

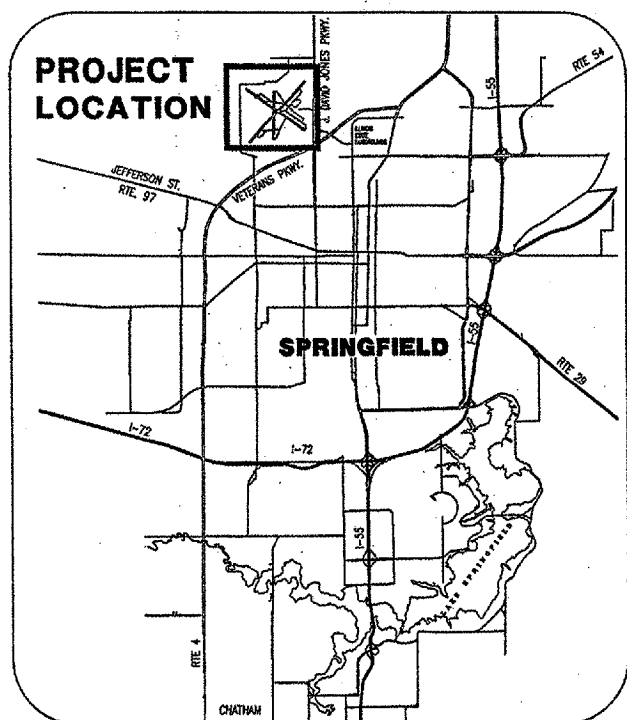
CONSTRUCTION PLANS FOR ABRAHAM LINCOLN CAPITAL AIRPORT

SPRINGFIELD AIRPORT AUTHORITY
SPRINGFIELD, ILLINOIS

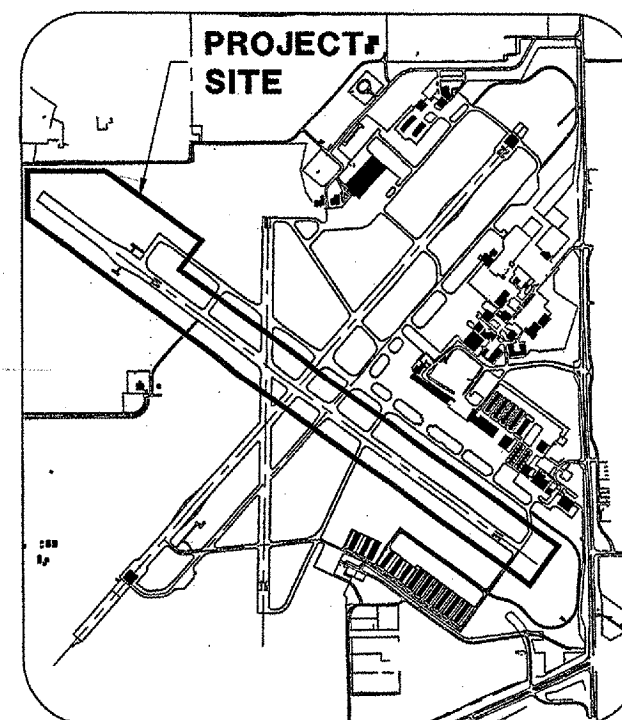
ILLINOIS PROJECT SPI - 3488
A. I. P. PROJECT 3-17-0096-42

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS; EXTEND TAXIWAY B

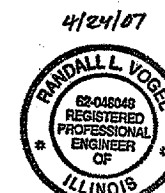
APRIL 24, 2007



LOCATION MAP



SITE PLAN



Randall L. Vogel
EXR. 11/30/07

DESIGN INFORMATION

GEOMETRIC CRITERIA

AIRPLANE DESIGN GROUP III
AIRCRAFT APPROACH CATEGORY C

PAVEMENT DESIGN CRITERIA

AIRCRAFT DUAL WHEEL GEAR
DEPARTURE WEIGHT = 101,500 LBS.
1200 EQUIV. ANNUAL DEPARTURES

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 16 NORTH
RANGE: 5 WEST OF THE 4TH P.M.
SECTION: 8, 16, & 17
COUNTY: SANGAMON
CIVIL TOWNSHIP: CAPITAL



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

APPROVED *Mark Zeffano*
EXECUTIVE DIRECTOR OF AVIATION
DATE 4/24/07



CMT

CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS

■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY *Randall L. Vogel*
DATE 4/24/07

CMT JOB NUMBER 05035-03-00

SUMMARY OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL PROJECT QUANTITY	TXY B EXTENSION	RWY 13/31 LIGHTING, MARKING & SIGNAGE	RWY 31 BLAST PAD	ATCT CONTROL, BEACON, WINDCONES
AR101510	AIRPORT ROTATING BEACON	EACH	1	0	0	0	1
AR107960	RELOCATE WIND CONE	EACH	2	0	0	0	2
AR108158	1/2" #8 5 KV UG CABLE IN UD	L.F.	34050	5250	27600	1200	0
AR800250	2-1/2" #8 5 KV UG CABLE IN UD	L.F.	1300	0	1300	0	0
AR800288	REIL HOMERUN	L.F.	2350	0	2350	0	0
AR800289	PAPI HOMERUN	L.F.	1350	0	1350	0	0
AR800290	MALSR THRESHOLD BAR CIRCUIT	L.F.	350	0	0	350	0
AR109210	VAULT MODIFICATIONS	L.S.	1	0	1	0	0
AR109342	20 KW REGULATOR, STYLE 2	EACH	2	0	2	0	0
AR109620	LIGHTING CONTROL SYSTEM	L.S.	1	0	0	0	1
AR800291	REMOVE L-821 SYSTEM	L.S.	1	0	0	0	1
AR110014	4" DIRECTIONAL BORE	L.F.	160	0	160	0	0
AR110212	2" STEEL DUCT, DIRECT BURY	L.F.	130	0	0	130	0
AR110504	4-WAY CONCRETE ENCASED DUCT	L.F.	290	80	0	210	0
AR800293	DUCT MARKER-IN PAVEMENT	EACH	62	6	54	2	0
AR125415	MILT-BASE MOUNTED	EACH	62	62	0	0	0
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	2	2	0	0	0
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	1	1	0	0	0
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	4	1	3	0	0
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	3	0	3	0	0
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	2	2	0	0	0
AR125515	HIRL, BASE MOUNTED	EACH	64	0	64	0	0
AR125525	HIRL, INPAVEMENT	EACH	6	0	6	0	0
AR125550	HI THRESHOLD LIGHT BASE MTD	EACH	24	0	24	0	0
AR125560	RUNWAY DISTANCE REMAINING SIGN	EACH	13	0	13	0	0
AR125565	SPLICE CAN	EACH	9	0	9	0	0
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	57	6	51	0	0
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	54	10	44	0	0
AR125903	REMOVE INPAVEMENT LIGHT	EACH	5	0	5	0	0
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	7	5	2	0	0
AR125905	REMOVE RWY DISTANCE REMAIN SIGN	EACH	6	0	6	0	0
AR125906	REMOVE SPLICE CAN	EACH	4	1	3	0	0
AR125923	REPLACE INPAVEMENT LIGHT	EACH	6	0	6	0	0
AR125967	RELOCATE REILS	PAIR	1	0	1	0	0
AR125968	RELOCATE PAPI	EACH	1	0	1	0	0
AR800294	REPLACE SIGN FACE	EACH	44	0	44	0	0
AR800295	RUNWAY THRESHOLD BAR	EACH	2	0	2	0	0
AR800296	ADJUST MALSR THRESHOLD BAR	L.S.	1	0	0	1	0
AR800297	REPLACE ISOLATION TRANSFORMER	EACH	6	0	6	0	0
AR800298	LIGHT GROUND ROD	EACH	184	68	116	0	0
AR800833	PORTABLE CLOSED RUNWAY MARKER	EACH	2	0	2	0	0
AR800345	SURFACE SENSOR SYSTEM UPGRADE	L.S.	1	0	0	0	1
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	0	1	0	0
AR151450	CLEARING AND GRUBBING	ACRE	2.5	2.5	0	0	0
AR800287	FOUNDATION REMOVAL	EACH	4	0	0	4	0
AR152455	EMBANKMENT IN PLACE	C.Y.	24000	24000	0	0	0
AR152460	TOPSOIL STRIPPING	C.Y.	8850	8850	0	0	0
AR155540	BY-PRODUCT LIME	TON	185	185	0	0	0
AR155608	SOIL PROCESSING-8"	S.Y.	11500	11500	0	0	0
AR156510	SILT FENCE	L.F.	4270	4270	0	0	0
AR156520	INLET PROTECTION	EACH	5	5	0	0	0
AR156540	RIPRAP	S.Y.	110	110	0	0	0
AR201610	BITUMINOUS BASE COURSE	TON	9715	9050	0	665	0
AR201630	BITUMINOUS BASE TEST SECTION	EACH	1	1	0	0	0
AR209604	CRUSHED AGG. BASE COURSE - 4"	S.Y.	16160	11500	0	4660	0
AR401610	BITUMINOUS SURFACE COURSE	TON	2825	2425	0	400	0
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1	1	0	0	0
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	210	210	0	0	0
AR401665	BITUMINOUS PAVEMENT SAWING	L.F.	675	450	0	225	0
AR602510	BITUMINOUS PRIME COAT	GAL.	6470	4800	0	1870	0
AR603510	BITUMINOUS TACK COAT	GAL.	875	525	0	350	0
AR620510	PAVEMENT MARKING	S.F.	171870	7185	161385	3300	0
AR620900	PAVEMENT MARKING REMOVAL	S.F.	44200	0	44200	0	0
AR701524	24" RCP, CLASS IV	L.F.	493	493	0	0	0
AR701542	42" RCP, CLASS IV	L.F.	427	427	0	0	0
AR701560	60" RCP, CLASS IV	L.F.	311	311	0	0	0
AR701900	REMOVE PIPE	L.F.	440	440	0	0	0
AR705524	4" PERFORATED UNDERDRAIN W/SOCK	L.F.	3165	3165	0	0	0
AR705544	4" NON PERFORATED UNDERDRAIN	L.F.	160	160	0	0	0
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EACH	3	3	0	0	0
AR705640	UNDERDRAIN CLEANOUT	EACH	7	7	0	0	0
AR705900	REMOVE UNDERDRAIN	L.F.	175	175	0	0	0
AR705945	ADJUST COLLECTION STRUCTURE	EACH	1	0	0	1	0
AR751415	INLET-SPECIAL	EACH	2	2	0	0	0
AR751550	MANHOLE 5'	EACH	1	1	0	0	0
AR751560	MANHOLE 6'	EACH	2	2	0	0	0
AR751900	REMOVE INLET	EACH	1	1	0	0	0
AR752460	PRECAST REINFORCED CONC. FES 60"	EACH	1	1	0	0	0
AR752900	REMOVE END SECTION	EACH	1	1	0	0	0
AR901510	SEEDING	ACRE	19	17	0	2	0
AR904510	SODDING	S.Y.	3585	3155	0	430	0
AR908510	MULCHING	ACRE	19	17	0	2	0
AR908520	EXCELSIOR BLANKET	S.Y.	4370	4370	0	0	0

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CA003
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REVISIONS

NUMBER	BY	DATE

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 AT FULL SCALE (34X22).

SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 INDEX TO SHEETS
 &
 SUMMARY OF QUANTITIES

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DESIGN BY: RLV
 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03
 ILL. PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 02 OF 79 SHEETS

GROUND CONTROL FREQUENCY - 121.90
 AIR CONTROL FREQUENCY - 124.30
 MAXIMUM HEIGHT OF EQUIPMENT - 25'

GENERAL NOTES

CA003

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REVISIONS		
NUMBER	BY	DATE

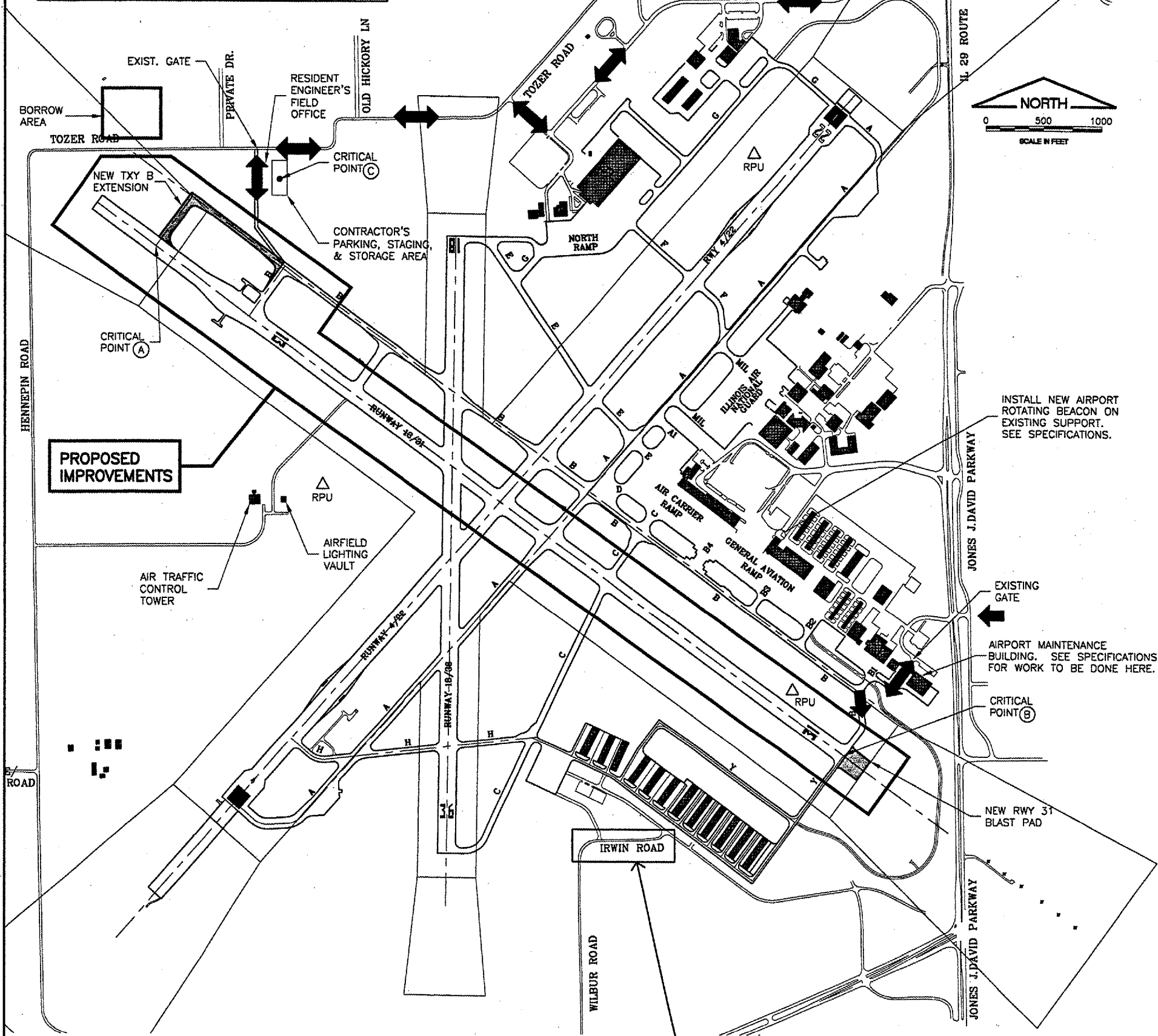
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
SITE PLAN

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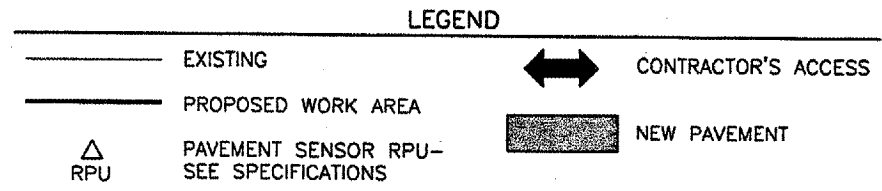
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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO. SPI-3488	
AIP PROJ. NO. 3-17-0096-42	
SHEET 03 OF 79 SHEETS	

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF A FLAGMAN IN RADIO CONTACT WITH AIR TRAFFIC CONTROL TOWER PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED AN APPROVED FAMILIARIZATION COURSE MAY OPERATE THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- BROKEN ASPHALT SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 95' FROM THE CENTERLINE OF ACTIVE TAXIWAYS, OR 250' FROM THE CENTERLINE OF ACTIVE RUNWAYS, OR BEYOND THE CONSTRUCTION LIMITS SHOWN IN THE PLANS.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUSLY CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN IN THE PLANS.
 - THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
 - THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER, S.A.A. MAINTENANCE SUPERVISOR, AND S.A.A. SECURITY CHIEF. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS. THE CONTRACTOR SHALL POST A COMPETENT SECURITY GUARD TO CONTROL ACCESS AT THE GATE. THE CONTRACTOR SHALL REPLACE ANY UNSATISFACTORY SECURITY GUARDS AS DIRECTED.
 - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY AND TAXIWAY SAFETY AREAS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING.



BLAST PAD EARTHWORK DISPOSAL AREA - TO BE COORDINATED WITH THE AIRPORT.

CRITICAL POINTS			
POINT	LATITUDE	LONGITUDE	ELEVATION
A	39°51'00.50"	89°41'19.45"	588.17
B	39°50'16.89"	89°40'03.28"	583.70
C	39°51'07.00"	89°41'05.00"	580.00



CONSTRUCTION ACTIVITY PLAN PHASE I NOTES

1. ACCESS THROUGH THIS AREA SHALL BE CLOSELY COORDINATED WITH TENANTS THROUGH THE AIRPORT. THE CONTRACTOR SHALL PROVIDE A FLAGMAN IN THIS AREA DURING HAULING OPERATIONS.

THE CONTRACTOR SHALL CONTINUOUSLY CLEAN THIS AREA DURING HAULING OPERATIONS.


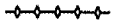




UPON COMPLETION OF THE WORK DAY, TAXIWAY B1 SHALL BE CLEANED AND RE-OPENED TO AIRCRAFT TRAFFIC.
2. WHEN TAXIWAY A AND TAXIWAY C ARE OPEN TO AIRCRAFT TRAFFIC, A FLAGMAN WILL BE REQUIRED TO COORDINATE CROSSINGS BY THE CONTRACTOR.
3. WHEN WORK IS TAKING PLACE WITHIN 100' OF THE TAXIWAY A CENTERLINE (PHASE 1-A) OR THE TAXIWAY C CENTERLINE (PHASE 1-B), THE TAXIWAY SHALL BE CLOSED.
4. TAXIWAY A AND TAXIWAY C SHALL BE CLEANED, EXCAVATIONS BACKFILLED AND PAVEMENTS RE-OPENED TO AIRCRAFT TRAFFIC AT THE END OF EACH WORKING DAY.
5. TAXIWAY A AND TAXIWAY C MAY NOT BE CLOSED AT THE SAME TIME.
6. WORK OUTSIDE THE LIMITS OF PHASES 1, 2 AND 3 SHALL BE COORDINATED IN ADVANCE WITH THE AIRPORT AND PERFORMED UNDER RADIO CONTROL.

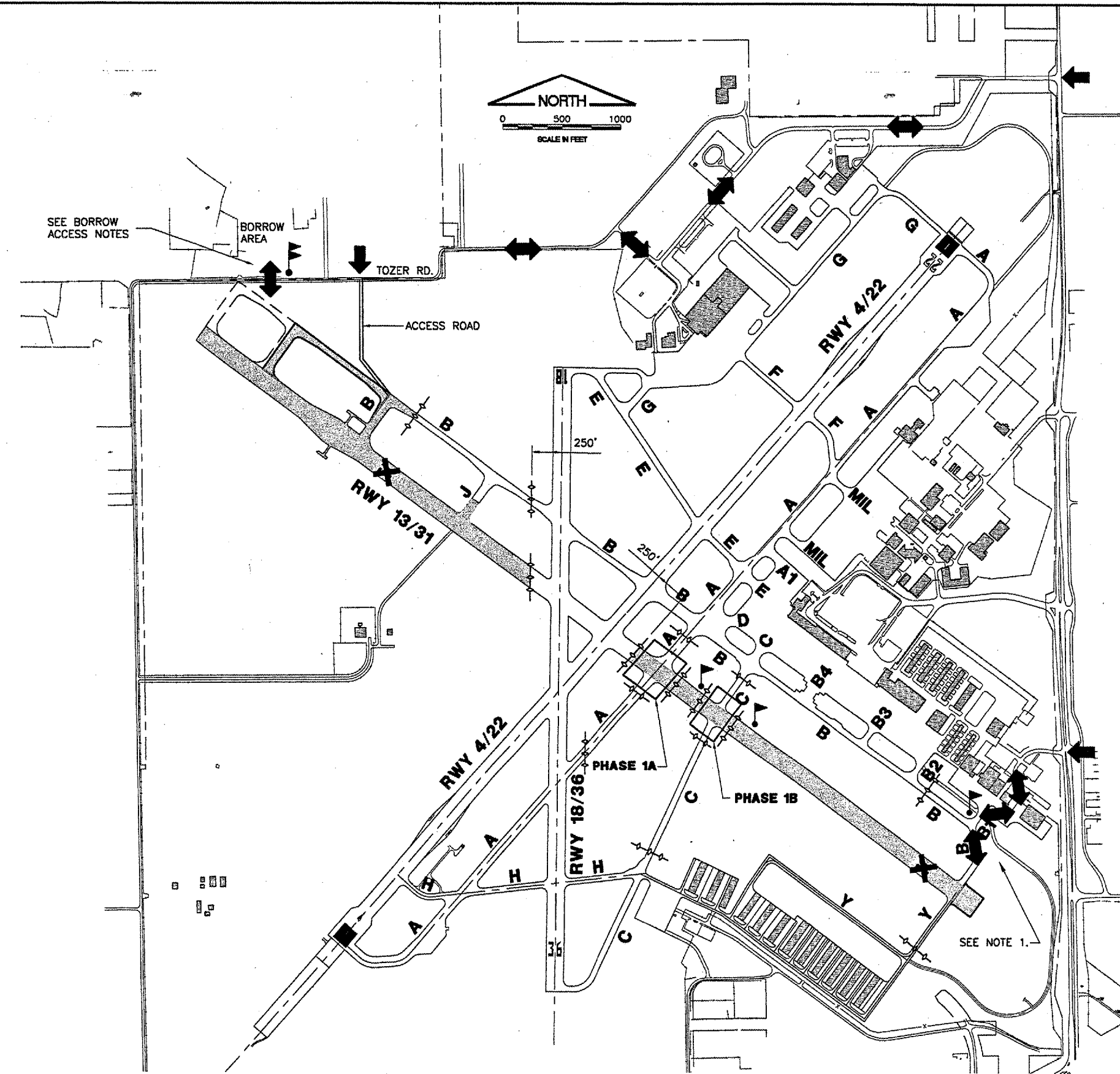
BORROW ACCESS NOTES

1. THE CONTRACTOR SHALL INSTALL A TEMPORARY ACCESS GATE AT A LOCATION APPROVED BY THE AIRPORT FOR ACCESS TO THE BORROW AREA.
2. THE CONTRACTOR SHALL PROVIDE A FLAGMAN AND WARNING SIGNAGE ON TOZER ROAD AS REQUIRED BY THE TOWNSHIP/AIRPORT DURING HAULING OPERATIONS.
3. THE CONTRACTOR SHALL TAKE MEASURES TO PROTECT THE TOZER ROAD CROSSING AND ADJACENT DITCHLINES/UTILITIES FROM DAMAGE. THE CROSSING AREA SHALL BE MAINTAINED AND RESTORED TO ITS ORIGINAL CONDITION UPON COMPLETION OF THE WORK - COST INCIDENTAL TO THE PROJECT.

CLOSED PAVEMENT AREAS - PHASE 1	
RUNWAY 4/22	OPEN
RUNWAY 13/31	CLOSED
RUNWAY 18/36	OPEN
TAXIWAY B	CLOSED
NW OF RUNWAY 18/36	CLOSED
TAXIWAY J	CLOSED
TAXIWAY A	CLOSED DURING PHASE 1-A.
FROM TXY. B TO RWY. 18-36	RE-OPEN EACH NIGHT
TAXIWAY C	CLOSED DURING PHASE 1-B.
FROM TXY. B TO TXY. H	RE-OPEN EACH NIGHT
TAXIWAY Y	CLOSED
CONNECTOR TO RUNWAY 31	CLOSED
TAXIWAY B1	CLOSED DURING WORK DAY.
	REOPEN EACH NIGHT
TAXIWAY B	CLOSED
FROM TXY. B1 TO RUNWAY 31	

LEGEND

-  PHASE 1 WORK AREA
-  BARRICADES LINE - SEE DETAIL
-  CONTRACTOR'S ACCESS
-  CLOSED RUNWAY MARKER - SEE DETAIL
-  FLAGMAN - IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
-  FLAGMAN - FOR TOZER ROAD CROSSING



SEE BORROW ACCESS NOTES

BORROW AREA

TOZER RD.

ACCESS ROAD

RWY 13/31

RWY 4/22

RWY 4/22

RWY 18/36

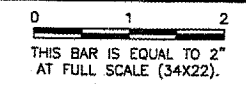
PHASE 1A

PHASE 1B

SEE NOTE 1.

GROUND CONTROL FREQUENCY: 121.90
 AIR CONTROL FREQUENCY: 121.30
 MAXIMUM HEIGHT OF EQUIPMENT: 25'

REVISIONS		
NUMBER	BY	DATE



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 CONSTRUCTION ACTIVITY PLAN 1

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JOB No:	05035-03
IL PROJ. NO. SPI-3488	
AIP PROJ. NO. 3-17-0096-42	
SHEET 04 OF 79 SHEETS	

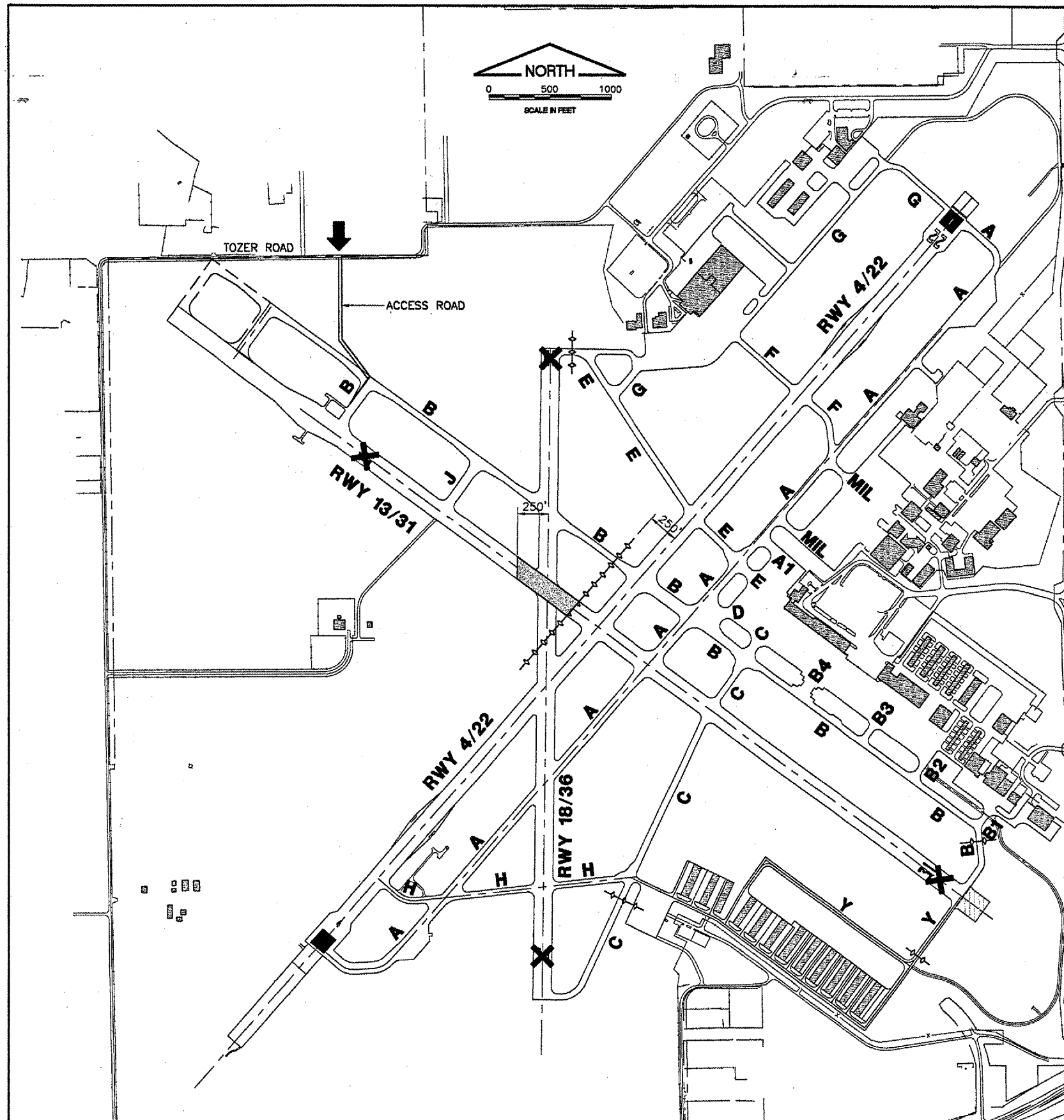
CONSTRUCTION ACTIVITY PLAN PHASE 2 NOTES

1. PHASE 2 MAY TAKE PLACE CONCURRENT WITH PHASE 1.
2. THE CONTRACTOR WILL BE ALLOWED TO CLOSE RUNWAY 18/36 FOR SEVEN (7) CONSECUTIVE CALENDAR DAYS. ANY REMAINING WORK WILL REQUIRE DAILY CLOSURES COORDINATED WITH THE AIRPORT.
3. AT THE END OF EACH DAILY CLOSURE, THE WORK AREA SHALL BE CLEANED, EXCAVATIONS BACKFILLED AND PAVEMENTS RE-OPENED TO AIRCRAFT TRAFFIC.

REVISIONS

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0 1 2
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CLOSED PAVEMENT AREAS - PHASE 2	
RUNWAY 4/22	OPEN
RUNWAY 13/31	CLOSED
RUNWAY 18/36	CLOSED FOR (7) CALENDAR DAYS, (5) DAILY CLOSURES THEREAFTER
TAXIWAY B NW OF RUNWAY 4/22	CLOSED
TAXIWAY J	CLOSED
TAXIWAY C FROM TAXIWAY H TO RWY. 36	CLOSED FOR (7) CALENDAR DAYS, DAILY CLOSURES THEREAFTER
TAXIWAY E AT RUNWAY 18	CLOSED FOR (7) CALENDAR DAYS, DAILY CLOSURES THEREAFTER

ALSO SEE PHASE 1 CLOSED PAVEMENT AREAS IF WORK IS CONCURRENT WITH PHASE 1.

LEGEND

- PHASE 2 WORK AREA
- BARRICADES LINE - SEE DETAIL
- CONTRACTORS ACCESS
- CLOSED RUNWAY MARKERS - SEE DETAIL
- FLAGMAN - IN CONTACT WITH AIR TRAFFIC CONTROL TOWER (ATCT)
- FLAGMAN - FOR TOZER ROAD CROSSING

GROUND CONTROL FREQUENCY: 121.90
 AIR CONTROL FREQUENCY: 121.30
 MAXIMUM HEIGHT OF EQUIPMENT: 25'



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 CONSTRUCTION ACTIVITY PLAN 2

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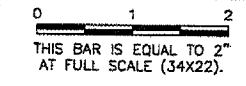
JOB No: 05035-03


IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42

CONSTRUCTION ACTIVITY PLAN PHASE 3 NOTES

1. PHASE 3 WORK SHALL TAKE PLACE AT NIGHT FROM 11:30 PM TO 5:30 AM.
2. THE AIRPORT WILL REQUIRE FOURTEEN CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE TENANTS.
3. THE CONTRACTOR SHALL PLACE RUNWAY CLOSURE MARKERS OVER THE EXISTING RUNWAY NUMERALS AND PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK IN THIS AREA.
4. PHASE 3 IS AN EXPEDITED WORK AREA AND SHALL RECEIVE PRIORITY OVER ALL OTHER WORK AREAS.
5. PHASE 3 SHALL NOT TAKE PLACE CONCURRENTLY WITH PHASE 2.
6. PHASE 3 MAY TAKE PLACE CONCURRENTLY WITH PHASE 1.
7. AT THE END OF THE NIGHT TIME WORK HOURS, ALL OPEN EXCAVATIONS WITHIN PHASE 3 SHALL BE BACKFILLED TO REDUCE DROP OFF TO LESS THAN 3".
8. CONSTRUCTION PHASE 3 REQUIRES THAT THE PRIMARY RUNWAY AND THE SECONDARY RUNWAY BE CLOSED AT THE AIRPORT. TO CONSTRUCT THE IMPROVEMENTS IN THE RUNWAY-RUNWAY INTERSECTION, THE CLOSURE OF THESE RUNWAYS PRESENTS A GREAT INCONVENIENCE TO THE AIRPORTS TENANTS AND USERS.
9. THE ABOVE IMPACTS MERIT ADDITIONAL CONSIDERATION FROM THE CONTRACTOR WHEN SCHEDULING THE WORK IN CONSTRUCTION PHASE 3 OF THE PROJECT AND AS A RESULT THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE SUFFICIENT PERSONNEL AND EQUIPMENT AND WORK SUFFICIENT HOURS TO COMPLETE THE CONSTRUCTION PHASE 3 WORK WITHIN THE FOLLOWING PARAMETERS:
 - A. THE CONTRACTOR WILL BE ALLOWED TO CLOSE THE RUNWAY 4/22 AND RUNWAY 13/31 INTERSECTION FOR FIVE (5) CONSECUTIVE NIGHTS TO COMPLETE THE WORK WITHIN THE PHASE 3 LIMITS.
 - B. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE ABOVE REFERENCED CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE. THE AIRPORT AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE ACTUAL DAYS FOR CLOSURE.
10. UPON COMPLETION OF EACH NIGHT'S PHASE 3 WORK, RUNWAY 4/22 SHALL BE RE-OPENED TO AIRCRAFT TRAFFIC. PRIOR TO REOPENING THE RUNWAY, THE CONTRACTOR SHALL CLEAN THE PAVEMENTS TO THE AIRPORT'S SATISFACTION.
11. A FLAGMAN IN CONTACT WITH THE AIR CONTROL FREQUENCY SHALL MONITOR THE RADIO DURING THIS PHASE.
12. ACCESS TO THE WORK SITE WILL BE BY RADIO CONTROL.

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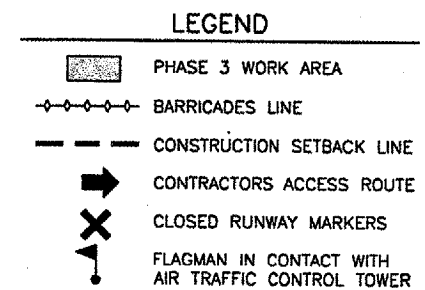



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SPRINGFIELD, ILLINOIS

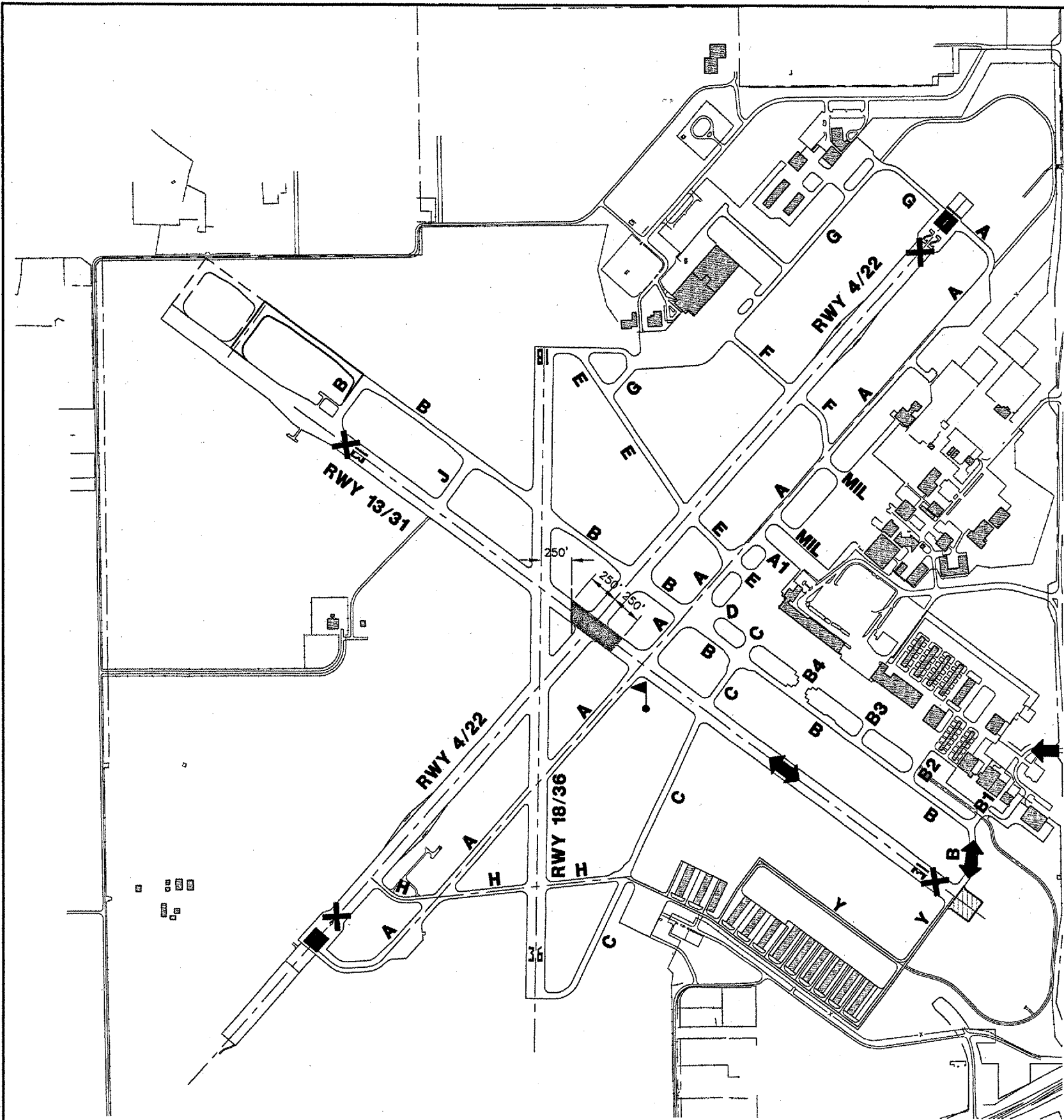
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
CONSTRUCTION ACTIVITY PLAN 3

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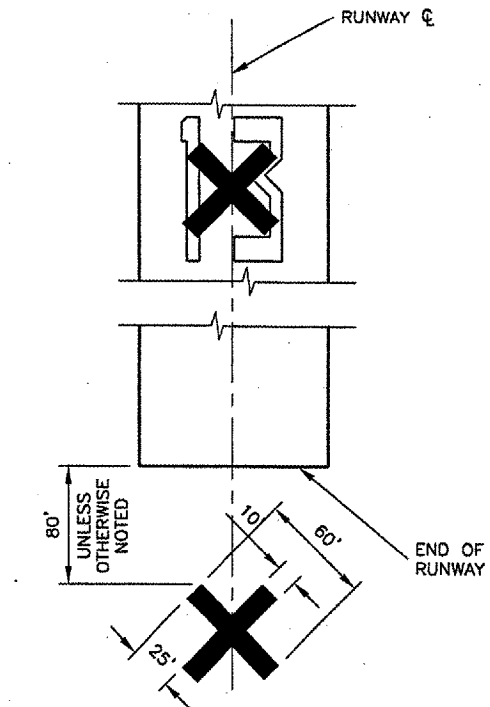
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SHEET	06 OF 70 SHEETS



CLOSED PAVEMENT AREAS - PHASE THREE	
RUNWAY 4/22	CLOSED FOR (5) NIGHTS BETWEEN 11:30 PM AND 5:30 AM. OPEN DURING THE DAY.
RUNWAY 13/31	CLOSED
RUNWAY 18/36	OPEN
ALSO SEE PHASE 1 CLOSED PAVEMENT AREAS IF WORK IS CONCURRENT WITH PHASE 1.	



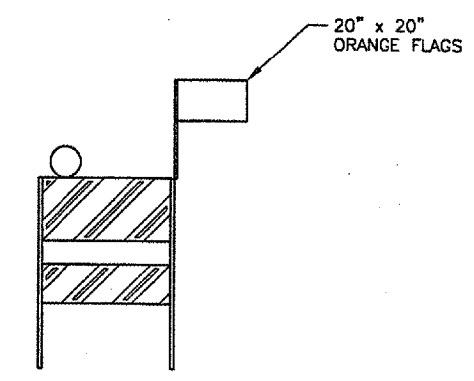
GROUND CONTROL FREQUENCY: 121.90
 AIR CONTROL FREQUENCY: 121.30
 MAXIMUM HEIGHT OF EQUIPMENT: 25'



CLOSED RUNWAY MARKER DETAIL
 N.T.S.

NOTES

1. MARKERS SHALL BE SOLID YELLOW.
2. MARKERS SHALL BE PAINTED BURLAP, PLYWOOD OR OTHER APPROVED SOLID MATERIALS.
3. CONTRACTOR SHALL MAINTAIN MARKERS.
4. COST OF FURNISHING, INSTALLING, MAINTAINING, RELOCATING AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
5. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS. HOWEVER, WHEN WORK IS NECESSARY FOR THAT AREA OR PAVEMENT IS OPEN TO TAXIING AIRCRAFT, THE MARKERS SHALL BE RELOCATED OFF THE PAVEMENT AS SHOWN ON THE DETAIL.
6. PORTABLE CLOSED RUNWAY MARKERS SHALL BE PROVIDED, INSTALLED, OPERATED AND MAINTAINED AS SPECIFIED.
7. THE PORTABLE CLOSED RUNWAY MARKERS SHALL BE INSTALLED ON RUNWAY 13/31. TEMPORARY CLOSED RUNWAY MARKERS AS DISCUSSED IN ITEM 2. ABOVE SHALL BE INSTALLED ON RUNWAY 18/36 AND RUNWAY 4/22.



FLASHER BARRICADE DETAIL
 N.T.S.

FLASHER BARRICADE NOTES

1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. PLACE AT 20' INTERVALS.

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0 1 2
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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
CONSTRUCTION ACTIVITY PLAN DETAILS

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EXISTING CONDITION & REMOVALS NOTES

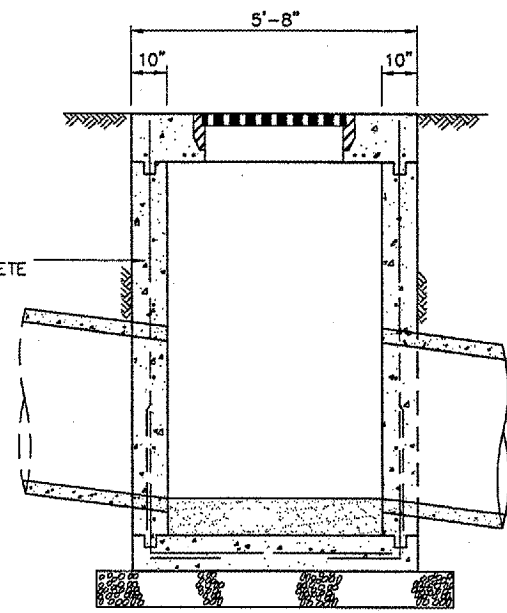
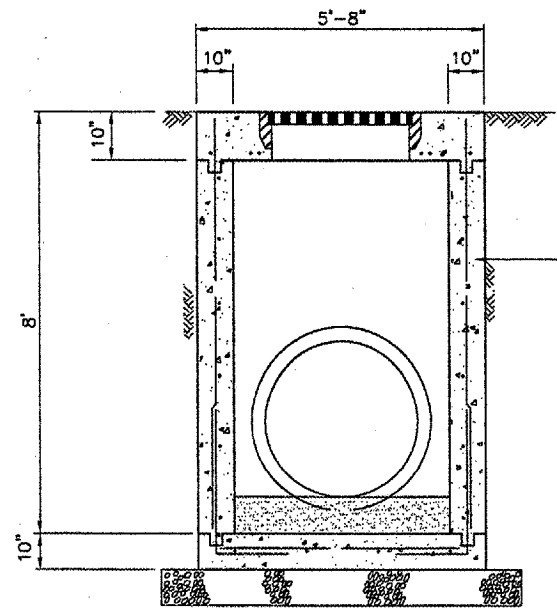
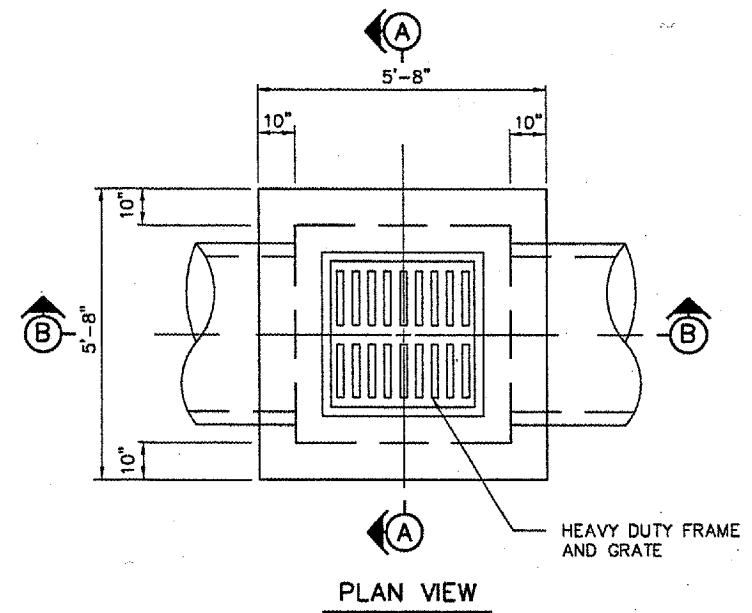
1. EXISTING EDGE LIGHT CABLING IS SHOWN DISTORTED TO IMPROVE CLARITY.
2. REMOVAL OF SPLICE CANS WHICH ARE PART OF SIGN BASES SHALL BE INCIDENTAL TO SIGN BASE REMOVAL.

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LEGEND

- EXISTING PAVEMENT
- NEW PAVEMENT
- FUTURE PAVEMENT
- E ——— EXIST. ELECTRICAL CABLE
- G ——— EXIST. GAS LINE
- · · · · · EXIST. DITCH/FLOWLINE
- FO ——— EXIST. FIBER—OPTIC CABLE
- SS ——— EXIST. STORM SEWER
- UD ——— EXIST. UNDERDRAIN
- W ——— EXIST. WATERMAIN
- X ——— EXIST. FENCE
- · · · · · EXIST. JOINT
- · · · · · EXIST. RWY 13/31 CIRCUIT
- · · · · · EXISTING TXY B — NORTH CIRCUIT (CKT. #6)
- /// /// NEW REMOVAL
- RR NEW RELOCATION
- A NEW ADJUSTMENT
- ⊠ EXIST. BASE MOUNTED TAXIWAY LIGHT
- EXIST. STAKE MOUNTED TXY LIGHT
- RWY ⊙ EXIST. FLUSH MOUNTED RUNWAY LIGHT
- TXY ⊙ EXIST. FLUSH MOUNTED TAXIWAY LIGHT
- ⊠ EXIST. BASE MOUNTED RUNWAY LIGHT
- ⊠ EXIST. STAKE MOUNTED RUNWAY LIGHT
- ⊠ EXIST. MALSR THRESHOLD LIGHT
- ⊠ EXIST. THRESHOLD LIGHT
- ⊠ EXIST. AIRFIELD SIGN
- FOS ⊠ EXIST. FIBER—OPTIC SPLICE, DIRECT BURY
- ⊙ EXIST. SPLICE CAN
- ⊙ EXIST. OVERHEAD LIGHT
- ⊠ HH EXIST. HANDHOLE
- ⊠ DM EXIST. DUCT MARKER
- ⊙ CS EXIST. COLLECTION STRUCTURE
- ⊙ EXIST. CLEANOUT
- ⊠ EXIST. INLET
- ⊙ EXIST. MANHOLE
- ▽ EXIST. REIL
- ⊠ EXIST. WINDSOCK
- ◆ EXIST. SOIL BORING
- ⊠ EXIST. SIGN
- /// /// EXIST. PAVEMENT — TO BE REMOVED
- NEW BUTT JOINT MILLING
- DC DIRECT CONNECTION — UNDERDRAIN TO STORM SEWER
- ☁ NEW CLEARING & GRUBBING LIMITS

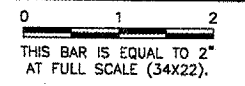


INLET (TYPE II) REMOVAL DETAILS

N.T.S.

NOTE: DIMENSIONS ARE APPROXIMATE.

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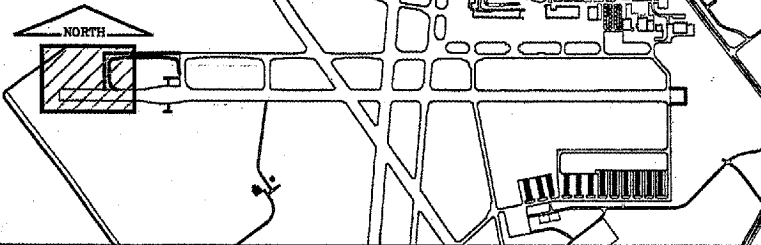
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EXISTING CONDITIONS & REMOVALS
LEGENDS & NOTES

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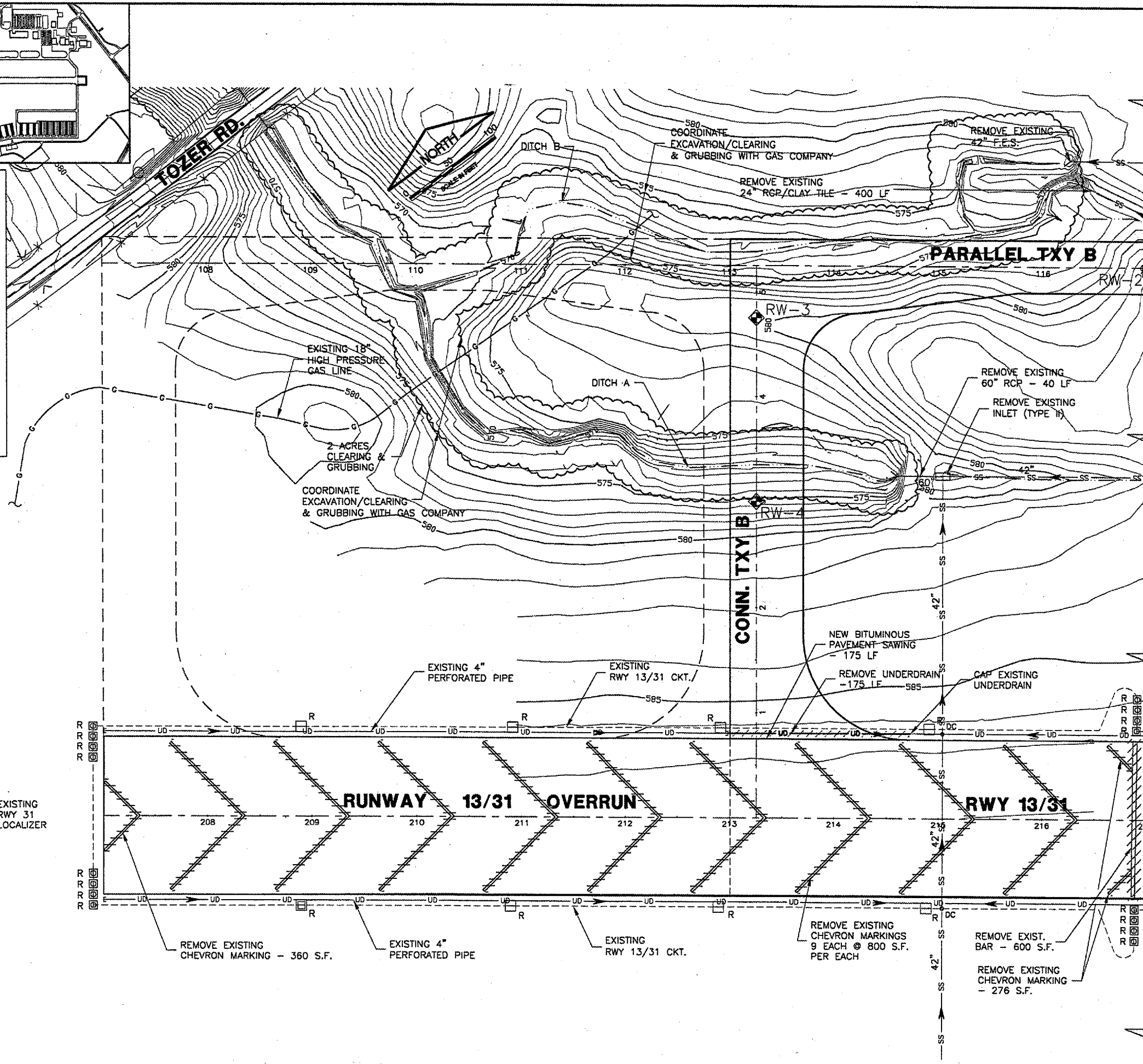
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KEYMAP



THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

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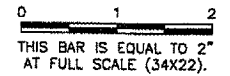
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MATCHLINE STA. 217+00

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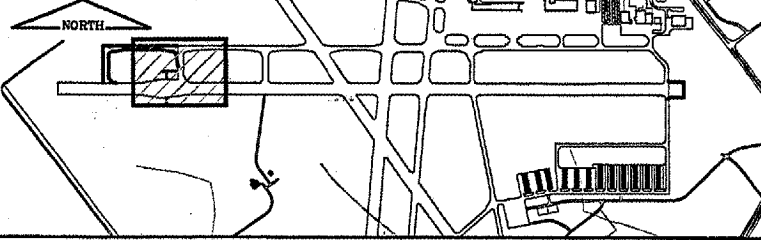
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 EXISTING CONDITIONS & REMOVALS 1

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KEYMAP



NOTES:

1. REPLACE EXISTING IN-PAVEMENT RUNWAY EDGE LIGHT FIXTURES - 6 EACH. LIGHT BASES TO BE RE-USED.

CA003

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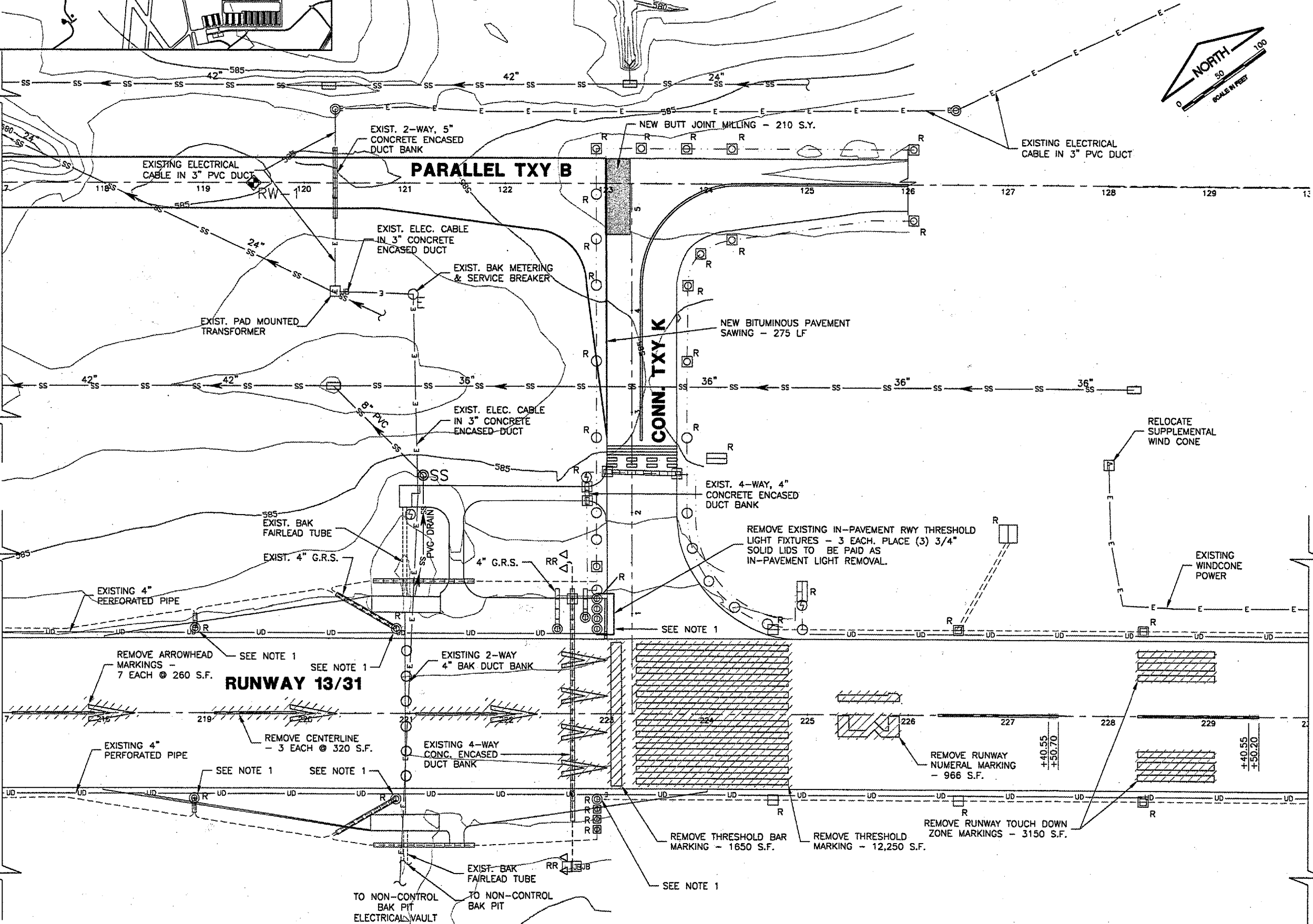
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MATCHLINE STA. 217+00

MATCHLINE STA. 230+00



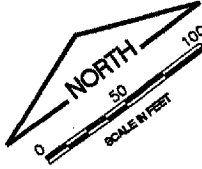
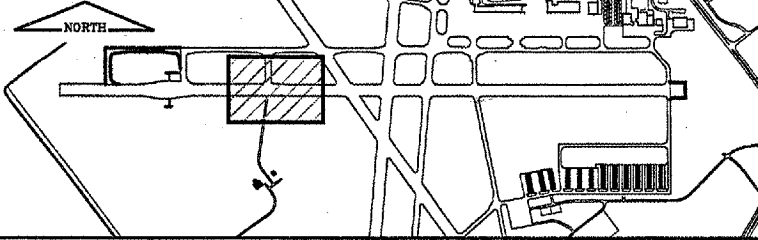

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SHEET 10 OF 70 SHEETS	

KEYMAP

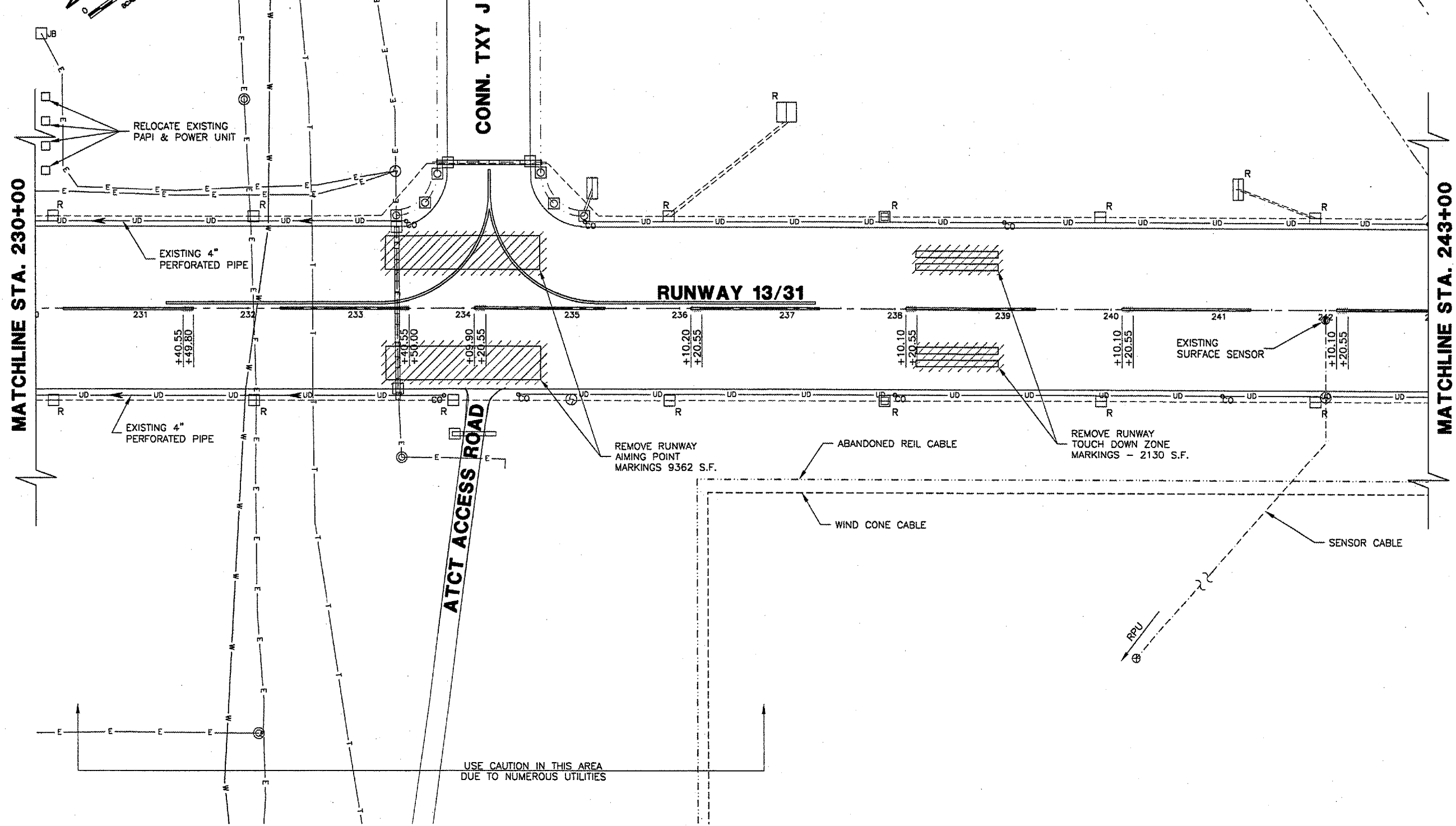
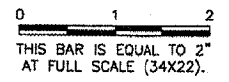


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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EXISTING CONDITIONS & REMOVALS 3

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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
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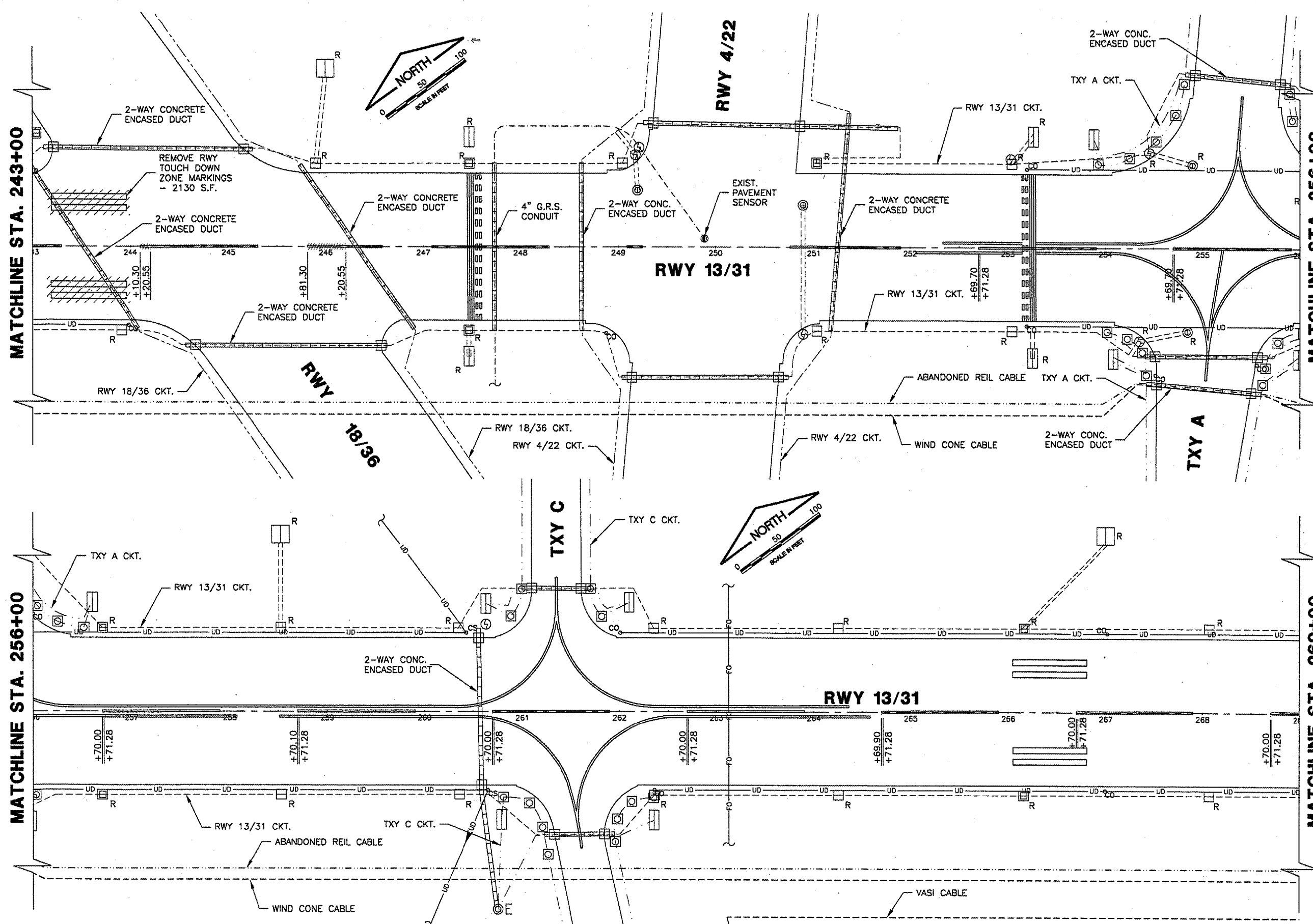
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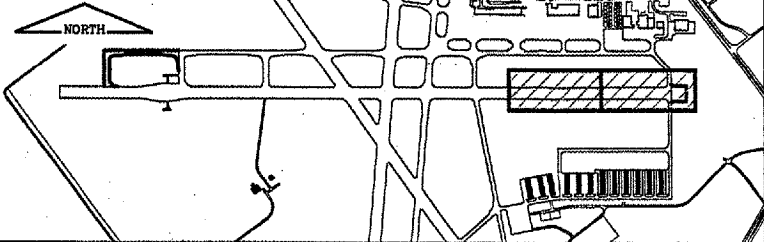
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MATCHLINE STA. 269+00

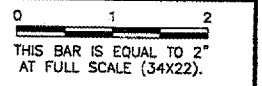
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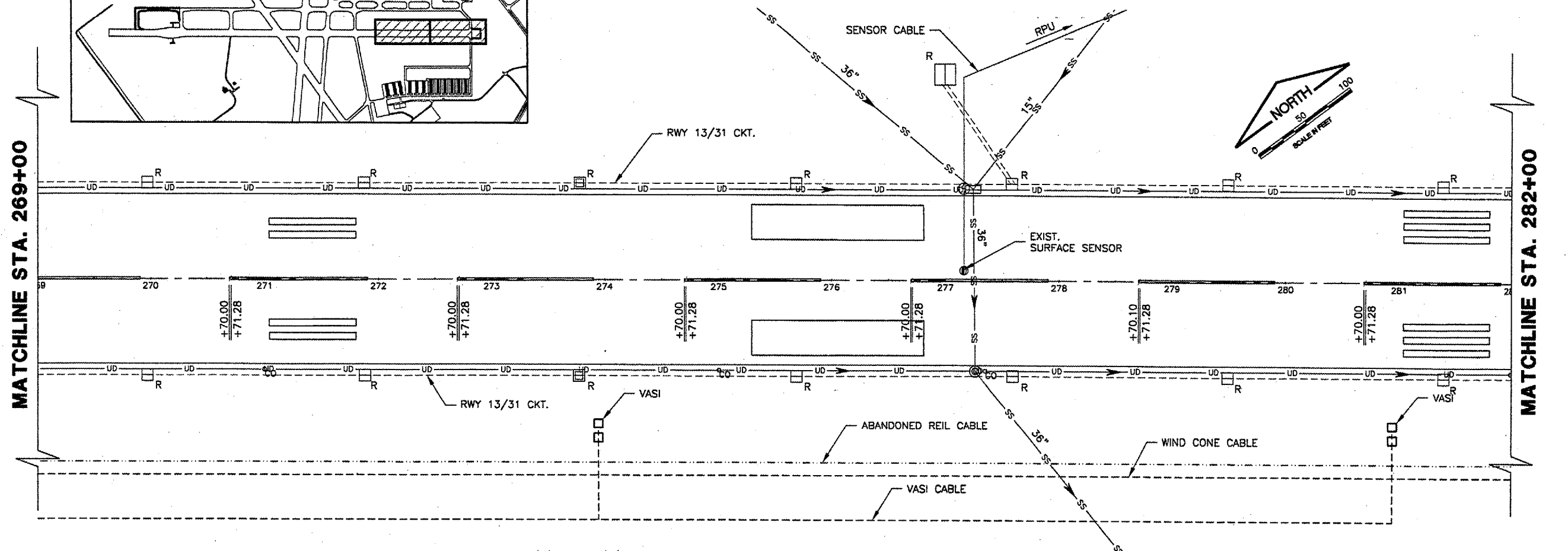
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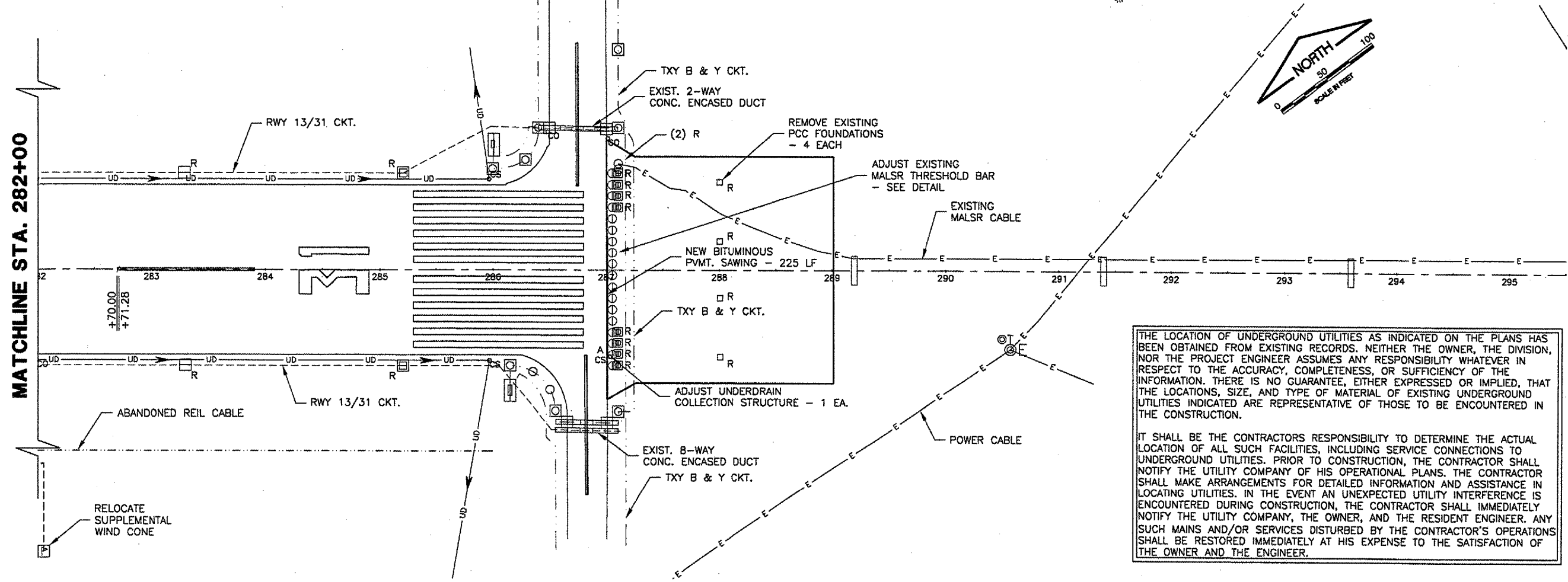


MATCHLINE STA. 269+00

MATCHLINE STA. 282+00



MATCHLINE STA. 282+00



THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

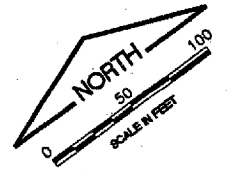
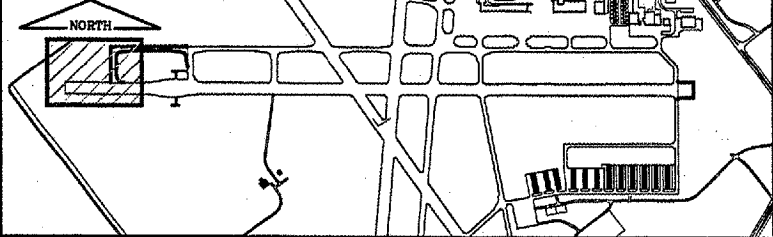
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EXISTING CONDITIONS & REMOVALS 5

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 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03

IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 13 OF 79 SHEETS

KEYMAP



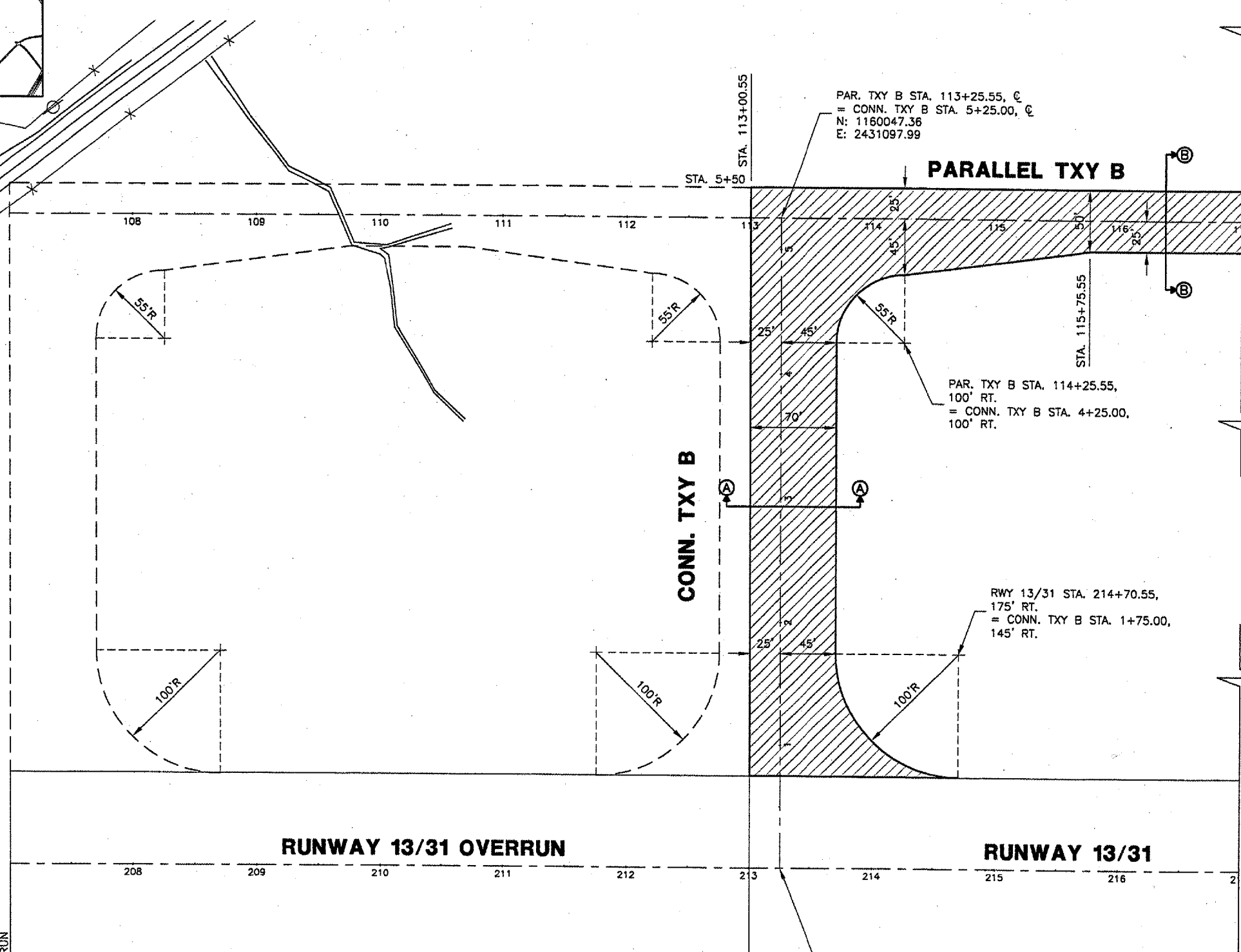
LEGEND

- FUTURE PAVEMENT
- NEW PAVEMENT
- EXISTING PAVEMENT
- NEW 4" BITUMINOUS SURFACE COURSE (401), 14" BITUMINOUS BASE COURSE (201), 4" CRUSHED AGGREGATE BASE COURSE (209), 8" LIME TREATED SUBGRADE (155) - CONTRACTOR'S OPTION
- NEW 1 1/2" BITUMINOUS SURFACE COURSE (401), 2 1/2" BITUMINOUS BASE COURSE (201), 4" CRUSHED AGGREGATE BASE COURSE (209)
- NEW BITUMINOUS BUTT JOINT - SEE STAKING PLANS

EXISTING START OF OVERRUN
STA. 207+00.55

NEW RWY 13 THRESHOLD
STA. 213+00.55

NEW RWY 31 END
STA. 217+00.55



PAR. TXY B STA. 113+25.55, C
= CONN. TXY B STA. 5+25.00, C
N: 1160047.36
E: 2431097.99

PARALLEL TXY B

PAR. TXY B STA. 114+25.55,
100' RT.
= CONN. TXY B STA. 4+25.00,
100' RT.

RWY 13/31 STA. 214+70.55,
175' RT.
= CONN. TXY B STA. 1+75.00,
145' RT.

RWY 13/31 STA. 213+25.55, C
= CONN. TXY B STA. 0+00.00, C
N: 159624.22
E: 2430787.21

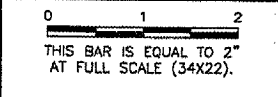
MATCHLINE STA. 117+00

MATCHLINE STA. 217+00

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PLOT DATE: 4/30/2007 12:52 PM
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KEY

REVISIONS		
NUMBER	BY	DATE



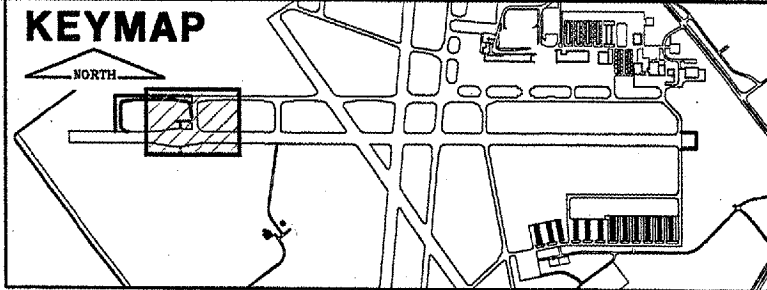
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
PROPOSED IMPROVEMENTS 1

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CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42



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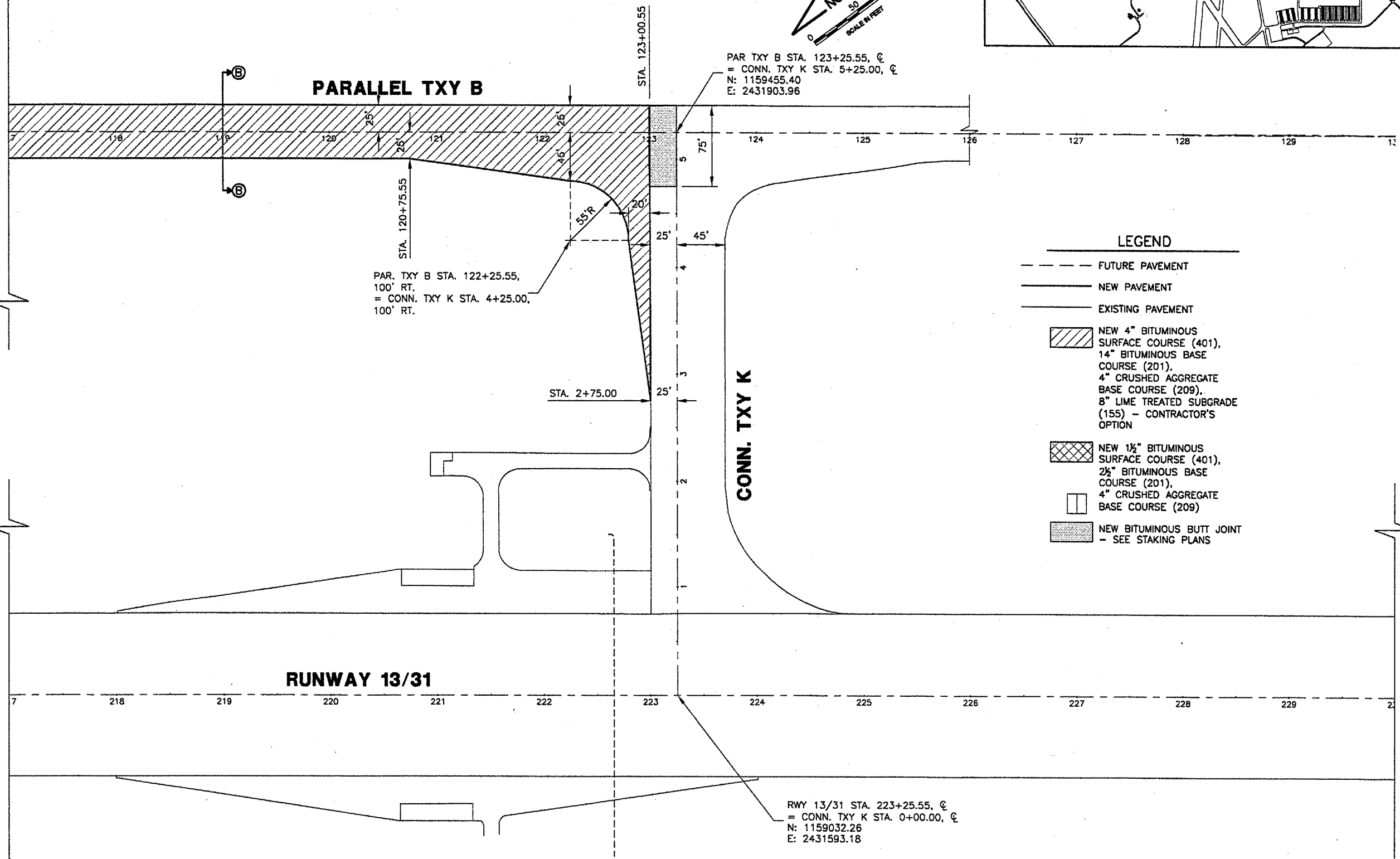
REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

MATCHLINE STA. 117+00

MATCHLINE STA. 217+00

MATCHLINE STA. 230+00



PAR TXY B STA. 123+25.55, ϕ
 = CONN. TXY K STA. 5+25.00, ϕ
 N: 1159455.40
 E: 2431903.96

PAR. TXY B STA. 122+25.55,
 100' RT.
 = CONN. TXY K STA. 4+25.00,
 100' RT.

RWY 13/31 STA. 223+25.55, ϕ
 = CONN. TXY K STA. 0+00.00, ϕ
 N: 1159032.26
 E: 2431593.18

LEGEND

- FUTURE PAVEMENT
- NEW PAVEMENT
- EXISTING PAVEMENT
- NEW 4" BITUMINOUS SURFACE COURSE (401), 14" BITUMINOUS BASE COURSE (201), 4" CRUSHED AGGREGATE BASE COURSE (209), 8" LIME TREATED SUBGRADE (155) - CONTRACTOR'S OPTION
- NEW 1 1/2" BITUMINOUS SURFACE COURSE (401), 2 1/2" BITUMINOUS BASE COURSE (201), 4" CRUSHED AGGREGATE BASE COURSE (209)
- NEW BITUMINOUS BUTT JOINT - SEE STAKING PLANS



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

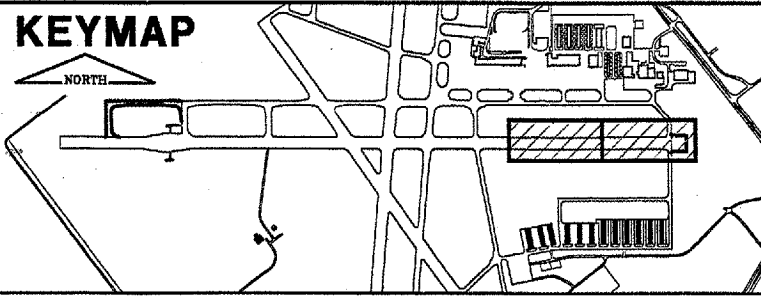
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 PROPOSED IMPROVEMENTS 2

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 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03

IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 15 OF 79 SHEETS

KEYMAP

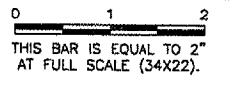


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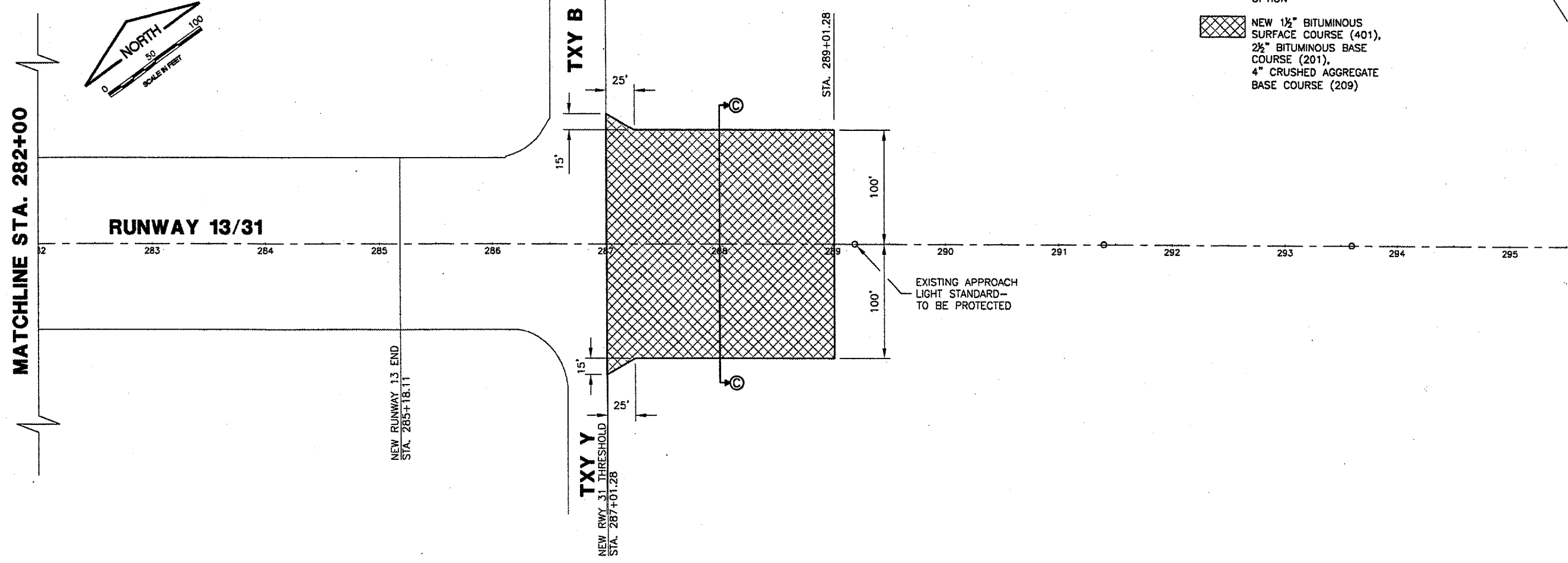
REVISIONS

NUMBER	BY	DATE



LEGEND

- FUTURE PAVEMENT
- NEW PAVEMENT
- EXISTING PAVEMENT
- NEW 4" BITUMINOUS SURFACE COURSE (401), 14" BITUMINOUS BASE COURSE (201), 4" CRUSHED AGGREGATE BASE COURSE (209), 8" LIME TREATED SUBGRADE (155) - CONTRACTOR'S OPTION
- NEW 1 1/2" BITUMINOUS SURFACE COURSE (401), 2 1/2" BITUMINOUS BASE COURSE (201), 4" CRUSHED AGGREGATE BASE COURSE (209)



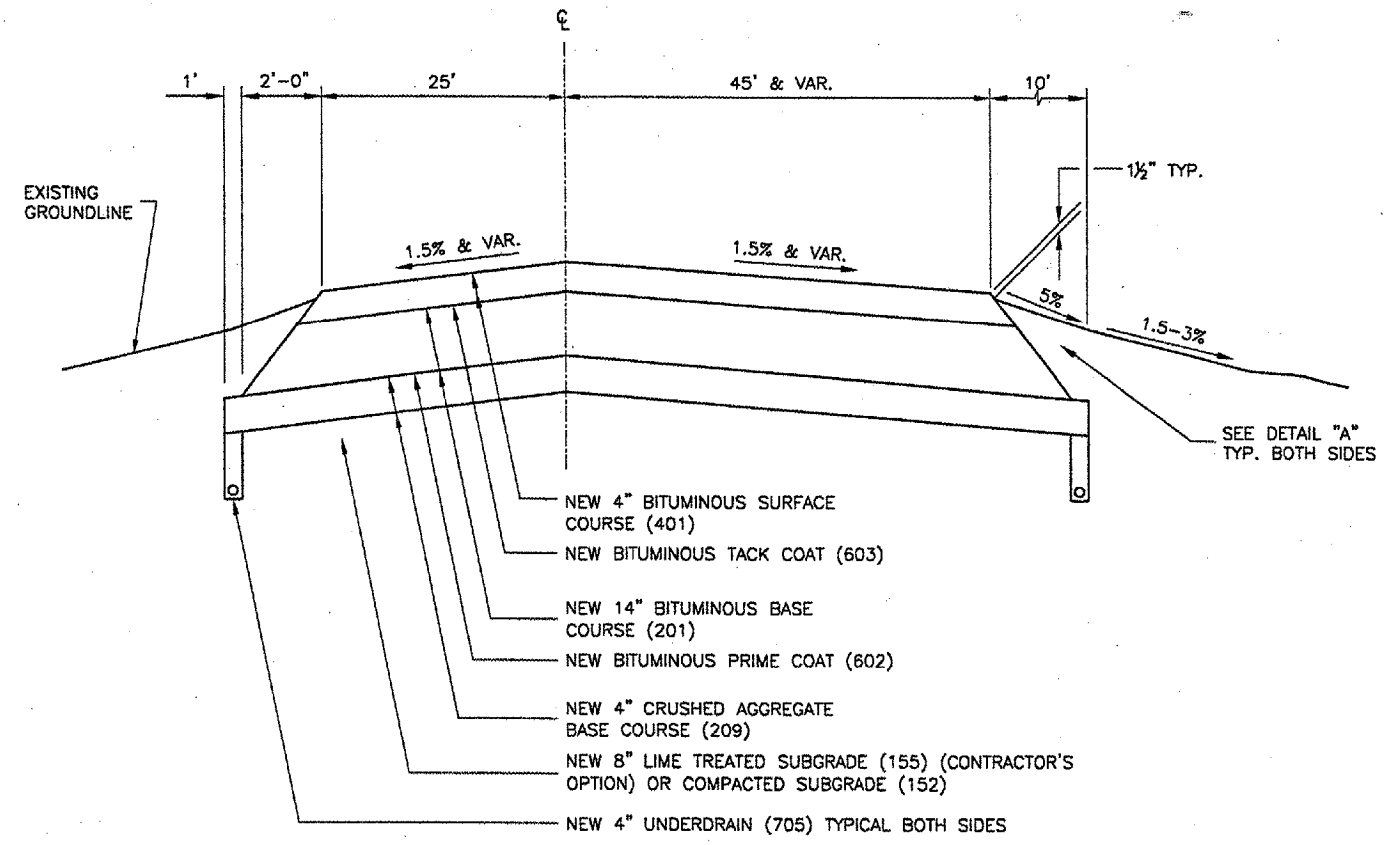
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
PROPOSED IMPROVEMENTS 3

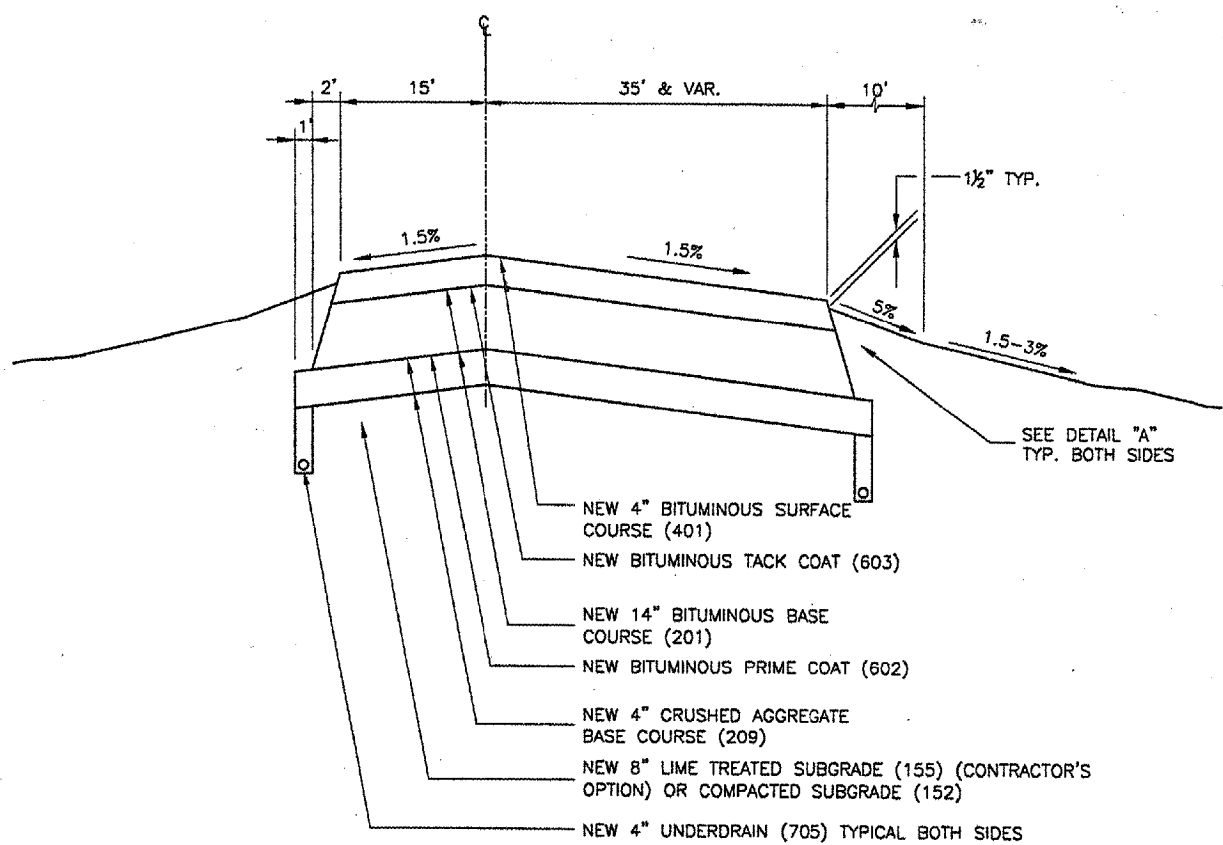
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CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03

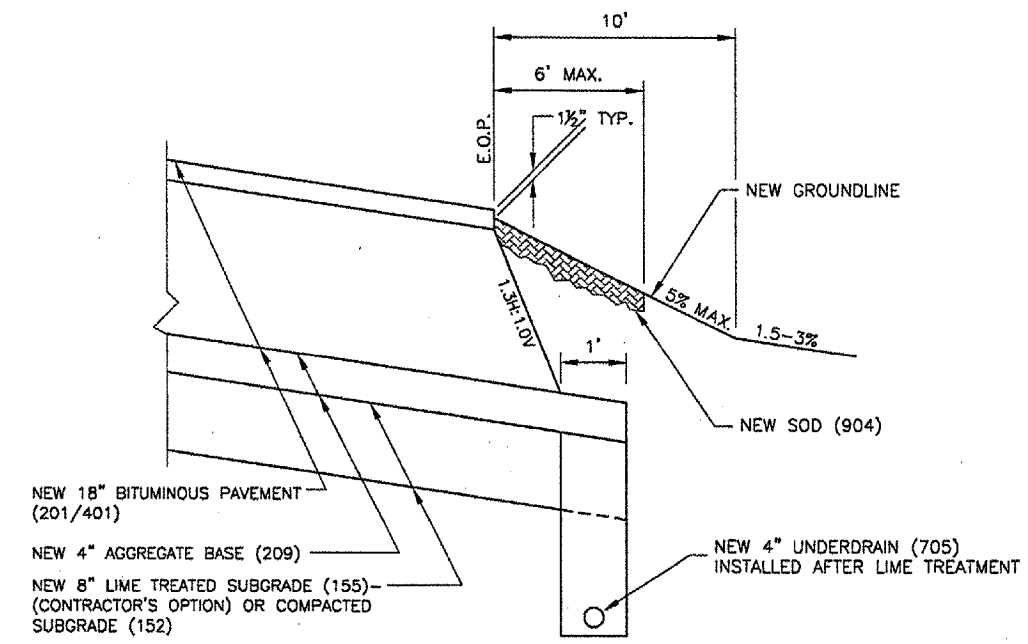
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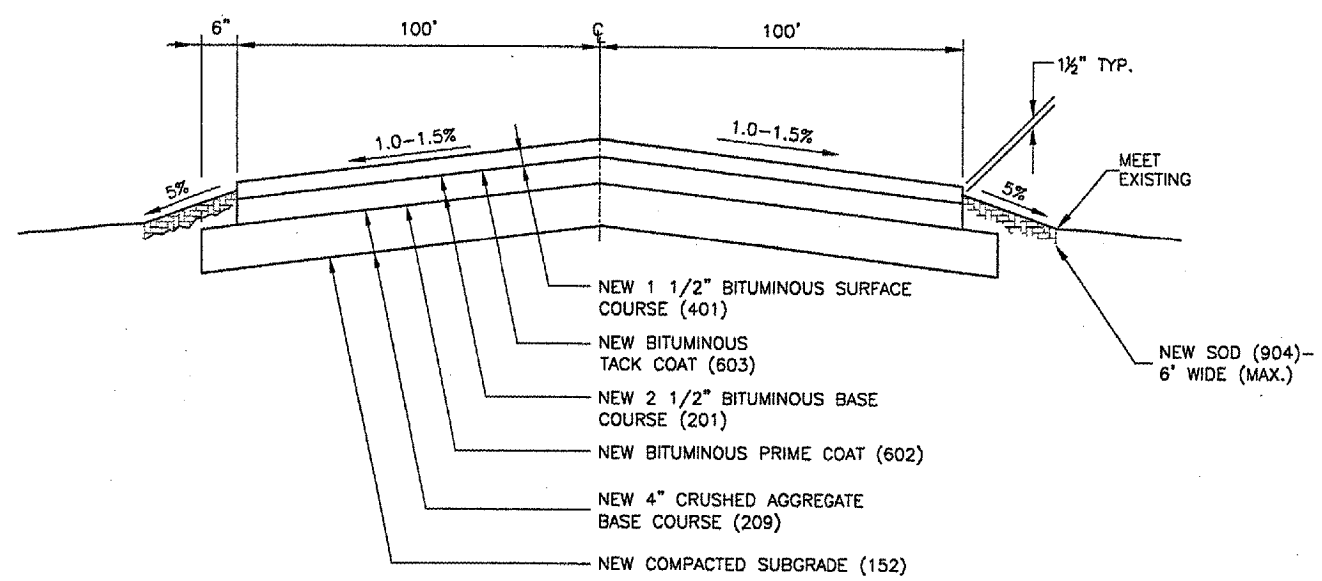
(A) NEW CONNECTING TAXIWAY B (A)
 N.T.S.



(B) NEW PARALLEL TAXIWAY B EXTENSION (B)
 N.T.S.



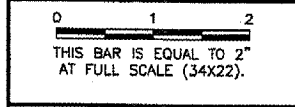
NEW SHOULDER DETAIL "A"
 N.T.S.



(C) NEW RUNWAY 31 BLAST PAD (C)
 N.T.S.

REVISIONS

NUMBER	BY	DATE

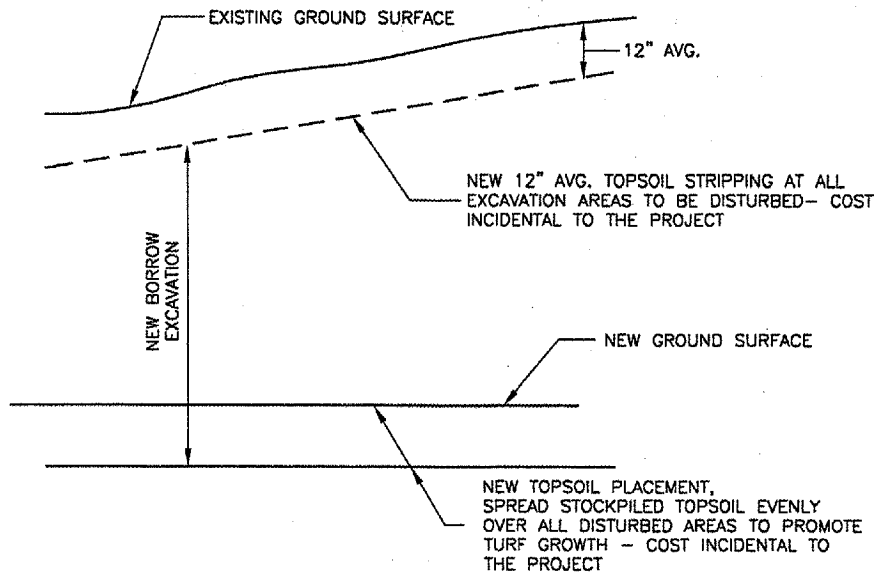


SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 TYPICAL SECTIONS 1

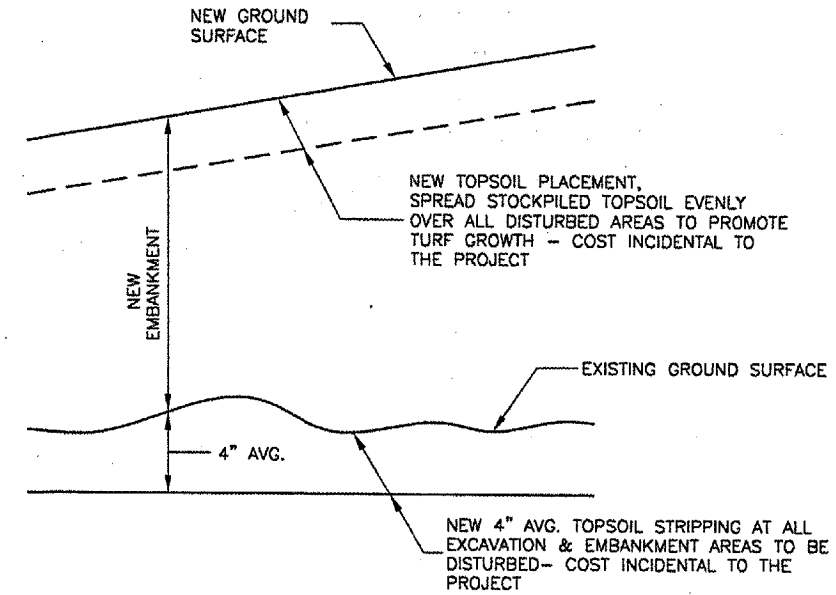
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APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03



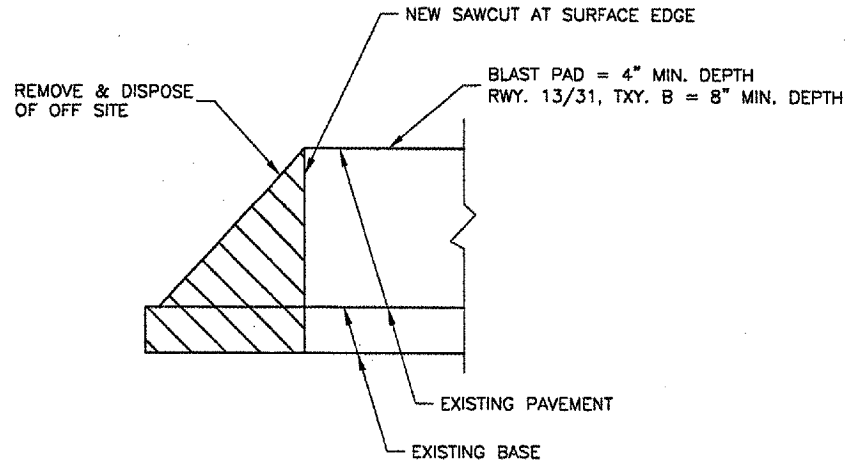
TOPSOIL STRIPPING & REPLACEMENT DETAIL
 - FOR BORROW AREA ONLY

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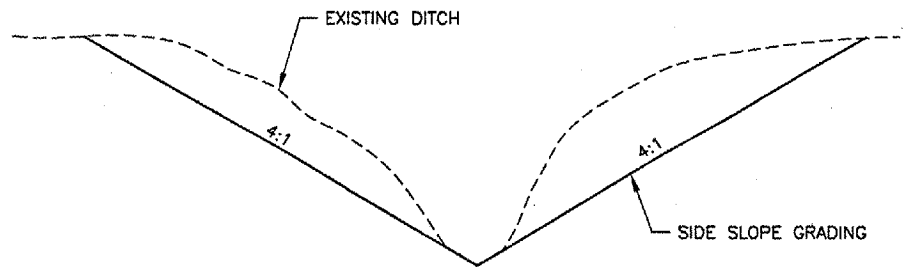
TOPSOIL STRIPPING DETAIL
 - FOR ALL LOCATIONS OTHER
 THAN BORROW AREA

N.T.S.



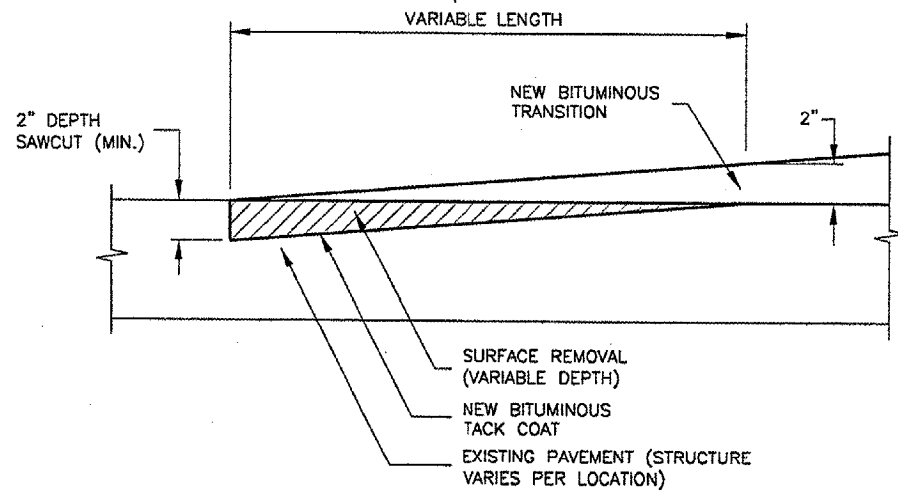
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N.T.S.



DITCH A GRADING DETAIL

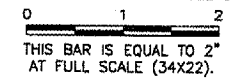
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BUTT JOINT DETAIL

N.T.S.

REVISIONS		
NUMBER	BY	DATE



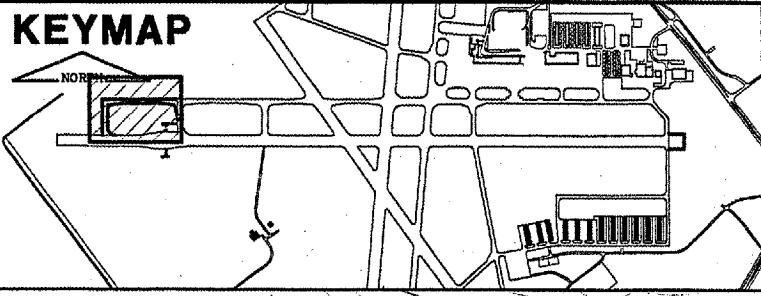
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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 TYPICAL SECTIONS 2

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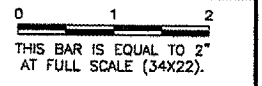
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JOB No:	05035-03

KEYMAP

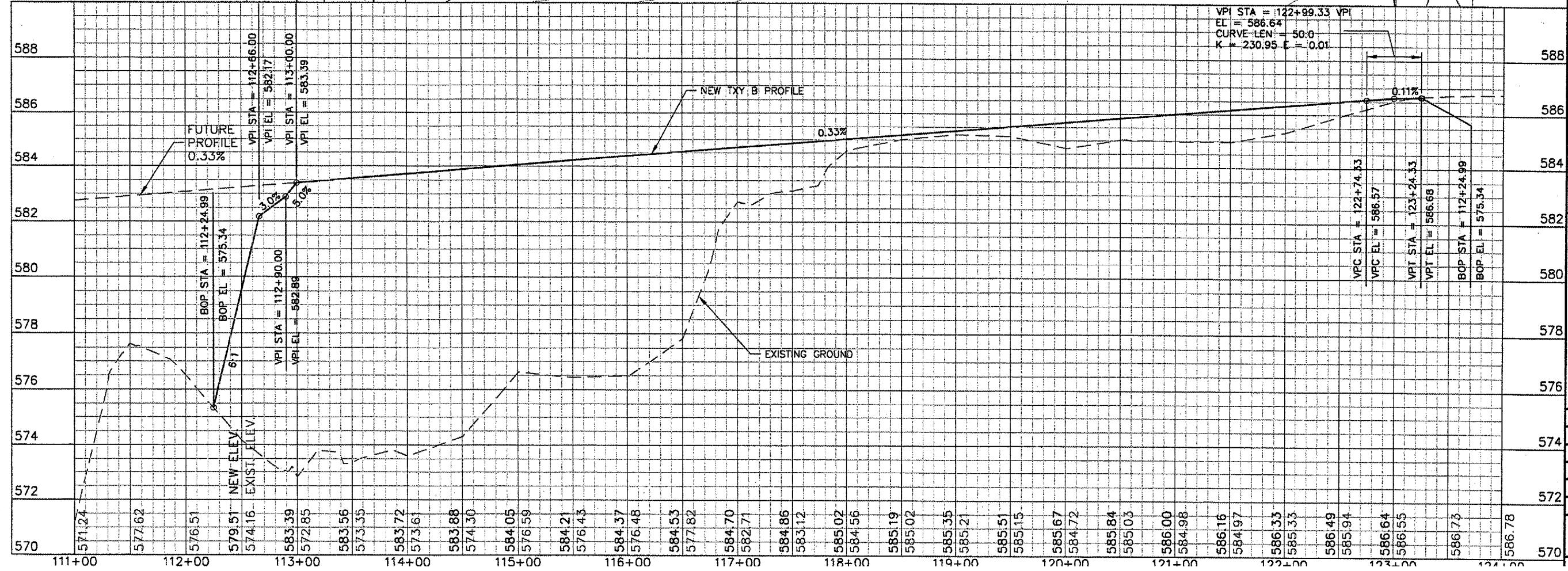
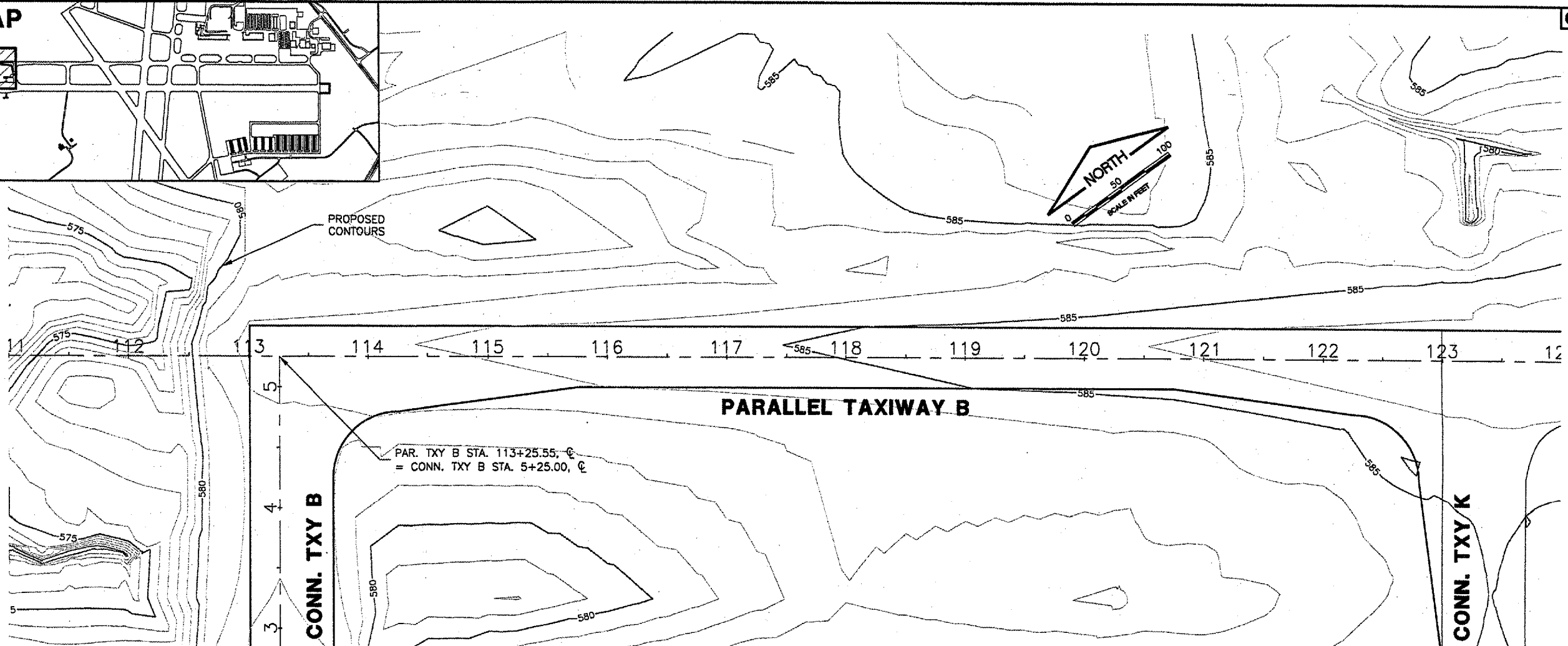


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 ContourBase
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 tbinfo
 KEY

REVISIONS		
NUMBER	BY	DATE



SCALES:
 1" = 50' HOR
 1" = 2' VER



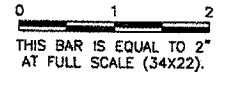
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
PARALLEL TAXIWAY B PLAN & PROFILE

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 CHECKED BY: RLN
 APPROVED BY: RLN
 DATE: 04/24/07
 JOB No: 05035-03
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 AIP PROJ. NO. 3-17-0096-42
 SHEET 10 OF 20 SHEETS

REVISIONS		
NUMBER	BY	DATE

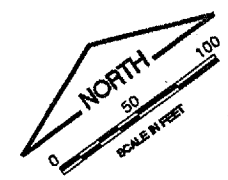
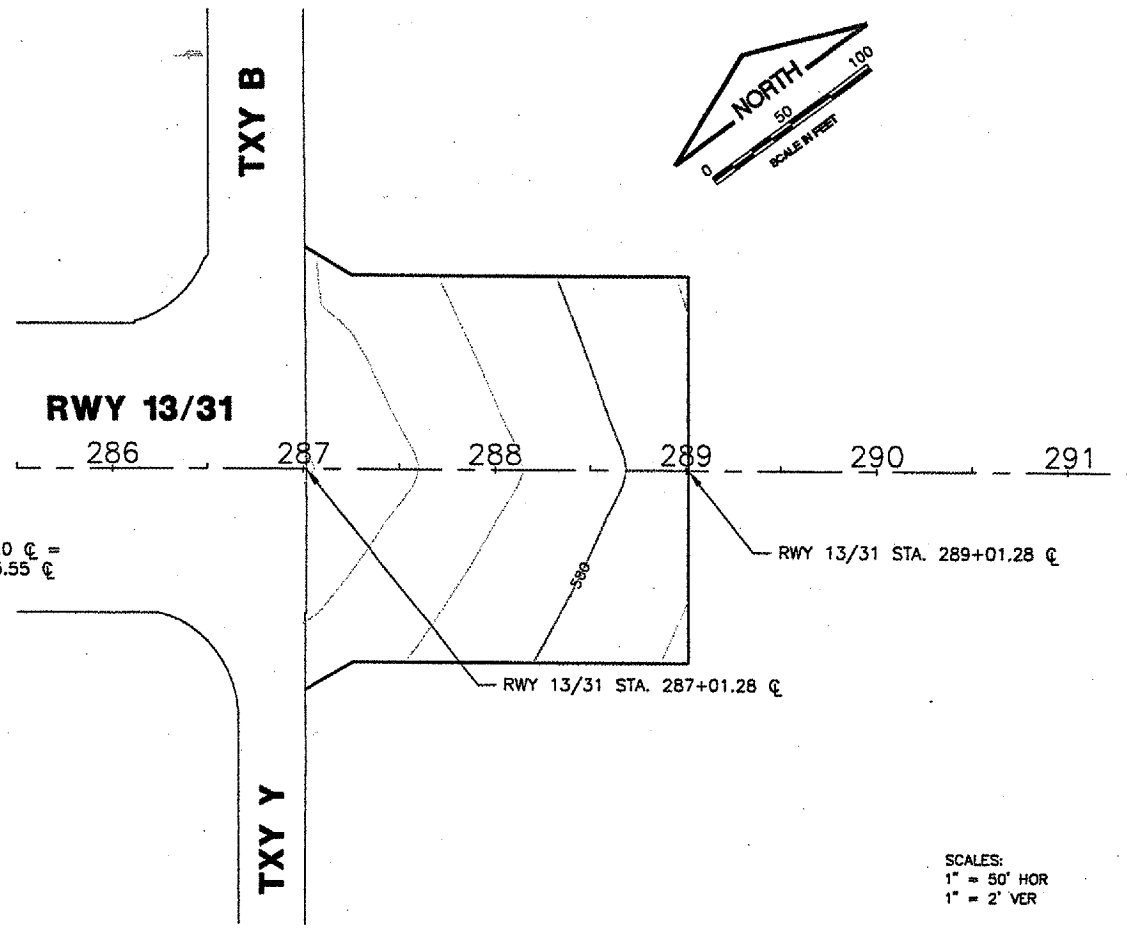
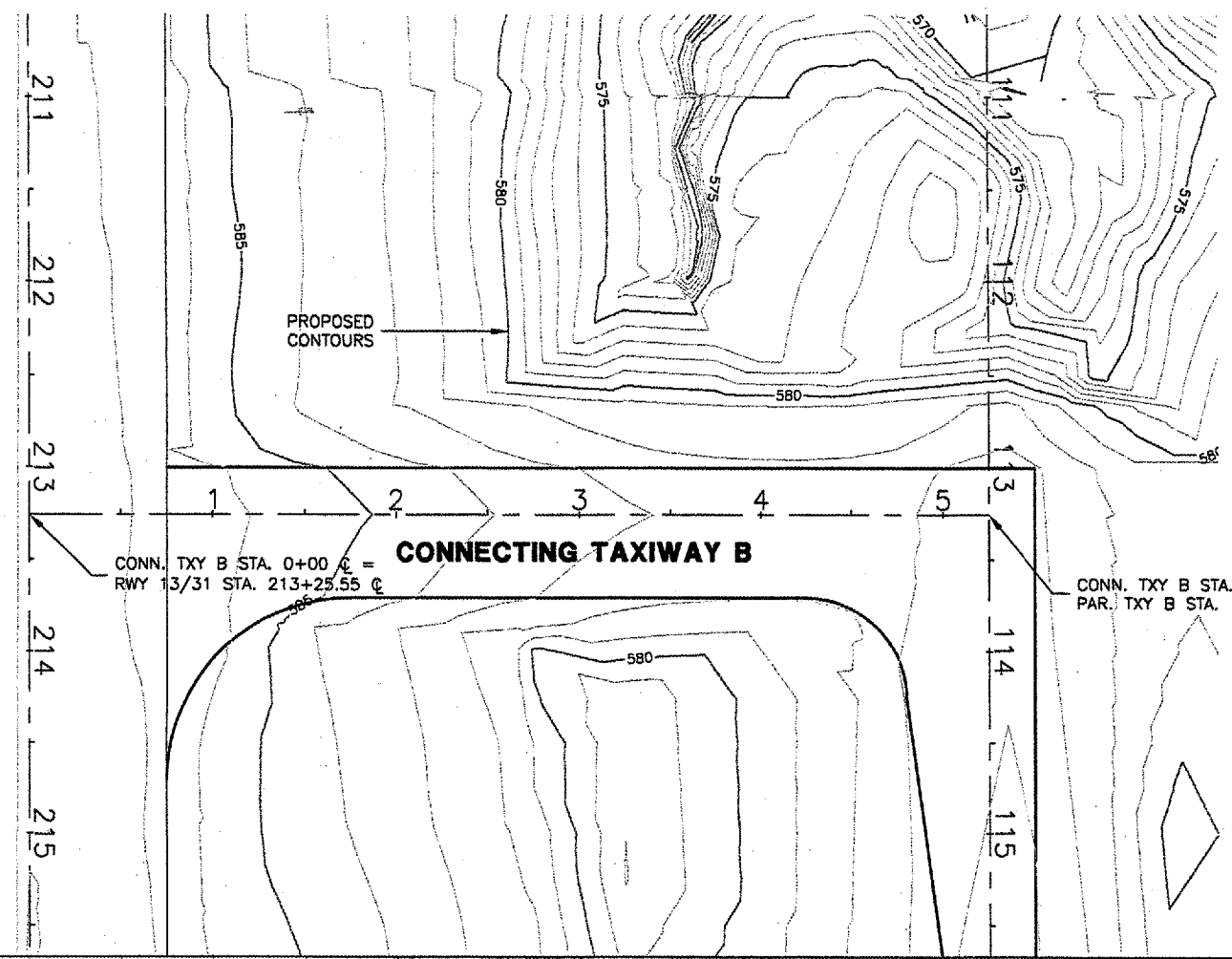


SPRINGFIELD AIRPORT AUTHORITY
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 SPRINGFIELD, ILLINOIS

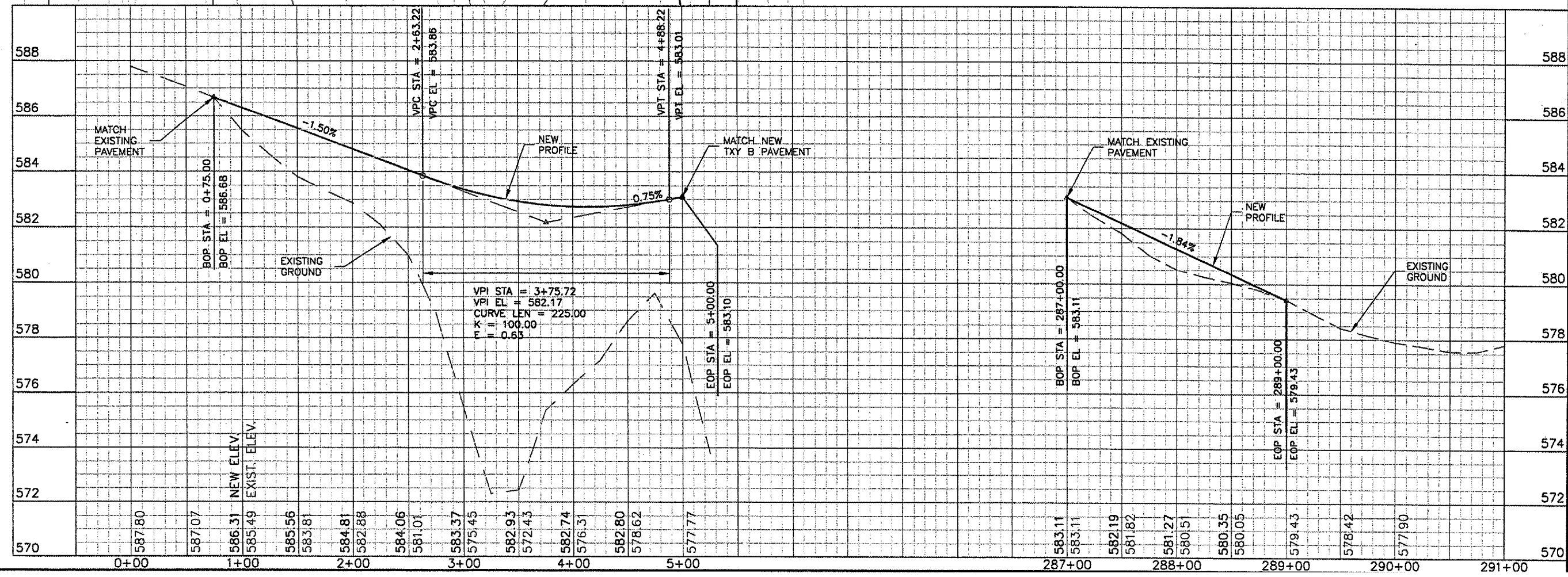
**CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 CONNECTING TAXIWAY B & BLAST PAD
 PLAN & PROFILES**

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APPROVED BY:	RLV
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JOB No:	05035-03
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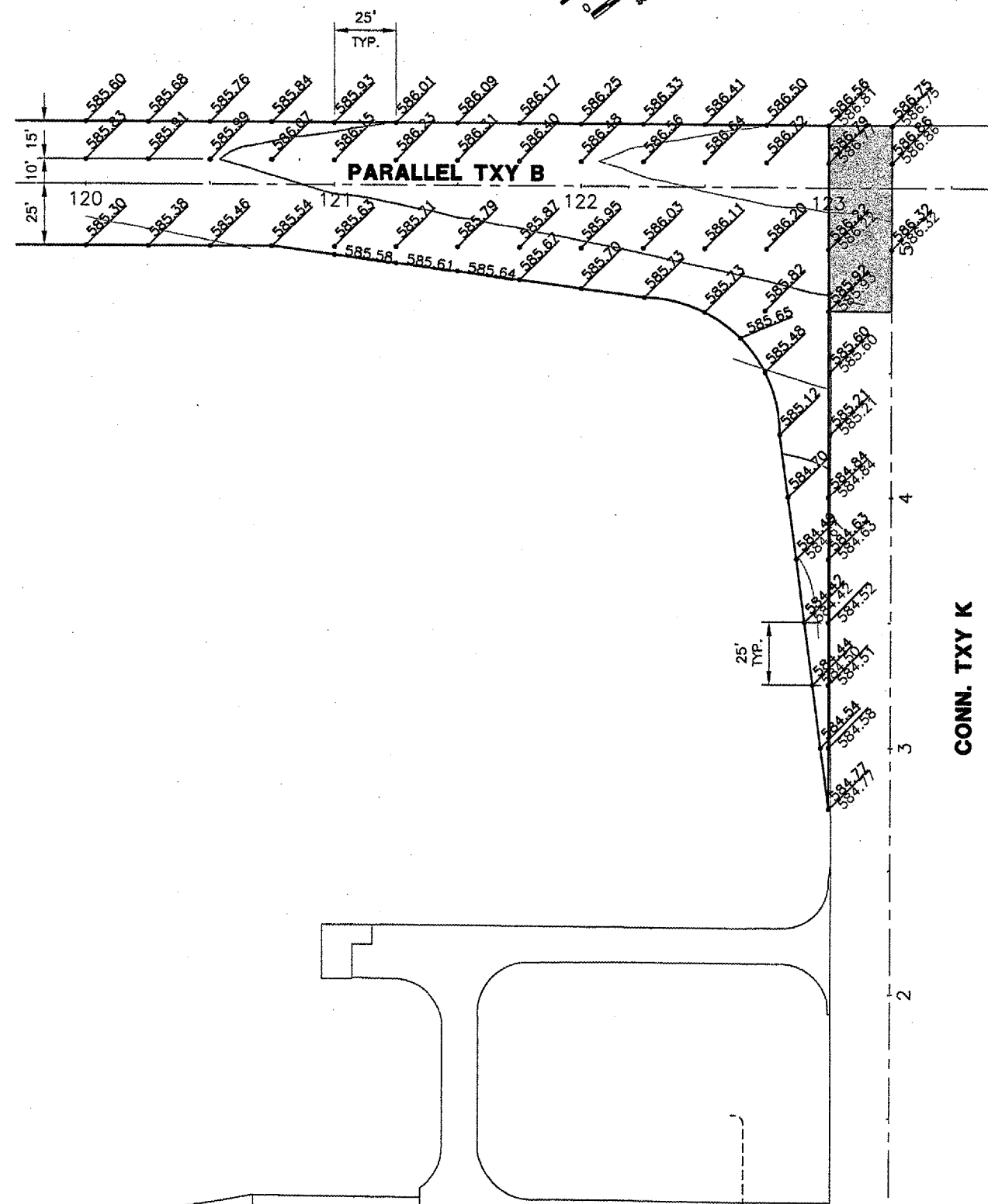
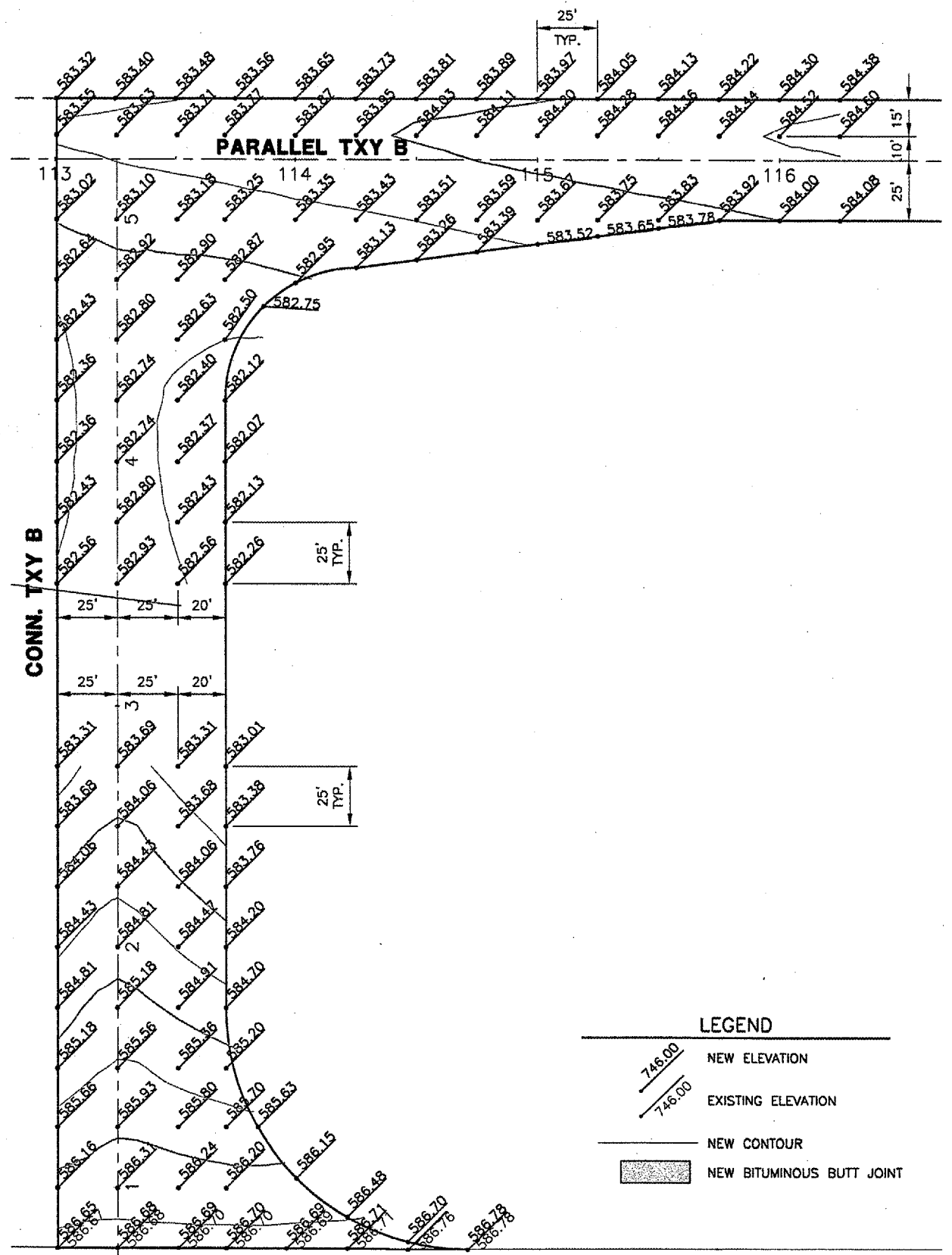
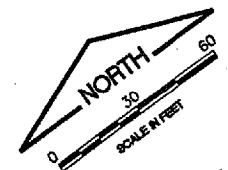


SCALES:
 1" = 50' HOR
 1" = 2' VER



CA003

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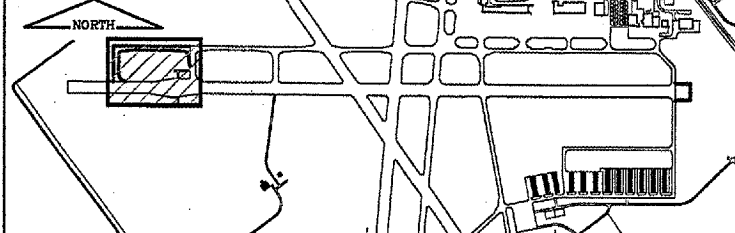
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	NEW ELEVATION
	EXISTING ELEVATION
	NEW CONTOUR
	NEW BITUMINOUS BUTT JOINT

RUNWAY 13/31

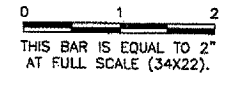
RUNWAY 13/31

KEYMAP



REVISIONS

NUMBER	BY	DATE



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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
STAKING PLAN 1

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CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42
SHEET	21 OF 79 SHEETS

CA003
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 tbinfo
 KEY
 ContourBase

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
STAKING PLAN 2

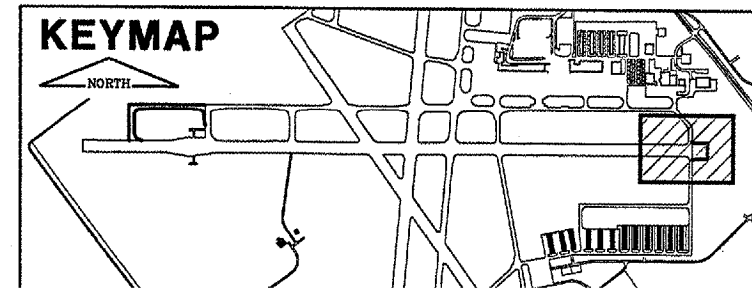
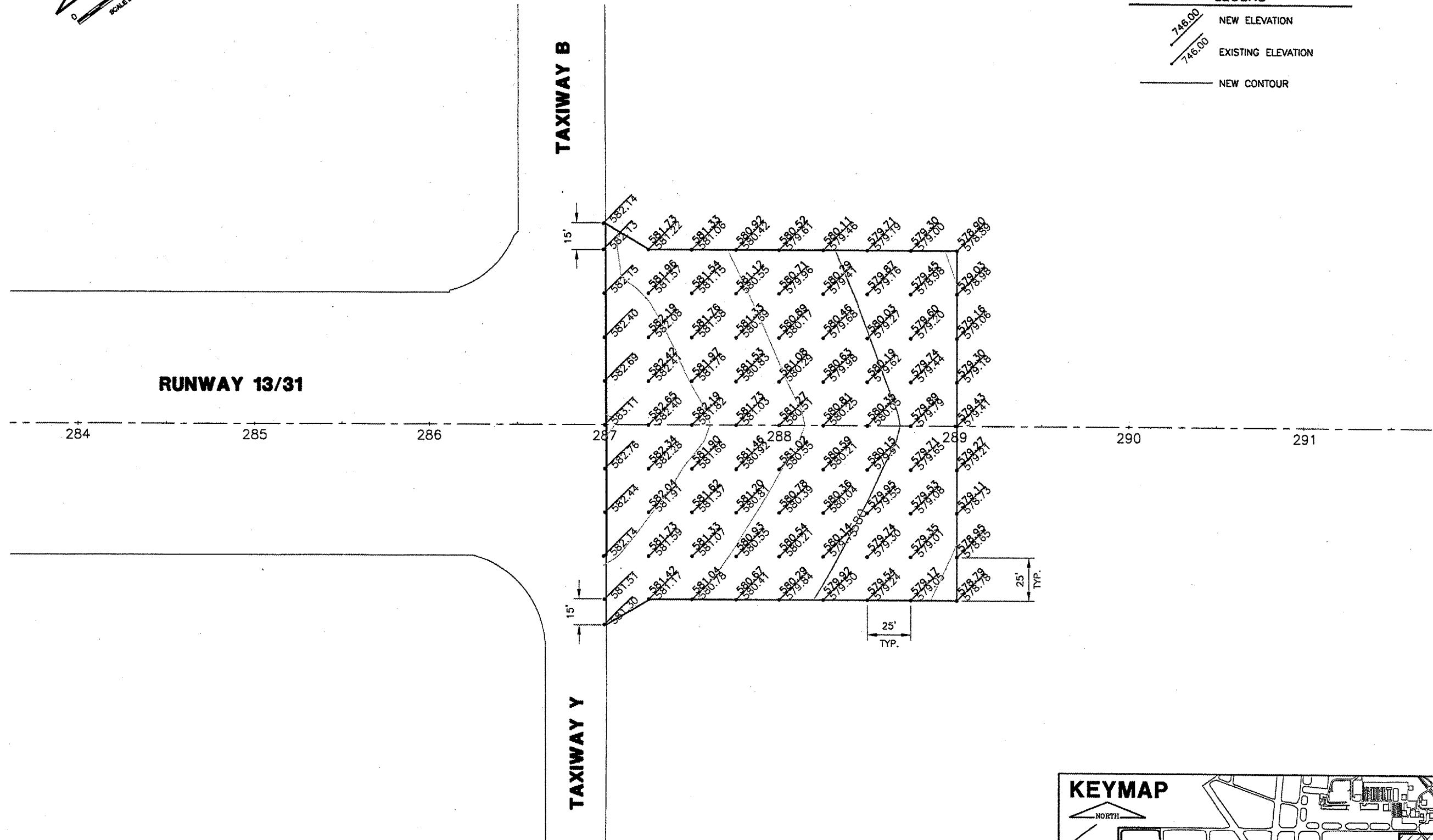
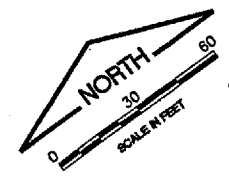
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DESIGN BY:	CBG
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL. PROJ. NO. SPI-3488	
AIP PROJ. NO. 3-17-0096-42	
SHEET 22 OF 70 SHEETS	

LEGEND

- NEW ELEVATION
- EXISTING ELEVATION
- NEW CONTOUR



REVISIONS		
NUMBER	BY	DATE

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 AT FULL SCALE (34x22).

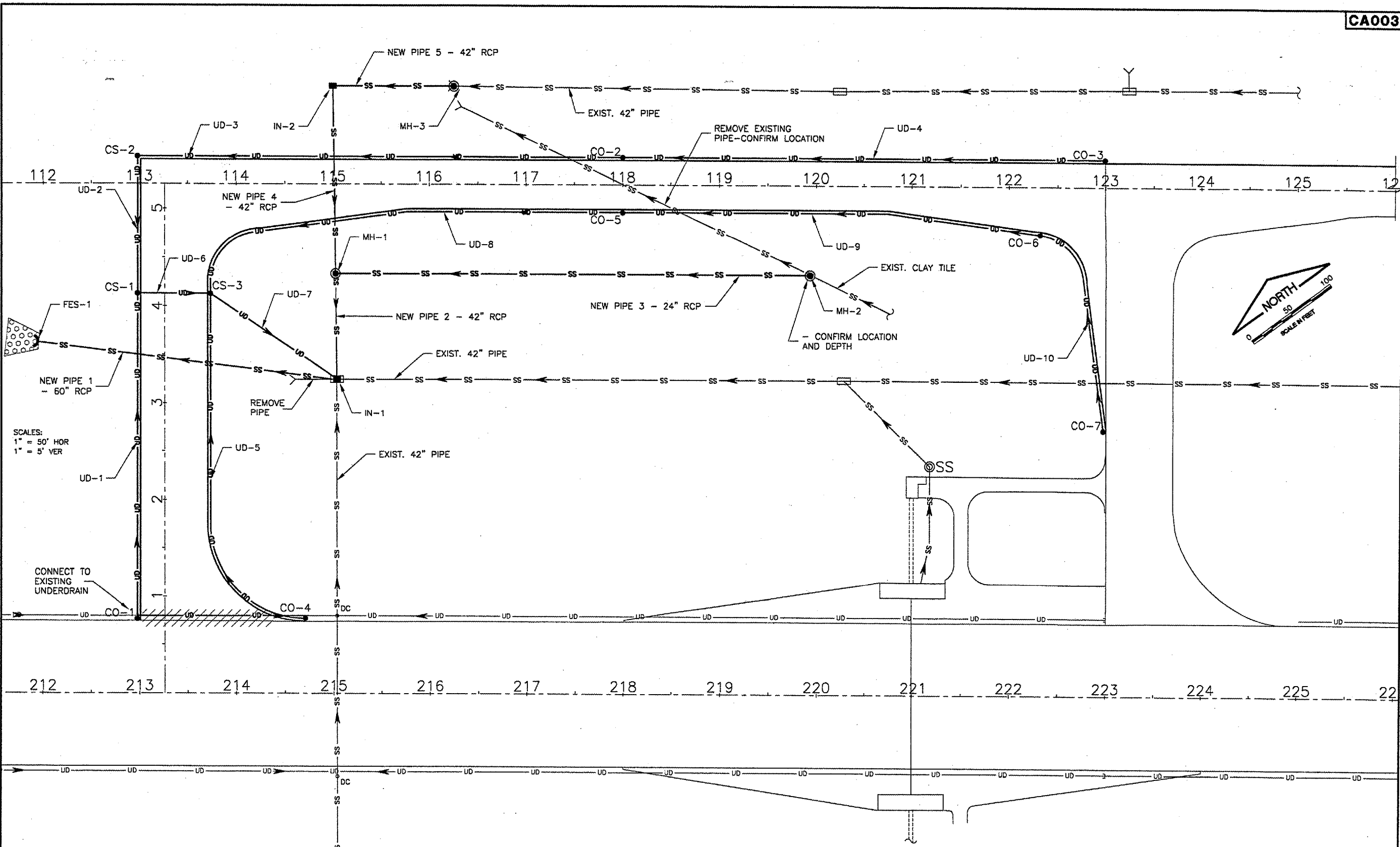


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
DRAINAGE PLAN

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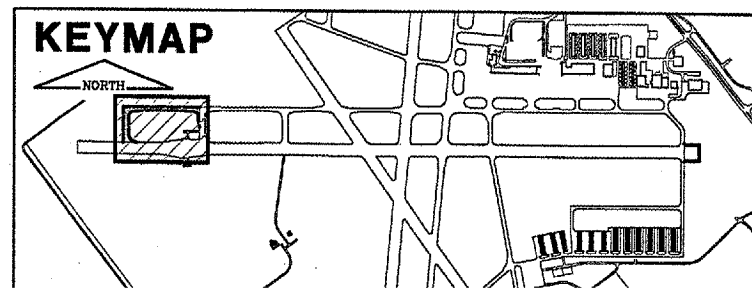
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IL PROJ. NO.	SPI-3488
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SHEET	23 OF 70 SHEETS



SCALES:
 1" = 50' HOR
 1" = 5' VER

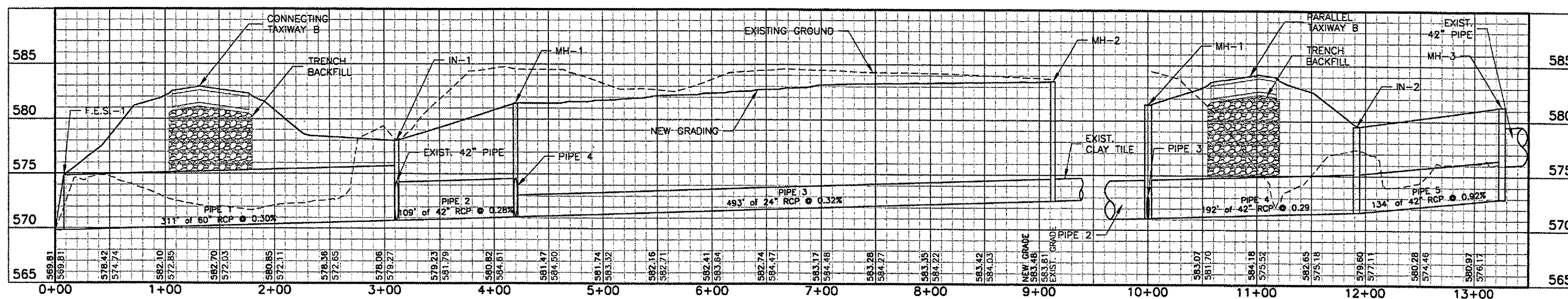
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	EXISTING PAVEMENT		NEW COLLECTION STRUCTURE		EXIST. F.E.S./HEADWALL
	NEW PAVEMENT		EXIST. CLEANOUT		NEW F.E.S.
	EXIST. STORM SEWER		NEW CLEANOUT		
	NEW STORM SEWER		EXIST. INLET		
	EXIST. UNDERDRAIN		NEW INLET		
	NEW UNDERDRAIN		EXIST. MANHOLE		
	EXIST. COLLECTION STRUCTURE		NEW MANHOLE		

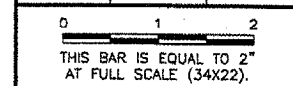



UNDERDRAIN STRUCTURE SCHEDULE									UNDERDRAIN PIPE SCHEDULE							
Structure	Alignment	Station	Offset	Northing	Easting	Rim	Invert	Type	Line	Structure	Structure	Up Inv	Down Inv	Length	Slope	Pipe
CO-1	Con Txy B	0+75.00	28' Lt.	1159703.84	2430810.94	586.54	582.54	3	UD-1	CO-1	CS-1	582.54	578.09	338	1.32%	Perf.
CO-2	Txy B	118+00.00	28' Lt.	1159789.06	2431496.96	584.67	580.67	3	UD-2	CS-2	CS-1	578.79	578.09	140	0.50%	Perf.
CO-3	Txy B	123+00.00	28' Lt.	1159493.54	2431899.32	586.29	582.29	1	UD-3	CO-2	CS-2	580.67	578.79	502	0.37%	Perf.
CO-4	Con Txy B	0+77.66	145' Rt.	1159600.98	2430950.05	586.65	582.65	1	UD-4	CO-3	CO-2	582.29	580.67	499	0.32%	Perf.
CO-5	Txy B	118+00.00	28' Rt.	1159743.93	2431463.81	584.37	580.37	3	UD-5	CO-4	CS-3	582.65	577.82	391	1.24%	Perf.
CO-6	Txy B	122+31.98	48.9' Rt.	1159471.38	2431799.61	585.52	581.52	3	UD-6	CS-1	CS-3	578.09	577.82	76	0.36%	Perf.
CO-7	Con Txy B	122+97.1	250' Rt.	1159270.14	2431733.18	584.75	581.75	1	UD-7	CS-3	IN-1	577.82	574.02	157	2.42%	Non
CS-1	Con Txy B	4+13.00	28' Lt.	1159973.66	2431009.12	582.09	578.09		UD-8	CO-5	CS-3	580.37	577.82	458	0.56%	Perf.
CS-2	Txy B	112+97.00	28' Lt.	1160086.50	2431092.00	582.79	578.79		UD-9	CO-6	CO-5	581.52	580.37	433	0.27%	Perf.
CS-3	Con Txy B	4+13.00	48' Rt.	1159928.67	2431070.37	581.82	577.82		UD-10	CO-7	CO-6	581.75	581.52	226	0.10%	Perf.


STORM SEWER STRUCTURE SCHEDULE									STORM SEWER PIPE SCHEDULE								
Structure	Station	Offset	Northing	Easting	Rim	Invert 1	Invert 2	Invert 3	Invert 4	Pipe	Structure	Structure	Up Inv	Down Inv	Length	Size	Slope
FES-1	111+94.83	162.2' Rt.	1159994.02	2430896.64	569.81	569.81				1	IN-1	FES-1	570.73	569.81	311	60"	0.30%
IN-1	115+03.75	200.2' Rt.	1159780.49	2431123.10	578.02	570.73	570.75	572.93	572.86	2	MH-1	IN-1	571.06	570.75	109	42"	0.28%
MH-1	115+02.41	91.6' Rt.	1159868.86	2431186.34	581.45	571.06	571.08	571.1		3	MH-2	MH-1	572.65	571.08	493	24"	0.32%
MH-2	119+95.00	91.6' Rt.	1159577.27	2431583.35	583.52	572.65	572.65			4	IN-2	MH-1	571.66	571.1	192	42"	0.29%
IN-2	115+00.00	100.0' Lt.	1160024.68	2431297.79	579.5	571.66	571.68			5	MH-3	IN-2	572.91	571.68	134	42"	0.92%
MH-3	116+33.84	100.2' Lt.	1159945.58	2431405.76	581.33	572.91	576.02										



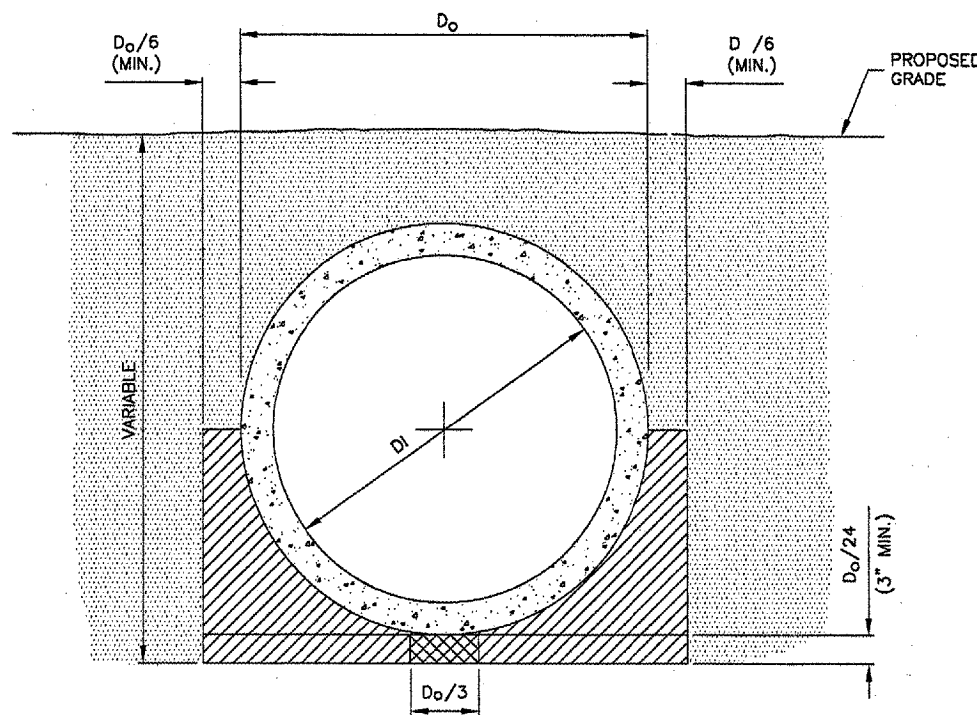
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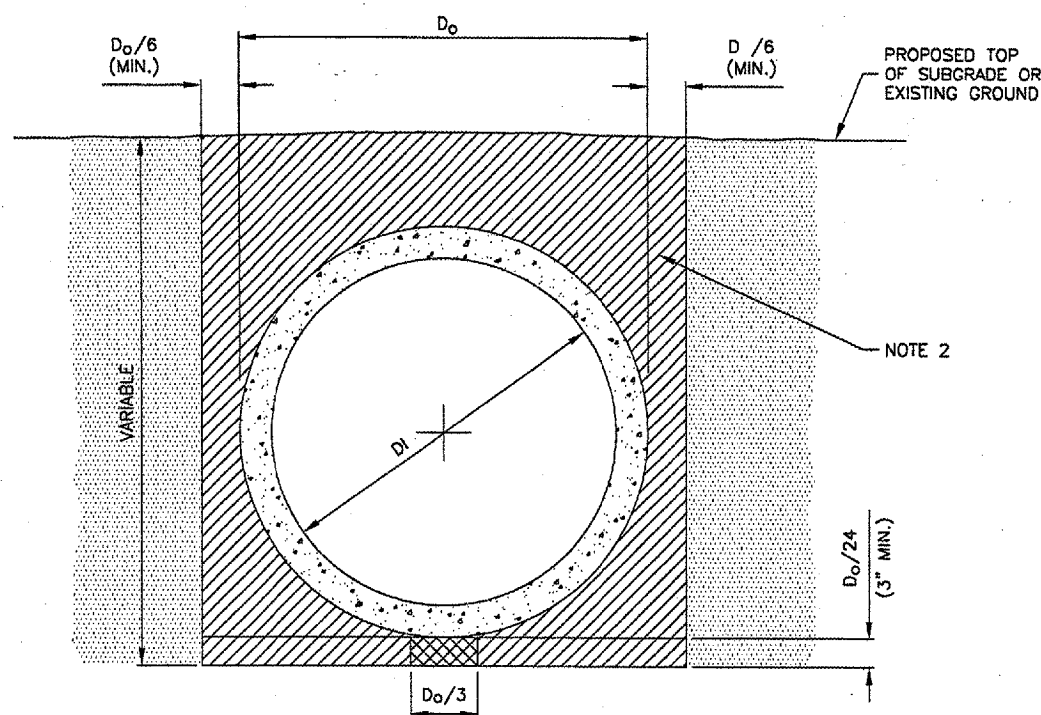

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ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
DRAINAGE PROFILE AND SCHEDULES


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DESIGN BY: CBG
 DRAWN BY: DPA
 CHECKED BY: *[Signature]*
 APPROVED BY: *[Signature]*
 DATE: 04/24/07
 JOB No: 05035-03
 IL PROJ. NO. SPI-3488
 AIP PROJ. NO. J-17-0096-42
 SHEET 24 OF 79 SHEETS



STANDARD TRENCH INSTALLATION
 NON-PAVED AREA
 N.T.S.



STANDARD TRENCH INSTALLATION
 PROPOSED PAVED AREAS
 N.T.S.

LEGEND

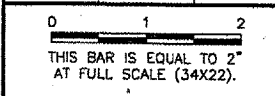
- DRAINAGE CONDUIT MATERIAL-CONCRETE
- MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
- HAUNCH AND OUTER BEDDING COMPACTION- TO ENGINEER'S SATISFACTION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
- LOWER SIDE AND OVERFILL COMPACTION- SAME AS EMBANKMENT REQUIREMENTS

D_o PIPE OUTSIDE DIAMETER
 D_i PIPE INSIDE DIAMETER

- NOTES**
- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
 - BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.

REVISIONS

NUMBER	BY	DATE



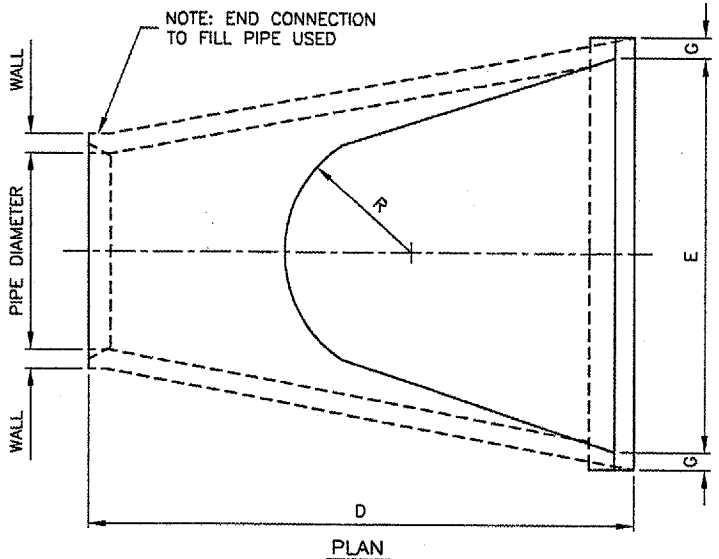
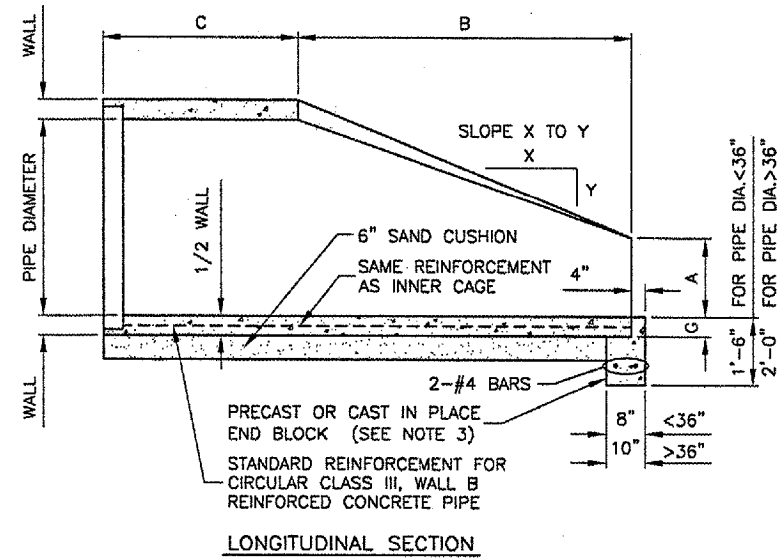
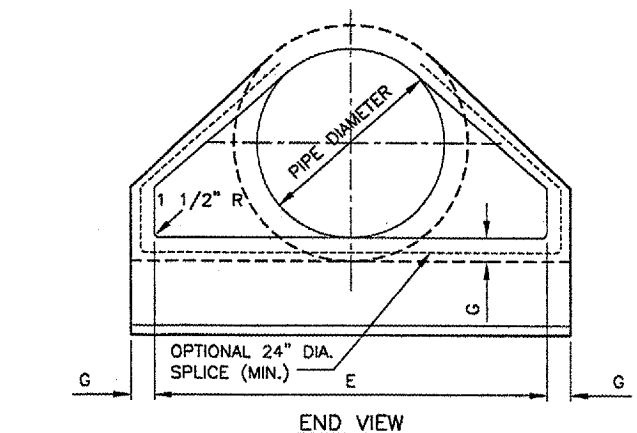
DIMENSIONS - TABLE 1

PIPE DIA.	APPROX WT.(lbs.)	WALL	A	B	C	D	E	G	R	SLOPE
12"	530	2"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	2"	9"	3:1
15"	740	2 1/4"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2 1/4"	11"	3:1
18"	990	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2 1/2"	12"	3:1
21"	1280	2 3/4"	9"	2'-11"	3'-2"	6'-1"	3'-6"	2 3/4"	13"	3:1
24"	1520	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3"	14"	3:1
27"	1930	3 1/4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	4'-6"	3 1/4"	14 1/2"	3:1
30"	2190	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	3 1/2"	15"	3:1
33"	3200	3 3/4"	1-1 1/2"	4'-10 1/2"	3'-3 1/4"	8'-1 3/4"	5'-6"	3 3/4"	17 1/2"	3:1
36"	4100	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	4"	20"	3:1
42"	5380	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4 1/2"	22"	3:1
48"	6550	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	5"	22"	3:1
54"	8240	5 1/2"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	5 1/2"	24"	2.4:1
60"	8730	6"	2'-11"	5'-0"	3'-3"	8'-3"	8'-0"	5"	*	2:1
66"	10710	6 1/2"	2'-6"	6'-0"	2'-3"	8'-3"	8'-6"	5 1/2"	*	2:1
72"	12520	7"	3'-0"	6'-6"	1'-9"	8'-3"	9'-0"	6"	*	1.86:1
78"	14770	7 1/2"	3'-0"	7'-6"	1'-9"	9'-3"	9'-6"	6 1/2"	*	1.82:1
84"	18160	8"	3'-0"	7'-6 1/2"	1'-9"	9'-3 1/2"	10'-0"	6 1/2"	*	1.5:1

* RADIUS AS FURNISHED BY MANUFACTURER.

NOTES

- PRECAST CONCRETE FLARED END SECTIONS SHALL CONFORM TO THE APPLICABLE REQUIREMENTS OF AASHTO M-170 CLASS III, WALL B REINFORCED CONCRETE PIPE.
- PRECAST CONCRETE FLARED END SECTION FOR PIPE DIAMETER REQUIRED SHALL BE AS INDICATED ON DETAIL PLAN FOR EACH INDIVIDUAL INSTALLATION.
- THE END BLOCK SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE FLARED END SECTION. THE END BLOCK SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 701.



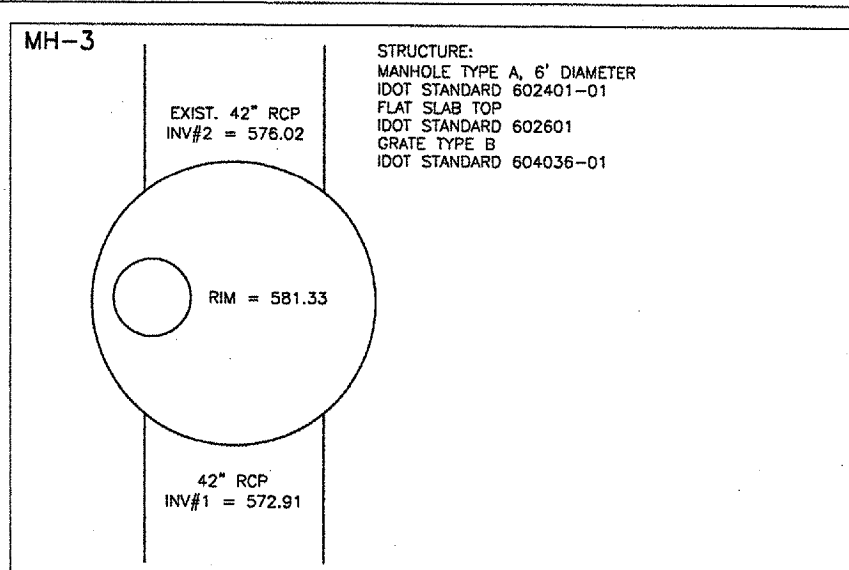
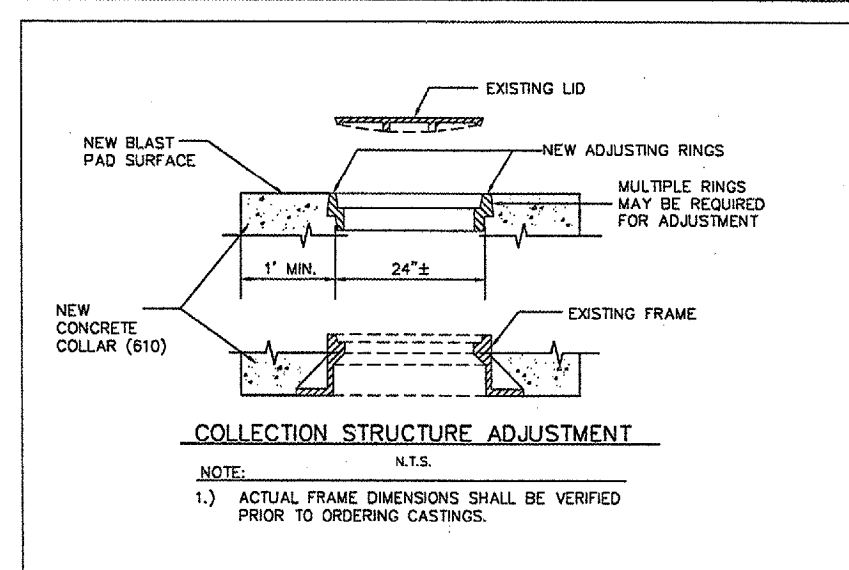
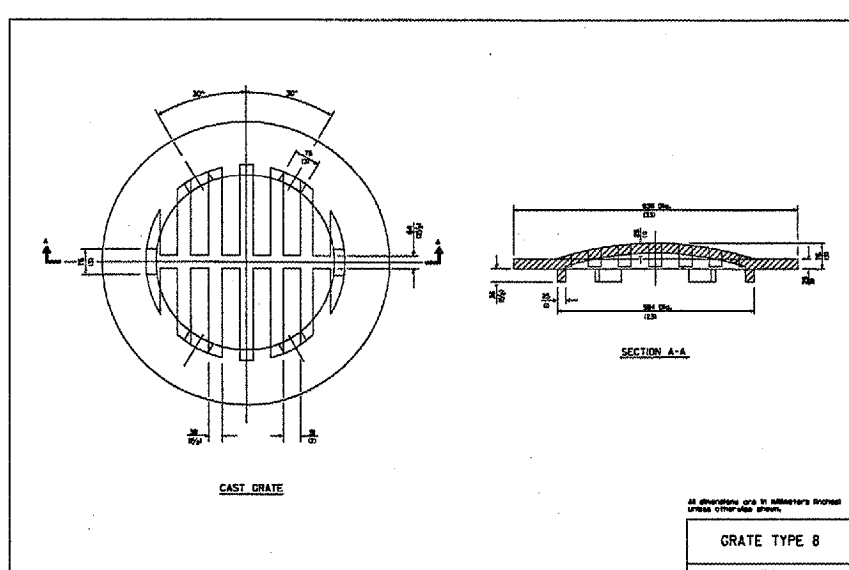
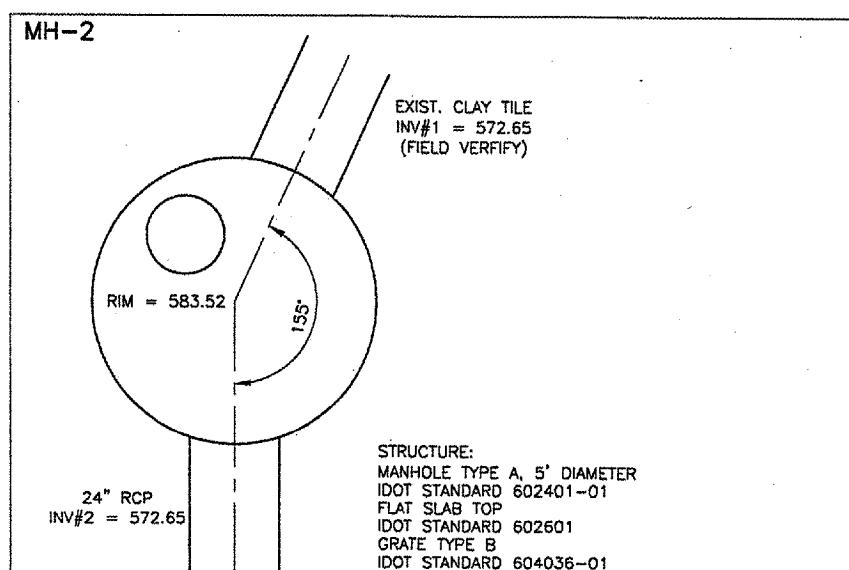
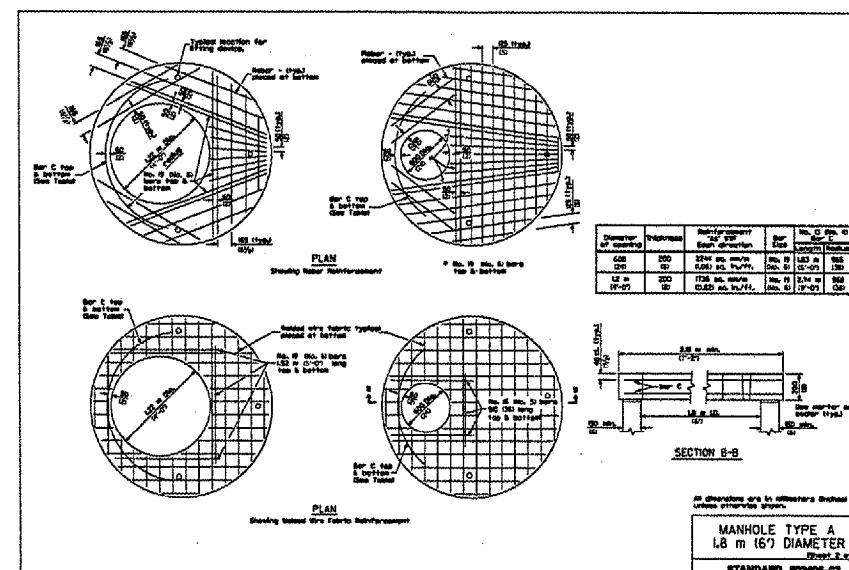
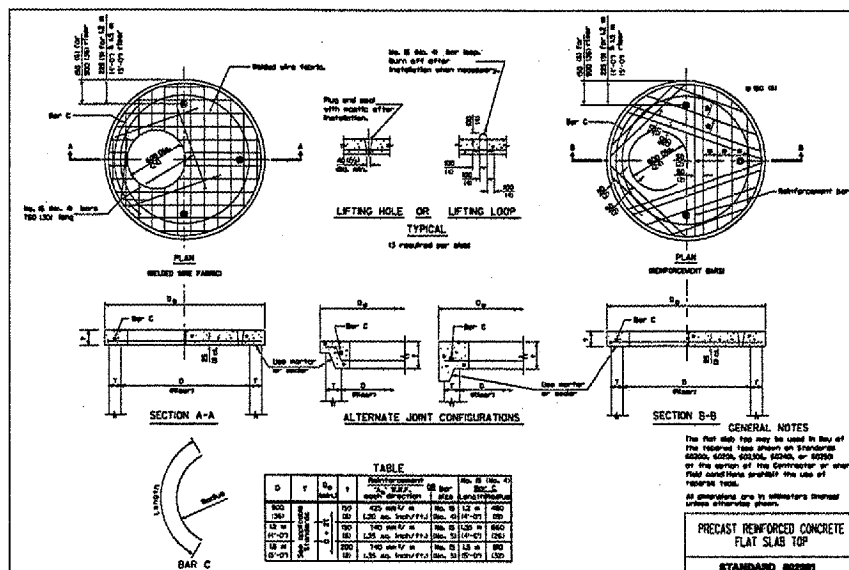
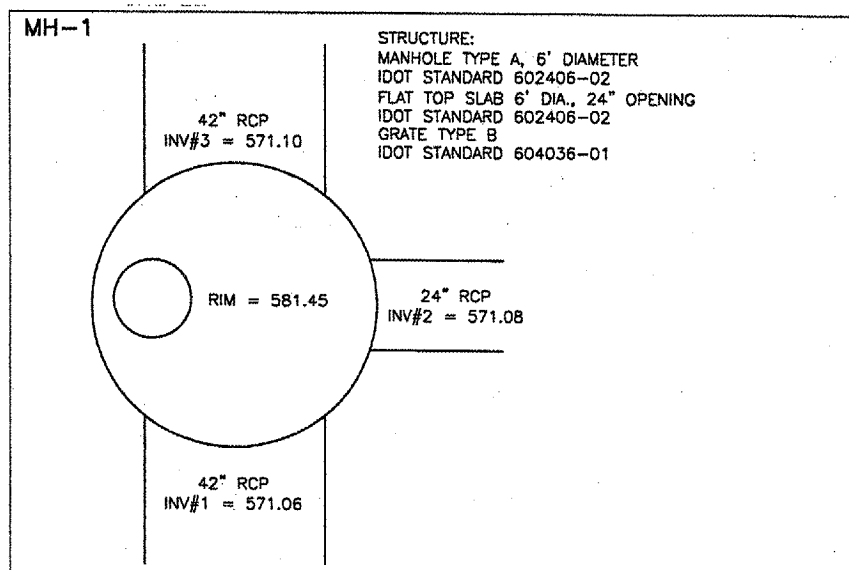
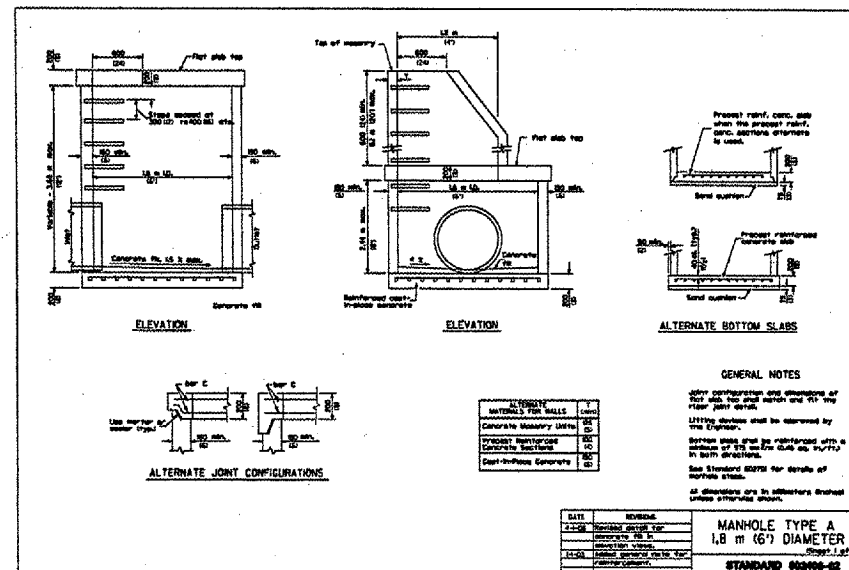
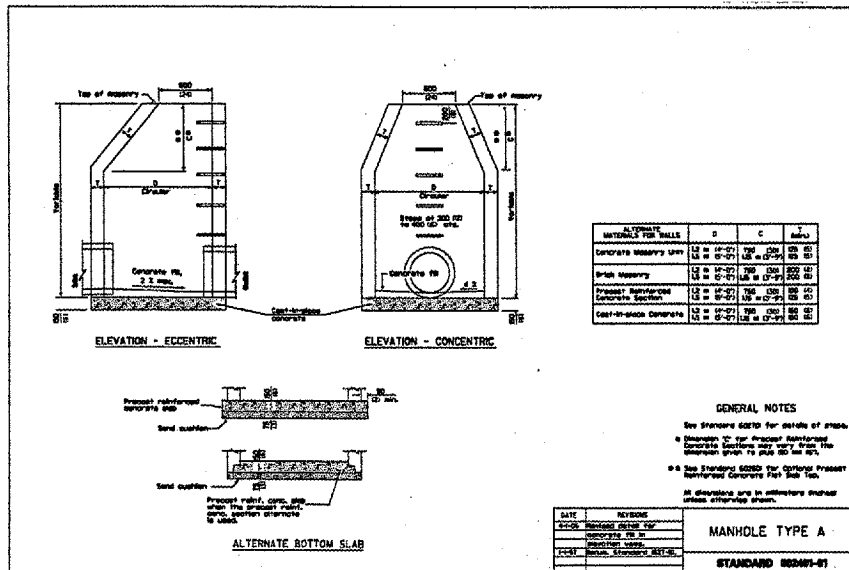
PRECAST REINFORCED CONCRETE FLARED END SECTION
 (I.D.O.T. STD. NO. 542301)
 N.T.S.

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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
DRAINAGE DETAILS 1

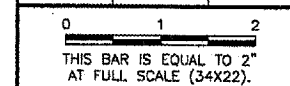
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DESIGN BY: RLV
 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03
 ILL. PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 25 OF 79 SHEETS



REVISIONS

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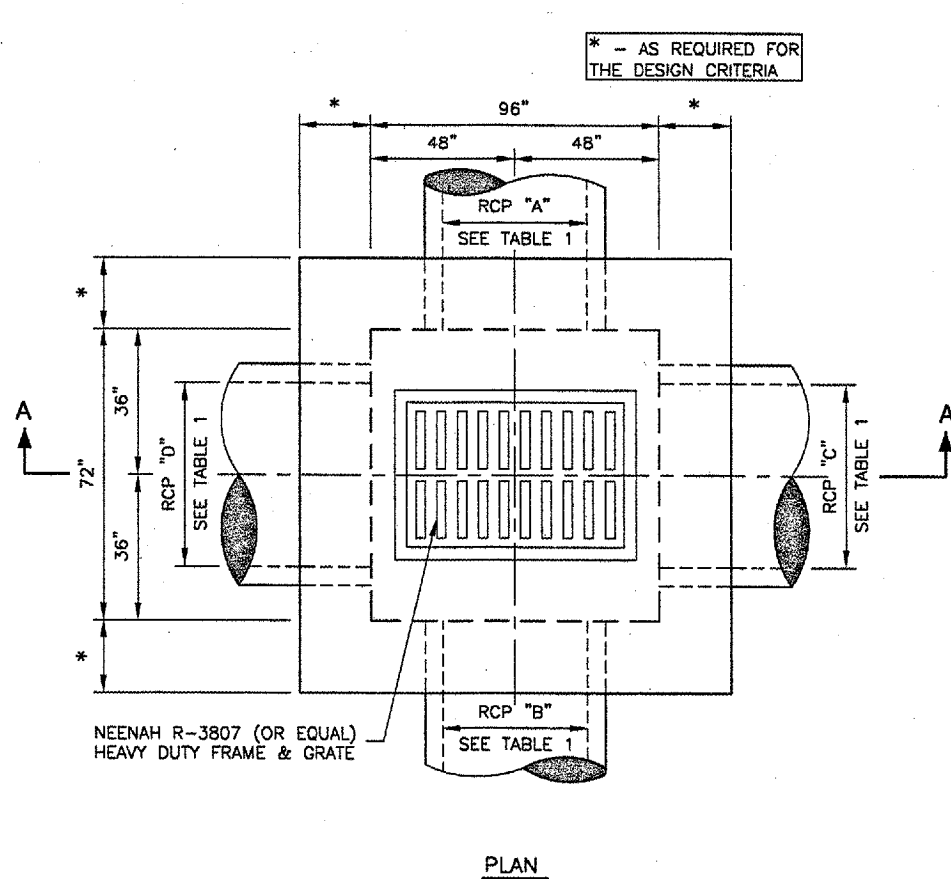
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS

DRAINAGE DETAILS 2

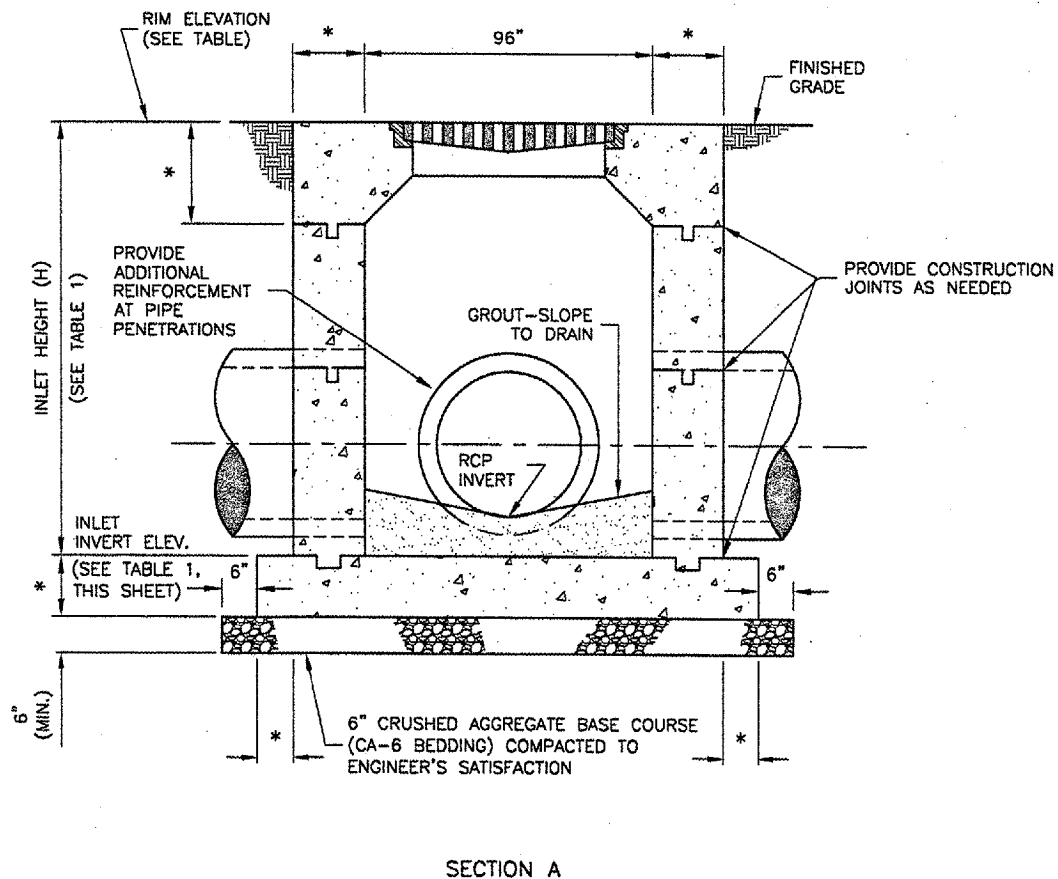
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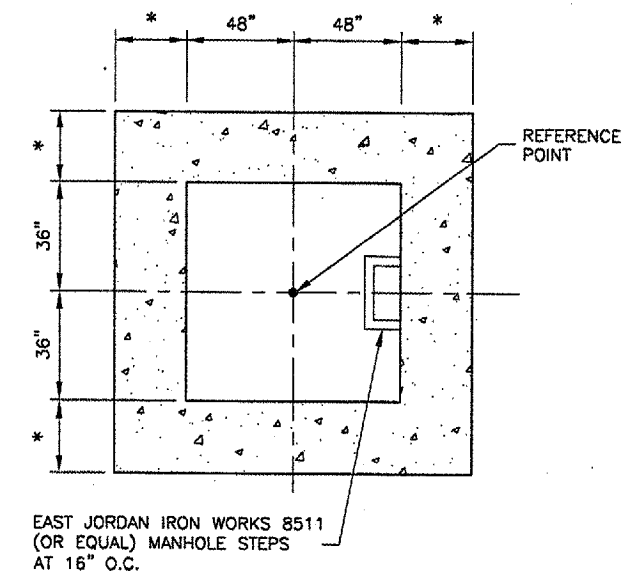
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 APPROVED BY: RLV
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 JOB No: 05035-03
 ILL. PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 26 OF 70 SHEETS



PLAN



SECTION A



WALL PLAN
N.T.S.

INLET DETAILS
N.T.S.

DESIGN CRITERIA

1. THE INLET SHALL BE CONSTRUCTED TO MEET OR EXCEED THE FOLLOWING LOADINGS:
 - A. EARTHLOAD = 2 FT. FILL AT 130 LBS./FT.²
 - B. SURCHARGE = 2 FT. FILL AT 130 LBS./FT.²
 - C. LIVE LOAD = A.A.S.H.T.O. HS-20 TRUCK WITH 20% IMPACT
 - D. $f'_c = 4,500$ P.S.I.
 - E. $f_y = 60,000$ P.S.I.
 - F. ULTIMATE STRENGTH DESIGN METHOD
 THE SUPPLIER SHALL PROVIDE CERTIFICATION THAT THE INLETS MEET OR EXCEED THESE REQUIREMENTS PRIOR TO INSTALLATION.

TABLE 1		
INLET	IN-1	IN-2
STA. OFFSET	115+05.75 200.22' RT.	115+00.00 100.00' RT.
RIM ELEVATION	578.02	579.50
RCP DIA. "A"	60" RCP	42" RCP
RCP DIA. "B"	42" RCP	-
RCP DIA. "C"	42" RCP	42" RCP
RCP DIA. "D"	42" RCP	-
RCP INVERT "A"	570.73	571.64
RCP INVERT "B"	572.86	-
RCP INVERT "C"	570.75	571.65
RCP INVERT "D"	572.93	-
INLET INVERT	570.23	571.26
INLET HEIGHT (H)	7.79	8.24

GENERAL NOTES

1. ALL REINFORCEMENT BARS SHALL CONFORM TO ASTM A615 GRADE 60 AND SHALL BE CLEAN AND FREE OF GREASE, SCALING RUST, AND OTHER FOREIGN MATERIALS.
2. INLET MAY BE CONSTRUCTED BY CAST-IN-PLACE CONCRETE OR PRE-CAST CONCRETE.
3. CAST-IN-PLACE CONCRETE AND PRE-CAST CONCRETE FOR THE INLETS SHALL HAVE A MINIMUM 14 DAYS COMPRESSIVE STRENGTH OF 3500 P.S.I.
4. THE CONTRACTOR SHALL VERIFY THE LOCATION AND SIZE OF PIPE OPENINGS WITH TABLE 1, THIS SHEET.
5. ALL FOOTING EXCAVATIONS SHALL BE CLEAN FREE OF DEBRIS, STANDING WATER AND LOOSE FOIL AND SHALL BE INSPECTED BY THE ENGINEER PRIOR TO THE PLACEMENT OF CONCRETE OR SUBBASE.
6. CONCRETE SHALL NOT BE PLACED OVER FROZEN OR MUDDY SOIL.
7. DIMENSIONS SHOWN ARE MINIMUMS.

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34x22).

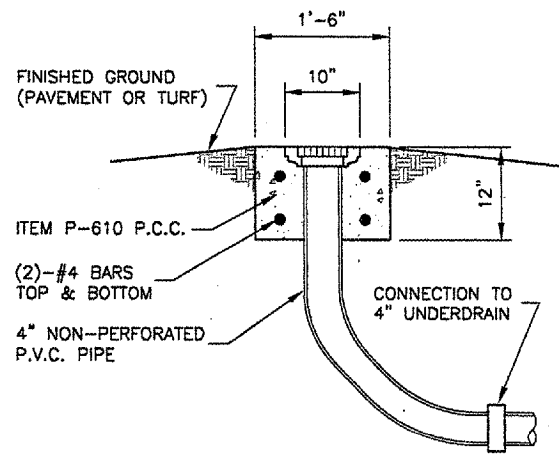
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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
INLET - SPECIAL DETAIL

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 DRAWN BY: CMT
 CHECKED BY: RLV
 APPROVED BY: RLV
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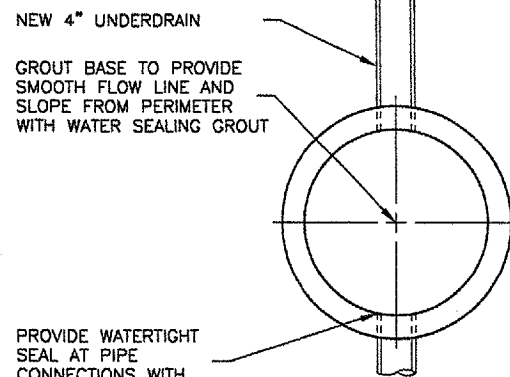
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SIDE VIEW

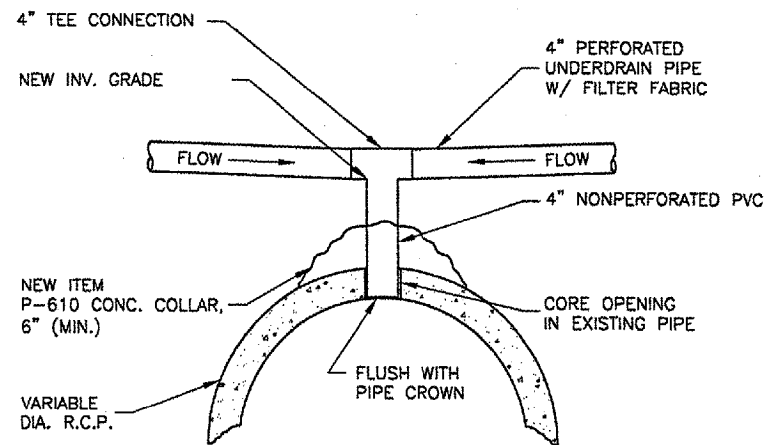
UNDERDRAIN CLEAN-OUT DETAIL - TYPE 1

N.T.S.



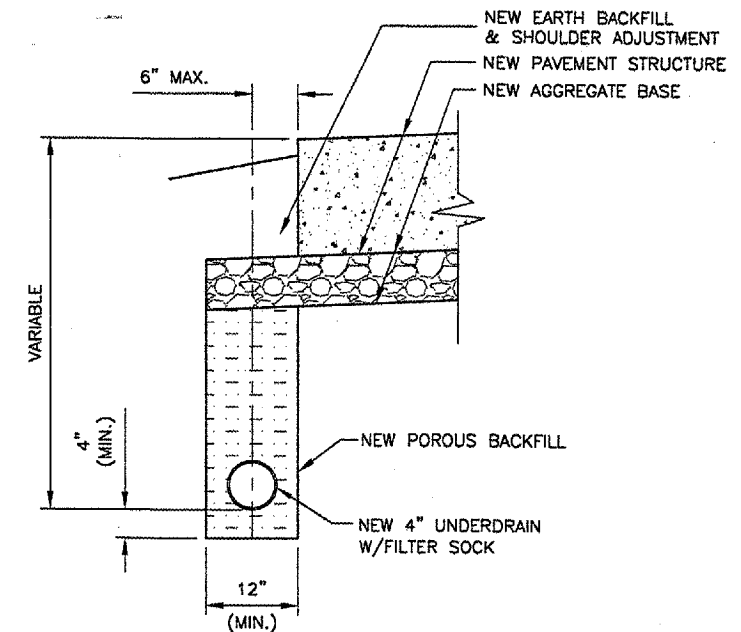
PLAN

PROVIDE WATERTIGHT SEAL AT PIPE CONNECTIONS WITH WATER SEALING GROUT



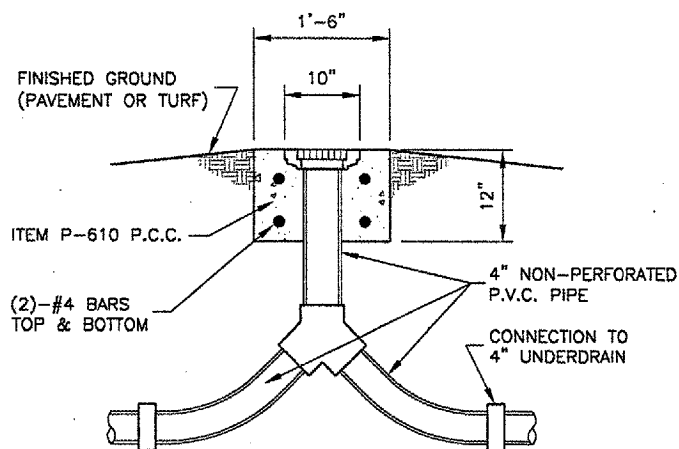
UNDERDRAIN DIRECT TOP CONNECTION DETAIL

N.T.S.



TYPICAL UNDERDRAIN DETAIL PAVEMENT EDGE

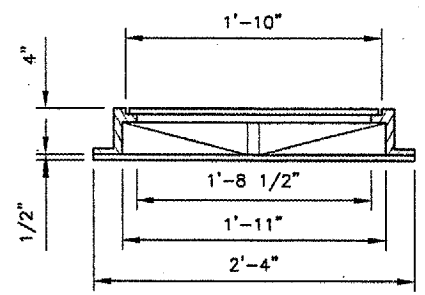
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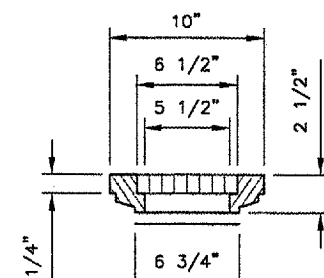
SIDE VIEW

UNDERDRAIN CLEAN-OUT DETAIL - TYPE 2

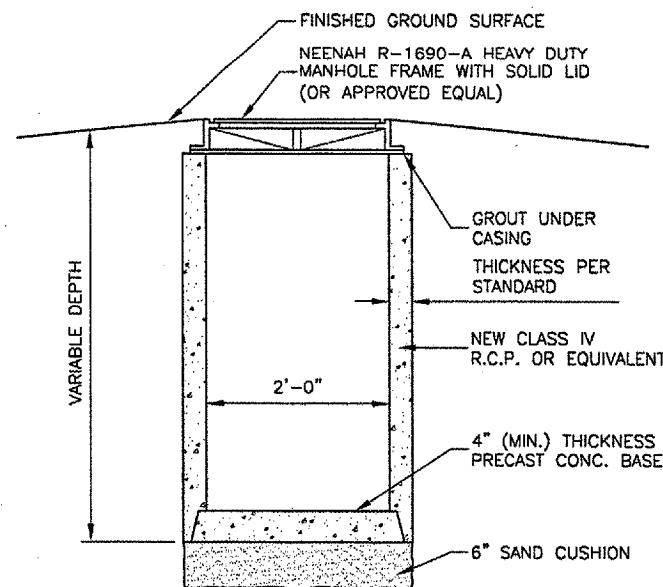
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FRAME AND LID



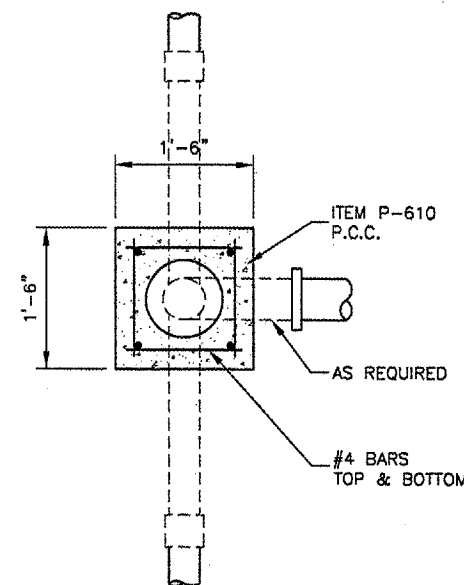
FRAME AND COVER



SIDE VIEW

UNDERDRAIN COLLECTION STRUCTURE DETAIL

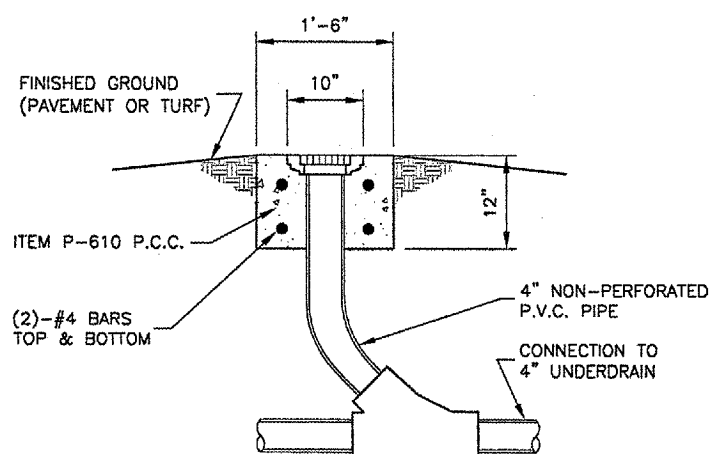
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PLAN

UNDERDRAIN CLEAN-OUT DETAILS

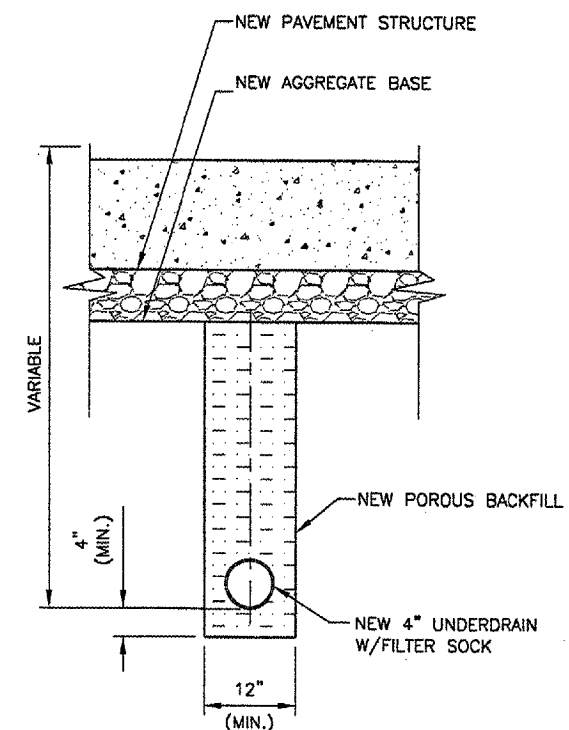
N.T.S.



SIDE VIEW

UNDERDRAIN CLEAN-OUT DETAIL - TYPE 3

N.T.S.



TYPICAL UNDERDRAIN DETAIL BELOW PAVEMENT

N.T.S.

NOTES

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE TO THE UNDERDRAIN SHALL BE REMOVED PRIOR TO THE PLACEMENT OF PROPOSED POROUS BACKFILL. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



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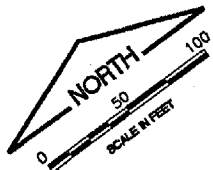
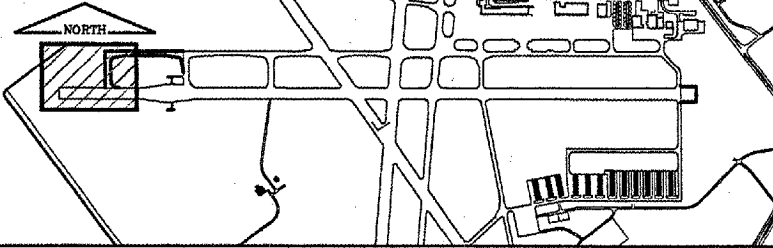
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 UNDERDRAIN DETAILS

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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42

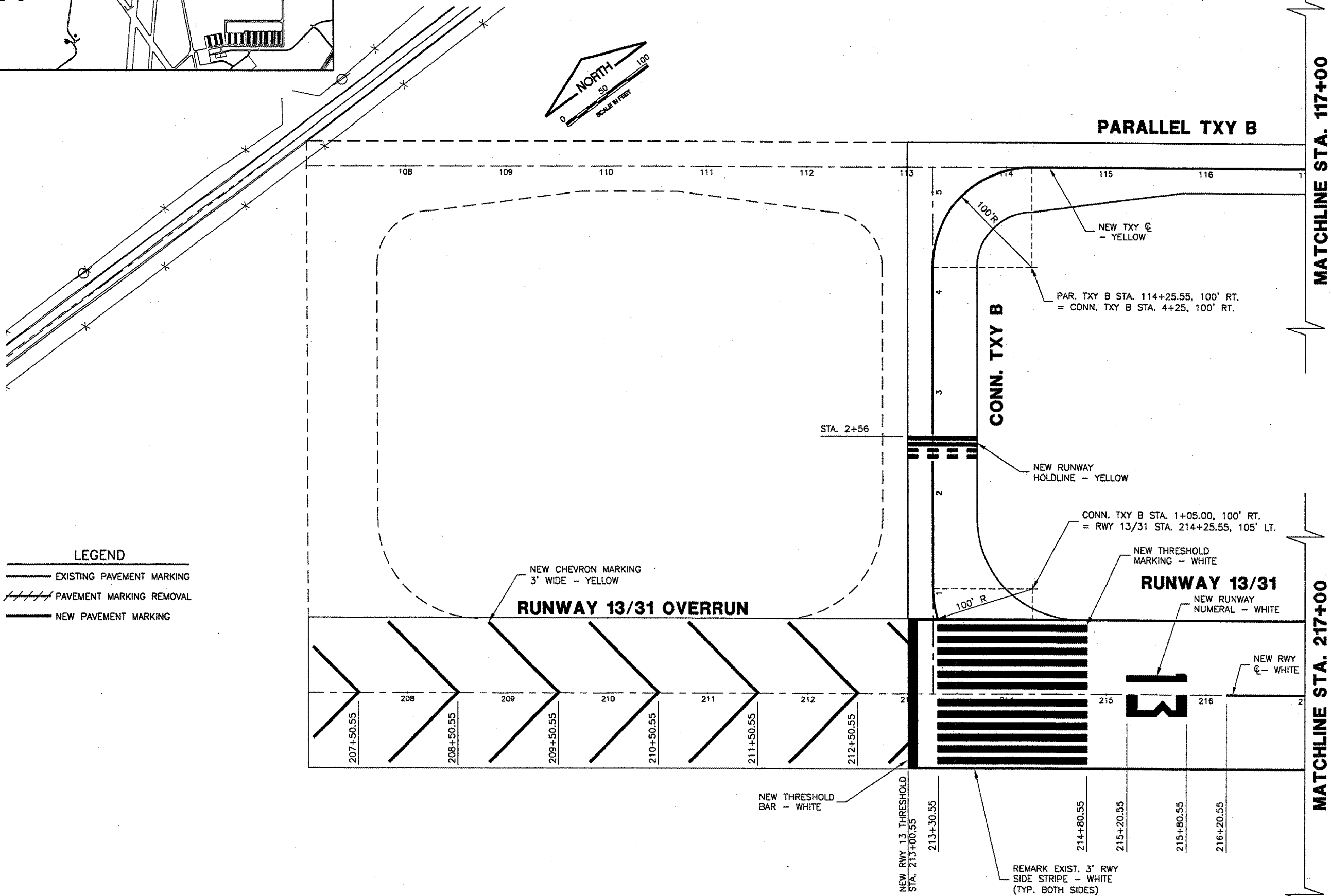
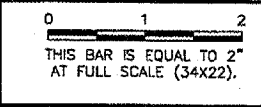
KEYMAP



CA003

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 UPDATE BY: Allan Draughon
 PLOT DATE: 5/2/2007 5:59 PM
 Capital_Base
 Base_ExistTopo_TxyB
 tbinfo
 BASE_PROPGEO
 BASE_PROPMARK
 KEY

REVISIONS		
NUMBER	BY	DATE



LEGEND

	EXISTING PAVEMENT MARKING
	PAVEMENT MARKING REMOVAL
	NEW PAVEMENT MARKING

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
MARKING PLAN 1

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 CONSULTING ENGINEERS
 License No. 84-00865

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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03

MATCHLINE STA. 117+00

MATCHLINE STA. 217+00

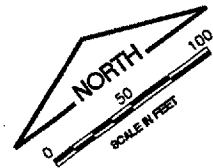
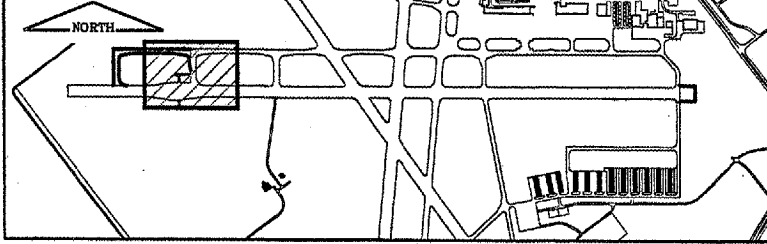
MATCHLINE STA. 230+00

PARALLEL TXY B

CONN. TXY K

RUNWAY 13/31

KEYMAP



CA003

K:\Springfield\0503503\Draw\Sheets
 FILE: MARK02.dwg
 UPDATE BY: Allen Draughon
 PLOT DATE: 5/2/2007 5:59 PM
 Capital_Base
 Base_ExistTopo_TxyB
 title
 BASE_PROPOSED
 BASE_PROPMARK
 KEY

REVISIONS

NUMBER	BY	DATE

0 1 2

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

PAR. TXY B STA. 122+35.55, 100' RT.
= CONN. TXY K STA. 4+25, 90' LT.

REMARK EXIST.
TXY ☉ - YELLOW

STA. 2+56

NEW RUNWAY
HOLDLINE - YELLOW

CONN. TXY K STA. 1+05.00, 110' RT.
= RWY 224+35.55, 105' LT.

NEW AIMING POINT
MARKING - WHITE

NEW RWY ☉
- WHITE

REMARK EXIST. 3' RWY
SIDE STRIPE - WHITE
(TYP. BOTH SIDES)

NEW TOUCHDOWN ZONE
MARKING - WHITE

NEW TOUCHDOWN ZONE
MARKINGS - WHITE

REMARK EXISTING
BAK CABLE MARKING
- YELLOW

LEGEND

- EXISTING PAVEMENT MARKING
- PAVEMENT MARKING REMOVAL
- NEW PAVEMENT MARKING



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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 MARKING PLAN 2

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DRAWN BY: DPA

CHECKED BY: RLV

APPROVED BY: RLV

DATE: 04/24/07

JOB No: 05035-03

IL PROJ. NO. SPI-3488

AIP PROJ. NO. 3-17-0096-42

SHEET 30 OF 79 SHEETS

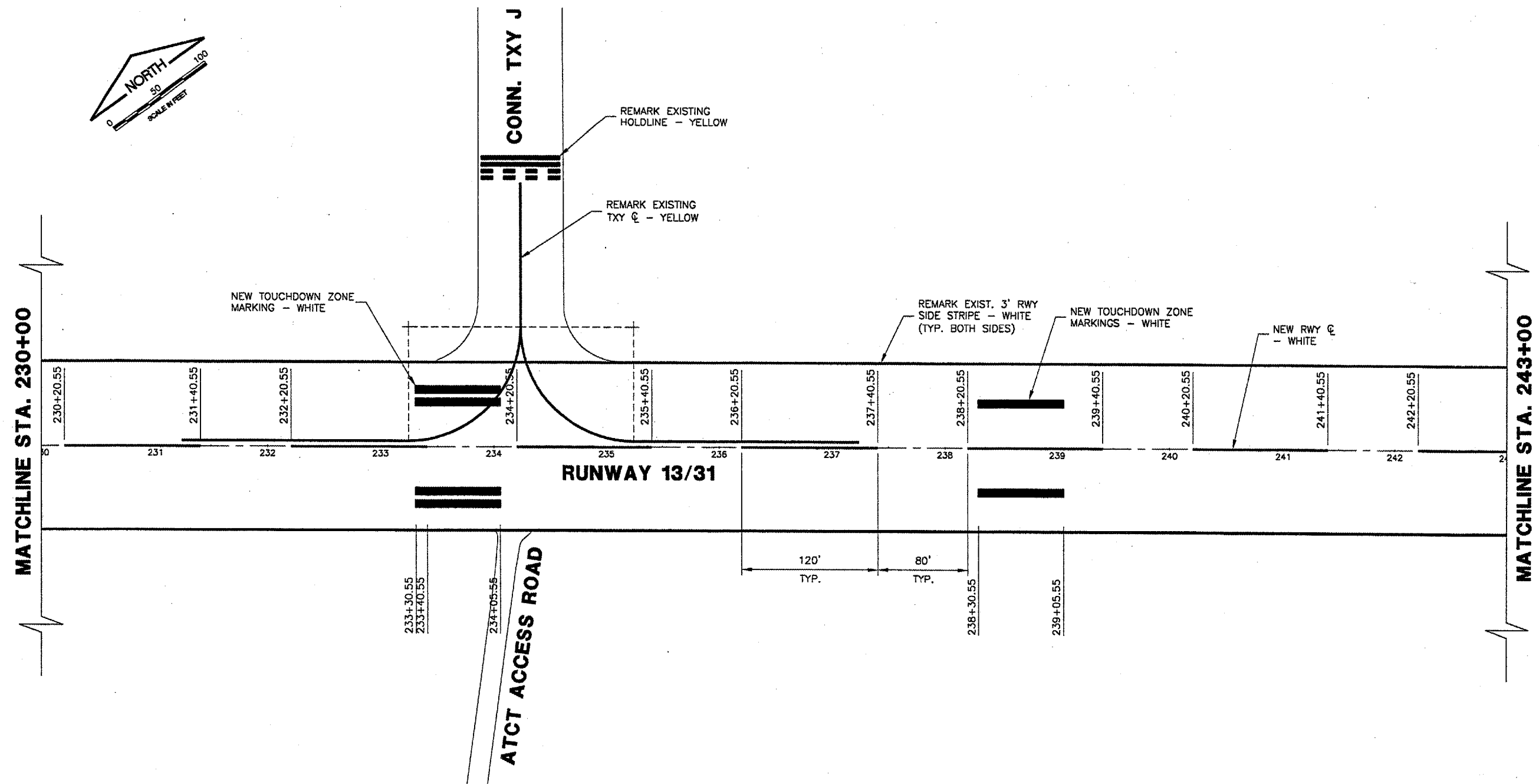
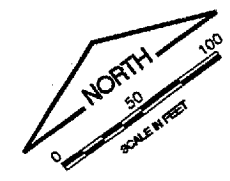
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 UPDATE BY: Allan Draughon
 PLOT DATE: 5/2/2007 5:59 PM
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 Capital_Base
 tbinfo
 BASE_PROPGE0
 BASE_PROPMARK

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



MATCHLINE STA. 230+00

MATCHLINE STA. 243+00

RUNWAY 13/31

ATCT ACCESS ROAD

CONN. TXY J

LEGEND

- EXISTING PAVEMENT MARKING
- PAVEMENT MARKING REMOVAL
- NEW PAVEMENT MARKING

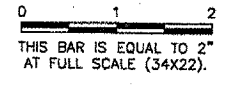
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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 MARKING PLAN 3

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CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42

REVISIONS		
NUMBER	BY	DATE



MATCHLINE STA. 243+00

MATCHLINE STA. 256+00

MATCHLINE STA. 256+00

MATCHLINE STA. 269+00

RUNWAY 13/31

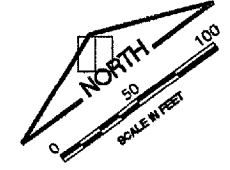
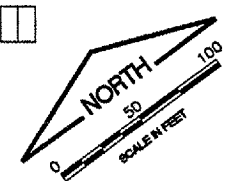
RUNWAY 13/31

RWY 18/36

RWY 4/22

TAXIWAY A

TAXIWAY C



LEGEND

	EXISTING PAVEMENT MARKING
	PAVEMENT MARKING REMOVAL
	NEW PAVEMENT MARKING

REMARK EXISTING HOLD LINE - YELLOW

REMARK EXISTING HOLD LINE - YELLOW

NEW RWY C - WHITE

REMARK EXISTING HOLD LINE

REMARK EXISTING TXY C - YELLOW

REMARK EXISTING HOLDLINE - YELLOW

NEW TOUCHDOWN ZONE MARKING - WHITE

REMARK EXIST. 3' RWY SIDE STRIPE - WHITE (TYP. BOTH SIDES)

NEW TOUCHDOWN ZONE MARKING - WHITE

REMARK EXISTING TXY C - YELLOW

243+40.55

244+20.55

245+40.55

246+20.55

247+40.55

248+20.55

250+71.28

251+91.28

252+71.28

253+91.28

254+71.28

255+91.28

80' TYP.

120' TYP.

80' TYP.

120' TYP.

256+71.28

257+91.28

258+71.28

259+91.28

260+71.28

261+91.28

262+71.28

263+91.28

264+71.28

265+91.28

267+91.28

268+71.28

261+06.28

261+81.28

266+06.28

266+81.28



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
MARKING PLAN 4

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DESIGN BY:	RLV
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CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42
SHEET	32 OF 79 SHEETS

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
MARKING PLAN 5

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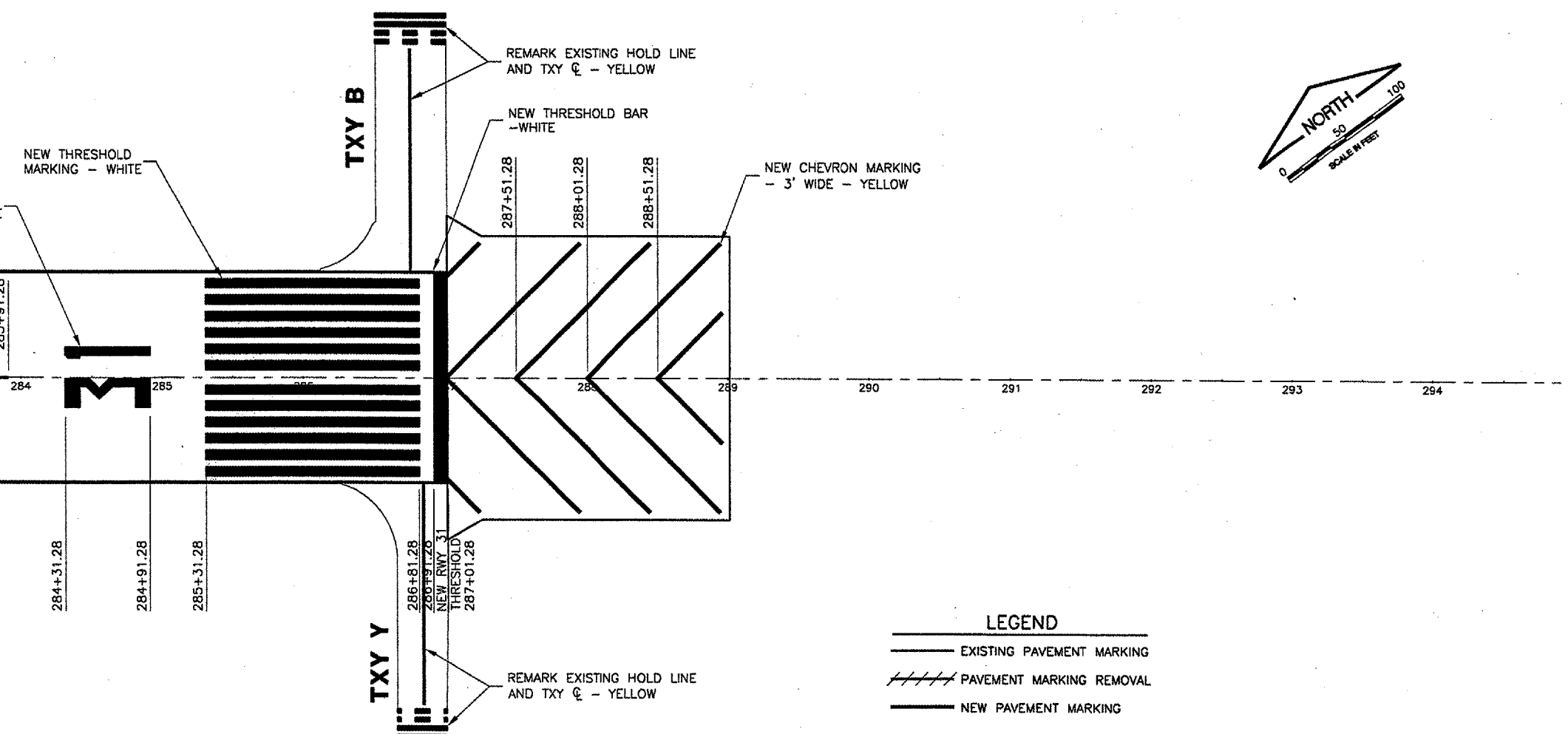
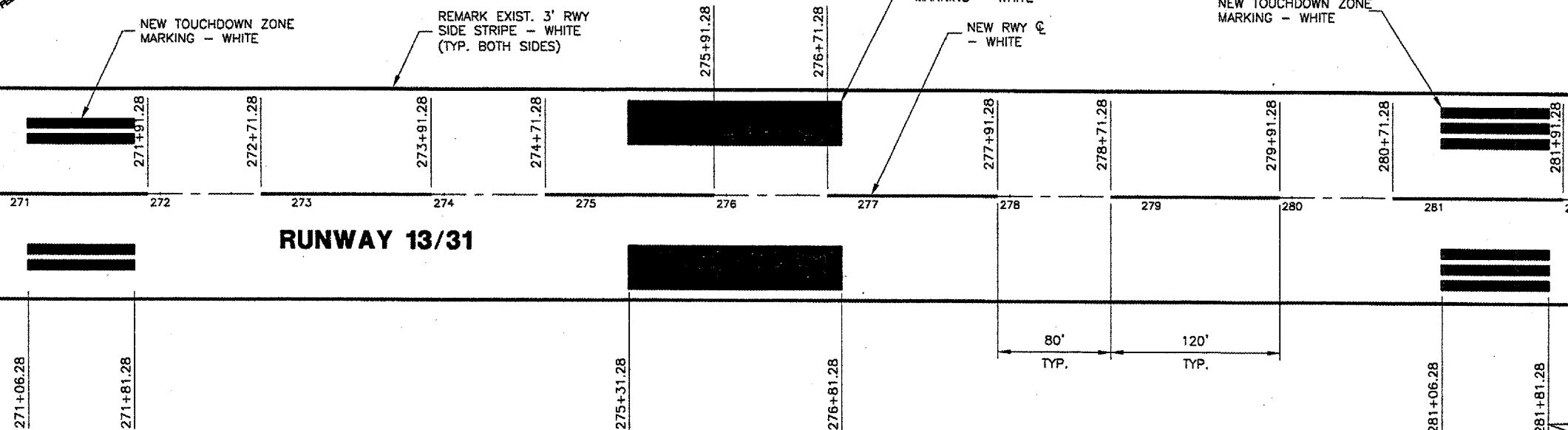
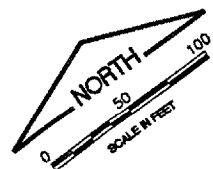
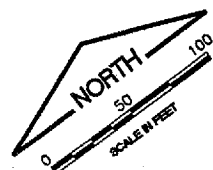
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DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03

IL PROJ. NO. SPI-3488
AIP PROJ. NO. 3-17-0096-42

MATCHLINE STA. 269+00

MATCHLINE STA. 282+00

MATCHLINE STA. 282+00



LEGEND

	EXISTING PAVEMENT MARKING
	PAVEMENT MARKING REMOVAL
	NEW PAVEMENT MARKING

RUNWAY 13/31

RWY 13/31

TXY B

TXY Y

NEW TOUCHDOWN ZONE MARKING - WHITE

REMARK EXIST. 3' RWY SIDE STRIPE - WHITE (TYP. BOTH SIDES)

NEW AIMING POINT MARKING - WHITE

NEW RWY ϕ - WHITE

NEW TOUCHDOWN ZONE MARKING - WHITE

271+06.28
271+81.28

275+31.28
276+81.28

281+06.28
281+81.28

NEW RUNWAY NUMERAL - WHITE

NEW THRESHOLD MARKING - WHITE

REMARK EXISTING HOLD LINE AND TXY ϕ - YELLOW

NEW THRESHOLD BAR - WHITE

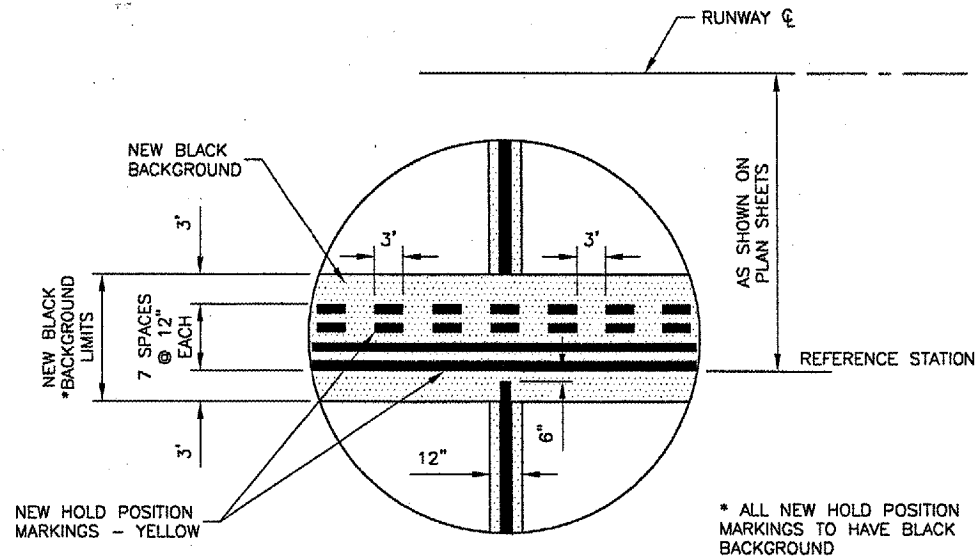
NEW CHEVRON MARKING - 3' WIDE - YELLOW

284+31.28
284+91.28
285+31.28

286+81.28
287+31.28
287+91.28
288+01.28
288+51.28

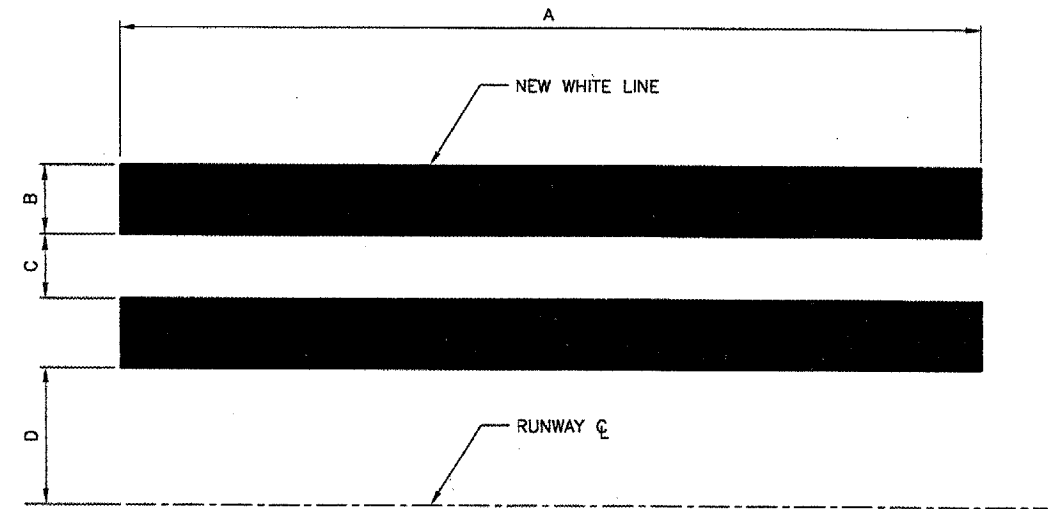
NEW RWY 31 THRESHOLD

REMARK EXISTING HOLD LINE AND TXY ϕ - YELLOW



HOLD POSITION MARKING DETAIL

N.T.S.

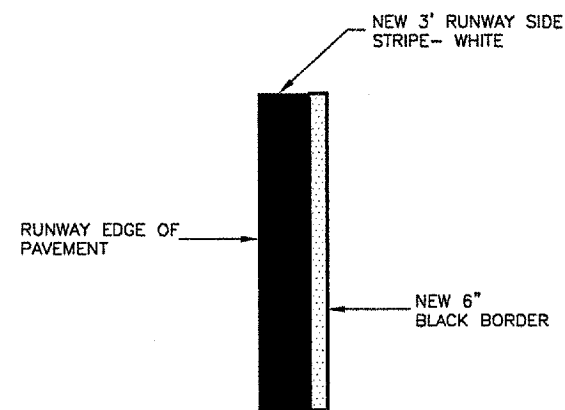


* DIMENSIONS DO NOT INCLUDE 6" BLACK BORDER.

MARKING (PER RUNWAY END)	DIMENSION *			
	A	B	C	D
THRESHOLD BAR	150'	10'	-	-
THRESHOLD MARKER	150'	5.75'	5.75'	5.75'
AIMING POINT MARKING	150'	30'	-	36'
TOUCHDOWN ZONE MARKER	75'	6'	5'	36'

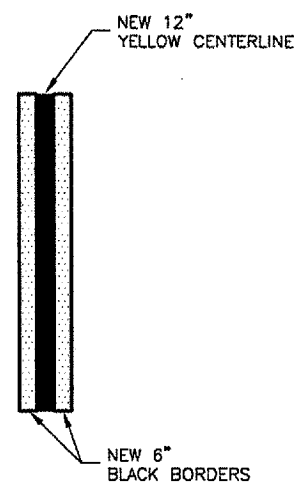
PRECISION RUNWAY MARKING DETAIL

N.T.S.



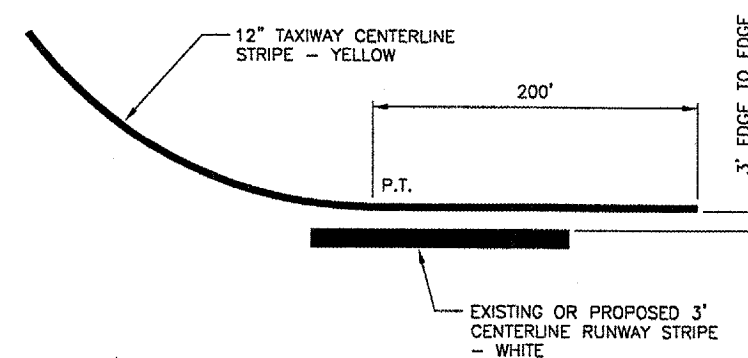
RUNWAY SIDE STRIPE DETAIL

N.T.S.



TAXIWAY CENTERLINE CONTINUOUS

N.T.S.

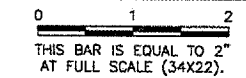


TAXIWAY EXIT MARKING DETAIL

N.T.S.

NOTE:
 ALL MARKING SHALL HAVE
 A 6" BLACK BORDER

REVISIONS		
NUMBER	BY	DATE



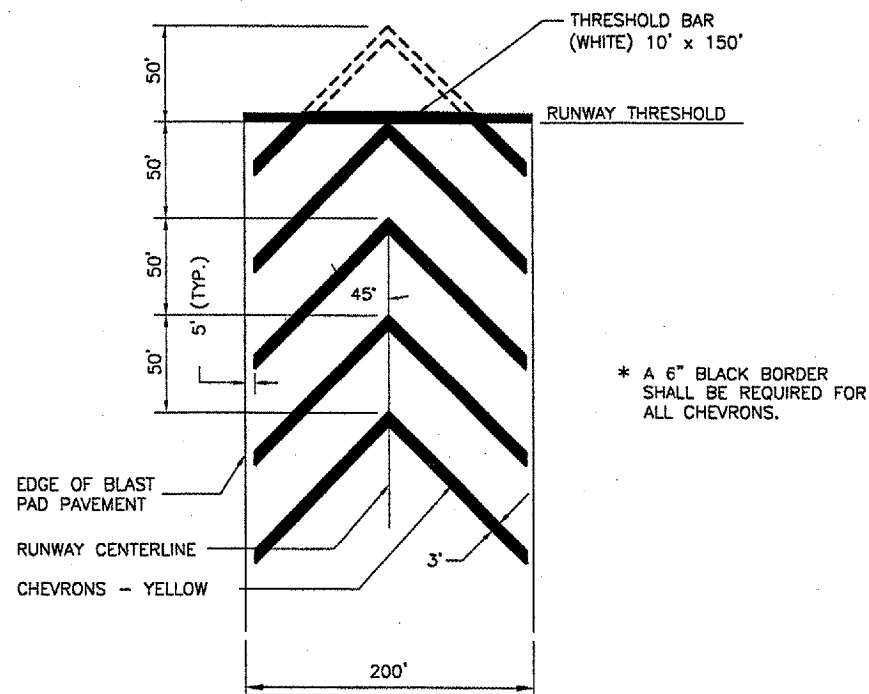
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
MARKING DETAILS 1

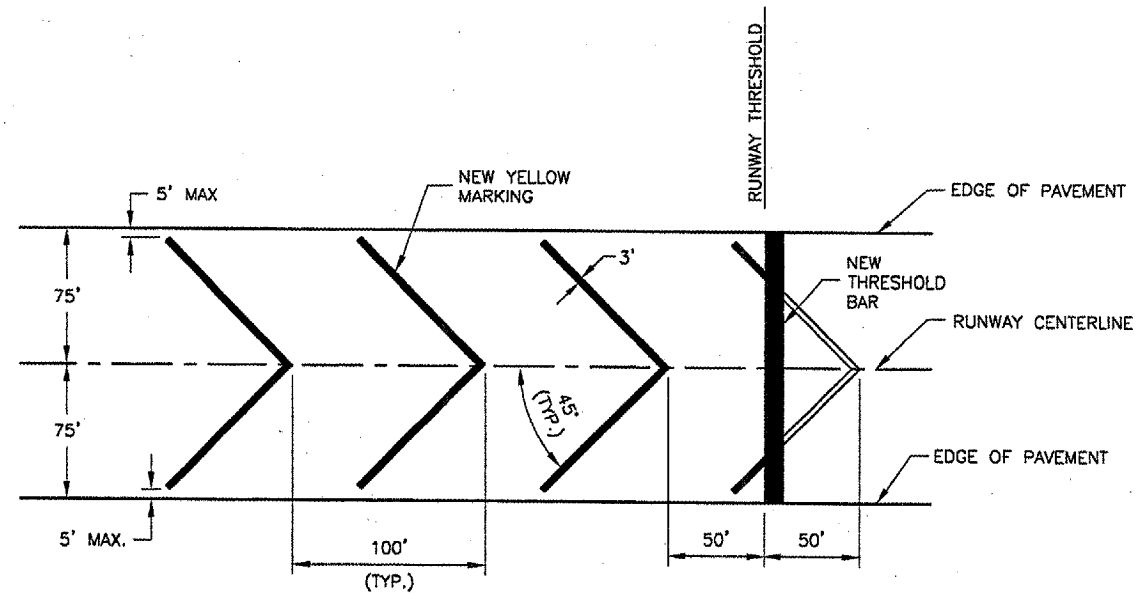
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DESIGN BY: RLV
 DRAWN BY: DPA
 CHECKED BY: *KCN*
 APPROVED BY: *RD*
 DATE: 04/24/07
 JOB No: 05035-03

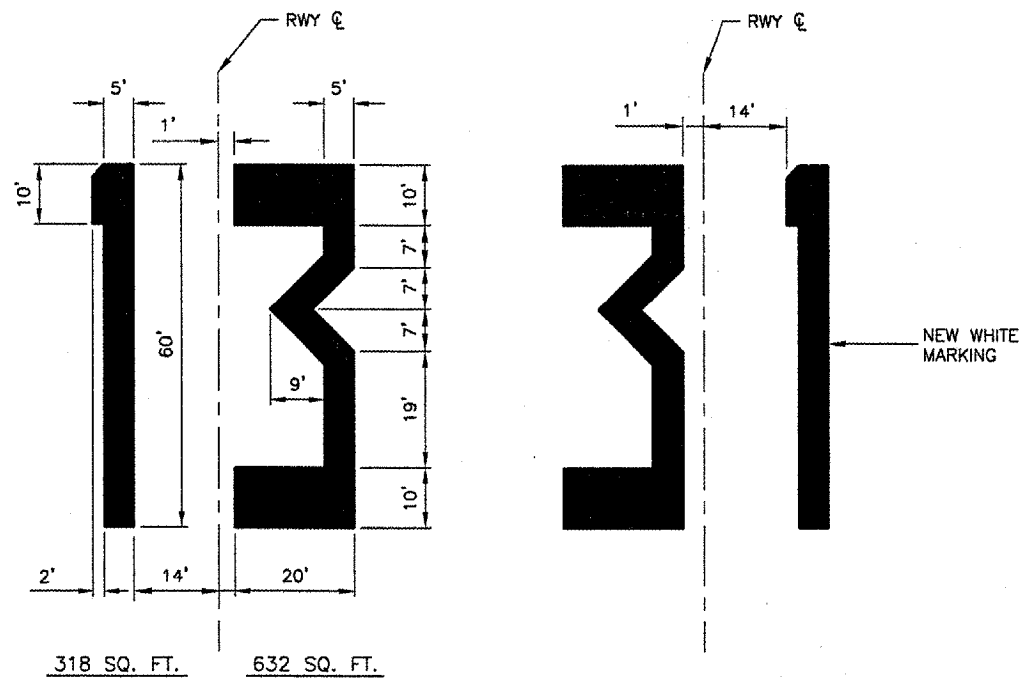
IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42



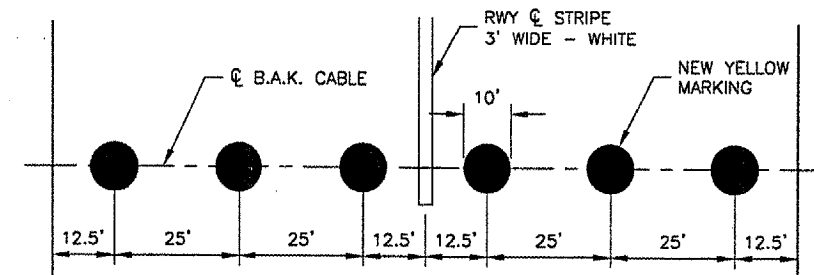
BLAST PAD CHEVRON DETAIL
 N.T.S.



OVERRUN CHEVRON DETAIL
 N.T.S.



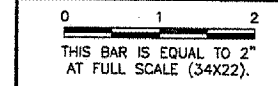
RUNWAY NUMERALS DETAIL
 N.T.S.



B.A.K. CABLE MARKING DETAIL
 N.T.S.

NOTE:
 ALL MARKING SHALL HAVE
 A 6" BLACK BORDER

REVISIONS		
NUMBER	BY	DATE



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

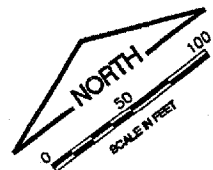
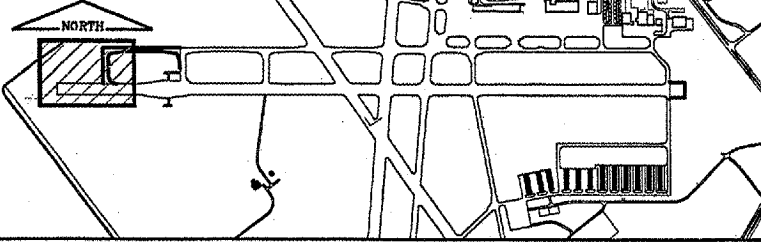
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
MARKING DETAILS 2

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 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03

IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42

KEYMAP



CA003
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 FILE: ELECO1.dwg
 UPDATE BY: Dave Allen
 PLOT DATE: 5/4/2007 10:40 AM
 Capital_Base
 Base_ExistTopo_TxyB
 tbinfo
 BASE_PROPGEO
 KEY
 BASE_PROPELEC

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

SIGNAGE TABLE

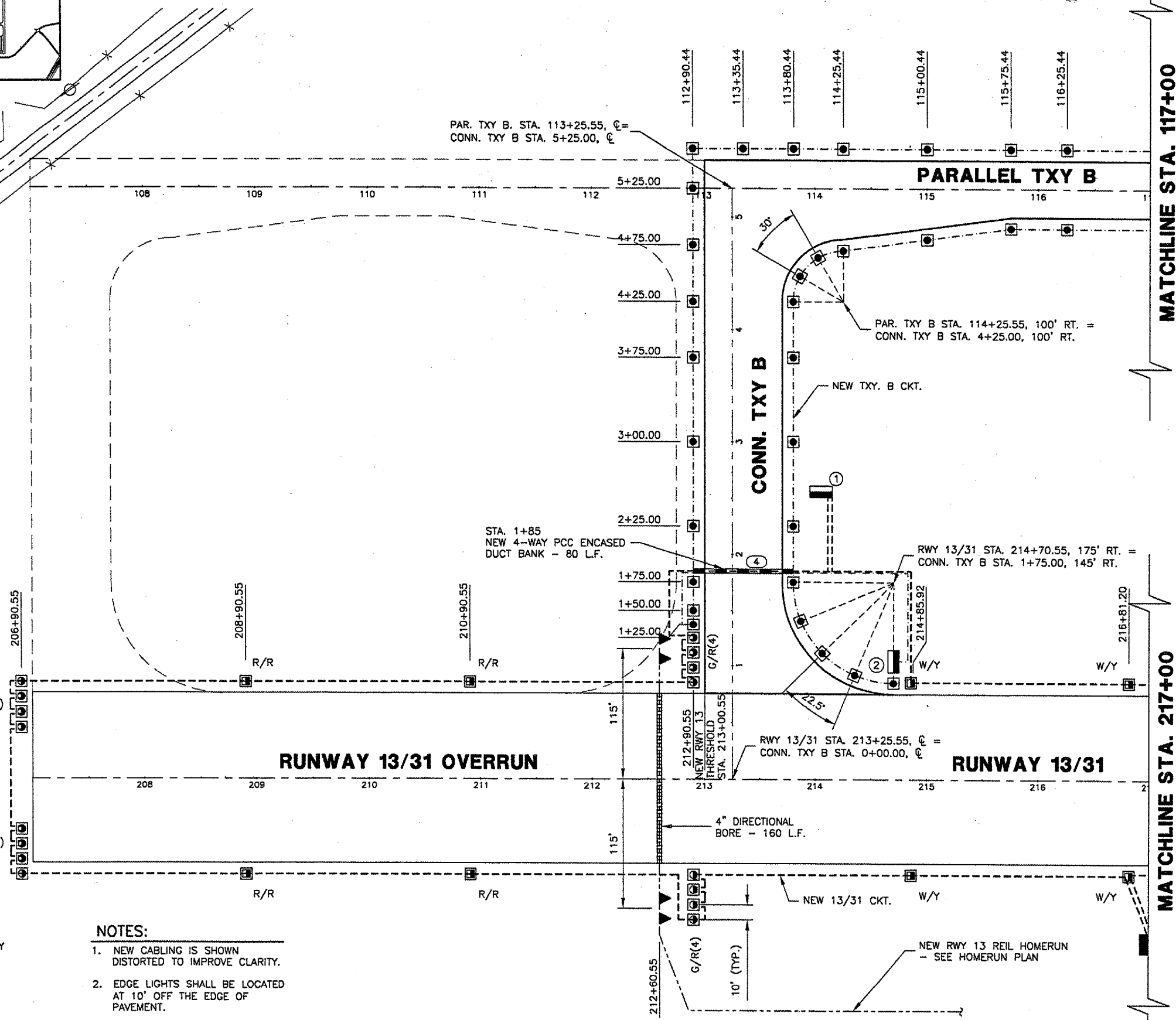
SIGN	STATION	OFFSET
1	2+56.00	80' RT.
2	214+70.50	110' LT.

LEGEND

- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RWY LIGHT
- NEW HIGH INTENSITY RWY EDGE LIGHT
- ▣ FUTURE HIGH INTENSITY RWY EDGE LIGHT
- ⊡ EXISTING BASE MOUNTED TXY LIGHT
- NEW BASE MOUNTED TXY LIGHT
- ⊙ EXISTING IN-PAVEMENT RWY EDGE LIGHT
- ⊕ NEW IN-PAVEMENT RWY EDGE LIGHT
- ⊞ NEW THRESHOLD LIGHT
- ④ NEW DISTANCE REMAINING SIGN & NO.
- ③ EXISTING AIRFIELD SIGN (SIGN PANELS TO BE REPLACED) & NO.
- ② NEW AIRFIELD SIGN & NO.
- ⊕ EXIST. SPLICE CAN
- NEW SPLICE CAN
- G- GREEN LENS
- R- RED LENS
- W- WHITE LENS
- Y- YELLOW LENS
- ▲ RELOCATED REIL
- R REMOVE
- ▬ EXIST. DUCT BANK
- ▬ NEW DUCT BANK & #-WAY
- ▬ EXIST. TXY CIRCUIT
- ▬ NEW TXY B CIRCUIT
- ▬ NEW RWY 13/31 CIRCUIT
- ▬ NEW REIL CIRCUIT
- ▬ NEW DIRECTIONAL BORE - 4"

NOTES:

- NEW CABLING IS SHOWN DISTORTED TO IMPROVE CLARITY.
- EDGE LIGHTS SHALL BE LOCATED AT 10' OFF THE EDGE OF PAVEMENT.
- EDGE LIGHT CABLE SHALL BE LOCATED AT 12' OFF THE EDGE OF PAVEMENT.



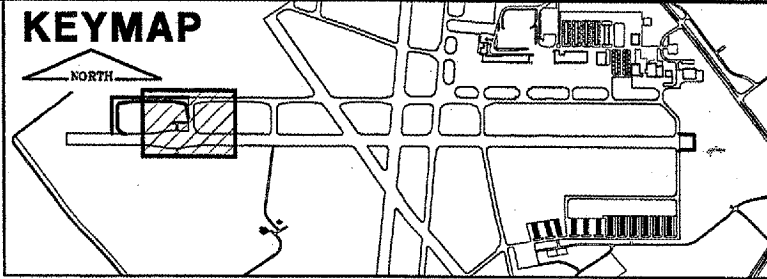
MATCHLINE STA. 117+00

MATCHLINE STA. 217+00

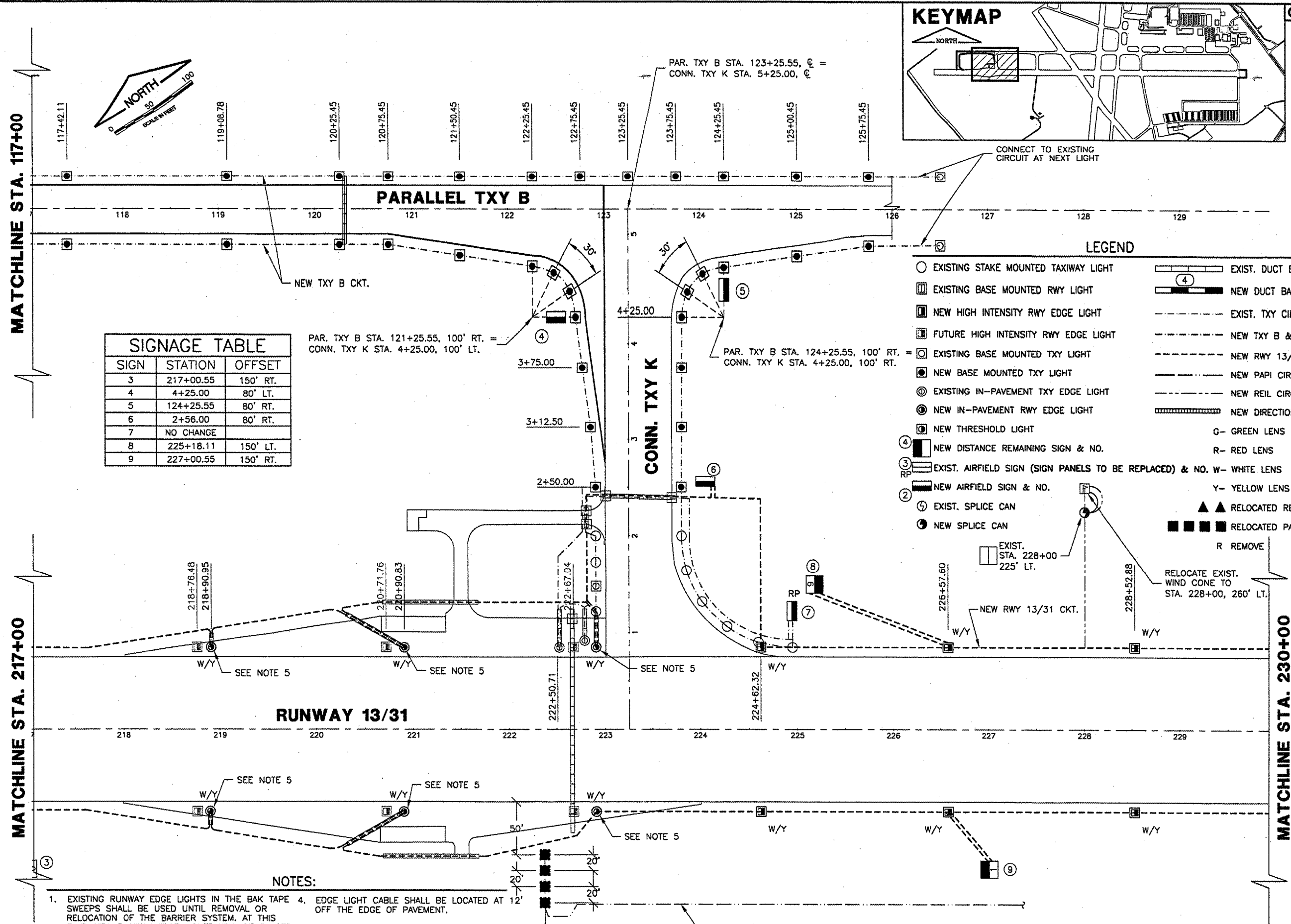
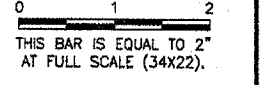
SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
ELECTRICAL & LIGHTING PLAN 1

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 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03
 I.L. PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 36 OF 70 SHEETS



REVISIONS		
NUMBER	BY	DATE



SIGN	STATION	OFFSET
3	217+00.55	150' RT.
4	4+25.00	80' LT.
5	124+25.55	80' RT.
6	2+56.00	80' RT.
7	NO CHANGE	
8	225+18.11	150' LT.
9	227+00.55	150' RT.

- LEGEND**
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
 - EXISTING BASE MOUNTED RWY LIGHT
 - NEW HIGH INTENSITY RWY EDGE LIGHT
 - FUTURE HIGH INTENSITY RWY EDGE LIGHT
 - EXISTING BASE MOUNTED TXY LIGHT
 - NEW BASE MOUNTED TXY LIGHT
 - ⊙ EXISTING IN-PAVEMENT TXY EDGE LIGHT
 - ⊙ NEW IN-PAVEMENT RWY EDGE LIGHT
 - ⊙ NEW THRESHOLD LIGHT
 - ④ NEW DISTANCE REMAINING SIGN & NO.
 - ③ EXIST. AIRFIELD SIGN (SIGN PANELS TO BE REPLACED) & NO. W- WHITE LENS
 - ② NEW AIRFIELD SIGN & NO.
 - ⊕ EXIST. SPLICE CAN
 - NEW SPLICE CAN
 - EXIST. DUCT BANK
 - ④ NEW DUCT BANK
 - EXIST. TXY CIRCUIT
 - NEW TXY B & K CKT.
 - NEW RWY 13/31 CKT.
 - NEW PAPI CIRCUIT
 - NEW REIL CIRCUIT
 - NEW DIRECTIONAL BORE
 - G- GREEN LENS
 - R- RED LENS
 - Y- YELLOW LENS
 - ▲ RELOCATED REIL
 - RELOCATED PAPI
 - R REMOVE

NOTES:

- EXISTING RUNWAY EDGE LIGHTS IN THE BAK TAPE SWEEPS SHALL BE USED UNTIL REMOVAL OR RELOCATION OF THE BARRIER SYSTEM. AT THIS TIME, NEW RUNWAY EDGE LIGHTS WILL BE PLACED IN THE FUTURE LOCATIONS.
- NEW CABLING IS SHOWN DISTORTED TO IMPROVE CLARITY.
- EDGE LIGHTS SHALL BE LOCATED AT 10' OFF THE EDGE OF PAVEMENT.
- EDGE LIGHT CABLE SHALL BE LOCATED AT 12' OFF THE EDGE OF PAVEMENT.
- REPLACE IN-PAVEMENT LIGHT. INSTALL NEW L-850-C RUNWAY EDGE LIGHT ON EXISTING L-86B, SIZE C BASE WITH NEW MOUNTING RING (CROUSE HINDS REL-850C1-C). CONTRACTOR SHALL VERIFY DIMENSIONS PRIOR TO ORDERING.

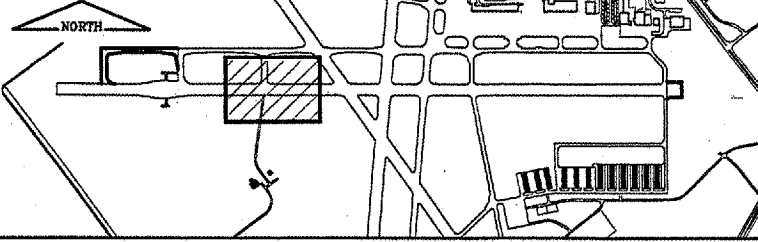
SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 ELECTRICAL & LIGHTING PLAN 2

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DESIGN BY:	RLV
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO. SPI-3488	
AIP PROJ. NO. 3-17-0096-42	
SHEET 37 OF 79 SHEETS	

KEYMAP

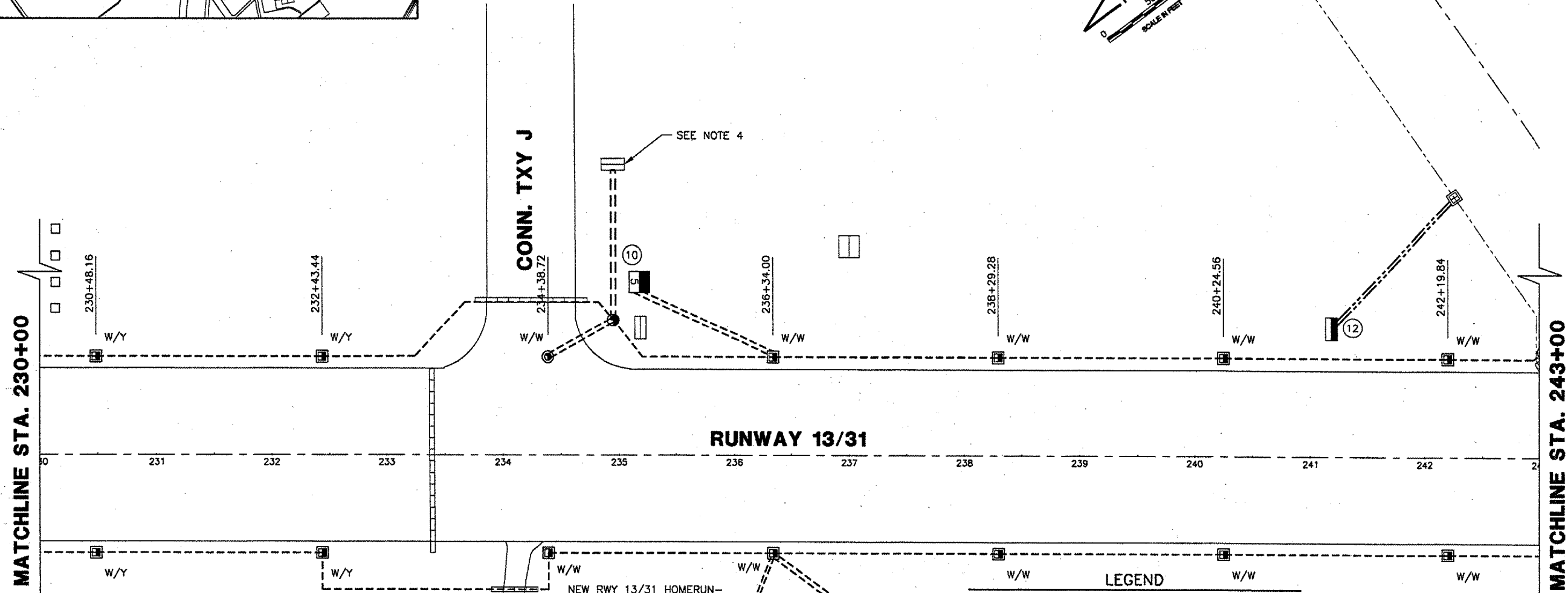


CA003
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 PLOT DATE: 5/4/2007 10:37 AM
 Base_ExistTopo_TxyB
 Capital_Base
 tbinfo
 BASE_PROPGEO
 BASE_PROPELEC
 KEY

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



RUNWAY 13/31

ATCT ACCESS ROAD

CONN. TXY J

SIGNAGE TABLE

SIGN	STATION	OFFSET
10	235+18.11	150' LT.
11	237+00.55	150' RT.
12	NO CHANGE	

- NOTES:**
- NEW CABLING IS SHOWN DISTORTED TO IMPROVE CLARITY.
 - EDGE LIGHTS SHALL BE LOCATED AT 10' OFF THE EDGE OF PAVEMENT.
 - EDGE LIGHT CABLE SHALL BE LOCATED AT 12' OFF THE EDGE OF PAVEMENT.
 - REMOVE EXISTING HOLD SIGN FROM TAXIWAY CIRCUIT AND ADD TO RUNWAY CIRCUIT. MAKE CABLING REVISIONS TO PROVIDE FOR A COMPLETE TAXIWAY CIRCUIT.

- LEGEND**
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
 - EXISTING BASE MOUNTED RWY LIGHT
 - NEW HIGH INTENSITY RWY EDGE LIGHT
 - ▣ FUTURE HIGH INTENSITY RWY EDGE LIGHT
 - EXISTING BASE MOUNTED TXY LIGHT
 - NEW BASE MOUNTED TXY LIGHT
 - ⊕ EXISTING IN-PAVEMENT RWY EDGE LIGHT
 - ⊙ NEW IN-PAVEMENT RWY EDGE LIGHT
 - NEW THRESHOLD LIGHT
 - NEW DISTANCE REMAINING SIGN & NO.
 - Ⓜ EXISTING AIRFIELD SIGN (SIGN PANELS TO BE REPLACED) & NO.
 - NEW AIRFIELD SIGN & NO.
 - Ⓢ EXIST. SPLICE CAN
 - Ⓢ NEW SPLICE CAN
 - EXIST. DUCT BANK
 - (4) NEW DUCT BANK & #-WAY
 - G- GREEN LENS
 - R- RED LENS
 - W- WHITE LENS
 - Y- YELLOW LENS
 - ▲ RELOCATED REIL
 - R REMOVE
 - EXIST. TXY CIRCUIT
 - NEW TXY B CIRCUIT
 - NEW RWY 13/31 CIRCUIT
 - NEW RWY 18/36 CIRCUIT
 - NEW DIRECTIONAL BORE - 4"

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
ELECTRICAL & LIGHTING PLAN 3

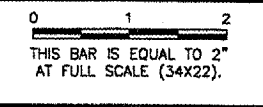
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DESIGN BY: RLV
 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03
 IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 38 OF 70 SHEETS

CA003

K:\Springfield\0503503\Draw\Sheets
 FILE: ELECO4.dwg
 UPDATE BY: Dave Allen
 PLOT DATE: 5/4/2007 10:41 AM
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 Base_ExistTopo_TxyB
 Ibmfo
 BASE_PROGEO
 BASE_PROPELEC
 KEY

REVISIONS		
NUMBER	BY	DATE



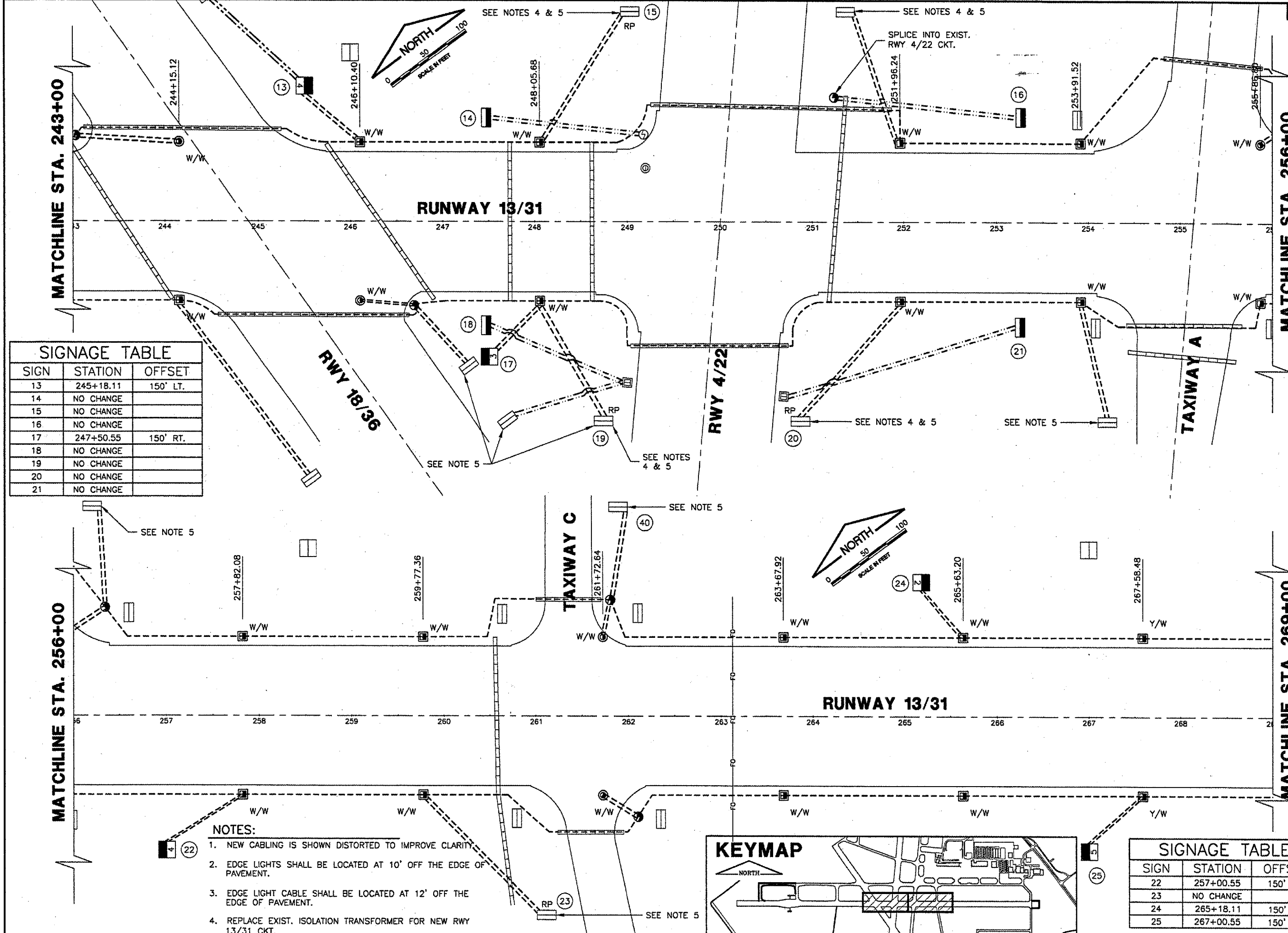

SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
ELECTRICAL & LIGHTING PLAN 4

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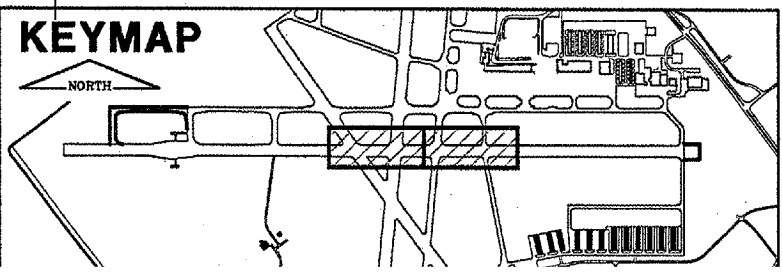
DESIGN BY: RLV
 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03

IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42

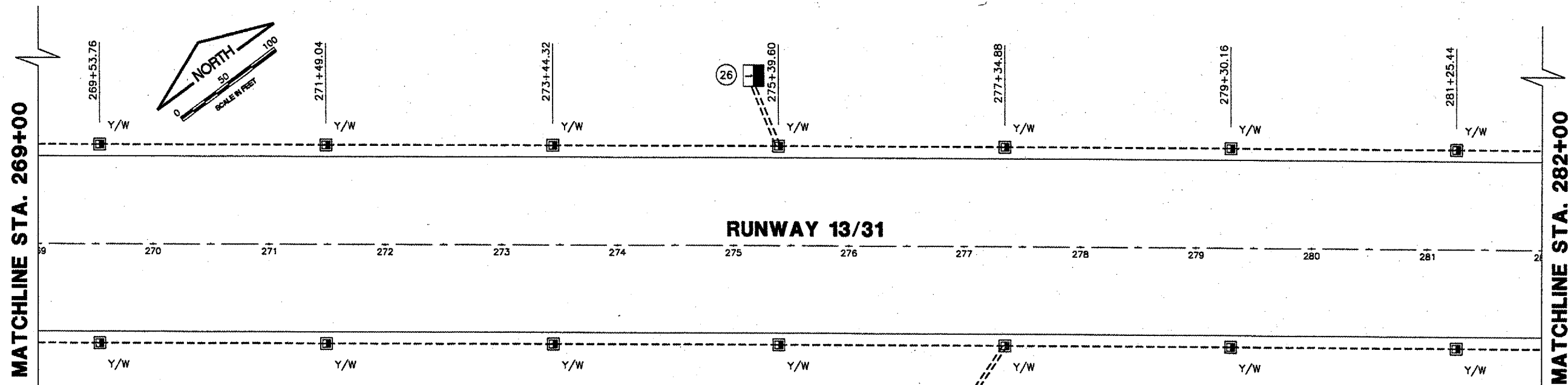


SIGN	STATION	OFFSET
13	245+18.11	150' LT.
14	NO CHANGE	
15	NO CHANGE	
16	NO CHANGE	
17	247+50.55	150' RT.
18	NO CHANGE	
19	NO CHANGE	
20	NO CHANGE	
21	NO CHANGE	

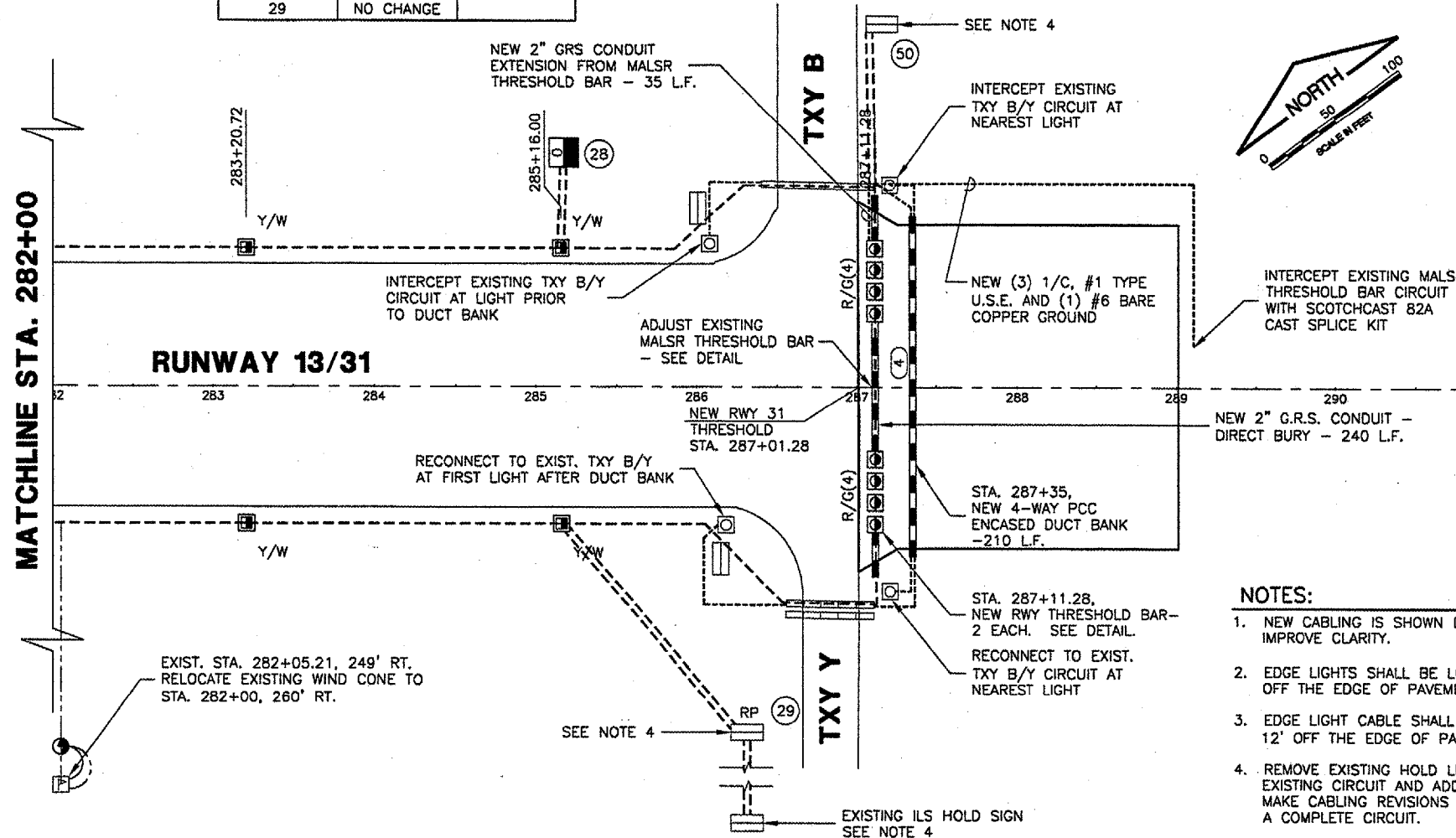
- NOTES:**
1. NEW CABLING IS SHOWN DISTORTED TO IMPROVE CLARITY.
 2. EDGE LIGHTS SHALL BE LOCATED AT 10' OFF THE EDGE OF PAVEMENT.
 3. EDGE LIGHT CABLE SHALL BE LOCATED AT 12' OFF THE EDGE OF PAVEMENT.
 4. REPLACE EXIST. ISOLATION TRANSFORMER FOR NEW RWY 13/31 CKT.
 5. REMOVE EXISTING HOLD SIGN FROM EXISTING CIRCUIT AND ADD TO NEW CIRCUIT. MAKE CABLING REVISIONS TO



SIGN	STATION	OFFSET
22	257+00.55	150' RT.
23	NO CHANGE	
24	265+18.11	150' LT.
25	267+00.55	150' RT.



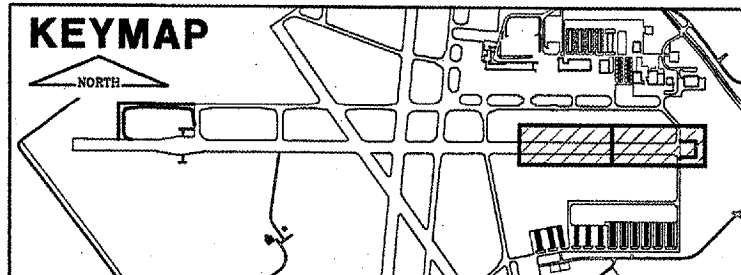
SIGNAGE TABLE		
SIGN	STATION	OFFSET
26	275+18.11	150' LT.
27	277+00.55	150' RT.
28	285+18.11	150' LT.
29	NO CHANGE	



LEGEND

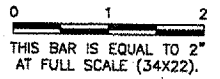
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RWY LIGHT
- NEW HIGH INTENSITY RWY EDGE LIGHT
- ▣ FUTURE HIGH INTENSITY RWY EDGE LIGHT
- EXISTING BASE MOUNTED TXY LIGHT
- NEW BASE MOUNTED TXY LIGHT
- ⊙ EXISTING IN-PAVEMENT RWY EDGE LIGHT
- ⊙ NEW IN-PAVEMENT RWY EDGE LIGHT
- ⊙ NEW THRESHOLD LIGHT
- ④ NEW DISTANCE REMAINING SIGN & NO.
- ③ EXISTING AIRFIELD SIGN (SIGN PANELS TO BE REPLACED) & NO.
- ② NEW AIRFIELD SIGN & NO.
- ⊕ EXIST. SPLICE CAN
- ⊕ NEW SPLICE CAN
- EXIST. DUCT BANK
- NEW DUCT BANK & #-WAY
- G- GREEN LENS
- R- RED LENS
- W- WHITE LENS
- Y- YELLOW LENS
- ▲ RELOCATED REIL
- R REMOVE
- EXIST. TXY CIRCUIT
- NEW TXY B CIRCUIT
- NEW RWY 13/31 CIRCUIT
- NEW REIL CIRCUIT
- NEW DIRECTIONAL BORE - 4"


- NOTES:**
- NEW CABLING IS SHOWN DISTORTED TO IMPROVE CLARITY.
 - EDGE LIGHTS SHALL BE LOCATED AT 10' OFF THE EDGE OF PAVEMENT.
 - EDGE LIGHT CABLE SHALL BE LOCATED AT 12' OFF THE EDGE OF PAVEMENT.
 - REMOVE EXISTING HOLD LINE SIGN FROM EXISTING CIRCUIT AND ADD TO NEW CIRCUIT. MAKE CABLING REVISIONS TO PROVIDE FOR A COMPLETE CIRCUIT.




REVISIONS

NUMBER	BY	DATE




SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
ELECTRICAL & LIGHTING PLAN 5


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 License No. 984-000613

DESIGN BY:	RLV
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42

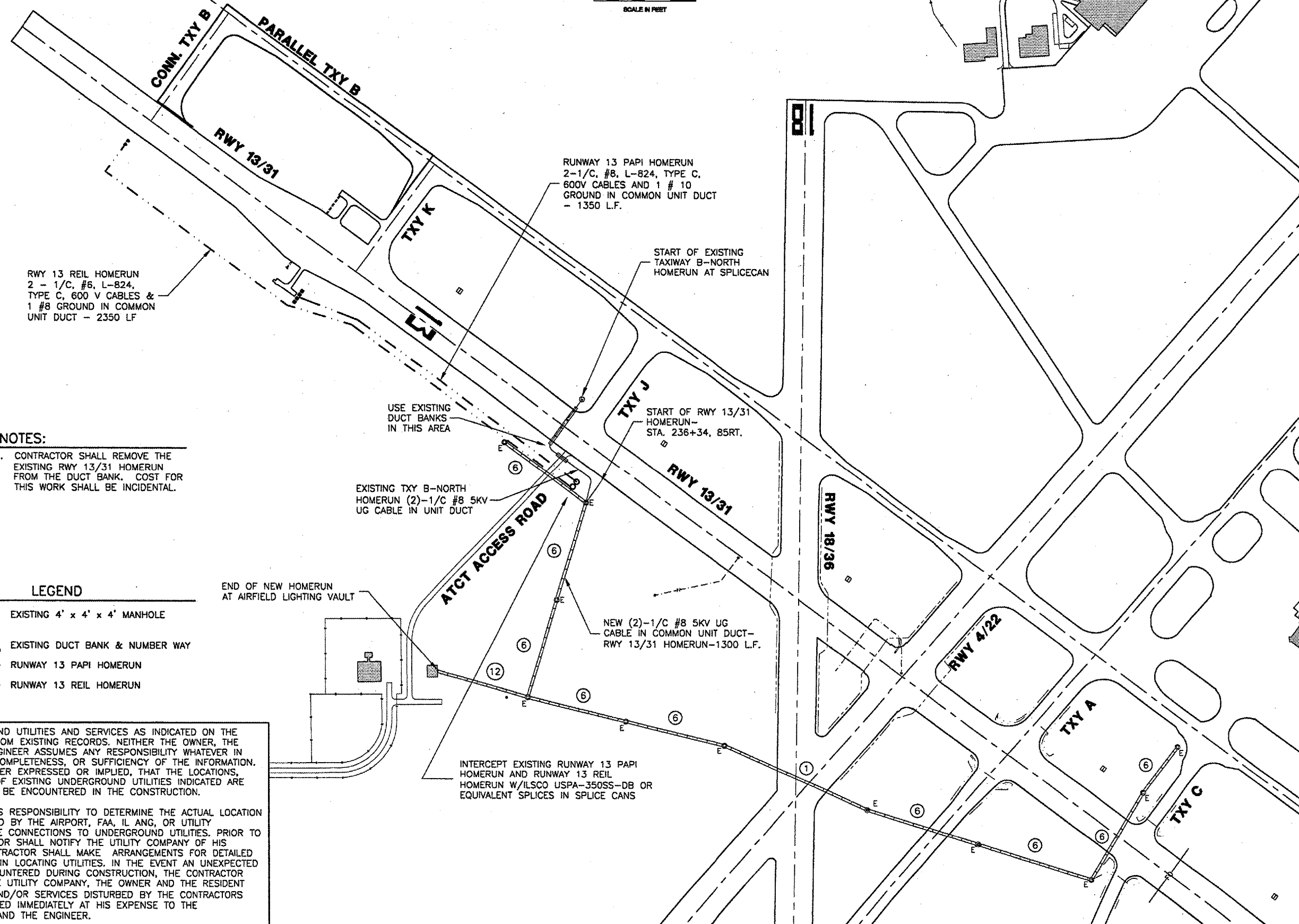
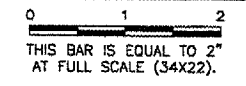
TOZER ROAD

HENNEPIN ROAD

CA003
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 UPDATE BY: Allan Draughon
 PLOT DATE: 5/9/2007 9:12 AM
 BASE_PROPGEO
 Base_ExistTopo_TxyB
 Capital_Base
 tbinfo
 Base_Existhomerun



REVISIONS		
NUMBER	BY	DATE



RWY 13 REIL HOMERUN
 2 - 1/C, #8, L-824,
 TYPE C, 600 V CABLES &
 1 #8 GROUND IN COMMON
 UNIT DUCT - 2350 LF

RUNWAY 13 PAPI HOMERUN
 2-1/C, #8, L-824, TYPE C,
 600V CABLES AND 1 # 10
 GROUND IN COMMON UNIT DUCT
 - 1350 L.F.

START OF EXISTING
 TAXIWAY B-NORTH
 HOMERUN AT SPLICECAN

START OF RWY 13/31
 HOMERUN-
 STA. 236+34, 85RT.

USE EXISTING
 DUCT BANKS
 IN THIS AREA

EXISTING TXY B-NORTH
 HOMERUN (2)-1/C #8 5KV
 UG CABLE IN UNIT DUCT

NEW (2)-1/C #8 5KV UG
 CABLE IN COMMON UNIT DUCT-
 RWY 13/31 HOMERUN-1300 L.F.

INTERCEPT EXISTING RUNWAY 13 PAPI
 HOMERUN AND RUNWAY 13 REIL
 HOMERUN W/ILSCO USPA-350SS-DB OR
 EQUIVALENT SPLICES IN SPLICE CANS

NOTES:

- CONTRACTOR SHALL REMOVE THE EXISTING RWY 13/31 HOMERUN FROM THE DUCT BANK. COST FOR THIS WORK SHALL BE INCIDENTAL.

LEGEND

- ⊙ E EXISTING 4' x 4' x 4' MANHOLE
- ⊗ EXISTING DUCT BANK & NUMBER WAY
- RUNWAY 13 PAPI HOMERUN
- - - RUNWAY 13 REIL HOMERUN

END OF NEW HOMERUN
 AT AIRFIELD LIGHTING VAULT

THE LOCATION OF UNDERGROUND UTILITIES AND SERVICES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, THE DIVISION, OR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES OWNED BY THE AIRPORT, FAA, IL ANG, OR UTILITY COMPANIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.



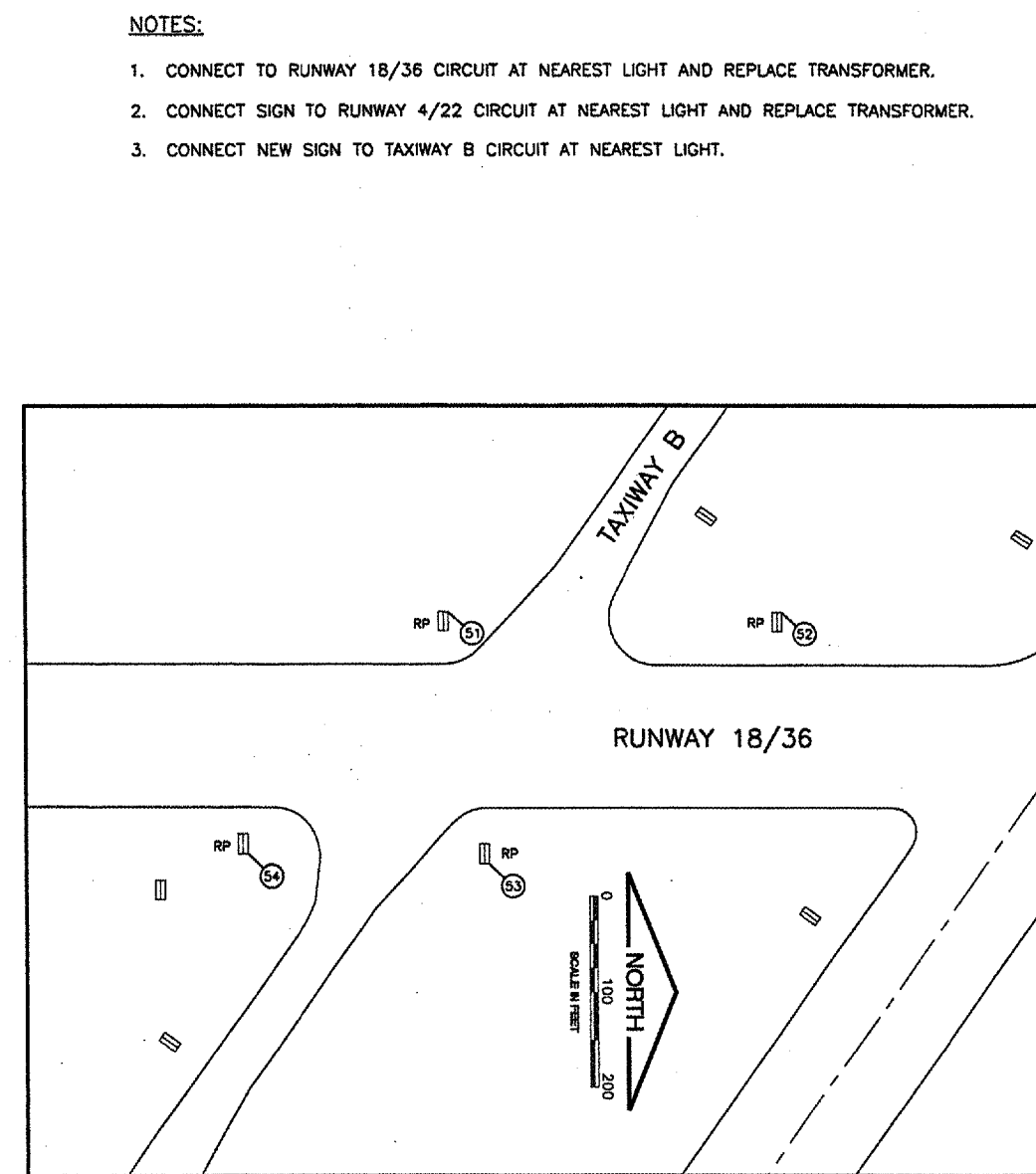
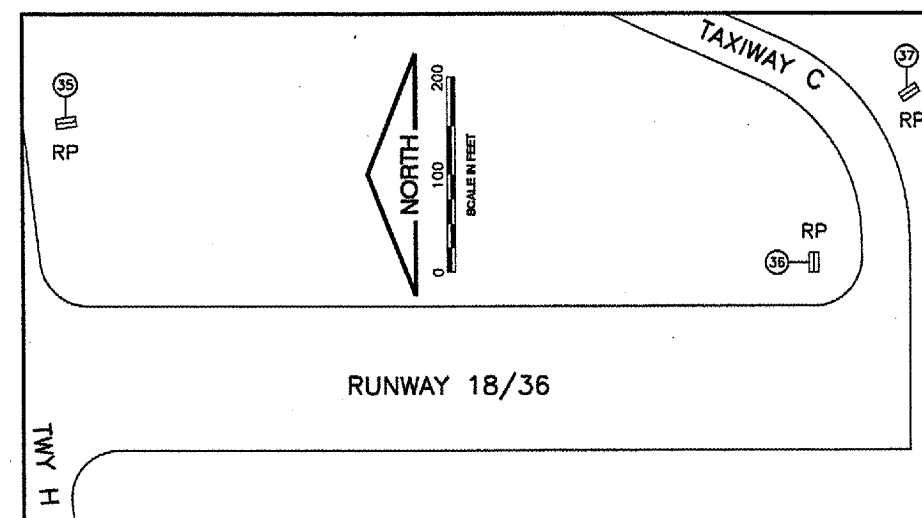
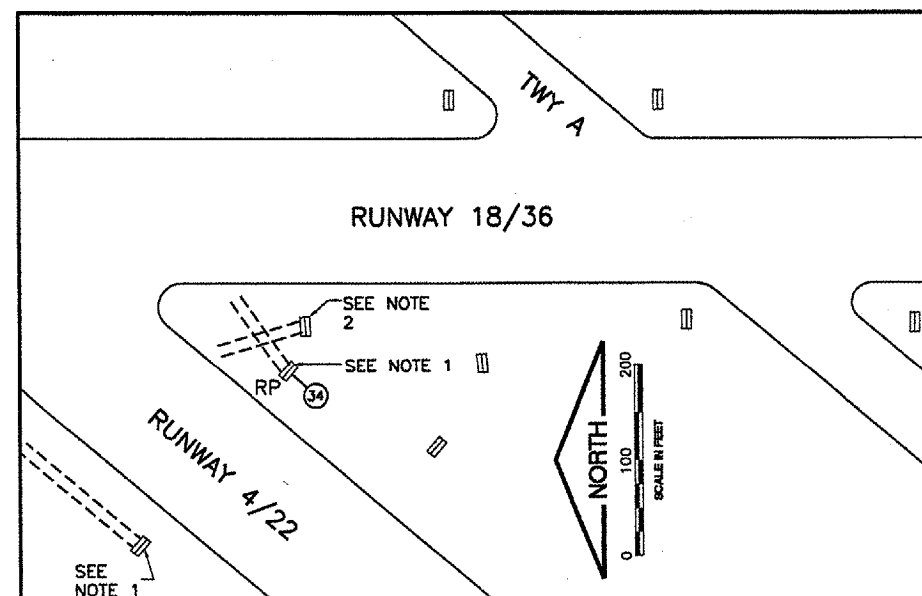
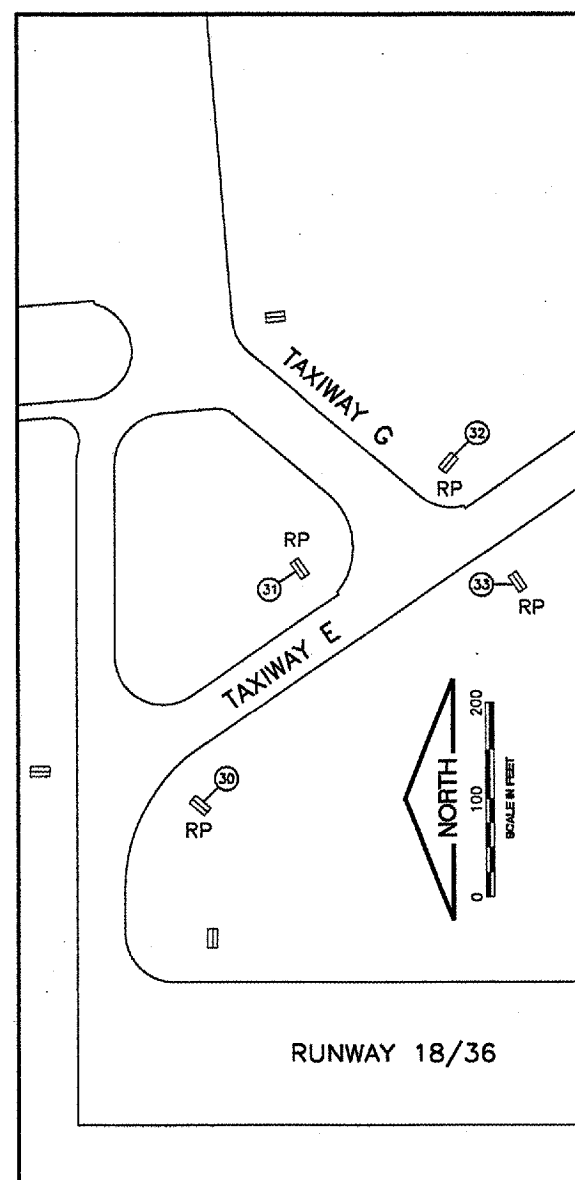
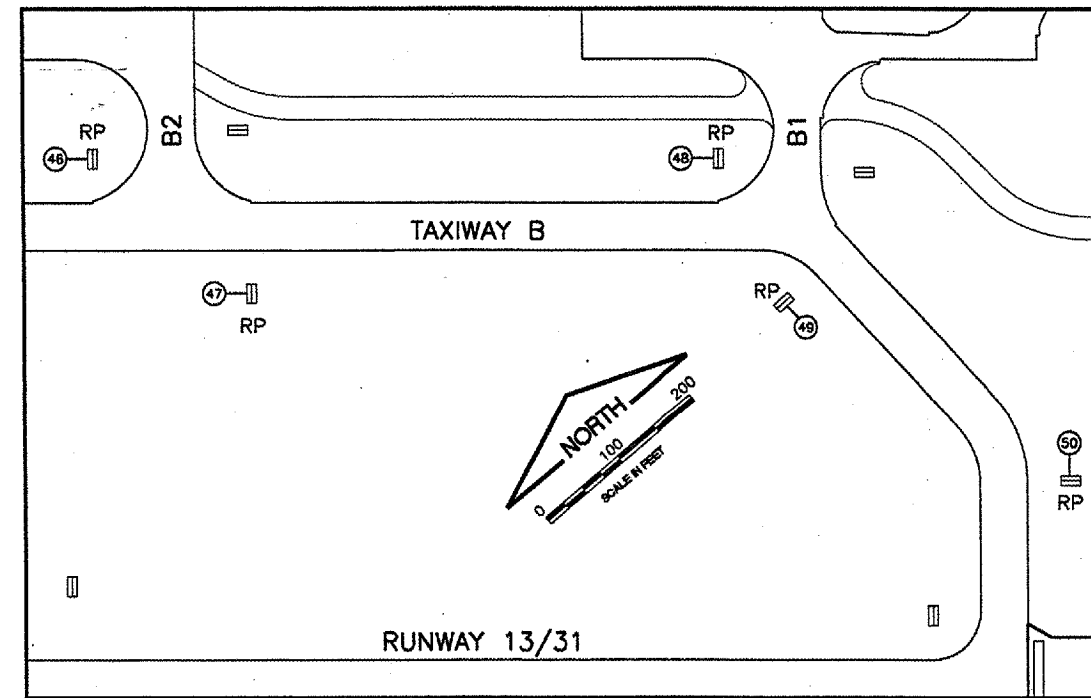
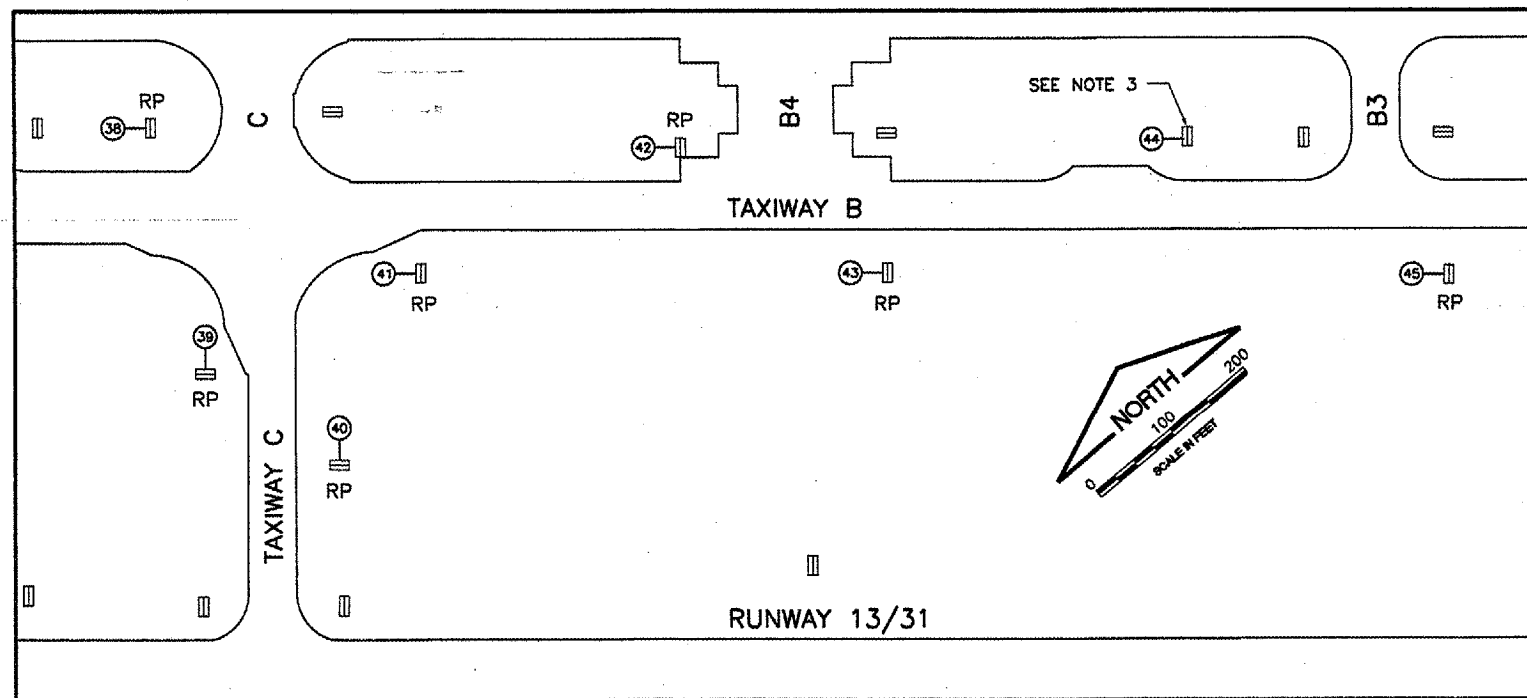
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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 HOMERUN PLAN

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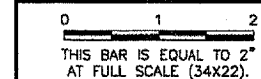
DESIGN BY:	RLV
DRAWN BY:	DPA
CHECKED BY:	ICLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42
SHEET 41 OF 79 SHEETS	



NOTES:

1. CONNECT TO RUNWAY 18/36 CIRCUIT AT NEAREST LIGHT AND REPLACE TRANSFORMER.
2. CONNECT SIGN TO RUNWAY 4/22 CIRCUIT AT NEAREST LIGHT AND REPLACE TRANSFORMER.
3. CONNECT NEW SIGN TO TAXIWAY B CIRCUIT AT NEAREST LIGHT.

REVISIONS		
NUMBER	BY	DATE



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 SPRINGFIELD, ILLINOIS

**CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 SIGNAGE PLAN**

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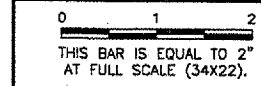
DESIGN BY:	CMT
DRAWN BY:	CMT
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03


CA003

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FILE: SIGNSCHED.dwg
UPDATE BY: Allan Draughon
PLOT DATE: 5/7/2007 2:33 PM
tbinfo

SIGN #	SIDE	NEW SIGN LEGEND	WHITE WITH BLACK OUTLINE ON RED BACKGROUND	BLACK LEGEND ON YELLOW BACKGROUND	YELLOW LEGEND ON BLACK BACKGROUND	WHITE LEGEND ON BLACK BACKGROUND	# OF NEW CHARACTERS	# OF FACES TO REPLACE	NEW POWER CIRCUIT	NOTES
1	NE SW	B 31-13 B	31-13	B	B		6		RWY 13/31	NEW SIGN
2	SE	B →		B →			2		TXY B	NEW SIGN
3	SE	0				0	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
4	NE SW	K K ← B →		← B →	K K		4		TXY B	NEW SIGN
5	NW SE	B ← K B		← K	B B		3		TXY B	NEW SIGN
6	NE SW	K 31-13 K	31-13	K	K		6		TXY B	NEW SIGN
7	SE	K →		K →			2		TXY B	NEW SIGN
8	NW	6				6	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
9	SE	1				1	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
10	NW	5				5	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
11	SE	2				2	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
12	NW	18-36	18-36				5		RWY 18/36	NEW SIGN
13	NW	4				4	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
14	NW SE	22-4 36-18	22-4 36-18				5		RWY 4/22	NEW SIGN - 20A/6.6A CKT.
15	NE	31-13	31-13				5	1	RWY 13/31	REPLACE EXISTING FACES & XFMR
16	SE	4-22	4-22				4		RWY 4/22	NEW SIGN - 20A/6.6A CKT
17	SE	3				3	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
18	NW SE	22-4 36-18	22-4 36-18				5		RWY 4/22	NEW SIGN - 20A/6.6A CKT.
19	NE SW	36-18 13-31	36-18 13-31				5	2	RWY 13/31	REPLACE EXISTING FACES
20	NE SW	36-18 13-31	36-18 13-31				5	2	RWY 13/31	REPLACE EXISTING FACES
21	SE	4-22	4-22				4		RWY 4/22	NEW SIGN - 20A/6.6A CKT
22	SE	4				4	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
23	N S	C C 13-31	13-31	C	C		6	2	RWY 13/31	REPLACE EXISTING FACES
24	NW	2				2	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
25	SE	5				5	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
26	NW	1				1	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
27	SE	6				6	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
28	NW	0				0	1		RWY 13/31	NEW RUNWAY DISTANCE REMAINING SIGN
29	NE SW	Y Y 13-31	13-31	Y	Y		6	2	RWY 13/31	REPLACE EXISTING FACES
30	NW SE	E E 18	18	E	E		3	2		REPLACE EXISTING FACES
31	NW SE	G E E		↙ G	E E		3	2		REPLACE EXISTING FACES
32	NE SW	G ← E → G		← E →	G G		4	2		REPLACE EXISTING FACES
33	SE	E G ↗		G ↗	E		3	1		REPLACE EXISTING FACES
34	SW	18-36	18-36				5	1		REPLACE EXISTING FACES
35	E W	H 36-18 H	36-18		H H		6	2		REPLACE EXISTING FACES
36	N	← C		← C			2	1		REPLACE EXISTING FACES
37	NE SW	C 36 C	36	C	C		3	2		REPLACE EXISTING FACES
38	NW	← C B C →		← C C →	B		5	1		REPLACE EXISTING FACES
39	NE SW	36+4 ↑ ← B C C ↑ B →		36+4 ↑ ← B C C ↑ B →	C		7	2		REPLACE EXISTING FACES
40	NE SW	C 31-13 C	31-13	C	C		6	2	RWY 13/31	REPLACE EXISTING FACES
41	NW SE	31 ↑ ← C B C →		31 ↑ ← C C →	B		5	2		REPLACE EXISTING FACES
42	NW	← B4 B		← B4	B		4	1		REPLACE EXISTING FACES
43	SE	B B4 →		B4 →	B		4	1		REPLACE EXISTING FACES
44	NW	← FBO		← FBO			4		TXY B	NEW SIGN
45	SE	B B3 →		B3 →	B		4	1		REPLACE EXISTING FACES
46	NW	← B2 B		← B2	B		4	1		REPLACE EXISTING FACES
47	NW SE	31 ↑ B B2 →		31 ↑ B2 →	B		4	2		REPLACE EXISTING FACES
48	NW	← B1 B B ↗		← B1 B ↗	B		6	1		REPLACE EXISTING FACES
49	N S	31 ↑ ↖ B B B1 ↗		31 ↑ ↖ B B1 ↗	B		6	2		REPLACE EXISTING FACES
50	NE SW	B 31-13 B	31-13	B	B		6	2		REPLACE EXISTING FACES
51	S	↖ B		↖ B			2	1		REPLACE EXISTING FACES
52		B ↘		B ↘			2	1		REPLACE EXISTING FACES
53		↖ B		↖ B			2	1		REPLACE EXISTING FACES
54		B ↘		B ↘			2	1		REPLACE EXISTING FACES


REVISIONS		
NUMBER	BY	DATE




SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
AIRFIELD SIGNAGE SCHEDULE

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DESIGN BY: RLV
 DRAWN BY: DPA
 CHECKED BY: *RLV*
 APPROVED BY: *RLV*
 DATE: 04/24/07
 JOB No: 05035-03
 ILL. PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 43 OF 70 SHEETS

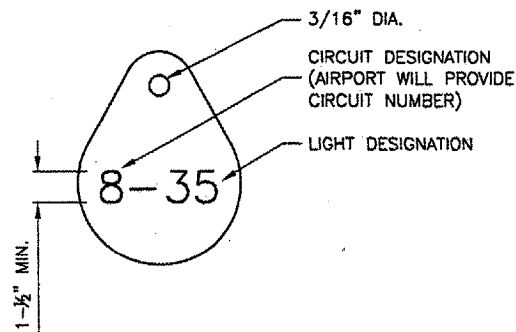
NOTES

INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.

LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.

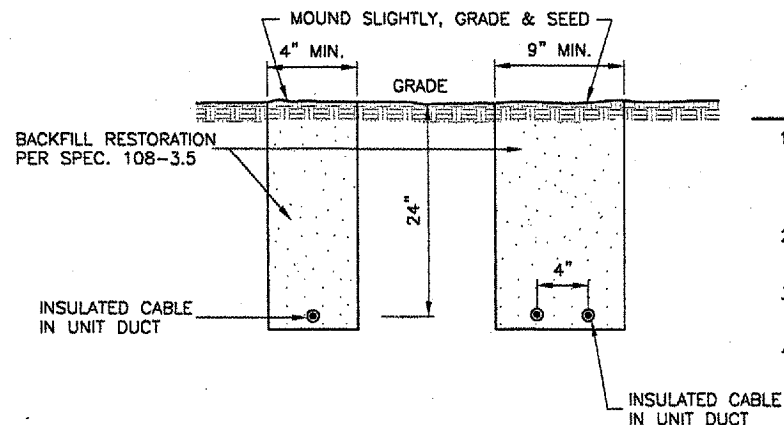
THE CONTRACTOR SHALL NUMBER THE EXISTING/PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.

AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.



LIGHT IDENTIFICATION DETAIL

N.T.S.



TRENCH DETAIL

N.T.S.

NOTES

1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO ITEM 108.

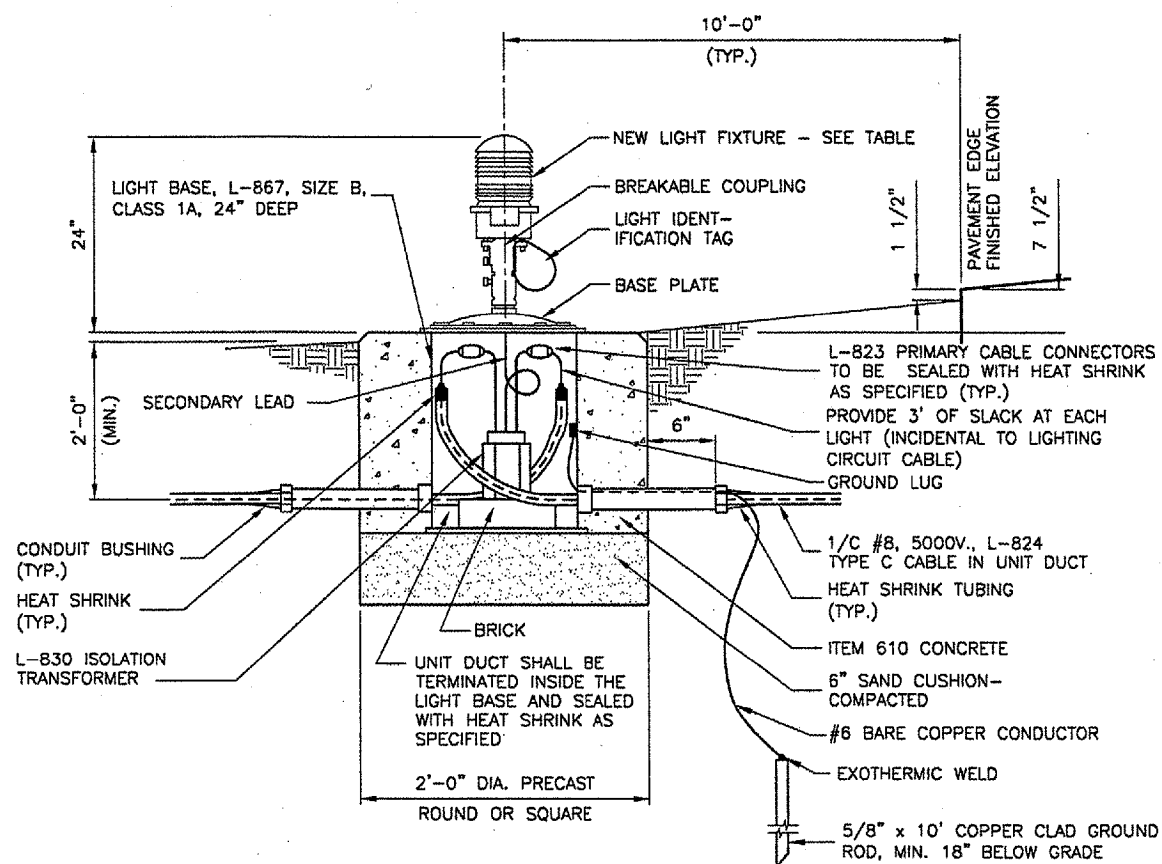
REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

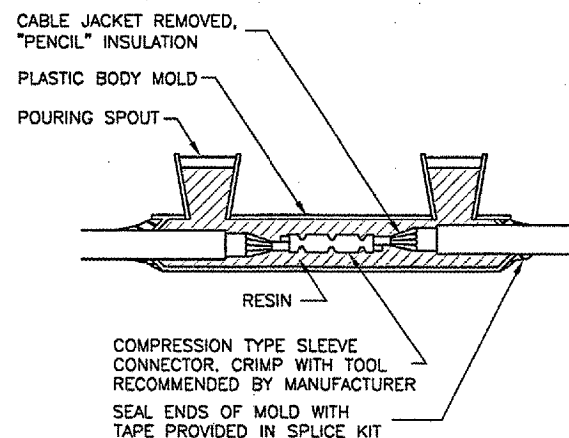
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 ELECTRICAL DETAILS 1



NEW BASE MOUNTED EDGE LIGHT

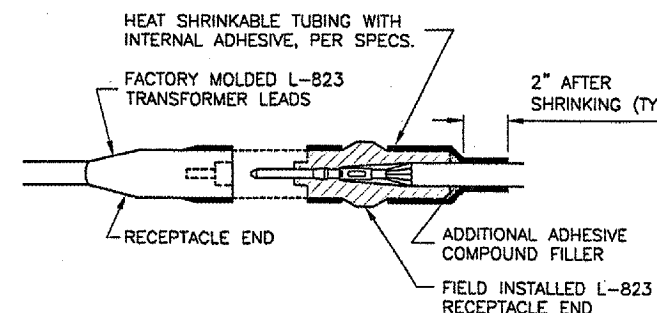
N.T.S.

FAA LIGHT FIXTURE TYPE	LAMP WATTAGE	LAMP DESCRIPTION	TRANSFORMER TYPE	TRANSFORMER PRI/SEC	HEIGHT
L-862	120	QUARTZ/BI-PIN	L-830	6.6A/6.6A	24"
L-862E	200	QUARTZ/BI-PIN	L-830	6.6A/6.6A	24"
L-861-T	30-45	INCAN. 6.6A	L-830	6.6A/6.6A	24"



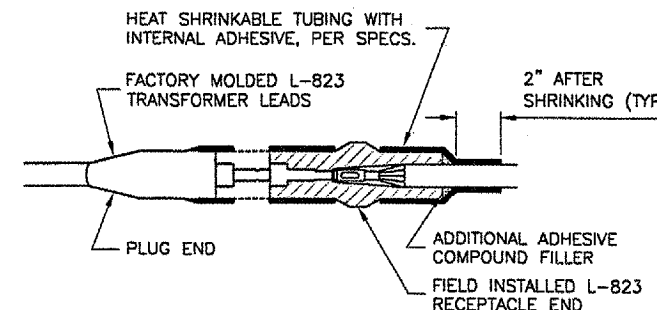
TYPE A

FOR IN-LINE CONNECTIONS OF EXISTING CABLES CUT DURING CONSTRUCTION.



TYPE C

FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.



TYPE D

FOR SPLICES AT TAXIWAY LIGHTS AND SIGNS.

CABLE SPLICES

N.T.S.

NOTES

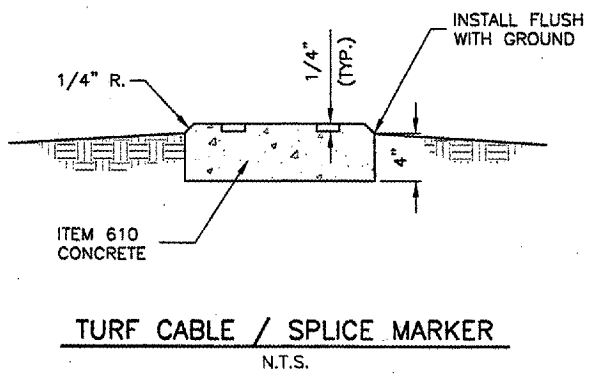
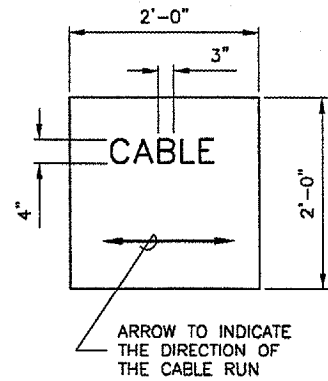
1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
2. THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
3. THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.

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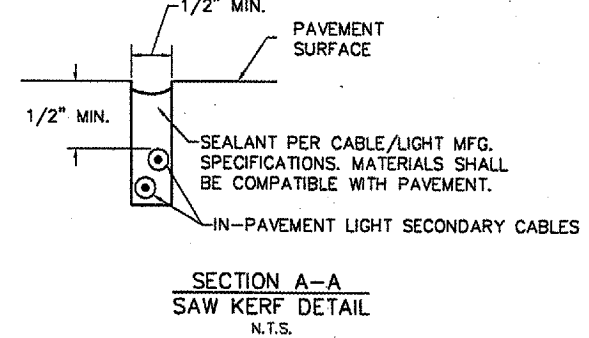
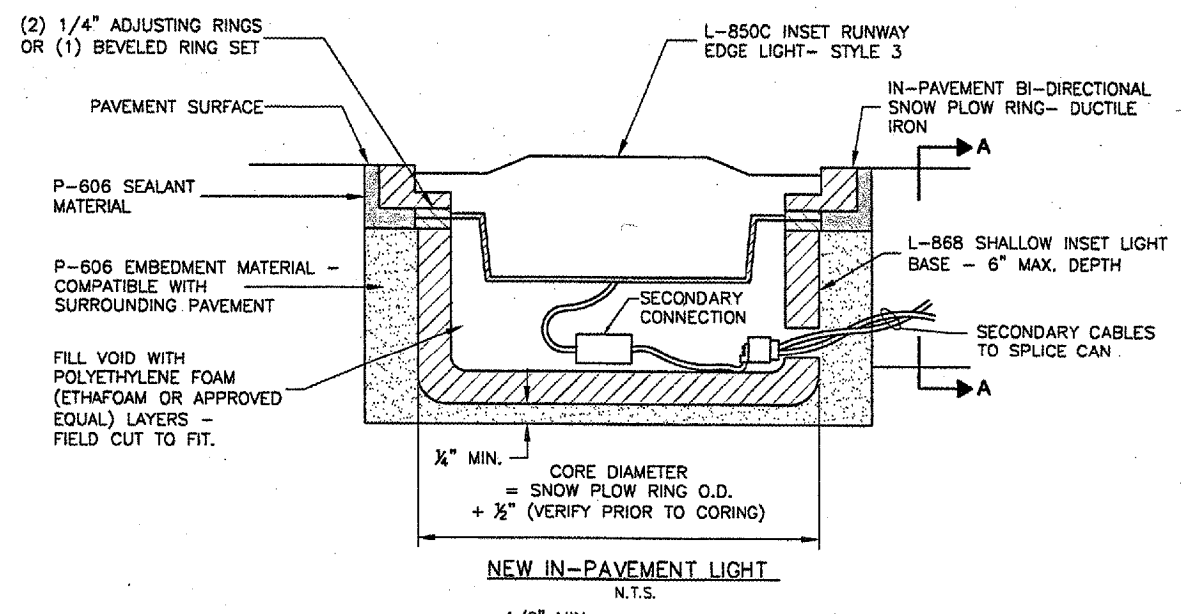
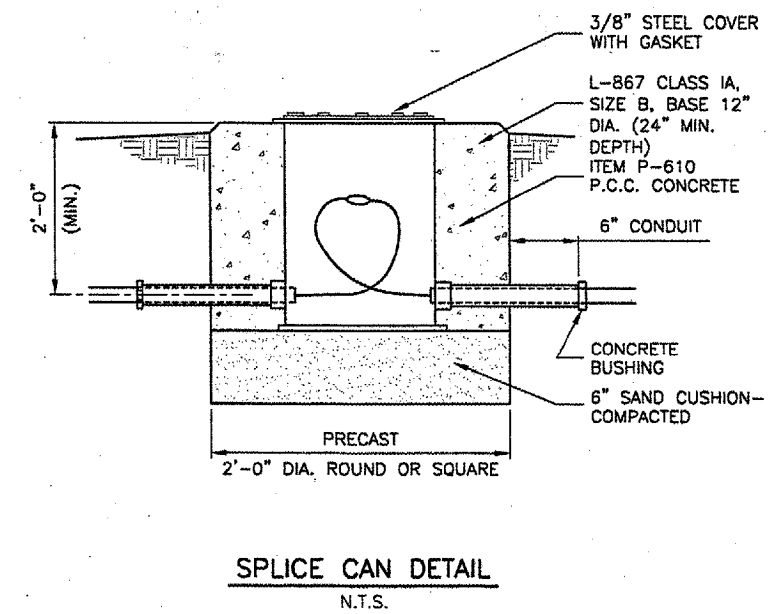
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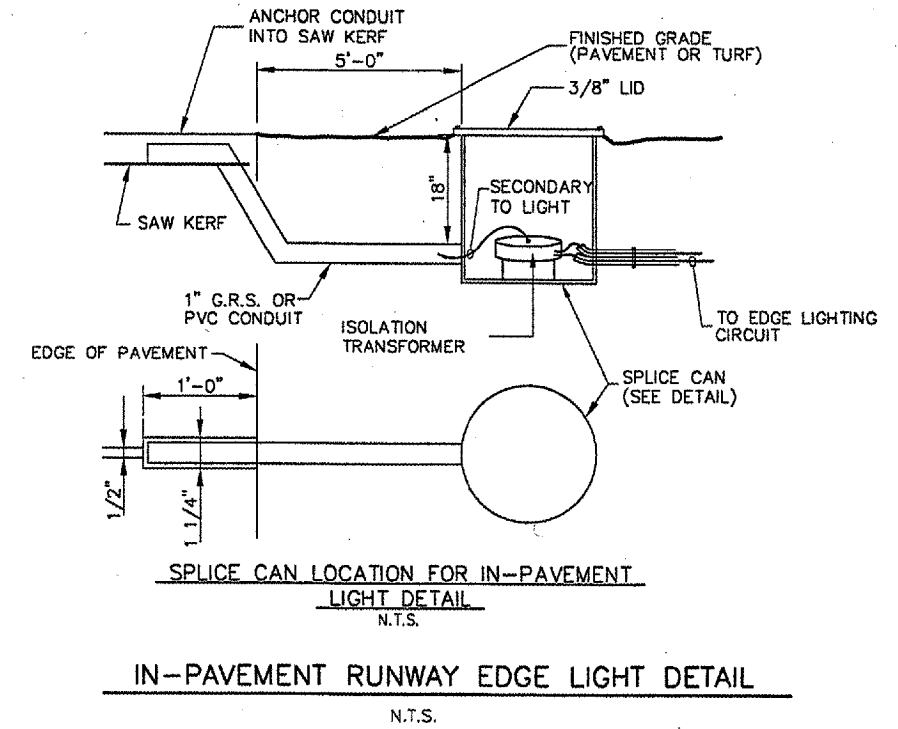
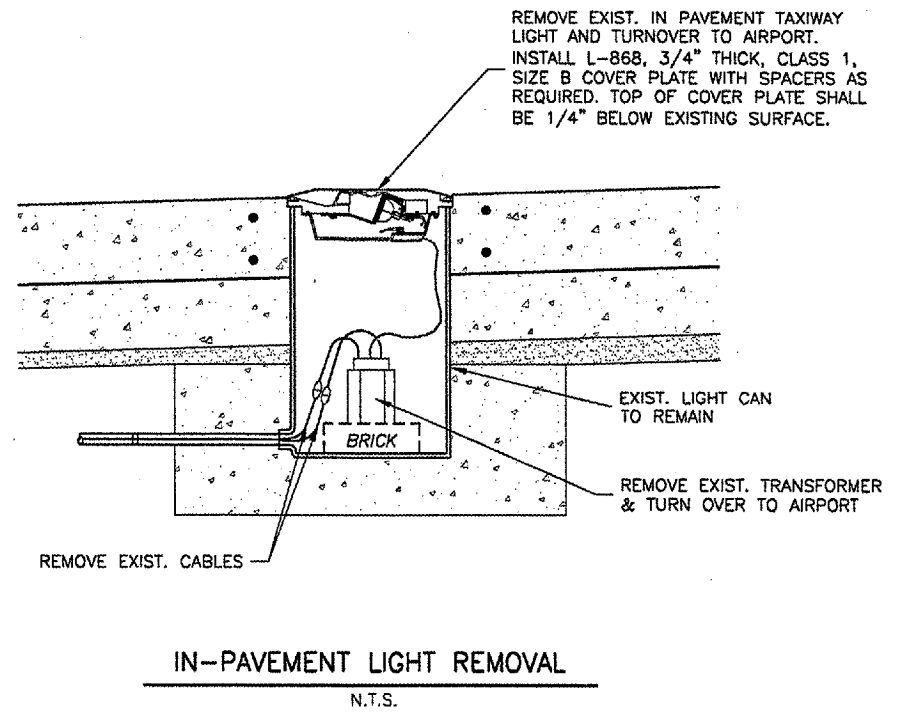


NOTES

1. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
2. ITEM 610 CONCRETE SHALL BE USED.
3. ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
4. THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
5. A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.

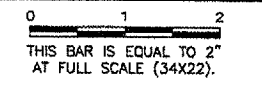


NOTE:
 SAW KERF SHALL UTILIZE PCC PAVEMENT JOINTS WHERE APPLICABLE. SAW KERF LOCATIONS SHALL BE APPROVED BY THE AIRPORT PRIOR TO START OF INSTALLATION OPERATIONS.



REVISIONS

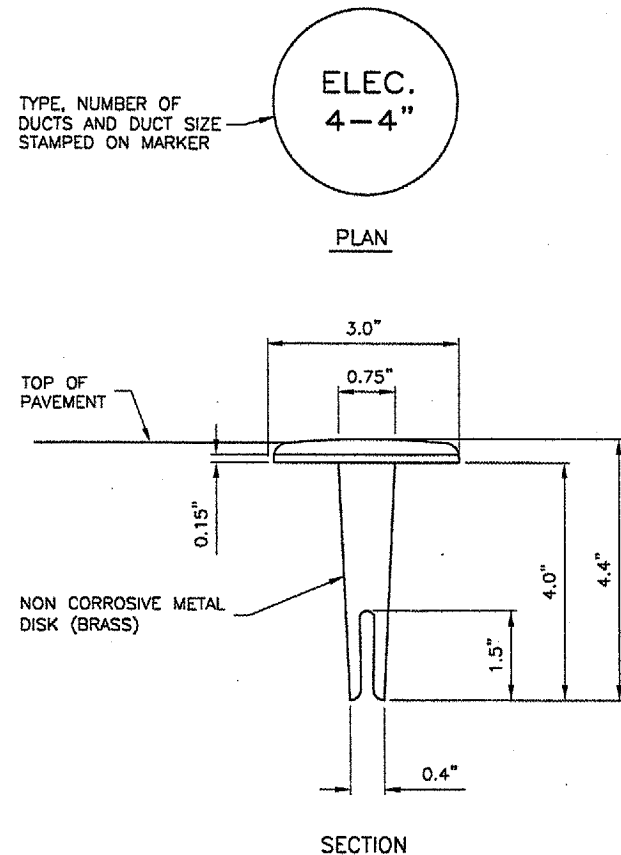
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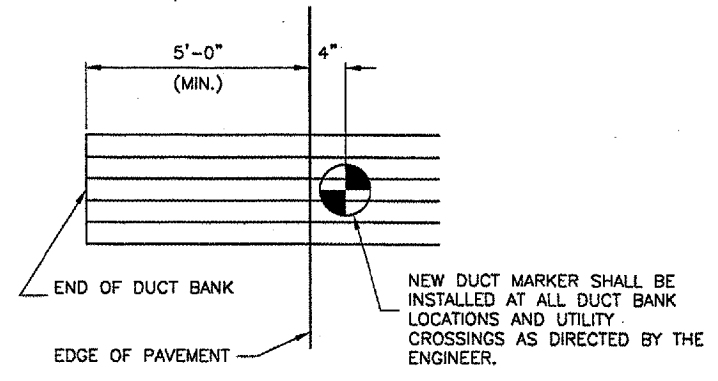
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ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
ELECTRICAL DETAILS 2

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 AIP PROJ. NO. 3-17-0096-42
 SHEET 45 OF 70 SHEETS

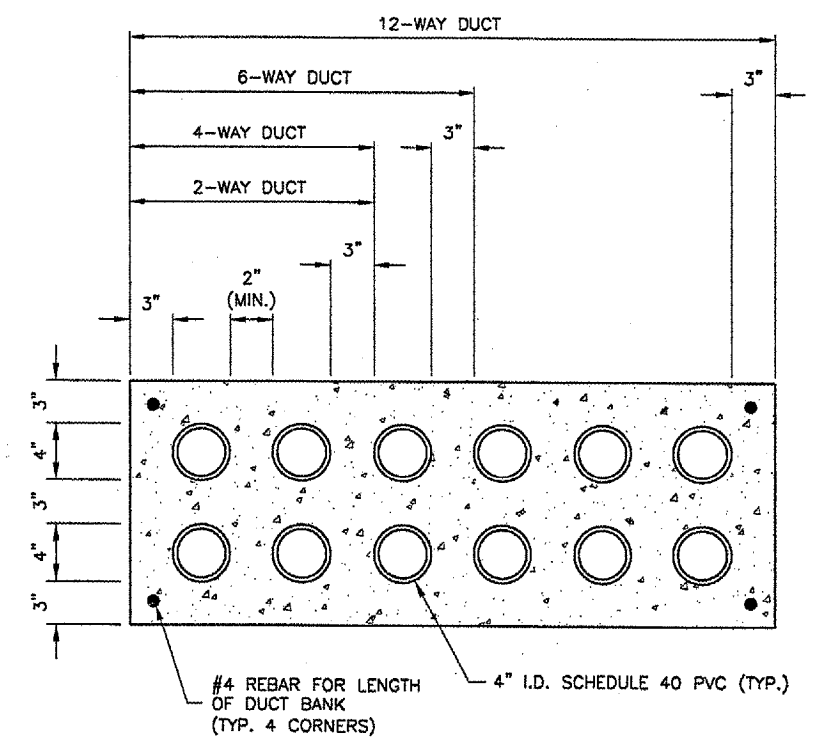


DUCT MARKER DETAILS
N.T.S.



DUCT MARKERS SHALL BE RECESSED AND GROUTED INTO THE PAVEMENTS.

MARKER PLACEMENT



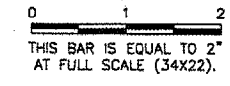
DUCT BANK DETAIL
N.T.S.

NOTES

- DIMENSIONS SHOWN ARE MINIMUM.
- TOP OF CONCRETE ENCASUREMENT TO BE NOT LESS THAN 18" BELOW FINISHED SUBGRADE.
- DUCT CONCRETE SHALL BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
- ALL DUCT SHALL BE 4" INSIDE DIAMETER.
- LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS SHALL BE INSTALLED AT LOCATIONS DESIGNATED BY THE ENGINEER.
- CONTRACTOR SHALL INSTALL DUCT BANKS AT A DEPTH WHICH WILL NOT CONFLICT WITH ELEVATION SENSITIVE UTILITIES.
- A PULL WIRE SHALL BE PROVIDED IN EACH UNUSED CONDUIT. UNUSED DUCT SHALL BE SEALED WITH PVC PLUGS/CAPS TO THE SATISFACTION OF THE ENGINEER.
- DUCT BANKS SHALL EXTEND A MINIMUM OF 5' BEYOND THE NEW EDGE OF PAVEMENT.

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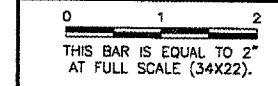
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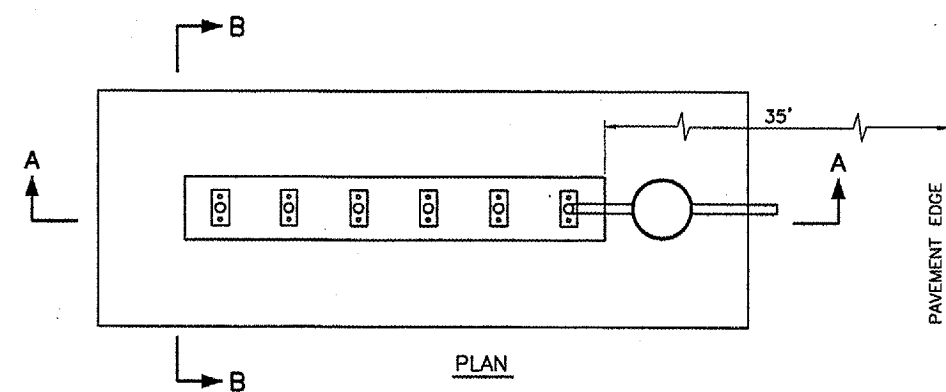
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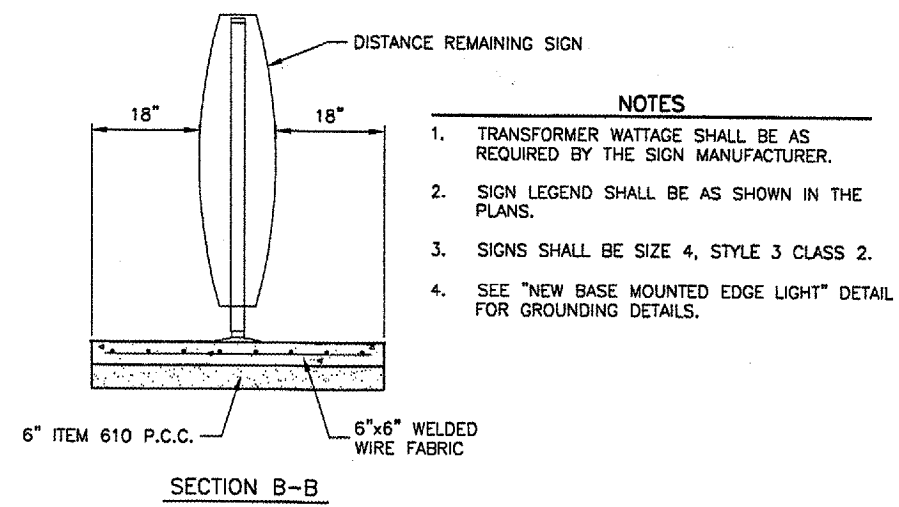
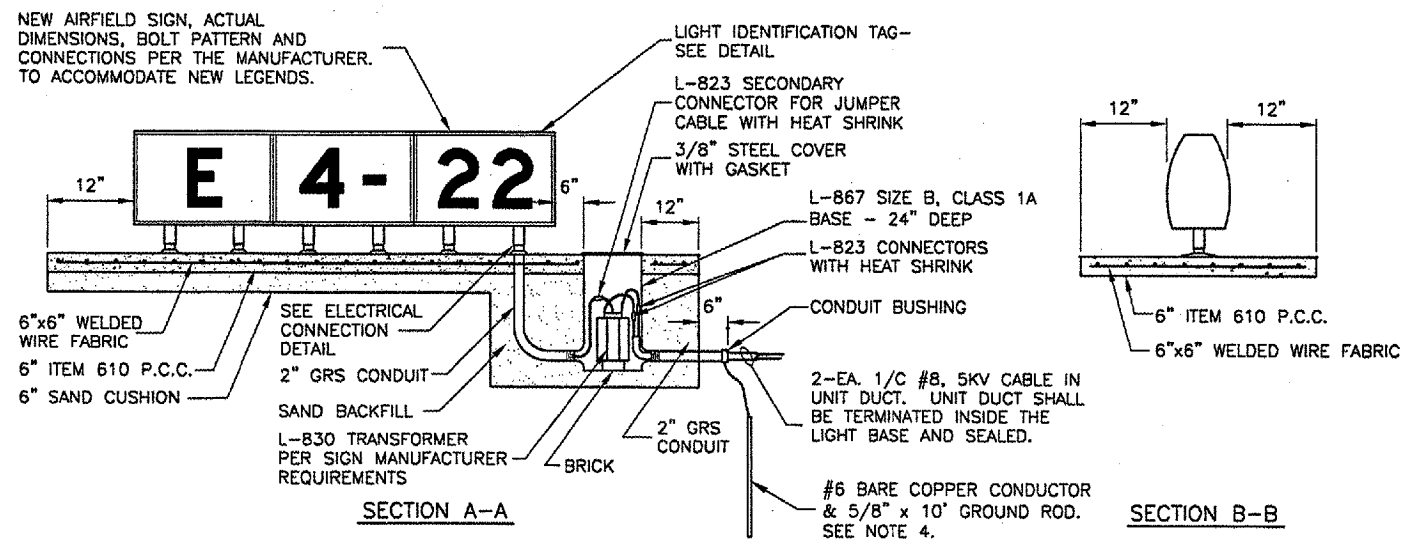
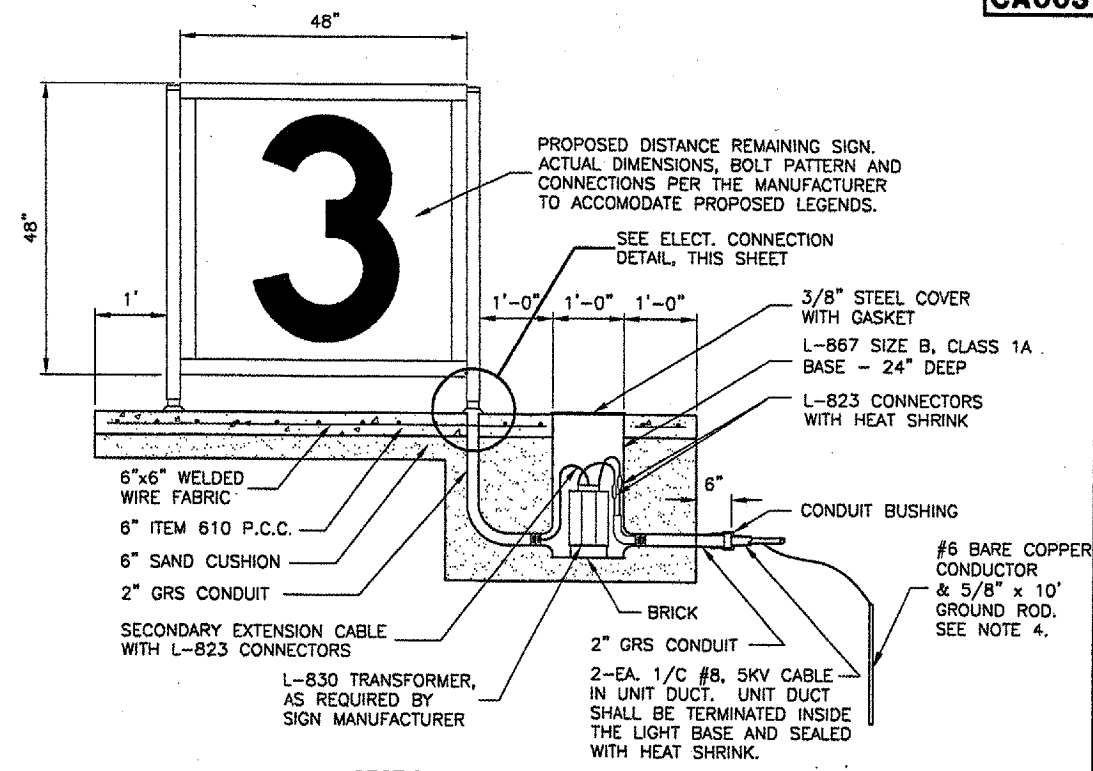
SPRINGFIELD AIRPORT AUTHORITY
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SPRINGFIELD, ILLINOIS
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
AIRFIELD SIGNAGE DETAIL

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AIP PROJ. NO. 3-17-0096-42	
SHEET 47 OF 79 SHEETS	

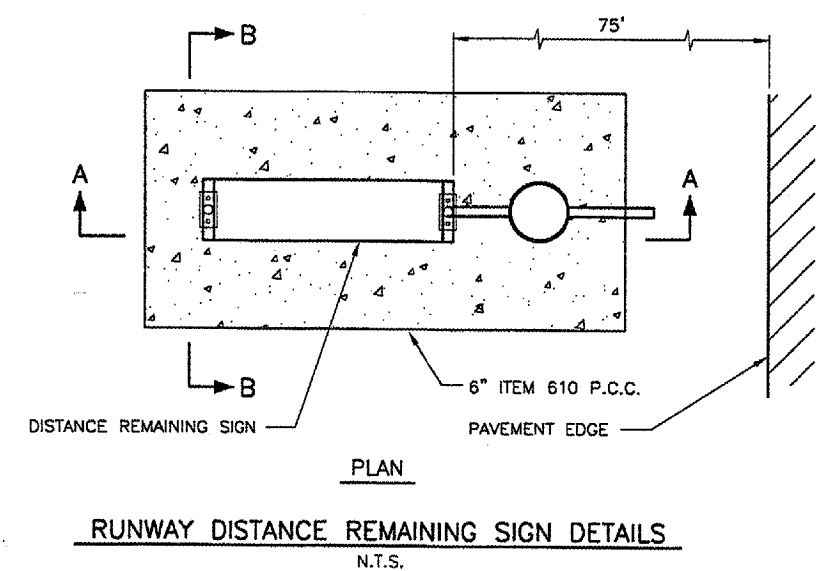
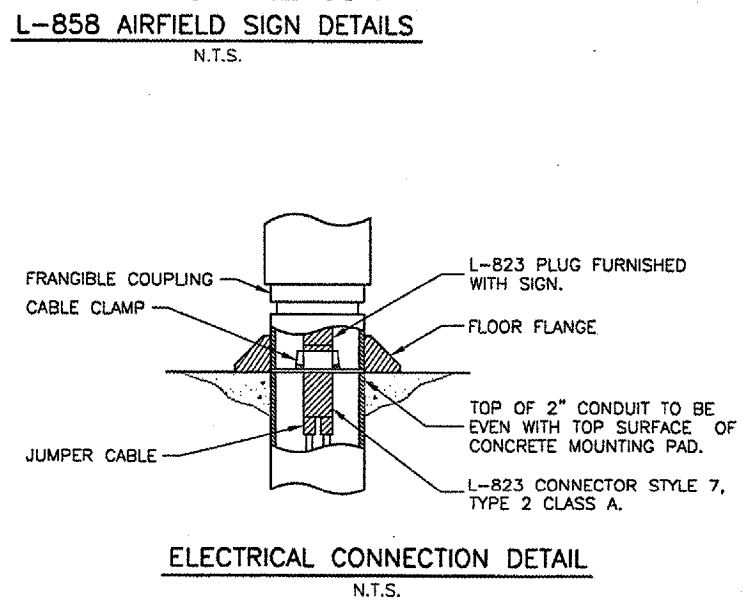


- AIRFIELD SIGNAGE NOTES**
- SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN.
 - CHANGES TO NEW LEGENDS MAY OCCUR SUBJECT TO 1 ABOVE.
 - CONTRACTOR SHALL SUBMIT SHOP DRAWINGS WITH NEW LEGENDS FOR APPROVAL PRIOR TO STARTING MANUFACTURE.
 - EXISTING SIGNS ARE LUMACURVE.

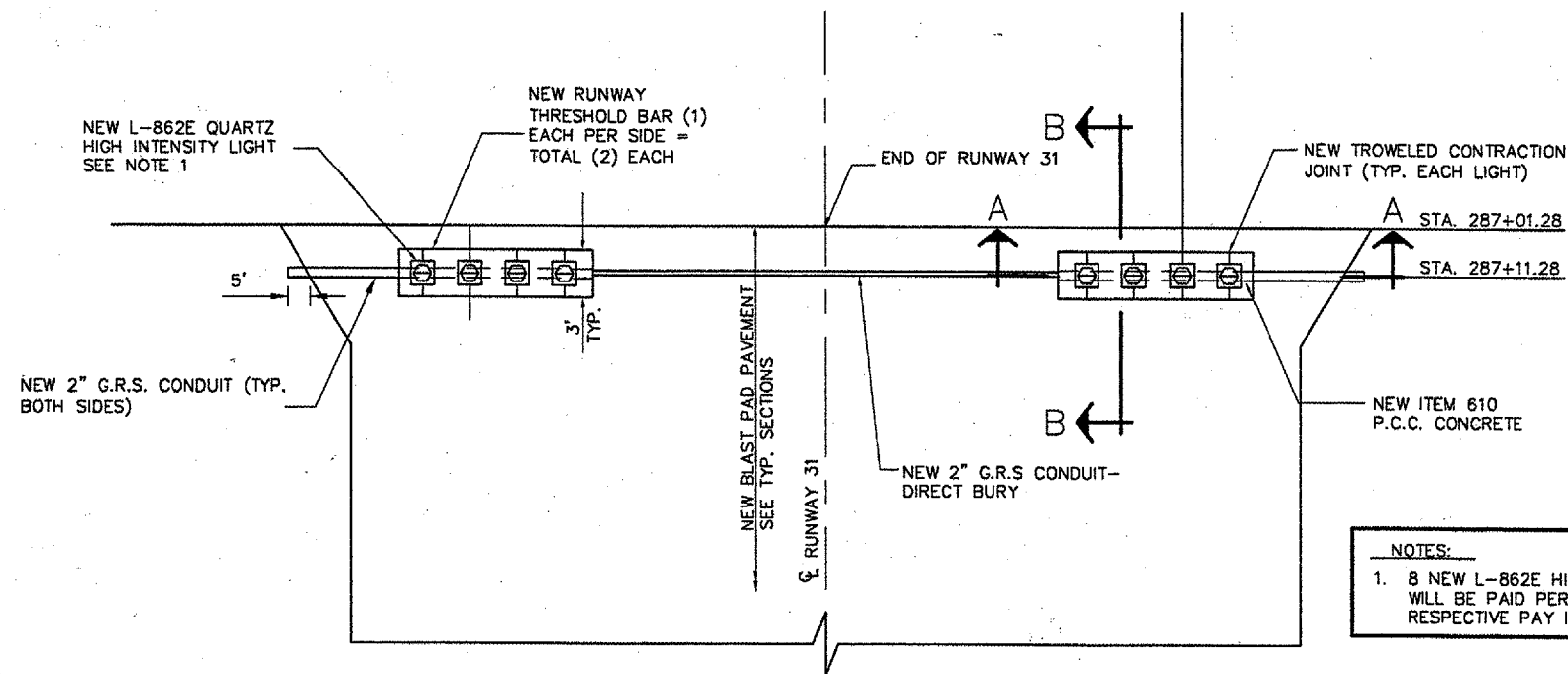


- NOTES**
- TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY THE SIGN MANUFACTURER.
 - SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
 - SIGNS SHALL BE SIZE 4, STYLE 3 CLASS 2.
 - SEE "NEW BASE MOUNTED EDGE LIGHT" DETAIL FOR GROUNDING DETAILS.

- NOTES**
- TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY SIGN MANUFACTURER.
 - SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS.
 - SIGNS SHALL BE SIZE 2, STYLE 2 OR 3, CLASS 2.
 - SEE "NEW BASE MOUNTED EDGE LIGHT" DETAIL FOR GROUNDING DETAILS.



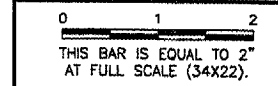
RUNWAY DISTANCE REMAINING SIGN DETAILS
N.T.S.



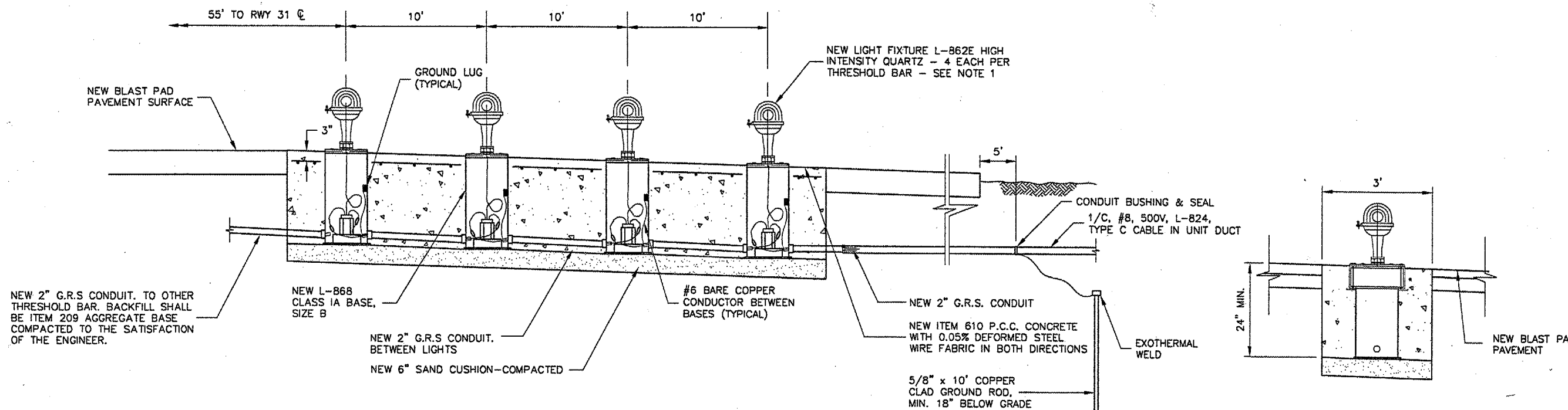
NOTES:
 1. 8 NEW L-862E HIRL THRESHOLD FIXTURES WILL BE PAID PER EACH UNDER THE RESPECTIVE PAY ITEM PER RUNWAY END.

RUNWAY 31 THRESHOLD BAR		
STATION	OFFSET	PROP. PAVEMENT ELEVATION
287+11.28	55' RT	582.19
287+11.28	65' RT	582.07
287+11.28	75' RT	581.95
287+11.28	85' RT	581.70
287+11.28	55' LT	582.26
287+11.28	65' LT	582.17
287+11.28	75' LT	582.07
287+11.28	85' LT	582.06

REVISIONS		
NUMBER	BY	DATE



PLAN
 N.T.S.



SECTION A-A TYPICAL
 LT. & RT. RUNWAY CL
 N.T.S.

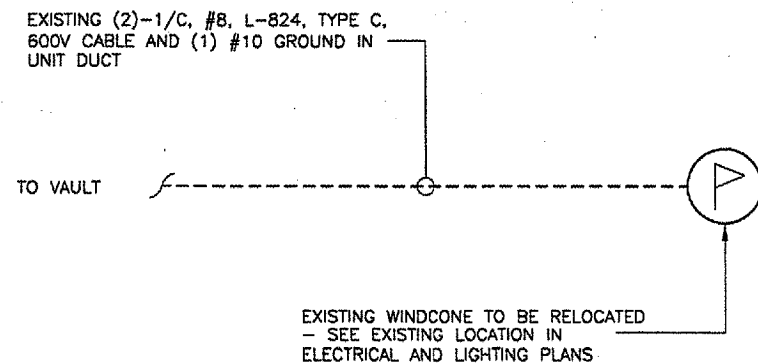
SECTION B-B
 N.T.S.

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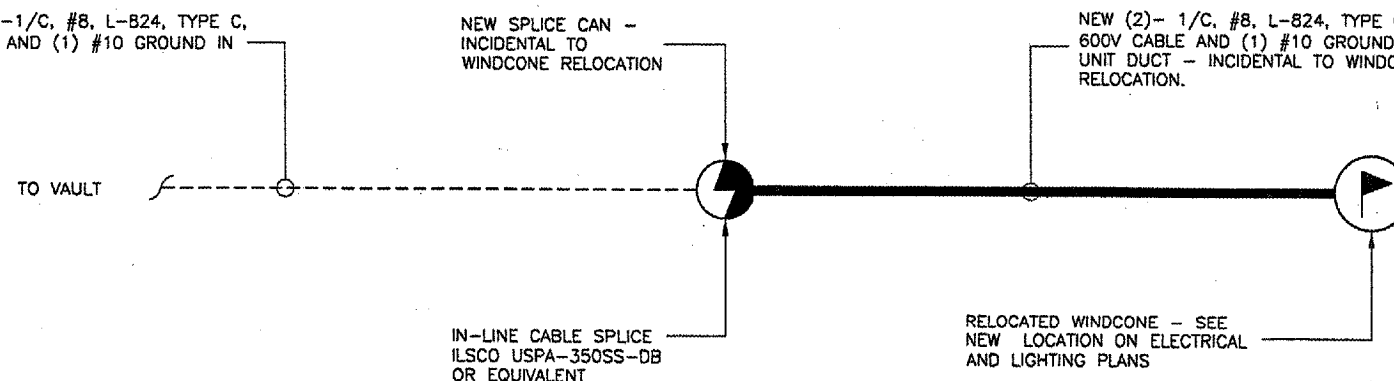
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 RUNWAY THRESHOLD BAR DETAILS

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 AIP PROJ. NO. 3-17-0096-42



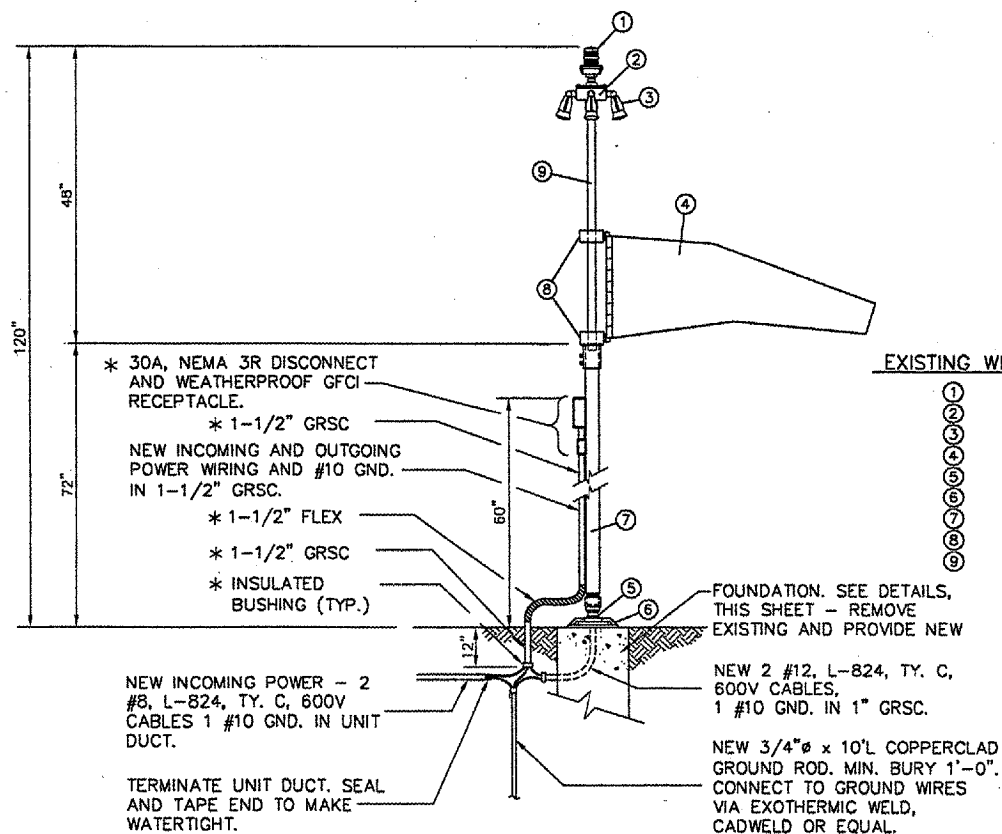
EXISTING PLAN
N.T.S.



NEW PLAN
N.T.S.

REVISIONS		
NUMBER	BY	DATE

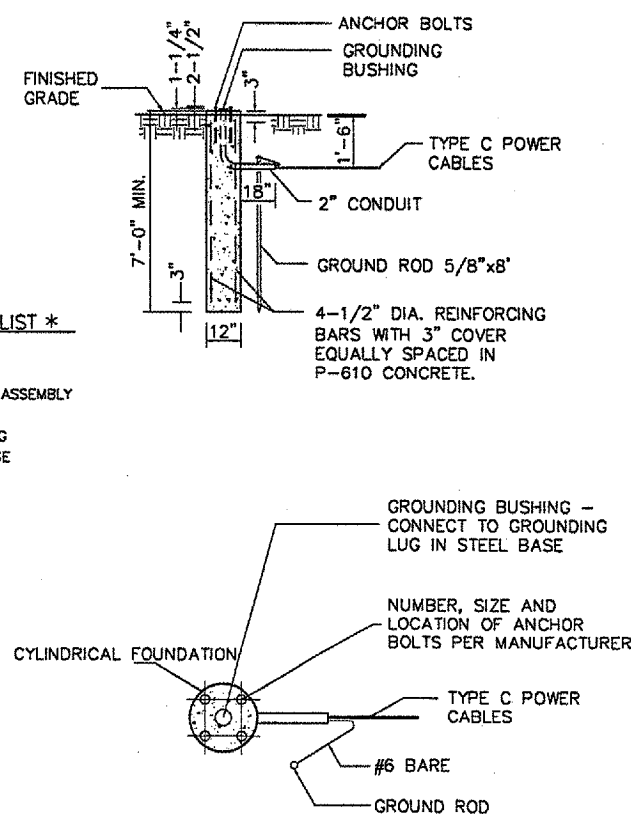
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



NOTE:
 * - INDICATES MATERIAL TO BE RELOCATED AND RE-USED.

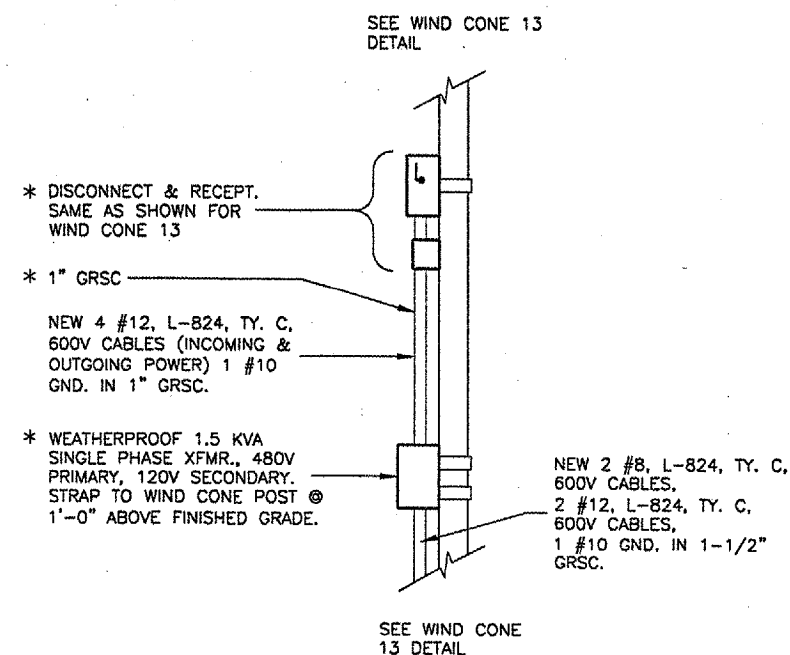
WIND CONE 13 DETAIL
N.T.S.

- EXISTING WIND CONE EQUIP. LIST ***
- ① OBSTRUCTION LIGHT
 - ② JUNCTION BOX
 - ③ EXTERNAL LIGHTING ASSEMBLY
 - ④ WIND SOCK 8 FT.
 - ⑤ FRANGIBLE COUPLING
 - ⑥ POLE-SUPPORT BASE
 - ⑦ ALUMINUM MAST
 - ⑧ BEARING ASSEMBLY
 - ⑨ SHAFT ASSEMBLY



NOTE:
 ALL MATERIAL, EQUIPMENT AND LABOR ARE INCIDENTAL TO WIND CONE RELOCATION.

EXISTING AND NEW FOUNDATION DETAILS
N.T.S.



NOTE:
 * - INDICATES MATERIAL TO BE RELOCATED AND RE-USED.

WIND CONE 31 DETAIL
N.T.S.

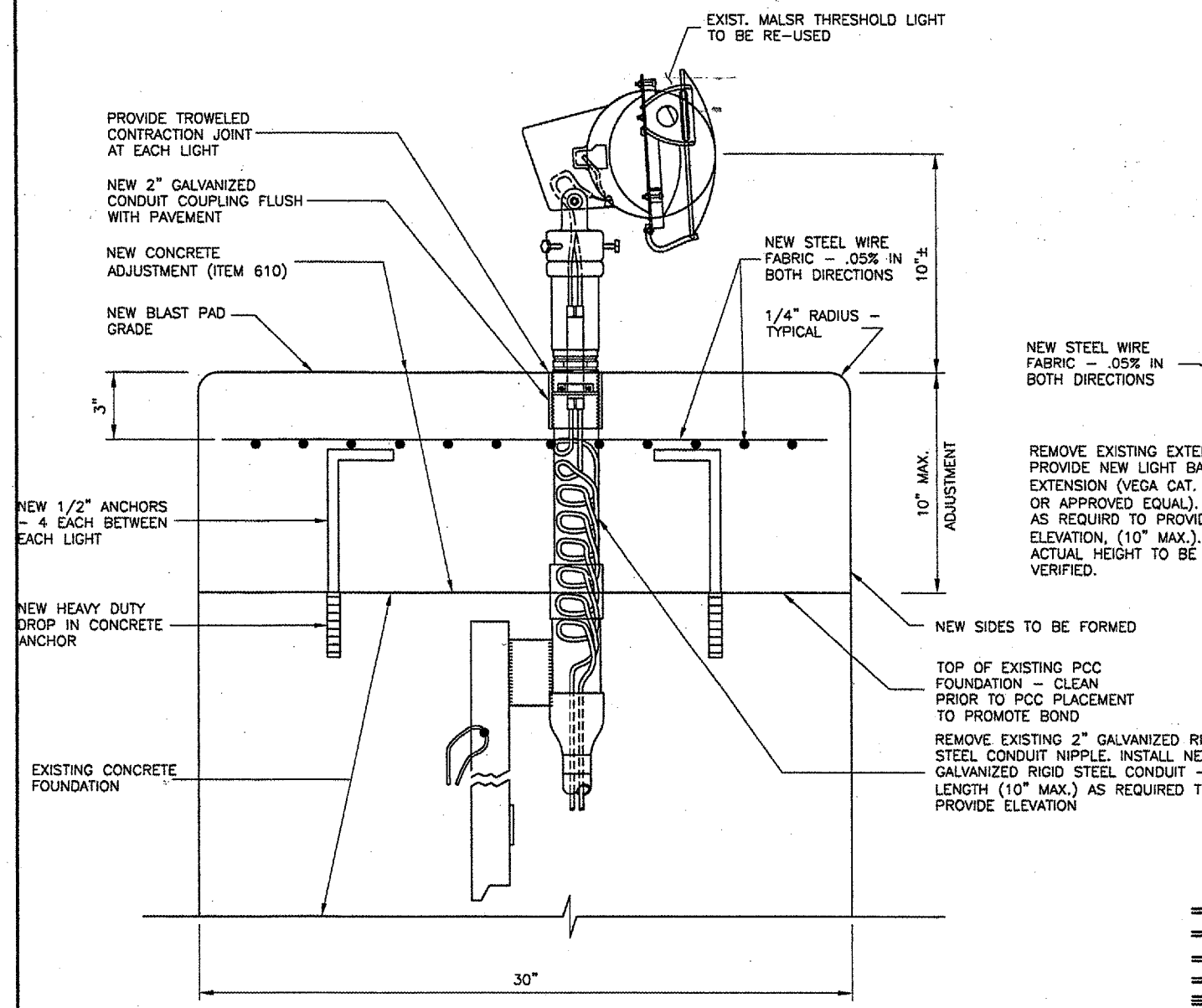
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SPRINGFIELD, ILLINOIS
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
WIND CONE DETAILS

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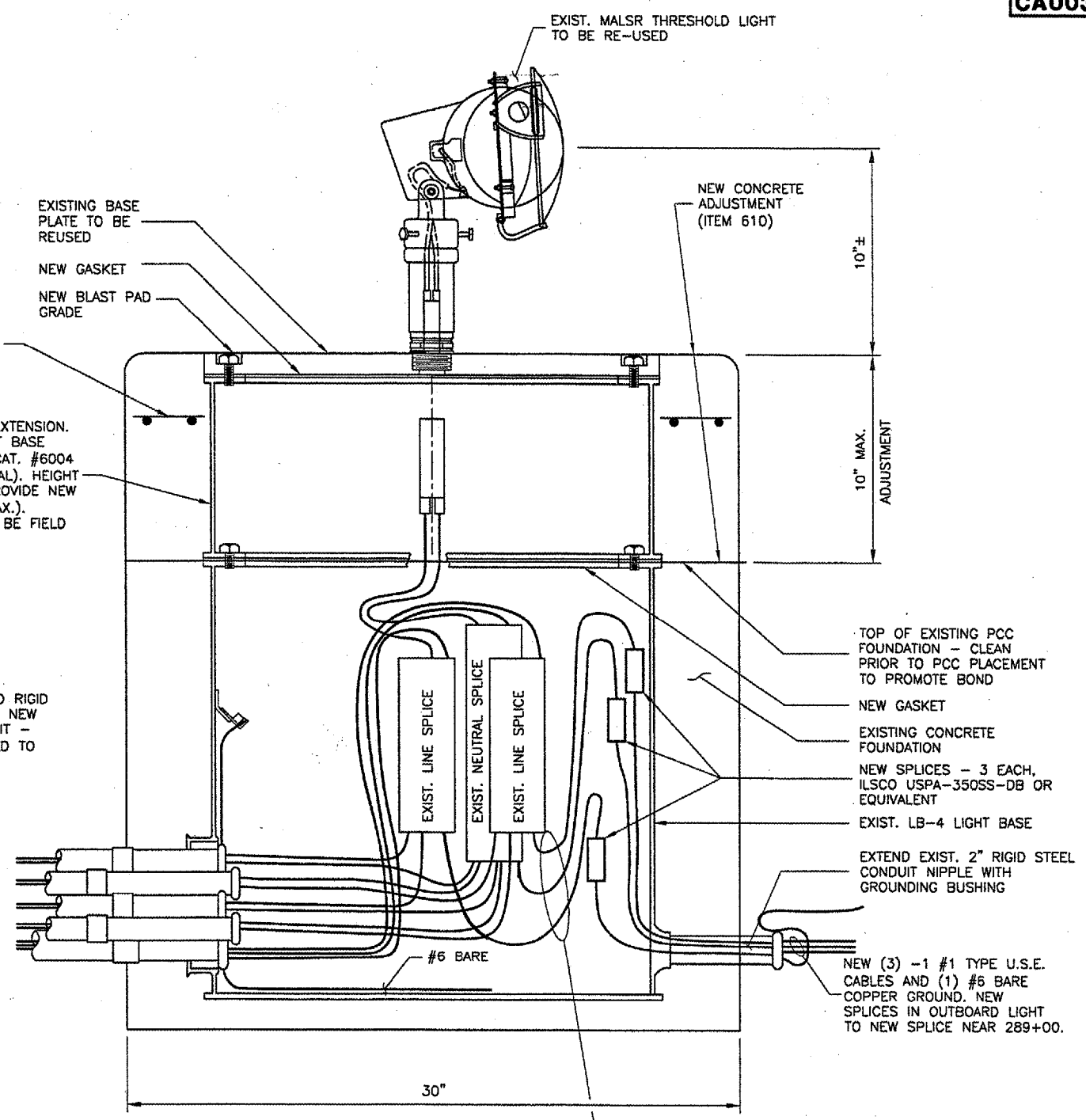
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SHEET 49 OF 79 SHEETS	

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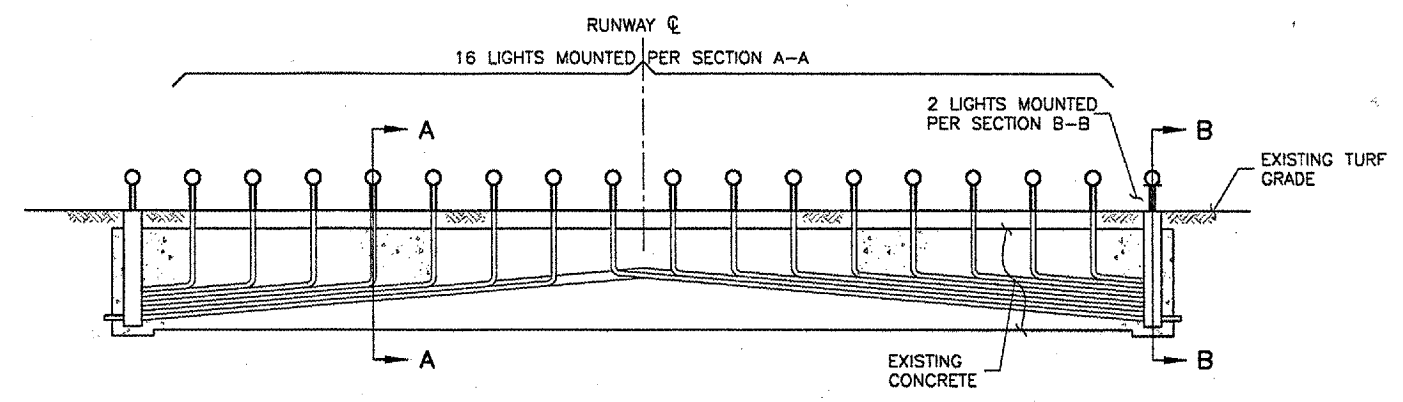
0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



SECTION A-A
 TYPICAL FRANGIBLE MALSR THRESHOLD
 CENTRAL LIGHT INSTALLATION
 TYPICAL 16 LOCATIONS
 N.T.S.



SECTION B-B
 OUTBOARD LIGHT INSTALLATION
 TYPICAL 2 LOCATIONS
 N.T.S.



VERTICAL CROSS SECTION OF EXISTING THRESHOLD BAR
 N.T.S.

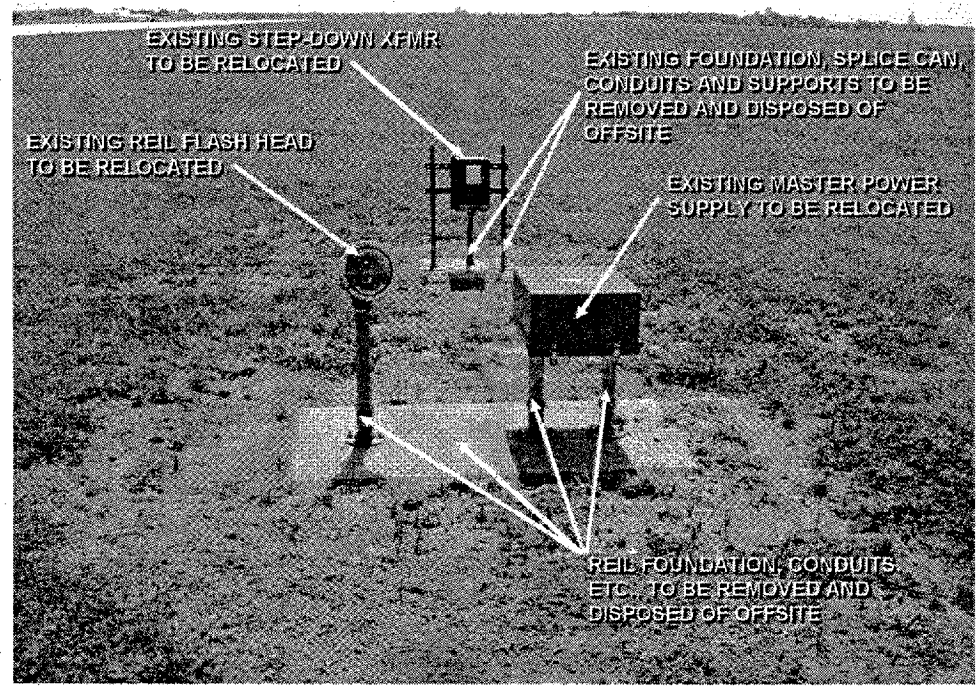
LIGHT OFFSET	NEW PAVEMENT ELEV.
85' LT.	582.10
75' LT.	582.11
65' LT.	582.21
55' LT.	582.30
45' LT.	582.41
35' LT.	582.53
25' LT.	582.63
15' LT.	582.79
5' LT.	582.96
5' RT.	582.95
15' RT.	582.80
25' RT.	582.68
35' RT.	582.55
45' RT.	582.42
55' RT.	582.29
65' RT.	582.17
75' RT.	582.04
85' RT.	581.81

SPRINGFIELD AIRPORT AUTHORITY
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SPRINGFIELD, ILLINOIS

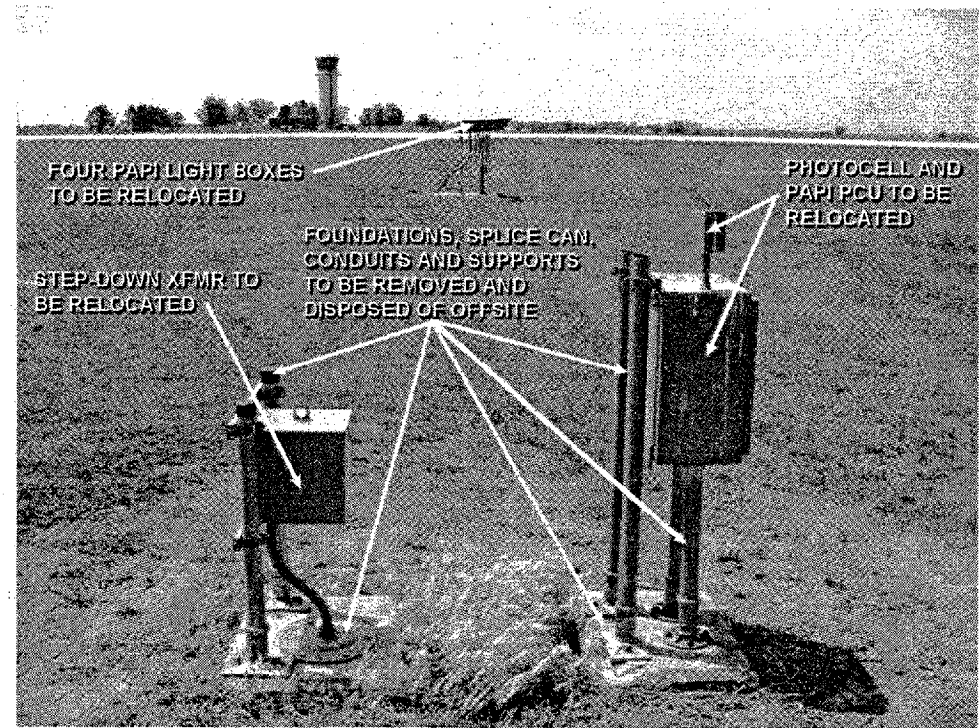
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
MALSR THRESHOLD ADJUSTMENT DETAIL

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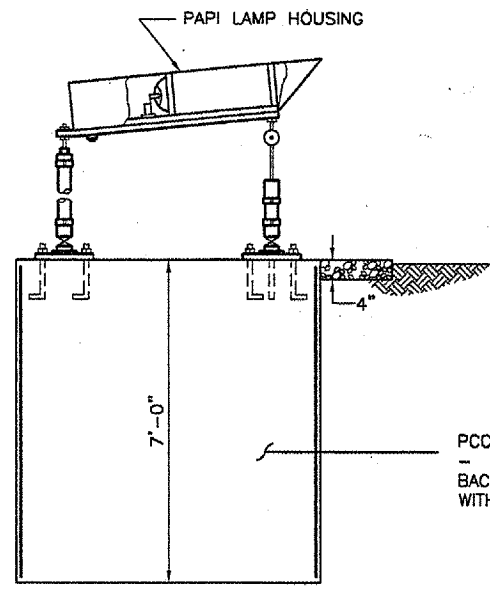
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 AIP PROJ. NO. 3-17-0096-42
 SHEET 50 OF 79 SHEETS



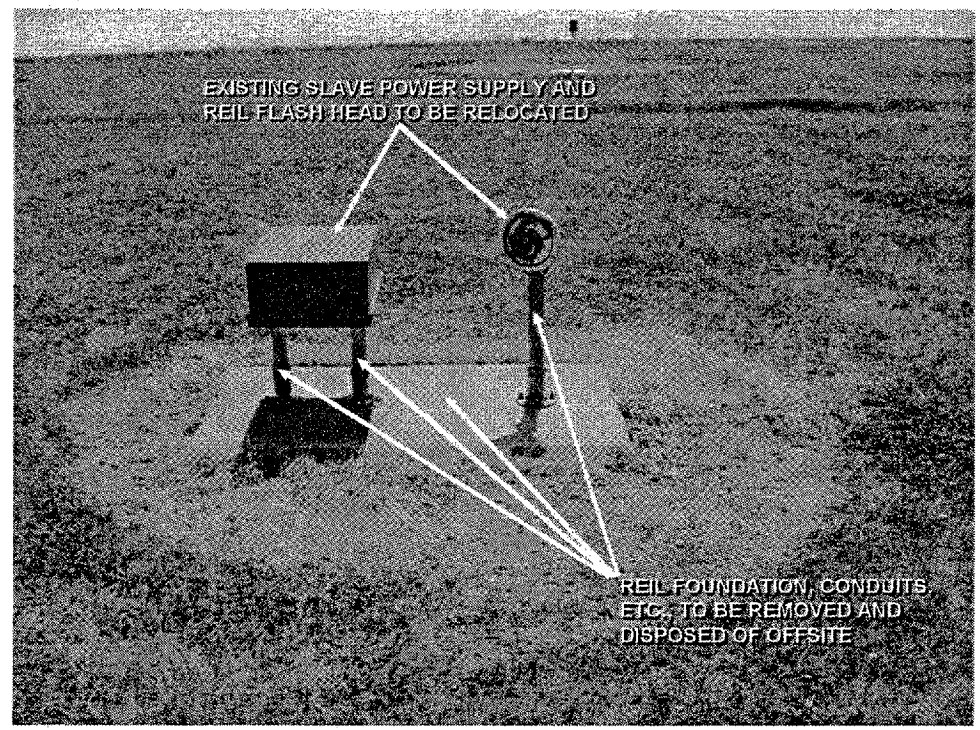
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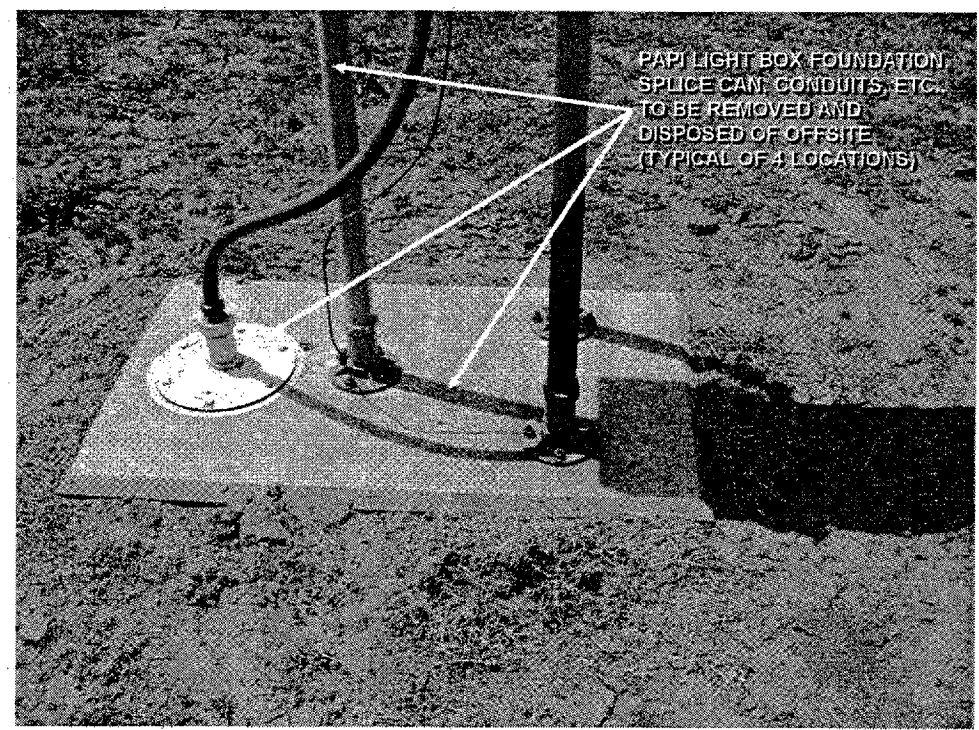
EXISTING PAPI



EXISTING PAPI FOUNDATION REMOVAL
 N.T.S.



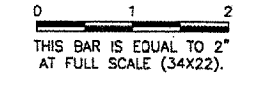
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EXISTING PAPI

REVISIONS

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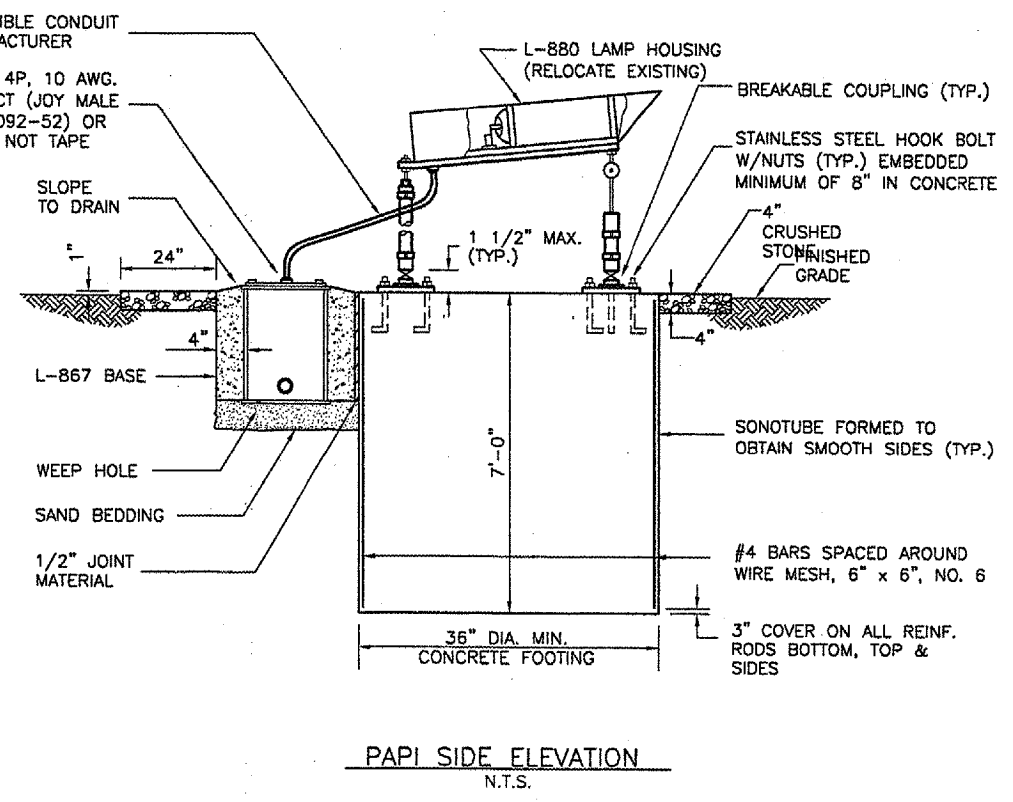
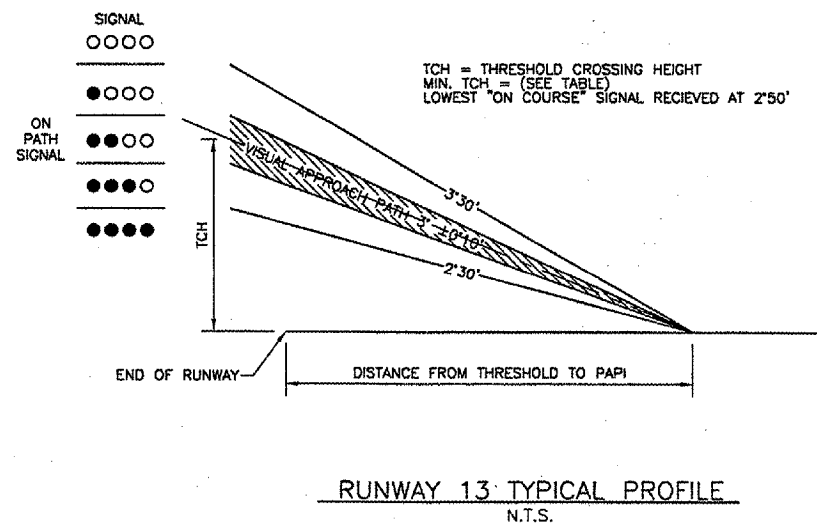
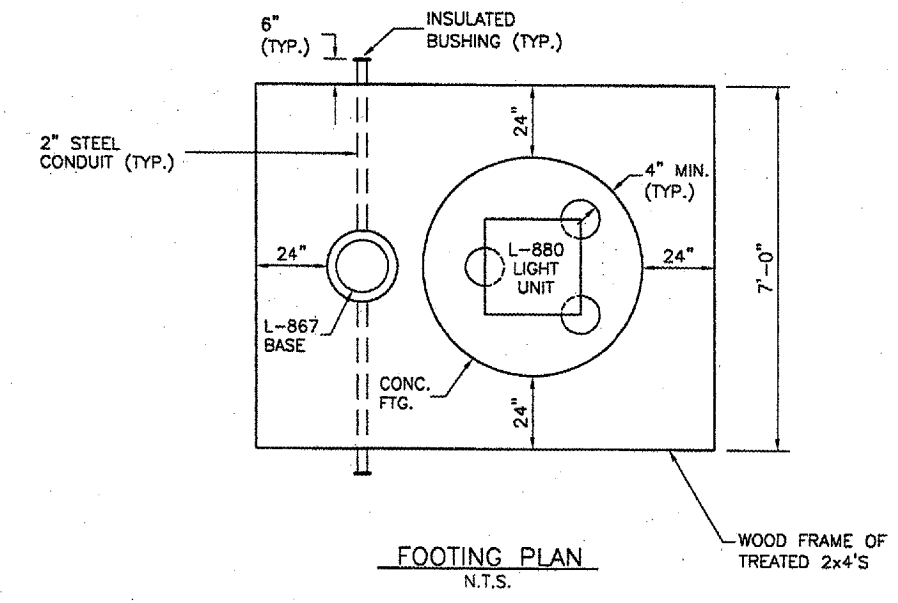
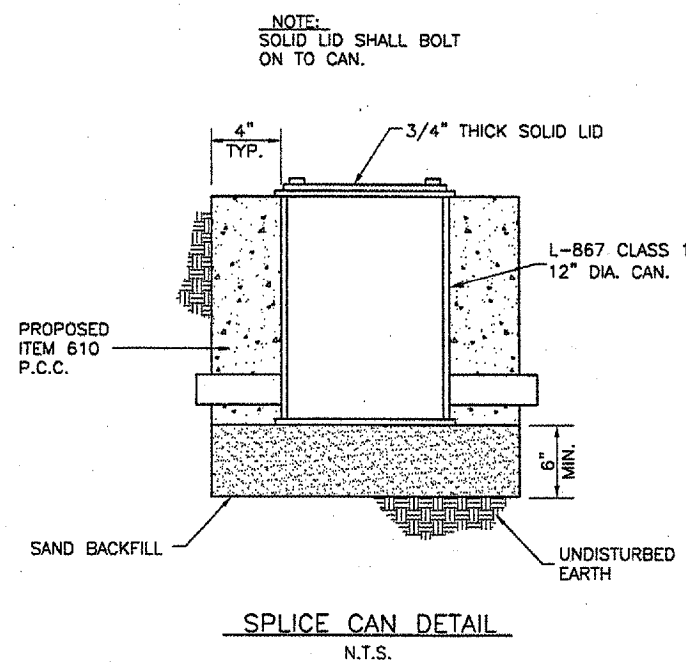
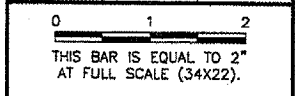
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EXISTING REIL & PAPI DETAILS

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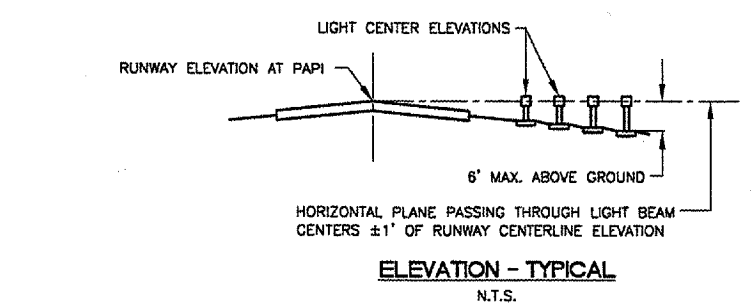
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REVISIONS		
NUMBER	BY	DATE



AIMING OF TYPE L-880 (4-BOX) PAPI RELATIVE TO
PRESLECTED GLIDE PATH (3'00')

LIGHT UNIT	AIMING ANGLE (IN MINUTES OF ARC)	STANDARD INSTALLATION
UNIT NEAREST RUNWAY	30' ABOVE GLIDE PATH	
NEXT ADJACENT UNIT	10' ABOVE GLIDE PATH	
NEXT ADJACENT UNIT	10' BELOW GLIDE PATH	
NEXT ADJACENT UNIT	30' BELOW GLIDE PATH	



FOUNDATIONS:
 FOUNDATIONS FOR MOUNTING LIGHT BOXES SHALL BE MADE OF ITEM 610 CONCRETE. ALL LIGHT BOXES SHALL BE FRANGIBLY MOUNTED TO THE FOUNDATION.

AZIMUTHAL AIMING:
 EACH LIGHT UNIT SHALL BE AIMED OUTWARD INTO THE APPROACH ZONE ON A LINE PARALLEL TO THE RUNWAY CENTERLINE WITHIN A TOLERANCE OF ±1/2 DEGREE.

MOUNTING HEIGHT TOLERANCES:
 THE BEAM CENTERS OF ALL LIGHT UNITS SHALL BE WITHIN ±1 INCH OF A HORIZONTAL PLANE AT THE ELEVATION GIVEN IN THE TABLE.

TOLERANCE ALONG LINE PERPENDICULAR TO RUNWAY:
 THE FRONT FACE OF EACH LIGHT UNIT IN A BAR SHALL BE LOCATED ON A LINE PERPENDICULAR TO THE RUNWAY CENTERLINE WITHIN ±6 INCHES.

PAPI INSTALLATION DETAIL
N.T.S.

RUNWAY	13
HEIGHT GROUP USED FOR SITING	3
THRESHOLD STATIONING	213+00.55
THRESHOLD ELEVATION	587.77'
THRESHOLD CROSSING HEIGHT	50'
STATION FOR MIDPOINT OF PROJECTORS	222+36.74
GLIDE PATH ANGLE *	3°
ELEVATION @ OF APERTURE	588.73
UNIT 1 EXISTING GROUND ELEVATION	586.65
UNIT 2 EXISTING GROUND ELEVATION	585.78
UNIT 3 EXISTING GROUND ELEVATION	585.19
UNIT 4 EXISTING GROUND ELEVATION	584.58

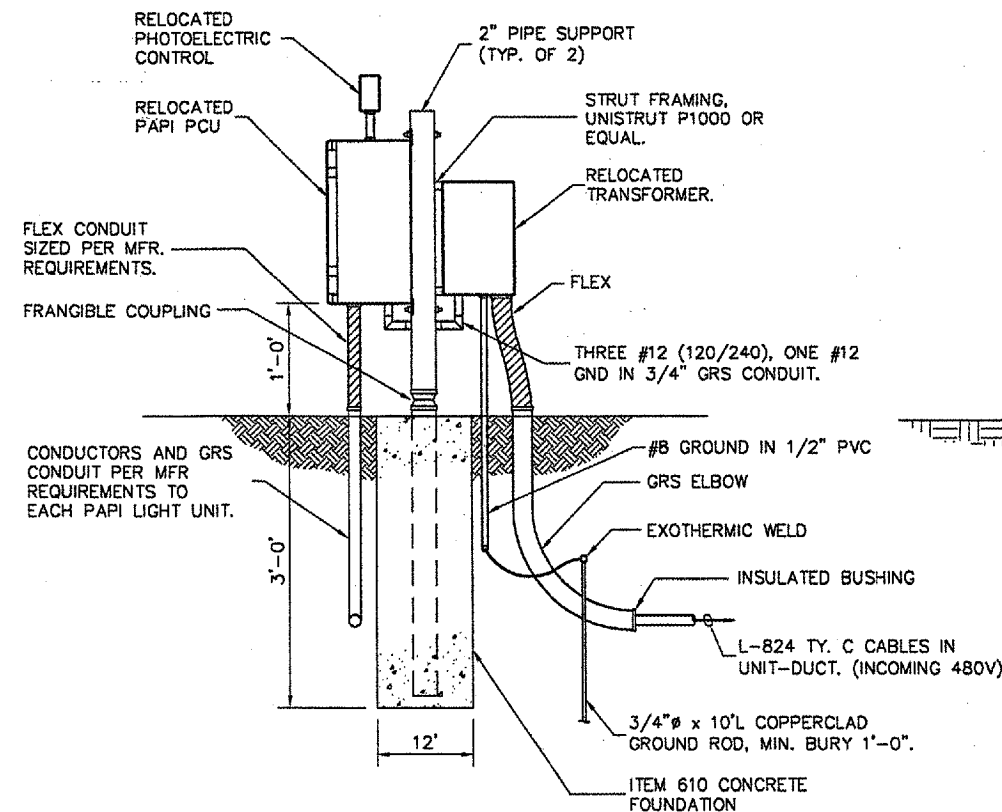
* THE VISUAL GLIDE PATH ANGLE IS THE CENTER OF THE ON COURSE ZONE AND IS MEASURED FROM THE HORIZONTAL

PAPI-L-880 (4BOX)
N.T.S.

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SPRINGFIELD, ILLINOIS
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PAPI DETAIL

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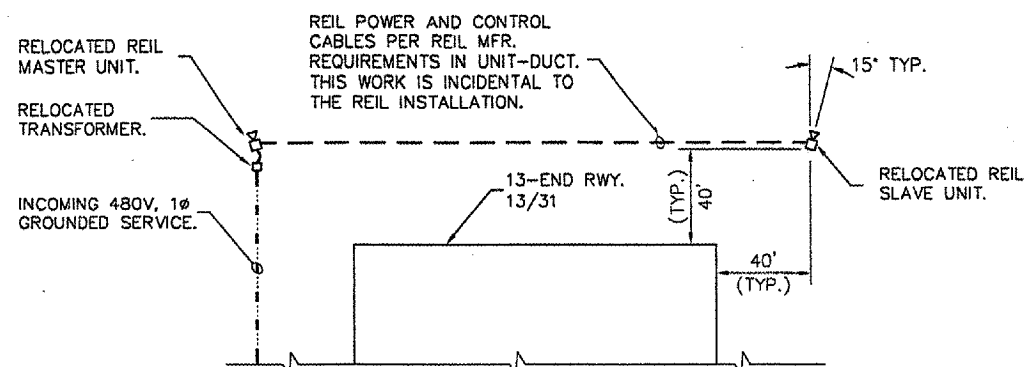
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IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42



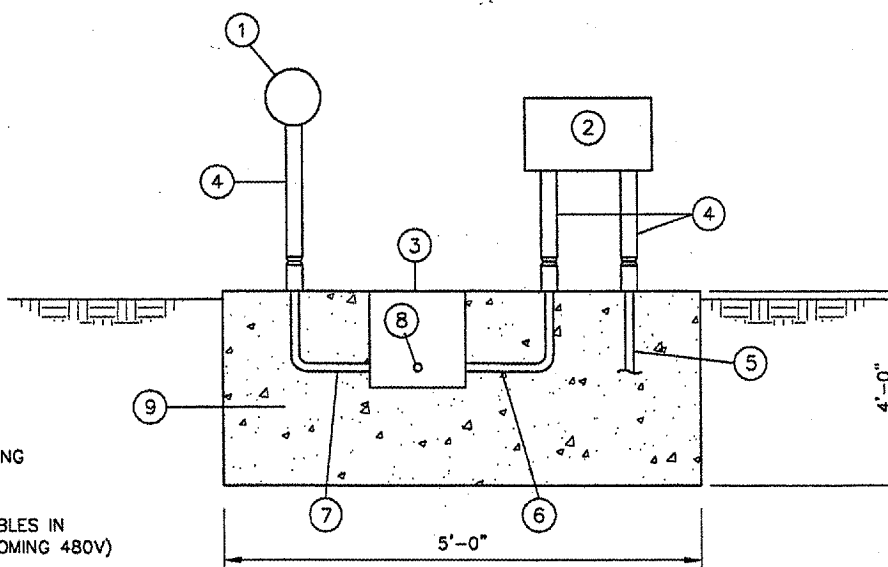
RELOCATED PAPI PCU WIRING DETAIL
 N.T.S.

RELOCATED PAPI & REIL NOTES

1. PAPI-13: 2-1/C #8, L-824, TY. C, 600V CABLES, 1 #10 GND. IN UNIT-DUCT.
2. REIL-13: 2-1/C #6, L-824, TY. C, 600V CABLES, 1 #8 GND. IN UNIT-DUCT.
3. GRS CONDUIT IN DIRECT CONTACT WITH THE EARTH SHALL RECEIVE 1 COAT 8 DRY MILS OF 46-413 TNEME-TAR COAL TAR EPOXY AS MANUFACTURED BY TNE MEC CO., OR EQUAL.
4. ALL FLEX AND GRS CONDUIT, PVC CONDUIT, UNIT-DUCT, GROUND ROD, TRANSFORMER, STRUT FRAMING, SUPPORTS, FOUNDATION, L-867 BASES, WIRING AND MISCELLANEOUS MATERIALS, LABOR AND EQUIPMENT NEEDED TO PROVIDE POWER AND CONTROL FROM PAPI PCU TO LIGHT BOXES OR REIL MASTER TO SLAVE UNIT SHALL BE CONSIDERED INCIDENTAL TO THE PAPI OR REIL INSTALLATION. SEE SPEC ITEM 125, INCOMING 480V, L-824, TY. C, 600V CABLES WITH GROUND IN UNIT-DUCT SHALL BE MEASURED AND PAID FOR UNDER APPLICABLE PAY ITEM IN ITEM 108 OF SPECIFICATIONS.



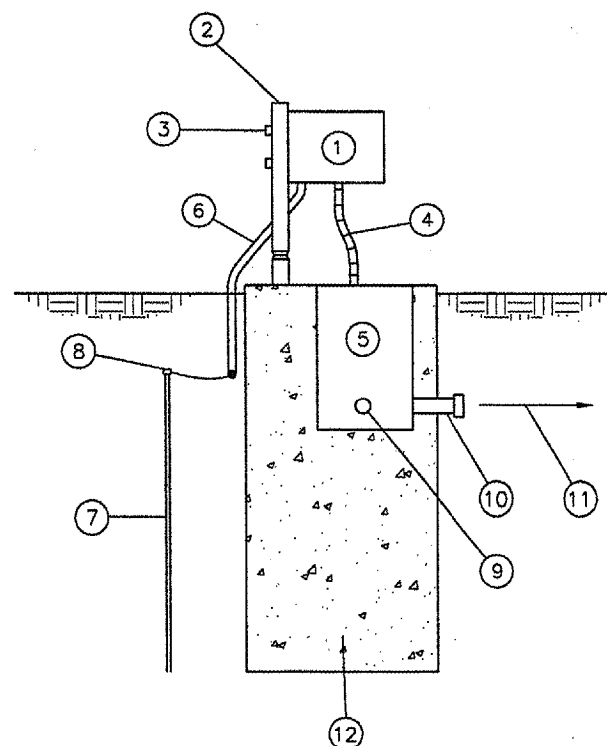
REIL-13 INSTALLATION PLAN VIEW
 N.T.S.



REIL ELEVATION
 N.T.S.

REIL ELEVATION NOMENCLATURE

- 1 RELOCATED REIL FLASH HEAD
- 2 RELOCATED REIL MASTER OR SLAVE POWER SUPPLY.
- 3 L-867 SPLICE CAN, 12" DIAMETER x 12" DEEP, WITH BLANK COVER. (REQUIRED AT REIL MASTER LOCATION ONLY.)
- 4 2" PIPE SUPPORT WITH FRANGIBLE COUPLING AND FLOOR FLANGE. CAP SUPPORT.
- 5 AT REIL MASTER LOCATION:
 - THREE #12 L-824, 600V CABLES (120/240V POWER), ONE #12 GROUND IN UNIT DUCT TO STEP-DOWN TRANSFORMER. INSTALL UNIT DUCT IN GRS CONDUIT WITHIN LIMITS OF FOUNDATION.
 AT REIL SLAVE LOCATION:
 - CONDUCTORS PER MANUFACTURER IN UNIT DUCT FROM REIL MASTER LOCATION. INSTALL UNIT DUCT IN GRS CONDUIT WITHIN LIMITS OF FOUNDATION.
- 6 AT REIL MASTER LOCATION:
 - REIL MASTER FLASH HEAD WIRING PER MANUFACTURER AND CONDUCTORS PER MANUFACTURER TO REIL SLAVE LOCATION IN GRS CONDUIT.
 AT REIL SLAVE LOCATION:
 - REIL SLAVE FLASH HEAD WIRING PER MANUFACTURER IN GRS CONDUIT. (L-867 SPLICE CAN NOT REQUIRED.)
- 7 REIL FLASH HEAD WIRING PER MANUFACTURER IN GRS CONDUIT.
- 8 AT REIL MASTER LOCATION:
 - CONDUCTORS PER MANUFACTURER TO REIL SLAVE LOCATION IN UNIT DUCT. INSTALL UNIT DUCT IN GRS CONDUIT WITHIN LIMITS OF FOUNDATION.
- 9 ITEM 610 CONCRETE FOUNDATION, 5'-0" LONG BY 2'-0" WIDE BY 4'-0" DEEP.

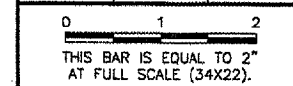


REIL TRANSFORMER ELEVATION
 N.T.S.

REIL TRANSFORMER ELEVATION NOMENCLATURE

- 1 RELOCATED STEP-DOWN TRANSFORMER.
- 2 2" PIPE SUPPORT WITH FRANGIBLE COUPLING AND FLOOR FLANGE. CAP SUPPORT. (TYPICAL OF TWO)
- 3 STRUT FRAMING, UNISTRUT P1000, OR EQUIVALENT.
- 4 TWO #6 L-824, 600V CABLES (INCOMING 480V), ONE #8 GROUND, THREE #12 L-824, 600V CABLES (OUTGOING 120/240V), ONE #12 GROUND IN LIQUIDTIGHT FLEXIBLE CONDUIT, O-Z GEDNEY FLEXI-GUARD TYPE UAG, OR EQUIVALENT.
- 5 L-867 SPLICE CAN, 12" DIAMETER x 18" DEEP, WITH THREADED HUB COVER SIZED AS REQUIRED FOR FLEXIBLE CONDUIT.
- 6 #6 GROUND IN 1/2" PVC CONDUIT.
- 7 3/4" DIAMETER x 10' LONG COPPERCLAD GROUND ROD. MINIMUM BURY: 1'-0".
- 8 EXOTHERMIC WELD, CADWELD, OR EQUIVALENT.
- 9 TWO #6 L-824, 600V CABLES (INCOMING 480V), ONE #8 GROUND IN UNIT DUCT. INSTALL UNIT DUCT IN GRS CONDUIT WITHIN LIMITS OF FOUNDATION. SEE CABLING HOMERUN PLAN SHEET FOR ADDITIONAL INFORMATION.
- 10 GRS CONDUIT WITH INSULATING BUSHING. (TYPICAL ALL GRS CONDUIT INSTALLATIONS IN PAPI AND REIL FOUNDATIONS.)
- 11 THREE #12 L-824, 600V CABLES (OUTGOING 120/240V TO REIL MASTER LOCATION), ONE #12 GROUND IN UNIT DUCT.
- 12 ITEM 610 CONCRETE FOUNDATION, 2'-0" SQUARE, 4'-0" DEEP.

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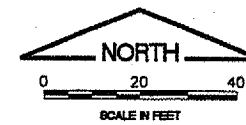
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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS
 CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 RELOCATED REIL AND PAPI DETAILS

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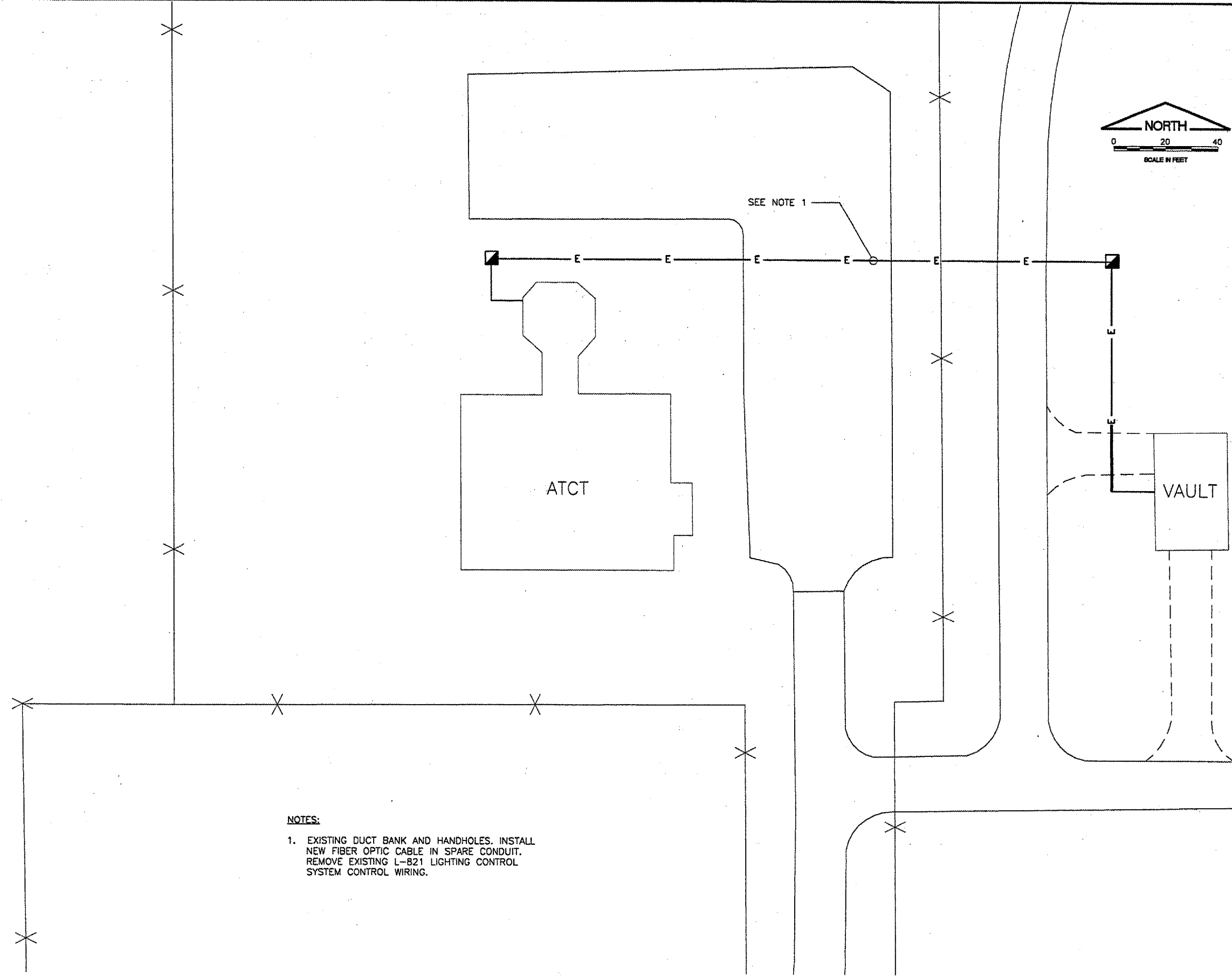
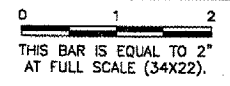
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PLOT DATE: 4/30/2007 3:40 PM
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NOTES:

- EXISTING DUCT BANK AND HANDHOLES. INSTALL NEW FIBER OPTIC CABLE IN SPARE CONDUIT. REMOVE EXISTING L-821 LIGHTING CONTROL SYSTEM CONTROL WIRING.

VAULT AND ATCT SITE PLAN


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EXISTING VAULT AND ATCT SITE PLAN

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 AIP PROJ. NO. 3-17-0096-42
 SHEET 54 OF 79 SHEETS

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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



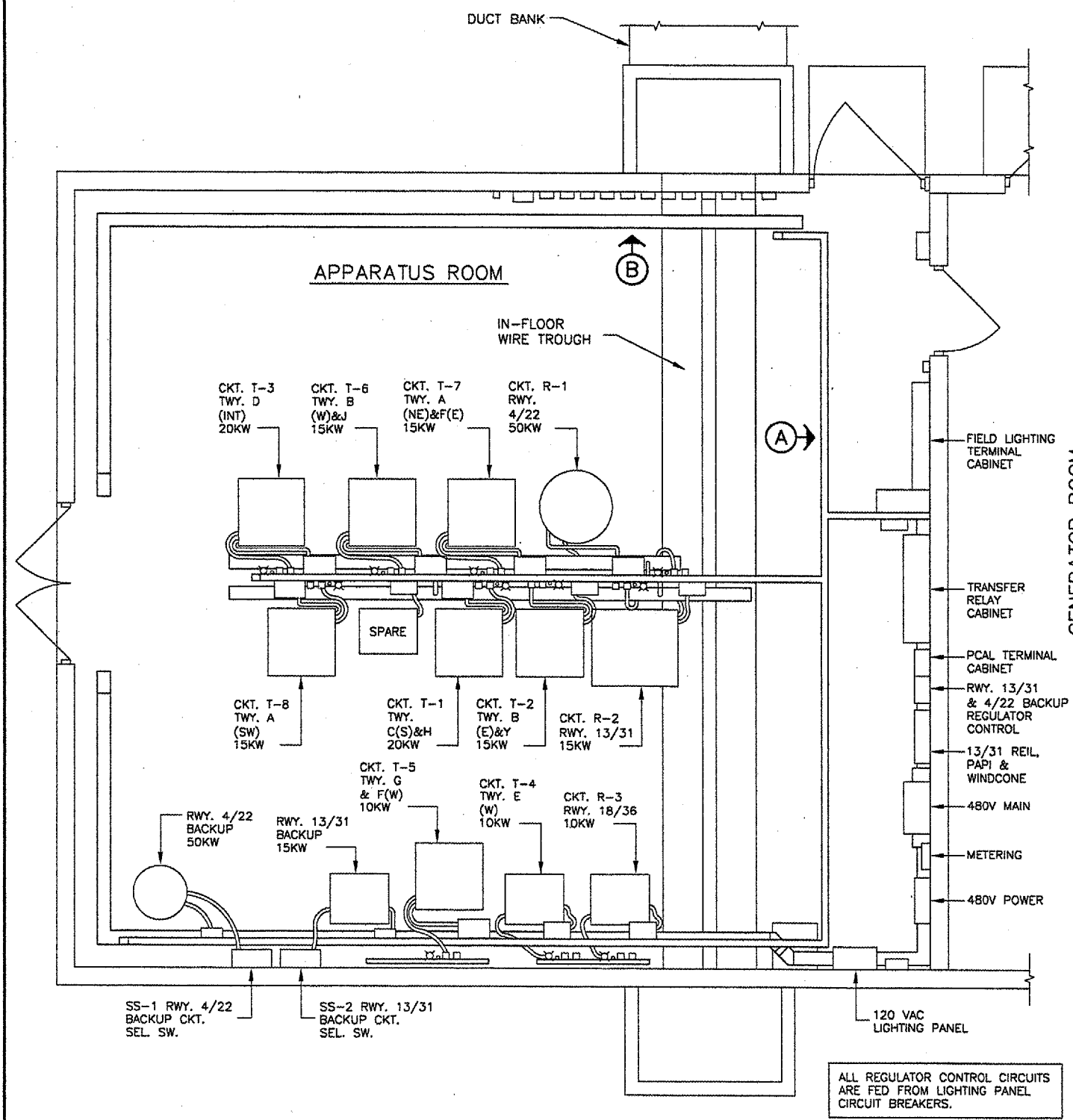
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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EXISTING VAULT PLAN & SECTIONS

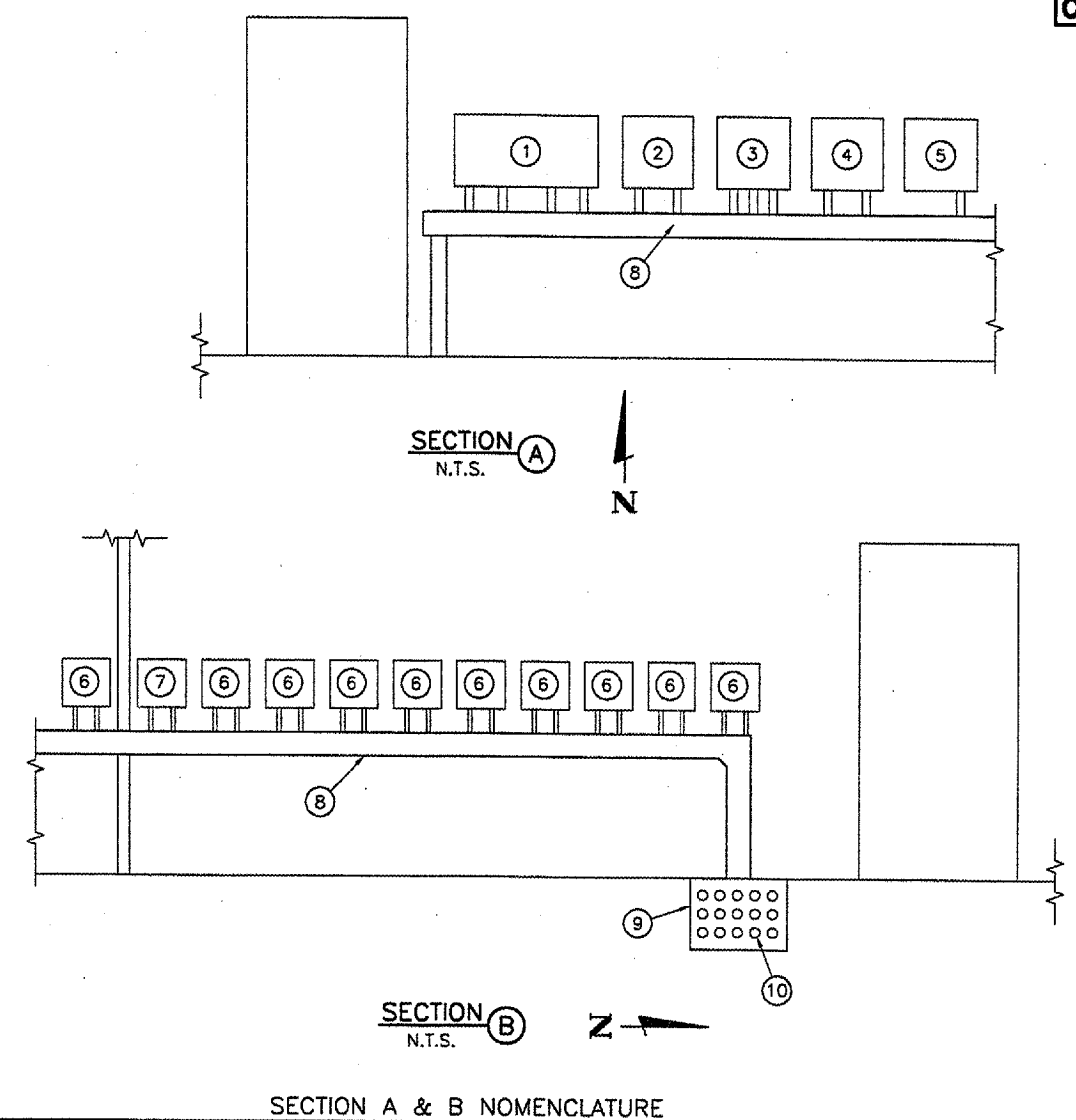
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SHEET	55 OF 70 SHEETS

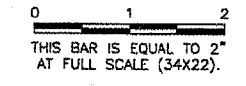


EXISTING VAULT PLAN
 1/4"=1'-0"



- ① EXISTING FIELD LIGHTING TERMINAL CABINET. EXISTING WIRING FROM ATCT L-821 PANEL TERMINATES HERE. CONTRACTOR SHALL DISCONNECT AND REMOVE CABINET AND WIRING. UNLESS DIRECTED OTHERWISE, THE CONTRACTOR SHALL TURN THIS CABINET OVER TO THE OWNER. PLUG OPENINGS IN EXISTING WIREWAY.
- ② EXISTING L-854 PCAL SYSTEM TRANSFER RELAY CABINET. CONTRACTOR SHALL DISCONNECT AND REMOVE CABINET AND WIRING. UNLESS DIRECTED OTHERWISE, THE CONTRACTOR SHALL TURN THIS CABINET OVER TO THE OWNER. PLUG OPENINGS IN EXISTING WIREWAY. SEE REGULATOR SELECT PANEL MODIFICATIONS SHEET FOR ADDITIONAL WORK TO BE PERFORMED INSIDE THIS CABINET.
- ③ EXISTING REGULATOR CONTROL TERMINAL CABINET. CONTRACTOR SHALL DISCONNECT AND REMOVE CABINET AND WIRING, INCLUDING WIRING FROM CABINET TO ASSOCIATED REGULATORS. UNLESS DIRECTED OTHERWISE BY THE OWNER, THE CONTRACTOR SHALL TURN THIS CABINET OVER TO THE OWNER. PLUG OPENINGS IN EXISTING WIREWAY.
- ④ EXISTING RUNWAY 13/13 & RUNWAY 04/22 REGULATOR SELECT CONTROL PANEL. TO REMAIN IN PLACE. CONTRACTOR SHALL DISCONNECT AND REMOVE CONTROL WIRING BETWEEN THIS PANEL AND THE REGULATOR CONTROL TERMINAL CABINET. SEE REGULATOR SELECT PANEL MODIFICATIONS SHEET FOR ADDITIONAL WORK TO BE PERFORMED INSIDE THIS CABINET.
- ⑤ EXISTING PAPI 13, REIL 13, WIND CONE 13 AND WIND CONE 31 CONTROL PANEL. TO REMAIN IN PLACE. CONTRACTOR SHALL DISCONNECT AND REMOVE CONTROL WIRING BETWEEN THIS PANEL AND THE FIELD LIGHTING TERMINAL CABINET.
- ⑥ EXISTING L-854 PCAL SYSTEM RADIO INTERFACE UNIT. CONTRACTOR SHALL DISCONNECT AND REMOVE RADIO INTERFACE UNIT AND WIRING. THE CONTRACTOR SHALL TURN THE RADIO INTERFACE UNIT OVER TO THE OWNER. PLUG OPENINGS IN EXISTING WIREWAY.
- ⑦ EXISTING L-854 RADIO CONTROLLER. TO REMAIN IN PLACE AND BE INCORPORATED INTO NEW L-890 AIRFIELD LIGHTING CONTROL AND MONITORING SYSTEM.
- ⑧ WIREWAY.
- ⑨ IN-FLOOR WIRING TROUGH.
- ⑩ DUCT BANK. CONTAINS CONTROL CABLES TO ATCT.

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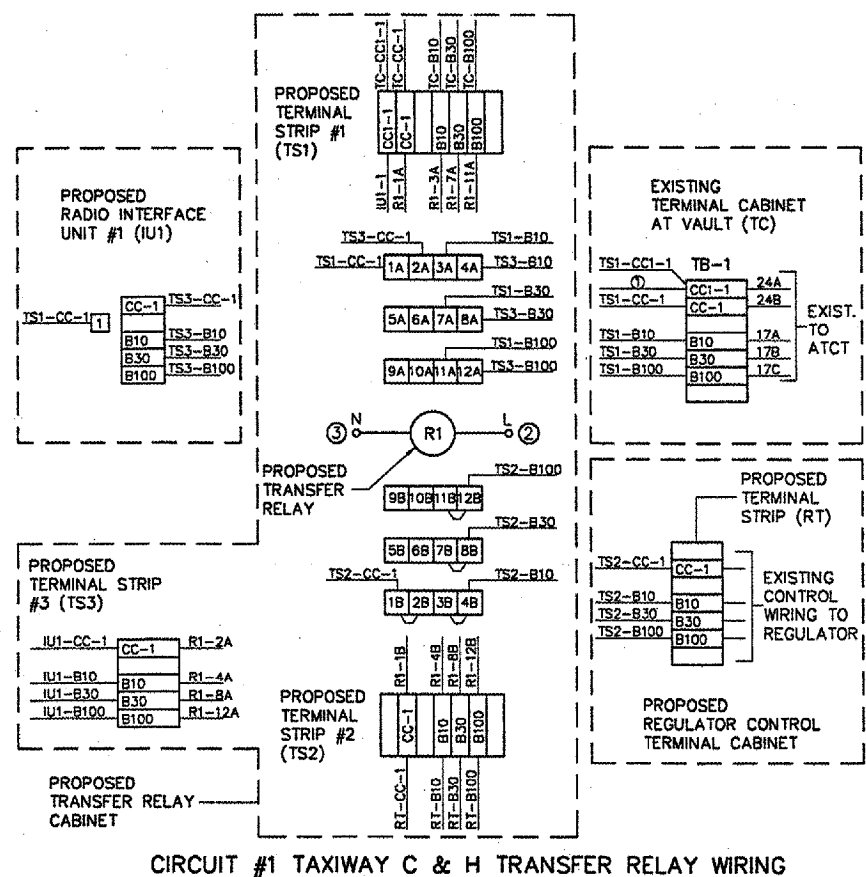
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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 RECORD DRAWING 1

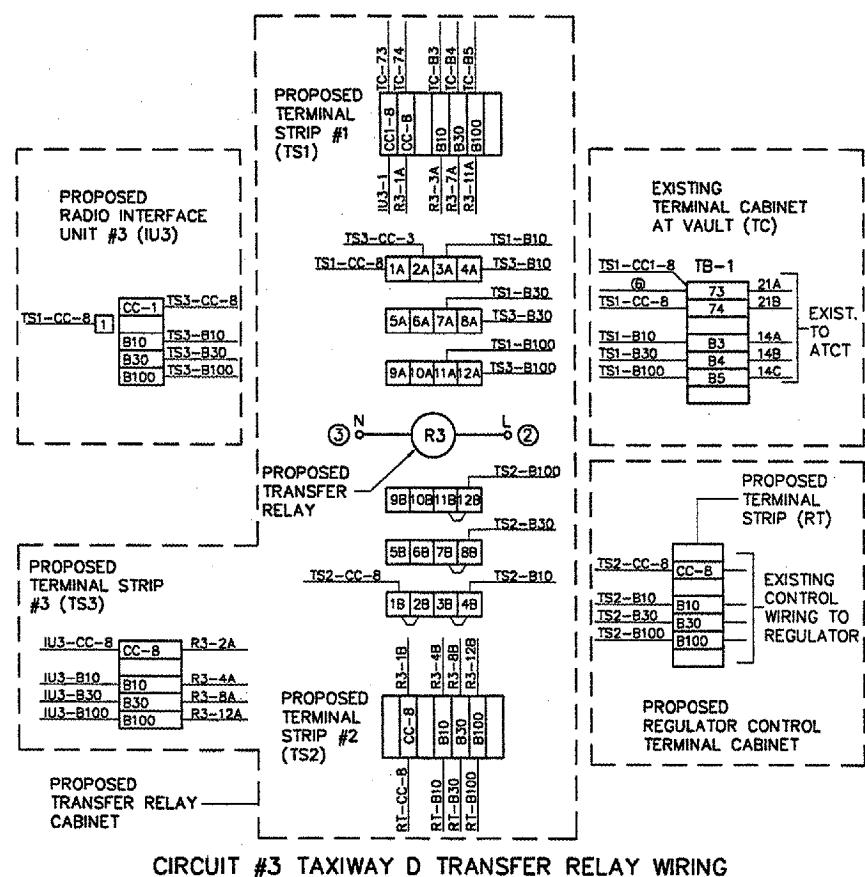
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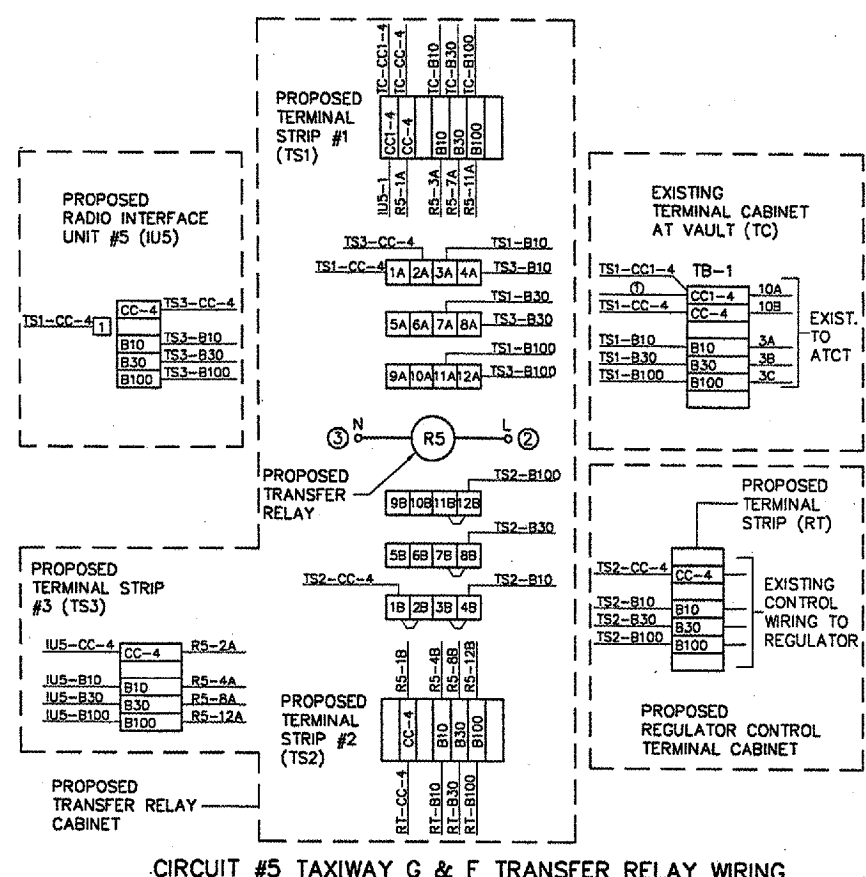
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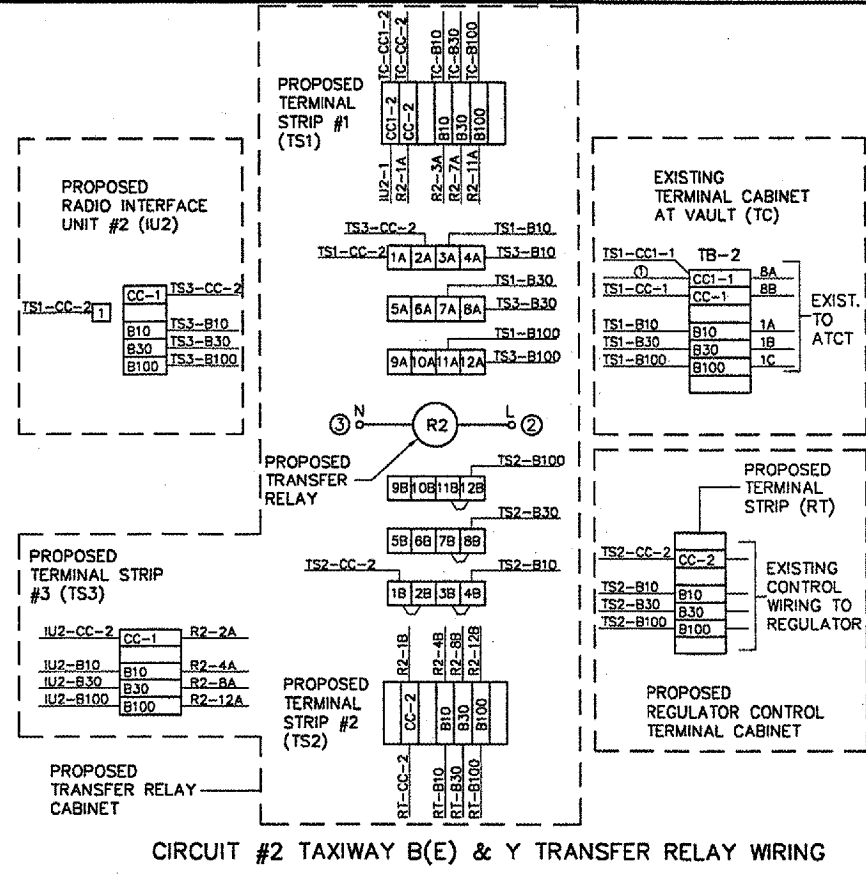
CIRCUIT #1 TAXIWAY C & H TRANSFER RELAY WIRING



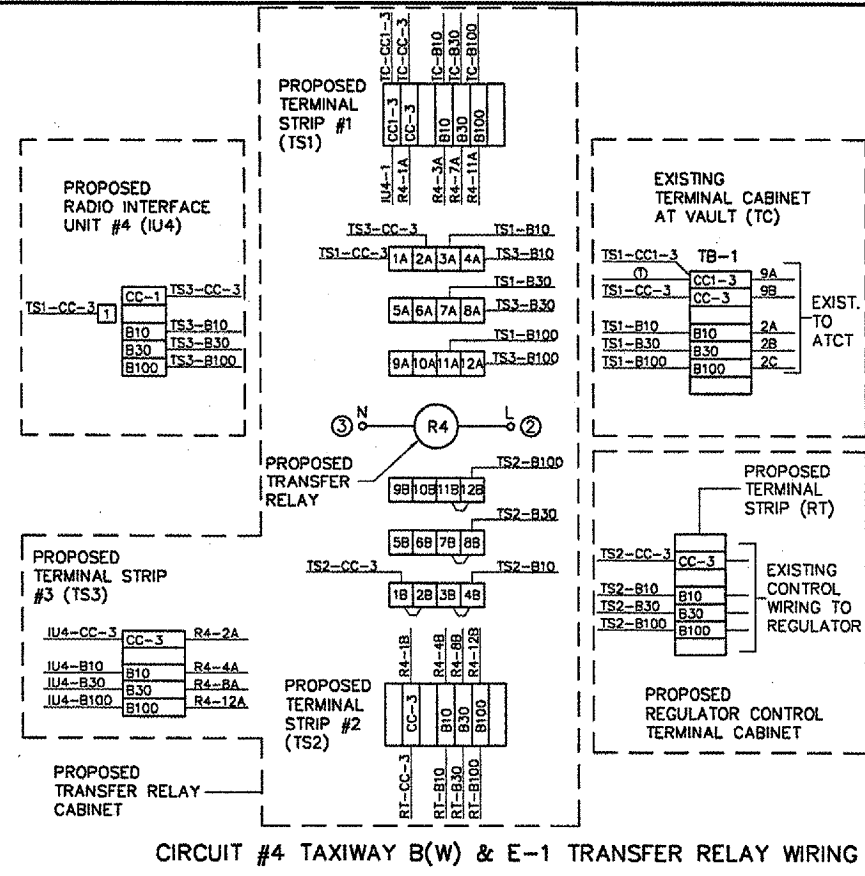
CIRCUIT #3 TAXIWAY D TRANSFER RELAY WIRING



CIRCUIT #5 TAXIWAY G & F TRANSFER RELAY WIRING



CIRCUIT #2 TAXIWAY B(E) & Y TRANSFER RELAY WIRING



CIRCUIT #4 TAXIWAY B(W) & E-1 TRANSFER RELAY WIRING

TRANSFER RELAY CABINET NOTES:

- 120 V CONTROL FROM EXISTING DISTRIBUTION PANEL.
- SEE AIR-TO-GROUND CONTROL WIRING DIAGRAM.
- SEE AIR-TO-GROUND CONTROL WIRING DIAGRAM
- ALL TRANSFER RELAYS TO BE 12-POLE, SQUARE D TYPE X040, OR EQUIVALENT. CONTACT BLOCKS TO BE ARRANGED AS SHOWN:

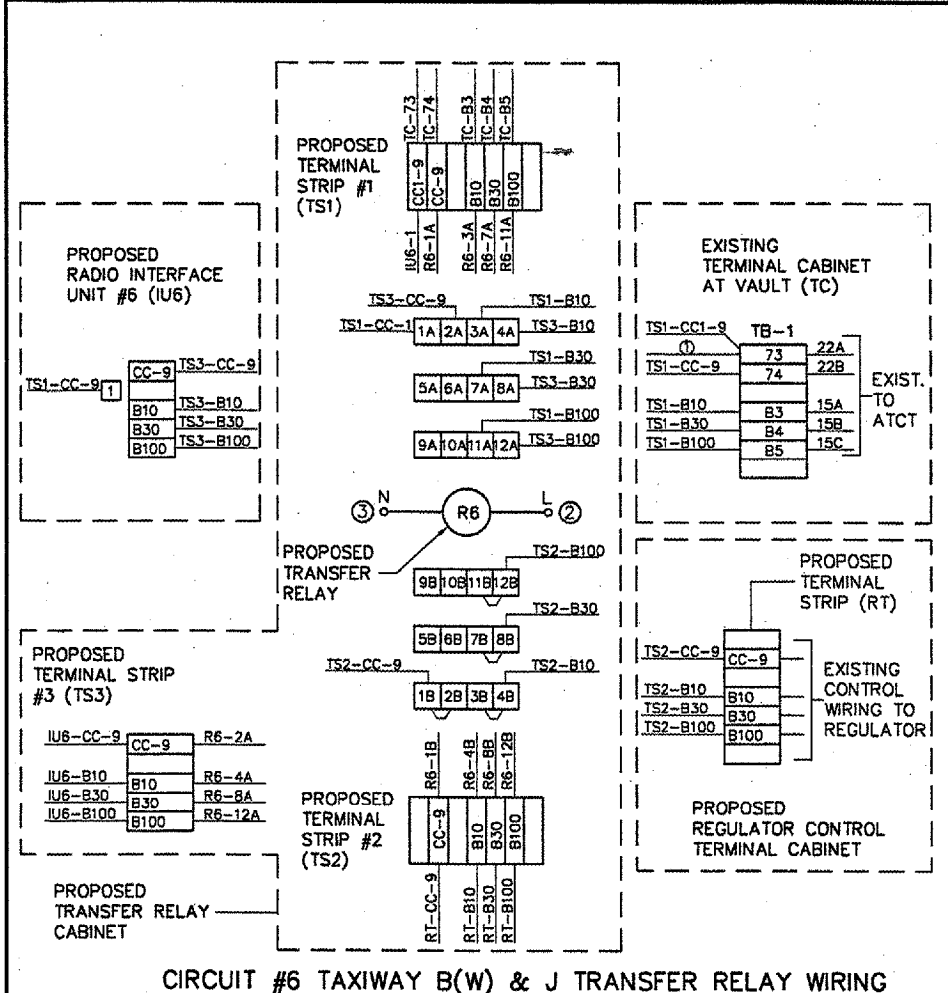
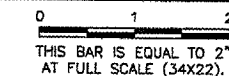
CONTACT	X	X	X	O
1	X	O	X	O
2	X	O	X	O
3	X	O	X	O
- PROPOSED TRANSFER RELAYS, PROPOSED TERMINAL STRIPS #1, #2 AND #3 PROPOSED AIR-TO-GROUND RELAY SHALL BE INSTALLED IN HINGED NEMA 12 ENCLOSURE, HOFFMAN, OR EQUIVALENT. PROVIDE SPACE FOR MINIMUM OF THREE FUTURE TRANSFER RELAYS.
- PROVIDE NEW 120V CONTROL POWER CIRCUIT FOR PROPOSED TAXIWAY CIRCUIT #3 AND TAXIWAY CIRCUIT #7 REGULATORS. INSTALL 20A, 1 POLE CIRCUIT BREAKER IN EXISTING DISTRIBUTION PANEL AND 2 # 12 THWN, 1 #12 GROUND TO EXISTING VAULT TERMINAL CABINET FOR EACH REGULATOR.

RECORD DRAWING

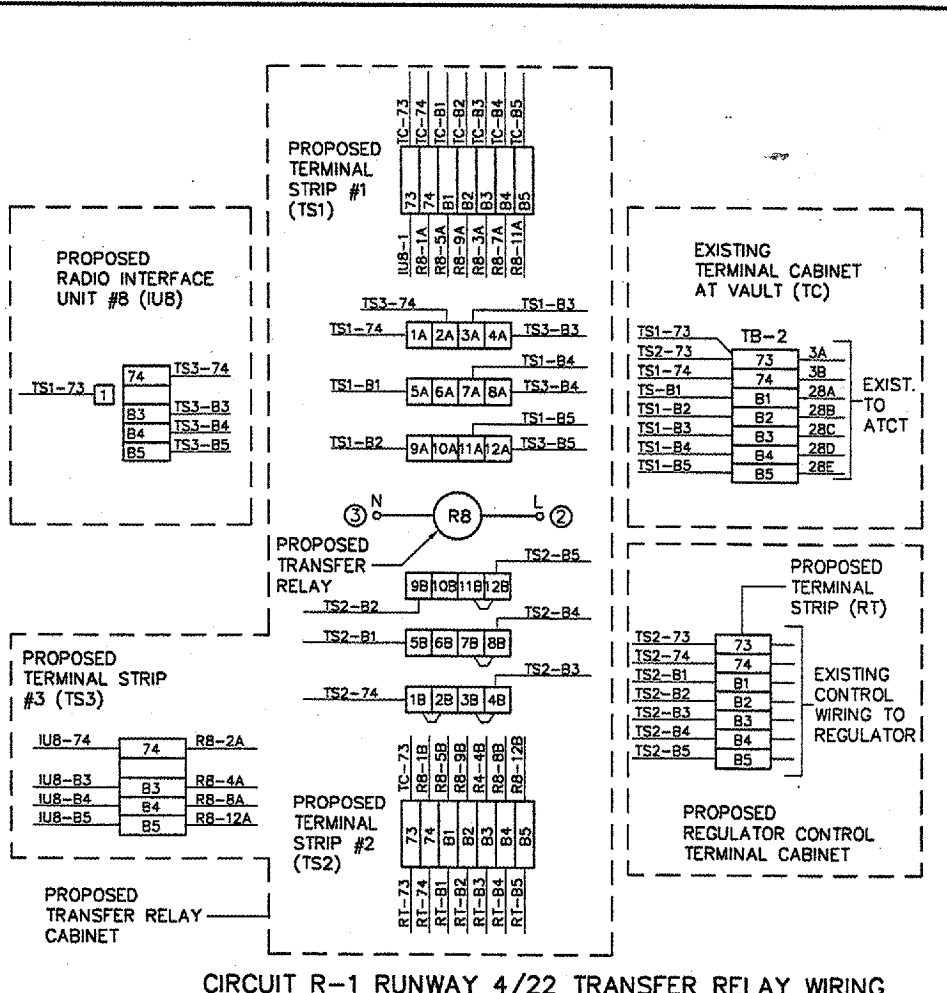
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REVISIONS

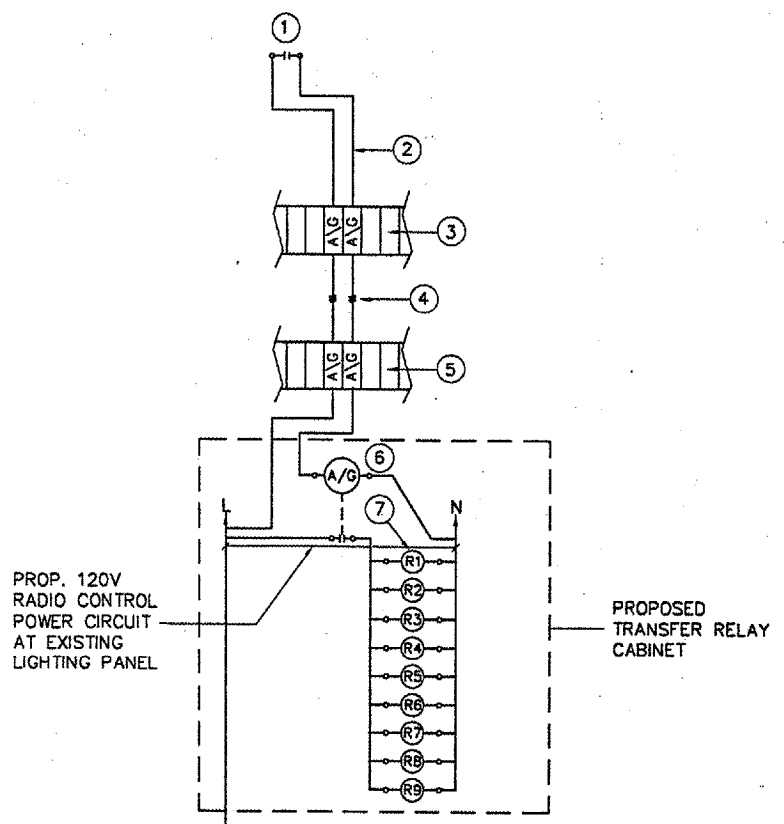
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CIRCUIT #6 TAXIWAY B(W) & J TRANSFER RELAY WIRING



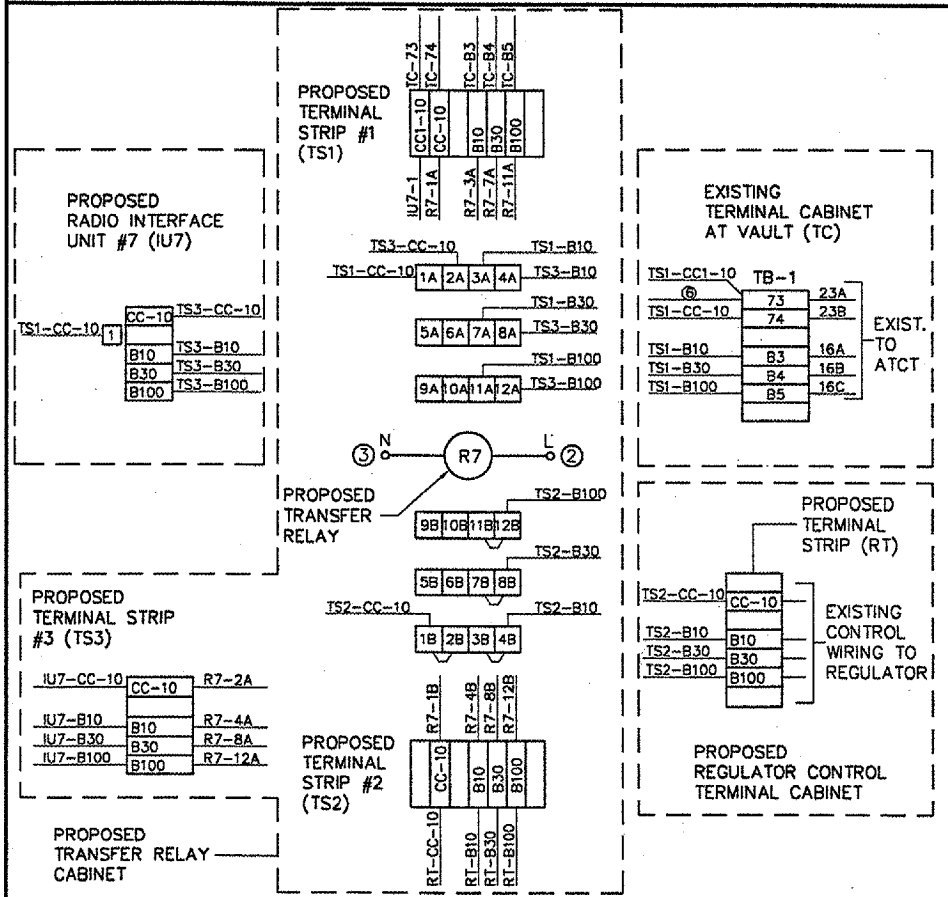
CIRCUIT R-1 RUNWAY 4/22 TRANSFER RELAY WIRING



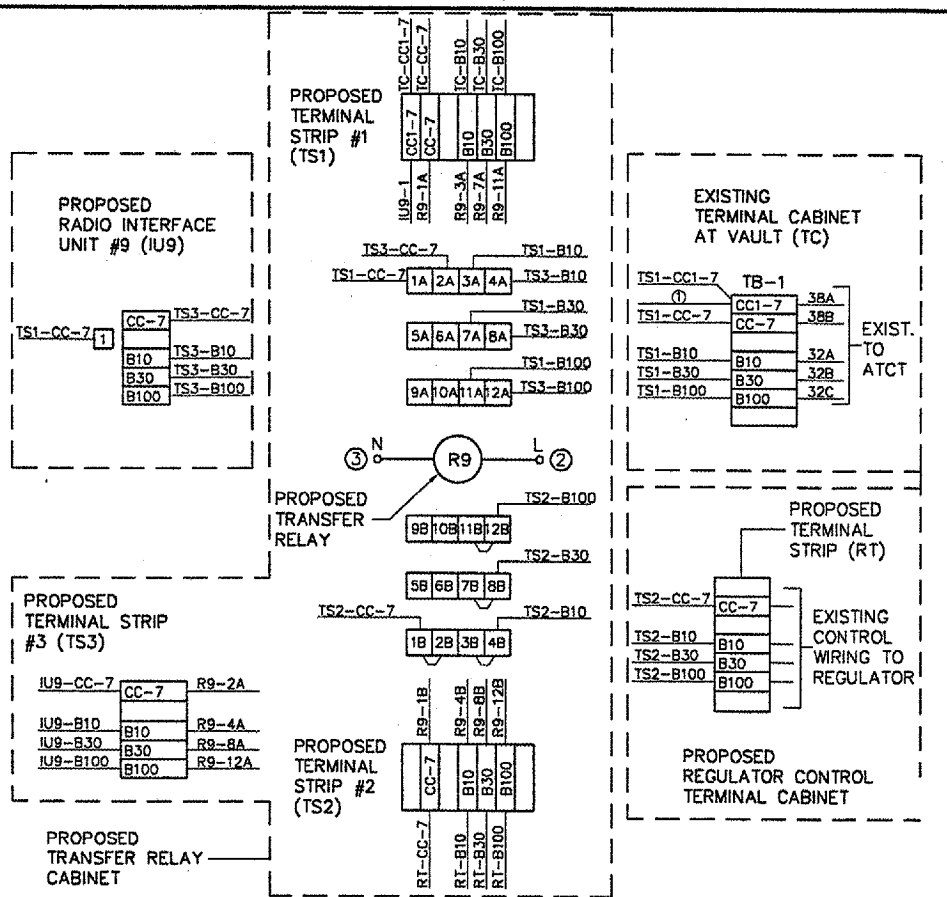
AIR-TO-GROUND WIRING DIAGRAM

AIR-TO-GROUND WIRING NOTES

- EXISTING SPARE AIR-TO-GROUND CONTROL AT ATCT L-821 PANEL. "NON-POWERED" CONTACT WIRING SHOWN - CLOSSES WHEN "PILOT" CONTROL IS SELECTED. SEE SPEC'S.
- PROPOSED CONTROL WIRING.
- EXISTING SPARE TERMINAL BLOCKS AT ATCT L-821 PANEL. RELABEL AS SHOWN.
- EXISTING SPARE CONTROL WIRES FROM ATCT L-821 PANEL TO VAULT TERMINAL CABINET.
- EXISTING SPARE TERMINAL BLOCKS AT VAULT TERMINAL CABINET. RELABEL AS SHOWN.
- PROPOSED AIR-TO-GROUND CONTROL RELAY, SQUARE D TYPE KP, OR EQUIVALENT.
- PROPOSED REGULATOR TRANSFER RELAYS.



CIRCUIT #7 TAXIWAY A(NE) & F TRANSFER RELAY WIRING



CIRCUIT R-2 RUNWAY 13/31 TRANSFER RELAY WIRING

RECORD DRAWING
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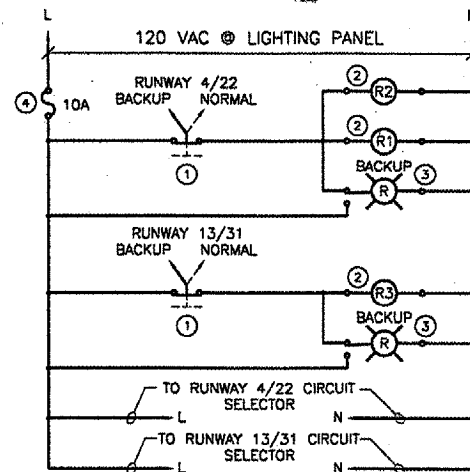


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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
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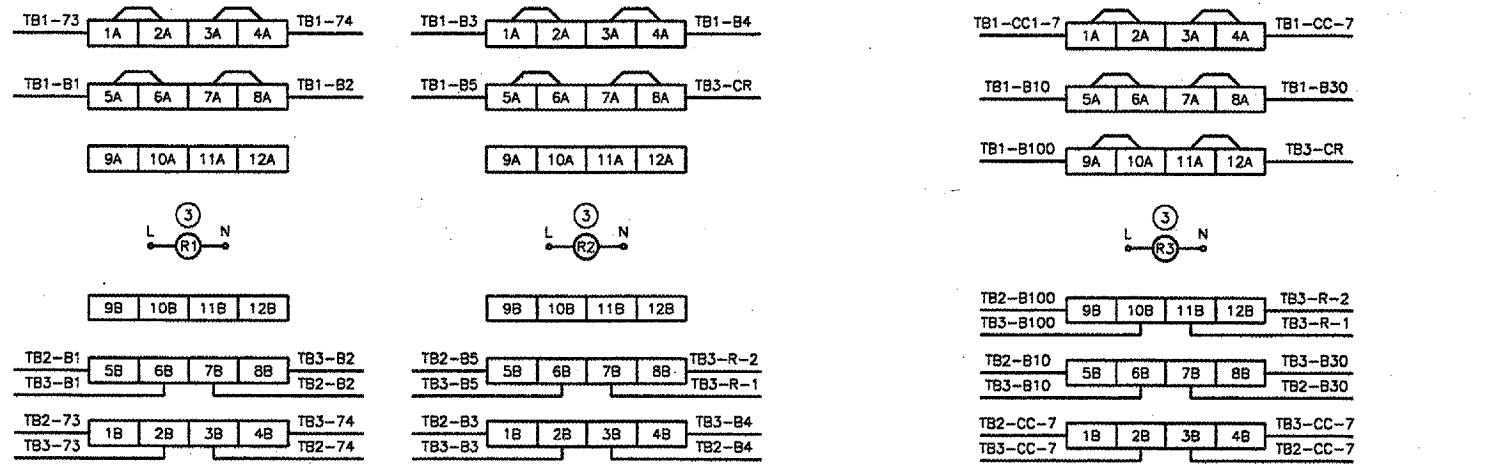
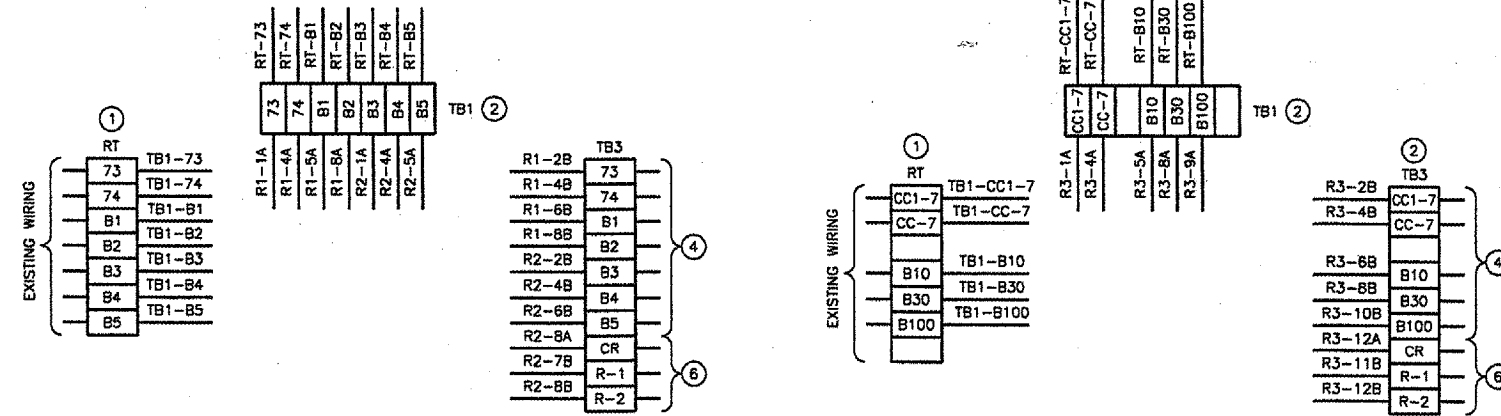
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SCHEMATIC LEGEND

- TWO-POSITION SELECTOR SWITCH, SQUARE D TYPE S, NEMA 4X, NON-METALLIC, 10A RATED CONTACTS. MOUNT ON COVER OF CONTROL PANEL. PROVIDE LEGEND PLATE AS SHOWN.
 - TRANSFER RELAY, SQUARE D TYPE X012, 12-POLE, 10A RATED CONTACTS. SEE TRANSFER RELAY CONTACT WIRING DETAIL, THIS SHEET, FOR ADDITIONAL INFORMATION. CONTACTS TO BE ARRANGED AS SHOWN:
- | | | | | |
|---------------|---|---|---|---|
| CONTACT ARRAY | X | O | X | O |
| | X | O | X | O |
| | X | O | X | O |
- X = NORMALLY CLOSED
 O = NORMALLY OPENED
- PUSH-TO-TEST INDICATING LIGHT, SQUARE D TYPE S, NON-METALLIC, TRANSFORMER TYPE, WITH RED LENS. MOUNT ON COVER OF CONTROL PANEL. PROVIDE LEGEND PLATE AS SHOWN.
 - IN-LINE FUSE HOLDER WITH 10A SLOW-BLOW FUSE.



LEGEND AND NOTES

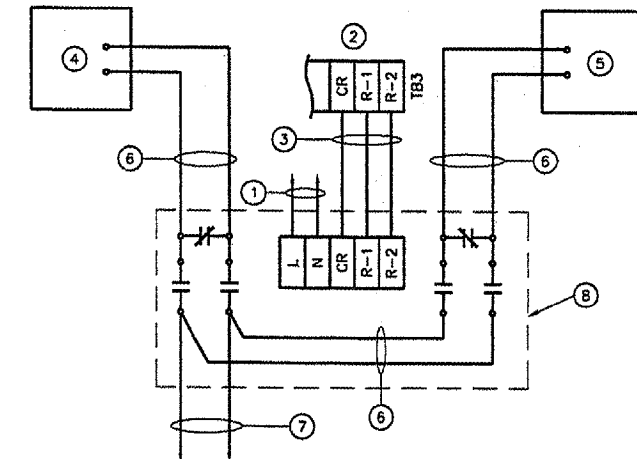
- EXISTING TERMINAL STRIP IN EXISTING TERMINAL CABINET AND TRANSFER RELAY CABINET FOR INTERFACING PCAL SYSTEM CONTROL WIRING AND VAULT CONTROL WIRING. (PART OF 1994 VAULT IMPROVEMENTS.) EXISTING CONTROL WIRING FROM THIS TERMINAL STRIP TO EXISTING RUNWAY 4/22 REGULATOR SHALL BE DISCONNECTED AND RELOCATED TO PROPOSED TERMINAL STRIP TB2. SEE NOTE #5.
- PROPOSED TERMINAL STRIPS, SQUARE D TYPE G, MINIMUM VOLTAGE RATING OF 300V. PROVIDE LABELS FOR TERMINALS AND TERMINAL STRIPS AS SHOWN. INSTALL IN PROPOSED RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECT CONTROL PANEL.
- TRANSFER RELAY, SQUARE D TYPE X012, 12-POLE, 10A RATED CONTACTS. SEE RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECT CONTROL PANEL SCHEMATIC DETAIL, THIS SHEET, FOR ADDITIONAL INFORMATION. INSTALL IN PROPOSED RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECT CONTROL PANEL.
- PROPOSED #12 THWN WIRING TO RUNWAY 4/22 BACKUP REGULATOR. ROUTE WIRING VIA EXISTING RACEWAYS AND WIREWAYS WHEREVER POSSIBLE.
- PROPOSED #12 THWN WIRING TO RUNWAY 4/22 REGULATOR. CONTRACTOR MAY RE-USE EXISTING CONTROL WIRING TO THIS REGULATOR AND EXTEND THE WIRING TO TERMINAL STRIP TB2 FROM EXISTING TERMINAL CABINET AND TRANSFER RELAY CABINET FOR INTERFACING PCAL SYSTEM CONTROL WIRING AND VAULT CONTROL WIRING. (PART OF 1994 VAULT IMPROVEMENTS.) IF SO, SPLICES SHALL BE MADE WITH IN-LINE BUTT SPLICES WITH TAPE OVERLAY. ROUTE WIRING VIA EXISTING RACEWAYS AND WIREWAYS WHEREVER POSSIBLE.
- PROPOSED #12 THWN WIRING TO RUNWAY 4/22 CIRCUIT SELECTOR SWITCH. ROUTE WIRING VIA EXISTING RACEWAYS AND WIREWAYS WHEREVER POSSIBLE.

RUNWAY 4/22 REGULATOR TRANSFER RELAY WIRING

LEGEND AND NOTES

- EXISTING TERMINAL STRIP IN EXISTING TERMINAL CABINET AND TRANSFER RELAY CABINET FOR INTERFACING PCAL SYSTEM CONTROL WIRING AND VAULT CONTROL WIRING. (PART OF 1994 VAULT IMPROVEMENTS.) EXISTING CONTROL WIRING FROM THIS TERMINAL STRIP TO EXISTING RUNWAY 13/31 REGULATOR SHALL BE DISCONNECTED AND RELOCATED TO PROPOSED TERMINAL STRIP TB2. SEE NOTE #5.
- PROPOSED TERMINAL STRIPS, SQUARE D TYPE G, MINIMUM VOLTAGE RATING OF 300V. PROVIDE LABELS FOR TERMINALS AND TERMINAL STRIPS AS SHOWN. INSTALL IN PROPOSED RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECT CONTROL PANEL.
- TRANSFER RELAY, SQUARE D TYPE X012, 12-POLE, 10A RATED CONTACTS. SEE RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECT CONTROL PANEL SCHEMATIC DETAIL, THIS SHEET, FOR ADDITIONAL INFORMATION. INSTALL IN PROPOSED RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECT CONTROL PANEL.
- PROPOSED #12 THWN WIRING TO RUNWAY 13/31 BACKUP REGULATOR. ROUTE WIRING VIA EXISTING RACEWAYS AND WIREWAYS WHEREVER POSSIBLE.
- PROPOSED #12 THWN WIRING TO RUNWAY 13/31 REGULATOR. CONTRACTOR MAY RE-USE EXISTING CONTROL WIRING TO THIS REGULATOR AND EXTEND THE WIRING TO TERMINAL STRIP TB2 FROM EXISTING TERMINAL CABINET AND TRANSFER RELAY CABINET FOR INTERFACING PCAL SYSTEM CONTROL WIRING AND VAULT CONTROL WIRING. (PART OF 1994 VAULT IMPROVEMENTS.) IF SO, SPLICES SHALL BE MADE WITH IN-LINE BUTT SPLICES WITH TAPE OVERLAY. ROUTE WIRING VIA EXISTING RACEWAYS AND WIREWAYS WHEREVER POSSIBLE.
- PROPOSED #12 THWN WIRING TO RUNWAY 13/31 CIRCUIT SELECTOR SWITCH. ROUTE WIRING VIA EXISTING RACEWAYS AND WIREWAYS WHEREVER POSSIBLE.

RUNWAY 13/31 REGULATOR TRANSFER RELAY WIRING



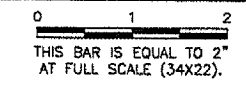
LEGEND AND NOTES

- PROPOSED 120V POWER WIRING FROM RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECT CONTROL PANEL. ROUTE WIRING VIA EXISTING RACEWAYS AND WIREWAYS WHEREVER POSSIBLE.
- PROPOSED TERMINAL STRIP IN PROPOSED RUNWAY 13/31 AND RUNWAY 4/22 REGULATOR SELECT CONTROL PANEL. SEE REGULATOR TRANSFER RELAY WIRING DETAILS, THIS SHEET FOR MORE INFORMATION.
- PROPOSED #12 THWN WIRING. ROUTE WIRING VIA EXISTING RACEWAYS AND WIREWAYS WHEREVER POSSIBLE.
- NORMAL REGULATOR.
- BACKUP REGULATOR.
- PROPOSED #8, L-B24, TYPE C, 5KV CABLES.
- PROPOSED #8, L-B24, TYPE C, 5KV CABLES. SPLICE TO EXISTING RUNWAY SERIES CIRCUIT HOMERUN WIRING USING L-825 CONNECTORS.
- PROPOSED CIRCUIT SELECTOR SWITCH, CROUSE-HINDS (OR EQUIVALENT) CATALOG NUMBER 30847-2-B-9, L-847-2, CLASS A (NEMA 1 ENCLOSURE), EXCLUSIVE CIRCUIT SELECTION, BACKUP REGULATOR SWITCHING.

TYPICAL CIRCUIT SELECTOR SWITCH WIRING DETAIL

REVISIONS

NUMBER	BY	DATE



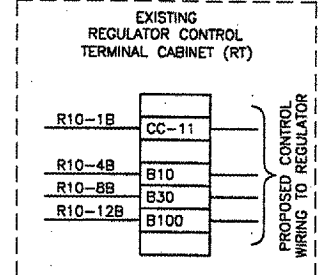
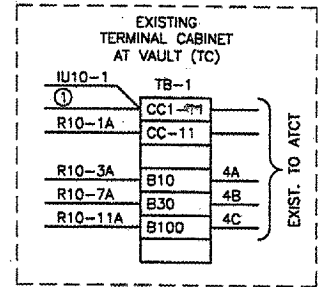
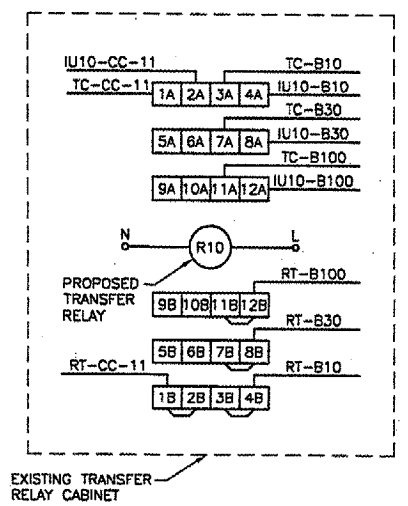
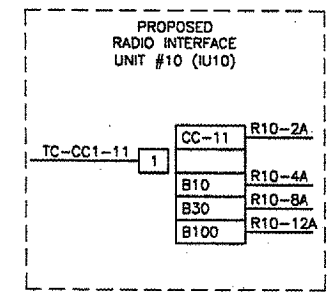
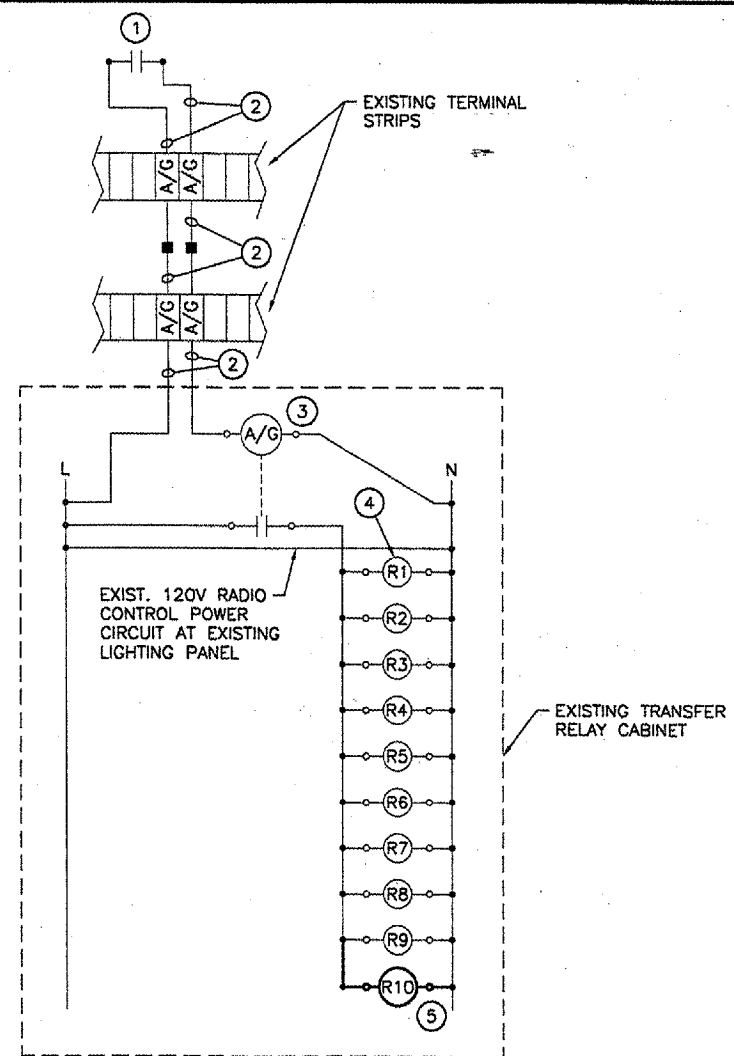
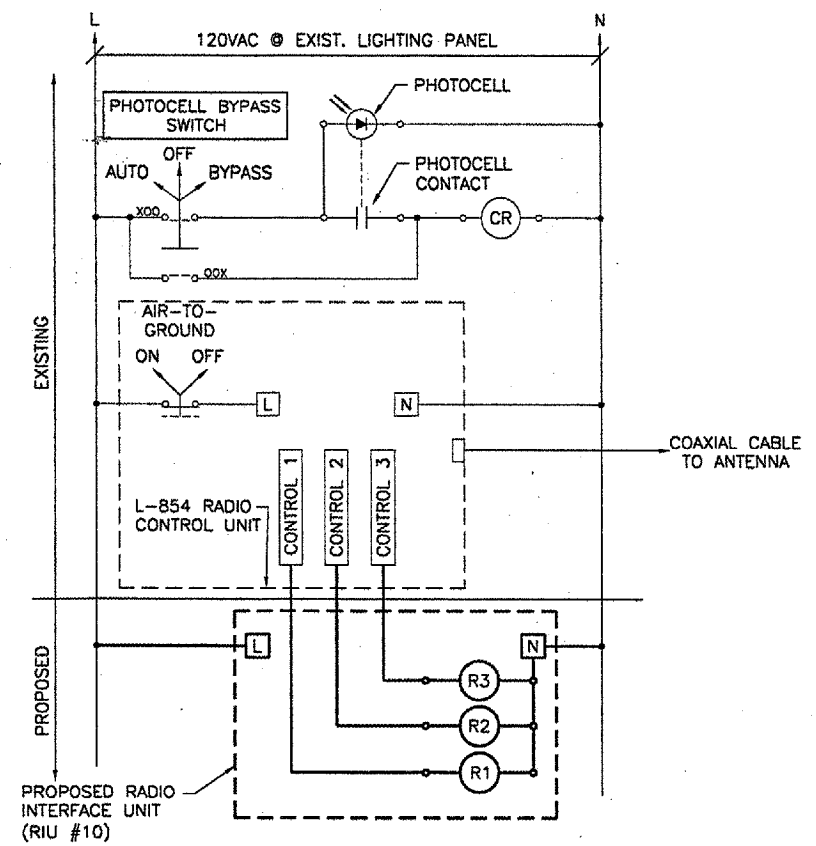
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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
RECORD DRAWING 3

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 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03
 IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 58 OF 70 SHEETS

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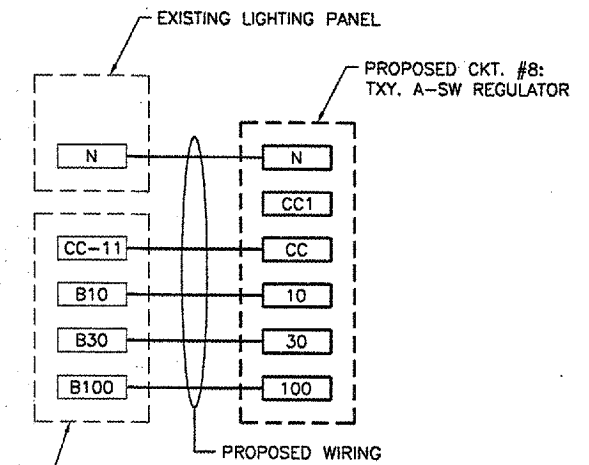
① PROPOSED 120V FROM EXISTING LIGHTING PANEL.

CIRCUIT #8: TAXIWAY A(SW) TRANSFER RELAY WIRING

NOTE
 ① PROPOSED TRANSFER RELAY R10 TO BE 12-POLE, SQUARE D TYPE X01200. INSTALL IN EXISTING TRANSFER RELAY CABINET. CONTACT BLOCKS TO BE ARRANGED AS SHOWN:

CONTACT ARRAY	X	O	X	O
	X	O	X	O
	X	O	X	O

X = NORMALLY CLOSED
 O = NORMALLY OPENED



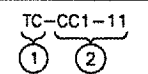
AIR-TO-GROUND WIRING DIAGRAM

AIR-TO-GROUND WIRING NOTES

- ① EXISTING PCAL CONTROL AT ATCT L-821 PANEL.
- ② EXISTING CONTROL WIRING.
- ③ EXISTING AIR-TO-GROUND CONTROL RELAY.
- ④ EXISTING REGULATOR TRANSFER RELAYS, R1-R9.
- ⑤ PROPOSED CKT #8: TXY. A-SW REGULATOR TRANSFER RELAY R10.

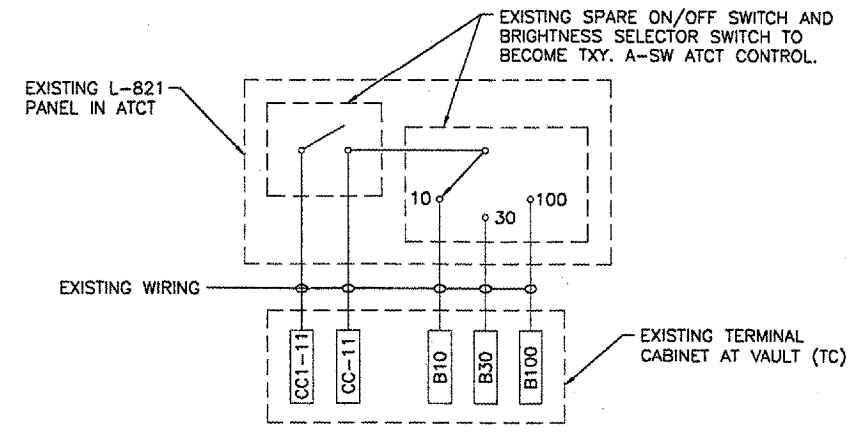
CKT. #8: TXY. A-SW RADIO INTERFACE WIRING

WIRING LEGEND



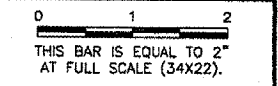
WIRING LEGEND NOTES

- ① DESIGNATES WIRE DESTINATION:
 TC = VAULT TERMINAL CABINET.
 R10 = TRANSFER RELAY R10
 IU10 = RADIO INTERFACE UNIT #10
 RT = REGULATOR CONTROL TERMINAL CABINET
- ② DESIGNATES WIRE TERMINATION IDENTIFICATION LABEL
 CC1-11 = UNSWITCHED 120V CONTROL POWER FROM EXISTING LIGHTING PANEL
 CC-11 = SWITCHED 120V CONTROL POWER
 B10 = SWITCHED 10% BRIGHTNESS 120V CONTROL POWER
 B30 = SWITCHED 30% BRIGHTNESS 120V CONTROL POWER
 B100 = SWITCHED 100% BRIGHTNESS 120V CONTROL POWER
 N = NEUTRAL FROM EXISTING LIGHTING PANEL



REVISIONS

NUMBER	BY	DATE



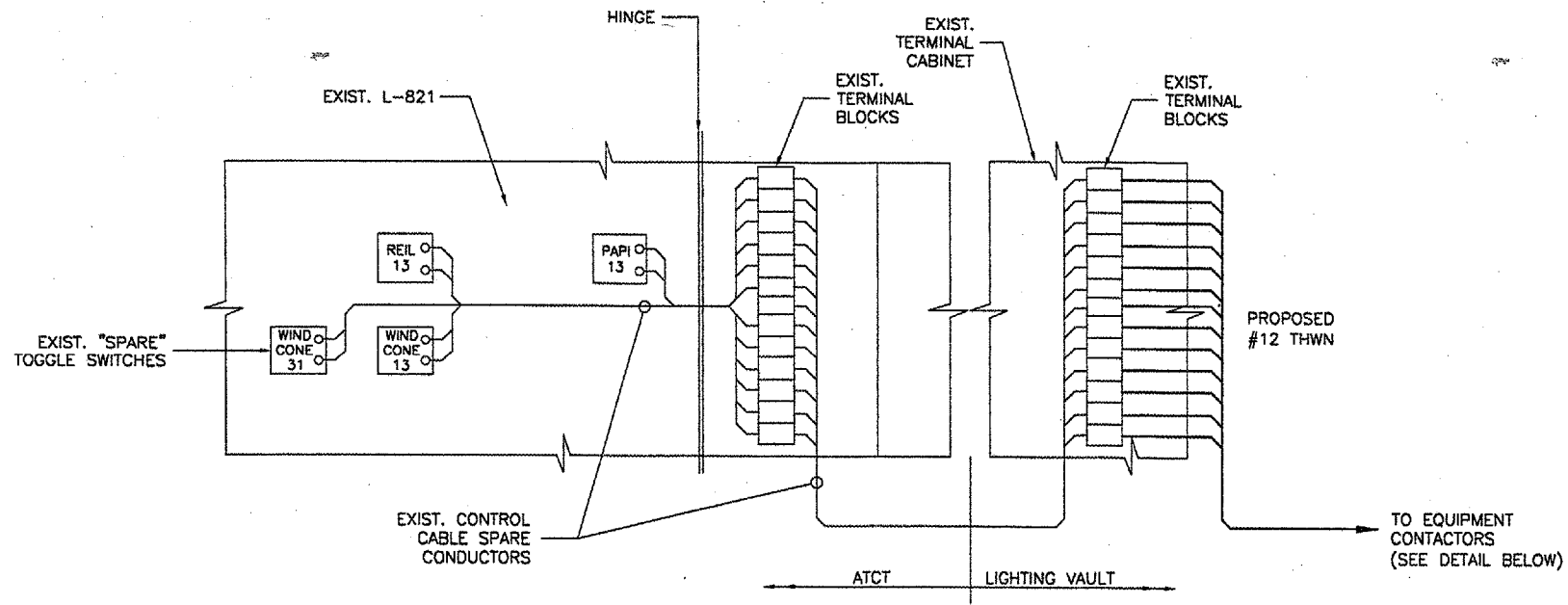
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 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 RECORD DRAWING 4

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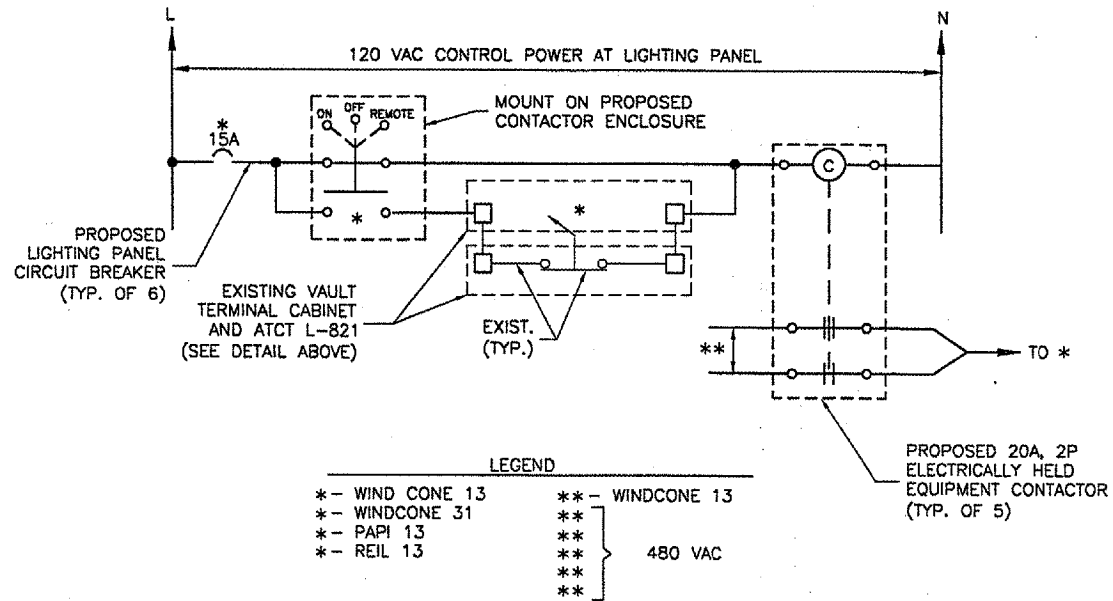
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CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO:	SP1-3488
AIP PROJ. NO:	3-17-0096-42

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ATCT EXISTING AND PROPOSED CONTROL WIRES

N.T.S.



ATCT CONTROL WIRING WINDCONE, PAPI AND REIL

N.T.S.

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 RECORD DRAWING 5

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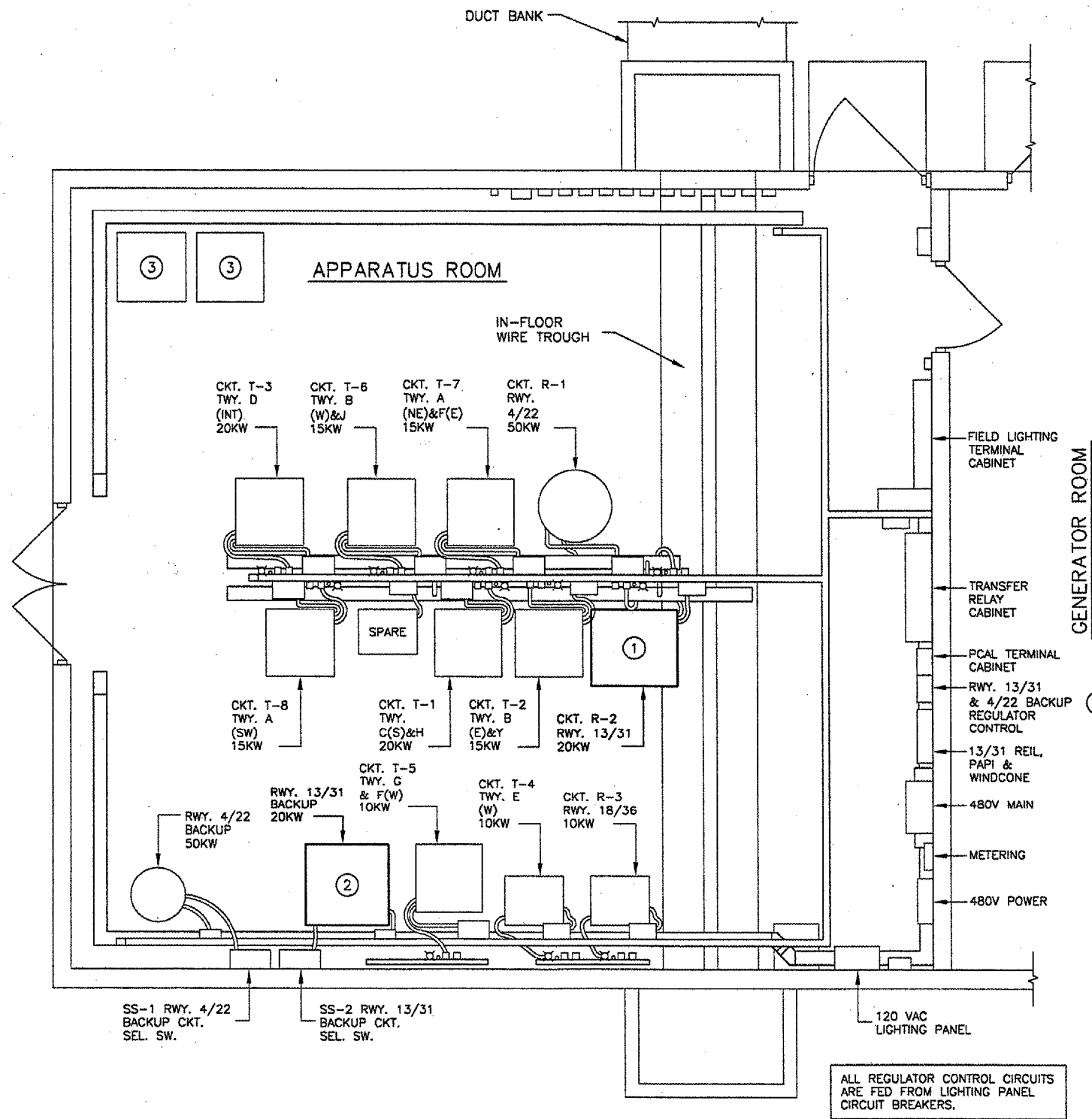
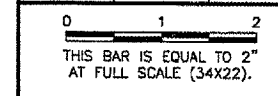
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RECORD DRAWING
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REVISIONS		
NUMBER	BY	DATE




GENERAL LEGEND


- NEW (PRIMARY) RUNWAY 13/31 REGULATOR, L-828, 20KW, 480V INPUT, 5-STEP, 6.6A OUTPUT, FLIGHT LIGHT (FORMERLY SOLA/HEVI-DUTY) 20KW-LB28-1-5-D-6-1A. INSTALL SQUARE D 480V, 60A, 3P PLUG-IN BUSWAY FEEDER CIRCUIT BREAKER IN OVERHEAD BUSWAY (NOTE: ONLY TWO POLES WILL BE USED.) INSTALL TWO #6 THWN, ONE #8 GROUND IN 1" LIQUIDTIGHT FLEXIBLE CONDUIT FROM PLUG-IN CIRCUIT BREAKER TO REGULATOR. RECONNECT EXISTING SERIES CIRCUIT WIRING IN EXISTING CONDUIT. SEE PLANS FOR CONTROL WIRING MODIFICATIONS. PROVIDE ENGRAVED LEGEND PLATE AS INDICATED IN PLAN VIEW.
- NEW (BACKUP) RUNWAY 13/31 REGULATOR, L-828, 20KW, 480V INPUT, 5-STEP, 6.6A OUTPUT, FLIGHT LIGHT (FORMERLY SOLA/HEVI-DUTY) 20KW-LB28-1-5-D-6-1A. INSTALL SQUARE D 480V, 60A, 3P PLUG-IN BUSWAY FEEDER CIRCUIT BREAKER IN OVERHEAD BUSWAY (NOTE: ONLY TWO POLES WILL BE USED.) INSTALL TWO #6 THWN, ONE #8 GROUND IN 1" LIQUIDTIGHT FLEXIBLE CONDUIT FROM PLUG-IN CIRCUIT BREAKER TO REGULATOR. RECONNECT EXISTING SERIES CIRCUIT WIRING IN EXISTING CONDUIT. SEE PLANS FOR CONTROL WIRING MODIFICATIONS. PROVIDE ENGRAVED LEGEND PLATE AS INDICATED IN PLAN VIEW.
- EXISTING PRIMARY AND BACKUP RUNWAY 13/31 REGULATORS SHALL BE DISCONNECTED AND RELOCATED TO HERE. DISCONNECT EXISTING PLUG-IN BUSWAY CIRCUIT BREAKERS AND STORE WITH REGULATORS.
- SEE REGULATOR SELECT PANEL MODIFICATIONS PLAN SHEET FOR WORK TO BE DONE INSIDE THIS CABINET.

NOTE:
 CONTRACTOR SHALL INSTALL NEW 120V POWER WIRING (NOT SHOWN) FROM EXISTING PANELBOARD RUNWAY AND TAXIWAY REGULATOR CIRCUIT BREAKERS TO NEW DISTRIBUTED CONTROL AND MONITORING INTERFACE UNITS AS PART OF THE NEW L-890 SYSTEM.

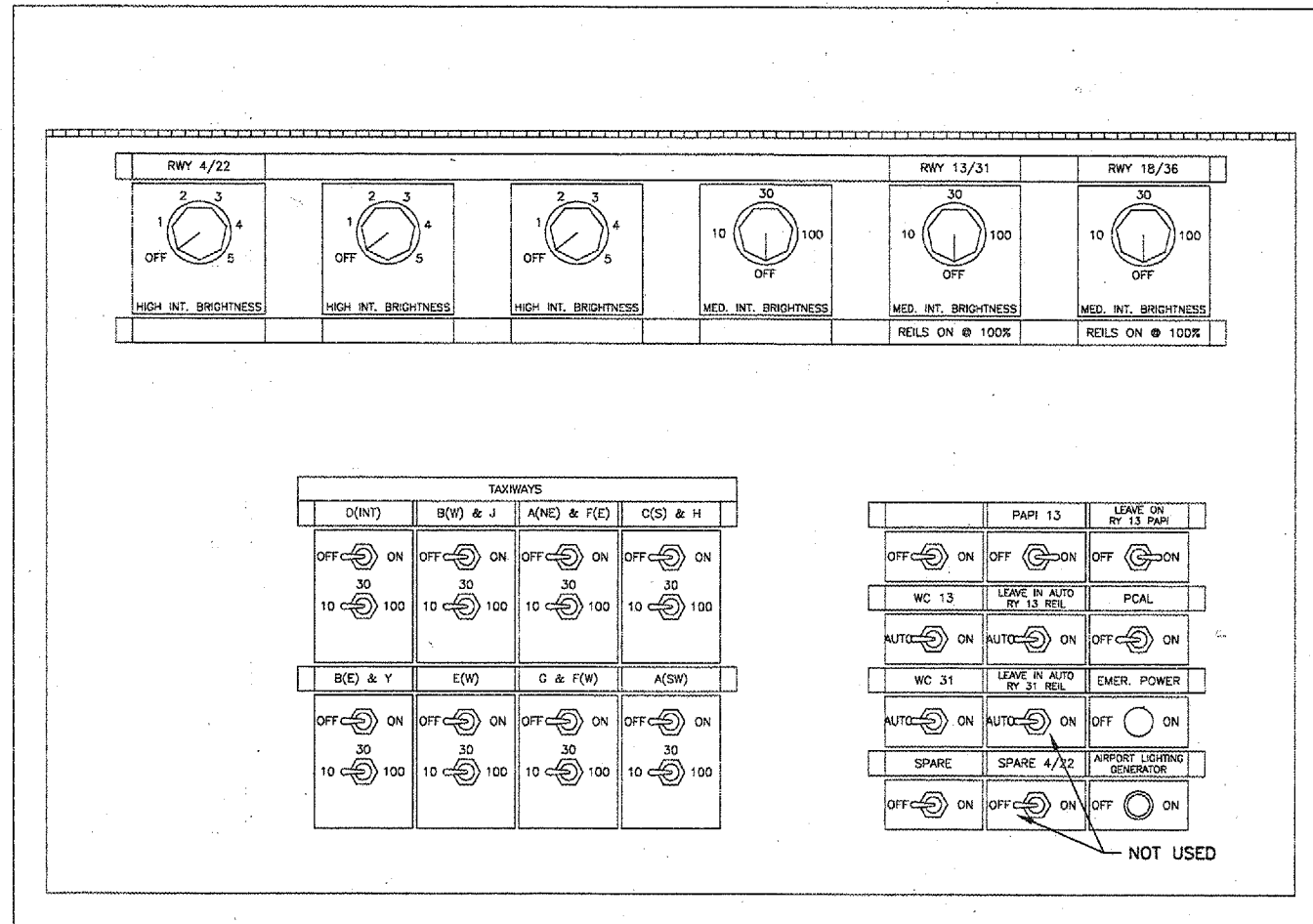
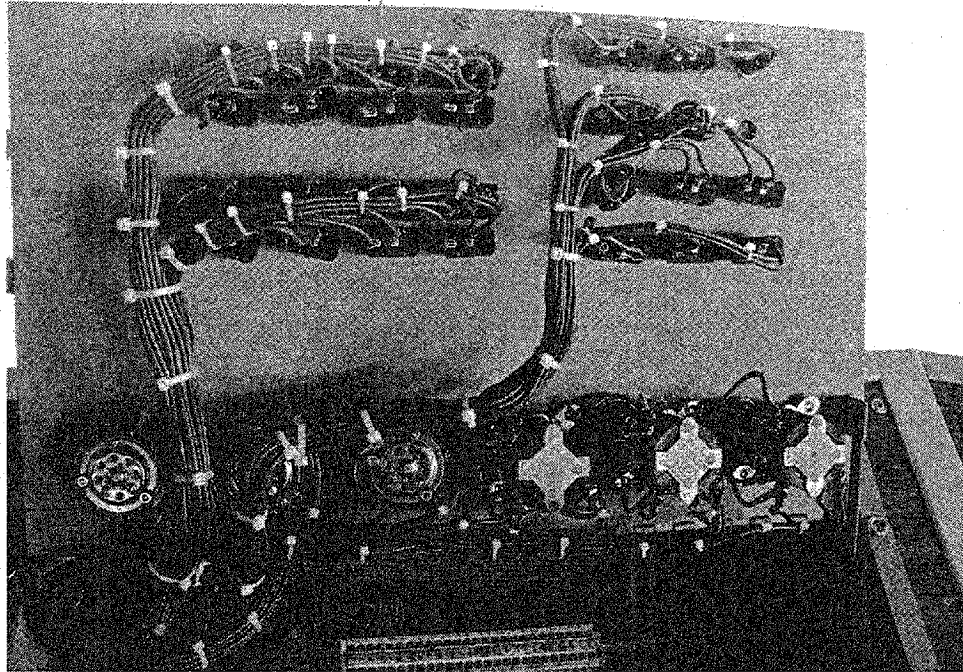
VAULT PLAN
 1/4" = 1'-0"


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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
RUNWAY 13-31 REGULATOR REPLACEMENT

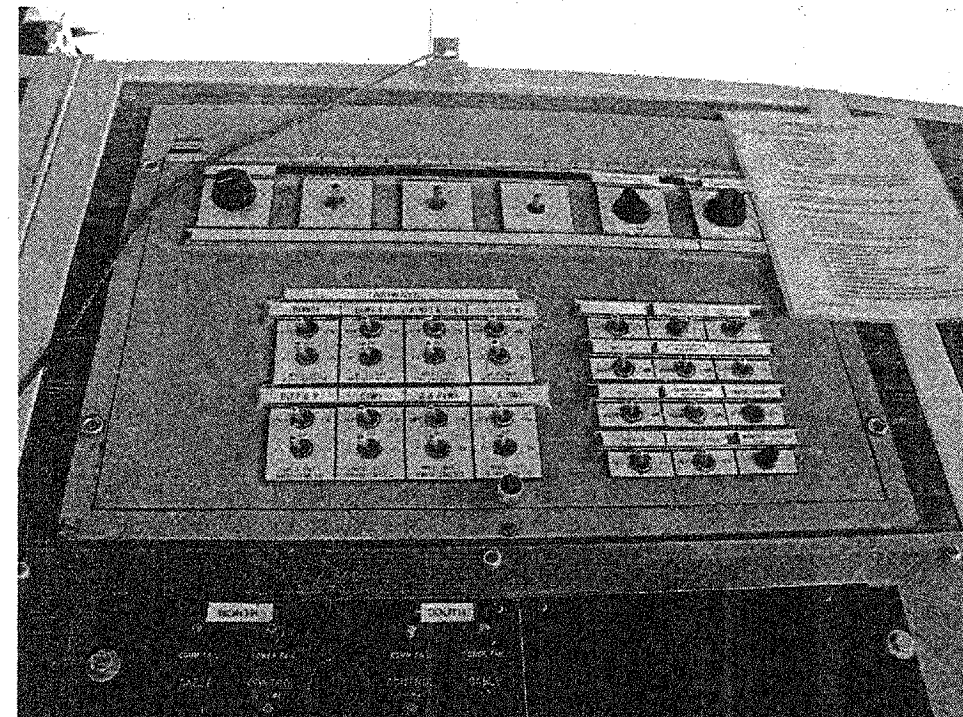
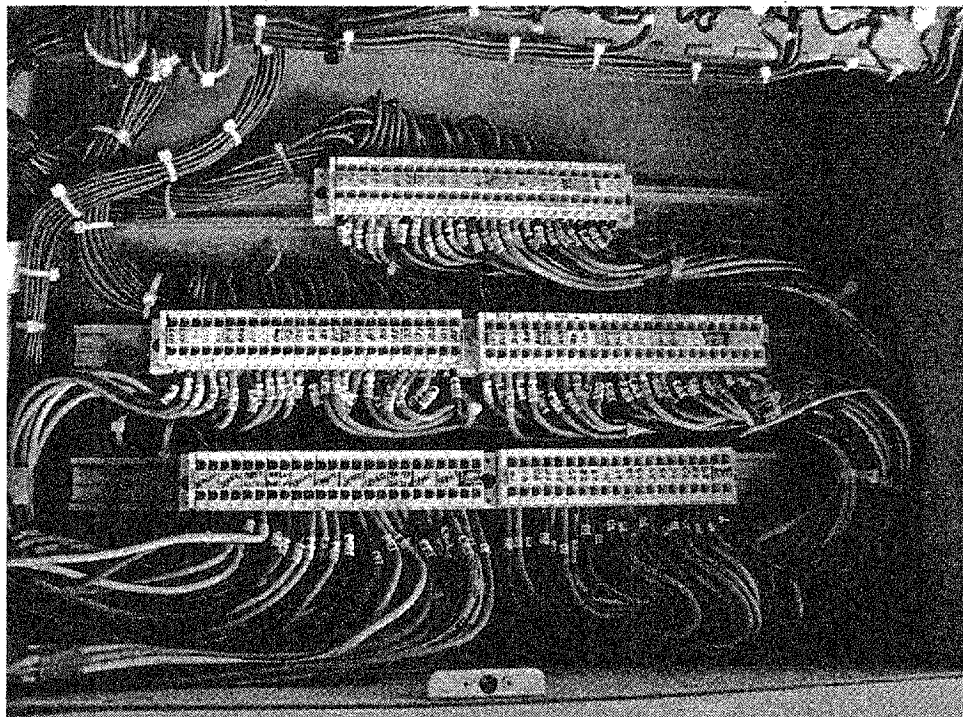

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AIP PROJ. NO. 3-17-0096-42	
SHEET 61 OF 79 SHEETS	



REVISIONS		
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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



L-821 REMOTE CONTROL PANEL LAYOUT
 N.T.S.

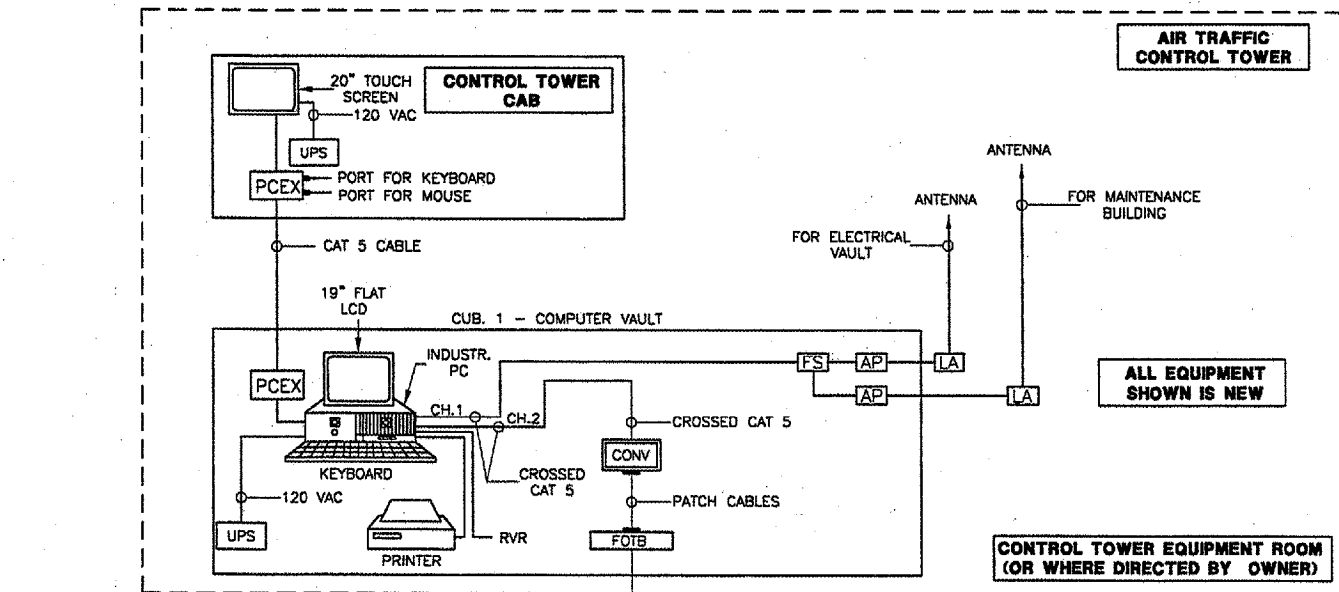
L-821 PANEL REMOVAL NOTES

1. THE EXISTING L-821 PANEL SHALL BE DISCONNECTED AND REMOVED, INCLUDING ALL WIRING FROM CONTROL TOWER TO VAULT. SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION.

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SPRINGFIELD, ILLINOIS
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EXISTING TOWER L-821 PANEL

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 JOB No: 05035-03
 ILL. PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 62 OF 70 SHEETS



L-890 BLOCK DIAGRAM NOMENCLATURE

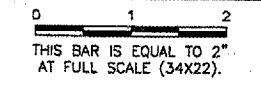
- ① EXISTING 900 MHZ SPREAD SPECTRUM RADIO TRANSMITTER. TRANSMITS ON/OFF CONTROL TO ROTATING BEACON.
- ② NEW ON/OFF CONTROL WIRING.
- ③ EXISTING 200 KW STANDBY GENERATOR, 277Y/480V, 3-PHASE, 4-WIRE.
- ④ NEW GENERATOR MONITOR WIRING AS REQUIRED TO ACCOMPLISH THE FOLLOWING:
 - LOW OIL PRESSURE
 - OVER CRANK (FAILURE TO START)
 - LOW ENGINE JACKET WATER TEMPERATURE
 - HIGH ENGINE JACKET WATER TEMPERATURE
 - OVERSPEED
 - LOW FUEL TANK LEVEL
- ⑤ EXISTING 600A AUTOMATIC TRANSFER SWITCH, 3-PHASE, 4-WIRE.
- ⑥ NEW AUTOMATIC TRANSFER SWITCH MONITOR WIRING AS REQUIRED TO ACCOMPLISH THE FOLLOWING:
 - GENERATOR AVAILABLE
 - GENERATOR ON-LINE
 - UTILITY AVAILABLE
 - UTILITY ON-LINE
- ⑦ EXISTING L-854 RADIO CONTROLLER
- ⑧ NEW PCAL BRIGHTNESS CONTROL WIRING
- ⑨ EXISTING CONTACTOR CABINET FOR WIND CONE 31 (WC 31), WIND CONE 13 (WC 13), PAPI 13 AND REIL 13.
- ⑩ EXISTING TAXIWAY REGULATOR, 3-STEP BRIGHTNESS CONTROL.
- ⑪ NEW REGULATOR CONTROL AND MONITOR WIRING AS REQUIRED TO ACCOMPLISH THE FOLLOWING:
 - 3-STEP BRIGHTNESS CONTROL
 - LOSS OF INPUT POWER TO THE CCR.
 - CCR SHUTDOWN BY OPEN-CIRCUIT / OVER-CURRENT PROTECTIVE DEVICES.
 - DROP OF MORE THAN 10% IN THE CCR VA LOAD.
 - FAILURE OF THE CCR TO DELIVER THE SELECTED OUTPUT CURRENT.
 - THE NUMBER OF BURNT-OUT LAMPS IN EACH SERIES CIRCUIT.
 - REMOTE / LOCAL STATUS OF THE CCR.
 - ACTUAL CCR OUTPUT CURRENT
 - ACTUAL CCR OUTPUT VOLTAGE
 - ACTUAL CCR OUTPUT LOAD (WATTAGE)
- ⑫ EXISTING RUNWAY 18/36 REGULATOR, 3-STEP BRIGHTNESS CONTROL.
- ⑬ NEW RUNWAY 13/31 "BACKUP" REGULATOR, 5-STEP BRIGHTNESS CONTROL.
- ⑭ NEW RUNWAY 13/31 "NORMAL" REGULATOR, 5-STEP BRIGHTNESS CONTROL.
- ⑮ NEW REGULATOR MONITOR WIRING AS REQUIRED TO ACCOMPLISH THE FOLLOWING:
 - LOSS OF INPUT POWER TO THE CCR.
 - CCR SHUTDOWN BY OPEN-CIRCUIT / OVER-CURRENT PROTECTIVE DEVICES.
 - DROP OF MORE THAN 10% IN THE CCR VA LOAD.
 - FAILURE OF THE CCR TO DELIVER THE SELECTED OUTPUT CURRENT.
 - THE NUMBER OF BURNT-OUT LAMPS IN EACH SERIES CIRCUIT.
 - REMOTE / LOCAL STATUS OF THE CCR.
 - ACTUAL CCR OUTPUT CURRENT
 - ACTUAL CCR OUTPUT VOLTAGE
 - ACTUAL CCR OUTPUT LOAD (WATTAGE)
- ⑯ EXISTING RUNWAY 13/31 & RUNWAY 04/22 REGULATOR SELECTOR CABINET.
- ⑰ EXISTING CONTROL WIRING.
- ⑱ NEW 5-STEP REGULATOR BRIGHTNESS CONTROL WIRING.
- ⑳ EXISTING RUNWAY 04/22 "NORMAL" REGULATOR, 5-STEP BRIGHTNESS CONTROL.
- ㉑ EXISTING RUNWAY 04/22 "BACKUP" REGULATOR, 5-STEP BRIGHTNESS CONTROL.
- ㉒ EXISTING L-847 CIRCUIT SELECTOR SWITCH.

L-890 BLOCK DIAGRAM LEGEND

- DCMI - DISTRIBUTED CONTROL AND MONITORING INTERFACE
- T - TERMINATOR
- UPS - UNINTERRUPTIBLE POWER SUPPLY
- CONV - COPPER TO FIBER CONVERSION MODULE
- FOTB - FIBER OPTIC TERMINATOR BOX
- AP - WIRELESS ACCESS POINT
- LA - LIGHTNING ARRESTOR
- FS - FAST ETHERNET SWITCH
- M - MODEM
- PCEX - VIDEO EXTENDER (TRANSMITTER)
- PCER - VIDEO EXTENDER (RECEIVER)
- RVR - EXISTING RUNWAY VISUAL RANGE WIRING IN TOWER CAB

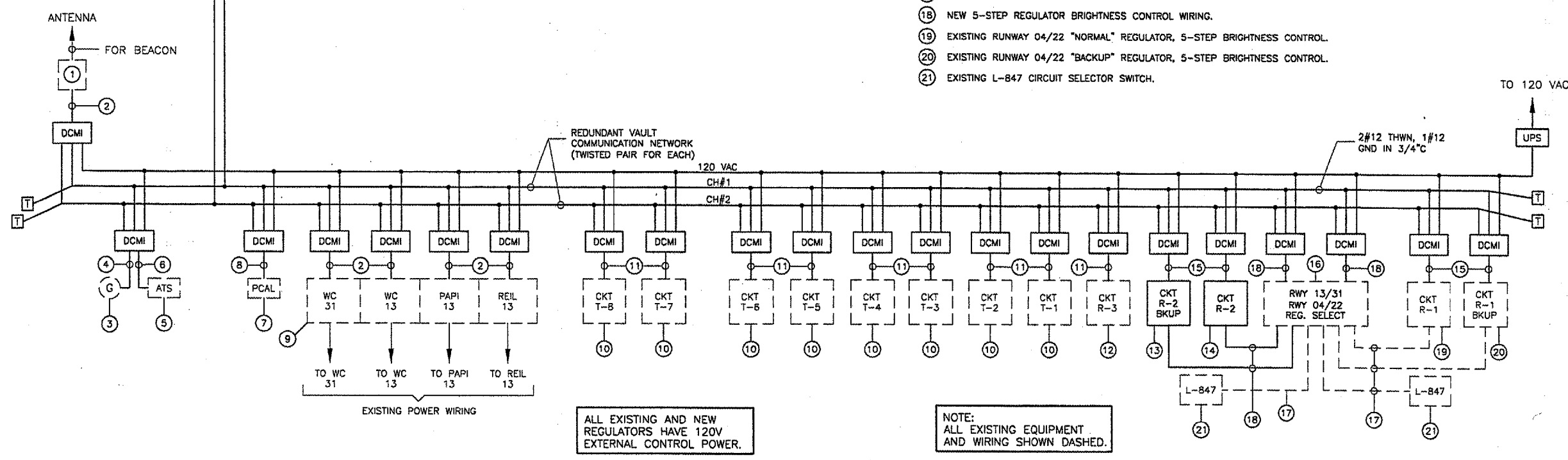
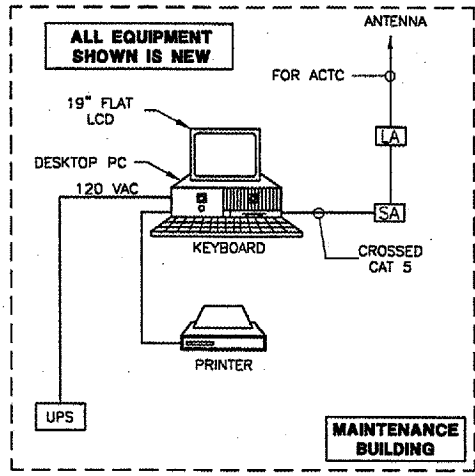
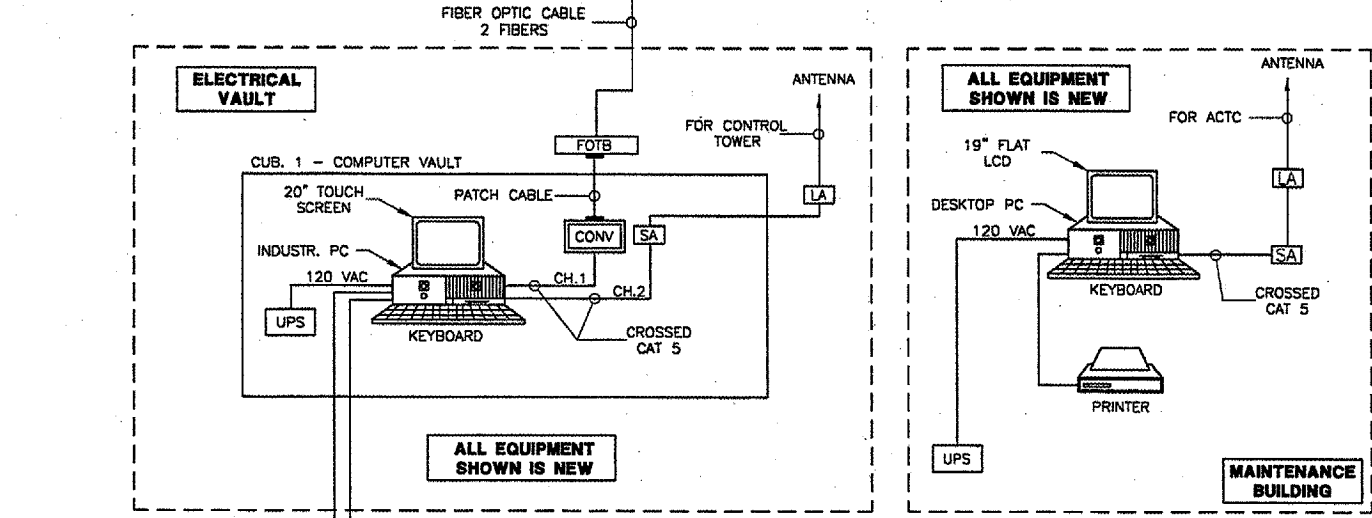
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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 L-890 AIRFIELD LIGHTING CONTROL AND MONITORING SYSTEM



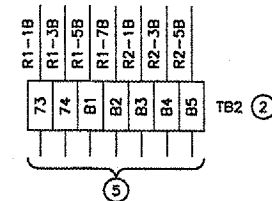
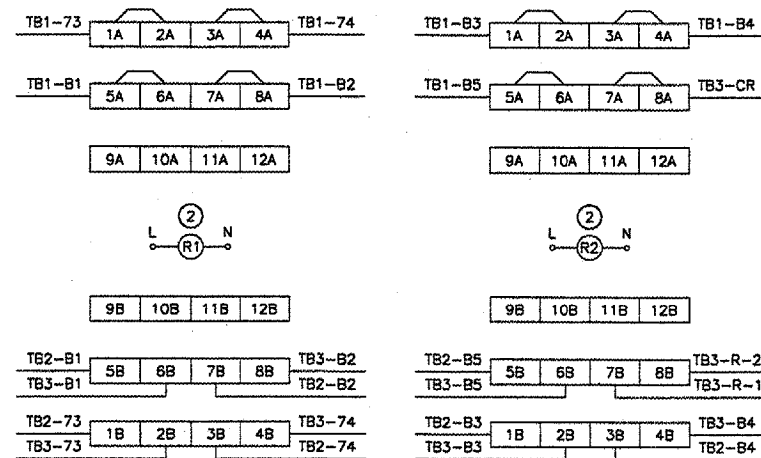
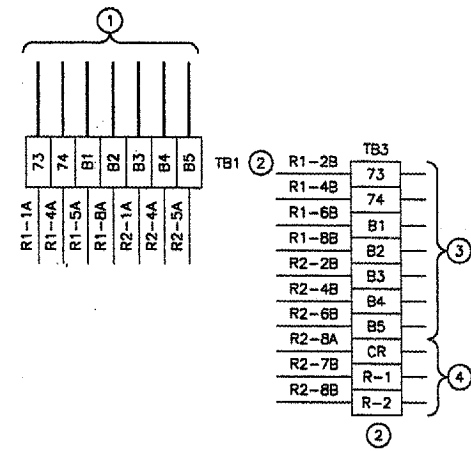
ALL EXISTING AND NEW REGULATORS HAVE 120V EXTERNAL CONTROL POWER.

NOTE:
 ALL EXISTING EQUIPMENT AND WIRING SHOWN DASHED.

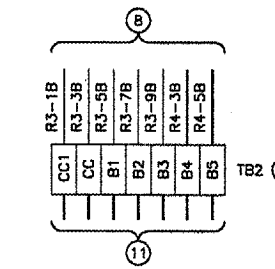
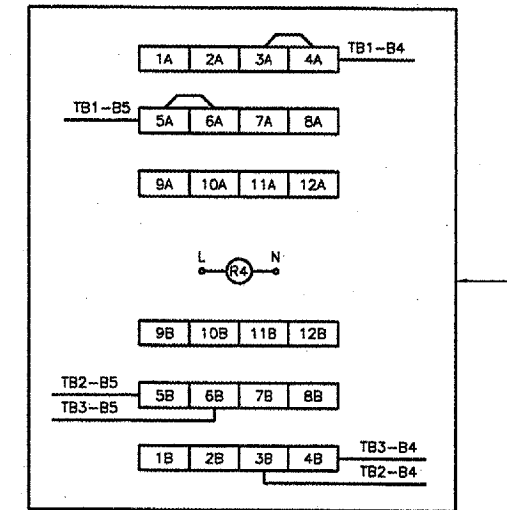
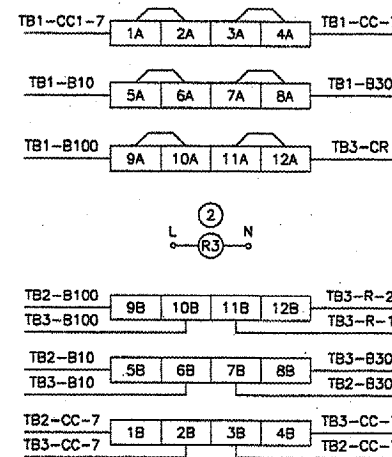
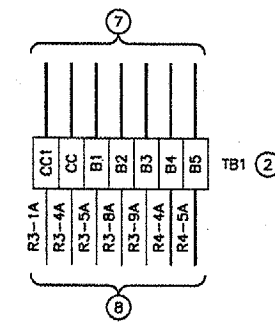
L-890 SYSTEM BLOCK DIAGRAM

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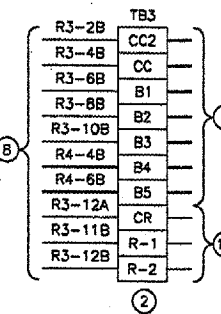
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 AIP PROJ. NO. 3-17-0096-42
 SHEET 63 OF 70



**RUNWAY 4/22 REGULATOR TRANSFER
 RELAY WIRING MODIFICATIONS**



**RUNWAY 13/31 REGULATOR TRANSFER
 RELAY WIRING MODIFICATIONS**



REVISIONS		
NUMBER	BY	DATE

0 1 2
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 AT FULL SCALE (34X22).

REGULATOR SELECT PANEL MODIFICATIONS NOMENCLATURE

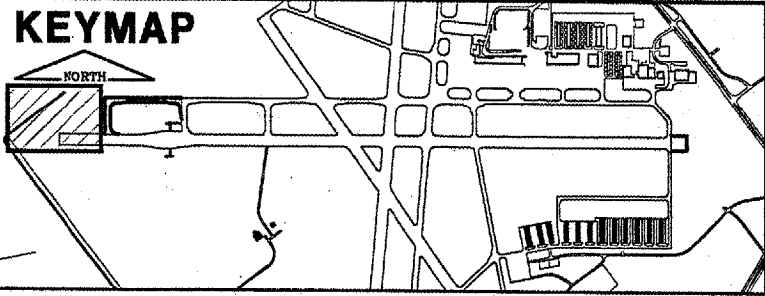
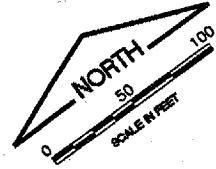
- NEW 120V WIRING FROM EXISTING PANELBOARD AND 5-STEP CONTROL WIRING FROM NEW DISTRIBUTED CONTROL AND MONITORING INTERFACE (DCMI). CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- EXISTING TERMINAL STRIPS AND TRANSFER RELAYS AND INTERNAL WIRING. CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- EXISTING CONTROL WIRING TO RWY 04/22 BACKUP REGULATOR. CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- EXISTING CONTROL WIRING TO RWY 04/22 L-847 CIRCUIT SELECTOR SWITCH. CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- EXISTING CONTROL WIRING TO RWY 04/22 REGULATOR. CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- CONTRACTOR SHALL DISCONNECT AND REMOVE ONE OF THE TRANSFER RELAYS FROM THE EXISTING L-854 PCAL SYSTEM TRANSFER RELAY CABINET AND RELOCATE TO REGULATOR SELECT CONTROL PANEL. INSTALL NEW INTERNAL WIRING AS SHOWN.
- NEW 120V WIRING FROM EXISTING PANELBOARD AND 5-STEP CONTROL WIRING FROM NEW DISTRIBUTED CONTROL AND MONITORING INTERFACE (DCMI). CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- EXISTING AND NEW INTERNAL WIRING. NOTE THAT SOME OF THE EXISTING WIRING HAS BEEN RELOCATED TO OTHER TERMINAL BLOCKS ON EACH TERMINAL STRIP. CONTRACTOR SHALL RELABEL EXISTING TERMINAL BLOCKS AS SHOWN. CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- NEW CONTROL WIRING TO RWY 13/31 BACKUP REGULATOR. CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- EXISTING CONTROL WIRING TO RWY 13/31 L-847 CIRCUIT SELECTOR SWITCH. CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.
- NEW CONTROL WIRING TO RWY 13/31 REGULATOR. CONTRACTOR SHALL VERIFY WIRING BEFORE COMMENCING THE WORK.

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 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS**

**CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 REGULATOR SELECT PANEL MODIFICATIONS**

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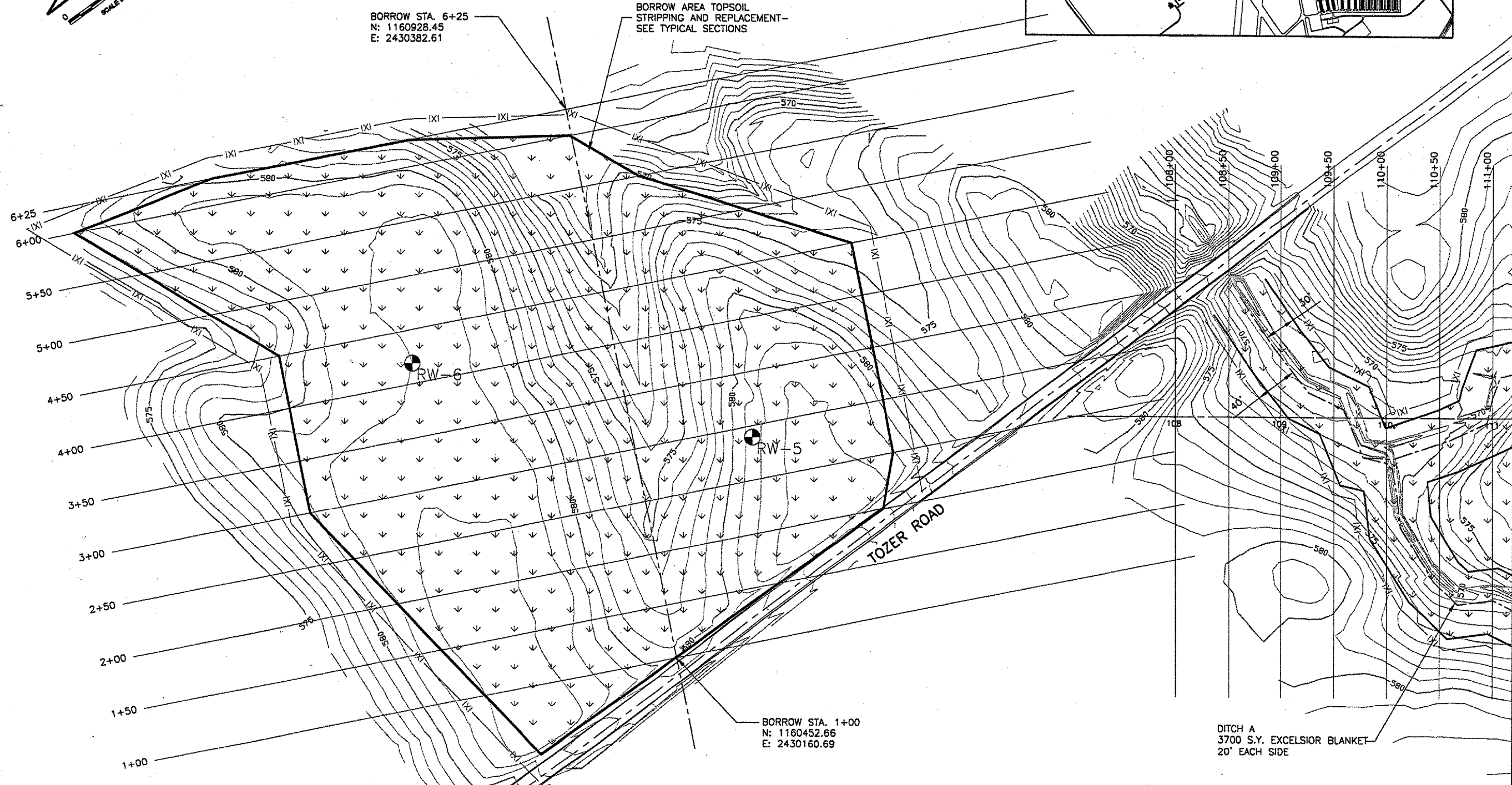
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IL PROJ. NO. SPI-3488 AIP PROJ. NO. 3-17-0096-42	



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 tbinfo
 BASE_PROPDRAIN
 BASE_PROPTURF
 CopAirBase
 ContourBase
 Capital_Base
 KEY

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



BORROW STA. 6+25
 N: 1160928.45
 E: 2430382.61

BORROW AREA TOPSOIL
 STRIPPING AND REPLACEMENT-
 SEE TYPICAL SECTIONS

BORROW STA. 1+00
 N: 1160452.66
 E: 2430160.69

DITCH A
 3700 S.Y. EXCELSIOR BLANKET
 20' EACH SIDE

LEGEND

- 112+50 CROSS SECTION
- NEW SEEDING & MULCHING
- NEW SILT FENCE
- NEW INLET PROTECTION
- NEW SOD LIMITS
- SOIL BORING LOCATION
SB-3

NOTE:

- TURFING LIMITS SHOWN ARE APPROXIMATE. ACTUAL TURFING LIMITS SHALL BE DETERMINED BY THE ENGINEER.
- AN ADDITIONAL 1000 SY OF EXCELSIOR BLANKET, 1200 L.F. OF SILT FENCE, AND 50 S.Y. OF SODDING HAVE BEEN INCLUDED IN THE QUANTITIES FOR ADDITIONAL DISTURBED AREAS AS REQUIRED BY THE ENGINEER.
- RESTORATION AND TURFING OF DISTURBED AREAS BEYOND THE LIMITS SHOWN (SUCH AS EDGE LIGHTING, CABLING, SIGNAGE, ACCESS, STAGING, ETC.) SHALL BE INCIDENTAL TO THE PROJECT.

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ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
INDEX TO CROSS SECTIONS - TURFING PLAN 1

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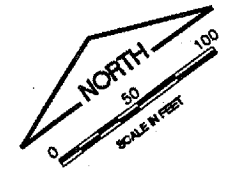
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DATE:	04/24/07
JOB No:	05035-03

IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0098-42

208	209	210	211
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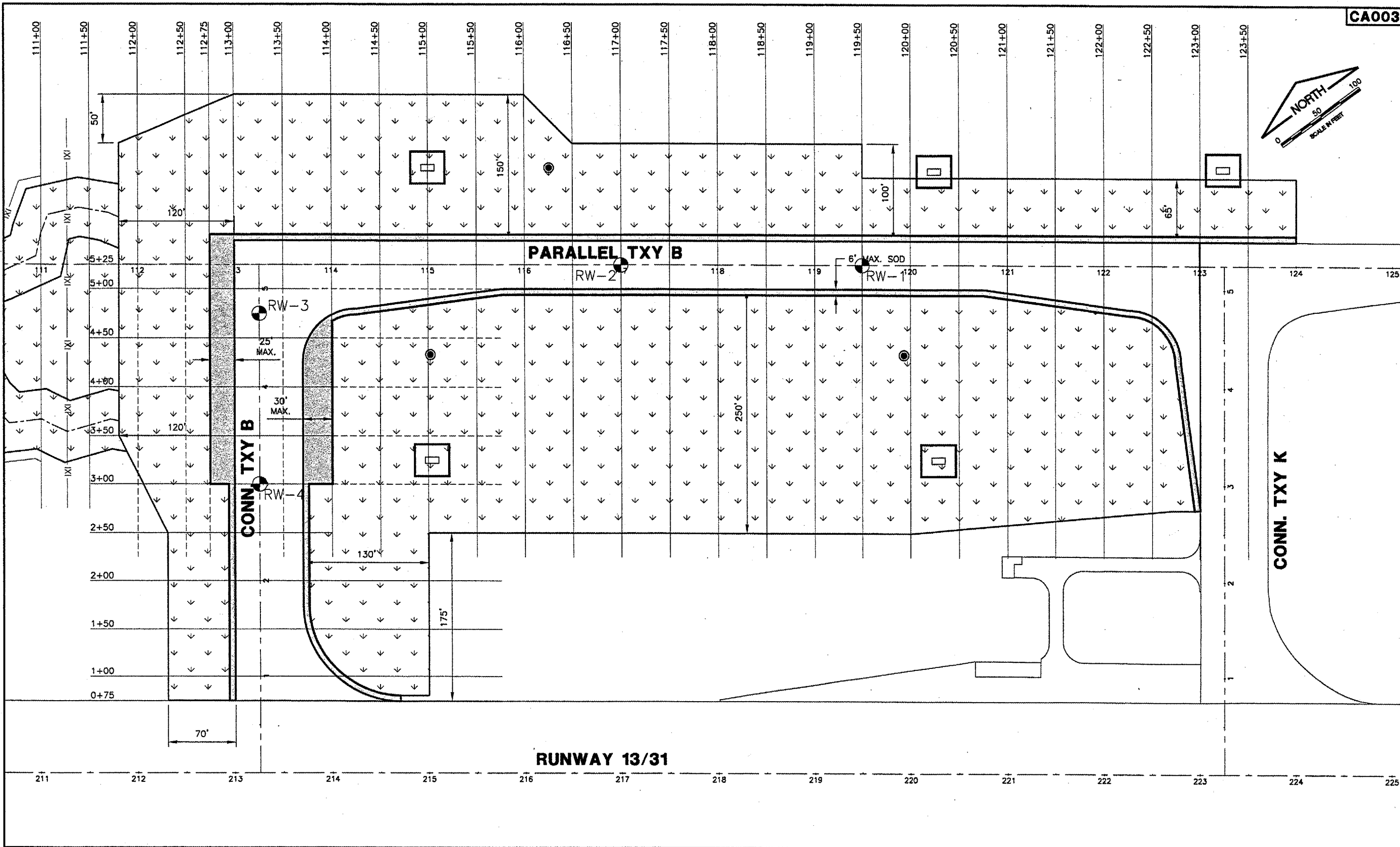
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 BASE_PROPTURF



REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
INDEX TO CROSS SECTIONS - TURFING PLAN 2

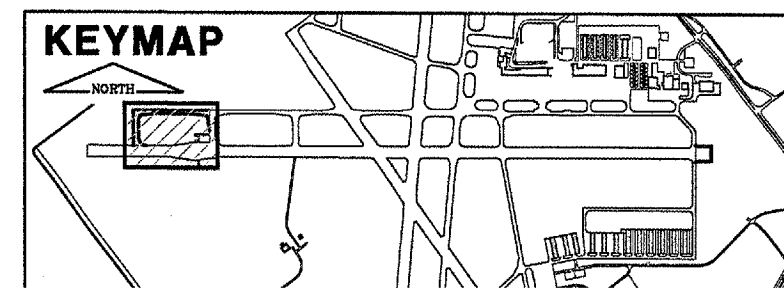
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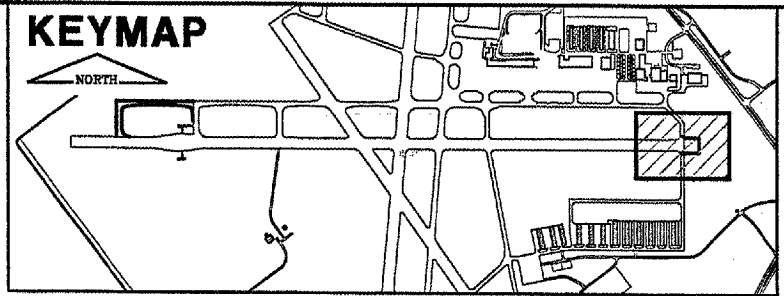
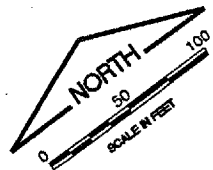
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JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42

LEGEND

112+50	CROSS SECTION
↓ ↓ ↓	NEW SEEDING & MULCHING
-IXI-	NEW SILT FENCE
□	NEW INLET PROTECTION
▨	NEW SOD LIMITS
SB-3	SOIL BORING LOCATIONS

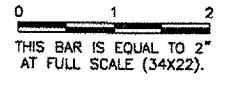
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 2. AN ADDITIONAL 1000 SY OF EXCELSIOR BLANKET, 1200 L.F. OF SILT FENCE, AND 50 S.Y. OF SODDING HAVE BEEN INCLUDED IN THE QUANTITIES FOR ADDITIONAL DISTURBED AREAS AS REQUIRED BY THE ENGINEER.
 3. RESTORATION AND TURFING OF DISTURBED AREAS BEYOND THE LIMITS SHOWN (SUCH AS EDGE LIGHTING, CABLING, SIGNAGE, ACCESS, STAGING, ETC.) SHALL BE INCIDENTAL TO THE PROJECT.



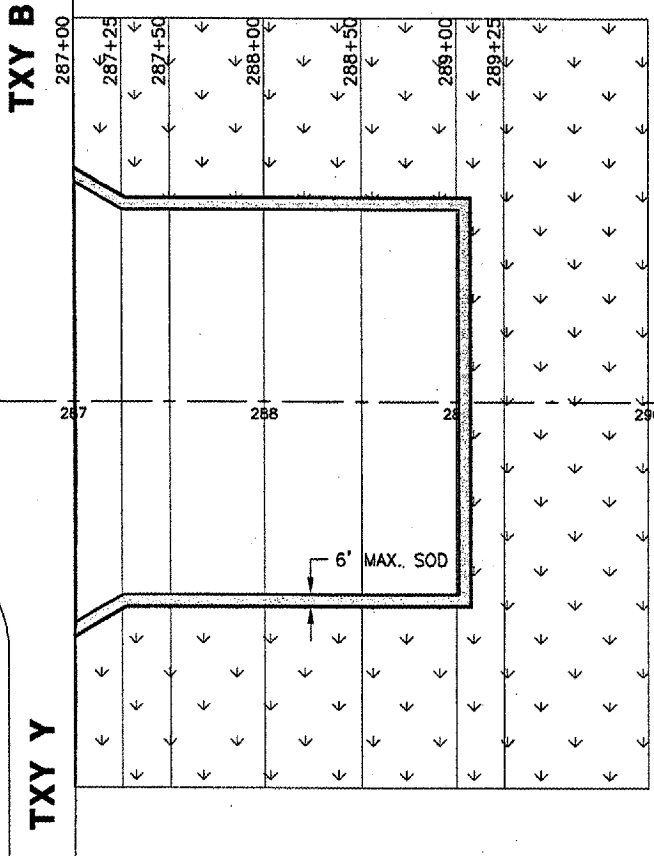
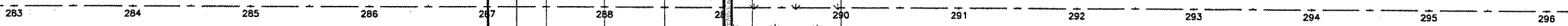


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REVISIONS		
NUMBER	BY	DATE




RUNWAY 13/31



LEGEND

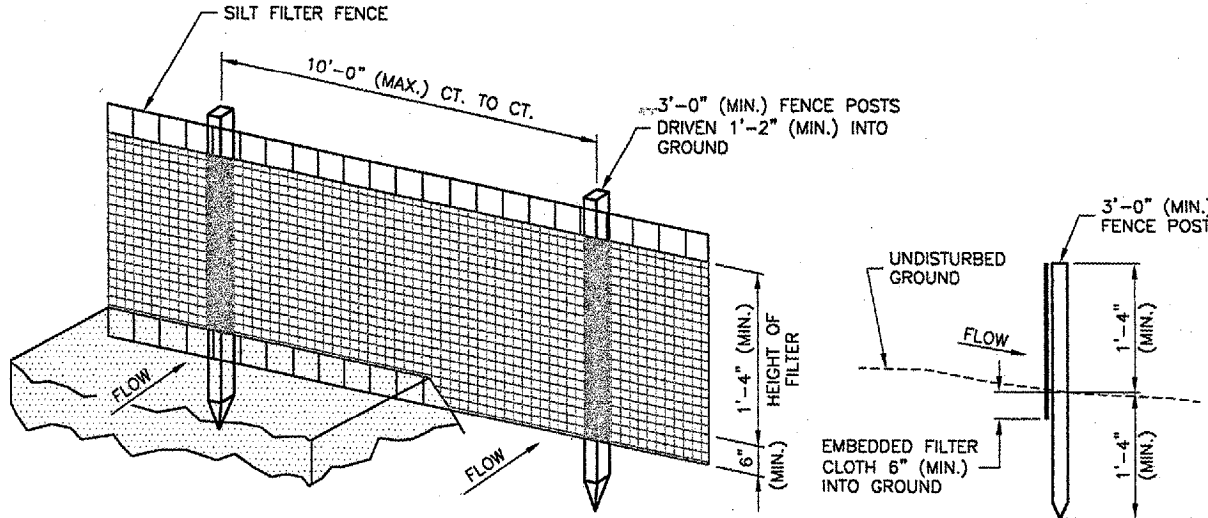
- 112+50 ——— CROSS SECTION
- ↙ ↘ ↙ ↘ ↙ ↘ NEW SEEDING & MULCHING
- NEW INLET PROTECTION
- ▨ NEW SOD LIMITS


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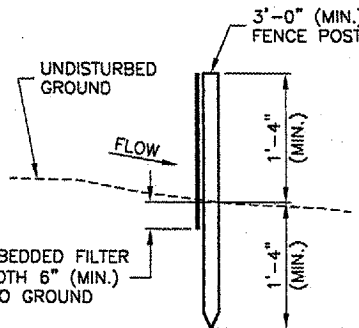
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
INDEX TO CROSS SECTIONS - TURFING PLAN 3

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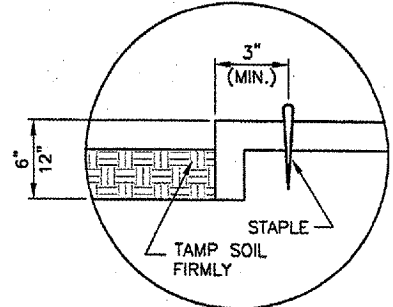
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IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42
SHEET	67 OF 79 SHEETS



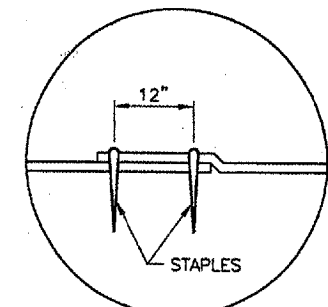
PERSPECTIVE VIEW
EROSION CONTROL FABRIC FENCE DETAIL
 N.T.S.



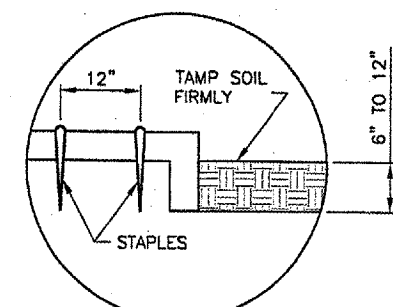
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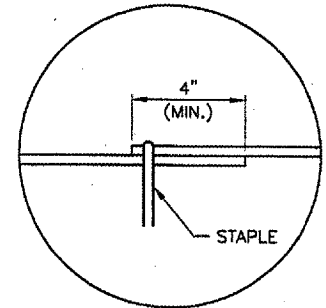
DETAIL 1 - TERMINAL FOLD



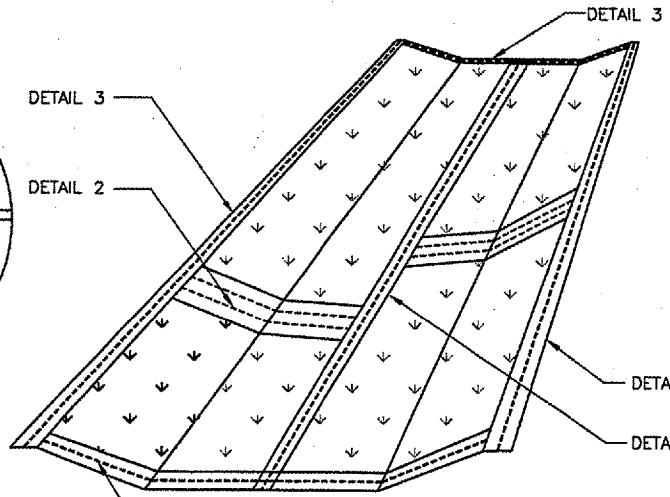
DETAIL 2 - JUNCTION SLOT



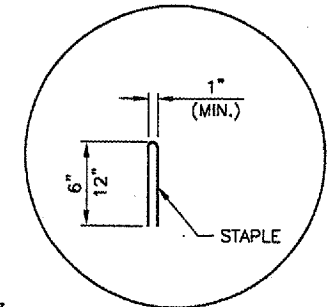
DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



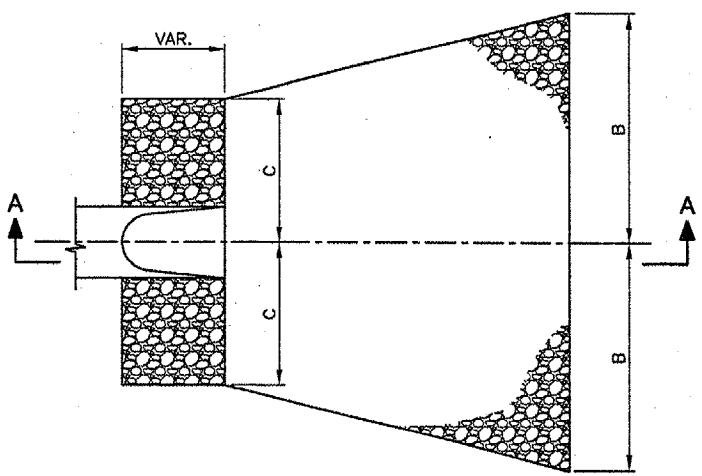
PLAN
EXCELSIOR BLANKET DETAILS
 N.T.S.



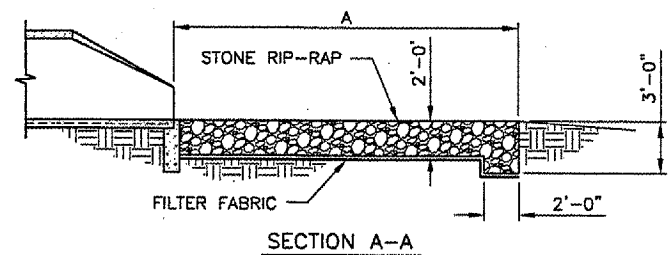
DETAIL 5 - STAPLE DETAIL

- NOTES**
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
 2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
 3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
 4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.

- NOTES**
1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
 2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
 3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



PLAN

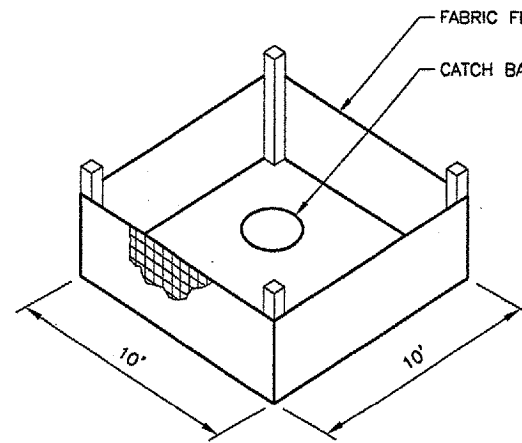


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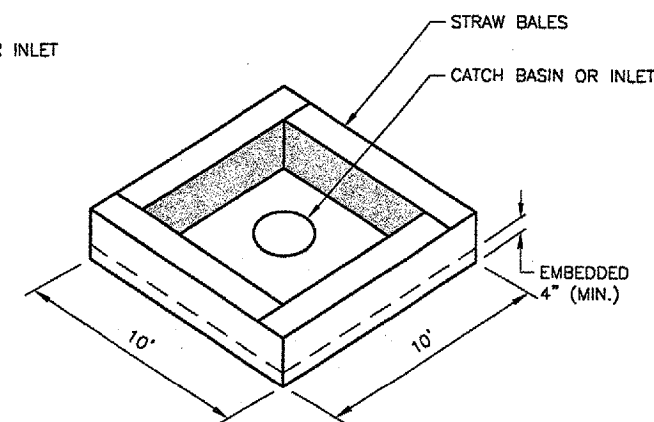
DIMENSIONS - TABLE 1

INSIDE DIAMETER STORM SEWER (IN.)	OUTLET DIMENSION (FT.)			INLET DIMENSION (FT.)		
	A	B	C	A	B	C
12" THRU 24"	20	11	3	14	8	3
27" AND 30"	22	12	4	16	9	4
36" AND 42"	28	16	5	20	12	5
48" AND 60"	34	20	8	26	16	8

RIP-RAP DETAILS
 N.T.S.



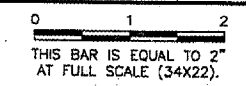
INLET PROTECTION WITH FABRIC
 N.T.S.



INLET PROTECTION WITH STRAW BALES
 N.T.S.

REVISIONS

NUMBER	BY	DATE



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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EROSION CONTROL DETAILS

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 DRAWN BY: DPA
 CHECKED BY: RLV
 APPROVED BY: RLV
 DATE: 04/24/07
 JOB No: 05035-03
 I.L. PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42

EARTHWORK SUMMARY

STATION	EXCAVATE END AREA (S.F.)	EMBANK END AREA (S.F.)
CONNECTING TAXIWAY B		
0+75.00	312.2	0.0
1+00.00	118.3	3.2
1+50.00	23.2	21.3
2+00.00	15.5	28.0
2+50.00	0.0	138.8
3+00.00	0.0	928.3
3+50.00	0.0	1419.7
4+00.00	0.0	650.8
4+50.00	0.0	308.2
5+00.00	0.0	424.5
5+25.00	0.0	0.0

BLAST PAD		
287+00.59	152.8	0.0
287+25.55	98.1	1.8
287+50.00	79.5	0.5
288+00.00	22.8	16.8
288+50.00	38.5	4.4
289+00.00	109.8	0.0
289+00.25	0.0	0.0

EARTHWORK SUMMARY

STATION	EXCAVATE END AREA (S.F.)	EMBANK END AREA (S.F.)
PARALLEL TAXIWAY B		
112+00.00	0.0	0.0
112+50.00	0.0	376.4
112+75.00	0.0	809.1
113+00.54	0.0	979.0
113+50.00	0.4	914.6
114+00.00	3.0	1602.9
114+50.00	60.1	1307.5
115+00.00	370.5	847.6
115+50.00	419.4	875.1
116+00.00	304.4	1054.0
116+50.00	224.1	634.9
117+00.00	323.3	196.3
117+50.00	356.6	109.8
118+00.00	286.4	0.6
118+50.00	282.1	0.1
119+00.00	271.1	0.0
119+50.00	216.3	1.2
120+00.00	114.9	11.7
120+50.00	59.7	14.1
121+00.00	48.3	24.5
121+50.00	48.3	28.9
122+00.00	67.0	15.6
122+50.00	122.5	1.0
122+99.33	531.0	0.0
123+50.00	0.0	0.0

EARTHWORK SUMMARY


STATION	TOPSOIL EXCAVATE END AREA (S.F.)	EXCAVATE END AREA (S.F.)
BORROW AREA		
0+00.00	0.0	0.0
0+50.00	0.0	0.0
1+00.00	176.4	0.6
1+50.00	231.1	80.2
2+00.00	436.8	368.8
2+50.00	483.8	680.6
3+00.00	515.4	1071.9
3+50.00	524.1	1331.3
4+00.00	496.8	1468.3
4+50.00	517.5	1766.5
5+00.00	460.3	1764.2
5+50.00	460.5	2019.8
6+00.00	495.4	2048.5
6+25.00	367.5	575.4

EARTHWORK SUMMARY

STATION	EXCAVATE END AREA (S.F.)	EMBANK END AREA (S.F.)
DITCH A		
0+00	44.6	0.0
0+50	36.8	0.6
1+00	16.8	0.0
1+50	15.9	0.0
2+00	30.7	0.0
2+50	19.1	0.0
3+00	18.8	1.2
3+50	3.8	2.0
4+00	5.5	0.0
4+50	8.6	1.0
5+00	16.9	0.8

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).


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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
EARTHWORK SUMMARY

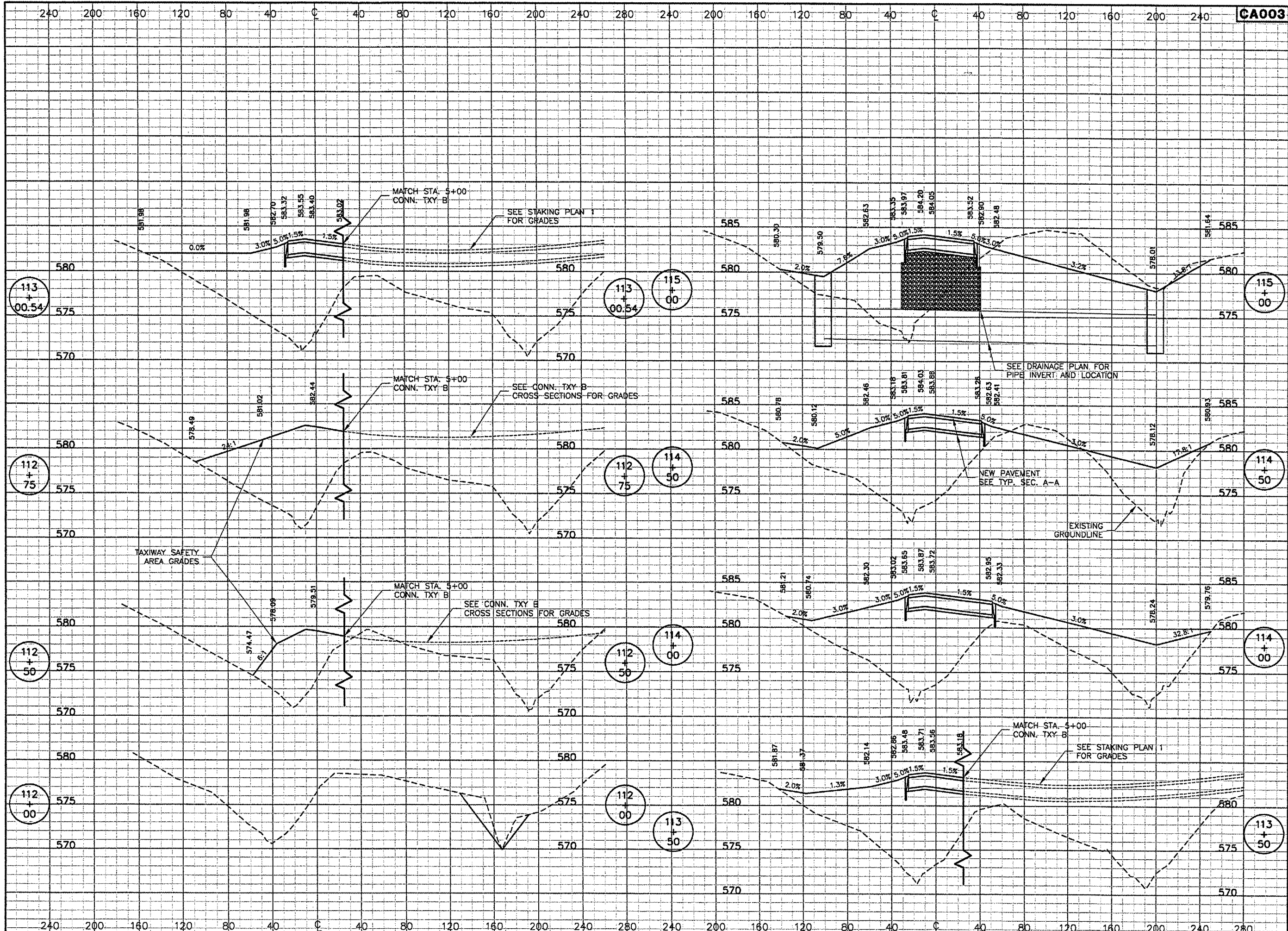
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APPROVED BY:	<i>RLW</i>
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42

BORROW AREA NOTES:

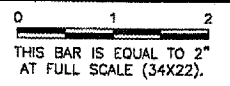
1. EMBANKMENT MATERIAL SHALL BE OBTAINED FROM THE BORROW AREA NORTH OF TOZER ROAD.
2. THE BORROW AREA SHALL ONLY BE EXCAVATED TO THE DEPTH REQUIRED TO CONSTRUCT THE NEW EMBANKMENTS.



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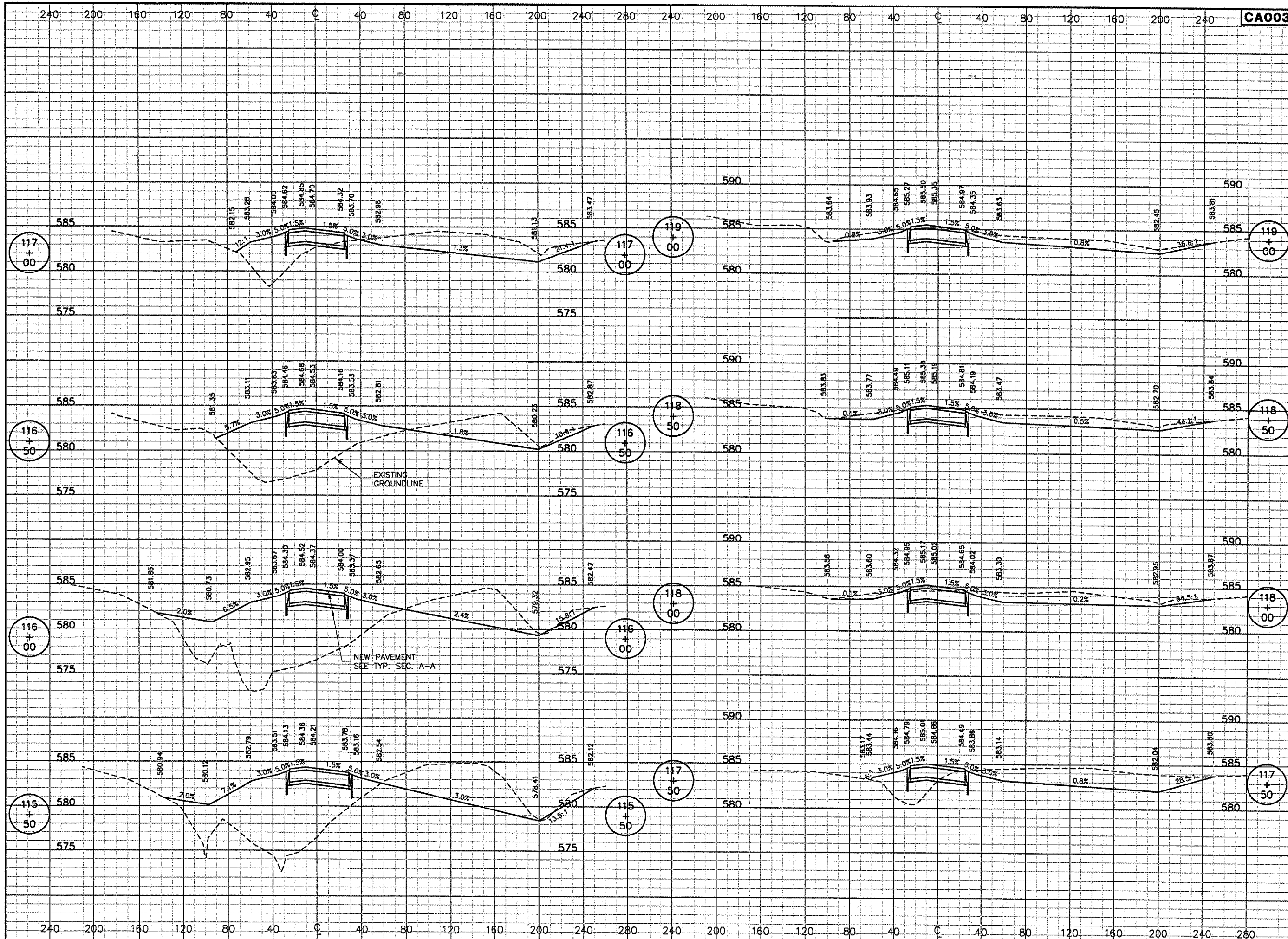
CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 TAXIWAY B CROSS SECTIONS 1
 STA. 112+00 TO STA. 115+00

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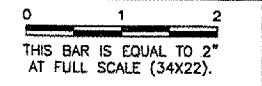
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IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 70 OF 70 SHEETS



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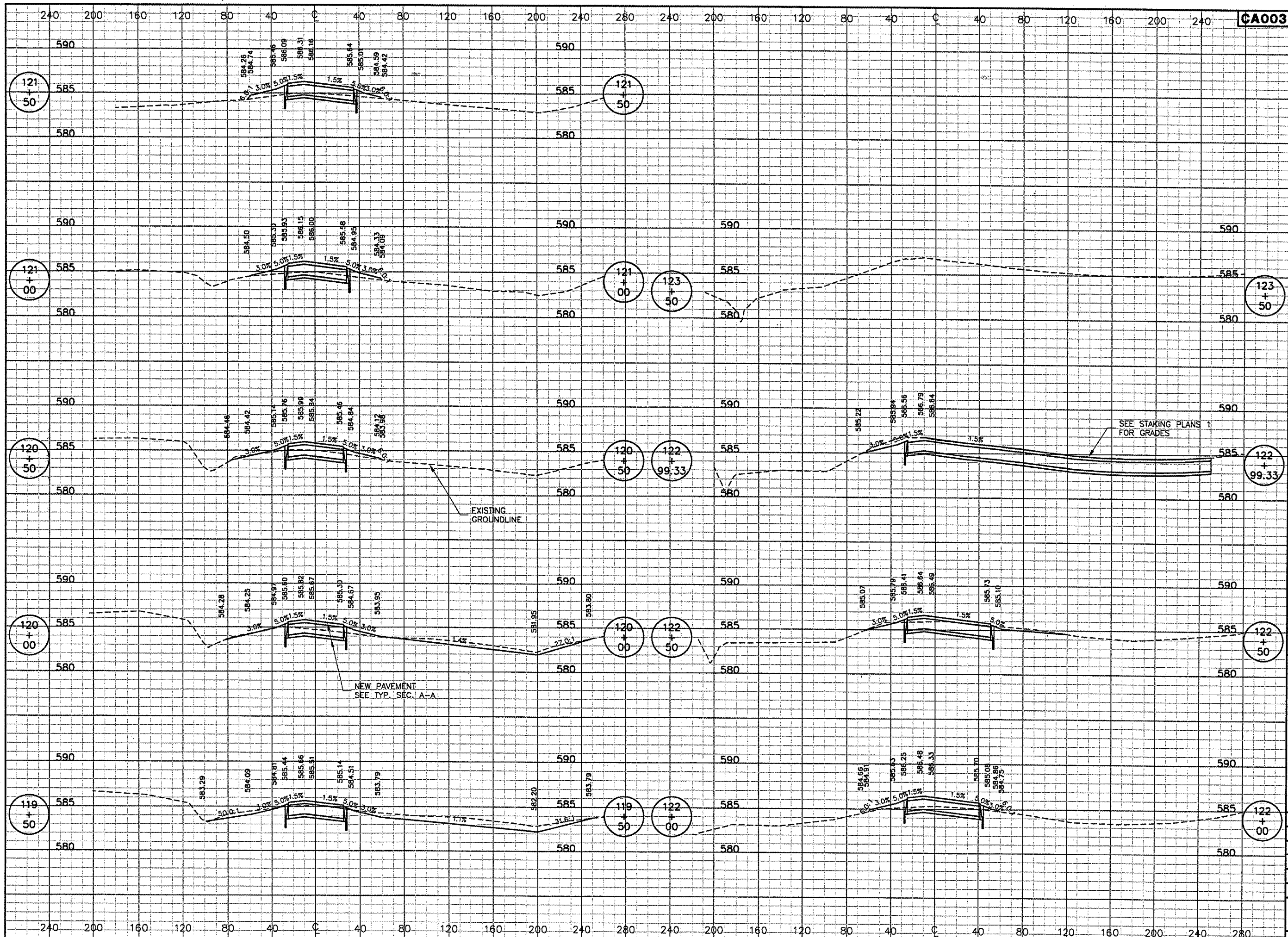
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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
TAXIWAY B CROSS SECTIONS 2
STA. 115+50 TO STA. 119+00

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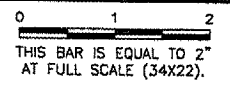


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 AIP PROJ. NO. 3-17-0096-42
 SHEET 71 OF 78 SHEETS



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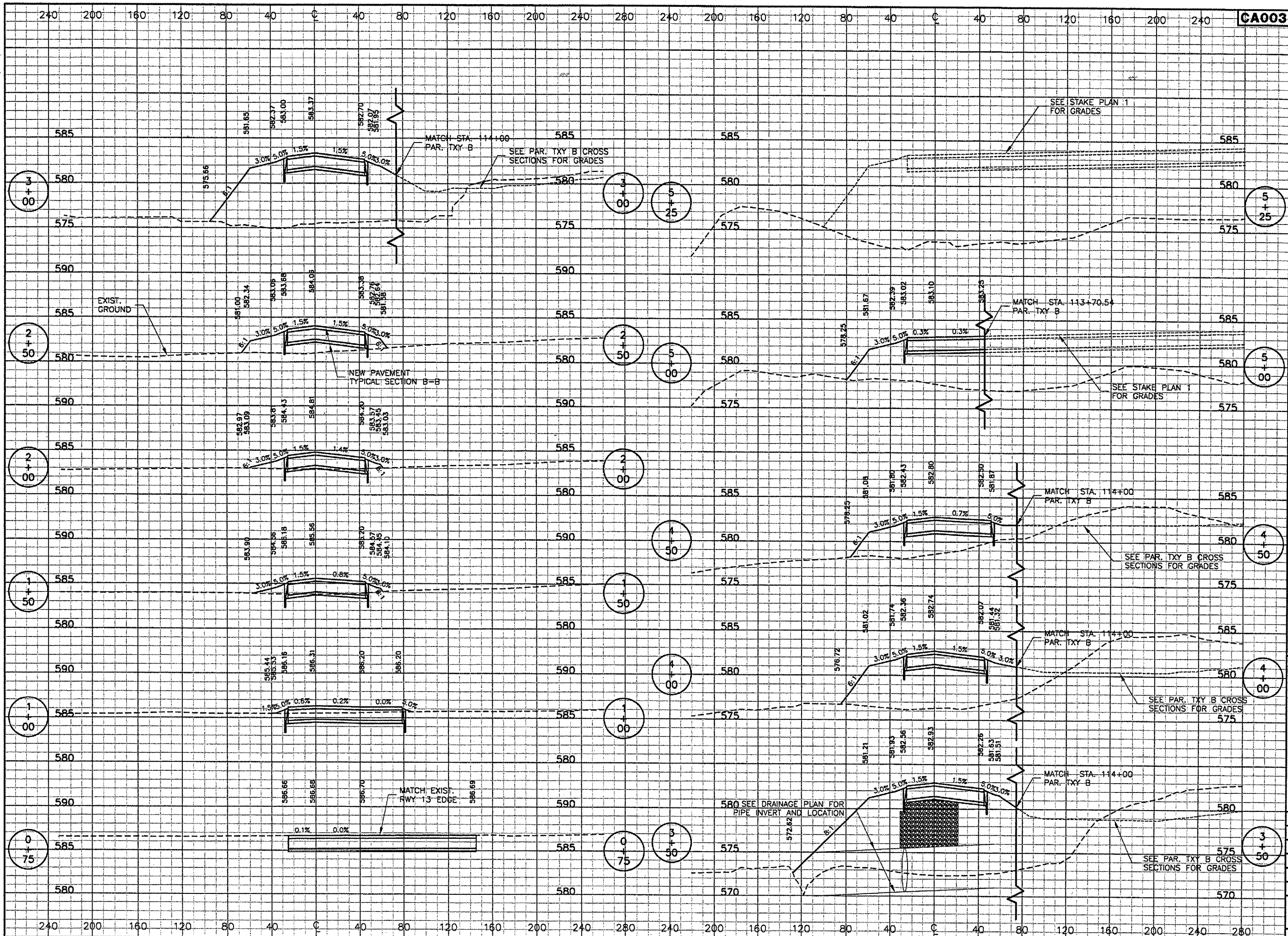


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SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS 3
TAXIWAY B CROSS SECTIONS 3
STA. 119+50 TO STA. 123+50

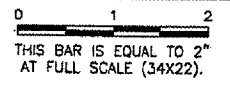
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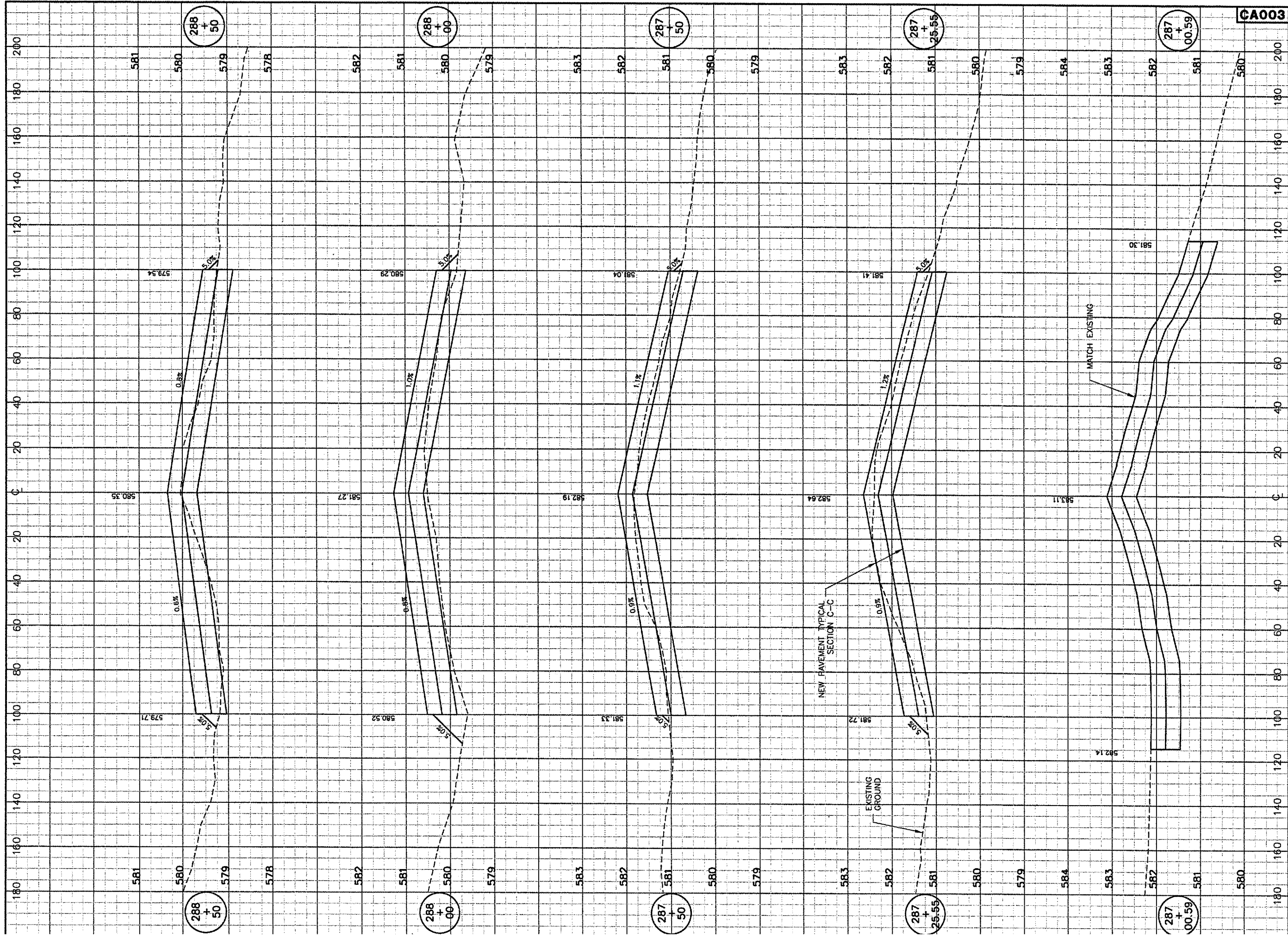
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ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
CONNECTING TAXIWAY B CROSS SECTION 1
STA. 0+75 TO STA. 5+25

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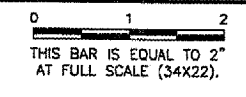


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JOB No:	05035-03



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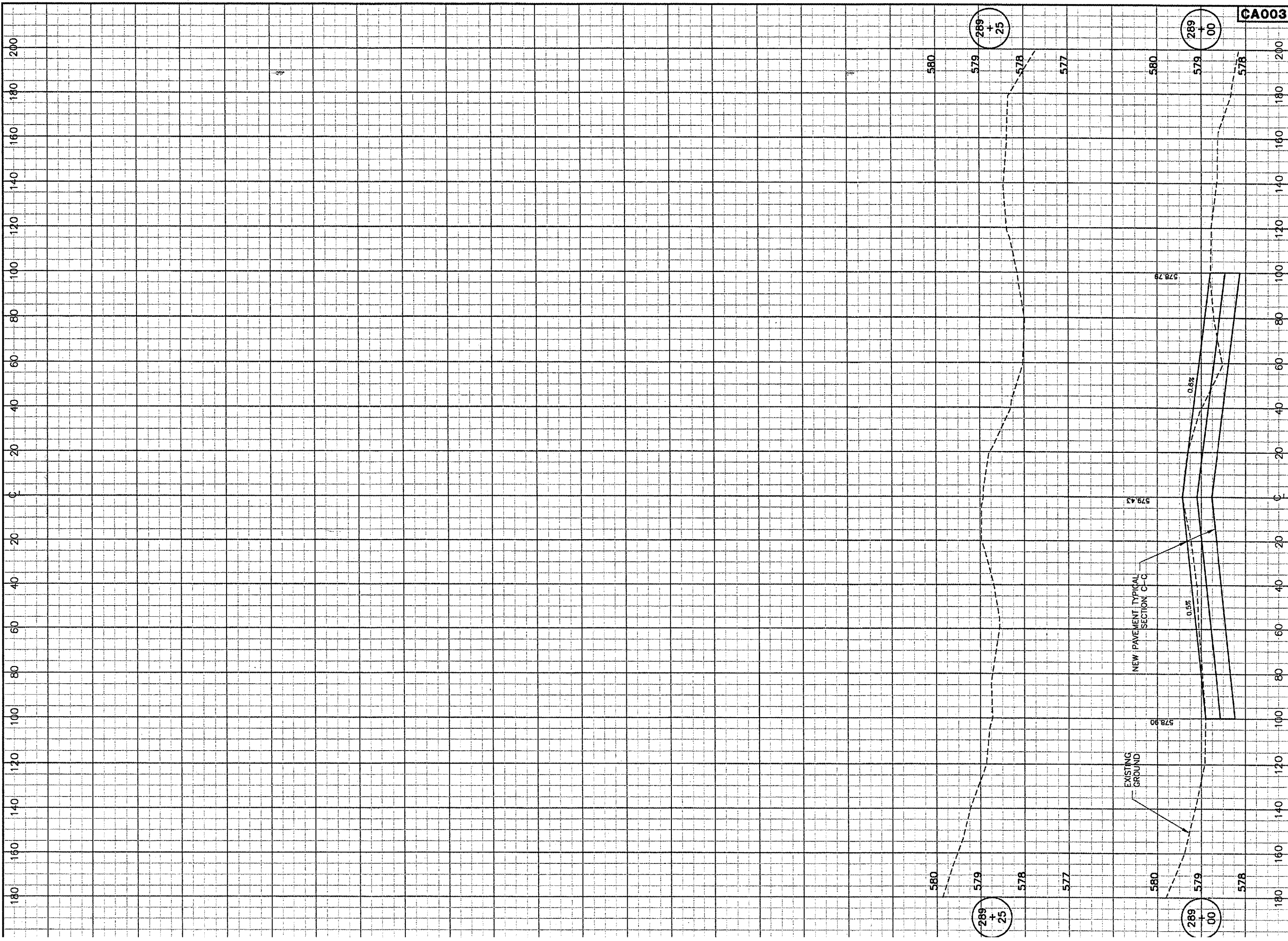


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CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 BLAST PAD CROSS SECTIONS 1
 STA. 287+00.59 TO STA. 288+50

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 FILE: XSEC06.dwg
 UPDATE BY: Allan Draughan
 PLOT DATE: 4/30/2007 2:49 PM
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 tbinfo

REVISIONS		
NUMBER	BY	DATE

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 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34X22).

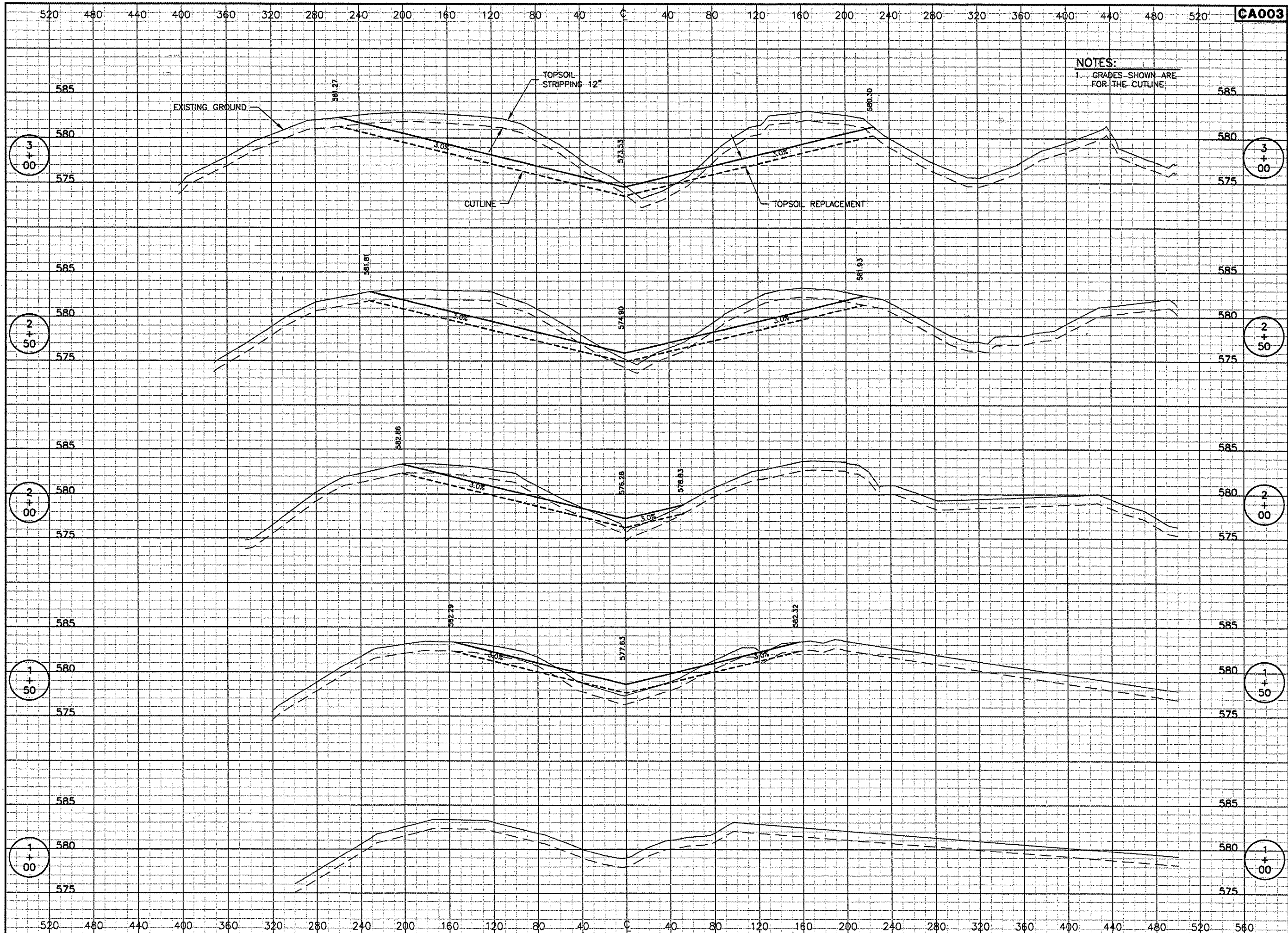


SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
BLAST PAD CROSS SECTIONS 2
STA. 289+00 TO STA. 289+50

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DESIGN BY:	KLB
DRAWN BY:	DPA
CHECKED BY:	RLV
APPROVED BY:	RLV
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO.	SPI-3488
AIP PROJ. NO.	3-17-0096-42
SHEET	75 OF 79 SHEETS

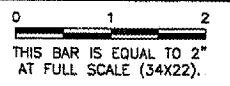


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 UPDATE BY: Dave Allen
 PLOT DATE: 5/7/2007 3:46 PM
 rd001006
 tbinfo

NOTES:
 1. GRADES SHOWN ARE FOR THE CUTLINE.

REVISIONS

NUMBER	BY	DATE



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
BORROW AREA CROSS SECTIONS 1
STA. 1+00 TO STA. 3+00

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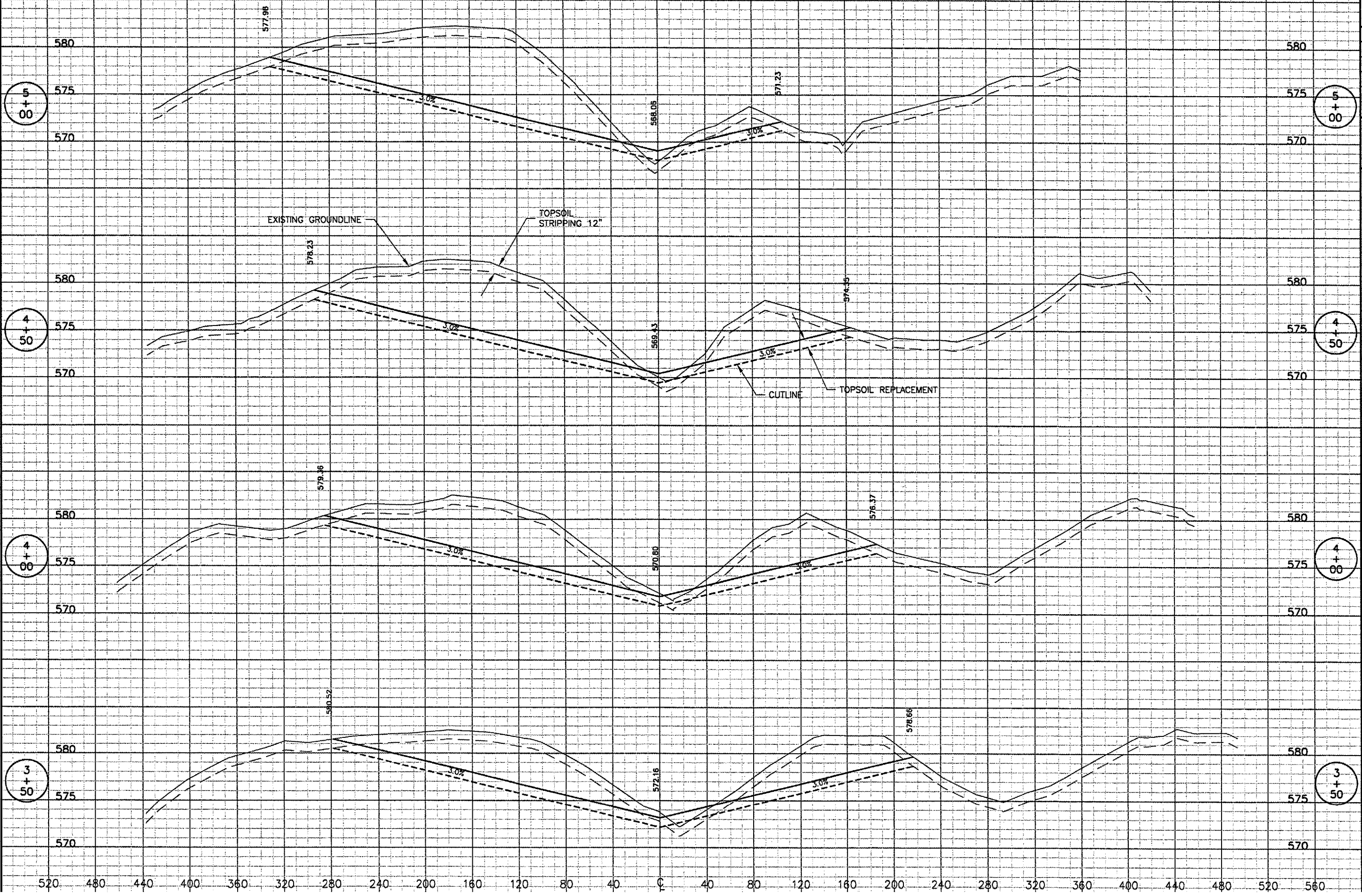
DESIGN BY: CBG
 DRAWN BY: DPA
 CHECKED BY: *RV*
 APPROVED BY: *RV*
 DATE: 04/24/07
 JOB No: 05035-03

IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 76 OF 79 SHEETS

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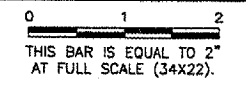
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 UPDATE BY: Dave Allen
 PLOT DATE: 5/7/2007 3:48 PM
 re002006
 tbinfo

NOTES:
 1. GRADES SHOWN ARE FOR THE CUTLINE.



REVISIONS

NUMBER	BY	DATE



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ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
BORROW AREA CROSS SECTIONS 2
STA. 3+50 TO STA. 5+00

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DESIGN BY:	CBG
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APPROVED BY:	REW
DATE:	04/24/07
JOB No:	05035-03
IL PROJ. NO. SPI-3488	
AIP PROJ. NO. 3-17-0096-42	
SHEET 77 OF 78 SHEETS	

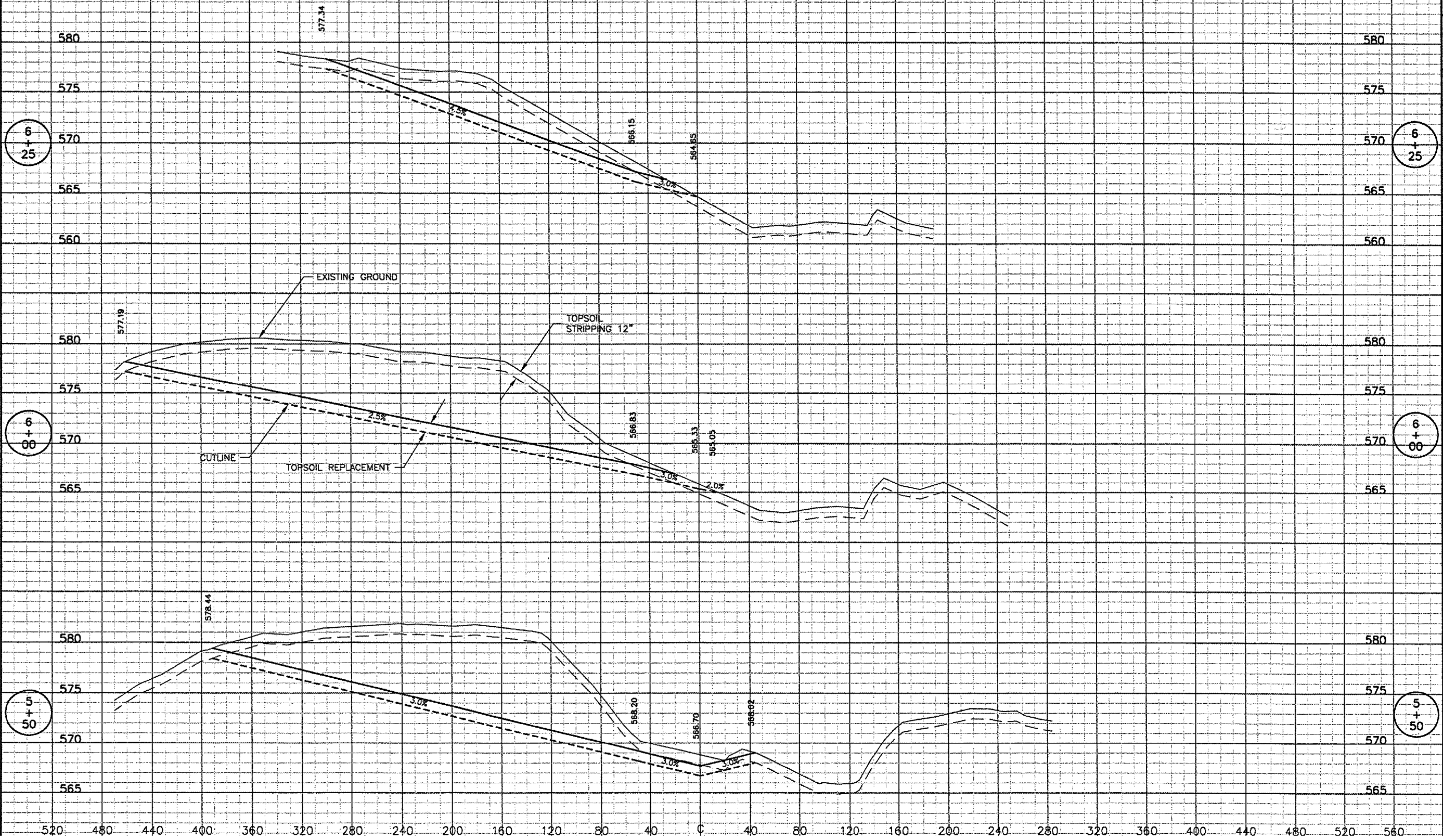
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CA003
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 UPDATE BY: Dove Allen
 PLOT DATE: 5/7/2007 3:49 PM
 re003006
 tbinfo

NOTES:
 1. GRADES SHOWN ARE FOR THE CUTLINE

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34x22).



SPRINGFIELD AIRPORT AUTHORITY
 ABRAHAM LINCOLN CAPITAL AIRPORT
 SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
 BORROW AREA CROSS SECTIONS 3
 STA. 5+50 TO STA. 6+25

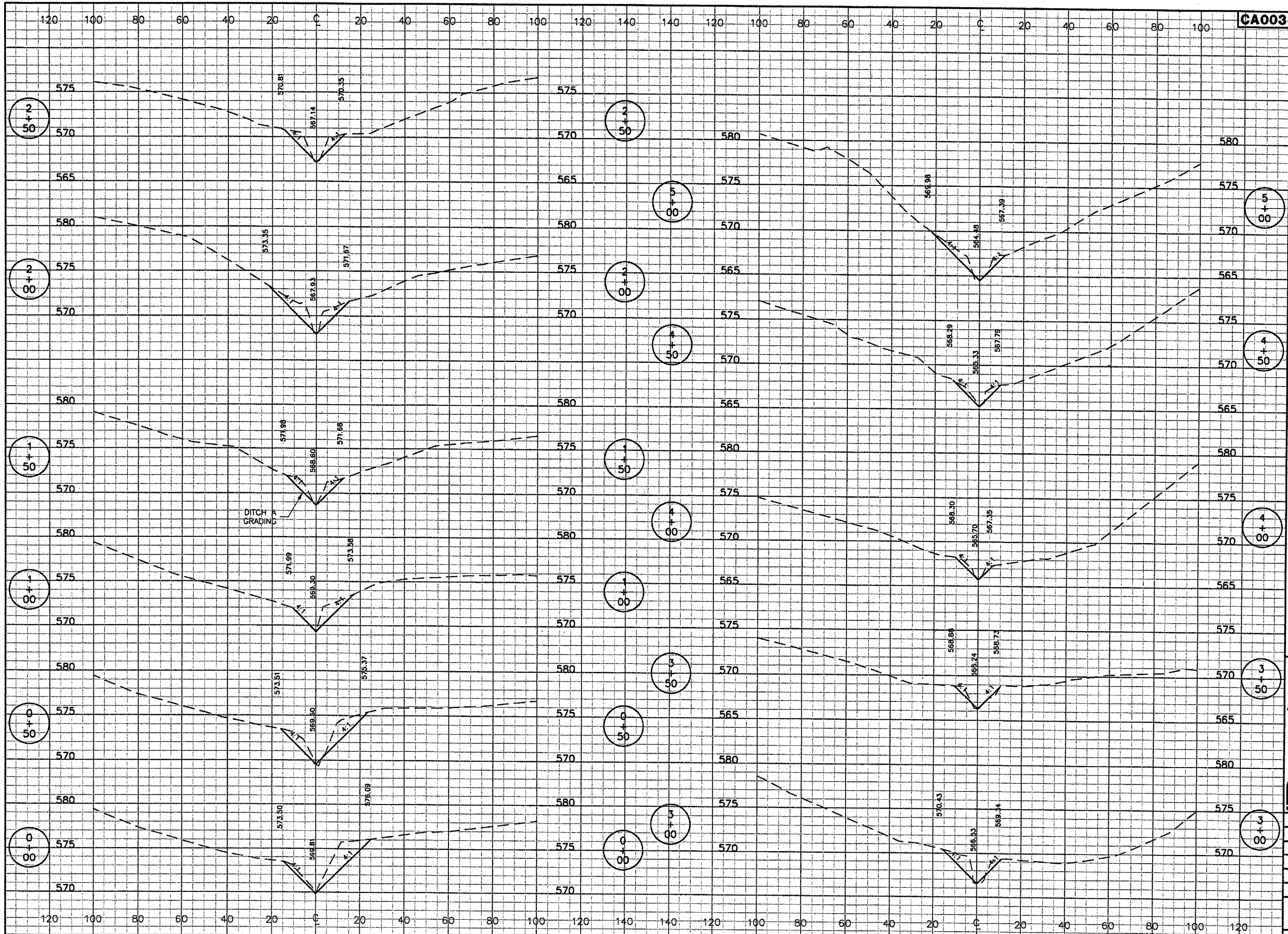
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DESIGN BY: CBG
 DRAWN BY: DPA
 CHECKED BY: RLW
 APPROVED BY: RLW
 DATE: 04/24/07
 JOB No: 05035-03

IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42

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CA003
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 FILE: XSEC10.dwg
 UPDATE BY: Allan Draughon
 PLOT DATE: 5/1/2007 8:59 AM
 rco01009
 tbinfa

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2'
 AT FULL SCALE (34x22).



SPRINGFIELD AIRPORT AUTHORITY
ABRAHAM LINCOLN CAPITAL AIRPORT
SPRINGFIELD, ILLINOIS

CONSTRUCT RUNWAY 13/31 RSA IMPROVEMENTS
DITCH A CROSS SECTIONS
STA. 0+00 TO STA. 5+00

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 DRAWN BY: DPA
 CHECKED BY: *PLN*
 APPROVED BY: *PLN*
 DATE: 04/24/07
 JOB No: 05035-03

IL PROJ. NO. SPI-3488
 AIP PROJ. NO. 3-17-0096-42
 SHEET 70 OF 70