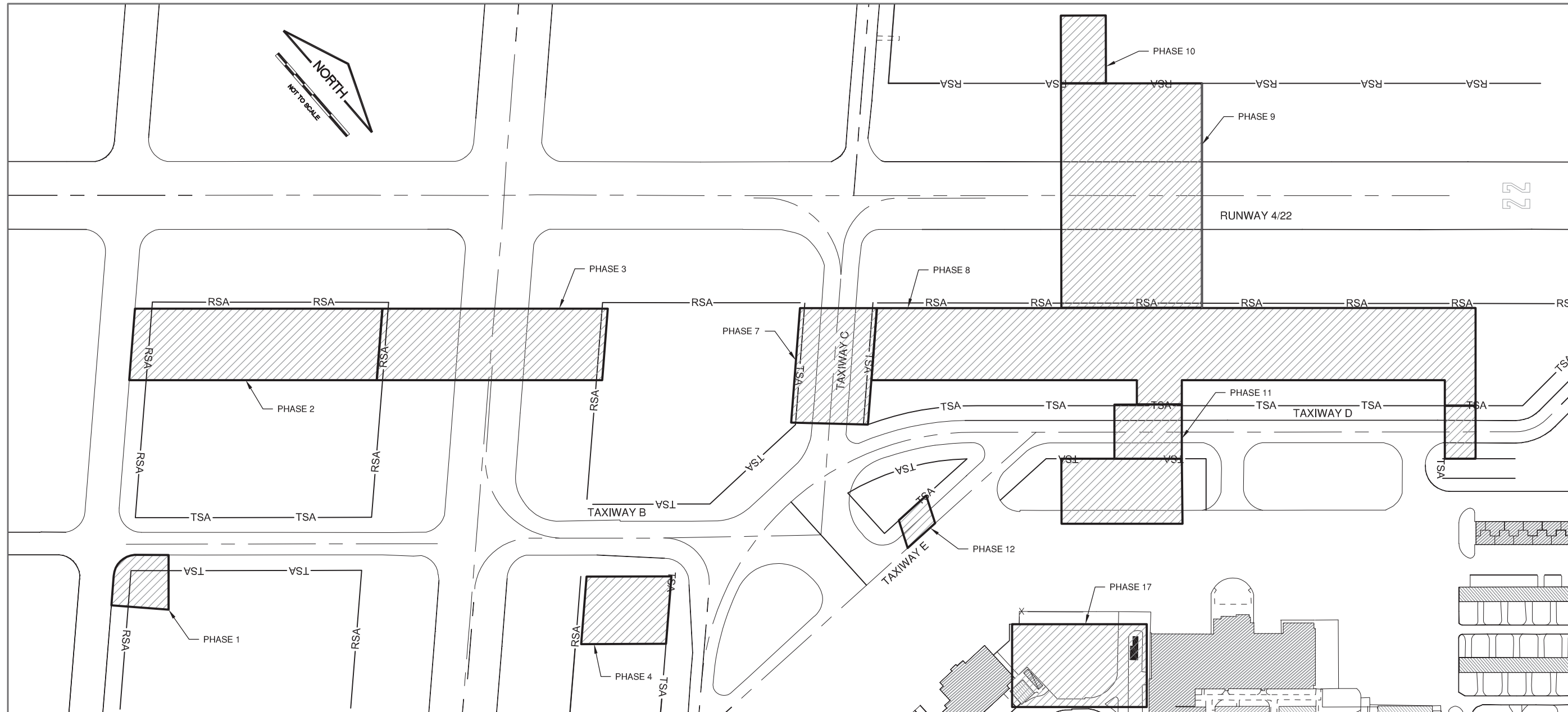
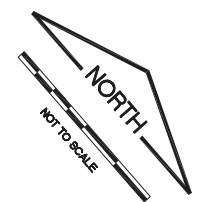
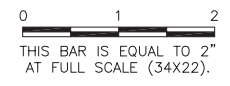


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SEQUENCE OF CONSTRUCTION NOTES

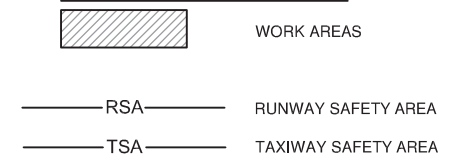
- PHASE 25 - 32
1. THE CONTRACTOR SHALL LOCATE EXISTING CIRCUITS AND DESIGNATE A LOCATION FOR CONNECTION OF THE EXISTING CIRCUIT TO THE NEW EXTENSION TO THE PROPOSED VAULT. WHEN THE EQUIPMENT IN THE NEW VAULT IS READY FOR OPERATION, THE CONTRACTOR SHALL COORDINATE THE "SWITCH OVER" OF EACH CIRCUIT INDIVIDUALLY WITH THE AIRPORT. THE TIMING OF THE SWITCH OVER SHALL BE APPROVED BY THE AIRPORT SO AS TO MINIMIZE THE DISTURBANCE TO OPERATIONS AT THE AIRPORT. THE CONTRACTOR SHALL PROVIDE THE AIRPORT WITH A TIME THAT EACH CIRCUIT WILL BE DISABLED. IF NECESSARY THE CONTRACTOR SHALL BE REQUIRED TO MAKE THE SWITCH OVER DURING NON PEAK HOURS. THE CONTRACTOR MAY BE REQUIRED TO INSTALL TEMPORARY JUMPERS TO COMPLETE EXISTING AND PROPOSED CIRCUITS OPERATIONAL DURING THE SWITCH OVER, TEMPORARY JUMPERS WILL BE INCIDENTAL TO CONSTRUCTION.
 2. PHASE 1 & 2 SHALL INCLUDE COMPLETING EXISTING CIRCUITS AT EXISTING SPLICE CANS AND SPLICING NEW HOMERUN TO THE RESPECTIVE PAPI 14R/32L, RUNWAY 14R/32L AND RUNWAY 18/36 CIRCUITS AT NEW SPLICE CANS AND EXISTING BASE MOUNTED LIGHT.
 3. PHASE 3 SHALL INCLUDE SPLICING RUNWAY 14L/32R HOMERUN AT EXISTING BASE MOUNTED LIGHT; DISCONNECTING EXISTING HOMERUN CABLES AT EDGE LIGHTS AND INSTALLING NEW JUMPER CABLE BETWEEN LIGHTS.
 4. PHASE 8 SHALL INCLUDE SPLICING PAPI 14L, TAXIWAY C CIRCUIT 2/3, AND RUNWAY GUARD LIGHTS CIRCUIT #1 AT EXISTING SPLICE CANS. THIS PHASE SHALL ALSO INCLUDE SPLICING PAPI 32R, TAXIWAY A CIRCUIT 4/5, AND RUNWAY GUARD LIGHTS CIRCUIT #2 IN NEW SPLICE CANS. THIS PHASE SHALL ALSO INCLUDE SPLICING NEW CENTER TAXIWAY CIRCUIT 1 HOMERUN AT EXISTING BASE MOUNTED LIGHT; DISCONNECTING EXISTING HOMERUN CABLES AT EDGE LIGHTS, & INSTALLING NEW JUMPER CABLE BETWEEN LIGHTS.
 5. PHASE 11 SHALL INCLUDE SPLICING PAPI 32R, TAXIWAY A CIRCUIT 4/5 & TAXIWAY GUARD LIGHTS CIRCUITS #2 TO PAPI 14L, TAXIWAY C CIRCUIT 2/3 & RUNWAY GUARD LIGHTS CIRCUIT #1 RESPECTIVELY AT EXISTING MANHOLE #2.
 6. PHASE 9 SHALL INCLUDE SPLICING NEW RUNWAY 4/22 HOMERUN AT EXISTING BASE MOUNTED LIGHT; DISCONNECTING EXISTING HOMERUN CABLES AT EDGE LIGHTS, AND INSTALLING NEW JUMPER CABLE BETWEEN LIGHTS.

7. PHASE 11 SHALL INCLUDE SPLICING NEW TAXIWAY D CIRCUIT 8 HOMERUN AT EXISTING BASE MOUNTED LIGHT; DISCONNECTING EXISTING HOMERUN CABLES AT EDGE LIGHTS, AND INSTALLING NEW JUMPER CABLE BETWEEN LIGHTS.
8. PHASE 10 SHALL INCLUDE SPLICING NEW WINDCONE CABLE TO EXISTING WINDCONE.
9. CONTRACTOR SHALL REMOVE ALL ABANDONED HOMERUNS FROM EXISTING DUCTS.

PHASE 17

1. THE CONTRACTOR SHALL DECOMMISSION THE EXISTING VAULT FOLLOWING THE COMPLETION OF THE NEW VAULT AND AFTER THE EQUIPMENT IN THE EXISTING VAULT IS NO LONGER NEEDED.
2. THE CONTRACTOR SHALL REMOVE THE EXISTING EQUIPMENT FROM THE EXISTING VAULT AND DISPOSE OF THE EQUIPMENT OFF OF AIRPORT PROPERTY. THE REMOVAL METHODS SHALL BE SUCH THAT THE EQUIPMENT MAY BE RE-USED IF DESIRED BY THE AIRPORT. PRIOR TO REMOVAL FROM THE SITE, THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT WHICH EQUIPMENT THE AIRPORT WISHES TO RETAIN. THIS EQUIPMENT SHALL THEN BE STORED ON THE AIRPORT AT A LOCATION SPECIFIED BY THE AIRPORT MANAGER.

LEGEND



WILLARD AIRPORT
 UNIVERSITY OF ILLINOIS

NEW AIRFIELD LIGHTING VAULT
 CONSTRUCTION ACTIVITY PLAN 4

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| DRAWN BY: | CMT |
| CHECKED BY: | CBG |
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| JOB No: | 11059-03 |
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