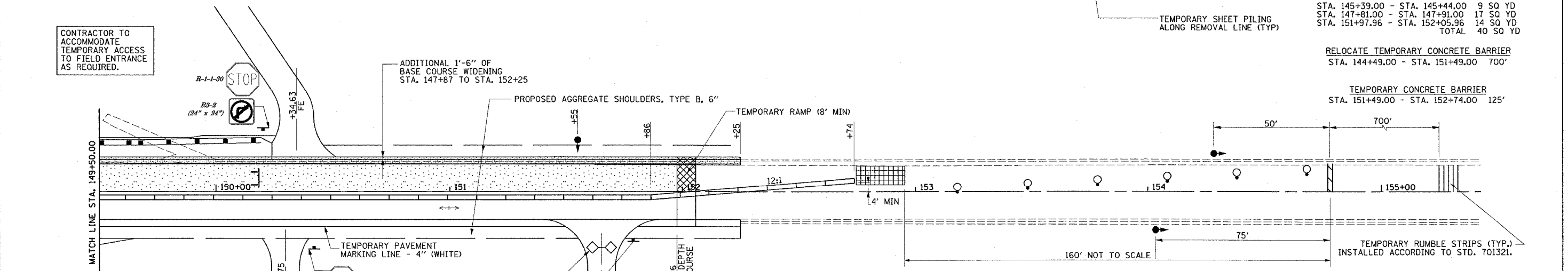
SUGGESTED SEQUENCE OF OPERATIONS

1. RELOCATE SIGNS, TRAFFIC SIGNALS, TEMPORARY BARRIERS, ETC. ACCORDING TO THE TRAFFIC CONTROL STANDARD 701321. INSTALL TRAFFIC CONTROL FOR PRIVATE ENTRANCE AS SHOWN.
2. REMOVE THE STAGE II PORTION OF THE EXISTING STRUCTURE, PAVEMENT AND GUARDRAIL.
3. CONSTRUCT EMBANKMENT AND RAISE ROAD PER CROSS SECTIONS.
4. CONSTRUCT THE STAGE II PORTION OF THE NEW BRIDGE, APPROACH PAVEMENT, TEMPORARY RAMPS AND GUARDRAIL.
5. REMOVE ALL STAGE CONSTRUCTION TRAFFIC CONTROL ITEMS.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | | EFFINGHAM | 409 | 30 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

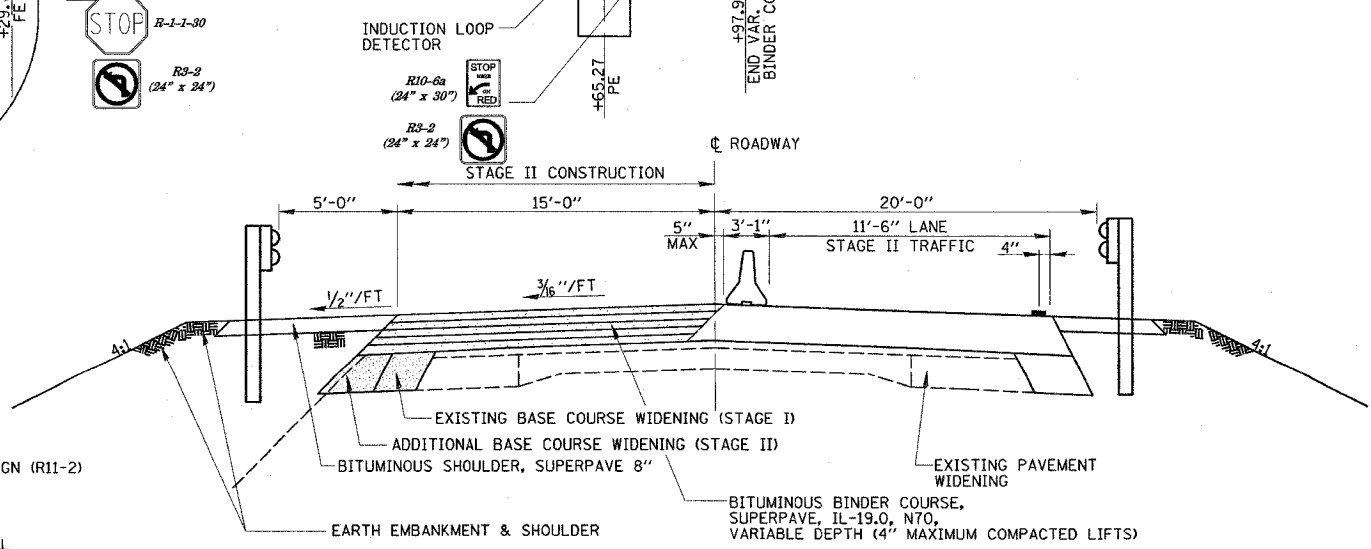


CONTRACTOR TO ACCOMMODATE TEMPORARY ACCESS TO FIELD ENTRANCE AS REQUIRED.



LEGEND

- PAVEMENT REMOVAL
- BITUMINOUS BINDER COURSE
- BASE COUSE WIDENING
- TEMPORARY RAMP
- TEMPORARY CONCRETE BARRIER
- STRUCTURE REMOVAL
- IMPACT ATTENUATOR
- TYPE III BARRICADE WITH ROAD CLOSED SIGN (R11-2)
- TRAFFIC SIGNALS
- BARRICADE OR DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
- STOP BAR



SECTION B-B

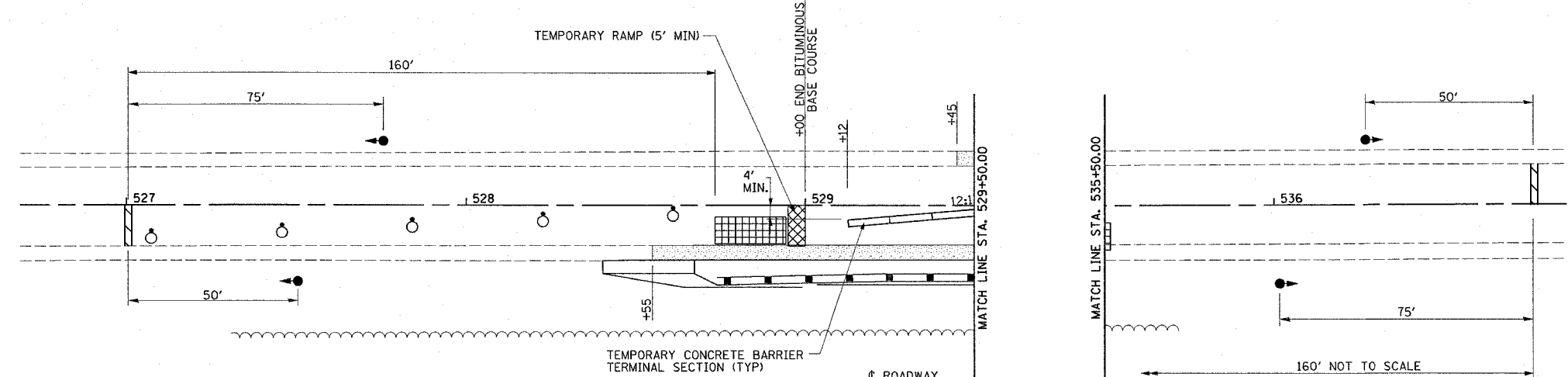
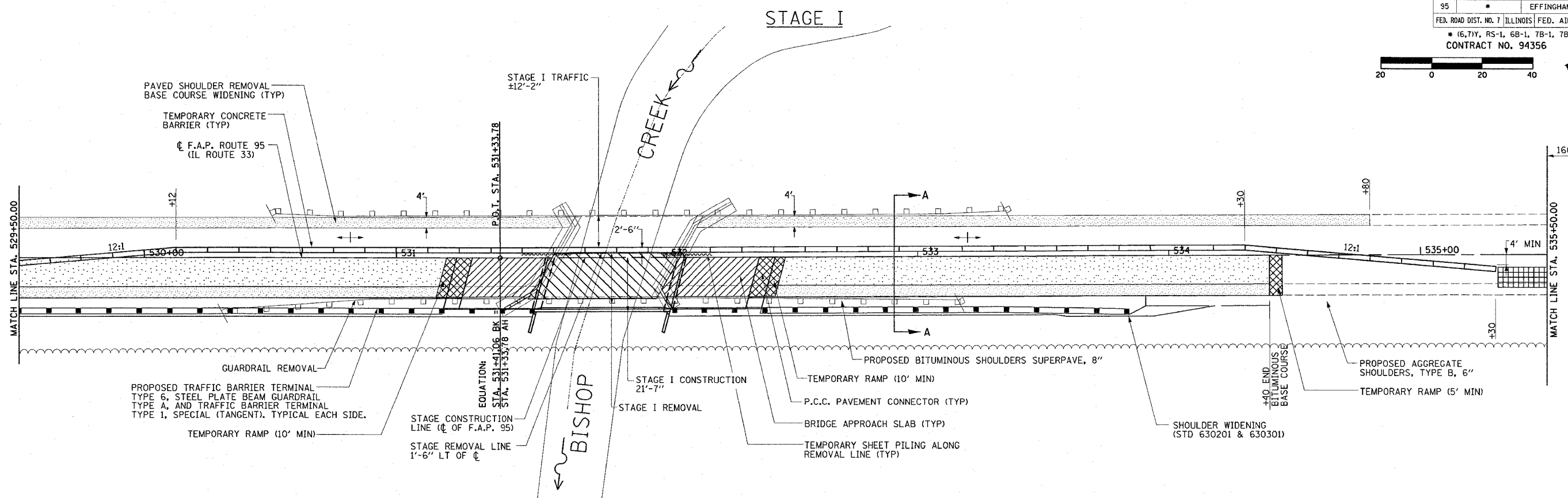
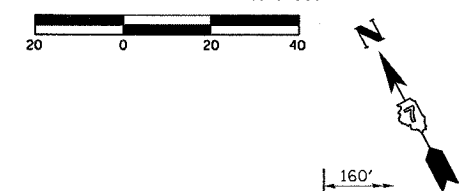
NOTES:

1. FOR ADDITIONAL DETAILS ON LANE CLOSURES WITH BARRIERS, SEE STANDARD 701321 AND SPECIAL PROVISION FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701321 (SPECIAL).
2. SEE BRIDGE PLANS FOR ADDITIONAL INFORMATION ON STAGE CONSTRUCTION.
3. UNLESS NOTED, BASE COURSE TO REMAIN IN PLACE AFTER BRIDGE CONSTRUCTION IS COMPLETE.
4. PROVIDE CONTINUOUS ACCESS TO PRIVATE ENTRANCE AT STA. 151+65.27.
5. FINAL SURFACE COURSE TO BE APPLIED UNDER TRAFFIC.

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ILLINOIS DEPARTMENT OF TRANSPORTATION
STAGE II CONSTRUCTION
IL RTE 33 OVER BIG SALT CREEK
STRUCTURE NO. 025-0101
STA. 146+62.50
 DATE 8/03
 REVISED 03/22/07
 DRAWN BY MLO
 CHECKED BY PBB

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | | EFFINGHAM | 409 | 31 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



TEMPORARY RAMP

| | |
|---------------------------------|-----------------|
| STA. 531+20.28 - STA. 531+30.28 | 16 SQ YD |
| STA. 532+37.00 - STA. 532+47.00 | 16 SQ YD |
| STA. 528+95.00 - STA. 529+00.00 | 8 SQ YD |
| TOTAL | 40 SQ YD |

TEMPORARY CONCRETE BARRIER

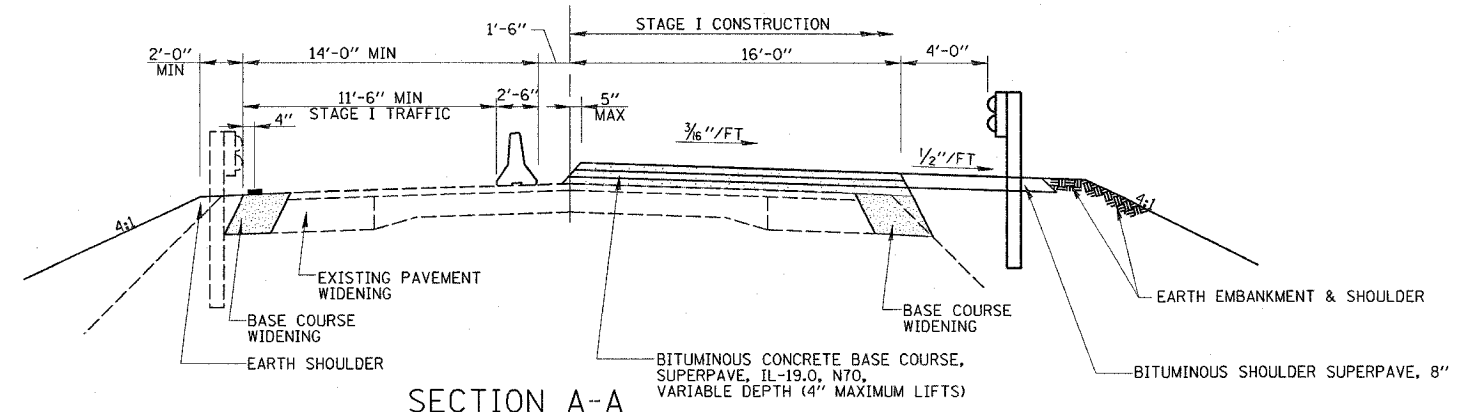
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| STA. 529+12.00 - STA. 535+30.00 | 625' |
|---------------------------------|------|

- NOTES:**
- FOR ADDITIONAL DETAILS ON LANE CLOSURES WITH BARRIERS, SEE STANDARD 701321 AND SPECIAL PROVISION FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701321 (SPECIAL).
 - SEE BRIDGE PLANS FOR ADDITIONAL INFORMATION ON STAGE CONSTRUCTION.
 - UNLESS NOTED, BASE COURSE WIDENING TO REMAIN IN PLACE AFTER BRIDGE CONSTRUCTION IS COMPLETE.
 - FINAL SURFACE COURSE TO BE APPLIED UNDER TRAFFIC.

- SUGGESTED SEQUENCE OF OPERATIONS**
- CONSTRUCT BASE COURSE WIDENING, LEFT SIDE
 - ERECT SIGNS, TRAFFIC SIGNALS, TEMPORARY BARRIERS, ETC. ACCORDING TO THE TRAFFIC CONTROL STANDARD 701321.
 - INSTALL TEMPORARY SHEET PILING. REMOVE THE STAGE I PORTION OF THE EXISTING STRUCTURE, PAVEMENT AND GUARDRAIL.
 - CONSTRUCT EMBANKMENT AND RAISE ROAD PER CROSS SECTIONS.
 - CONSTRUCT THE STAGE I PORTION OF THE NEW BRIDGE, APPROACH PAVEMENT, TEMPORARY RAMPS AND GUARDRAIL.

LEGEND

- PAVEMENT REMOVAL
- BITUMINOUS BINDER COURSE
- BASE COURSE WIDENING
- TEMPORARY RAMP
- TEMPORARY CONCRETE BARRIER
- STRUCTURE REMOVAL
- IMPACT ATTENUATOR
- TRAFFIC SIGNALS
- BARRICADE OR DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
- STOP BAR



| REVISIONS | |
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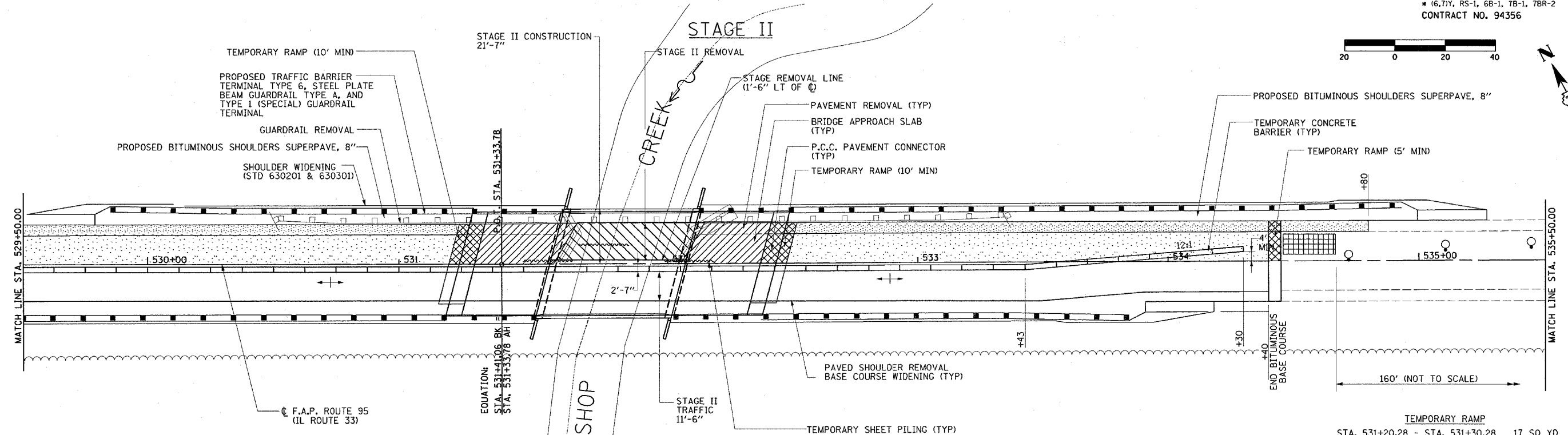
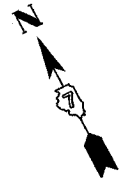
ILLINOIS DEPARTMENT OF TRANSPORTATION

STAGE I CONSTRUCTION
IL RTE 33 OVER BISHOP CREEK
STRUCTURE NO. 025-0097
STA. 531+80.00

DATE 8/03
 REVISED DATE 03/26/07

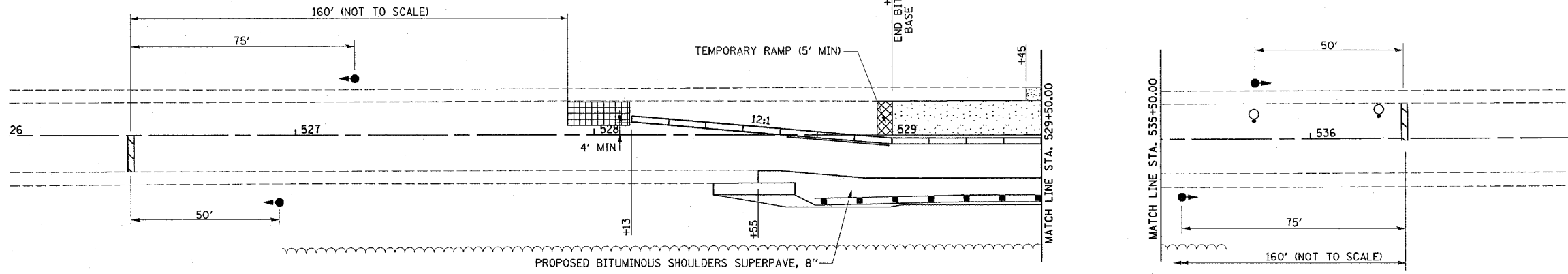
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 32 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6.7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



| TEMPORARY RAMP | | |
|---------------------------------|-----------------|--|
| STA. 531+20.28 - STA. 531+30.28 | 17 SQ YD | |
| STA. 532+37.00 - STA. 532+47.00 | 17 SQ YD | |
| STA. 528+95.00 - STA. 529+00.00 | 9 SQ YD | |
| TOTAL | 43 SQ YD | |

RELOCATE TEMPORARY CONCRETE BARRIER
 STA. 528+12.00 - STA. 534+30.00 625'



NOTES:

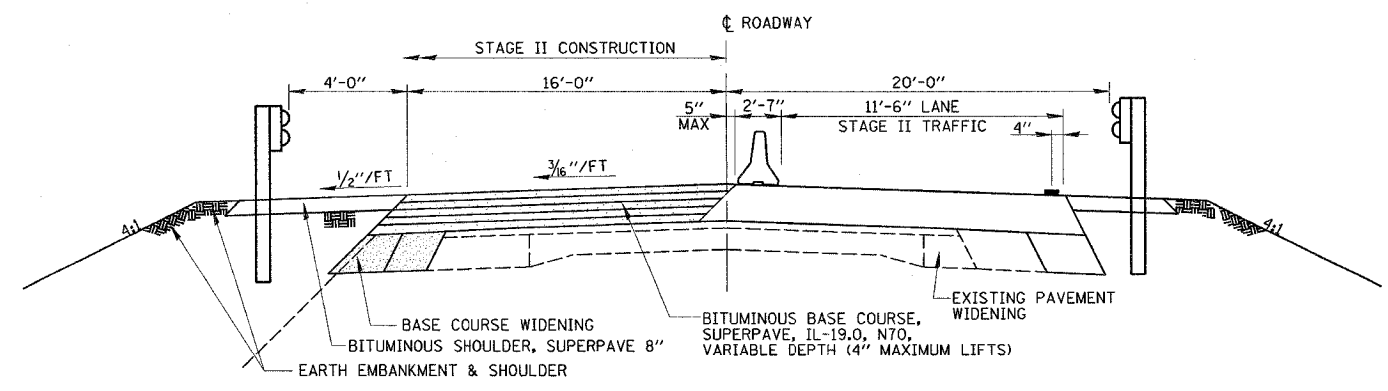
- FOR ADDITIONAL DETAILS ON LANE CLOSURES WITH BARRIERS, SEE STANDARD 701321 AND SPECIAL PROVISION FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701321 (SPECIAL).
- SEE BRIDGE PLANS FOR ADDITIONAL INFORMATION ON STAGE CONSTRUCTION.
- UNLESS NOTED, BASE COURSE WIDENING TO REMAIN IN PLACE AFTER BRIDGE CONSTRUCTION IS COMPLETE.
- FINAL SURFACE COURSE TO BE APPLIED UNDER TRAFFIC.

SUGGESTED SEQUENCE OF OPERATIONS

- RELOCATE SIGNS, TRAFFIC SIGNALS, TEMPORARY BARRIERS, ETC. ACCORDING TO THE TRAFFIC CONTROL STANDARD 701321.
- REMOVE THE STAGE II PORTION OF THE EXISTING STRUCTURE, PAVEMENT AND GUARDRAIL.
- CONSTRUCT EMBANKMENT AND RAISE ROAD PER CROSS SECTIONS.
- CONSTRUCT THE STAGE II PORTION OF THE NEW BRIDGE, PAVEMENT, APPROACH PAVEMENT, TEMPORARY RAMPS AND GUARDRAIL.
- REMOVE ALL STAGE CONSTRUCTION TRAFFIC CONTROL ITEMS.

LEGEND

- PAVEMENT REMOVAL
- BITUMINOUS BINDER COURSE
- BASE COURSE WIDENING
- TEMPORARY RAMP
- TEMPORARY CONCRETE BARRIER
- STRUCTURE REMOVAL
- IMPACT ATTENUATOR
- TRAFFIC SIGNALS
- BARRICADE OR DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
- STOP BAR



SECTION B-B

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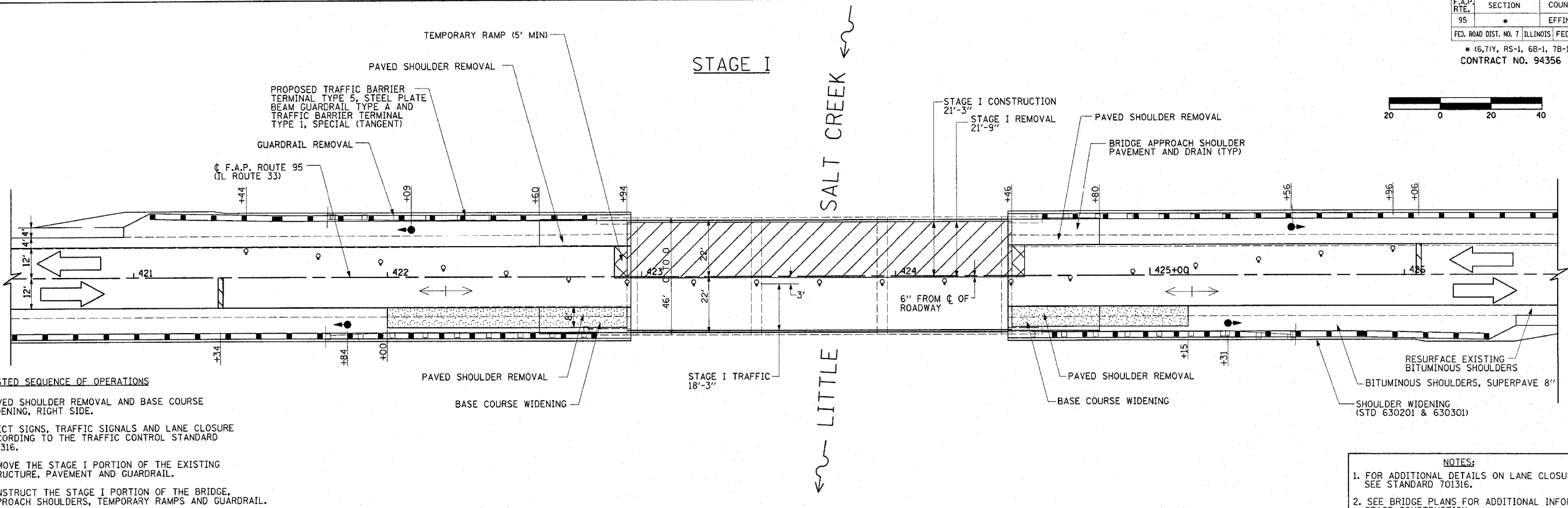
ILLINOIS DEPARTMENT OF TRANSPORTATION

STAGE II CONSTRUCTION
 IL RTE 33 OVER BISHOP CREEK
 STRUCTURE NO. 025-0097
 STA. 531+80.00

DATE 8/03
 REVISED DATE 03/26/07

DRAWN BY MLO
 CHECKED BY PBB

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|---|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 33 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



SUGGESTED SEQUENCE OF OPERATIONS

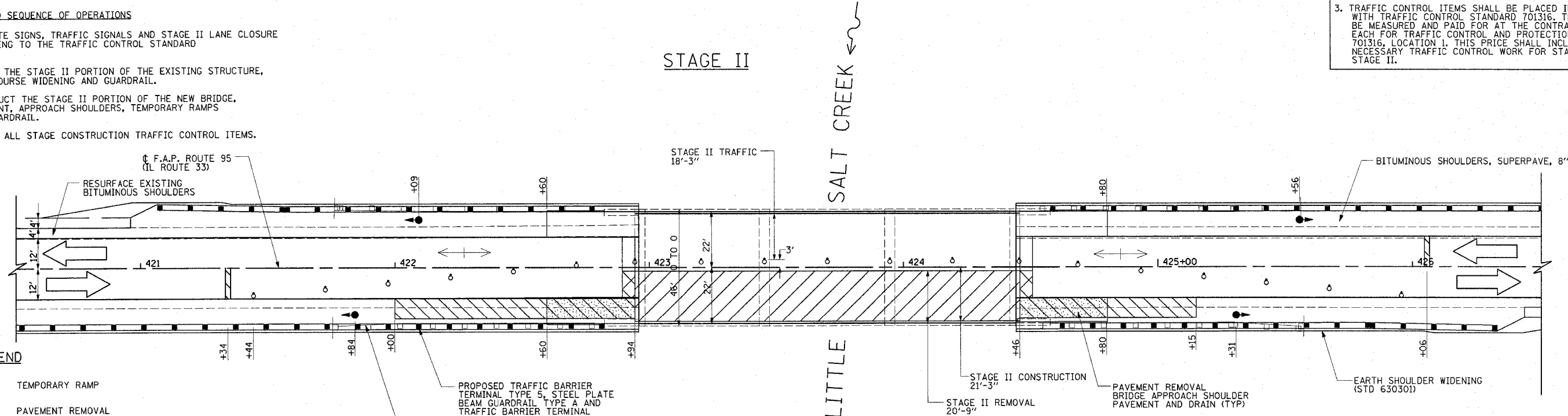
1. PAVED SHOULDER REMOVAL AND BASE COURSE WIDENING, RIGHT SIDE.
2. ERECT SIGNS, TRAFFIC SIGNALS AND LANE CLOSURE ACCORDING TO THE TRAFFIC CONTROL STANDARD 701316.
3. REMOVE THE STAGE I PORTION OF THE EXISTING STRUCTURE, PAVEMENT AND GUARDRAIL.
4. CONSTRUCT THE STAGE I PORTION OF THE BRIDGE, APPROACH SHOULDERS, TEMPORARY RAMPS AND GUARDRAIL.

NOTES:

1. FOR ADDITIONAL DETAILS ON LANE CLOSURE, SEE STANDARD 701316.
2. SEE BRIDGE PLANS FOR ADDITIONAL INFORMATION ON STAGE CONSTRUCTION.
3. TRAFFIC CONTROL ITEMS SHALL BE PLACED IN ACCORDANCE WITH TRAFFIC CONTROL STANDARD 701316. THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701316, LOCATION 1. THIS PRICE SHALL INCLUDE ALL NECESSARY TRAFFIC CONTROL WORK FOR STAGE 1 AND STAGE II.

SUGGESTED SEQUENCE OF OPERATIONS

1. RELOCATE SIGNS, TRAFFIC SIGNALS AND STAGE II LANE CLOSURE ACCORDING TO THE TRAFFIC CONTROL STANDARD 701316.
2. REMOVE THE STAGE II PORTION OF THE EXISTING STRUCTURE, BASE COURSE WIDENING AND GUARDRAIL.
3. CONSTRUCT THE STAGE II PORTION OF THE NEW BRIDGE, PAVEMENT, APPROACH SHOULDERS, TEMPORARY RAMPS AND GUARDRAIL.
4. REMOVE ALL STAGE CONSTRUCTION TRAFFIC CONTROL ITEMS.



LEGEND

- TEMPORARY RAMP
- PAVEMENT REMOVAL
- WORK AREA
- BASE COURSE WIDENING
- BARRICADE OR DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
- STOP BAR
- TRAFFIC SIGNALS
- PROPOSED TRAFFIC BARRIER TERMINAL TYPE 5, STEEL PLATE BEAM GUARDRAIL TYPE A AND TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT)
- GUARDRAIL REMOVAL

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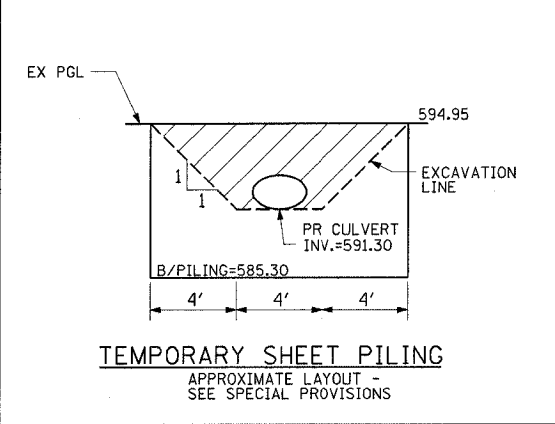
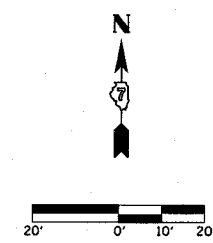
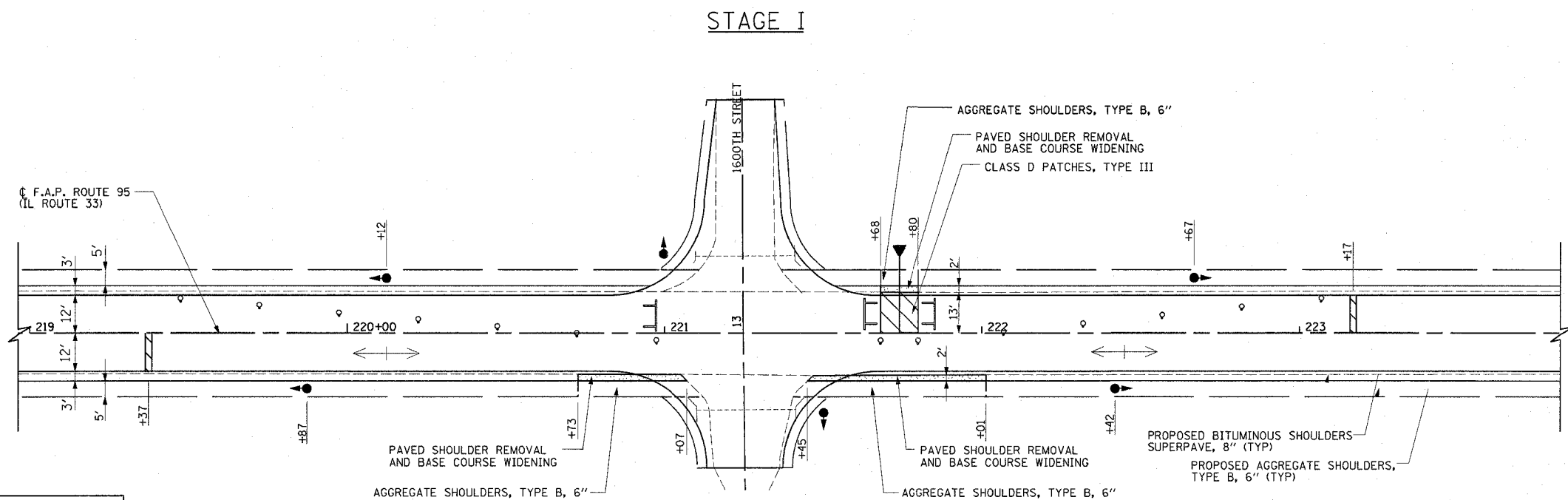
ILLINOIS DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION
 IL RTE 33 OVER LITTLE SALT CREEK
 STRUCTURE NO. 025-0082
 STA. 423+70.00

DATE 10/01

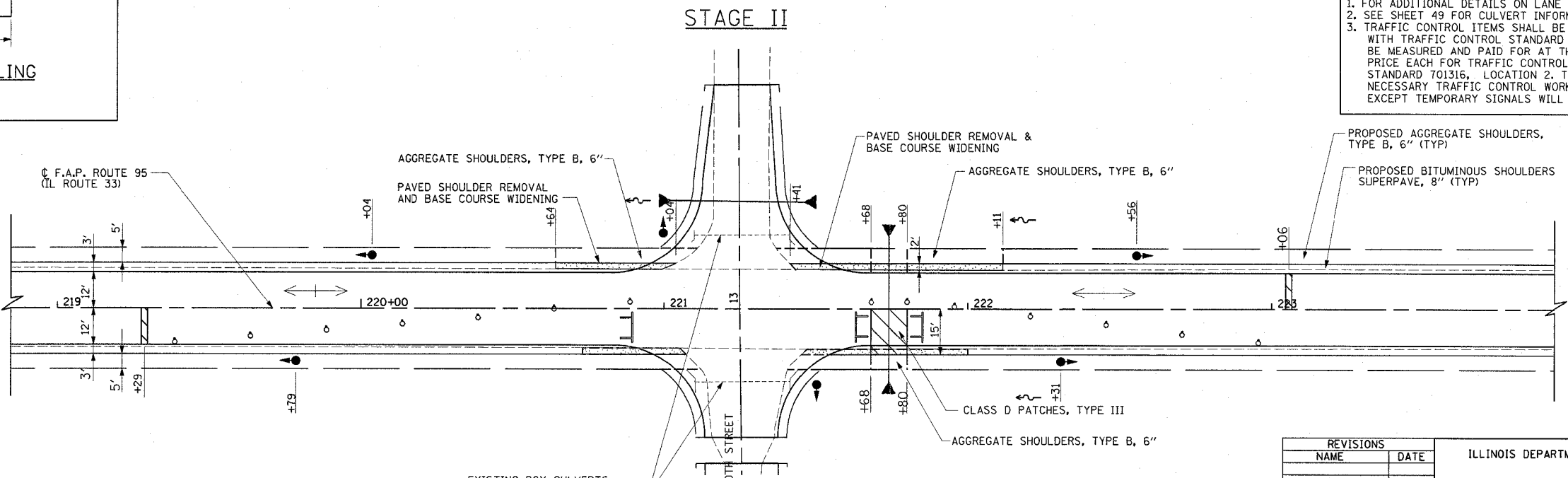
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | | EFFINGHAM | 409 | 34 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *16,71Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



TEMPORARY SHEET PILING
STA. 221+68.00 TO STA 221+80.00 116 SQ FT

- SUGGESTED SEQUENCE OF OPERATIONS**
1. CONSTRUCT WIDENING FOR STAGE I.
 2. INSTALL TEMPORARY TRAFFIC SIGNALS.
 3. SET DRUMS FOR STAGE I ALLOWING MINIMUM 12 FEET FOR TRAFFIC LANE.
 4. DRIVE SHEET PILING AT CENTERLINE OF IL RTE 33 FLUSH WITH THE EXISTING ROADWAY ELEVATION. CONSTRUCT NEW TEMPORARY PIPE CULVERT ON NORTH SIDE OF CENTERLINE. REMOVE WIDENING.
 5. CONSTRUCT WIDENING FOR STAGE II AND SWITCH TRAFFIC TO NORTH SIDE.
 6. CONSTRUCT REMAINDER OF NEW PIPE CULVERT.
- NOTES:**
1. FOR ADDITIONAL DETAILS ON LANE CLOSURE, SEE STANDARD 701316.
 2. SEE SHEET 49 FOR CULVERT INFORMATION.
 3. TRAFFIC CONTROL ITEMS SHALL BE PLACED IN ACCORDANCE WITH TRAFFIC CONTROL STANDARD 701316. THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701316. LOCATION 2. THIS PRICE SHALL INCLUDE ALL NECESSARY TRAFFIC CONTROL WORK FOR STAGE I AND STAGE II, EXCEPT TEMPORARY SIGNALS WILL BE PAID SEPARATELY.



- LEGEND**
- TEMPORARY RAMP
 - WORK AREA
 - BASE COURSE WIDENING
 - BARRICADE OR DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
 - STOP BAR
 - TRAFFIC SIGNALS

EXISTING BOX CULVERTS TO REMAIN IN PLACE UNTIL PROPOSED PIPE CULVERT INSTALLATION IS COMPLETE

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ILLINOIS DEPARTMENT OF TRANSPORTATION

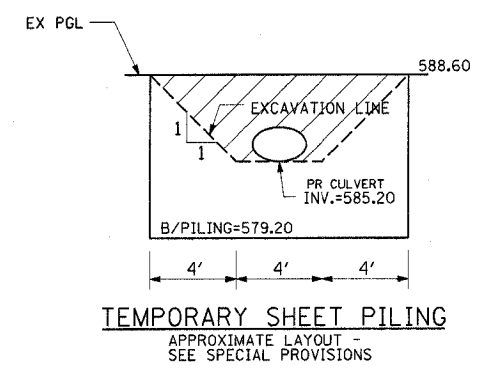
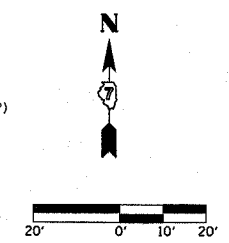
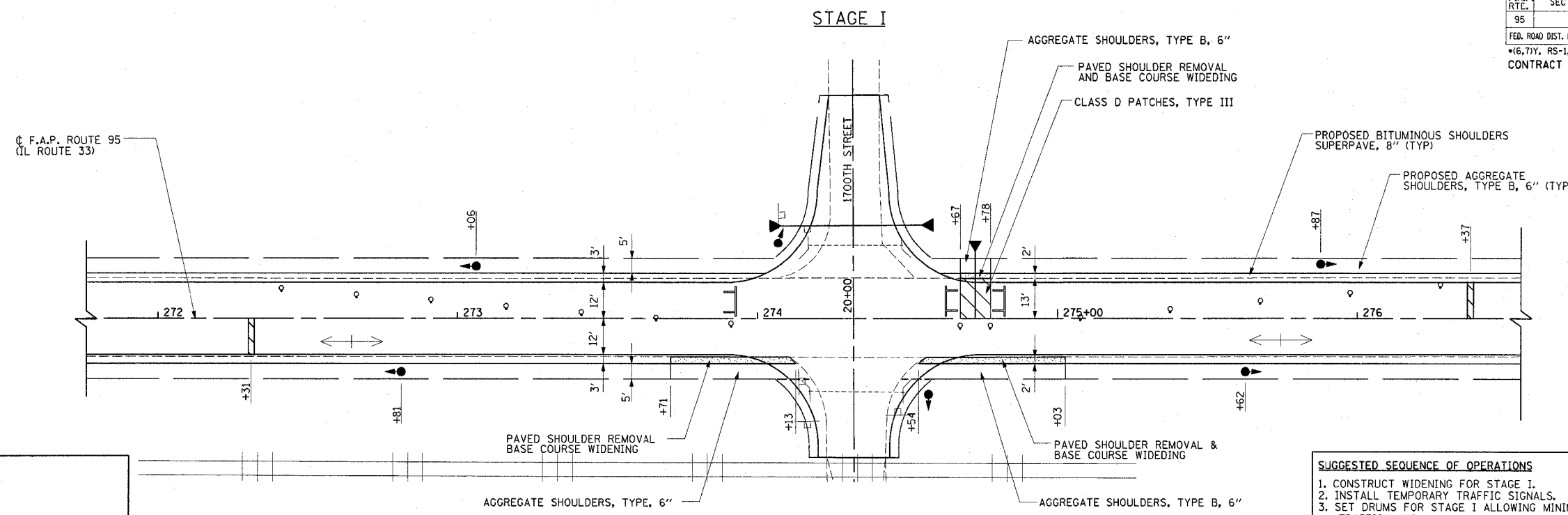
STAGE CONSTRUCTION
NEW 24" EQUIVALENT ELLIPTICAL RCP CULVERT
STA. 221+74.03

DATE 10/01

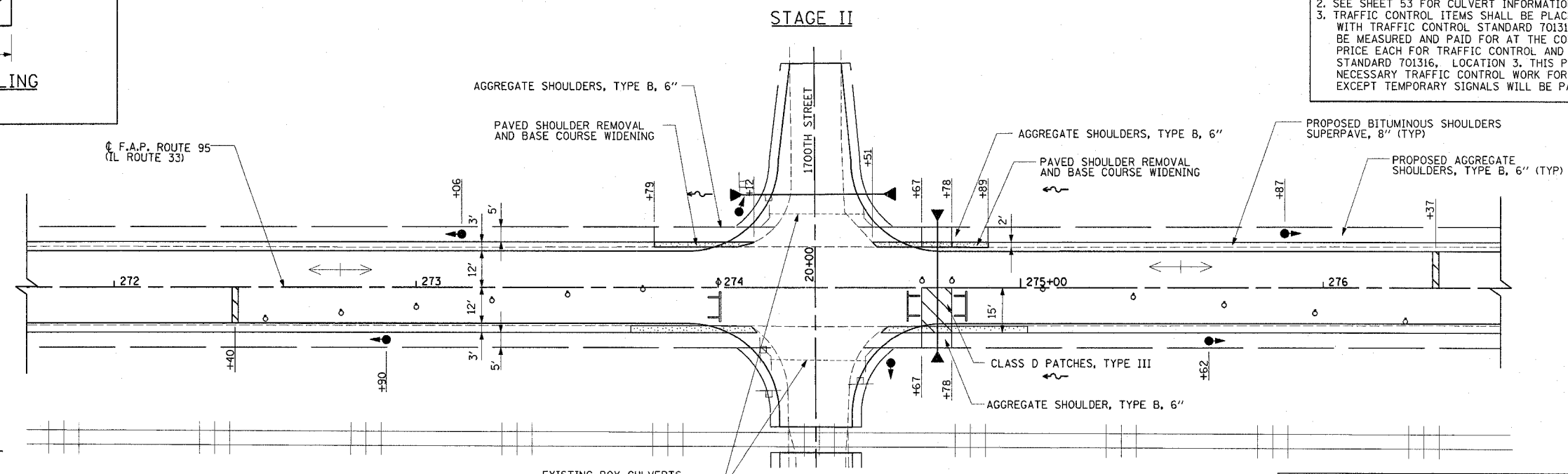
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Revised 5/2/07

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | | EFFINGHAM | 409 | 35 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



- SUGGESTED SEQUENCE OF OPERATIONS**
1. CONSTRUCT WIDENING FOR STAGE I.
 2. INSTALL TEMPORARY TRAFFIC SIGNALS.
 3. SET DRUMS FOR STAGE I ALLOWING MINIMUM 12 FEET FOR TRAFFIC LANE.
 4. DRIVE SHEET PILING AT CENTERLINE OF IL RTE 33 FLUSH WITH THE EXISTING ROADWAY ELEVATION. CONSTRUCT NEW TEMPORARY PIPE CULVERT ON NORTH SIDE OF CENTERLINE. REMOVE WIDENING.
 5. CONSTRUCT WIDENING FOR STAGE II AND SWITCH TRAFFIC TO NORTH SIDE.
 6. CONSTRUCT REMAINDER OF NEW PIPE CULVERT.
- NOTES:**
1. FOR ADDITIONAL DETAILS ON LANE CLOSURE, SEE STANDARD 701316.
 2. SEE SHEET 53 FOR CULVERT INFORMATION.
 3. TRAFFIC CONTROL ITEMS SHALL BE PLACED IN ACCORDANCE WITH TRAFFIC CONTROL STANDARD 701316. THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701316, LOCATION 3. THIS PRICE SHALL INCLUDE ALL NECESSARY TRAFFIC CONTROL WORK FOR STAGE I AND STAGE II, EXCEPT TEMPORARY SIGNALS WILL BE PAID SEPARATELY.



- LEGEND**
- CLASS D PATCHES
 - PAVED SHOULDER REMOVAL AND BASE COURSE WIDENING
 - DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
 - STOP BAR
 - TYPE III BARRICADE
 - TRAFFIC SIGNALS

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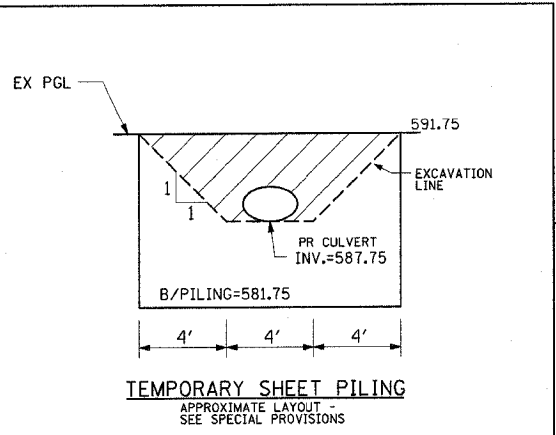
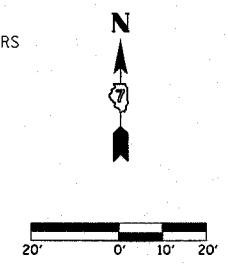
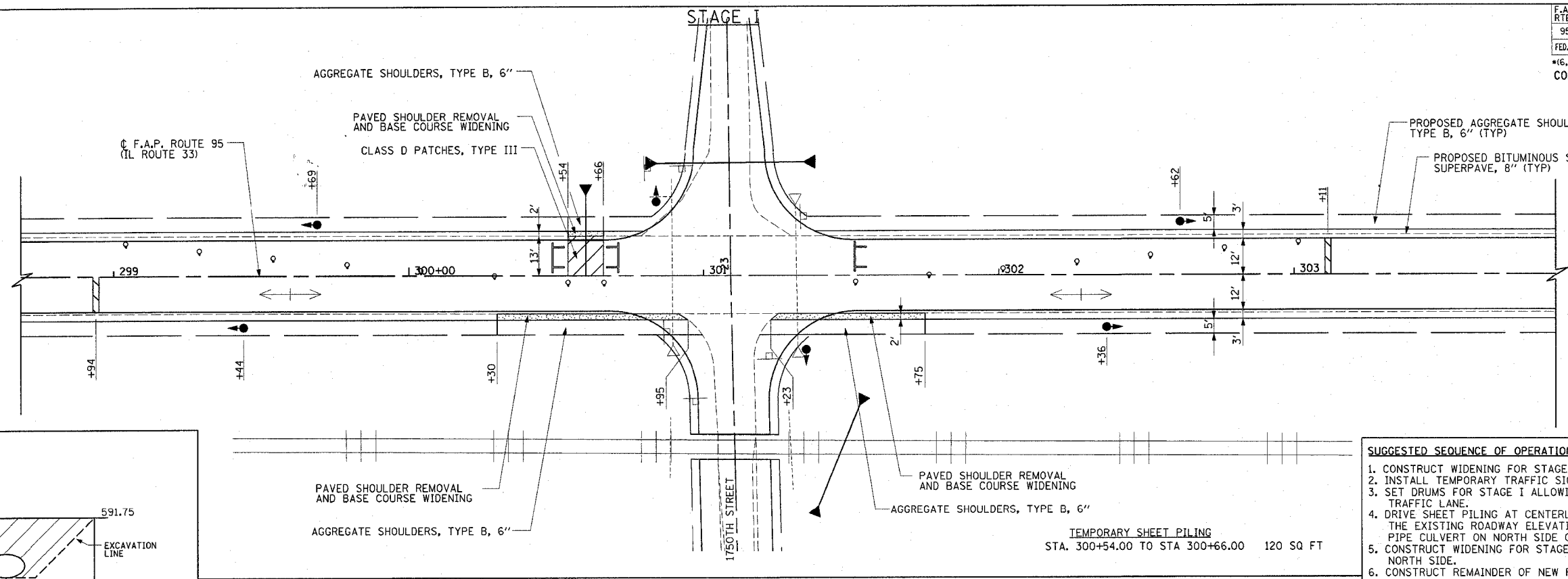
ILLINOIS DEPARTMENT OF TRANSPORTATION

**STAGE CONSTRUCTION
NEW 24" EQUIVALENT
ELLIPTICAL RCP CULVERT
STA. 274+72.57**

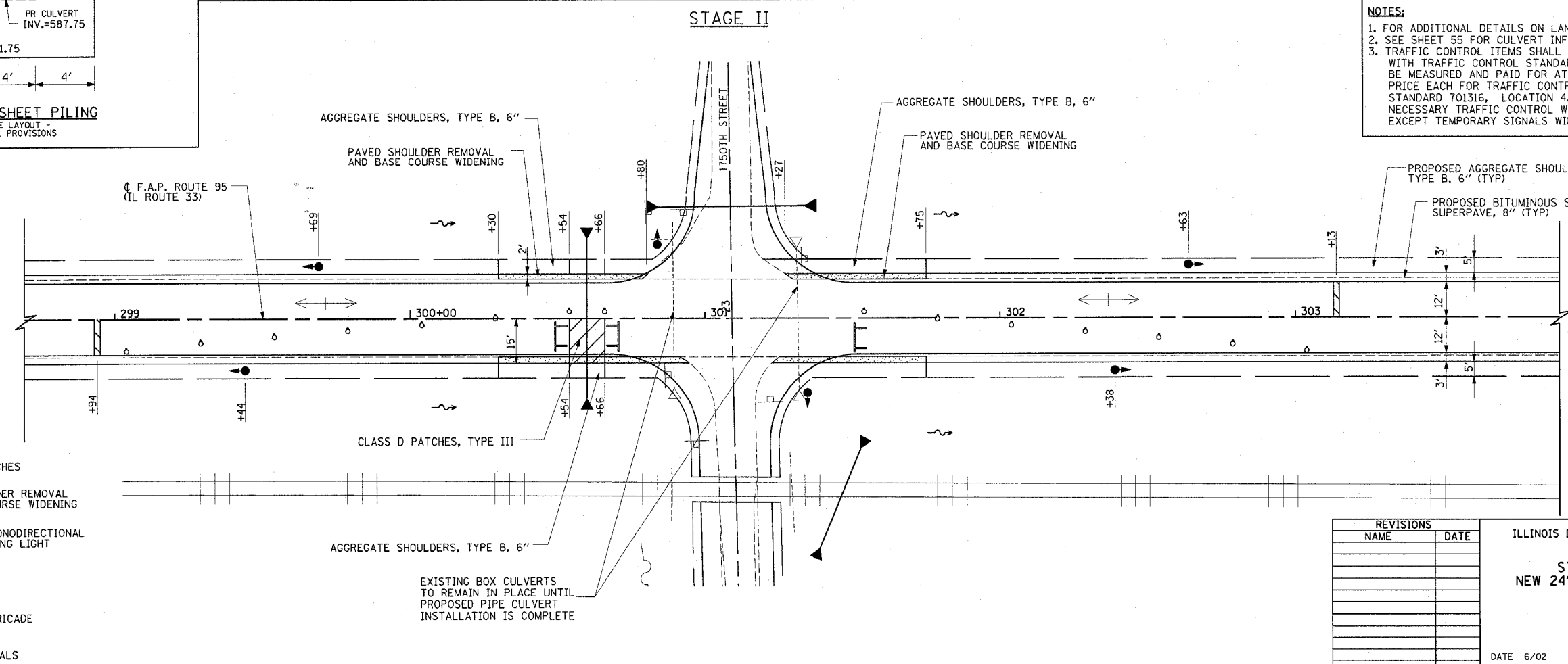
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 36 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *6.7Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



- SUGGESTED SEQUENCE OF OPERATIONS**
1. CONSTRUCT WIDENING FOR STAGE I.
 2. INSTALL TEMPORARY TRAFFIC SIGNALS.
 3. SET DRUMS FOR STAGE I ALLOWING MINIMUM 12 FEET FOR TRAFFIC LANE.
 4. DRIVE SHEET PILING AT CENTERLINE OF IL RTE 33 FLUSH WITH THE EXISTING ROADWAY ELEVATION. CONSTRUCT NEW TEMPORARY PIPE CULVERT ON NORTH SIDE OF CENTERLINE. REMOVE WIDENING.
 5. CONSTRUCT WIDENING FOR STAGE II AND SWITCH TRAFFIC TO NORTH SIDE.
 6. CONSTRUCT REMAINDER OF NEW PIPE CULVERT.
- NOTES:**
1. FOR ADDITIONAL DETAILS ON LANE CLOSURE, SEE STANDARD 701316.
 2. SEE SHEET 55 FOR CULVERT INFORMATION.
 3. TRAFFIC CONTROL ITEMS SHALL BE PLACED IN ACCORDANCE WITH TRAFFIC CONTROL STANDARD 701316. THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701316, LOCATION 4. THIS PRICE SHALL INCLUDE ALL NECESSARY TRAFFIC CONTROL WORK FOR STAGE I AND STAGE II, EXCEPT TEMPORARY SIGNALS WILL BE PAID SEPARATELY.



- LEGEND**
- CLASS D PATCHES
 - PAVED SHOULDER REMOVAL AND BASE COURSE WIDENING
 - DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
 - STOP BAR
 - TYPE III BARRICADE
 - TRAFFIC SIGNALS

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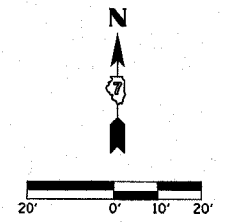
ILLINOIS DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION
NEW 24" ELLIPTICAL EQUIVALENT RCP CULVERT
STA. 300+60.00

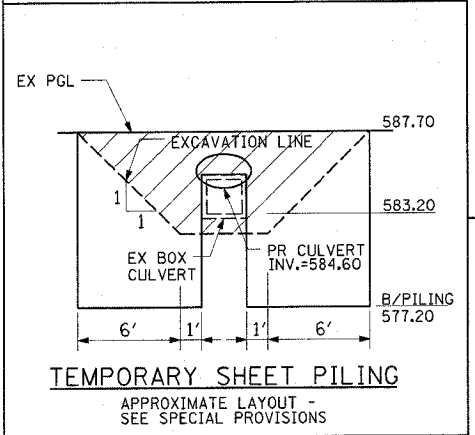
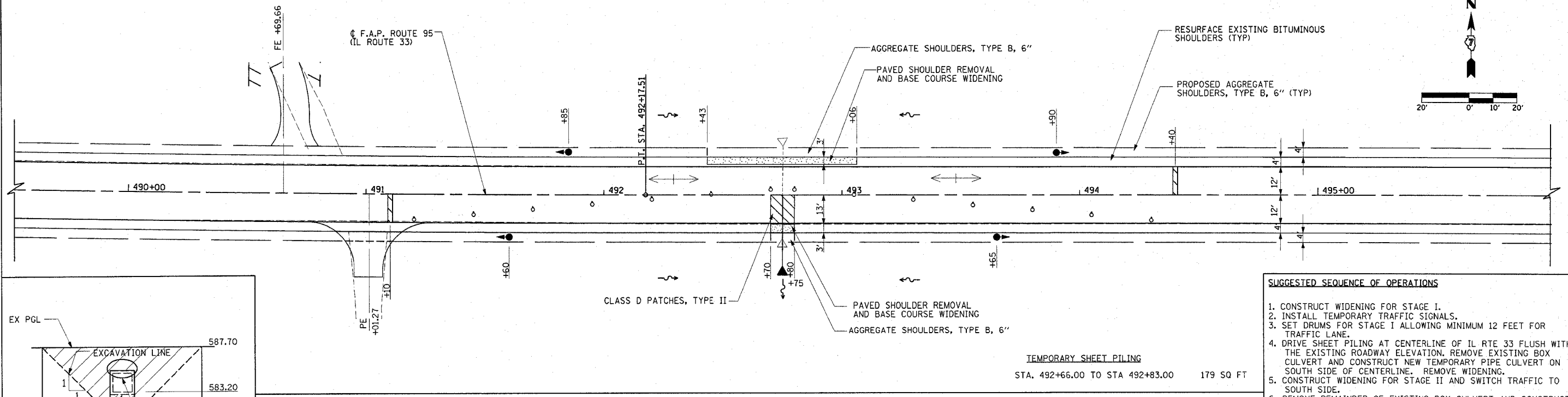
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | | EFFINGHAM | 409 | 37 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *6.7Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

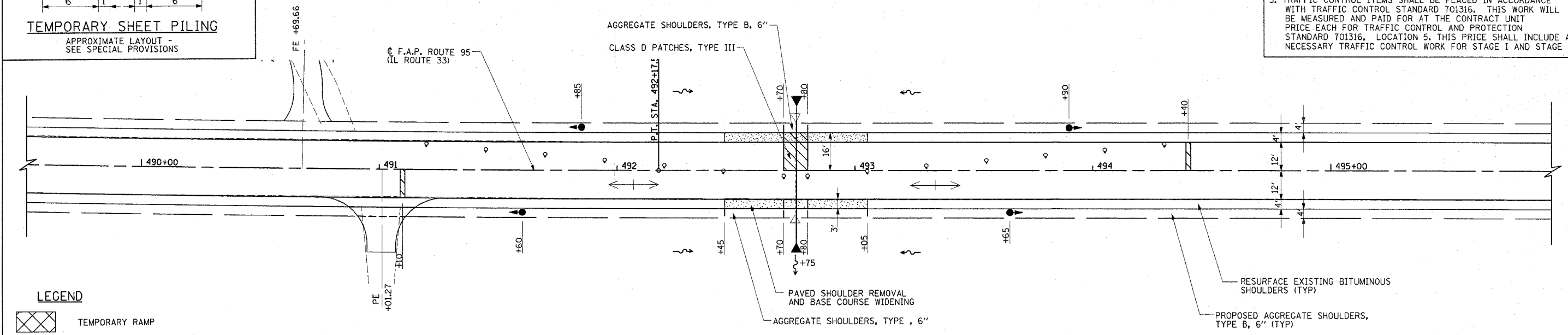


STAGE I



- SUGGESTED SEQUENCE OF OPERATIONS**
1. CONSTRUCT WIDENING FOR STAGE I.
 2. INSTALL TEMPORARY TRAFFIC SIGNALS.
 3. SET DRUMS FOR STAGE I ALLOWING MINIMUM 12 FEET FOR TRAFFIC LANE.
 4. DRIVE SHEET PILING AT CENTERLINE OF IL RTE 33 FLUSH WITH THE EXISTING ROADWAY ELEVATION. REMOVE EXISTING BOX CULVERT AND CONSTRUCT NEW TEMPORARY PIPE CULVERT ON SOUTH SIDE OF CENTERLINE. REMOVE WIDENING.
 5. CONSTRUCT WIDENING FOR STAGE II AND SWITCH TRAFFIC TO SOUTH SIDE.
 6. REMOVE REMAINDER OF EXISTING BOX CULVERT AND CONSTRUCT REMAINDER OF NEW PIPE CULVERT.
- NOTES:**
1. FOR ADDITIONAL DETAILS ON LANE CLOSURE, SEE STANDARD 701316.
 2. SEE SHEET 72 FOR CULVERT INFORMATION.
 3. TRAFFIC CONTROL ITEMS SHALL BE PLACED IN ACCORDANCE WITH TRAFFIC CONTROL STANDARD 701316. THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701316, LOCATION 5. THIS PRICE SHALL INCLUDE ALL NECESSARY TRAFFIC CONTROL WORK FOR STAGE I AND STAGE II.

STAGE II



- LEGEND**
- TEMPORARY RAMP
 - WORK AREA
 - BASE COURSE WIDENING
 - BARRICADE OR DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
 - STOP BAR
 - TRAFFIC SIGNALS

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ILLINOIS DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION
NEW 24" ELLIPTICAL EQUIVALENT
RCP CULVERT
STA. 492+75.11

DATE 10/01

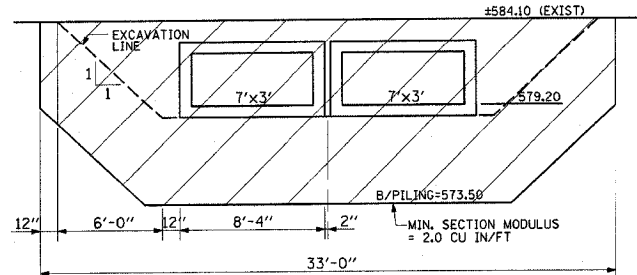
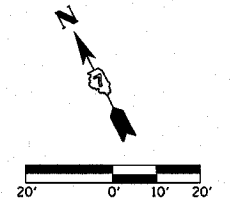
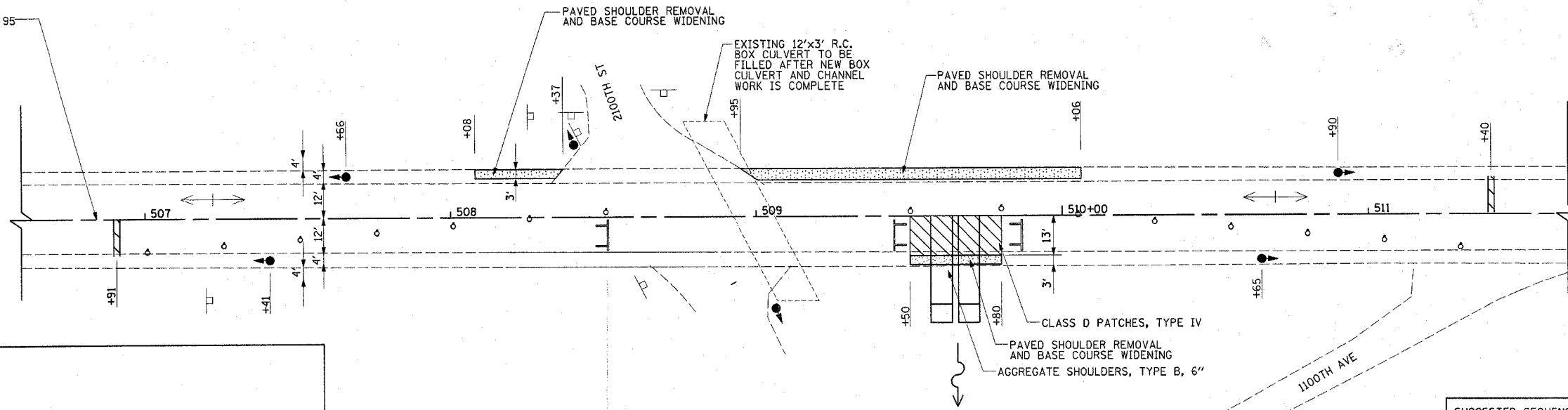
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Revised 5/2/07

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | | EFFINGHAM | 409 | 38 |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT | |
| | | *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | |
| CONTRACT NO. 94356 | | | | |

STAGE I

CL F.A.P. ROUTE 95
(IL ROUTE 33)



TEMPORARY SHEET PILING

APPROXIMATE LAYOUT -
SEE SOIL BORINGS, SHEET 148 B

TEMPORARY SHEET PILING
STA. 509+48.50 TO STA 509+81.50 308 SQ FT

SUGGESTED SEQUENCE OF OPERATIONS

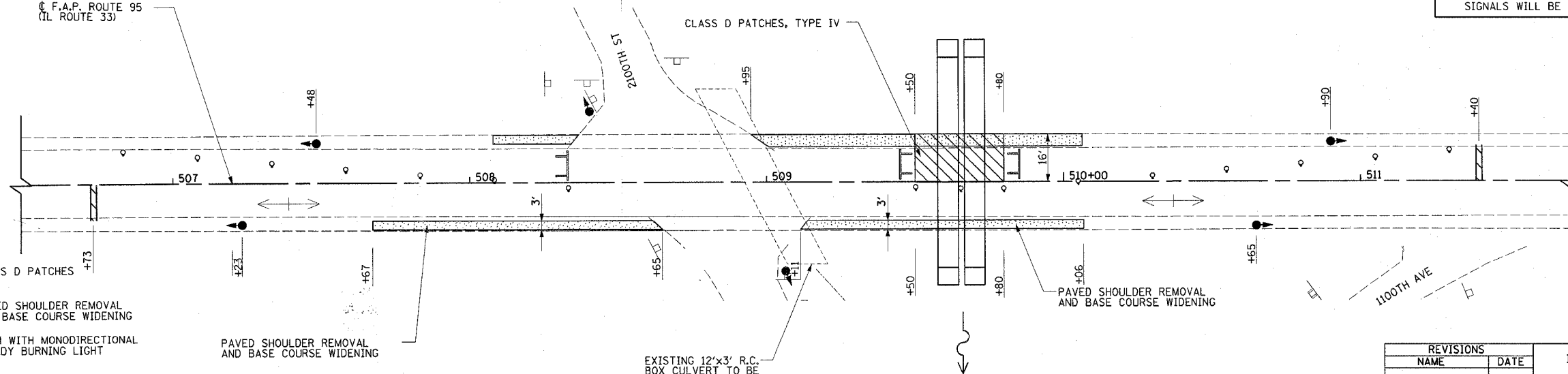
1. CONSTRUCT WIDENING FOR STAGE I.
2. INSTALL TEMPORARY TRAFFIC SIGNALS.
3. SET DRUMS FOR STAGE I ALLOWING MINIMUM 12 FEET FOR TRAFFIC LANE.
4. DRIVE SHEET PILING AT CENTERLINE OF IL RTE 33. CONSTRUCT NEW BOX OR PIPE CULVERT ON SOUTH SIDE OF CENTERLINE. REMOVE WIDENING.
5. CONSTRUCT WIDENING FOR STAGE II AND SWITCH TRAFFIC TO SOUTH SIDE.
6. CONSTRUCT REMAINDER OF NEW BOX OR PIPE CULVERT.

NOTES:

1. DOUBLE BOX CULVERT TO BE CONSTRUCTED PRIOR TO ROADWAY WIDENING. SEE SHEETS 41 AND 42 FOR STAGE CONSTRUCTION AT 2100TH ST AND 1100TH AVE.
2. FOR ADDITIONAL DETAILS ON LANE CLOSURE, SEE STANDARD 701316.
3. SEE SHEET 73 FOR CULVERT INFORMATION.
3. TRAFFIC CONTROL ITEMS SHALL BE PLACED IN ACCORDANCE WITH TRAFFIC CONTROL STANDARD 701316. THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701316, LOCATION 6. THIS PRICE SHALL INCLUDE ALL NECESSARY TRAFFIC CONTROL WORK FOR STAGE I AND STAGE II, EXCEPT TEMPORARY SIGNALS WILL BE PAID SEPARATELY.

STAGE II

CL F.A.P. ROUTE 95
(IL ROUTE 33)



LEGEND

- CLASS D PATCHES
- PAVED SHOULDER REMOVAL AND BASE COURSE WIDENING
- DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT
- STOP BAR
- TYPE III BARRICADE
- TRAFFIC SIGNALS
- PAVED SHOULDER REMOVAL AND BASE COURSE WIDENING

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ILLINOIS DEPARTMENT OF TRANSPORTATION

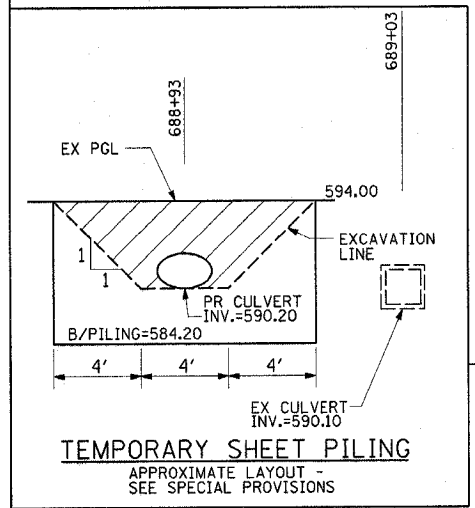
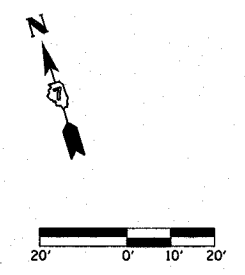
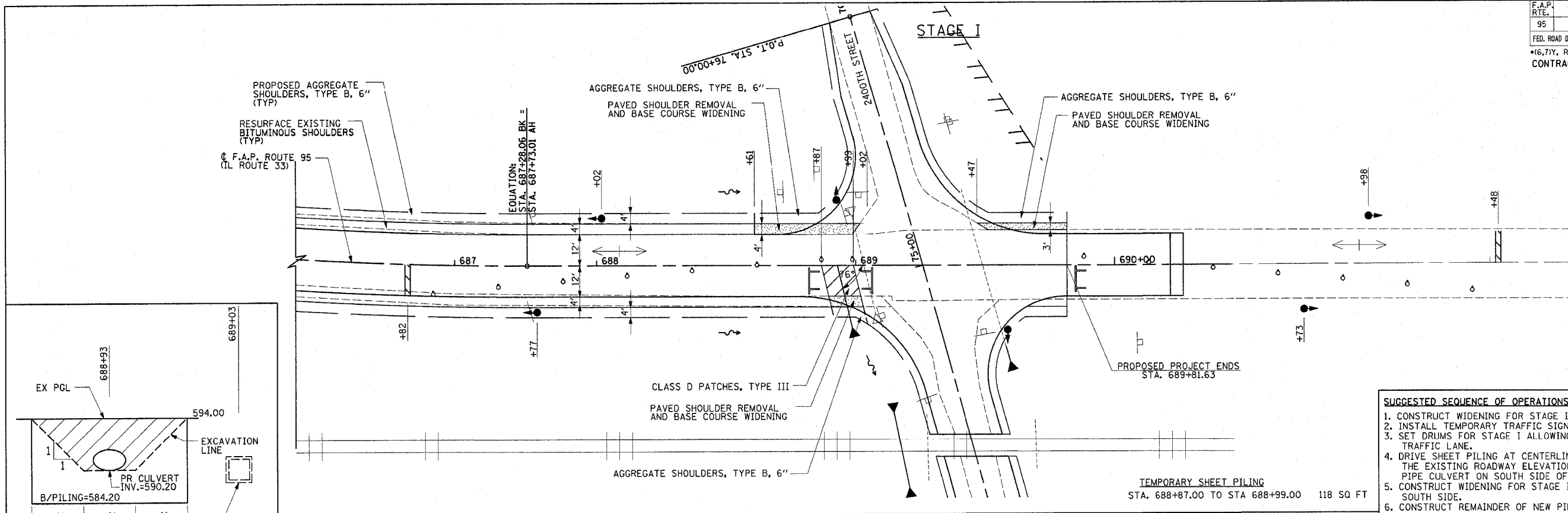
**STAGE CONSTRUCTION
NEW DOUBLE 7' X 3' REINFORCED
CONCRETE BOX CULVERT
STA. 509+65.00**

DATE 10/01

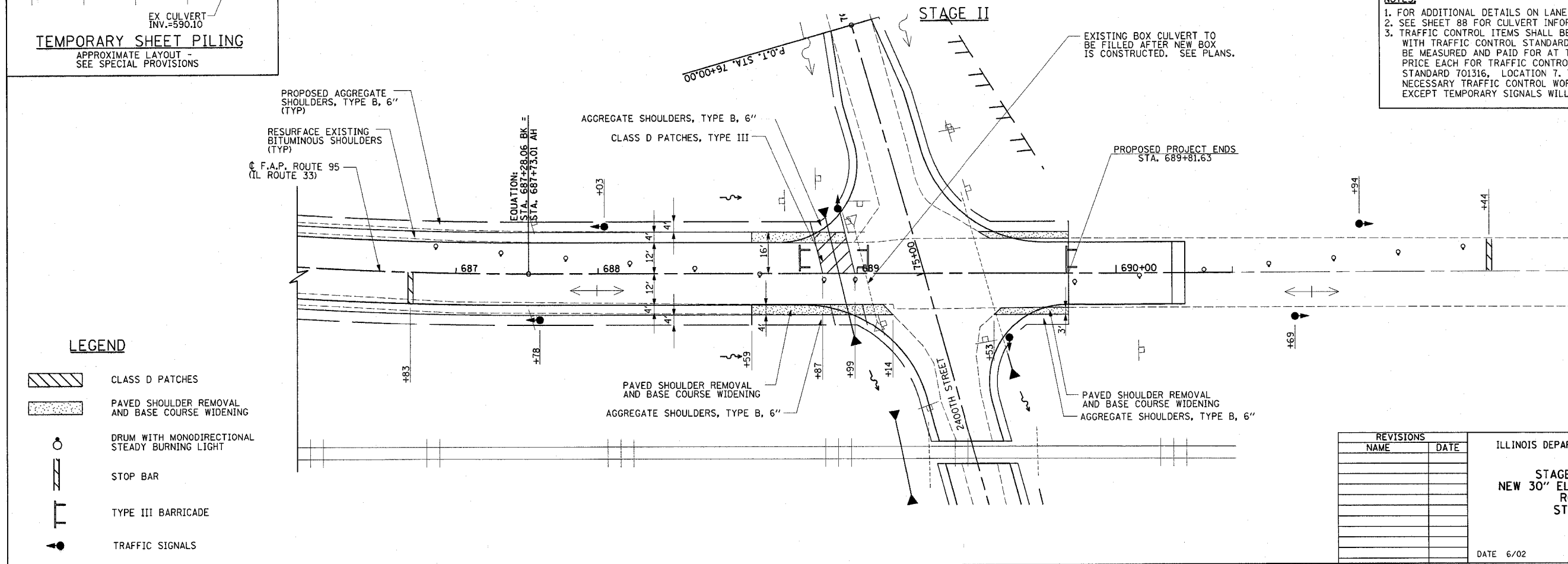
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| 95 | | EFFINGHAM | 409 | 39 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



- SUGGESTED SEQUENCE OF OPERATIONS**
1. CONSTRUCT WIDENING FOR STAGE I.
 2. INSTALL TEMPORARY TRAFFIC SIGNALS.
 3. SET DRUMS FOR STAGE I ALLOWING MINIMUM 12 FEET FOR TRAFFIC LANE.
 4. DRIVE SHEET PILING AT CENTERLINE OF IL RTE 33 FLUSH WITH THE EXISTING ROADWAY ELEVATION. CONSTRUCT NEW TEMPORARY PIPE CULVERT ON SOUTH SIDE OF CENTERLINE. REMOVE WIDENING.
 5. CONSTRUCT WIDENING FOR STAGE II AND SWITCH TRAFFIC TO SOUTH SIDE.
 6. CONSTRUCT REMAINDER OF NEW PIPE CULVERT.
- NOTES:**
1. FOR ADDITIONAL DETAILS ON LANE CLOSURE, SEE STANDARD 701316.
 2. SEE SHEET 88 FOR CULVERT INFORMATION.
 3. TRAFFIC CONTROL ITEMS SHALL BE PLACED IN ACCORDANCE WITH TRAFFIC CONTROL STANDARD 701316. THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701316, LOCATION 7. THIS PRICE SHALL INCLUDE ALL NECESSARY TRAFFIC CONTROL WORK FOR STAGE I AND STAGE II, EXCEPT TEMPORARY SIGNALS WILL BE PAID SEPARATELY.



LEGEND

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| | CLASS D PATCHES |
| | PAVED SHOULDER REMOVAL AND BASE COURSE WIDENING |
| | DRUM WITH MONODIRECTIONAL STEADY BURNING LIGHT |
| | STOP BAR |
| | TYPE III BARRICADE |
| | TRAFFIC SIGNALS |

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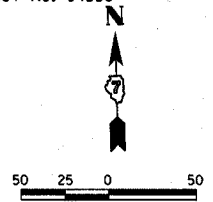
ILLINOIS DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION
NEW 30" ELLIPTICAL EQUIVALENT RCP CULVERT
STA. 688+92.98

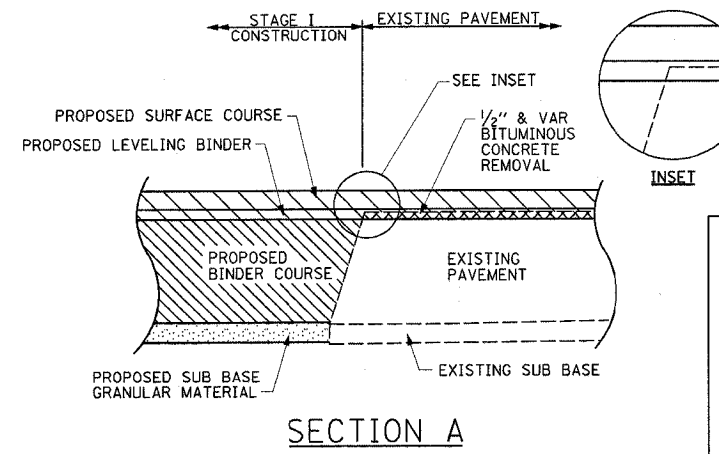
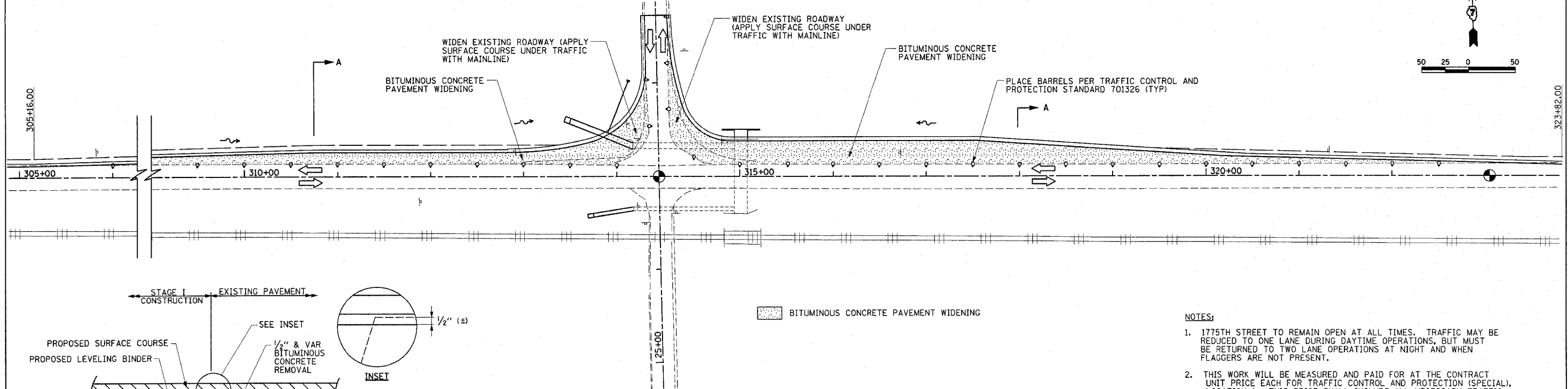
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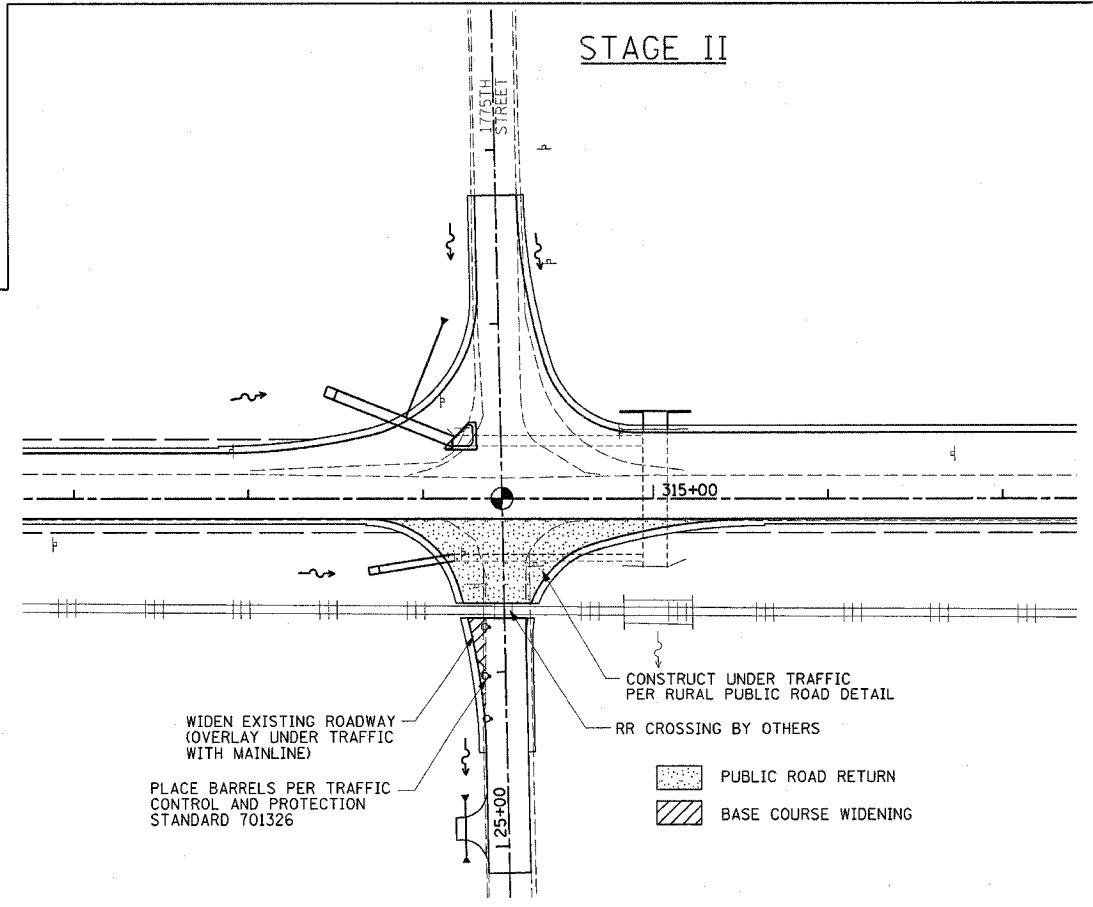
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | | EFFINGHAM | 409 | 40 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



STAGE I



STAGE II



NOTES:

- 1775TH STREET TO REMAIN OPEN AT ALL TIMES. TRAFFIC MAY BE REDUCED TO ONE LANE DURING DAYTIME OPERATIONS, BUT MUST BE RETURNED TO TWO LANE OPERATIONS AT NIGHT AND WHEN FLAGGERS ARE NOT PRESENT.
- THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL), LOCATION 1. THIS PRICE SHALL INCLUDE ALL NECESSARY TRAFFIC CONTROL STANDARDS AND WORK REQUIRED FOR STAGE I AND STAGE II.

SUGGESTED SEQUENCE OF OPERATIONS

STAGE IA

- TRAFFIC CONTINUES AS IS ON 1775TH STREET & IL RTE 33. (UTILIZE TRAFFIC CONTROL STANDARDS 701326 AND 701011).
- CONSTRUCT CULVERT EXTENSIONS (3 LOCATIONS) AND PROVIDE TEMPORARY DRAINAGE TO NEW CULVERT EXTENSIONS.

STAGE IB

- TRAFFIC CONTINUES AS IS ON 1775TH STREET & IL RTE 33. (UTILIZE TRAFFIC CONTROL STANDARDS 701326 AND 701011).
- UNDERCUT NORTH DITCH OF IL RTE 33. BITUMINOUS CONCRETE PAVEMENT WIDENING (EXCEPT FOR SURFACE COURSE) LT, STA. 305+16.00 TO STA. 323+42.00 (SEE SECTION A).
- UNDERCUT EAST & WEST DITCH OF 1775TH STREET NORTH OF IL RTE 33. CONSTRUCT BASE COURSE WIDENING.

STAGE II

- TRAFFIC CONTINUES AS IS ON 1775TH STREET & IL RTE 33. (UTILIZE TRAFFIC CONTROL STANDARDS 701326 AND 701011).
- UNDERCUT EAST AND WEST DITCH OF 1775TH STREET SOUTH OF IL RTE 33. CONSTRUCT BASE COURSE WIDENING.
- REBUILD BASE COURSE OF RADIUS RETURN SOUTH OF IL RTE 33 UNDER TRAFFIC.
- PLACE SURFACE COURSE UNDER TRAFFIC IN CONJUNCTION WITH MAINLINE RESURFACING.

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ILLINOIS DEPARTMENT OF TRANSPORTATION
STAGE I & II CONSTRUCTION
IL RTE 33 AND 1775TH STREET
TRAFFIC CONTROL AND PROTECTION
(SPECIAL), LOCATION 1
 DRAWN BY MLO
 CHECKED BY BKB
 DATE 6/02

Revised 5/2/07

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 41 |
| FED. ROAD DIST. NO. 7 ILLINOIS | | FED. AID PROJECT | | |
| *6,7Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | CONTRACT NO. 94356 | | |

NOTES:

1. THIS WORK WILL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL), LOCATION 2. THIS PRICE SHALL INCLUDE ANY NECESSARY TRAFFIC CONTROL STANDARDS AND ALL TRAFFIC CONTROL WORK REQUIRED FOR STAGE I AND STAGE II.

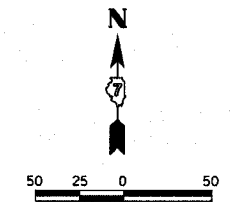
SUGGESTED SEQUENCE OF OPERATIONS

PRE-STAGE I:

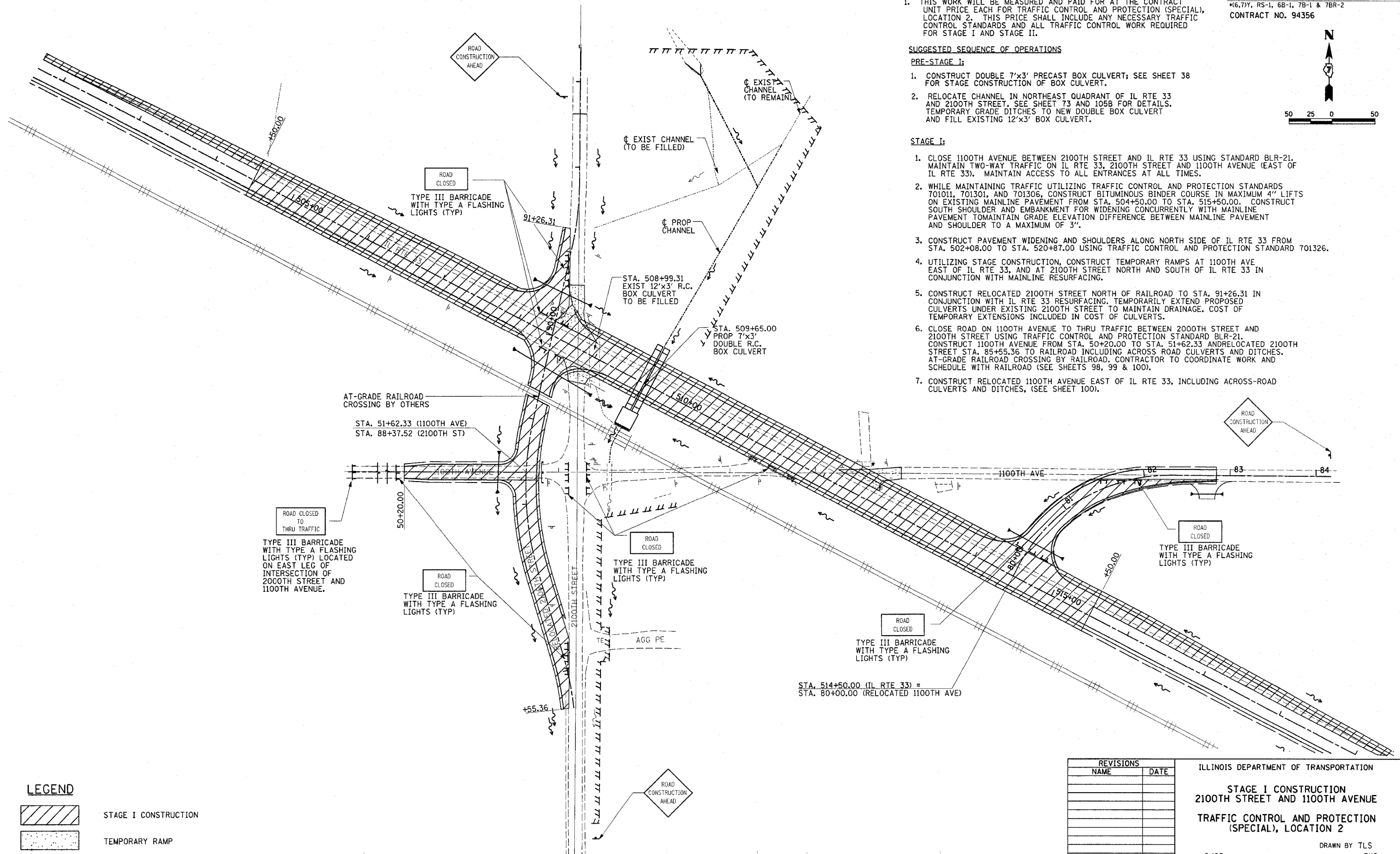
1. CONSTRUCT DOUBLE 7'x3' PRECAST BOX CULVERT; SEE SHEET 38 FOR STAGE CONSTRUCTION OF BOX CULVERT.
2. RELOCATE CHANNEL IN NORTHEAST QUADRANT OF IL RTE 33 AND 2100TH STREET. SEE SHEET 73 AND 105B FOR DETAILS. TEMPORARY GRADE DITCHES TO NEW DOUBLE BOX CULVERT AND FILL EXISTING 12'x3' BOX CULVERT.

STAGE I:

1. CLOSE 1100TH AVENUE BETWEEN 2100TH STREET AND IL RTE 33 USING STANDARD BLR-21. MAINTAIN TWO-WAY TRAFFIC ON IL RTE 33, 2100TH STREET AND 1100TH AVENUE (EAST OF IL RTE 33). MAINTAIN ACCESS TO ALL ENTRANCES AT ALL TIMES.
2. WHILE MAINTAINING TRAFFIC UTILIZING TRAFFIC CONTROL AND PROTECTION STANDARDS 70101, 701301, AND 701306, CONSTRUCT BITUMINOUS BINDER COURSE IN MAXIMUM 4" LIFTS ON EXISTING MAINLINE PAVEMENT FROM STA. 504+50.00 TO STA. 515+50.00. CONSTRUCT SOUTH SHOULDER AND EMBANKMENT FOR WIDENING CONCURRENTLY WITH MAINLINE PAVEMENT TO MAINTAIN GRADE ELEVATION DIFFERENCE BETWEEN MAINLINE PAVEMENT AND SHOULDER TO A MAXIMUM OF 3".
3. CONSTRUCT PAVEMENT WIDENING AND SHOULDERS ALONG NORTH SIDE OF IL RTE 33 FROM STA. 502+08.00 TO STA. 520+87.00 USING TRAFFIC CONTROL AND PROTECTION STANDARD 701326.
4. UTILIZING STAGE CONSTRUCTION, CONSTRUCT TEMPORARY RAMPS AT 1100TH AVE EAST OF IL RTE 33, AND AT 2100TH STREET NORTH AND SOUTH OF IL RTE 33 IN CONJUNCTION WITH MAINLINE RESURFACING.
5. CONSTRUCT RELOCATED 2100TH STREET NORTH OF RAILROAD TO STA. 91+26.31 IN CONJUNCTION WITH IL RTE 33 RESURFACING. TEMPORARILY EXTEND PROPOSED CULVERTS UNDER EXISTING 2100TH STREET TO MAINTAIN DRAINAGE. COST OF TEMPORARY EXTENSIONS INCLUDED IN COST OF CULVERTS.
6. CLOSE ROAD ON 1100TH AVENUE THRU TRAFFIC BETWEEN 2000TH STREET AND 2100TH STREET USING TRAFFIC CONTROL AND PROTECTION STANDARD BLR-21. CONSTRUCT 1100TH AVENUE FROM STA. 50+20.00 TO STA. 51+62.33 AND RELOCATED 2100TH STREET STA. 85+55.36 TO RAILROAD INCLUDING ACROSS ROAD CULVERTS AND DITCHES. AT-GRADE RAILROAD CROSSING BY RAILROAD. CONTRACTOR TO COORDINATE WORK AND SCHEDULE WITH RAILROAD (SEE SHEETS 98, 99 & 100).
7. CONSTRUCT RELOCATED 1100TH AVENUE EAST OF IL RTE 33, INCLUDING ACROSS-ROAD CULVERTS AND DITCHES, (SEE SHEET 100).



STAGE I



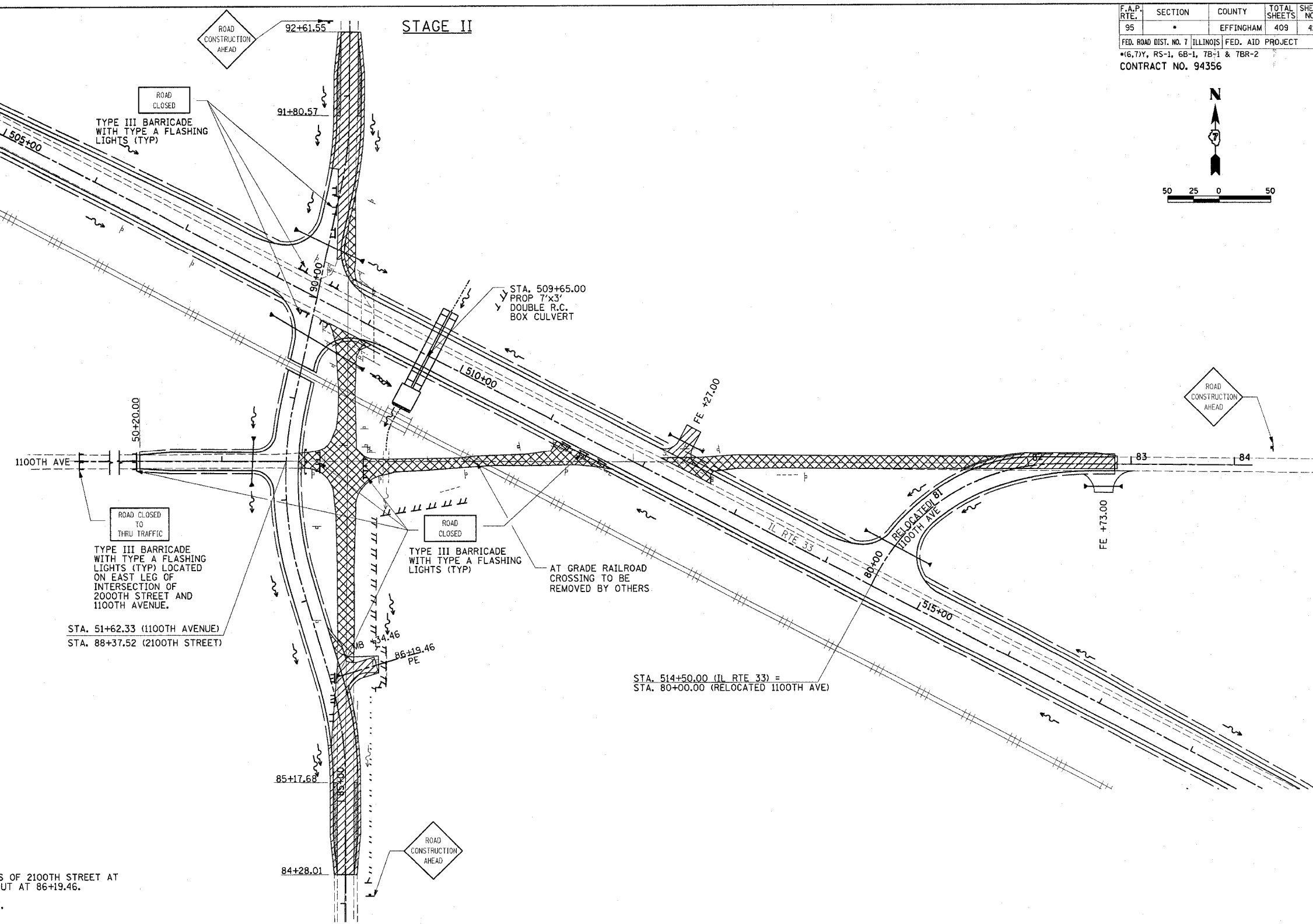
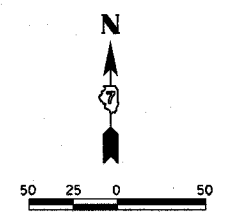
LEGEND

- STAGE I CONSTRUCTION
- TEMPORARY RAMP

| REVISIONS | |
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| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
STAGE I CONSTRUCTION
2100TH STREET AND 1100TH AVENUE
TRAFFIC CONTROL AND PROTECTION
(SPECIAL), LOCATION 2
 DATE 6/02
 DRAWN BY TLS
 CHECKED BY BKB

| | | | | |
|---|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 42 |
| FED. ROAD DIST. NO. 7 (ILLINOIS) FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



SUGGESTED SEQUENCE OF OPERATIONS

STAGE II:

1. MAINTAIN ACCESS TO ENTRANCES AT ALL TIMES.
2. UTILIZING STAGE CONSTRUCTION, CONSTRUCT ALTERNATE SIDES OF 2100TH STREET AT LOCATIONS SHOWN, INCLUDING ENTRANCE AND MAIL BOX TURNOUT AT 86+19.46.
3. SHIFT 2100TH STREET TRAFFIC TO RELOCATED 2100TH STREET.
4. OPEN 1100TH AVENUE WEST OF 2100TH STREET TO TRAFFIC.
5. UTILIZING STAGE CONSTRUCTION, CONSTRUCT ALTERNATE SIDES OF RELOCATED 1100TH ST INCLUDING FIELD ENTRANCE AT 82+73.00.
6. CONSTRUCT FIELD ENTRANCE AT STA. 512+27.00.
7. REMOVE REMAINING 2100TH STREET PAVEMENT AND A-3 SURFACING AT LOCATIONS SHOWN. REMOVE TEMPORARY CULVERT EXTENSION. REGRADE TO EAST DITCH ON 2100TH STREET (SEE CROSS SECTIONS).
8. REMOVE 1100TH AVENUE PAVEMENT AND A-3 SURFACING AT LOCATIONS SHOWN AND REGRADE (SEE CROSS SECTIONS).

LEGEND

- STAGE II CONSTRUCTION
- BASE COURSE WIDENING
- STAGE II REMOVAL

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
STAGE II CONSTRUCTION
2100TH STREET AND 1100TH AVENUE
TRAFFIC CONTROL AND PROTECTION
(SPECIAL), LOCATION 2
 DRAWN BY TLS
 CHECKED BY BKB
 DATE 6/02

CONCRETE CURB TRANSITION
 LT STA. 145+34.00 - STA. 145+44.00 10'
 RT STA. 145+34.00 - STA. 145+44.00 10'
 TOTAL 20'

PAVEMENT REMOVAL
 STA. 145+38.00 - STA. 145+99.00 176 SQ YD
 STA. 147+28.00 - STA. 147+87.00 170 SQ YD
 STA. 145+38.00 - STA. 145+99.00 20 SQ YD
 STA. 147+28.00 - STA. 147+87.00 20 SQ YD
 TOTAL 386 SQ YD

BITUMINOUS SURFACE REMOVAL - BUTT JOINT
 STA. 142+00.00 - STA. 142+30.00 87 SQ YD

TEMPORARY RAMP
 STA. 142+00.00 - STA. 142+05.00 14 SQ YD

REMOVE INLET BOX
 LT STA. 145+63.62 1 EACH
 RT STA. 145+65.27 1 EACH
 LT STA. 147+50.99 1 EACH
 RT STA. 147+51.19 1 EACH
 TOTAL 4 EACH

GUTTER OUTLET REMOVAL
 LT STA. 149+51.60 - STA. 150+00.00 48'
 RT STA. 149+54.80 - STA. 150+00.00 45'
 TOTAL 93'

PERMANENT SURVEY MARKERS, TYPE 1
 P.O.T. STA. 150+00.00 1 EACH

FILLING EXISTING CULVERTS
 Q STA. 154+75.34 12 CU YD

NOTE: EXIST WATER LINE AND GAS LINE ABANDONED STA. 146+00.00 TO STA. 180+00.00. WHEN ENCOUNTERED DURING CONSTRUCTION WATER AND GAS LINE TO BE REMOVED. COST INCLUDED IN COST OF EARTHWORK

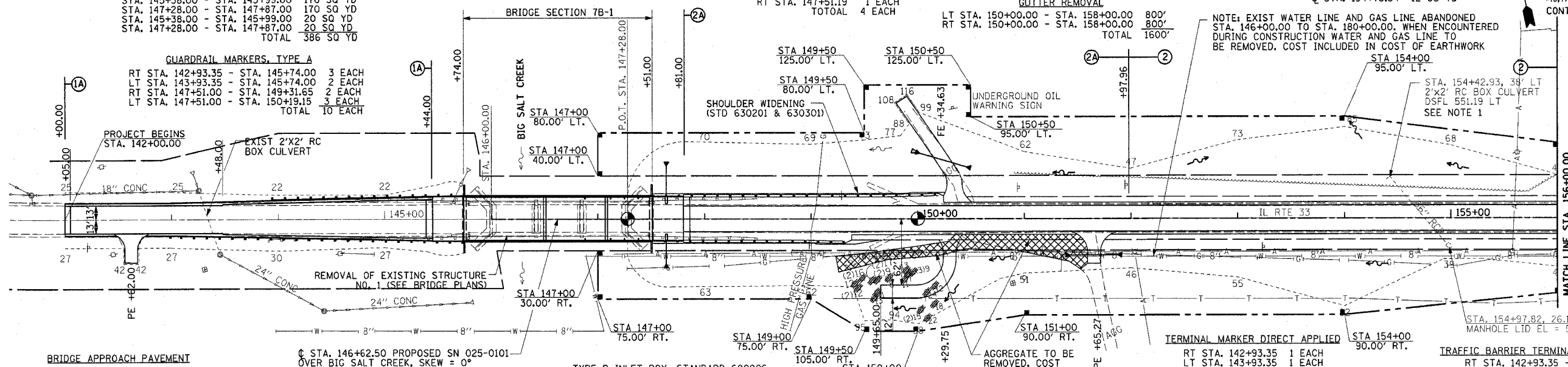
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 43 |
| STA. 142+00.00 TO STA. 156+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *6.7Y, RS-1, 6B-1, 7B-1 & 7B-2 | | | | |
| CONTRACT NO. 94356 | | | | |



NOTE 1:
 PARTIALLY REMOVE HEADWALL AS REQUIRED TO CLEAR FINAL GRADING. COST INCLUDED IN COST OF FILLING EXISTING CULVERTS.

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

| | | |
|---------------|-------------------|--------|
| 30' | RT STA. 147+00.00 | 1 EACH |
| 75' | RT STA. 147+00.00 | 1 EACH |
| 40' | LT STA. 147+00.00 | 1 EACH |
| 80' | LT STA. 147+00.00 | 1 EACH |
| 75' | RT STA. 149+00.00 | 1 EACH |
| 125' | LT STA. 149+00.00 | 1 EACH |
| 108' | RT STA. 149+50.00 | 1 EACH |
| 80' | LT STA. 149+50.00 | 1 EACH |
| 105' | RT STA. 150+00.00 | 1 EACH |
| 125' | LT STA. 150+00.00 | 1 EACH |
| 95' | LT STA. 150+50.00 | 1 EACH |
| 90' | RT STA. 151+00.00 | 1 EACH |
| 90' | RT STA. 154+00.00 | 1 EACH |
| 95' | LT STA. 154+00.00 | 1 EACH |
| TOTAL 14 EACH | | |



BRIDGE APPROACH PAVEMENT
 STA. 145+44.00 - STA. 145+74.00 137 SQ YD
 STA. 147+51.00 - STA. 147+81.00 137 SQ YD
 TOTAL 274 SQ YD

BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)
 STA. 145+38.00 - STA. 145+44.00 27 SQ YD
 STA. 147+81.00 - STA. 147+87.00 27 SQ YD
 TOTAL 54 SQ YD

COMBINATION CONCRETE CURB & GUTTER REMOVAL
 LT STA. 142+00.00 - STA. 145+79.00 379'
 RT STA. 142+00.00 - STA. 145+79.00 379'
 TOTAL 758'

COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24 (ABUTTING EXISTING PAVEMENT)
 LT STA. 142+00.00 - STA. 145+34.00 334'
 RT STA. 142+00.00 - STA. 145+34.00 334'
 TOTAL 668'

TIE BARS
 LT STA. 142+00.00 - STA. 145+44.00 138 EACH
 RT STA. 142+00.00 - STA. 145+44.00 138 EACH
 TOTAL 276 EACH

TYPE D INLET BOX, STANDARD 609006
 LT STA. 147+64.17 1 EACH
 RT STA. 147+64.17 1 EACH
 TOTAL 2 EACH

PIPE DRAINS, 12"
 LT STA. 147+64.17 49'
 RT STA. 147+64.17 42'
 TOTAL 91'

CONCRETE THRUST BLOCKS
 LT STA. 147+64.17 1 EACH
 RT STA. 147+64.17 1 EACH
 TOTAL 2 EACH

END SECTIONS, 12"
 LT STA. 147+64.17 1 EACH
 RT STA. 147+64.17 1 EACH
 TOTAL 2 EACH

GUARDRAIL REMOVAL
 RT STA. 143+90.25 - STA. 145+79.43 189'
 LT STA. 144+65.70 - STA. 145+79.57 114'
 RT STA. 147+48.38 - STA. 148+49.27 101'
 LT STA. 147+48.32 - STA. 148+49.44 101'
 TOTAL 505'

BARRIER WALL MARKERS, TYPE B
 RT STA. 145+74.00 - STA. 147+51.00 2 EACH
 LT STA. 145+74.00 - STA. 147+51.00 2 EACH
 TOTAL 4 EACH

TRAFFIC BARRIER TERMINAL TYPE 6
 LT & RT STA. 145+43.35 - STA. 145+74.00 2 EACH
 LT & RT STA. 147+51.00 - STA. 147+81.65 2 EACH
 TOTAL 4 EACH

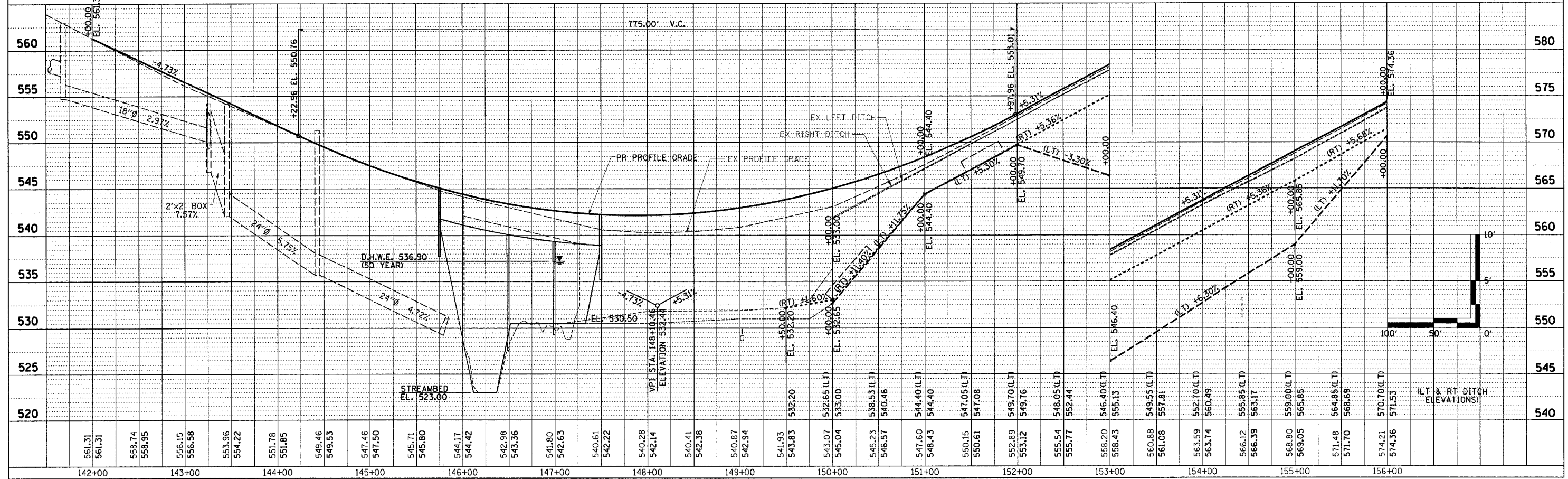
TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (TANGENT)
 RT STA. 142+93.35 - STA. 143+43.35 1 EACH
 LT STA. 143+93.35 - STA. 144+43.35 1 EACH
 RT STA. 148+81.65 - STA. 149+31.65 1 EACH
 LT STA. 149+69.15 - STA. 150+19.15 1 EACH
 TOTAL 4 EACH

STEEL PLATE BEAM GUARDRAIL, TYPE A
 RT STA. 143+43.35 - STA. 145+43.35 200.0'
 LT STA. 144+43.35 - STA. 145+43.35 100.0'
 LT STA. 147+81.65 - STA. 149+69.15 187.5'
 RT STA. 147+81.65 - STA. 148+81.65 100.0'
 TOTAL 587.5'

FILLING CATCH BASIN
 26' RT STA. 154+97.82 1 EACH

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PIPE CULVERTS, CLASS D, TYPE 1
EQUIVALENT ROUND-SIZE, 15"
STA. 160+04.70, 31' RT
LENGTH = 73'
UPSTREAM FLOWLINE = 587.09
DOWNSTREAM FLOWLINE = 584.98

PCRC 2'x2' EXTENSIONS
STA. 162+82.39
LENGTH = 5' LT
DOWNSTREAM FLOWLINE = 584.56 LT

MANHOLE TYPE A, 6' DIA., TY 1
FRAM & CLOSED LID
STA. 162+82.46, 30' LT
TOP LID = 589.30
30" NORTH = 571.50
2'x2' BOX SOUTH = 584.56

PIPE CULVERTS, CLASS A, TYPE 1, 30"
STA. 162+81.80, 33' LT TO
STA. 162+73.00, 63.2' LT
LENGTH = 31'
UPSTREAM FLOWLINE = 571.50
DOWNSTREAM FLOWLINE = 570.00

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

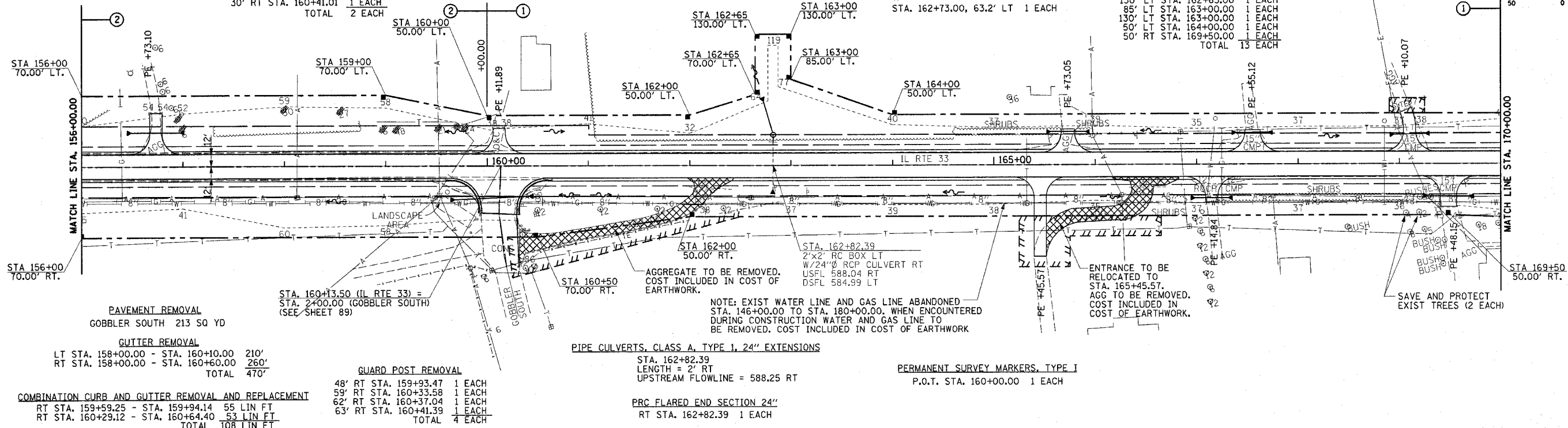
70' LT STA. 156+00.00 1 EACH
70' RT STA. 156+00.00 1 EACH
70' LT STA. 159+00.00 1 EACH
50' LT STA. 160+00.00 1 EACH
70' RT STA. 160+50.00 1 EACH
50' LT STA. 162+00.00 1 EACH
50' RT STA. 162+00.00 1 EACH
70' LT STA. 162+65.00 1 EACH
130' LT STA. 162+65.00 1 EACH
85' LT STA. 163+00.00 1 EACH
130' LT STA. 163+00.00 1 EACH
50' LT STA. 164+00.00 1 EACH
50' RT STA. 169+50.00 1 EACH
TOTAL 13 EACH

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 44 |
| STA. 156+00.00 TO STA. 170+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *6.7Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

END SECTIONS, EQUIVALENT ROUND-SIZE, 15"

32' RT STA. 159+68.22 1 EACH
30' RT STA. 160+41.01 1 EACH
TOTAL 2 EACH

PRECAST REINFORCED CONCRETE FLARED
END SECTION, 30"
STA. 162+73.00, 63.2' LT 1 EACH



PAVEMENT REMOVAL
GOBBLER SOUTH 213 SQ YD

GUTTER REMOVAL
LT STA. 158+00.00 - STA. 160+10.00 210'
RT STA. 158+00.00 - STA. 160+60.00 260'
TOTAL 470'

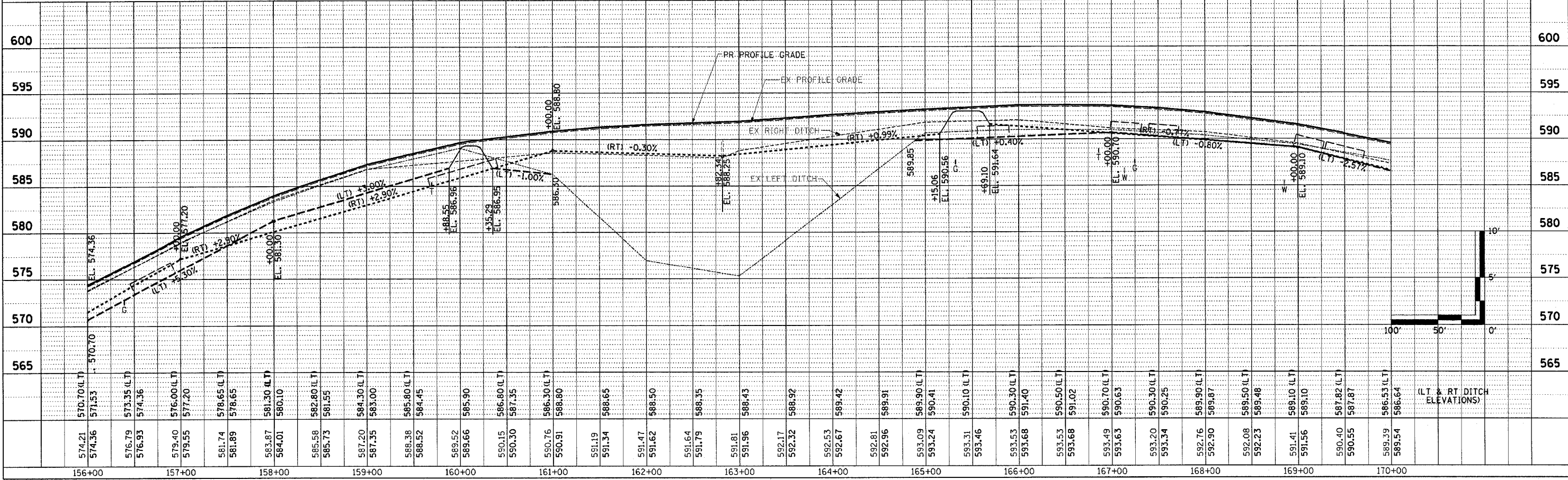
COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT
RT STA. 159+59.25 - STA. 159+94.14 55 LIN FT
RT STA. 160+29.12 - STA. 160+64.40 53 LIN FT
TOTAL 108 LIN FT

GUARD POST REMOVAL
48' RT STA. 159+93.47 1 EACH
59' RT STA. 160+33.58 1 EACH
62' RT STA. 160+37.04 1 EACH
63' RT STA. 160+41.39 1 EACH
TOTAL 4 EACH

PIPE CULVERTS, CLASS A, TYPE 1, 24" EXTENSIONS
STA. 162+82.39
LENGTH = 2' RT
UPSTREAM FLOWLINE = 588.25 RT

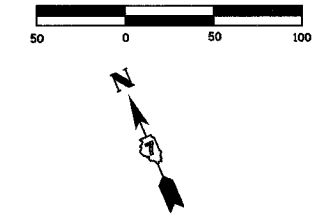
PRC FLARED END SECTION 24"
RT STA. 162+82.39 1 EACH

PERMANENT SURVEY MARKERS, TYPE 1
P.O.T. STA. 160+00.00 1 EACH



Revised 5/2/07

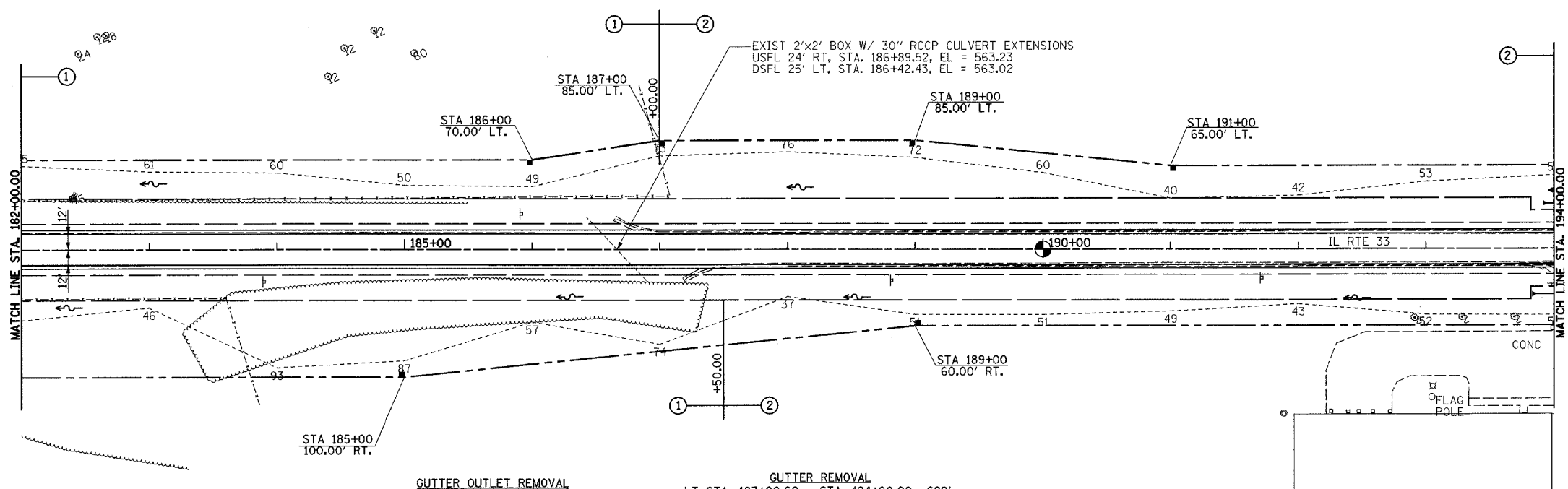
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 46 |
| STA. 182+00.00 TO STA. 194+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7B-2 | | | | |
| CONTRACT NO. 94356 | | | | |



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| NO. 1 | |
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| NO. 4 | |
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| NO. 6 | |
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| NO. 9 | |
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| NO. 10 | |

FILLING EXISTING CULVERTS
 STA. 186+65.98 10 CU YD

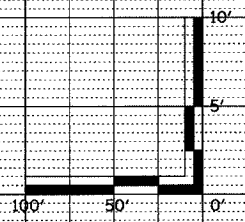
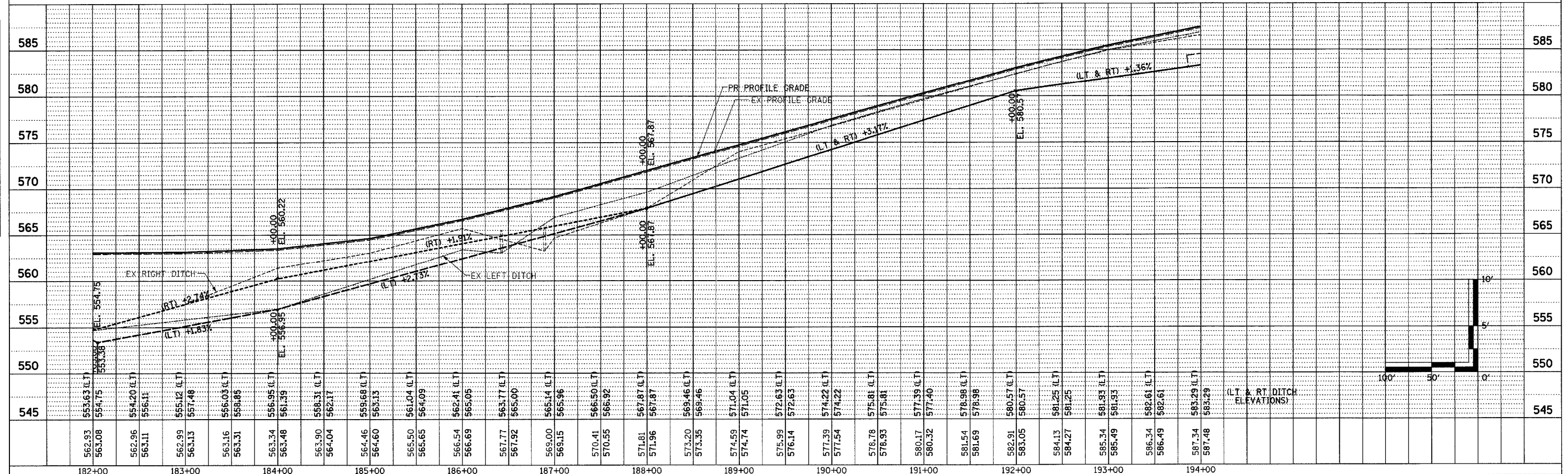


| GUTTER OUTLET REMOVAL | | GUTTER REMOVAL | |
|------------------------------------|-----|------------------------------------|-------|
| LT STA. 186+63.60 - STA. 187+00.60 | 37' | LT STA. 187+00.60 - STA. 194+00.00 | 699' |
| RT STA. 187+18.00 - STA. 187+52.70 | 35' | RT STA. 187+52.70 - STA. 194+00.00 | 647' |
| TOTAL | 72' | TOTAL | 1346' |

PERMANENT SURVEY MARKERS, TYPE I
 P.O.T. STA. 190+00.00 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

| | |
|------------------------|--------|
| 70' LT STA. 181+64.21 | 1 EACH |
| 100' RT STA. 181+64.21 | 1 EACH |
| 100' RT STA. 185+00.00 | 1 EACH |
| 70' LT STA. 186+00.00 | 1 EACH |
| 85' LT STA. 187+00.00 | 1 EACH |
| 85' LT STA. 189+00.00 | 1 EACH |
| 60' RT STA. 189+00.00 | 1 EACH |
| 65' LT STA. 191+00.00 | 1 EACH |
| TOTAL | 8 EACH |



PIPE CULVERTS, CLASS A, TYPE 1 30" EXTENSIONS

STA. 198+62.63
 LENGTH = 5' LT
 LENGTH = 9' RT
 UPSTREAM FLOWLINE = 586.77 LT
 DOWNSTREAM FLOWLINE = 585.95 RT

GUTTER REMOVAL

LT STA. 194+00.00 - STA. 197+20.00 320'
 RT STA. 194+00.00 - STA. 197+20.00 320'
 LT STA. 201+34.68 15'
 TOTAL 655'

CORRUGATED MEDIAN

LT STA. 201+34.68 425 SQ FT

PCBC 3'x2' EXTENSIONS

STA. 203+98.97
 LENGTH = 8' LT
 LENGTH = 9' RT
 UPSTREAM FLOWLINE = 589.68 LT
 DOWNSTREAM FLOWLINE = 589.39 RT

REMOVE CONCRETE BOX CULVERT END SECTION

LT & RT STA. 203+98.97 2 EACH

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 47 |
| STA. 194+00.00 TO STA. 206+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

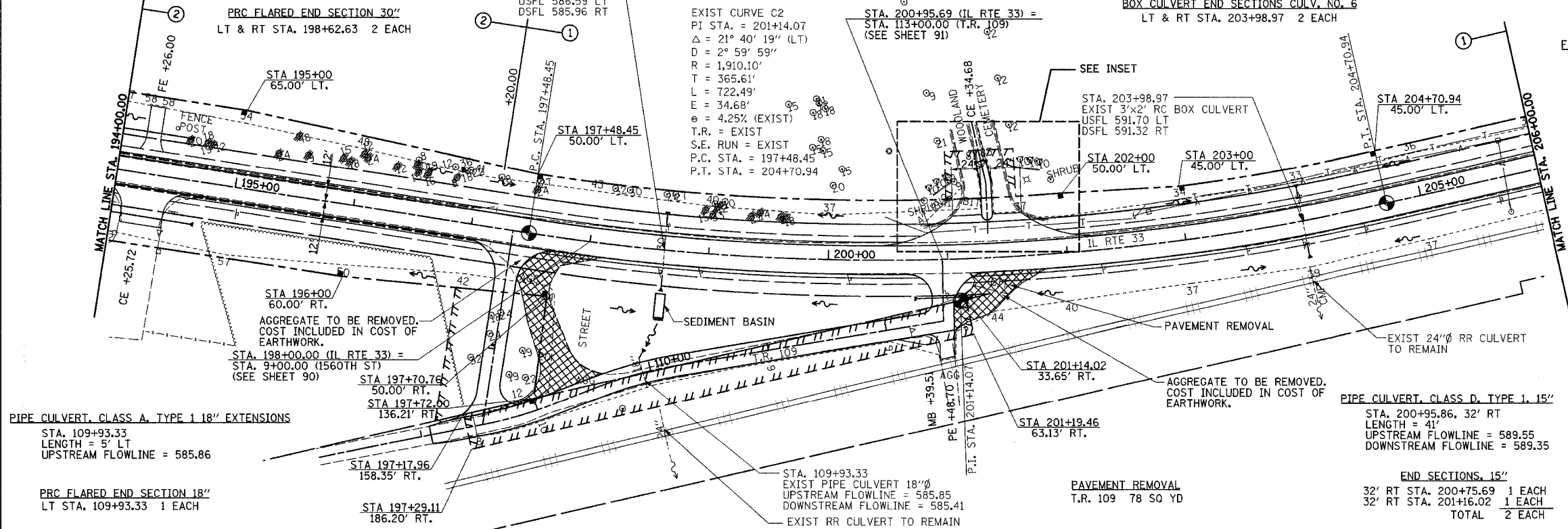


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FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

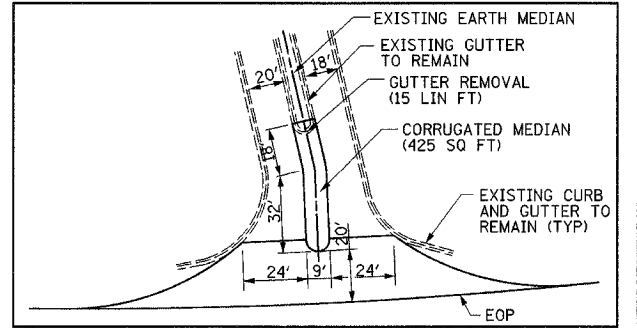
- 65' LT STA. 195+00.00 1 EACH
- 60' RT STA. 196+00.00 1 EACH
- 50' LT STA. 197+48.45 1 EACH
- 50' RT STA. 197+70.76 1 EACH
- 136.21 RT STA. 197+72.00 1 EACH
- 50' LT STA. 202+00.00 1 EACH
- 45' LT STA. 203+00.00 1 EACH
- 45' LT STA. 204+70.94 1 EACH
- TOTAL 8 EACH

PERMANENT SURVEY MARKERS, TYPE I

- P.C. STA. 197+48.45 1 EACH
- P.T. STA. 204+70.94 1 EACH
- TOTAL 2 EACH

PERMANENT SURVEY MARKERS, TYPE II

- P.I. STA. 201+14.07 1 EACH



PIPE CULVERT, CLASS A, TYPE 1 18" EXTENSIONS

STA. 109+93.33
 LENGTH = 5' LT
 UPSTREAM FLOWLINE = 585.86

AGGREGATE TO BE REMOVED.
 COST INCLUDED IN COST OF EARTHWORK.
 STA. 198+00.00 (IL RTE 33) =
 STA. 9+00.00 (1560TH ST)

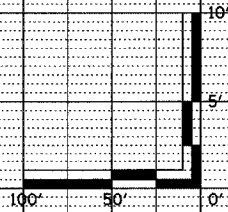
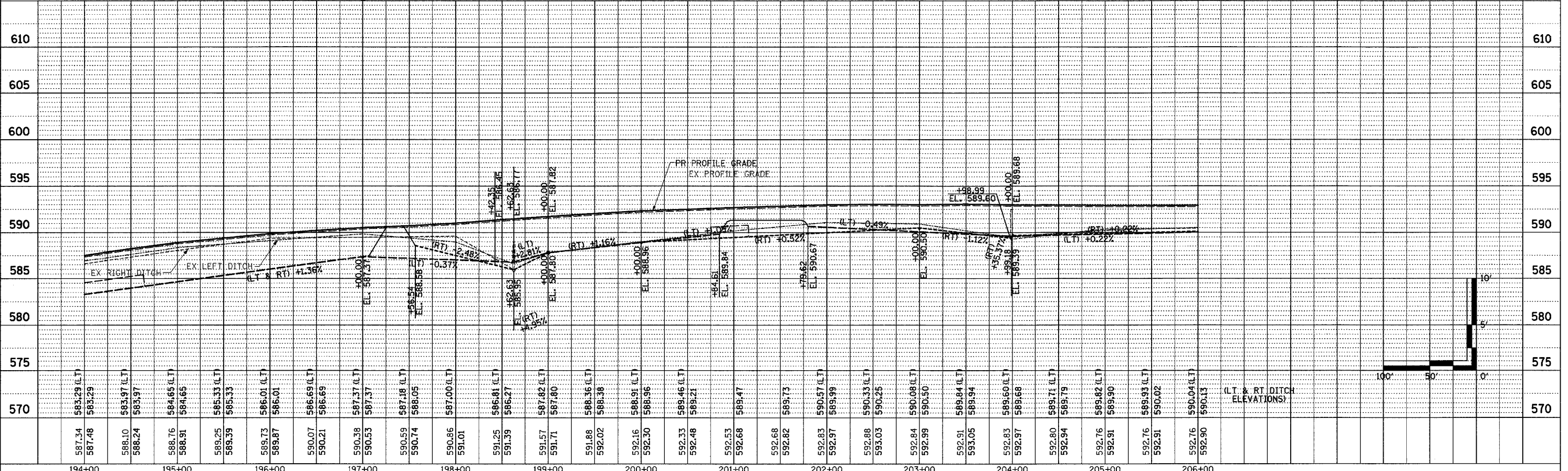
EXIST RR CULVERT TO REMAIN

PAVEMENT REMOVAL
 T.R. 109 78 SQ YD

PIPE CULVERT, CLASS D, TYPE 1, 15"
 STA. 200+95.86, 32' RT
 LENGTH = 4'
 UPSTREAM FLOWLINE = 589.55
 DOWNSTREAM FLOWLINE = 589.35

END SECTIONS, 15'

32' RT STA. 200+75.69 1 EACH
 32' RT STA. 201+16.02 1 EACH
 TOTAL 2 EACH



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 48 |
| STA. 206+00.00 TO STA. 217+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



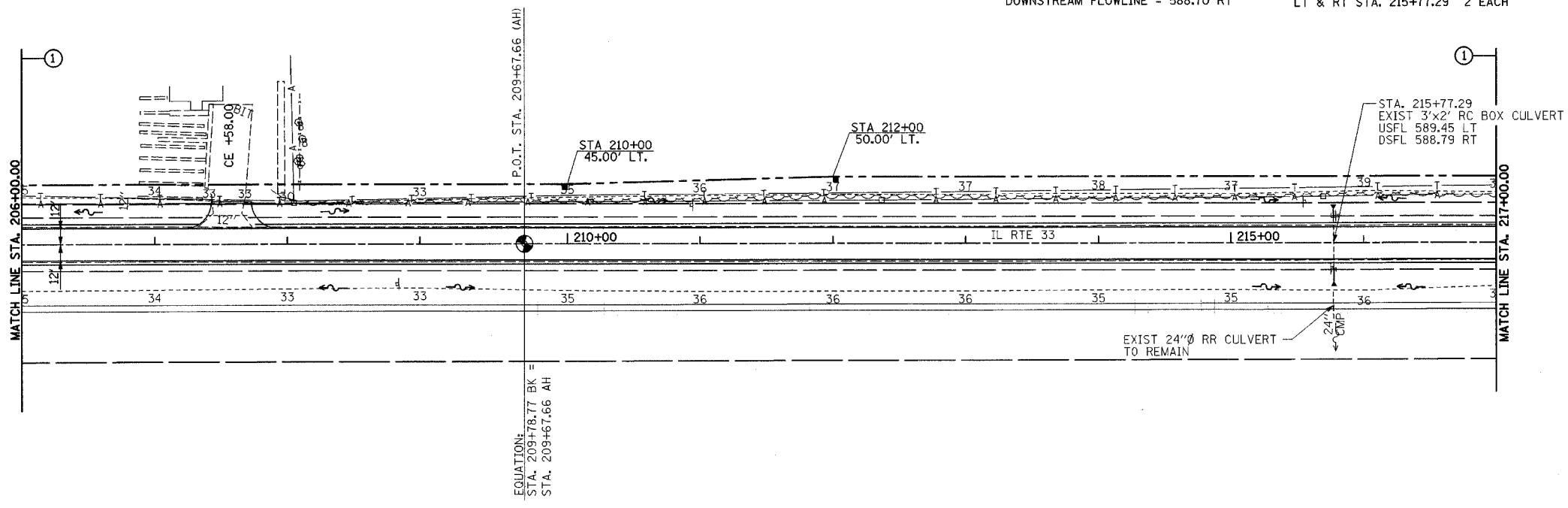
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PCBC 3'x2' EXTENSIONS
 STA. 215+77.29
 LENGTH = 6' LT
 LENGTH = 10' RT
 UPSTREAM FLOWLINE = 589.45 LT
 DOWNSTREAM FLOWLINE = 588.70 RT

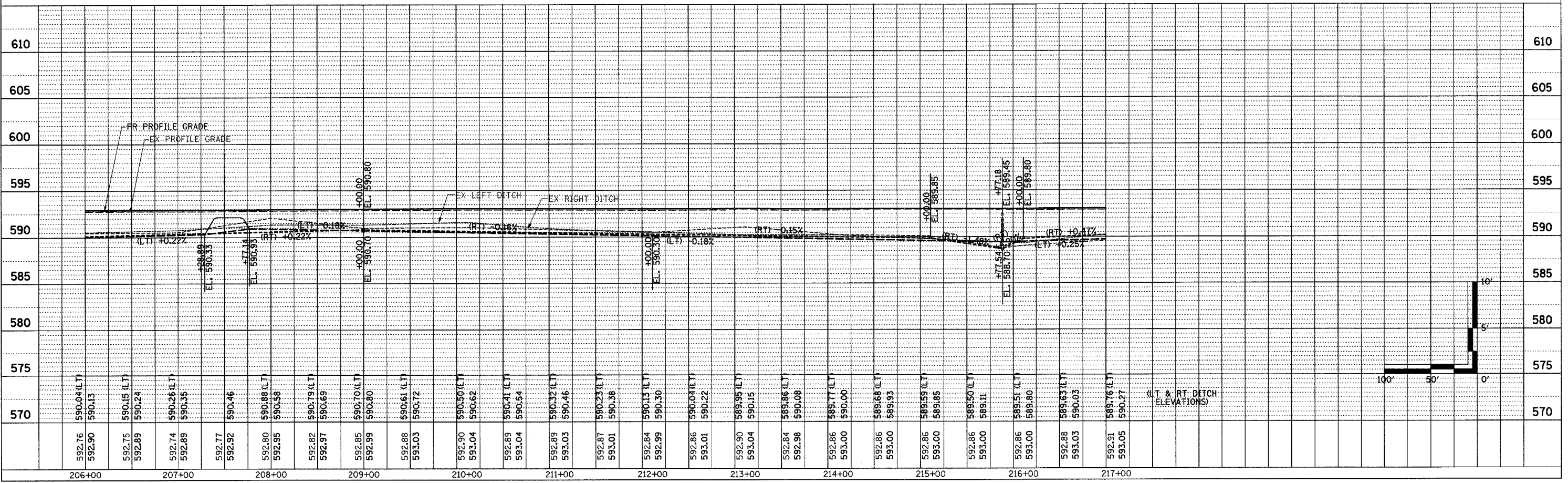
BOX CULVERT END SECTIONS CULV. NO. 7
 LT & RT STA. 215+77.29 2 EACH

REMOVE CONCRETE BOX CULVERT END SECTION
 LT & RT STA. 215+77.29 2 EACH

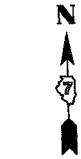


PERMANENT SURVEY MARKERS, TYPE I
 P.O.T. STA. 209+67.66 (AH) 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 45' LT STA. 210+00.00 1 EACH
 50' LT STA. 212+00.00 1 EACH
 TOTAL 2 EACH



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 49 |
| STA. 217+00.00 TO STA. 229+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

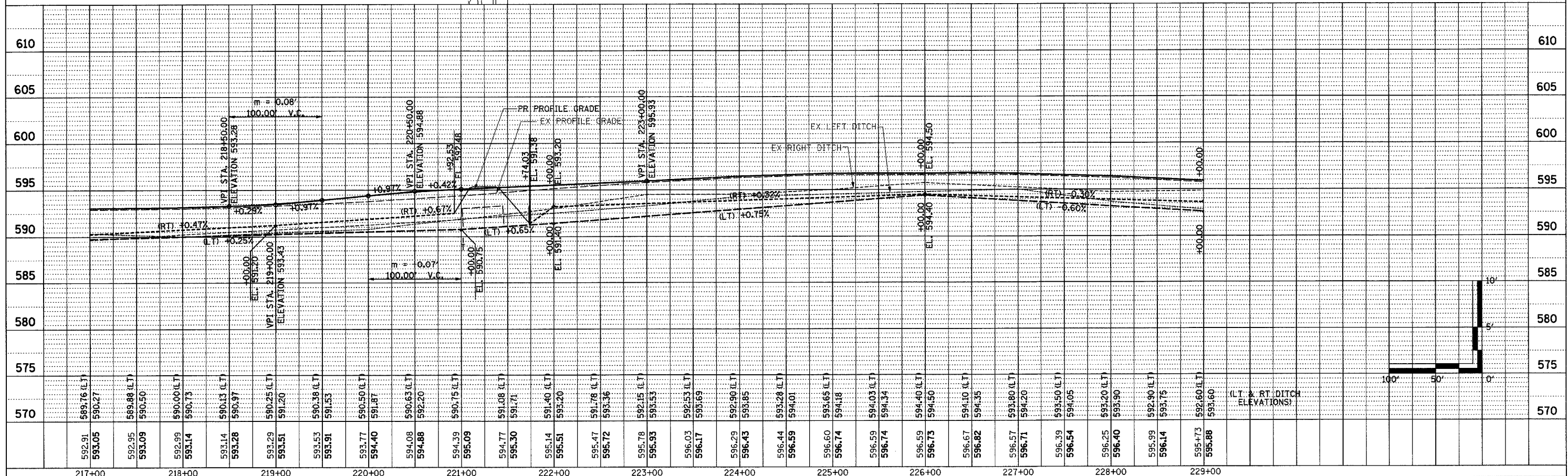
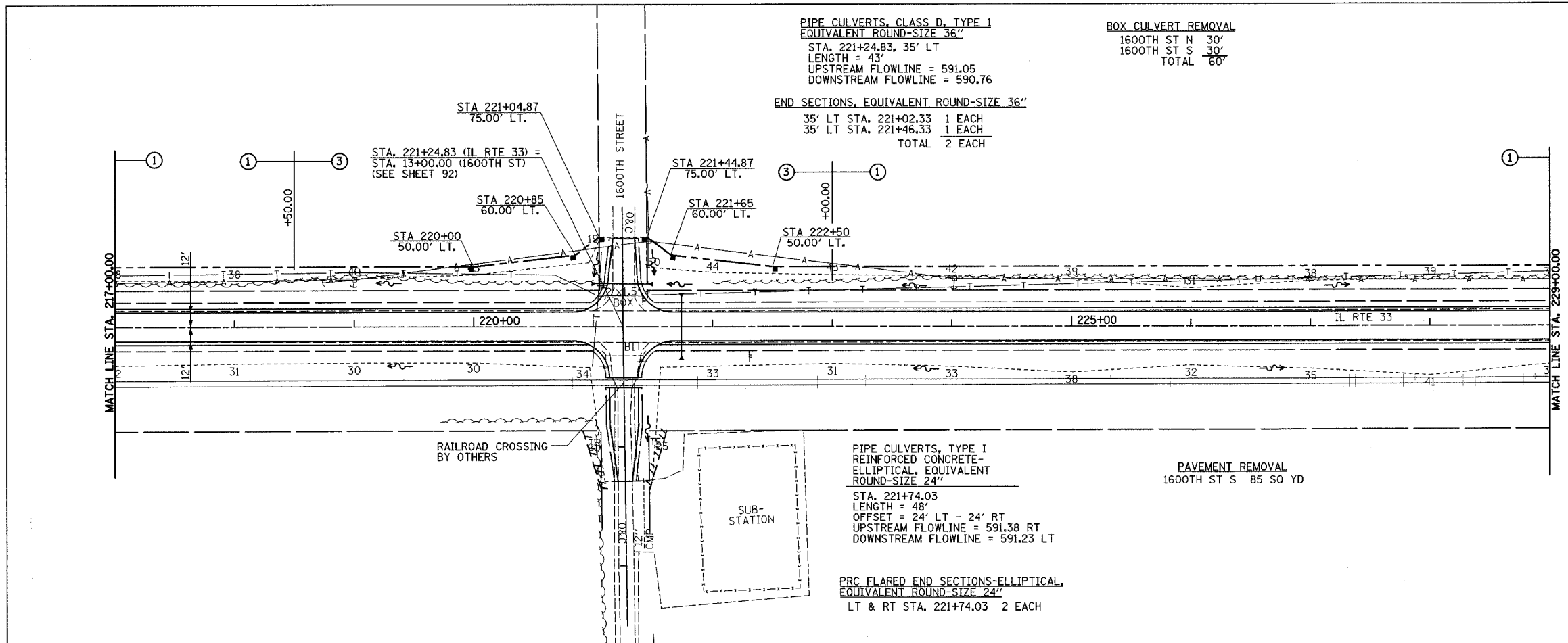


PERMANENT SURVEY MARKERS, TYPE I
P.O.T. STA. 222+00.00 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 50' LT STA. 220+00.00 1 EACH
 60' LT STA. 220+85.00 1 EACH
 75' LT STA. 221+04.87 1 EACH
 75' LT STA. 221+44.87 1 EACH
 60' LT STA. 221+65.00 1 EACH
 50' LT STA. 222+50.00 1 EACH
 TOTAL 6 EACH

| | |
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| PLAN | DATE |
| BY | |
| REVISIONS | |
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| DESCRIPTION | |

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| PROFILE | DATE |
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| REVISIONS | |
| NO. | |
| DESCRIPTION | |

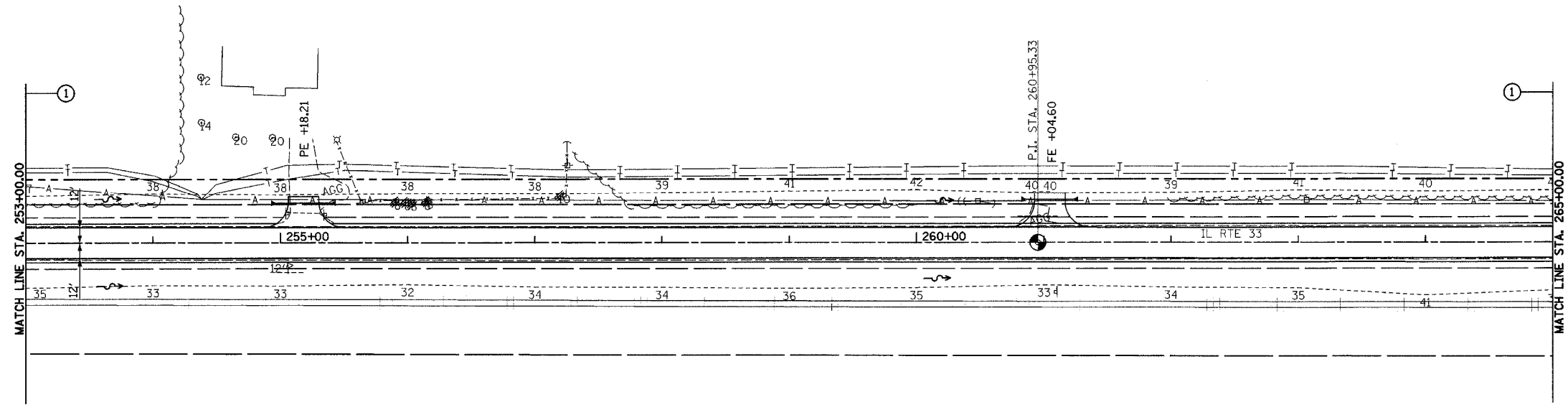


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|---|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 52 |
| STA. 253+00.00 TO STA. 265+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

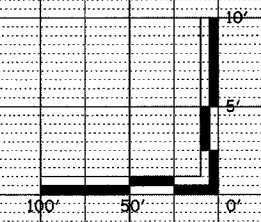
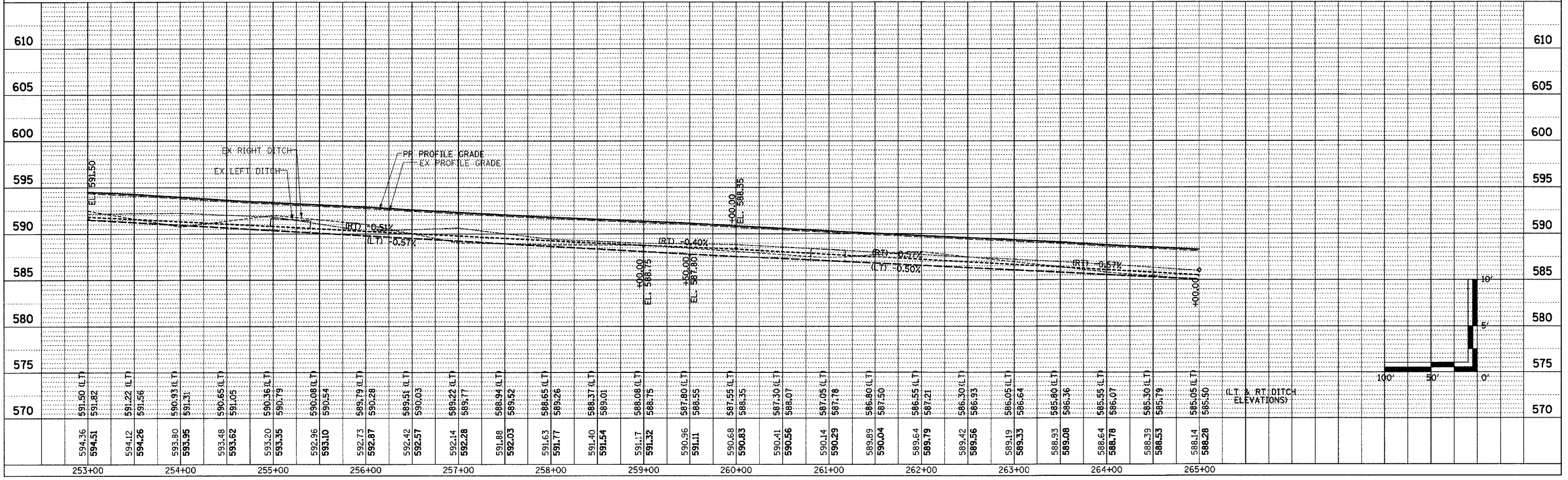


PERMANENT SURVEY MARKERS, TYPE I
P.I. STA. 260+95.33 1 EACH

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| PLAN | DATE |
| DATE | BY |
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| DATE | BY |



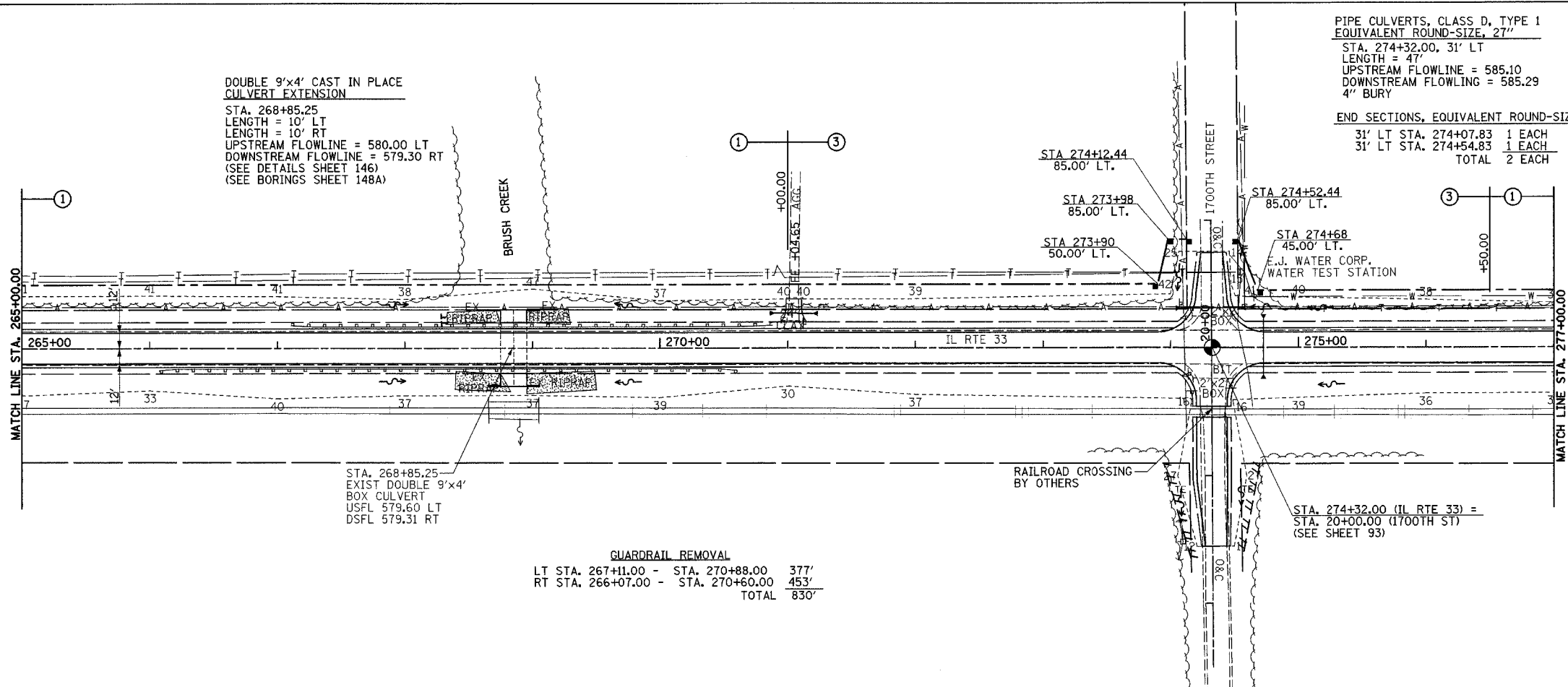
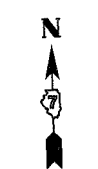
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| PROFILE | DATE |
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| PROFILE | |
| REVISIONS | |
| NO. | |
| DATE | |
| BY | |
| DESCRIPTION | |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 53 |
| STA. 265+00.00 TO STA. 277+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7B-2 | | | | |
| CONTRACT NO. 94356 | | | | |



GUARDRAIL REMOVAL
 LT STA. 267+11.00 - STA. 270+88.00 377'
 RT STA. 266+07.00 - STA. 270+60.00 453'
 TOTAL 830'

PIPE CULVERTS, TYPE 1 REINFORCED CONCRETE-ELLIPTICAL, EQUIVALENT ROUND SIZE 24"
 STA. 274+72.57
 LENGTH = 43'
 OFFSET = 22' LT - 21' RT
 UPSTREAM FLOWLINE = 585.85 RT
 DOWNSTREAM FLOWLINE = 585.69 LT

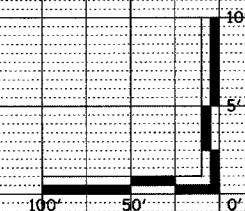
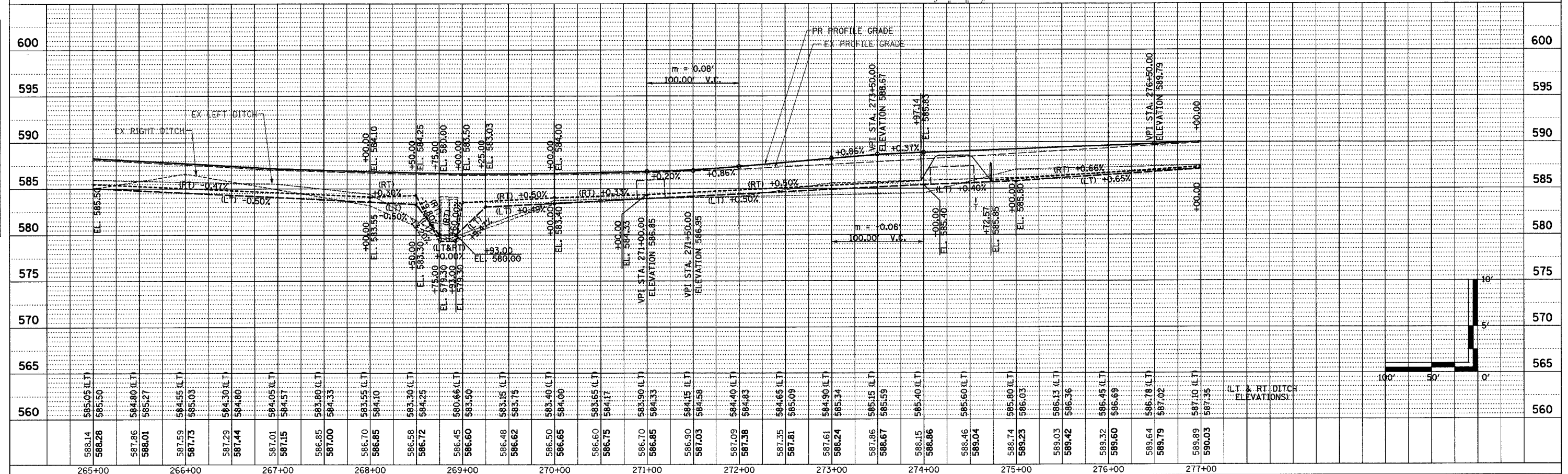
PRC FLARED END SECTIONS ELLIPTICAL, EQUIVALENT ROUND SIZE 24"
 LT & RT STA. 274+72.57 2 EACH

BOX CULVERT REMOVAL
 1700TH ST N 32'
 1700TH ST S 32'
 TOTAL 64'

PAVEMENT REMOVAL
 1700TH ST S 94 SQ YD

PERMANENT SURVEY MARKERS, TYPE I
 P.O.T. STA. 274+32.00 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 50' LT STA. 273+90.00 1 EACH
 85' LT STA. 273+98.00 1 EACH
 85' LT STA. 274+12.44 1 EACH
 85' LT STA. 274+52.44 1 EACH
 45' LT STA. 274+68.00 1 EACH
 TOTAL 5 EACH



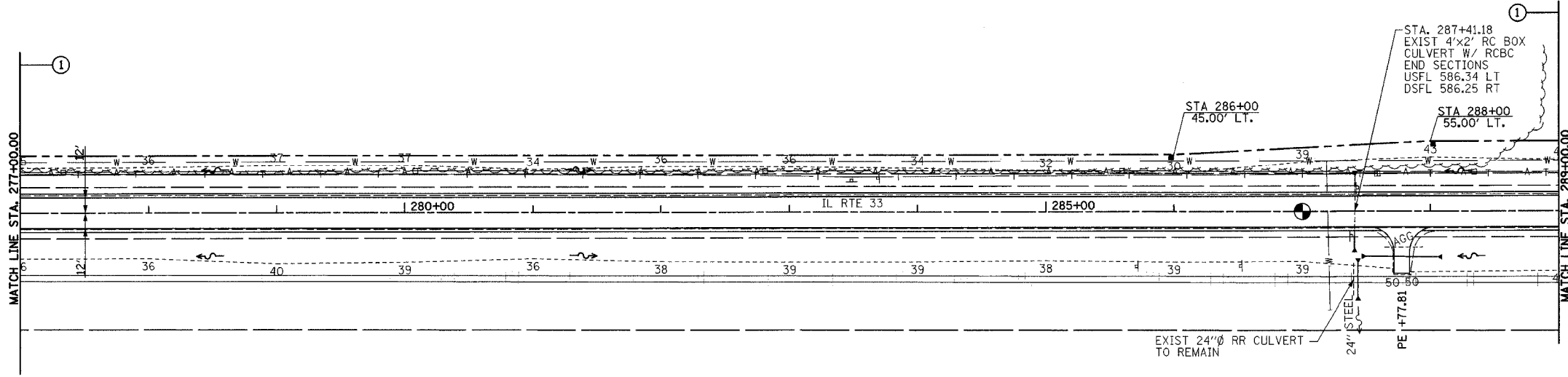
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 54 |
| STA. 277+00.00 TO STA. 289+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * 16.7Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



BOX CULVERT END SECTIONS CULV. NO. 8
 LT & RT STA. 287+41.18 2 EACH

REMOVE CONCRETE BOX CULVERT END SECTION
 LT & RT STA. 287+41.18 2 EACH

PCBC 4'x2' EXTENSIONS
 STA. 287+41.18
 LENGTH = 8' LT
 LENGTH = 9' RT
 UPSTREAM FLOWLINE = 585.50 LT
 DOWNSTREAM FLOWLINE = 585.38 RT



PERMANENT SURVEY MARKERS, TYPE I
 P.O.T. STA. 287+00.00 1 EACH

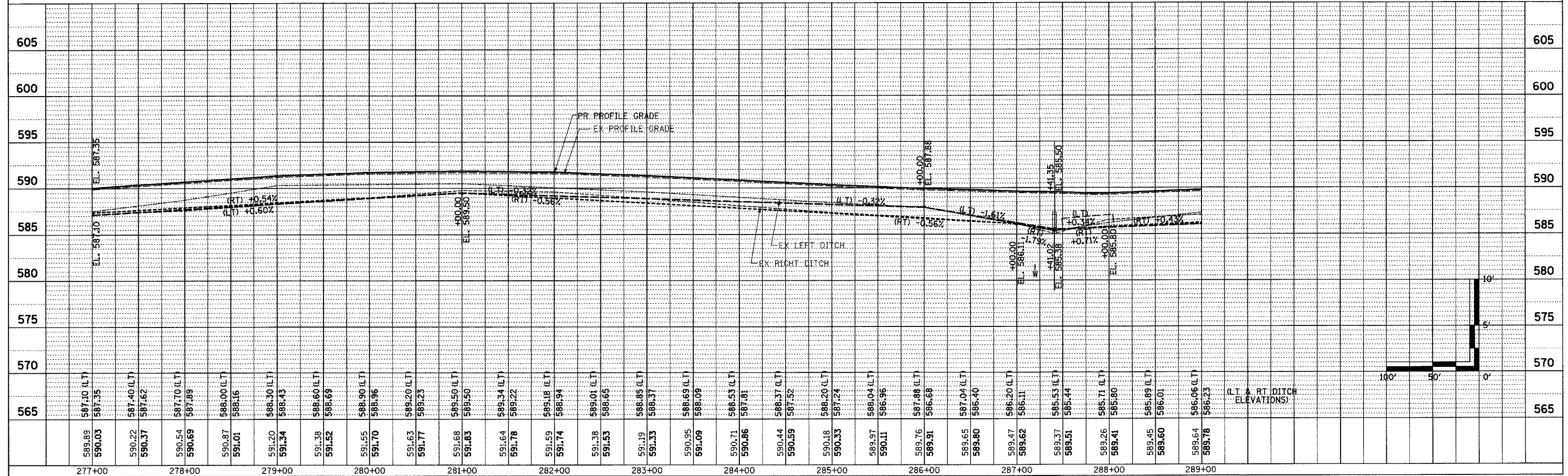
FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 45' LT STA. 286+00.00 1 EACH
 55' LT STA. 288+00.00 1 EACH
 TOTAL 2 EACH

PIPE CULVERTS, CLASS A, 24" (JACKED)
 STA. 287+43.65 RT
 LENGTH = 33'
 UPSTREAM FLOWLINE = 585.35
 DOWNSTREAM FLOWLINE = 584.66

PRC FLARED END SECTIONS 24"
 40' RT STA. 287+43.65 1 EACH
 66' RT STA. 287+43.65 1 EACH
 TOTAL 2 EACH

| | | |
|------|-----------|-----------|
| DATE | BY | DATE |
| | | |
| PLAN | SURVEYED | CHECKED |
| | NOTE BOOK | ALIGNED |
| | NO. | FILE NAME |

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| | NOTE BOOK | GRADES |
| | NO. | STATUS |



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| DATE | |
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| | CHECKED |
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| | NOTE BOOK |
| | STRUCTURE |
| | NOTATION |
| | NO. |

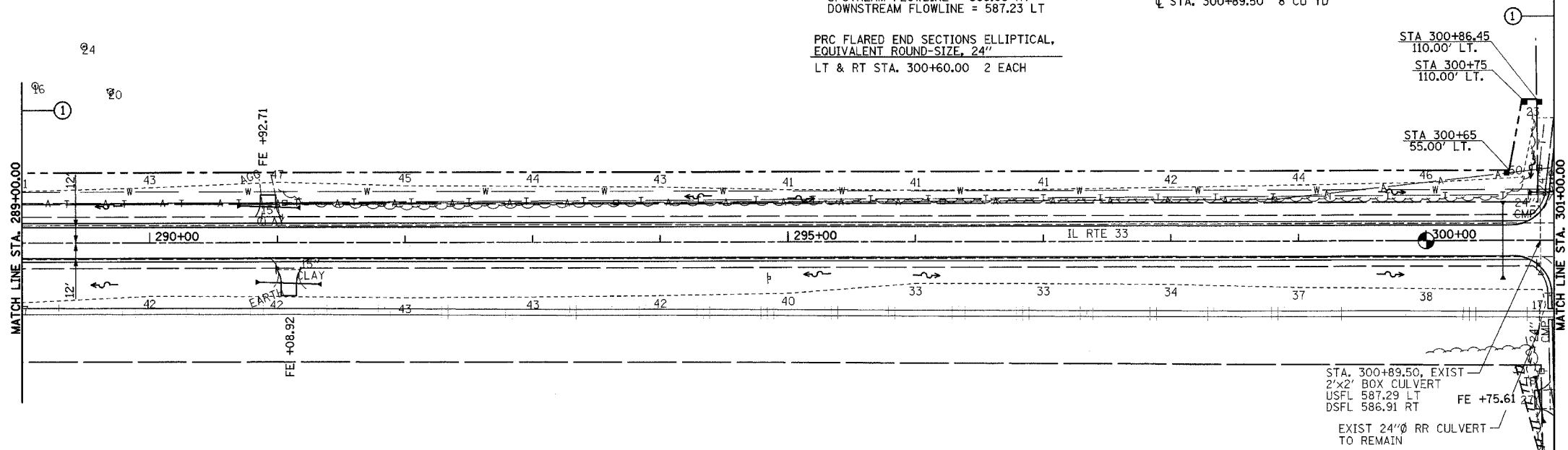
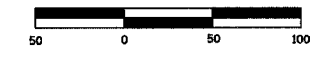
PIPE CULVERTS, TYPE 1,
REINFORCED CONCRETE
ELLIPTICAL, EQUIVALENT
ROUND-SIZE, 24"
STA. 300+60.00
LENGTH = 52'
OFFSET = 27' LT - 25' RT
UPSTREAM FLOWLINE = 588.05 RT
DOWNSTREAM FLOWLINE = 587.23 LT

FLARED END SECTION REMOVAL
RT STA. 300+89.50 1 EACH

FILLING EXISTING CULVERTS
LT STA. 300+89.50 8 CU YD

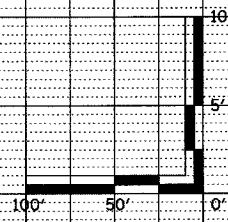
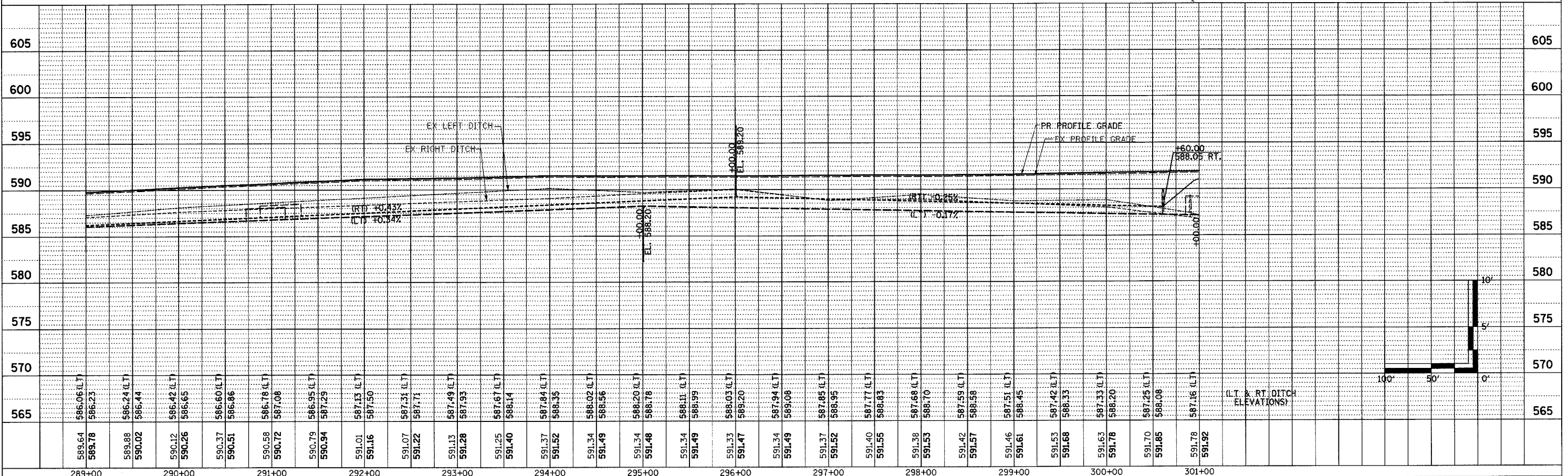
PRC FLARED END SECTIONS ELLIPTICAL,
EQUIVALENT ROUND-SIZE, 24"
LT & RT STA. 300+60.00 2 EACH

| | | | | |
|------------------------------------|---------|-------------------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 55 |
| STA. 289+00.00 | | TO STA. 301+00.00 | | |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



PERMANENT SURVEY MARKERS, TYPE J
P.O.T. STA. 300+00.00 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
55' LT STA. 300+65.00 1 EACH
110' LT STA. 300+75.00 1 EACH
110' LT STA. 300+86.45 1 EACH
TOTAL 3 EACH



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | • | EFFINGHAM | 409 | 58 |
| STA. 324+00.00 TO STA. 336+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6.7)Y, RS-1, 6B-1, 7B-1 & 7B-2 | | | | |
| CONTRACT NO. 94356 | | | | |



PERMANENT SURVEY MARKERS, TYPE I

| | |
|---------------------|--------|
| P.I. STA. 326+34.67 | 1 EACH |
| P.T. STA. 329+64.80 | 1 EACH |
| TOTAL | 2 EACH |

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

| | |
|------------------------|--------|
| 50' LT STA. 327+05.00 | 1 EACH |
| 105' LT STA. 327+30.19 | 1 EACH |
| 105' LT STA. 327+70.37 | 1 EACH |
| 60' LT STA. 327+90.00 | 1 EACH |
| 60' LT STA. 329+64.80 | 1 EACH |
| 50' LT STA. 331+00.00 | 1 EACH |
| TOTAL | 6 EACH |

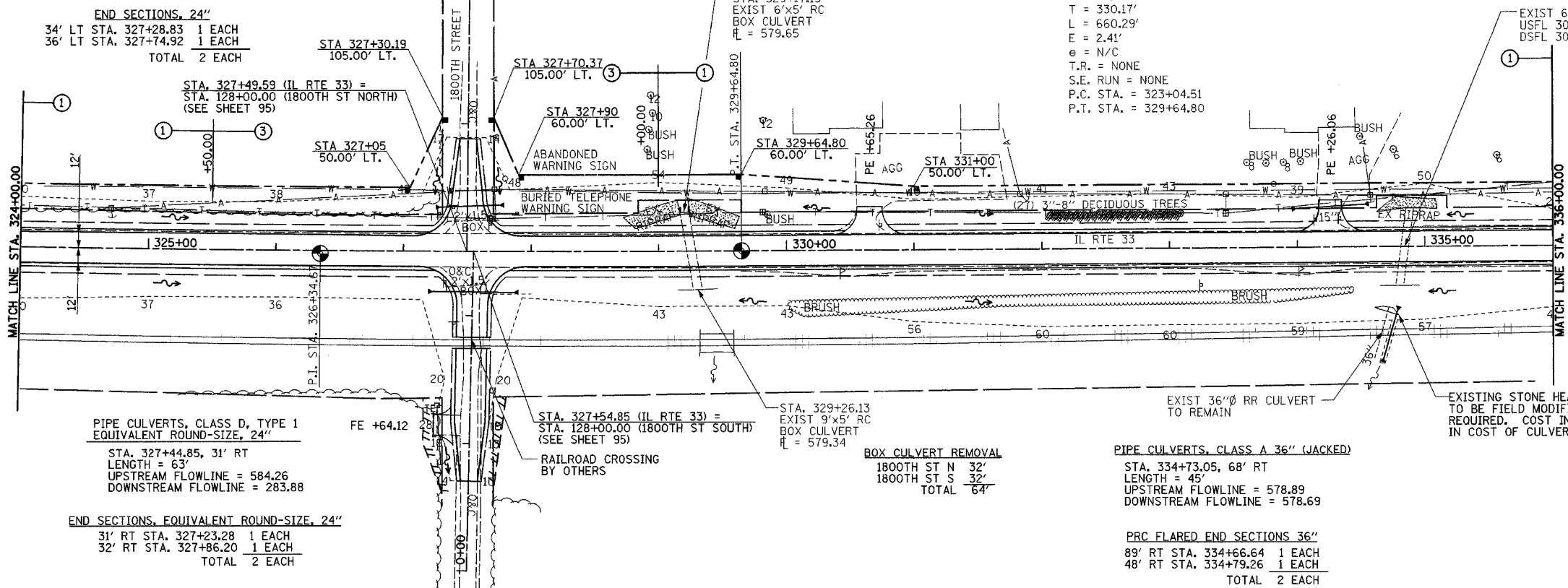
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| REVISIONS | |
| NO. | |
| DESCRIPTION | |

PIPE CULVERTS, CLASS D, TYPE I, 24"
 STA. 327+49.59, 35' LT
 LENGTH = 46'
 UPSTREAM FLOWLINE = 583.51
 DOWNSTREAM FLOWLINE = 582.94

END SECTIONS, 24"
 34' LT STA. 327+28.83 1 EACH
 36' LT STA. 327+74.92 1 EACH
 TOTAL 2 EACH

EXIST CURVE C3
 PI STA. = 326+34.67
 $\Delta = 1^\circ 40' 13''$ (LT)
 $D = 0^\circ 15' 11''$
 $R = 22,650.00'$
 $T = 330.17'$
 $L = 660.29'$
 $E = 2.41'$
 $e = N/C$
 $T.R. = NONE$
 $S.E. RUN = NONE$
 $P.C. STA. = 323+04.51$
 $P.T. STA. = 329+64.80$



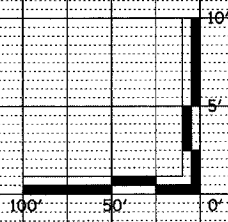
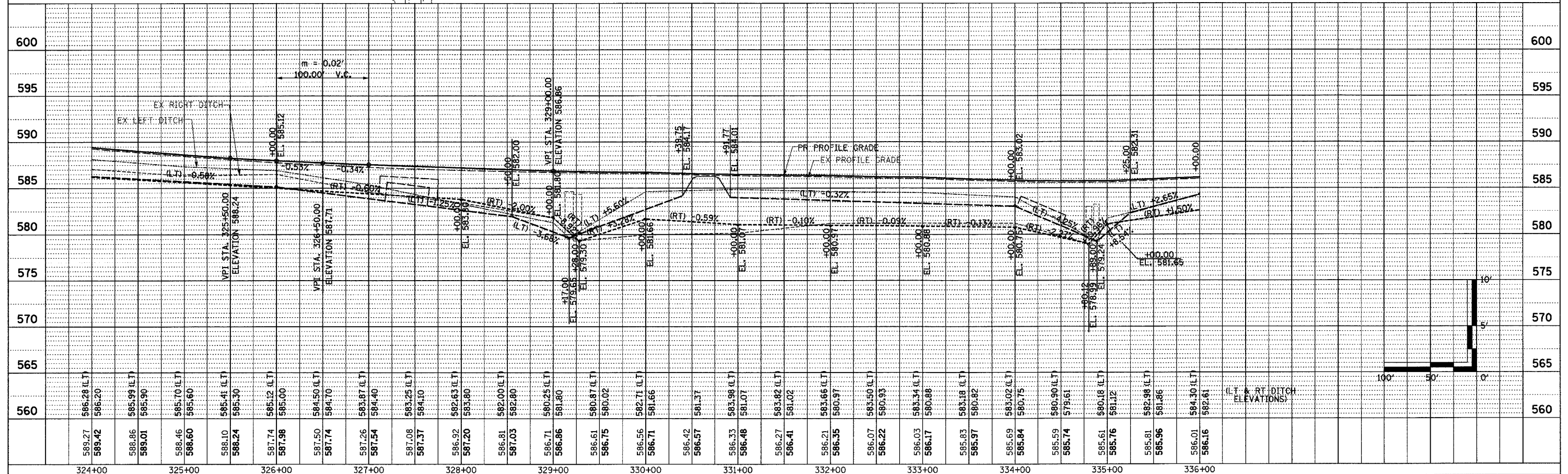
PIPE CULVERTS, CLASS D, TYPE I
 EQUIVALENT ROUND-SIZE, 24"
 STA. 327+44.85, 31' RT
 LENGTH = 63'
 UPSTREAM FLOWLINE = 584.26
 DOWNSTREAM FLOWLINE = 283.88

END SECTIONS, EQUIVALENT ROUND-SIZE, 24"
 31' RT STA. 327+23.28 1 EACH
 32' RT STA. 327+86.20 1 EACH
 TOTAL 2 EACH

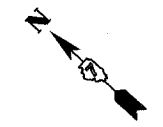
BOX CULVERT REMOVAL
 1800TH ST N 32'
 1800TH ST S 32'
 TOTAL 64'

PIPE CULVERTS, CLASS A 36" (JACKED)
 STA. 334+73.05, 68' RT
 LENGTH = 45'
 UPSTREAM FLOWLINE = 578.89
 DOWNSTREAM FLOWLINE = 578.69

PRC FLARED END SECTIONS 36"
 89' RT STA. 334+66.64 1 EACH
 48' RT STA. 334+79.26 1 EACH
 TOTAL 2 EACH

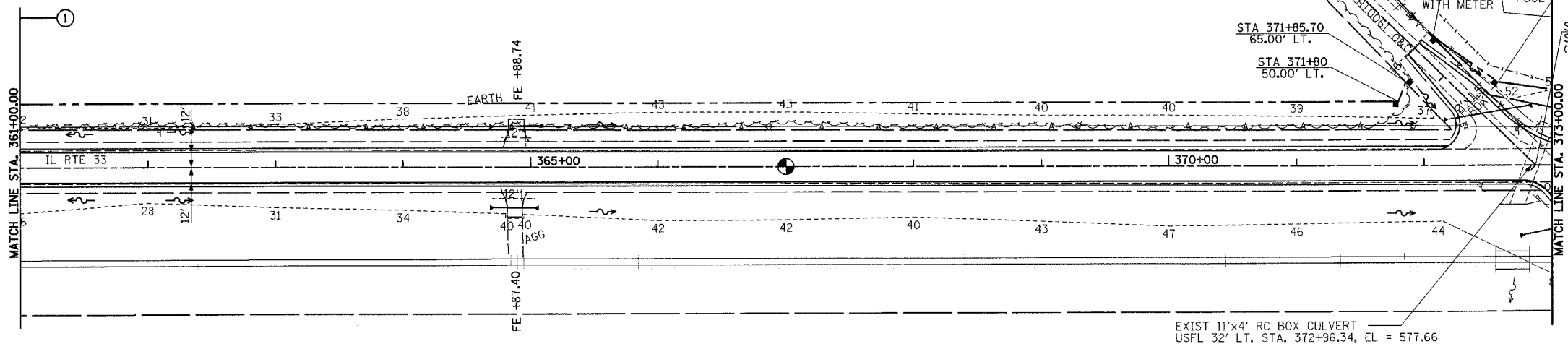


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 61 |
| STA. 361+00.00 TO STA. 373+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7B-2 | | | | |
| CONTRACT NO. 94356 | | | | |



FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

| | |
|---------------------------|---------------|
| 50' LT STA. 371+80.00 | 1 EACH |
| 65' LT STA. 371+85.70 | 1 EACH |
| 100.45' LT STA. 372+07.10 | 1 EACH |
| 65' LT STA. 372+58.00 | 1 EACH |
| TOTAL | 4 EACH |



PERMANENT SURVEY MARKERS, TYPE I
P.O.T. STA. 367+00.00 1 EACH

PIPE CULVERTS, CLASS D, TYPE I
EQUIVALENT ROUND-SIZE, 24"
STA. 372+50.06, 41' LT
LENGTH = 62'
UPSTREAM FLOWLINE = 581.60
DOWNSTREAM FLOWLINE = 578.52

END SECTIONS, EQUIVALENT ROUND-SIZE, 24"
36' LT STA. 372+19.50 1 EACH
47' LT STA. 372+80.62 1 EACH
TOTAL 2 EACH

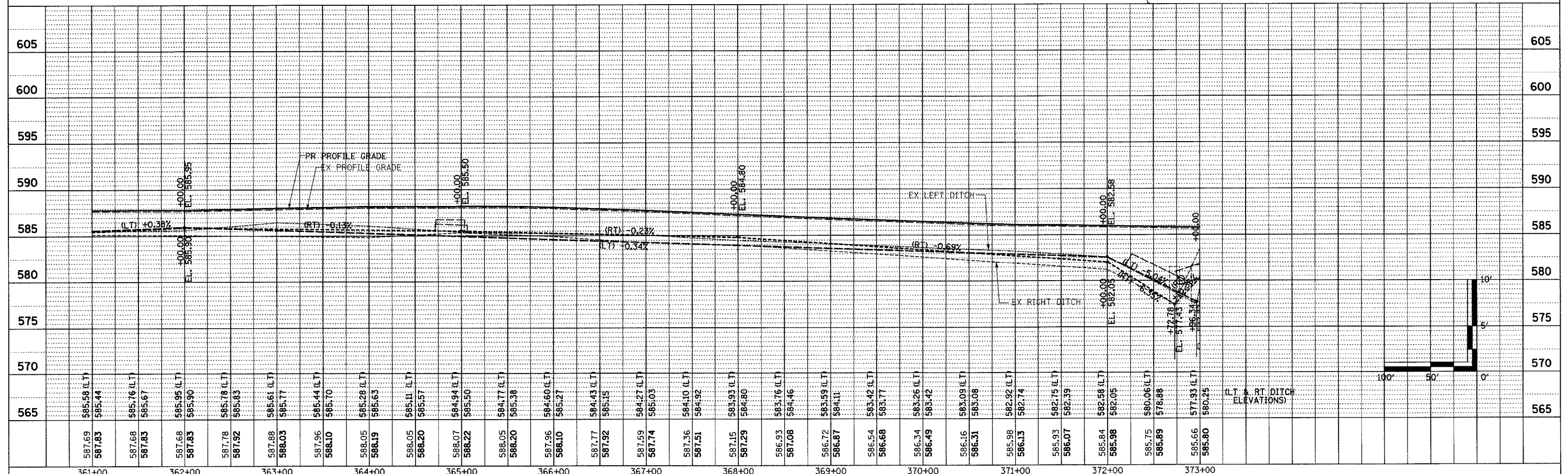
EXIST 11'x4' RC BOX CULVERT
USFL 32' LT, STA. 372+96.34, EL = 577.66
DSFL 31' RT, STA. 372+72.78, EL = 577.49

BOX CULVERTS TO BE CLEANED
31' LT STA. 372+96.34 TO
32' RT STA. 372+72.78 1 EACH

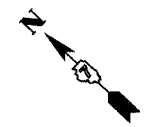
BOX CULVERT REMOVAL
1900TH ST N 25'

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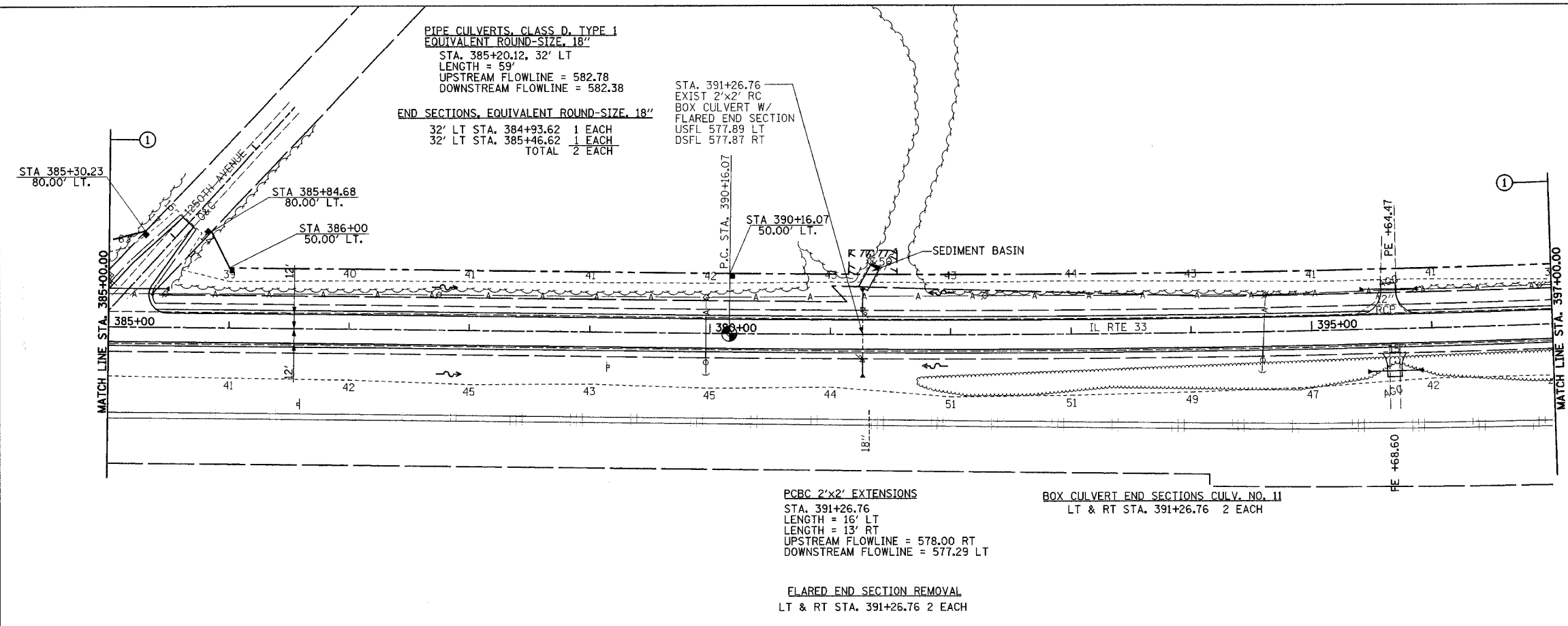


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 63 |
| STA. 385+00.00 TO STA. 397+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



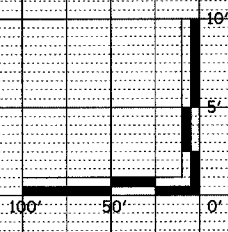
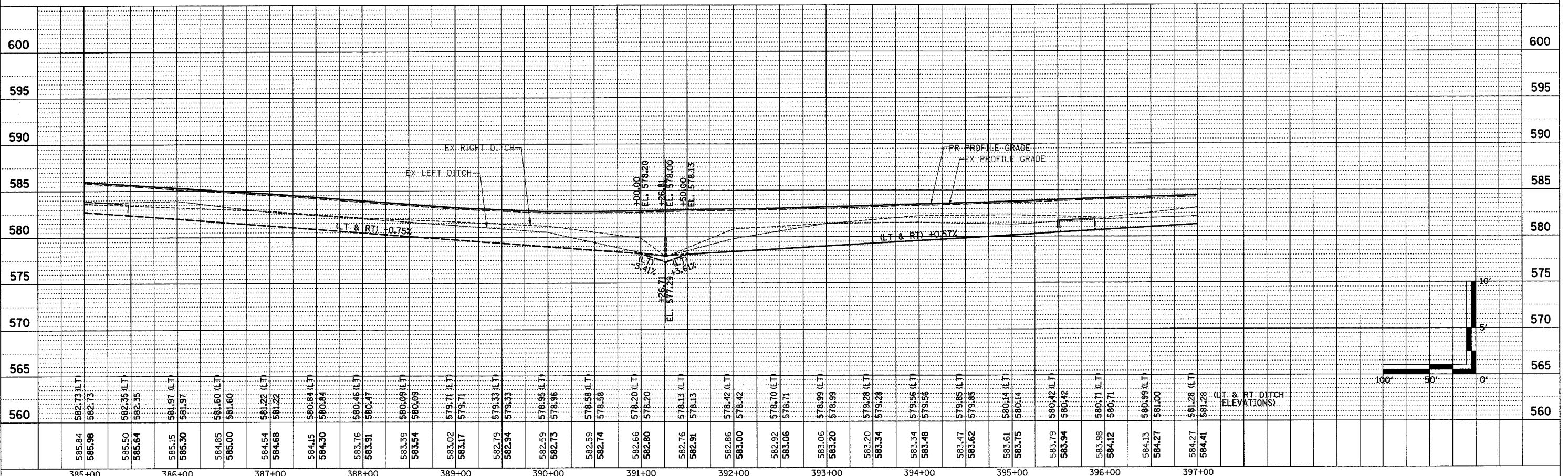
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PERMANENT SURVEY MARKERS, TYPE I
P.C. STA. 390+16.07 1 EACH

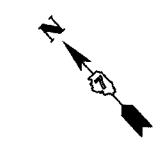
FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
80' LT STA. 385+30.23 1 EACH
80' LT STA. 385+84.68 1 EACH
50' LT STA. 386+00.00 1 EACH
50' LT STA. 390+16.07 1 EACH
TOTAL 4 EACH



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| DATE | |
| BY | |
| PLAN | |
| SURVEYED | |
| ALIGNED | |
| CHECKED | |
| BY | |
| NOTE BOOK | |
| NO. | |
| ROAD FILE NAME | |

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|---------------------|--|
| DATE | |
| BY | |
| PROFILE | |
| GRADES | |
| CHECKED | |
| BY | |
| NOTE BOOK | |
| NO. | |
| STRUCTURE NOTATIONS | |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 64 |
| STA. 397+00.00 TO STA. 409+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



EXIST CURVE C5
 PI STA. = 399+29.12
 $\Delta = 6^\circ 50' 23''$ (LT)
 $D = 0^\circ 22' 30''$
 $R = 15,278.83'$
 $T = 913.05'$
 $L = 1,823.92'$
 $E = 27.26'$
 $e = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 390+16.07
 P.T. STA. = 408+39.99

PIPE CULVERTS, CLASS D, TYPE I,
 EQUIVALENT ROUND-SIZE 24"
 STA. 408+77.11, 38' LT
 LENGTH = 81'
 UPSTREAM FLOWLINE = 577.84
 DOWNSTREAM FLOWLINE = 576.61

END SECTIONS, EQUIVALENT ROUND-SIZE 24"
 37' LT STA. 408+36.61 1 EACH
 38' LT STA. 409+17.61 1 EACH
 TOTAL 2 EACH

PAVEMENT REMOVAL
 1950TH ST N 134 SQ YD

EXIST 2'x1.5' RC BOX CULVERT W/ END SECTION
 USFL 44' LT, STA. 408+43.40, EL = 576.80
 DSFL 37' RT, STA. 409+05.68, EL = 575.50

FILLING EXISTING CULVERTS
 11 CU YD

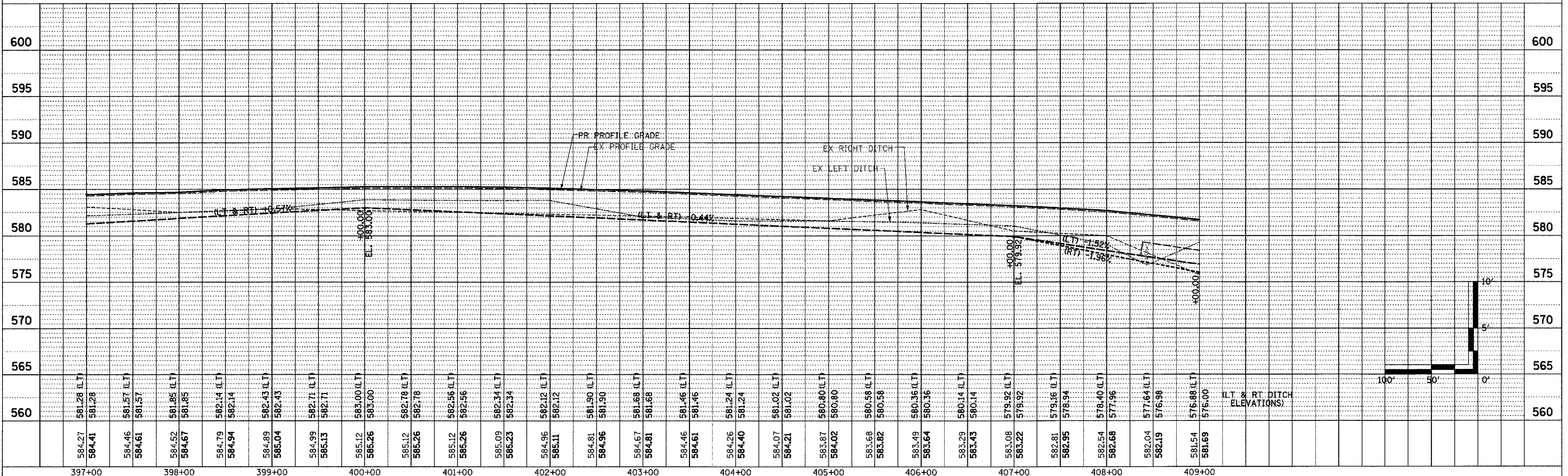
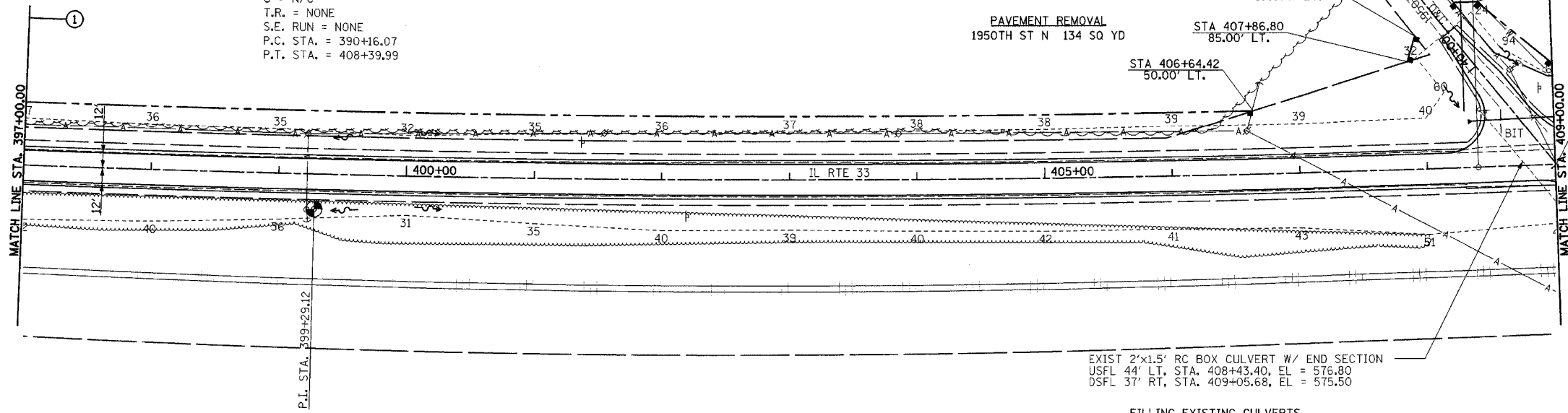
REMOVE CONCRETE BOX CULVERT END SECTION
 LT STA. 408+43.40 1 EACH
 RT STA. 409+05.68 1 EACH
 TOTAL 2 EACH

STA. 409+01.06 (IL RTE 33) =
 STA. 39+00.00 (1950TH ST NORTH)
 (SEE SHEET 97)

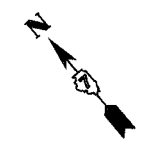
PERMANENT SURVEY MARKERS, TYPE I
 P.T. STA. 408+39.99 1 EACH

PERMANENT SURVEY MARKERS, TYPE II
 P.I. STA. 399+29.12 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 50' LT STA. 406+64.42 1 EACH
 85.00' LT STA. 407+86.80 1 EACH
 105.00' LT STA. 407+93.33 1 EACH
 130' LT STA. 408+24.83 1 EACH
 130' LT STA. 408+43.00 1 EACH
 TOTAL 5 EACH

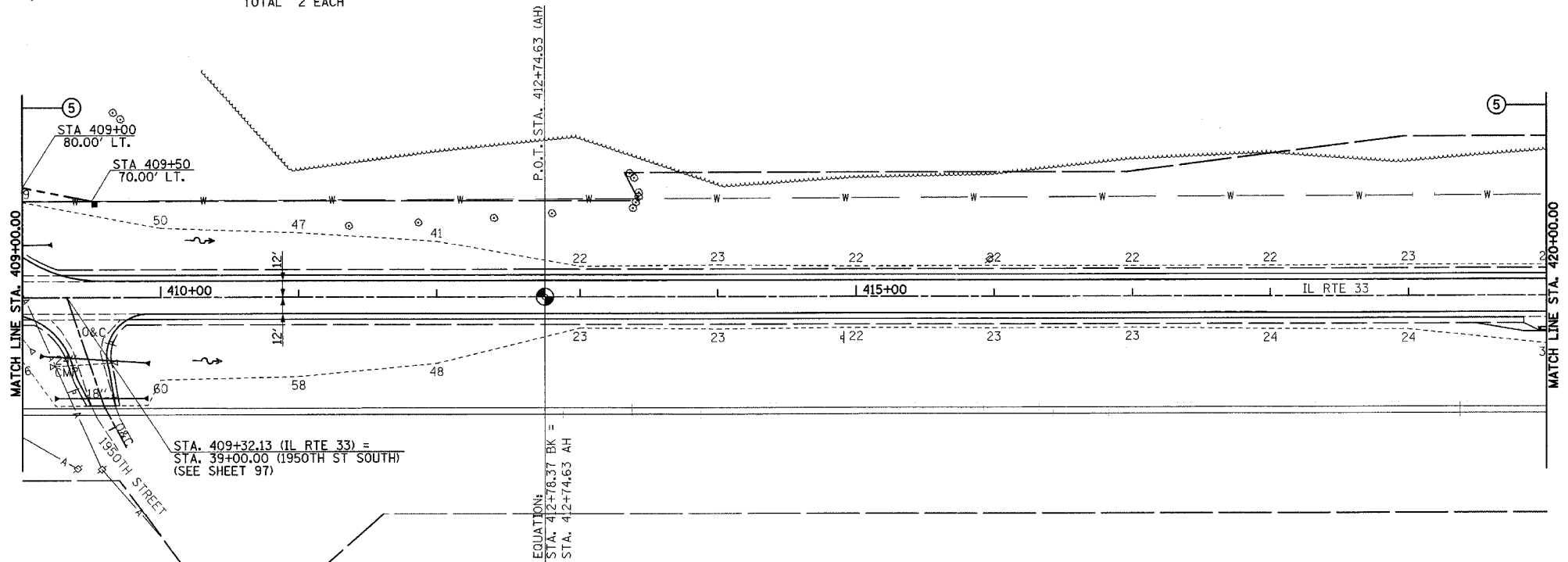


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 65 |
| STA. 409+00.00 TO STA. 420+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7B-2 | | | | |
| CONTRACT NO. 94356 | | | | |



FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

80' LT STA. 409+00.00 1 EACH
 70' LT STA. 409+50.00 1 EACH
 TOTAL 2 EACH



PERMANENT SURVEY MARKERS, TYPE I
 P.O.T. STA. 412+74.63 (AH) 1 EACH

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| DATE | |
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| REVISION | |
| NO. | |

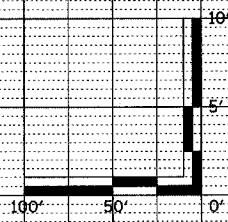
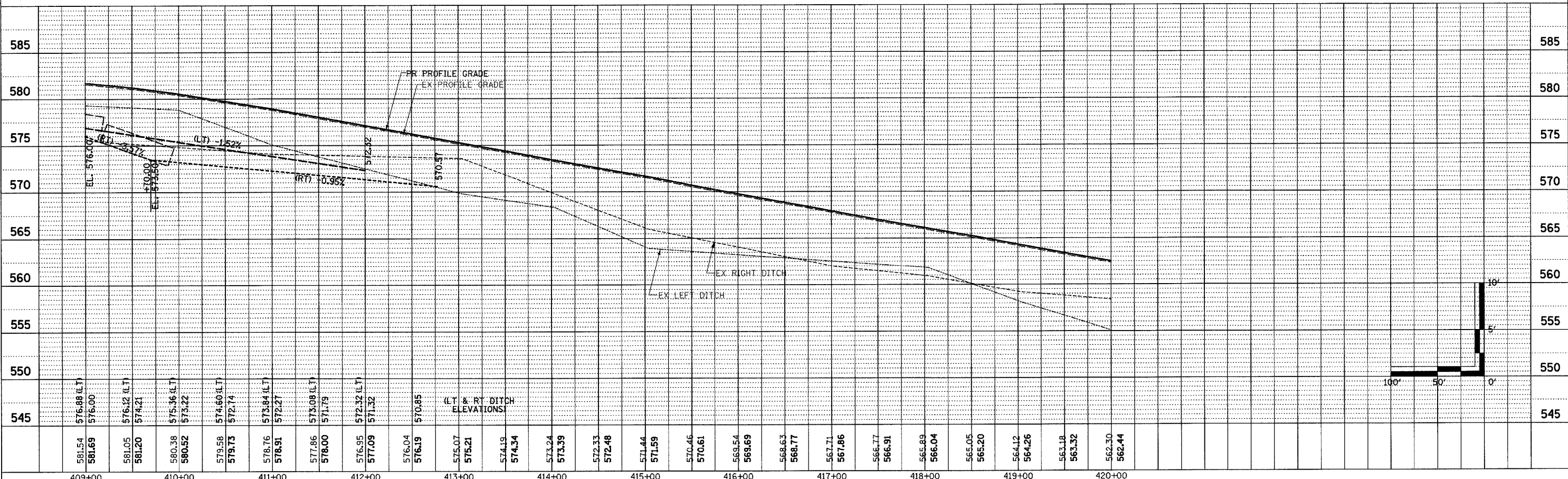
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| REVISION | |
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PIPE CULVERTS, CLASS D, TYPE 1, 24"
 STA. 409+52.36, 45' RT
 LENGTH = 71'
 UPSTREAM FLOWLINE = 575.41
 DOWNSTREAM FLOWLINE = 573.33

END SECTIONS, 24"
 43' RT STA. 409+16.46 1 EACH
 47' RT STA. 409+88.27 1 EACH
 TOTAL 2 EACH

PIPE CULVERTS, CLASS D, TYPE 1, 18"
 STA. 409+55.14, 73' RT
 LENGTH = 60'
 UPSTREAM FLOWLINE = 574.50
 DOWNSTREAM FLOWLINE = 573.80

END SECTIONS, 18"
 73' RT STA. 409+27.04 1 EACH
 73' RT STA. 409+87.04 1 EACH
 TOTAL 2 EACH



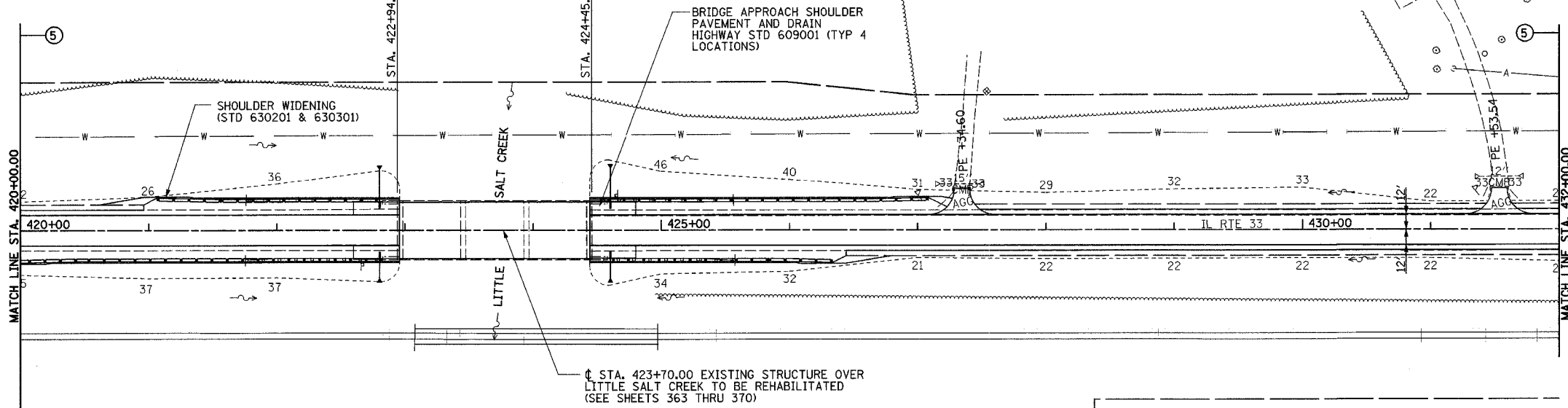
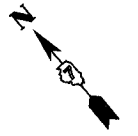
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 PLAN: _____ NO. _____

DATE: _____ BY: _____
 SURVEYED: _____ CHECKED: _____
 PROFILE: _____ NO. _____

GUARDRAIL MARKERS, TYPE A
 RT STA. 419+94.08 - STA. 422+82.33 3 EACH
 LT STA. 421+06.58 - STA. 422+82.33 2 EACH
 RT STA. 424+57.67 - STA. 426+33.42 2 EACH
 LT STA. 424+57.67 - STA. 427+08.42 3 EACH
 TOTAL 10 EACH

BARRIER WALL MARKERS, TYPE B
 RT STA. 422+82.33 - STA. 424+57.67 2 EACH
 LT STA. 422+82.33 - STA. 424+57.67 2 EACH
 TOTAL 4 EACH

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|------------------------------------|---------|---------------------------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 66 |
| STA. 420+00.00 | | TO STA. 432+00.00 | | |
| FED. ROAD DIST. NO. 7 | | ILLINOIS FED. AID PROJECT | | |
| * (6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



PERMANENT SURVEY MARKERS, TYPE I
 P.O.T. STA. 421+00.00 1 EACH

TRAFFIC BARRIER TERMINAL, TYPE 5
 LT STA. 422+69.08 - STA. 422+82.33 1 EACH
 RT STA. 422+69.08 - STA. 422+82.33 1 EACH
 LT STA. 424+57.67 - STA. 424+70.92 1 EACH
 RT STA. 424+57.67 - STA. 424+70.92 1 EACH
 TOTAL 4 EACH

STEEL PLATE BEAM GUARDRAIL, TYPE A
 RT STA. 419+56.58 - STA. 422+69.08 312.5'
 LT STA. 419+94.08 - STA. 422+69.08 175.0'
 RT STA. 424+70.92 - STA. 426+33.92 175.0'
 LT STA. 424+70.92 - STA. 427+58.42 187.5'
 TOTAL 850.0'

TERMINAL MARKER DIRECT APPLIED
 RT STA. 419+06.58 1 EACH
 LT STA. 419+94.08 1 EACH
 RT STA. 426+83.92 1 EACH
 LT STA. 427+08.42 1 EACH
 TOTAL 4 EACH

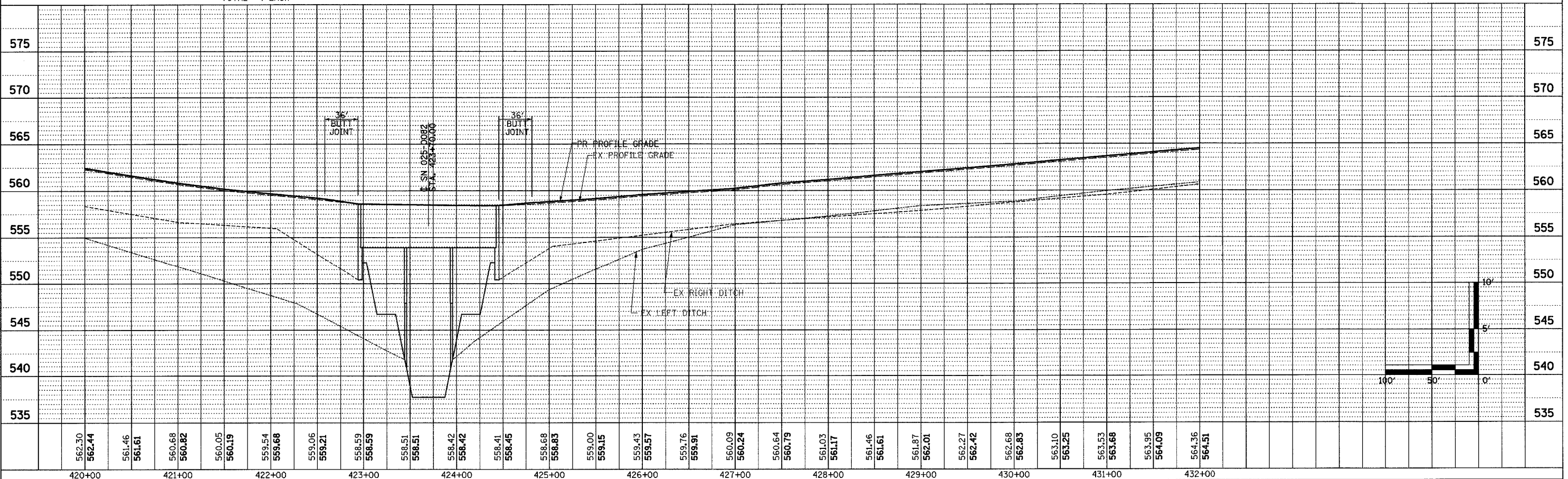
TEMPORARY RAMP
 STA. 422+89.33 - STA. 422+94.33 13 SQ YD
 STA. 424+45.67 - STA. 424+50.67 13 SQ YD
 TOTAL 26 SQ YD

| LOCATION | PCC BRIDGE APPROACH SHOULDER PAVEMENT | TYPE D INLET BOX STD 609001 | PIPE DRAINS 12" | END SECTIONS 12" | CONC. THRUST BLOCKS |
|--------------|---------------------------------------|-----------------------------|-----------------|------------------|---------------------|
| LT 422+80.00 | 40 (10'x36') | 1 | 42' | 1 | 1 |
| RT 422+80.00 | 40 (10'x36') | 1 | 20' | 1 | 1 |
| LT 424+60.00 | 40 (10'x36') | 1 | 31' | 1 | 1 |
| RT 424+60.00 | 40 (10'x36') | 1 | 20' | 1 | 1 |
| TOTAL | 160 SQ YD | 4 EACH | 113 L.F. | 4 EACH | 4 EACH |

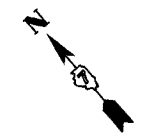
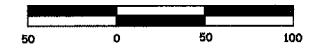
TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT)
 RT STA. 419+06.58 - STA. 419+56.58 1 EACH
 LT STA. 419+94.08 - STA. 420+44.08 1 EACH
 RT STA. 426+33.92 - STA. 426+83.92 1 EACH
 LT STA. 426+58.42 - STA. 427+08.42 1 EACH
 TOTAL 4 EACH

GUARDRAIL REMOVAL
 LT STA. 421+77.50 - STA. 422+78.64 101'
 RT STA. 421+77.44 - STA. 422+78.43 101'
 LT STA. 424+53.60 - STA. 425+54.85 101'
 RT STA. 424+53.75 - STA. 425+56.33 103'
 TOTAL 406'

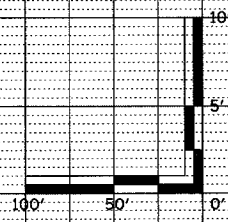
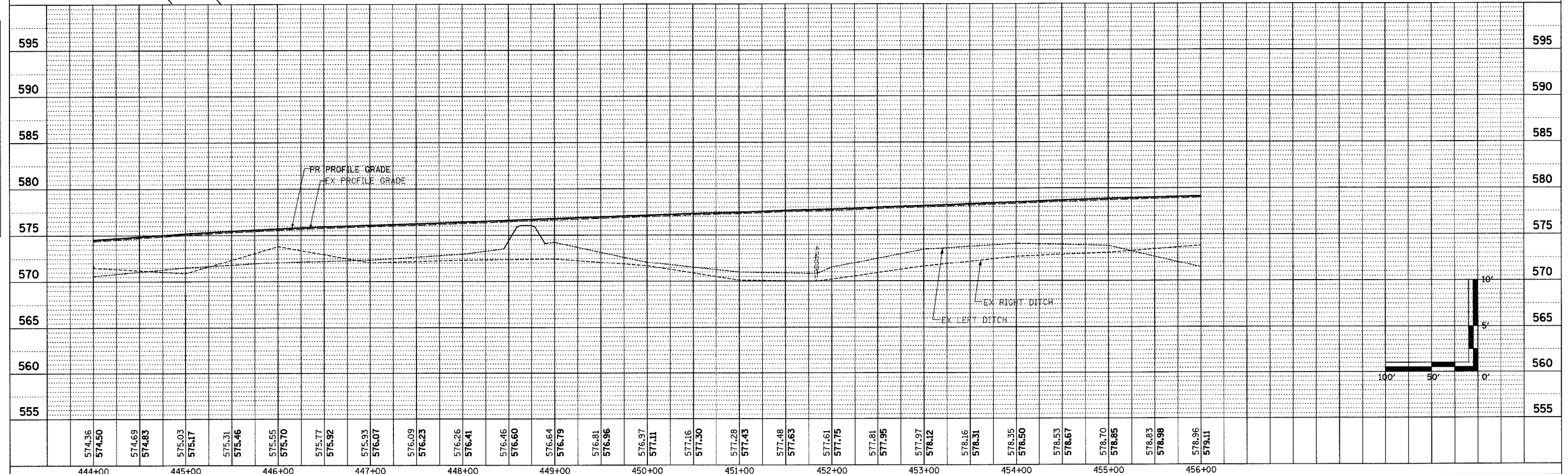
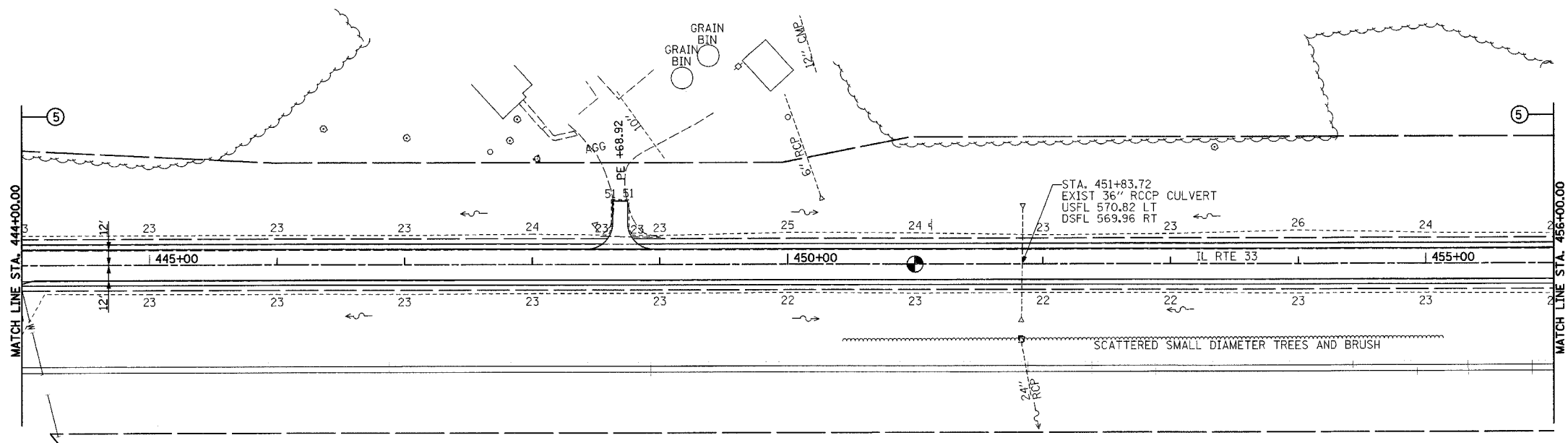
BITUMINOUS SURFACE REMOVAL-BUTT JOINT
 STA. 422+74.33 - STA. 422+94.33 53 SQ YD
 STA. 424+45.67 - STA. 424+65.67 53 SQ YD
 TOTAL 106 SQ YD



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 68 |
| STA. 444+00.00 TO STA. 456+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



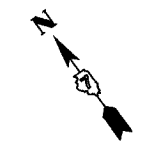
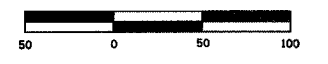
PERMANENT SURVEY MARKERS, TYPE I
P.O.T. STA. 451+00.00 1 EACH



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| PLAN | SURVEYED | DATE |
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| PROFILE | SURVEYED | DATE |
| | NOTE BOOK | |
| | ALIGNED | |
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| | BY | |
| | NO. | |

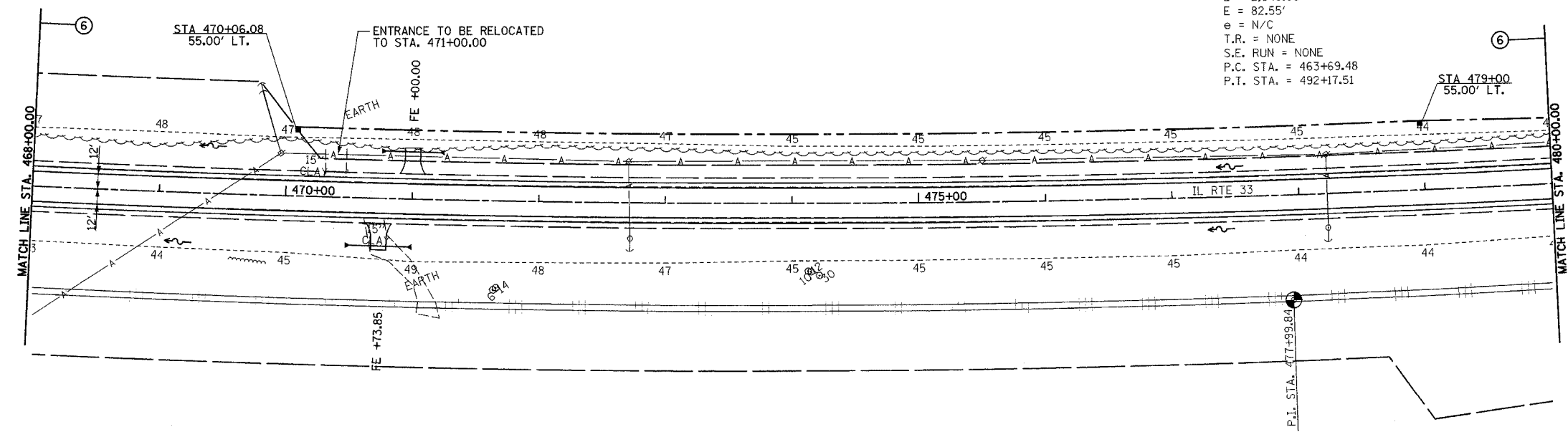
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 70 |
| STA. 468+00.00 | | TO STA. 480+00.00 | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



EXIST CURVE C6
 PI STA. = 477+99.84
 $\Delta = 13^\circ 12' 42''$ (LT)
 $D = 0^\circ 27' 50''$
 $R = 12,351.18'$
 $T = 1,430.36'$
 $L = 2,848.03'$
 $E = 82.55'$
 $e = N/C$
 $T.R. = NONE$
 $S.E. RUN = NONE$
 $P.C. STA. = 463+69.48$
 $P.T. STA. = 492+17.51$

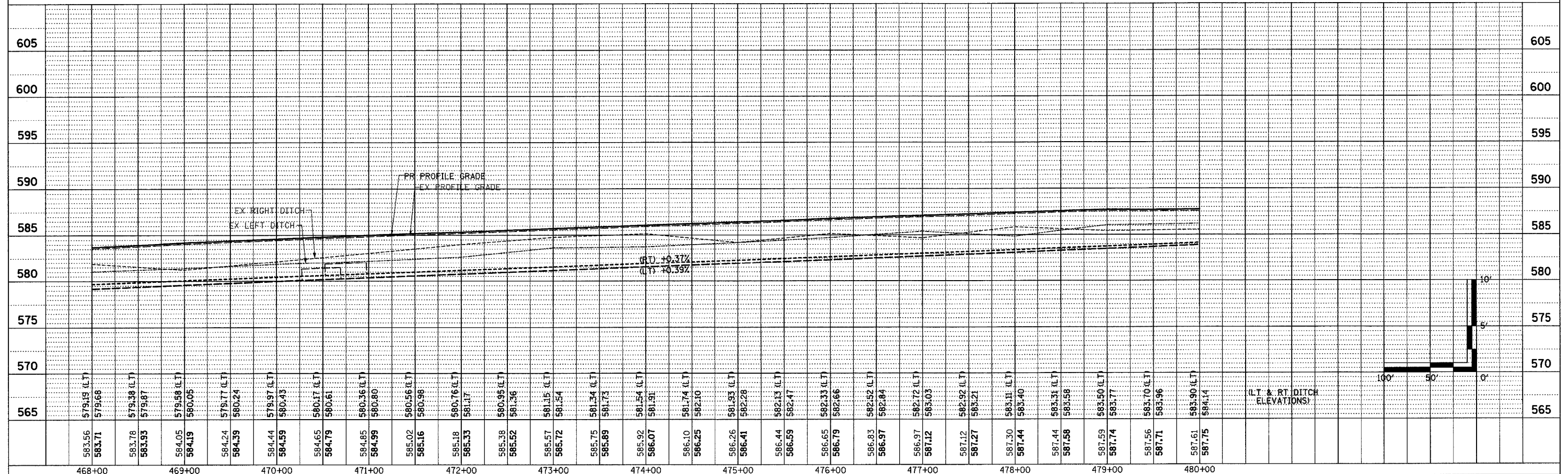
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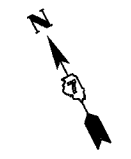


PERMANENT SURVEY MARKERS, TYPE II
 P.I. STA. 477+99.84 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 55' LT STA. 470+06.08 1 EACH
 55' LT STA. 479+00.00 1 EACH
 TOTAL 2 EACH



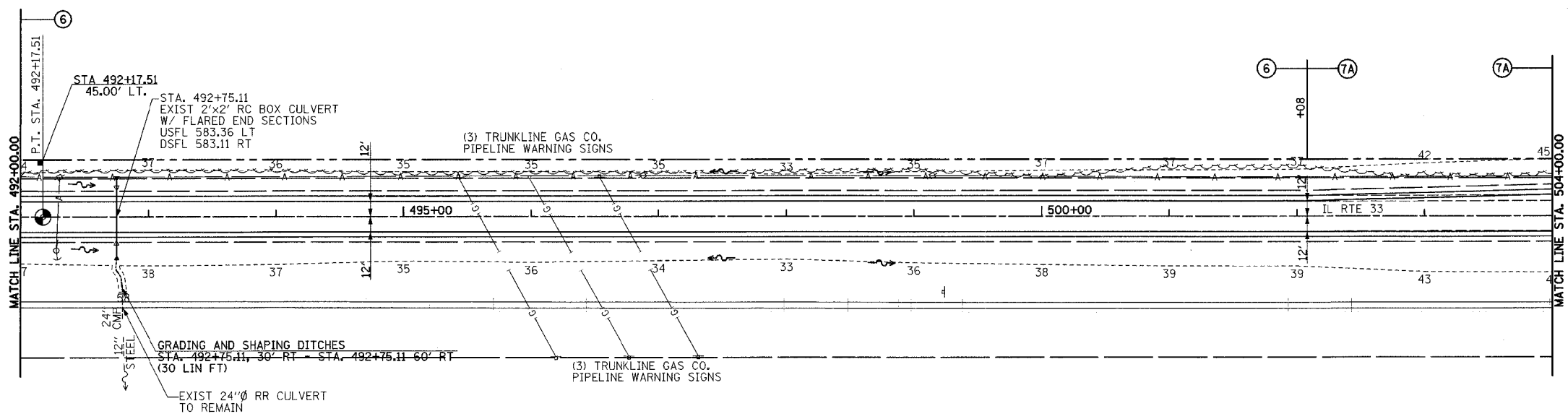
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 72 |
| STA. 492+00.00 TO STA. 504+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6.7Y, RS-1, 6B-1, 7B-1 & 7BR-2) | | | | |
| CONTRACT NO. 94356 | | | | |



PIPE CULVERTS, TYPE 1 REINFORCED CONCRETE - ELLIPTICAL, EQUIVALENT ROUND-SIZE 24"
 STA. 492+75.11
 LENGTH = 46'
 OFFSET = 22' LT - 24' RT
 UPSTREAM FLOWLINE = 584.70 LT
 DOWNSTREAM FLOWLINE = 584.50 RT

PRC FLARED END SECTIONS - ELLIPTICAL, EQUIVALENT ROUND-SIZE 24"
 LT & RT STA. 492+75.11 2 EACH

BOX CULVERT REMOVAL
 21' LT - 18' RT STA. 492+75.11 39 LIN FT

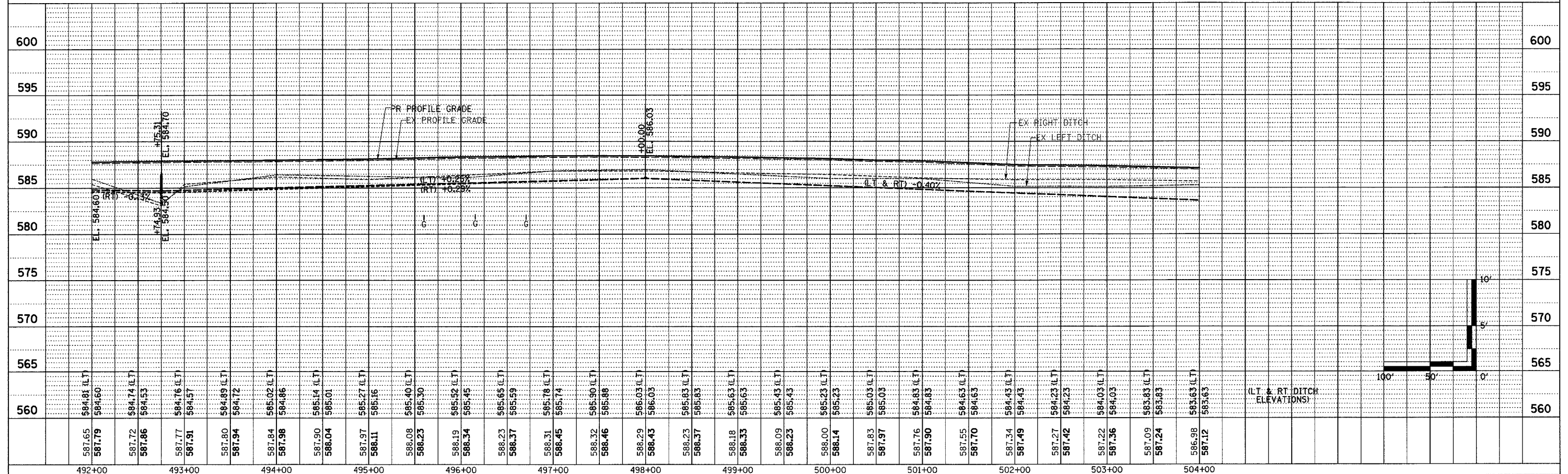


PERMANENT SURVEY MARKERS, TYPE I
 P.T. STA. 492+17.51 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 45' LT STA. 492+17.51 1 EACH
 TOTAL 1 EACH

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| DATE | BY |
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 73 |
| STA. 504+00.00 TO STA. 516+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *16.7Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



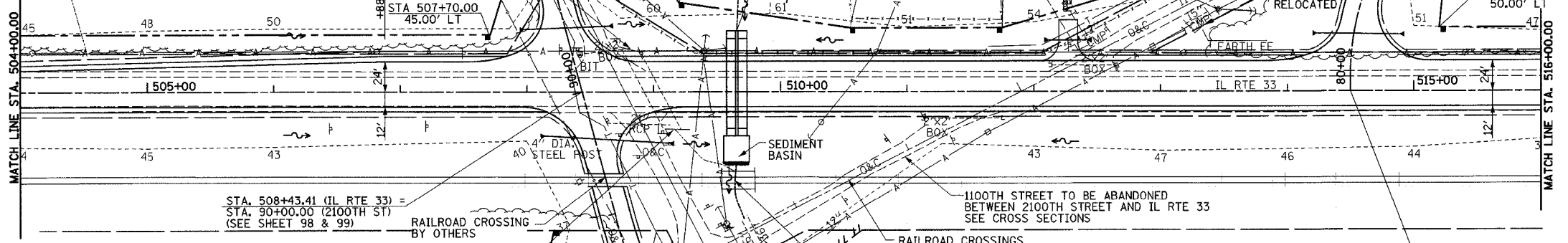
PIPE CULVERTS CLASS D, TYPE 1, 18"
 STA. 508+29.93, 51' LT
 LENGTH = 66'
 UPSTREAM FLOWLINE = 582.04
 DOWNSTREAM FLOWLINE = 580.95

END SECTIONS, 18"
 49' LT STA. 507+97.95 1 EACH
 52' LT STA. 508+63.91 1 EACH
 TOTAL 2 EACH

PIPE CULVERTS CLASS D, TYPE 1, 21"
 STA. 508+51.96, 37' RT
 LENGTH = 98'
 UPSTREAM FLOWLINE = 581.80
 DOWNSTREAM FLOWLINE = 580.29

END SECTIONS, 21"
 35' RT STA. 508+14.42 1 EACH
 41' RT STA. 509+12.26 1 EACH
 TOTAL 2 EACH

PAVEMENT REMOVAL
 2100TH ST N 97 SQ YD



PIPE CULVERTS CLASS D, TYPE 1, 24"
 STA. 51+29.85
 LENGTH = 41'
 UPSTREAM FLOWLINE = 582.44
 DOWNSTREAM FLOWLINE = 581.99

END SECTIONS, 24"
 20.5' LT STA. 51+30.73 1 EACH
 20.5' RT STA. 51+28.96 1 EACH
 TOTAL 2 EACH

FILLING EXISTING CULVERTS
 STA. 508+92.54 95 CU YD

NOTE 1:
 PARTIALLY REMOVE HEADWALL AS REQUIRED TO CLEAR FINAL GRADING. COST OF REMOVAL INCLUDED IN COST OF FILLING EXISTING CULVERTS.

RAILROAD CROSSING BY OTHERS
 STA. 509+15.25
 EXIST 12'x3' RC BOX CULVERT
 H = 578.15
 SEE NOTE 1

GRADING AND SHAPING DITCHES
 STA. 509+65.00, 55' RT - STA. 509+65.00 100' RT (46 LIN FT)

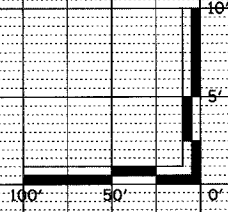
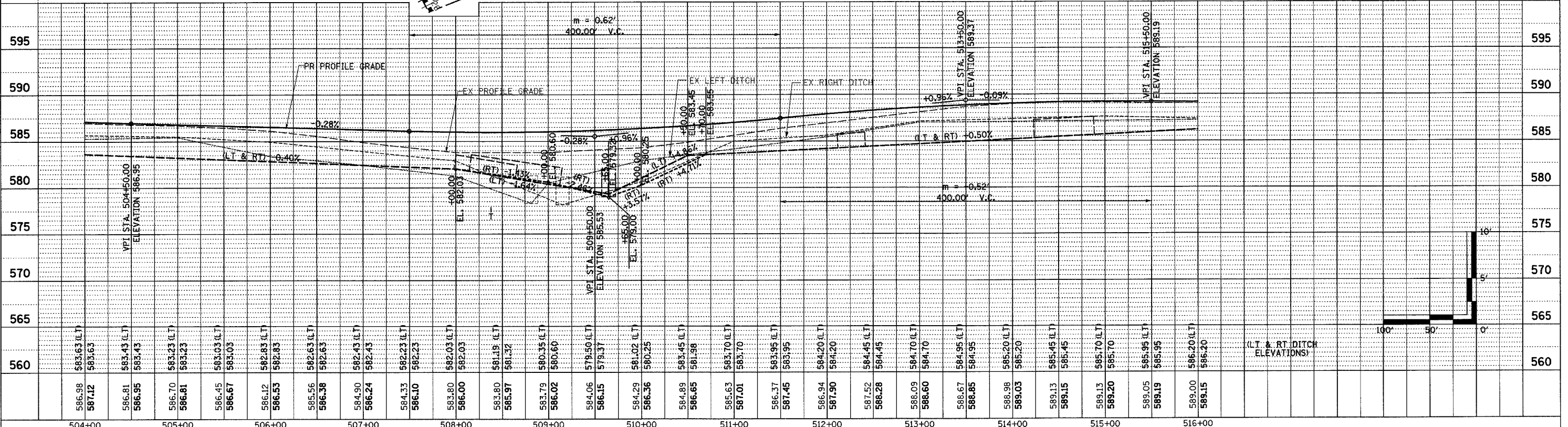
BOX CULVERT REMOVAL
 1100TH AVE N 50'
 1100TH AVE S 60'
 2100TH ST N 42'
 TOTAL 152'

PIPE CULVERTS CLASS D, TYPE 1, EQUIVALENT ROUND-SIZE 24"
 STA. 514+52.80, 45' LT
 LENGTH = 66'
 UPSTREAM FLOWLINE = 585.64
 DOWNSTREAM FLOWLINE = 585.32

END SECTIONS, EQUIVALENT ROUND-SIZE 24"
 44' LT STA. 514+23.76 1 EACH
 45' LT STA. 514+88.80 1 EACH
 TOTAL 2 EACH

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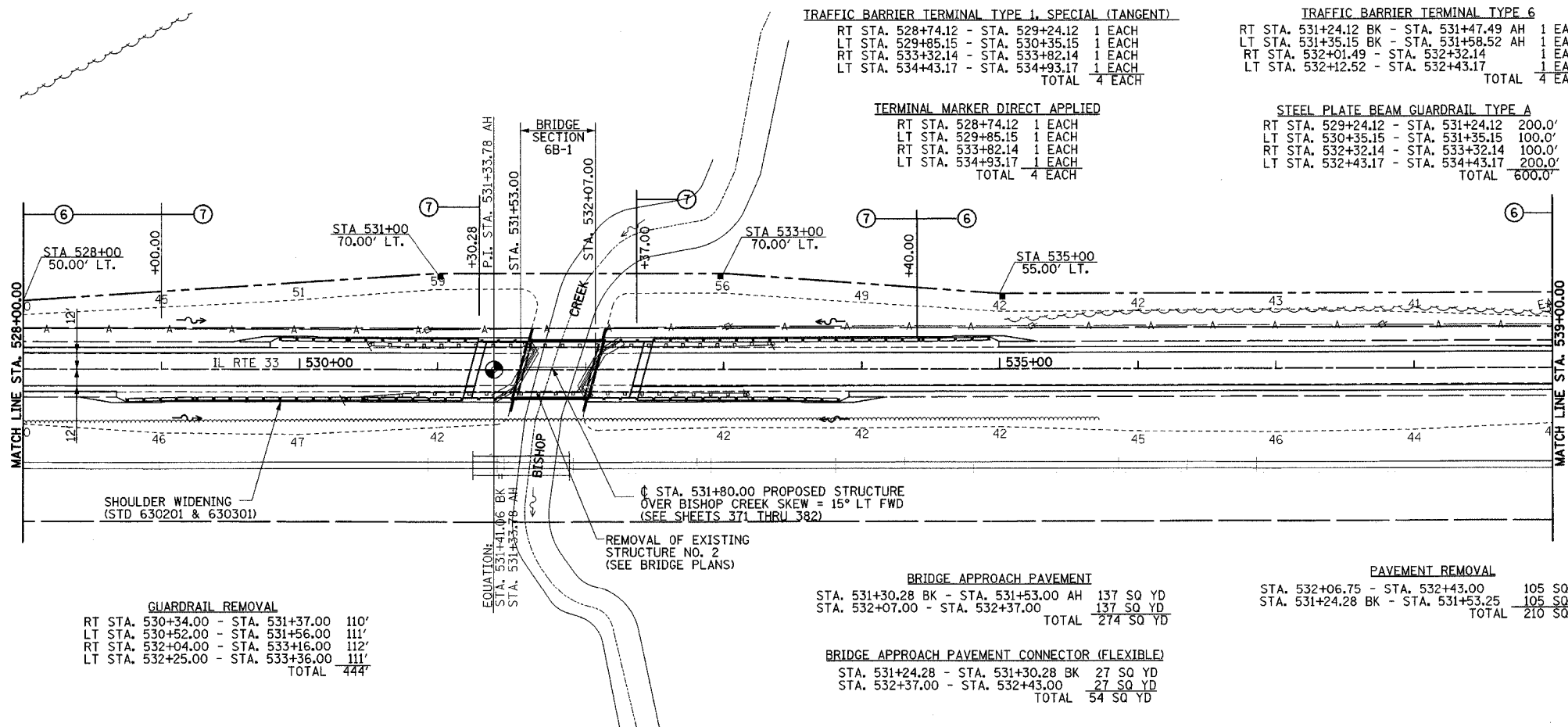
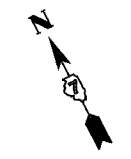
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| DATE | |
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| PROFILE | SURVEYED |
| | NOTE BOOK |
| | GRADES |
| | CHECKED |
| | BY |
| | DATE |
| | NO. |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 75 |
| STA. 528+00.00 TO STA. 539+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



GUARDRAIL REMOVAL

| | | | |
|-------------------|---|----------------|------|
| RT STA. 530+34.00 | - | STA. 531+37.00 | 110' |
| LT STA. 530+52.00 | - | STA. 531+56.00 | 111' |
| RT STA. 532+04.00 | - | STA. 533+16.00 | 112' |
| LT STA. 532+25.00 | - | STA. 533+36.00 | 111' |
| TOTAL 444' | | | |

BRIDGE APPROACH PAVEMENT

| | | | |
|-------------------|---|-------------------|-----------|
| STA. 531+30.28 BK | - | STA. 531+53.00 AH | 137 SQ YD |
| STA. 532+07.00 | - | STA. 532+37.00 | 137 SQ YD |
| TOTAL 274 SQ YD | | | |

PAVEMENT REMOVAL

| | | | |
|-------------------|---|----------------|-----------|
| STA. 532+06.75 | - | STA. 532+43.00 | 105 SQ YD |
| STA. 531+24.28 BK | - | STA. 531+53.25 | 105 SQ YD |
| TOTAL 210 SQ YD | | | |

BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)

| | | | |
|----------------|---|-------------------|----------|
| STA. 531+24.28 | - | STA. 531+30.28 BK | 27 SQ YD |
| STA. 532+37.00 | - | STA. 532+43.00 | 27 SQ YD |
| TOTAL 54 SQ YD | | | |

PERMANENT SURVEY MARKERS, TYPE 1

| | |
|--------------------------|--------|
| P.I. STA. 531+33.78 (AH) | 1 EACH |
|--------------------------|--------|

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

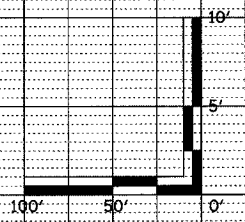
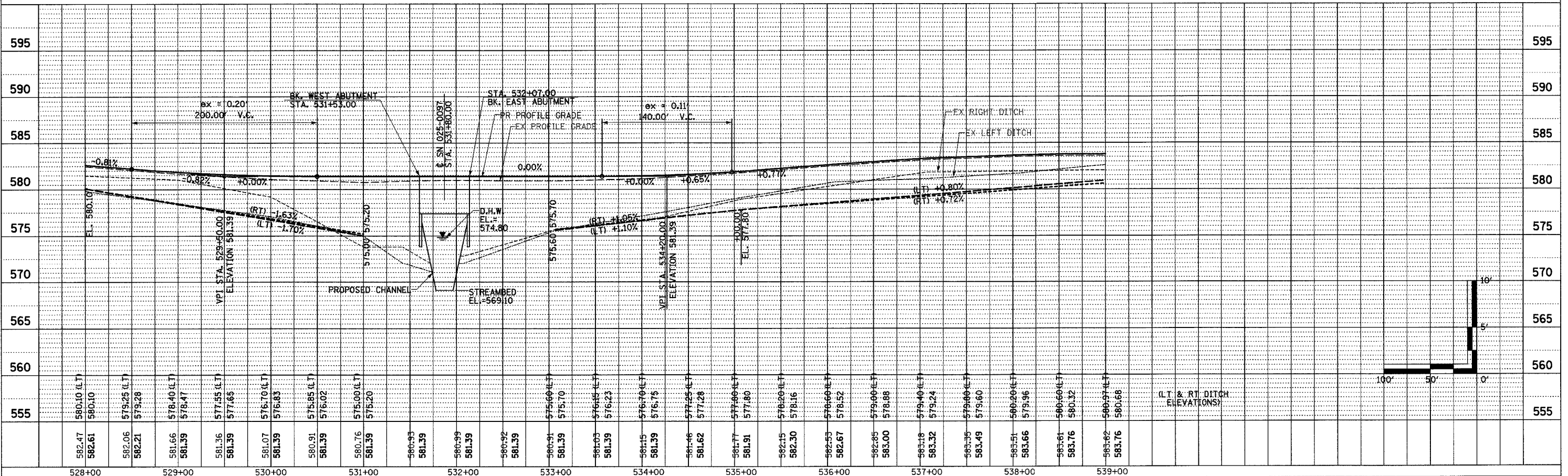
| | |
|-----------------------|--------|
| 50' LT STA. 528+00.00 | 1 EACH |
| 70' LT STA. 531+00.00 | 1 EACH |
| 70' LT STA. 533+00.00 | 1 EACH |
| 55' LT STA. 535+00.00 | 1 EACH |
| TOTAL 4 EACH | |

GUARDRAIL MARKERS, TYPE A

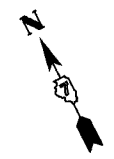
| | | | |
|-------------------|---|----------------|--------|
| RT STA. 528+74.12 | - | STA. 531+47.49 | 3 EACH |
| LT STA. 529+85.15 | - | STA. 531+58.52 | 2 EACH |
| RT STA. 532+01.49 | - | STA. 533+82.14 | 2 EACH |
| LT STA. 532+12.52 | - | STA. 534+93.17 | 3 EACH |
| TOTAL 10 EACH | | | |

BARRIER WALL MARKERS, TYPE B

| | | | |
|-------------------|---|----------------|--------|
| RT STA. 531+49.99 | - | STA. 531+98.99 | 2 EACH |
| LT STA. 531+61.02 | - | STA. 532+10.02 | 2 EACH |
| TOTAL 4 EACH | | | |



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-------------------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 76 |
| STA. 539+00.00 | | TO STA. 551+00.00 | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

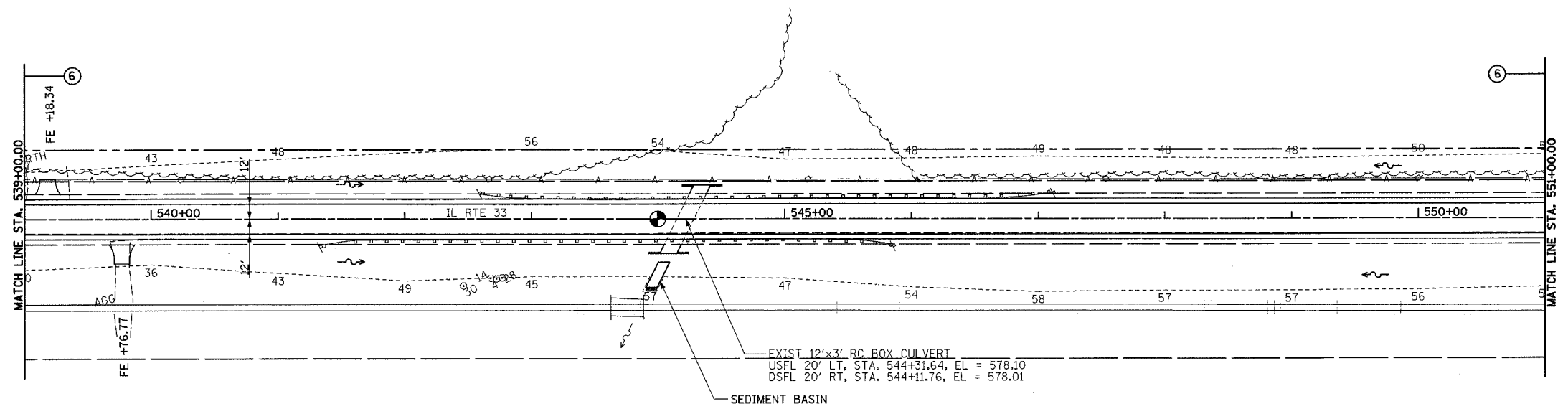


12'x3' CAST IN PLACE CULVERT EXTENSIONS

STA. 544+21.63
 LENGTH = 8' LT
 LENGTH = 8' RT
 UPSTREAM FLOWLINE = 578.20 LT
 DOWNSTREAM FLOWLINE = 578.00 RT
 (SEE DETAIL SHEET 148)

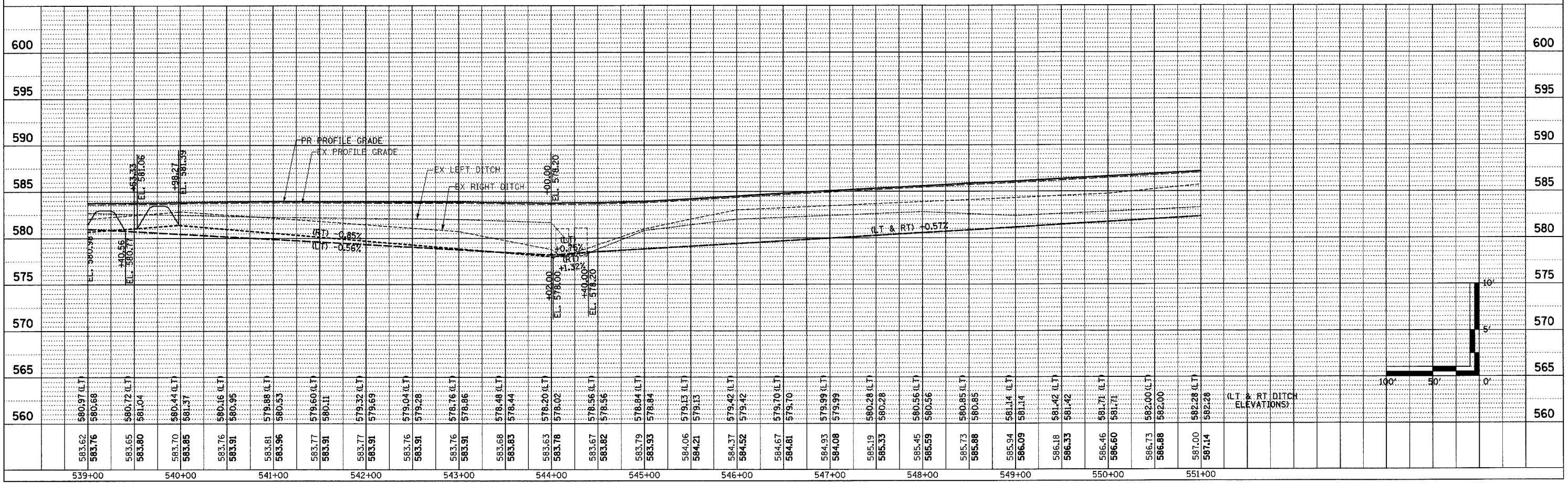
PERMANENT SURVEY MARKERS, TYPE I
 P.O.T. STA. 544+00.00 1 EACH

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| | BY | |
| | NO. OF WAY CHECKED | |
| | CADD FILE NAME | |



GUARDRAIL REMOVAL
 LT STA. 542+60.50 - STA. 547+10.50 450'
 RT STA. 541+36.00 - STA. 545+85.00 449'
 TOTAL 899'

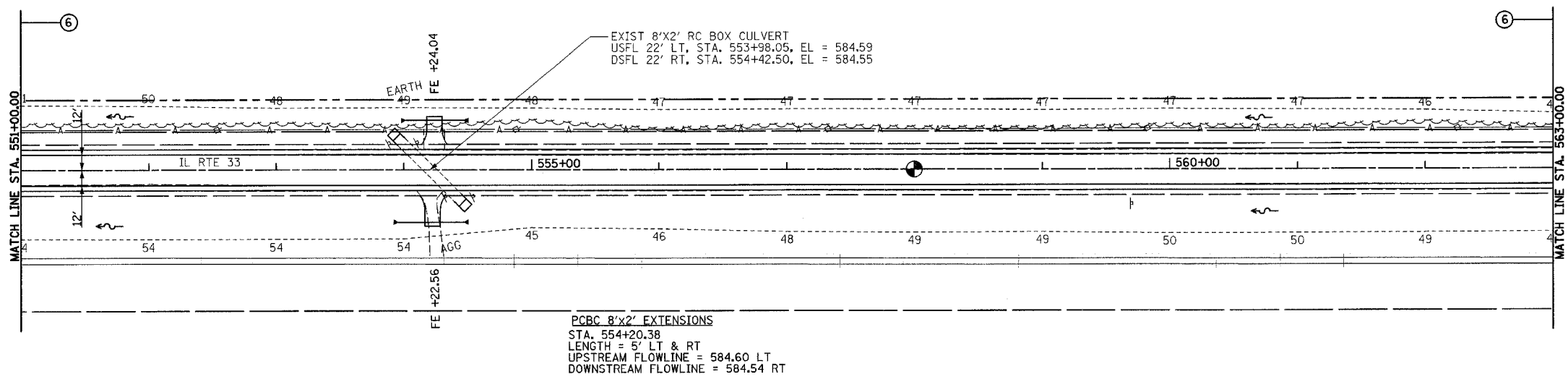
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| | NO. OF WAY CHECKED | |
| | STRUCTURE NOTATION CRKD | |



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|-----------------------------------|---------|-------------------|------------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 77 |
| STA. 551+00.00 | | TO STA. 563+00.00 | | |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



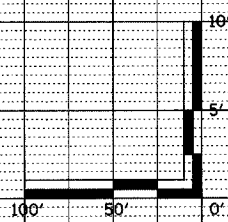
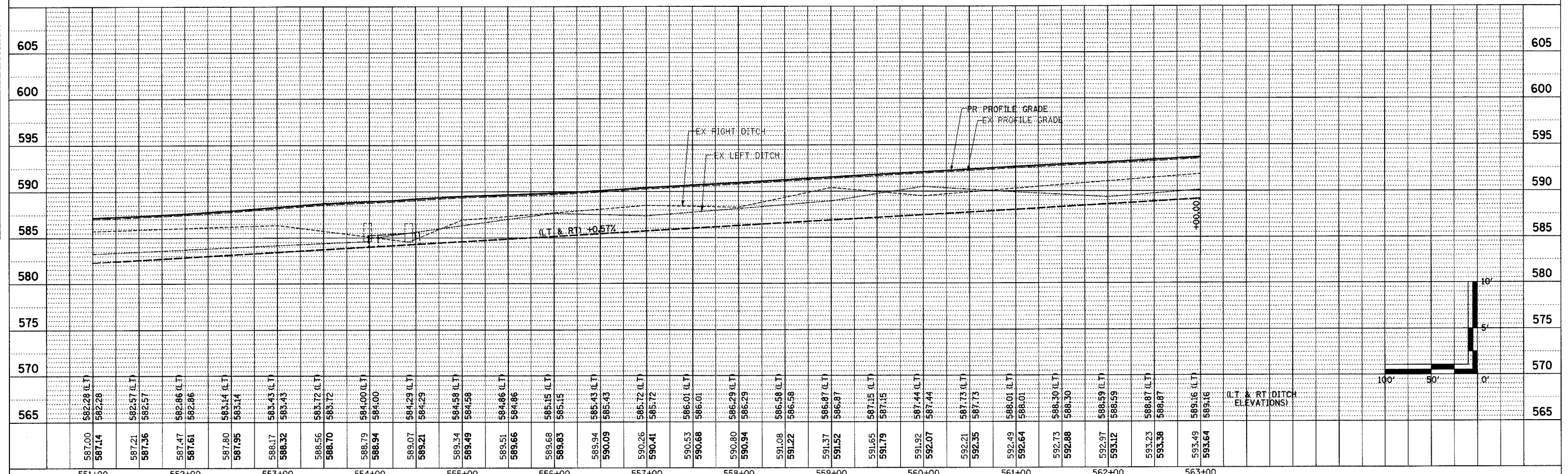
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| PLAN | SURVEYED | BY | DATE |
| | ALIGNED | | |
| | CHECKED | | |
| | NO. | | |



PERMANENT SURVEY MARKERS, TYPE I
P.O.T. STA. 558+00.00 1 EACH

BOX CULVERT END SECTIONS CULVERT NO. 13
LT & RT STA. 554+20.38 2 EACH

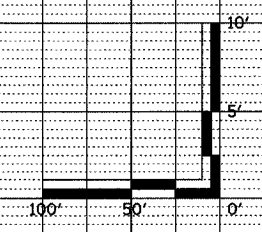
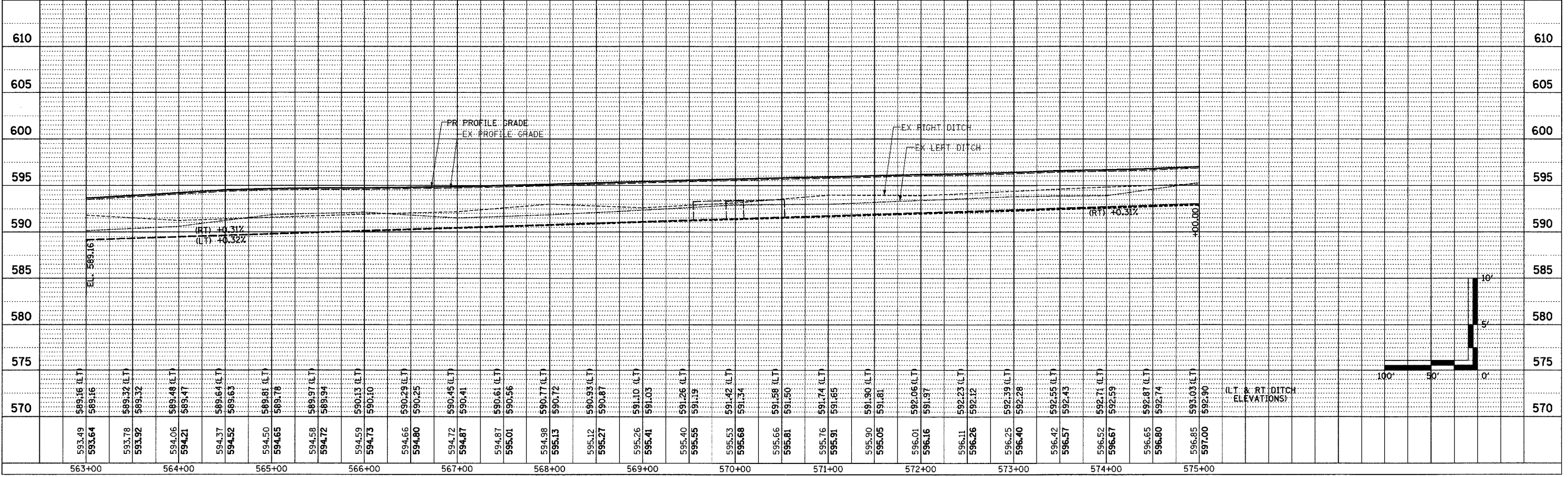
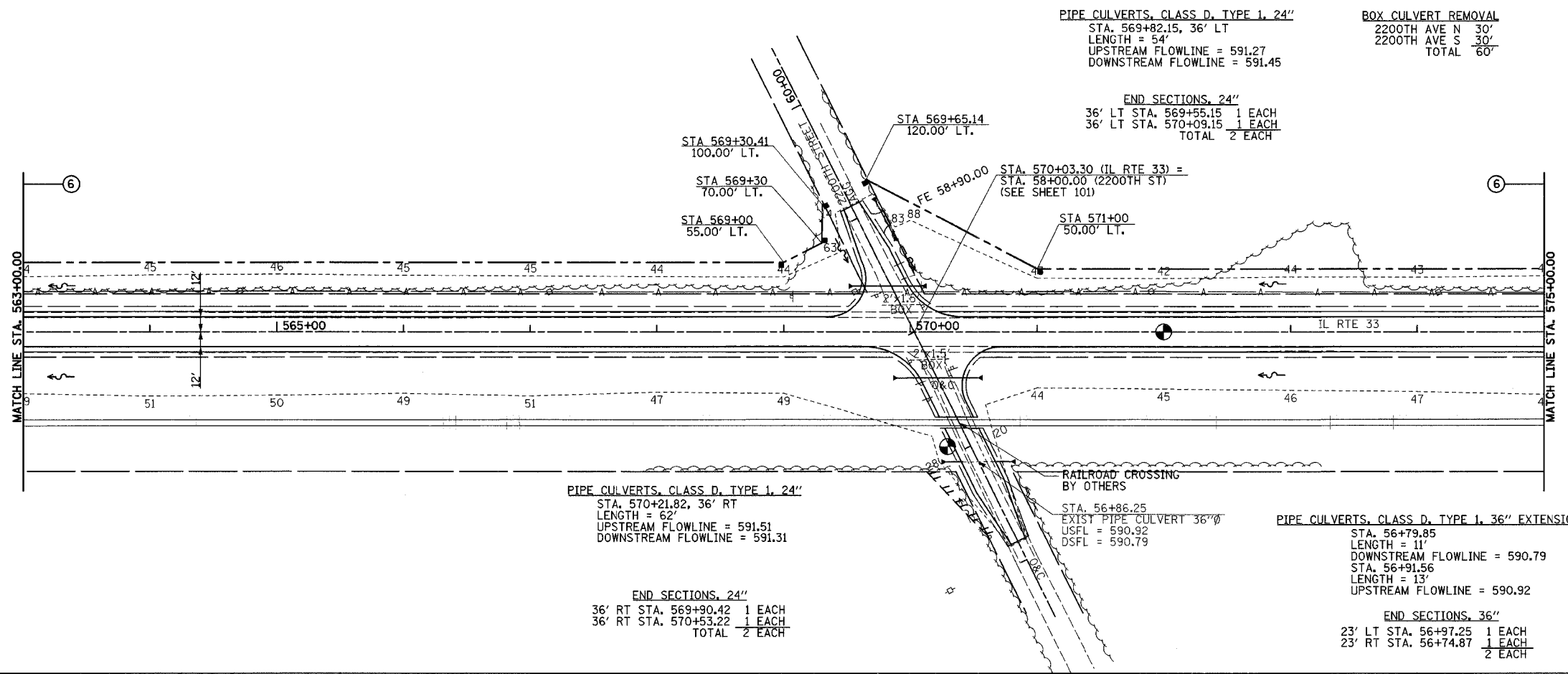
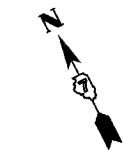
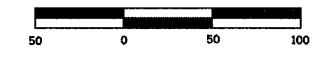
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|---------|----------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | GRADES | | |
| | CHECKED | | |
| | NO. | | |



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| DATE | |
| BY | |
| PLAN | |
| SURVEYED | |
| ALIGNED | |
| CHECKED | |
| RT. OF WAY CHECKED | |
| PAID FILE NAME | |
| NO. | |

| | |
|-----------------------------|--|
| DATE | |
| BY | |
| PROFILE | |
| SURVEYED | |
| GRADES CHECKED | |
| BLK. NOTED | |
| STRUCTURE NOTATIONS CHECKED | |
| NO. | |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 78 |
| STA. 563+00.00 TO STA. 575+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



PIPE CULVERTS, CLASS D, TYPE 1, 15"
 STA. 591+95.31, 33' LT
 LENGTH = 133'
 UPSTREAM FLOWLINE = 602.12
 DOWNSTREAM FLOWLINE = 601.32

END SECTIONS, 15"
 34' LT STA. 591+28.98 1 EACH
 33' LT STA. 592+61.64 1 EACH
 TOTAL 2 EACH

EXIST CURVE C7
 P.I. STA. = 590+74.46
 $\Delta = 0^\circ 23' 49"$ (RT)
 $D = 0^\circ 02' 15"$
 $R = 152,759.23'$
 $T = 529.26'$
 $L = 1,058.52'$
 $E = 0.92'$
 $e = N/C$
 T.R. = NONE
 S.E. RUN = NONE
 P.C. STA. = 585+45.20
 P.T. STA. = 596+03.72

SEE SHEET 136 FOR ISLAND GEOMETRY
 STA 591+41.69
 50.00' LT.

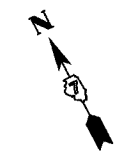
ISLAND REMOVAL
 LT STA. 591+74.73 - STA. 591+89.31 115 SQ FT
 CONCRETE MEDIAN, TYPE SM-6.24
 LT STA. 591+73.48 - STA. 591+89.37 131 SQ FT

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
 LT STA. 591+38.00 - STA. 591+51.00 15'
 LT STA. 592+50.00 - STA. 592+58.00 12'
 TOTAL 27'

PAVEMENT REMOVAL
 NORB AVE 372 SQ YD

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 80 |

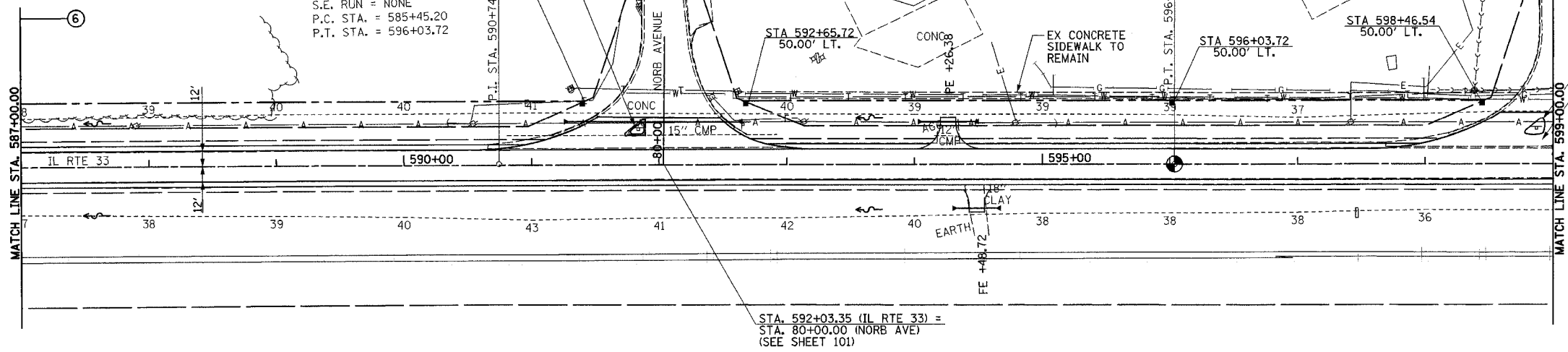
STA. 587+00.00 TO STA. 599+00.00
 FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT
 *(6,7)Y, RS-1, 6B-1, 7B-1 & 7B-2
 CONTRACT NO. 94356



EXISTING CORNER ISLAND TO REMAIN
 BITUMINOUS MIXTURE COMPLETE FEATHERED TO MATCH EXISTING CONCRETE SURFACE. SEE ENTRANCE SCHEDULE FOR QUANTITIES.

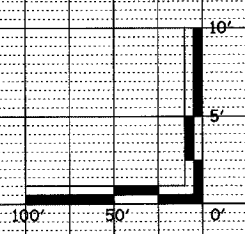
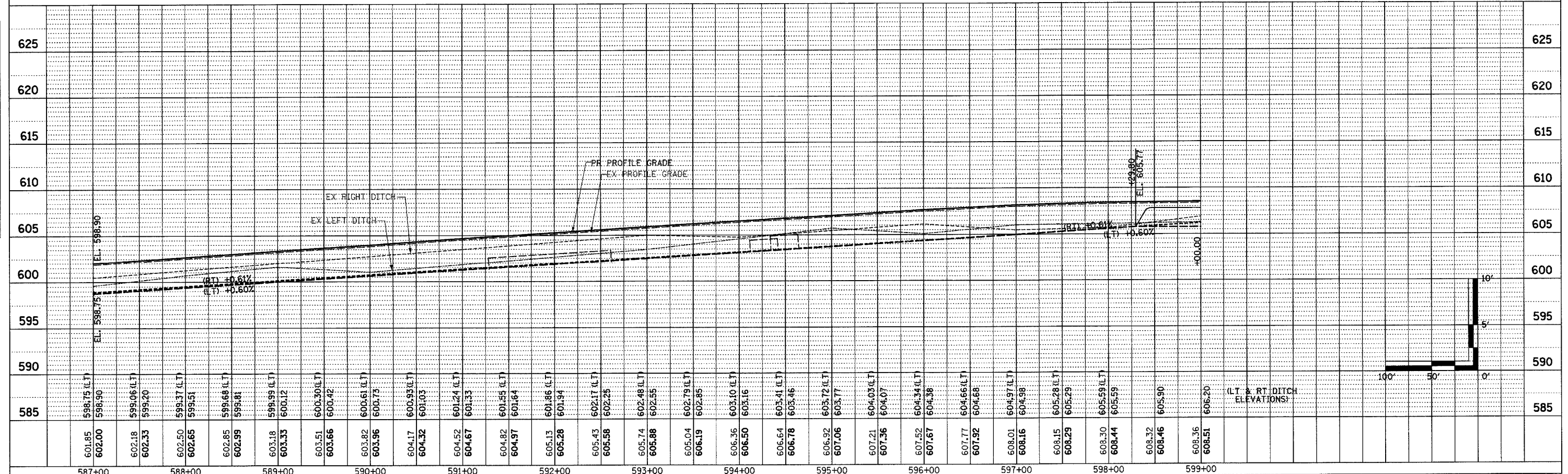
PERMANENT SURVEY MARKERS, TYPE I
 P.I. STA. 590+74.46 1 EACH
 P.T. STA. 596+03.72 1 EACH
 TOTAL 2 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 50' LT STA. 591+41.69 1 EACH
 50' LT STA. 592+65.72 1 EACH
 50' LT STA. 596+03.72 1 EACH
 50' LT STA. 598+46.54 1 EACH
 TOTAL 4 EACH



| DATE | BY | REVISION |
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| DATE | BY | REVISION |
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| BY | |
| SURVEYED | |
| PLAN | |
| NO. | |
| CHECKED | |
| FILE NAME | |
| NO. | |

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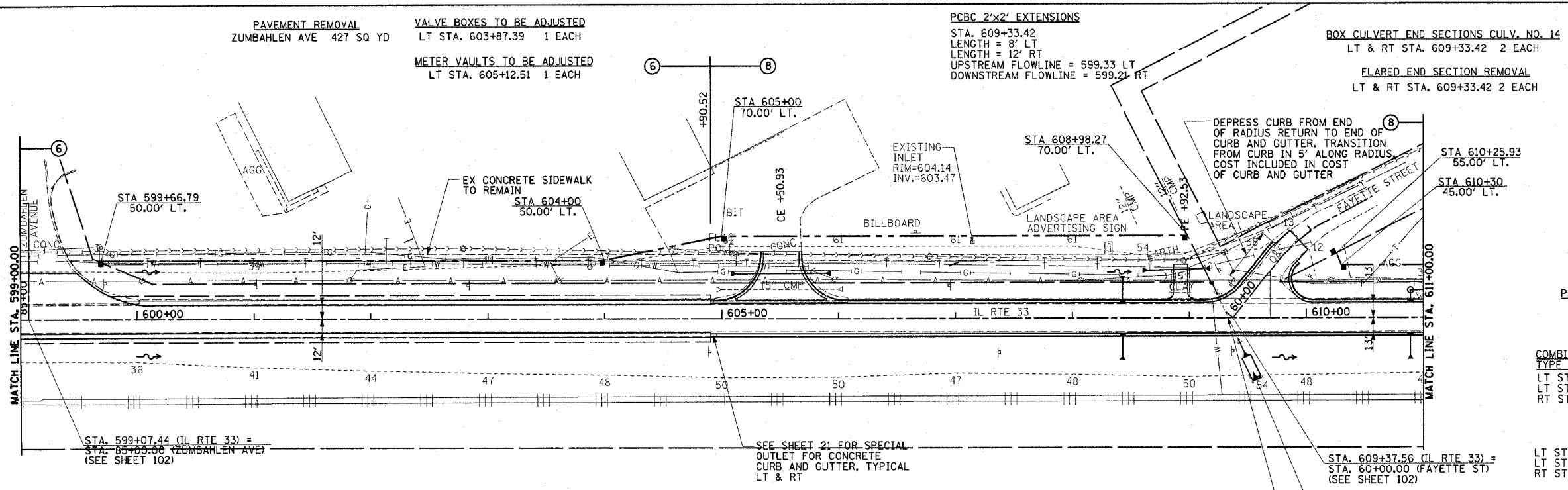
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 81 |
| STA. 599+00.00 TO STA. 611+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



PERMANENT SURVEY MARKERS, TYPE I
P.O.T. STA. 605+00.00 1 EACH

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (ABUTTING EXISTING PAVEMENT)
LT STA. 604+90.52 - STA. 609+73.69 509'
LT STA. 609+92.13 - STA. 611+00.00 140'
RT STA. 604+90.52 - STA. 611+00.00 610'
TOTAL 1259'

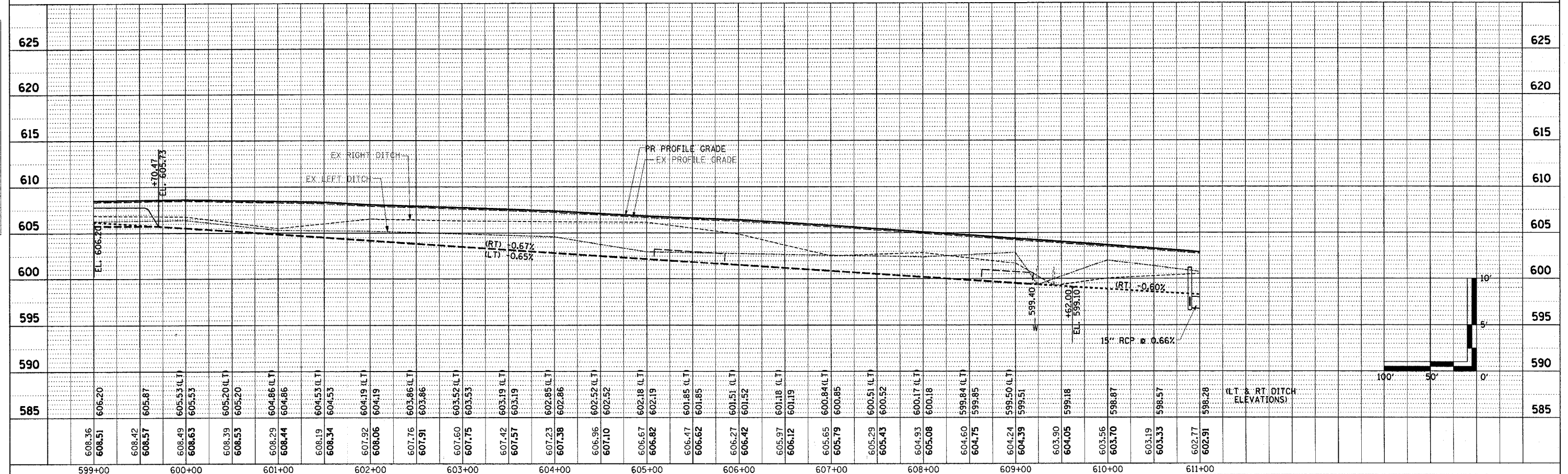
TIE BARS
LT STA. 604+90.52 - STA. 609+73.69 204 EACH
LT STA. 609+92.13 - STA. 611+00.00 56 EACH
RT STA. 604+90.52 - STA. 611+00.00 244 EACH
TOTAL 504 EACH



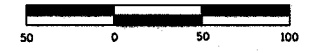
FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

- 50' LT STA. 599+66.79 1 EACH
- 50' LT STA. 604+00.00 1 EACH
- 70' LT STA. 605+00.00 1 EACH
- 70' LT STA. 608+98.27 1 EACH
- 55' LT STA. 610+25.93 1 EACH
- 45' LT STA. 610+30.00 1 EACH
- TOTAL 6 EACH

SEE SHEET 22 FOR STORM SEWER SCHEDULE



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 82 |
| STA. 611+00.00 TO STA. 623+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



PERMANENT SURVEY MARKERS, TYPE I
P.C. STA. 614+61.85 1 EACH
P.T. STA. 620+02.52 1 EACH
TOTAL 2 EACH

PERMANENT SURVEY MARKERS, TYPE II
P.I. STA. 617+37.31 1 EACH

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (ABUTTING EXISTING PAVEMENT)
LT STA. 611+00.00 - STA. 613+15.57 244'
LT STA. 613+35.74 - STA. 617+10.72 405'
RT STA. 611+00.00 - STA. 617+55.00 655'
TOTAL 1304'

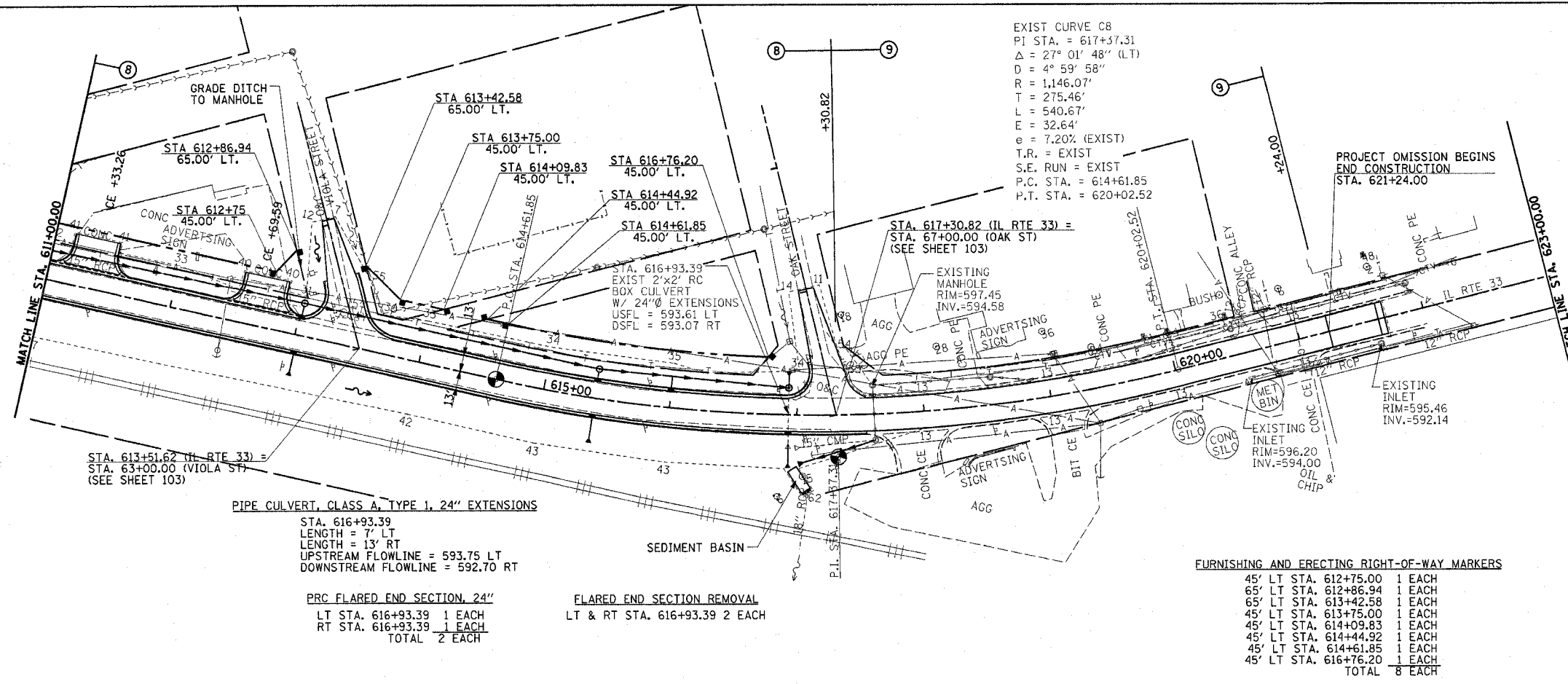
TIE BARS
LT STA. 611+00.00 - STA. 613+15.57 98 EACH
LT STA. 613+35.74 - STA. 617+10.72 162 EACH
RT STA. 611+00.00 - STA. 617+55.00 262 EACH
TOTAL 522 EACH

BITUMINOUS SURFACE REMOVAL - BUTT JOINT
STA. 621+39.00 - STA. 621+69.00 80 SQ YD

TEMPORARY RAMP
STA. 621+64.00 - STA. 621+69.00 13 SQ YD

SEE SHEET 22 FOR STORM SEWER SCHEDULE

EXIST CURVE C8
PI STA. = 617+37.31
Δ = 27° 01' 48" (LT)
D = 4° 59' 58"
R = 1,146.07'
T = 275.46'
L = 540.67'
E = 32.64'
e = 7.20% (EXIST)
T.R. = EXIST
S.E. RUN = EXIST
P.C. STA. = 614+61.85
P.T. STA. = 620+02.52



PIPE CULVERT, CLASS A, TYPE 1, 24" EXTENSIONS
STA. 616+93.39
LENGTH = 7' LT
LENGTH = 13' RT
UPSTREAM FLOWLINE = 593.75 LT
DOWNSTREAM FLOWLINE = 592.70 RT

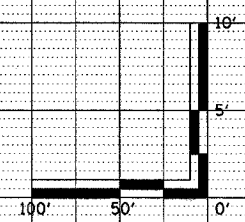
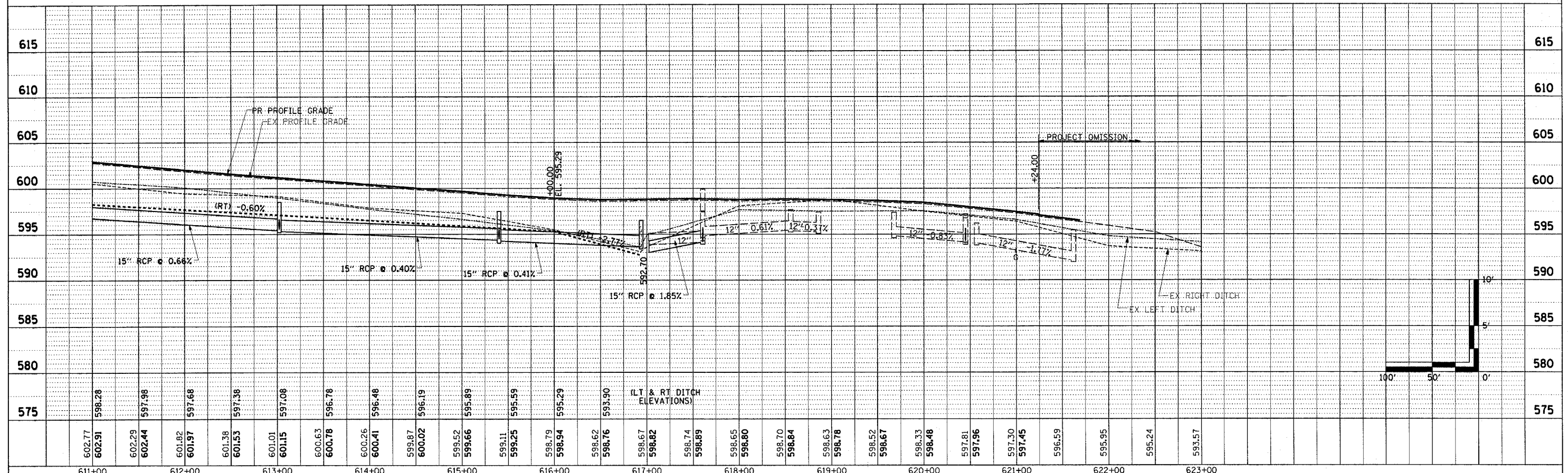
PRC FLARED END SECTION, 24"
LT STA. 616+93.39 1 EACH
RT STA. 616+93.39 1 EACH
TOTAL 2 EACH

FLARED END SECTION REMOVAL
LT & RT STA. 616+93.39 2 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
45' LT STA. 612+75.00 1 EACH
65' LT STA. 612+86.94 1 EACH
65' LT STA. 613+42.58 1 EACH
45' LT STA. 613+75.00 1 EACH
45' LT STA. 614+09.83 1 EACH
45' LT STA. 614+44.92 1 EACH
45' LT STA. 614+61.85 1 EACH
45' LT STA. 616+76.20 1 EACH
TOTAL 8 EACH

| DATE | BY | REVISION |
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Revised 5/2/07

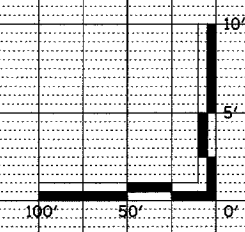
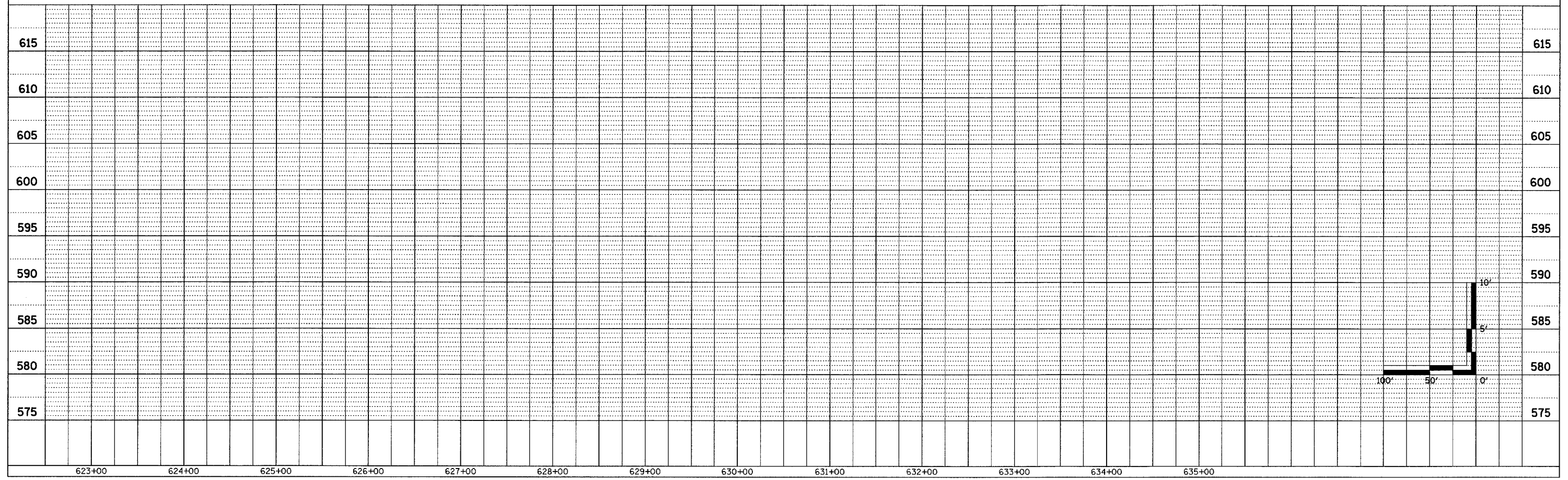
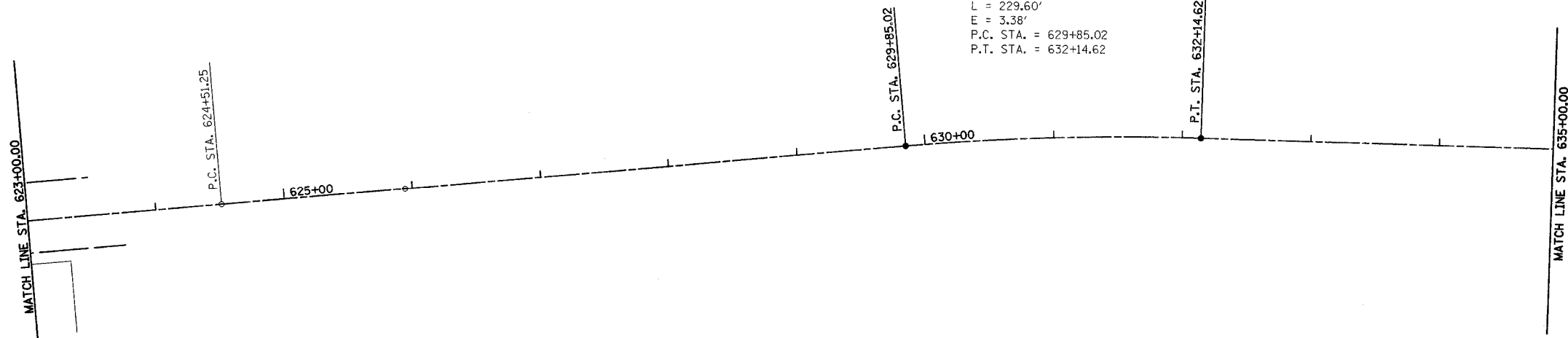
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| DATE | |
| BY | |
| SURVEYED | |
| GRADES | |
| CHECKED | |
| FILED | |
| DATE | |
| BY | |
| NO. | |

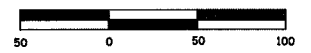
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|------------------------------------|---------|-------------------|------------------|-----------|
| 95 | * | EFFINGHAM | 409 | 83 |
| STA. 623+00.00 | | TO STA. 635+00.00 | | |
| FED. ROAD DIST. NO. 7 | | ILLINOIS | FED. AID PROJECT | |
| * (6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



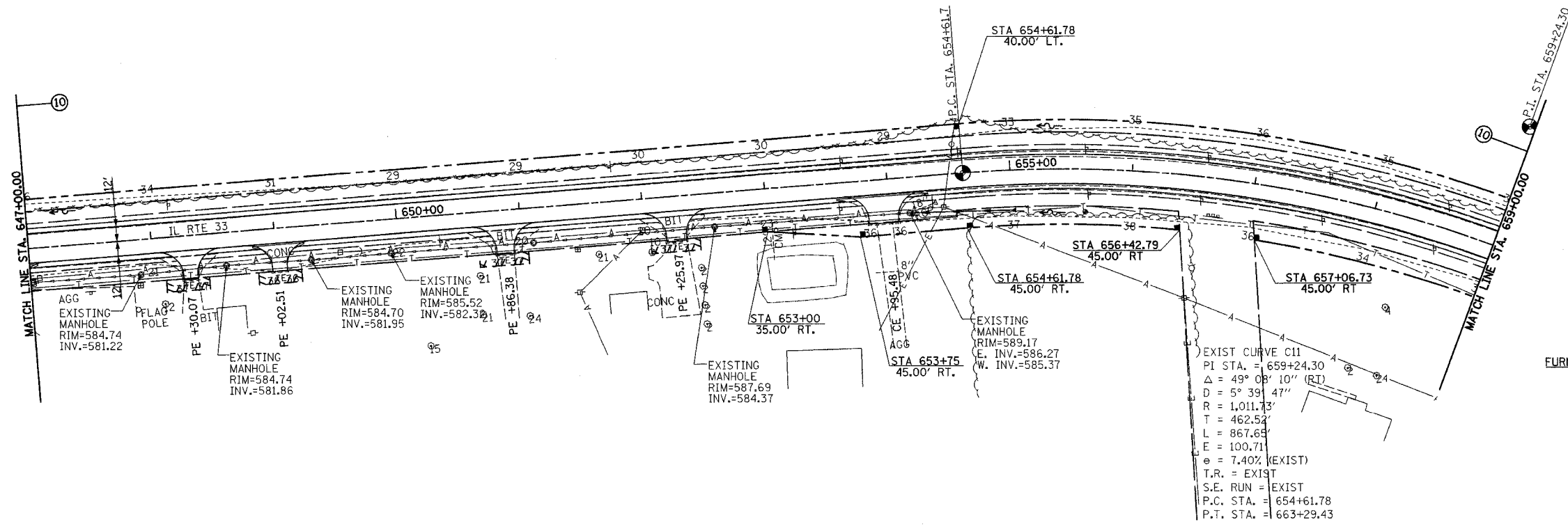
EXIST CURVE RC1
 PI STA. = 630+99.96
 $\Delta = 6^\circ 44' 46''$ (RT)
 $D = 2^\circ 56' 18''$
 $R = 1,950.00'$
 $T = 114.93'$
 $L = 229.60'$
 $E = 3.38'$
 P.C. STA. = 629+85.02
 P.T. STA. = 632+14.62



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|---|---------|-----------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 85 |
| STA. 647+00.00 TO STA. 659+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



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| PLAN | SURVEYED | DATE |
| NOTE BOOK | BY | |
| NO. | | |
| | CHECKED | |
| | DATE | |



PERMANENT SURVEY MARKERS, TYPE I
P.C. STA. 654+61.78 1 EACH

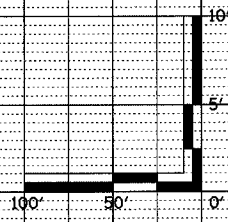
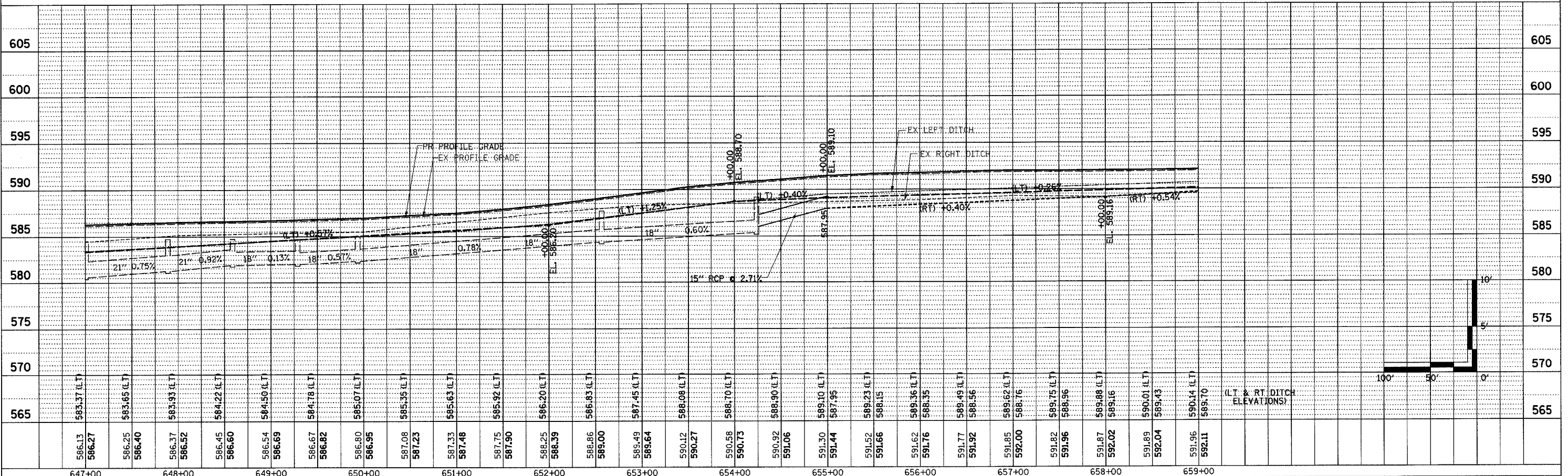
PERMANENT SURVEY MARKERS, TYPE II
P.I. STA. 659+24.30 1 EACH

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

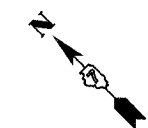
| | |
|-----------------------|---------------|
| 35' RT STA. 653+00.00 | 1 EACH |
| 45' RT STA. 653+75.00 | 1 EACH |
| 40' LT STA. 654+61.78 | 1 EACH |
| 45' RT STA. 654+61.78 | 1 EACH |
| 45' RT STA. 656+42.79 | 1 EACH |
| 45' RT STA. 657+06.73 | 1 EACH |
| TOTAL | 6 EACH |

SEE SHEET 22 FOR STORM SEWER SCHEDULE

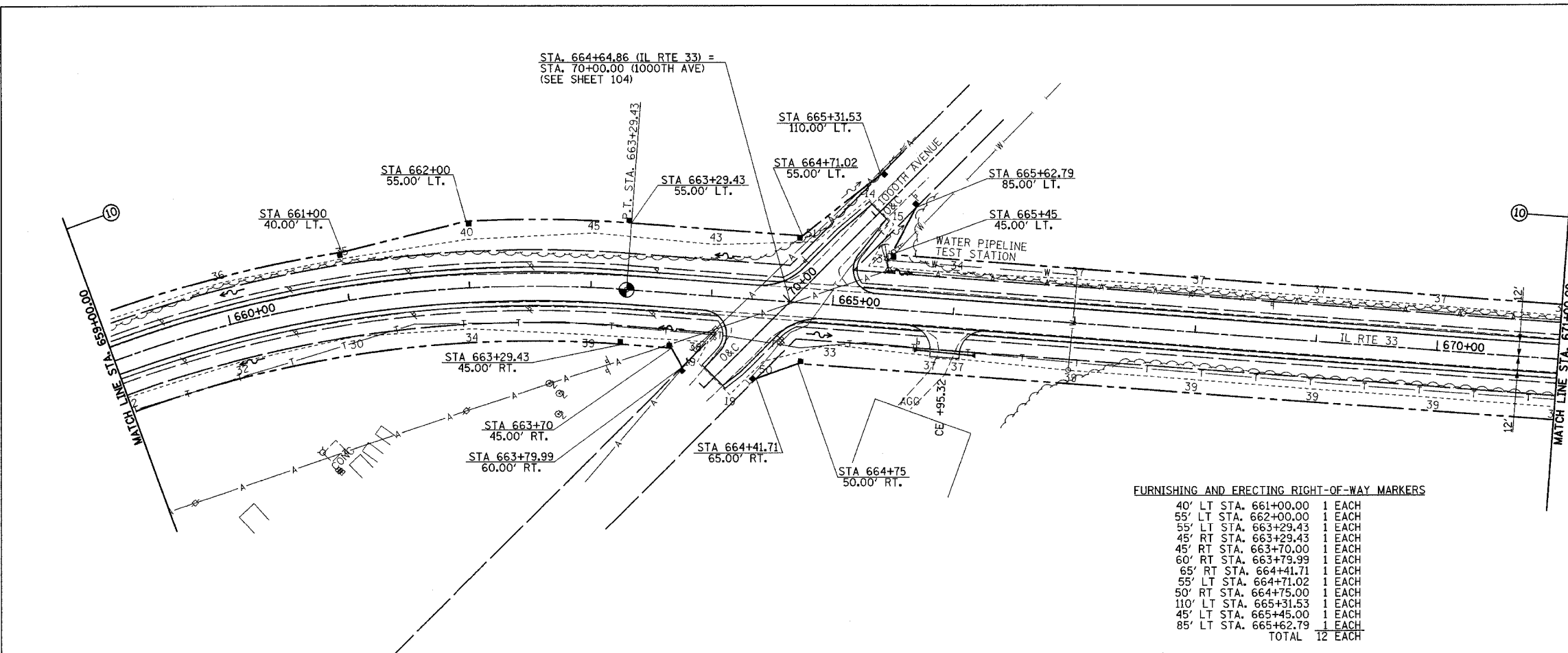
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|-----------|----------|------|
| PROFILE | SURVEYED | DATE |
| NOTE BOOK | BY | |
| NO. | | |
| | CHECKED | |
| | DATE | |



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 86 |
| STA. 659+00.00 TO STA. 671+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1 & 7B-2 | | | | |
| CONTRACT NO. 94356 | | | | |



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| DATE | |
| BY | |
| NO. | |
| DATE | |
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| NO. | |



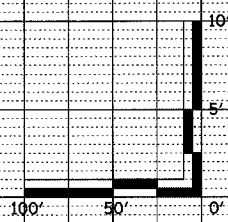
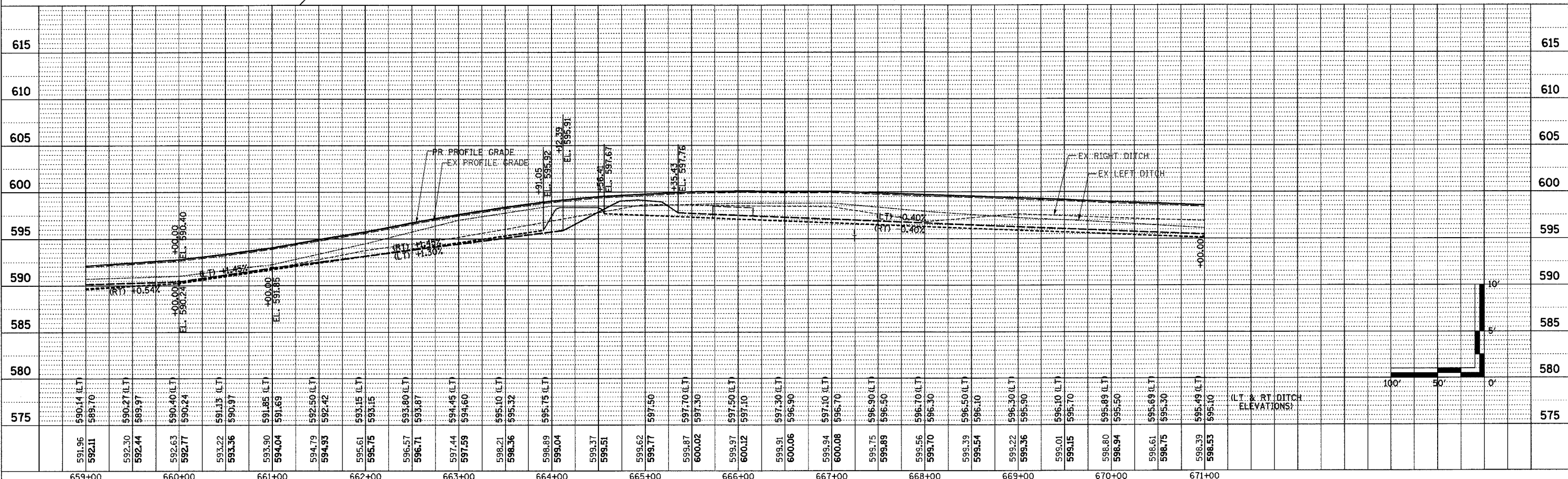
PERMANENT SURVEY MARKERS, TYPE 1
 P.T. STA. 663+29.43 1 EACH
 P.O.T. STA. 671+00.00 1 EACH
 TOTAL 2 EACH

METER VAULTS TO BE ADJUSTED
 RT STA. 666+97.73 1 EACH

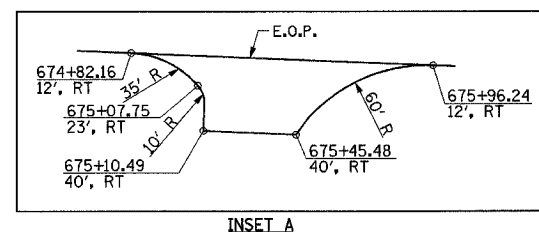
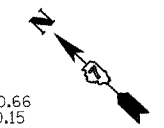
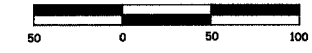
FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

| | |
|------------------------|---------|
| 40' LT STA. 661+00.00 | 1 EACH |
| 55' LT STA. 662+00.00 | 1 EACH |
| 55' LT STA. 663+29.43 | 1 EACH |
| 45' RT STA. 663+29.43 | 1 EACH |
| 45' RT STA. 663+70.00 | 1 EACH |
| 60' RT STA. 663+79.99 | 1 EACH |
| 65' RT STA. 664+41.71 | 1 EACH |
| 55' LT STA. 664+71.02 | 1 EACH |
| 50' RT STA. 664+75.00 | 1 EACH |
| 110' LT STA. 665+31.53 | 1 EACH |
| 45' LT STA. 665+45.00 | 1 EACH |
| 85' LT STA. 665+62.79 | 1 EACH |
| TOTAL | 12 EACH |

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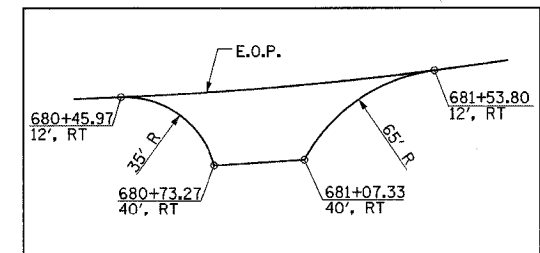
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 87 |
| STA. 671+00.00 TO STA. 683+00.00 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6.7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



EXIST CURVE C12
 PI STA. = 683+37.79
 $\Delta = 32^\circ 01' 10''$ (LT)
 $D = 3^\circ 59' 31''$
 $R = 1,435.26'$
 $T = 411.82'$
 $L = 802.08'$
 $E = 57.91'$
 $e = 7.00\%$ (EXIST)
 T.R. = EXIST
 S.E. RUN = EXIST
 P.C. STA. = 679+25.98
 P.T. STA. = 687+28.06

EXIST 3'x2' RC BOX CULVERT
 USFL 24' LT, STA. 682+75.26, EL = 590.66
 DSFL 21' RT, STA. 682+99.33, EL = 590.15

PERMANENT SURVEY MARKERS, TYPE I
 P.C. STA. 679+25.98 1 EACH



INSET B

AGGREGATE TO BE REMOVED. COST INCLUDED IN COST OF EARTHWORK.

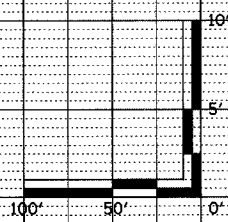
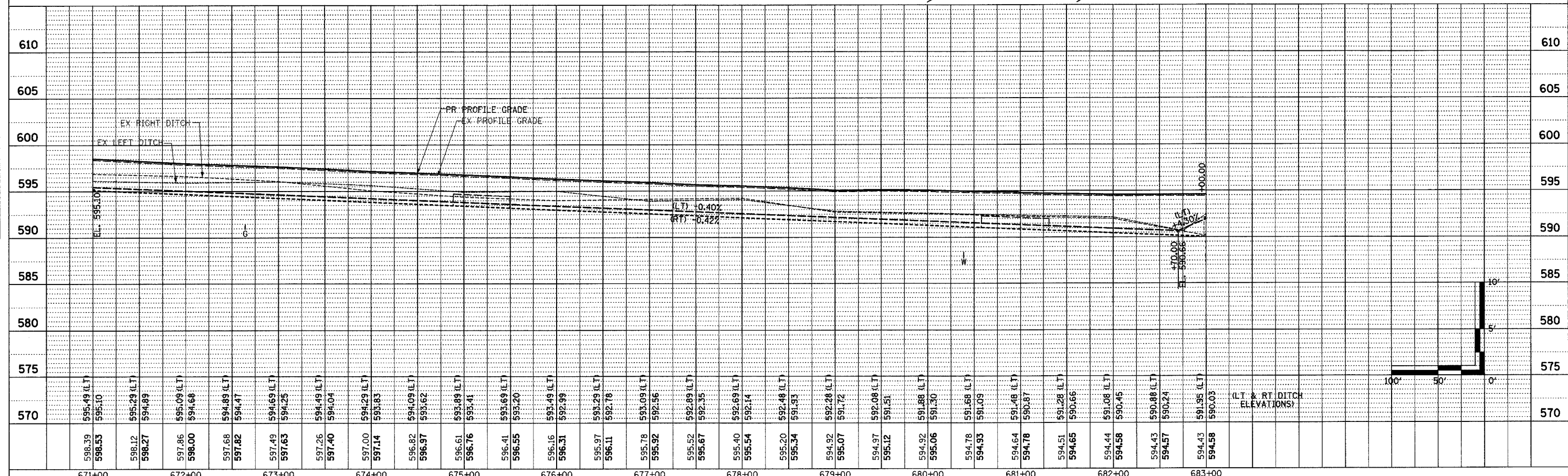
PCBC 3'x2' EXTENSIONS
 STA. 682+90.23
 LENGTH = 9' RT
 DOWNSTREAM FLOWLINE = 590.00 RT

FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS
 45' LT STA. 679+25.98 1 EACH
 50' RT STA. 679+25.98 1 EACH
 TOTAL 2 EACH

BOX CULVERT END SECTIONS CULV. NO. 15
 RT STA. 682+90.23 1 EACH

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| DATE | BY | DATE |
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| DATE | BY | DATE |
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 88 |
| STA. 683+00.00 TO STA. 689+36.68 | | | | |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| *(6,7)Y, RS-1, 6B-1, 7B-1 & 7BR-2 | | | | |

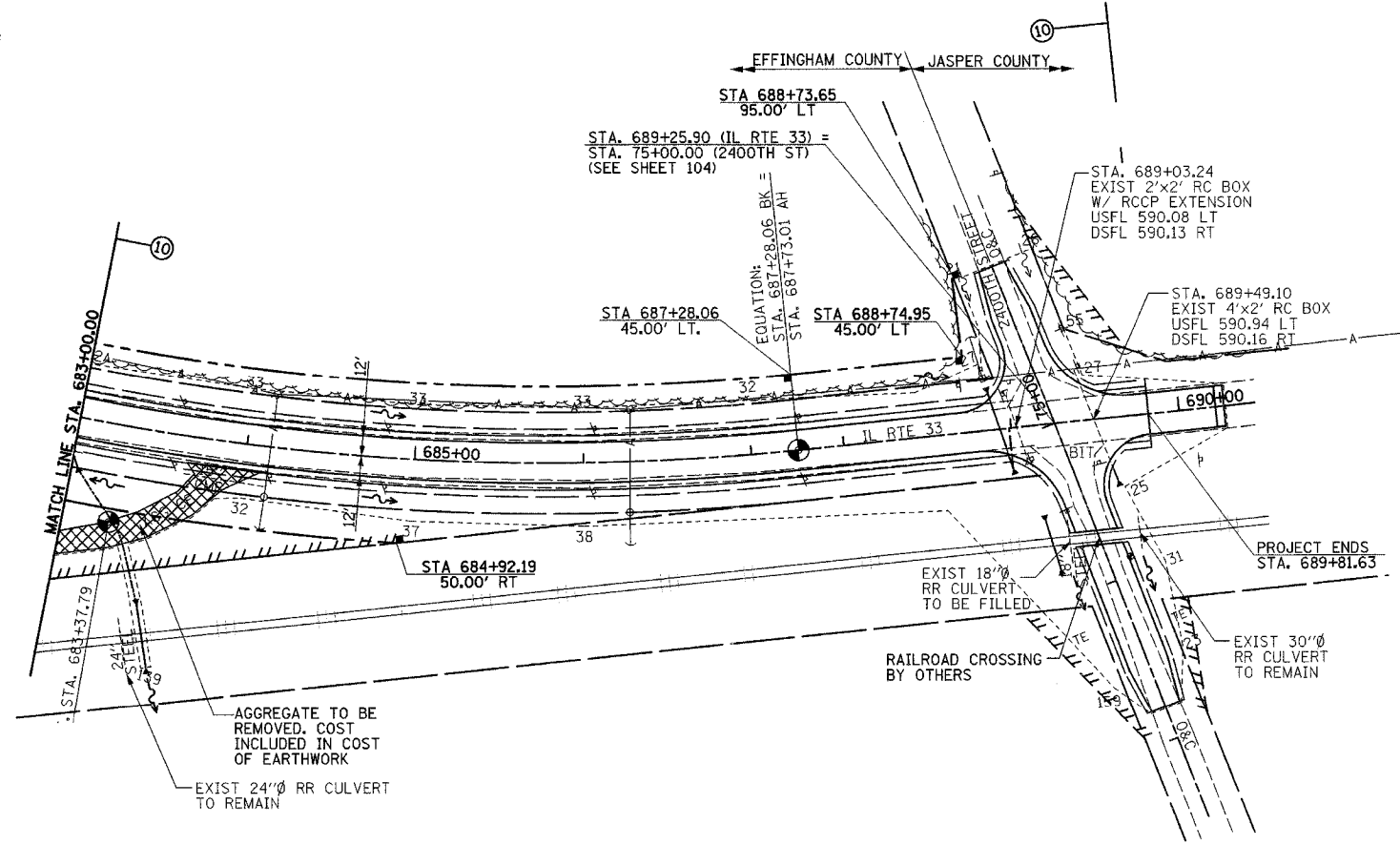


FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

- 50' LT STA. 684+92.19 1 EACH
- 45' LT STA. 687+28.06 1 EACH
- 95' LT STA. 688+73.65 1 EACH
- 45' LT STA. 688+74.95 1 EACH
- TOTAL 4 EACH

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GRADING AND SHAPING DITCHES
RT STA. 683+03.44 - STA. 683+50.42 89 LIN FT

PIPE CULVERTS, CLASS A, 24" (JACKED)
STA. 683+60.26, 122' RT
LENGTH = 40'
UPSTREAM FLOWLINE = 589.90
DOWNSTREAM FLOWLINE = 589.82

PRC FLARED END SECTIONS 24"
102' RT STA. 683+60.26 1 EACH
142' RT STA. 683+60.26 1 EACH
TOTAL 2 EACH

PIPE CULVERTS REINFORCED CONCRETE - ELLIPTICAL, TYPE I, EQUIVALENT ROUND-SIZE 30"
STA. 688+92.98
LENGTH = 46'
OFFSET = 22' LT - 24' RT
UPSTREAM FLOWLINE = 590.35
DOWNSTREAM FLOWLINE = 590.10

PRC FLARED END SECTION, ELLIPTICAL EQUIVALENT ROUND-SIZE 30"
LT & RT STA. 688+92.98 2 EACH

FILLING EXISTING CULVERTS
STA. 689+03.24 6 CU YD
RT STA. 689+27.79 2 CU YD
TOTAL 8 CU YD

FLARED END SECTION REMOVAL
LT & RT STA. 689+04.95 2 EACH

PCRC 4'x2' EXTENSIONS
STA. 689+49.10
LENGTH = 6' RT
DOWNSTREAM FLOWLINE = 590.16

BOX CULVERT END SECTIONS CULV. NO. 16
RT STA. 689+49.10 1 EACH

REMOVE CONCRETE BOX CULVERT END SECTION
RT STA. 689+49.10 1 EACH

PIPE CULVERTS, CLASS A, 18" (JACKED)
STA. 689+17.73, 72' RT
LENGTH = 29'
UPSTREAM FLOWLINE = 590.10
DOWNSTREAM FLOWLINE = 590.00

PRC FLARED END SECTIONS 18"
57' RT STA. 689+14.78 1 EACH
86' RT STA. 689+20.69 1 EACH
TOTAL 2 EACH

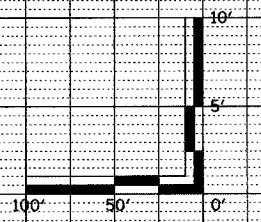
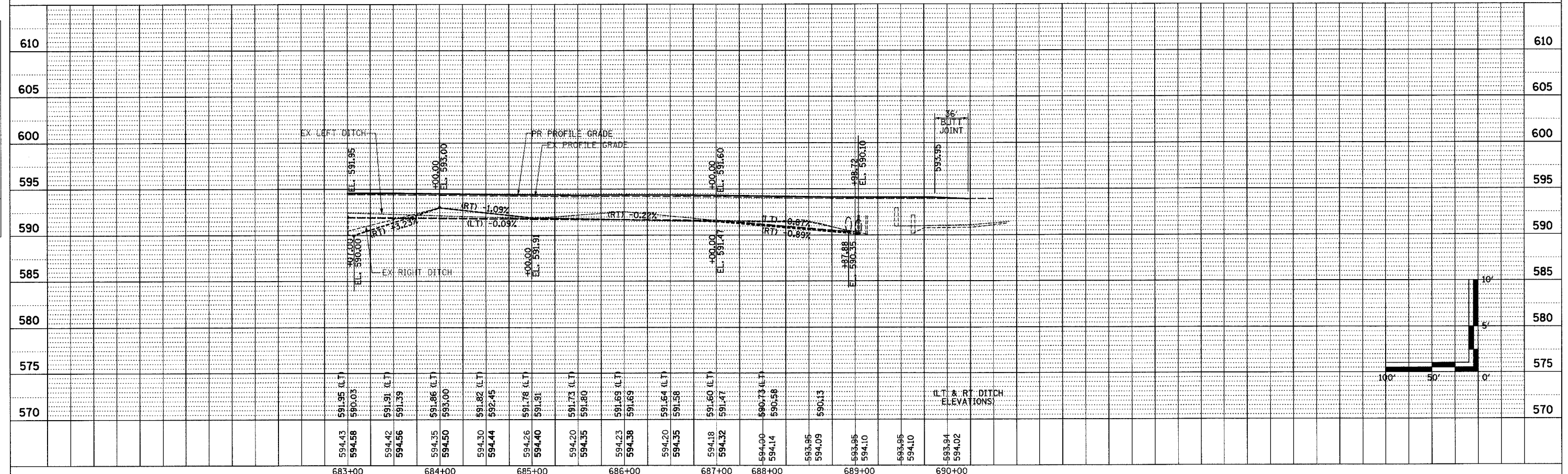
PERMANENT SURVEY MARKERS, TYPE I
P.T. STA. 687+73.01 (AH) 1 EACH

PERMANENT SURVEY MARKERS, TYPE II
P.I. STA. 683+37.79 1 EACH

BITUMINOUS SURFACE REMOVAL - BUTT JOINT
STA. 689+96.63 - STA. 690+26.63 80 SQ YD

TEMPORARY RAMP
STA. 690+21.63 - STA. 690+26.63 13 SQ YD

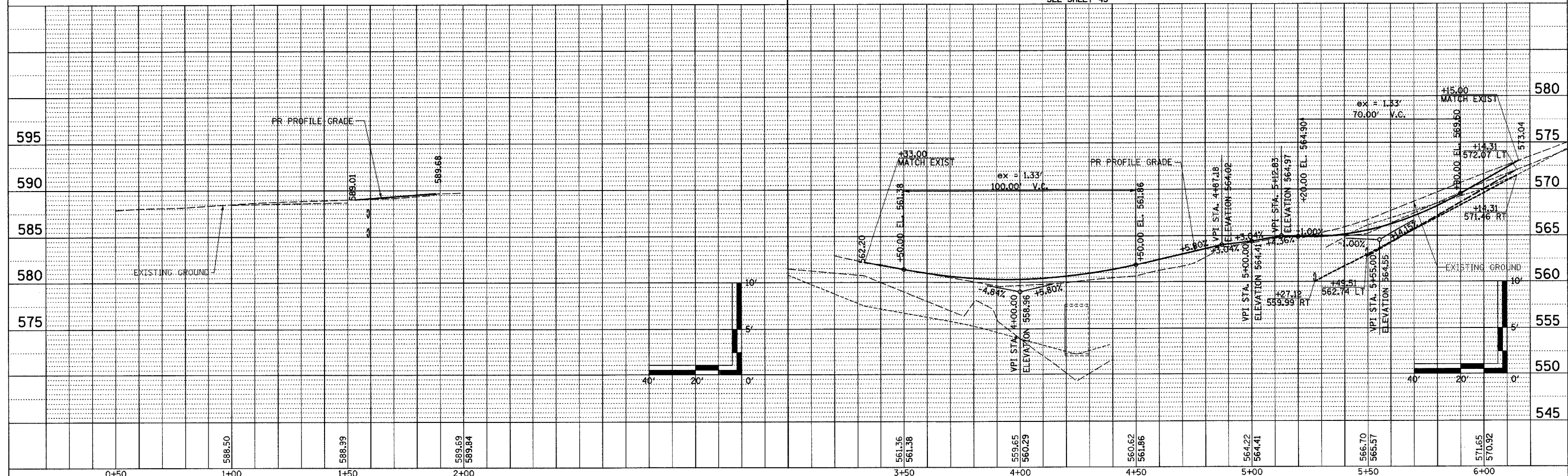
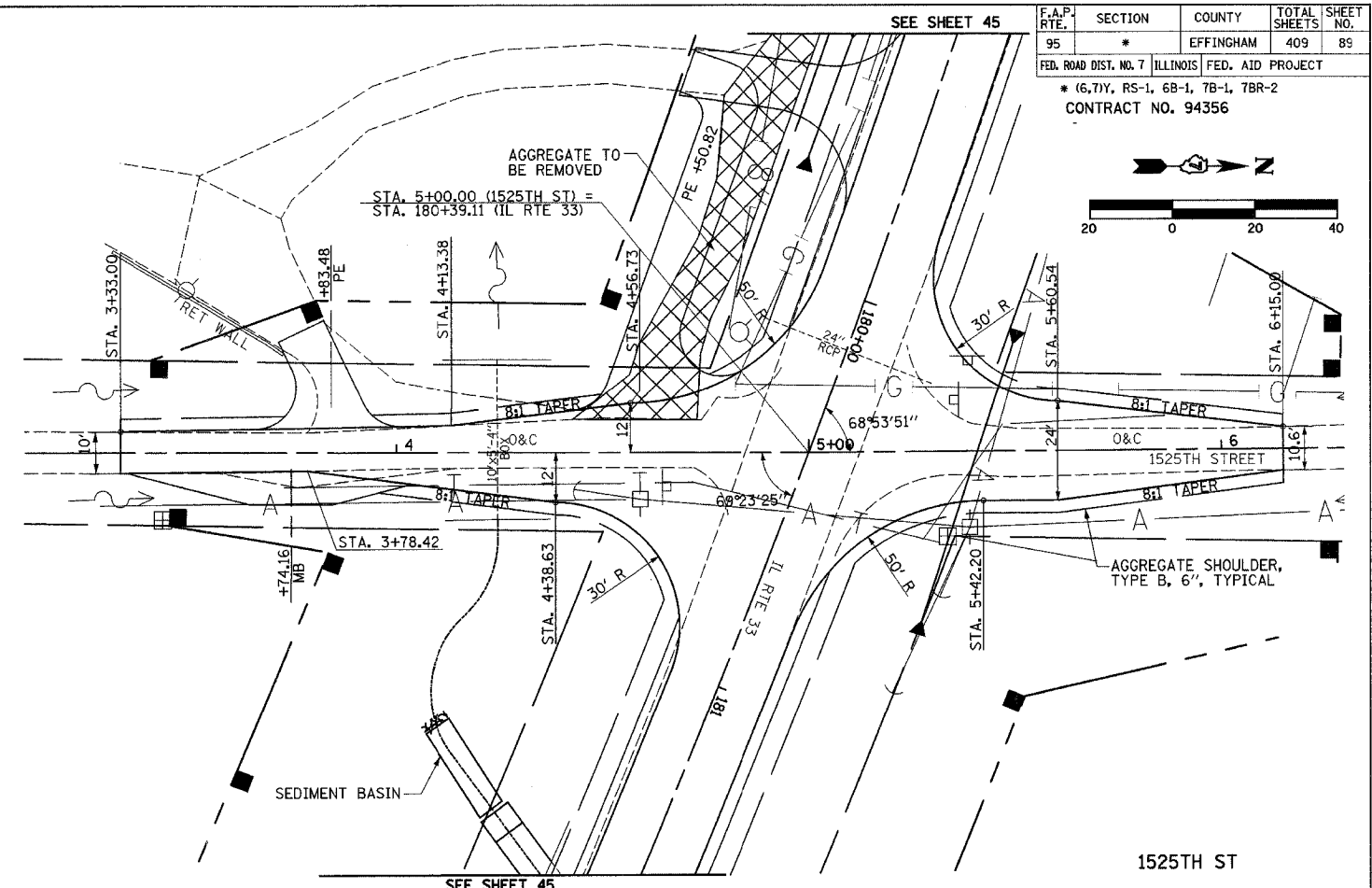
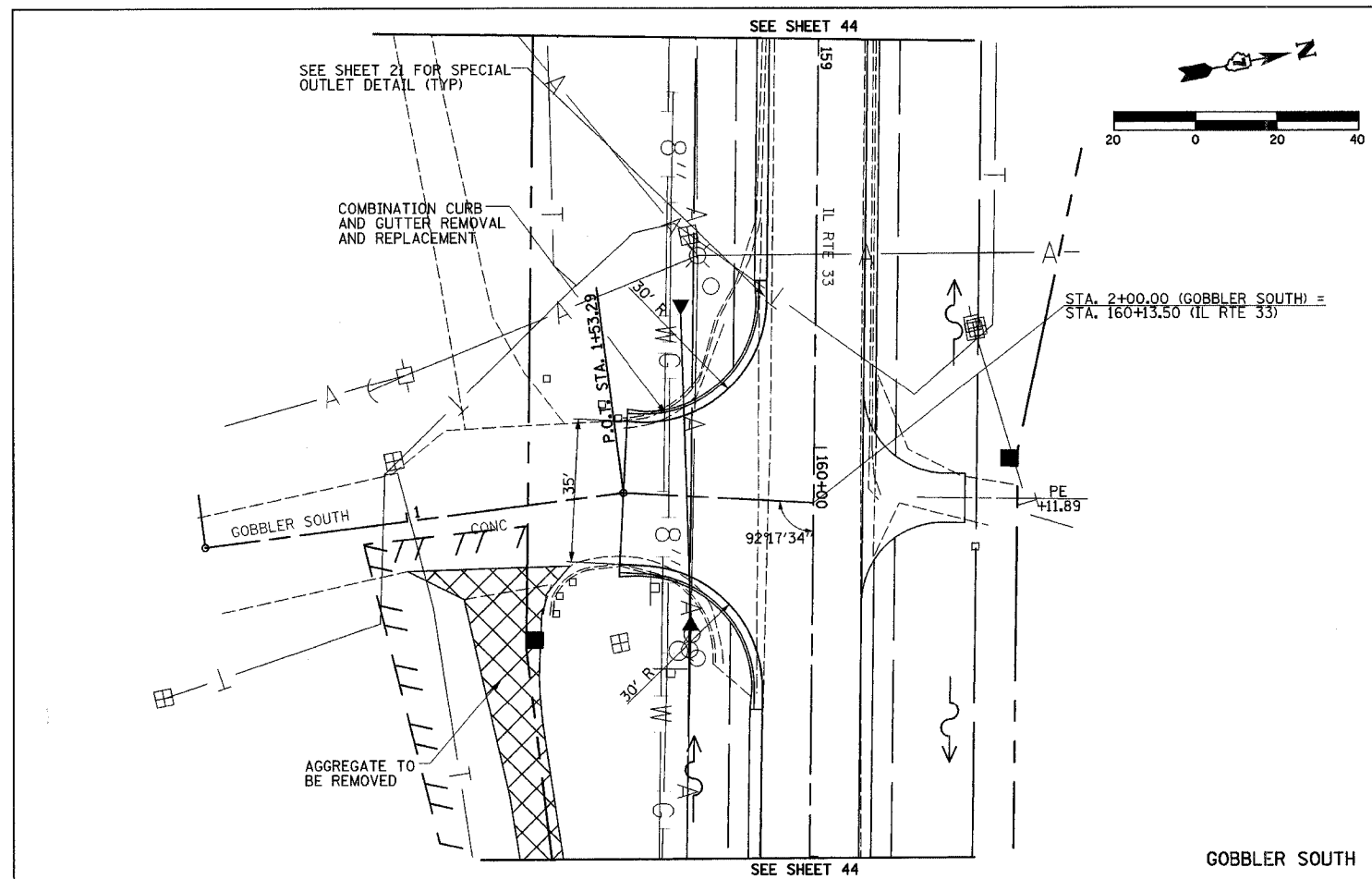
PAVEMENT REMOVAL
2400TH ST S 122 SQ YD



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| NOTE BOOK | |
| NO. | |
| STRUCTURE | |
| NOTATION | |
| CHRD | |

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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 89 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



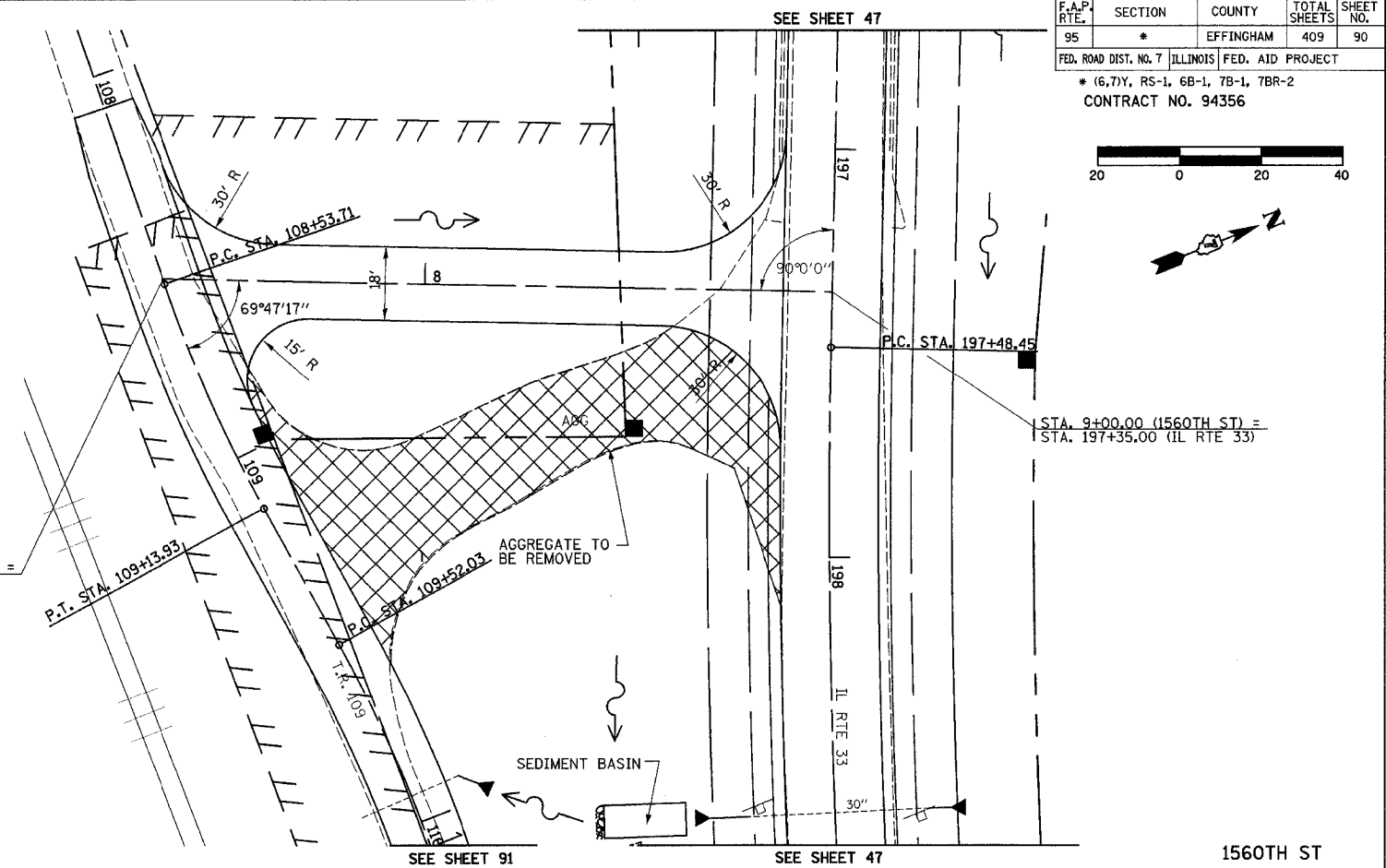
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 NOTE BOOK NO.: _____
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 BY: _____
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 GRADES CHECKED: _____
 STRUCTURE NOTATION: _____
 NOTE BOOK NO.: _____

PROP CURVE C56
 PI STA. = 108+83.89
 $\Delta = 9^\circ 51' 24''$ (LT)
 $D = 16^\circ 22' 06''$
 $R = 350.04'$
 $T = 30.18'$
 $L = 60.22'$
 $E = 1.30'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. \text{ RUN} = \text{---}$
 $P.C. \text{ STA.} = 108+53.71$
 $P.T. \text{ STA.} = 109+13.93$

PROP CURVE C57
 PI STA. = 109+88.01
 $\Delta = 11^\circ 44' 13''$ (RT)
 $D = 16^\circ 22' 13''$
 $R = 350.00'$
 $T = 35.97'$
 $L = 71.70'$
 $E = 1.84'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. \text{ RUN} = \text{---}$
 $P.C. \text{ STA.} = 109+52.03$
 $P.T. \text{ STA.} = 110+23.73$

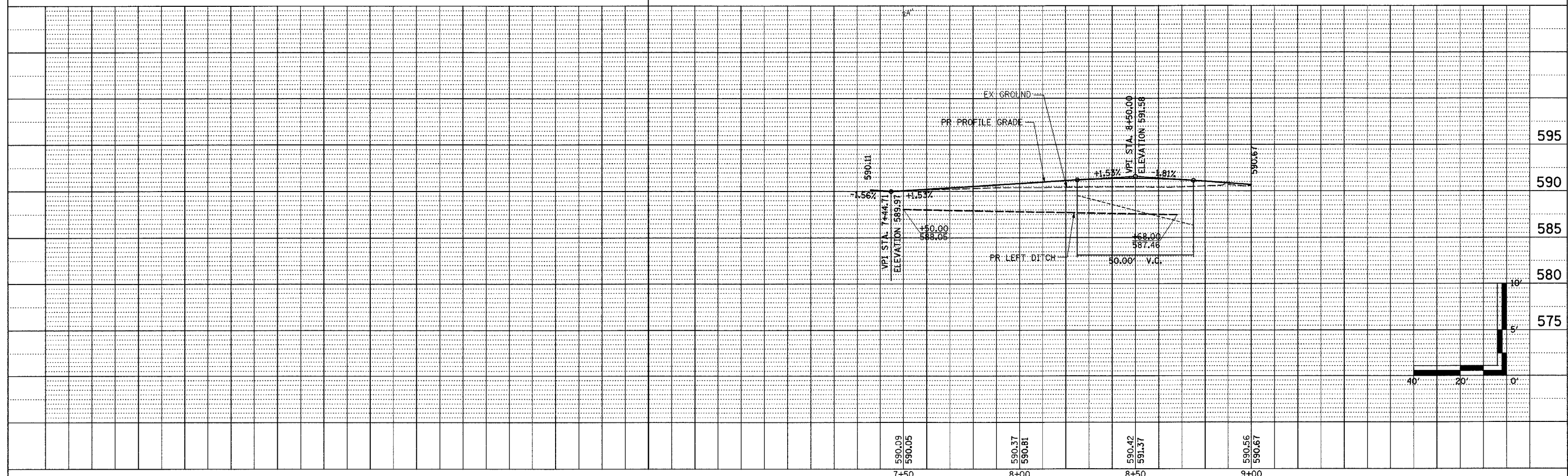
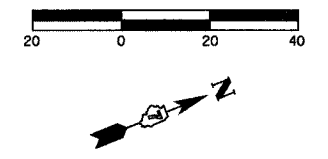
STA. 7+37.71 (1560TH ST) =
 STA. 108+52.35 (TR109)



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|-----------|--------------|-----------|
| 95 | * | EFFINGHAM | 409 | 90 |

FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT

* (6.7)Y, RS-1, 6B-1, 7B-1, 7BR-2
 CONTRACT NO. 94356



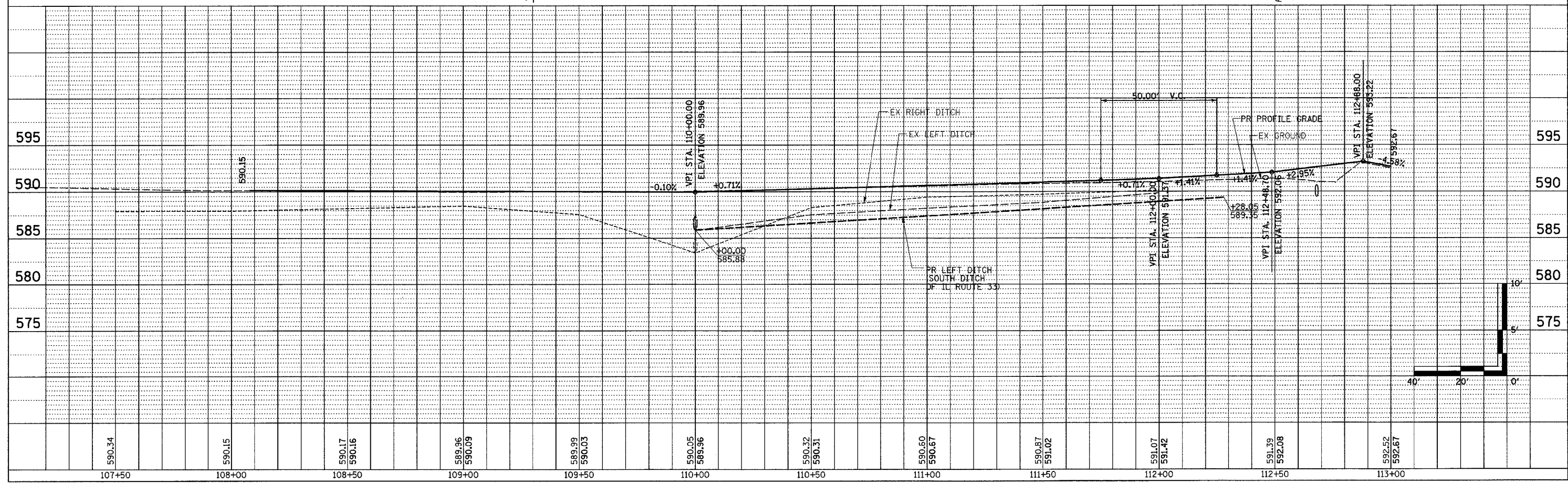
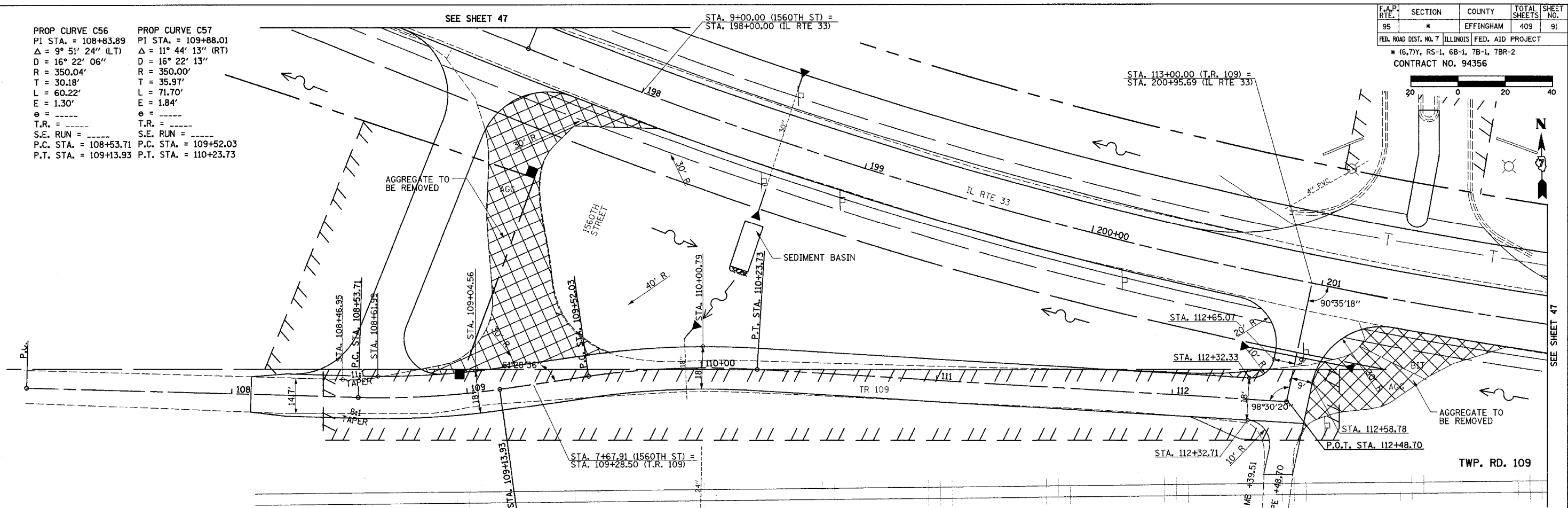
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| 95 | * | EFFINGHAM | 409 | 91 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

PROP CURVE C56
 PI STA. = 108+83.89
 $\Delta = 9^\circ 51' 24''$ (LT)
 $D = 16^\circ 22' 06''$
 $R = 350.04'$
 $T = 30.18'$
 $L = 60.22'$
 $E = 1.30'$
 $\theta = \dots$
 $T.R. = \dots$
 $S.E. RUN = \dots$
 $P.C. STA. = 108+53.71$
 $P.T. STA. = 109+13.93$

PROP CURVE C57
 PI STA. = 109+88.01
 $\Delta = 11^\circ 44' 13''$ (RT)
 $D = 16^\circ 22' 13''$
 $R = 350.00'$
 $T = 35.97'$
 $L = 71.70'$
 $E = 1.84'$
 $\theta = \dots$
 $T.R. = \dots$
 $S.E. RUN = \dots$
 $P.C. STA. = 109+52.03$
 $P.T. STA. = 110+23.73$

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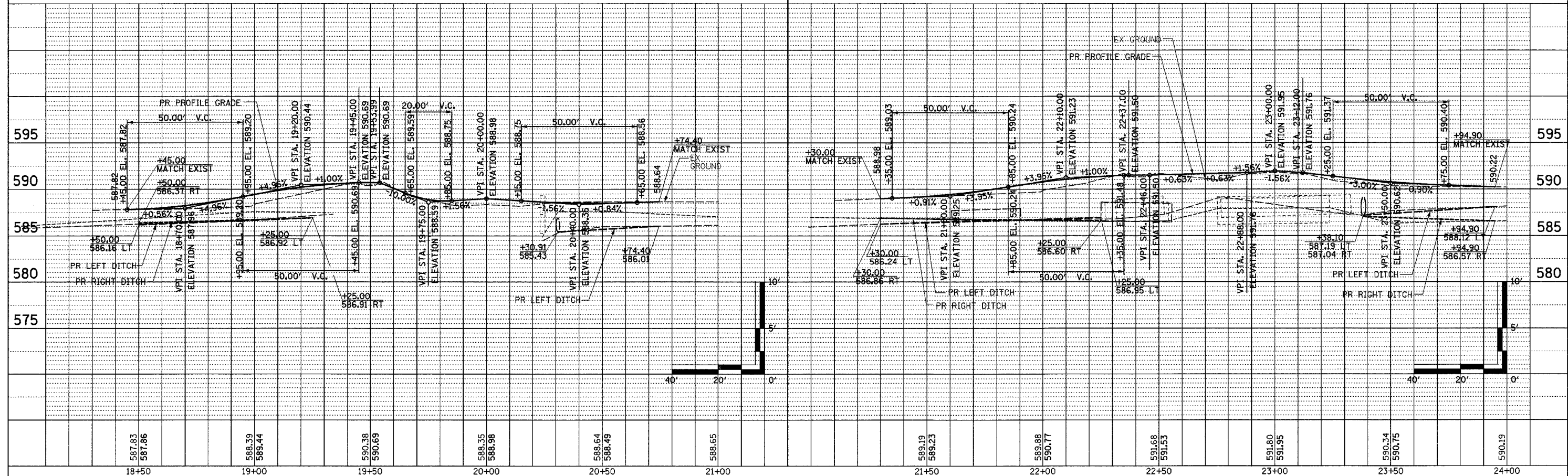
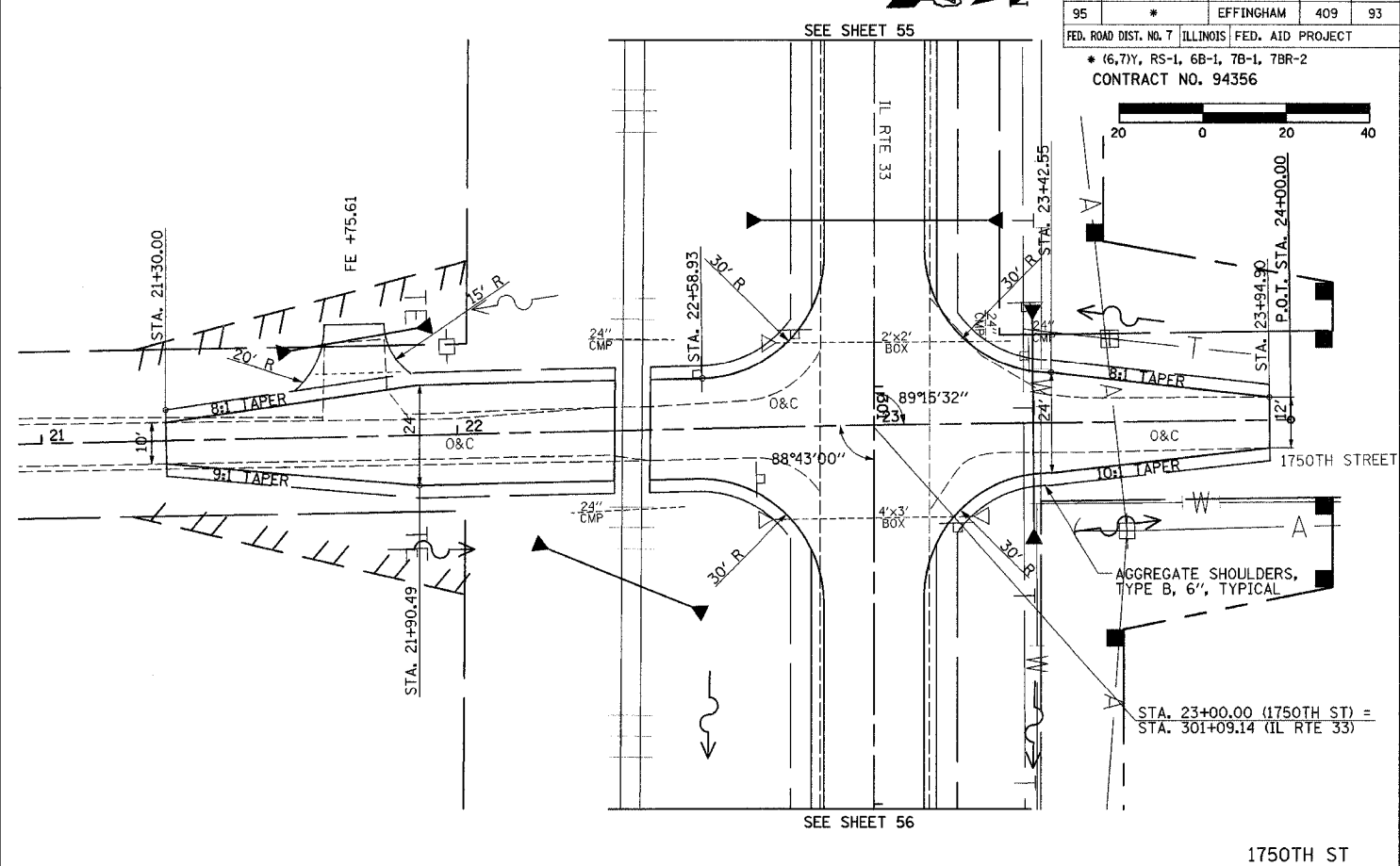
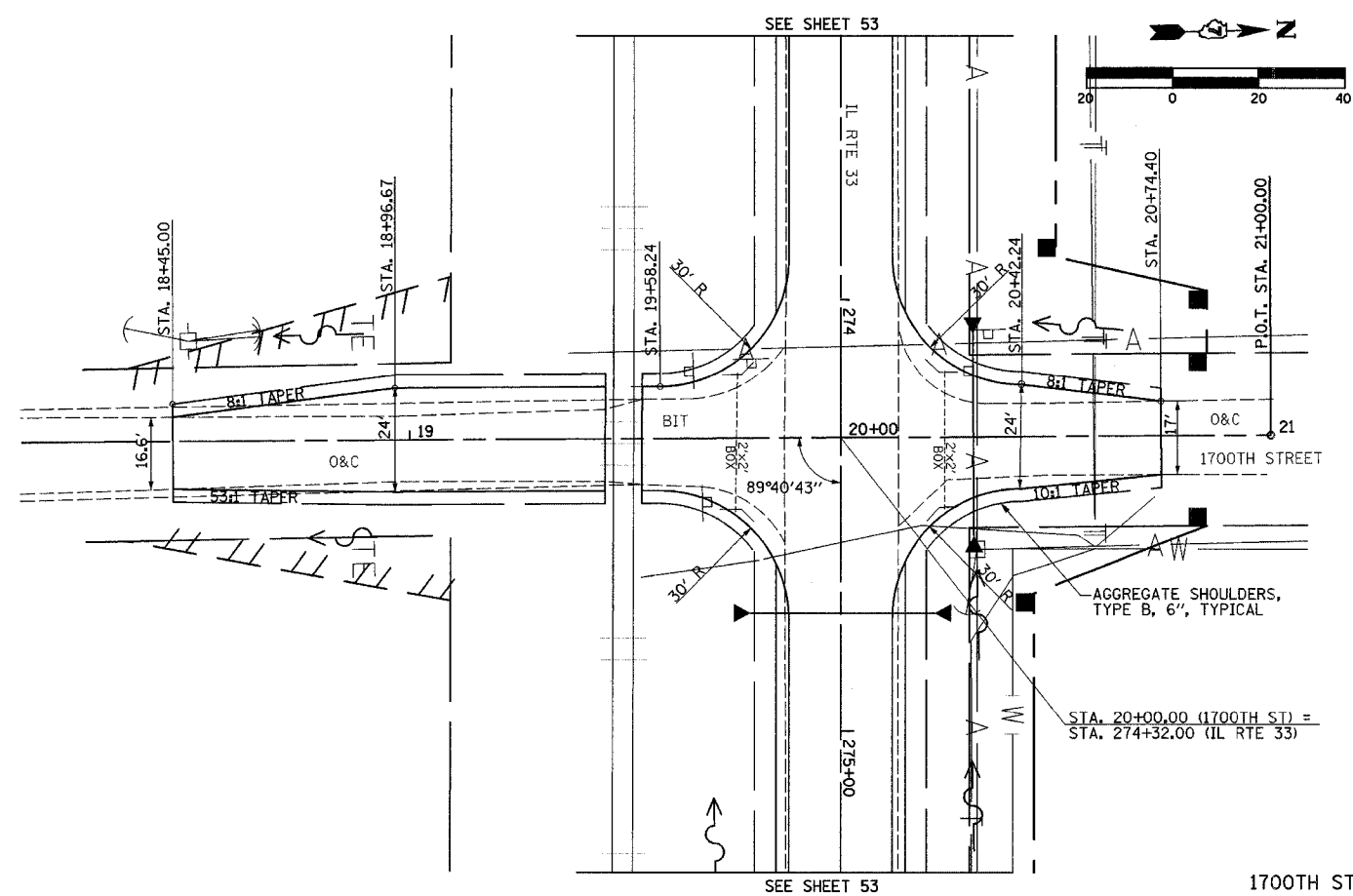
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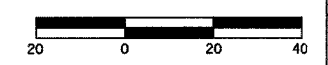
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 93 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |

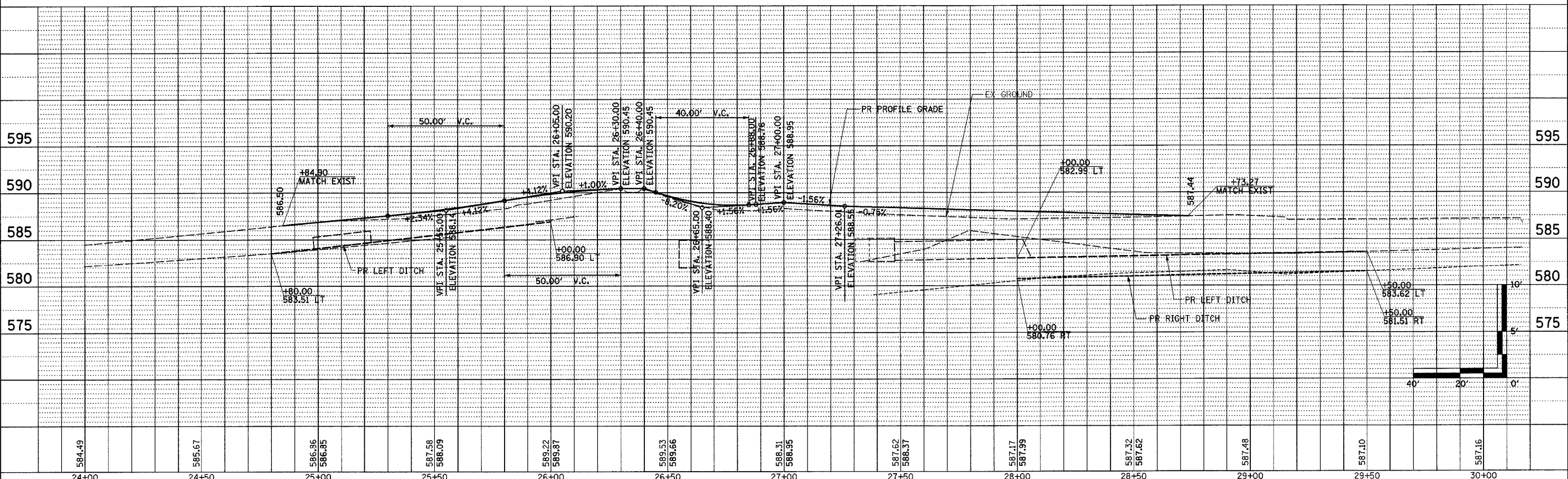
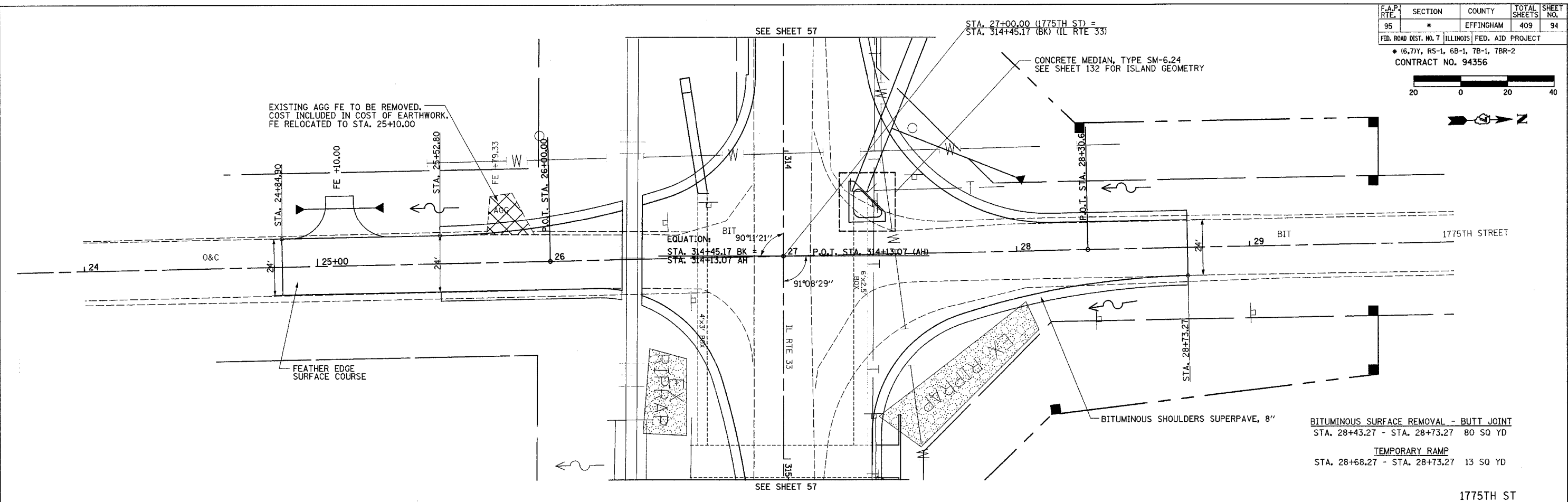


| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| 95 | * | EFFINGHAM | 409 | 94 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



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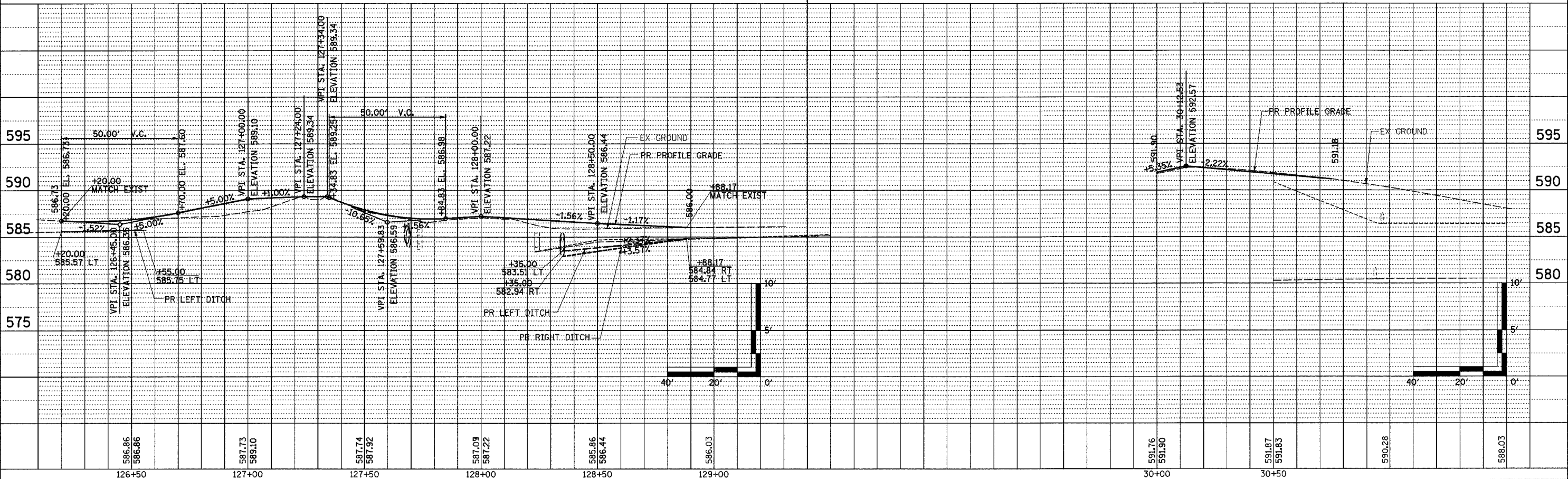
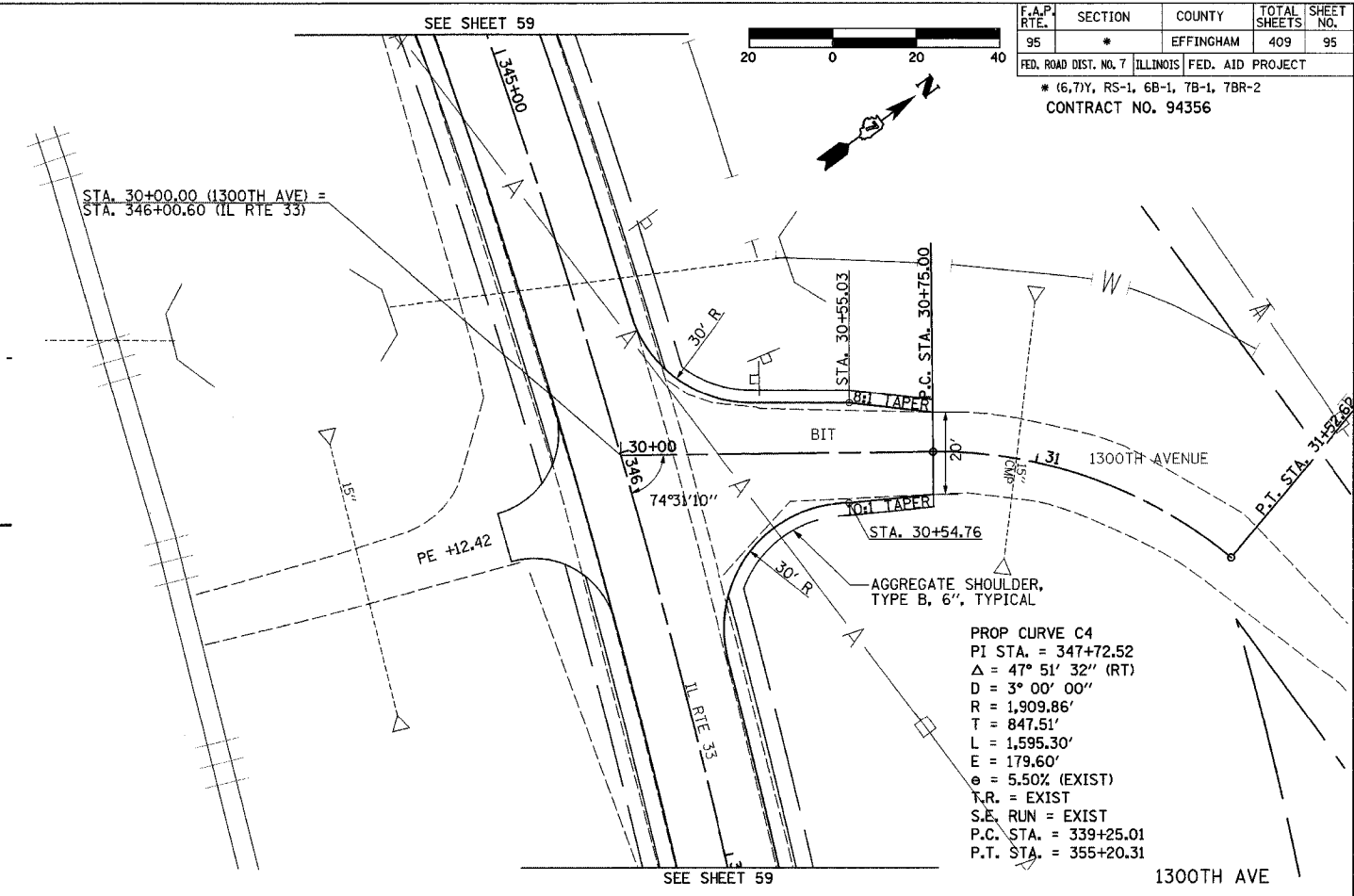
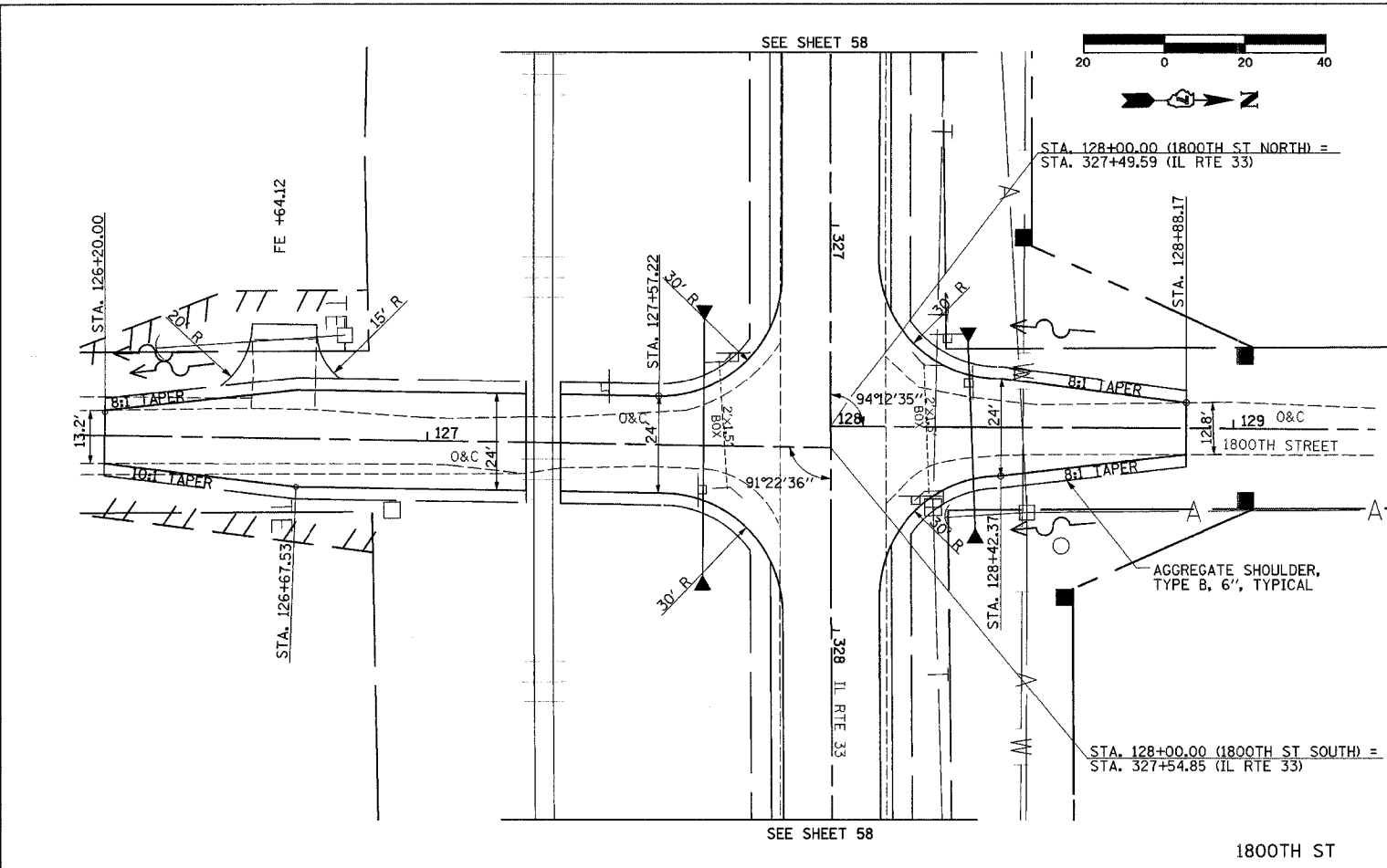
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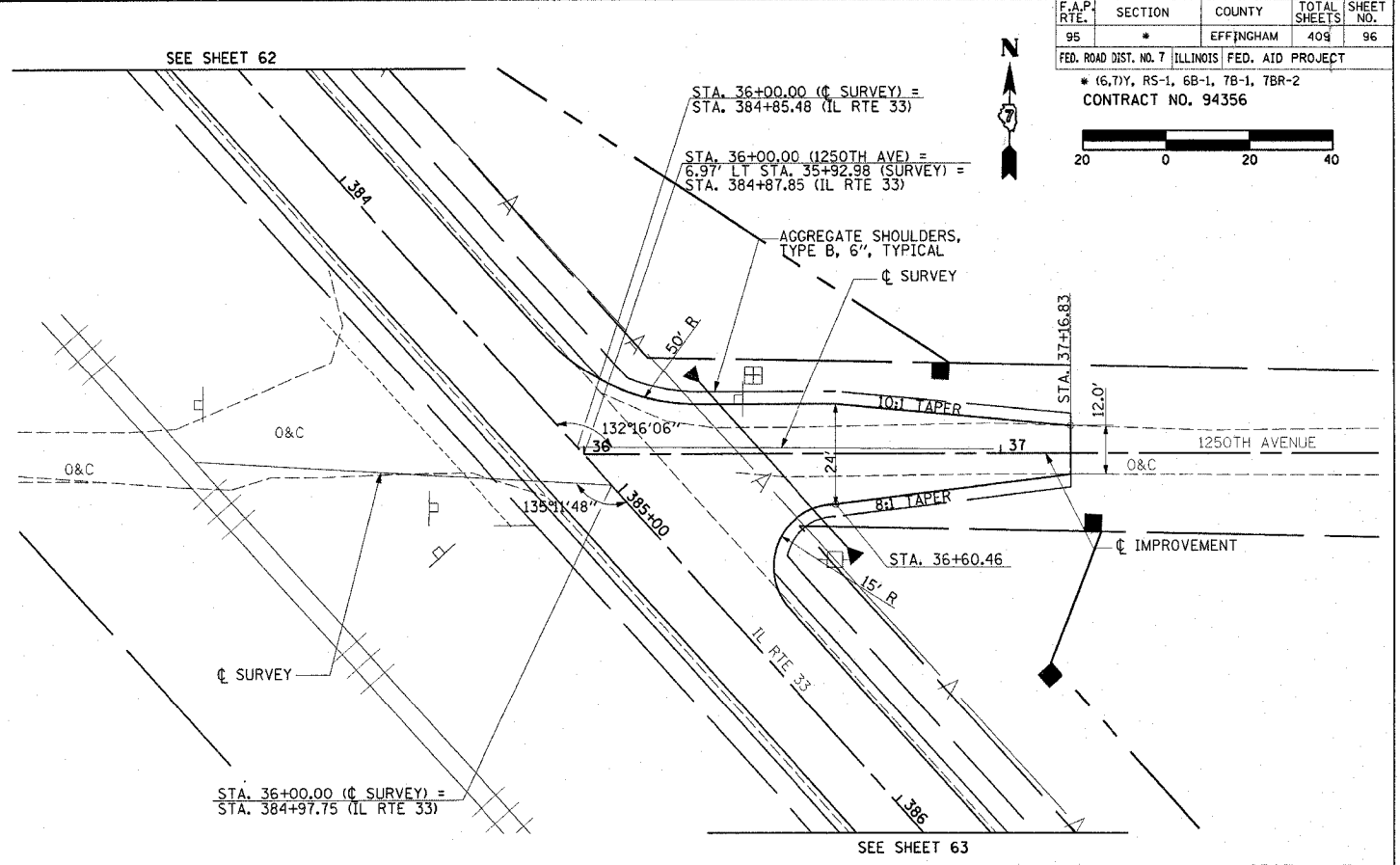
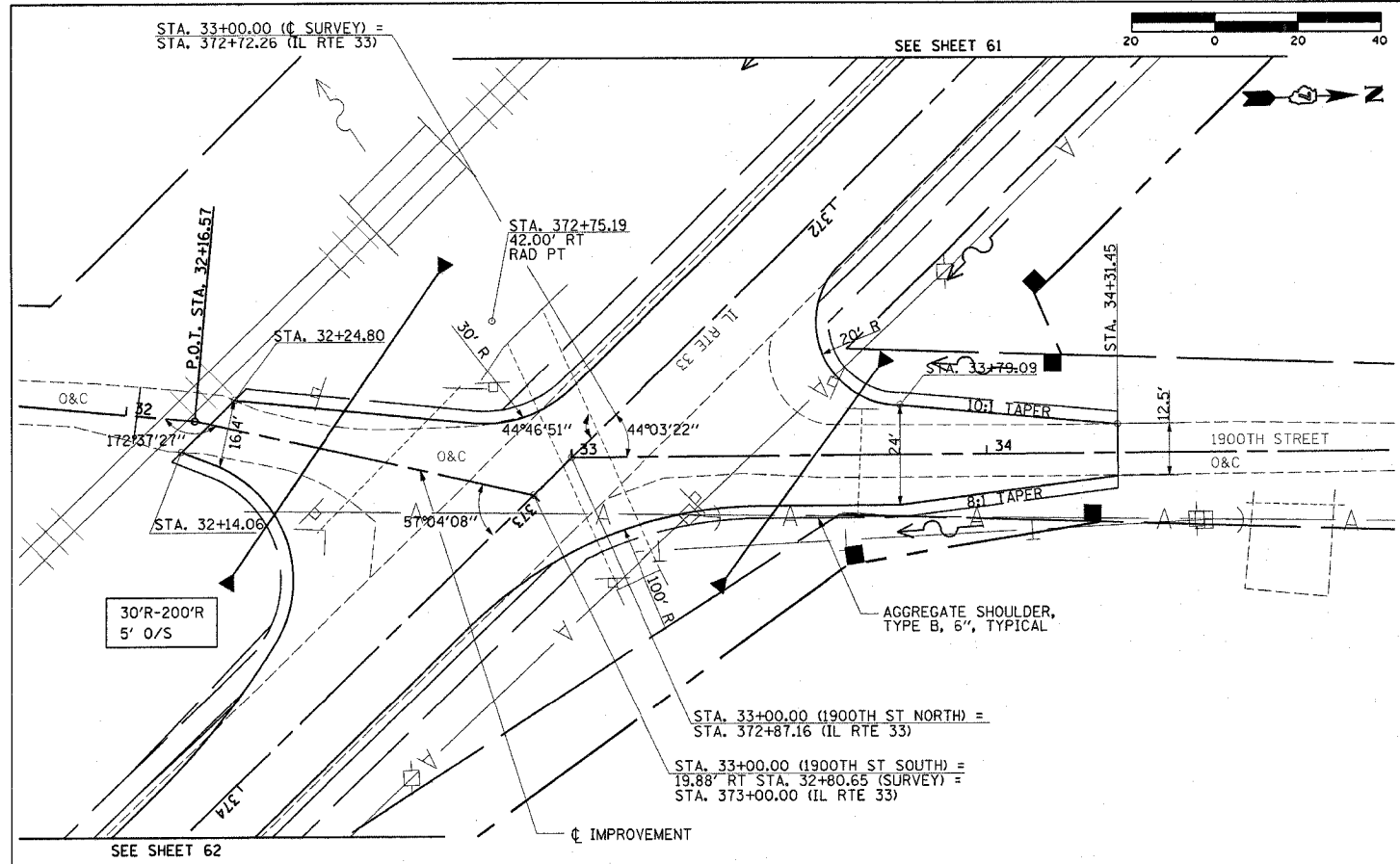
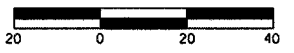
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 95 | * | EFFINGHAM | 409 | 95 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | | | | |
| * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 | | | | |
| CONTRACT NO. 94356 | | | | |



| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| 95 | * | EFFINGHAM | 409 | 96 |

FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT
 * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2
 CONTRACT NO. 94356

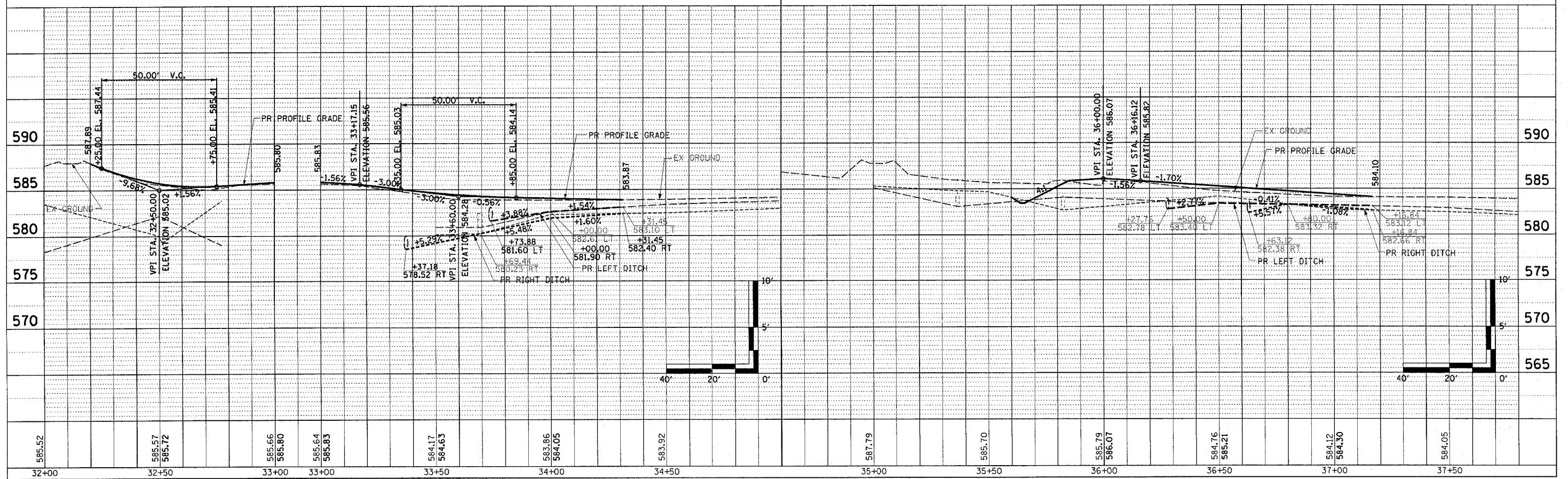


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 2. 10/1/01
 3. 10/1/01
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 6. 10/1/01
 7. 10/1/01
 8. 10/1/01
 9. 10/1/01
 10. 10/1/01

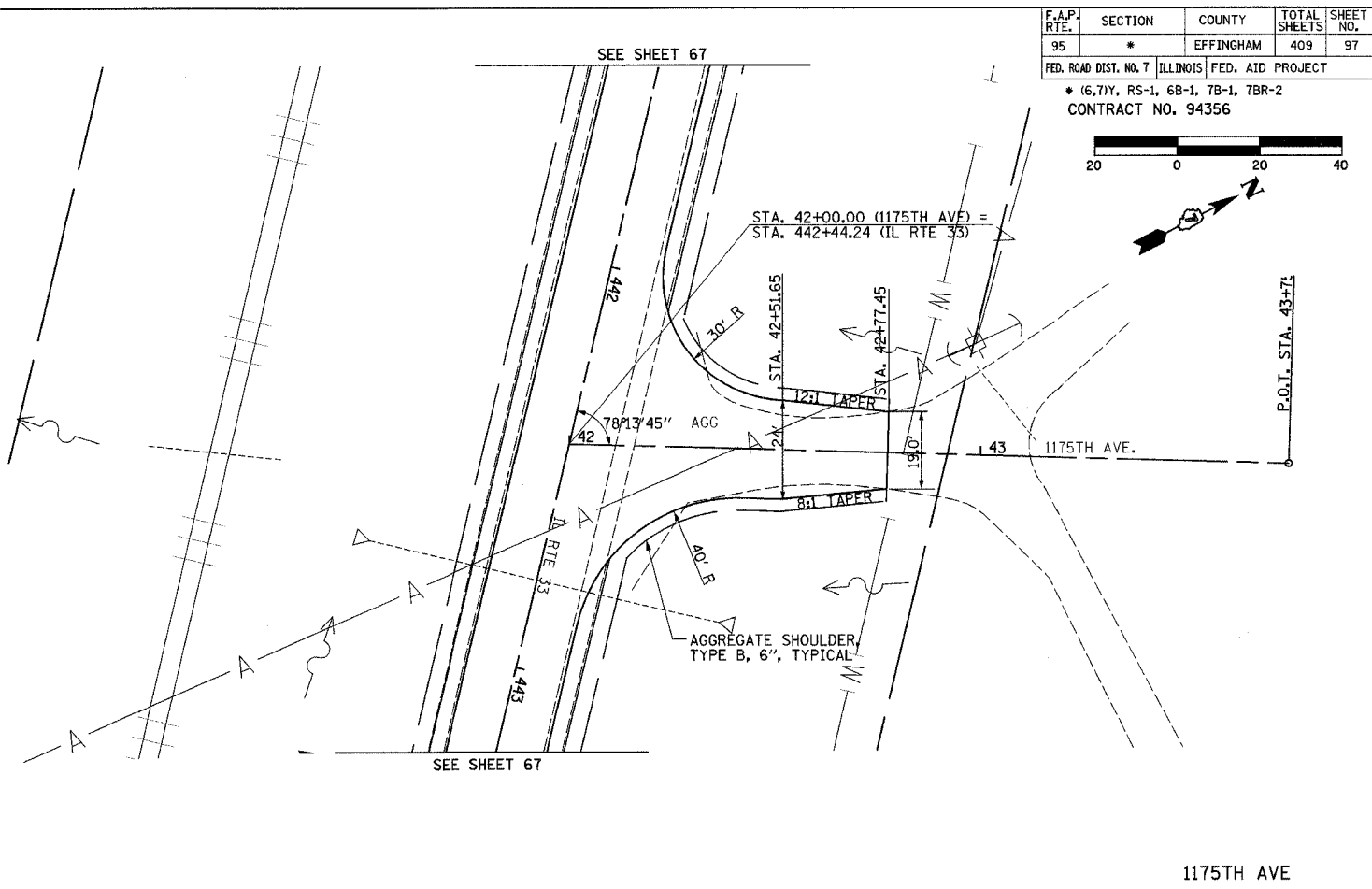
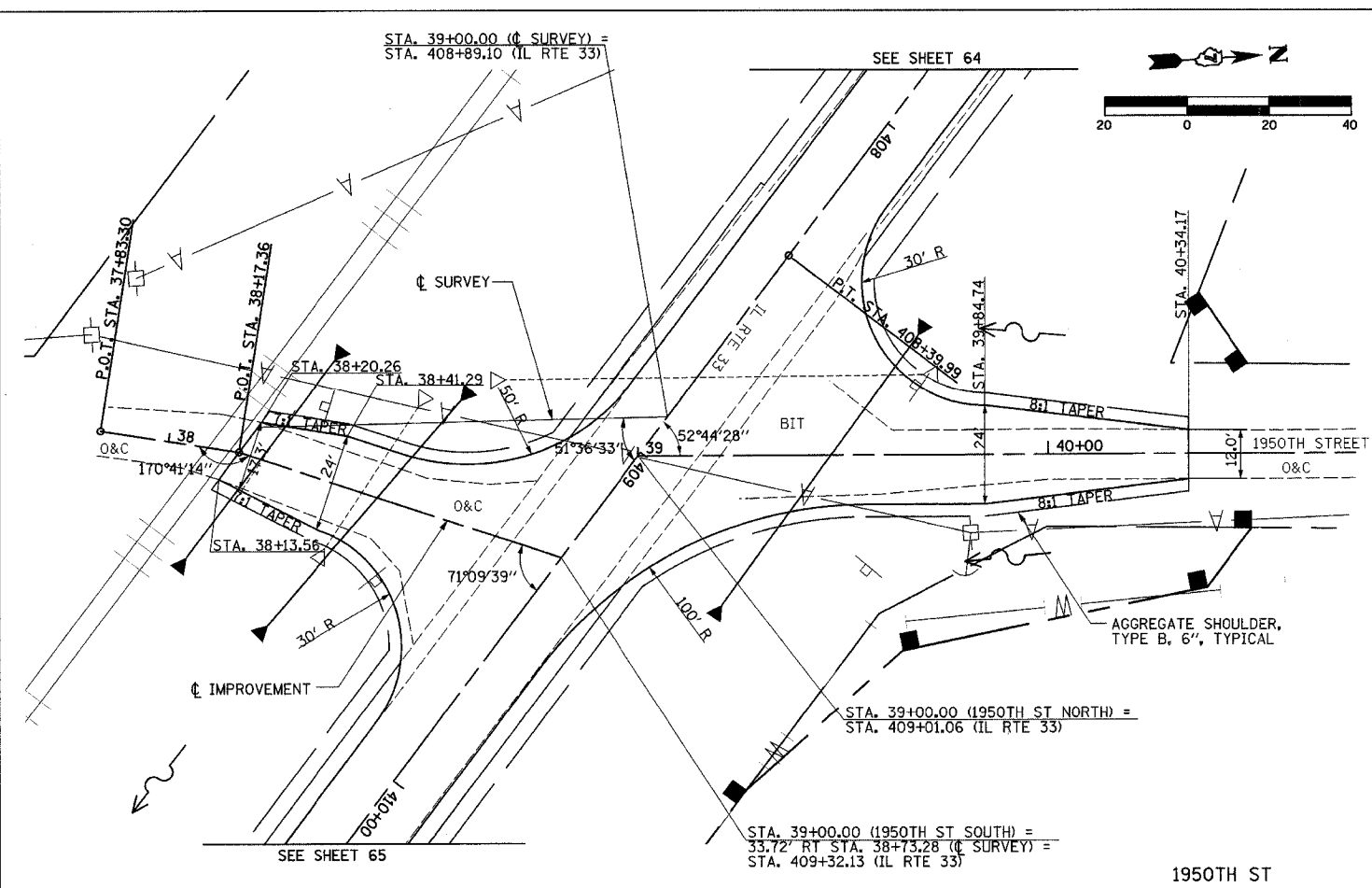
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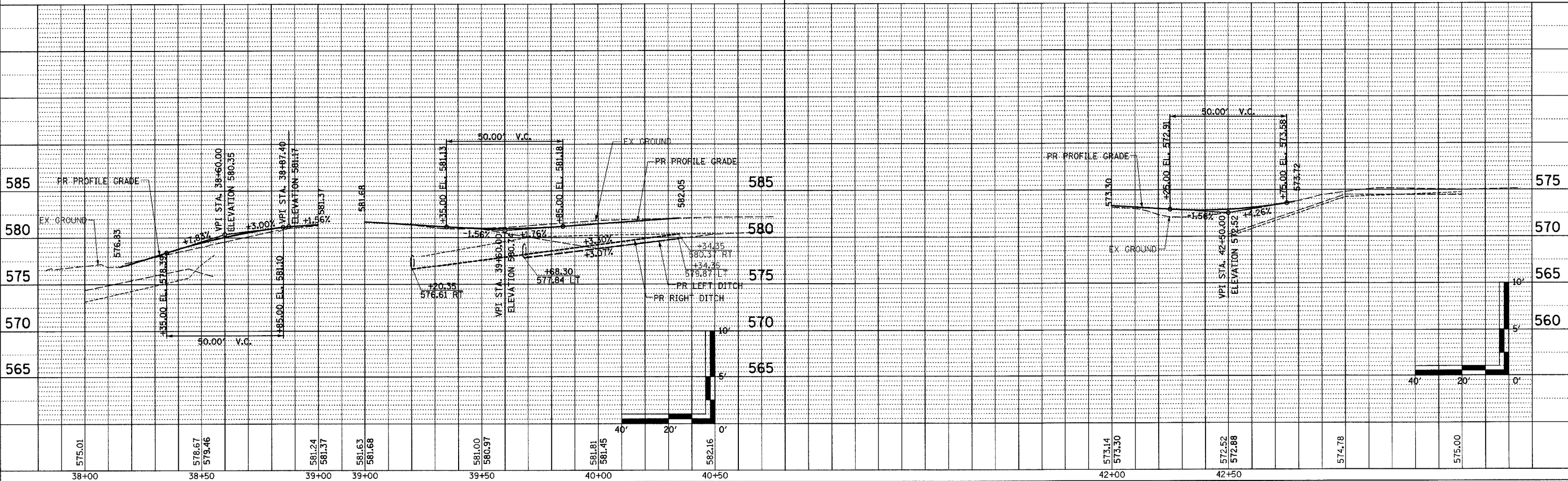


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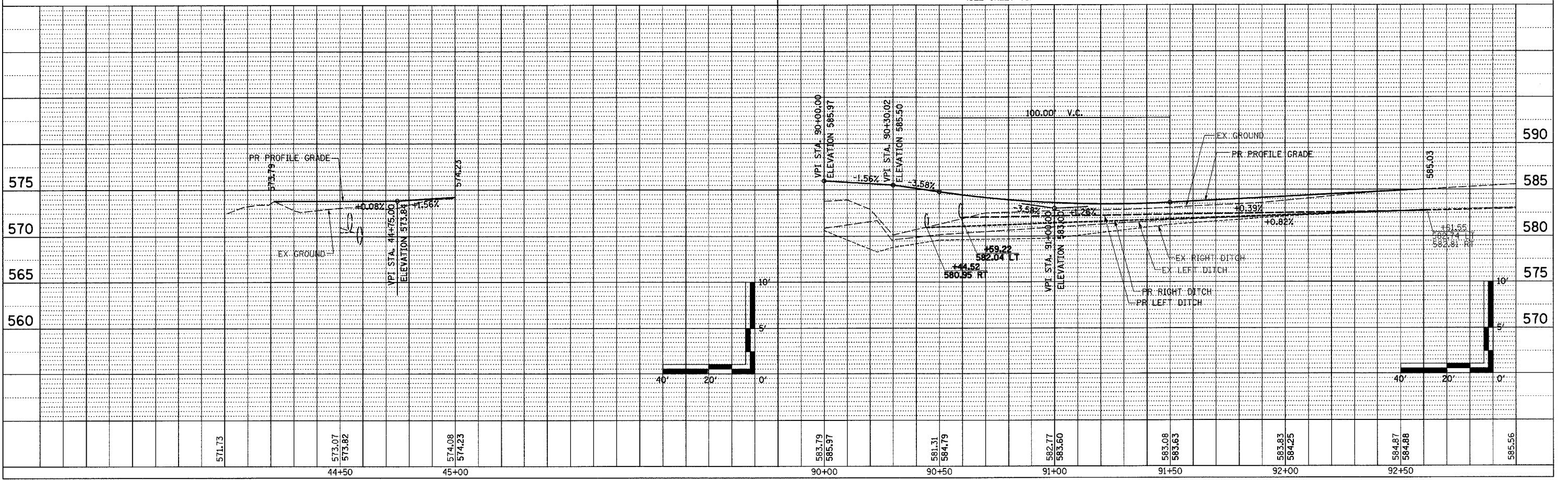
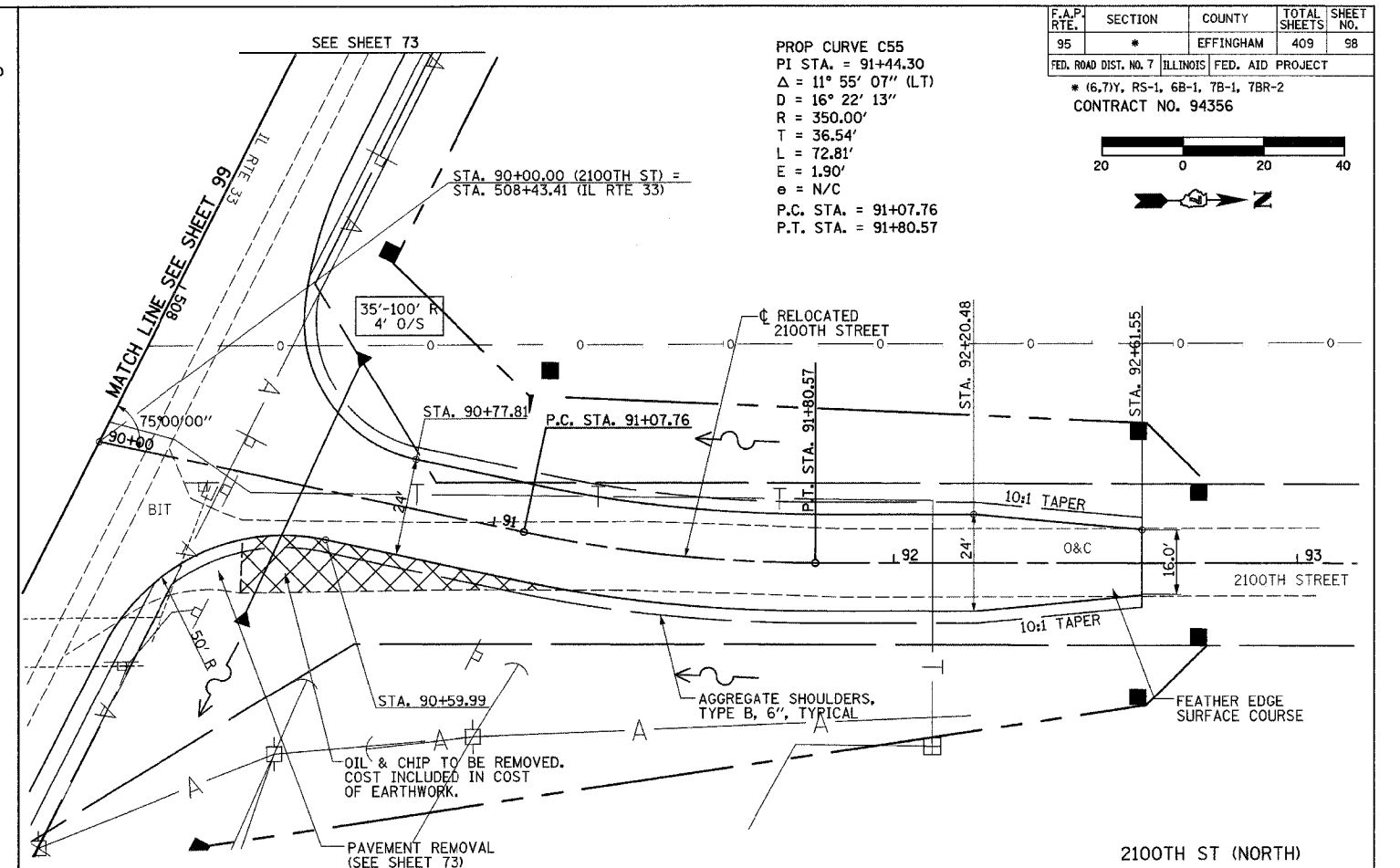
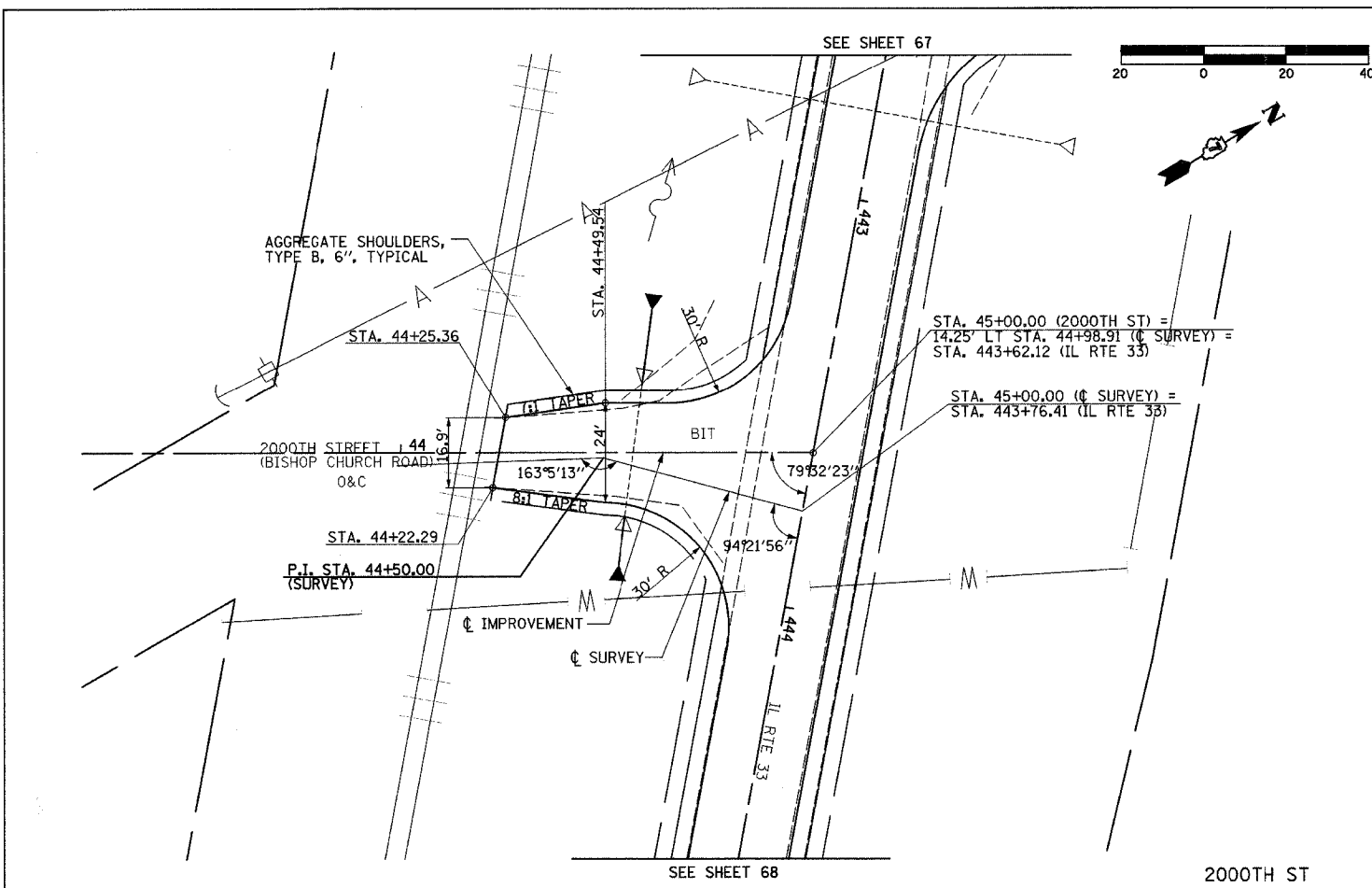


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| F.A.P. SECTION COUNTY TOTAL SHEETS SHEET NO. | 95 * EFFINGHAM 409 97 |
| FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT | * (6,7)Y, RS-1, 6B-1, 7B-1, 7BR-2 |
| CONTRACT NO. 94356 | |

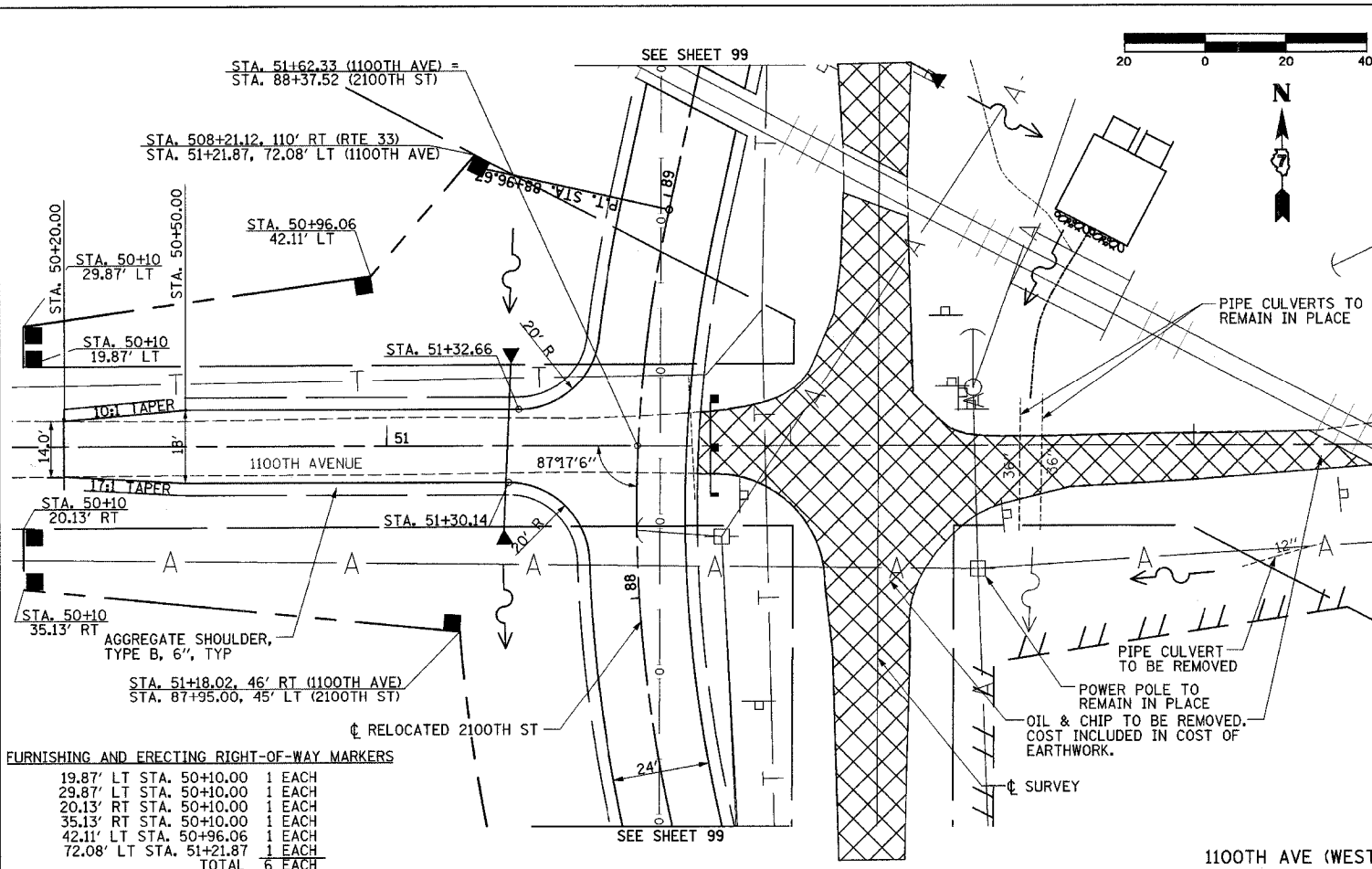


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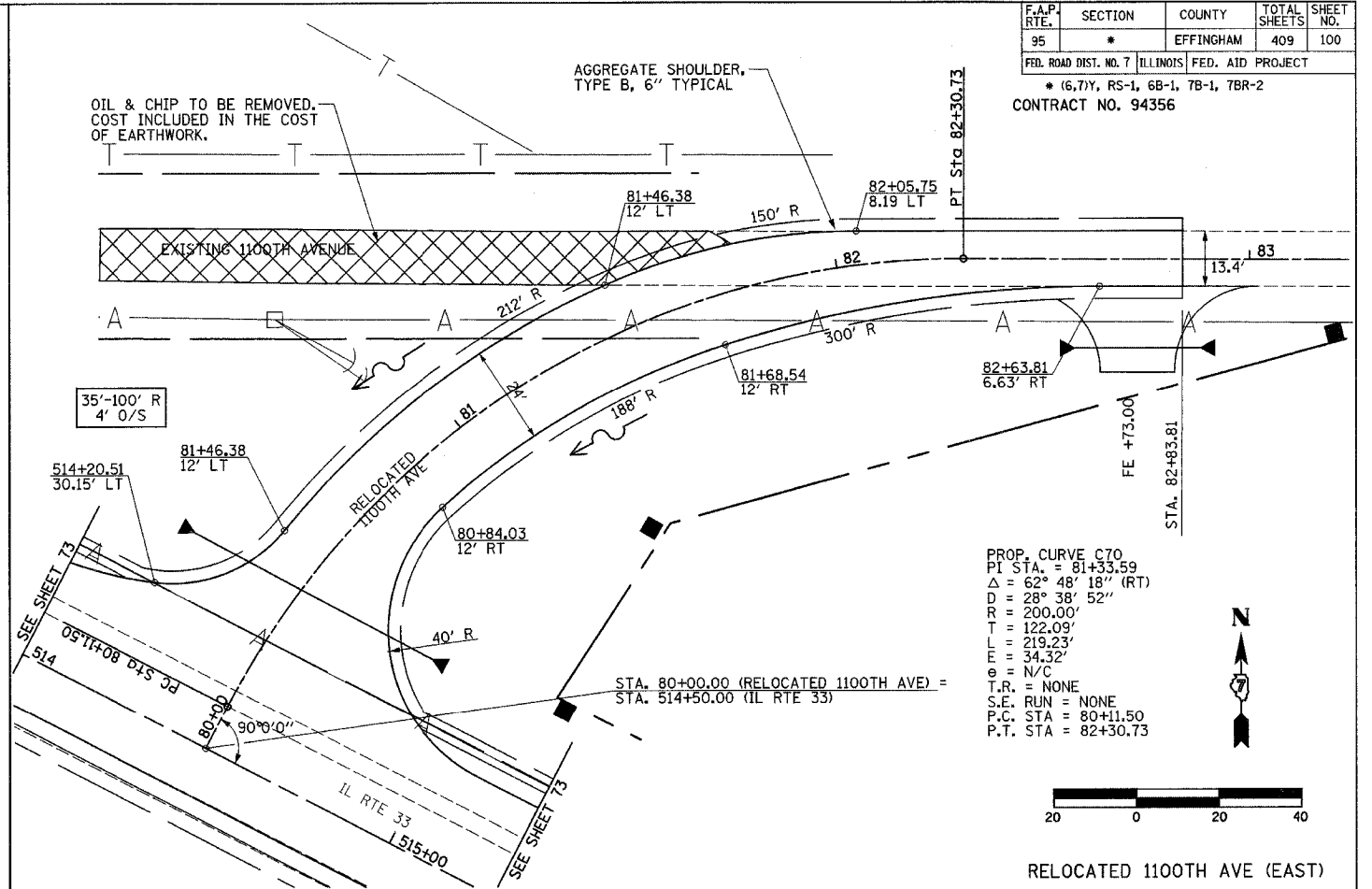


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 NOTE BOOK NO.: _____
 STRIPPLING INDICATING CARD NO.: _____



FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS

| | | |
|-----------|---------------|--------|
| 19.87' LT | STA. 50+10.00 | 1 EACH |
| 29.87' LT | STA. 50+10.00 | 1 EACH |
| 20.13' RT | STA. 50+10.00 | 1 EACH |
| 35.13' RT | STA. 50+10.00 | 1 EACH |
| 42.11' LT | STA. 50+96.06 | 1 EACH |
| 72.08' LT | STA. 51+21.87 | 1 EACH |
| TOTAL | | 6 EACH |



PROP. CURVE C70
 PI STA. = 81+33.59
 $\Delta = 62^\circ 48' 18''$ (RT)
 $D = 28^\circ 38' 52''$
 $R = 200.00'$
 $T = 122.09'$
 $L = 219.23'$
 $E = 34.32'$
 $\theta = N/C$
 $T.R. = NONE$
 $S.E. RUN = NONE$
 $P.C. STA. = 80+11.50$
 $P.T. STA. = 82+30.73$

