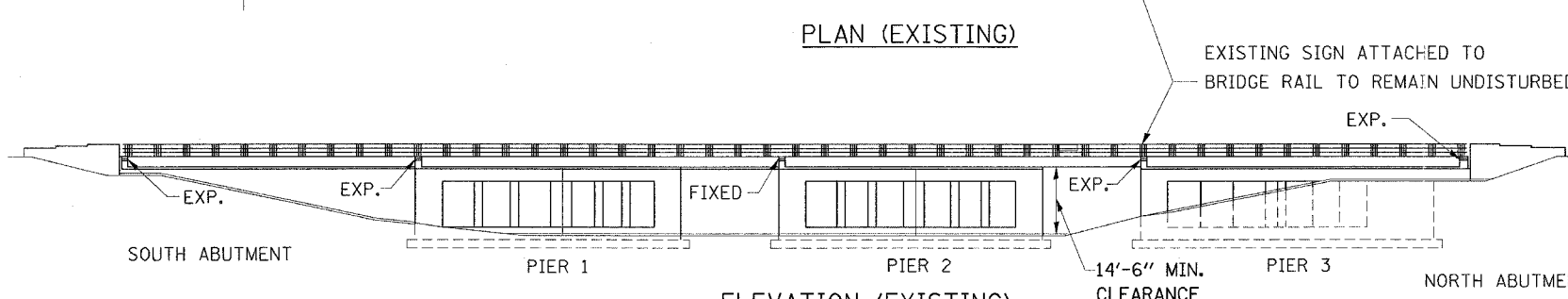
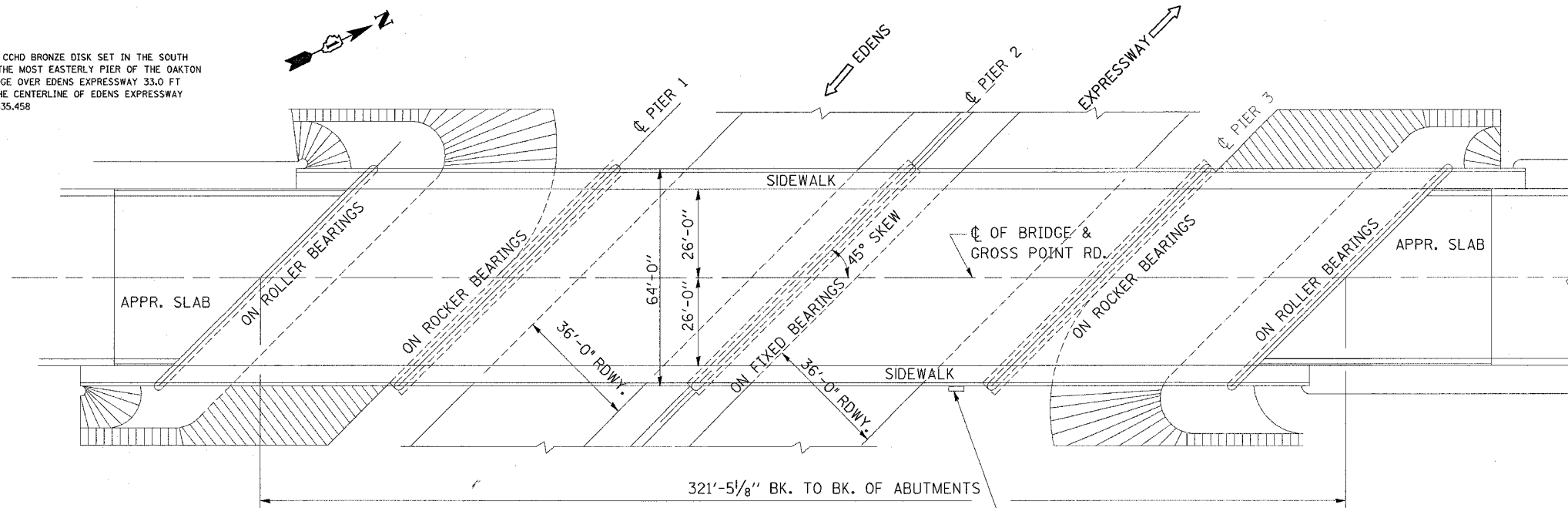


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3520	2005-001 RS	COOK	37	13
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62903				

BENCHMARK:
A STANDARD CCHD BRONZE DISK SET IN THE SOUTH CORNER OF THE MOST EASTERLY PIER OF THE DAKTON STREET BRIDGE OVER EDENS EXPRESSWAY 33.0 FT SOUTH OF THE CENTERLINE OF EDENS EXPRESSWAY ELEVATION 635.458



PLAN (EXISTING)

ELEVATION (EXISTING)

TOTAL BILL OF MATERIALS

DESCRIPTION	UNIT	QUANTITY
BRIDGE DECK GROOVING	SQ. YDS.	1860
BITUMINOUS CONCRETE REMOVAL (DECK)	SQ. YDS.	1865
BRIDGE DECK HYDRO-SCARIFICATION 1/2" (*)	SQ. YDS.	2450
DECK SLAB REPAIR FULL DEPTH (TYPE I)	SQ. YDS.	2
DECK SLAB REPAIR FULL DEPTH (TYPE II)	SQ. YDS.	63
BRIDGE DECK LATEX CONCRETE OVERLAY	SQ. YDS.	1865
SILICONE JOINT SEALER, 2"	FOOT	281
JACK AND REMOVE EXISTING BEARING	EACH	24
FURNISHING AND ERECTING STRUCTURAL STEEL	POUNDS	6990
ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	24
BITUMINOUS CONCRETE REMOVAL (**)	SQ. YDS.	2450
POLYMER CONCRETE	CU. FT.	20
PROTECTIVE COAT	SQ. YDS.	1865
APPROACH SLAB REPAIR FULL DEPTH	SQ. YDS.	32
POLYMERIZED LEVELING BINDER, SUPERPAVE, IL 4.75, N-50, 3/4"	TONS	25
BITUMINOUS SURFACE COURSE, SUPERPAVE MIX D, N-50	TONS	50
PROTECTIVE SHIELD (TEMPORARY)	SQ. YDS.	1015
PROTECTIVE SHIELD (PERMANENT)	SQ. YDS.	234
FORMED CONCRETE REPAIR (DEPTH > 5")	SQ. FT.	970

(*) INCLUDES APPROACH SLAB HYDRO-SCARIFICATION QUANTITIES
(**) FOR APPROACH SLAB AND FOR INFORMATION ONLY

SCOPE OF WORK (NOT IN SEQUENCE):

- INSTALL TEMPORARY PROTECTIVE SHIELD SYSTEM
- INSTALL PERMANENT PROTECTIVE SHIELD SYSTEM
- REMOVE EXISTING 1 3/4" THICK BITUMINOUS CONCRETE OVERLAY FROM THE BRIDGE DECK AND ALSO FROM THE TWO APPROACH SLABS
- HYDRO-SCARIFY 1/2" OF CONCRETE DECK AND THE TWO APPROACH SLABS SURFACES
- PERFORM FORMED CONCRETE REPAIR ALONG THE EDGES OF THE SIDEWALK AS SHOWN
- PERFORM FULL DEPTH AND PARTIAL DEPTH PATCHING ON THE BRIDGE DECK AND THE TWO APPROACH SLABS AT AREAS SHOWN AND AS DIRECTED BY THE ENGINEER. INSTALL 2 1/4" OF LATEX CONCRETE OVERLAY ON THE BRIDGE DECK
- REMOVE THE NEOPRENE EXPANSION JOINT FILLERS ON THE TWO ABUTMENTS AND INSTALL SILICON JOINT SEALER WITH POLYMER CONCRETE NOSING
- REMOVE THE FILLERS IN THE TWO RELIEF JOINTS AT THE ENDS OF THE TWO APPROACH SLABS AND INSTALL SILICON JOINT SEALER WITH POLYMER CONCRETE NOSING.
- ON APPROACH SLABS INSTALL POLYMERIZED LEVELING BINDER SUPERPAVE, IL-4.75, N50, 3/4"
- ON APPROACH SLABS INSTALL SURFACE COURSE, SUPERPAVE, MIX. D, N-50, 1 1/2"
- REMOVE CONSTRUCTION DEBRIS AND TEMPORARY PROTECTIVE SHIELD SYSTEM AND RESTORE THE SITE TO ITS ORIGINAL CONDITION AS FAR AS PRACTICAL.

DESCRIPTION:

GROSS POINT ROAD BRIDGE OVER EDENS EXPRESSWAY (I-94) WAS BUILT IN 1954. IT HAS FOUR SPANS. ITS REINFORCED CONCRETE DECK RESTS OVER TWELVE 36WF10 STEEL BEAMS. IT HAS TWO ABUTMENTS AND THREE PIERS AND ALL OF THEM ARE SUPPORTED ON SPREAD FOOTINGS. IN 1982 THE BRIDGE WAS REHABILITATED. INFRARED THERMOGRAPHIC AND GROUND PENETRATING SURVEYS WERE CONDUCTED IN YEAR 1999 THAT IDENTIFIED AREAS THAT NEEDED PARTIAL DEPTH PATCHING, AND FULL DEPTH PATCHING. AND HAD DELAMINATION AND OTHER ANOMALIES.

DESIGN LOADING: HS 20-44

BRIDGE DATA:

DECK SLAB THICKNESS = ± 7"
THICKNESS OF EXISTING OVERLAY = ± 1 3/4"
BRIDGE LENGTH = 318.25'
OUT TO OUT WIDTH = 64.0'
SKEW ANGLE = 45°

TRAFFIC DATA:

FOR GROSS POINT BRIDGE
ADT = 7,800 (YEAR 2003)
PROJECTED ADT = 10,125 (YEAR 2021)
SPEED LIMIT = 35 MPH
FOR EDENS EXPRESSWAY (I-94)
ADT = 174,600 (YEAR 2003)
PROJECTED ADT = 236,115 (YEAR 2021)
SPEED LIMIT = 55 MPH

GENERAL NOTES:

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING STRUCTURE HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE FOR THE WORK.

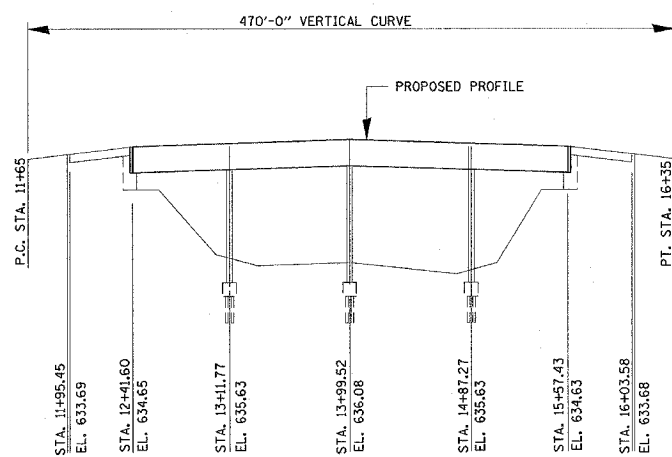
PARTIAL DEPTH PATCHING SHALL BE INCIDENTAL TO HYDRO-SCARIFYING.

THE SIZES AND LOCATIONS OF PATCHING AREAS AS SHOWN ON THE DRAWINGS ARE APPROXIMATE ONLY AND ARE SUBJECT TO THE ENGINEER'S FIELD VERIFICATION. THE ENGINEER SHALL SHOW ACTUAL LOCATIONS OF DECK REPAIRS ON AS-BUILT PLANS.

HYDRO-SCARIFICATION OF APPROACH SLAB SHALL BE PAID AS BRIDGE DECK HYDRO-SCARIFICATION.

BRIDGE DECK GROOVING SHALL COMFORM TO ARTICLE 503.17 (4)(b) OF STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

DO NOT SCALE DIMENSIONS.



PROFILE ALONG C OF ROAD AND BRIDGE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
GROSS POINT ROAD OVER I-94
PLAN, ELEVATION AND
GENERAL INFORMATION
S.N. 016-0725

SCALE: VERT. 1" = 20'
HORIZ. 1" = 20'
DATE: MAY, 2005

DRAWN BY: RFL/MVT
CHECKED BY: RSS

Rev. Sheet 6-5-06