

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITIES	AS BUILT QUANTITIES
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150520	MOBILIZATION	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152480	SHOULDER ADJUSTMENT	S.Y.	1,960	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	766	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	150	
AR401613	BIT. SURF. CSE.-METHOD 1, SUPERPAVE	TON	300	
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	392	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	1,093	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	3.3	
AR402622	POROUS FRICTION COURSE, 0.10'	S.Y.	1,617	
AR501900	REMOVE PCC PAVEMENT	S.Y.	18	
AR501910	REMOVE & REPLACE PCC PAVEMENT	S.Y.	8	
AR602510	BITUMINOUS PRIME COAT	GAL.	377	
AR603510	BITUMINOUS TACK COAT	GAL.	644	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	794	
AR800599	SUBGRADE STABILIZATION	S.Y.	135	

INDEX TO SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	PROPOSED SAFETY PLAN
4	PROPOSED PAVEMENT PREPARATION PLAN
5	PROPOSED CONSTRUCTION PLAN
6	PROPOSED TYPICAL SECTIONS
7	PLAN AND PROFILE WEST ACCESS TAXIWAY STA. 10+00 TO STA. 14+50
8	PLAN AND PROFILE SOUTH ACCESS TAXIWAY STA. 1+60 TO STA. 4+00
9	PROPOSED STAKING PLAN
10	PROPOSED MARKING PLAN
11	CROSS-SECTIONS WEST ACCESS TAXIWAY 10+00.00 TO 11+08.46
12	CROSS-SECTIONS WEST ACCESS TAXIWAY 11+39.96 TO 12+48.00
13	CROSS-SECTIONS WEST ACCESS TAXIWAY 12+75.00 TO 13+75.00
14	CROSS-SECTIONS EAST ACCESS TAXIWAY 21+19.36 TO 22+03.45
15	CROSS-SECTIONS EAST ACCESS TAXIWAY 22+35.61 TO 23+25.00

REVISION	DATE

**ROBERT F. TRACY MUNICIPAL AIRPORT
MT. STERLING, ILLINOIS**

IL PROJ.: 63-4027 A.I.P. PROJ.: 3-17-0147-B11

Hanson Proj. No. 11A0108	Filename G-002-FLP.dwg	Scale NOT TO SCALE	Date 02/29/12
LAYOUT	JEO	02/21/12	02/21/12
DRAWN	JEO	02/21/12	02/21/12
REVIEWED	CAH	02/29/12	02/29/12



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REHABILITATE
T-HANGAR PAVEMENT

SUMMARY OF QUANTITIES
AND INDEX TO SHEETS

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UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

HAUL ROUTE AND VEHICLE PARKING

THE CONTRACTOR WILL BE ALLOWED TO USE THE EXISTING AIRPORT ENTRANCE ROAD, PARKING LOT, APRON AND T-HANGAR ACCESS TAXIWAYS AS HIS ACCESS TO THE PROPOSED CONSTRUCTION SITE. CONTRACTOR WILL USE THE EXISTING DOUBLE SWING GATE BY THE DOT HANGAR AS HIS ACCESS TO THE AIRFIELD SIDE. THIS GATE WILL REMAIN CLOSED UNLESS THE CONTRACTOR IS IN A CONTINUOUS HAULING OPERATION. THE CONTRACTOR AND THE RESIDENT ENGINEER WILL WALK THE HAUL ROUTE PRIOR TO THE START OF CONSTRUCTION TO NOTE ANY DEFICIENCIES. ANY DAMAGE TO THE HAUL ROUTE WILL BE REPAIRED BY THE CONTRACTOR AT THE END OF THE PROJECT AT NO ADDITIONAL COST TO THE PROJECT. THE AIRPORT WILL BE SEAL COATING THE EXISTING PARKING LOT AND APRON THIS SUMMER. THE CONTRACTOR WILL BE REQUIRED TO REPLACE THE SEAL COAT ON ANY AREA ON THE PARKING LOT AND APRON THAT HE DAMAGES

THE PORTION OF THE APRON THAT WILL BE USED AS THE PROPOSED HAUL ROUTE WILL BE DELINEATED BY USING TRAFFIC CONES TO KEEP THE CONTRACTOR'S VEHICLES SEPARATED FROM THE REST OF THE APRON. THE CONTRACTOR WILL WORK WITH THE AIRPORT MANAGER TO PROVIDE AIRCRAFT ACCESS TO THE DOT HANGAR IN THE MORNINGS AND EVENINGS..

THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR PERSONAL VEHICLES IN THE AIRPORT AUTO PARKING LOT. THE CONTRACTOR WILL TRANSPORT HIS PERSONNEL TO THE PROPOSED CONSTRUCTION SITE. NO EMPLOYEE VEHICLES WILL BE ALLOWED ON THE CONSTRUCTION SITE.

DURING HAULING OPERATIONS, THE CONTRACTOR SHALL PROVIDE A FLAG PERSON TO MONITOR AND CONTROL CONSTRUCTION TRAFFIC ACROSS THE AIRCRAFT PARKING APRON. THE FLAG PERSON WILL BE PROVIDED WITH A STOP-GO SIGN AND ALL VEHICLES WILL OBEY THE FLAG PERSON'S DIRECTIVES. THE FLAG PERSON SHALL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED HAUL ROUTE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONTRACTOR RESPONSIBILITIES

THE CONTRACTOR WILL BE ALLOWED TO STORE ANY MATERIAL AND PARK HIS EQUIPMENT ON THE NORTH SIDE OF THE T-HANGAR BETWEEN THE TWO TAXIWAYS BEING REHABILITATED AS SHOWN ON THIS SHEET. ANY AREA THAT IS DAMAGED BY THE CONTRACTOR WILL BE RESTORED BY HIM PRIOR TO COMPLETING THIS PROJECT AND AT HIS OWN EXPENSE.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR SHALL KEEP THE RUNWAY AND PARALLEL TAXIWAY OPEN AT ALL TIMES AND MAINTAIN CONTINUOUS TAXIWAY ACCESS TO THE ADMINISTRATIVE AREAS.

THE RUNWAY SHALL NOT BE CLOSED.

BARRICADES AND TRAFFIC CONES

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS SHOWN AND AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

SCOPE OF WORK

THIS PROJECT CONSISTS OF THE REHABILITATION OF THE PAVEMENTS ASSOCIATED WITH THE EXISTING T-HANGAR.

AIRPORT SECURITY NOTE

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE IN THE HAUL ROUTE AT THE END OF EACH WORKING DAY.

HEIGHT OF CONSTRUCTION EQUIPMENT

THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A DUMP TRUCK.

NOTE

ALL CONSTRUCTION/OPERATIONS ARE TO BE PERFORMED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2F "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND AC 150/5300-13 "AIRPORT DESIGN".

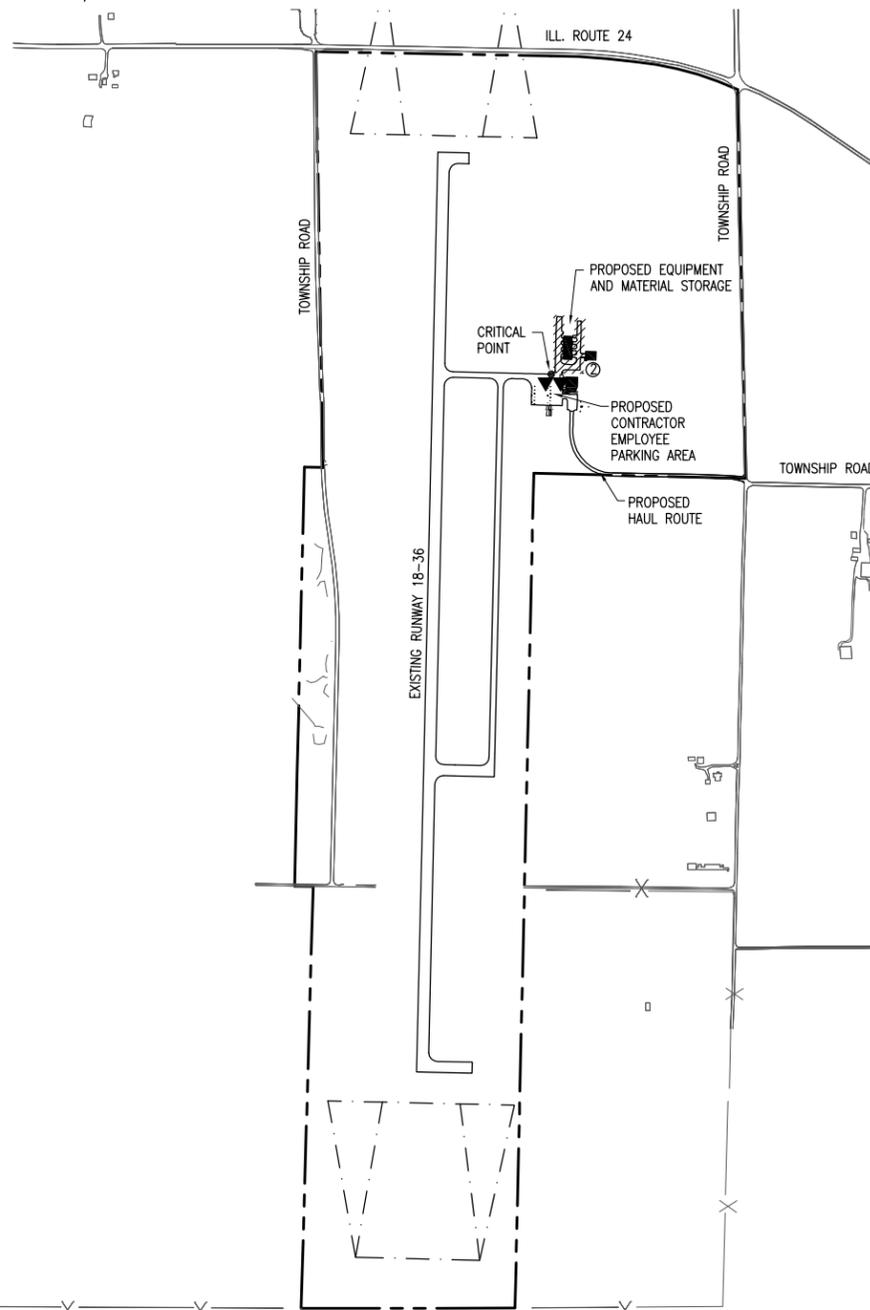
BENCHMARK DATA		
NO.	DESCRIPTION	ELEV.
1	CB-1	723.29
2	CB-2A CHISELED "□" ON SW ALONG SOUTH SIDE OF EAST HANGAR	729.66
3	CB-3	726.20

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER **CANNOT** FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL **CERTIFIED PAYROLLS** FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATION

COMPLETED WORK **CANNOT** BE PLACED ON A CONSTRUCTION REPORT UNTIL ALL MATERIAL CERTIFICATIONS FOR THAT PAY ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER.



LEGEND

	EXISTING IMPROVEMENTS
	PROPOSED IMPROVEMENTS
	EXISTING BUILDINGS
	PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA
	PROPOSED BENCHMARK
	PROPOSED BARRICADES OR TRAFFIC CONES

J.U.L.I.E. INFORMATION

COUNTY _____ BROWN
 CITY _____ MT. STERLING
 TOWNSHIP _____ LEE
 SECTION NO. _____ 12 & 13
 ADDRESS _____ ROBERT F. TRACY MUNICIPAL AIRPORT
 145 W. MAIN STREET
 MT. STERLING, ILLINOIS 62353-1296

CRITICAL POINT DATA

LATITUDE: 39° 59' 22.64"
 LONGITUDE: 90° 48' 05.21"
 ELEVATION: 729.68 M.S.L.

PROPOSED SAFETY PLAN

GENERAL - THE ROBERT F. TRACY MUNICIPAL AIRPORT IS COMPRISED OF ONE RUNWAY. NO RUNWAY WILL BE CLOSED DURING THIS PROJECT.

THIS PROJECT WILL CLOSE BOTH ACCESS TAXIWAYS TO THE HANGARS. THE CONTRACTOR WILL PROVIDE THE AIRPORT AT LEAST 2 WEEKS NOTICE PRIOR TO STARTING CONSTRUCTION ACTIVITIES THAT WILL NECESSITATE THE CLOSURE OF THE TAXIWAYS BEING REHABILITATED. THIS NOTICE WILL ALLOW THE AIRPORT TO CONTACT PLANE OWNERS AND GIVE THEM THE OPTION TO REMOVE THEIR PLANES FROM THE T-HANGAR IF THEY SO DESIRE.

IDENTIFICATION - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.80 MHz.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

150-ENGINEER'S FIELD OFFICE NOTES

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 49 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS NOV. 2, 2009.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE CONTRACTOR WILL FURNISH A WIRELESS PHONE TO THE RESIDENT ENGINEER FOR HIS EXCLUSIVE USE FOR THE DURATION OF THIS PROJECT. THE RESIDENT ENGINEER WILL USE THIS PHONE FOR PROJECT BUSINESS ONLY. THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL CHARGES ASSOCIATED WITH THIS CELL PHONE.

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS: AR150510 ENGINEER'S FIELD OFFICE _____ 1 L.S.

EROSION CONTROL

THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND, THEREFORE NO N.P.D.E.S. PERMIT WILL BE REQUIRED.



0' 250' 500' 1000'
 HALF SIZE SCALE: 1" = 1000'
 FULL SIZE SCALE: 1" = 500'

REVISION	DATE	REVISED AS PER I.D.A. COMMENTS
	04/19/12	

**ROBERT F. TRACY MUNICIPAL AIRPORT
 MT. STERLING, ILLINOIS**

IL. PROJ.: 63-4027
 A.I.P. PROJ.: 3-17-0147-B11

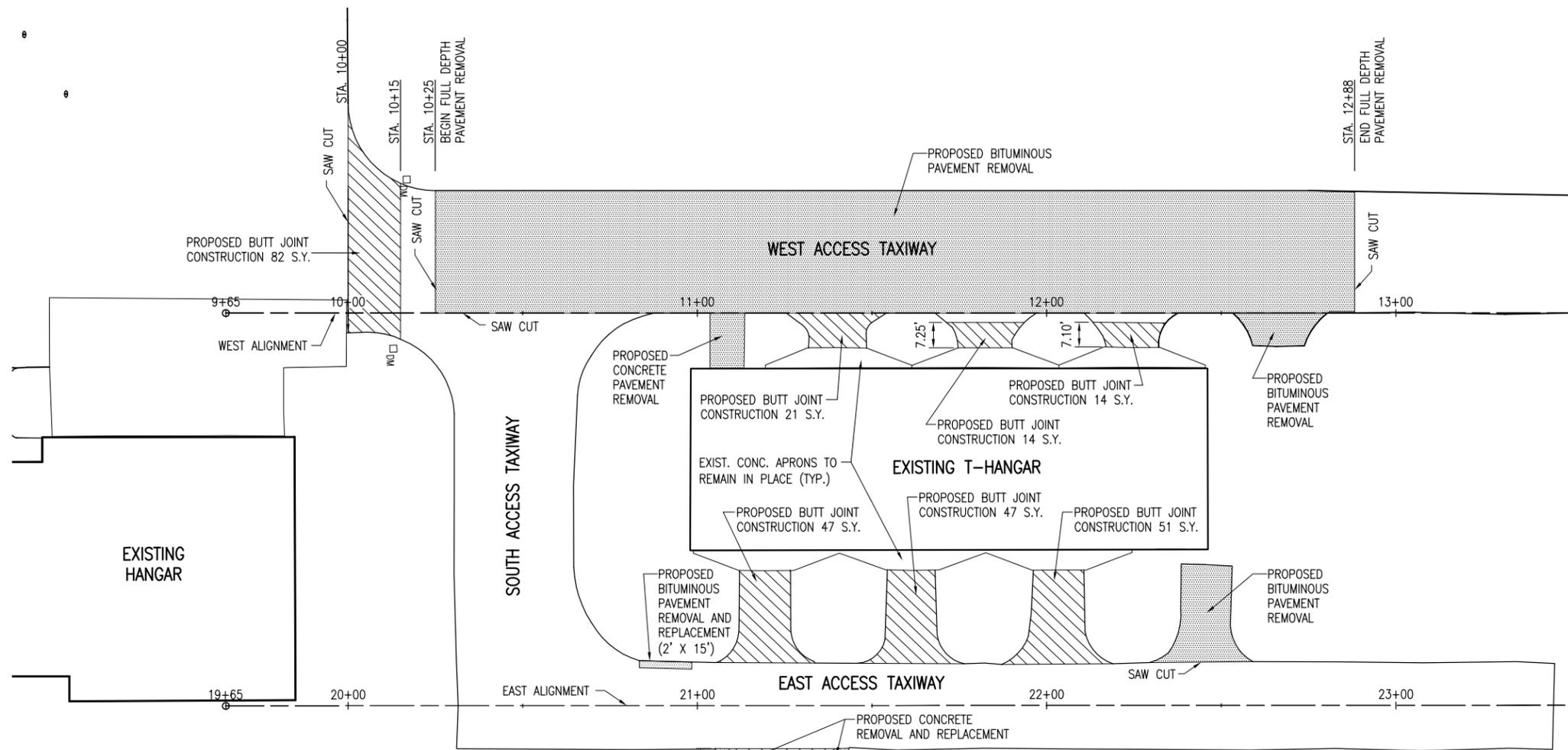
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Filename G-003-SFY.dwg	JEO	02/21/12
Scale 1" = 500'	CAH	02/29/12
Date 02/29/12		

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REHABILITATE
 T-HANGAR PAVEMENT

PROPOSED
 SAFETY PLAN

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REMOVE BITUMINOUS PAVEMENT

THE PROPOSED BITUMINOUS PAVEMENT REMOVAL AREAS ON THIS SHEET CONSISTS OF THE FOLLOWING TWO TYPES:

1. THE SECTION OF THE WEST ACCESS TAXIWAY FROM STATION 10+25 TO STATION 12+88 WILL HAVE THE EXISTING BITUMINOUS PAVEMENT REMOVED TO THE AGGREGATE SUBBASE. THE SUBBASE WILL BE GRADED TO ACCOMMODATE THE PROPOSED PAVEMENT DEPTH AND COMPACTED TO THE SATISFACTION OF THE RESIDENT ENGINEER. ANY ADDITIONAL AGGREGATE BASE COURSE MATERIAL REQUIRED TO BRING THE SUBBASE TO GRADE WILL BE PAID FOR UNDER ITEM AR209510 "CRUSHED AGGREGATE BASE COURSE" - PER TON.
2. THE TWO LEAD-IN TAXIWAYS THAT ARE SHOWN FOR REMOVAL WILL BE REMOVED FULL DEPTH (BITUMINOUS PAVEMENT AND AGGREGATE BASE). THIS AREA WILL BE FILLED IN WITH EARTH MATERIAL FROM AN OFF-SITE BORROW AREA.
3. WHERE THE PROPOSED BITUMINOUS PAVEMENT REMOVAL ABUTS EXISTING PAVEMENT THE CONTRACTOR WILL SAW THE PAVEMENT FULL DEPTH. THE BITUMINOUS PAVEMENT SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED BITUMINOUS PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
4. ANY ADJACENT PAVEMENT DAMAGED DURING THE PROPOSED BITUMINOUS PAVEMENT REMOVAL OPERATION WILL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT ENGINEER AT THE CONTRACTORS' OWN EXPENSE.
5. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE AT THE CONTRACTORS' OWN EXPENSE.
6. THE EARTH MATERIAL NEEDED TO FILL IN THE TWO REMOVED LEAD-IN TAXIWAYS WILL BE OBTAINED FROM AN OFF-SITE BORROW AREA. THE EARTH MATERIAL WILL BE PAID FOR UNDER ITEM AR152480 "SHOULDER ADJUSTMENT" - PER S.Y.
7. BOTH OF THE TYPES OF BITUMINOUS PAVEMENT REMOVAL AS DESCRIBED ABOVE WILL BE PAID FOR UNDER ITEM AR401900 "REMOVE BITUMINOUS PAVEMENT" - PER S.Y.

AR401655 BUTT JOINT CONSTRUCTION NOTES

THE BUTT JOINT CONSTRUCTION AREAS WILL BE CUT OR TRIMMED FROM A DEPTH OF 1-1/2" AT THE BUTT JOINT TO 0" AS SHOWN. THE CUTTING OR TRIMMING WILL BE DONE BY ROTO-MILLING. THE TOLERANCE OF THE MILLING WILL BE AS STATED IN THE STANDARD SPECIFICATIONS.

THE PROPOSED BUTT JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ITEM AR401655 "BUTT JOINT CONSTRUCTION" AS STATED ON PAGE 154 OF THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 02, 2009.

IF A SQUARE STRAIGHT EDGE IS NOT OBTAINED FROM THE MILLING OPERATION THE EXISTING PAVEMENT WILL BE SAWS AS SHOWN ON THIS SHEET. SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT MILLING AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL MILLED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

PRIOR TO APPLYING THE BITUMINOUS OVERLAY ALL MILLED MATERIAL WILL BE BROOMED AND BLOWN CLEAN AND A BITUMINOUS TACK COAT APPLIED. THE VERTICAL FACE OF ALL SAW CUTS WILL BE PAINTED WITH A LIQUID ASPHALT.

ANY ADJACENT PAVEMENT DAMAGED BY THE MILLING OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

ALL BITUMINOUS PAVEMENT MILLING AREAS WILL BE LOCATED AND MARKED BY THE RESIDENT ENGINEER.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR401655 BUTT JOINT CONSTRUCTION _____ PER S.Y.

BASELINE COORDINATE TABLE

	STATION	NORTHING	EASTING
WEST ALIGNMENT	9+65.00	1210675.965	2118778.491
	14+26.624	1211136.450	2118789.819
EAST ALIGNMENT	19+65.00	1210673.205	2118890.697
	24+25.68	1211133.745	2118901.843

P.C.C. PAVEMENT REMOVAL

THE P.C.C. PAVEMENT REMOVAL AREAS SHOWN ON THIS SHEET WILL HAVE THE EXISTING P.C.C. PAVEMENT REMOVED (FULL DEPTH) TO A SUITABLE SUBGRADE. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

ANY ADJACENT PAVEMENT DAMAGED BY THE P.C.C. PAVEMENT REMOVAL OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR501900 "REMOVE PCC PAVEMENT" - PER S.Y.

P.C.C. PAVEMENT REMOVAL AND REPLACEMENT

THE P.C.C. PAVEMENT REMOVAL AND REPLACEMENT AREAS SHOWN ON THIS SHEET WILL HAVE THE EXISTING P.C.C. PAVEMENT ADJUSTED TO MATCH THE PROPOSED PAVEMENT. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

THE EXISTING P.C.C. PAVEMENT WILL BE REMOVED TO THE JOINT THAT IS NEAREST TO THE DIMENSIONS SHOWN ON THIS SHEET.

THE REMOVAL OF THE P.C.C. PAVEMENT WILL BE CONSIDERED AS AN INCIDENTAL ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE PROPOSED P.C.C. PAVEMENT TO BE REPLACED WILL BE ITEM 610 OR IDOT CLASS SI MIX CONCRETE.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR501910 "REMOVE & REPLACE PCC PAVEMENT" - PER S.Y.

REMOVE AND REPLACE BITUMINOUS PAVEMENT

THE REMOVE AND REPLACE BITUMINOUS PAVEMENT AREAS SHOWN ON THIS SHEET WILL HAVE THE EXISTING BITUMINOUS PAVEMENT REMOVED (FULL DEPTH) AND REPLACED WITH BITUMINOUS MATERIAL. THE BITUMINOUS MATERIAL USED IN THE REPLACEMENT WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED REMOVAL AND REPLACEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. ALL REMOVED MATERIAL WILL BE DISPOSED OF OFF THE AIRPORT SITE.

WHERE THE PROPOSED REMOVAL AND REPLACEMENT AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWS AS SHOWN ON THIS SHEET. THE SAWING WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PAVEMENT REMOVAL AND REPLACEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

THIS ITEM OF WORK SHALL BE PAID FOR UNDER ITEM:
AR401910 "REMOVE & REPLACE BIT. PAVEMENT" - PER S.Y.

201-CLEANING AND SEALING BITUMINOUS CRACKS

THE CLEANING AND SEALING OF BITUMINOUS CRACKS (201) SHALL BE PLACED IN ACCORDANCE WITH ITEM (AR201661) "CLEANING & SEALING BITUMINOUS CRACKS" AS STATED ON PAGE 82 OF THE STANDARD SPECIFICATION FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE PAVEMENT CONDITIONS AND CRACKS WERE IDENTIFIED DURING A SURVEY (FEBRUARY 2012) OF THE EXISTING T-HANGAR PAVEMENT AREA. THE EXACT AMOUNT OF CRACKS TO BE CLEANED AND SEALED WILL BE THE NUMBER OF LINEAR FEET OF CRACKS MARKED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

ALL CRACKS DESIGNATED BY THE RESIDENT ENGINEER FOR CLEANING AND SEALING WILL BE DONE SO AS STATED IN THE SPECIAL PROVISIONS.

THE QUANTITY OF CRACKS TO BE CLEANED AND SEALED HAS BEEN INCREASED BY 10% IN ANTICIPATION OF ADDITIONAL CRACKS THAT DEVELOP BETWEEN THE SURVEY AND CONSTRUCTION.

THE CONTRACTOR WILL GIVE A MINIMUM OF 2 DAYS NOTICE BEFORE STARTING THIS WORK SO THE RESIDENT ENGINEER CAN LOCATE THE CRACKS THAT WILL BE CLEANED AND SEALED.

ALL CLEANING AND SEALING OF BITUMINOUS CRACKS SHALL BE PAID FOR UNDER ITEM:
AR201661 "CLEANING & SEALING BITUMINOUS CRACKS" _____ PER L.F.

PAVEMENT SAWING NOTES

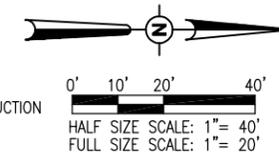
THE RESIDENT ENGINEER WILL MARK THE PROPOSED SAW LINE IN THE FIELD.

ANY BITUMINOUS PAVEMENT CUT FROM THE SAWING OPERATIONS WILL BE COLLECTED AND DISPOSED OF OFF THE AIRPORT SITE.

THE SAWING AND DISPOSAL OF ANY WASTE MATERIAL WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED

LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDING
- PROPOSED BUTT JOINT CONSTRUCTION
- PROPOSED PAVEMENT REMOVAL



REVISION	DATE	REVISED AS PER	COMMENTS
	04/19/12		

**ROBERT F. TRACY MUNICIPAL AIRPORT
MT. STERLING, ILLINOIS**

IL PROJ.: 63-4027 A.I.P. PROJ.: 3-17-0147-B11

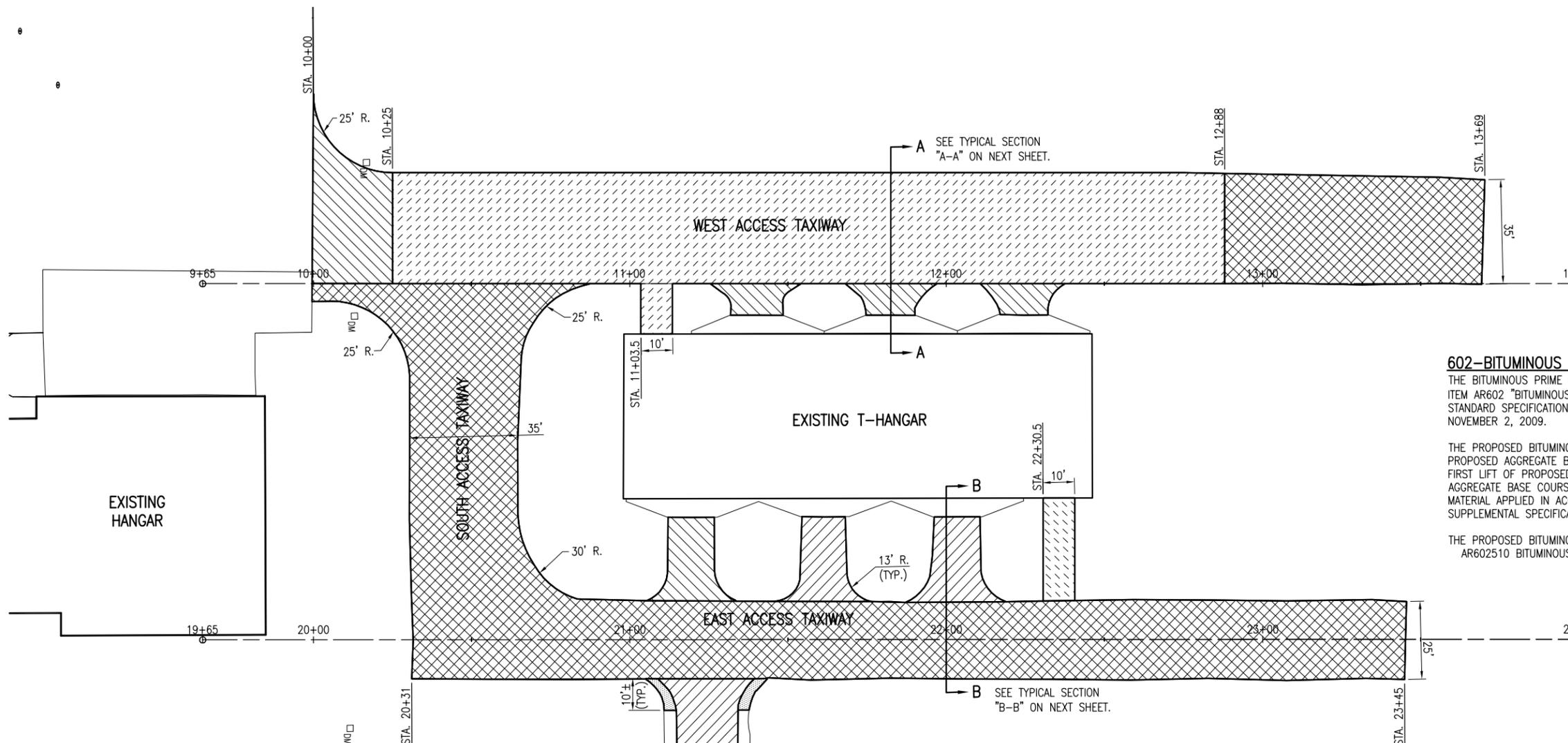
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Filename C-111-PRP.dwg	JFO	02/21/12
Scale 1" = 20'	CAH	02/29/12
Date 02/29/12		
LAYOUT		
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REVIEWED		

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**REHABILITATE
T-HANGAR PAVEMENT**

PROPOSED PAVEMENT
PREPARATION PLAN

APR 20, 2012 3:00 PM H:\GLO00382 p:\sps-sv306\hanson\dom\hanson\Projects\Documents\11A0108\CAD\Drawings\11A0108-PRP.dwg



602-BITUMINOUS PRIME COAT NOTES:
 THE BITUMINOUS PRIME COAT (602) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR602 "BITUMINOUS PRIME COAT" AS STATED ON PAGE 251 OF THE STANDARD SPECIFICATION FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE PROPOSED BITUMINOUS PRIME COAT SHALL BE PLACED ON THE PROPOSED AGGREGATE BASE COURSE PRIOR TO THE PLACEMENT OF THE FIRST LIFT OF PROPOSED BITUMINOUS BASE COURSE. THE PROPOSED AGGREGATE BASE COURSE SHALL HAVE A PRIME COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.

THE PROPOSED BITUMINOUS PRIME COAT WILL BE PAID FOR UNDER ITEM:
 AR602510 BITUMINOUS PRIME COAT _____ PER GAL.

AR209-CRUSHED AGGREGATE BASE COURSE NOTES:
 THE CRUSHED AGGREGATE BASE COURSE (209) SHALL BE PLACED IN ACCORDANCE WITH ITEM 209 "CRUSHED AGGREGATE BASE COURSE" AS STATED ON PAGE 93 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE CRUSHED AGGREGATE BASE COURSE MATERIAL (CA-6) WILL BE USED TO CONSTRUCT A BASE COURSE FOR THE PROPOSED BITUMINOUS SURFACE COURSE (401). THE CRUSHED AGGREGATE BASE COURSE MATERIAL WILL BE 6" IN DEPTH AND COMPACTED TO NOT LESS THAN 95 PERCENT DENSITY.

THE PROPOSED AGGREGATE FOR THE BASE COURSE MATERIAL SHALL MEET THE REQUIREMENTS OF GRADATION "B" IN TABLE 1 OF THE SUPPLEMENTAL SPECIFICATIONS.

209-3.2 EQUIPMENT. ADD THE FOLLOWING PARAGRAPHS TO THIS SECTION:

"PROVISIONS SHALL BE MADE BY THE CONTRACTOR FOR FURNISHING WATER AT THE PLANT AND AT THE SITE OF THE WORK BY EQUIPMENT OF AMPLE CAPACITY AND OF SUCH DESIGN AS TO ASSURE UNIFORM MIXING AND APPLICATION."

THE CONTRACTOR WILL PROVIDE THE RESIDENT ENGINEER A PROCTOR CURVE SHOWING OPTIMUM DENSITY AND MOISTURE FOR THE SUPPLIED BASE COURSE MATERIAL.

THE COMPACTION CONTROL TEST TO BE USED SHALL BE FAA COMPACTION CONTROL TEST T-611 FOR AIRCRAFT WEIGHING LESS THAN 60,000 LBS.

209-4.1. DELETE THE FIFTH SENTENCE AND REPLACE IT WITH THE FOLLOWING:

"IF AT THE TIME THE AGGREGATES ARE WEIGHED THEY CONTAIN MORE THAN SIX (6) PERCENT OF ABSORBED AND FREE MOISTURE BY WEIGHT, A DEDUCTION FOR THE MOISTURE IN EXCESS OF THIS AMOUNT SHALL BE MADE IN DETERMINING THE PAY QUANTITY."

209-5.1 ADD THE FOLLOWING TO THIS SECTION
 AR209510 CRUSHED AGGREGATE BASE COURSE _____ PER TON

POROUS FRICTION COURSE NOTES
 THE PROPOSED POROUS FRICTION COURSE WILL BE CONSTRUCTED IN ONE LAYER, HAVING A COMPACTED NOMINAL THICKNESS OF ONE TENTH OF A FOOT (0.10') IN ACCORDANCE WITH ITEM AR402622 "POROUS FRICTION COURSE, 0.10'" AS STATED ON PAGE 156 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE POROUS FRICTION COURSE SHALL BE PLACED ON A CLEAN AND PREPARED SURFACE ONLY AFTER THE APPROVAL OF THE RESIDENT ENGINEER.

THE SURFACES TO BE OVERLAYED WILL BE SPRAYED WITH A APPLICATION OF A BITUMINOUS TACK COAT. AN APPLICATION RATE OF 0.25 GAL./S.Y. (DILUTED) WAS USED FOR CALCULATING THE QUANTITY OF TACK FOR THIS APPLICATION. THE EXACT RATE OF APPLICATION WILL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION. TACKED AREAS WORN FROM HAULING OPERATIONS SHALL BE RETACKED AT THE DISCRETION OF THE RESIDENT ENGINEER. THE TACK COAT MUST BE DRY (CURED) TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR TO THE PLACEMENT OF THE POROUS FRICTION COURSE.

PAYMENT FOR THIS ITEM OF WORK WILL BE UNDER ITEM:
 AR402622 "POROUS FRICTION COURSE, 0.10'" _____ PER S.Y.

AR401613 BITUMINOUS SURFACE COURSE-METHOD 1, SUPERPAVE
 THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR401003 "BITUMINOUS SURFACE COURSE-METHOD 1, SUPERPAVE" AS STATED ON PAGE 129 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING 2 LIFTS OF BITUMINOUS SURFACE COURSE-METHOD 1, SUPERPAVE. TOP LIFT OF 1-1/2" ON THE BOTTOM LIFT OF 2-1/2" ON CRUSHED AGGREGATE BASE COURSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE QUALITY CONTROL IN THE PRODUCTION AND CONSTRUCTION OF THE BITUMINOUS SURFACE COURSE METHOD 1, SUPERPAVE.

PRIOR TO STARTING THE BITUMINOUS SURFACE COURSE-METHOD 1, SUPERPAVE OPERATION, THE CONTRACTOR SHALL SUBMIT TO THE RESIDENT ENGINEER A DETAILED OUTLINE SHOWING AREAS AND ORDER OF PAVING WIDTHS OF PAVING LANES, AND REQUIRED OFFSETS FOR ELECTRONIC GRADE.

THE PROPOSED BITUMINOUS SURFACE COURSE METHOD 1, SUPERPAVE WILL BE DESIGNED TO A SUPERPAVE DESIGN OF LESS THAN 60,000 POUNDS.

401-4.9 ADD THE FOLLOWING TO THIS SECTION:

WHEN HAND SPREADING IS PERMITTED, THE MIXTURE WILL BE DISTRIBUTED AND SPREAD USING HAND TOOLS. WHEN THE WORK IS COMPLETED, THE LAYER WILL HAVE THE REQUIRED THICKNESS AND CONFORM TO THE GRADE AND SURFACE CONTOUR SHOWN ON THE PLANS.

401-4.12 ADD THE FOLLOWING TO THIS PARAGRAPH:

ALL PAVEMENT EDGES (LONGITUDINAL, RADIUS, AND PAVEMENT ENDS) MUST BE LEFT IN PROPER ALIGNMENT AS SHOWN ON THE PLANS. THIS MAY BE ACCOMPLISHED BY THE TRIMMING METHOD OUTLINED ABOVE OR AT THE CONTRACTOR'S OPTION BY SAWING AFTER THE PAVING HAS BEEN COMPLETED. NO ADDITIONAL COMPENSATION WILL BE MADE IF THE SAWING METHOD IS USED.

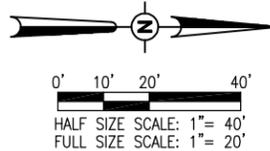
401-6.1 ADD THE FOLLOWING TO THIS SECTION
 AR401613 BIT. SURF. CSE.-METHOD 1, SUPERPAVE _____ PER TON

603-BITUMINOUS TACK COAT NOTES:
 THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 254 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED ON THE PROPOSED BITUMINOUS PAVEMENT PRIOR TO THE PLACEMENT OF THE NEXT LIFT OF PROPOSED BITUMINOUS SURFACE COURSE. THE PROPOSED BITUMINOUS PAVEMENT SHALL HAVE A TACK COAT OF BITUMINOUS MATERIAL APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS.

THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM:
 AR603510 BITUMINOUS TACK COAT _____ PER GAL.

- LEGEND**
- EXISTING PAVEMENT
 - EXISTING BUILDING
 - PROPOSED 401 BITUMINOUS PAVEMENT
 - PROPOSED 401 (4") ON 209 (6")
 - PROPOSED POROUS FRICTION COURSE
 - PROPOSED 610 STRUCTURAL CONCRETE



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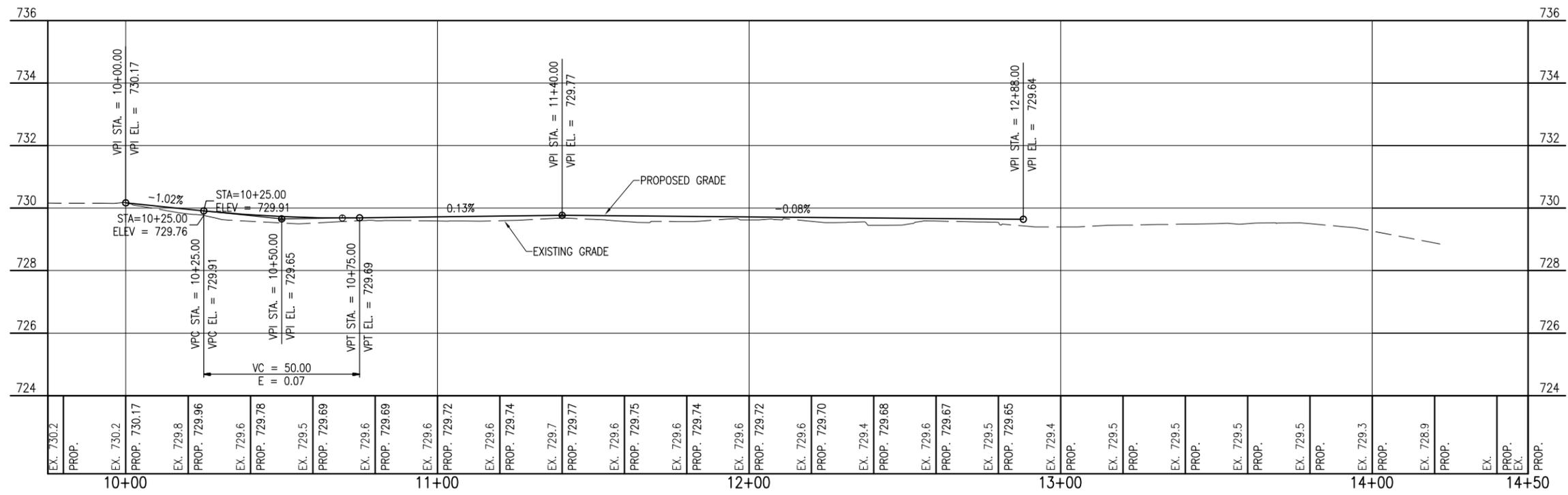
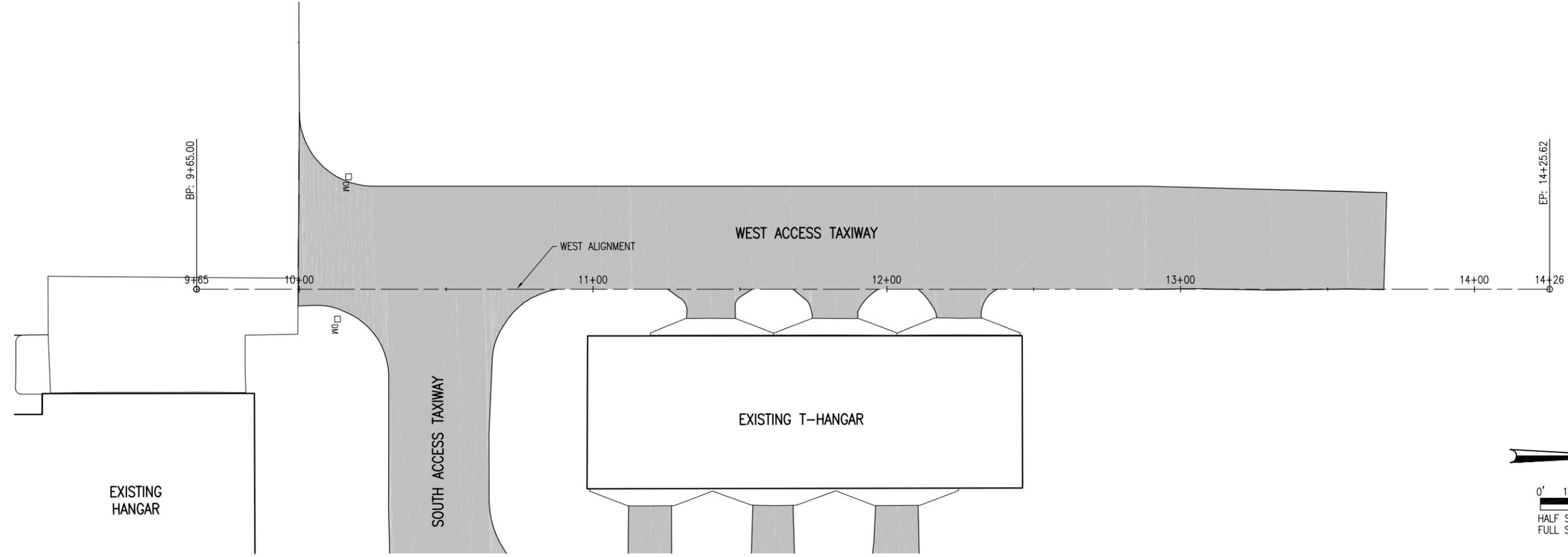
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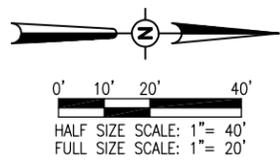
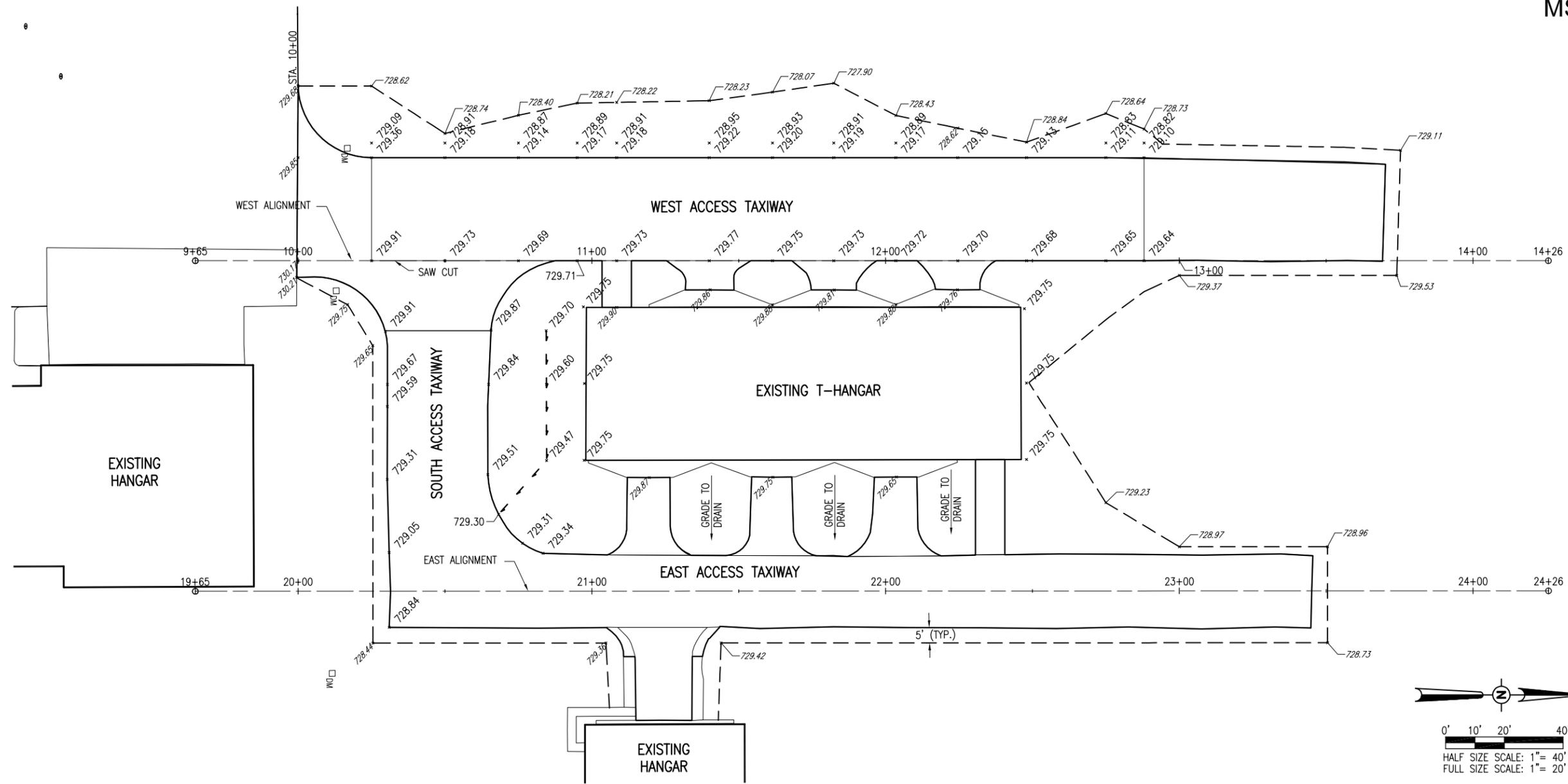
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901 SEEDING NOTES

THE PROPOSED SEEDING SHALL BE ACCOMPLISHED IN ACCORDANCE WITH ITEM 901 "SEEDING" AS STATED ON PAGE 324 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

ALL DISTURBED AREAS LOCATED WITHIN THE PROPOSED GRADING AND SEEDING LIMITS WILL BE SEEDED IN ACCORDANCE WITH THE ABOVE NOTED SPECIFICATION. ALL AREAS OUTSIDE THE DESIGNATED GRADING AND SEEDING LIMITS WILL ALSO BE SEEDED BUT AT THE CONTRACTOR'S OWN EXPENSE.

ALL MATERIALS AND/OR DEBRIS RESULTING FROM THE SEEDING OPERATIONS WILL BE REMOVED FROM THE PAVEMENTS AND MISCELLANEOUS STRUCTURES PRIOR TO OPENING THE RUNWAY.

901-3.4 MAINTENANCE OF SEEDED AREAS. DELETE THE SECOND PARAGRAPH OF THIS SECTION AND ADD THE FOLLOWING:

"THE CONTRACTOR WILL BE REQUIRED TO ESTABLISH A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE RESIDENT ENGINEER. IF AT THE TIME WHEN THE CONTRACT HAS BEEN OTHERWISE COMPLETED, IT IS NOT POSSIBLE TO MAKE AN ADEQUATE DETERMINATION OF COLOR, DENSITY, AND UNIFORMITY OF SUCH STAND OF GRASS, THE ITEM OF WORK WILL BE REVIEWED AT A LATER DATE DETERMINED BY THE ILLINOIS DIVISION OF AERONAUTICS."

DATE SEEDING COMPLETED _____

THE COST FOR PROPOSED SEEDING IS INCLUDED IN COST FOR "SHOULDER ADJUSTMENT"

908 MULCHING NOTES

THE PROPOSED MULCHING SHALL BE ACCOMPLISHED IN ACCORDANCE WITH ITEM 908 "MULCHING" AS STATED ON PAGE 334 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOVEMBER 2, 2009.

THIS ITEM SHALL CONSIST OF THE FURNISHING, TRANSPORTING, AND PLACING MULCH OVER THE SEEDED AREA. DISTURBED AREAS OUTSIDE THE GRADING LIMITS SHALL ALSO BE MULCHED AND PARTICIPATION WILL BE THE SAME AS FOR SEEDED AREAS.

908-2.1 MULCH MATERIAL: THE CONTRACTOR MAY EITHER FURNISH STRAW OR HYDROMULCH AS THE TYPE OF MULCH MATERIAL TO BE USED ON THIS PROJECT.

908-3.1 MULCHING: THE HYDRAULIC MULCH SHALL BE APPLIED AS A SLURRY OF 2,500 POUNDS OF MULCH AND NOT LESS THAN 2,500 GALLONS OF WATER PER ACRE.

908-3.4 STRUCTURE CLEANING: AFTER THE PROPOSED MULCH HAS BEEN APPLIED, THE CONTRACTOR WILL CLEAN THE MULCH OFF ALL STRUCTURES (DRAINAGE, ELECTRICAL, LIGHTS, ETC.).

DATE MULCHING COMPLETED _____

THE COST FOR PROPOSED MULCHING IS INCLUDED IN COST FOR "SHOULDER ADJUSTMENT"

SHOULDER ADJUSTMENT

AFTER THE PROPOSED PFC AND BITUMINOUS PAVEMENT HAS BEEN PLACED, THE CONTRACTOR WILL ADD TOPSOIL MATERIAL TO THE VARIOUS UNPAVED AREAS TO RAISE THE GRADE IN THESE AREAS TO PROVIDE ADEQUATE DRAINAGE FROM THEM. THE CONTRACTOR WILL LIME, FERTILIZE, AND SEED THE DISTURBED AREA. AFTER THE SEEDING HAS BEEN COMPLETED, THE CONTRACTOR WILL COVER THE DISTURBED AREAS WITH MULCH. THE TILLING OF THE EXISTING SOD AREA, ADDITIONAL TOPSOIL MATERIAL, LIMING, FERTILIZING, SEEDING, AND MULCH WILL BE CONSIDERED AS AN INTRICATE PART OF THE PROPOSED SHOULDER ADJUSTMENT, AND WILL NOT BE PAID FOR SEPARATELY.

THE MATERIAL FOR THE PROPOSED EARTH SHOULDERS WILL BE PAID FOR UNDER:
ITEM AR152480 "SHOULDER ADJUSTMENT" _____ PER S.Y.

LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- PROPOSED DRAINAGE DITCH
- EXISTING GRADE
- PROPOSED GRADE
- PROPOSED GRADING AND SEEDING LIMITS

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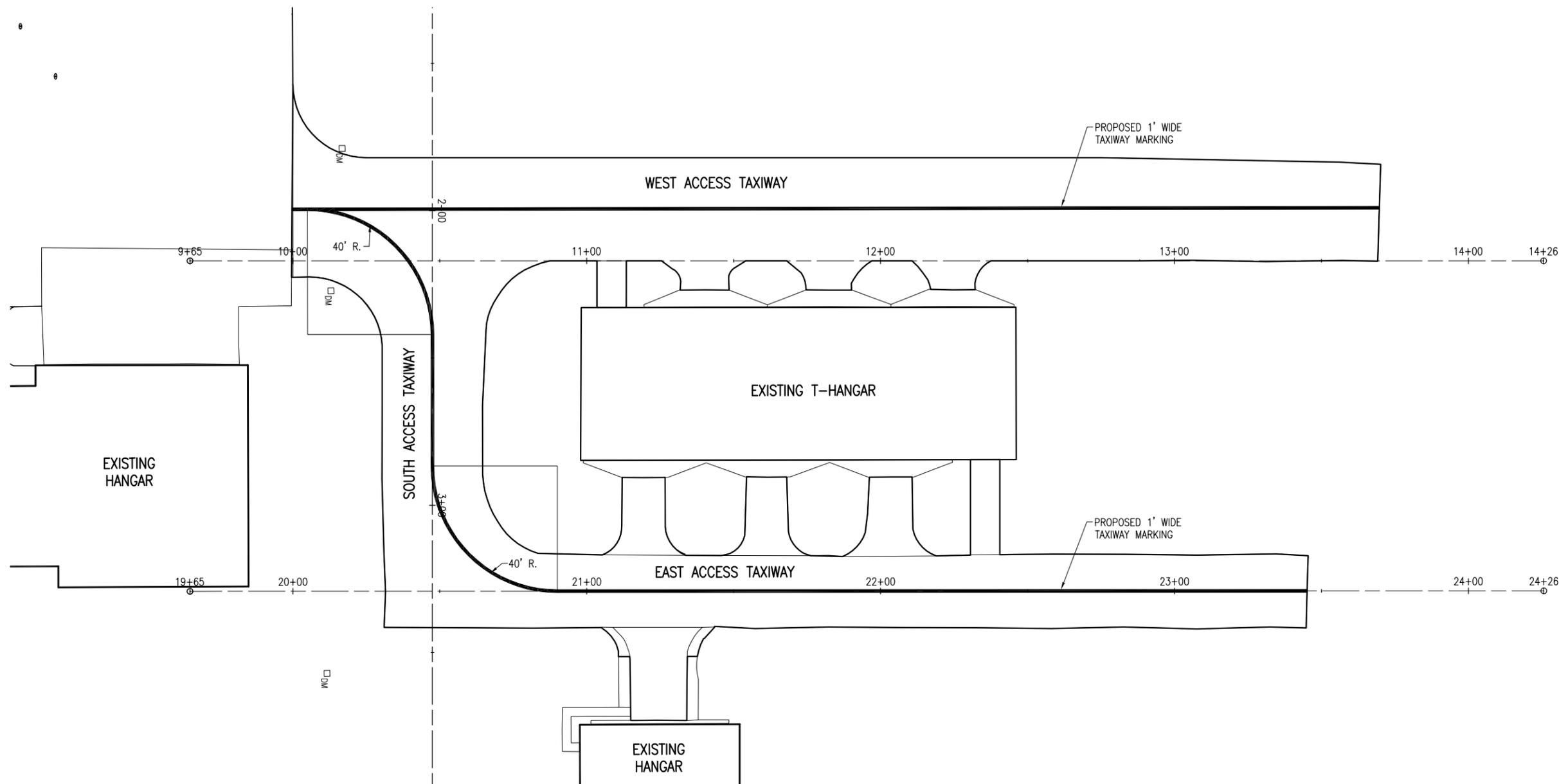
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620-PAVEMENT MARKING-WATERBORNE NOTES

THE PAVEMENT MARKING-WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED ON PAGE 277 OF THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED NOV. 2, 2009.

THIS ITEM SHALL CONSIST OF TAXIWAY CENTERLINE MARKING IN ACCORDANCE WITH THESE SPECIFICATIONS AND AT THE LOCATIONS SHOWN ON THIS SHEET. ALL MARKING WILL BE YELLOW IN COLOR. THE PROPOSED PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS.

ANY MATERIAL DELIVERED THAT FAILS TO MEET THE SPECIFICATIONS SHALL BE DISPOSED OF BY THE VENDOR AND IMMEDIATELY REPLACED WITH ACCEPTABLE MATERIAL ENTIRELY AT THE VENDOR'S EXPENSE, INCLUDING HANDLING AND TRANSPORTATION CHARGES.

ALL CURING COMPOUND WILL BE CLEANED FROM CONCRETE PAVEMENT PRIOR TO APPLYING PAINT. NO EXCEPTIONS.

ALL PROPOSED MARKING WILL BE COMPLETED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE CONSTRUCTION PLANS.

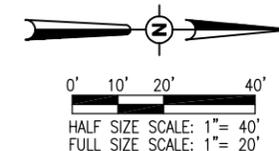
GLASS BEADS SHALL BE REQUIRED ONLY ON THE SECOND APPLICATION OF YELLOW MARKING.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

THE PROPOSED MARKING WILL BE PAID FOR UNDER ITEM:
AR620520 PAVEMENT MARKING-WATERBORNE ____ PER S.F.

LEGEND

-  EXISTING PAVEMENT
-  EXISTING BUILDING
-  PROPOSED MARKING



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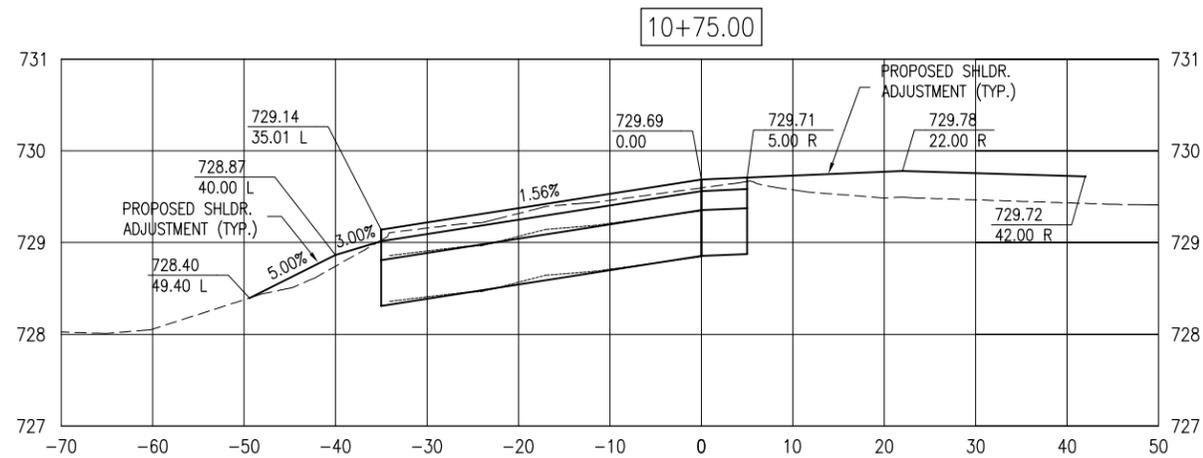
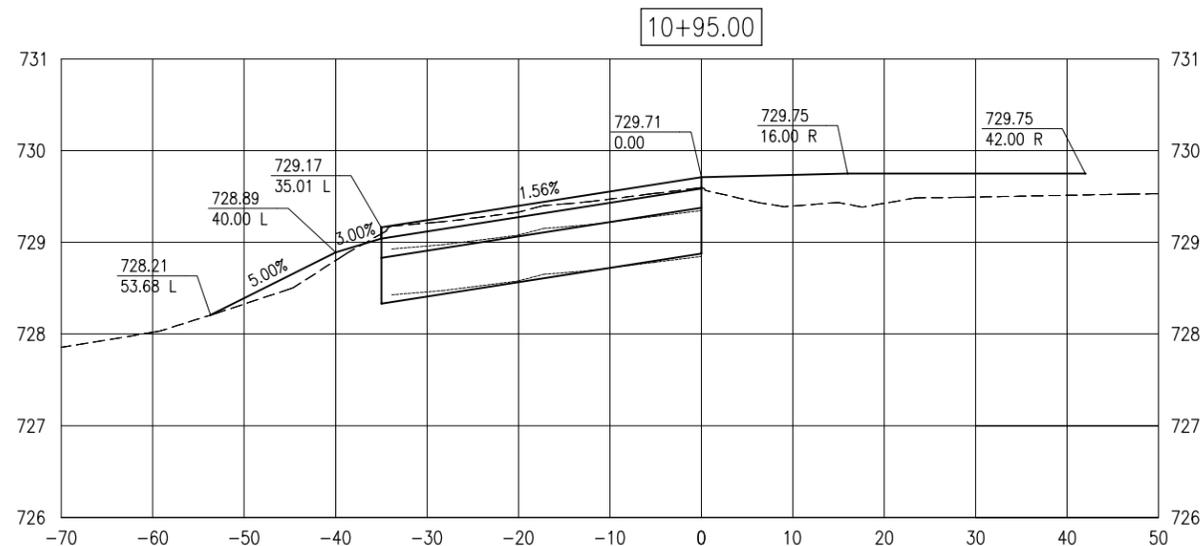
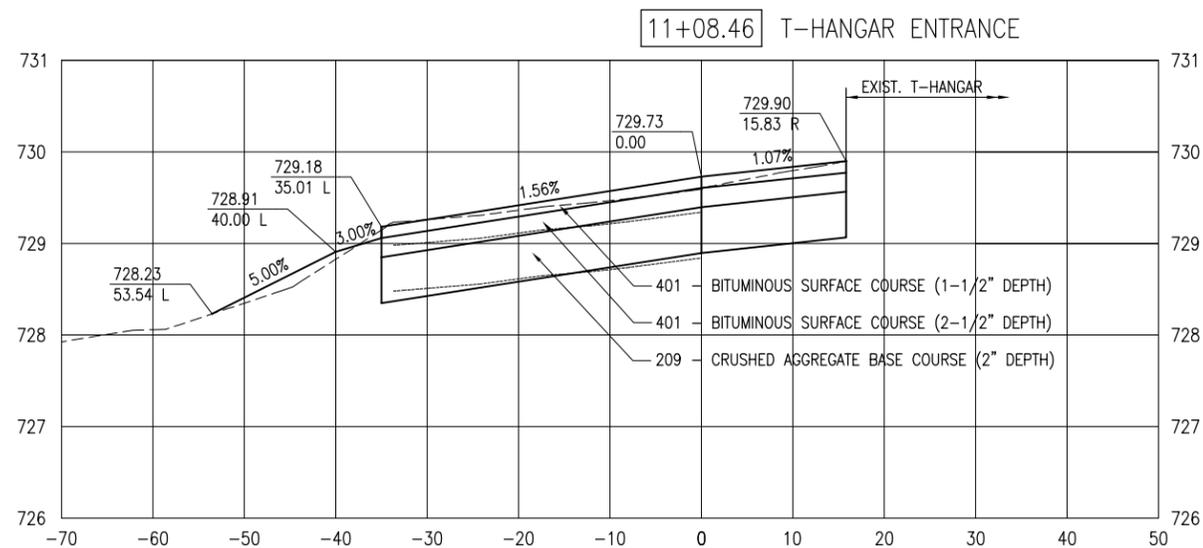
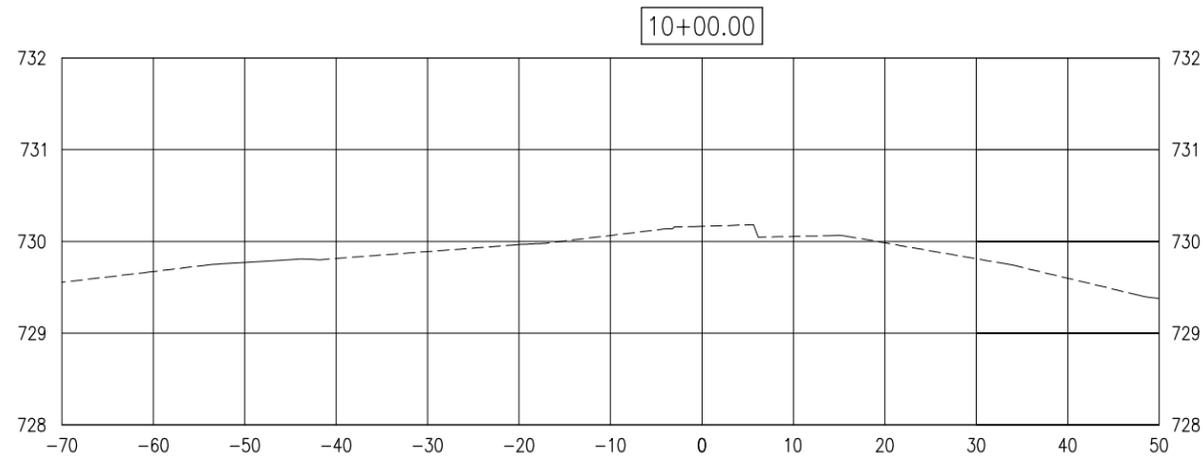
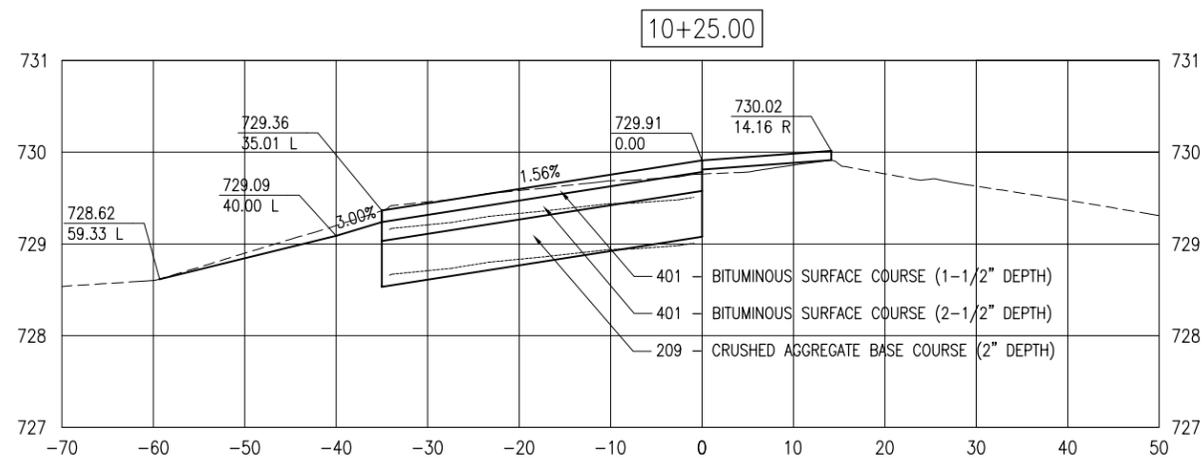
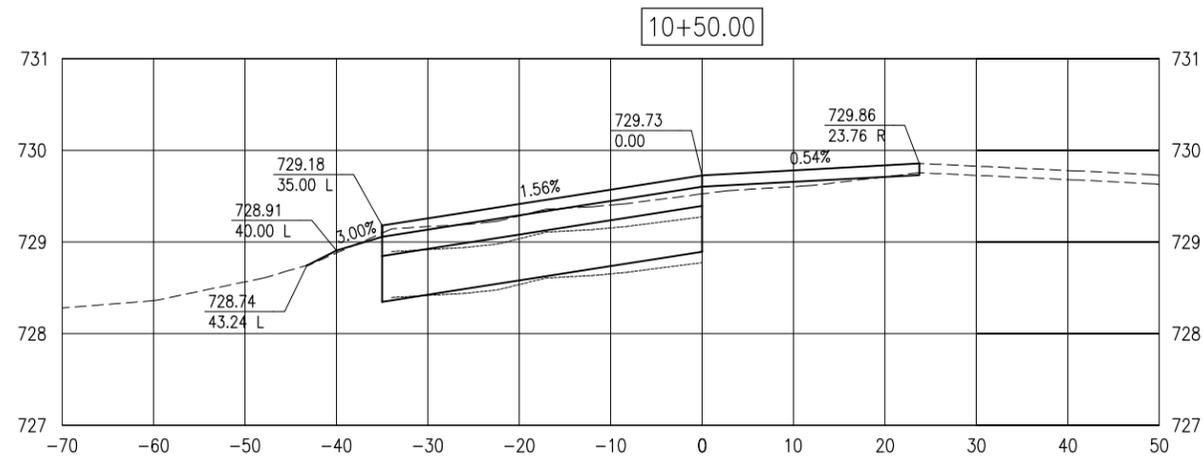
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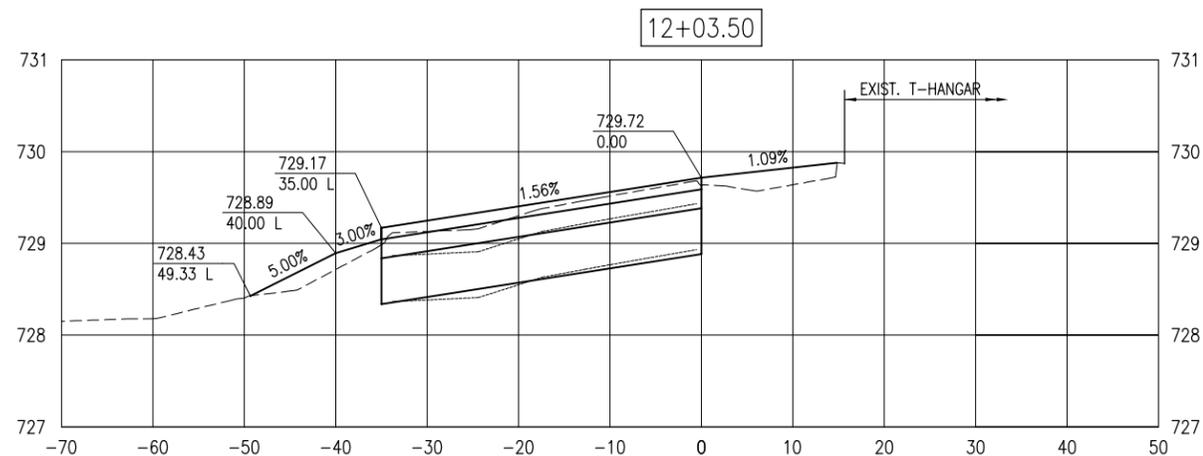
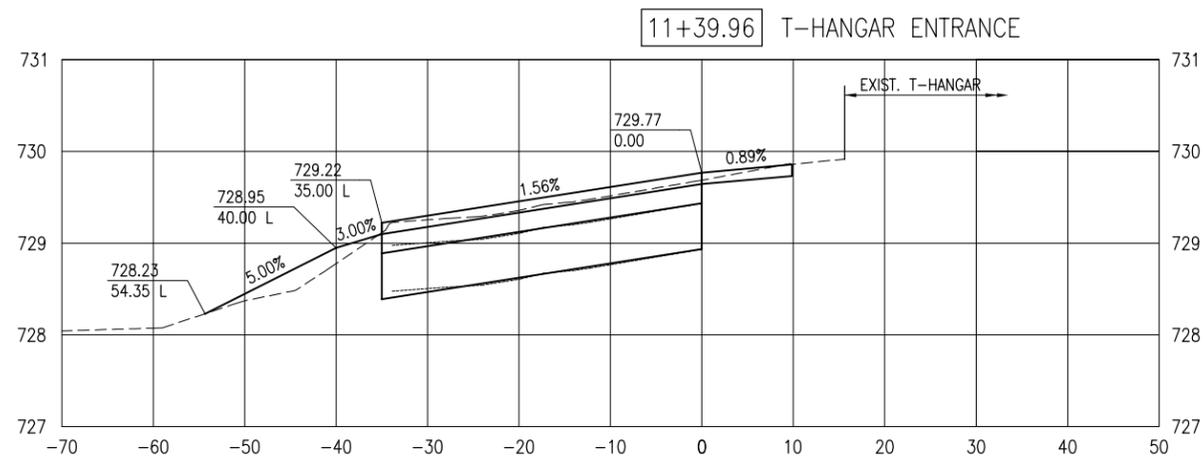
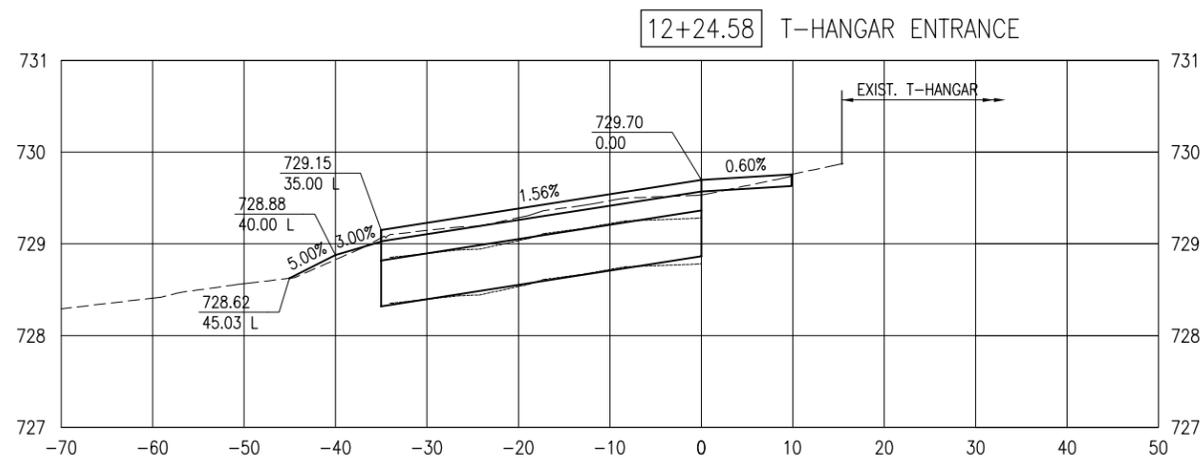
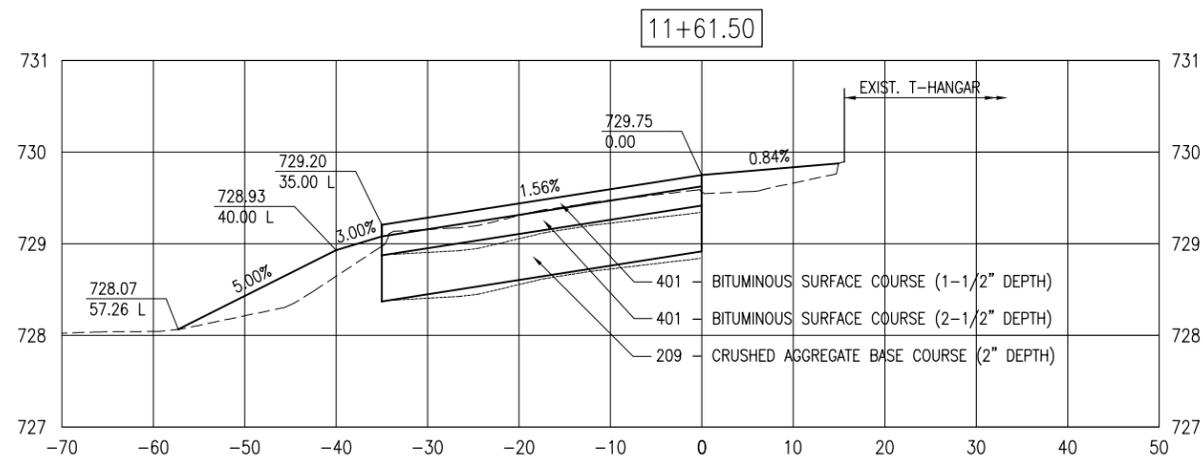
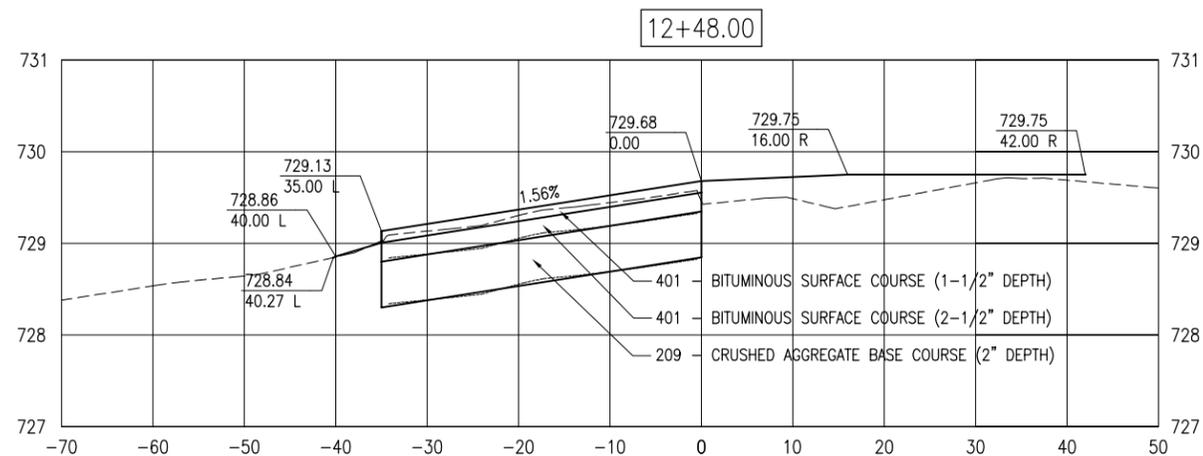
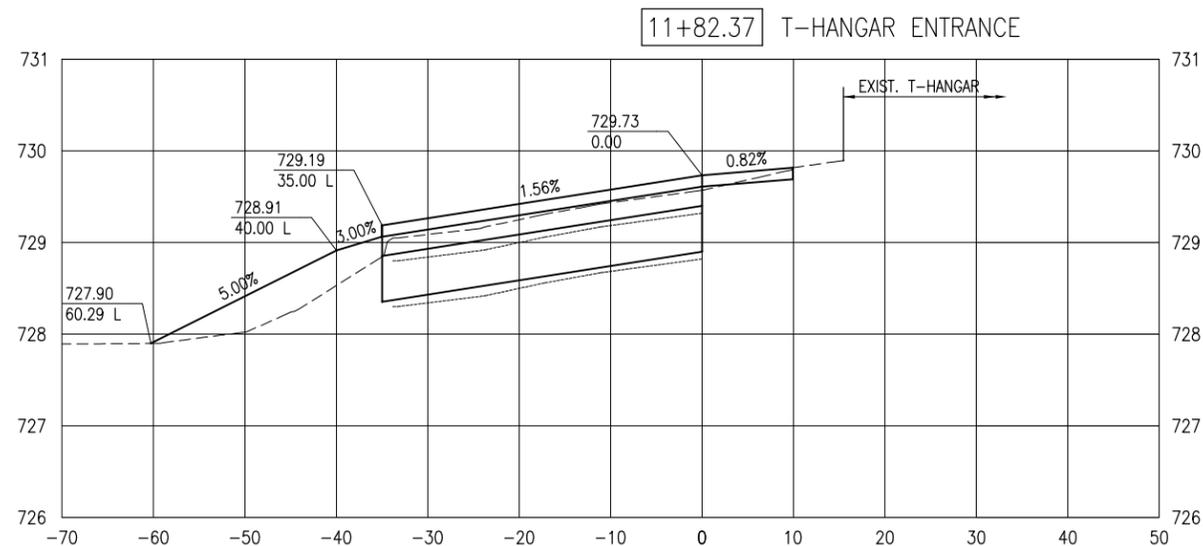
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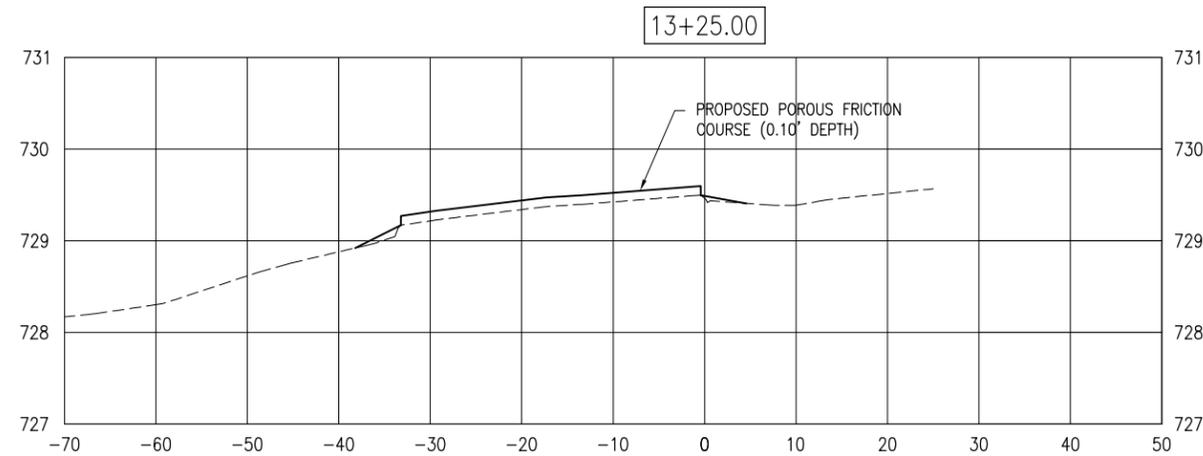
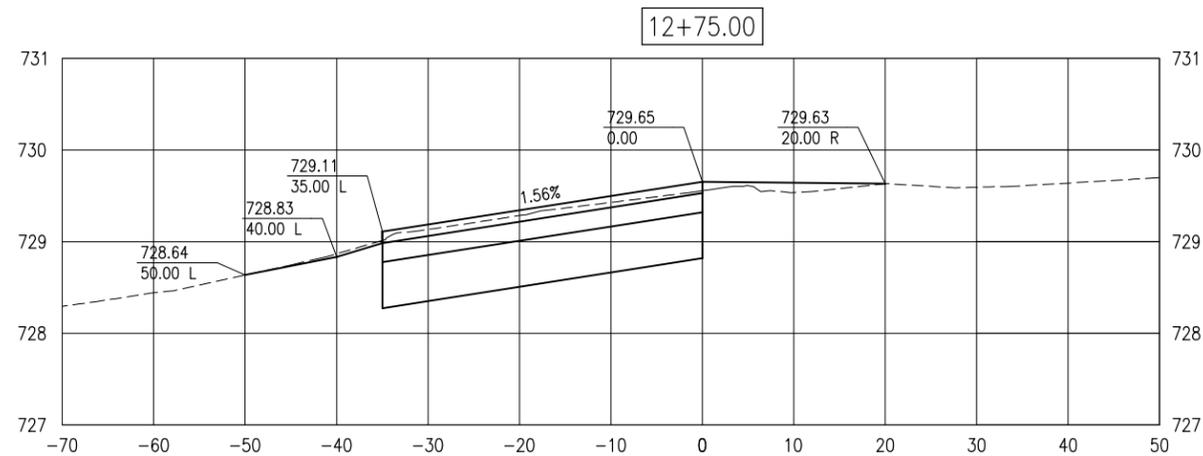
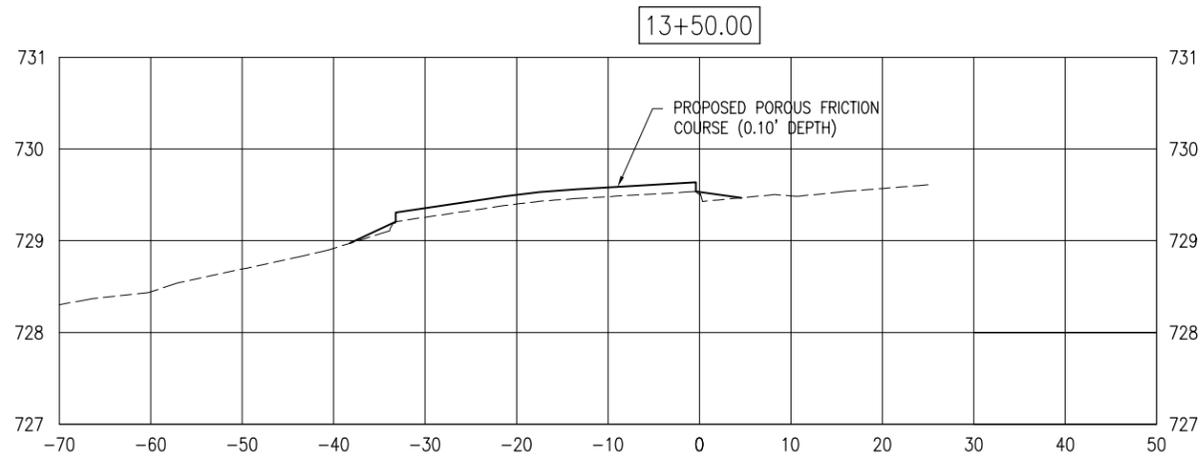
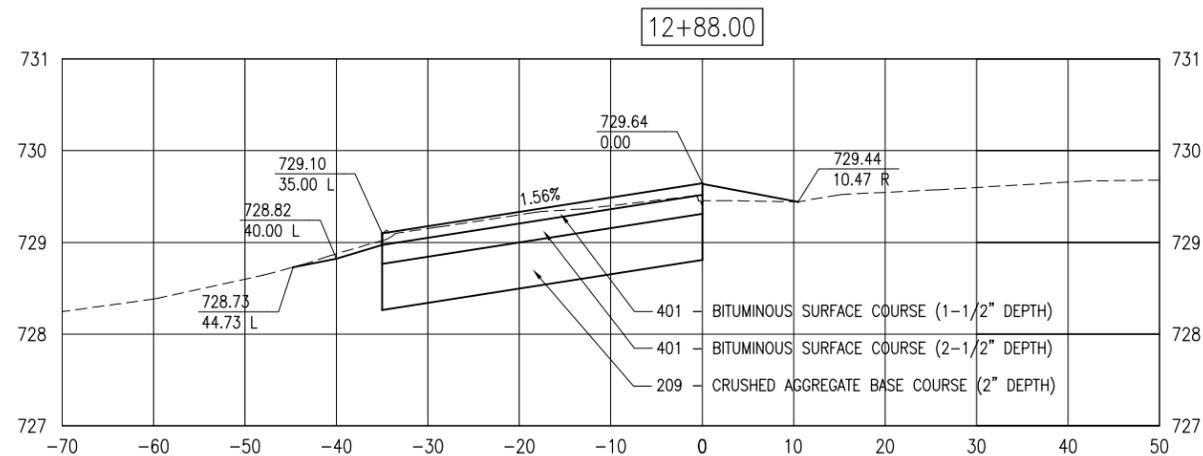
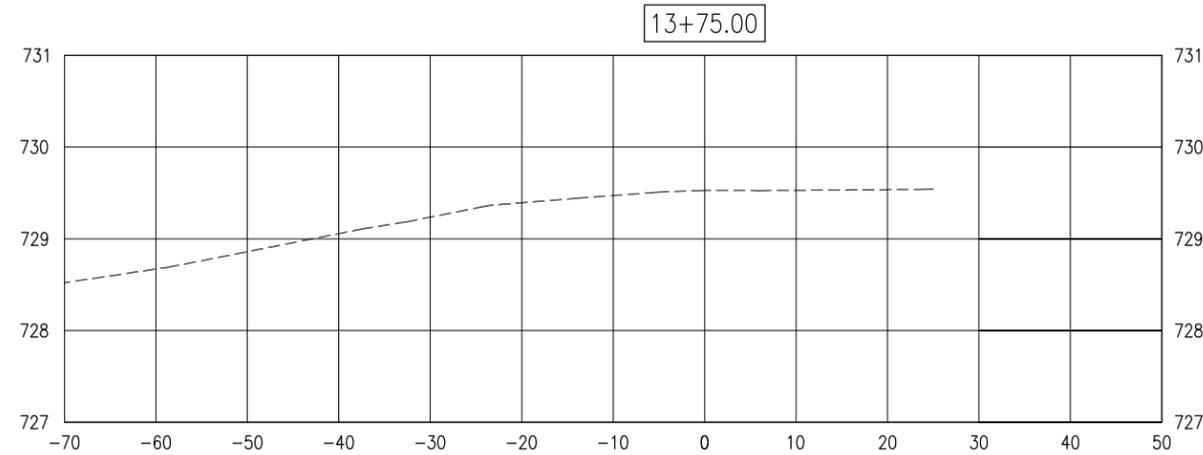
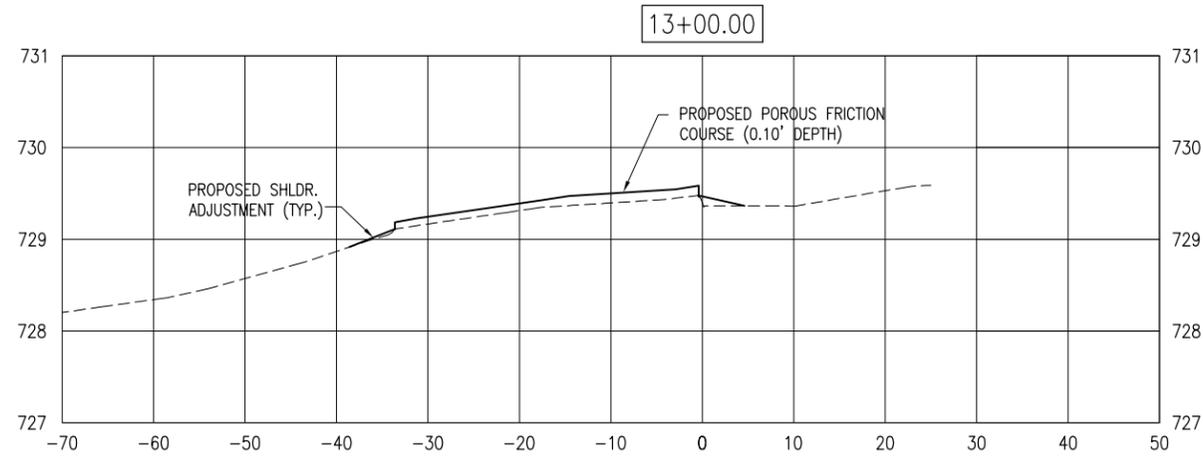
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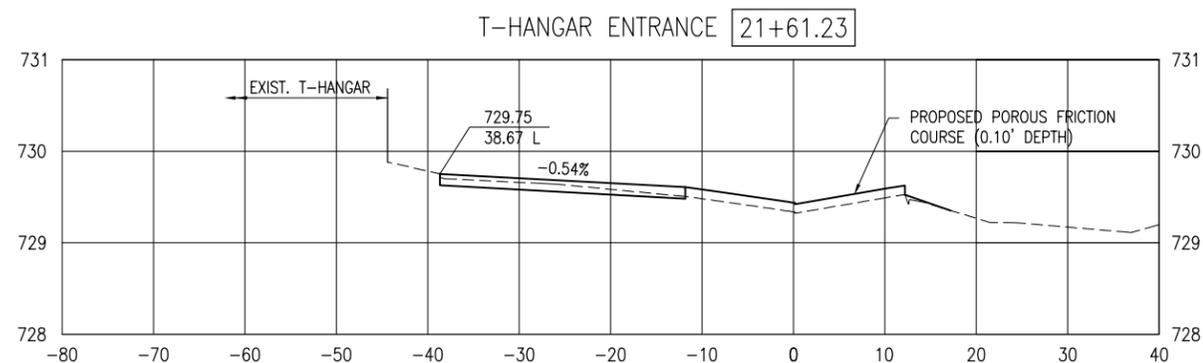
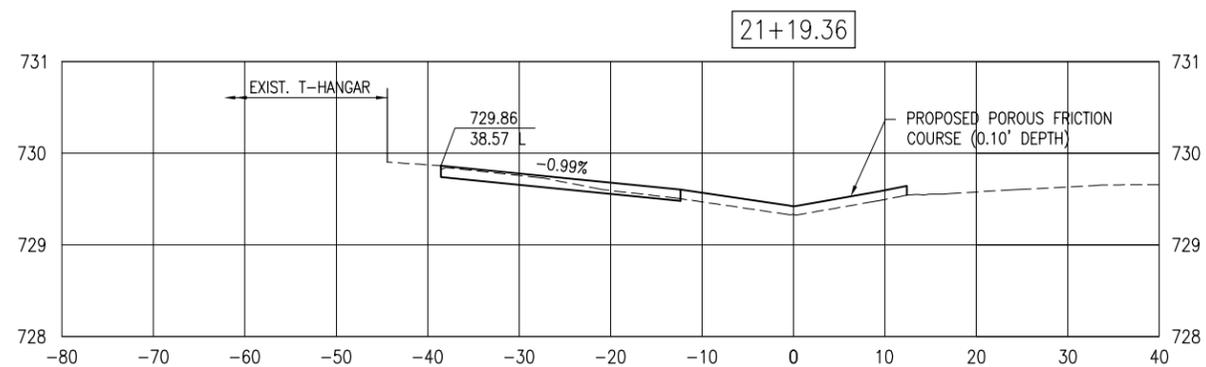
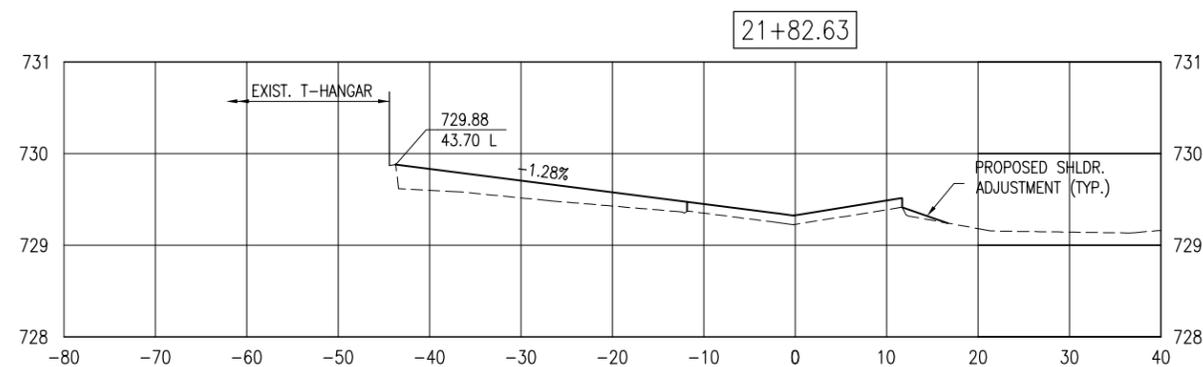
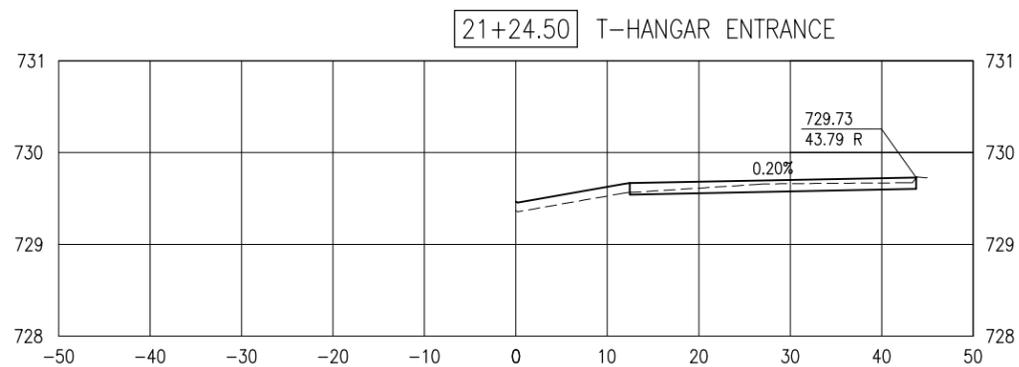
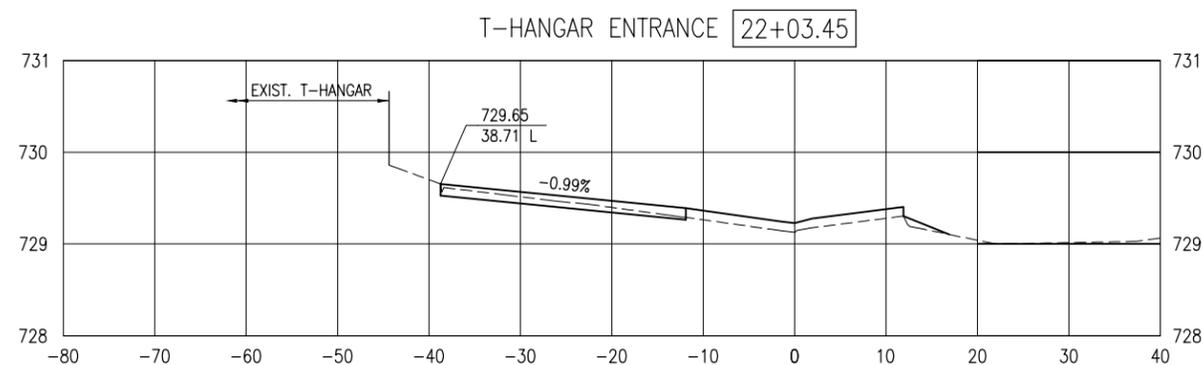
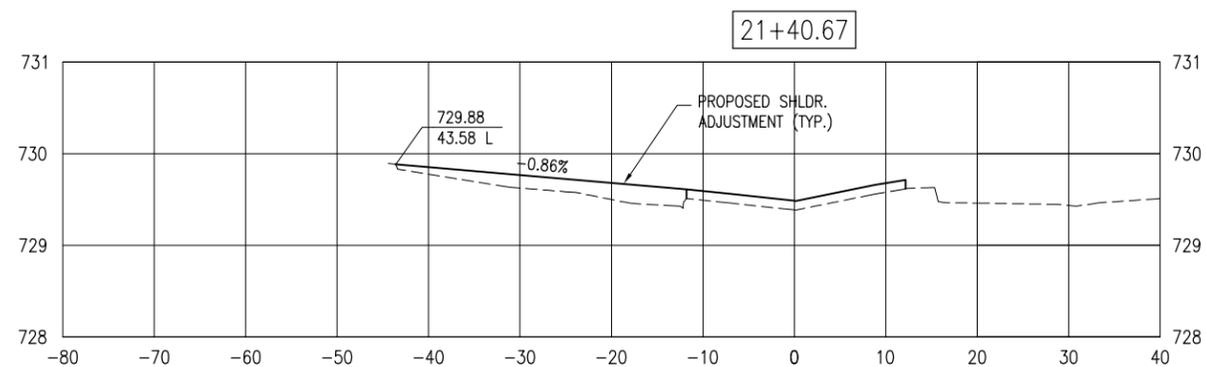
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LAYOUT	JEO	02/21/12	
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