

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2360	04-00058-08-FP	WOODFORD	8	1

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	GENERAL NOTES & TYPICAL SECTIONS
3	SCHEDULE OF QUANTITIES
4-7	PLAN SHEETS
8	DETAIL SHEETS

IDOT HIGHWAY STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
630001-06	STEEL PLATE BEAM GUARDRAIL
635006-02	REFLECTOR AND TERMINAL MARKER PLACEMENT
701101-01	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-01	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
702001-06	TRAFFIC CONTROL DEVICES
BLR 24-1	MAILBOX TURNOUT FOR LOCAL ROADS

SUMMARY OF QUANTITIES

ITEM #	DESCRIPTION	UNIT	QUANTITY
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	580
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	475
40600300	AGGREGATE (PRIME COAT)	TON	10
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	50 YD	170
48101200	AGGREGATE SHOULDERS, TYPE B	TON	6,145
63301000	REMOVE AND RE-ERECT STEEL PLATE BEAM GUARD RAIL	FOOT	8,100
63301990	REMOVE AND RE-ERECT TRAFFIC BARRIER TERMINAL, TY 1	EACH	46
67100100	MOBILIZATION	L SUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1,960
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	250
X4066514	POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50	TON	3,470
X4066765	LEVELING BINDER (MACHINE METHOD), SUPERPAVE N50	TON	5,550
XX004949	INCIDENTAL BITUMINOUS SURFACING, SUPERPAVE	TON	530
X4066514	BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-12.0, N50	TON	4,160

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

PLANS FOR PROPOSED LOCAL AGENCY IMPROVEMENT FEDERAL-AID SECONDARY PROJECT

STU PROGRAM

F.A.S. ROUTE 2360 (SANTA FE TRAIL)

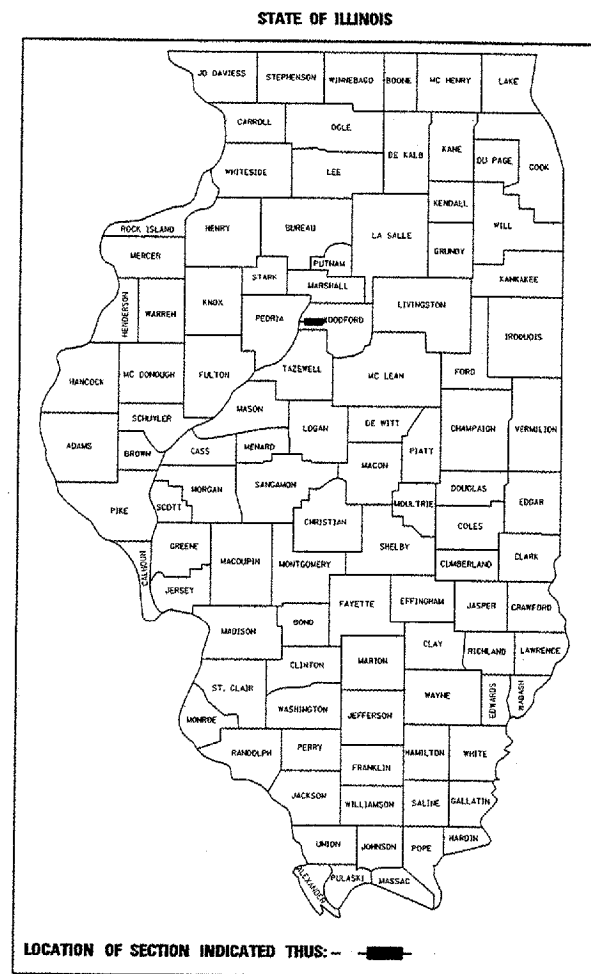
COUNTY HIGHWAY #25

PROJECT NO. 5093(124)

JOB NO. C-94-026-06

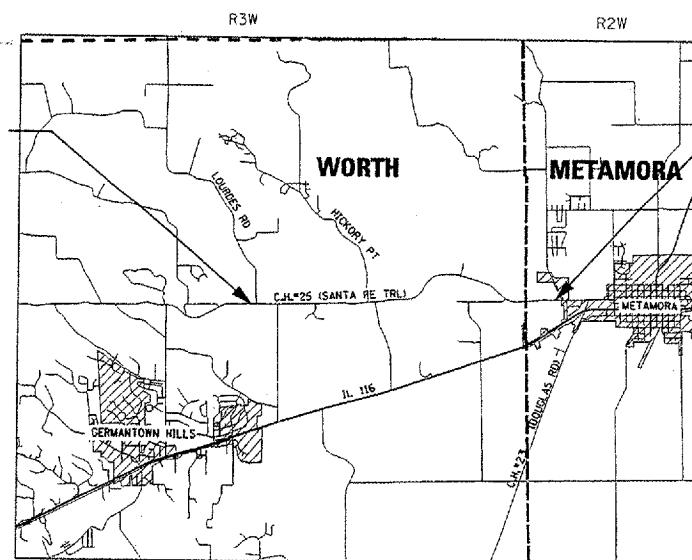
SECTION 04-00058-08-FP

WOODFORD COUNTY

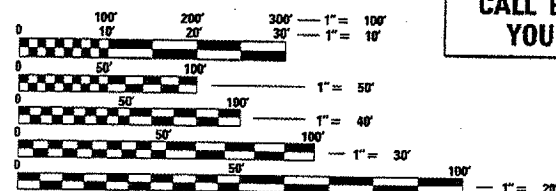


IMPROVEMENT BEGINS STA 233+00

IMPROVEMENT ENDS STA 428+40



LAYOUT MAP



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 89389

DESIGN INFORMATION:

LOCAL ROADS FUNCTIONAL CLASS: MAJOR COLLECTOR
2016 ADT = 1770 (7% TRUCKS)
DESIGN SPEED: 40 MPH
DESIGN GUIDELINES: URBAN
VARIANCES GRANTED: NONE
COMMITMENTS: NONE

UTILITIES

(TO BE DETERMINED)

PLANS DEVELOPED IN ACCORDANCE WITH THE IDOT LOCAL ROADS MANUAL (APRIL 2005)

IMPROVEMENT CONSISTS OF THE CONSTRUCTION OF BITUMINOUS CONCRETE BINDER AND SURFACE COURSE OVER AN EXISTING OIL & CHIP ROADWAY, INCLUDING PROPOSED AGGREGATE SHOULDERS AND REMOVAL AND RE-ERECTION OF GUARDRAIL AND TERMINALS.

GROSS AND NET LENGTH OF PROJECT = 19,500 FT. (3.7 MI)

APPROVED 4/18 2006
Dennis D. Bachman
COUNTY ENGINEER

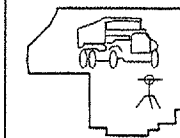
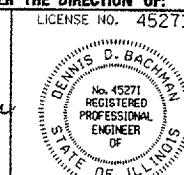
PASSED 4-20 2006
David Laballe
DISTRICT FOUR ENGINEER OF LOCAL ROADS & STREETS

Releasing For Bid Based on Limited Review
Amey W 2006
DEPUTY DIRECTOR OF HIGHWAYS, REGION THREE ENGINEER
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

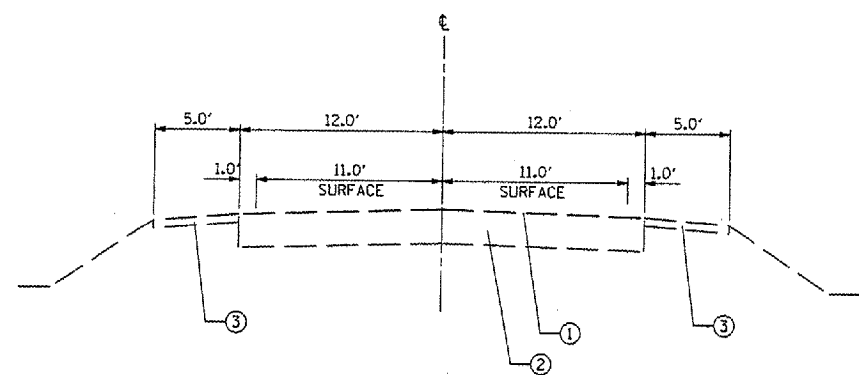
THESE PLANS HAVE BEEN PREPARED BY OR UNDER THE DIRECTION OF:

DATE: 4/18/06
SIGNED: *Dennis D. Bachman*
LICENSE EXPIRES: NOVEMBER 30, 2007

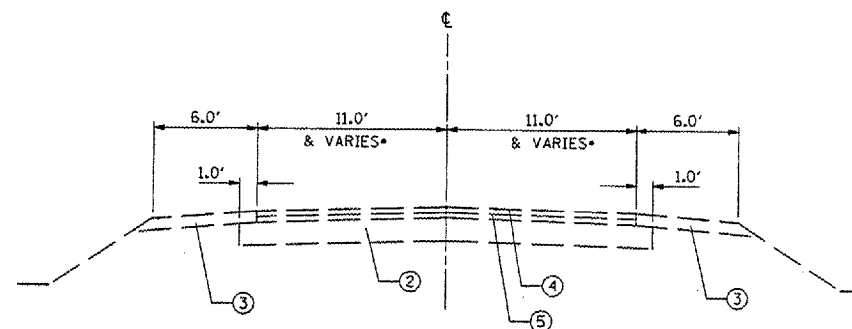


WOODFORD COUNTY HIGHWAY DEPARTMENT
DENNIS BACHMAN, P.E., COUNTY ENGINEER
301 S. Main St., P.O. Box 467
Roanoke, IL 61561-0467
TEL: 309-923-2891
FAX: 309-923-7928

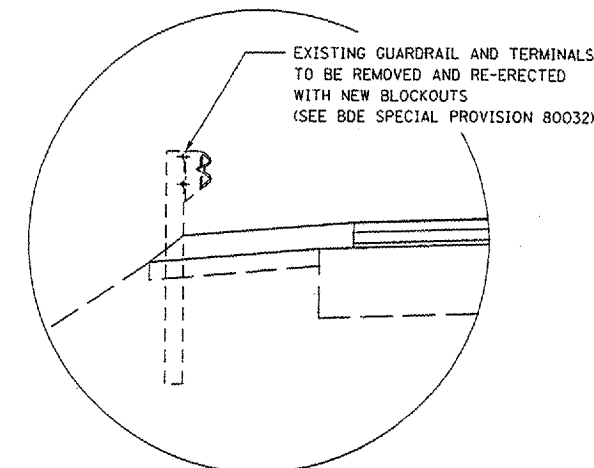
CONTRACT NO. 89389				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2360	04-00058-08-FP	WOODFORD	8	2
STA. TO STA.				
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				



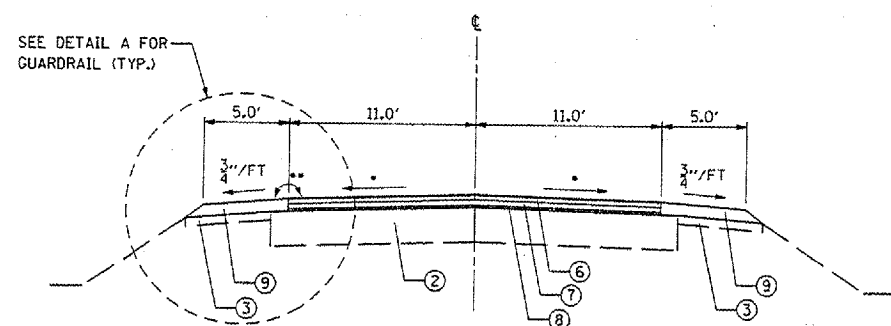
EXISTING TYPICAL SECTION
SANTA FE TRAIL (C.H. #25)
STA 235+00 TO STA 423+50



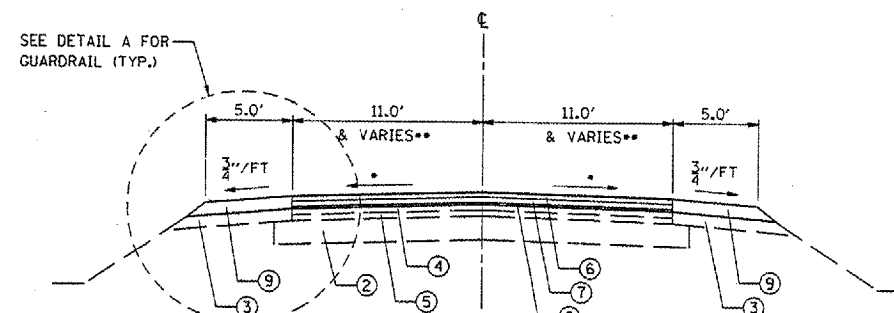
EXISTING TYPICAL SECTION
SANTA FE TRAIL (C.H. #25)
STA 423+50 TO STA 428+40



DETAIL A



PROPOSED TYPICAL SECTION
SANTA FE TRAIL (C.H. #25)
STA 235+00 TO STA 423+50

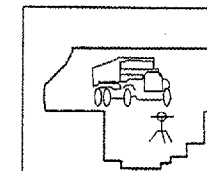


PROPOSED TYPICAL SECTION
SANTA FE TRAIL (C.H. #25)
STA 423+50 TO STA 428+40

- ① EXISTING BITUMINIOUS A-3 SURFACE
- ② EXISTING AGGREGATE BASE COURSE- 12"
- ③ EXISTING AGGREGATE SHOULDERS
- ④ EXISTING BITUMINIOUS SURFACE COURSE- 2"
- ⑤ EXISTING BITUMINIOUS BINDER COURSE- 2"
- ⑥ POLYMERIZED BITUMINIOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX D, N50- 1 1/4"
- ⑦ BITUMINIOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-12.0, N50- 1 1/2"
- ⑧ LEVELING BINDER (MACHINE METHOD), SUPERPAVE, N50- 2"
- ⑨ AGGREGATE SHOULDERS, TY B - 4 1/2"

STRUCTURAL DESIGN TRAFFIC: YEAR 2021	
PV = 1646 SU = 106 MU = 18 AADT = 1770	
ROAD/STREET CLASSIFICATION: CLASS III	
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE: P = 93% S = 6% M = 1%	
TRAFFIC FACTOR:	ACTUAL TF = 0.14 MIN TF = N/A
PG GRADE:	BINDER = PG 58-28 SURFACE = SBS PG 64-28
SUBGRADE SUPPORT RATING: SSR = N/A	

MIX DESIGN	LVL BINDER	BINDER	SURFACE	INCIDENTAL
PG:	PG 58-28	PG 64-28	SBS PG 64-28	SBS PG 64-28
RAP (%)max	10%	10%	0%	0%
DESIGN AIR VOIDS	4%	4%	4%	4%
GRADATION MIXTURE	IL 19.0	IL 12.0	IL 9.5	IL 9.5
FRICTION AGGREGATE	B	B	C	C
N DESIGN	N 50	N 50	N 50	N 50



**WOODFORD COUNTY
HIGHWAY DEPARTMENT**
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FAX: 309-923-7928

F.A.S. RTE. 2360
SECTION 04-00058-08-FP
SANTA FE TRAIL (C.H. #25)
PROPOSED
TYPICAL SECTION

SCALE: 1"=5'
DATE: 3/1/06

DRAWN BY: JJH
CHECKED BY: DDS

PLOT DATE: * DATE *
FILE NAME: * FILE *
PLOT SCALE: * SCALE *
USER NAME: * USER *

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2360	04-00058-08-FP	WOODFORD	8	3
STA.	TO STA.			
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				

#	ENTRANCE*	TYPE	STATION	WIDTH (FT)	SURFACE	40200800 AGG SURFACE COURSE, TY B		40800040 INCIDENTAL BIT SURFACING	
						TOTAL (TONS)	TOTAL (TONS)		
1	FIELD ENTRANCE		237+74.35 LT	20	AGG	55.6			
2	FIELD ENTRANCE		234+45.86 LT	18	AGG	51.1			
3	PRIVATE ENTRANCE		245+61.88 LT	18	AGG	51.1			
4	FIELD ENTRANCE		247+20.61 RT	19	AGG	53.3			
5	PRIVATE ENTRANCE		248+16.89 LT	19	AGG	53.3			
6	FIELD ENTRANCE		248+76.17 LT	13	AGG	40			
7	FIELD ENTRANCE		255+00.36 LT	17	AGG	48.9			
8	PRIVATE ENTRANCE		260+38.03 RT	20	ASPH	55.6	14.8		
9	FIELD ENTRANCE		263+06.28 RT	16	AGG	46.7			
10	FIELD ENTRANCE		264+76.89 LT	18	AGG	51.1			
11	PRIVATE ENTRANCE		264+87.61 RT	26	AGG	68.9			
12	PRIVATE ENTRANCE		266+17.69 LT	15	ASPH	44.4	11.8		
13	PRIVATE ENTRANCE		266+14.40 RT	24	ASPH	64.4	17.1		
14	PRIVATE ENTRANCE		267+26.43 RT	23	ASPH	62.2	16.5		
15	PRIVATE ENTRANCE		267+91.73 RT	23	ASPH	62.2	16.5		
16	PRIVATE ENTRANCE		267+93.24 LT	20	CONC	55.6			
17	N/A								
18	PRIVATE ENTRANCE		271+86.41 LT	18	AGG	51.1			
19	FIELD ENTRANCE		274+34.88 RT	14	AGG	42.2			
20	PRIVATE ENTRANCE		275+44.17 LT	15	ASPH	44.4	11.8		
21	PRIVATE ENTRANCE		276+48.50 RT	16	ASPH	46.7	12.4		
22	FIELD ENTRANCE		277+72.03 RT	22	AGG	60			
23	FIELD ENTRANCE		284+04.37 LT	18	ASPH	51.1	13.6		
24	FIELD ENTRANCE		284+07.93 RT	30	ASPH	77.8	20.7		
25	PRIVATE ENTRANCE		289+33.47 LT	19	ASPH	53.3	14.2		
26	PRIVATE ENTRANCE		295+63.28 LT	37	ASPH	93.3	24.8		
27	COMMERCIAL ENTRANCE		296+34.63 RT	24	AGG	64.4			
28	FIELD ENTRANCE		305+20.00 RT	15	AGG	44.4			
29	FIELD ENTRANCE		307+78.74 RT	18	AGG	51.1			
30	PRIVATE ENTRANCE		310+89.05 RT	20	AGG	55.6			
31	PRIVATE ENTRANCE		324+43.30 LT	30	ASPH	77.8	20.7		
32	PRIVATE ENTRANCE		326+41.35 RT	42	ASPH	104.4	27.8		
33	FIELD ENTRANCE		331+65.89 RT	20	AGG	55.6			
34	FIELD ENTRANCE		332+07.00 LT	20	AGG	55.6			
35	FIELD ENTRANCE		341+50.00 RT	18	AGG	51.1			
36	PRIVATE ENTRANCE		341+73.55 LT	26	ASPH	68.9	18.3		
37	FIELD ENTRANCE		345+50.00 RT	15	AGG	44.4			
38	FIELD ENTRANCE		345+50.00 LT	15	AGG	44.4			
39	FIELD ENTRANCE		360+38.34 LT	24	AGG	64.4			
40	PRIVATE ENTRANCE		361+50.00 RT	18	ASPH	51.1	13.6		
41	PRIVATE ENTRANCE		362+38.76 LT	16	AGG	46.7			
42	PRIVATE ENTRANCE		362+51.29 RT	15	AGG	44.4			
43	PRIVATE ENTRANCE		364+43.60 RT	24	AGG	64.4			
44	PRIVATE ENTRANCE		367+43.06 LT	24	ASPH	64.4	17.1		
45	FIELD ENTRANCE		369+59.72 RT	20	AGG	55.6			
46	FIELD ENTRANCE		375+87.57 RT	24	ASPH	64.4	17.1		
47	PRIVATE ENTRANCE		381+03.82 LT	36	ASPH	91.1	24.2		
48	PRIVATE ENTRANCE		381+92.05 RT	48	ASPH	117.8	31.3		
49	PRIVATE ENTRANCE		383+45.06 LT	24	AGG	64.4			
50	PRIVATE ENTRANCE		386+65.48 RT	20	AGG	55.6			
51	PRIVATE ENTRANCE		388+27.35 LT	20	AGG	55.6			
52	PRIVATE ENTRANCE		389+28.39 RT	24	AGG	64.4			
53	PRIVATE ENTRANCE		389+64.28 LT	20	AGG	55.6			
54	FIELD ENTRANCE		397+31.25 LT	20	AGG	55.6			
55	FIELD ENTRANCE		398+00.00 LT	15	AGG	44.4			
56	PRIVATE ENTRANCE		400+25.82 RT	44	ASPH	108.9	29.0		
57	COMMERCIAL ENTRANCE		401+99.26 RT	24	AGG	64.4			
58	PRIVATE ENTRANCE		405+64.38 LT	40	ASPH	100	26.6		
59	FIELD ENTRANCE		406+11.84 RT	20	AGG	55.6			
60	FIELD ENTRANCE		411+50.00 LT	18	AGG	51.1			
61	PRIVATE ENTRANCE		420+33.20 LT	30	ASPH	77.8	20.7		
62	PRIVATE ENTRANCE		421+74.46 LT	34	ASPH	86.7	23.1		
63	FIELD ENTRANCE		422+09.49 RT	20	AGG	55.6			
64	FIELD ENTRANCE		424+28.75 LT	18	AGG	51.1			
65	FIELD ENTRANCE		428+81.57 RT	18	AGG	51.1			
MAILBOX TURNOUTS (PER STANDARD BLR 24-1) 22 @ 3.8 TONS EACH =								83.6	
Total =								527.3 TONS	
USE								530 TONS	

63301000		63301990	
REMOVE AND RE-ERECT STEEL PLATE	REMOVE AND RE-ERECT TRAFFIC BEAM GUARD RAIL	REMOVE AND RE-ERECT TRAFFIC BARR TERM, TY 1	
LENGTH (FT)	LENGTH (FT)	(EACH)	
250+44.00 - 253+81.50 RT	337.5	2	
251+91.00 - 255+03.50 LT	312.5	2	
261+33.00 - 264+33.00 LT	300	2	
292+20.00 - 295+20.00 RT	300	2	
282+97.00 - 285+59.50 LT	262.5	2	
311+70.00 - 316+70.00 LT	500	2	
312+68.00 - 316+18.00 RT	350	2	
319+25.00 - 323+75.00 LT	450	2	
320+74.00 - 323+74.00 RT	300	2	
332+75.00 - 337+37.50 RT	462.5	2	
348+25.00 - 353+75.00 LT	550	2	
349+25.00 - 354+25.00 RT	500	2	
370+67.00 - 373+67.00 RT	300	2	
370+67.00 - 374+17.00 LT	350	2	
375+00.00 - 377+75.00 RT	275	2	
375+25.00 - 377+75.00 LT	250	2	
393+50.00 - 396+50.00 LT	300	2	
402+20.00 - 404+70.00 RT	250	2	
402+20.00 - 404+70.00 LT	250	2	
407+20.00 - 410+20.00 RT	300	2	
407+70.00 - 410+70.00 LT	300	2	
415+20.00 - 419+70.00 LT	450	2	
415+20.00 - 419+70.00 RT	450	2	
TOTAL =	8,100	46	
USE	8,100	46	

48101200 AGGREGATE SHOULDERS, TYPE B			
STATION	TO STATION	LENGTH (FT)	TOTAL TONS
233+00.00	- 248+04.47	1504.5	244.2
248+04.47	- 248+98.55	110.0	17.8
248+98.55	- 271+03.20	2204.7	357.7
271+03.20	- 272+04.81	94.3	15.4
272+04.81	- 305+89.61	3384.8	549.4
305+89.61	- 307+90.72	278.4	45.1
307+90.72	- 394+86.06	8695.3	1411.2
394+86.06	- 395+77.06	107.0	17.4
395+77.06	- 428+40.00	3262.9	529.5
233+00.00	- 235+00.55	200.5	32.6
235+00.55	- 236+23.09	153.5	25.0
236+23.09	- 302+19.64	6596.6	1070.5
302+19.64	- 303+13.96	110.2	17.8
303+13.96	- 364+02.66	6088.7	988.1
364+02.66	- 365+09.37	133.3	21.7
365+09.37	- 428+40.00	6330.6	1027.5
(SUBTRACT QUANTITY FOR ENTRANCES)		-1419.0	-230.2
TOTAL =		6140.7	
USE		6,145	

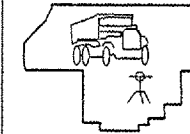
40600980 BITUMINOUS SURFACE REMOVAL - BUTT JOINT			
STATION	TO	STATION	AREA (SY)
423+50.00	TO	423+80.00	73
428+10.00	TO	428+40.00	95
TOTAL =			168
USE			170

STATION	TO STATION	LENGTH (FT)	X4066514	*4001715	X4066765
			POLY BITUMINOUS CONC SURF CSE. SUPER MIX C N50	BITUMINOUS CONC BINDER COURSE, SUPER 1L12.5 N50	LEVELING BINDER (MACHINE METHOD) SUPERPAVE N50
TONS	TONS	TONS			
233+00	- 239+00	600	120.1	144.2	192.2
239+00	- 248+00	900	153.8	184.6	246.1
248+00	- 258+00	1000	181.8	216.2	290.9
258+00	- 271+00	1300	222.7	267.3	356.4
271+00	- 281+00	1000	179.8	215.8	287.7
281+00	- 291+00	1000	171.3	205.5	274.0
291+00	- 302+00	1100	188.0	225.6	300.8
302+00	- 312+00	1000	225.5	270.6	360.8
312+00	- 322+00	1000	171.6	206.0	274.6
322+00	- 332+00	1000	170.9	205.0	273.4
332+00	- 342+00	1000	170.8	205.0	273.3
342+00	- 352+00	1000	171.3	205.5	274.0
352+00	- 364+00	1200	205.4	246.4	328.6
364+00	- 374+00	1000	184.8	221.8	295.7
374+00	- 384+00	1000	171.0	205.3	273.7
384+00	- 395+00	1100	188.4	226.1	301.5
395+00	- 405+00	1000	181.9	216.3	291.0
405+00	- 415+00	1000	171.3	205.6	274.1
415+00	- 423+00	800	136.6	164.0	218.6
423+00	- 428+40	540	97.6	117.1	156.1
TOTAL =			3464.8	4157.7	5543.7
USE			3,470	4,160	5,550

STATION	TO STATION	40300100	40600300
		BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)
GALLONS		TONS	
287+25.00	- 314+44.53	332	7
349+50.00	- 353+75.00	52	1
367+00.00	- 374+20.00	88	2
TOTAL =		472	9
USE		475	10

STATION	TO STATION	70300100
		SHORT-TERM PAVEMENT MARKING
SPACING		
233+00	- 428+40	19540 FT @ 40 FT = 1954.0
USE		1,960

PLT DATE * DATE *
FILE NAME * FILE *
PLOT SCALE * SCALE *
USER NAME * USER



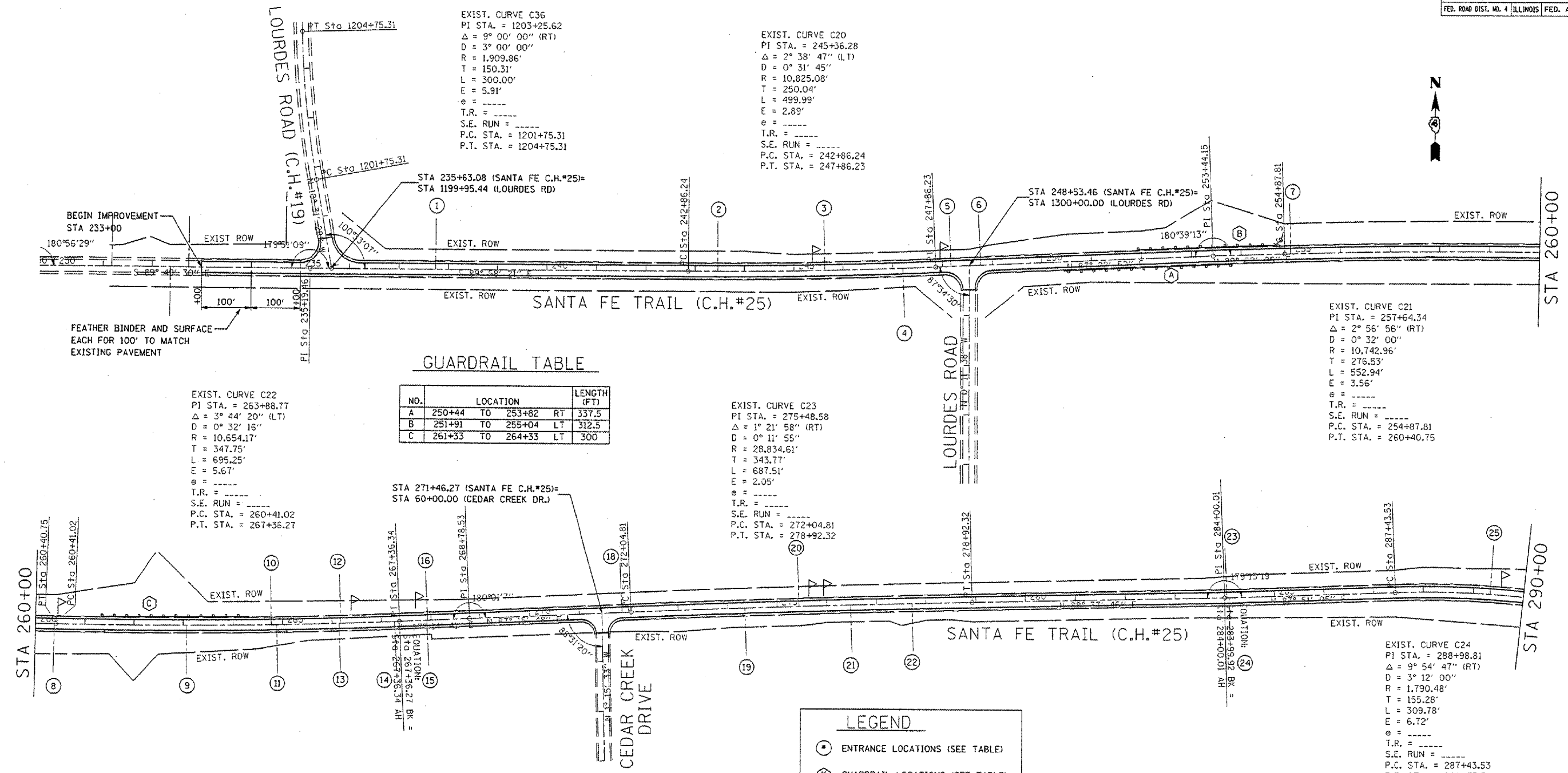
**WOODFORD COUNTY
HIGHWAY DEPARTMENT**
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301 S. Main St., P.O. Box 467
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FAX: 309-923-7928

F.A.S. RTE. 2360
SECTION 04-00058-08-FP
SANTA FE TRAIL (C.H.#25)
SCHEDULE OF QUANTITIES

SCALE: 1"=5'
DATE: 3/1/06

DRAWN BY: JJJ
CHECKED BY: DDB

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2360	04-00058-08-FP	WOODFORD	8	4
STA. 230+00.00		TO STA. 290+00.00		
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				



GUARDRAIL TABLE

NO.	LOCATION	LENGTH (FT)
A	250+44 TO 253+82 RT	337.5
B	251+91 TO 255+04 LT	312.5
C	261+33 TO 264+33 LT	300

ENTRANCE TABLES

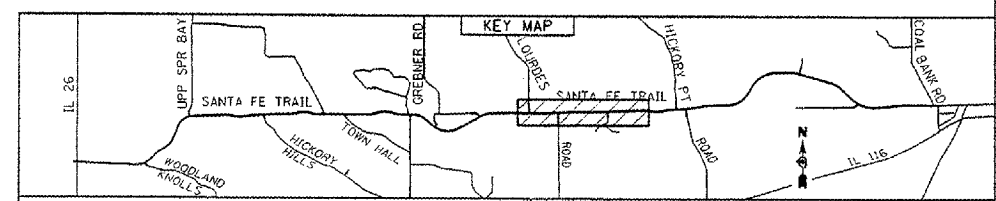
NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
1	FIELD ENTRANCE	237+74.35 LT	20	AGG
2	FIELD ENTRANCE	234+45.86 LT	18	AGG
3	PRIVATE ENTRANCE	245+61.88 LT	18	AGG
4	FIELD ENTRANCE	247+20.61 RT	19	AGG
5	PRIVATE ENTRANCE	248+16.89 LT	19	AGG
6	FIELD ENTRANCE	248+76.17 LT	13	AGG
7	FIELD ENTRANCE	255+00.36 LT	17	AGG
8	PRIVATE ENTRANCE	260+38.03 RT	20	ASPH
9	FIELD ENTRANCE	263+06.28 RT	16	AGG
10	FIELD ENTRANCE	264+76.89 LT	18	AGG

NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
11	PRIVATE ENTRANCE	264+87.61 RT	26	AGG
12	PRIVATE ENTRANCE	266+17.69 LT	15	ASPH
13	PRIVATE ENTRANCE	266+14.40 RT	24	ASPH
14	PRIVATE ENTRANCE	267+26.43 RT	23	ASPH
15	PRIVATE ENTRANCE	267+91.73 RT	23	ASPH
16	PRIVATE ENTRANCE	267+93.24 LT	20	CONC
17	N/A			
18	PRIVATE ENTRANCE	271+86.41 LT	18	AGG
19	FIELD ENTRANCE	274+34.88 RT	14	AGG
20	PRIVATE ENTRANCE	275+44.17 LT	15	ASPH

NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
21	PRIVATE ENTRANCE	276+48.50 RT	16	ASPH
22	FIELD ENTRANCE	277+72.03 RT	22	AGG
23	FIELD ENTRANCE	284+04.37 LT	18	ASPH
24	FIELD ENTRANCE	284+07.93 RT	30	ASPH
25	PRIVATE ENTRANCE	289+33.47 LT	19	ASPH

LEGEND

- ENTRANCE LOCATIONS (SEE TABLE)
- ⊗ GUARDRAIL LOCATIONS (SEE TABLE)
- ⏏ MAIL BOX TURNOUT



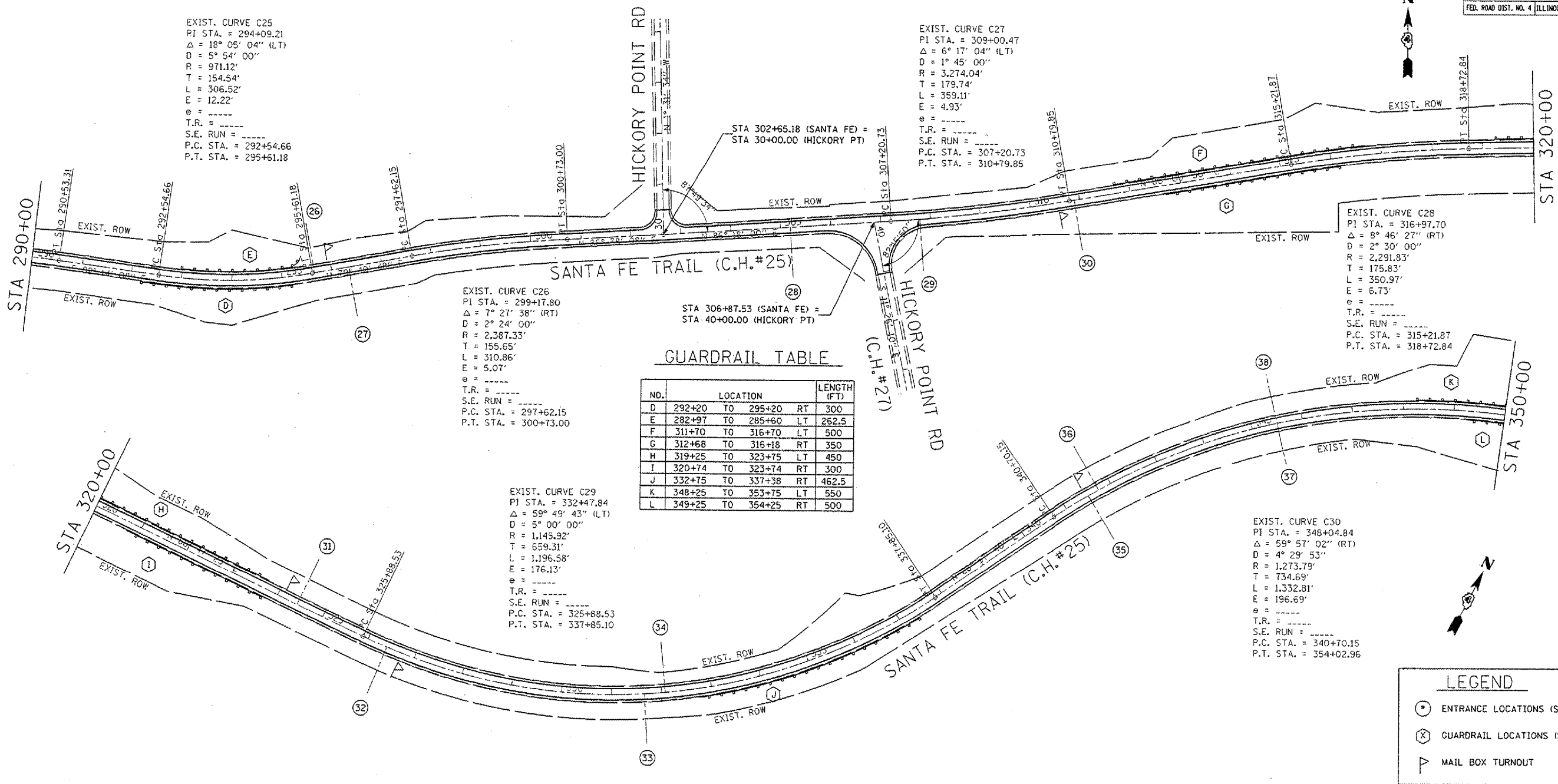
WOODFORD COUNTY HIGHWAY DEPARTMENT
 DENNIS BACHMAN, P.E., COUNTY ENGINEER
 301 S. Main St., P.O. Box 467
 Roodnoke, IL 61561-0467
 TEL: 309-923-2891
 FAX: 309-923-7928

F.A.S. RTE. 2360
 SECTION 04-00058-08-FP
 SANTA FE TRAIL (C.H.#25)
PLAN SHEET

SCALE: 1"=100'
 DATE 3/1/06
 DRAWN BY: JHH
 CHECKED BY: DDB

PLOT DATE = 04/01/06
 FILE NAME = WILEY
 USER NAME = T. MOSEY

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
04-00058-08-FP	WOODFORD	8	5
STA. 290+00.00 TO STA. 350+00.00			
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT			



EXIST. CURVE C25
 PI STA. = 294+09.21
 $\Delta = 18^\circ 05' 04''$ (LT)
 $D = 5^\circ 54' 00''$
 $R = 971.12'$
 $T = 154.54'$
 $L = 306.52'$
 $E = 12.22'$
 $\theta = \dots$
 T.R. = \dots
 S.E. RUN = \dots
 P.C. STA. = 292+54.66
 P.T. STA. = 295+61.18

EXIST. CURVE C27
 PI STA. = 309+00.47
 $\Delta = 6^\circ 17' 04''$ (LT)
 $D = 1^\circ 45' 00''$
 $R = 3,274.04'$
 $T = 179.74'$
 $L = 359.11'$
 $E = 4.93'$
 $\theta = \dots$
 T.R. = \dots
 S.E. RUN = \dots
 P.C. STA. = 307+20.73
 P.T. STA. = 310+79.85

EXIST. CURVE C26
 PI STA. = 299+17.80
 $\Delta = 7^\circ 27' 38''$ (RT)
 $D = 2^\circ 24' 00''$
 $R = 2,387.33'$
 $T = 155.65'$
 $L = 310.86'$
 $E = 5.07'$
 $\theta = \dots$
 T.R. = \dots
 S.E. RUN = \dots
 P.C. STA. = 297+62.15
 P.T. STA. = 300+73.00

EXIST. CURVE C28
 PI STA. = 316+97.70
 $\Delta = 8^\circ 46' 27''$ (RT)
 $D = 2^\circ 30' 00''$
 $R = 2,291.83'$
 $T = 175.83'$
 $L = 350.97'$
 $E = 6.73'$
 $\theta = \dots$
 T.R. = \dots
 S.E. RUN = \dots
 P.C. STA. = 315+21.87
 P.T. STA. = 318+72.84

EXIST. CURVE C29
 PI STA. = 332+47.84
 $\Delta = 59^\circ 49' 43''$ (LT)
 $D = 5^\circ 00' 00''$
 $R = 1,145.92'$
 $T = 659.31'$
 $L = 1,196.58'$
 $E = 176.13'$
 $\theta = \dots$
 T.R. = \dots
 S.E. RUN = \dots
 P.C. STA. = 325+88.53
 P.T. STA. = 337+85.10

EXIST. CURVE C30
 PI STA. = 348+04.84
 $\Delta = 59^\circ 57' 02''$ (RT)
 $D = 4^\circ 29' 53''$
 $R = 1,273.79'$
 $T = 734.69'$
 $L = 1,332.81'$
 $E = 196.69'$
 $\theta = \dots$
 T.R. = \dots
 S.E. RUN = \dots
 P.C. STA. = 340+70.15
 P.T. STA. = 354+02.96

GUARDRAIL TABLE

NO.	LOCATION	LENGTH (FT)
D	292+20 TO 295+20 RT	300
E	282+97 TO 285+60 LT	262.5
F	311+70 TO 316+70 LT	500
G	312+68 TO 316+18 RT	350
H	319+25 TO 323+75 LT	450
I	320+74 TO 323+74 RT	300
J	332+75 TO 337+38 RT	462.5
K	348+25 TO 353+75 LT	550
L	349+25 TO 354+25 RT	500

ENTRANCE TABLES

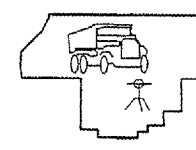
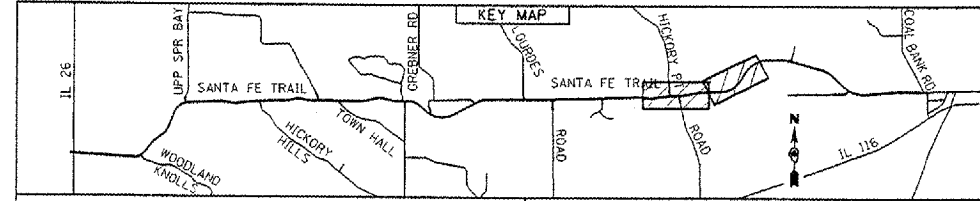
NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
26	PRIVATE ENTRANCE	295+63.28 LT	37	ASPH
27	COMMERCIAL ENTRANCE	296+34.63 RT	24	AGG
28	FIELD ENTRANCE	305+20.00 RT	15	AGG
29	FIELD ENTRANCE	307+78.74 RT	18	AGG
30	PRIVATE ENTRANCE	310+89.05 RT	20	AGG
31	PRIVATE ENTRANCE	324+43.30 LT	30	ASPH
32	PRIVATE ENTRANCE	326+41.35 RT	42	ASPH
33	FIELD ENTRANCE	331+65.89 RT	20	AGG
34	FIELD ENTRANCE	332+07.00 LT	20	AGG
35	FIELD ENTRANCE	341+50.00 RT	18	AGG

NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
36	PRIVATE ENTRANCE	341+73.55 LT	26	ASPH
37	FIELD ENTRANCE	345+50.00 RT	15	AGG
38	FIELD ENTRANCE	345+50.00 LT	15	AGG

NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE

LEGEND

- ⊙ ENTRANCE LOCATIONS (SEE TABLE)
- ⊗ GUARDRAIL LOCATIONS (SEE TABLE)
- ⚡ MAIL BOX TURNOUT



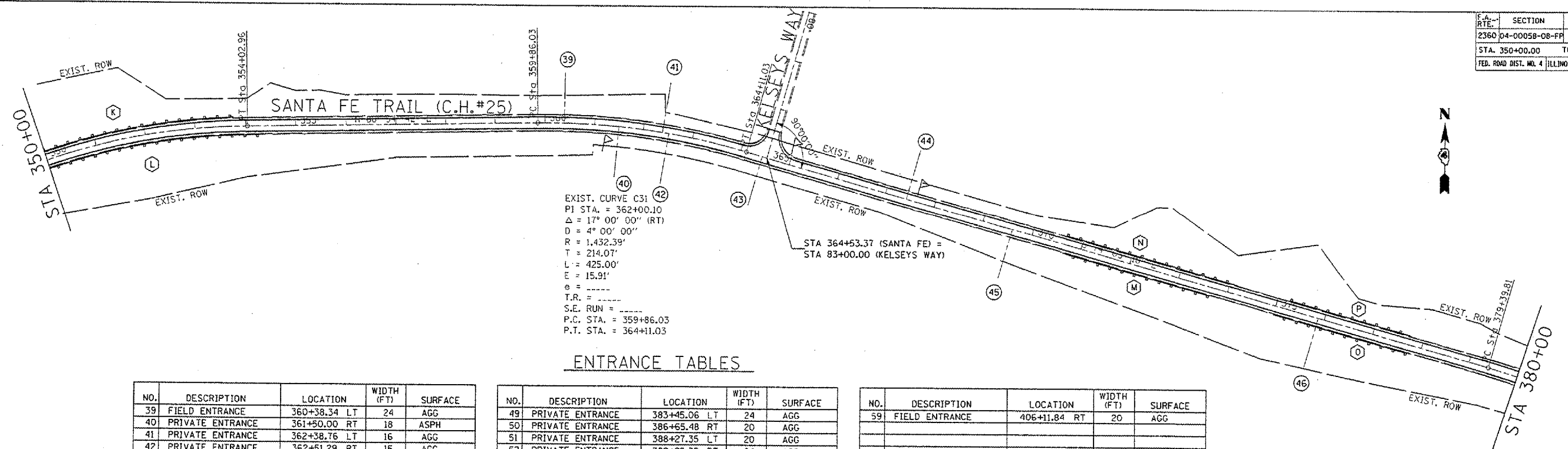
**WOODFORD COUNTY
 HIGHWAY DEPARTMENT**
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F.A.S. RTE. 2360
 SECTION 04-00058-08-FP
 SANTA FE TRAIL (C.H.#25)
PLAN SHEET

SCALE: 1"=100'
 DATE: 3/1/06
 DRAWN BY: JJH
 CHECKED BY: DDB

PLT: GLE
 FILE: MFLS
 USER: MFLS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2360	04-00058-08-FP	WOODFORD	8	6
STA. 350+00.00		TO STA. 410+00.00		
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				

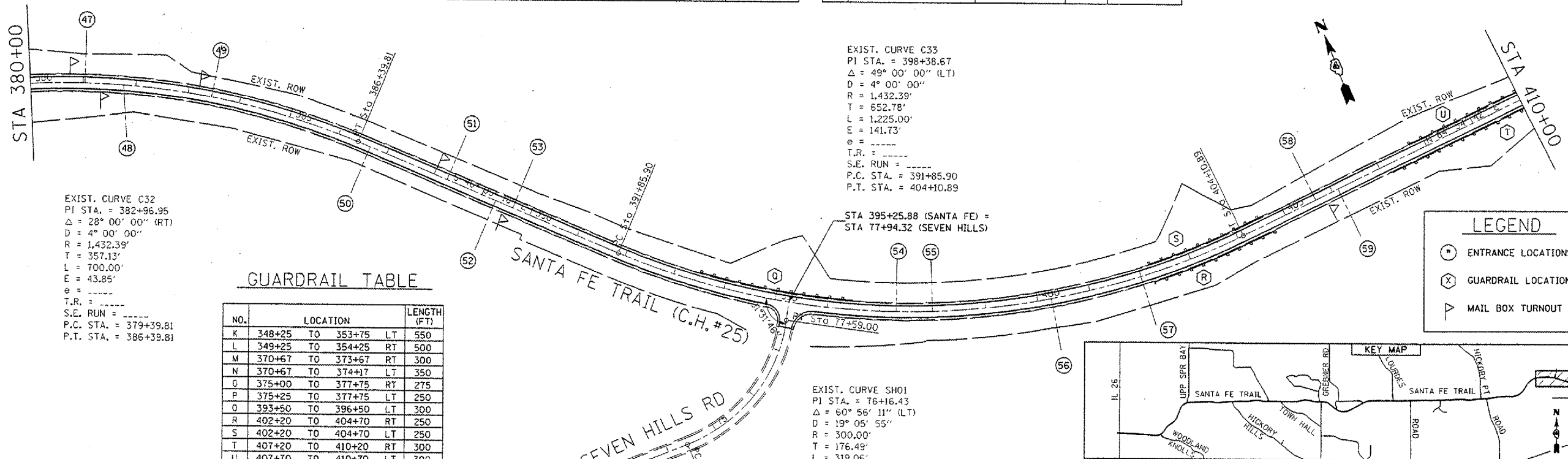


ENTRANCE TABLES

NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
39	FIELD ENTRANCE	360+38.34 LT	24	AGG
40	PRIVATE ENTRANCE	361+60.00 RT	18	ASPH
41	PRIVATE ENTRANCE	362+38.76 LT	16	AGG
42	PRIVATE ENTRANCE	362+51.29 RT	15	AGG
43	PRIVATE ENTRANCE	364+43.60 RT	24	AGG
44	PRIVATE ENTRANCE	367+43.06 LT	24	ASPH
45	FIELD ENTRANCE	369+59.72 RT	20	AGG
46	FIELD ENTRANCE	375+87.57 RT	24	ASPH
47	PRIVATE ENTRANCE	381+03.82 LT	36	ASPH
48	PRIVATE ENTRANCE	381+92.05 RT	48	ASPH

NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
49	PRIVATE ENTRANCE	383+45.06 LT	24	AGG
50	PRIVATE ENTRANCE	386+65.48 RT	20	AGG
51	PRIVATE ENTRANCE	388+27.35 LT	20	AGG
52	PRIVATE ENTRANCE	389+28.39 RT	24	AGG
53	PRIVATE ENTRANCE	389+64.28 LT	20	AGG
54	FIELD ENTRANCE	397+31.25 LT	20	AGG
55	FIELD ENTRANCE	398+00.00 LT	15	AGG
56	PRIVATE ENTRANCE	400+25.82 RT	44	ASPH
57	COMMERCIAL ENTRANCE	401+99.26 RT	24	AGG
58	PRIVATE ENTRANCE	405+64.38 LT	40	ASPH

NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
59	FIELD ENTRANCE	406+11.84 RT	20	AGG



EXIST. CURVE C32
 PI STA. = 382+96.95
 $\Delta = 28^\circ 00' 00''$ (RT)
 $D = 4^\circ 00' 00''$
 $R = 1,432.39'$
 $T = 357.13'$
 $L = 700.00'$
 $E = 43.85'$
 $\theta = \dots$
 $T.R. = \dots$
 $S.E. RUN = \dots$
 $P.C. STA. = 379+39.81$
 $P.T. STA. = 386+39.81$

GUARDRAIL TABLE

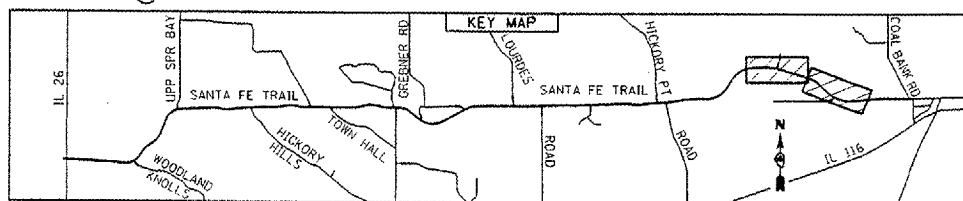
NO.	LOCATION	LENGTH (FT)
K	348+25 TO 353+75 LT	550
L	349+25 TO 354+25 RT	500
M	370+67 TO 373+67 RT	300
N	370+67 TO 374+17 LT	350
O	375+00 TO 377+75 RT	275
P	375+25 TO 377+75 LT	250
Q	393+50 TO 396+50 LT	300
R	402+20 TO 404+70 RT	250
S	402+20 TO 404+70 LT	250
T	407+20 TO 410+20 RT	300
U	407+70 TO 410+70 LT	300

EXIST. CURVE C33
 PI STA. = 398+38.67
 $\Delta = 49^\circ 00' 00''$ (LT)
 $D = 4^\circ 00' 00''$
 $R = 1,432.39'$
 $T = 652.78'$
 $L = 1,225.00'$
 $E = 141.73'$
 $\theta = \dots$
 $T.R. = \dots$
 $S.E. RUN = \dots$
 $P.C. STA. = 391+85.90$
 $P.T. STA. = 404+10.89$

EXIST. CURVE SH01
 PI STA. = 76+16.43
 $\Delta = 60^\circ 56' 11''$ (LT)
 $D = 19^\circ 05' 55''$
 $R = 300.00'$
 $T = 176.49'$
 $L = 319.06'$
 $E = 48.06'$
 $\theta = \dots$
 $T.R. = \dots$
 $S.E. RUN = \dots$
 $P.C. STA. = 74+39.94$
 $P.T. STA. = 77+59.00$

LEGEND

- ⊙ ENTRANCE LOCATIONS (SEE TABLE)
- ⊗ GUARDRAIL LOCATIONS (SEE TABLE)
- ⏏ MAIL BOX TURNOUT



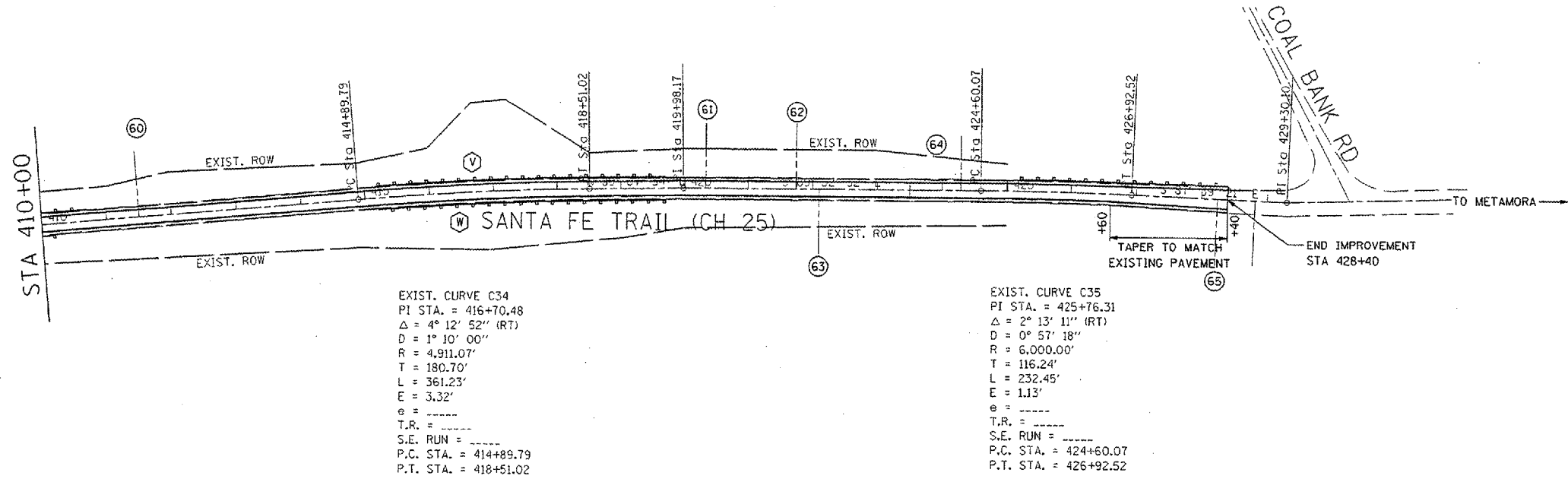
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F.A.S. RTE. 2360
 SECTION 04-00058-08-FP
 SANTA FE TRAIL (C.H.#25)
PLAN SHEET

SCALE: 1"=100'
 DATE 3/1/06
 DRAWN BY: JHH
 CHECKED BY: DDB

PLT DATE: 3/1/06
 DATE: 3/1/06
 USER: JHH
 USER: JHH

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2360	04-00058-08-FP	WOODFORD	8	7
STA. 410+00.00		TO STA. 428+40.00		
FED. ROAD DIST. NO. 4 ILLINOIS FED. AID PROJECT				



EXIST. CURVE C34
 PI STA. = 416+70.48
 $\Delta = 4^\circ 12' 52''$ (RT)
 $D = 1^\circ 10' 00''$
 $R = 4,911.07'$
 $T = 180.70'$
 $L = 361.23'$
 $E = 3.32'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 414+89.79$
 $P.T. STA. = 418+51.02$

EXIST. CURVE C35
 PI STA. = 425+76.31
 $\Delta = 2^\circ 13' 11''$ (RT)
 $D = 0^\circ 57' 18''$
 $R = 6,000.00'$
 $T = 116.24'$
 $L = 232.45'$
 $E = 1.13'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 424+60.07$
 $P.T. STA. = 426+92.52$

ENTRANCE TABLES

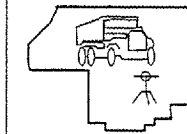
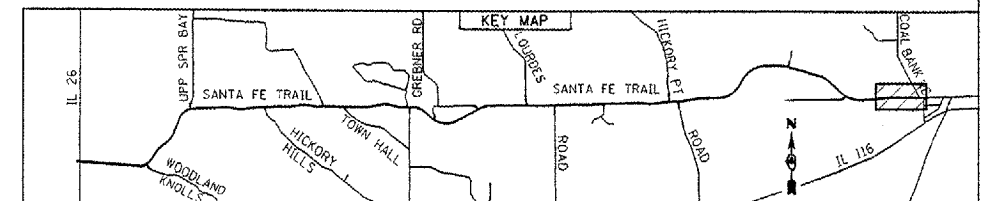
NO.	DESCRIPTION	LOCATION	WIDTH (FT)	SURFACE
60	FIELD ENTRANCE	411+50.00 LT	18	AGG
61	PRIVATE ENTRANCE	420+33.20 LT	30	ASPH
62	PRIVATE ENTRANCE	421+74.46 LT	34	ASPH
63	FIELD ENTRANCE	422+09.49 RT	20	AGG
64	FIELD ENTRANCE	424+28.75 LT	18	AGG
65	FIELD ENTRANCE	428+81.57 RT	18	AGG

GUARDRAIL TABLE

NO.	LOCATION	LENGTH (FT)
V	415+20 TO 419+70 LT	450
W	415+20 TO 419+70 RT	450

LEGEND

- ⊙ ENTRANCE LOCATIONS (SEE TABLE)
- ⊗ GUARDRAIL LOCATIONS (SEE TABLE)
- ⏏ MAIL BOX TURNOUT



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F.A.S. RTE. 2360
 SECTION 04-00058-08-FP
 SANTA FE TRAIL (C.H.#25)
 PLAN SHEET

SCALE: 1"=100'
 DATE 3/1/06

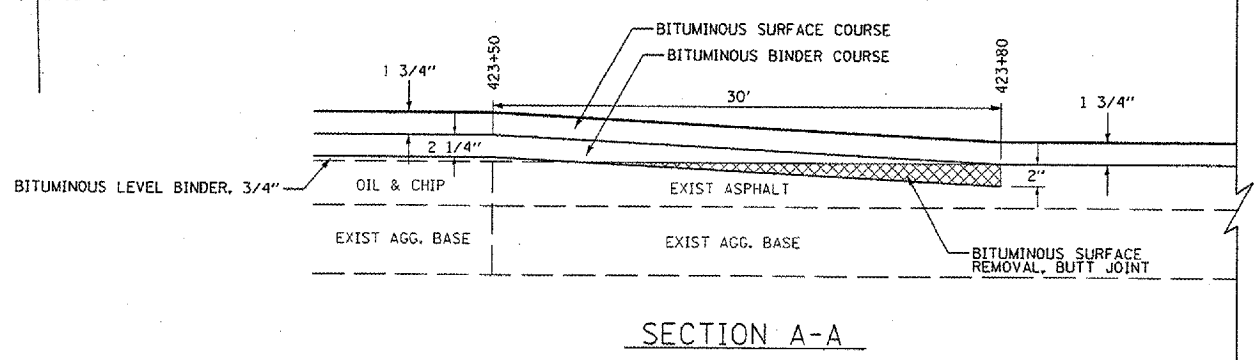
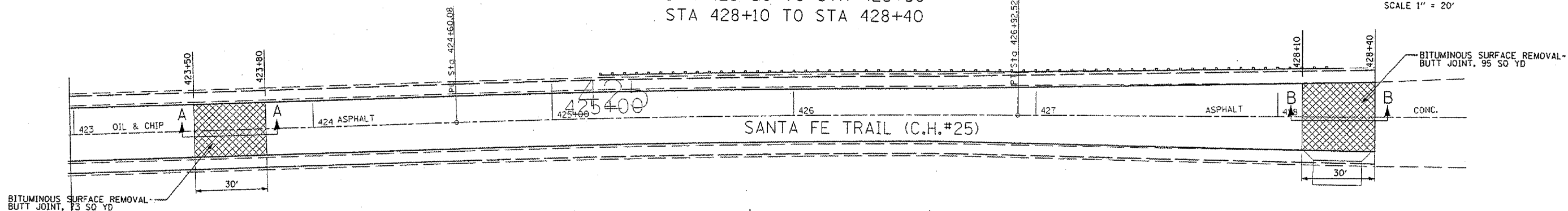
DRAWN BY: JHM
 CHECKED BY: DDB

CONTRACT NO. 89389				
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2360	04-00058-08-FP	WOODFORD	8	8
STA.		TO STA.		
FED. ROAD DIST. NO. 4		ILLINOIS FED. AID PROJECT		

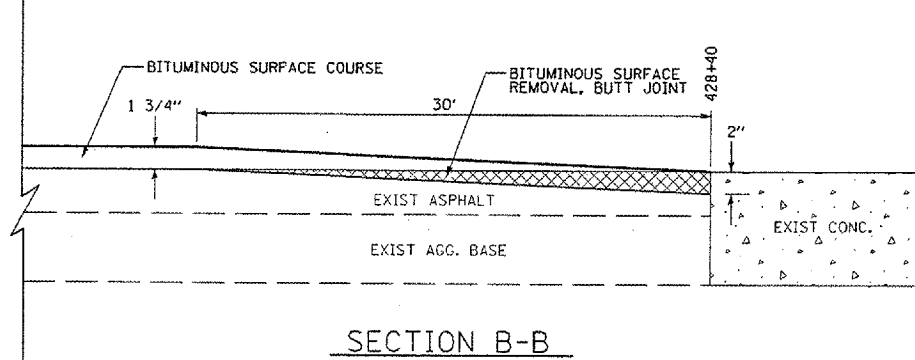
BUTT JOINT DETAILS

STA 423+50 TO STA 423+80
STA 428+10 TO STA 428+40

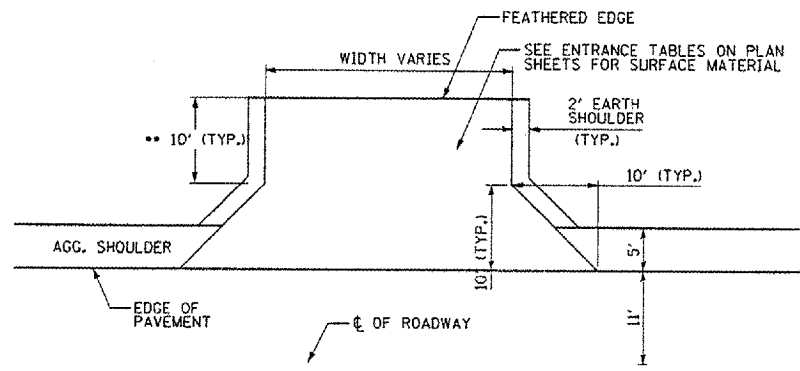
N
SCALE 1" = 20'



SECTION A-A



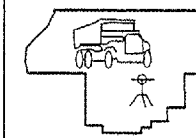
SECTION B-B



ENTRANCE DETAILS

** THICKNESS OF SURFACE MATERIAL TO BE FEATHERED FOR LAST 10' TO MATCH EXISTING SURFACE

PLOT DATE * DATES
FILE NAME * #FILES
PLOT SCALE * RESCALE
USER NAME * NUMBER



**WOODFORD COUNTY
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F.A.S. RTE. 2360
SECTION 04-00058-08-FP
SANTA FE TRAIL (C.H.#25)
MISC DETAILS

SCALE: VARIES
DATE: 3/1/06
DRAWN BY: JHH
CHECKED BY: DDS