

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-------------|--------|--------------|-----------|
| 75 | 84-12; 11-3 | * | 729 | 24 |
| STA. N/A TO STA. N/A | | | | |
| FED. ROAD DIST. NO. 5 ILLINOIS FED. AID PROJECT | | | | |
| * SANGAMON AND CHRISTIAN | | | | |

PROPOSED LEGEND

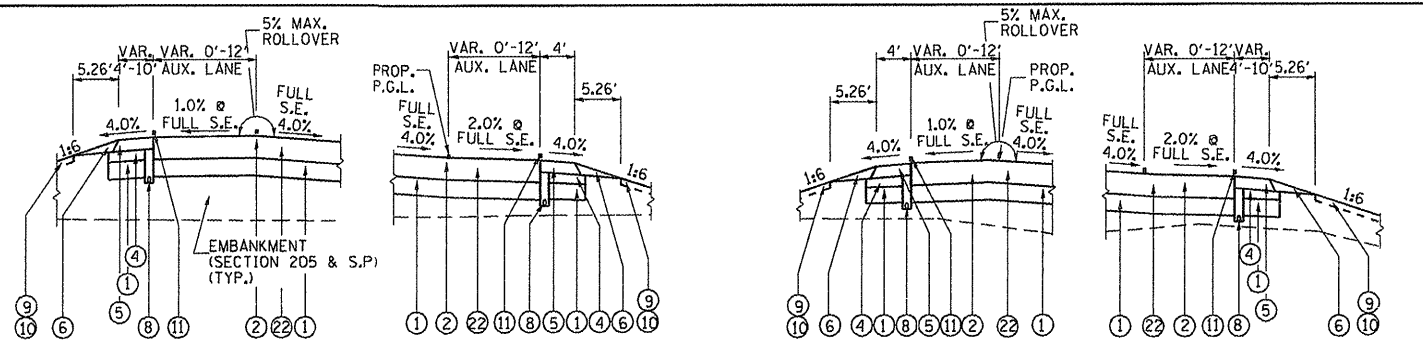
- 1 PROP. PROCESSING MODIFIED SOIL 12"
- 2 PROP. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 13 3/4"
- 3 PROP. BITUMINOUS MATERIALS (PRIME COAT)
- 4 PROP. SUB-BASE GRANULAR MATERIAL, TYPE C
- 5 PROP. HOT-MIX ASPHALT SHOULDERS, 8" (W - W/O RUMBLE STRIP)
- 6 PROP. AGGREGATE SHOULDERS, TYPE A
- 7 PROP. AGGREGATE SHOULDERS, TYPE A, 8"
- 8 PROP. PIPE UNDERDRAINS, 4"
- 9 PROP. SEEDING CLASS 2, WITH MULCH METHOD 2
- 10 PROP. TOPSOIL PLACEMENT, 4"
- 11 PROP. LANE STRIPING (SEE PAVEMENT MARKING PLAN)
- 12 PROP. AGGREGATE BASE COURSE, TYPE A, 10"
- 13 PROP. A-3 BITUMINOUS SURFACE TREATMENT
- 14 PROP. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 9 1/2"
- 15 PROP. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- 16 PROP. PAVEMENT REMOVAL
- 17 FOG TACK COAT PER ENGINEER (ART. 407.13)
- 18 PROP. HOT MIX ASPHALT SURFACE REMOVAL, VARIABLE (1/2" TO 3/4")
- 19 PROP. TOPSOIL PLACEMENT (SPECIAL), 4"
- 20 PROP. INTERSEEDING, CLASS 5 (MODIFIED)
- 21 PROP. STORM SEWER, 18"
- 22 PROP. TOPSOIL EXCAVATION (SPECIAL), 12"
- 23 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- 24 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 1 1/2"
- 25 PROP. HOT-MIX ASPHALT SHOULDERS, 2" (W - W/O RUMBLE STRIP)
- 26 PROP. HOT-MIX ASPHALT LEVELING BINDER (MACHINE METHOD), N50, 3/4"
- 27 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 1 1/2"
- 28 PROP. HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N50, 2 1/2"

EXISTING LEGEND

- 1 EXISTING P.C.C. CONCRETE (9"-6"-9") WITH BASE COURSE WIDENING AND BITUMINOUS OVERLAY
- 2 EXISTING STABILIZED SUB-BASE (12")
- 3 EXISTING BITUMINOUS CONCRETE PAVEMENT, 13 3/4"
- 4 EXISTING BASE COURSE WIDENING (9")
- 5 EXISTING BITUMINOUS CONCRETE OVERLAY (2 1/4" TO 5 1/4")
- 6 EXISTING SUB-BASE GRANULAR MATERIAL
- 7 EXISTING BITUMINOUS CONCRETE SHOULDER (8")
- 8 EXISTING BITUMINOUS CONCRETE SHOULDER (9 1/2")
- 9 EXISTING AGGREGATE SHOULDER (6")
- 10 EXISTING AGGREGATE SHOULDER (8")
- 11 EXISTING PIPE UNDERDRAIN, 4"
- 12 EXISTING OIL AND CHIP PAVEMENT (VARIES)

INDEX OF TYPICAL SHEETS

- SHEET 24 - IL. RTE. 29 DETAILS
- SHEET 25 - SIDEROAD DETAILS
- SHEET 26 - IL. RTE. 29 STA. 1415+00.00 TO STA. 1424+00.00
- IL. RTE. 29 STA. 1424+00.00 TO STA. 1428+17.55
- SHEET 27 - IL. RTE. 29 STA. 1428+17.55 TO STA. 1459+64.30
- IL. RTE. 29 STA. 1459+64.30 TO STA. 1484+15.81
- IL. RTE. 29 STA. 1536+97.95 TO STA. 1586+58.05
- SHEET 28 - IL. RTE. 29 STA. 1484+15.81 TO STA. 1536+97.95
- IL. RTE. 29 STA. 1586+58.05 TO STA. 1610+74.59
- IL. RTE. 29 STA. 53+42.47 TO STA. 105+00.00
- SHEET 29 - IL. RTE. 29 STA. 1610+74.59 TO STA. 1665+04.46
- IL. RTE. 29 STA. 0+00.00 TO STA. 53+42.47
- IL. RTE. 29 STA. 105+00.00 TO STA. 115+98.51
- SHEET 30 - IL. RTE. 29 STA. 115+98.51 TO STA. 130+00.00
- SHEET 31 - TR 81 (2000N)
- SHEET 32 - TR 73 (2050N)
- SHEET 33 - TR 73 CONNECTOR (2050N)
- SHEET 34 - CH 21 (2100N)
- SHEET 35 - CH 23 (800E)
- SHEET 36 - TR 88 (725E) STA. 584+69.04 TO STA. 599+39.00
- SHEET 37 - TR 88 (725E) STA. 600+61.00 TO STA. 604+95.01
- SHEET 38 - TR 74 (625E) / TR 63 (2300N)
- SHEET 39 - TR 486 (11.5E) / TR 321 (7.5S)
- SHEET 40 - TR 299 (7S) / ELM STREET
- SHEET 41 - TR 299 (7S) / ELM STREET
- SHEET 42 - EXISTING IL ROUTE 29

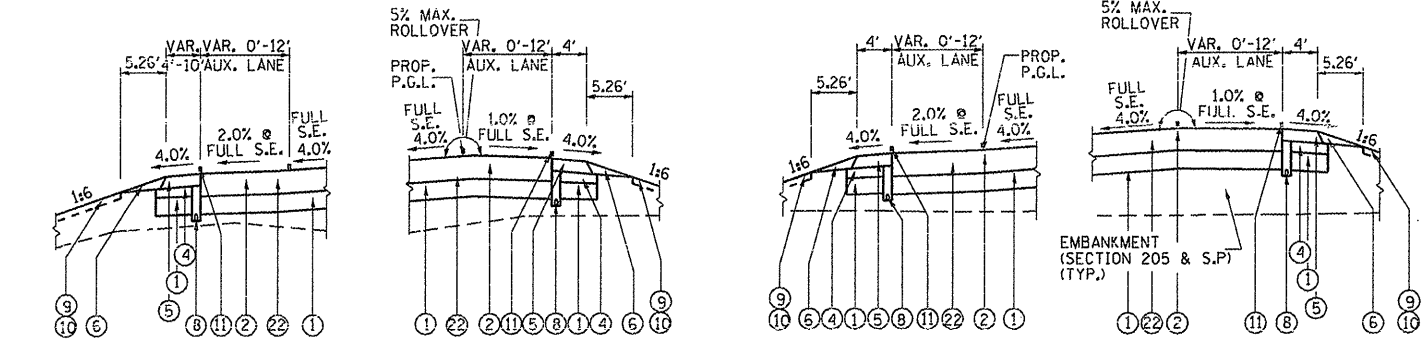


S.B. RIGHT TURN LANE CURVE RIGHT
 STA. 1431+00.79 TO STA. 1435+30.79
 STA. 1459+47.20 TO STA. 1463+77.20

S.B. LEFT TURN LANE CURVE RIGHT
 STA. 1430+48.46 TO STA. 1434+78.46
 STA. 1459+23.26 TO STA. 1463+53.26
 STA. 1512+75.11 TO STA. 1516+48.65

N.B. LEFT TURN LANE CURVE RIGHT
 STA. 1424+73.35 TO STA. 1429+03.35
 STA. 1453+55.76 TO STA. 1457+85.76

N.B. RIGHT TURN LANE CURVE RIGHT
 STA. 1424+11.48 TO STA. 1428+41.48
 STA. 1453+33.43 TO STA. 1457+63.43



S.B. RIGHT TURN LANE CURVE LEFT
 STA. 1485+92.83 TO STA. 1490+22.83

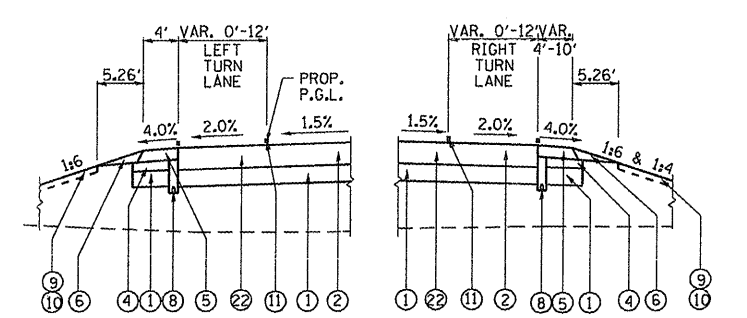
S.B. LEFT TURN LANE CURVE LEFT
 STA. 1485+62.11 TO STA. 1489+92.11

N.B. LEFT TURN LANE CURVE LEFT
 STA. 1479+93.30 TO STA. 1484+23.30
 STA. 1539+56.49 TO STA. 1539+86.49
 STA. 1507+33.35 TO STA. 1511+05.07

N.B. RIGHT TURN LANE CURVE LEFT
 STA. 1479+68.30 TO STA. 1483+98.30
 STA. 1535+32.31 TO STA. 1539+62.31

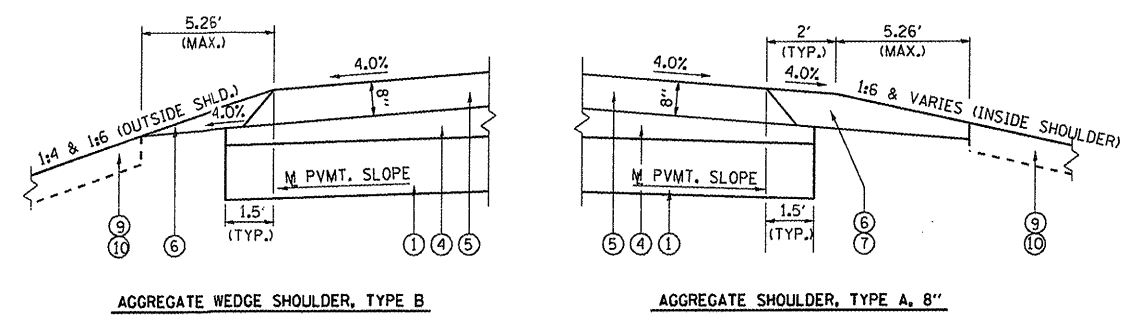
TYPICAL AUXILIARY LANES WITHIN MAINLINE S.E. LIMITS

FOR SUPERELEVATION TRANSITION RATES AND LOCATIONS, SEE SUPERELEVATION DETAILS, CROSS-SECTION SHEETS, AND INTERSECTION DETAIL SHEETS



TYPICAL AUXILIARY LANES WITHIN MAINLINE N.C. LIMITS

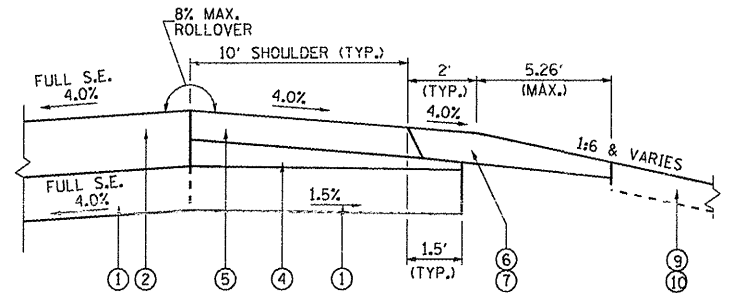
STA. 1511+ TO STA. 1511+
 STA. 1541+26.20 TO STA. 1545+68.20 (LT. & RT. TURN LANES - S.B.)
 STA. 1579+88.05 TO STA. 1593+73.16 (LT. & RT. TURN LANES - N.B. & S.B.)
 STA. 1643+21.94 TO STA. 1653+70.85 (LT. & RT. TURN LANES - N.B. & S.B.)
 STA. 26+27.59 TO STA. 36+44.28 (LT. & RT. TURN LANES - N.B.; LT. TURN LANE - S.B.)
 STA. 66+25.22 TO STA. 76+74.28 (LT. & RT. TURN LANES - N.B. & S.B.)



AGGREGATE WEDGE SHOULDER, TYPE B

AGGREGATE SHOULDER, TYPE A, 8"

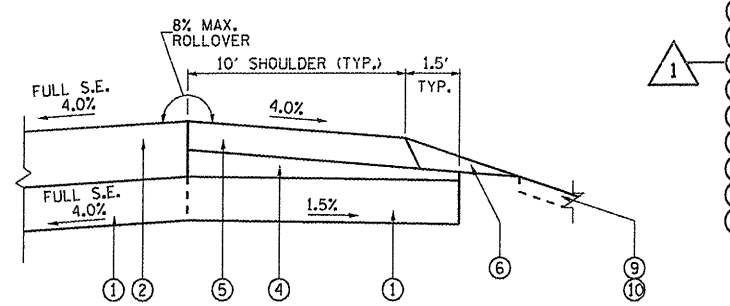
PROPOSED AGGREGATE SHOULDER DETAIL



NOTE: THE CONTRACTOR SHALL BREAK GRADE OF THE PROPOSED LIME MODIFIED SUBGRADE AS DIRECTED BY THE ENGINEER AT THE PROPOSED SHOULDER LINE ON THE HIGH SIDE OF THE S.E. IN ORDER TO MAINTAIN FULL 12" THICKNESS OF THE LIME MODIFIED SUBGRADE.

LIME MODIFIED SUBGRADE DETAIL

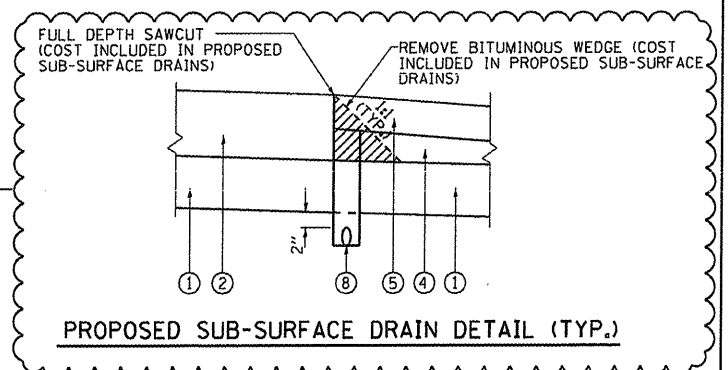
FOR MAINLINE PAVEMENT IN FULL S.E. (INSIDE SHOULDER)



NOTE: THE CONTRACTOR SHALL BREAK GRADE OF THE PROPOSED LIME MODIFIED SUBGRADE AS DIRECTED BY THE ENGINEER AT THE PROPOSED SHOULDER LINE ON THE HIGH SIDE OF THE S.E. IN ORDER TO MAINTAIN FULL 12" THICKNESS OF THE LIME MODIFIED SUBGRADE.

LIME MODIFIED SUBGRADE DETAIL

FOR MAINLINE PAVEMENT IN FULL S.E. (OUTSIDE SHOULDER)



PROPOSED SUB-SURFACE DRAIN DETAIL (TYP.)

| REVISIONS | |
|-----------|---------|
| NAME | DATE |
| REV 1 | 5/23/12 |
| | |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TYPICAL SECTIONS
 IL. RTE. 29 - DETAILS**

SCALE: NONE
 DATE: 03/06/2012

DRAWN BY: B.J.D.
 CHECKED BY: J.M.M.

TYPICAL SECTIONS - STANDARD DETAILS / INDEX

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