

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90	2003-029	COOK	344	15
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

62581

CURVE EHB-1
 PI STA. = 75+08.28
 N=1885747.83
 E=1175493.82
 $\Delta = 1^\circ 37' 12''$ (LT)
 D = 0° 31' 33"
 R = 10,896.14'
 T = 154.06'
 L = 308.10'
 E = 1.09'
 e = N/A
 T.R. = N/A
 S.E. RUN = 0.053 FT/FT
 P.C. STA. = 73+54.22
 N=1885593.83
 E=1175498.00
 P.T. STA. = 76+62.32
 N=1885901.66
 E=1175485.30

CURVE EHB-2
 PI STA. = 78+55.79
 N=1886094.82
 E=1175474.59
 $\Delta = 19^\circ 50' 45''$ (LT)
 D = 5° 10' 51"
 R = 1,105.89'
 T = 193.46'
 L = 383.05'
 E = 16.79'
 e = N/A
 T.R. = N/A
 S.E. RUN = 0.053 FT/FT
 P.C. STA. = 76+62.32
 N=1885901.66
 E=1175485.30
 P.T. STA. = 80+45.37
 N=1886272.88
 E=1175398.93

CURVE HL-1
 SPIRAL SEGMENT
 DELTA=71° 15' 49.43' LS=150.00 FT DELTA(S)=03° 22' 30.00'
 X=149.95 FT Y=02.94 FT P=0.74 FT
 K=74.99 FT ST=50.02 FT LT=100.02 FT
 LC =149.98 FT TS=988.37 FT ES=294.27 FT

CURVE EHB-3
 PI STA. = 81+96.84
 N=1886412.29
 E=1175339.70
 $\Delta = 13^\circ 28' 11''$ (LT)
 D = 4° 28' 01"
 R = 1,282.65'
 T = 151.47'
 L = 301.54'
 E = 8.91'
 e = N/A
 T.R. = N/A
 S.E. RUN = 0.053 FT/FT
 P.C. STA. = 80+45.37
 N=1886272.88
 E=1175398.93
 P.T. STA. = 83+46.91
 N=1886534.07
 E=1175249.63

CURVE EHB-4
 PI STA. = 87+56.94
 N=1886863.72
 E=1175005.80
 $\Delta = 36^\circ 19' 40''$ (LT)
 D = 4° 35' 04"
 R = 1,249.78'
 T = 410.03'
 L = 792.41'
 E = 65.54'
 e = N/A
 T.R. = N/A
 S.E. RUN = 0.053 FT/FT
 P.C. STA. = 83+46.91
 N=1886534.07
 E=1175249.63
 P.T. STA. = 91+39.32
 N=1886984.86
 E=1174614.07

CIRCULAR SEGMENT
 TS Sta 74+73.48
 N= 1885718.50
 E=1175669.10
 SC Sta 76+23.48
 N=1885868.31
 E=1175662.09
 CS Sta 90+57.12
 N=1886951.08
 E=1174840.67
 ST Sta 92+07.12
 N=1886998.19
 E=1174698.28

CURVE HL-1
 PI STA. = 84+27.05
 N=1886668.91
 E=1175593.06
 $\Delta = 64^\circ 30' 49''$ (LT)
 D = 4° 30' 00"
 R = 1,273.24'
 T = 803.57'
 L = 1,433.64'
 E = 232.37'
 e = N/A
 T.R. = N/A
 S.E. RUN = 0.053 FT/FT

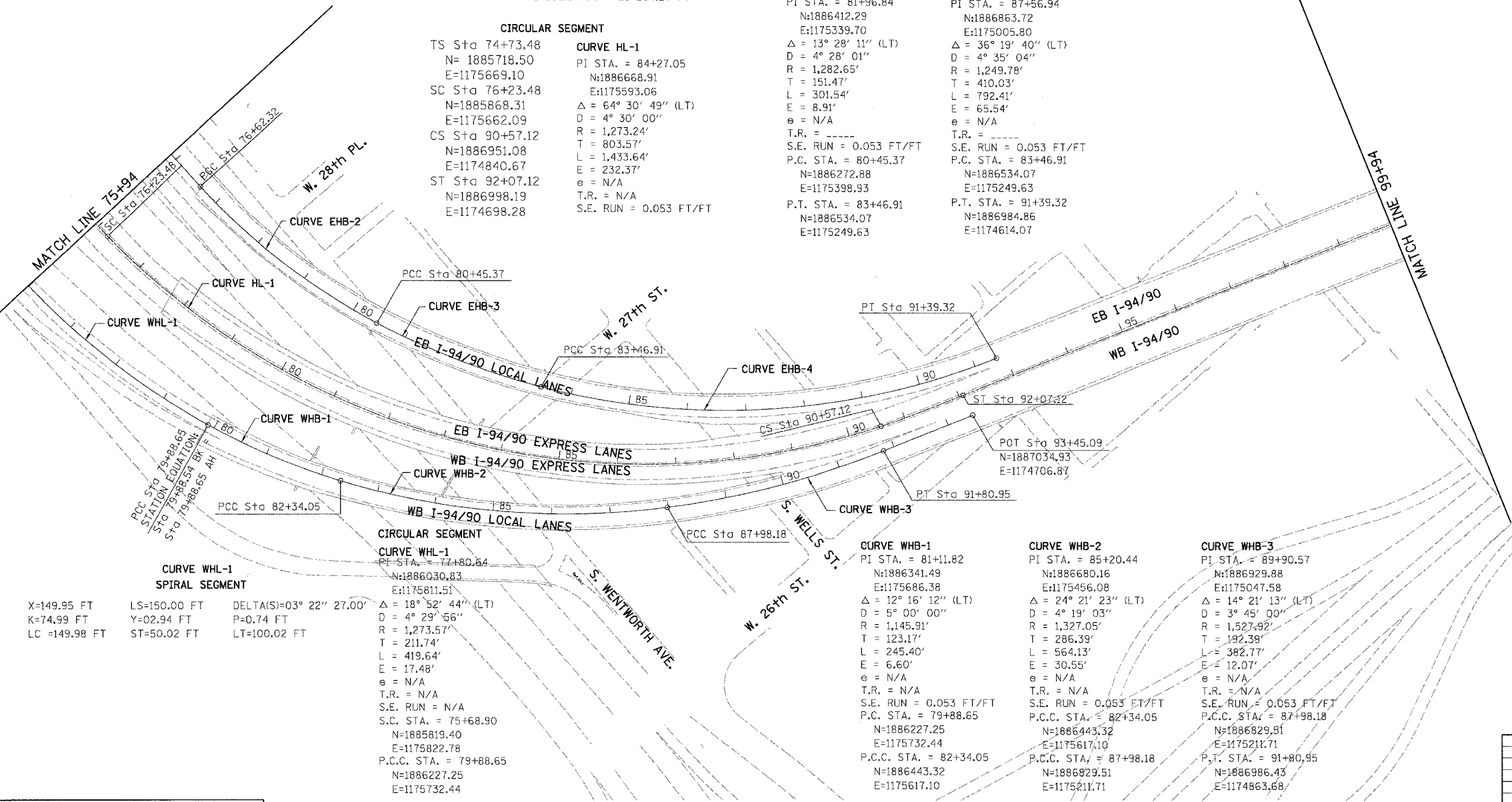
CURVE WHL-1
 SPIRAL SEGMENT
 X=149.95 FT LS=150.00 FT DELTA(S)=03° 22' 27.00'
 K=74.99 FT Y=02.94 FT P=0.74 FT
 LC =149.98 FT ST=50.02 FT LT=100.02 FT

CURVE WHL-1
 PI STA. = 77+80.84
 N=1886030.83
 E=1175811.51
 $\Delta = 18^\circ 52' 44''$ (LT)
 D = 4° 29' 56"
 R = 1,273.57'
 T = 211.74'
 L = 419.64'
 E = 17.48'
 e = N/A
 T.R. = N/A
 S.E. RUN = N/A
 S.C. STA. = 75+68.90
 N=1885819.40
 E=1175822.78
 P.C.C. STA. = 79+88.65
 N=1886227.25
 E=1175732.44

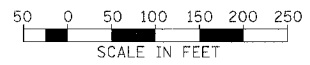
CURVE WHB-1
 PI STA. = 81+11.82
 N=1886341.49
 E=1175686.38
 $\Delta = 12^\circ 16' 12''$ (LT)
 D = 5° 00' 00"
 R = 1,145.91'
 T = 123.17'
 L = 245.40'
 E = 6.60'
 e = N/A
 T.R. = N/A
 S.E. RUN = 0.053 FT/FT
 P.C. STA. = 79+88.65
 N=1886227.25
 E=1175732.44
 P.C.C. STA. = 82+34.05
 N=1886443.32
 E=1175617.10

CURVE WHB-2
 PI STA. = 85+20.44
 N=1886680.16
 E=1175456.08
 $\Delta = 24^\circ 21' 23''$ (LT)
 D = 4° 19' 03"
 R = 1,327.05'
 T = 286.39'
 L = 564.13'
 E = 30.55'
 e = N/A
 T.R. = N/A
 S.E. RUN = 0.053 FT/FT
 P.C.C. STA. = 82+34.05
 N=1886443.32
 E=1175617.10
 P.C.C. STA. = 87+98.18
 N=1886929.51
 E=1175211.71

CURVE WHB-3
 PI STA. = 89+90.57
 N=1886929.88
 E=1175047.58
 $\Delta = 14^\circ 21' 13''$ (LT)
 D = 3° 45' 00"
 R = 1,527.92'
 T = 192.39'
 L = 382.77'
 E = 12.07'
 e = N/A
 T.R. = N/A
 S.E. RUN = 0.053 FT/FT
 P.C.C. STA. = 87+98.18
 N=1886829.91
 E=1175211.71
 P.T. STA. = 91+80.95
 N=1886986.43
 E=1174863.68



BOWMAN, BARRETT & ASSOCIATES INC.
 CONSULTING ENGINEERS
 Chicago, Illinois
 312.228.0100
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
 SB DAN RYAN ELEVATED BRIDGE
 REPAIR FROM 15TH TO 28TH STREETS
 ALIGNMENT PLAN
 STA. 75+94 TO STA. 99+94

SCALE: 1"=100'
 DATE: 4/14/2006

DRAWN BY: RA
 CHECKED BY: RS

CAL02

4/14/2006 9:50:55 AM