

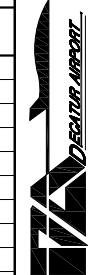
SUMMARY OF QUANTITIES

| ITEM NO. | DESCRIPTION | UNIT | TOTAL QUANTITIES | AS BUILT QUANTITIES |
|----------|-------------------------------------|------|------------------|---------------------|
| AR150510 | ENGINEER'S FIELD OFFICE | L.S. | 1 | |
| AR150530 | TRAFFIC MAINTENANCE | L.S. | 1 | |
| AR150540 | HAUL ROUTE | L.S. | 1 | |
| AR152480 | SHOULDER ADJUSTMENT | S.Y. | 288 | |
| AR401614 | BIT. SURF. CSE.-METHOD II, SUPERPAV | TON | 3,600 | |
| AR401630 | BITUMINOUS SURFACE TEST SECTION | EA. | 1 | |
| AR401650 | BITUMINOUS PAVEMENT MILLING | S.Y. | 14,464 | |
| AR401910 | REMOVE & REPLACE BIT. PAVEMENT | S.Y. | 178 | |
| AR401915 | REM & REP BIT PAVEMENT -- TYPE A | S.Y. | 170 | |
| AR510510 | TIE DOWN | EA. | 36 | |
| AR510900 | REMOVE TIE DOWN | EA. | 66 | |
| AR603510 | BITUMINOUS TACK COAT | GAL. | 5,063 | |
| AR620520 | PAVEMENT MARKING-WATERBORNE | S.F. | 6,313 | |
| AE620525 | PAVEMENT MARKING-BLACK BORDER | S.F. | 5,752 | |
| AR620900 | PAVEMENT MARKING REMOVAL | S.F. | 660 | |
| AR800534 | CONSTRUCT PAVED CRICKET | S.Y. | 118 | |
| AR904510 | SODDING | S.F. | 288 | |

INDEX TO SHEETS

| SHEET NO. | DESCRIPTION |
|-----------|---|
| 1 | COVER SHEET |
| 2 | SUMMARY OF QUANTITIES AND INDEX TO SHEETS |
| 3 | PROPOSED SAFETY PLAN |
| 4 | PROPOSED HAUL ROUTE DETAIL |
| 5 | PROPOSED PAVEMENT PREPARATION PLAN |
| 6 | PROPOSED CONSTRUCTION PLAN |
| 7 | EXISTING AND PROPOSED TYPICAL SECTIONS |
| 8 | PROPOSED TYPICAL SECTIONS FOR CRICKETS |
| 9 | PROPOSED MARKING PLAN |
| 10 | PROPOSED MARKING COORDINATE TABLE |

| REVISION | DATE |
|----------|------|
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |
| | |



DECATUR, ILLINOIS

IL PROJ.: DEC-4102 A.I.P. PROJ.: 3-17-0033-B3

Hanson Proj. No. 10A01360_0001
 Filename R-002ELP_1.DWG
 Scale NOT TO SCALE
 Date 05/11/12

| LAYOUT | JSL | 03/21/11 |
|----------|-----|----------|
| DRAWN | BAK | 03/21/11 |
| REVIEWED | RAW | 04/07/11 |



© Copyright Hanson Professional Services Inc. 2012
Hanson Professional Services Inc.
 1525 South Sixth Street
 Springfield, Illinois 62703-2886
 Ph: (217) 788-2450 Fax: (217) 788-2503
 www.hanson-inc.com
 Offices Nationwide

RECONSTRUCT
 NORTH GA RAMP

SUMMARY OF QUANTITIES
 AND INDEX TO SHEETS

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

HAUL ROUTE AND VEHICLE PARKING

THE HAUL ROUTE AND EQUIPMENT PARKING WILL BE AS SHOWN ON THIS SHEET. SEE SHEET 4 FOR DETAILS.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET AND SHEET 4. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THE PROPOSED EMPLOYEE PARKING AREA AS SHOWN ON THIS SHEET. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR WILL FURNISH ALL HIS EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.

WHEN THE CONTRACTOR'S VEHICLES ARE ON THE AIRPORT SITE THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE (3) FOOT SQUARE FLAG CONSISTING OF A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN (1) FOOT ON EACH SIDE DISPLAYED IN FULL ABOVE WORK VEHICLE.

BARRICADES AND TRAFFIC CONES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND BARRIERS AS SHOWN ON SHEET 4 AND AS DIRECTED BY THE RESIDENT ENGINEER. THE BARRICADES WILL BE IDOT TYPE 1, EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI-TRUCK WITH A TRAILER IN THE UP POSITION.

150-ENGINEER'S FIELD OFFICE NOTES

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 49 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS NOV. 2, 2009.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE CONTRACTOR WILL FURNISH A WIRELESS PHONE TO THE RESIDENT ENGINEER FOR HIS EXCLUSIVE USE FOR THE DURATION OF THIS PROJECT. THE RESIDENT ENGINEER WILL USE THIS PHONE FOR PROJECT BUSINESS ONLY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CHARGES ASSOCIATED WITH THIS CELL PHONE.

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS: AR150510 ENGINEER'S FIELD OFFICE _____ 1 L.S.

SCOPE OF WORK

AIRPORT SECURITY NOTE

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. ONLY CONTRACTOR EMPLOYEES OR MATERIAL SUPPLIERS SHALL BE ALLOWED WITHIN THE PROJECT LIMITS. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE IN THE HAUL ROUTE AT THE END OF EACH WORKING DAY. THE GATES WILL BE SECURED USING DOUBLE LOCKS. THIS WILL ALLOW THE CONTRACTOR AND THE AIRPORT PERSONNEL TO USE THE GATE INDEPENDENT OF EACH OTHER.

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATION

COMPLETED WORK CANNOT BE PLACED ON A CONSTRUCTION REPORT UNTIL ALL MATERIAL CERTIFICATIONS FOR THAT PAY ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER.

BENCHMARK DATA

| NO. | DESCRIPTION | ELEV. |
|-----|--|--------|
| 1 | BM #6 CHISELED SQUARE ON SOUTHEAST CORNER OF CONCRETE FOUNDATION OF TAXIWAY SIGN | 676.13 |
| 2 | CB-51 CHISELED SQUARE ON SOUTH SIDE OF CONCRETE LID OF DROP INLET, NORTH END OF GRASS MEDIAN | 672.91 |
| 3 | TP #41 USGS BRASS DISC STAMPED "AP STA A 1965, STA. 171+57, 111' RT. RUNWAY 18-36 | 676.19 |
| 4 | CB-53 CHISELED SQUARE ON NORTHWEST CORNER OF CONCRETE LID OF DROP INLET IN APRON | 676.22 |

DE073

J.U.L.I.E. INFORMATION

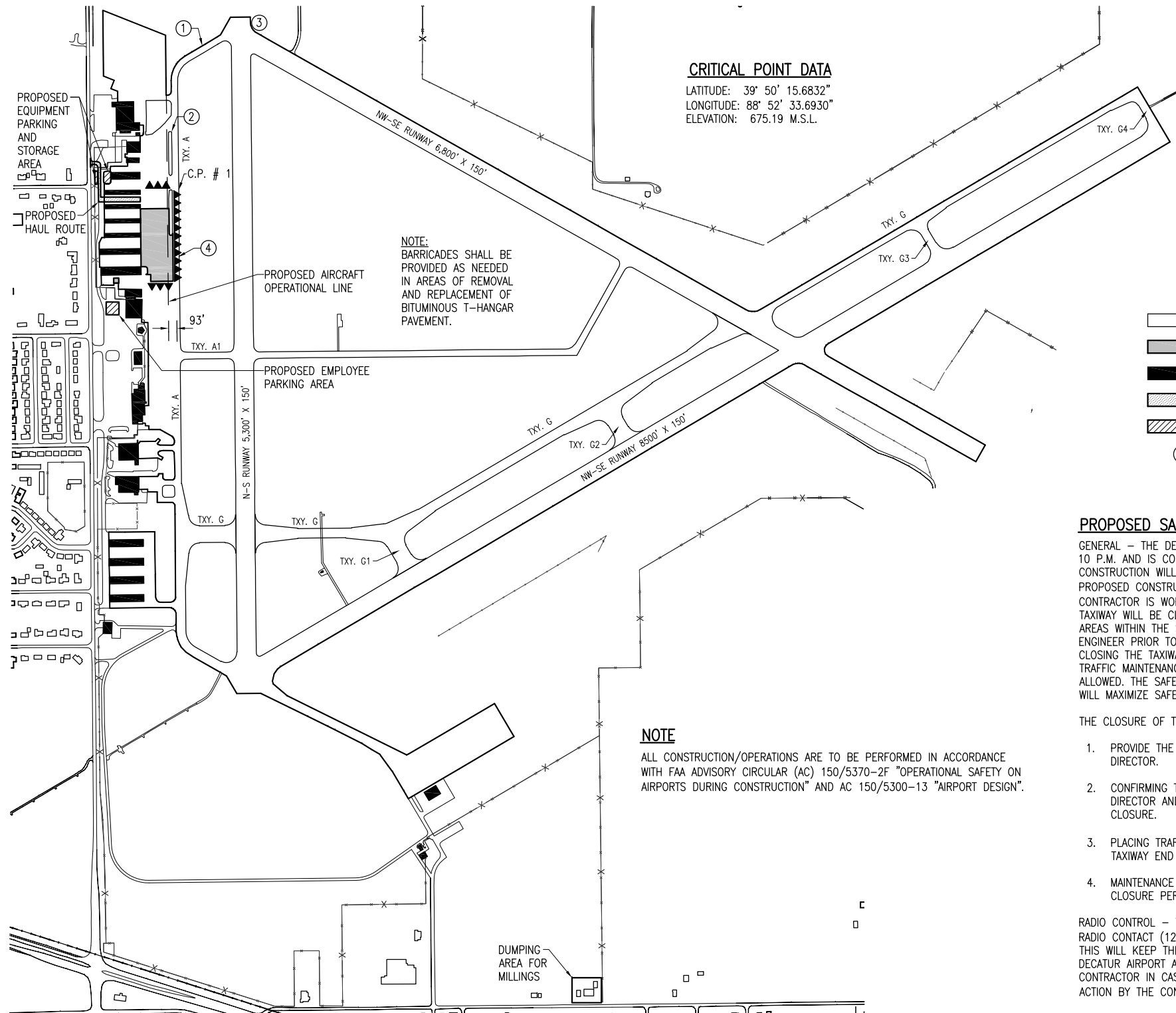
COUNTY _____ MACON
 CITY _____ DECATUR
 TOWNSHIP _____ LONG CREEK
 SECTION NO. _____ 20 & 21
 ADDRESS _____ DECATUR AIRPORT
 AIRPORT ROAD
 DECATUR, ILLINOIS 62524

EROSION CONTROL

THE PROPOSED CONSTRUCTION WILL NOT DISTURB MORE THAN 1 ACRE OF LAND. THEREFORE A N.P.D.E.S. PERMIT WILL NOT BE REQUIRED.

AIRCRAFT OPERATION LINE

THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION AND WILL PLACE A FRANGIBLE MARKER (TRAFFIC CONE) EVERY 150' ALONG IT. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN THE TAXIWAY IS NOT CLOSED.



CRITICAL POINT DATA

LATITUDE: 39° 50' 15.6832"
 LONGITUDE: 88° 52' 33.6930"
 ELEVATION: 675.19 M.S.L.

NOTE: BARRICADES SHALL BE PROVIDED AS NEEDED IN AREAS OF REMOVAL AND REPLACEMENT OF BITUMINOUS T-HANGAR PAVEMENT.

NOTE

ALL CONSTRUCTION/OPERATIONS ARE TO BE PERFORMED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2F "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND AC 150/5300-13 "AIRPORT DESIGN".

LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE
- PROPOSED EQUIPMENT AND EMPLOYEE PARKING
- PROPOSED BENCHMARK
- PROPOSED BARRICADES OR TRAFFIC CONES

PROPOSED SAFETY PLAN

GENERAL - THE DECATUR AIRPORT IS TOWER CONTROLLED FROM 6 A.M. TO 10 P.M. AND IS COMPRISED OF THREE RUNWAYS. THE PROPOSED CONSTRUCTION WILL NOT NECESSITATE CLOSING ANY RUNWAYS. THE PROPOSED CONSTRUCTION WILL EFFECT TAXIWAY "A". ANY TIME THE CONTRACTOR IS WORKING WITHIN 93' OF THE TAXIWAY CENTERLINE, THE TAXIWAY WILL BE CLOSED. THE CONTRACTOR WILL SMOOTH GRADE ALL AREAS WITHIN THE SAFETY AREA TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR TO REOPENING THE TAXIWAY. ALL WORK IN OPENING AND CLOSING THE TAXIWAY WILL BE CONSIDERED INCIDENTAL TO THE AR150530 TRAFFIC MAINTENANCE PAY ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE SAFETY PLAN OUTLINED HERE AND IN THE SPECIAL PROVISION WILL MAXIMIZE SAFETY AND ALLOW MINIMUM TAXIWAY CLOSURE.

THE CLOSURE OF THE TAXIWAY WILL REQUIRE THE FOLLOWING:

1. PROVIDE THE SPECIFIED ADVANCE NOTIFICATION TO THE AIRPORT DIRECTOR.
2. CONFIRMING THE ISSUANCE OF THE NOTAM WITH THE AIRPORT DIRECTOR AND CONTROL TOWER PRIOR TO STARTING THE ACTUAL CLOSURE.
3. PLACING TRAFFIC CONES AND BARRICADES (SEE NOTE BELOW) AT THE TAXIWAY END OR AS DIRECTED BY THE RESIDENT ENGINEER.
4. MAINTENANCE OF THE TRAFFIC CONES AND BARRICADES DURING THE CLOSURE PERIOD.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (121.75 MHz.) WITH THE AIRPORT ATCT GROUND CONTROL. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE DECATUR AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

| REVISION | DATE |
|----------|------|
| | |
| | |
| | |
| | |

DECATUR AIRPORT
 DECATUR, ILLINOIS
 I.L. PROJ.: DEC-4102 A.I.P. PROJ.: 3-17-0033-B3

| | | |
|--------------------------------|-----|----------|
| Hanson Proj. No. 10A01360_0001 | JSL | 03/21/11 |
| Filename R-0035SY.DWG | BAK | 03/21/11 |
| Scale 1" = 500' | RAW | 04/07/11 |
| Date 05/11/12 | | |

HANSON
 Hanson Professional Services Inc. 2012
 Hanson Professional Services Inc.
 1525 South Sixth Street
 Springfield, Illinois 62703-2886
 Ph: (217) 788-2450 Fax: (217) 788-2503
 www.hanson-inc.com
 Offices Nationwide

RECONSTRUCT NORTH GA RAMP
PROPOSED SAFETY PLAN
3
 3 of 10 sheets

CONSTRUCTION SEQUENCING:

THE CONTRACTOR SHALL COMPLETE THE RAMP REHABILITATION IN TWO PHASES, DIVIDED BY A NORTH/SOUTH LINE AS SHOWN ON THIS SHEET. THE EASTERN PORTION SHALL BE COMPLETED FIRST.

PHASE 1

PRIOR TO STARTING WORK IN THE PHASE 1 AREA, LIGHTED BARRICADES SHALL BE POSITIONED ON ALL SIDES OF THE CLOSED AREA AND AS DIRECTED TO CLOSE TAXIWAY A AND THE PORTION OF THE RAMP WITHIN THE LIMITS SHOWN. THE CONTRACTOR SHALL COMPLETE ALL PAVEMENT REMOVAL AND PAVING WORK ON THE EAST PORTION OF THE RAMP, INCLUDING THE REMOVAL AND INSTALLATION OF THE TIE DOWNS. THE PAVEMENT MARKINGS AND SHOULDER WORK MAY BE COMPLETED AT THE END OF PHASE 2. ACCESS TO THE T-HANGARS SHALL BE KEPT OPEN DURING THIS PHASE AND A FLAGGER WITH AN AIRFIELD RADIO WILL BE POSITIONED AT THE NORTH END OF THE PROJECT LIMITS TO DIRECT TRAFFIC WHEN CONTINUOUS HAULING OPERATIONS ARE IN EFFECT, NOTING THAT AIRCRAFT WILL HAVE THE RIGHT-OF-WAY AT ALL TIMES.

ONCE WORK IS COMPLETED IN PHASE 1 AREA AS SPECIFIED, THE CONTRACTOR SHALL CLEAN TAXIWAY A AND THE AFFECTED RAMP AREA AND REOPEN. ALL BARRICADES SHALL BE REMOVED FROM THE TAXIWAY OBJECT FREE AREA - WHICH FOR TAXIWAY A EXISTS AT A DISTANCE OF 93' FROM THE TAXIWAY CENTERLINE, MINIMUM. THE CONTRACTOR SHALL HAVE 21 CONSECUTIVE CALENDAR DAYS TO COMPLETE THIS PHASE OF WORK.

PHASE 2

ONCE PHASE 1 WORK IS COMPLETE, THE CONTRACTOR SHALL CLOSE THE PHASE 2 AREA AND COMPLETE ALL WORK WEST OF THE DIVIDING LINE, INCLUDING THE CRICKETS AND THE FULL DEPTH PATCH SOUTH OF HANGAR G. THE CONTRACTOR SHALL COMPLETE THE PHASE 2 WORK IN 14 CONSECUTIVE CALENDAR DAYS, INCLUDING THE PAVEMENT MARKINGS. AT THE END OF THE 14 CALENDAR DAYS, THE PHASE 2 AREA MUST BE SWEEP BROOM CLEAN AND RE OPENED TO TRAFFIC. ONCE THE SHOULDER WORK IS COMPLETED AND THE ENTIRE JOBSITE HAS BEEN SWEEP CLEAN, THE CONTRACTOR SHALL REMOVE ALL BARRICADES AND OPEN RAMP TO TRAFFIC.

HAUL ROUTE, CONSTRUCTION ACCESS, AND EQUIPMENT PARKING

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE, CONSTRUCTION ACCESS, EQUIPMENT STORAGE, AND AUTO PARKING AREA AS SHOWN ON THIS SHEET. THE CONTRACTOR SHALL IDENTIFY 24' WIDTH HAUL ROUTE CENTERED BETWEEN THE HANGARS USING TRAFFIC CONES AND SHALL NOT DEVIATE FROM THIS PATH. THE PAVEMENTS WILL BE KEPT BROOMED CLEAN. THE PROPOSED PARKING AREA WILL BE 175' X 100' (APPROXIMATE). THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE, CONSTRUCTION ACCESS, EQUIPMENT STORAGE, AND AUTO PARKING AREA THROUGH OUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL RESTORE THE HAUL ROUTE, CONSTRUCTION ACCESS, EQUIPMENT STORAGE AND AUTO PARKING AREA IN ACCORDANCE TO THE SPECIAL PROVISIONS. RESTORATION OF THE HAUL ROUTE, CONSTRUCTION ACCESS, EQUIPMENT STORAGE, AND AUTO PARKING AREA WILL BE PAID FOR IN ACCORDANCE WITH ITEM:

AR150540 "HAUL ROUTE" PER LUMP SUM, AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.

MAINTENANCE OF TRAFFIC

AIRPORT MANAGEMENT (AIRPORT OPERATIONS MANAGER) WILL AT ALL TIMES HAVE JURISDICTION OVER THE SAFETY OF AIR AND GROUND TRAFFIC DURING CONSTRUCTION. WHEREVER SAFETY OF AIRPORT USERS (AIRCRAFT OR VEHICLE) IS CONCERNED, HIS DECISIONS AS TO METHODS, PROCEDURES AND MEASURES USED SHALL BE FINAL, AND ANY AND ALL CONTRACTORS PERFORMING WORK WILL BE GOVERNED BY SUCH DECISIONS.

THE CONTRACTOR SHALL NOT BE ENTITLED TO ANY EXTRA COMPENSATION DUE TO DELAYS OR INCONVENIENCE CAUSED BY SAID NECESSARY METHODS, PROCEDURES AND MEASURES.

FLAG PERSON






WHEN USING THE HAUL ROUTE, THE CONTRACTOR SHALL POSITION A FLAG PERSON AS APPROPRIATE TO DIRECT TRAFFIC OF BOTH CONTRACTOR AND NON-CONTRACTOR VEHICLES USING THIS ACCESS POINT. AREA SHOWN IS APPROXIMATE.

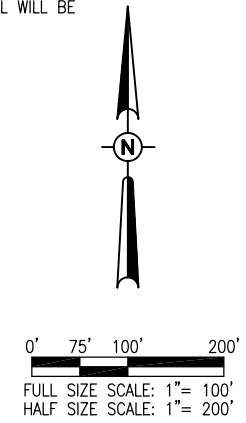
THE FLAG PERSON WILL BE EQUIPPED WITH AN AIRCRAFT RADIO AND TRAINED ON ITS USE, SO THAT THE TOWER CONTROL PERSONNEL CAN BE IN COMMUNICATION IN THE EVENT OF AN EMERGENCY. THE FLAG PERSON SHALL ALSO BE EQUIPPED WITH A PORTABLE STOP AND GO SIGN TO CONTROL VEHICULAR TRAFFIC.

PAYMENT FOR THE FLAG PERSON AND EQUIPMENT AND ALL ASPECTS OF TRAFFIC CONTROL WILL BE PAID FOR UNDER ITEM:

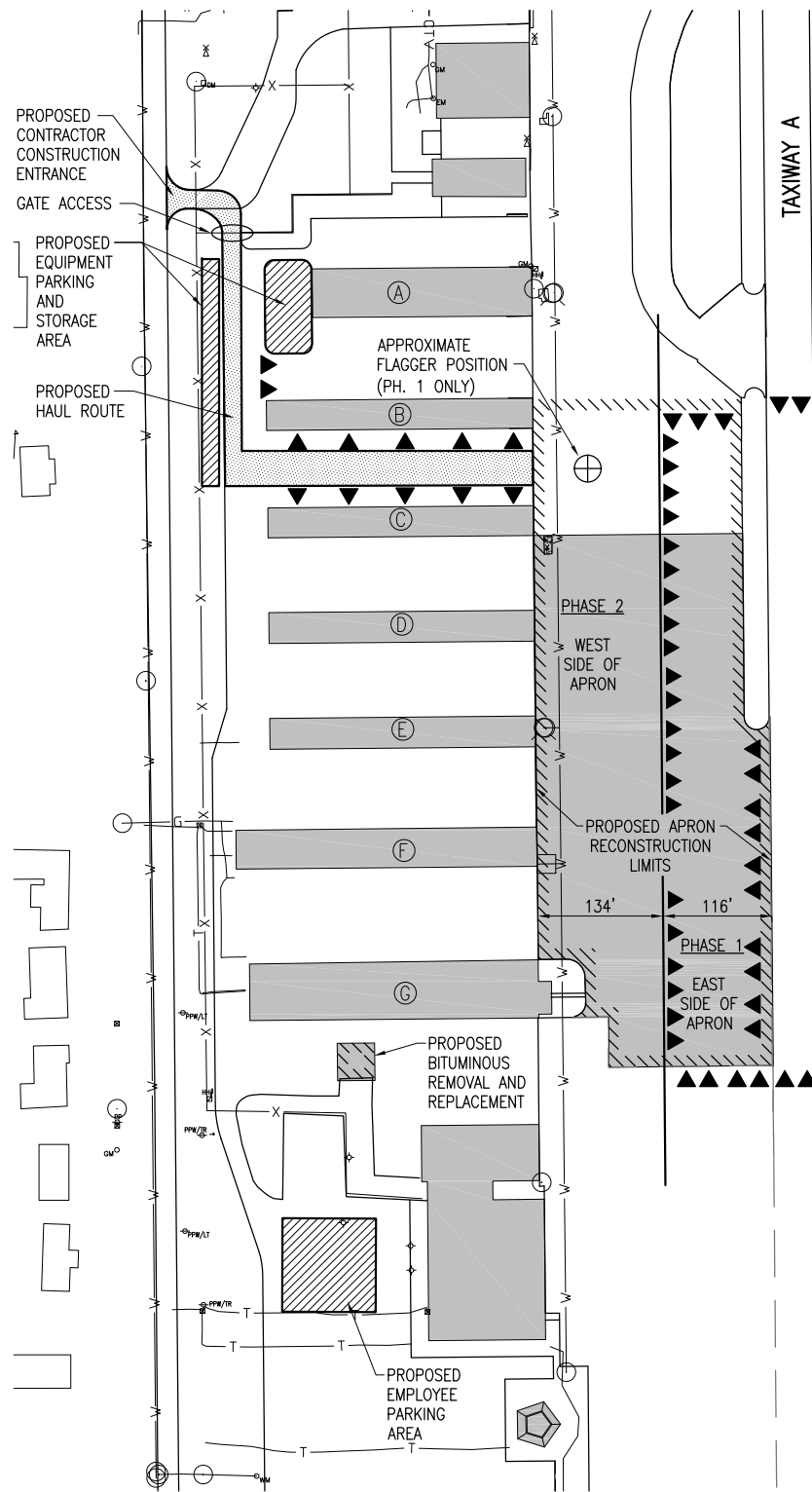
AR150530 TRAFFIC MAINTENANCE PER LUMP SUM.

LEGEND

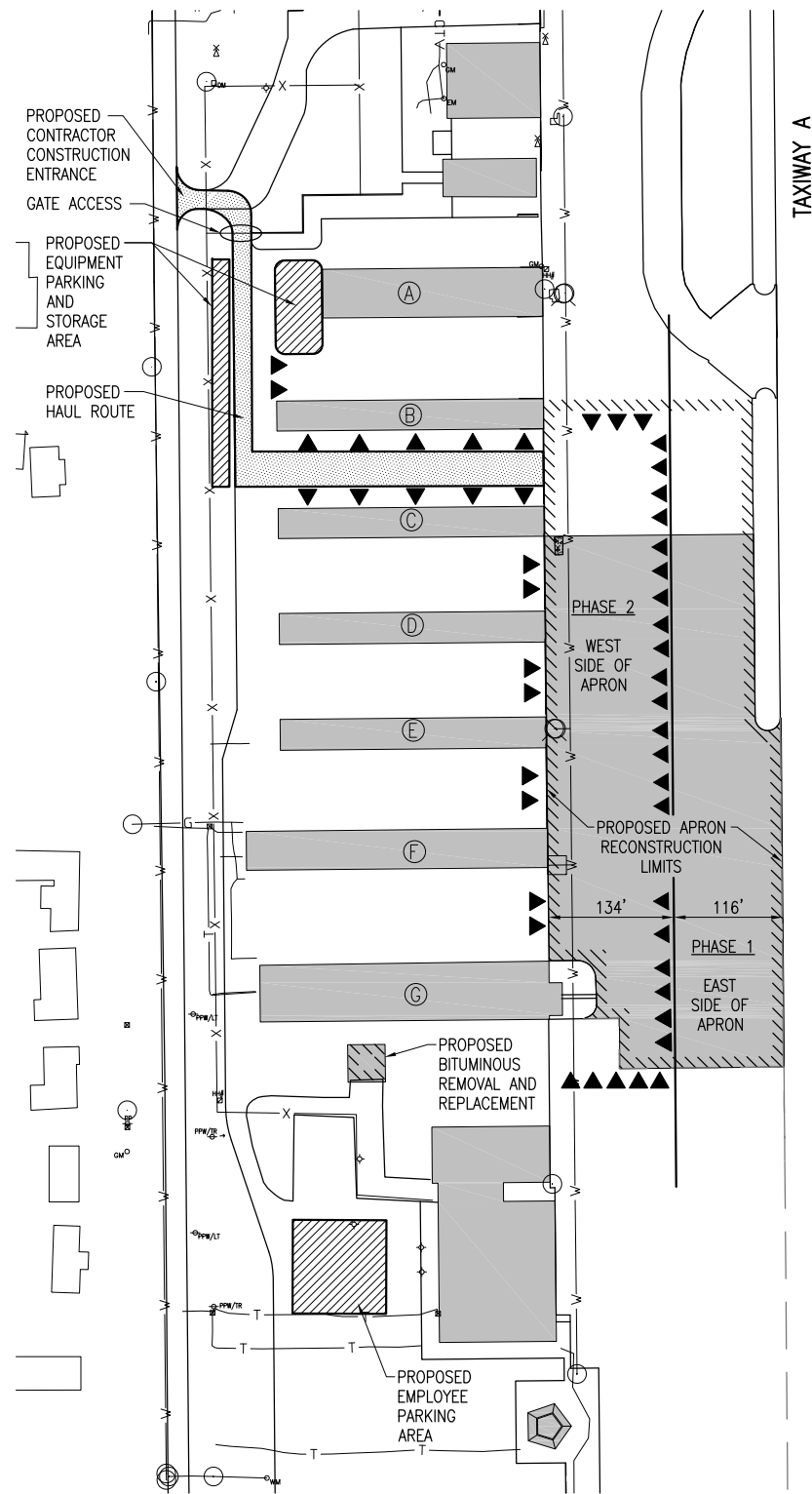
-  EXISTING PAVEMENT
-  EXISTING BUILDING
-  PROPOSED WORK AREA
-  PROPOSED HAUL ROUTE
-  PROPOSED EQUIPMENT STORAGE AND AUTO PARKING LOT





PHASE 1 STAGING PLAN

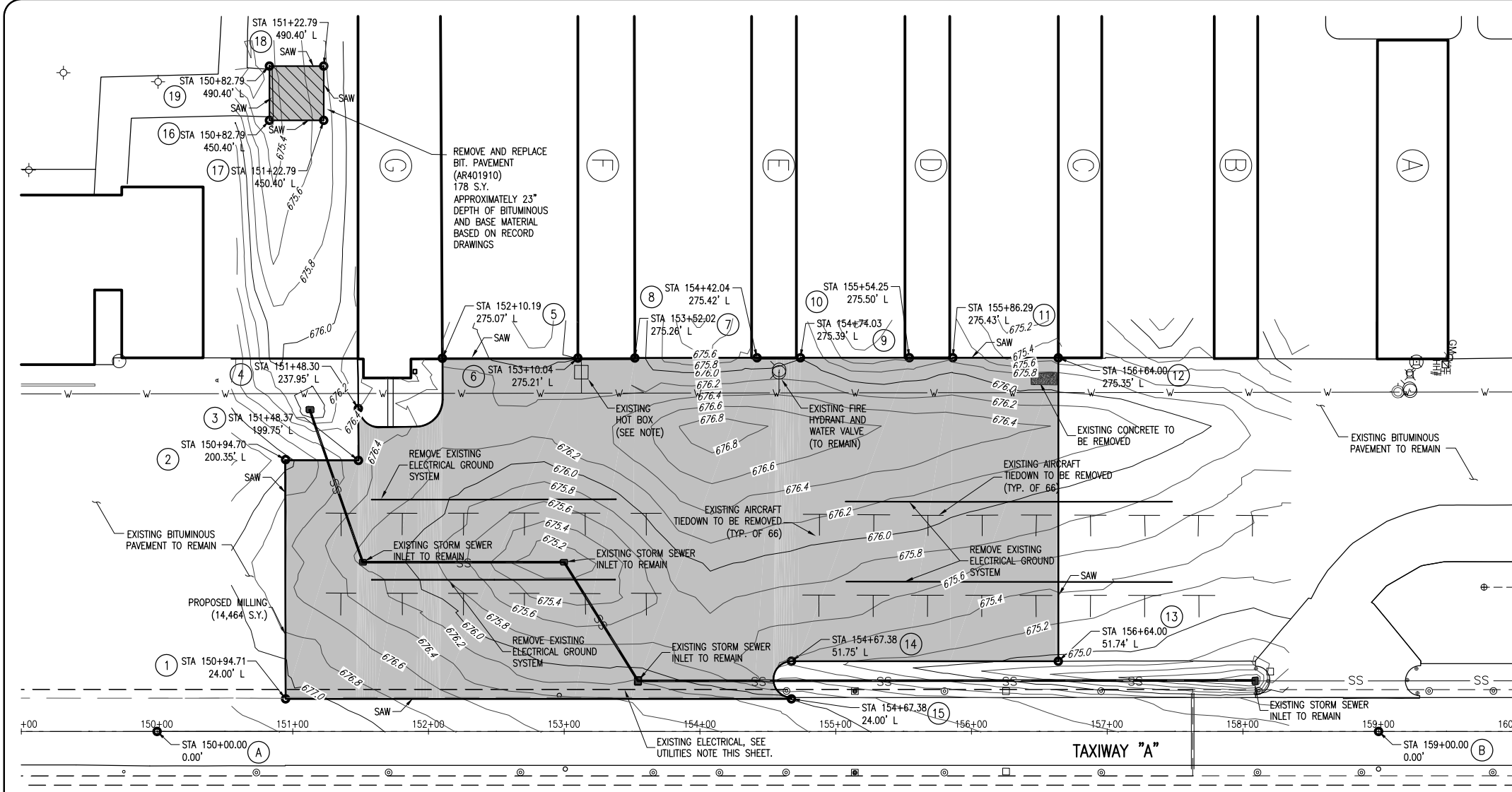


STAGE 2 STAGING PLAN



MAY 14, 2012 9:00 AM HARR01115
C:\PWISE_WORK\DO_NOT_DELETE\DM627480\R-008RTE.DWG

| | | | | | |
|---|--|--|--|--|--|
| REVISION | | | | | |
| DATE | | | | | |
|  DECATUR, ILLINOIS I.L. PROJ.: DEC-4102 A.I.P. PROJ.: 3-17-0033-B3 | | | | | |
| Hanson Proj. No. 10A0136D_0001 Filename R-008RTE.DWG Scale 1" = 100' Date 05/11/12 | JSL 03/21/11 BAK 03/21/11 RAW 04/07/11 | | | | |
|  HANSON © Copyright Hanson Professional Services Inc. 2012 Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Ph: (217) 788-2450 Fax: (217) 788-2503 www.hanson-inc.com Offices Nationwide | | | | | |
| RECONSTRUCT NORTH GA RAMP | PROPOSED HAUL ROUTE DETAIL | | | | |
| 4 | | | | | |
| 4 of 10 sheets | | | | | |



LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDING
- PROPOSED BITUMINOUS PAVEMENT MILLING
- PROPOSED PAVEMENT PATCH
- EXISTING ELECTRICAL CABLE
- EXISTING BASE MOUNT TAXIWAY LIGHT
- EXISTING STAKE MOUNT TAXIWAY LIGHT
- EXISTING INLET
- EXISTING MANHOLE
- EXISTING GAS VALVE
- EXISTING WATER VALVE
- EXISTING HYDRANT
- EXISTING HANDHOLE

REM & REPL BIT PAVEMENT – TYPE A (AR401915)

A NOMINAL AMOUNT OF BITUMINOUS CRACK REPAIR HAS BEEN INCLUDED IN THE PLAN QUANTITIES BASED ON THE ANTICIPATION THAT SEVERAL, BUT NOT ALL, EXISTING CRACKS THAT ARE GREATER THAN 1 INCH IN WIDTH AT THE EXISTING SURFACE, BUT NOT EXHIBITING ROLLED EDGES, WILL NEED TO BE REPAIRED. THE QUANTITY INCLUDED IS BASED UPON A PAVEMENT CONDITION AND CRACK SURVEY WHICH WAS PERFORMED IN THE WINTER OF 2010. THE ACTUAL LENGTH AND NUMBER OF CRACKS TO BE REPAIRED AS PART OF THIS PROJECT WILL BE DESIGNATED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION, AFTER THE MILLING OPERATIONS.

THE CRACKS SHALL BE CUT OPEN USING A MILLING MACHINE AT A WIDTH OF 24", AND A DEPTH OF 3", FOLLOWING THE CRACK CENTERLINE AS CLOSELY AS OBTAINABLE WITH THE SUGGESTED EQUIPMENT. SEE SHEET 5 FOR THE TYPICAL SECTION OF THE EXISTING PAVEMENT (BASED ON RECORD DRAWING CONSTRUCTION PLANS).

THE EXCAVATED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE BY THE CONTRACTOR.

THE HOLES SHALL BE BLOWN DUST FREE WITH COMPRESSED AIR PRIOR TO THE APPLICATION OF THE BITUMINOUS TACK MATERIAL. AN APPLICATION OF BITUMINOUS TACK MATERIAL WILL BE APPLIED TO THE VERTICAL BITUMINOUS FACE. NO AGGREGATE BASE IS ANTICIPATED NECESSARY BASED ON RECORD DRAWING CONSTRUCTION PLANS. THE REPAIR TRENCH WILL BE BACKFILLED WITH BITUMINOUS SURFACE COURSE MATERIAL (401), IN ONE 3 INCH LIFT. THE REPAIRED AREA SHALL BE FLUSH WITH THE SURFACE OF THE EXISTING PAVEMENT. THE PATCH SHALL BE COMPACTED TO THE SATISFACTION OF THE RESIDENT ENGINEER AND ACCEPTED. NO NUCLEAR DENSITY TESTING SHALL BE REQUIRED FOR THIS TASK.

ANY DAMAGE TO THE EXISTING ADJACENT PAVEMENT WILL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE. THE QUANTITY OF PAVEMENT REMOVAL, MILLING AND/OR SAWCUTS, IF UTILIZED, TACK MATERIAL, AND BITUMINOUS SURFACE COURSE MATERIAL (401) NECESSARY TO COMPLETE THIS TASK WILL BE CONSIDERED INCIDENTAL TO THE TASK, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

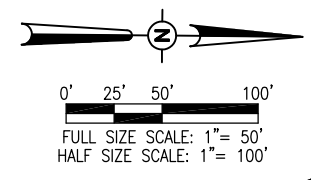
THE BITUMINOUS CRACK REPAIR WILL BE PAID FOR UNDER ITEM:
AR401915 REM & REPL BIT PAVEMENT – TYPE A – PER S.Y.

ELECTRICAL GROUNDING SYSTEM REMOVAL

THE EXISTING GROUNDING SYSTEM CONSISTS OF PERIODIC GROUND RODS AND WIRING, PRESUMABLY COPPER, THAT IS NO LONGER NEEDED. WHERE ENCOUNTERED WITHIN THE CONSTRUCTION LIMITS, THE CONTRACTOR IS TO EITHER COMPLETELY REMOVE THE RODS AND WIRING OR CUT OFF FLUSH WITH THE PROPOSED MILLED SURFACE. THE REMOVAL AND OFFSITE DISPOSAL OF THE GROUNDING SYSTEM SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE BITUMINOUS PAVEMENT MILLING.

HOT BOX NOTE

MILLING SHALL BE COMPLETED WITH IN A FOOT OF THE EXISTING HOT BOX. THE WORK TO SAW CUT, IF NECESSARY, AROUND THIS UTILITY SHALL BE INCIDENTAL IN ORDER TO PROVIDE A NEAT AND CONSTRUCTABLE PAVEMENT.



BITUMINOUS PAVEMENT MILLING (AR401650)

THE CONTRACTOR WILL REMOVE THE EXISTING BITUMINOUS PAVEMENT IN THE AREA SHOWN BY MEANS OF ROTO-MILLING 4" NOMINAL DEPTH. THE AIRPORT WILL RETAIN 15 TANDEM TRUCK LOADS OF MILLINGS; DELIVERED AND DUMPED AT THE LOCATION SHOWN ON SHEET 3. THE REMAINDER WILL BE HAUL OFF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THE EXISTING GRADE AND CONTOURS ARE REESTABLISHED IN THE PROPOSED PAVEMENT AND THAT NO PONDING OF WATER OR BUMPS ARE FORMED. ALL EDGES SHALL BE SAW CUT A MINIMUM TWO INCHES IN DEPTH TO CREATE A UNIFORM AND STRAIGHT FACE. THE REMOVAL OF ALL CUTTING AND EDGE SAWING SHALL BE CONSIDERED INCIDENTAL TO THE UNIT PRICE FOR:

AR401650 "BITUMINOUS PAVEMENT MILLING" _____ PER S.Y.

REMOVE & REPLACE BIT. PAVEMENT (AR401910)

THE CONTRACTOR SHALL SAWCUT THE OUTER EDGES OF THE AREA TO BE PATCHED, REMOVE THE EXISTING MATERIAL, INSTALL A STABILIZATION GEOGRID, CONSTRUCT A NEW CRUSHED AGGREGATE BASE COURSE, AND BACKFILL IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR THIS ITEM.

THE PAVEMENT REPAIR SHALL BE PAID FOR WITHIN ITEM:
AR401910 REMOVE & REPLACE BIT. PAVEMENT – PER S.Y..

PCC PAVEMENT PATCH REMOVAL

AN EXISTING PCC PAVEMENT PATCH EXISTS (FROM A PREVIOUS UTILITY CUT) WITHIN THE LIMITS OF THE PROPOSED PAVEMENT MILLING. THE PATCH WILL BE REMOVED FULL DEPTH PRIOR TO THE MILLING OPERATIONS, BUT SHALL BE CONSIDERED INCIDENTAL TO THE MILLING PAY ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. MATERIAL SHALL BE DISPOSED OF OFFSITE. THE VOID LEFT BY THE REMOVAL SHALL BE BACKFILLED WITH BITUMINOUS SURFACE COURSE MATERIAL, THE QUANTITY OF WHICH IS INCLUDED WITHIN AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH PAY ITEM AR401614 – BIT. SURF. CSE.–METHOD II, SUPERPAV.

UTILITIES

ELECTRICAL CABLES PARALLEL THE PAVEMENTS WITH 10' TO 15' SEPARATION AND ARE BURIED AT A DEPTH OF APPROXIMATELY 18 INCHES. ALSO, OTHER CABLES ARE BURIED IN THE VICINITY. BEFORE ANY DIGGING OR TRENCHING, ALL CABLES ARE TO BE LOCATED BY THE CONTRACTOR.

STORMWATER INLETS AND UNDERGROUND CONCRETE PIPES EXIST WITHIN THE LIMITS OF THE RECONSTRUCTION. THE CONTRACTOR SHALL PROTECT THESE IMPROVEMENTS FROM ANY DAMAGE DURING MILLING AND PAVING OPERATIONS. DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AT HIS OWN EXPENSE

REMOVE TIE DOWN (AR510900)

THE EXISTING TIE DOWNS LOCATED IN THE AREAS TO BE IMPROVED WILL BE REMOVED AND DISPOSED OF OFF THE AIRPORT SITE.

THE CONTRACTOR WILL SEPARATE THE TIE DOWNS TO BE REMOVED FROM THE SURROUNDING PAVEMENT PRIOR TO PULLING THE TIE DOWN OUT. REMOVAL OF THE TIE DOWN SHALL NOT CAUSE ANY DISPLACEMENT OF THE SURROUNDING PAVEMENT. ANY DISPLACEMENT WILL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER.

THE CONTRACTOR SHALL HAVE THE OPTION OF REMOVING THE TIE DOWNS BY EITHER: COMPLETE REMOVAL BY MEANS OF EXCAVATION, OR MAY REMOVE THE TIE DOWNS FLUSH WITH THE PROPOSED MILLED SURFACE BY MEANS OF MILLING OR JACKHAMMER. IF THE TIE DOWNS ARE REMOVED IN THEIR ENTIRETY BY MEANS OF EXCAVATION, THE VOID SHALL BE FILLED WITH LOW STRENGTH MATERIAL BACKFILL IN ACCORDANCE WITH IDOT STANDARD SPECIFICATION FOR ROAD AND BRIDGE CONSTRUCTION SECTION 593 TO THE SATISFACTION OF THE RESIDENT ENGINEER, AND CONSIDERED INCIDENTAL.

ALL EXCAVATED MATERIALS SHALL BE DISPOSED OF OFF-SITE AND SHALL BE CONSIDERED INCIDENTAL TO THE REMOVAL ITEM. THE REMOVAL AND DISPOSAL OF THE TIE DOWNS AND ANY REQUIRED BACKFILL FOR THE RESULTING HOLES WILL BE PAID FOR UNDER ITEM:
AR510900 REMOVE TIE DOWN _____ PER EACH.

COORDINATE DATA – TXY "A" CENTERLINE

| ID. | STATION | OFFSET | NORTHING | EASTING |
|-----|-----------|--------|--------------|-------------|
| A | 150+00.00 | CL | 1154451.1977 | 831819.9045 |
| B | 159+00.00 | CL | 1155351.1589 | 831811.5221 |

COORDINATE DATA – G.A. RAMP

| NO. | STATION | OFFSET | NORTHING | EASTING |
|-----|-----------|-------------|--------------|-------------|
| 1 | 150+94.71 | 24.00' LT. | 1154545.6819 | 831795.0234 |
| 2 | 150+94.70 | 200.35' LT. | 1154543.7247 | 831619.0623 |
| 3 | 151+48.37 | 199.75' LT. | 1154597.7041 | 831618.7834 |
| 4 | 151+48.30 | 237.95' LT. | 1154597.2969 | 831582.1227 |
| 5 | 152+10.19 | 275.07' LT. | 1154659.1447 | 831543.5601 |
| 6 | 153+10.04 | 275.21' LT. | 1154758.6588 | 831541.8180 |
| 7 | 153+52.02 | 275.26' LT. | 1154800.6416 | 831541.3741 |
| 8 | 154+42.04 | 275.42' LT. | 1154890.6553 | 831540.3775 |
| 9 | 154+74.03 | 275.39' LT. | 1154922.6460 | 831540.1084 |
| 10 | 155+54.25 | 275.50' LT. | 1155002.8550 | 831539.2543 |
| 11 | 155+86.29 | 275.43' LT. | 1155034.8940 | 831539.0212 |
| 12 | 156+64.00 | 275.35' LT. | 1155112.6043 | 831538.3790 |
| 13 | 156+64.00 | 51.74' LT. | 1155114.6949 | 831762.0171 |
| 14 | 154+67.38 | 51.75' LT. | 1154918.0764 | 831763.8036 |
| 15 | 154+67.38 | 24.00' LT. | 1154918.3349 | 831791.5524 |
| 16 | 150+82.79 | 450.40' LT. | 1154529.7899 | 831368.7497 |
| 17 | 151+22.79 | 450.40' LT. | 1154569.7882 | 831368.3772 |
| 18 | 151+22.79 | 490.40' LT. | 1154569.4157 | 831328.3789 |
| 19 | 150+82.79 | 490.40' LT. | 1154529.4174 | 831328.7515 |

MAY 14, 2012 9:00 AM HARR01115
C:\PWSE-WORK\DD_NOT_DELETE\DM527480\111PRP.DWG

DE073

REVISION

| NO. | DATE | DESCRIPTION |
|-----|------|-------------|
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |
| | | |

DATE

A.I.P. PROJ.: 3-17-0033-B3
IL PROJ.: DEC-4102

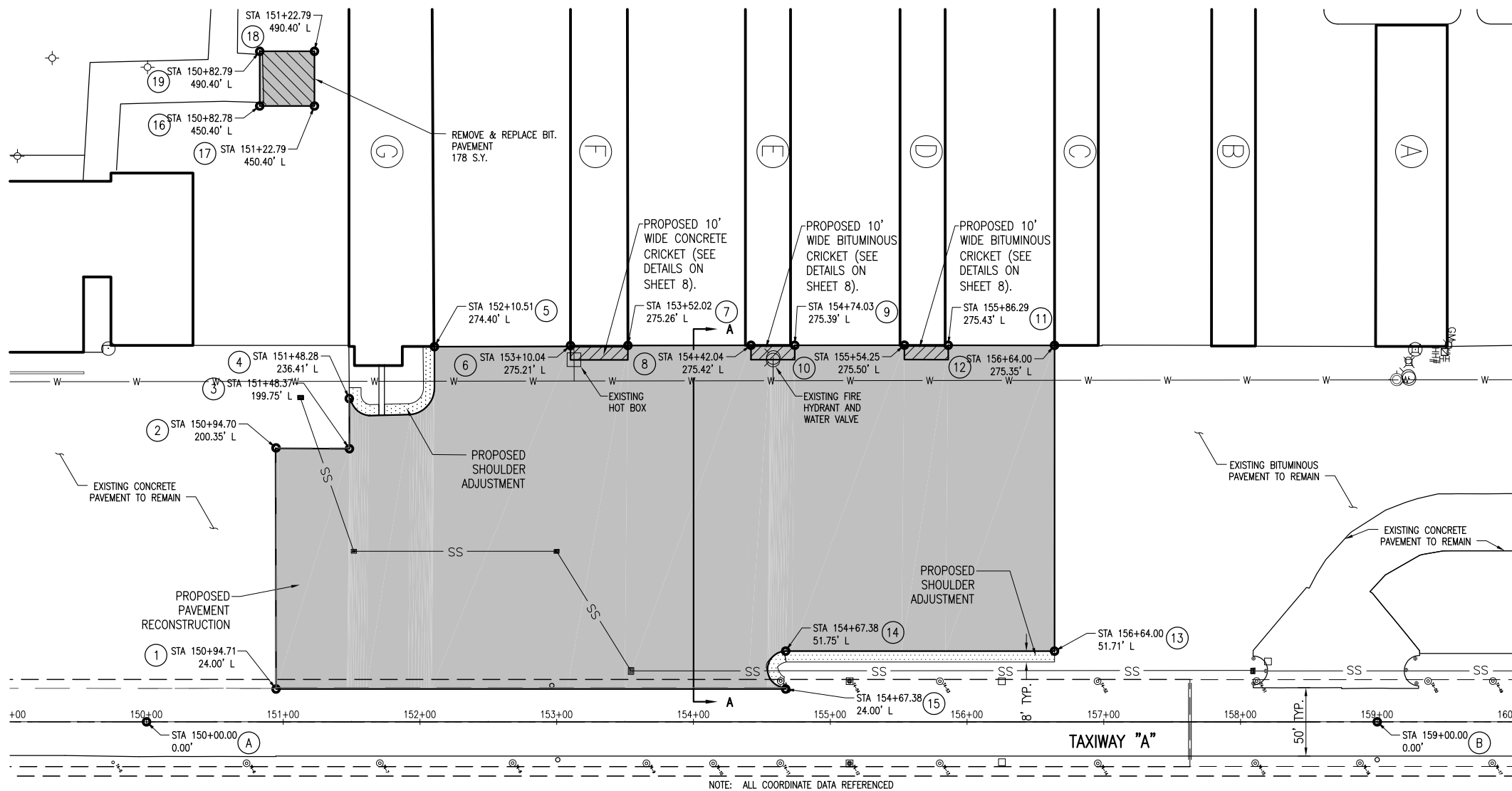
DECATUR, ILLINOIS

RECONSTRUCT NORTH GA RAMP

PROPOSED PAVEMENT PREPARATION PLAN

5

5 of 10 sheets



LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDING
- PROPOSED BITUMINOUS PAVEMENT
- PROPOSED PAVEMENT PATCH
- PROPOSED SHOULDER ADJUSTMENT
- EXISTING ELECTRICAL CABLE
- EXISTING BASE MOUNT TAXIWAY LIGHT
- EXISTING STAKE MOUNT TAXIWAY LIGHT
- EXISTING INLET
- EXISTING MANHOLE
- EXISTING GAS VALVE
- EXISTING WATER VALVE
- EXISTING HYDRANT
- EXISTING HANDHOLE

COORDINATE DATA - TXY "A" CENTERLINE

| ID. | STATION | OFFSET | NORTHING | EASTING |
|-----|-----------|--------|--------------|-------------|
| A | 150+00.00 | 0 | 1154451.1977 | 831819.9045 |
| B | 159+00.00 | 0 | 1155351.1589 | 831811.5221 |

COORDINATE DATA - G.A. RAMP

| NO. | STATION | OFFSET | NORTHING | EASTING |
|-----|-----------|-------------|--------------|-------------|
| 1 | 150+94.71 | 24.00' LT. | 1154545.6819 | 831795.0234 |
| 2 | 150+94.70 | 200.35' LT. | 1154543.7247 | 831619.0623 |
| 3 | 151+48.37 | 199.75' LT. | 1154597.7041 | 831618.7834 |
| 4 | 151+48.28 | 236.41' LT. | 1154597.2969 | 831582.1227 |
| 5 | 152+10.51 | 274.40' LT. | 1154659.1447 | 831543.5601 |
| 6 | 153+10.04 | 275.21' LT. | 1154758.6588 | 831541.8180 |
| 7 | 153+52.02 | 275.26' LT. | 1154800.6416 | 831541.3741 |
| 8 | 154+42.04 | 275.42' LT. | 1154890.6553 | 831540.3775 |
| 9 | 154+74.03 | 275.39' LT. | 1154922.6460 | 831540.1084 |
| 10 | 155+54.25 | 275.50' LT. | 1155002.8550 | 831539.2543 |
| 11 | 155+86.29 | 275.43' LT. | 1155034.8940 | 831539.0212 |
| 12 | 156+64.00 | 275.35' LT. | 1155112.6043 | 831538.3790 |
| 13 | 156+64.00 | 51.71' LT. | 1155114.6873 | 831762.0172 |
| 14 | 154+67.38 | 51.75' LT. | 1154918.0764 | 831763.8036 |
| 15 | 154+67.38 | 24.00' LT. | 1154918.3349 | 831791.5524 |
| 16 | 150+82.78 | 450.40' LT. | 1154529.7899 | 831368.7497 |
| 17 | 151+22.79 | 450.40' LT. | 1154569.7882 | 831368.3772 |
| 18 | 151+22.79 | 490.40' LT. | 1154569.4157 | 831328.3789 |
| 19 | 150+82.79 | 490.40' LT. | 1154529.4174 | 831328.7515 |

NOTE: ALL COORDINATE DATA REFERENCED FROM TAXIWAY "A" CENTERLINE.

AR401614 BITUMINOUS SURFACE COURSE-METHOD II, SUPERPAVE

THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM 401 "BITUMINOUS SURFACE COURSE-SUPERPAVE" AS STATED ON PAGE 129 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

ON THE APRON, THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING 2 LIFTS (2 @ 2") OF BITUMINOUS SURFACE COURSE-SUPERPAVE (METHOD II) ON THE PROPOSED MILLED SURFACE.

A STRING LINE WILL NOT BE REQUIRED, HOWEVER THE USE OF A TRAVELING SKI (LONG AND SHORT) AND MATCHING SHOE IS NECESSARY TO MAINTAIN THE CONTOURS AND DRAINAGE CHARACTERISTICS OF THE PAVEMENT AS IT CURRENTLY EXISTS.

PRIOR TO STARTING THE BITUMINOUS SURFACE COURSE-SUPERPAVE (METHOD II) OPERATION, THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A DETAILED OUTLINE SHOWING AREAS AND ORDER OF PAVING WIDTHS OF PAVING LANES, AND REQUIRED OFFSETS FOR ELECTRONIC GRADE.

THE PROPOSED BITUMINOUS SURFACE COURSE-SUPERPAVE (METHOD II) WILL BE DESIGNED TO A SUPERPAVE DESIGN FOR AIRCRAFT OF LESS THAN 60,000 POUNDS.

THE PAVEMENT REPAIR AREA (AR401910) SHALL BE BACKFILLED WITH BITUMINOUS SURFACE COURSE IN ACCORDANCE WITH THE SPECIAL PROVISION. THE BITUMINOUS QUANTITY FOR THIS WORK IS INCLUDED IN THE QUANTITY FOR BITUMINOUS SURFACE COURSE SUPERPAVE (METHOD II).

401-4.9 ADD THE FOLLOWING TO THIS SECTION:

WHEN HAND SPREADING IS PERMITTED, THE MIXTURE WILL BE DISTRIBUTED AND SPREAD USING HAND TOOLS. WHEN THE WORK IS COMPLETED, THE LAYER WILL HAVE THE REQUIRED THICKNESS AND CONFORM TO THE GRADE AND SURFACE CONTOUR SHOWN ON THE PLANS.

401-4.12 ADD THE FOLLOWING TO THIS PARAGRAPH:

ALL PAVEMENT EDGES (LONGITUDINAL, RADIUS, AND PAVEMENT ENDS) MUST BE LEFT IN PROPER ALIGNMENT AS SHOWN ON THE PLANS. THIS MAY BE ACCOMPLISHED BY THE TRIMMING METHOD OUTLINED ABOVE OR AT THE CONTRACTOR'S OPTION BY SAWING AFTER THE PAVING HAS BEEN COMPLETED. NO ADDITIONAL COMPENSATION WILL BE MADE IF THE SAWING METHOD IS USED.

SHOULDER ADJUSTMENT NOTE

MINIMAL GRADING IS ANTICIPATED TO RESTORE EARTH SHOULDERS ONCE THE PAVING IS COMPLETE. SHOULDER ADJUSTMENT SHALL BE IN ACCORDANCE WITH THE TYPICAL SECTIONS, SPECIAL PROVISIONS AND AS DIRECTED BY THE RESIDENT ENGINEER.

PAYMENT FOR SHOULDER ADJUSTMENT SHALL INCLUDE ALL GRADING AND PREPARATION FOR SODDING NECESSARY TO COMPLETE THE WORK TO THE SATISFACTION OF THE RESIDENT ENGINEER. SODDING WILL BE PAID SEPARATELY, PER SQUARE YARD. SODDING SHALL BE COMPLETED IN ACCORDANCE WITH THE APPLICABLE SPECIFICATIONS.

SHOULDER ADJUSTMENT SHALL BE PAID FOR UNDER ITEM: AR152480 "SHOULDER ADJUSTMENT" _____ PER S.Y.

SODDING NOTE

THE GRADING LIMITS ARE SHOWN ON THE CONSTRUCTION PLAN SHEETS BY (). ALL AREAS WITHIN THESE LIMITS, EXCEPT THE PROPOSED PAVEMENT, SHALL BE LIMED, FERTILIZED AND SODDED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

ALL ROCK, ASPHALT OR CONCRETE DEBRIS LEFT FROM THE PAVING OPERATION WILL BE DISPOSED OF OFF THE AIRPORT SITE.

THE PRIME CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATION OF ALL SUB-CONTRACTORS AS TO THEIR RESPONSIBILITIES PERTAINING TO THE SODDING OPERATIONS.

THIS WORK WILL BE PAID FOR UNDER THE FOLLOWING: ITEM NO. AR904510 "SODDING" _____ PER S.Y.

UTILITIES

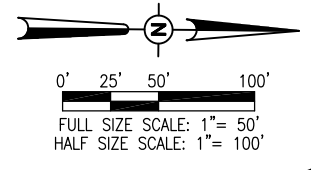
ELECTRICAL CABLES PARALLEL THE PAVEMENTS WITH 10' TO 15' SEPARATION AND ARE BURIED AT A DEPTH OF APPROXIMATELY 18 INCHES. ALSO, OTHER CABLES ARE BURIED IN THE VICINITY. BEFORE ANY DIGGING OR TRENCHING, ALL CABLES ARE TO BE LOCATED BY THE CONTRACTOR.

603-BITUMINOUS TACK COAT NOTES:

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 254 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

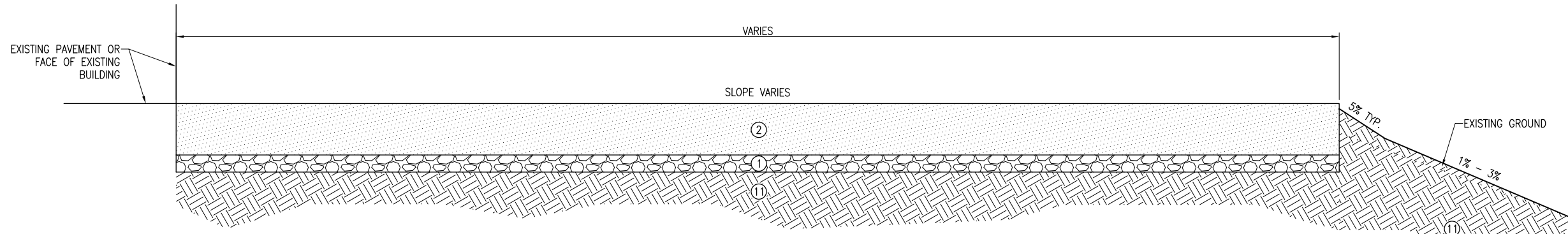
THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED ON THE PROPOSED BITUMINOUS PAVEMENT PRIOR TO THE PLACEMENT OF THE NEXT LIFT OF PROPOSED BITUMINOUS SURFACE COURSE. THE PROPOSED BITUMINOUS PAVEMENT SHALL HAVE A TACK COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM: AR603510 BITUMINOUS TACK COAT _____ PER GAL.

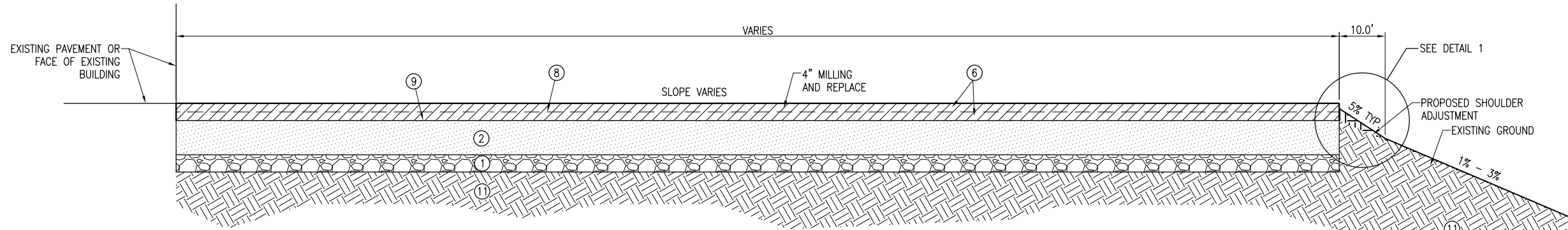


MAY 14, 2012 9:00 AM HARR01115 C:\PWSE-WORK\DO_NOT_DELETE\DM527480\R-121CON.DWG

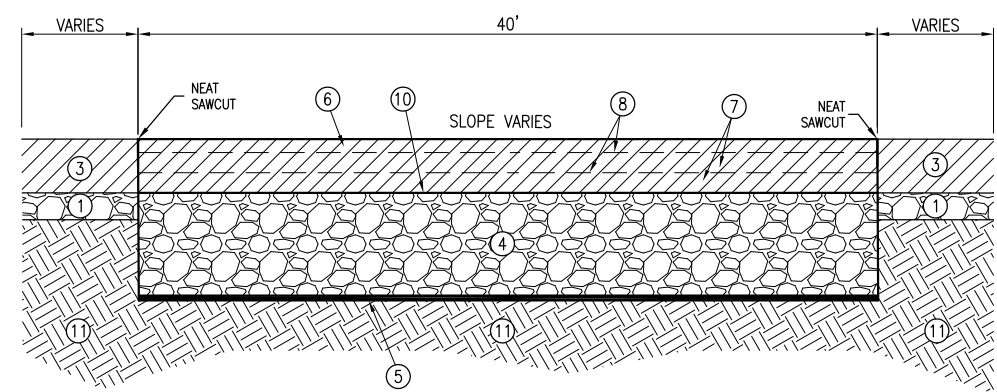
| | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|
| REVISION | | | | | | | | | |
| DATE | | | | | | | | | |
| DECATUR, ILLINOIS A.I.P. PROJ.: 3-17-0033-B3 I.L. PROJ.: DEC-4102 | | | | | | | | | |
| Hanson Proj. No. 10A01360_0001 Filename R-121CON.DWG Scale 1" = 50' Date 05/11/12 | JSL 03/21/11 BAK 03/21/11 RAW 04/07/11 | | | | | | | | |
| HANSON Professional Services Inc. 2012 1525 South Sixth Street Springfield, Illinois 62703-2886 Ph: (217) 788-2450 Fax: (217) 788-2503 www.hanson-inc.com Offices Nationwide | | | | | | | | | |
| RECONSTRUCT NORTH GA RAMP | PROPOSED CONSTRUCTION PLAN | | | | | | | | |
| 6 | | | | | | | | | |
| 6 of 10 sheets | | | | | | | | | |



EXISTING APRON PAVEMENT – TYPICAL SECTION "A-A"
 "NOT TO SCALE"
 STA. 154+00 TO STA. 156+54



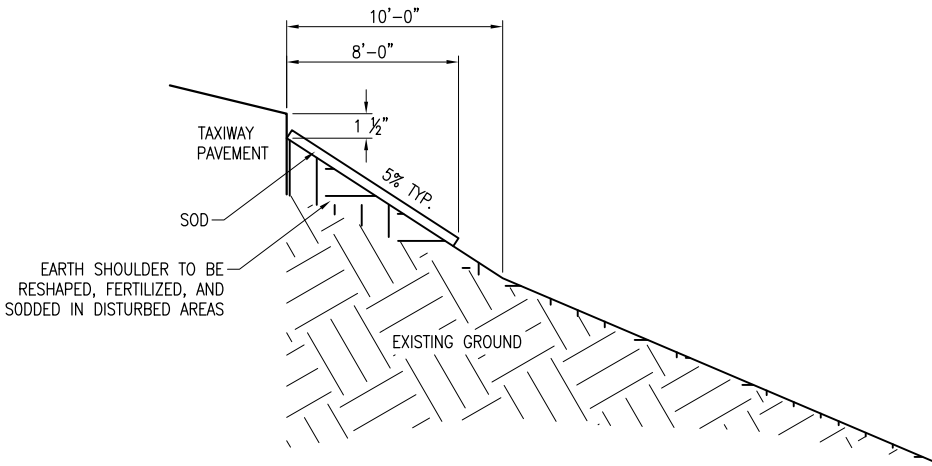
PROPOSED APRON PAVEMENT – TYPICAL SECTION "A-A"
 "NOT TO SCALE"
 STA. 154+00 TO STA. 166+54



PROPOSED PAVEMENT PATCH – TYPICAL SECTION
 "NOT TO SCALE"

LEGEND FOR TYPICAL SECTIONS

- ① EXISTING AR208 AGGREGATE BASE COURSE – 6"
- ② EXISTING AR401 BITUMINOUS – 7"-15" VARIABLE
- ③ EXISTING AR401 BITUMINOUS – 8"
- ④ PROPOSED AR209 CRUSHED AGGREGATE BASE COURSE – 15"
- ⑤ PROPOSED AR209 GEOTEXTILE FABRIC
- ⑥ PROPOSED AR401 BITUMINOUS SURFACE COURSE – 2"
- ⑦ PROPOSED AR401 BITUMINOUS SURFACE COURSE – 3"
- ⑧ PROPOSED AR603 BITUMINOUS TACK COAT (0.10 GAL./S.Y.)
- ⑨ PROPOSED AR603 BITUMINOUS TACK COAT (0.25 GAL./S.Y.)
- ⑩ PROPOSED AR602 BITUMINOUS PRIME COAT (0.30 GAL./S.Y.)
- ⑪ EXISTING SUBGRADE



DETAIL 1
 EARTH SHOULDER

NOTE:
 THE CONTRACTOR WILL REGRADE AND SOD THE DISTURBED EARTH SHOULDER. THE GRADING SHALL SHAPE THE EARTH SHOULDER TO CONFORM TO THE EARTH SHOULDER DETAIL. SODDING SHALL BE ACCOMPLISHED IN ACCORDANCE TO SPECIFICATION ITEM AR904510.

| REVISION | DATE |
|----------|------|
| | |
| | |
| | |

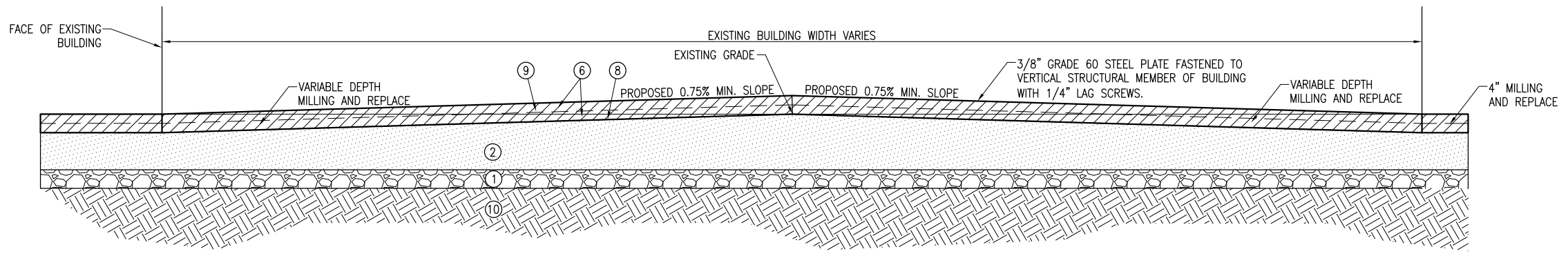


| | | |
|--------------------------------|-----|----------|
| Hanson Proj. No. 10A01360_0001 | JSL | 03/21/11 |
| Filename R-521TYP.DWG | BAK | 03/21/11 |
| Scale NOT TO SCALE | RAW | 04/07/11 |
| Date 05/11/12 | | |

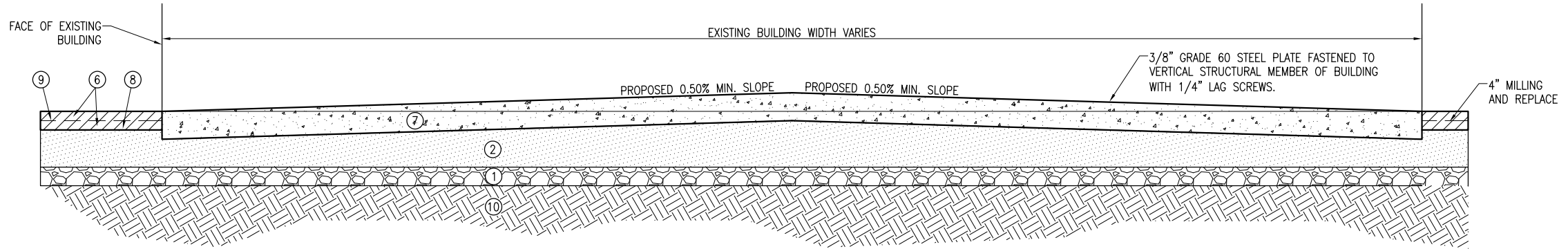
HANSON
 Hanson Professional Services Inc. 2012
 1525 South Sixth Street
 Springfield, Illinois 62703-2886
 Ph: (217) 788-2450 Fax: (217) 788-2503
 www.hanson-inc.com
 Offices Nationwide

RECONSTRUCT NORTH GA RAMP
 EXISTING AND PROPOSED TYPICAL SECTIONS

MAY 14, 2012 9:00 AM HARR01115
 C:\PWSE-WORK\DO_NOT_DELETE\DM527480\R-521TYP.DWG

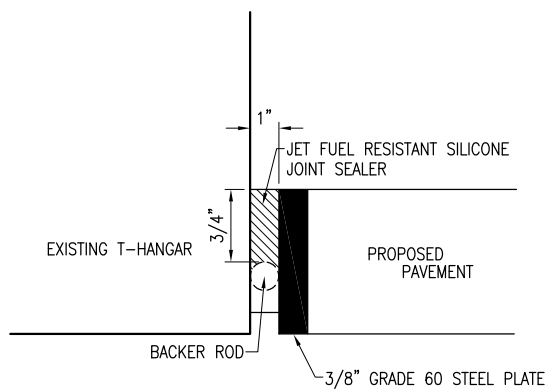
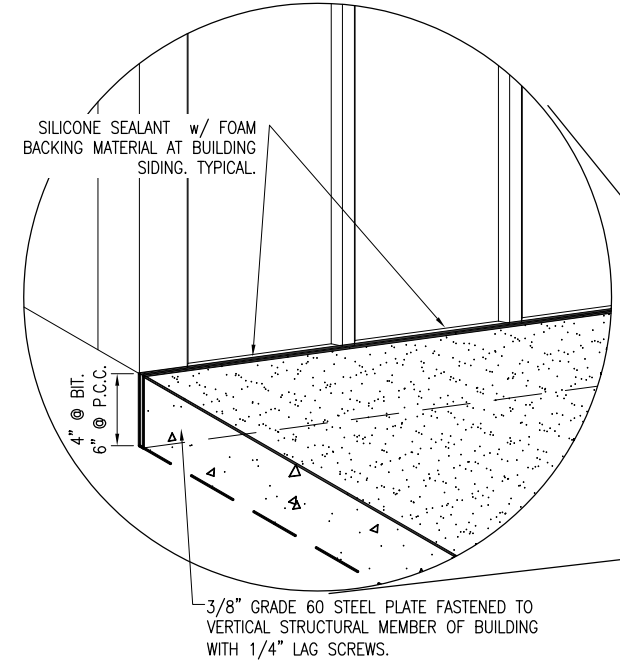


PROPOSED BITUMINOUS CRICKET 4" THICKNESS
"NOT TO SCALE"

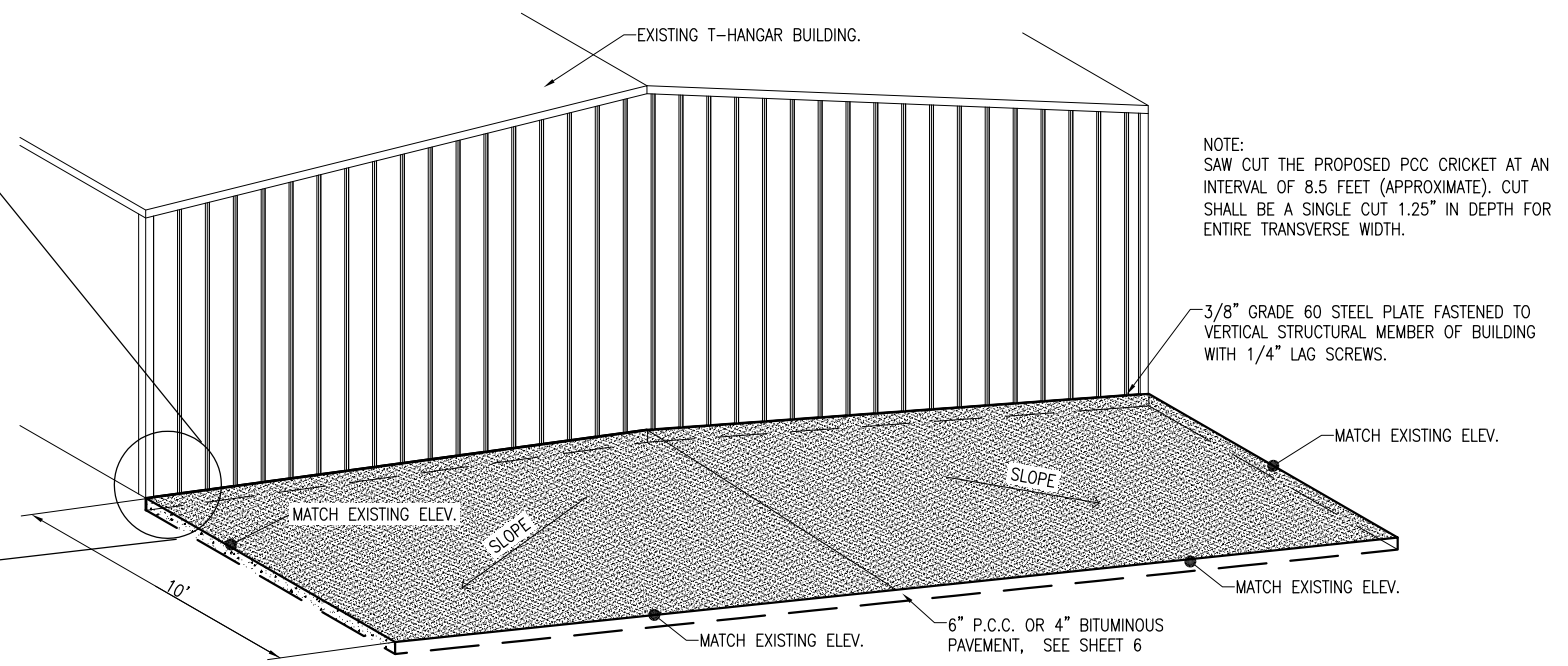


PROPOSED CONCRETE CRICKET 6" THICKNESS
"NOT TO SCALE"

- LEGEND FOR TYPICAL SECTIONS**
- ① EXISTING AR208 AGGREGATE BASE COURSE - 6"
 - ② EXISTING AR401 BITUMINOUS - (VARIABLE)
 - ③ NOT USED ON THIS SHEET
 - ④ NOT USED ON THIS SHEET
 - ⑤ NOT USED ON THIS SHEET
 - ⑥ PROPOSED AR401 BITUMINOUS SURFACE COURSE - 2"
 - ⑦ PROPOSED AR610 6" P.C.C.
 - ⑧ PROPOSED AR603 BITUMINOUS TACK COAT (0.25 GAL./S.Y.)
 - ⑨ PROPOSED AR603 BITUMINOUS TACK COAT (0.10 GAL./S.Y.)
 - ⑩ EXISTING SUBGRADE



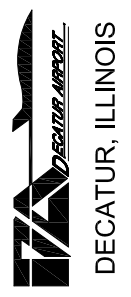
CRICKET EXPANSION JOINT DETAIL
"NOT TO SCALE"



PROPOSED BITUMINOUS / PCC CRICKET
"NOT TO SCALE"

NOTE:
SAW CUT THE PROPOSED PCC CRICKET AT AN INTERVAL OF 8.5 FEET (APPROXIMATE). CUT SHALL BE A SINGLE CUT 1.25" IN DEPTH FOR ENTIRE TRANSVERSE WIDTH.

| REVISION | DATE |
|----------|------|
| | |
| | |
| | |



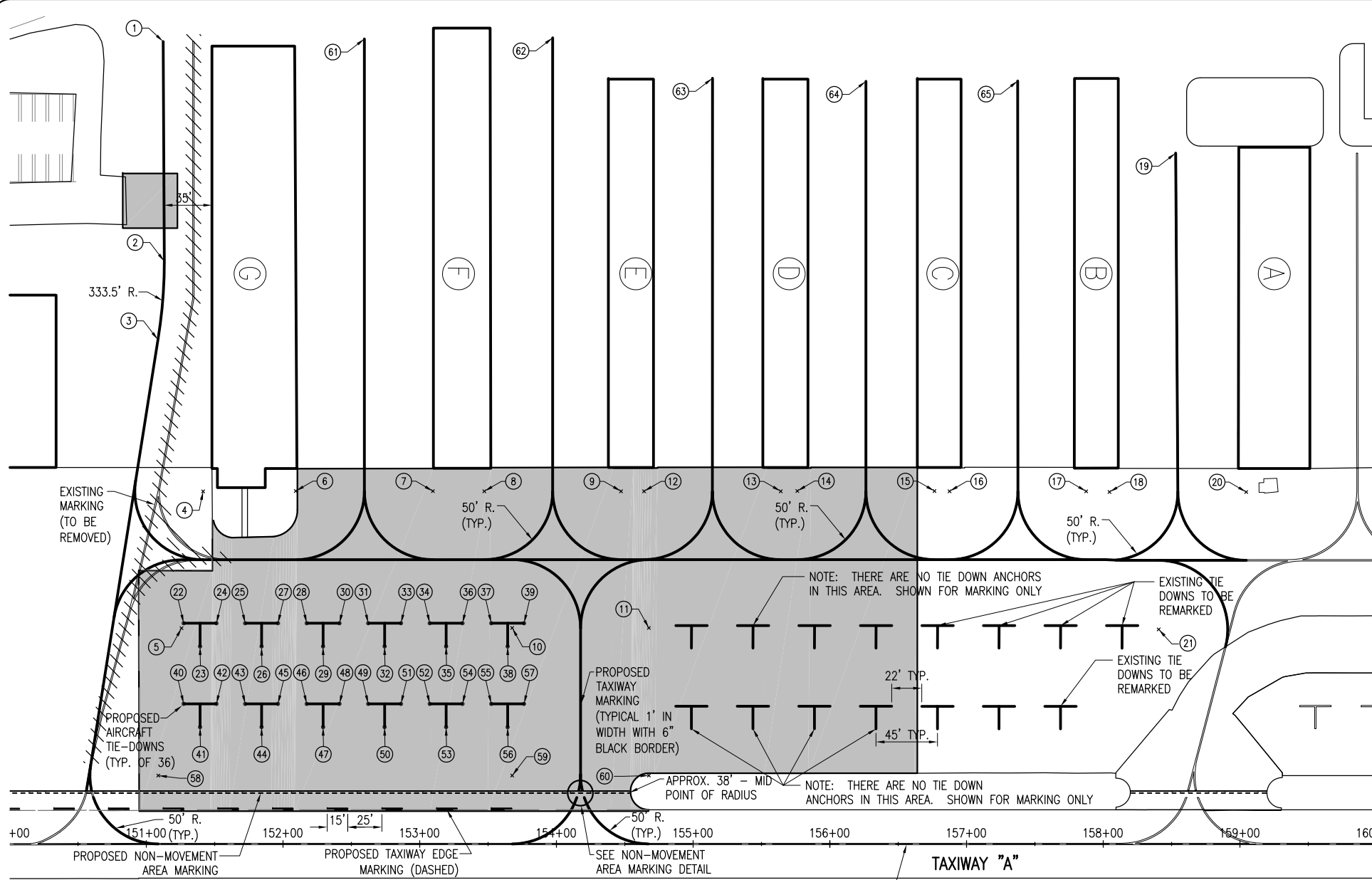
DECATUR, ILLINOIS

IL PROJ.: DEC-4102 A.I.P. PROJ.: 3-17-0033-B3

| | | |
|--------------------------------|-----|----------|
| Hanson Proj. No. 10A01360_0001 | JSL | 04/13/11 |
| Filename R-521TYP.DWG | CWS | 04/13/11 |
| Scale NOT TO SCALE | RAW | 04/07/11 |
| Date 05/11/12 | | |
| LAYOUT | | |
| DRAWN | | |
| REVIEWED | | |

HANSON
Professional Services Inc. 2012
1525 South Sixth Street
Springfield, Illinois 62703-2886
Ph: (217) 788-2450 Fax: (217) 788-2503
www.hanson-inc.com
Offices Nationwide

RECONSTRUCT NORTH GA RAMP
PROPOSED TYPICAL SECTIONS FOR CRICKETS



MARKING NOTES

THE TAXIWAY CENTERLINE STRIPE, TAXIWAY EDGE MARKING, NON-MOVEMENT AREA MARKING, AND TIE-DOWN MARKING WILL BE SOLID AND YELLOW IN COLOR. THE NON-MOVEMENT AREA MARKINGS WILL BE AS SPECIFIED IN THE DETAILS SHOWN ON THIS SHEET.

ALL PROPOSED MARKING WILL BE LOCATED AS SHOWN ON THIS SHEET.

ALL YELLOW PROPOSED MARKING WILL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIFICATIONS.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

WITH EXCEPTION TO TIE DOWN MARKING, ALL PROPOSED MARKING SHALL HAVE A 6 INCH WIDE (MIN.) BLACK BORDER.

BLACK BORDER WILL NOT REQUIRE REFLECTIVE MEDIA.

ALL PROPOSED MARKING WILL BE PAINTED WITH TWO APPLICATIONS AT A RATE OF 115 S.F./GAL. MAXIMUM PER THE SUPPLEMENTAL SPECIFICATIONS.

ALL PROPOSED MARKING ITEMS WILL BE PAID FOR UNDER ITEMS:
 AR620520 "PAVEMENT MARKING-WATERBORNE" _____ PER S.F.
 AR620525 "PAVEMENT MARKING-BLACK BORDER" _____ PER S.F.

620900-PAVEMENT MARKING REMOVAL NOTES

THE EXISTING PAVEMENT MARKING SHALL BE REMOVED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 277 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE AREAS THAT ARE DESIGNATED FOR REMOVAL ARE SHOWN ON THE CONSTRUCTION PLANS.

ALL AREAS TO BE REMOVED ARE CALCULATED AREAS. ANY ADDITIONAL AREAS, DUE TO OVER SPRAY, SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE PROPOSED MARKING REMOVAL WILL BE PAID FOR UNDER ITEM:
 AR620900 "PAVEMENT MARKING REMOVAL" _____ PER S.F.

TIE DOWN NOTES

NEW TIE DOWNS WILL BE INSTALLED IN ACCORDANCE WITH DETAIL "B" AT THE LOCATIONS SHOWN ON THIS SHEET.

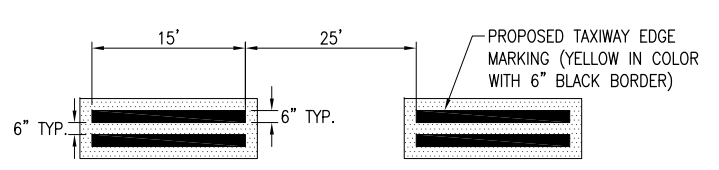
THE PERIMETER OF EACH PROPOSED TIE DOWN SHALL BE SAWED.

TIE DOWN CASTINGS SHALL BE NEENAH R-3490A OR EQUAL.

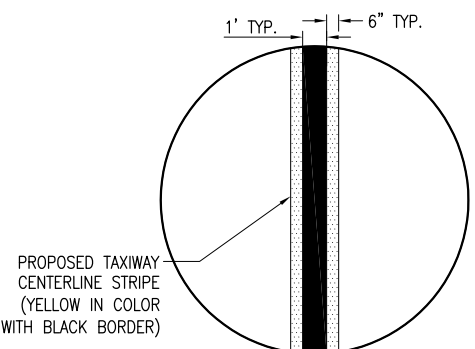
CONCRETE SHALL MEET THE REQUIREMENTS OF 610 - STRUCTURAL CONCRETE.

THE PROPOSED TIE DOWN MARKING WILL BE IN ACCORDANCE WITH THE PROPOSED TIE-DOWN MARKING DETAIL ON THE PROPOSED PAVEMENT MARKING PLAN SHEET AND THE SPECIFICATIONS.

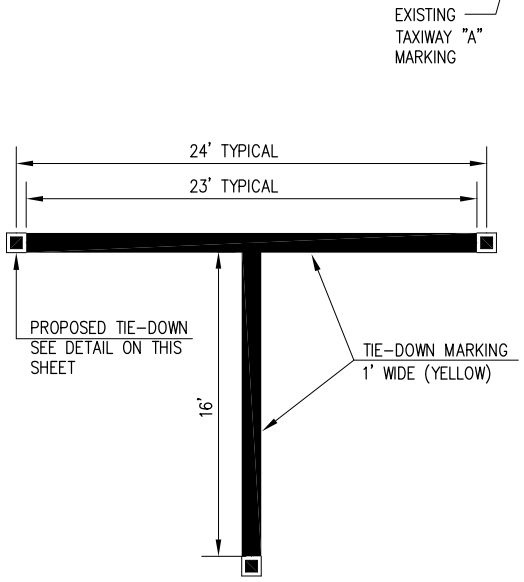
THE TIE DOWNS WILL BE PAID FOR UNDER ITEM:
 AR510510 "TIE DOWN" _____ PER EACH.



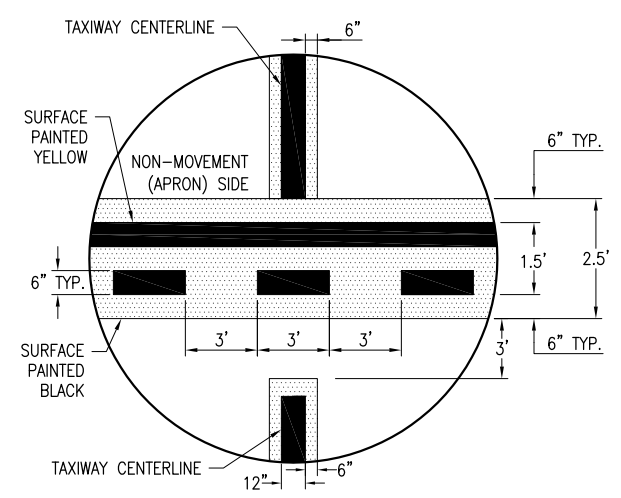
TAXIWAY EDGE MARKING (DASHED)
"NOT TO SCALE"



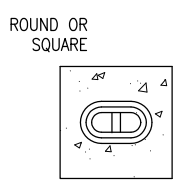
TAXIWAY CENTERLINE DETAIL
"NOT TO SCALE"



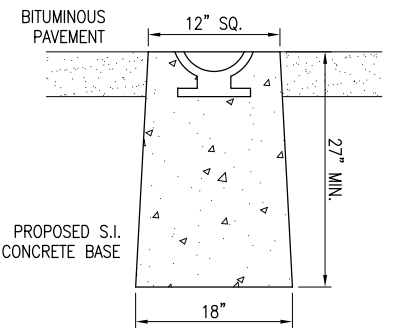
PROPOSED TIE-DOWN MARKING DETAIL
"NOT TO SCALE"



NON-MOVEMENT AREA MARKING DETAIL
"NOT TO SCALE"



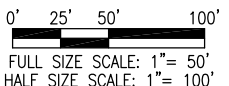
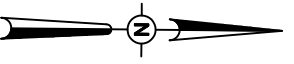
ROUND OR SQUARE
PROPOSED TIE DOWN
NEENAH R-3490A OR EQUAL



TIE DOWN DETAIL "B"
"NOT TO SCALE"

LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDING
- PROPOSED BITUMINOUS PAVEMENT
- EXISTING MARKING
- PROPOSED MARKING
- EXISTING MARKING (TO BE REMOVED)



FULL SIZE SCALE: 1" = 50'
HALF SIZE SCALE: 1" = 100'

| REVISION | DATE |
|----------|------|
| | |
| | |
| | |

DECATUR AIRPORT
DECATUR, ILLINOIS

IL PROJ.: DEC-4102 A.I.P. PROJ.: 3-17-0033-B3

| | | |
|--------------------------------|-----|----------|
| Hanson Proj. No. 10A01360_0001 | JSL | 03/21/11 |
| Filename R-151MRK.DWG | BAK | 03/21/11 |
| Scale 1" = 50' | RAW | 04/07/11 |
| Date 05/11/12 | | |

HANSON
 © Copyright Hanson Professional Services Inc. 2012
Hanson Professional Services Inc.
 1525 South Sixth Street
 Springfield, Illinois 62703-2886
 Ph: (217) 788-2450 Fax: (217) 788-2503
 www.hanson-inc.com
 Offices Nationwide

RECONSTRUCT NORTH GA RAMP

PROPOSED MARKING PLAN

MAY 14, 2012 9:01 AM HARR01115
C:\PWISE-WORK\DO_NOT_DELETE\DM627480\R-151MRK.DWG

