

SCOPE OF WORK

THE PROJECT SCOPE CONSISTS OF WIDENING TAXIWAY "C" FROM 50 FT. TO 60 FT. USING A BITUMINOUS PAVEMENT SECTION PRIOR TO A BITUMINOUS OVERLAY AS DETAILED IN THESE CONSTRUCTION PLANS. ASSOCIATED KEY WORK ITEMS WILL INCLUDE EXCAVATION, SOIL PROCESSING, DRAINAGE, MEDIUM INTENSITY TAXIWAY LIGHTS AND UNDERGROUND CABLE, PAVEMENT MARKING, SEEDING, MULCHING, AND INCIDENTALS.

AIRPORT SECURITY NOTE

THE AIRPORT SPONSOR MAY PROVIDE ADDITIONAL GUIDANCE AND TRAINING FOR AIRPORT SECURITY MEASURES. AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE IN THE HAUL ROUTE AT THE END OF EACH WORKING DAY. ANY ACCESS GATES WILL BE CLOSED AT ALL TIMES WHEN NOT IN USE. IF CONTINUOUS HAULING IS REQUIRED, THEN THE GATE ACCESS WILL BE MONITORED TO PREVENT A BREACH FROM OUTSIDE, NON-CONSTRUCTION RELATED TRAFFIC.

UTILITY NOTE

THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO ACCOMPLISH THE ABOVE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ALL UNDERGROUND NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UNDERGROUND IMPROVEMENTS WILL BE LOCATED AT THE CONTRACTOR'S OWN EXPENSE PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI TRUCK WITH TRAILER IN THE UP POSITION.

HAUL ROUTE AND VEHICLE PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING AREA AS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS. THE PROPOSED PARKING AREA WILL BE APPROXIMATELY 250' X 250', POSITIONED AS SHOWN. THE HAUL ROUTE WILL BE LOCATED AS SHOWN. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL GRADE, FERTILIZE, SEED AND MULCH THE HAUL ROUTE AND PARKING AREA AS NEEDED TO RESTORE IT TO ITS ORIGINAL STATE. CONSTRUCTION, MAINTENANCE, AND ANY REQUIRED RESTORATION OF THE HAUL ROUTE AND PARKING AREA, INCLUDING LABOR, MATERIALS, EQUIPMENT, PERMIT FEES, AND OTHER INCIDENTAL ITEMS ASSOCIATED WITH ESTABLISHING THE TEMPORARY ACCESS TO IL ROUTE 104 WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR SHALL KEEP RUNWAYS 4-22 AND 18-36 OPEN AT ALL TIMES AND MAINTAIN CONTINUOUS ACCESS TO ALL HANGARS AND ADMINISTRATIVE AREAS.

ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE ALL RUNWAY CLOSURES.

TRENCHES AND/OR HOLES THAT REMAIN OPEN OVERNIGHT WILL REQUIRE BARRICADES AND/OR CONES TO INDICATE THEIR LOCATION AND PREVENT ACCIDENTAL ENTRANCE, WITHIN THE PROJECT AREA AND OUTSIDE ANY ACTIVE SAFETY AREAS.

BARRICADES AND TRAFFIC CONES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS DIRECTED BY THE AIRPORT MANAGER OR HIS DESIGNATED REPRESENTATIVE. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

BENCHMARK DATA				
NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	CP #61 STAINLESS STEEL ROD IN SLEEVE ±200' SE OF WINDSOCK POST	1,192,998.5670	2,009,012.5220	760.63
2	CB-24 CHISELED "□" NE CORNER CONCRETE AROUND DROP INLET	-	-	757.77
3	CB-25 CHISELED "□" NE CORNER CONCRETE AROUND DROP INLET	-	-	759.85
4	PK NAIL AT RUNWAY END 31	1,191,936.0250	2,010,470.4040	-
5	NGS "UN B2" BRASS DISK	1,192,474.3570	2,006,921.7740	-

CRITICAL POINT DATA				
POINT NO.	DESCRIPTION	LATITUDE	LONGITUDE	ELEVATION
1	AIRCRAFT OPERATION LINE @ C OF TXY C	39°56'05.87"	91°11'15.26"	767.20'

J.U.L.I.E. INFORMATION

COUNTY: ADAMS
 CITY: QUINCY
 TOWNSHIP: GLIMMER
 SECTION NO.: 34
 ADDRESS: QUINCY REGIONAL AIRPORT - BALDWIN FIELD
 1645 HIGHWAY 104
 QUINCY, ILLINOIS 62305
 217-885-3285

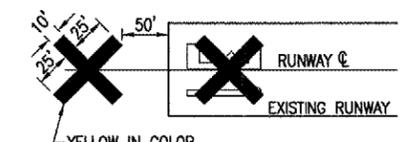
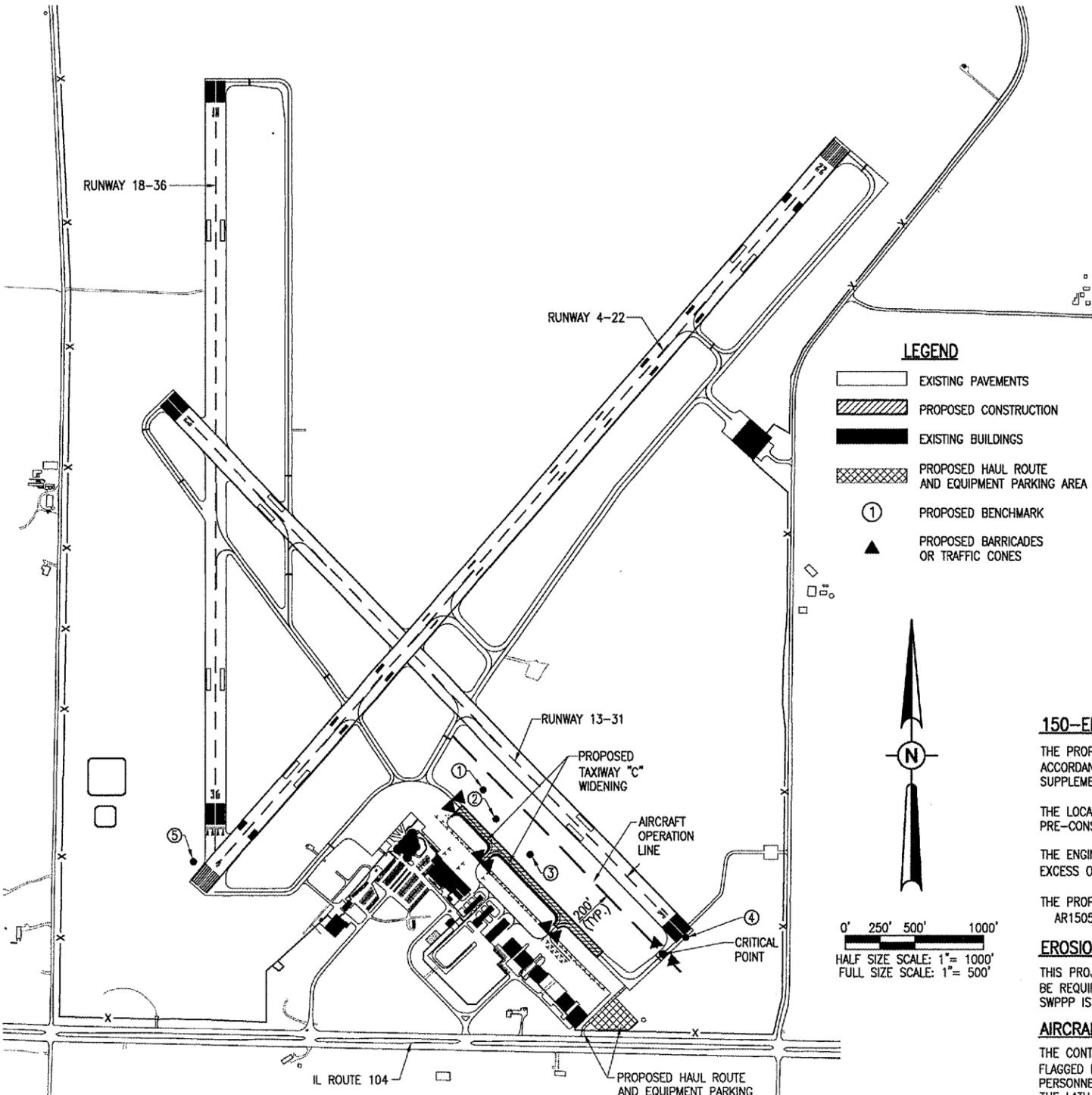
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PROPOSED SAFETY PLAN

GENERAL - THE QUINCY REGIONAL AIRPORT IS COMPRISED OF THREE RUNWAYS. RUNWAYS 4-22 AND 18-36 WILL REMAIN OPEN AT ALL TIMES. THE PROPOSED CONSTRUCTION MAY NECESSITATE CLOSING RUNWAY 13-31 IF THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE. PRIOR TO OPENING RUNWAY 13-31 FOR USE, THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER, ENSURE RUNWAY LIGHTING IS OPERABLE, AND REMOVE ALL TRAFFIC CONES, BARRICADES, AND CLOSURE CROSSES. IF NECESSARY, ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE AIRPORT MANAGER, OR HIS DESIGNATED REPRESENTATIVE, WILL ISSUE ALL NOTICES TO AIRMAN (NOTAM) RELATED TO OPENING AND CLOSING PAVEMENTS THROUGHOUT THE PROJECT.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (123.00 MHz) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE QUINCY REGIONAL AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.



NOTE:
 COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. ALL RUNWAY CLOSURES WILL BE IN ACCORDANCE WITH ALL FAA ADVISORY CIRCULARS THAT RELATE TO RUNWAY CLOSURES DURING CONSTRUCTION OPERATIONS. THE RUNWAY CLOSURE PROCEDURES SHALL BE REVIEWED BY THE AIRPORT MANAGER AND COORDINATED WITH THE RESIDENT ENGINEER. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER AND REVIEWED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED OVER THE NUMERALS AND SECURED IN A MANNER APPROVED BY THE AIRPORT MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

150-ENGINEER'S FIELD OFFICE NOTES

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 168 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS:
 AR150510 ENGINEER'S FIELD OFFICE _____ 1 L.S.

EROSION CONTROL

THIS PROJECT WILL DISTURB MORE THAN 1 ACRE OF LAND, THEREFORE A N.P.D.E.S. PERMIT WILL BE REQUIRED AND A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IMPLEMENTED. THE SWPPP IS SHOWN ON SHEET 6.

AIRCRAFT OPERATION LINE

THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION AND WILL PLACE FLAGGED LATH EVERY 150' ALONG IT. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN A RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THE LATH LINE FOR RUNWAYS THROUGHOUT THE PROJECT.

BY	
REVISION	
DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
A.I.P. PROJ.: 3-17-0085-B1	
IL PROJ.: UN-3680	
Project No.	82407TXD_0100
Filename	R-003SEY.DWG
Scale	1" = 500'
Date	03/14/07
LAYOUT	JDW 03/13/07
DRAWN	ESC 03/14/07
REVIEWED	JDW 03/15/07
<p>Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Chicago, Illinois</p>	
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2	
PROPOSED SAFETY PLAN	
3	
3 of 29 sheets	

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EROSION CONTROL BLANKET NOTES

AN EROSION CONTROL MATERIAL (EXCELSIOR BLANKET) WILL BE INSTALLED AT THE LOCATIONS SHOWN ON THESE CONSTRUCTION PLANS AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR156531 EROSION CONTROL BLANKET

602-BITUMINOUS PRIME COAT NOTES

THE BITUMINOUS PRIME COAT (602) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR602 BITUMINOUS PRIME COAT AS STATED IN THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS PRIME COAT SHALL BE PLACED ON THE PROPOSED AGGREGATE BASE COURSE PRIOR TO THE PLACEMENT OF THE FIRST LIFT OF PROPOSED BITUMINOUS BASE COURSE IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE PROPOSED BITUMINOUS PRIME COAT WILL BE PAID FOR UNDER ITEM:
AR602510 BITUMINOUS PRIME COAT

603-BITUMINOUS TACK COAT NOTES

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 BITUMINOUS TACK COAT AS STATED IN THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED BETWEEN LIFTS OF PROPOSED BITUMINOUS BASE AND SURFACE COURSES AT THE APPLICATION RATES DETAILED ON THE PROPOSED TYPICAL SECTION PROVIDED IN THESE CONSTRUCTION PLANS AND IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM:
AR603510 BITUMINOUS TACK COAT

EXCAVATION NOTES

TOP SOIL IN THE PROPOSED WIDENING AREA SHALL BE STRIPPED AND STOCKPILED FOR FINISH GRADE. EARTH EXCAVATED FOR THE WIDENING SHALL BE USED TO CONSTRUCT THE NEW SHOULDER IN ACCORDANCE WITH THE CROSS SECTIONS. ANY EXCESS MATERIAL WILL BE HAULLED TO A LOCATION ON AIRPORT PROPERTY DESIGNATED BY THE AIRPORT DIRECTOR OR HIS REPRESENTATIVE.

UNDERDRAIN AND INSPECTION HOLE REMOVAL

NOTES PERTAINING TO THE REMOVAL OF EXISTING UNDERDRAIN AND INSPECTION HOLES ARE LOCATED ON THE PROPOSED DRAINAGE PLAN PROVIDED IN THESE CONSTRUCTION PLANS.

AR401611 BITUMINOUS SURFACE COURSE - METHOD 1

THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR401001 BITUMINOUS SURFACE COURSE-METHOD 1 AS STATED IN THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING A BITUMINOUS OVERLAY OVER EXISTING AND WIDENED TAXIWAY AS DEPICTED IN THE TYPICAL SECTION PROVIDED IN THESE PLANS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY CONTROL IN THE PRODUCTION AND CONSTRUCTION OF THE BITUMINOUS SURFACE COURSE.

PRIOR TO STARTING THE BITUMINOUS SURFACE COURSE OPERATION, THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A DETAILED OUTLINE SHOWING AREAS AND ORDER OF PAVING WIDTHS OF PAVING LANES, AND REQUIRED OFFSETS FOR ELECTRONIC GRADE.

THE PROPOSED BITUMINOUS SURFACE COURSE WILL BE DESIGNED TO A MARSHALL DESIGN FOR AIRCRAFT GREATER THAN 60,000 POUNDS.

AR401655 BUTT JOINT CONSTRUCTION NOTES

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 BUTT JOINT CONSTRUCTION AS STATED IN THE SUPPLEMENTAL SPECIFICATION AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM SHALL CONSIST OF REMOVING THE EXISTING BITUMINOUS PAVEMENT SURFACE BY MILLING IN ORDER TO CONSTRUCT A BUTT JOINT FOR THE TRANSITION OF THE PROPOSED BITUMINOUS OVERLAY AT THE LOCATIONS AS SHOWN ON THE PLANS.

THE PAVEMENT SURFACE WILL BE MILLED TO ACCOMMODATE A MINIMUM DEPTH OF 1.5" OF BITUMINOUS SURFACE AT THE BUTT JOINT AND WILL TAPER. THE BUTT JOINT AT THE PCC END WILL MATCH THE GRADE OF THE PCC PAVEMENT. THE PLANNER MUST BE CAPABLE OF MILLING THE SURFACE TO THE DESIRED ELEVATIONS AND GRADES TO ACCOMMODATE THE PROPOSED OVERLAY.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

IF A TRUE VERTICAL FACE IS NOT ESTABLISHED ALONG THE BUTT JOINT FROM THE MILLING OPERATIONS, THE CONTRACTOR IS REQUIRED TO SAWCUT THE JOINT. THE SAWING, IF NECESSARY, ASSOCIATED WITH THE BUTT JOINT CONSTRUCTION SHALL BE CONSIDERED INCIDENTAL TO THE BUTT JOINT CONSTRUCTION AND NO ADDITIONAL COMPENSATION ALLOWED.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THE PAVEMENT MILLING WILL BE PAID FOR UNDER ITEM:
AR401655 BUTT JOINT CONSTRUCTION

IMPORTANT NOTE

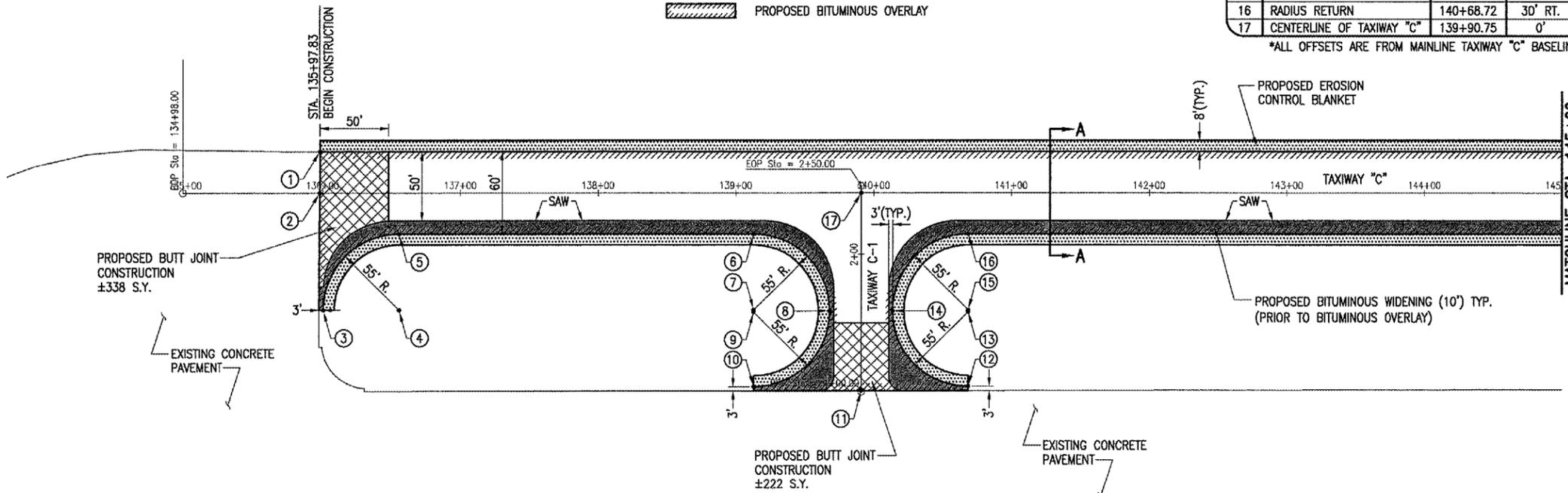
PHASE I REALIGNMENT AND WIDENING OF TAXIWAY "C" (STA. 150+58 TO STA. 158+98) HAD NOT BEEN COMPLETED AT THE TIME THESE CONSTRUCTION PLANS WERE PREPARED. THESE PLANS WERE DESIGNED UNDER THE EXPECTATION THAT PHASE I WOULD BE CONSTRUCTED AS DESIGNED. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFICATION AND ADJUSTMENT TO GRADE, CONFIGURATION OR QUANTITY TO MEET THE DESIGN INTENT INCLUDED IN THESE CONSTRUCTION PLANS.

Q1058

POINT DATA TABLE					
NO.	DESCRIPTION	STATION	OFFSET	NORTHING	EASTING
1	PAVEMENT EDGE	135+97.83	30' LT.	1,119,882.9336	2,008,859.8922
2	CENTERLINE OF TAXIWAY "C"	135+97.83	0'	1,192,862.2215	2,008,838.1887
3	RADIUS RETURN	136+00.29	85' RT.	1,192,801.3891	2,008,778.7706
4	CENTER OF RADIUS	136+55.29	85' RT.	1,192,761.8330	2,008,816.9847
5	RADIUS RETURN	136+55.29	30' RT.	1,192,800.0471	2,008,856.5408
6	RADIUS RETURN	139+12.72	30' RT.	1,192,614.8857	2,009,035.4201
7	CENTER OF RADIUS	139+12.72	85' RT.	1,192,576.6798	2,008,995.8561
8	RADIUS RETURN	139+67.73	85.66' RT.	1,192,536.6686	2,009,033.5935
9	CENTER OF RADIUS	139+12.72	86.39' RT.	1,192,575.7148	2,008,994.8585
10	RADIUS RETURN	139+12.72	141.39' RT.	1,192,537.5152	2,008,955.2884
11	PAVEMENT EDGE	139+90.75	144.29' RT.	1,192,479.4025	2,009,007.3960
12	RADIUS RETURN	140+68.72	141.19' RT.	1,192,425.4574	2,009,063.8200
13	CENTER OF RADIUS	140+68.72	86.19' RT.	1,192,463.6625	2,009,103.3847
14	RADIUS RETURN	140+13.73	85.63' RT.	1,192,503.8056	2,009,065.5753
15	CENTER OF RADIUS	140+68.72	85' RT.	1,192,464.4897	2,009,104.2399
16	RADIUS RETURN	140+68.72	30' RT.	1,192,502.8902	2,009,143.8091
17	CENTERLINE OF TAXIWAY "C"	139+90.75	0'	1,192,579.6261	2,009,111.1964

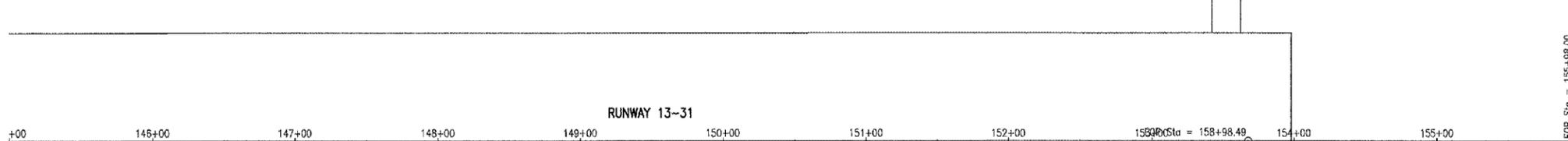
*ALL OFFSETS ARE FROM MAINLINE TAXIWAY "C" BASELINE.

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED TAXIWAY WIDENING (10' TYP.) - PRIOR TO BITUMINOUS OVERLAY
 - PROPOSED EROSION CONTROL BLANKET
 - PROPOSED BUTT JOINT CONSTRUCTION
 - PROPOSED BITUMINOUS OVERLAY



DATE	REVISION	BY			
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS					
A.I.P. PROJ.: 3-17-0085-B1 IL PROJ.: UIN-3690					
Job Project No. 82407TXD_0100 Filename R-121CON.DWG Plotted Date 03/14/07 Scale 1" = 50' Date 03/14/07					
LAYOUT	JDW	03/14/07	REVIEWED	JDW	03/16/07
DRAWN	ESC	03/14/07			
HANSON Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Chicago Nationwide					
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 PROPOSED CONSTRUCTION PLAN STA. 134+98 TO STA. 145+00					
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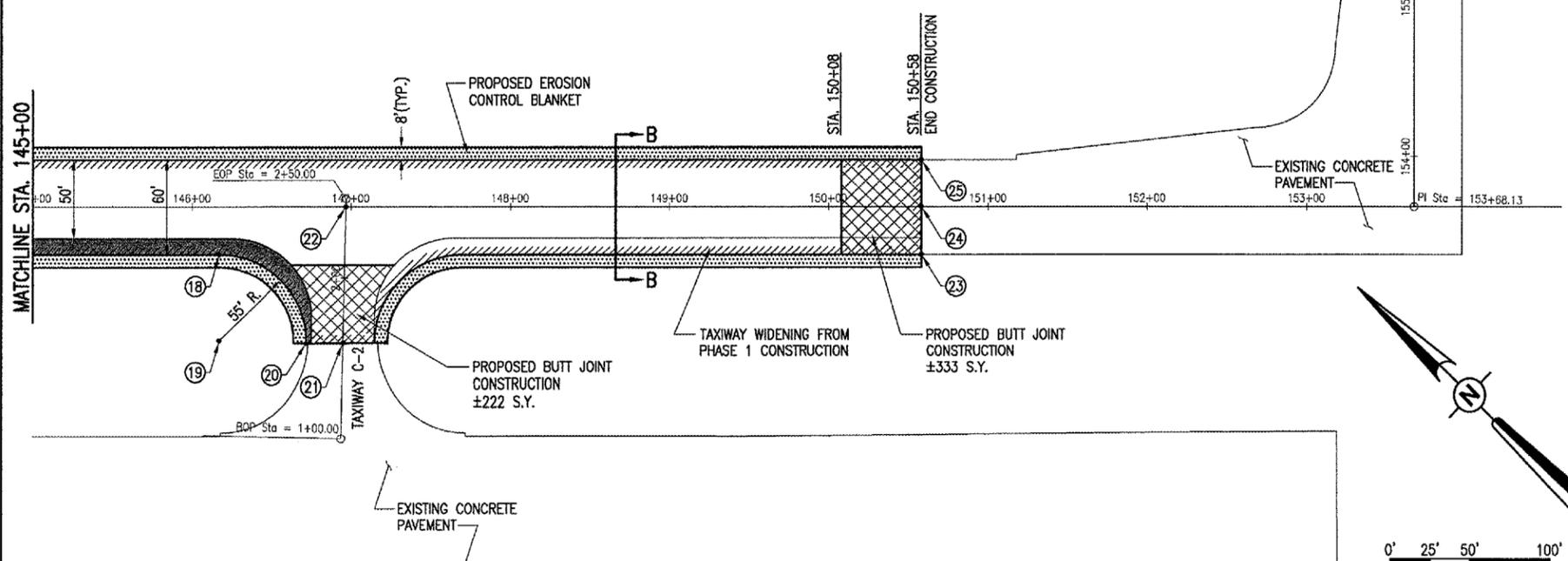
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POINT DATA TABLE					
NO.	DESCRIPTION	STATION	OFFSET	NORTHING	EASTING
18	RADIUS RETURN	146+16.89	30' RT.	1,192,108.4348	2,009,524.6884
19	CENTER OF RADIUS	146+16.89	85' RT.	1,192,070.2453	2,009,485.0960
20	RADIUS RETURN	146+71.88	86.77' RT.	1,192,029.4708	2,009,522.0401
21	PAVEMENT EDGE	146+94.89	86.55' RT.	1,192,013.0705	2,009,538.1852
22	CENTERLINE OF TAXIWAY "C"	146+96.74	0'	1,192,071.8767	2,009,601.7190
23	PAVEMENT EDGE	150+58.00	30' RT.	1,191,791.2169	2,009,831.1438
24	CENTERLINE OF TAXIWAY "C"	150+58.00	0'	1,191,812.0609	2,009,852.7199
25	PAVEMENT EDGE	150+58.00	30' LT.	1,191,832.9049	2,009,874.2960

*ALL OFFSETS ARE FROM MAINLINE TAXIWAY "C" BASELINE.

- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED TAXIWAY WIDENING (10' TYP.) - PRIOR TO BITUMINOUS OVERLAY
 - PROPOSED EROSION CONTROL BLANKET
 - PROPOSED BUTT JOINT CONSTRUCTION
 - PROPOSED BITUMINOUS OVERLAY



SOIL PROCESSING AND COMPACTION NOTES:

THE SUBGRADE COMPACTION REQUIREMENT FOR PROPOSED TAXIWAY "C" WIDENING WILL ADHERE TO ITEM AR152 FOR AIRCRAFT GREATER THAN 60,000 LBS.

LIME MODIFIED SOIL PROCESSING SHALL CONSIST OF CONSTRUCTING A 16 INCH COURSE OF A MIXTURE OF SOIL, LIME, AND WATER IN ACCORDANCE WITH THE RATES AND METHODS PROVIDED.

THE BY-PRODUCT LIME QUANTITY WAS CALCULATED AT 6% OF THE DRY SOIL WEIGHT (120 LBS/CF) MAXIMUM DENSITY. THE ACTUAL AMOUNT SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO CONSTRUCTION.

THE SUBGRADE SHALL BE CUT AND WITHIN GRADE TOLERANCE PRIOR TO LIME MODIFICATION. THE CONTRACTOR SHALL PROCESS THE SOIL TO 1.5' OUTSIDE THE PROPOSED PAVEMENT SURFACE ON ALL SIDES.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ACCOUNT FOR ANY "FLUFF" OR "SWELL" IN THE MODIFIED SUBGRADE.

ANY SWELL SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. NO ADDITIONAL PAYMENT WILL BE MADE FOR REMOVAL OF SWELL.

THE ENTIRE THICKNESS OF THE TREATED SUBGRADE SHALL BE COMPACTED TO A DENSITY OF NOT LESS THAN 95% OF THE MAXIMUM DRY DENSITY. THE MAXIMUM DRY DENSITY OF THE LIME-MODIFIED SOIL SHALL BE DETERMINED BY ASTM D1557 FOR AIRCRAFT 60,000 LBS OR MORE. DETERMINATION OF OPTIMUM MOISTURE CONTENT AND MAXIMUM DRY DENSITY OF LIME MODIFIED SOIL IS THE CONTRACTOR'S RESPONSIBILITY.

ONCE THE SPECIFIED DENSITY OF THE LIME-SOIL MIXTURE IS ACHIEVED, THE CONTRACTOR MAY START FINAL TRIMMING OPERATIONS AND PLACEMENT OF THE OVERLYING COURSE IF THE COMPACTED LIME MODIFIED LAYER IS NOT RUTTED OR DISTORTED BY THE EQUIPMENT.

THE CONTRACTOR SHALL KEEP THE LIME-SOIL MIXTURE MOIST CURED FOR A MINIMUM OF SEVEN (7) DAYS BY WATERING OR BY PLACEMENT OF THE OVERLYING COURSE.

AR209-CRUSHED AGGREGATE BASE COURSE NOTES

THE CRUSHED AGGREGATE BASE COURSE (209) SHALL BE PLACED IN ACCORDANCE WITH ITEM 209 CRUSHED AGGREGATE BASE COURSE OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE CRUSHED AGGREGATE BASE COURSE MATERIAL (CA-6) WILL BE USED TO CONSTRUCT A BASE COURSE FOR THE PROPOSED PAVEMENTS. THE CRUSHED AGGREGATE BASE COURSE MATERIAL WILL BE CONSTRUCTED TO THE DEPTHS SHOWN IN THE TYPICAL SECTIONS AND COMPACTED TO NOT LESS THAN 95 PERCENT MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D1557 (MODIFIED PROCTOR).

THE PROPOSED AGGREGATE FOR THE BASE COURSE MATERIAL SHALL MEET THE REQUIREMENTS OF (GRADATION "B") IN TABLE 1 OF THE SUPPLEMENTAL SPECIFICATIONS.

PROVISIONS SHALL BE MADE BY THE CONTRACTOR FOR FURNISHING WATER AT THE PLANT AND AT THE SITE OF THE WORK BY EQUIPMENT OF AMPLE CAPACITY AND OF SUCH DESIGN AS TO ASSURE UNIFORM MIXING AND APPLICATION.

THE CONTRACTOR WILL PROVIDE THE RESIDENT ENGINEER A PROCTOR CURVE SHOWING OPTIMUM MOISTURE AND MAXIMUM DRY DENSITY FOR THE SUPPLIED BASE COURSE MATERIAL AS DETERMINED BY ASTM D1557 (MODIFIED PROCTOR).

THE COMPACTION CONTROL TEST TO BE USED SHALL BE FOR AIRCRAFT GREATER THAN 60,000 LBS.

IF AT THE TIME THE AGGREGATES ARE WEIGHED THEY CONTAIN MORE THAN SIX (6) PERCENT OF ABSORBED AND FREE MOISTURE BY WEIGHT, A DEDUCTION FOR THE MOISTURE IN EXCESS OF THIS AMOUNT SHALL BE MADE IN DETERMINING THE PAY QUANTITY.

DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

IL PROJ.: UIN-3690
A.I.P. PROJ.: 3-17-00B5-B1

FILE Project No. 82407TXID_0100	DATE 03/14/07
Drawn R-121 CON.DWG	DATE 03/14/07
Scale 1" = 50'	DATE 03/14/07
Date 03/14/07	DATE 03/14/07
LAYOUT JDW	DATE 03/14/07
DRAWN ESC	DATE 03/14/07
REVIEWED JDW	DATE 03/16/07

HANSON

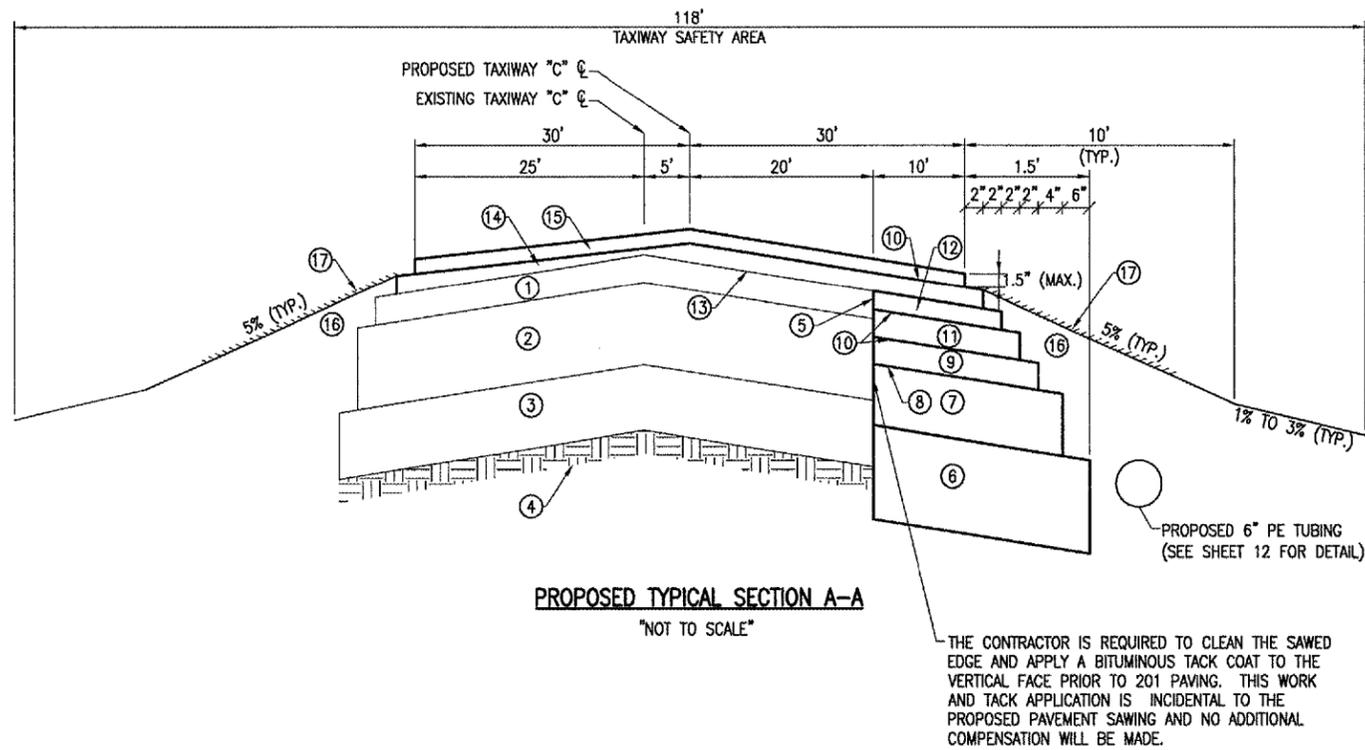
Hanson Professional Services Inc.
1525 South Sixth Street
Springfield, Illinois 62705-2886
Chicago Nationwide

REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2

PROPOSED CONSTRUCTION PLAN

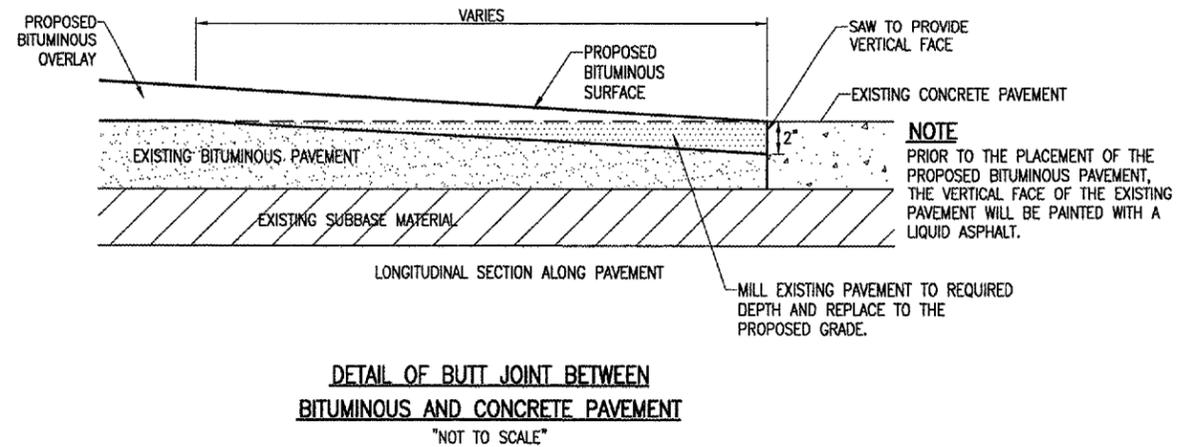
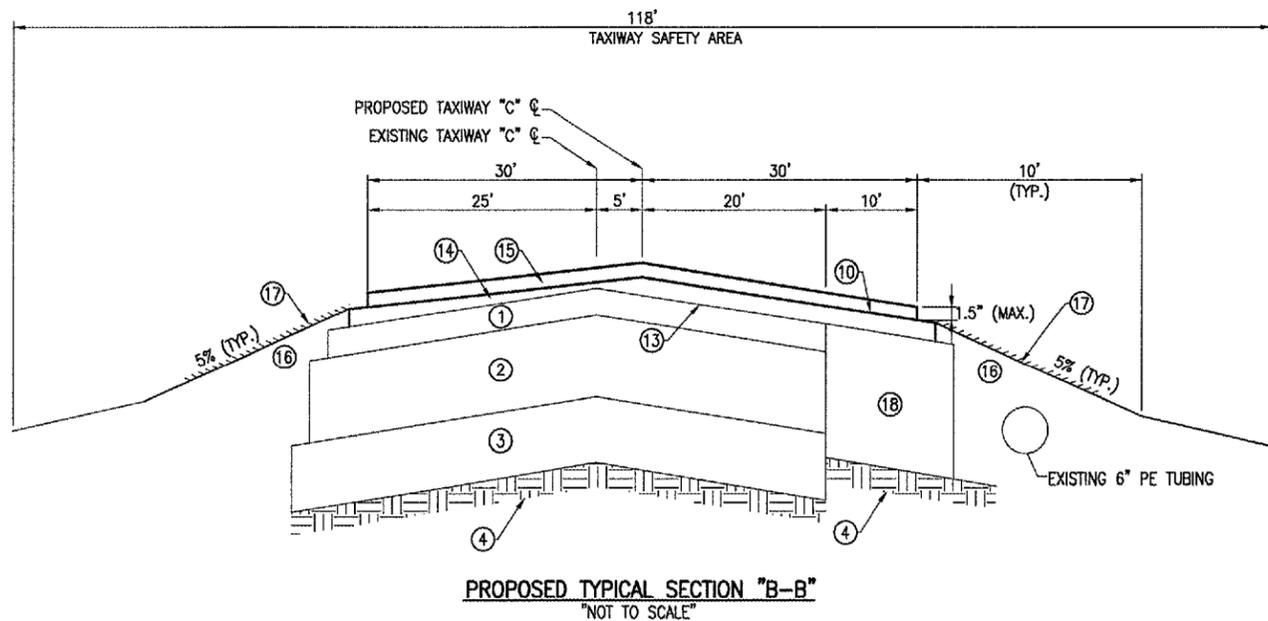
STA. 145+00 TO STA. 153+68.13

MAY 09, 2007 1:59 PM MLH
L:\AIRPORTS\QUINCY\82407TXID\AIRPORT\SHETS\R-121.CON.DWG - 145+00 TO 151+00



LEGEND FOR TYPICAL SECTIONS A-A, B-B

- ① EXISTING 401 BITUMINOUS SURFACE COURSE (3" AVG. DEPTH)
- ② EXISTING 501 PCC PAVEMENT (9" DEPTH)
- ③ EXISTING 209 CRUSHED AGGREGATE BASE COURSE (7" DEPTH)
- ④ EXISTING SUBGRADE
- ⑤ PROPOSED PAVEMENT SAWING (12" MIN. DEPTH)
- ⑥ PROPOSED 155 LIME MODIFIED SUBGRADE (16" DEPTH)
- ⑦ PROPOSED 209 CRUSHED AGGREGATE BASE COURSE (10" DEPTH)
- ⑧ PROPOSED 602 BITUMINOUS PRIME COAT (0.35 GAL./S.Y.)
- ⑨ PROPOSED 201 BITUMINOUS BASE COURSE WIDENING (FIRST LIFT, 3" DEPTH, COMPACTED)
- ⑩ PROPOSED 603 BITUMINOUS TACK COAT (0.05 GAL./S.Y.)
- ⑪ PROPOSED 201 BITUMINOUS BASE COURSE WIDENING (SECOND LIFT, 3" DEPTH, COMPACTED)
- ⑫ PROPOSED 201 BITUMINOUS BASE COURSE WIDENING (THIRD LIFT, 2" DEPTH, COMPACTED)
- ⑬ PROPOSED 603 BITUMINOUS TACK COAT (0.10 GAL./S.Y.)
- ⑭ PROPOSED 401 BITUMINOUS SURFACE COURSE, LEVELING (3" DEPTH, COMPACTED)
- ⑮ PROPOSED 401 BITUMINOUS SURFACE COURSE (2" DEPTH, COMPACTED)
- ⑯ PROPOSED SHOULDER ADJUSTMENT (INCIDENTAL TO ITEM 152 UNCLASSIFIED EXCAVATION)
- ⑰ PROPOSED EROSION CONTROL BLANKET (8' WIDTH)
- ⑱ EXISTING BITUMINOUS WIDENING FROM PHASE 1



MAY 09, 2007 2:00 PM MLH
I:\AIRPORTS\QUINCY\82407TXID\AIRPORT SHEETS\R-501TYP.DWG - DETAILS

BY		REVISION		DATE	
<p>QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS</p> <p style="font-size: small;">IL PROJ.: UIN-3680 A.I.P. PROJ.: 3-17-0085-B1</p>					
Heli Project No. 82407TXID_0100 Name R-501TYP.DWG Scale NOT TO SCALE Date 03/14/07	LAYOUT MLH 03/14/07 DRAWN MLH 03/19/07 REVIEWED JGW 3/20/07	<p style="font-size: x-small;">Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62705-2886 On-site Nationwide</p>			
<p>REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2</p> <p style="font-size: small;">TYPICAL SECTIONS AND DETAILS</p>					
<p style="font-size: 2em; font-weight: bold;">6</p> <p style="font-size: x-small;">6 of 28 sheets</p>					

BY	
REVISION	
DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
A.I.P. PROJ.: 3-17-0085-B1 I.L. PROJ.: UIN-3690	

EROSION CONTROL NOTES

ALL EROSION CONTROL MEASURES WILL BE PLACED PRIOR TO ANY DISRUPTION OF THE EXISTING SOD AREAS. THE CONTRACTOR WILL REMOVE THE INLET PROTECTION AND DITCH CHECKS ONCE A STAND OF GRASS IS ESTABLISHED AND ACCEPTED BY THE RESIDENT ENGINEER. REMOVAL OF THE EROSION CONTROL MEASURES WILL BE INCIDENTAL TO THEIR RESPECTIVE PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL PROPOSED EROSION CONTROL MEASURES SHALL BE COMPLETED AS DETAILED ON THIS EROSION CONTROL PLAN AND IN ACCORDANCE WITH THE SPECIFICATIONS.

EROSION CONTROL MEASURES ARE GOVERNED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY'S STANDARDS AND SPECIFICATIONS FOR SOIL EROSIONS AND SPECIFICATION FOR SOIL EROSIONS AND SEDIMENT CONTROL.

SILT FENCE WILL BE USED FOR INLET PROTECTION AS SHOWN ON THIS SHEET.

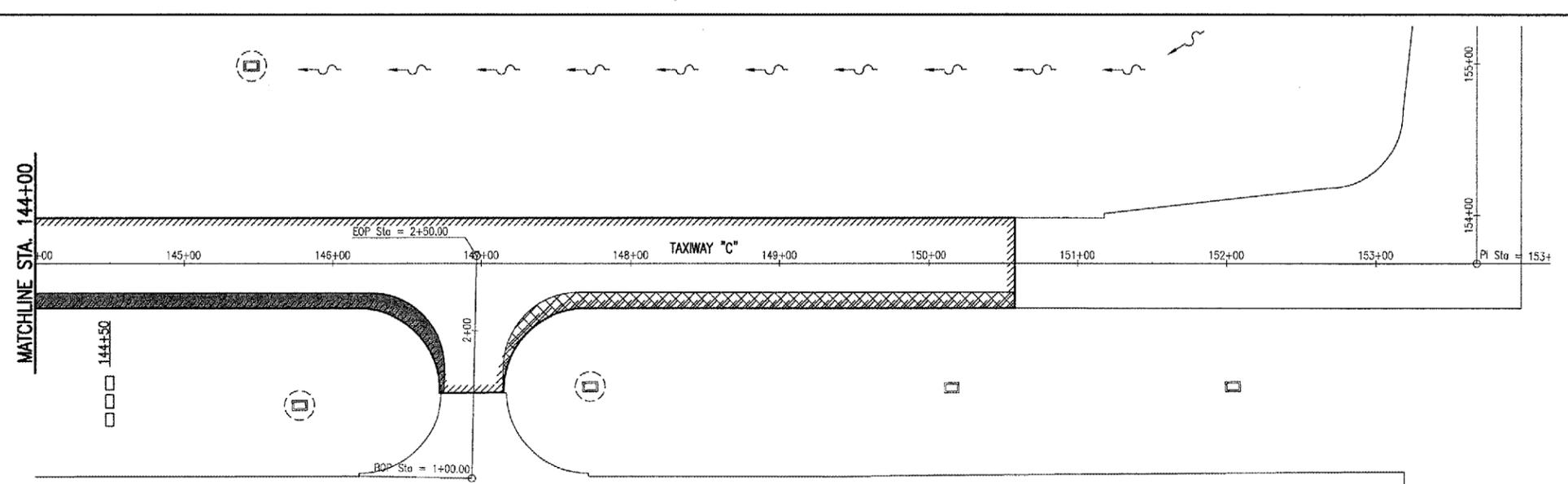
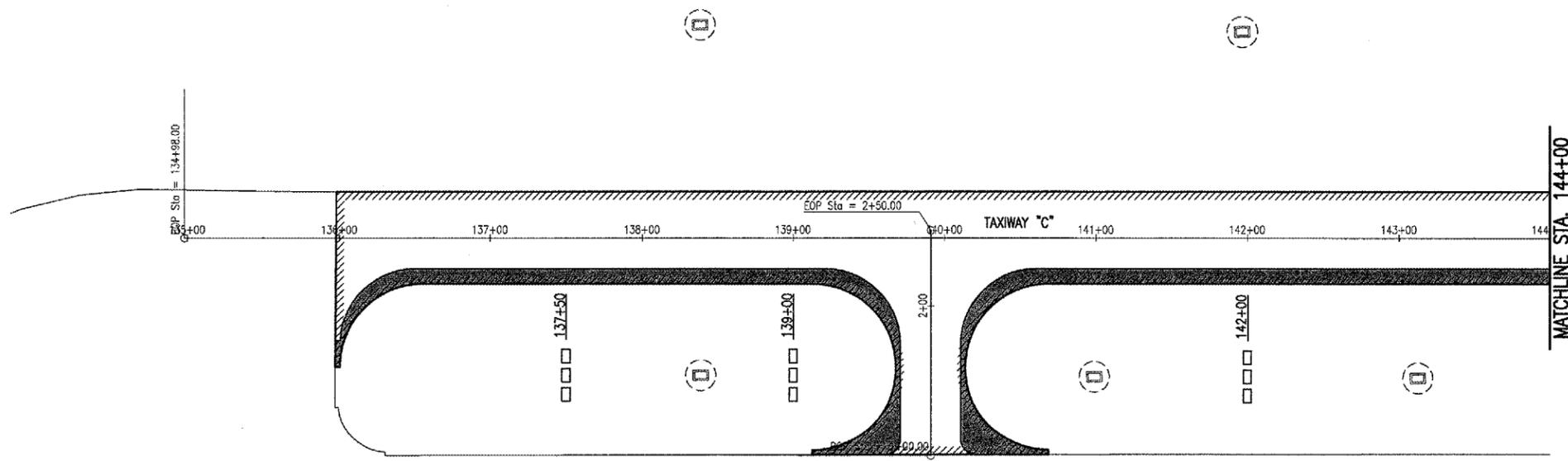
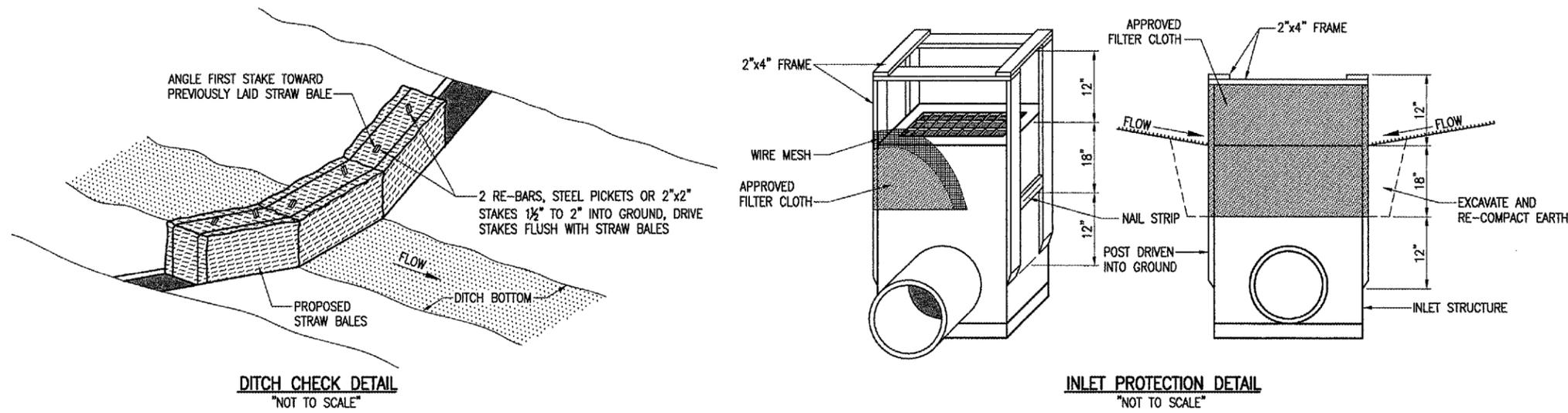
THE PROPOSED INLET PROTECTION WILL BE PLACED AT THE LOCATIONS SHOWN IN THESE CONSTRUCTION PLANS.

INSPECTION OF THE INLET PROTECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY, AS NEEDED. ANY REPAIR/REPLACEMENT MADE WILL BE CONSIDERED INCIDENTAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

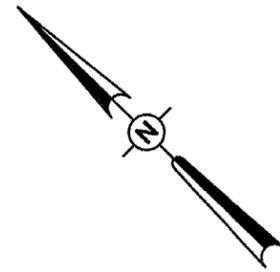
INLET PROTECTION IS TO INCLUDE ALL INCIDENTALS NECESSARY TO CONSTRUCT THESE ITEMS AND WILL BE PAID FOR UNDER ITEM: AR156520 INLET PROTECTION

THE PROPOSED DITCH CHECKS WILL BE CONSTRUCTED AS SHOWN ON THIS SHEET AND SHALL CONSIST OF A MINIMUM OF 3 BALES PER LOCATION. INSPECTION OF THE DITCH CHECKS SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY, AS NEEDED. ANY REPAIR/REPLACEMENT MADE WILL BE CONSIDERED INCIDENTAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE DITCH CHECKS WILL INCLUDE ALL INCIDENTALS NECESSARY TO COMPLETE THE ITEM AND WILL BE PAID FOR UNDER ITEM: AR156511 DITCH CHECK



- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED TAXIWAY WIDENING
 - PROPOSED BITUMINOUS OVERLAY
 - PHASE ONE WIDENING
 - PROPOSED INLET PROTECTION
 - EXISTING INLET
 - DITCH CHECK
 - SWALE FLOWLINE



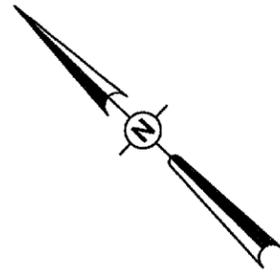
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 HALF SIZE SCALE: 1" = 100'
 FULL SIZE SCALE: 1" = 50'

HE Project No. 82407TXVD_0100	
Filename: R-181SMP.DWG	
Scale: 1" = 50'	
Date: 03/16/07	
LAYOUT: JDW	03/14/07
DRAWN: ESC	03/16/07
REVIEWED: JDW	03/17/07

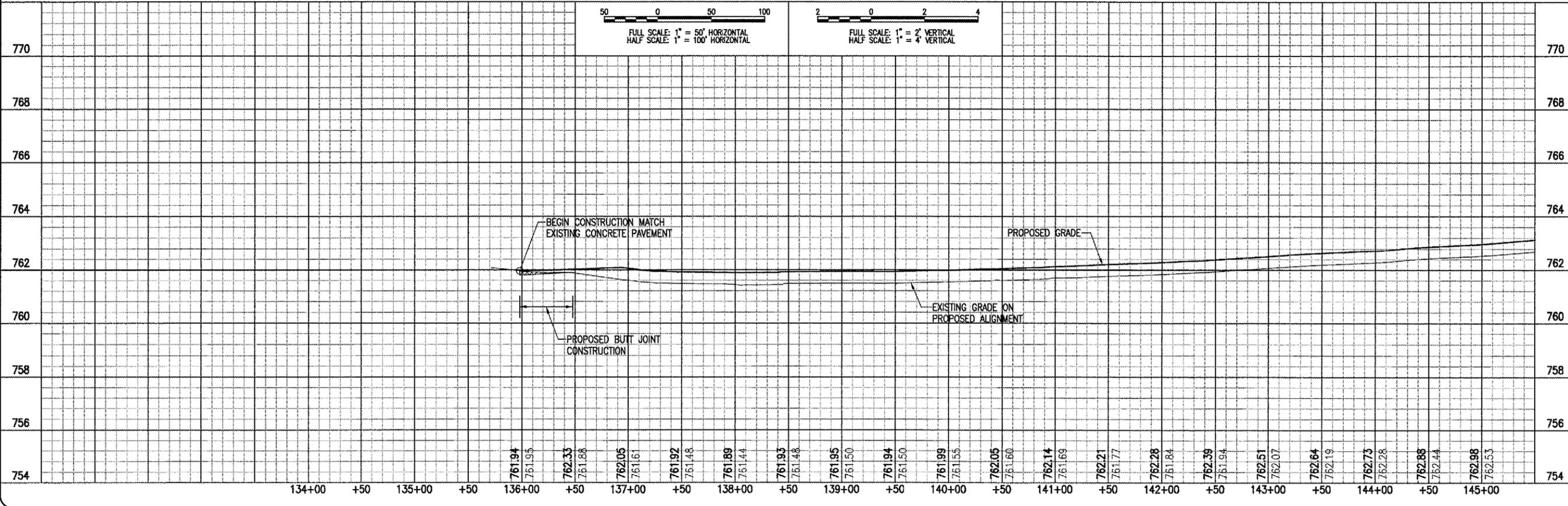
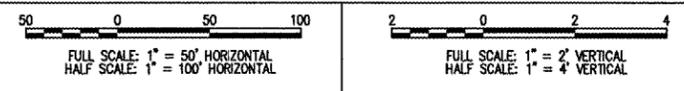
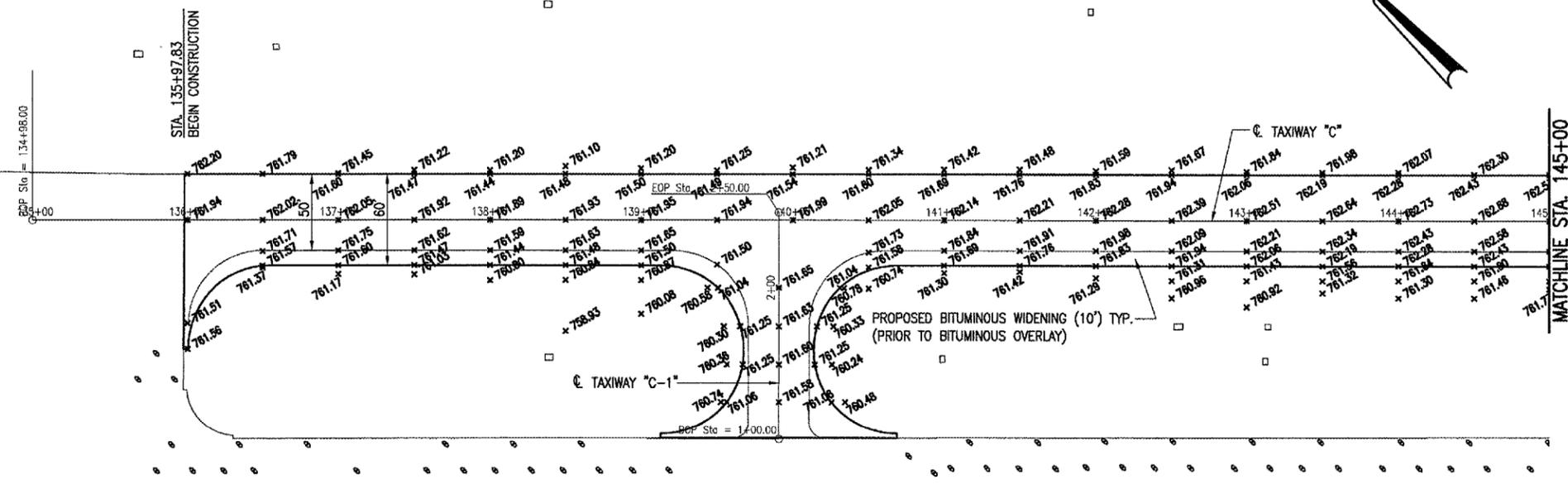
HANSON
 Hanson Professional Services Inc.
 1525 South 50th Street
 Springfield, Illinois 62703-2888
 Chicago, Illinois

REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2
 PROPOSED STORM WATER POLLUTION PREVENTION PLAN

MAY 09, 2007 2:04 PM MLH
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LEGEND
 [Symbol] EXISTING PAVEMENT
 [Symbol] PROPOSED IMPROVEMENTS
 * 761.32 PROPOSED SPOT ELEVATION

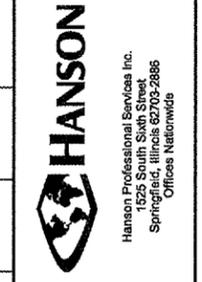


DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
 BALDWIN FIELD
 ADAMS COUNTY, ILLINOIS

IL PROJ.: UIN-3680 A.I.P. PROJ.: 3-17-0085-B1

HEL Project No. 82407X/D. 0100	DATE 03/21/07
Filename: R-701.PNP.DWG	DAK 03/19/07
Scale: AS NOTED	ESC 03/21/07
Date: 03/21/07	REVIEWED DAK 03/21/07

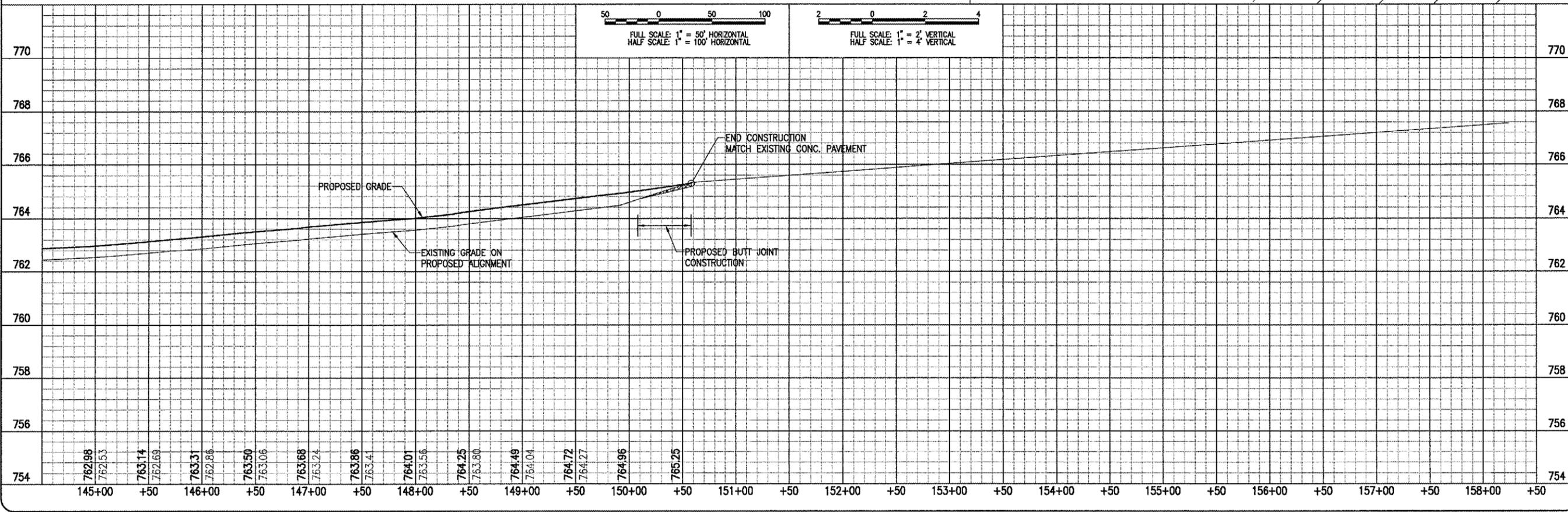
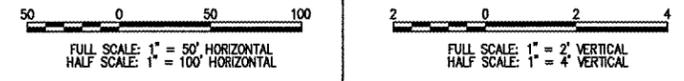
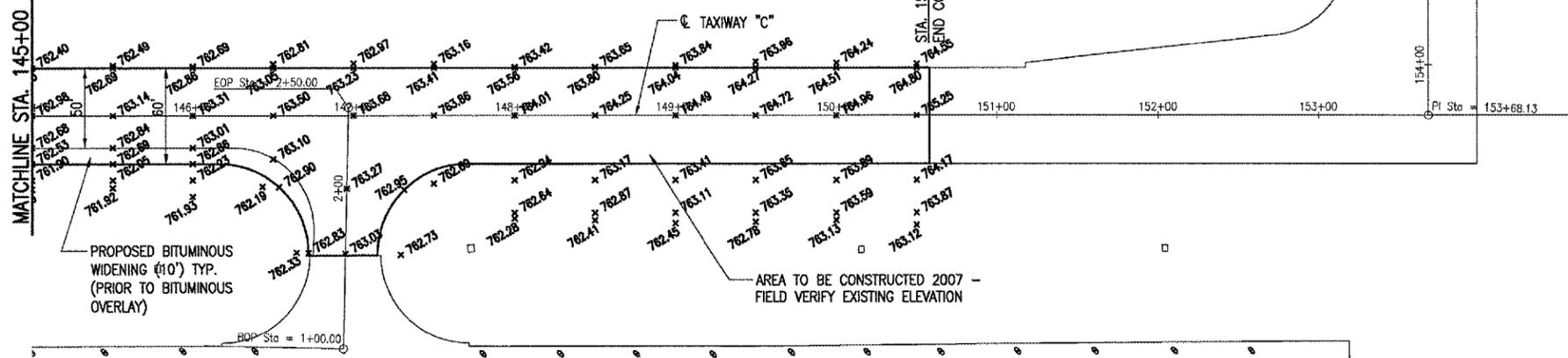
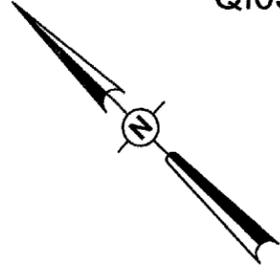


REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2
 PROPOSED PLAN AND PROFILE
 STA. 134+98 TO STA. 145+00

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Q1058

- LEGEND**
-  EXISTING PAVEMENT
 -  PROPOSED IMPROVEMENTS
 -  PROPOSED SPOT ELEVATION



DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS



ILL. PROJ. UN-3690 A.I.P. PROJ. 3-17-0085-B1

HEL Project No. 82407TXD.0100	FILENAME R-702PMP.DWG	DATE 03/21/07
SCALE AS NOTED	SCALE AS NOTED	DATE 03/21/07
DATE 03/21/07	DATE 03/21/07	DATE 03/21/07
LAYOUT DAK	MLH	MLH
DRAWN MLH	DAK	DAK
REVIEWED DAK	DAK	DAK



Hanson Professional Services Inc.
1525 South Sixth Street
Springfield, Illinois 62703-8886
Offices Nationwide

REALIGN, WIDEN
AND REHABILITATE
TAXIWAY "C" - PHASE 2
PROPOSED
PLAN AND PROFILE
STA. 145+00 TO STA. 153+68.13

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INSPECTION HOLE/CLEANOUT NOTE

TOP OF PROPOSED CLEANOUTS AND INSPECTION HOLES SHALL BE 1 INCH ABOVE FINISHED GRADE.

THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFICATION OF TOP ELEVATION PROVIDED.

REMOVE UNDERDRAIN AND INSPECTION HOLE NOTE

EXISTING UNDERDRAIN AND INSPECTION HOLES EXIST ADJACENT TO THE PAVEMENT PROPOSED TO BE WIDENED. THE UNDERDRAIN AND INSPECTION HOLES, IN THEIR ENTIRETY, WILL BE REMOVED AND DISPOSED OF OFF SITE BY THE CONTRACTOR IN A LEGAL MANNER. CARE SHOULD BE TAKEN NOT TO DAMAGE THE EXISTING UNDERDRAIN (TO BE REUSED) WHEN REMOVING THE UNDERDRAIN AND INSPECTION HOLES. ANY DAMAGE TO EXISTING UNDERDRAIN TO BE REUSED WILL BE REPLACED AT THE COST OF THE CONTRACTOR.

AT LOCATIONS WHERE FIELD VERIFICATION DETERMINES THE EXISTING UNDERDRAINS TO BE REMOVED ARE BELOW THE PROPOSED SOIL PROCESSING DEPTH, THEY SHALL BE PLUGGED BY THE CONTRACTOR AND ABANDONED IN PLACE AT THE DIRECTION OF THE RESIDENT ENGINEER. ALL UNDERDRAIN PLUGGING SHALL BE INCIDENTAL TO THE REMOVAL AND MEET THE APPROVAL OF THE RESIDENT ENGINEER. ONLY UNDERDRAIN PIPE REMOVED AND ACCEPTED BY THE RESIDENT ENGINEER WILL BE PAID PER LINEAR FOOT.

THIS ITEM WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER LINEAL FOOT OF UNDERDRAIN REMOVED. THE REMOVAL OF THE INSPECTION HOLES OR CLEAN OUT WILL BE CONSIDERED INCIDENTAL TO THE REMOVAL OF THE UNDERDRAIN AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THIS PRICE WILL CONSTITUTE FULL COMPENSATION FOR THE REMOVAL AND DISPOSAL OF THE UNDERDRAIN AND INSPECTION HOLES AND EXCAVATING AND BACKFILLING FOR THE REMOVAL IN ACCORDANCE TO ITEM 705.

THE REMOVAL OF THE UNDERDRAIN AND INSPECTION HOLES WILL BE PAID FOR UNDER ITEM:
AR705900 - REMOVE UNDERDRAIN

705-UNDERDRAIN NOTES:

THE PROPOSED UNDERDRAIN PIPE WILL BE CONSTRUCTED IN ACCORDANCE WITH ITEM 705 PIPE UNDERDRAINS FOR AIRPORTS AS STATED IN THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM OF WORK SHALL CONSIST OF FURNISHING AND INSTALLING 6" PERFORATED P.E. TUBING (WRAPPED) AND UNDERDRAIN INSPECTION HOLES AND CLEAN OUTS AT THE LOCATIONS AND TO THE GRADES SHOWN ON THE CONSTRUCTION PLANS.

PIPE DRAINS SHALL BE INSTALLED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. THE PIPE SHALL BE BEDDED IN THE UNDERLYING MATERIAL TO A DEPTH NOT LESS THAN 10 PERCENT OF THE EXTERNAL DIAMETER OF THE PIPE, AND WHERE TRENCHING IS REQUIRED, THE TRENCH SHALL HAVE A WIDTH OF NOT LESS 12 IN. THE BOTTOM OF THE TRENCH SHALL BE COMPACTED IN A MANNER MEETING THE APPROVAL OF THE RESIDENT ENGINEER.

JOINTS AND FITTINGS MAY BE ASSEMBLED WITHOUT GASKETS OR SOLVENT CEMENT IF THE JOINT IS TIGHT AND THE SPIGOT ENTERS THE SOCKET NOT LESS THAN 1/3 OF THE SOCKET DEPTH FOR SOLVENT CEMENT JOINTS AND FULL-DEPTH FOR ELASTOMERIC GASKET JOINTS.

NO PIPE SHALL BE PLACED IN THE TRENCH UNTIL IT AND THE PREPARED FOUNDATION HAVE BEEN APPROVED BY THE RESIDENT ENGINEER. THE PIPE SHALL BE LAID SO THAT THE FLOWLINE WILL BE AT THE GRADE SHOWN ON THE PLANS OR ESTABLISHED BY THE RESIDENT ENGINEER. THE PERMISSIBLE MINIMUM COVER OVER A PIPE SHALL BE 6 IN.

LAYING OF PIPES SHALL COMMENCE AT THE OUTLET END AND PROCEED TOWARD THE INLET END WITH THE PIPES TRUE TO LINE AND GRADE.

THE ENDS OF THE PIPE SHALL BE CAREFULLY CLEANED BEFORE THEY ARE PLACED, AND SHALL BE PLACED TO AVOID UNNECESSARY HANDLING ON THE FOUNDATION. AS EACH LENGTH OF PIPE IS LAID, THE ENDS OF THE PIPE SHALL BE PROTECTED TO PREVENT THE ENTRANCE OF ANY MATERIAL.

LONGITUDINAL LAPS SHALL BE PLACED AT THE SIDES AND SEPARATE SECTIONS OF PIPE SHALL BE JOINED WITH TIGHTLY-DRAWN, APPROVED CONNECTING BANDS.

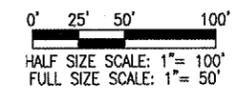
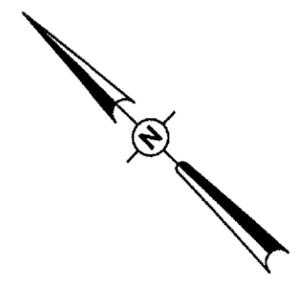
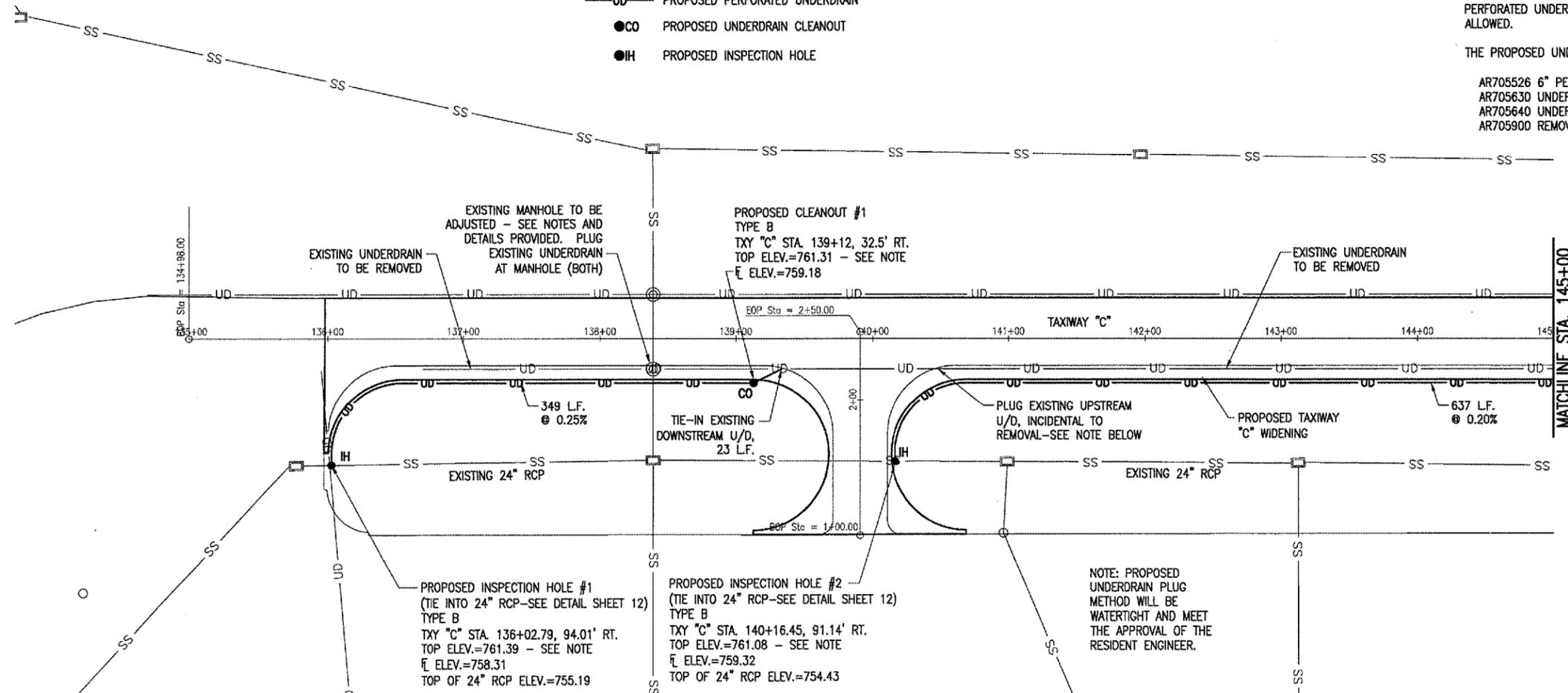
THE TRENCH SHALL BE BACKFILLED WITH CA-14 OR CA-16, IN ACCORDANCE WITH ITEM 705, AND CONSOLIDATED TO THE RESIDENT ENGINEER'S SATISFACTION. THE POROUS BACKFILL WILL BE CONSIDERED INCIDENTAL TO ITEM AR705526 - 6" PERFORATED UNDERDRAIN W/SOCK, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE PROPOSED UNDERDRAIN PIPE WILL BE PAID FOR UNDER ITEMS:

- AR705526 6" PERFORATED UNDERDRAIN W/SOCK
- AR705630 UNDERDRAIN INSPECTION HOLE
- AR705640 UNDERDRAIN CLEANOUT
- AR705900 REMOVE UNDERDRAIN

LEGEND

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING INLET
- EXISTING MANHOLE
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING UNDERDRAIN CLEANOUT (TO BE REMOVED)
- PROPOSED PERFORATED UNDERDRAIN
- PROPOSED UNDERDRAIN CLEANOUT
- PROPOSED INSPECTION HOLE



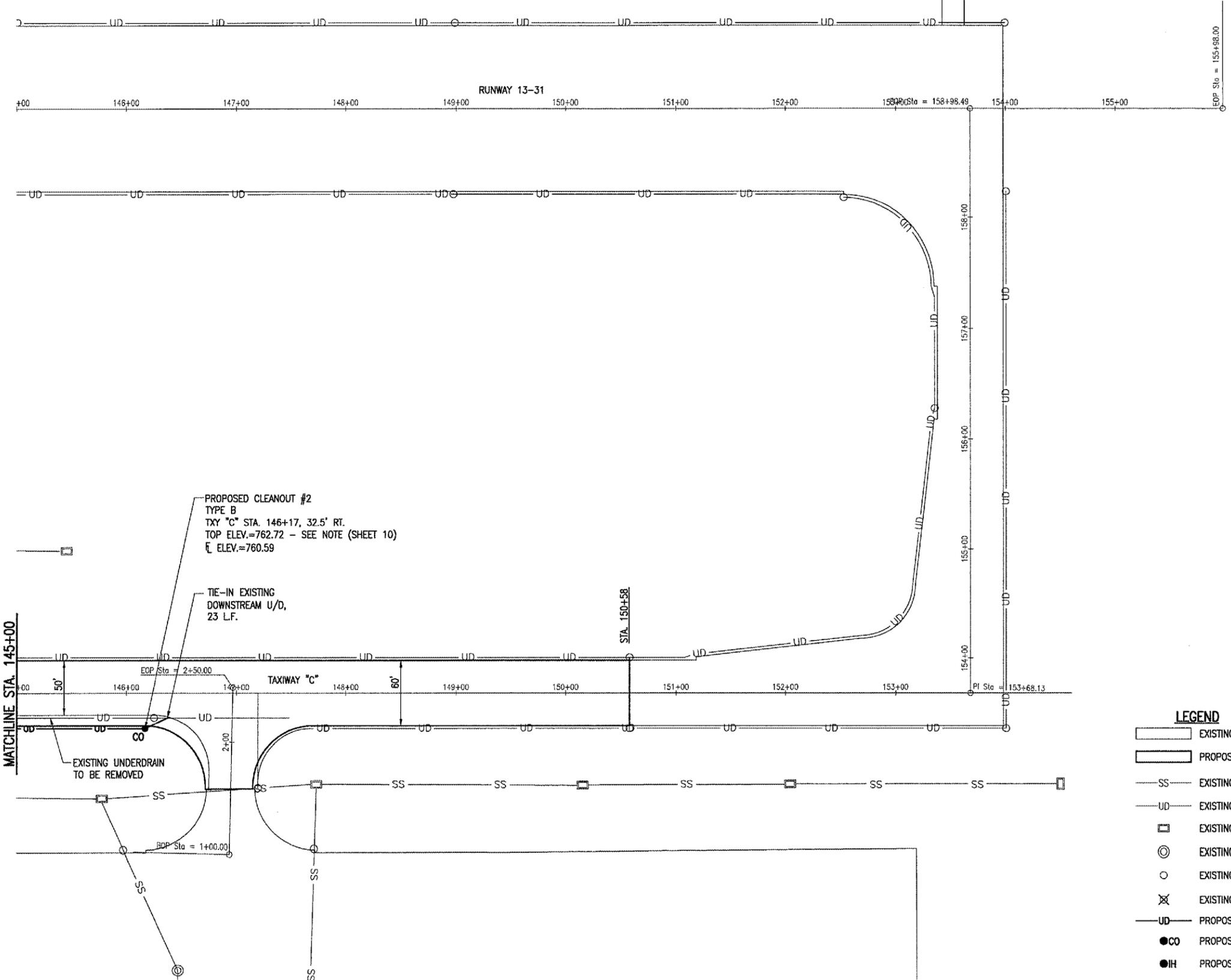
BY	
REVISION	
DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
A.I.P. PROJ.: 3-17-0085-B1	

Heli Project No. 82407TXVD_0100 Filename R-131DRN.DWG Scale 1" = 50' Date 03/19/07	LAYOUT MLH 03/15/07 DRAWN ESC 03/19/07 REVIEWED JDW 03/19/07
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REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2
 PROPOSED DRAINAGE PLAN
 STA. 134+98 TO STA. 145+00

MAY 09, 2007 2:07 PM MLH
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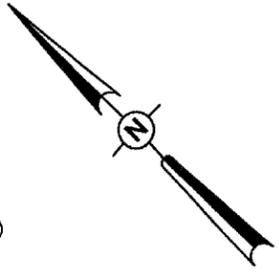
PROPOSED CLEANOUT #2
 TYPE B
 TXY "C" STA. 146+17, 32.5' RT.
 TOP ELEV.=762.72 - SEE NOTE (SHEET 10)
 ELEV.=760.59

TIE-IN EXISTING
 DOWNSTREAM U/D,
 23 L.F.

MATCHLINE STA. 145+00

LEGEND

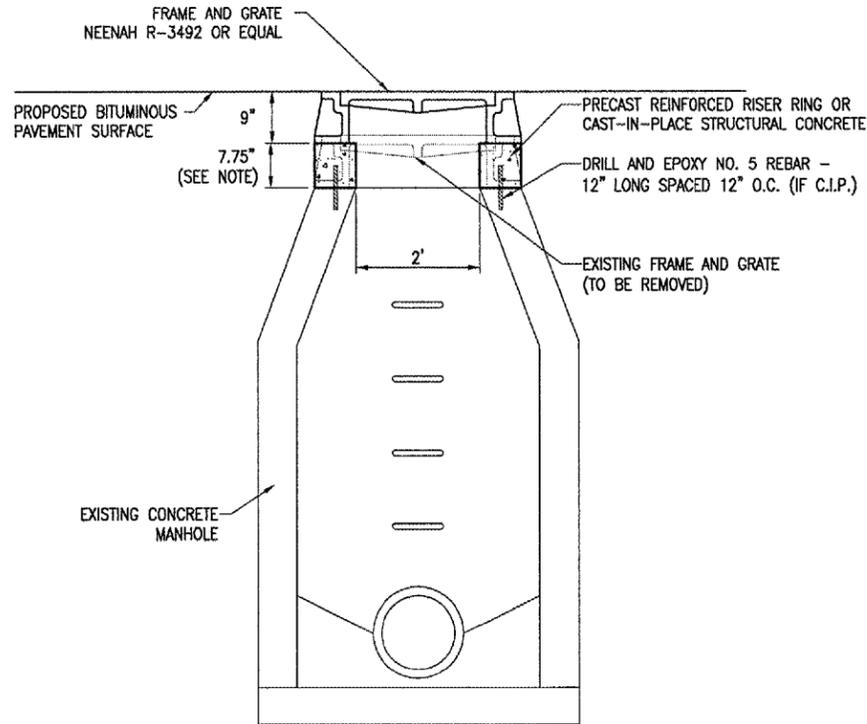
- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING INLET
- EXISTING MANHOLE
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING UNDERDRAIN CLEANOUT (TO BE REMOVED)
- PROPOSED PERFORATED UNDERDRAIN
- PROPOSED UNDERDRAIN CLEANOUT
- PROPOSED INSPECTION HOLE



0' 25' 50' 100'
 HALF SIZE SCALE: 1" = 100'
 FULL SIZE SCALE: 1" = 50'

MAY 09, 2007 2:08 PM MLH
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DATE	REVISION	BY					
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS							
I.L. PROJ.: UIN-3690 A.L.P. PROJ.: 3-17-0085-B1							
H.E. Project No. B2407TXD.0100 Drawing: R-13DRN.DWG Scale: 1" = 50' Date: 03/19/07		LAYOUT MLH 03/15/07 DRAWN ESC 03/19/07 REVIEWED JDW 03/19/07					
Hanson Professional Services Inc. 1625 South Sixth Street Springfield, Illinois 62703-2986 Chicago Nationwide							
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 PROPOSED DRAINAGE PLAN STA. 145+00 TO STA. 153+68.13							
11							
11 of 29 sheets							



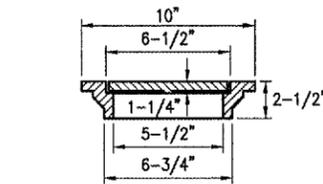
MANHOLE ADJUSTMENT DETAIL
"NOT TO SCALE"

MANHOLE ADJUSTMENT NOTES

FIELD VERIFY HEIGHT OF ADJUSTMENT PRIOR TO ORDERING MATERIALS. TOP OF FRAME AND GRATE SHALL BE FLUSH WITH FINISHED PAVEMENT ELEVATION AS SHOWN ON THE ATTACHED PLANS AND CROSS SECTIONS.

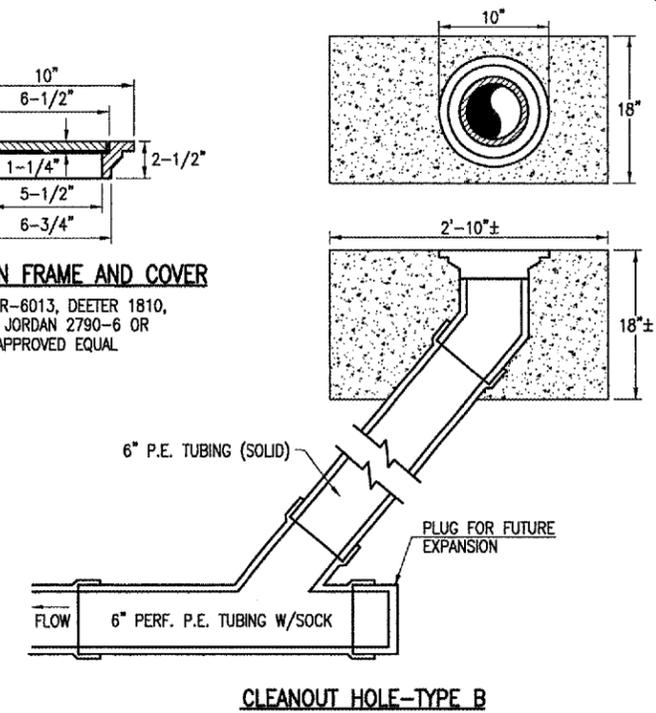
IF RISER RING IS CAST-IN-PLACE, THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE IN ACCORDANCE WITH ITEM 610.

THE CONTRACTOR IS RESPONSIBLE FOR PREPARING AND CURING CONCRETE CYLINDERS (1 SET REQUIRED FOR EACH 50 CY.) ONCE CURED, THE CYLINDERS SHALL BE TURNED OVER TO THE RESIDENT ENGINEER FOR TESTING.



CAST IRON FRAME AND COVER

NEENAH R-6013, DEETER 1810,
EAST JORDAN 2790-6 OR
APPROVED EQUAL



CLEANOUT HOLE-TYPE B

INSPECTION HOLE NOTES

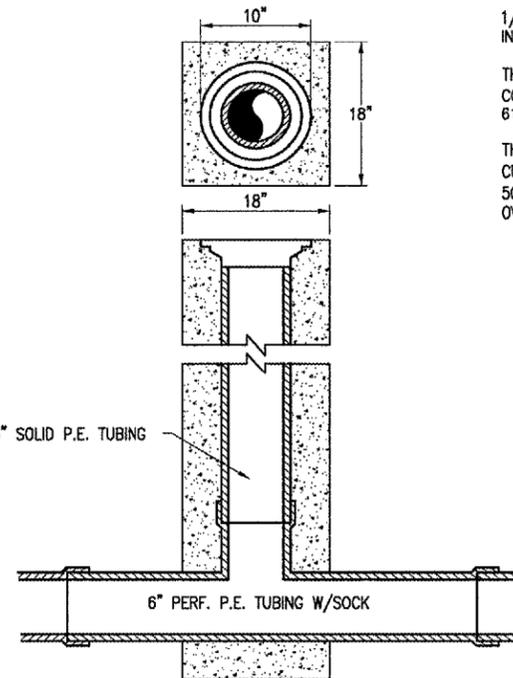
DIAMETER OF PIPE AS SPECIFIED.

TOP OF INSPECTION HOLES SHALL BE 1" ABOVE FINISH GROUND LINE AT LOCATION SHOWN ON PLANS.

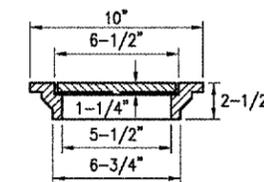
1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF INSPECTION HOLES.

THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE (NON- REINFORCED) IN ACCORDANCE WITH ITEM 610.

THE CONTRACTOR IS RESPONSIBLE FOR PREPARING AND CURING CONCRETE CYLINDERS (1 SET REQUIRED FOR EACH 50 CY.) ONCE CURED, THE CYLINDERS SHALL BE TURNED OVER TO THE RESIDENT ENGINEER FOR TESTING.

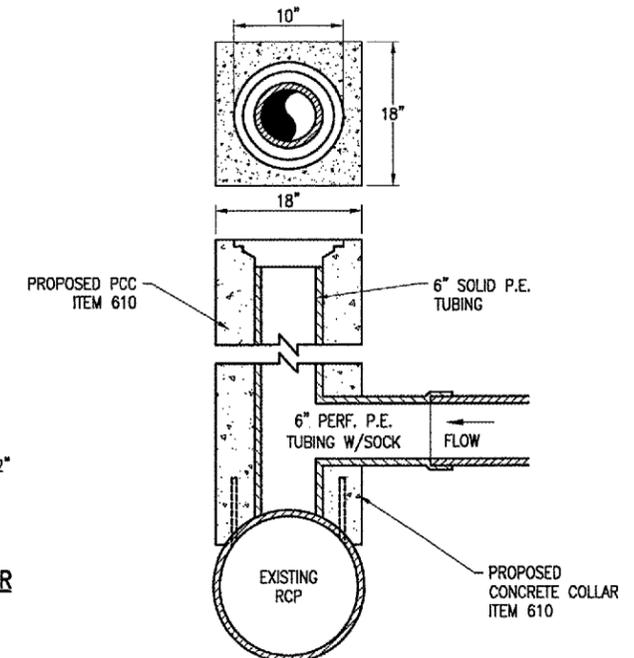


INSPECTION HOLE-TYPE A
"NOT TO SCALE"

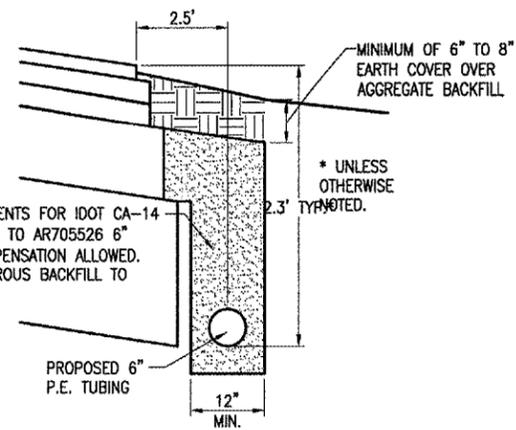


CAST IRON FRAME AND COVER

NEENAH R-6013, DEETER 1810,
EAST JORDAN 2790-6 OR
APPROVED EQUAL



INSPECTION HOLE DETAIL - TYPE B
"NOT TO SCALE"



UNDERDRAIN DETAIL
"NOT TO SCALE"

POROUS BACKFILL SHALL CONFORM TO THE REQUIREMENTS FOR IDOT CA-14 OR IDOT CA-16 AND WILL BE CONSIDERED INCIDENTAL TO AR705526 6" PERF. UNDERDRAIN W/SOCK AND NO ADDITIONAL COMPENSATION ALLOWED. CONTRACTOR SHALL PLACE AND CONSOLIDATE THE POROUS BACKFILL TO THE SATISFACTION OF THE RESIDENT ENGINEER.

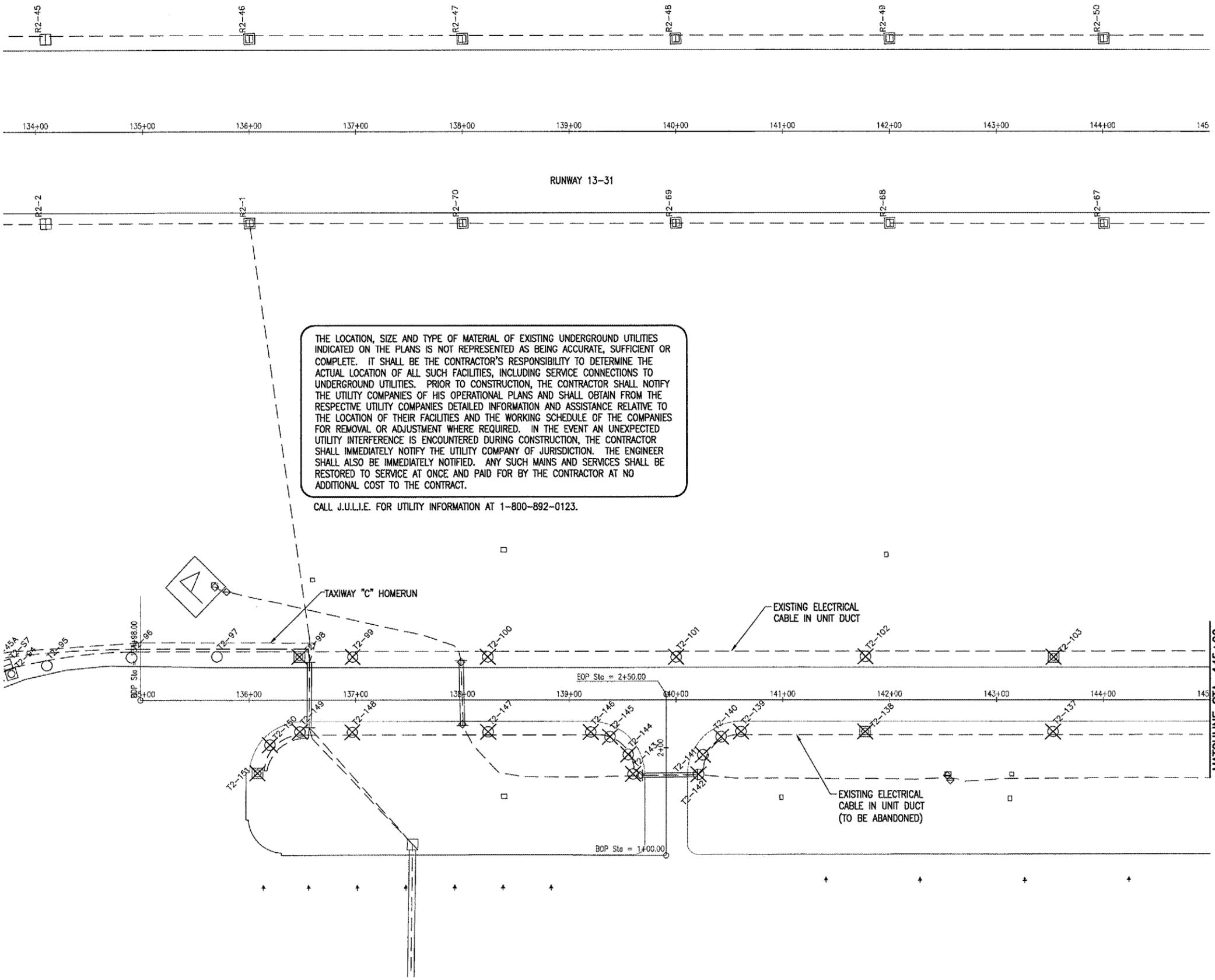
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E:\AIRPORTS\QUINCY\82407XX\AIRPORT_SHEETS\R-131DRN.DWG - DETAILS

BY	
REVISION	
DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
A.I.P. PROJ.: 3-17-0085-B1	
IL PROJ.: UIN-3690	

H&E Project No.	82407XX.DWG	0100
Revision	R-131DRN.DWG	
Scale	NOT TO SCALE	
Date	03/19/07	
LAYOUT	MLH	03/15/07
DRAWN	ESC	03/19/07
REVIEWED	JDW	03/19/07

HANSON
Hanson Professional Services Inc.
1225 South Sixth Street
Springfield, Illinois 62703-2886
Chicago National Office

REALIGN, WIDEN
AND REHABILITATE
TAXIWAY "C" - PHASE 2
PROPOSED
DRAINAGE
DETAILS



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

- LEGEND**
- EXISTING PAVEMENT
 - EXISTING ELECTRICAL DUCT
 - EXISTING ELECTRICAL CABLE (TO REMAIN IN PLACE)
 - EXISTING STAKE MOUNTED TAXIWAY LIGHT
 - EXISTING BASE MOUNTED RUNWAY LIGHT
 - EXISTING BASE MOUNTED THRESHOLD LIGHT
 - EXISTING TAXI GUIDANCE SIGN
 - EXISTING STAKE MOUNTED TAXIWAY LIGHT (TO BE REMOVED)
 - EXISTING BASE MOUNTED TAXIWAY LIGHT (TO BE REMOVED)
 - EXISTING WIND SOCK

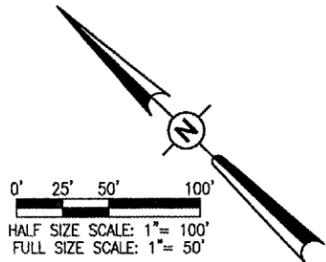
EXISTING ELECTRICAL INFORMATION NOTE
 THE EXISTING ELECTRICAL INFORMATION SHOWN WAS OBTAINED FROM RECORD DRAWINGS AND IS PROVIDED FOR REFERENCE. PLEASE REFER TO THE PROPOSED ELECTRICAL PLANS FOR IMPROVEMENTS REQUIRED IN THIS PROJECT.

CABLING CONFLICT NOTE
 IN AREAS WHERE THE PROPOSED CABLES ARE TO BE PLACED ADJACENT TO THE EXISTING CABLES TO BE ABANDONED, THE PROPOSED CABLES WILL BE PLACED AT 15' OFF THE PROPOSED EDGE OF PAVEMENT TO AVOID CONFLICT WITH ABANDONED CABLES.

EQUIPMENT REMOVAL NOTE
 THE CONTRACTOR SHALL REMOVE THE LIGHTS SHOWN TO BE REMOVED IN THEIR ENTIRETY, TO INCLUDE TRANSFORMERS, BASES, STAKES, AND ASSOCIATED MATERIAL. THE LIGHTS SHALL BE TURNED OVER TO THE AIRPORT. ALL REMAINING MATERIALS REMOVED WILL BE DISPOSED OF OFF THE AIRPORT IN A LEGAL MANNER. ALL HOLES REMAINING AFTER EACH PROPOSED REMOVAL WILL BE BACKFILLED IN ACCORDANCE TO ITEM 108, TO THE SATISFACTION OF THE RESIDENT ENGINEER, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE COST OF THE PROPOSED REMOVAL OF THE LIGHTS WILL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, AND DISPOSAL, AND WILL BE PAID FOR UNDER EACH RESPECTIVE REMOVAL PAY ITEM.

TEMPORARY CABLE NOTE
 THE CONTRACTOR SHALL PROVIDE TEMPORARY CABLE, AS NECESSARY, TO KEEP THE LIGHTS OPERABLE DURING CONSTRUCTION. THE COST OF THE TEMPORARY CABLE WILL INCLUDE ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THIS TASK, AND WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT, WITH NO ADDITIONAL COMPENSATION ALLOWED.

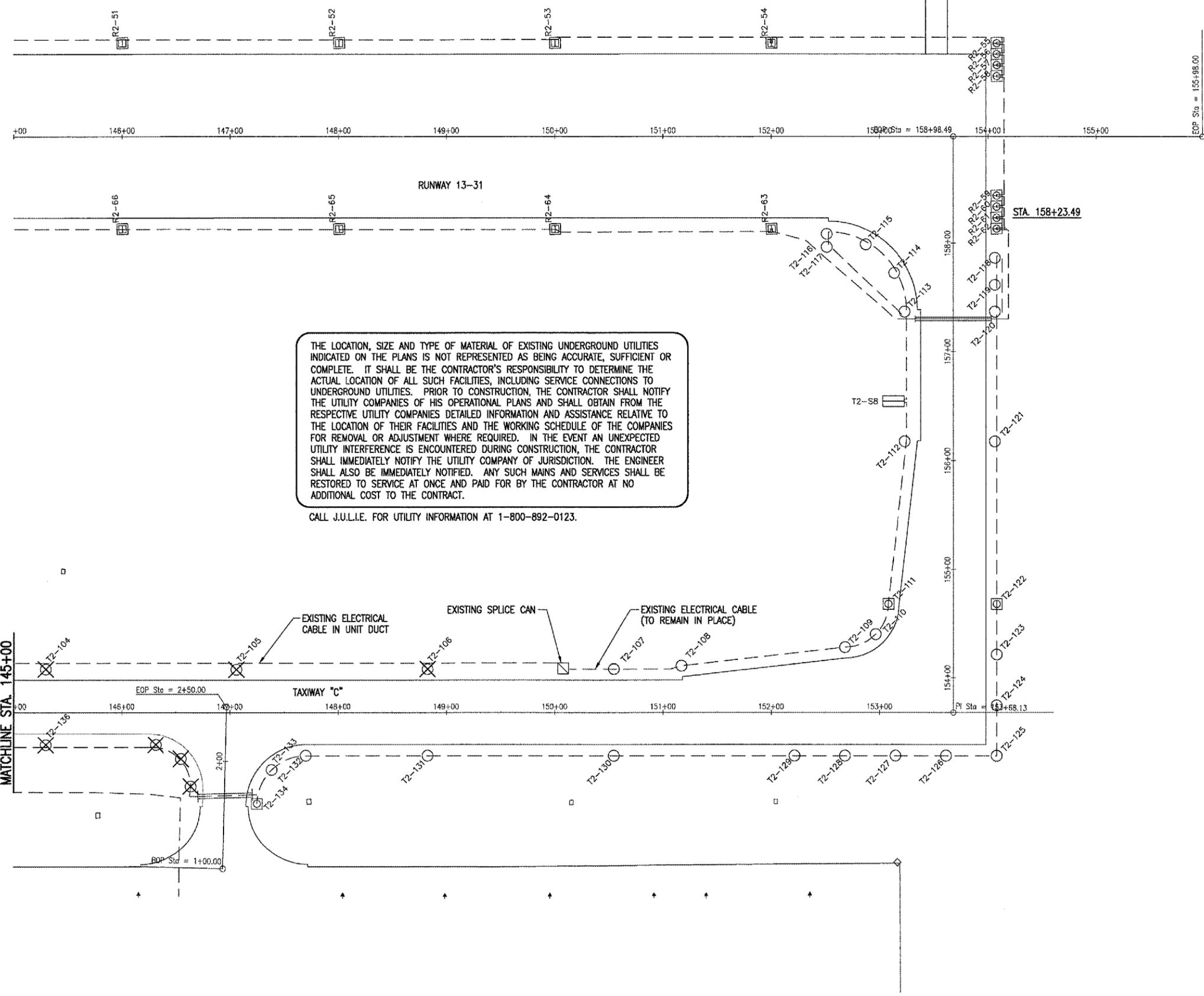
IMPORTANT NOTE
 PHASE I REALIGNMENT AND WIDENING OF TAXIWAY "C" (STA. 150+58 TO STA. 158+98) HAD NOT BEEN COMPLETED AT THE TIME THESE CONSTRUCTION PLANS WERE PREPARED. THESE PLANS WERE DESIGNED UNDER THE EXPECTATION THAT PHASE I WOULD BE CONSTRUCTED AS DESIGNED. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFICATION AND ADJUSTMENT TO GRADE, CONFIGURATION OR QUANTITY TO MEET THE DESIGN INTENT INCLUDED IN THESE CONSTRUCTION PLANS.



BY	
REVISION	
DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
ILL. PROJ.: UIN-3690 A.I.P. PROJ.: 3-17-0085-B1	
IEL Project No.: 82407XND_0100 Element: R-141ELEL.DWG Scale: 1" = 50' Date: 03/14/07	LAYOUT: DAK 03/14/07 DRAWN: MLH 03/14/07 REVIEWED: JDW 03/19/07
Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Chicago, Illinois	
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 EXISTING ELECTRICAL PLAN STA. 134+98 TO STA. 145+00	
13 13 of 29 sheets	

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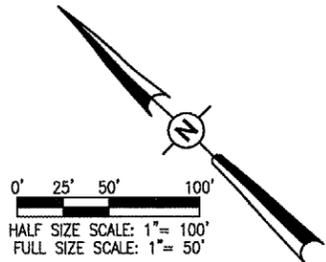
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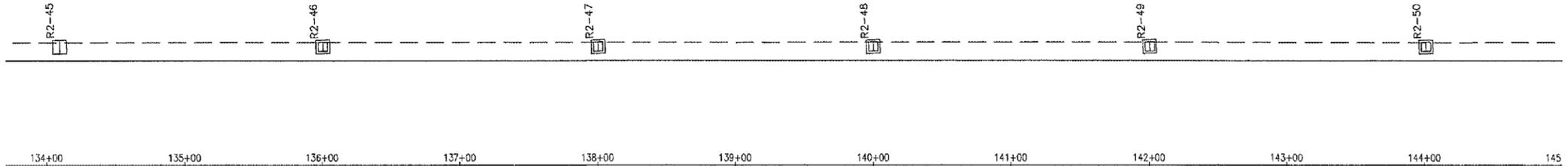
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

- LEGEND**
- EXISTING PAVEMENT
 - EXISTING ELECTRICAL DUCT
 - EXISTING ELECTRICAL CABLE (TO REMAIN IN PLACE)
 - EXISTING STAKE MOUNTED TAXIWAY LIGHT
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 - EXISTING BASE MOUNTED THRESHOLD LIGHT
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 - EXISTING STAKE MOUNTED TAXIWAY LIGHT (TO BE REMOVED)
 - EXISTING BASE MOUNTED TAXIWAY LIGHT (TO BE REMOVED)



	BY				
	REVISION	DATE			
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS					
IL PROJ.: UIN-3690 A.I.P. PROJ.: 3-17-0085-B1					
H&E Project No. 82407XYD_0100 Filename: R-141ELE.DWG Scale: 1" = 50' Date: 03/14/07		LAYOUT: DAK 03/14/07 DRAWN: MLH 03/14/07 REVIEWED: JDW 03/19/07			
Hanson Professional Services Inc. 1626 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide					
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 EXISTING ELECTRICAL PLAN STA. 145+00 TO STA. 153+68.13					
14					
14 of 29 sheets					

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RUNWAY 13-31

CONCRETE NOTE

CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE (NON- REINFORCED) IN ACCORDANCE WITH ITEM 610.
 THE CONTRACTOR IS RESPONSIBLE FOR PREPARING AND CURING CONCRETE CYLINDERS (1 SET REQUIRED FOR EACH 50 CY.) ONCE CURED, THE CYLINDERS SHALL BE TURNED OVER TO THE RESIDENT ENGINEER FOR TESTING.

NUMBERING TAG NOTE

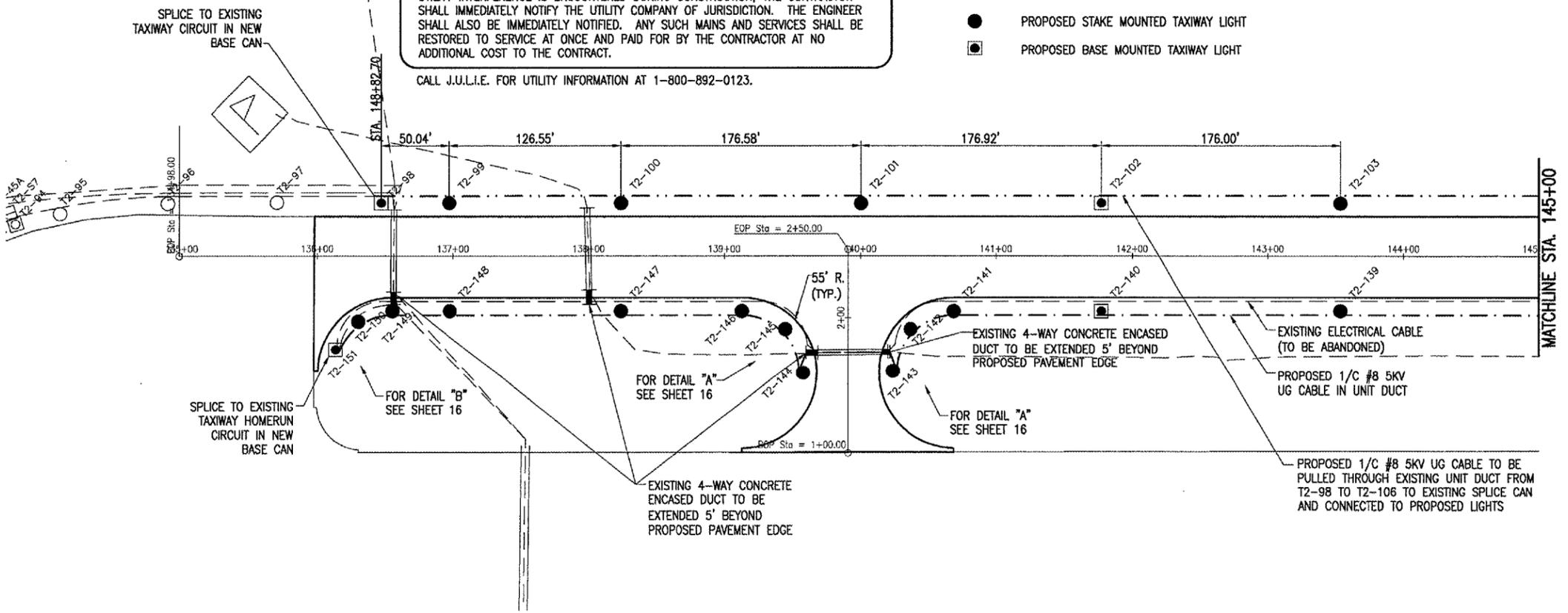
ALL OF THE PROPOSED LIGHTING WILL REQUIRE NUMBERING TAGS TO BE ATTACHED IN THE SEQUENCE SHOWN. THE CONTRACTOR WILL SUPPLY AND PLACE ALL NEW TAGS FOR THE PROPOSED LIGHTS. THE COST OF THE TAGS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

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LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING ELECTRICAL CABLE
- EXISTING ELECTRICAL DUCT
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING BASE MOUNTED THRESHOLD LIGHT
- PROPOSED 1/C, #8 5KV CABLE IN UNIT DUCT
- PROPOSED 1/C, #8 5KV CABLE IN EXISTING UNIT DUCT
- PROPOSED CONCRETE ENCASED DUCT
- PROPOSED STAKE MOUNTED TAXIWAY LIGHT
- PROPOSED BASE MOUNTED TAXIWAY LIGHT



ELECTRICAL NOTES

Q1058

CABLES:
 ALL PROPOSED CABLE WILL BE PLACED 12' FROM THE PAVEMENT EDGE UNLESS THEY CONFLICT WITH THE EXISTING CABLES TO BE ABANDONED, WHERE THEY WILL BE PLACED AT 15' OFF OF PROPOSED EDGE OF PAVEMENT.

ALL PROPOSED CABLE WILL BE PLACED 18" BELOW THE PROPOSED GRADE.

THE PROPOSED ELECTRICAL CABLE WILL BE NO. 8, 5000 V., 1/C, TYPE C UNDERGROUND CABLE.

PROPOSED TAXIWAY LIGHTS T2-98 THROUGH T2-106 SHALL BE INSTALLED IN THE SAME LOCATIONS AS THE EXISTING TAXIWAY EDGE LIGHTS (T2-98 THROUGH T2-106).

THE EXISTING UNIT DUCT WILL BE REUSED FROM TAXIWAY LIGHT T2-98 THROUGH T2-106 TO THE EXISTING SPLICE CAN AT STA. 150+08. NEW 1 CONDUCTOR, NO. 8 5KV UNDERGROUND CABLE SHALL BE PULLED THROUGH, CONNECTED TO THE PROPOSED LIGHTS IN ACCORDANCE WITH THE DETAILS PROVIDED AND THE SPECIFICATIONS. NEW CABLE PULLED THROUGH EXISTING UNIT DUCT SHALL BE PAID UNDER ITEM AR108108 1/C #8 5KV UG CABLE, PER LINEAR FOOT.

IN AREAS WHERE THERE IS A CONGESTION OF CABLES OR WHERE THE PROPOSED CABLE CROSSES AN EXISTING CABLE, THE PROPOSED CABLE WILL BE HAND DUG INTO PLACE. AT ALL OTHER LOCATIONS, THE PROPOSED CABLE IN UNIT DUCT MAY BE EITHER TRENCHED OR PLOWED INTO PLACE. THE TRENCHING AND/OR PLOWING WILL BE CONSIDERED INCIDENTAL TO THE PROPOSED CABLE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL CABLE SHOWN BY A (---) DASHED LINE ARE EXISTING CIRCUITS THAT ARE NOT PART OF THIS CONTRACT. THE LOCATION OF THESE CABLES ARE APPROXIMATE AND FOR INFORMATIONAL PURPOSES ONLY. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND IDENTIFY THESE EXISTING CIRCUITS PRIOR TO THE INSTALLATION OF THE PROPOSED CABLE. ANY DAMAGE TO THE EXISTING CIRCUITS SHALL BE REPAIRED IMMEDIATELY TO THE SATISFACTION OF THE RESIDENT ENGINEER AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

LIGHTS:
 ALL PROPOSED TAXIWAY LIGHTS WILL BE PLACED 10' FROM THE PAVEMENT EDGE UNLESS SHOWN OTHERWISE.

ALL PROPOSED TAXIWAY LIGHTS WILL BE CONSTRUCTED AT THE LOCATION SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE DETAILS PROVIDED AND THE SPECIFICATIONS.

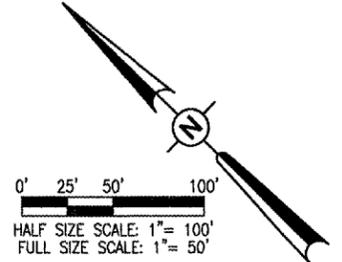
ALL PROPOSED TAXIWAY LIGHTS WILL USE 360° BLUE LENSES.

DUCT:
 ALL PROPOSED 4-WAY CONCRETE DUCT EXTENSIONS WILL BE CONSTRUCTED AT THE LOCATION SHOWN ON THIS SHEET AND IN ACCORDANCE WITH THE DETAILS PROVIDED AND THE SPECIFICATIONS.

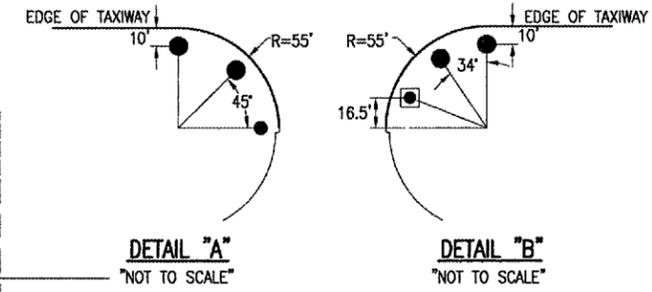
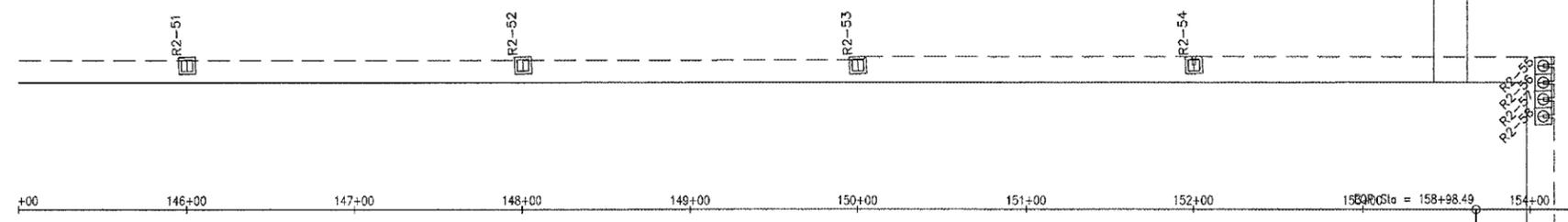
THE PROPOSED DUCT EXTENSIONS WILL MATCH EXISTING DUCT.

EXISTING UNDERGROUND ELECTRICAL DUCTS SHOWN TO BE EXTENDED ARE BELIEVED TO BE 4-WAY CONCRETE ENCASED DUCTS AND SHOULD BE BID ACCORDINGLY. HOWEVER, FIELD VERIFICATION IS REQUIRED AND ALTERNATIVE MATERIALS MAY BE REQUIRED IF DIFFERENT DUCT TYPES ARE DISCOVERED. THE CONTRACTOR SHOULD NOT ORDER MATERIALS UNTIL THE DUCT TYPE IS FIELD VERIFIED AND A PRICE CHANGE, IF NECESSARY, IS APPROVED.

DURING CONSTRUCTION, THE EXISTING ACTIVE RUNWAY AND TAXIWAY LIGHTING CIRCUITS MAY BE INOPERABLE DURING THE DAY. HOWEVER, THE CIRCUIT WILL BE FUNCTIONING AT THE END OF THE CONSTRUCTION DAY. THE CONTRACTOR WILL PROVIDE TEMPORARY CABLE, FIXTURES, OR DO WHATEVER IS NECESSARY TO HAVE THE CIRCUITS ACTIVE AT THE END OF THE CONSTRUCTION DAY. THE WORK WILL BE CONSIDERED AS PART OF THE LIGHTING INSTALLATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



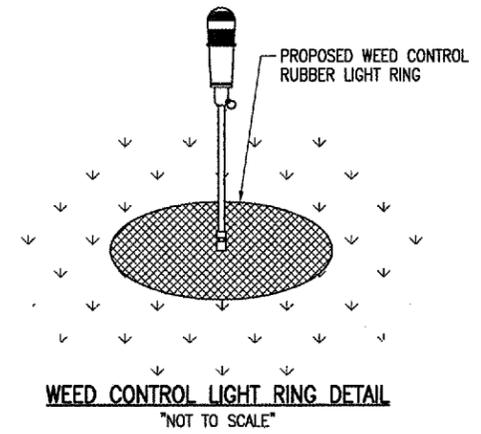
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 PROPOSED ELECTRICAL PLAN STA. 134+98 TO STA. 145+00	QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS 	IEL Project No. B2407TXD_Q100 Filename: R-142ELE.DWG Scale: 1" = 50' Date: 03/14/07	LAYOUT MLH 03/14/07 DRAWN MLH 03/16/07 REVIEWED JDW 03/19/07	BY: _____ REVISION: _____ DATE: _____ A.I.P. PROJ.: 3-17-0085-B1 I.L. PROJ.: UIN-3690
15		15 of 29 sheets		



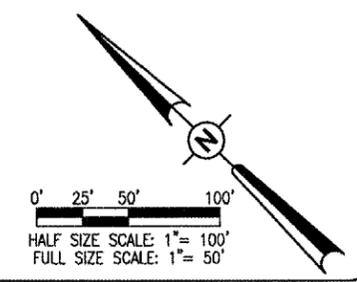
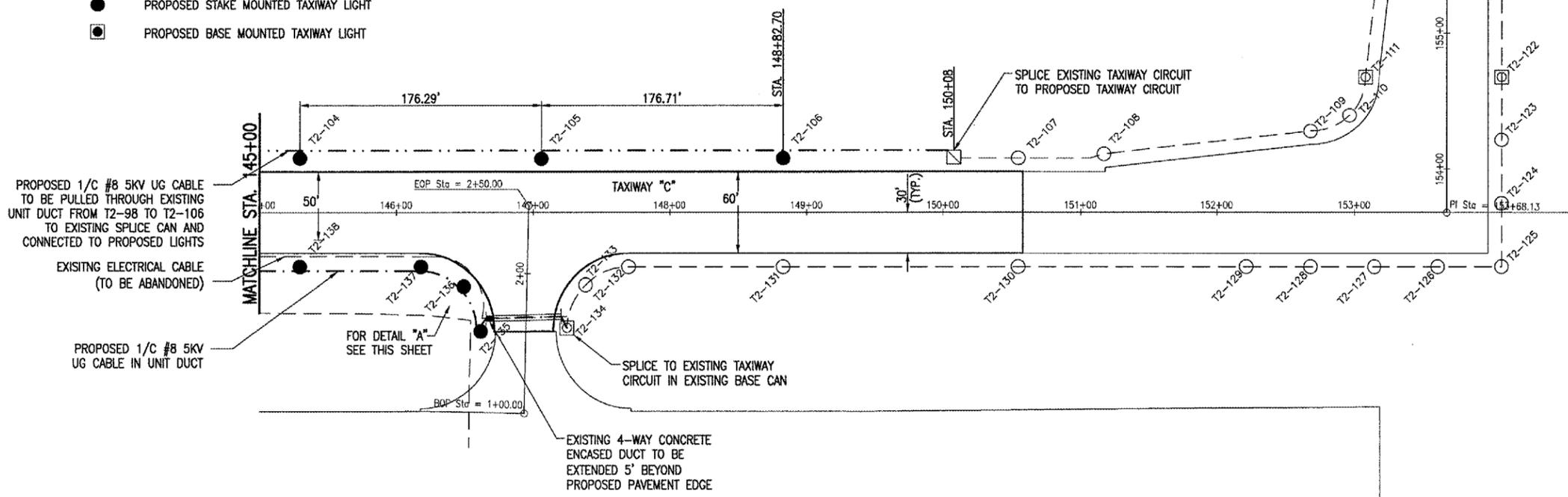
- LEGEND**
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 - PROPOSED PAVEMENT
 - EXISTING ELECTRICAL CABLE
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 - PROPOSED CONCRETE ENCASED DUCT
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WEED CONTROL LIGHT RING NOTE
ALL PROPOSED LIGHTS SHALL HAVE A WEED CONTROL LIGHT RING PLACED AT ITS BASE. SEE SPECIAL PROVISIONS FOR ITEM AR800449.



MAY 09, 2007 2:14 PM MLH I:\AIRPORTS\QUINCY\B2407TXD\AIRPORT\SHEETS\R-142ELE.DWG - 145+00 TO 151+00

REVISION	DATE	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS



HEL Project No.: B2407TXD_0100	DATE: 03/14/07
Filename: R-142ELE.DWG	MLH 03/14/07
Scale: 1" = 50'	MLH 03/16/07
Date: 03/14/07	MLH 03/16/07
	REVIEWED: JDW 03/19/07



REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2
PROPOSED ELECTRICAL PLAN
STA. 145+00 TO STA. 153+68.13

REVISION	
DATE	

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS



HEL Project No.	82407XVD_0100
Element	R-541ELEC.DWG
Sheet	N/A
Date	03/19/07
LAYOUT	MLH 03/19/07
DRAWN	MLH 03/19/07
REVIEWED	JDW 03/19/07



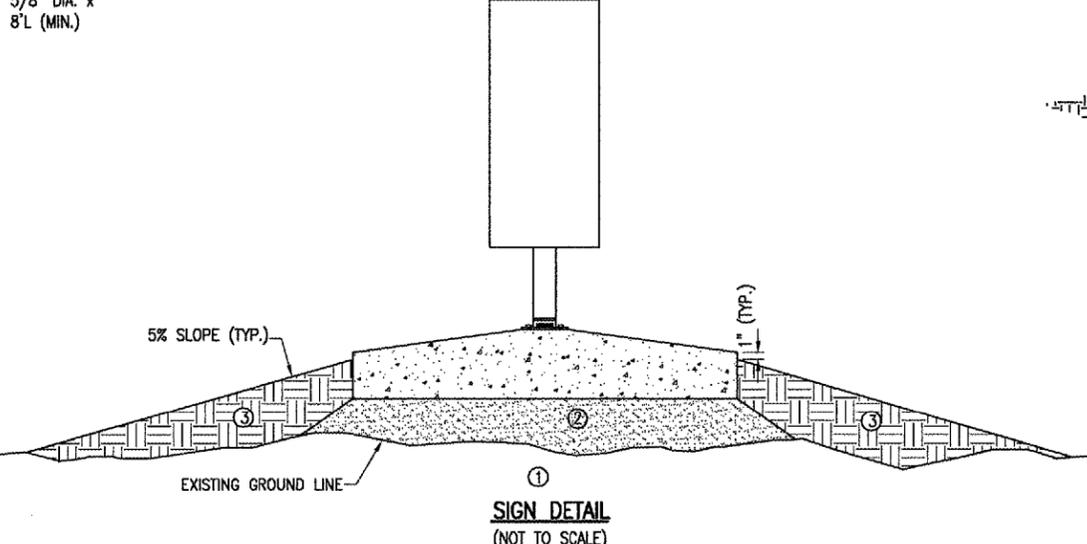
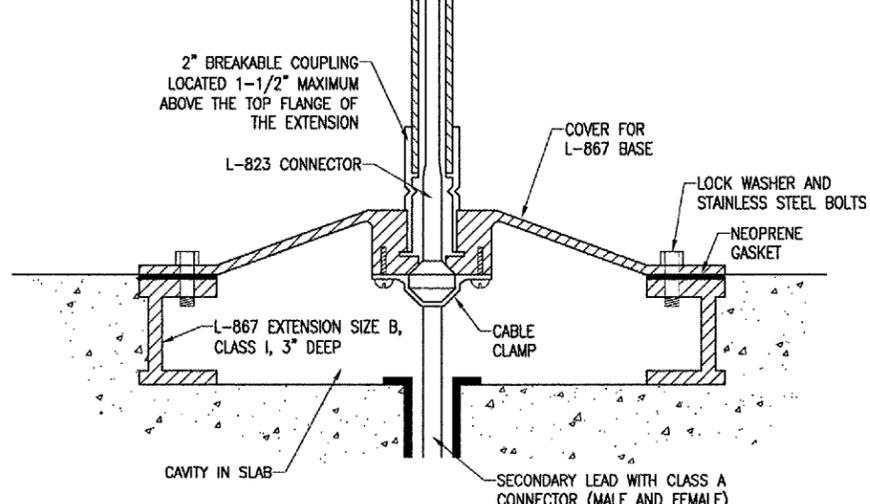
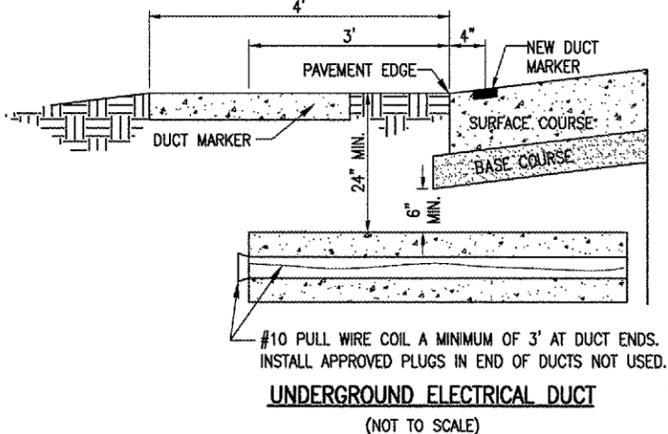
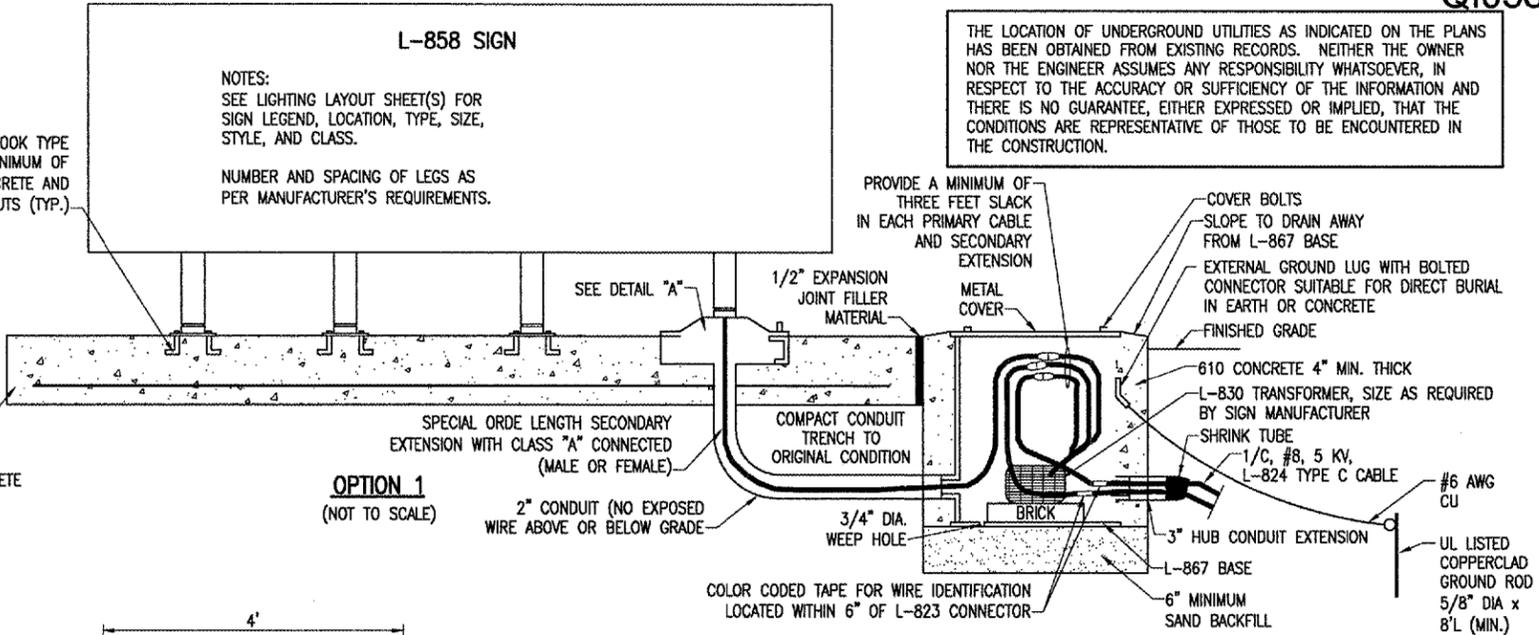
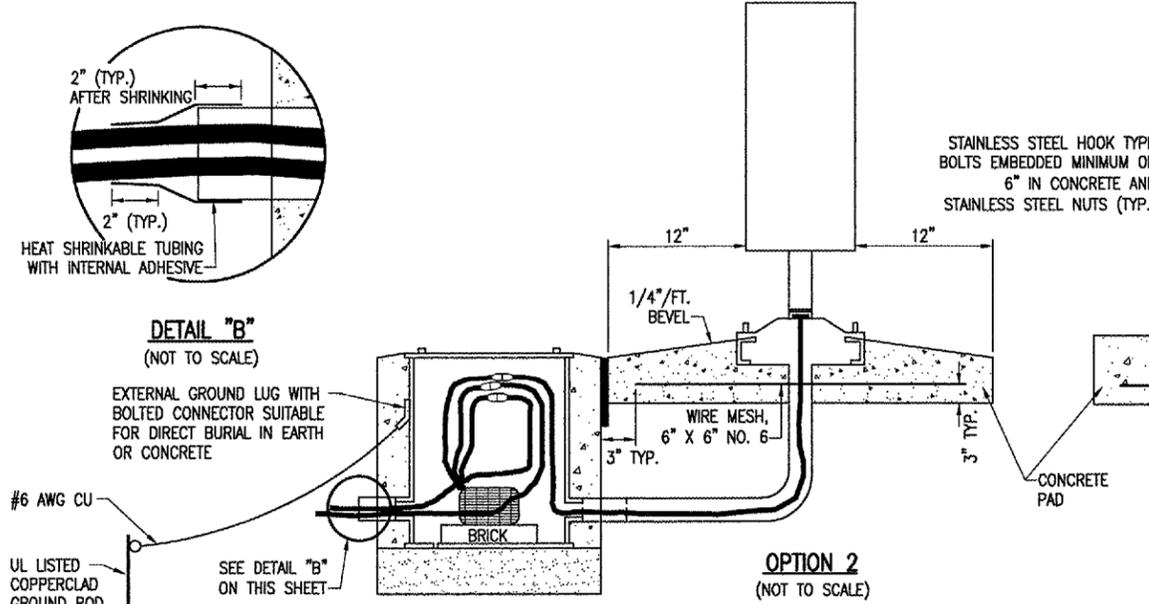
REALIGN, WIDEN
AND REHABILITATE
TAXIWAY "C" - PHASE 2
PROPOSED
ELECTRICAL
DETAILS

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER, IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

L-858 SIGN

NOTES:
SEE LIGHTING LAYOUT SHEET(S) FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND CLASS.

NUMBER AND SPACING OF LEGS AS PER MANUFACTURER'S REQUIREMENTS.

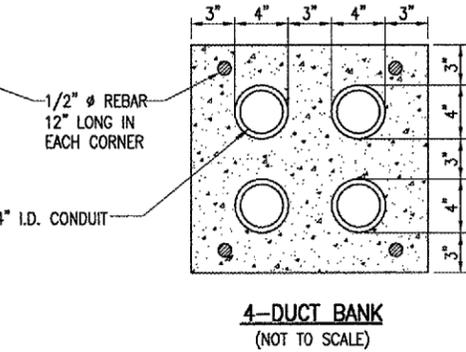
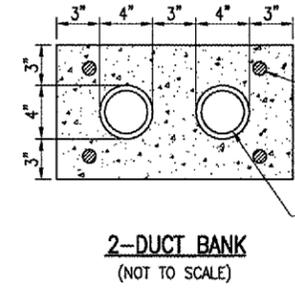
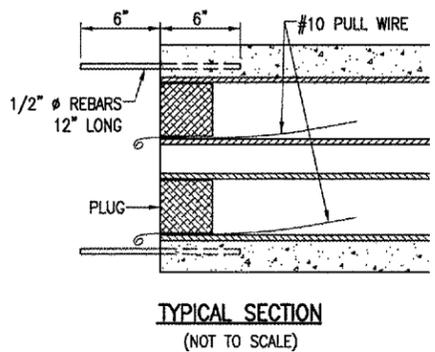
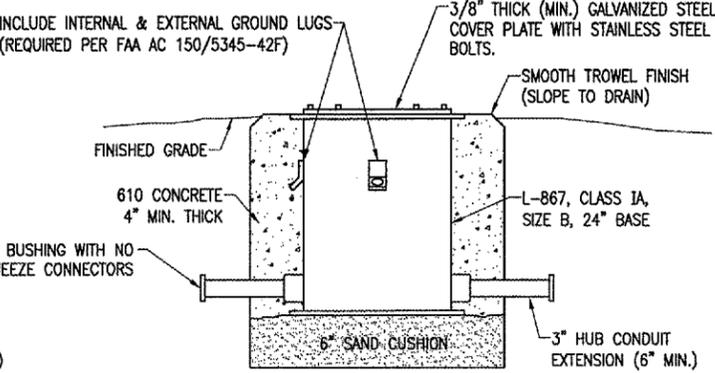
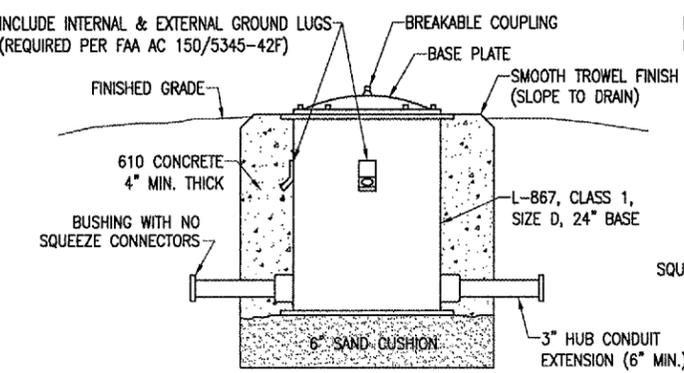


- ① EXISTING SOD TO BE STRIPPED AND REMOVED
- ② SAND BACKFILL, VARIABLE DEPTH
- ③ PROPOSED TOPSOIL BACKFILL MATERIAL

GENERAL NOTES

SEE LIGHTING LAYOUT SHEET FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND CLASS.

SEE SHEETS _____ THROUGH _____ FOR ELECTRICAL NOTES.



MAY 09, 2007 2:15 PM MLH E:\AIRPORTS\QUINCY\82407XVD\AIRPORT SHEETS\R-541ELEC.DWG - ELEC. DETAILS

GENERAL

1. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE (LATEST RECOGNIZED VERSION) AND LOCAL REGULATIONS.
2. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM, INCLUDING FAA APPROVED EQUIPMENT, ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
3. IN CASE THE CONTRACTOR ELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT REQUIRING ADDITIONAL WIRING, TRANSFORMERS, ADAPTORS, MOUNTINGS, ETC., TO THOSE SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATION, ANY COST FOR THESE ITEMS SHALL BE INCIDENTAL TO THE EQUIPMENT COST.
4. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
5. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE APPROVED.
6. ANY AND ALL INSTRUCTIONS FROM THE ENGINEER TO THE CONTRACTOR REGARDING CHANGES IN OR DEVIATIONS FROM THE PLANS AND SPECIFICATIONS SHALL BE IN WRITING WITH COPIES SENT TO THE AIRPORT SPONSOR AND THE FAA FIELD OFFICE (ADO/AFO). THE CONTRACTOR SHALL NOT ACCEPT ANY VERBAL INSTRUCTIONS FROM THE RESIDENT ENGINEER REGARDING ANY CHANGES FROM THE PLANS AND SPECIFICATIONS.
7. A MINIMUM OF THREE COPIES OF THE INSTRUCTION BOOK SHALL BE SUPPLIED WITH EACH DIFFERENT TYPE OF EQUIPMENT. THE BOOKS DESCRIBING A MORE SOPHISTICATED TYPE OF EQUIPMENT, SUCH AS REGULATORS, PAPI, REIL, ETC. AS A MINIMUM SHALL CONTAIN THE FOLLOWING:
 - A. A DETAILED DESCRIPTION OF THE OVERALL EQUIPMENT AND ITS INDIVIDUAL COMPONENTS.
 - B. THEORY OF OPERATION INCLUDING THE FUNCTION OF EACH COMPONENT.
 - C. INSTALLATION INSTRUCTIONS.
 - D. START-UP INSTRUCTIONS.
 - E. PREVENTATIVE MAINTENANCE REQUIREMENTS.
 - F. CHART FOR TROUBLE-SHOOTING.
 - G. COMPLETE POWER AND CONTROL DETAILED WIRING DIAGRAM(S), SHOWING EACH CONDUCTOR/CONNECTION/COMPONENT - "BLACK" BOXES ARE NOT ACCEPTABLE. THE DIAGRAM OF THE NARRATIVE SHALL SHOW VOLTAGE/CURRENTS/WAVE SHAPES AT STRATEGIC LOCATIONS TO BE USED WHEN CHECKING AND/OR TROUBLE-SHOOTING THE EQUIPMENT. WHEN THE EQUIPMENT HAS SEVERAL MODES OF OPERATION, SUCH AS SEVERAL BRIGHTNESS STEPS, THESE PARAMETERS SHALL BE INDICATED FOR ALL DIFFERENT MODES.
 - H. PARTS LIST WHICH WILL INCLUDE ALL MAJOR AND MINOR COMPONENTS SUCH AS RESISTORS, DIODES, ETC. IT SHALL INCLUDE A COMPLETE NOMENCLATURE OF EACH COMPONENT AND, IF APPLICABLE, THE NAME OF ITS MANUFACTURER AND THE CATALOG NUMBER.
 - I. SAFETY INSTRUCTIONS.

POWER AND CONTROL

1. STENCIL ALL ELECTRICAL EQUIPMENT TO IDENTIFY FUNCTION, CIRCUIT VOLTAGE AND PHASE. WHERE THE EQUIPMENT CONTAINS FUSES, ALSO STENCIL THE FUSE OR FUSE LINK AMPERE RATING. WHERE THE EQUIPMENT DOES NOT HAVE SUFFICIENT STENCILING AREA, THE STENCILING SHALL BE DONE ON THE WALL NEXT TO THE UNIT. THE LETTERS SHALL BE ONE INCH HIGH AND PAINTED IN WHITE OR BLACK TO PROVIDE THE HIGHEST CONTRAST WITH THE BACKGROUND.
2. COLOR CODE ALL PHASE WIRING BY THE USE OF COLORED WIRE INSULATION AND/OR COLORED TAPE. WHERE TAPE IS USED, THE WIRE INSULATION SHALL BE BLACK, BLACK AND RED SHALL BE USED FOR SINGLE-PHASE, THREE WIRE SYSTEMS AND BLACK, RED AND BLUE SHALL BE USED FOR THREE-PHASE SYSTEMS. NEUTRAL CONDUCTORS, SIZE NO. 6 AWG OR SMALLER, SHALL BE IDENTIFIED BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH. NEUTRAL CONDUCTORS LARGER THAN NO. 6 AWG SHALL BE IDENTIFIED EITHER BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH OR BY THE USE OF WHITE TAPE AT ITS TERMINATIONS AND INSIDE ACCESSIBLE WIREWAYS.
3. ALL BRANCH CIRCUIT CONDUCTORS CONNECTED TO A PARTICULAR PHASE SHALL BE IDENTIFIED WITH THE SAME COLOR. THE COLOR CODING SHALL BE EXTENDED TO THE POINT OF UTILIZATION.
4. IN CONTROL WIRING THE SAME COLOR SHALL BE USED THROUGHOUT THE SYSTEM FOR THE SAME FUNCTION, SUCH AS 10%, 30%, 100% BRIGHTNESS CONTROL, ETC.
5. LOW VOLTAGE (600 V.) AND HIGH VOLTAGE (5000 V.) CONDUCTORS SHALL BE INSTALLED IN SEPARATE WIREWAYS.
6. NEATLY LACE WIRING IN DISTRIBUTION PANELS, WIREWAYS, SWITCHES AND JUNCTION/PULL BOXES.
7. THE MINIMUM SIZE OF PULL/JUNCTION BOXES, REGARDLESS OF THE QUANTITY AND SIZE OF THE CONDUCTORS SHOWN, SHALL BE AS FOLLOWS:
 - A. IN STRAIGHT PULLS THE LENGTH OF THE BOX SHALL NOT BE LESS THAN EIGHT TIMES THE TRADE DIAMETER OF THE LARGER CONDUIT. THE TOTAL AREA (INCLUDING THE CONDUIT CROSS-SECTIONAL AREA) OF A BOX END SHALL BE AT LEAST 3 TIMES GREATER THAN THE TOTAL TRADE CROSS-SECTIONAL AREA OF THE CONDUITS TERMINATING AT THE END.
 - B. IN ANGLE PULLS OR 'U' PULLS THE DISTANCE BETWEEN EACH CONDUIT ENTRY INSIDE THE BOX AND THE OPPOSITE WALL OF THE BOX SHALL NOT BE LESS THAN SIX (6) TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT. THIS DISTANCE SHALL BE INCREASED FOR ADDITIONAL ENTRIES BY THE AMOUNT OF THE SUM OF THE DIAMETERS OF ALL OTHER CONDUIT ENTRIES ON THE SAME WALL AS THE BOX. THE DISTANCE BETWEEN CONDUIT ENTRIES ENCLOSING THE SAME CONDUCTOR SHALL NOT BE LESS THAN SIX TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT.
8. A RUN OF CONDUIT BETWEEN TERMINATIONS AT EQUIPMENT ENCLOSURES, SQUARE DUCTS AND PULL/JUNCTION BOXES, SHALL NOT CONTAIN MORE THAN THE EQUIVALENT OF FOUR QUARTER BENDS (360 DEGREES TOTAL), INCLUDING THOSE BENDS LOCATED IMMEDIATELY AT THE TERMINATIONS, CAST CONDUIT TYPE OUTLETS SHALL NOT BE TREATED AS PULL/JUNCTION BOXES.
9. EQUIPMENT CABINETS SHALL NOT BE USED AS PULL/JUNCTION BOXES. ONLY WIRING TERMINATING AT THE EQUIPMENT SHALL BE BROUGHT INTO THESE ENCLOSURES.
10. SPLICES AND JUNCTION POINTS SHALL BE PERMITTED ONLY IN JUNCTION BOXES, DUCTS EQUIPPED WITH REMOVABLE COVERS, AND AT EASILY ACCESSIBLE LOCATIONS.
11. CIRCUIT BREAKERS IN POWER DISTRIBUTION PANEL(S) SHALL BE THERMAL-MAGNETIC MOLDED CASE, PERMANENT TRIP WITH 100 AMPERE, MINIMUM FRAME.
12. DUAL LUGS SHALL BE USED WHERE TWO (2) WIRES, SIZE NO. 6 OR LARGER, ARE TO BE CONNECTED TO THE SAME TERMINAL.
13. ALL WALL MOUNTED EQUIPMENT ENCLOSURES SHALL BE MOUNTED ON WOODEN MOUNTING BOARDS.
14. WOODEN EQUIPMENT MOUNTING BOARDS SHALL BE PLYWOOD, EXTERIOR TYPE, 3/4 INCH, MINIMUM, THICKNESS, BOTH SIDES PAINTED WITH ONE COAT OF PRIMER AND TWO COATS OF GRAY OIL-BASED PAINT.
15. RIGID STEEL CONDUIT SHALL BE USED THROUGHOUT THE INSTALLATION UNLESS OTHERWISE SPECIFIED. THE MINIMUM TRADE SIZE SHALL BE 3/4 INCH.
16. ALL RIGID CONDUIT SHALL BE TERMINATED AT CONSTANT CURRENT REGULATORS WITH A SECTION (10" MINIMUM) OF FLEXIBLE CONDUIT.
17. UNLESS OTHERWISE SHOWN, ALL EXPOSED CONDUITS SHALL BE RUN PARALLEL TO OR AT RIGHT ANGLES WITH THE LINES OF THE STRUCTURE.
18. ALL STEEL CONDUITS, FITTINGS, NUTS, BOLTS, ETC. SHALL BE GALVANIZED.
19. USE CONDUIT BUSHINGS AT EACH CONDUIT TERMINATION. WHERE NO. 4 AWG OR LARGER UNDERGROUND WIRE IS INSTALLED, USE INSULATED BUSHINGS.
20. USE DOUBLE LOCK NUTS AT EACH CONDUIT TERMINATION.
21. WRAP ALL PRIMARY AND SECONDARY POWER TRANSFORMER CONNECTIONS WITH SUFFICIENT LAYERS OF INSULATING TAPE AND COVER WITH INSULATING VARNISH FOR FULL VALUE OF CABLE INSULATION VOLTAGE.
22. UNLESS OTHERWISE NOTED, ALL INDOOR SINGLE CONDUCTOR CONTROL WIRING SHALL BE NO. 12 AWG. MINIMUM.
23. THE FOLLOWING SHALL APPLY TO RELAY/CONTACTOR PANELS/ENCLOSURES:
 - A. ALL COMPONENTS SHALL BE MOUNTED IN DUST PROOF ENCLOSURE(S) WITH VERTICALLY HINGED COVERS.
 - B. THE ENCLOSURE(S) SHALL HAVE AMPLE SPACE FOR THE CIRCUIT COMPONENTS, TERMINAL BLOCKS AND INCOMING AND INTERNAL WIRING.
 - C. ALL CONTROL CONDUCTOR TERMINATIONS SHALL BE OF THE OPEN-EYE CONNECTOR/SCREW TYPE. SOLDERED CLOSED-EYE TERMINATIONS, OR TERMINATIONS WITHOUT CONNECTORS ARE NOT ACCEPTABLE.
 - D. WHEN THE ENCLOSURE COVER IS OPENED, ALL CIRCUIT COMPONENTS, WIRING AND TERMINALS SHALL BE EXPOSED AND ACCESSIBLE WITHOUT REMOVAL OF ANY PANELS, COVERS, ETC., EXCEPT THOSE COVERING HIGH VOLTAGE COMPONENTS.
 - E. ACCESS TO, OR REMOVAL OF A CIRCUIT COMPONENT OR TERMINAL BLOCK WILL NOT REQUIRE THE REMOVAL OF ANY OTHER CIRCUIT COMPONENT OR TERMINAL BLOCK.
 - F. EACH CIRCUIT COMPONENT SHALL BE CLEARLY IDENTIFIED INDICATING ITS CORRESPONDING NUMBER SHOWN ON THE DRAWINGS AND ITS FUNCTION.
 - G. A COMPLETE WIRING DIAGRAM (NOT A SCHEMATIC DIAGRAM) SHALL BE MOUNTED ON THE INSIDE OF THE COVER. THE DIAGRAM SHALL REPRESENT EACH CONDUCTOR BY A SEPARATE LINE.
 - H. THE DIAGRAM SHALL IDENTIFY EACH CIRCUIT COMPONENT AN NUMBERING AND COLOR OF EACH TERMINAL CONDUCTOR AND TERMINAL.
 - I. ALL WIRING SHALL BE NEATLY TRAINED AND LACED.
 - J. MINIMUM WIRE SIZE SHALL BE NO. 12 AWG.

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BY	
REVISION	
DATE	

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS

IL PROJ.: UIN-3680 A.I.P. PROJ.: 3-17-0085-B1

HEL Project No. 82407TXD_0100	
Element R-543ELEC.DWG	
Scale N/A	
Date 03/13/07	
LAYOUT DAK 03/13/07	
DRAWN MLH 03/19/07	
REVIEWED JDW 03/19/07	



REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2

PROPOSED ELECTRICAL NOTES

FIELD LIGHTING NOTES

1. UNLESS OTHERWISE NOTED, ALL UNDERGROUND FIELD POWER MULTIPLE AND SERIES CIRCUIT CONDUCTORS WHETHER DEB OR IN DUCT/CONDUIT SHALL BE FAA APPROVED L-824 TYPE. INSULATION VOLTAGE AND SIZE SHALL BE AS SPECIFIED.
2. NO COMPONENTS OF PRIMARY CIRCUIT SUCH AS CABLE, CONNECTORS AND TRANSFORMERS SHALL BE BROUGHT ABOVE GROUND AT EDGE LIGHTS, SIGNS, REIL, PAPI, ETC.
3. THERE SHALL BE NO EXPOSED POWER/CONTROL CABLES BETWEEN THE POINT WHERE THEY LEAVE THE UNDERGROUND (DEB OR L-867 BASES) AND WHERE THEY ENTER THE EQUIPMENT (SUCH AS TAXIWAY SIGNS, PAPI, REIL, ETC.) ENCLOSURES. THESE CABLES SHALL BE ENCLOSED IN RIGID CONDUIT OR IN FLEXIBLE, WATERTIGHT CONDUIT WITH BREAKABLE COUPLING(S) AT THE GRADE OR THE HOUSING COVER, AS SHOWN IN APPLICABLE DETAILS.
4. THE JOINTS OF THE L-823 PRIMARY CONNECTORS SHALL BE WRAPPED WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF THE JOINT, AS SHOWN ON SHEET NO. 16.
5. THE CABLE ENTRANCE INTO THE FIELD-ATTACHED L-823 CONNECTORS SHALL BE ENCLOSED BY A HEAT-SHRINKABLE TUBING WITH CONTINUOUS INTERNAL ADHESIVE, AS SHOWN ON SHEET NO. 16.
6. L-823 TYPE II, TWO-CONDUCTOR SECONDARY CONNECTORS SHALL BE CLASS 'A' (FACTORY MOLDED).
7. THERE SHALL BE NO SPLICES IN THE SECONDARY CABLE(S) WITHIN THE STEMS OF A RUNWAY/TAXIWAY EDGE/THRESHOLD LIGHTING FIXTURE AND THE WIREWAYS LEADING TO TAXIWAY SIGNS AND PAPI/REIL EQUIPMENT.
8. ELECTRICAL INSULATING GREASE SHALL BE APPLIED WITHIN THE L-823, SECONDARY, TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THESE CONNECTORS SHALL NOT BE TAPED.
9. DEB ISOLATION TRANSFORMERS SHALL BE BURIED AT A DEPTH OF TEN (10") INCHES ON A LINE CROSSING THE LIGHT AND PERPENDICULAR TO THE RUNWAY/TAXIWAY CENTERLINE AT A LOCATION TWELVE (12") INCHES FROM THE LIGHT OPPOSITE FROM THE RUNWAY/TAXIWAY.
10. A SLACK OF THREE (3') FEET, MINIMUM, SHALL BE PROVIDED IN THE PRIMARY CABLE AT EACH TRANSFORMER/CONNECTOR TERMINATION. AT STAKE-MOUNTED LIGHTS, THE SLACK SHALL BE LOOSELY COILED IMMEDIATELY BELOW THE ISOLATION TRANSFORMER.
11. DIRECTION OF PRIMARY CABLES SHALL BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK TO PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO RIGHT IS CODED BLUE. THIS APPLIES TO STAKE MOUNTED LIGHTS AND BASE MOUNTED LIGHTS WHERE THE BASE HAS ONLY ONE ENTRANCE.
12. L-867 BASES SHALL BE SIZE B, 24" DEEP, CLASS I, UNLESS OTHERWISE NOTED.
13. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL BE A 1/4" DIAMETER, MINIMUM, OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE SECONDARY CONNECTOR INTO THE L-867 BASE.
14. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1-1/2" ABOVE THE EDGE OF THE COVER IN CASE OF BASE MOUNTED COUPLINGS, OR THE TOP OF THE STAKE IN CASE OF STAKE MOUNTED COUPLINGS.

15. WHERE THE BREAKABLE COUPLING IS NOT AN INTEGRAL PART OF THE LIGHT FIXTURE STEM OR MOUNTING LEG, A BEAD OF SILICON SEAL SHALL BE APPLIED COMPLETELY AROUND LIGHT STEM OR WIREWAY AT BREAKABLE COUPLING TO PROVIDE A WATERTIGHT SEAL.
16. TOPS OF THE STAKES SUPPORTING LIGHT FIXTURES SHALL BE FLUSH WITH THE SURROUNDING GRADE.
17. PLASTIC LIGHTING FIXTURE COMPONENTS, SUCH AS LAMP HEADS, STEMS, BREAKABLE COUPLINGS, BASE COVERS, BRACKETS, STAKES, SHALL NOT BE ACCEPTABLE.
18. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE: ONE (1) INCH. IN CASE OF STAKE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE STAKE AND THE TOP OF THE LENS. IN CASE OF BASE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE BASE FLANGE AND THE TOP OF THE LENS, THUS INCLUDING THE BASE COVER, THE FRANGIBLE COUPLING, THE STEM, THE LAMP HOUSING AND THE LENS.
19. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE ONE (1) INCH. THIS ALSO APPLIES AT INTERSECTIONS TO LATERAL SPACING BETWEEN LIGHTS OF A RUNWAY/TAXIWAY AND THE INTERSECTING RUNWAY/TAXIWAY.
20. ENTRANCES INTO L-867 BASES SHALL BE SEALED WITH HEAT SHRINK AS SHOWN IN DETAIL "B" ON SHEET NO. 15.
21. GALVANIZED/PAINTED EQUIPMENT/COMPONENT SURFACES SHALL NOT BE DAMAGED BY DRILLING, FILING, ETC. DRAIN HOLES IN METAL TRANSFORMER HOUSINGS SHALL BE MADE BEFORE GALVANIZING.
22. EDGE LIGHT NUMBERING TAGS SHALL BE FACING THE PAVEMENT.
23. CABLE/SPLICE/DUCT MARKERS SHALL BE PRECAST CONCRETE OF THE SIZE SHOWN. LETTERS/NUMBERS/ARROWS FOR THE LEGEND TO BE IMPRESSED INTO THE TOPS OF THE MARKERS SHALL BE PRE-ASSEMBLED AND SECURED IN THE MOLD BEFORE THE CONCRETE IS POURED. LEGEND INSCRIBED BY HAND IN WET CONCRETE SHALL NOT BE ACCEPTABLE.
24. ALL UNDERGROUND CABLE RUNS SHALL BE IDENTIFIED BY CABLE MARKERS AT 200 FEET MAXIMUM SPACING, WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS SHALL BE INSTALLED IMMEDIATELY ABOVE THE CABLES.
25. THERE SHALL BE NO SPLICES BETWEEN THE ISOLATION TRANSFORMERS. L-823 CONNECTORS ARE ALLOWED AT TRANSFORMER CONNECTIONS ONLY, UNLESS OTHERWISE SHOWN.
26. APPLY AN OXIDE INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS AND BREAKAGE COUPLING THREADS.
27. LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS SHALL BE IDENTIFIED BY DUCT MARKERS.
28. WHERE A PARALLEL, CONSTANT VOLTAGE PAPI SYSTEM IS PROVIDED, THE "T" SPLICES SHALL BE OF THE CAST TYPE.
29. CONCRETE USED FOR SLABS, FOOTINGS, BACKFILL AROUND TRANSFORMER HOUSINGS, MARKINGS, ETC. SHALL BE 3000 PSI, AIR-ENTRAINED.
30. ALL POWER AND CONTROL CABLES IN MAN/HAND HOLES SHALL BE TAGGED. USE EMBOSSED COPPER STRIPS TO BE ATTACHED AT BOTH ENDS TO THE CABLE BY THE USE OF PLASTIC STRAPS. MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A MAN/HAND HOLE-ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT.

GROUNDING NOTES

1. ALL GROUND CONNECTIONS TO GROUND RODS, BUSSES, PANELS, ETC. SHALL BE MADE WITH PRESSURE TYPE SOLDERLESS LUGS AND GROUND CLAMPS SOLDERED OR BOLT AND WASHER TYPE CONNECTIONS ARE NOT ACCEPTABLE. CLEAN ALL METAL SURFACES BEFORE MAKING GROUND CONNECTIONS. CONNECTIONS TO GROUND RODS SHALL BE EXOTHERMIC WELD WHERE SPECIFIED HEREIN.
2. TOP OF GROUND RODS SHALL BE TEN (10) INCHES BELOW GRADE, UNLESS SPECIFIED OTHERWISE HEREIN, FOR RESPECTIVE APPLICATIONS.
3. THE RESISTANCE TO GROUND OF THE VAULT GROUNDING SYSTEM WITH THE COMMERCIAL POWER LINE NEUTRAL DISCONNECTED SHALL NOT EXCEED 10 OHMS.

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DATE	REVISION						
 <p>QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS</p>							
ILL. PROJ.: UIN-3690 A.I.P. PROJ.: 3-17-0085-B1							
IEL Project No. 82407TXD.0100 Element R-544ELEC.DWG Size N/A Date 03/13/07		LAYOUT DAK 03/13/07 DRAWN MLH 03/19/07 REVIEWED JDW 03/19/07		 <p>HANSON Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62705-2886 Chicago Nationwide</p>			
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2				PROPOSED ELECTRICAL NOTES			
20 20 of 29 sheets							

Q1058

MARKING NOTES

THE TAXIWAY CENTERLINE STRIPE MARKING WILL BE SOLID AND YELLOW IN COLOR. THE TAXIWAY CENTERLINE WILL BE 1 FT IN WIDTH AND REQUIRE TWO APPLICATIONS.

ALL PROPOSED MARKING WILL BE LOCATED AS SHOWN ON THIS SHEET.

ALL YELLOW PROPOSED MARKING WILL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIFICATIONS.

BLACK MARKING WILL NOT REQUIRE REFLECTIVE MEDIA.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

ALL PROPOSED MARKING SHALL HAVE A 6 INCH WIDE (MIN.) BLACK OUTLINE.

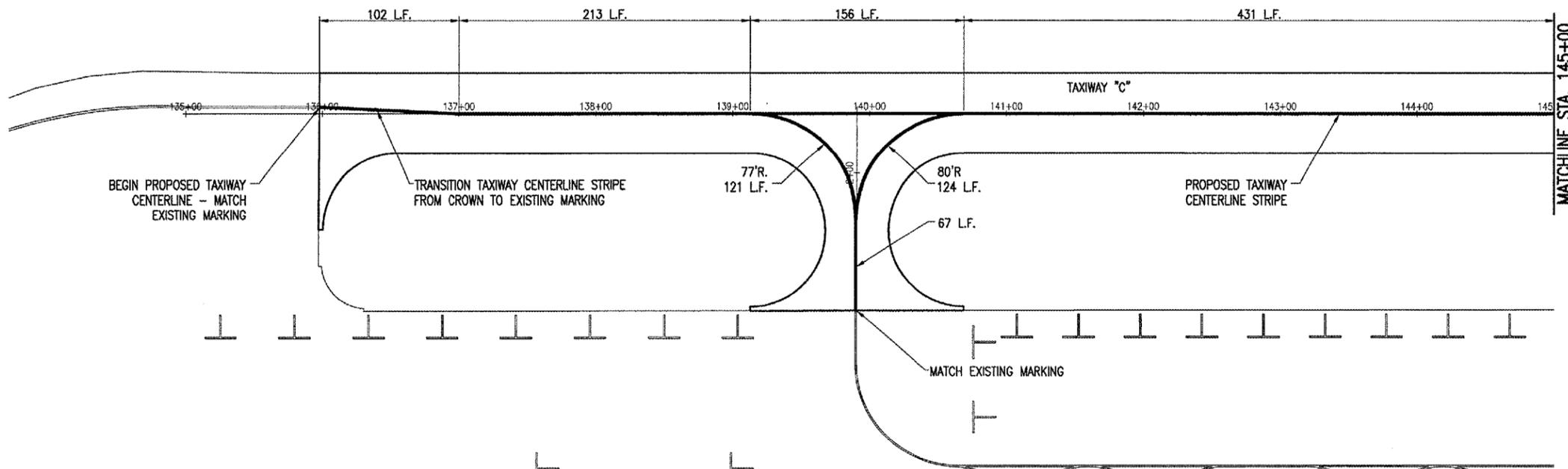
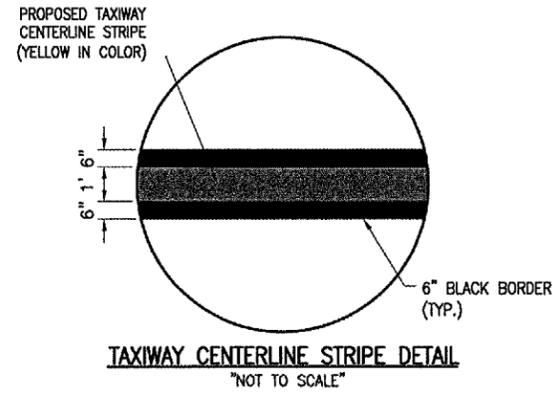
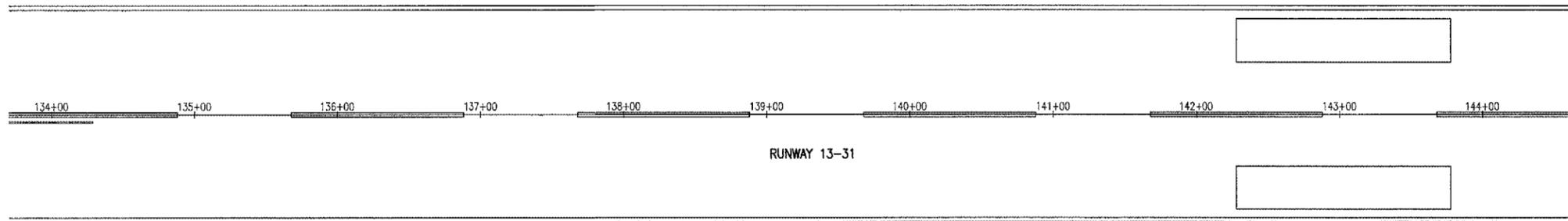
ALL PROPOSED MARKING ITEMS WILL BE PAID FOR AS FOLLOWS:
 AR620520 PAVEMENT MARKING-WATERBORNE - PER SQ. FT.
 AR620525 PAVEMENT MARKING-BLACK BORDER - PER SQ. FT.

MARKING QUANTITIES - AR620520

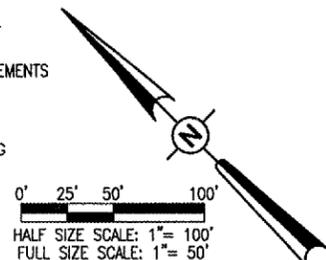
DESCRIPTION	TOTAL (S.F.)
TAXIWAY CENTERLINE STRIPING - YELLOW	2,024
TOTAL YELLOW:	2,024

MARKING QUANTITIES - AR620525

DESCRIPTION	TOTAL (S.F.)
TAXIWAY CENTERLINE OUTLINE - BLACK	2,024
TOTAL BLACK:	2,024



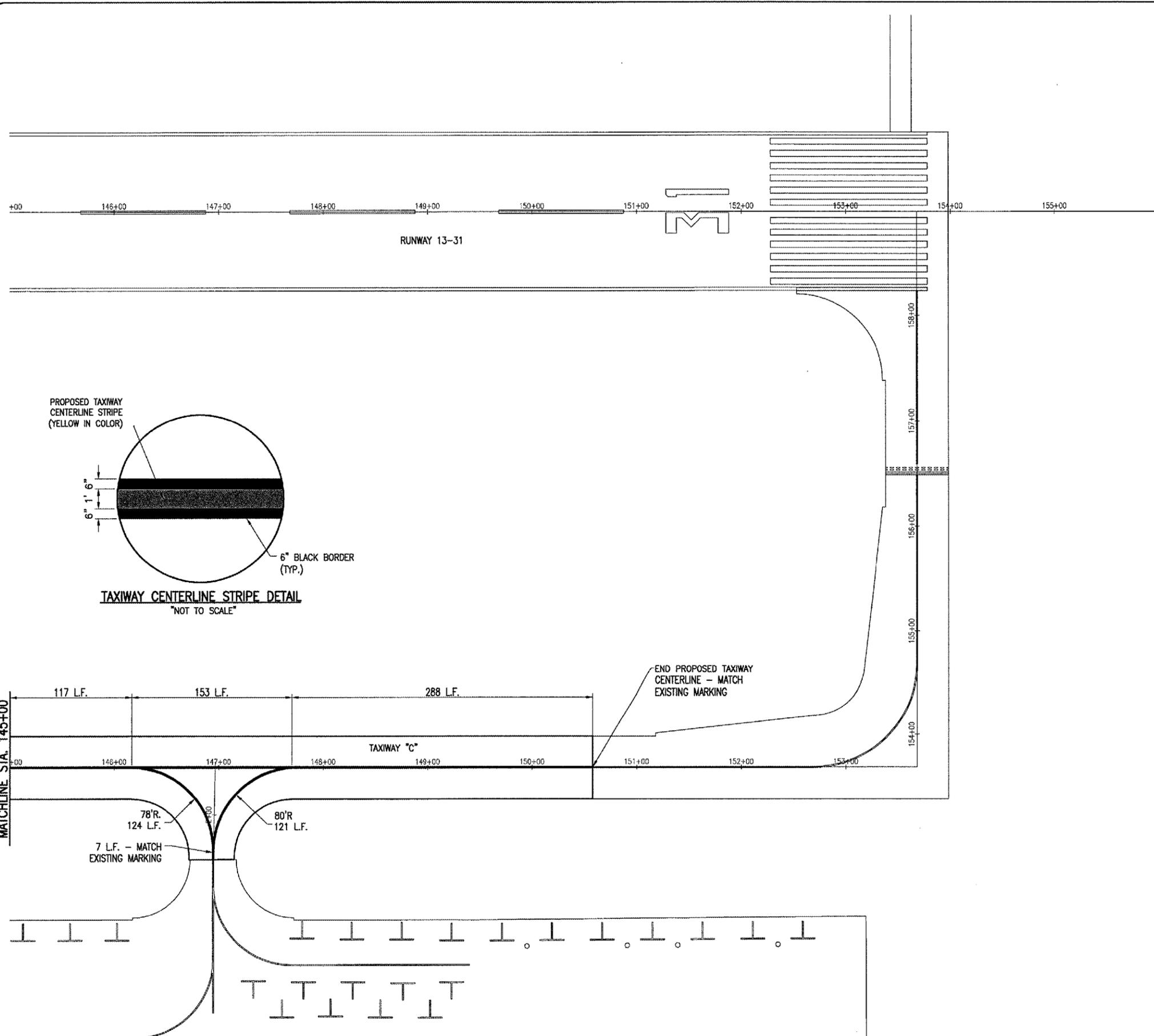
- LEGEND**
- EXISTING PAVEMENT
 - PROPOSED IMPROVEMENTS
 - EXISTING MARKING
 - PROPOSED MARKING



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DATE	REVISION	BY			
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS					
<small>Project No. 82407TXID_0100 Drawn: R-151MRK.DWG Scale: 1" = 50' Date: 03/15/07</small>					
LAYOUT	DAK	03/15/07			
DRAWN	MLH	03/15/07			
REVIEWED	JDW	03/19/07			
HANSON <small>Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide</small>					
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 PROPOSED MARKING PLAN STA. 134+98 TO STA. 145+00					
21 <small>21 of 28 sheets</small>					

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MARKING NOTES Q1058

THE TAXIWAY CENTERLINE STRIPE MARKING WILL BE SOLID AND YELLOW IN COLOR. THE TAXIWAY CENTERLINE WILL BE 1 FT IN WIDTH AND REQUIRE TWO APPLICATIONS.

ALL PROPOSED MARKING WILL BE LOCATED AS SHOWN ON THIS SHEET.

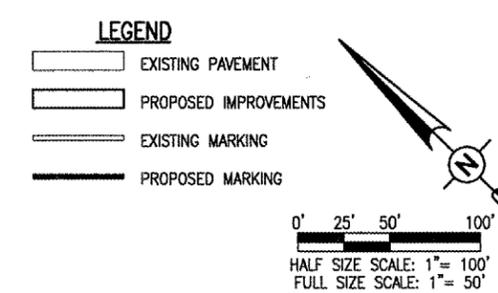
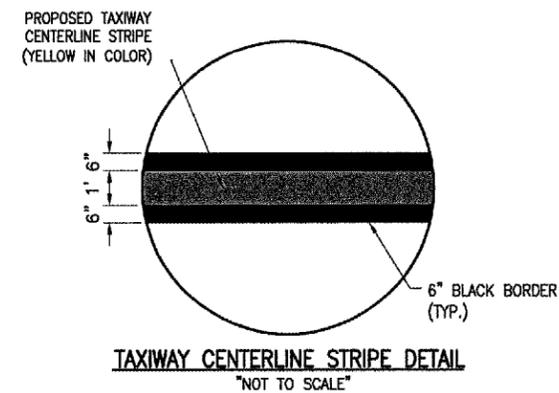
ALL YELLOW PROPOSED MARKING WILL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIFICATIONS.

BLACK MARKING WILL NOT REQUIRE REFLECTIVE MEDIA.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

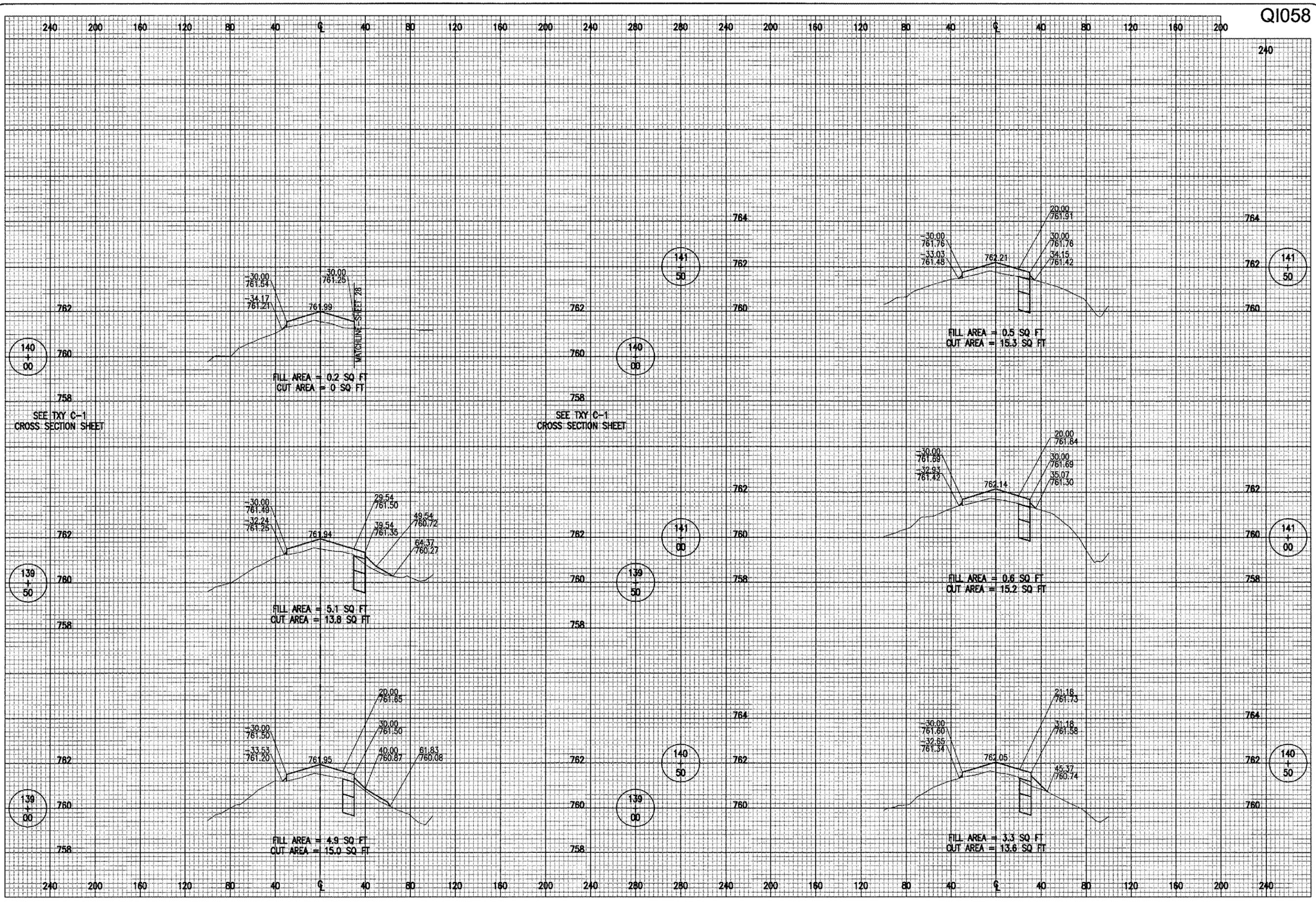
ALL PROPOSED MARKING SHALL HAVE A 6 INCH WIDE (MIN.) BLACK OUTLINE.

ALL PROPOSED MARKING ITEMS WILL BE PAID FOR AS FOLLOWS:
 AR620520 PAVEMENT MARKING-WATERBORNE - PER SQ. FT.
 AR620525 PAVEMENT MARKING-BLACK BORDER - PER SQ. FT.



DATE	REVISION	BY	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS			
IL. PROJ.: JIN-3680 A.I.P. PROJ.: 3-17-0085-B1			
Heli. Project No. B2407XXD_0100 Filename R-151MRK.DWG Scale 1" = 50' Date 03/15/07	LAYOUT DAK 03/15/07 DRAWN MLH 03/15/07 REVIEWED JDW 03/19/07		
HANSON Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62705-2888 Chicago, Nationwide			
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 PROPOSED MARKING PLAN STA. 145+00 TO STA. 153+68.13			
22 22 of 28 sheets			

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SEE TXY C-1
 CROSS SECTION SHEET

SEE TXY C-1
 CROSS SECTION SHEET

FILL AREA = 0.2 SQ FT
 CUT AREA = 0 SQ FT

FILL AREA = 5.1 SQ FT
 CUT AREA = 13.8 SQ FT

FILL AREA = 4.9 SQ FT
 CUT AREA = 15.0 SQ FT

FILL AREA = 0.5 SQ FT
 CUT AREA = 15.3 SQ FT

FILL AREA = 0.6 SQ FT
 CUT AREA = 15.2 SQ FT

FILL AREA = 3.3 SQ FT
 CUT AREA = 13.6 SQ FT

Q1058

DATE	REVISION

QUINCY REGIONAL AIRPORT
 BALDWIN FIELD
 ADAMS COUNTY, ILLINOIS



IL PROJ.: UIN-3690 A.I.P. PROJ.: 3-17-0085-B1

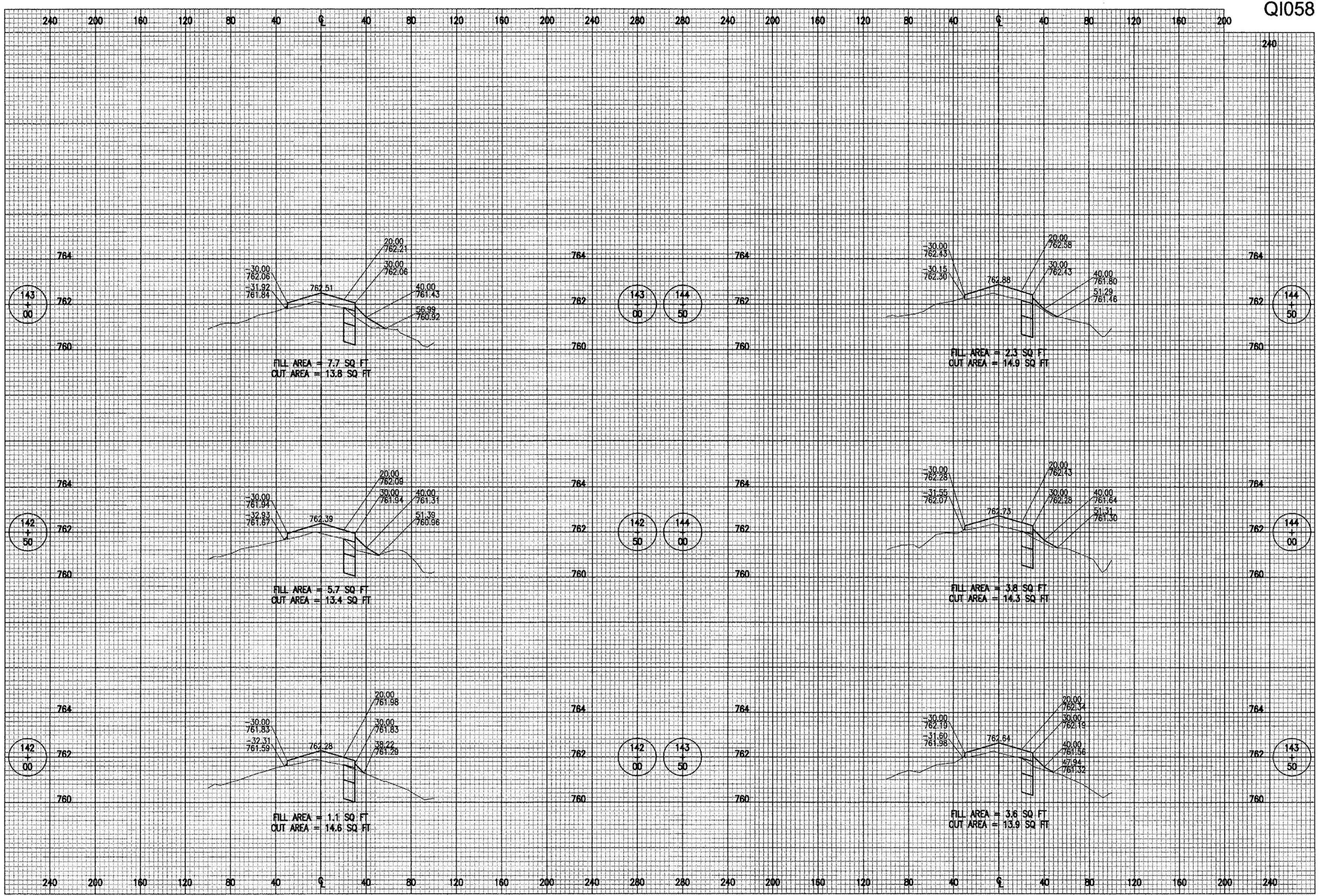
IEL Project No. 82407TXD 01.00 Filename R-302XS.DWG Plot Scale 1"=40'H. & 2"V. Date 03/19/07	LAYOUT DAK 03/19/07 DRAWN MLH 03/19/07 REVIEWED JDW 03/20/07
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Hanson Professional Services Inc.
 1525 South Sixth Street
 Springfield, Illinois 62703-2886
 Offices Nationwide

REALIGN, WIDEN
 AND REHABILITATE
 TAXIWAY "C" - PHASE 2
 PROPOSED
 CROSS SECTIONS
 STA. 139+00 TO STA. 141+50

MAY 09, 2007 2:34 PM MLH
I:\AIRPORTS\QUINCY\82407TXD\AIRPORT\SHIETS\R-303XS.DWG - STA. 142+00 - STA. 144+50



DATE	REVISION	BY

QUINCY REGIONAL AIRPORT
BALDWIN FIELD
ADAMS COUNTY, ILLINOIS



IL PROJ.: UIN-3690 A.I.P. PROJ.: 3-17-0085-B1

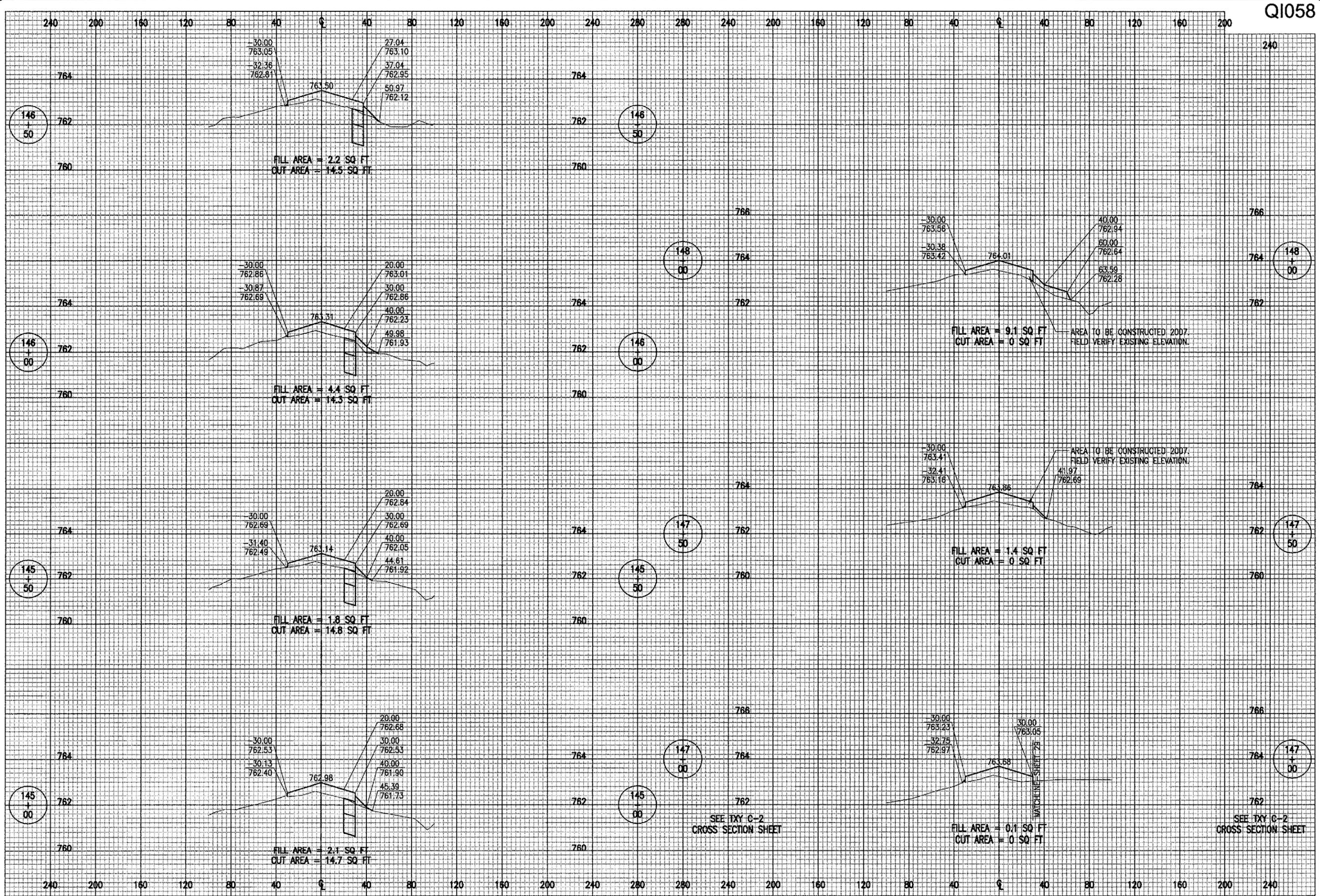
Proj. No. 82407TXD_0100	DATE 03/19/07
File R-303XS.DWG	DRAWN MLH
Scale 1"=40'H. & 2"V.	REVIEWED JDW
Date 03/19/07	



Hanson Professional Services Inc.
1625 South Sixth Street
Springfield, Illinois 62703-2886
Onosa Naramwe

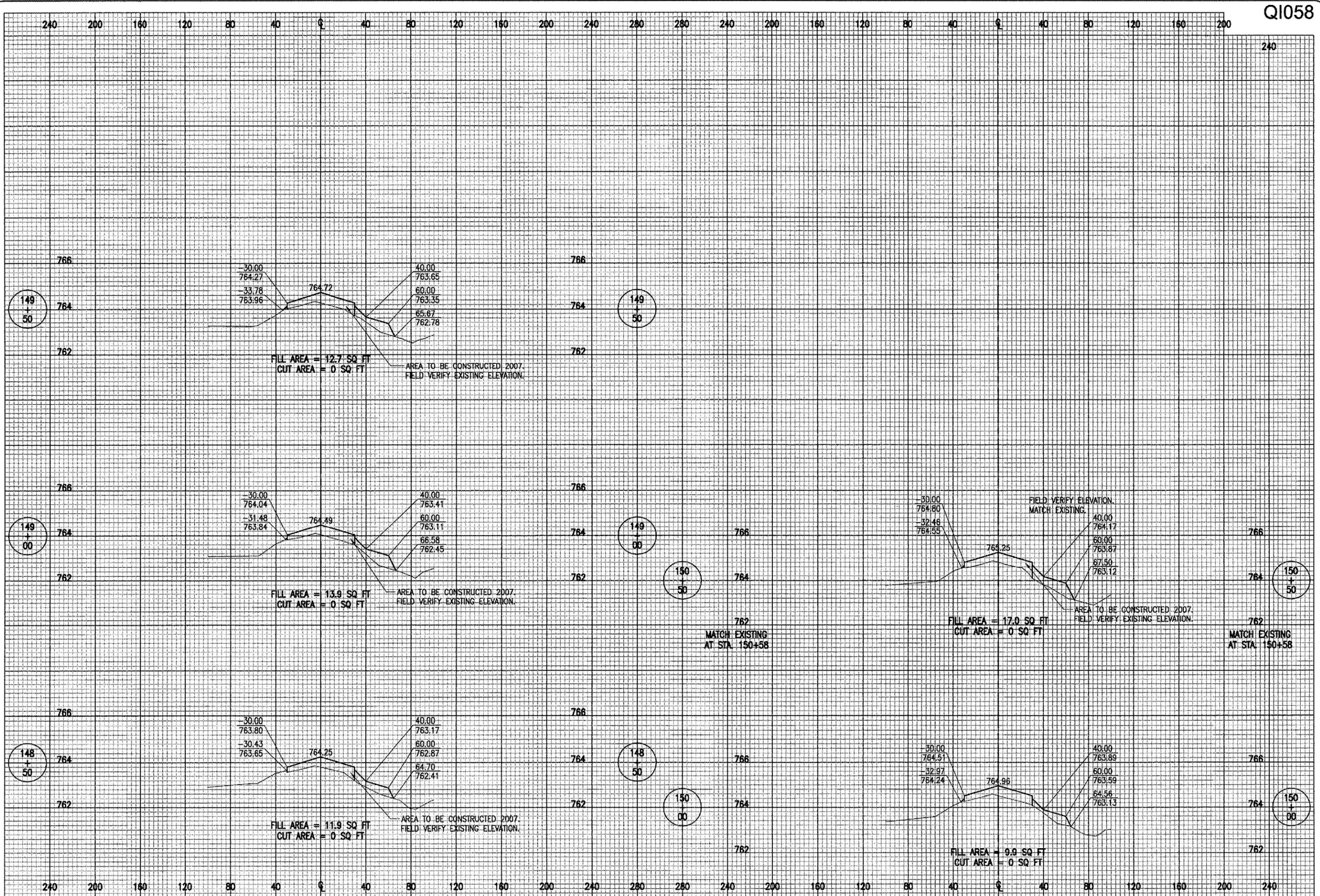
REALIGN, WIDEN
AND REHABILITATE
TAXIWAY "C" - PHASE 2
PROPOSED
CROSS SECTIONS
STA. 142+00 TO STA. 144+50

MAY 09, 2007 2:35 PM MLH
I:\AIRPORTS\QUINCY\82407TXD\AIRPORT\SHEETS\R-304XS.DWG - STA. 145+00 - STA. 148+00



BY	REVISION
DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
ILL. PROJ.: UIN-3690 A.I.P. PROJ.: 3-17-0085-B1	
I&L Project No. 82407TXD.0100 Filename R-304XS.DWG Scale 1"=40 H. & 2"V Date 03/19/07	LAYOUT DAK 03/19/07 DRAWN ESC 03/19/07 REVIEWED JDW 03/20/07
Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62705-2886 Offices Nationwide	
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 PROPOSED CROSS SECTIONS STA. 145+00 TO STA. 148+00	
26 26 of 29 sheets	

MAY 09, 2007 2:37 PM MLH
 I:\AIRPORTS\QUINCY\82407\XD\AIRPORT\SHEETS\R-305XS.DWG - STA. 148+00 - STA. 150+50



Q1058

BY	REVISION	DATE

QUINCY REGIONAL AIRPORT
 BALDWIN FIELD
 ADAMS COUNTY, ILLINOIS



IL PROJ.: LIN-3690 A.I.P. PROJ.: 3-17-0065-B1

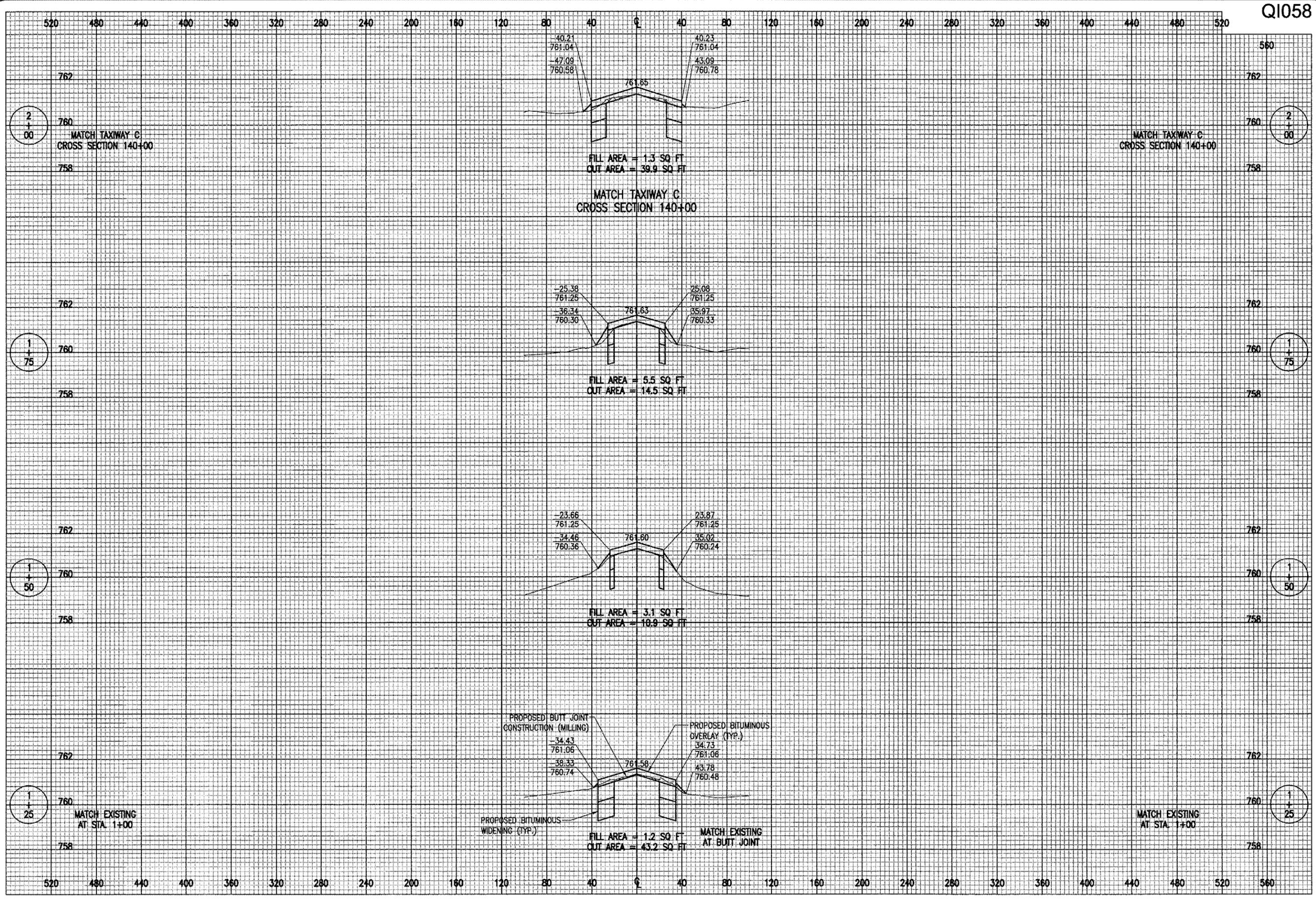
REL Project No. 82407\XD_0100	DATE 03/19/07
ENGINEER R-305XS.DWG	DAK
SCALE 1"=40'H. & 2'V.	ESC
DATE 03/19/07	JDW
LAYOUT	REVIEWED
DRAWN	



Hanson Professional Services Inc.
 1325 South Sixth Street
 Springfield, IL 62766
 Office: 217-226-8888

REALIGN, WIDEN
 AND REHABILITATE
 TAXIWAY "C" - PHASE 2
 PROPOSED
 CROSS SECTIONS
 STA. 148+00 TO STA. 150+50

MAY 09, 2007 2:37 PM MLH
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Q1058

BY	REVISION	DATE

QUINCY REGIONAL AIRPORT
 BALDWIN FIELD
 ADAMS COUNTY, ILLINOIS



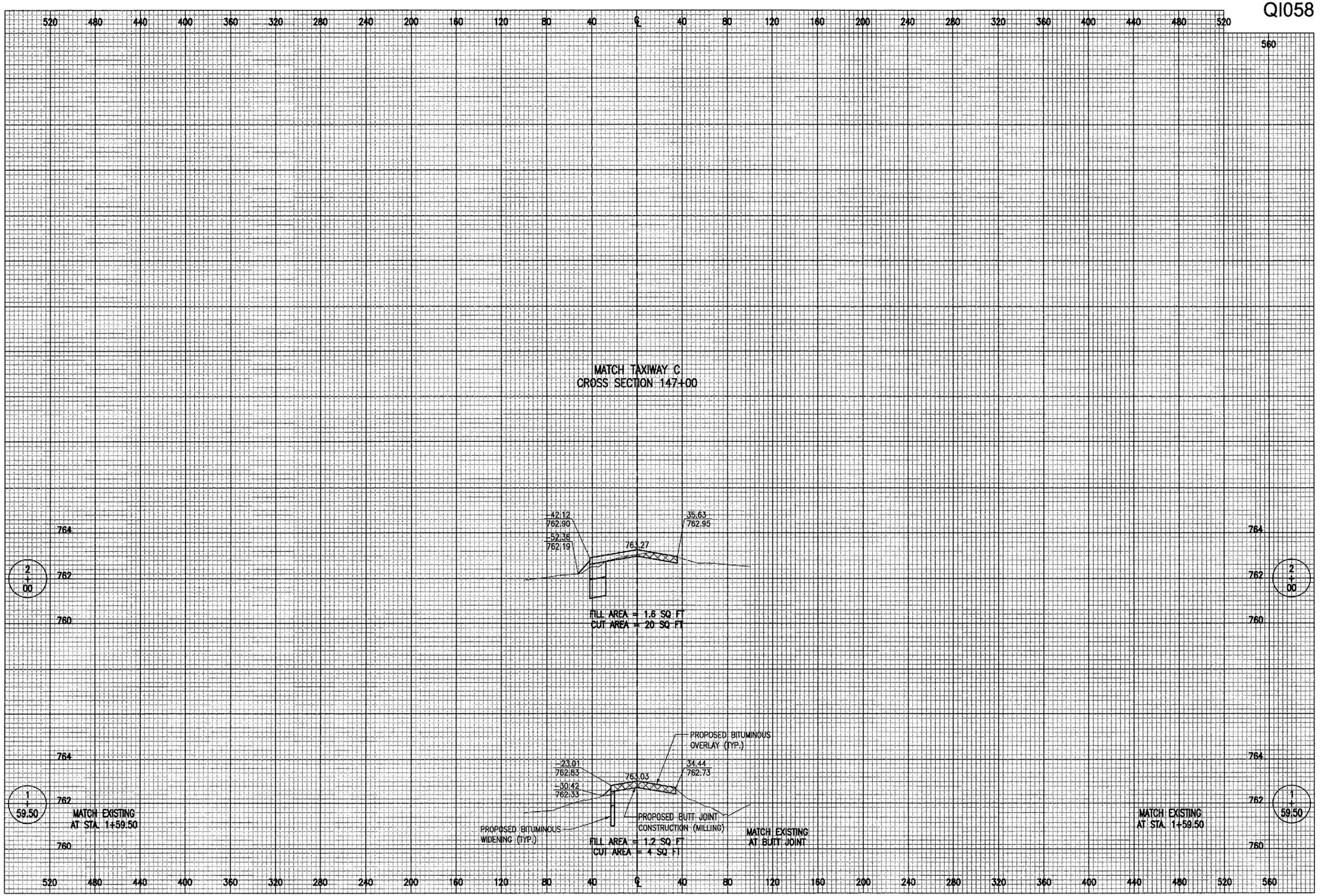
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Scale 1"=40'H. & 2"V.	JDW	03/20/07
Date 03/19/07		
LAYOUT		
DRAWN		
REVIEWED		



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REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2
 PROPOSED CROSS SECTIONS - TXY C-1 STA. 1+25 TO STA. 2+00



MAY 09, 2007 2:38 PM MLH
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BY	
REVISION	
DATE	
QUINCY REGIONAL AIRPORT BALDWIN FIELD ADAMS COUNTY, ILLINOIS	
	
I.L. PROJ.: UIN-3680 A.I.P. PROJ.: 3-17-0085-B1	
I.H.E. Project No. B2407TXD 0100 Filename R-307XS.DWG Scale 1" = 40 H. & 2 V. Date 03/19/07	LAYOUT DAK 03/19/07 DRAWN ESC 03/19/07 REVIEWED JDW 03/20/07
 <p>Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Chicago Nationwide</p>	
REALIGN, WIDEN AND REHABILITATE TAXIWAY "C" - PHASE 2 PROPOSED CROSS SECTIONS - TXY C-2 STA. 1+59.50 - STA. 2+00	
29 29 of 29 sheets	