

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAI 55 (INTERSTATE 55)
SECTION: 112-636T
AT NB RAMP TO IL 50 (CICERO AVE)
STORM SEWER REPAIR
COOK COUNTY
C-91-334-12

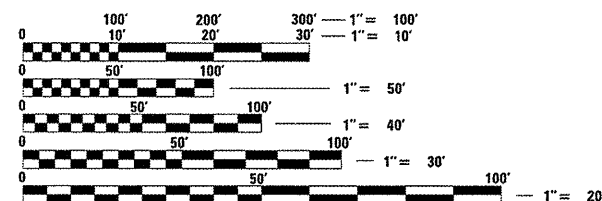
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 60T37	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

TRAFFIC DATA:

EXISTING RAMP ADT= 9900 (2002)
RAMP SPEED LIMIT= 30 MPH

THE IMPROVEMENT IS LOCATED WITHIN
THE VILLAGE OF STICKNEY AND
THE CITY OF CHICAGO



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

DIGGER
CHICAGO UTILITY ALERT NETWORK
1-312-744-7000

PROJECT ENGINEER: DARCIE GABRISKO (STRAND) 815-744-4200
PROJECT MANAGER: KEN ENG (IDOT) 847-705-4247

CONTRACT NO. 60T37

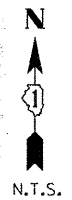


Michael R. Waldron
3/29/12
3/29/12



END IMPROVEMENTS
CICERO AVE. RAMP
STA. 23+09.37

BEGIN IMPROVEMENTS
CICERO AVE. RAMP
STA. 12+24.00



GROSS LENGTH = 1085 FT. = 0.206 MILE
NET LENGTH = 1085 FT. = 0.206 MILE



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

SA
STRAND
ASSOCIATES*

1170 SOUTH HOBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200
IDFPR NO. 184-001273

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED MARCH 30, 2012

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 11, 2012
John D. Baranzelli, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

May 11, 2012
William R. Frey, Jr.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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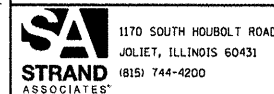
INDEX OF SHEETS

<u>DESCRIPTION</u>	
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS
3	GENERAL NOTES
4-8	SUMMARY OF QUANTITIES
9-10	TYPICAL SECTIONS
11	SCHEDULE OF QUANTITIES
12	ALIGNMENT, TIES AND BENCHMARKS
13	REMOVAL PLANS
14-15	PLAN AND PROFILE
16	EROSION AND SEDIMENT CONTROL DETAILS
17-18	EROSION CONTROL AND PERMANENT LANDSCAPE PLAN
19-21	DRAINAGE AND GRADING PLAN AND PROFILE
22	PAVEMENT MARKING, SIGNING AND LANDSCAPING PLAN
23	BD-07 - STORM SEWER CONNECTION TO EXISTING SEWER
24	BD-08 - DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
25	BD-32 - BUTT JOINT AND HMA TAPER DETAILS
26	BD-33 - HMA TAPER AT EDGE OF P.C.C. PAVEMENT
27	BM-20 - PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE
28	TC-08 - FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS
29-30	TC-12 - MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS
31	TC-13 - DISTRICT ONE TYPICAL PAVEMENT MARKINGS
32	TC-17 - TRAFFIC CONTROL FOR SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES
33	TS-07 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING
34-36	CROSS SECTIONS

HIGHWAY STANDARDS

000001 - 00	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001 - 00	TEMPORARY EROSION CONTROL SYSTEMS
442201 - 03	CLASS C AND D PATCHES
542301 - 03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602001 - 02	CATCH BASIN TYPE A
602011 - 02	CATCH BASIN TYPE C
604001 - 03	FRAME AND LIDS TYPE 1
604036 - 02	GRATE TYPE 8
604091 - 02	FRAME AND GRATE, TYPE 24
606001 - 04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101 - 02	OFF-RD OPERATIONS, MULTILANE, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701106 - 02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5M) AWAY
701411 - 08	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS LESS THAN OR EQUAL TO 45 MPH
701427	LANE CLOSURE, MULTILANE, INTERMITTANT OR MOVING OPER., FOR SPEEDS LESS THAN OR EQUAL TO 40 MPH
701901 - 02	TRAFFIC CONTROL DEVICES
720001 - 01	SIGN PANEL MOUNTING DETAILS
720006 - 03	SIGN PANEL ERECTION DETAILS
781001 - 03	TYPICAL PAVEMENT MARKINGS

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	DATE - 4-2-12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 NORTHBOUND EXIT RAMP
INDEX OF SHEETS, HIGHWAY STANDARDS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	2
CONTRACT NO. 60T37			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE ACCORDING TO THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JAN. 1, 2012, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2012, THE DETAILS IN THESE PLANS, AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.
2. THE SCALE SHOWN ON THE DRAWINGS APPLIES ONLY TO FULL SIZE PLANS AND NOT TO THE REDUCED SIZE PLANS. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
3. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
5. THE LOCATION AND ELEVATION OF THE VARIOUS UNDERGROUND UTILITIES AS SHOWN ON THE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL USE SPECIAL CARE WHEN CONDUCTING CONSTRUCTION OPERATIONS NEAR THEM AND IS RESPONSIBLE FOR ANY DAMAGE CAUSED.
6. THE CONTRACTOR SHALL NOTIFY THE AGENCIES AND UTILITIES AT LEAST 10 DAYS PRIOR TO ANY CONSTRUCTION IN THE AREA AND SHALL COMPLY WITH ALL RESTRICTIONS FOR EQUIPMENT MOVEMENTS AND CLEARANCES IN REGARDS TO THEIR FACILITIES.
7. 48 HOURS PRIOR TO THE START OF ANY EXCAVATION, THE CONTRACTOR SHALL CALL DIGGER AND THE CITY OF CHICAGO FOR VERIFICATION OF ALL UTILITY LOCATIONS IN THE FIELD.
8. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
9. THE CONTRACTOR SHALL ENSURE ALL PERMITS HAVE BEEN OBTAINED PRIOR TO COMMENCEMENT OF WORK
10. WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKER MONUMENTS UNTIL THE OWNER, AN AUTHORIZED AGENT, OR LAND SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR WILL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR REESTABLISH ANY SECTION OR SUB-SECTION MONUMENTS DISRUPTED BY THEIR OPERATIONS.
11. THE CONTRACTOR SHALL ENSURE THAT POSITIVE DRAINAGE IS MAINTAINED AT ALL TIMES DURING AND AFTER THE CONSTRUCTION OF THE PROPOSED COMBINATION CONCRETE CURB AND GUTTER. THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE APPLICABLE TYPE OF IMPROVEMENTS.
12. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
13. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
14. WHENEVER A SEWER CROSSES UNDER A WATER MAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATER MAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SEWERS AND WATER MAINS SHALL BE MAINTAINED UNLESS; THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATER MAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18" VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CANNOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATER MAIN, THE SEWER SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS.
15. THE CITY OF CHICAGO WATER DEPARTMENT SHALL BE RESPONSIBLE FOR TURNING WATER MAIN VALVES ON AND OFF. THE CONTRACTOR IS NOT ALLOWED TO TURN THE WATER MAIN VALVES ON AND OFF.
16. THE FRAMES AND LIDS OF EXISTING UTILITY OR DRAINAGE STRUCTURES WITHIN THE LIMITS OF THIS IMPROVEMENT PROJECT THAT ARE NOT NOTED ON THE PLANS FOR REPLACEMENT OR RECONSTRUCTION SHALL BE ADJUSTED TO MATCH THE PROPOSED ROADWAY GRADES. THE FRAMES AND LIDS OF PRIVATE UTILITY STRUCTURES SHALL BE ADJUSTED BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT. HOWEVER, THE CONTRACTOR SHALL COORDINATE WITH PRIVATE UTILITY OWNERS TO HAVE ADJUSTMENTS PERFORMED.
17. ALL STORM SEWER SHALL BE O-RING GASKETED OR EQUAL CONFORMING TO ASTM C443. THIS WORK SHALL BE INCLUDED IN THE COST FOR STORM SEWERS OF THE CLASS, TYPE AND SIZE SPECIFIED.
18. LAYOUT AND STAKING FOR ALL CONSTRUCTION OPERATIONS SHALL BE PROVIDED BY THE CONTRACTOR.
19. TOP SOIL SHALL BE FERTILE, FRIABLE, NATURAL LOAM, SURFACE SOIL, REASONABLY FREE OF SUBSOIL, CLAY LUMPS, BRUSH WEEDS AND OTHER LITTER AND FREE OF ROOT STUMPS, STONES AND OTHER MATERIAL HARMFUL TO PLANT GROWTH, ALONG WITH MEETING THE REQUIREMENTS OF ARTICLE 1081.05 OF THE DEPARTMENTS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
20. CLEARING OF BRUSH, BUSHES AND TREES WITH A DIAMETER LESS THAN 6" WILL NOT BE PAID FOR SEPARATELY PER SECTION 202 OF THE STANDARD SPECIFICATIONS.
21. WHERE SPECIFIED ON THE PLANS, OR DIRECTED BY THE ENGINEER, TRIMMING OF VEGETATION WILL BE CONSIDERED TO BE CLEARING AND NOT MEASURED FOR PAYMENT.
22. THE COST OF CONNECTING EXISTING STORM SEWERS TO THE PROPOSED DRAINAGE SYSTEM AND/OR CONNECTING PROPOSED STORM SEWER TO EXISTING STRUCTURES SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED DRAINAGE STRUCTURE AND/OR THE COST OF THE PROPOSED STORM SEWER. ALL NECESSARY ADDITIONAL PIPE USED WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR STORM SEWER OF THE SIZE REQUIRED.
23. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, OR CATCH BASINS. THEY SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE SAME. THEY SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT ROADSIDE DRAINAGE SYSTEM IS BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
24. DRAINAGE STRUCTURE GRADES SHALL BE VERIFIED IN THE FIELD PRIOR TO INSTALLATION OF DRAINAGE ITEMS. DRAINAGE STRUCTURES MAY REQUIRE REVISIONS TO MEET EXISTING FIELD CONDITIONS. ANY ADJUSTMENT SHALL BE AS DIRECTED BY THE ENGINEER.
25. UNLESS OTHERWISE NOTED, LOCATIONS SHOWN ON THE PLANS ARE TO THE EDGE OF PAVEMENT FOR STRUCTURES IN THE CURB AND GUTTER AND TO THE CENTER OF THE STRUCTURE FOR ALL OTHER STRUCTURES. FLAT TOPS AND CONES ARE TO BE TURNED SO THAT THE FRAME IS CLOSEST TO THE CENTER LINE OF THE ROAD. UNLESS OTHERWISE NOTED ON THE STRUCTURE IN THE PLANS. ALL FLAT TOPS AND CONES ARE ASSUMED TO BE ECCENTRIC.
26. ALL SUBSURFACE DRAINS AND/OR FIELD TILES ENCOUNTERED IN THE ROADWAY EXCAVATION SHALL BE REMOVED AND CONSIDERED TO BE INCLUDED IN THE COST OF EARTH EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CAPPING THE EXISTING SUBSURFACE DRAINS OR CONNECTING TO THE PROPOSED DITCHES AT THE LOCATIONS SHOWN IN THE PLANS OR AS DIRECTED BY THE OWNER. THIS WORK SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
27. THE PROPOSED DRAINAGE STRUCTURES AND PIPES SHALL BE PROTECTED IMMEDIATELY AFTER THEY ARE CONSTRUCTED UNTIL THE SURFACES THAT DRAIN TO THEM ARE FULLY PAVED OR VEGETATED.
28. ALL SIGN RELOCATIONS AND PROPOSED SIGNS SHALL BE PLACED IN THE LOCATION INDICATED IN THE SIGN SCHEDULE IN THE SCHEDULE OF QUANTITIES. ALL SIGNS SHALL BE INSTALLED ACCORDING TO THE MUTCD AND THE ILLINOIS STANDARD SPECIFICATIONS.
29. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AS WELL AS SUPERVISION, DIRECTION AND MEANS/METHOD OF CONSTRUCTION.
30. THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE CITY OF CHICAGO. ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.
31. FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.
32. CONTRACTOR TO REPAIR ANY DAMAGE DUE TO EROSION IN THE AREA OF THE TURF REINFORCEMENT MAT AND SHALL BE INCLUDED IN THE COST OF TOPSOIL FURNISH AND PLACE 4".
33. THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
34. REMOVAL OF PRECAST FLARED END SECTION SHALL BE PAID FOR PER FOOT UNDER STORM SEWER REMOVAL, 10".
35. REFERENCE SPECIAL PROVISION FOR KEEPING EXPRESSWAY OPEN TO ALL TRAFFIC FOR ALLOWABLE PARTIAL AND FULL LANE AND SHOULDER CLOSURES ON THE RAMP.
36. ALL PAVEMENT RESTORATION WORK INCLUDING PAVEMENT AND SURFACE REMOVAL, CURB AND GUTTER REMOVAL AND REPLACEMENT, SUBGRADE IMPROVEMENTS, PAVEMENT PATCHING, AND RESURFACING SHALL BE PERFORMED UNDER FULL RAMP CLOSURES.
37. NO HOLES SHALL BE LEFT OPEN IN THE PAVEMENT OR IN THE CURB AND GUTTER THAT RESULT IN DROP OFFS ADJACENT TO TRAVEL LANES GREATER THAN 4 INCHES WHEN THE RAMP IS OPEN TO TRAFFIC.
38. THE CONTRACTOR SHALL COORDINATE WITH ALL ABOVE AND UNDERGROUND UTILITY COMPANIES IF UTILITIES NEED TO BE RELOCATED OR ADJUSTED.
39. THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, AT LEAST (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.
40. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 4 INCHES (101.6 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
41. ALL SAW CUTS NECESSARY FOR PAVEMENT PATCHING OR CURB AND GUTTER REMOVAL SHALL BE INCIDENTAL TO PAVEMENT PATCHING AND NOT MEASURED SEPARATELY FOR PAYMENT.

 DENOTES WORK INCLUDED IN THE COST OF OTHER ITEMS.

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PLLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-55 NORTHBOUND EXIT RAMP
GENERAL NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	3
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

URBAN 0040

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONST. CODE
				100% STATE
				DRAINAGE
				0044
				URBAN
• 20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	35	35
• 20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	5	5
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	296	296
20400800	FURNISHED EXCAVATION	CU YD	75	75
20800150	TRENCH BACKFILL	CU YD	33	33
• 21101605	TOPSOIL FURNISH AND PLACE, 2"	SO YD	505	505
• 21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	1, 214	1, 214
• 25000210	SEEDING, CLASS 2A	ACRE	. 25	. 25
• 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	23	23
• 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	23	23
• 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	23	23
• 25100630	EROSION CONTROL BLANKET	SO YD	1, 214	1, 214
• 25100900	TURF REINFORCEMENT MAT	SO YD	505	505
• 28000305	TEMPORARY DITCH CHECKS	FOOT	96	96

• SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-55 NORTHBOUND EXIT RAMP SUMMARY OF QUANTITIES		
SCALE: N/A	SHEET 1 OF 4 SHEETS	STA. TO STA.

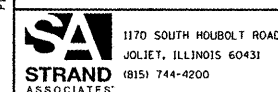
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	4
CONTRACT NO. 6037				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONST. CODE
				100% STATE
				DRAINAGE
				0044
				URBAN
• 28000500	INLET AND PIPE PROTECTION	EACH	3	3
• 28000510	INLET FILTERS	EACH	13	13
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SO YD	448	448
35800100	PREPARATION OF BASE	SO YD	2,438	2,438
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	509	509
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	3	3
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	60	60
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	226	226
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	234	234
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	127	127
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	232	232
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	374	374
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	1,161	1,161
54213655	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 10"	EACH	1	1

• SPECIALTY ITEM

Rev.



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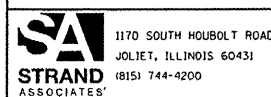
I-55 NORTHBOUND EXIT RAMP SUMMARY OF QUANTITIES		
SCALE: N/A	SHEET 2 OF 4 SHEETS	STA. TO STA.

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55	112-636T	COOK	36	5
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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONST. CODE
				100% STATE
				DRAINAGE
				0044 URBAN
550A0330	STORM SEWERS, CLASS A, TYPE 2 10"	FOOT	131	131
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	14	14
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	456	456
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	40	40
55100400	STORM SEWER REMOVAL 10"	FOOT	133	133
55100500	STORM SEWER REMOVAL 12"	FOOT	8	8
55100900	STORM SEWER REMOVAL 18"	FOOT	40	40
60200205	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1
60200805	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 8 GRATE	EACH	2	2
60201340	CATCH BASINS, TYPE A, 4' -DIAMETER, TYPE 24 FRAME AND GRATE	EACH	2	2
60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	3	3
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1
60500050	REMOVING CATCH BASINS	EACH	4	4
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	232	232

* SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 NORTHBOUND EXIT RAMP
SUMMARY OF QUANTITIES

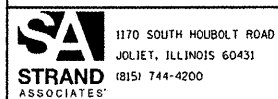
SCALE: N/A SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	6
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T37	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONST. CODE	
				100% STATE	
				DRAINAGE	
				0044	URBAN
• 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	70	70	
• 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1	
• 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3	
67100100	MOBILIZATION	L SUM	1	1	
• 70106800	CHANGEABLE MESSAGE SIGN	CAL MO	3	3	
• 70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,308	1,308	
• 70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	648	648	
• 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	2	2	
• 72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1	
• 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	146	146	
• 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1,286	1,286	
• 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	527	527	
• 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	64	64	

• SPECIALTY ITEM

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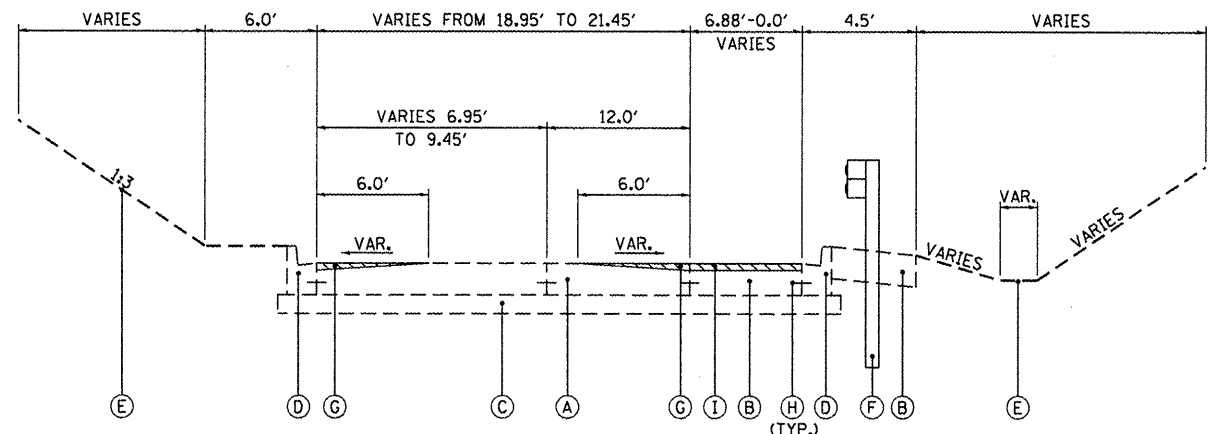
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	DRAWN - DJW	REVISED -
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PLOT DATE = 4/2/2012	DATE - 4-2-12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

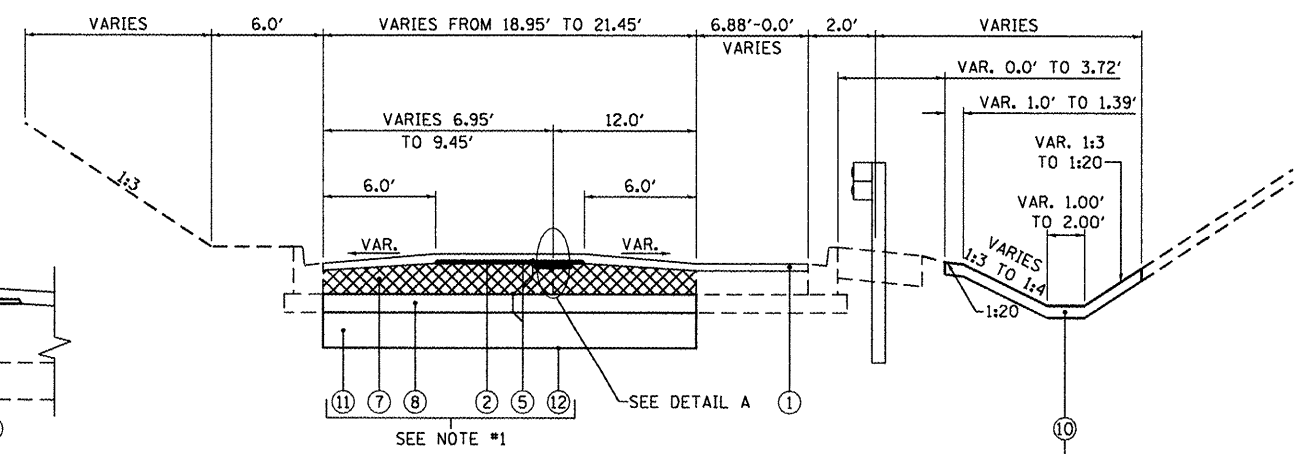
I-55 NORTHBOUND EXIT RAMP
SUMMARY OF QUANTITIES

SCALE: N/A SHEET 4 OF 4 SHEETS STA. TO STA.

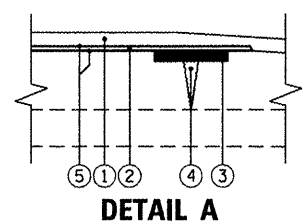
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	7
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T37	



- EXISTING LEGEND**
- (A) PCC PAVEMENT 10"
 - (B) HMA SHOULDER 10"
 - (C) 6" (NOMINAL) SUB-BASE GRANULAR MATERIAL TYPE "A"
 - (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - (E) GROUND
 - (F) GUARDRAIL
 - (G) PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) VARIABLE DEPTH
 - (H) TIE BARS
 - (I) HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
 - REMOVAL ITEM



- PROPOSED LEGEND**
- ① 1/2" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
 - ② 3/4" LEVELING BINDER (MACHINE METHOD), N70
 - ③ STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - ④ MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS
 - ⑤ BITUMINOUS MATERIALS (PRIME COAT)
 - ⑥ COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
 - ⑦ CLASS D PATCHES, TYPE IV, 10 INCHES
 - ⑧ SUBBASE GRANULAR MATERIAL, TYPE B 6"
 - ⑨ ~~PREPARATION OF BASE~~ DELETED
 - ⑩ TOP SOIL FURNISH AND PLACE, 4"
 - ⑪ 12" POROUS GRANULAR EMBANKMENT, SUBGRADE
 - ⑫ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION



HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5MM); 1.5"	4% @ 70 GYR.
LEVELING BINDER (MACHINE METHOD), N70 (IL-9.5MM); 3/4"	4% @ 70 GYR.
PATCHES	
CLASS D PATCHES (HMA BINDER IL-25MM)	4% @ 105 GYR.

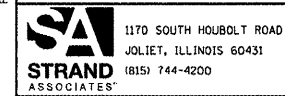
THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SO YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

NOTES

1. REFER TO PLAN AND PROFILE SHEETS OF ENGINEERING DRAWINGS TO SEE PAVEMENT PATCHING LOCATIONS AND QUANTITIES
2. REFER TO REMOVAL PLAN SHEETS OF ENGINEERING DRAWINGS TO SEE COMBINATION CURB & GUTTER REMOVAL LOCATIONS AND LENGTHS.
3. REFER TO EROSION CONTROL AND PERMANENT LANDSCAPING PLAN TO SEE TOPSOIL FURNISH AND PLACE LOCATIONS.

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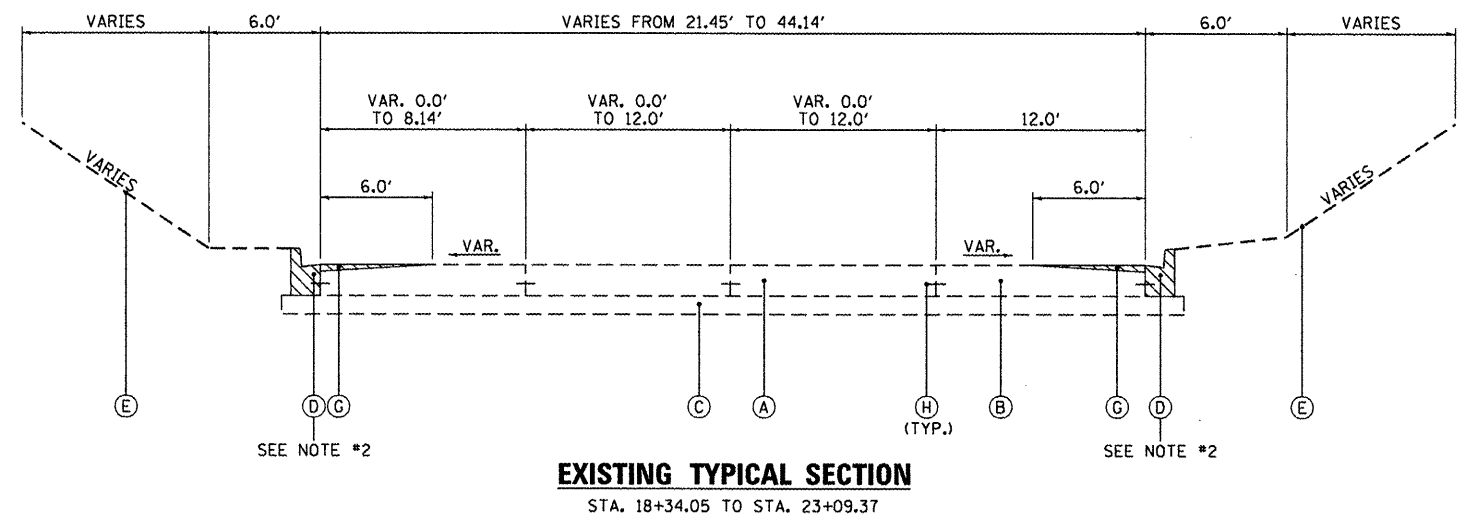


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PLDT SCALE = 10.0000' / 1"	DRAWN - DJW	REVISED -
PLDT DATE = 4/11/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

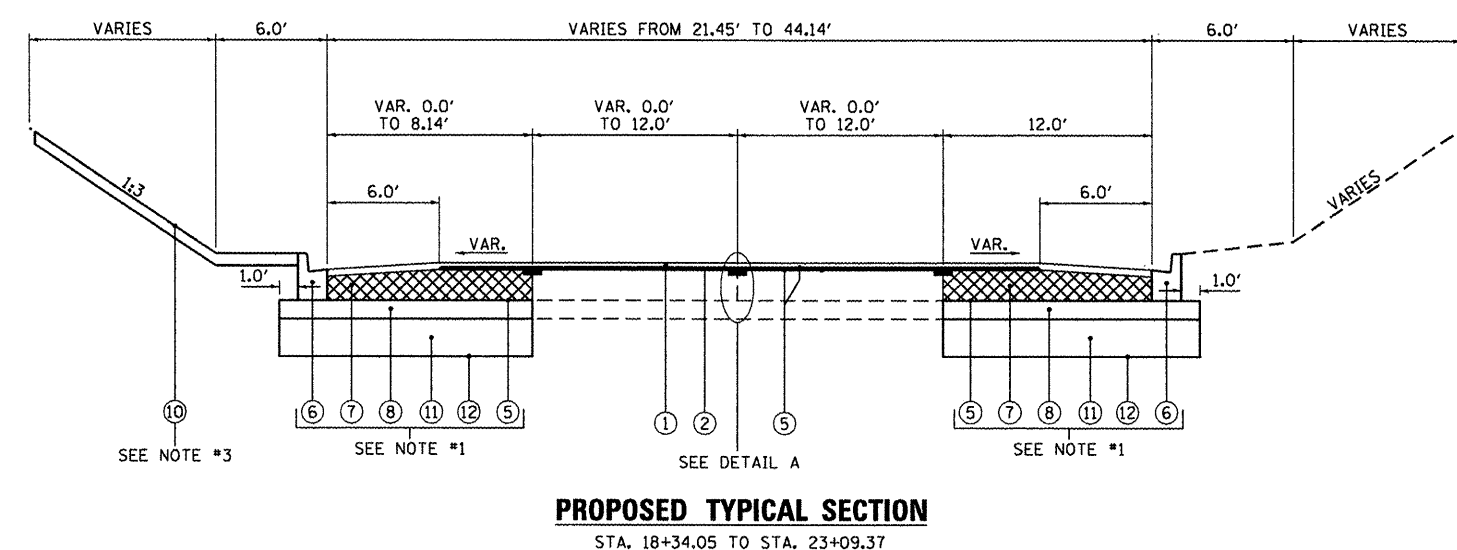
I-55 NORTHBOUND EXIT RAMP	
TYPICAL SECTIONS	
SCALE: NTS	SHEET 1 OF 2 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	9
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION
STA. 18+34.05 TO STA. 23+09.37

- EXISTING LEGEND**
- (A) PCC PAVEMENT 10"
 - (B) HMA SHOULDER 10"
 - (C) 6" (NOMINAL) SUB-BASE GRANULAR MATERIAL TYPE "A"
 - (D) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - (E) GROUND
 - (F) GUARDRAIL
 - (G) PORTLAND CEMENT CONCRETE SURFACE REMOVAL (COLD MILLING) VARIABLE DEPTH
 - (H) TIE BARS
 - (I) HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
 - [Hatched Box] REMOVAL ITEM

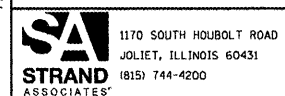


PROPOSED TYPICAL SECTION
STA. 18+34.05 TO STA. 23+09.37

- PROPOSED LEGEND**
- (1) 1/2" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
 - (2) 3/4" LEVELING BINDER (MACHINE METHOD), N70
 - (3) STRIP REFLECTIVE CRACK CONTROL TREATMENT
 - (4) MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS
 - (5) BITUMINOUS MATERIALS (PRIME COAT)
 - (6) COMBINATION CONCRETE CURB & GUTTER TYPE B-6.12
 - (7) CLASS D PATCHES, TYPE IV, 10 INCHES
 - (8) SUBBASE GRANULAR MATERIAL, TYPE B 6"
 - (9) ~~PREPARATION OF BASE~~ DELETED
 - (10) TOP SOIL FURNISH AND PLACE, 4"
 - (11) 12" POROUS GRANULAR EMBANKMENT, SUBGRADE
 - (12) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

- NOTES**
1. REFER TO PLAN AND PROFILE SHEETS OF ENGINEERING DRAWINGS TO SEE PAVEMENT PATCHING LOCATIONS AND QUANTITIES
 2. REFER TO REMOVAL PLAN SHEETS OF ENGINEERING DRAWINGS TO SEE COMBINATION CURB & GUTTER REMOVAL LOCATIONS AND LENGTHS.
 3. REFER TO EROSION CONTROL AND PERMANENT LANDSCAPING PLAN TO SEE TOPSOIL FURNISH AND PLACE LOCATIONS.

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	DRAWN - DJW	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - MAG	REVISED -
PLOT DATE = 4/11/2012	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-55 NORTHBOUND EXIT RAMP
TYPICAL SECTIONS**

SCALE: NTS SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE. 55	SECTION 112-636T	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 10
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TREE REMOVAL (6 TO 15 UNITS DIAMETER)				20100110
LOCATION			TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNITS
STATION	OFFSET	RT/LT		
22+13.91	-74.26	LT		8
22+15.50	-74.45	LT		8
22+18.39	-76.81	LT		7
22+25.86	-83.62	LT		6
22+26.93	-79.10	LT		6
TOTAL				35

STORM SEWERS TO BE CLEANED 10"					X5537700
LOCATION	UPSTREAM	DOWNSTREAM	RT/LT	LENGTH (FT)	STORM SEWERS TO BE CLEANED 10"
STATION	OFFSET	OFFSET	RT/LT		
12+52.77	6.72	17.13	RT		10.41
14+14.25	6.37	18.44	RT		12.07
15+59.78	7.17	13.04	RT		5.87
TOTAL					29.00

SIGNS						72400100	72400600
EXISTING LOCATION		PROPOSED LOCATION		DESCRIPTION	REMOVE SIGN PANEL ASSEMBLY - TYPE A	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	
STATION	OFFSET	STATION	OFFSET		EACH	EACH	
17+07.70	13.19	---	---	NON-STANDARD	1	---	
19+46.94	6.04	19+46.94	6.04	AIRPORT DIRECTION AND TURN DIRECTION	---	1	
21+77.47	49.37	---	---	NON-STANDARD	1	---	
TOTAL					2	1	

TRENCH BACKFILL				20800150
STATION		PIPE SIZE (IN)	TRENCH BACKFILL	CU YD
UPSTREAM	DOWNSTREAM			
15+59.80	15+59.80	10		0.3
19+31.35	19+80.85	18		4.9
21+19.93	21+68.44	12		3.0
21+68.44	21+72.55	10		1.1
21+68.44	21+78.79	10		2.4
21+68.96	22+07.66	10		19.0
22+07.66	22+13.67	12		2.0
TOTAL				33.0

SUBBASE GRANULAR MATERIAL, TYPE B 6"							31101400
LOCATION						SUBBASE GRANULAR MATERIAL, TYPE B 6"	AREA (SQ YD)
TYPE	STATION	STATION	RT/LT	LENGTH	WIDTH		
PATCH	17+57.78	18+08.78	LT	17.00	6.64		112.85
PATCH	19+25.84	19+95.24	RT	23.13	4.00		92.53
PATCH	21+09.91	22+10.49	RT	33.53	4.00		134.11
PATCH	21+62.26	22+10.49	LT	16.08	2.60		41.80
CURB	19+25.84	19+95.24	RT	23.13	0.86		19.89
CURB	21+09.91	22+10.49	RT	33.53	0.86		28.83
CURB	21+62.26	22+23.88	LT	20.54	0.86		17.66
TOTAL							448.00

LOCATION		REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL 20201200	TOPSOIL FURNISH AND PLACE 2" 21101605	TOPSOIL FURNISH AND PLACE 4" 21101615	FURNISHED EXCAVATION 20400800	EROSION CONTROL BLANKET 25100630
STATION	STATION	CU YD	SO YD	SO YD	CU YD	SO YD
12+24	12+50	3.02		18.37	0.46	18.37
12+50	13+00	9.35		61.63	1.94	61.63
13+00	13+50	9.93		54.21	4.35	54.21
13+50	14+00	17.84		62.79	10.89	62.79
14+00	14+50	17.28		79.88	13.99	79.88
14+50	15+00	10.72		85.71	13.62	85.71
15+00	15+50	9.29		76.33	12.89	76.33
15+50	16+00	7.68		62.96	7.86	62.96
16+00	16+50	8.34		64.67	4.70	64.67
16+50	17+00	8.21		61.92	3.43	61.92
17+00	17+07	0.88		6.88	0.26	6.88
17+57.78	18+08.78	50.17				
19+25.82	19+95.21	46.27		33.53		33.53
21+09.91	22+10.49	72.02		23.13		23.13
21+62.26	22+23.88	24.53		16.08		16.08
21+67.97	22+37.97			505.00		505.00
TOTALS		295.5		505.0	1213.1	74.4

TEMPORARY DITCH CHECKS				28000305
LOCATION			TEMPORARY DITCH CHECKS	FOOT
STATION	OFFSET	RT/LT		
12+48.69	18.53	RT		12
12+99.66	18.55	RT		12
14+01.66	18.85	RT		12
14+52.72	13.04	RT		12
15+03.71	13.03	RT		12
15+54.74	13.03	RT		12
16+05.69	12.19	RT		12
16+56.73	11.25	RT		12
TOTAL				96

DRAINAGE STRUCTURES TO BE CLEANED				Z0018500
LOCATION			DRAINAGE STRUCTURES TO BE CLEANED	EACH
STATION	OFFSET	RT/LT		
12+52.78	6.73	RT		1
14+14.25	6.37	RT		1
15+59.78	7.17	RT		1
TOTAL				3

INLET AND PIPE PROTECTION				28000500
LOCATION			INLET AND PIPE PROTECTION	EACH
STATION	OFFSET	RT/LT		
12+52.77	18.55	RT		1
15+59.87	13.04	RT		1
17+07.66	10.32	RT		1
TOTAL				3

TREE PRUNING (1 TO 10 INCH DIAMETER)				20101300
LOCATION			TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH
STATION	OFFSET	RT/LT		
19+49.98	21.23	RT		1
19+68.64	24.13	RT		1
21+53.33	19.94	RT		1
21+62.54	17.08	RT		1
21+78.19	18.17	RT		1
TOTAL				5

INLET FILTERS				28000510
LOCATION			INLET FILTERS	EACH
STATION	OFFSET	RT/LT		
12+52.78	6.73	RT		1
14+14.29	6.32	RT		1
15+59.80	7.17	RT		1
17+07.54	7.49	RT		1
17+88.55	0.09	RT		1
18+06.94	4.16	RT		1
19+30.38	0.42	RT		1
19+81.87	0.29	RT		1
21+19.93	0.00	RT		1
21+68.44	0.00	RT		1
21+78.79	0.00	RT		1
22+07.66	45.62	LT		1
22+13.67	45.62	LT		1
TOTAL				13

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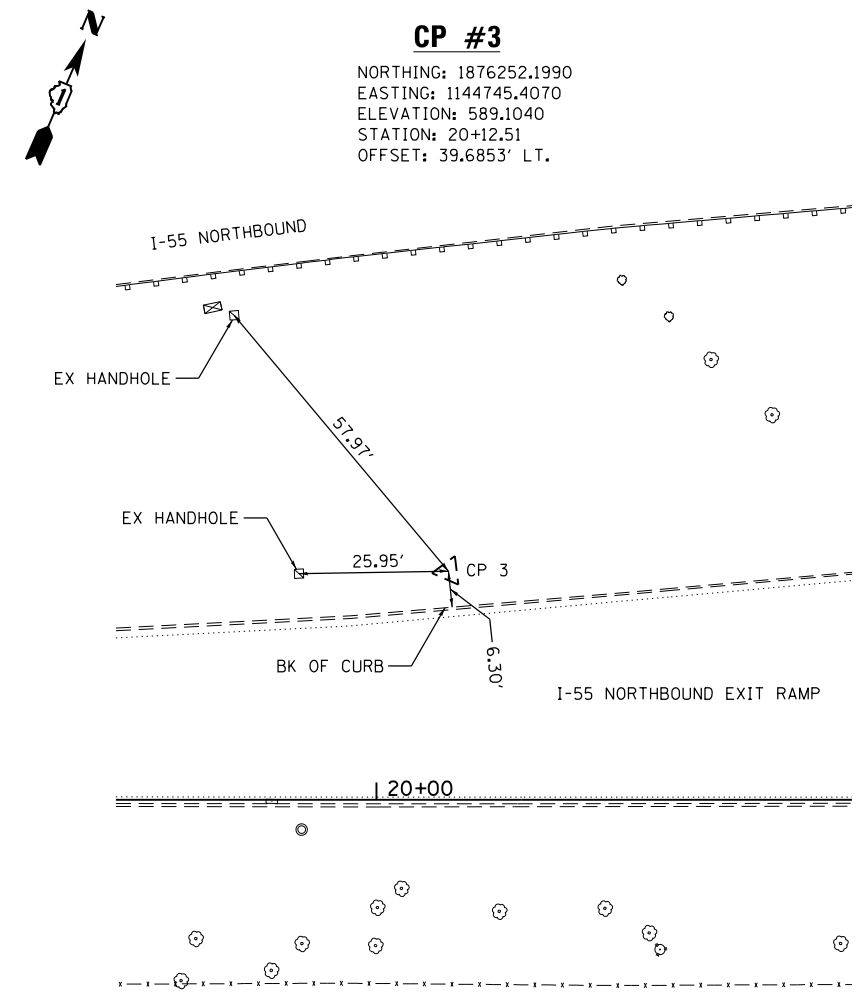
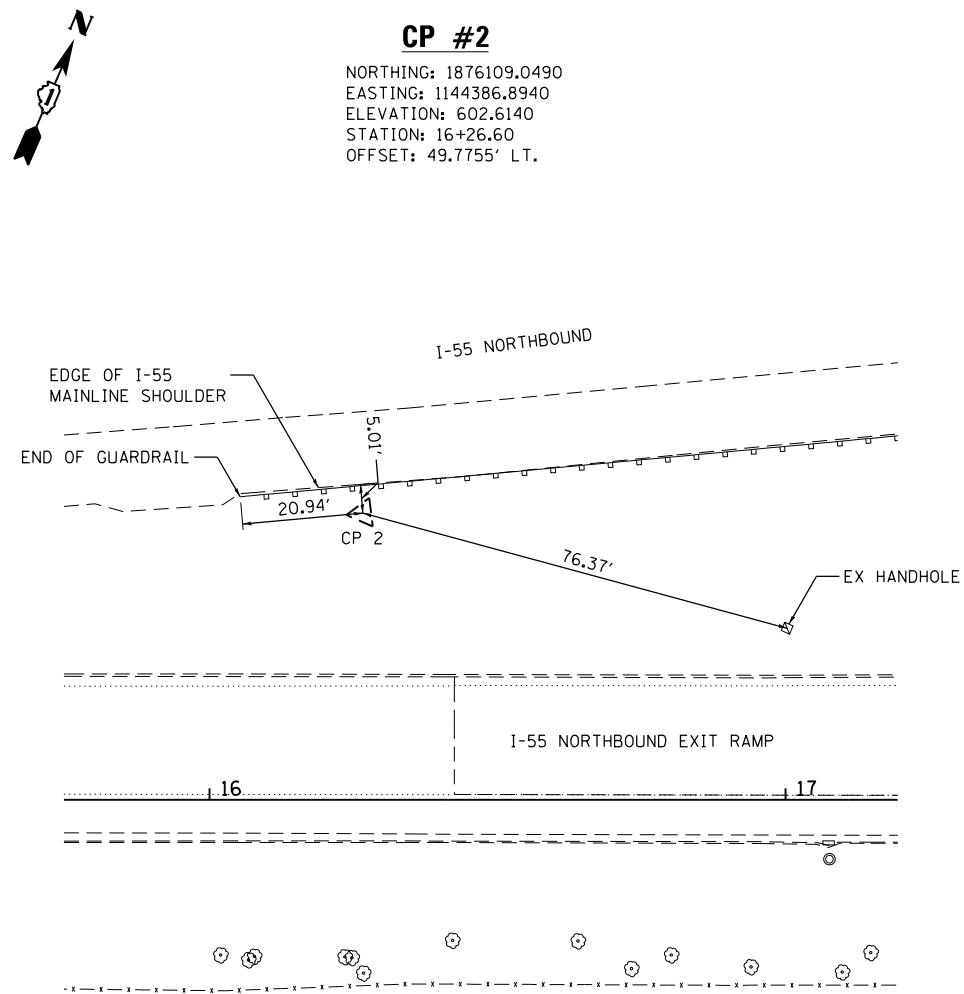
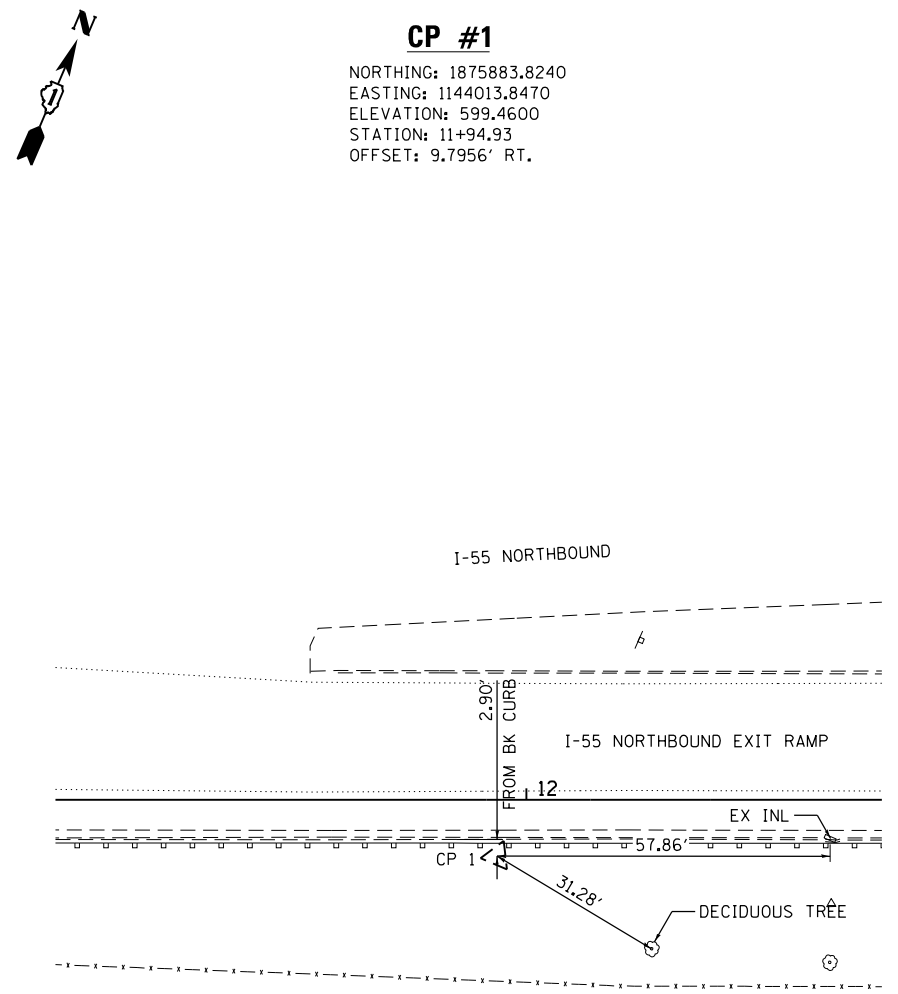
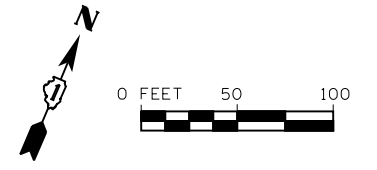
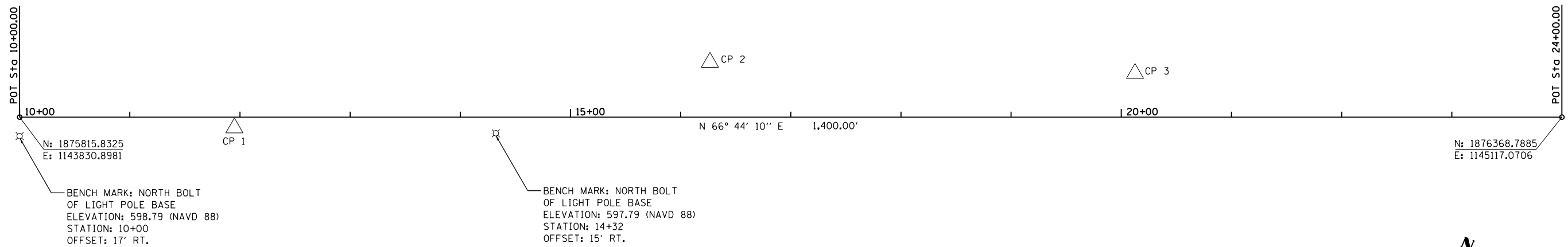
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PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 NORTHBOUND EXIT RAMP
SCHEDULE OF QUANTITIES

SCALE: N/A SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	11
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T37	



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USER NAME = dennisw	DESIGNED - TJS	REVISED -
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PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 NORTHBOUND EXIT RAMP
ALIGNMENT, TIES AND BENCHMARKS

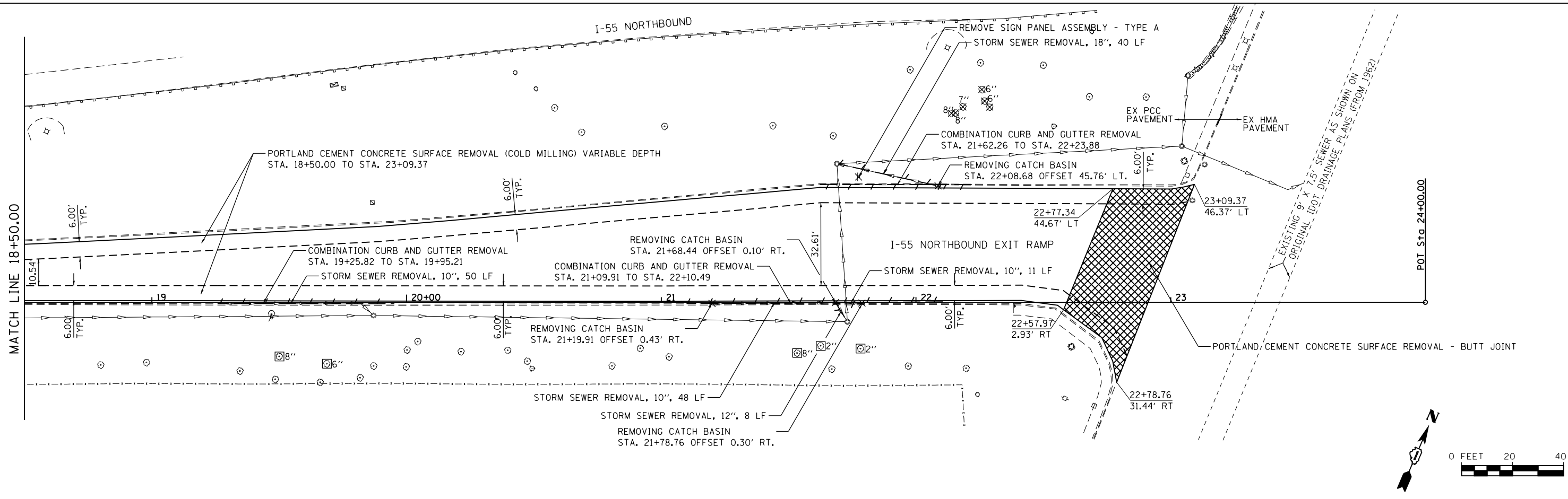
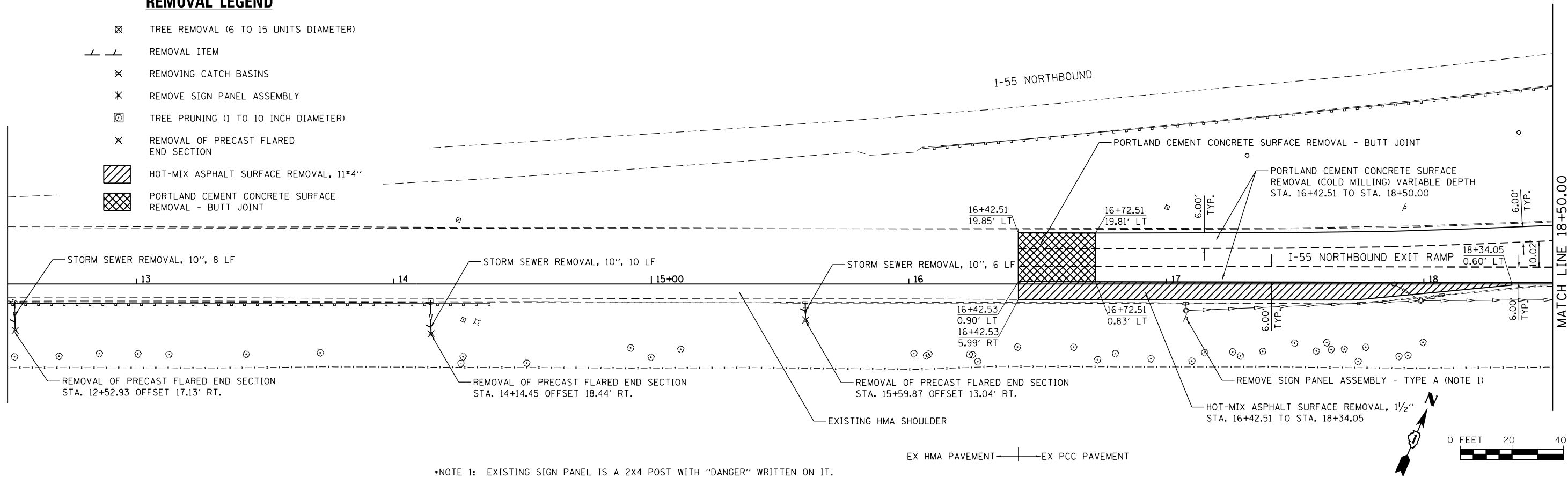
SCALE: 1" = 100'

SHEET 1	OF 1	SHEETS	STA.	TO STA.
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	12
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

REMOVAL LEGEND

- ⊗ TREE REMOVAL (6 TO 15 UNITS DIAMETER)
- / — REMOVAL ITEM
- ✕ REMOVING CATCH BASINS
- ✕ REMOVE SIGN PANEL ASSEMBLY
- ⊗ TREE PRUNING (1 TO 10 INCH DIAMETER)
- ✕ REMOVAL OF PRECAST FLARED END SECTION
- ▨ HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" THICK
- ▩ PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT



FILE NAME = SA\JUL16300-6399\6346-035\Micros\CADD Sheets\01680391-sht-rem1.dgn

SA STRAND ASSOCIATES
 1170 SOUTH HOUBOLT ROAD
 JOLIET, ILLINOIS 60431
 (815) 744-4200

USER NAME = dennisw	DESIGNED - TJS	REVISED -
PLOT SCALE = 48.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-55 NORTHBOUND EXIT RAMP
 REMOVAL PLANS**

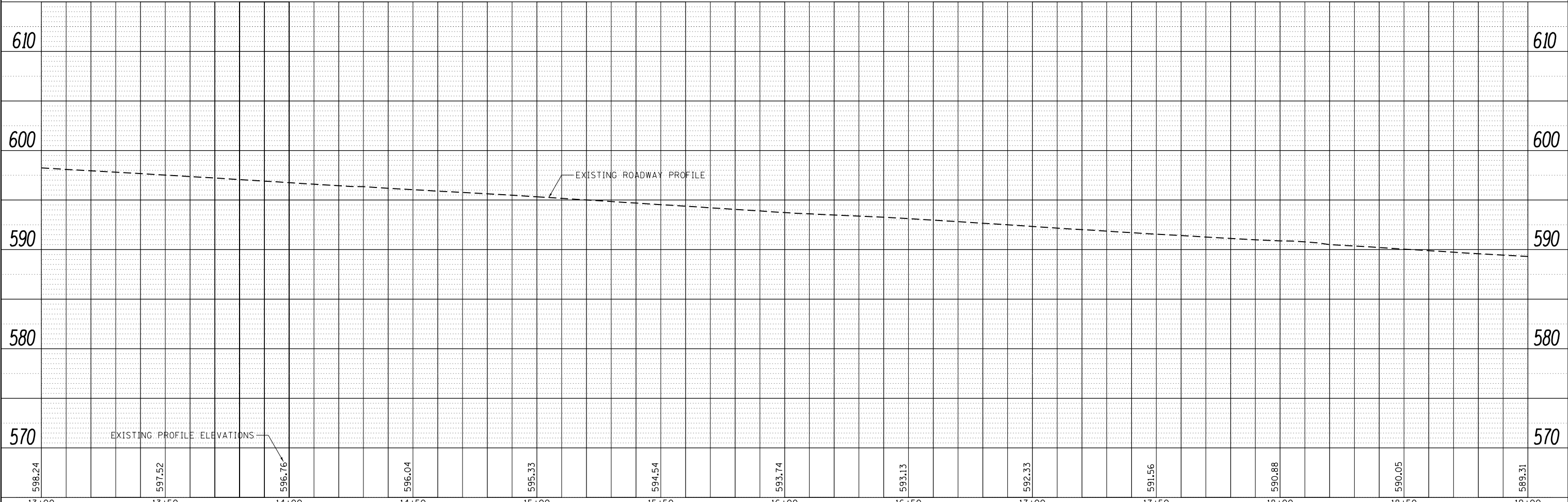
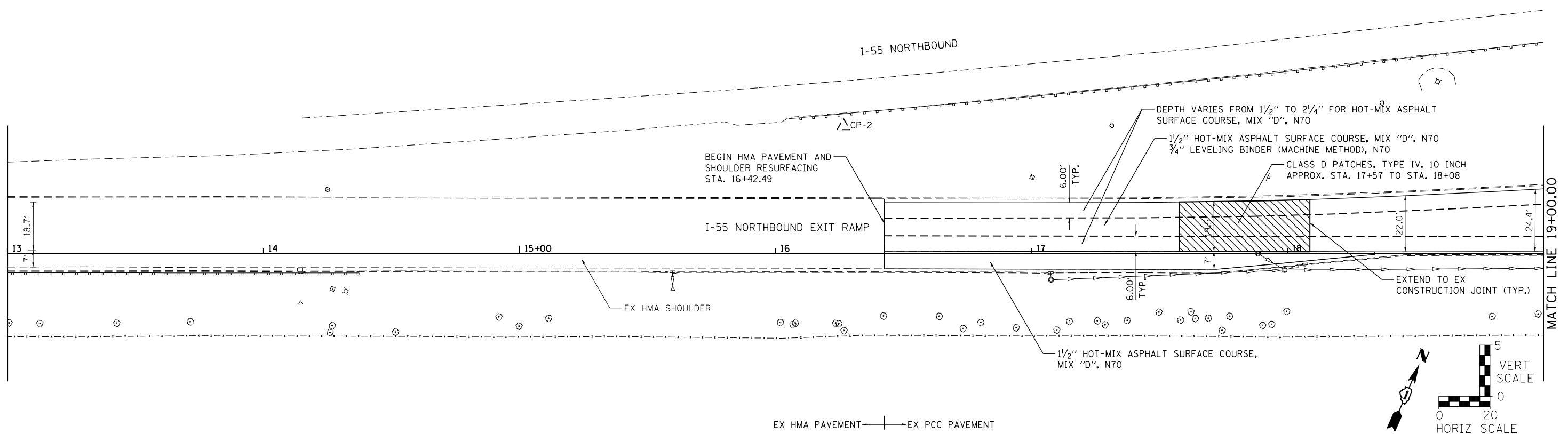
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F.A.I. RTE. 55	SECTION 112-636T	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 13
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOT AT THIS OFFICE	
	NO. _____	
	NOTE BOOK	
	NO. _____	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOT AT THIS OFFICE	
	NO. _____	
	NOTE BOOK	
	NO. _____	
	FILE NAME	

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13+00	13+50	14+00	14+50	15+00	15+50	16+00	16+50	17+00	17+50	18+00	18+50	19+00
598.24	597.52	596.76	596.04	595.33	594.54	593.74	593.13	592.33	591.56	590.88	590.05	589.31

STRAND ASSOCIATES*
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = dennisw	DESIGNED - TJS	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

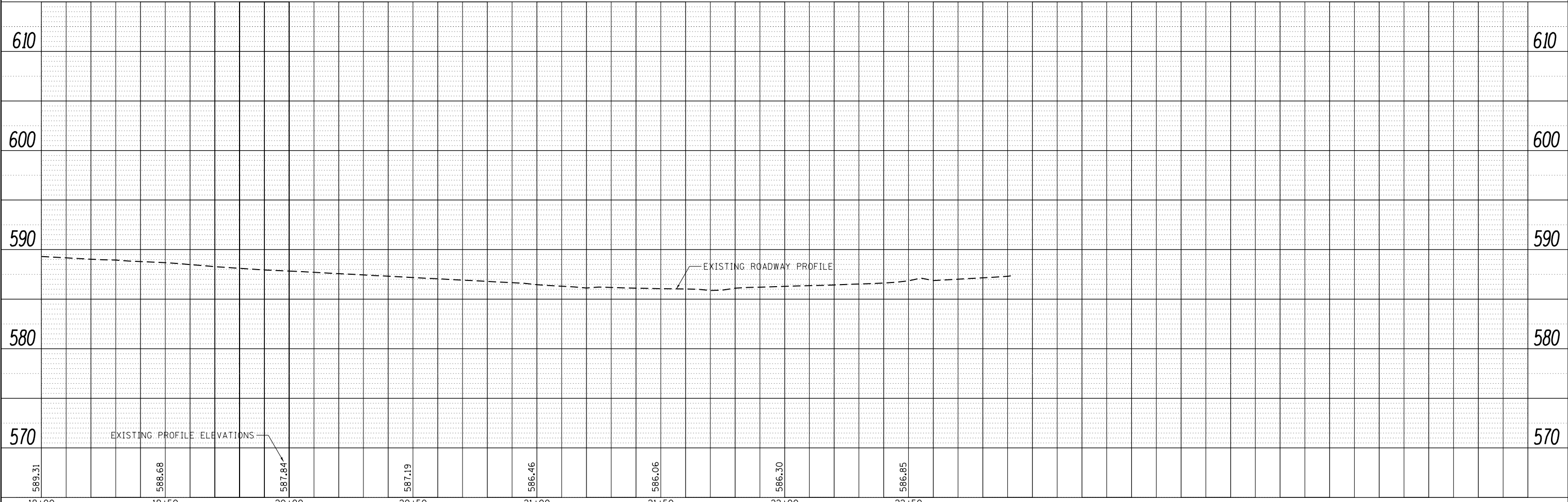
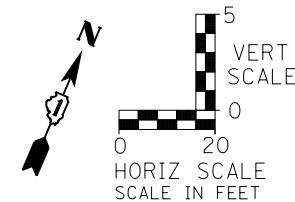
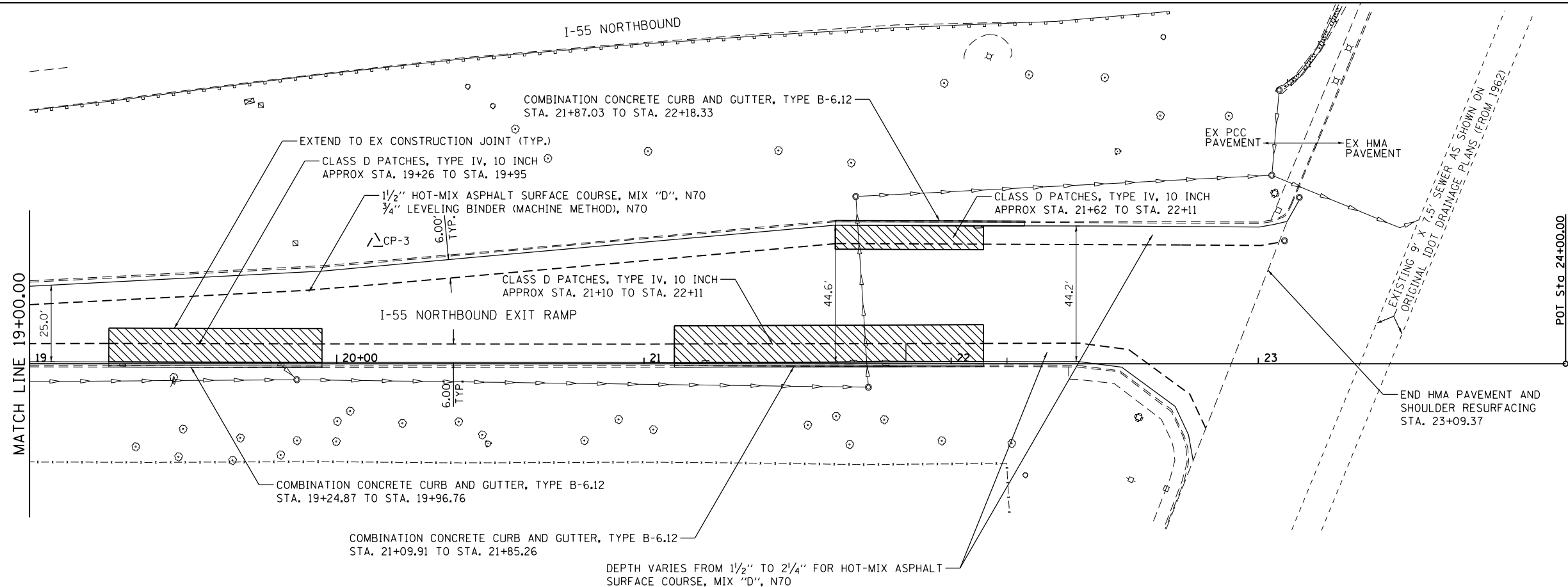
I-55 NORTHBOUND EXIT RAMP ROADWAY PLAN & PROFILE		
SCALE: 1" = 20'	SHEET 1 OF 2 SHEETS	STA. 13+00.00 TO STA. 19+00.00

F.A.I. RTE. 55	SECTION 112-636T	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 14
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T37	

PLAN	SURVEYED	DATE
	PLOTTED	BY
	ALIGNED	
	CHECKED	
	FILED	
	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	
	NO.	
	FILE NAME	

FILE NAME = SA:\JOL\63800-63999\6346\035\Microa\CA00_Sheets\01608039-shr-hr-pln-prf2.dgn



STRAND ASSOCIATES*
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = dennisw	DESIGNED - TJS	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-55 NORTHBOUND EXIT RAMP
ROADWAY PLAN & PROFILE**

SCALE: 1" = 20' SHEET 1 OF 2 SHEETS STA. 19+00.00 TO STA. 24+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	15
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

EROSION AND SEDIMENT CONTROL

1. ALL AREAS WITHIN R.O.W. DISTURBED WILL BE RESTORED WITH 4' TOPSOIL AND SEEDING, CLASS 2A.
2. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE PROJECT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
3. PROPERTIES AND CHANNELS ADJOINING THE PROJECT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION.
4. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
5. DISTRIBUTED AREAS SHALL BE STABILIZED WITH PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
6. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
7. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
8. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DISPOSAL OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIALS DEBRIS UPON OR INTO ANY DEVELOPMENT SITE. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
9. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.
10. INLET FILTERS TO BE INSTALLED BY CONTRACTOR AFTER PROPOSED WORK IS COMPLETED ON DRAINAGE STRUCTURES. IF NO WORK IS PROPOSED ON A DRAINAGE STRUCTURE, THE INLET FILTER SHALL BE INSTALLED BEFORE WORK BEGINS.

EROSION AND SEDIMENT CONTROL MAINTENANCE

1. EROSION AND SEDIMENT CONTROL: AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL EROSION AND SEDIMENT CONTROL WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP ALL EROSION AND SEDIMENT CONTROL FUNCTIONING AS DESIGNED.

CONSTRUCTION SEQUENCE

1. INSTALL SEDIMENT AND EROSION CONTROL SYSTEMS
2. COMPLETE TREE REMOVAL, CLEARING, AND GRUBBING
3. REMOVE EXISTING CURB AND GUTTER, PAVEMENT, AND STORM SEWER PIPE AND STRUCTURES.
4. CONSTRUCT PROPOSED STORM SEWER PIPES AND STRUCTURES, CURB & GUTTERS AND PATCHWORK.
5. BEGIN MASS GRADING.
6. EDGE GRIND PAVEMENT.
7. RESURFACE PPC RAMP SEGMENT WITH LEVELING BINDER AND HMA SURFACE COURSE.
8. COMPLETE PAVEMENT MARKINGS AND RESTORATION.
9. REMOVE EROSION CONTROL MEASURES AND RESTORE.

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


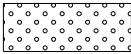
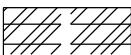
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	DRAWN - DJW	REVISED -
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PLOT DATE = 4/2/2012	DATE - 4-2-12	REVISED -

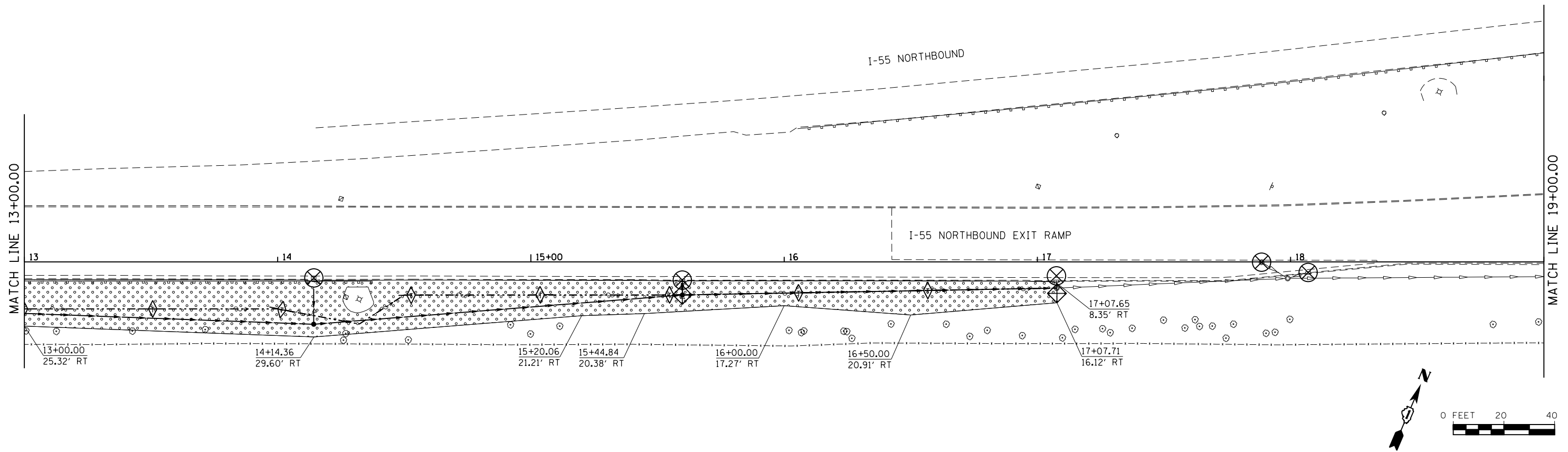
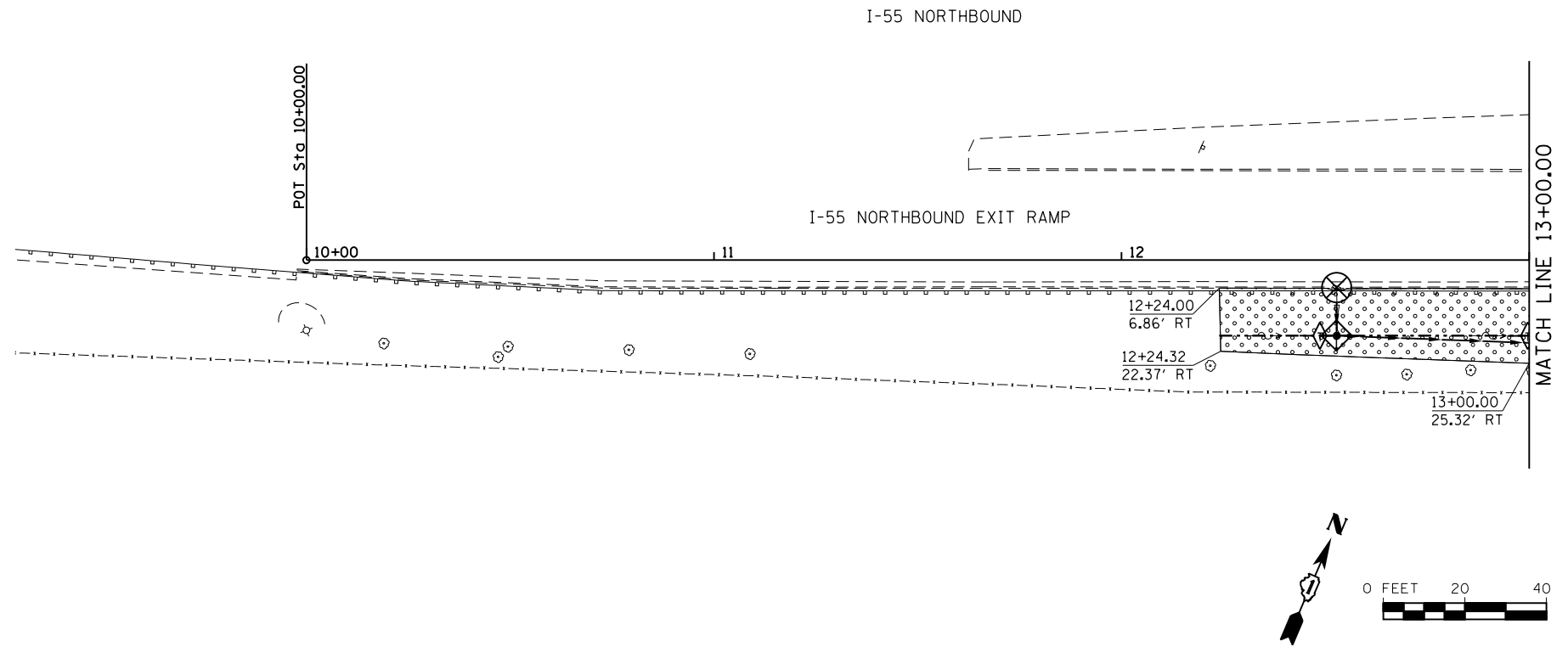
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-55 NORTHBOUND EXIT RAMP
EROSION AND SEDIMENT CONTROL DETAILS**

SCALE: N/A SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	16
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60T37	

- LEGEND**
-  TEMPORARY DITCH CHECKS
 -  INLET AND PIPE PROTECTION
 -  INLET FILTER
 -  SEEDING CLASS 2A
EROSION CONTROL BLANKET
 -  TURF REINFORCEMENT MAT



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SA
STRAND
ASSOCIATES

1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

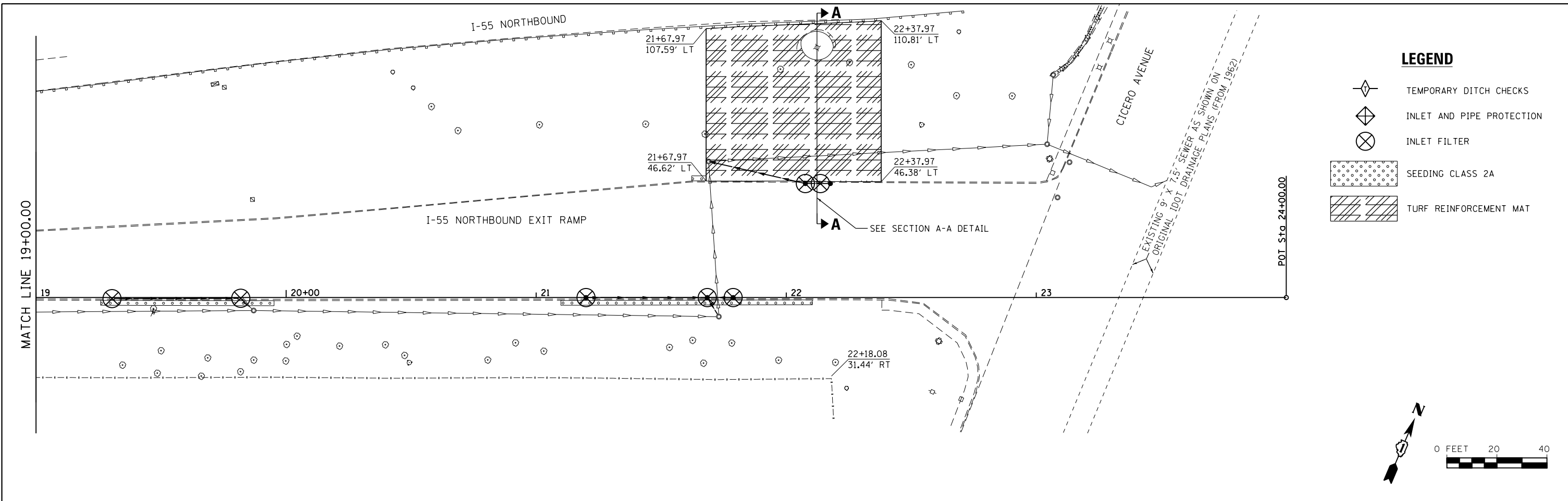
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PLOT SCALE = 48.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

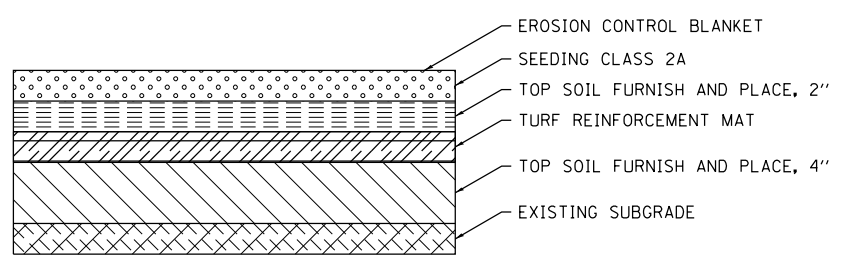
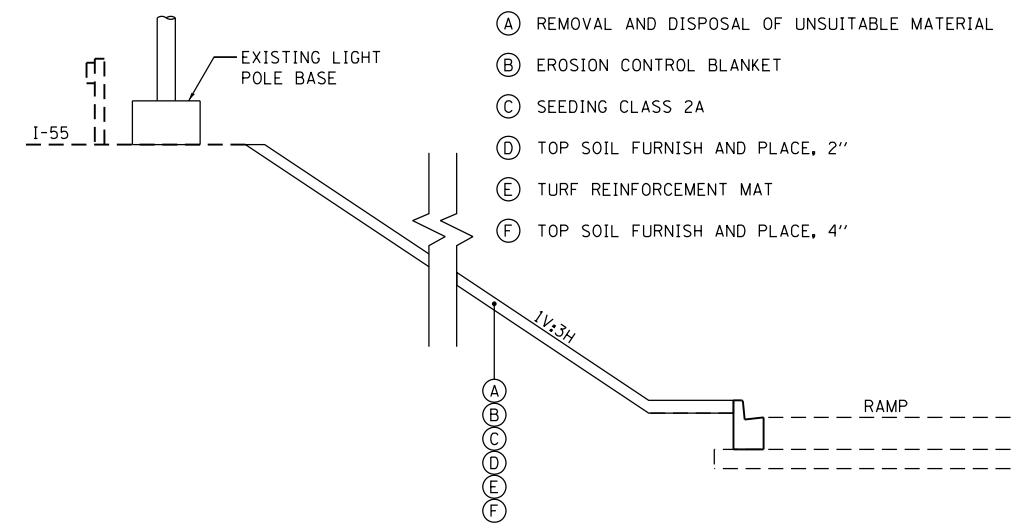
I-55 NORTHBOUND EXIT RAMP
EROSION CONTROL AND PERMANENT LANDSCAPING PLAN

SCALE: 1" = 20' SHEET 1 OF 2 SHEETS STA. 10+00.00 TO STA. 19+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	17
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- LEGEND**
- TEMPORARY DITCH CHECKS
 - INLET AND PIPE PROTECTION
 - INLET FILTER
 - SEEDING CLASS 2A
 - TURF REINFORCEMENT MAT

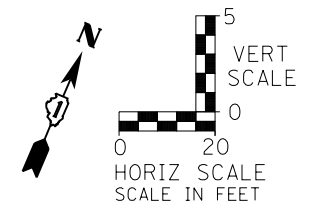


PERMANENT TURF REINFORCEMENT MAT DETAIL

DETAIL FOR CROSS SECTION A-A

FILE NAME = S:\JOLI\63300-6399\6346\035\Micro\CAD Sheets\11680391-sh-eros2.dgn

1170 SOUTH HOUBOLT ROAD JOLIET, ILLINOIS 60431 (815) 744-4200	USER NAME = dennisw DESIGNED - TJS DRAWN - DJW CHECKED - MAG DATE - 4-2-12	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-55 NORTHBOUND EXIT RAMP EROSION CONTROL AND PERMANENT LANDSCAPING PLAN		F.A.I. RTE. = 55 SECTION = 112-636T COUNTY = COOK TOTAL SHEETS = 36 SHEET NO. = 18
	PLOT SCALE = 48.0000' / in. PLOT DATE = 4/2/2012	SCALE: 1" = 20' SHEET 2 OF 2 SHEETS STA. 19+00.00 TO STA. 24+00.00		CONTRACT NO. 60T37 <small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>		

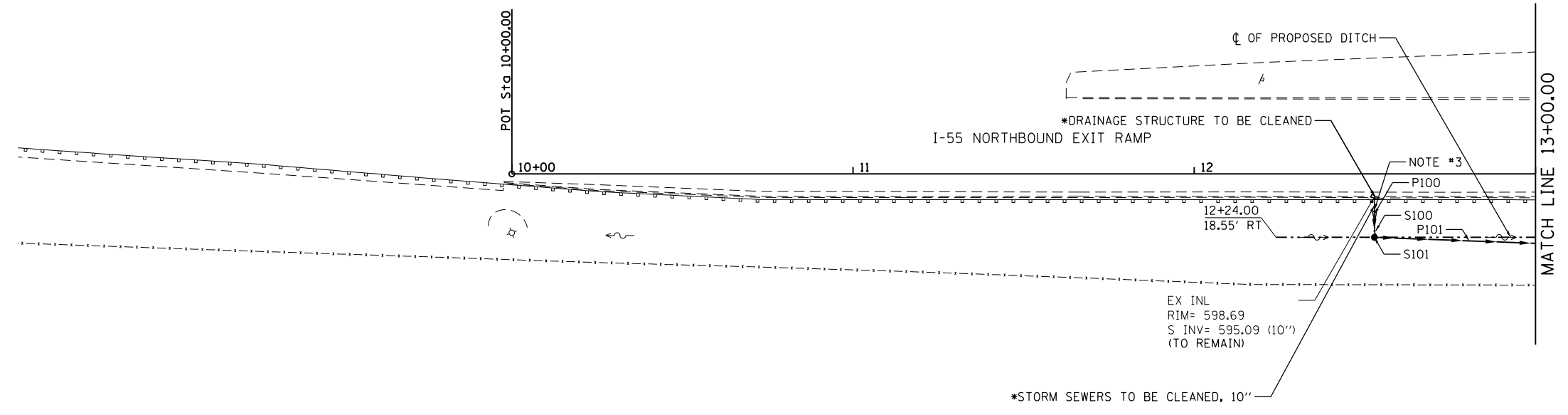


- NOTES:
- *1. CONTRACTOR TO VERIFY UPSTREAM AND DOWNSTREAM INVERTS OF STRUCTURES AND PIPES BEFORE CONSTRUCTION.
 2. THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER PROTECTION AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
 3. CONNECT TO EXISTING STORM SEWER WITH CONCRETE COLLAR (INCIDENTAL TO STORM SEWER).

LEGEND

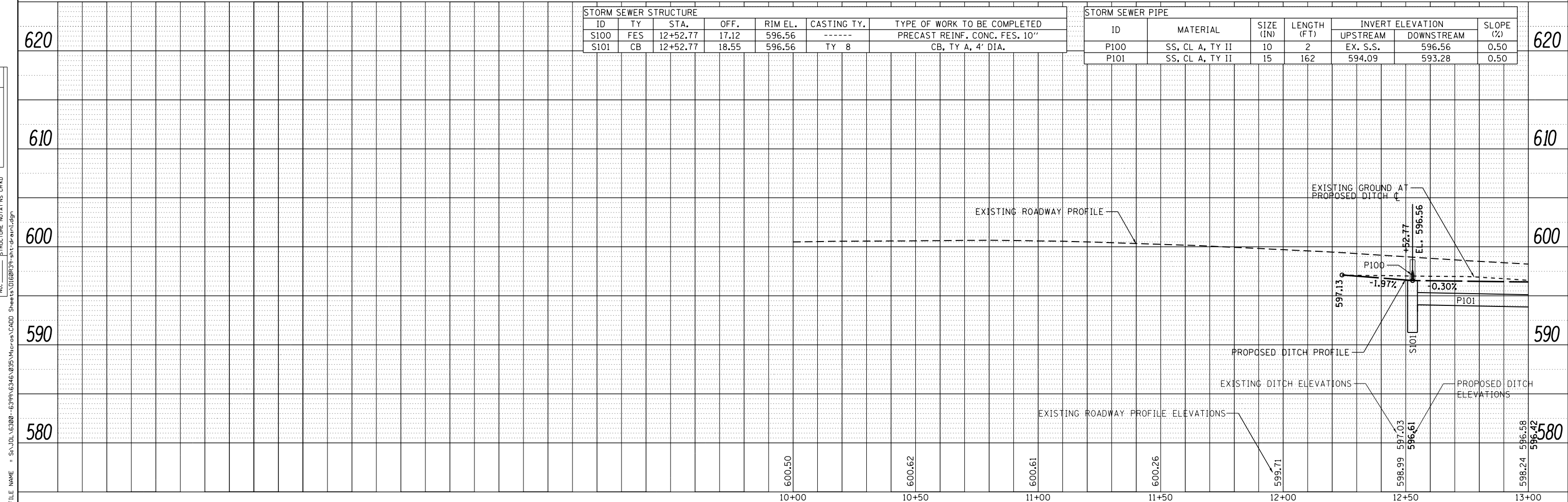
BOUNDARY LINES/SYMBOLS	EXISTING	PROPOSED
DITCH & SWALE BOTTOM	---	---
ROADWAY DITCH FLOW	~>	~>
SUMMIT	~<~>	~<~>
STORM SEWER	—▶	—▶
CATCH BASIN	○	●
INLET	□	—
MANHOLE	○	⊙

I-55 NORTHBOUND



PLAN	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOT AT THIS OFFICE	
	NO. _____	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOT AT THIS OFFICE	
	NO. _____	



STORM SEWER STRUCTURE						
ID	TY	STA.	OFF.	RIM EL.	CASTING TY.	TYPE OF WORK TO BE COMPLETED
S100	FES	12+52.77	17.12	596.56	-----	PRECAST REINF. CONC. FES. 10"
S101	CB	12+52.77	18.55	596.56	TY 8	CB, TY A, 4' DIA.

STORM SEWER PIPE						
ID	MATERIAL	SIZE (IN)	LENGTH (FT)	INVERT ELEVATION		SLOPE (%)
				UPSTREAM	DOWNSTREAM	
P100	SS, CL A, TY II	10	2	EX. S.S.	596.56	0.50
P101	SS, CL A, TY II	15	162	594.09	593.28	0.50

STRAND ASSOCIATES
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = dennisw	DESIGNED - TJS	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-55 NORTHBOUND EXIT RAMP
DRAINAGE & GRADING PLAN & PROFILE**
SCALE: 1" = 20' SHEET 1 OF 3 SHEETS STA. 10+00.00 TO STA. 13+00.00

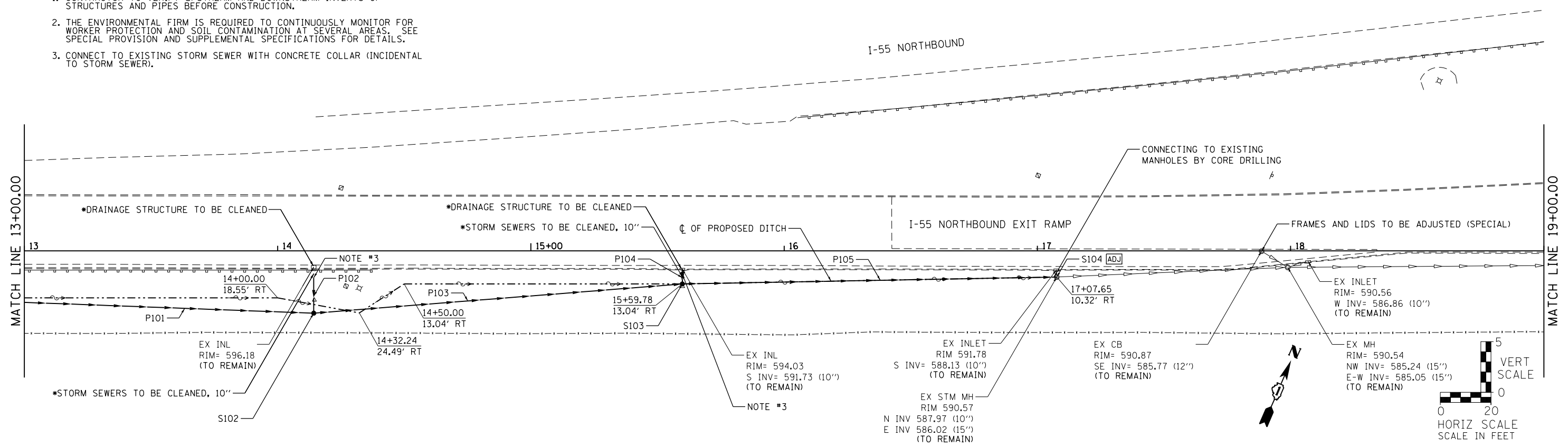
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	19
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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NOTES:

- *1. CONTRACTOR TO VERIFY UPSTREAM AND DOWNSTREAM INVERTS OF STRUCTURES AND PIPES BEFORE CONSTRUCTION.
- 2. THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER PROTECTION AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
- 3. CONNECT TO EXISTING STORM SEWER WITH CONCRETE COLLAR (INCIDENTAL TO STORM SEWER).

DATE	
BY	
PLAN	
SURVEYED	
PLOTTED	
ALIGNED	
CHECKED	
NO. _____	
FILE NAME	

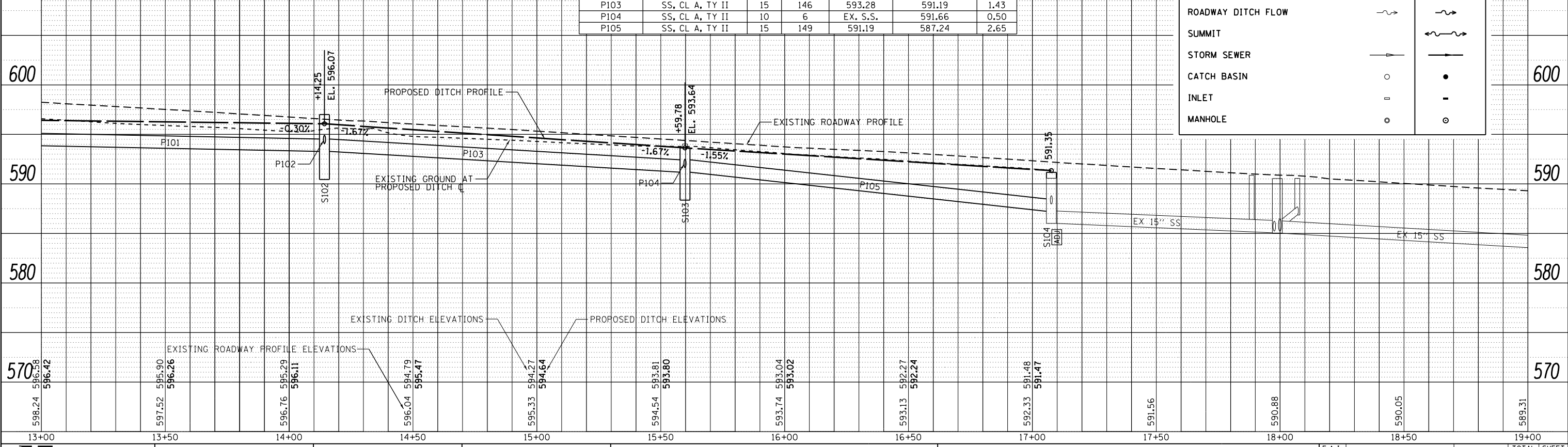


STORM SEWER STRUCTURE						
ID	TY	STA.	OFF.	RIM EL.	CASTING TY.	TYPE OF WORK TO BE COMPLETED
S102	CB	14+14.25	24.59	597.00	TY 1, CL	CB, TY A, 4' DIA.
S103	CB	15+59.8	13.04	593.64	TY 8	CB, TY A, 4' DIA.
S104	CB	17+07.65	10.32	591.35	TY 8	CATCH BASINS TO BE ADJUSTED

STORM SEWER PIPE						
ID	MATERIAL	SIZE (IN)	LENGTH (FT)	INVERT ELEVATION		SLOPE (%)
				UPSTREAM	DOWNSTREAM	
P101	SS, CL A, TY II	15	162	594.09	593.28	0.50
P102	SS, CL A, TY II	10	14	EX. S.S.	594.08	0.50
P103	SS, CL A, TY II	15	146	593.28	591.19	1.43
P104	SS, CL A, TY II	10	6	EX. S.S.	591.66	0.50
P105	SS, CL A, TY II	15	149	591.19	587.24	2.65

LEGEND		
BOUNDARY LINES/SYMBOLS	EXISTING	PROPOSED
DITCH & SWALE BOTTOM	---	---
ROADWAY DITCH FLOW	~	~
SUMMIT	~	~
STORM SEWER	—	—
CATCH BASIN	○	●
INLET	—	—
MANHOLE	○	○

DATE	
BY	
PROFILE	
SURVEYED	
PLOTTED	
GRADES CHECKED	
NO. _____	
FILE NAME	



STRAND ASSOCIATES*
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = dennisw	DESIGNED - TJS	REVISED -
PLOT SCALE = 48.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
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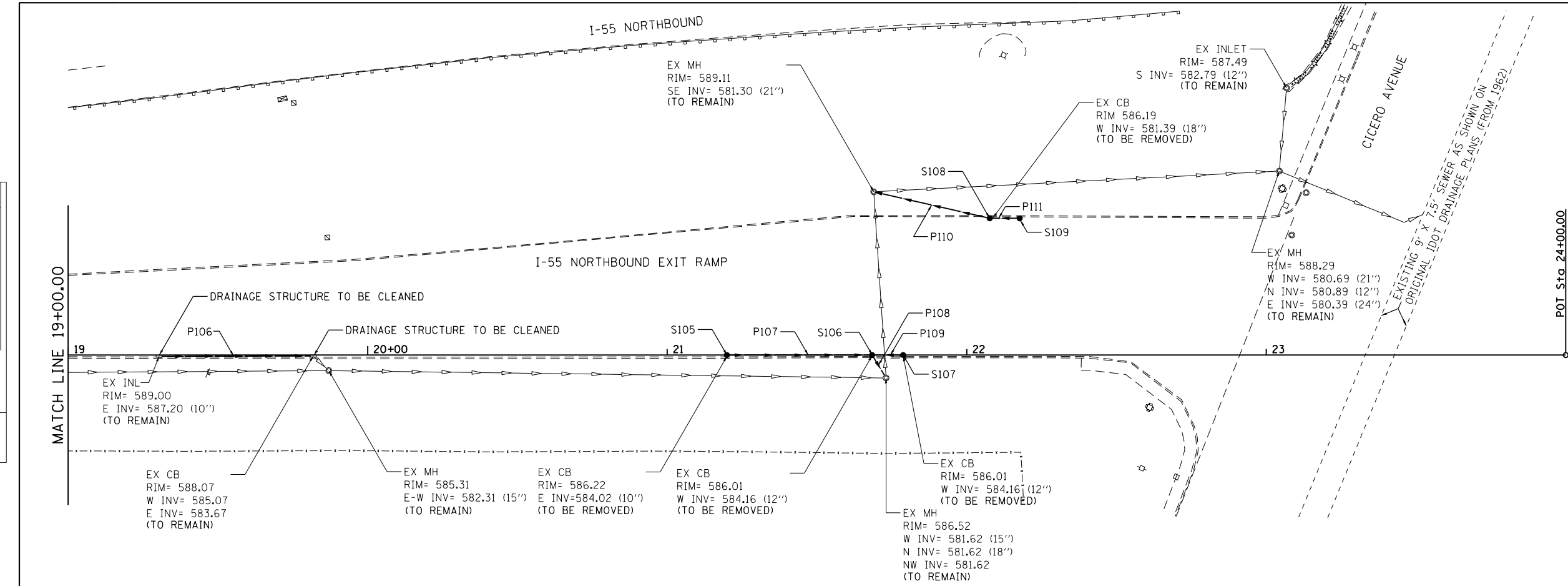
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 NORTHBOUND EXIT RAMP
DRAINAGE & GRADING PLAN & PROFILE
SCALE: 1" = 20' SHEET 2 OF 3 SHEETS STA. 13+00.00 TO STA. 19+00.00

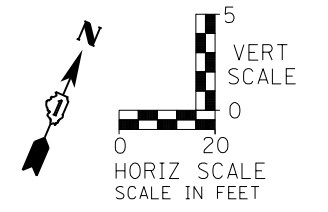
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55	112-636T	COOK	36	20
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOT AT THIS OFFICE	
	NO. _____	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOT AT THIS OFFICE	
	NO. _____	
	FILE NAME	



- NOTES:
- *1. CONTRACTOR TO VERIFY UPSTREAM AND DOWNSTREAM INVERTS OF STRUCTURES AND PIPES BEFORE CONSTRUCTION.
 2. THE ENVIRONMENTAL FIRM IS REQUIRED TO CONTINUOUSLY MONITOR FOR WORKER PROTECTION AND SOIL CONTAMINATION AT SEVERAL AREAS. SEE SPECIAL PROVISION AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
 3. CONNECT TO EXISTING STORM SEWER WITH CONCRETE COLLAR (INCIDENTAL TO STORM SEWER).



610

STORM SEWER STRUCTURE						
ID	TY	STA.	OFF.	RIM EL.	CASTING TY.	TYPE OF WORK TO BE COMPLETED
S105	CB	21+19.93	1.08	586.22	TY 24	CB, TY C, 2' DIA.
S106	CB	21+68.44	0.92	586.01	TY 24	CB, TY A, 4' DIA.
S107	CB	21+78.79	0.96	586.01	TY 24	CB, TY C, 2' DIA.
S108	CB	22+07.66	46.48	586.19	TY 24	CB, TY A, 4' DIA.
S109	CB	22+17.66	46.48	586.30	TY 24	CB, TY C, 2' DIA.

STORM SEWER PIPE						
ID	MATERIAL	SIZE (IN)	LENGTH (FT)	INVERT ELEVATION		SLOPE (%)
				UPSTREAM	DOWNSTREAM	
P106	SS, CL A, TY II	10	50	587.2	585.07	4.26
P107	SS, CL A, TY II	10	49	584.02	582.00	4.12
P108	SS, CL A, TY II	12	8	581.9	581.62	3.50
P109	SS, CL A, TY II	10	11	583.88	583.35	4.82
P110	SS, CL A, TY II	18	40	581.48	581.30	0.45
P111	SS, CL A, TY II	12	10	582.07	582.02	0.45

LEGEND		
BOUNDARY LINES/SYMBOLS	EXISTING	PROPOSED
DITCH & SWALE BOTTOM	---	---
ROADWAY DITCH FLOW	~	~
SUMMIT	~	~
STORM SEWER	—▶—	—▶—
CATCH BASIN	○	●
INLET	□	■
MANHOLE	○	⊙

610

600

590

580

570

600

590

580

570

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19+00	19+50	20+00	20+50	21+00	21+50	22+00	22+50
589.31	588.68	587.84	587.19	586.46	586.06	586.30	586.85

STRAND ASSOCIATES
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
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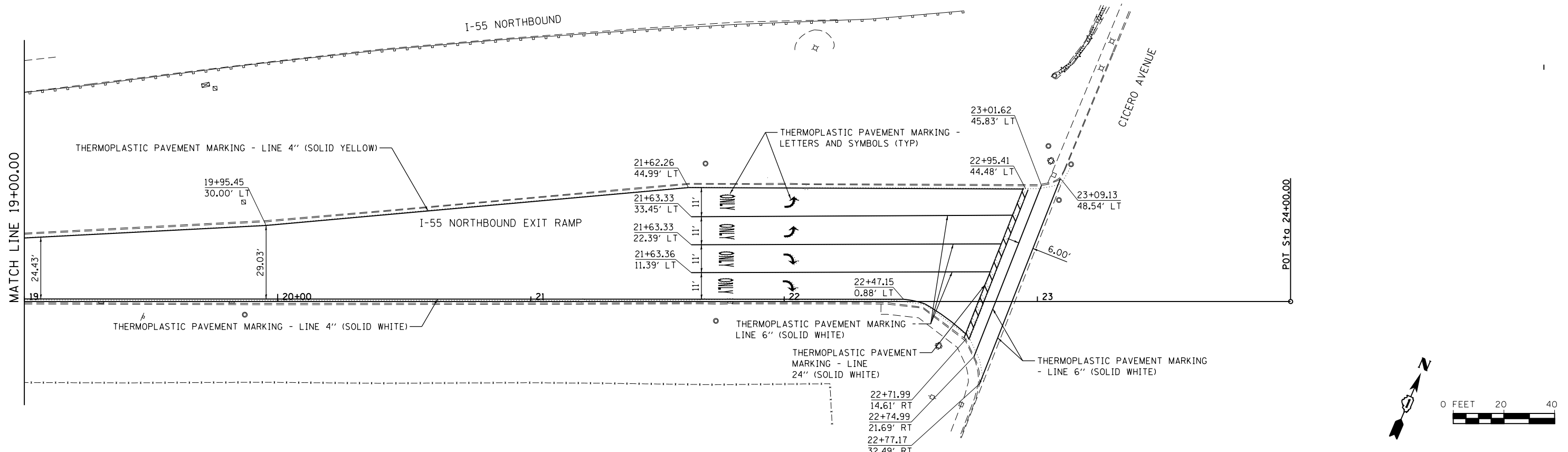
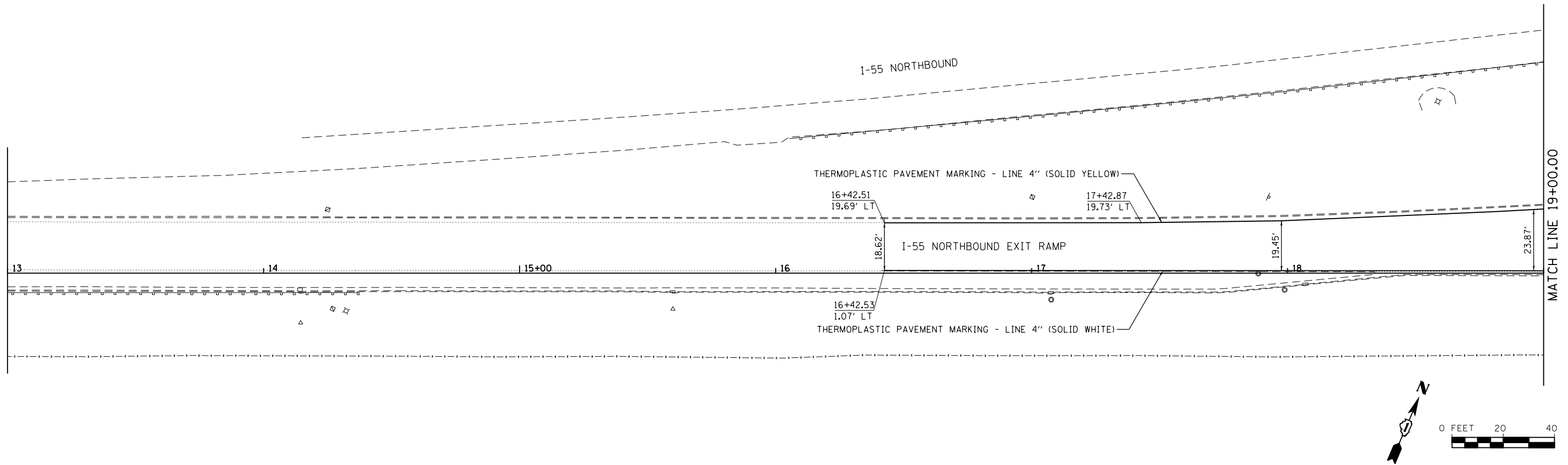
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	DRAWN - DJW	REVISED -
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	DATE - 4-2-12	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-55 NORTHBOUND EXIT RAMP
DRAINAGE & GRADING PLAN & PROFILE
SCALE: 1" = 20'
SHEET 3 OF 3 SHEETS
STA. 19+00.00 TO STA. 24+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	21
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME = S:\JOL\6300-6399\6346-0395\Microsta\CADD Sheets\01680395-sh1-pmk1.dgn



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 1170 SOUTH HOUBOLT ROAD
 JOLIET, ILLINOIS 60431
 (815) 744-4200

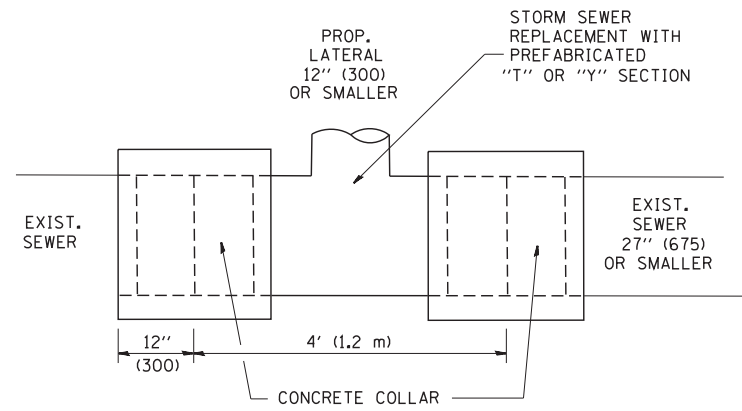
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PLOT SCALE = 40.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-55 NORTHBOUND EXIT RAMP
 PAVEMENT MARKING AND SIGNING PLAN**

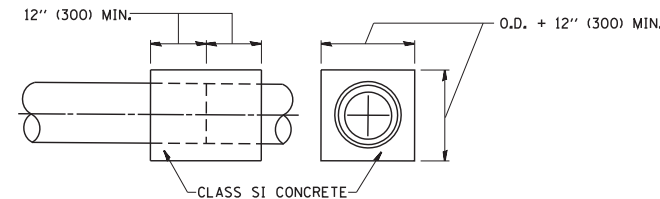
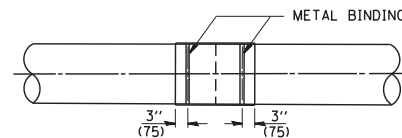
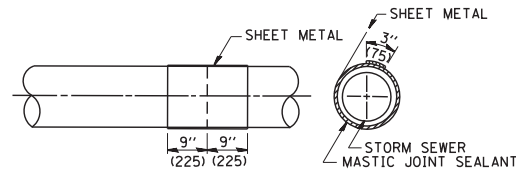
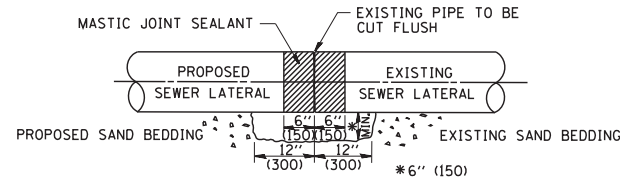
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	22
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

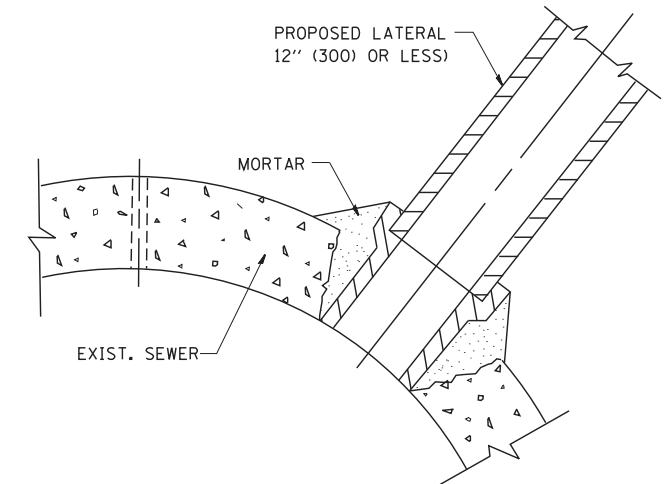


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

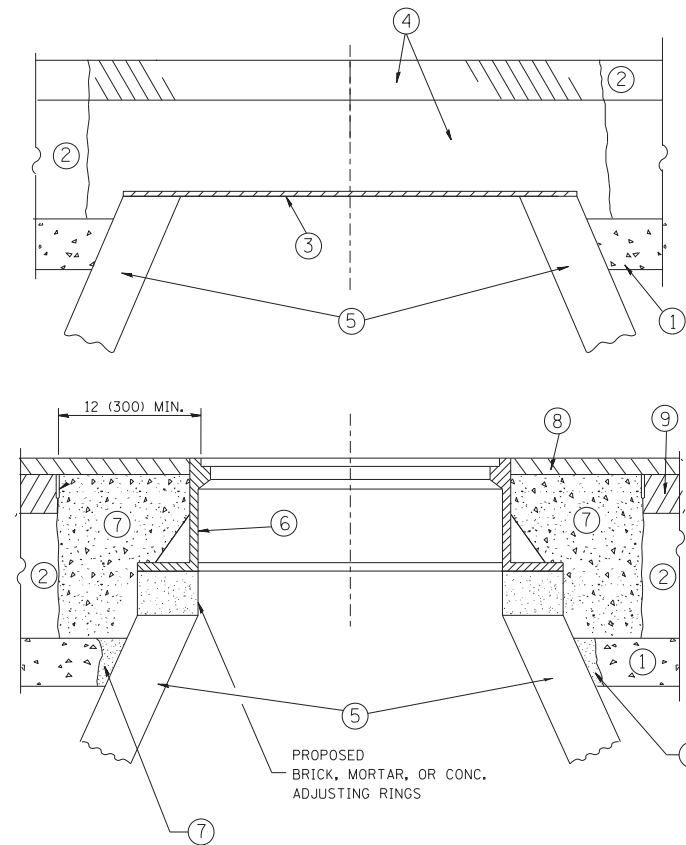
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. SHAH 10-25-94
	PLOT DATE = 1/4/2008	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	23
BD500-01 (BD-7)		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

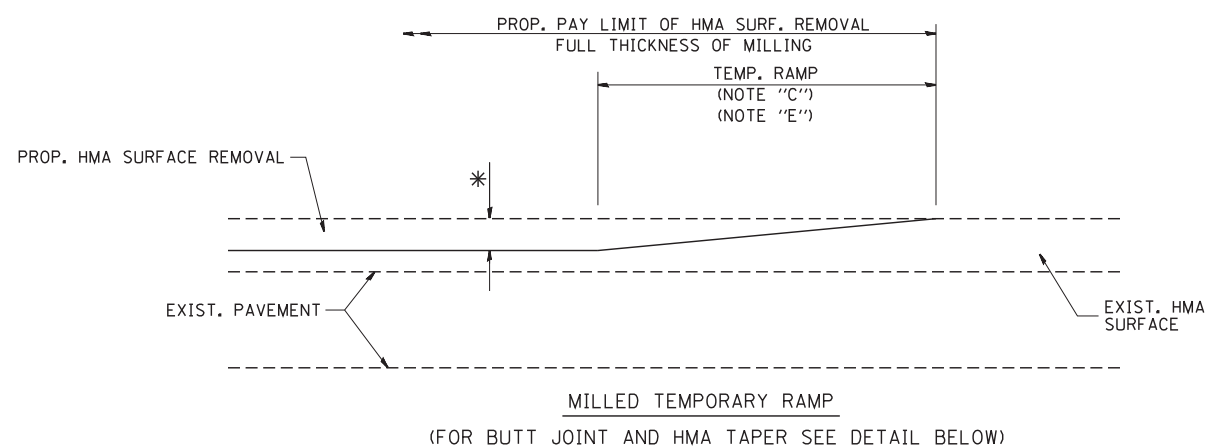
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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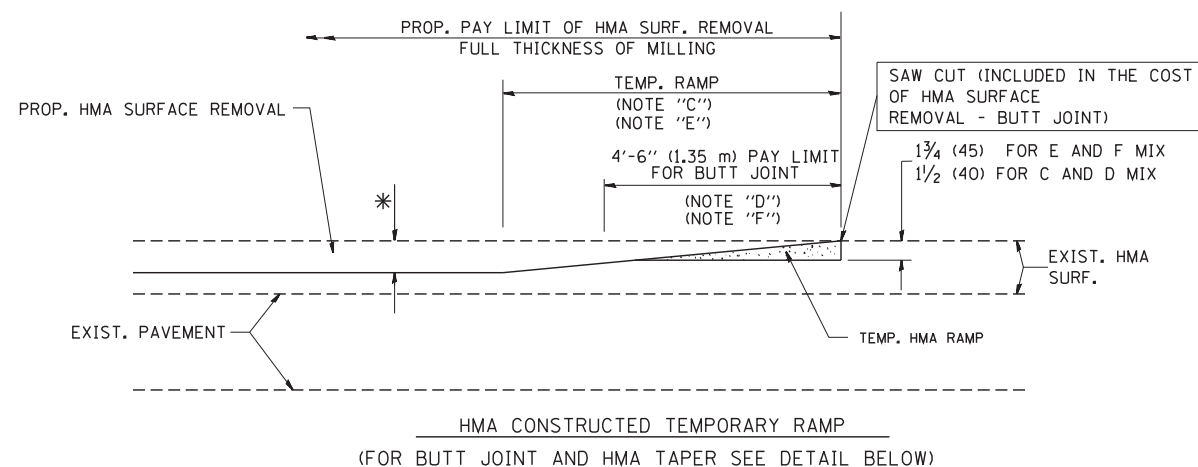
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	24
BD600-03 (BD-8)		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

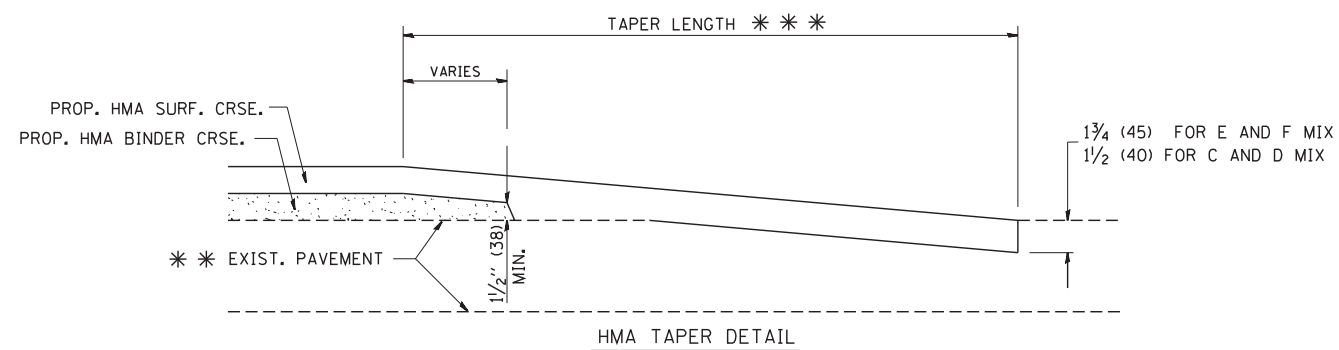
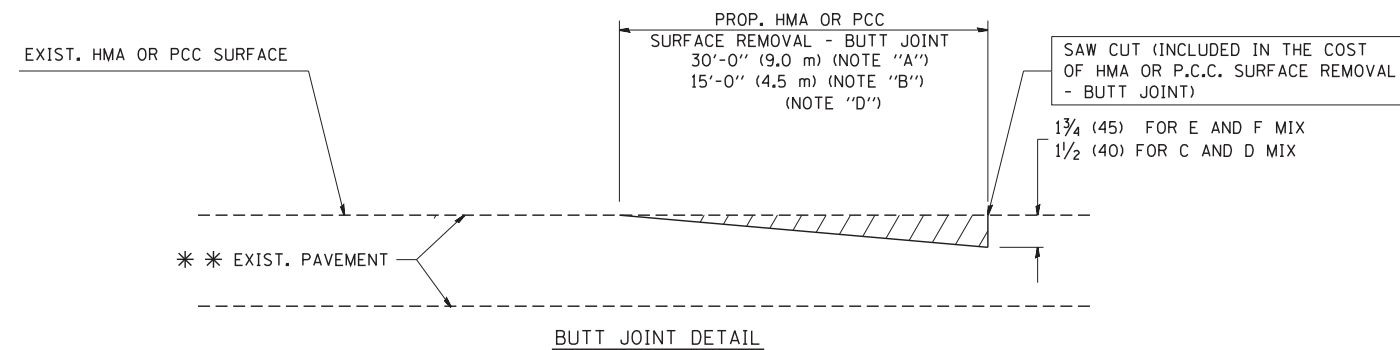


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

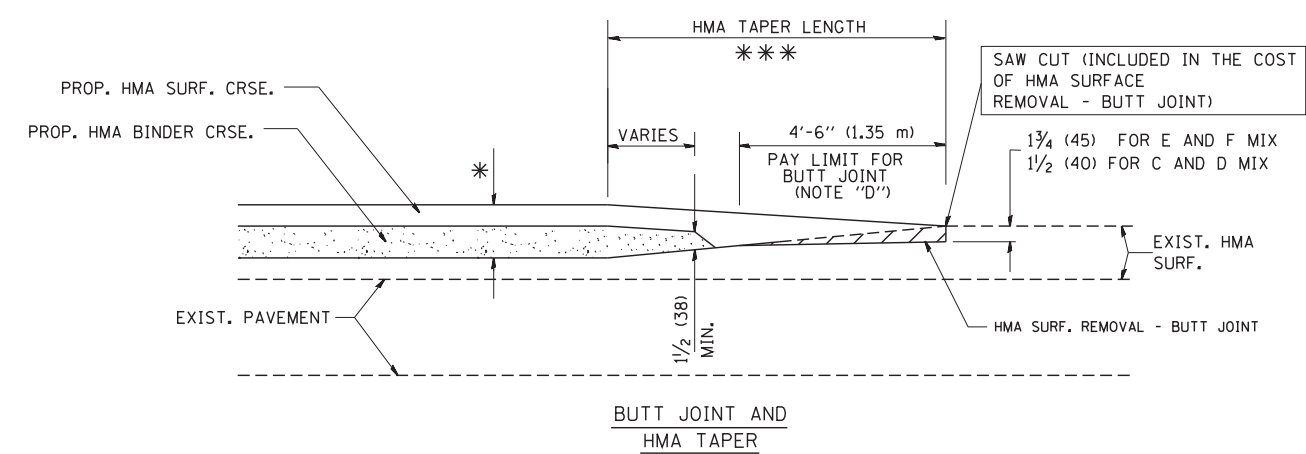
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

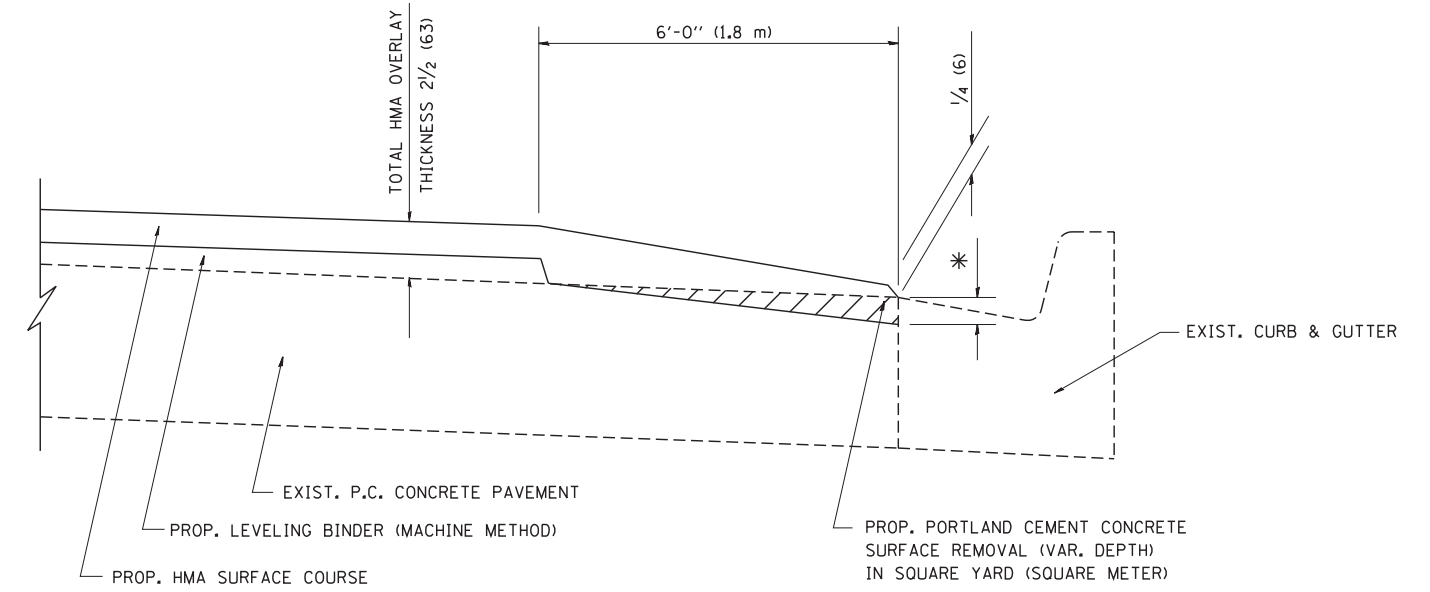
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	55	112-636T	COOK	36 25
		PLOT SCALE = 50.0000' / IN.	CHECKED -		REVISED - M. GOMEZ 04-06-01			BD400-05 BD32		CONTRACT NO. 60T37		
		PLOT DATE = 1/4/2008	DATE - 06-13-90		REVISED - R. BORO 01-01-07			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

HMA SURFACE	THICKNESS	LEVELING BINDER THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1 1/2 (38)	1 (25)	1/4 (33)
F	1 3/4 (44)	3/4 (19)	1/2 (38)

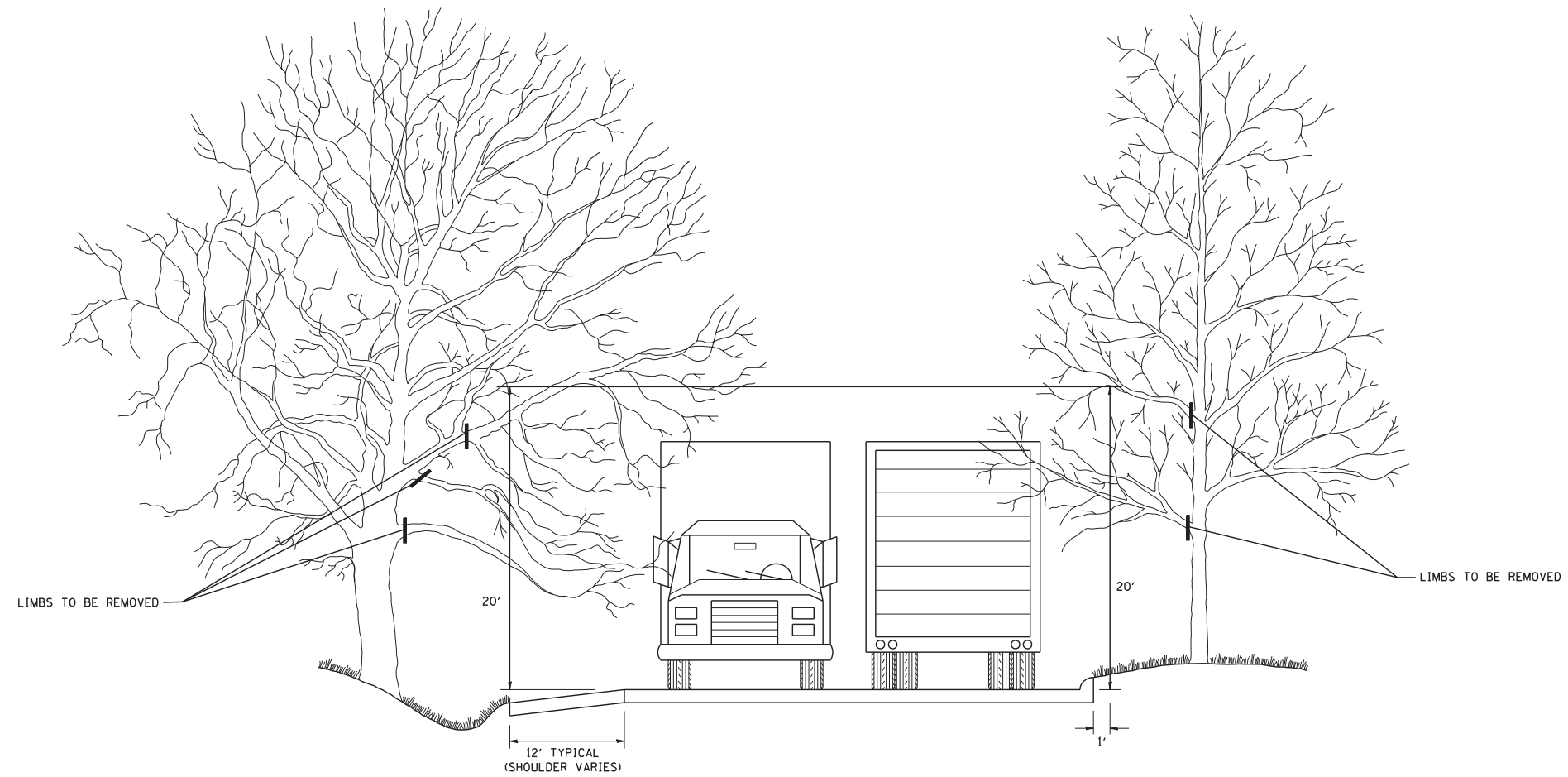
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT DATE = 1/4/2008	DATE - 09-10-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT EDGE OF P.C.C. PAVEMENT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	26
BD400-06 (BD33)		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



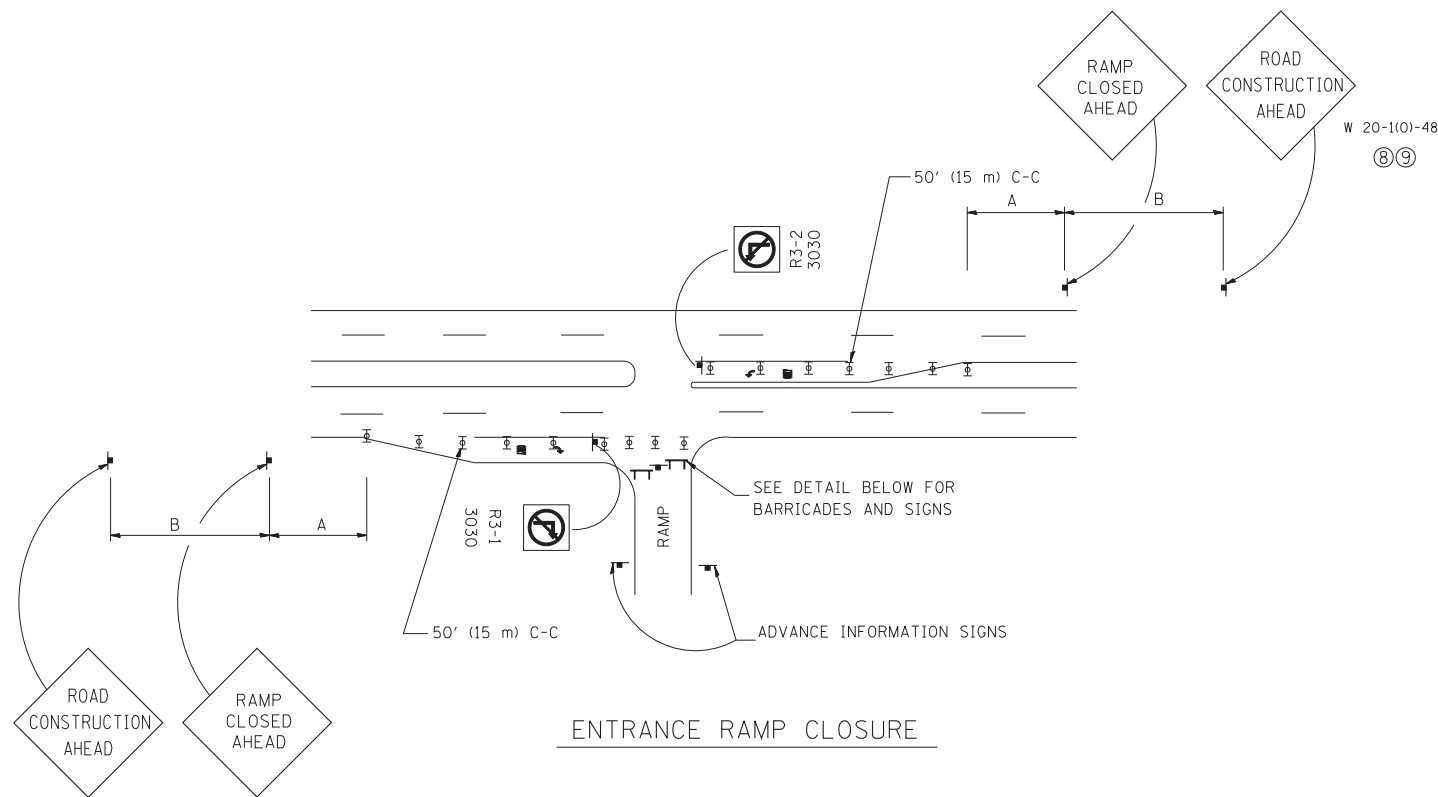
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	PLOT DATE = 1/4/2008	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PRUNING FOR SAFETY AND
EQUIPMENT CLEARANCE**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	27
BM-20		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



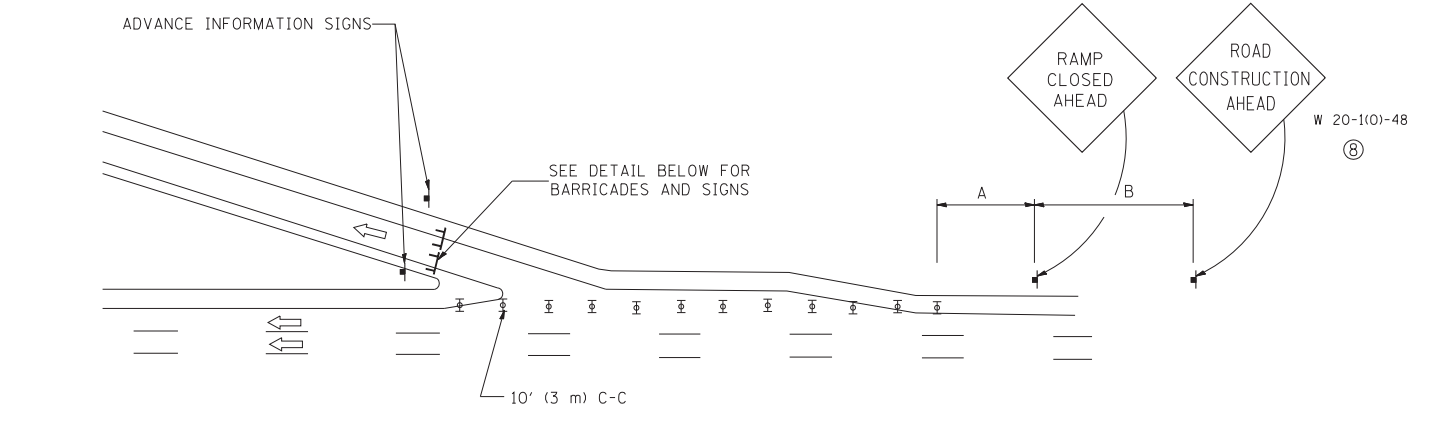
ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL ≥45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

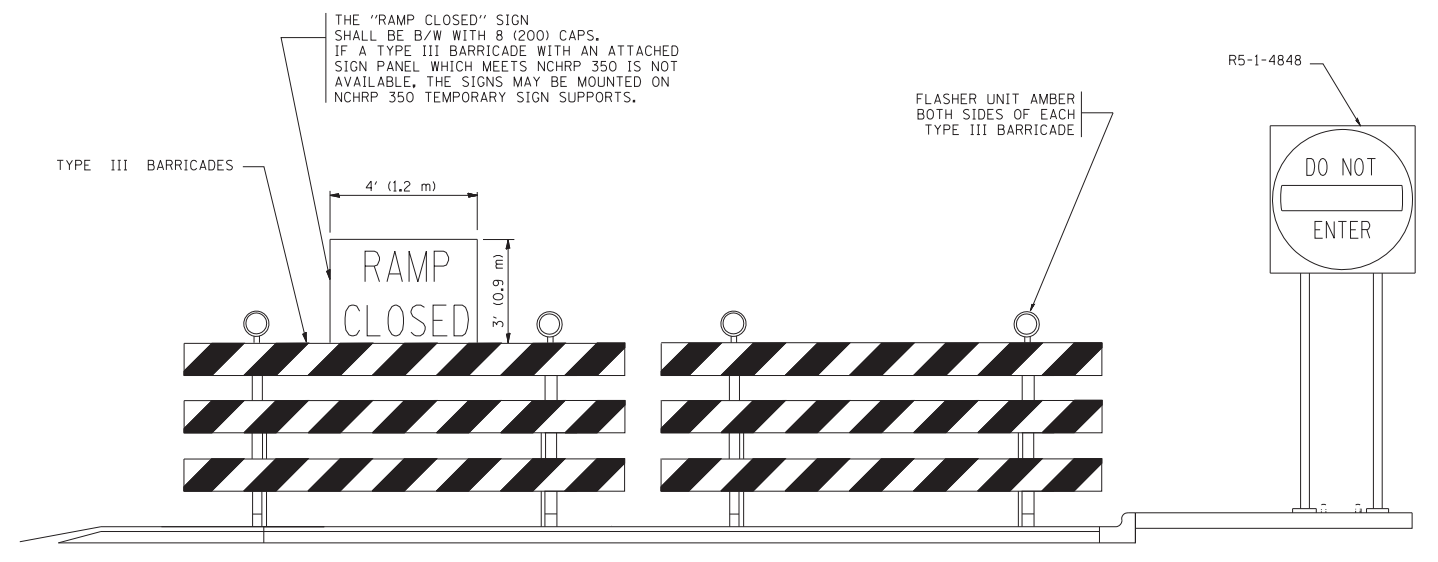
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

W 20-1(0)-48
⑧⑨



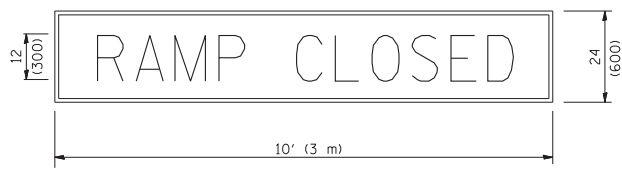
EXIT RAMP CLOSURE

- SYMBOLS**
- ⊥ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ⊥ TYPE III BARRICADE WITH FLASHING LIGHT



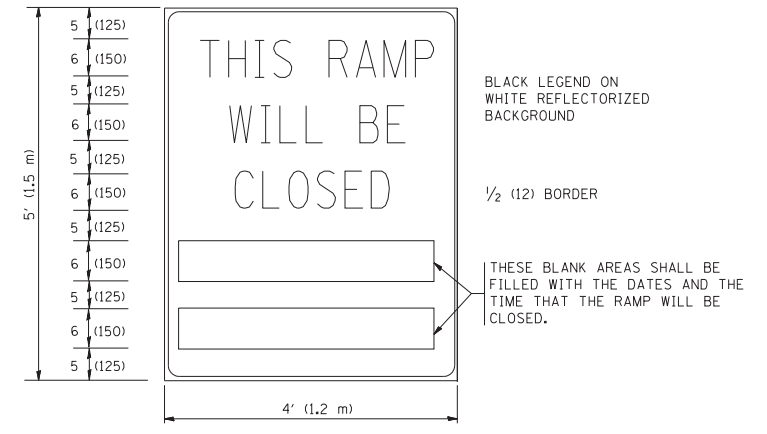
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR THE CLOSED EXIT RAMPS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

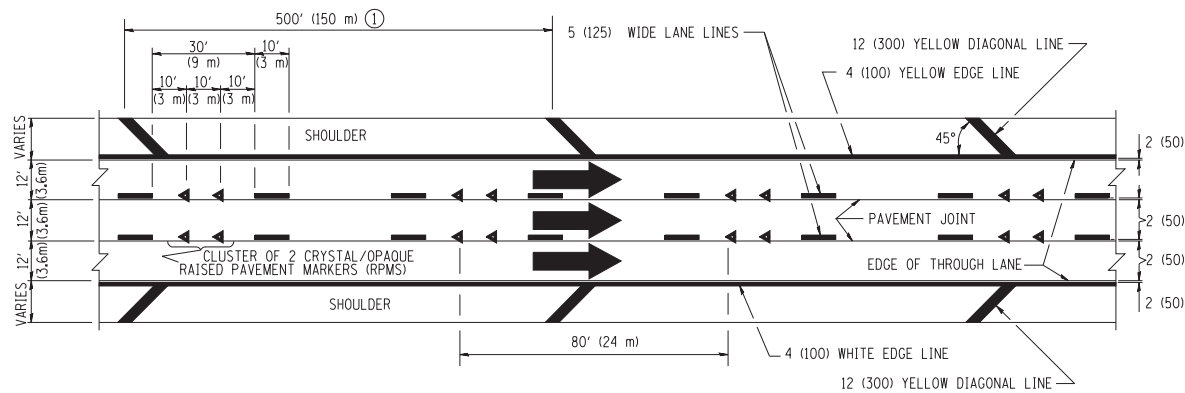
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		CHECKED -	REVISED - SPB 01-07
		DATE - 02-83	REVISED - SPB 12-09

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FREEWAY ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

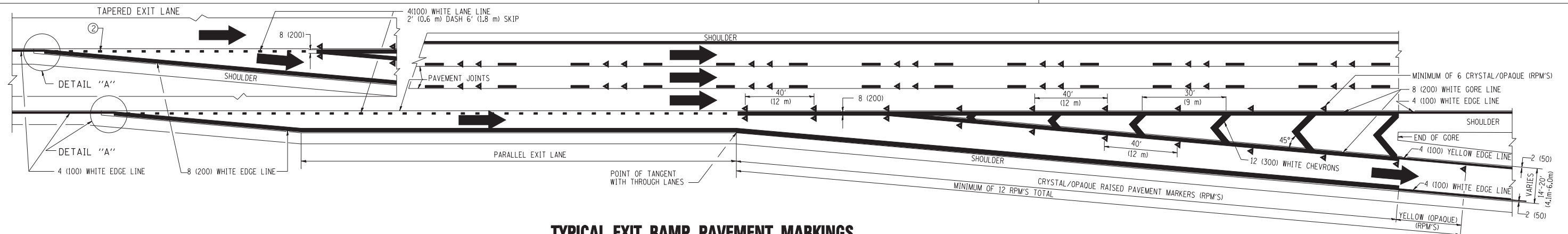
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	28
TC-08		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



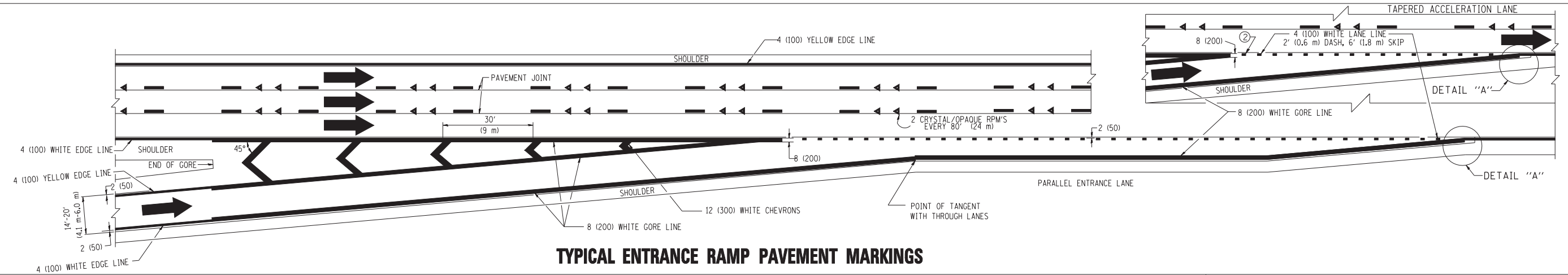
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

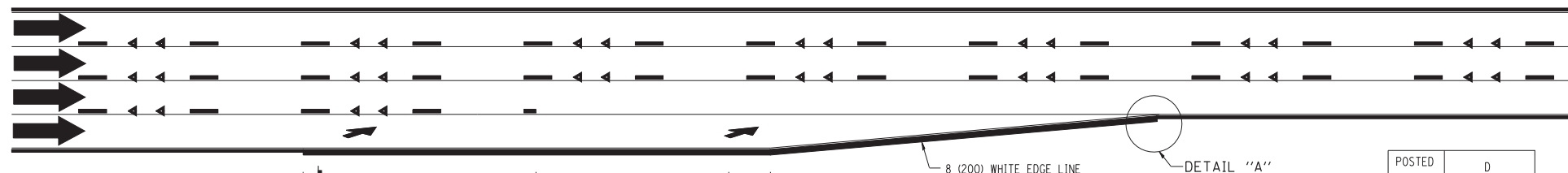
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

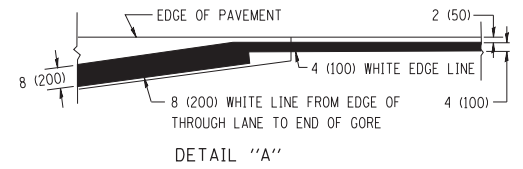


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

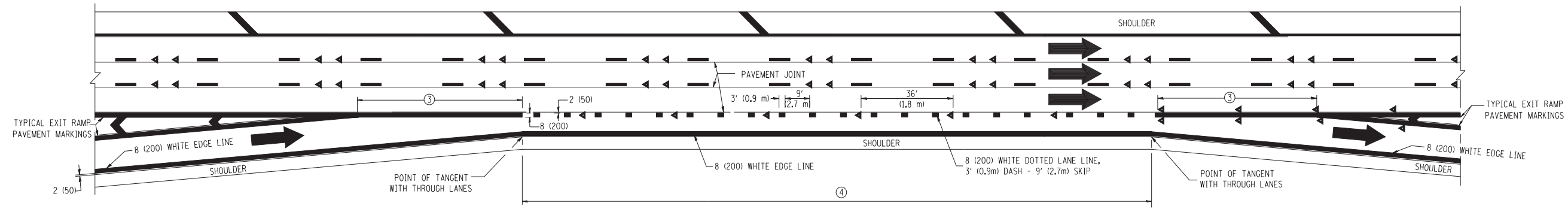
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	PLOT DATE = 1/22/2010	DATE - 01-90	REVISED - S.P.B. 01-10

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

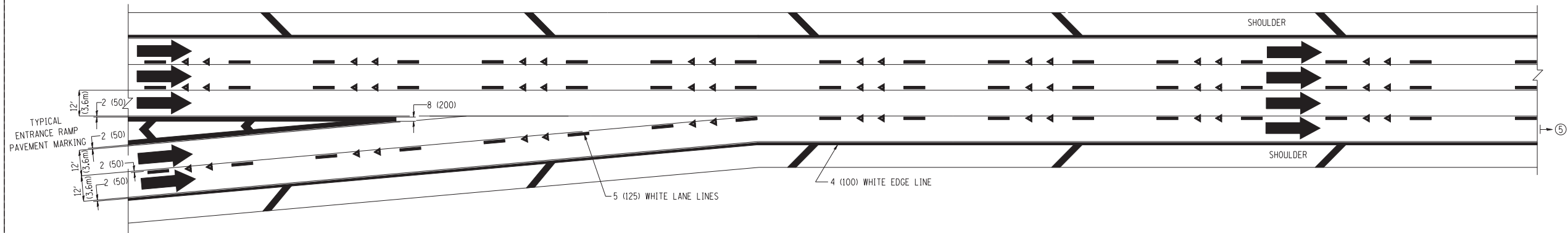
**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

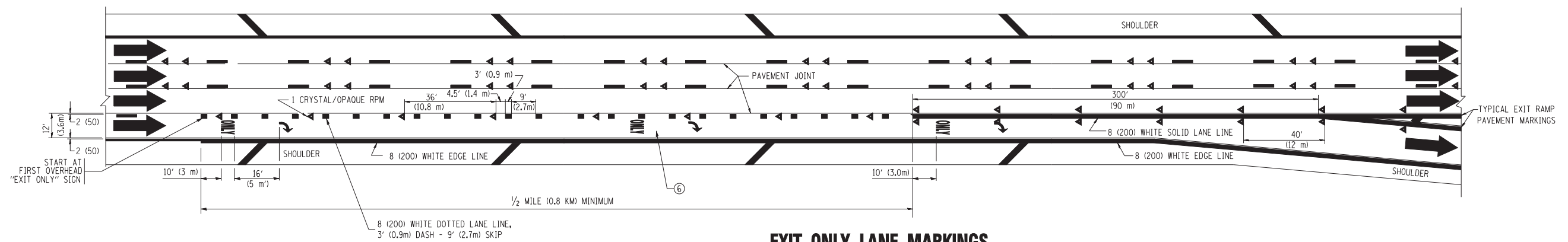
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TC-12		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



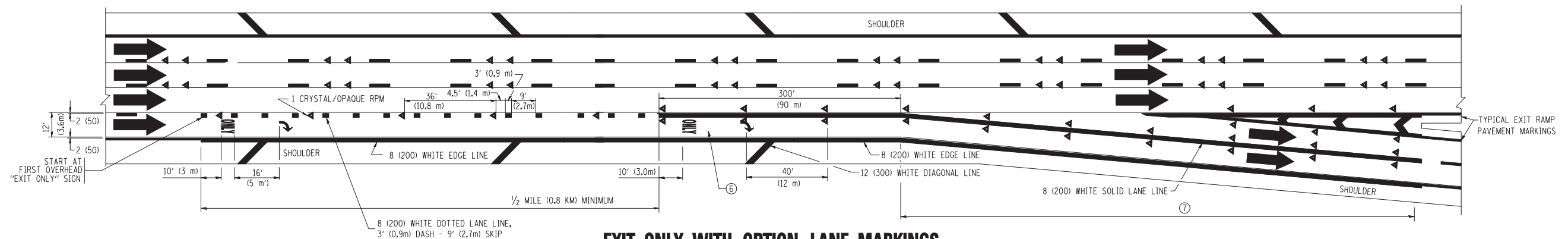
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

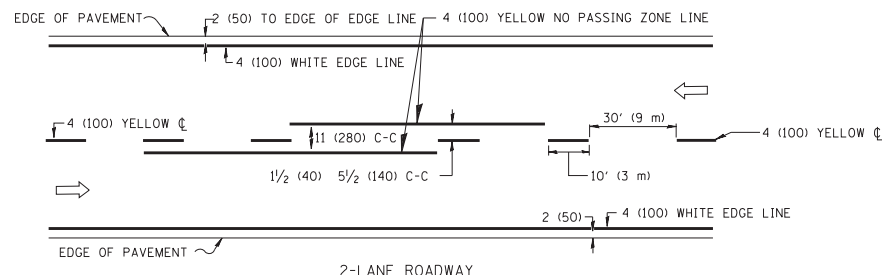
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = leysa	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96
ce:\pwwork\pwidot\LEYSAN\0108315\c12.dgn		DRAWN -	REVISED - J.A.F. 02-06
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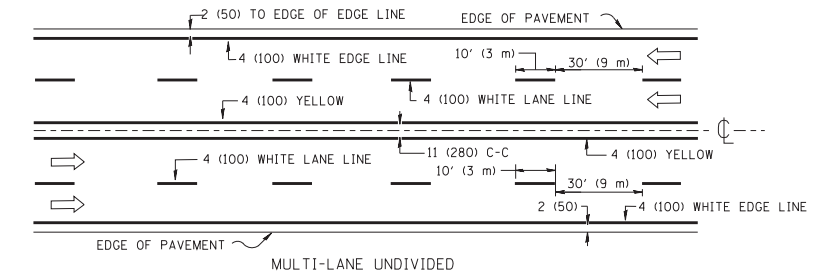
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			
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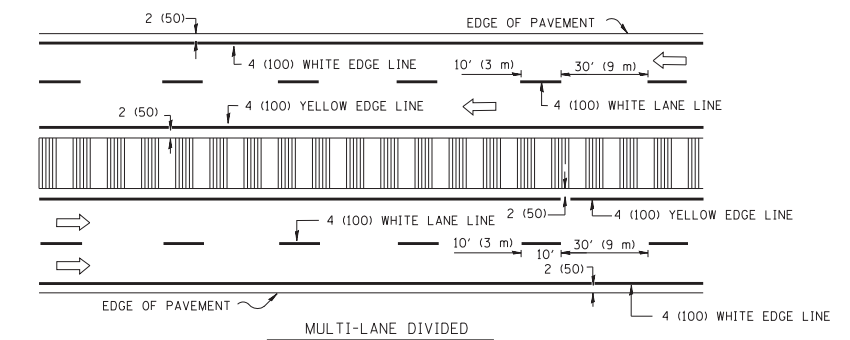
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	30
TC-12		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



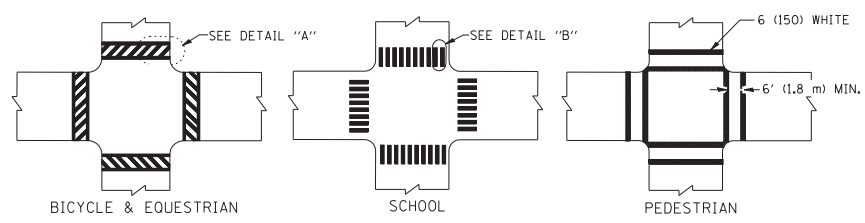
MULTI-LANE UNDIVIDED



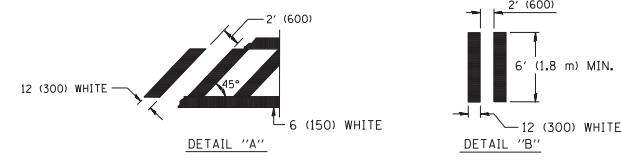
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

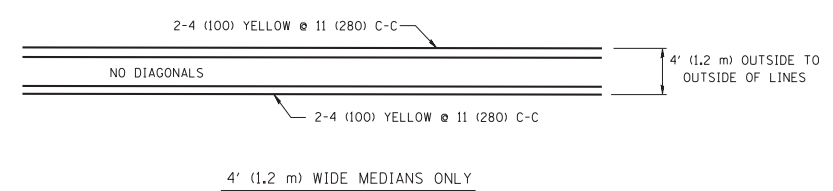
TYPICAL LANE AND EDGE LINE MARKING



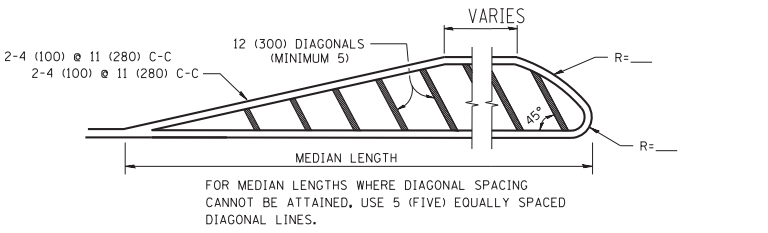
BICYCLE & EQUESTRIAN SCHOOL PEDESTRIAN



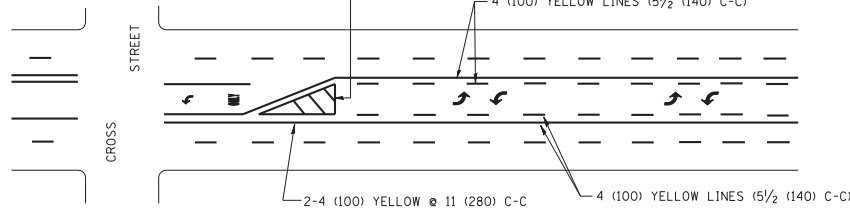
TYPICAL CROSSWALK MARKING



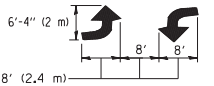
4' (1.2 m) WIDE MEDIANS ONLY



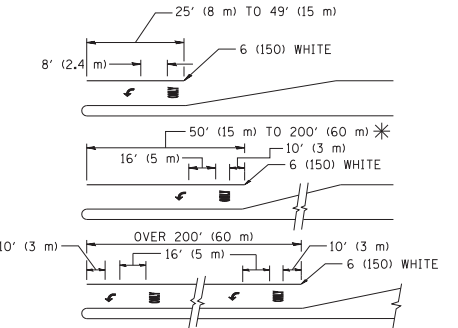
MEDIANS OVER 4' (1.2 m) WIDE



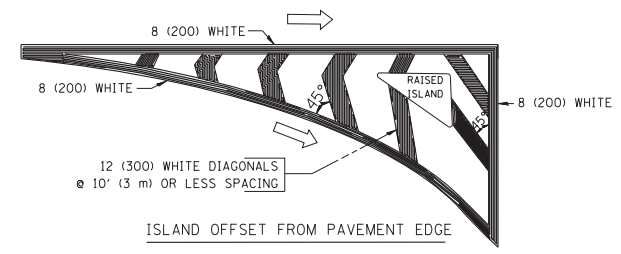
TYPICAL PAINTED MEDIAN MARKING



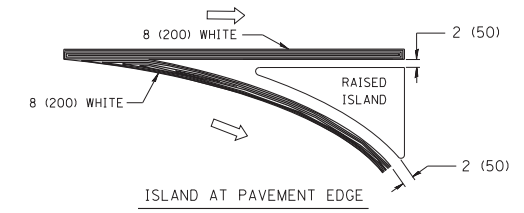
MEDIAN WITH TWO-WAY LEFT TURN LANE



TYPICAL LEFT (OR RIGHT) TURN LANE



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

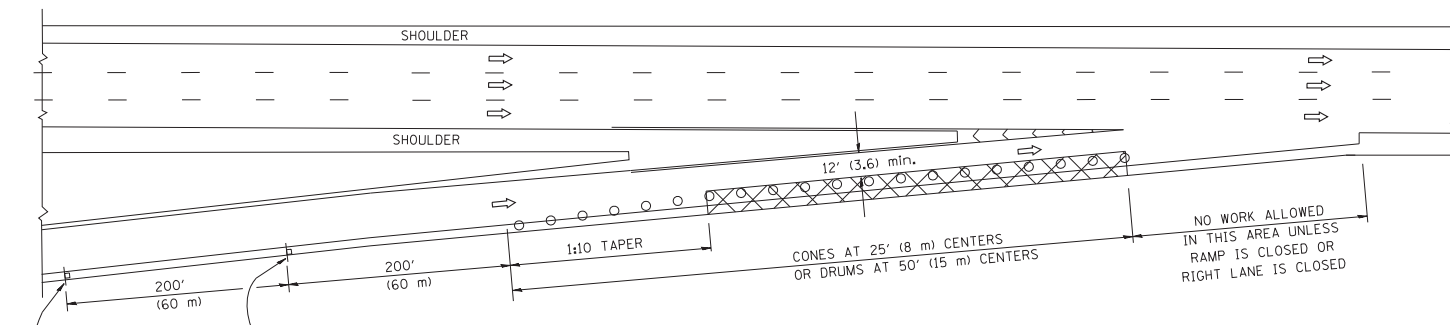
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

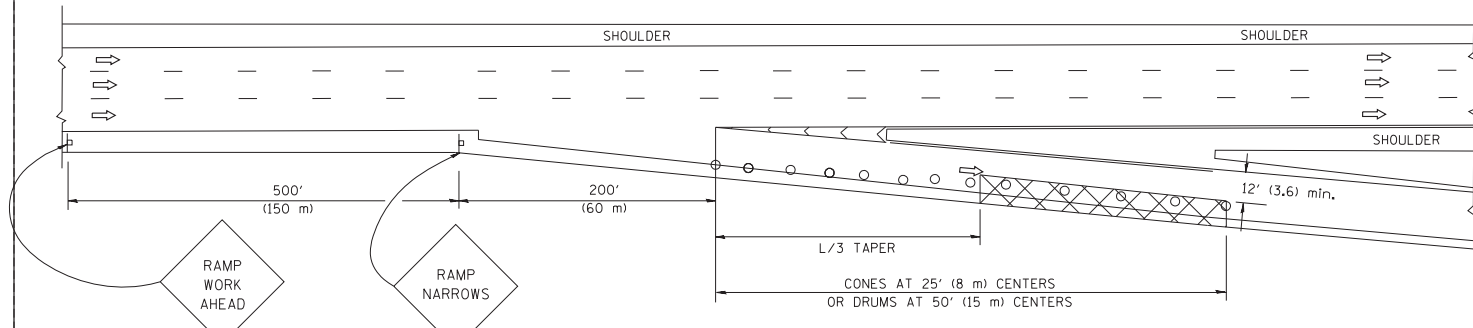
DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-13		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

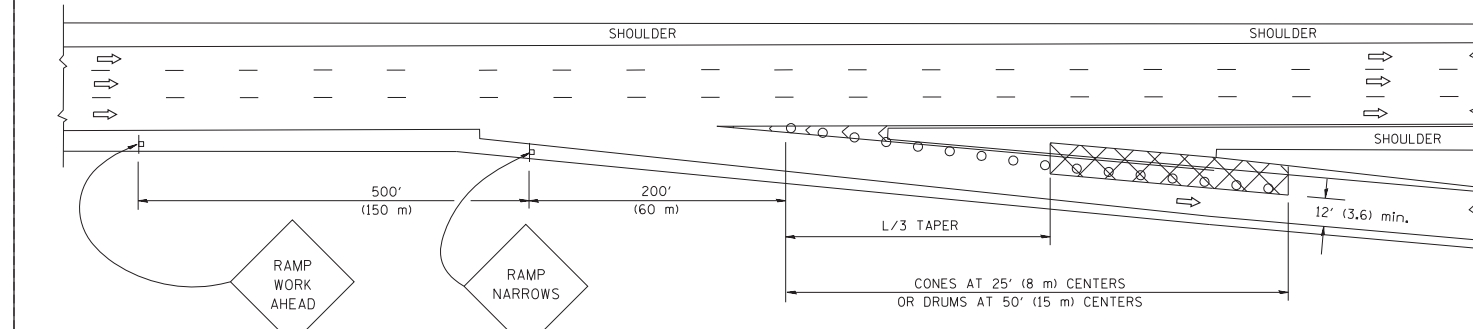
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

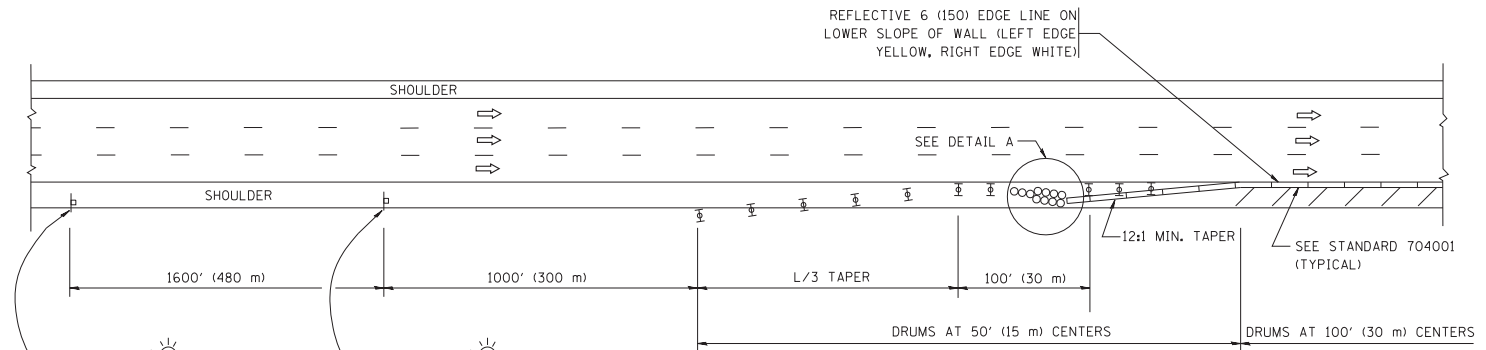
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

GENERAL NOTES

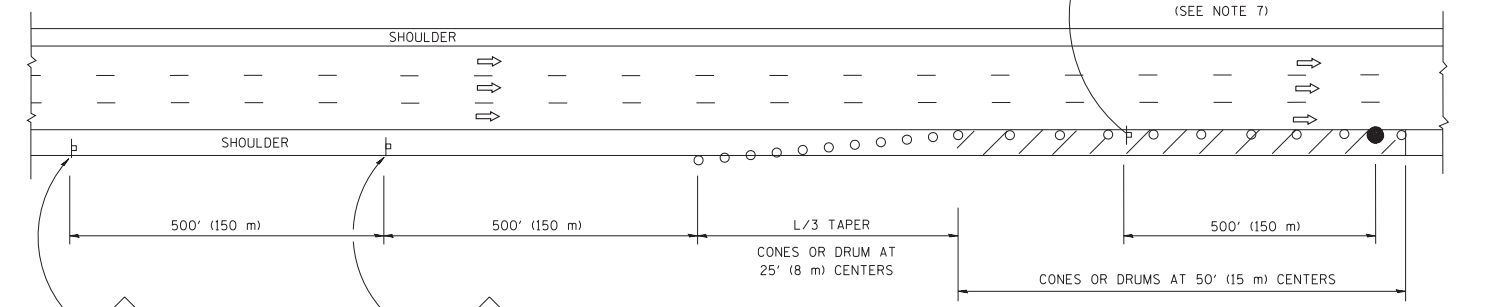
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

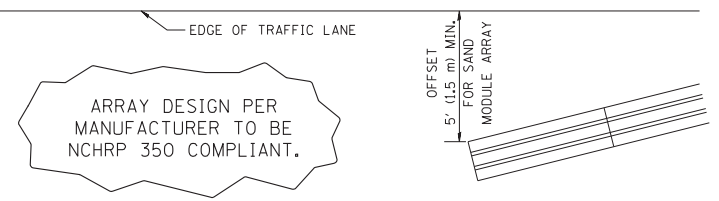


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN - D.W.S.	REVISED - J.A.F. 12-06
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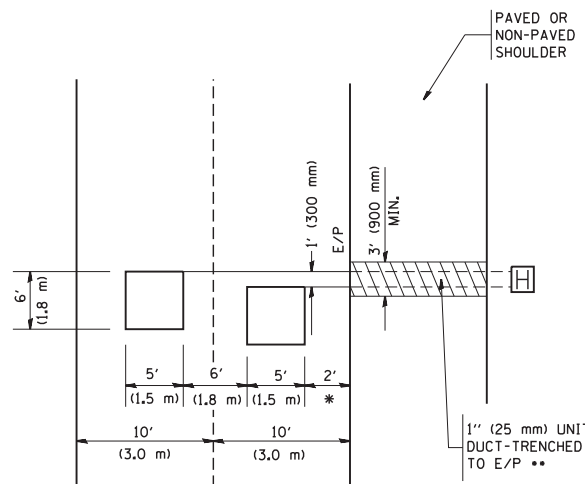
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	32
TC-17		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



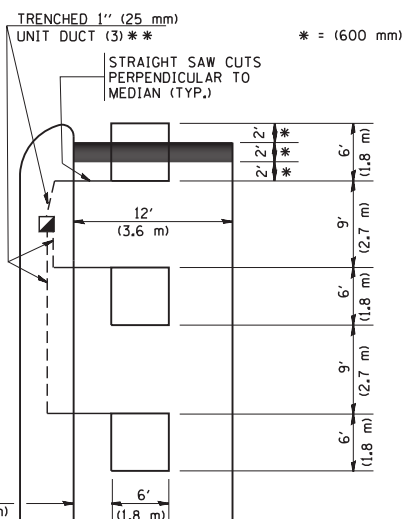
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

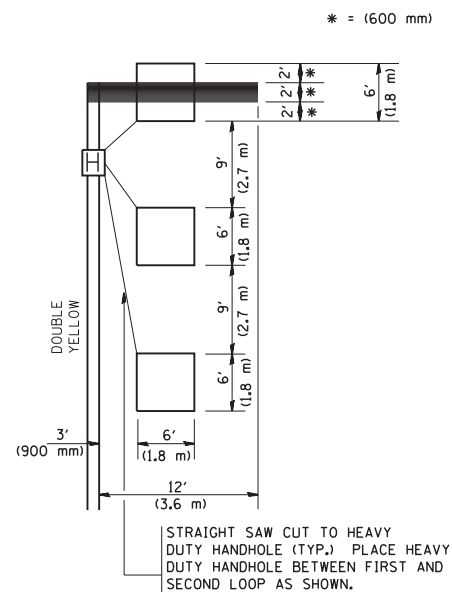


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

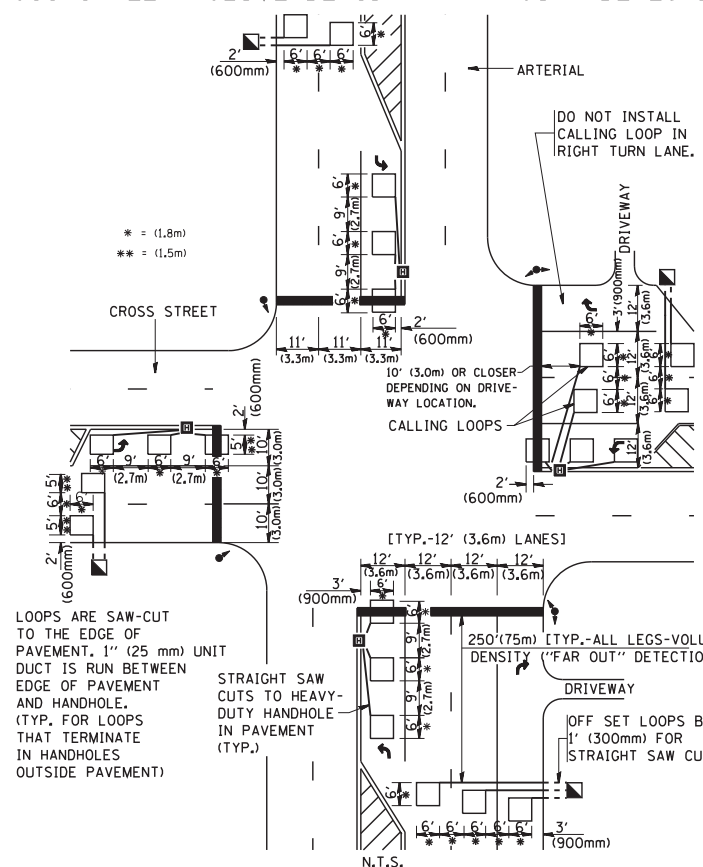
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



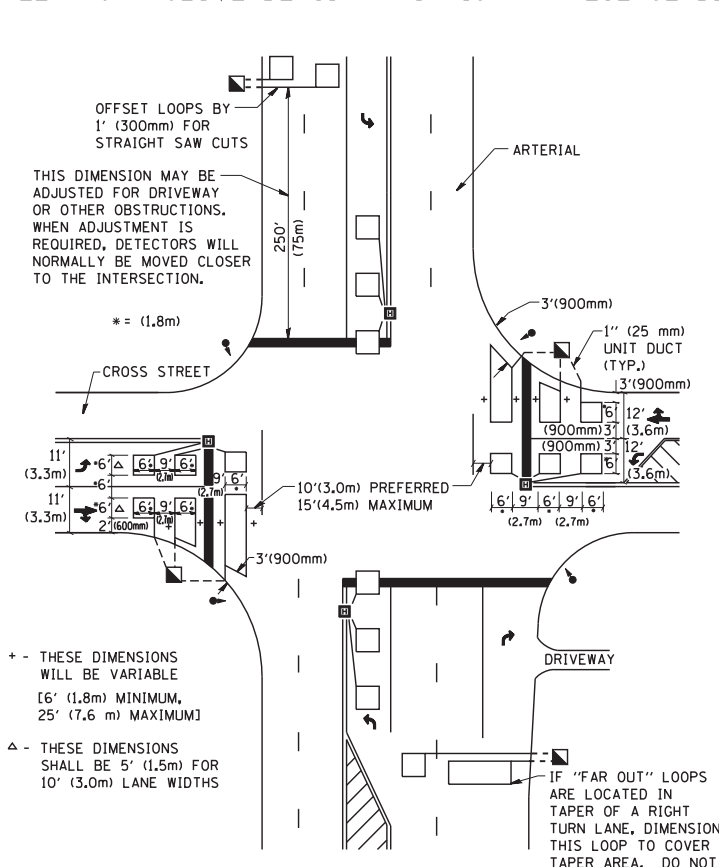
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

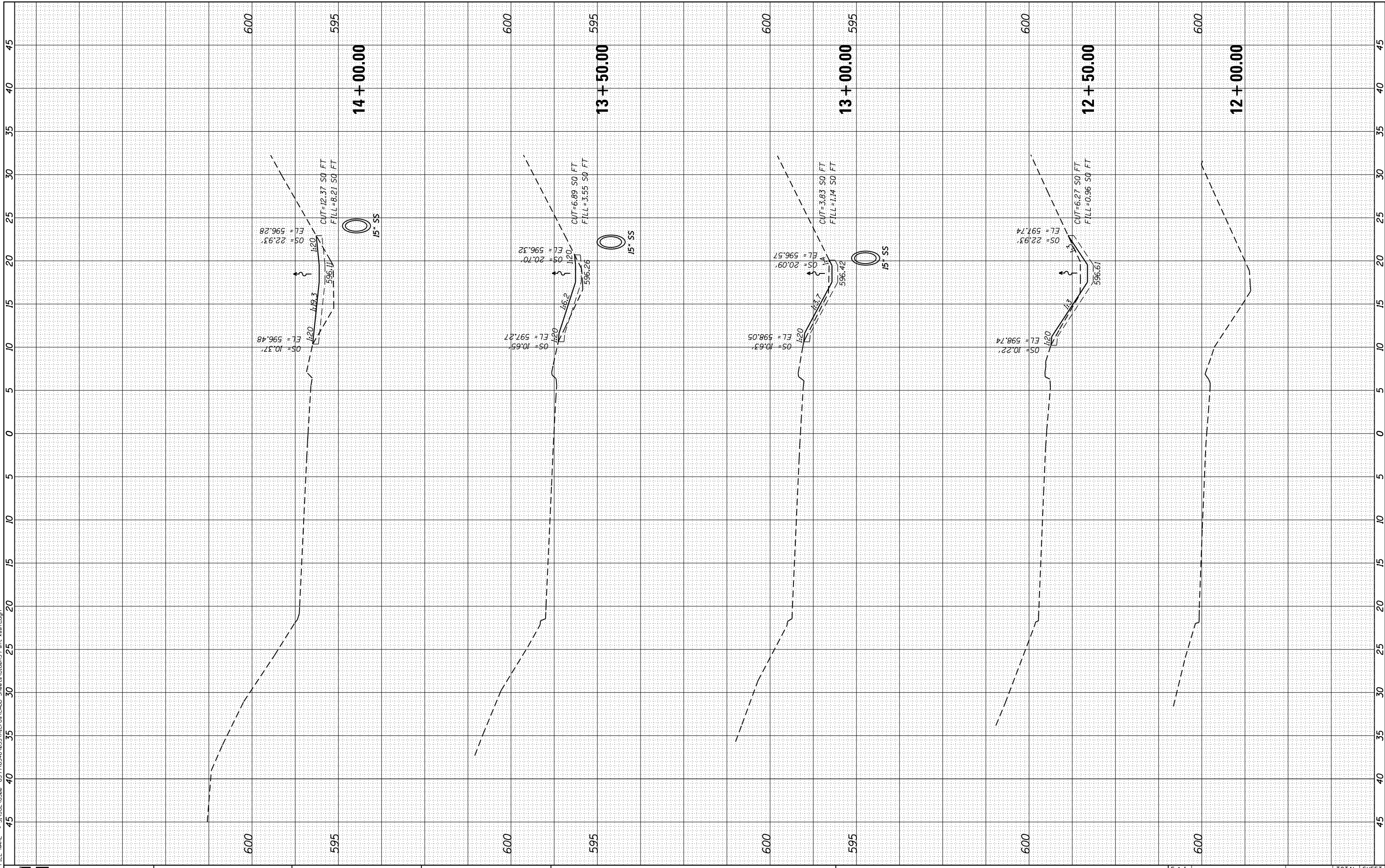
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	33
TS-07		CONTRACT NO. 60T37		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
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AREAS CHECKED	TEMPLATE		
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STRAND ASSOCIATES*
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = dennisw	DESIGNED - TJS	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS

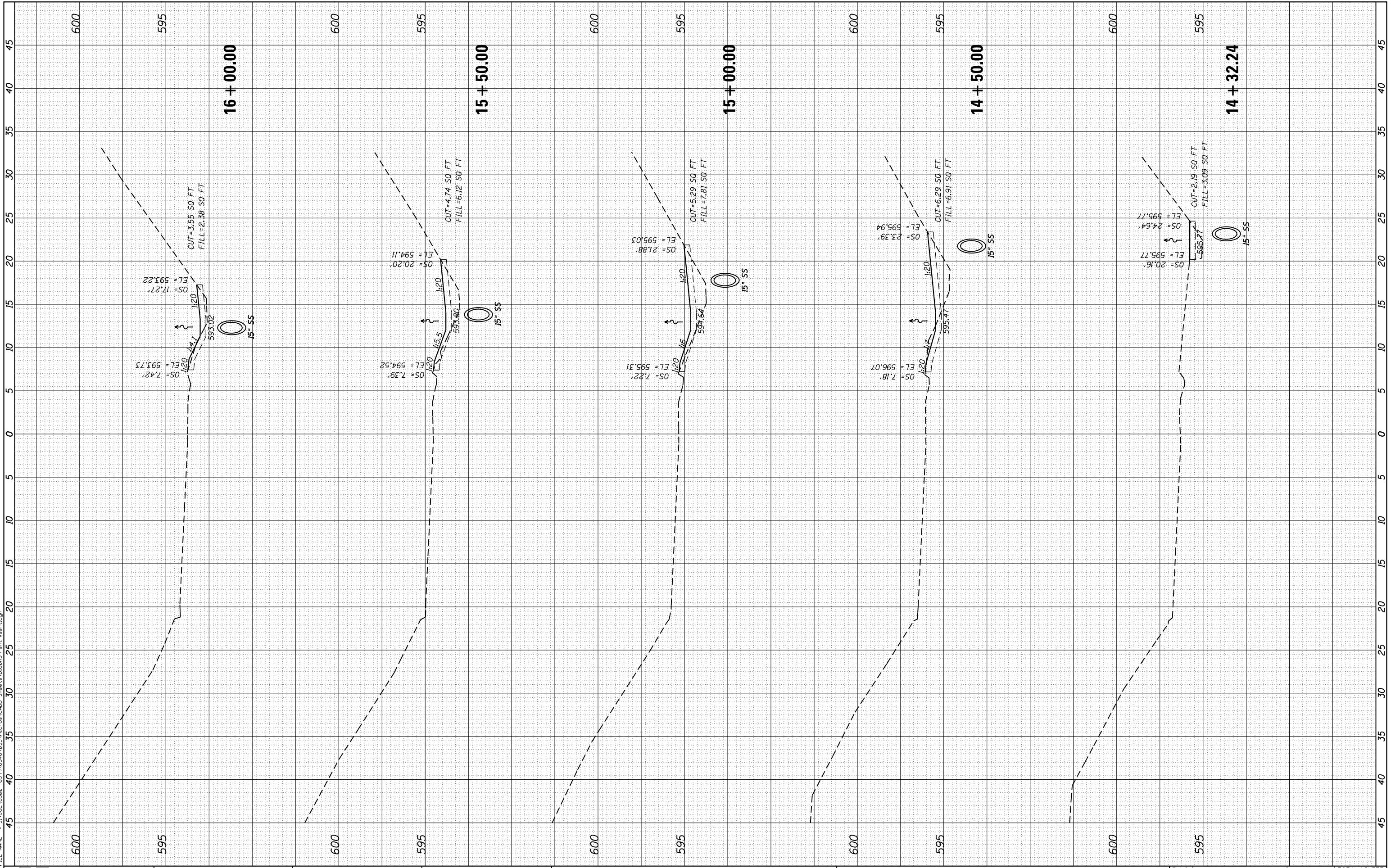
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	34
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

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STRAND ASSOCIATES*
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = dennisw	DESIGNED - TJS	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS

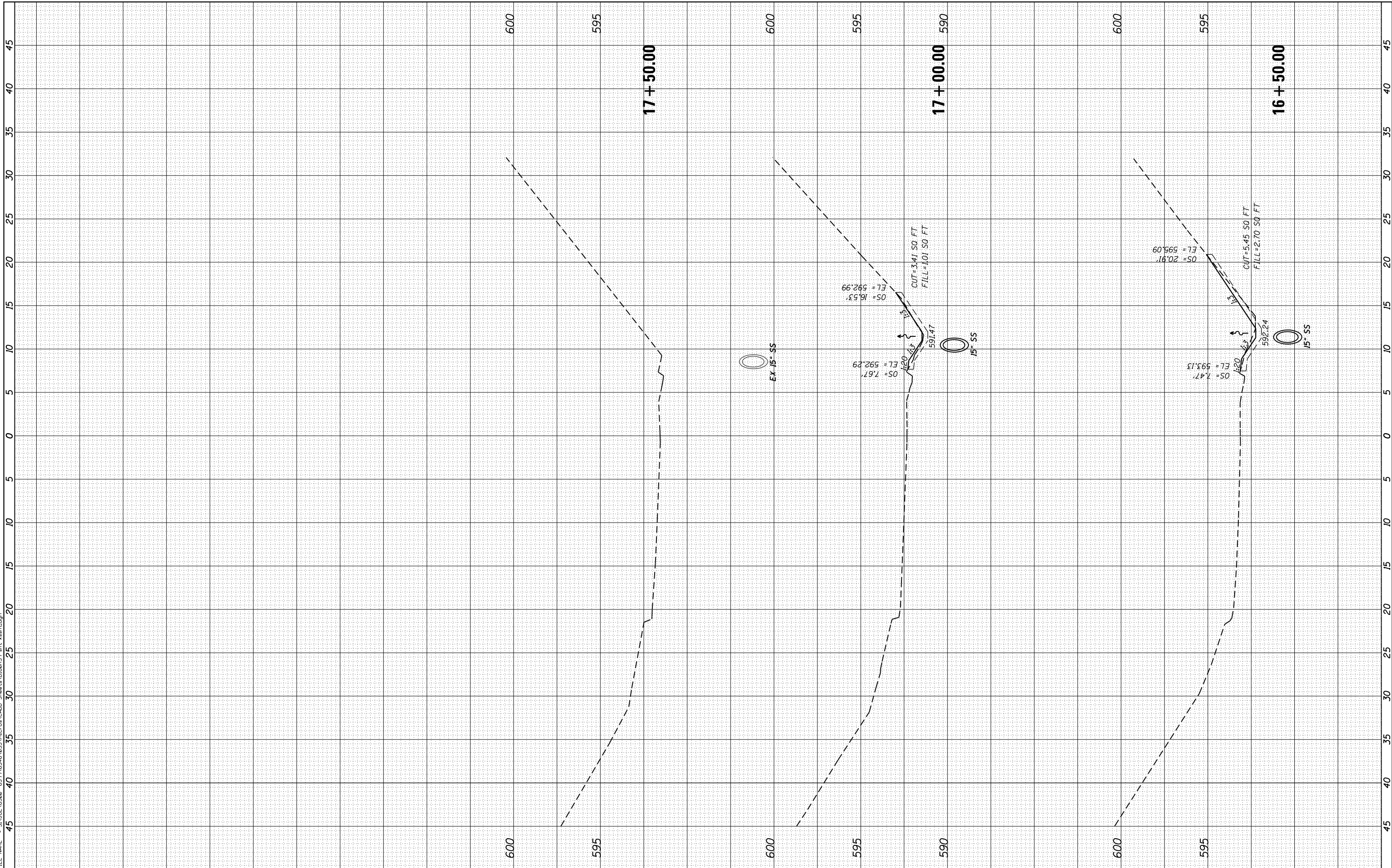
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F.A.I. RTE. 55	SECTION 112-636T	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 35
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
AREAS CHECKED	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
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AREAS CHECKED	TEMPLATE		
AREAS CHECKED	AREAS CHECKED		

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STRAND ASSOCIATES*
1170 SOUTH HOUBOLT ROAD
JOLIET, ILLINOIS 60431
(815) 744-4200

USER NAME = dennisw	DESIGNED - TJS	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - DJW	REVISED -
PLOT DATE = 4/2/2012	CHECKED - MAG	REVISED -
	DATE - 4-2-12	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. 16+50.00 TO STA. 17+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	112-636T	COOK	36	36
CONTRACT NO. 60T37				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				