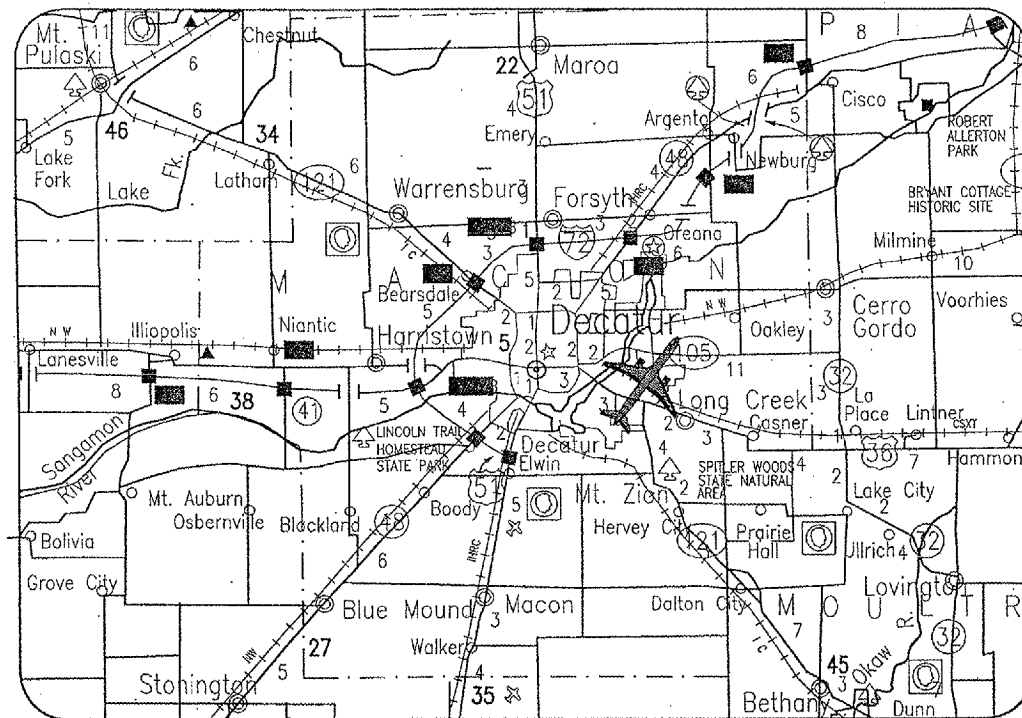


# CONSTRUCTION PLANS FOR DECATUR AIRPORT DECATUR, MACON COUNTY, ILLINOIS TAXIWAY "C" RECONSTRUCTION

### SCOPE OF WORK

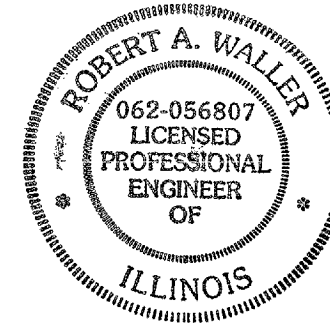
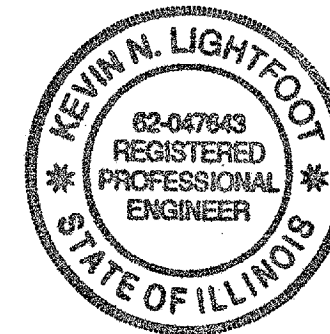
BASE BID: THE PROPOSED IMPROVEMENTS INCLUDE THE RECONSTRUCTION AND WIDENING OF TAXIWAY 'C' FROM 50' TO 75'. THE WORK INCLUDES THE REMOVAL OF THE EXISTING PAVEMENT, CONSTRUCTION OF A 14.5 IN. PORTLAND CEMENT CONCRETE PAVEMENT ON A 4" STABILIZED BITUMINOUS BASE, SUBGRADE STABILIZATION, EXCAVATION, INSTALLATION OF UNDERDRAINS, TURFING, MARKING, AND THE INSTALLATION OF A MEDIUM INTENSITY TAXIWAY LIGHTING SYSTEM (MITL).

ADDITIVE ALTERNATE BID: THE PROPOSED IMPROVEMENTS INCLUDE THE INSTALLATION OF SIX (6) LIGHTED WIND CONES, ASSOCIATED CABLING, AND PROVISION OF ELECTRICAL SERVICES.



### LOCATION

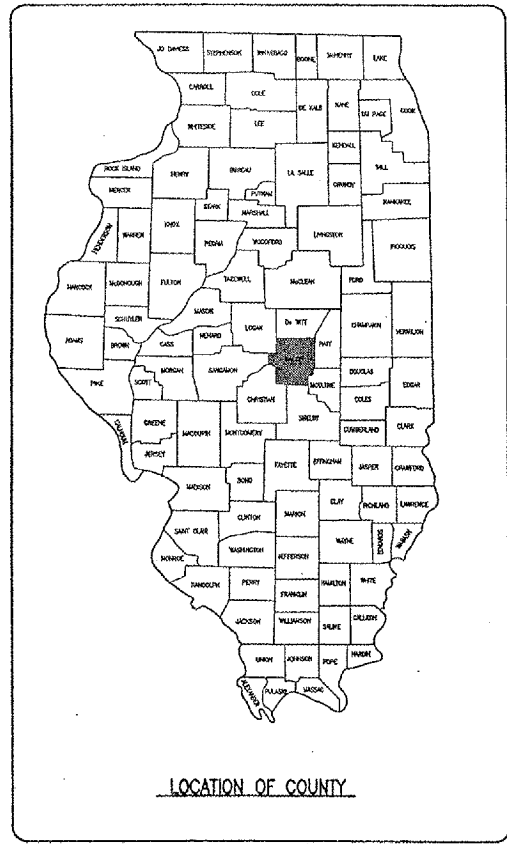
ILL. PROJ.: DEC-3588  
A.I.P. PROJ.: 3-17-  
LATITUDE: 39° 50' 05"  
LONGITUDE: 88° 51' 59"  
ELEVATION: 682.0' M.S.L.  
DATE: APRIL 21, 2006



PLANS PREPARED BY:  
**HANSON**  
Hanson Professional Services Inc.  
ELECTRICAL ENGINEER  
Submitted by: *Kevin N. Lightfoot* ENG'R  
Date Submitted: 4/28/2006  
Lic. Exp. Date: NOVEMBER 30, 2007

PLANS PREPARED BY:  
**HANSON**  
Hanson Professional Services Inc.  
DESIGN ENGINEER  
Submitted by: *Robert A. Waller* ENG'R  
Date Submitted: 4/28/2006  
Lic. Exp. Date: NOVEMBER 30, 2007

**DECATUR AIRPORT**  
DECATUR PARK DISTRICT  
DECATUR, ILLINOIS  
Approved: *Jane Hancock* AIRPORT DIRECTOR  
Date: 4/5/06



DATE	REVISION

**DECATUR AIRPORT**  
DECATUR, ILLINOIS  
A.I.P. PROJ.: 3-17-00

FILE PROJECT No.	DATE	BY
802-08TAXD_0800	01/25/06	CCC
R-001COV.DWG	01/25/06	CCC
N/A	01/25/06	RAW

**HANSON**  
Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62703-2996  
Offices Nationwide

TAXIWAY "C"  
RECONSTRUCTION  
COVER SHEET

APR 28, 2006 3:37 PM RAW  
I:\AIRPORTS\DECATUR\802-08TAXD\AIRPORT SHEETS\R-001COV.DWG - Cover

**SUMMARY OF QUANTITIES - BASE BID**

ITEM No.	DESCRIPTION	UNIT	BID QUANTITIES	AS-BUILT QUANTITIES
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	5,800	
AR109200	INSTALL ELECTRICAL EQUIPMENT	L.S.	1	
AR109430	POWER AND CONTROL WIRING	L.S.	1	
AR109600	L-821 CONTROL PANEL	EACH	1	
AR110014	4" DIRECTIONAL BORE	L.F.	156	
AR110504	4-WAY CONCRETE ENCASED DUCT	L.F.	81	
AR110550	SPLIT DUCT	L.F.	12	
AR110558	EXTEND 8-WAY DUCT	L.F.	16	
AR110710	ELECTRICAL MANHOLE	EACH	1	
AR110906	REMOVE ELECTRICAL HANDHOLE	EACH	1	
AR125410	MIL-STAKE MOUNTED	EACH	33	
AR125415	MIL-BASE MOUNTED	EACH	3	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	1	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	2	
AR125452	TAXI GUIDANCE SIGN, 12 CHARACTER	EACH	1	
AR125565	SPLICE CAN	EACH	4	
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	36	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	4	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	3	
AR125961	RELOCATE STAKE MOUNTED LIGHT	EACH	4	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	2	
AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	1	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	2,581	
AR155540	BY-PRODUCT LIME	TON	362	
AR155616	SOIL PROCESSING - 16"	S.Y.	9,298	
AR156520	INLET PROTECTION	EACH	3	
AR201610	BITUMINOUS BASE COURSE	TON	2,203	
AR208540	OVERSIZE AGGREGATE	TON	881	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	606	
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	7,659	
AR401910	REMOVE & REPLACE BIF. PAVEMENT	S.Y.	107	
AR501524	14.5" PCC PAVEMENT	S.Y.	11,051	
AR501530	PCC TEST BATCH	EACH	1	
AR501900	REMOVE PCC PAVEMENT	S.Y.	71	
AR501910	REMOVE & REPLACE PCC PAVEMENT	S.Y.	63	
AR603510	BITUMINOUS TACK COAT	GAL.	1,377	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	6,248	
AR620590	TEMPORARY MARKING	S.F.	125	
AR620900	PAVEMENT MARKING REMOVAL	S.F.	421	
AR701512	12" RCP, CLASS IV	L.F.	313	
AR701900	REMOVE PIPE	L.F.	390	
AR705411	POROUS BACKFILL No. 1	C.Y.	448	
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	L.F.	2,503	
AR705630	UNDERDRAIN INSPECTION HOLE	EACH	8	
AR705900	REMOVE UNDERDRAIN	L.F.	2,689	
AR751410	INLET	EACH	1	
AR751540	MANHOLE 4'	EACH	2	
AR751900	REMOVE INLET	EACH	1	
AR751903	REMOVE MANHOLE	EACH	2	
AR751943	ADJUST MANHOLE	EACH	3	
AR800501	OFF PEAK CONSTRUCTION	L.S.	1	
AR901510	SEEDING	ACRE	4.3	
AR908510	MULCHING	ACRE	4.3	

**SUMMARY OF QUANTITIES - ADDITIVE ALTERNATE**

ITEM No.	DESCRIPTION	UNIT	BID QUANTITIES	AS-BUILT QUANTITIES
AS107408	L-806 WIND CONE-8' LIGHTED	EACH	6	
AS108656	3/C #6 600 V UG CABLE IN UD	L.F.	12,060	
AS110014	4" DIRECTIONAL BORE	L.F.	675	
AS800579	ELECTRICAL SERVICE FOR RWY 12 AND RWY 18 WIND CONES	L.S.	1	
AS800580	ELECTRICAL SERVICE FOR RWY 24 AND RWY 30 WIND CONES	L.S.	1	
AS800581	ELECTRICAL POWER FOR RWY 6 AND RWY 36 WIND CONES	L.S.	1	

DE066

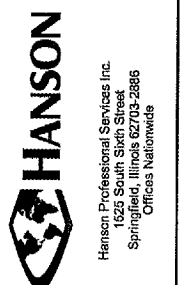
**INDEX TO SHEETS**

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES, INDEX TO SHEETS
3	PROPOSED SAFETY PLAN
4	PROPOSED OPENING AND CLOSING SCHEDULE
5	PROPOSED GEOMETRICS PLAN
6	PROPOSED TYPICAL SECTIONS
7	PROPOSED PLAN & PROFILE: STA. 118+04.56 TO STA. 124+00.00
8	PROPOSED PLAN & PROFILE: STA. 124+00.00 TO STA. 131+75.00
9	PROPOSED PAVEMENT PREPARATION PLAN
10	EXISTING DRAINAGE REMOVAL
11	PROPOSED DRAINAGE PLAN
12	DRAINAGE DETAILS
13	PROPOSED JOINTING PLAN: STA. 118+04.56 TO STA. 124+50.00
14	PROPOSED JOINTING PLAN: STA. 124+50.00 TO STA. 131+75.00
15	JOINTING DETAILS
16	EXISTING ELECTRICAL REMOVAL: STA. 117+99 TO STA. 124+80
17	EXISTING ELECTRICAL REMOVAL: STA. 124+80 TO STA. 133+21
18	PROPOSED ELECTRICAL PLAN: STA. 117+99 TO STA. 124+80
19	PROPOSED ELECTRICAL PLAN: STA. 124+80 TO STA. 133+21
20	ELECTRICAL DETAILS
21	ELECTRICAL DETAILS
22	ELECTRICAL DETAILS
23	ELECTRICAL NOTES
24	ELECTRICAL NOTES
25	PROPOSED MARKING PLAN
26	PROPOSED STAKING, GRADING, SEEDING, AND MULCHING PLAN: STA. 117+99 TO STA. 124+80
27	PROPOSED STAKING, GRADING, SEEDING, AND MULCHING PLAN: STA. 124+80 TO STA. 132+50
28	LOW PROFILE BARRIER DETAIL AND NOTES
29	PROPOSED CROSS-SECTIONS: STA. 117+50 TO STA. 118+00
30	PROPOSED CROSS-SECTIONS: STA. 118+50 TO STA. 119+50
31	PROPOSED CROSS-SECTIONS: STA. 120+00 TO STA. 121+00
32	PROPOSED CROSS-SECTIONS: STA. 121+48.17 TO STA. 122+00
33	PROPOSED CROSS-SECTIONS: STA. 122+50 TO STA. 123+50
34	PROPOSED CROSS-SECTIONS: STA. 123+52.29 TO STA. 124+50
35	PROPOSED CROSS-SECTIONS: STA. 125+00 TO STA. 126+00
36	PROPOSED CROSS-SECTIONS: STA. 126+50 TO STA. 127+50
37	PROPOSED CROSS-SECTIONS: STA. 128+00 TO STA. 128+50
38	PROPOSED CROSS-SECTIONS: STA. 129+00 TO STA. 129+50
39	PROPOSED CROSS-SECTIONS: STA. 130+00 TO STA. 130+50
40	PROPOSED CROSS-SECTIONS: STA. 131+00 TO STA. 131+75
41	ELECTRICAL LEGEND AND ABBREVIATIONS
42	ELECTRICAL SITE PLAN: RUNWAY 12 & RUNWAY 18 WIND CONES
43	ELECTRICAL SITE PLAN: RUNWAY 24 & RUNWAY 30 WIND CONES
44	ELECTRICAL SITE PLAN: RUNWAY 6 & RUNWAY 36 WIND CONES
45	ELECTRICAL VAULT PLAN: RUNWAY 6 & RUNWAY 36 WIND CONES
46	ELECTRICAL ONE LINE DIAGRAM FOR RUNWAY 12 & RUNWAY 18 WIND CONES
47	ELECTRICAL ONE LINE DIAGRAM FOR RUNWAY 24 & RUNWAY 30 WIND CONES
48	ELECTRICAL ONE LINE DIAGRAM FOR RUNWAY 6 & RUNWAY 36 WIND CONES
49	CONTROL PANEL FOR WIND CONES
50	WIND CONE ELEVATION DETAIL
51	ELECTRICAL VAULT GROUNDING PLAN
52	VAULT GROUNDING RISER
53	GROUNDING DETAILS
54	CONTROL BLOCK DIAGRAM FOR RWY & TXY LIGHTING
55	TRANSFER RELAY PANEL DETAILS
56	L-821 CONTROL PANEL DETAILS

DATE	REVISION

**DECATUR, ILLINOIS**

FILE PROJECT No. 802-06TXXD 0800	DATE 02/21/06
Drawings: R-002FLP.DWG	SCALE N/A
Scale	DATE 02/21/06
LAYOUT	CCC 02/21/06
DRAWN	CCC 02/21/06
REVIEWED	WJM 03/31/06



TAXIWAY "C" RECONSTRUCTION  
SUMMARY OF QUANTITIES, INDEX TO SHEETS  
**2**  
2 of 66 sheets

**SCOPE OF WORK**

**BASE BID:**  
THE PROPOSED IMPROVEMENTS INCLUDE THE RECONSTRUCTION AND WIDENING OF TAXIWAY "C" FROM 50' TO 75'. THE WORK INCLUDES THE REMOVAL OF THE EXISTING PAVEMENT, CONSTRUCTION OF A 14.5" PORTLAND CEMENT CONCRETE PAVEMENT ON 4" STABILIZED BITUMINOUS BASE, SUBGRADE STABILIZATION, EXCAVATION, INSTALLATION OF UNDERDRAINS, TURFING, MARKING, THE INSTALLATION OF A MEDIUM INTENSITY TAXIWAY LIGHTING SYSTEM (MITL), AND CONTROL & GROUNDING MODIFICATIONS IN THE VAULT AND AIR TRAFFIC CONTROL TOWER.

**ADDITIVE ALTERNATE BID:**  
THE PROPOSED IMPROVEMENTS INCLUDE THE INSTALLATION OF SIX (6) LIGHTED WIND CONES, ASSOCIATED CABLING, AND PROVISION OF ELECTRICAL SERVICES.

**PROPOSED SAFETY PLAN**

**GENERAL** - THE DECATUR AIRPORT IS COMPRISED OF THREE RUNWAYS. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING RUNWAYS 6-24 AND 18-36 PERIODICALLY AS DESCRIBED ON THE "PROPOSED OPENING AND CLOSING SCHEDULE" ON SHEET 4. TAXIWAY "C" (SOUTH OF TAXIWAY "G") AND TAXIWAY "G" (WEST OF RUNWAY 18-36) WILL BE CLOSED FOR THE DURATION OF THE PROJECT. ANY TIME THE CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY CENTERLINE THE RUNWAY WILL BE CLOSED. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**IDENTIFICATION** - WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR WILL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.

**RADIO CONTROL** - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (121.75 MHz.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE DECATUR AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

**AIRPORT SECURITY NOTE**

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE AND LOCK THE EXISTING GATE IN THE HAUL ROUTE AT THE END OF EACH WORKING DAY.

**CONTRACTOR RESPONSIBILITIES**

THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THIS AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.

THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.

THE CONTRACTOR SHALL KEEP ONE RUNWAY OPEN AT ALL TIMES AND MAINTAIN CONTINUOUS TAXIWAY ACCESS TO ALL HANGARS AND ADMINISTRATIVE AREAS.

ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE RUNWAY CLOSURE.

**HAUL ROUTE, VEHICLE PARKING, MATERIAL STORAGE**

THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING/MATERIAL STORAGE AREA AS SHOWN ON THIS SHEET. THE PROPOSED PARKING/MATERIAL STORAGE AREA WILL BE 350'x100'. ALL EQUIPMENT WILL BE STORED WITHIN THE PROJECT AREA WHEN NOT IN USE. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING/MATERIAL STORAGE AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL GRADE, FERTILIZE, SEED AND MULCH THE HAUL ROUTE AND PARKING AREA AS NEEDED TO RESTORE IT TO ITS ORIGINAL STATE. RESTORATION OF THE HAUL ROUTE AND PARKING/MATERIAL STORAGE AREA WILL BE CONSIDERED INCIDENTAL TO ITEM AR150540 - HAUL ROUTE, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

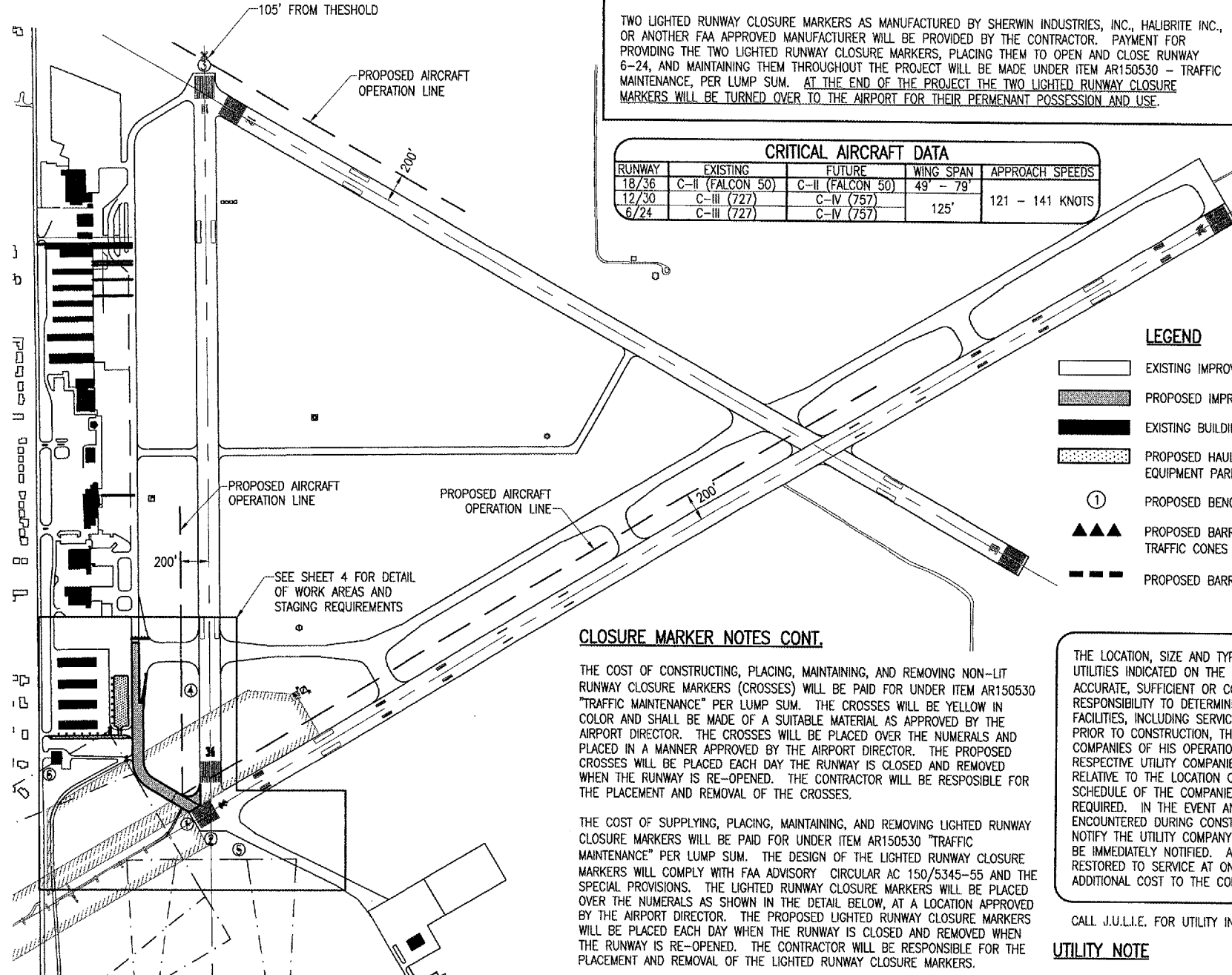
**BARRICADES, BARRIERS, AND TRAFFIC CONES**

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND BARRIERS AT THE LOCATIONS SHOWN ON THIS SHEET AND THE TEMPORARY BARRIER SHEET IN THE PLANS.

THE BARRICADES WILL BE I.D.O.T. TYPE 1, EQUIPPED WITH RED CONTINUOUS LIGHTS AND EXTENDED 18 INCH SQUARE ORANGE FLAG.

THE BARRIER WILL BE CONSTRUCTED AS DETAILED ON THE TEMPORARY BARRIER DETAIL SHEET.

PAYMENT FOR BARRICADES AND BARRIERS, THEIR PLACEMENT AND REMOVALS AS RUNWAYS AND TAXIWAYS ARE OPENED AND CLOSED, MAINTENANCE, AND REPAIRS WILL BE MADE BY ITEM: AR150530 "TRAFFIC MAINTENANCE" PER LUMP SUM.



**CLOSURE MARKER NOTES**

TWO LIGHTED RUNWAY CLOSURE MARKERS AS MANUFACTURED BY SHERWIN INDUSTRIES, INC., HALIBRITE INC., OR ANOTHER FAA APPROVED MANUFACTURER WILL BE PROVIDED BY THE CONTRACTOR. PAYMENT FOR PROVIDING THE TWO LIGHTED RUNWAY CLOSURE MARKERS, PLACING THEM TO OPEN AND CLOSE RUNWAY 6-24, AND MAINTAINING THEM THROUGHOUT THE PROJECT WILL BE MADE UNDER ITEM AR150530 - TRAFFIC MAINTENANCE, PER LUMP SUM. AT THE END OF THE PROJECT THE TWO LIGHTED RUNWAY CLOSURE MARKERS WILL BE TURNED OVER TO THE AIRPORT FOR THEIR PERMANENT POSSESSION AND USE.

CRITICAL AIRCRAFT DATA				
RUNWAY	EXISTING	FUTURE	WING SPAN	APPROACH SPEEDS
18/36	C-II (FALCON 50)	C-II (FALCON 50)	49' - 79'	121 - 141 KNOTS
12/30	C-III (727)	C-IV (757)	125'	
6/24	C-III (727)	C-IV (757)		

**DE066**  
**CRITICAL POINT DATA**  
(SEE SHEET 4)  
LATITUDE: 39° 49' 39.12"  
LONGITUDE: 88° 52' 30.17"  
ELEVATION: 672.00 M.S.L.

**J.U.L.I.E. INFORMATION**  
COUNTY: MACON  
CITY: DECATUR  
TOWNSHIP: LONG CREEK  
SECTION NO.: 20 & 21  
ADDRESS: DECATUR AIRPORT  
AIRPORT ROAD  
DECATUR, ILLINOIS 62524

**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA (SEE SHEET 4 FOR CLARITY)
- PROPOSED BENCHMARK
- PROPOSED BARRICADES AND TRAFFIC CONES (SEE SHEET 4 FOR CLARITY)
- PROPOSED BARRIERS (SEE SHEET 4 FOR CLARITY)

**CLOSURE MARKER NOTES CONT.**

THE COST OF CONSTRUCTING, PLACING, MAINTAINING, AND REMOVING NON-LIT RUNWAY CLOSURE MARKERS (CROSSES) WILL BE PAID FOR UNDER ITEM AR150530 "TRAFFIC MAINTENANCE" PER LUMP SUM. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT DIRECTOR. THE CROSSES WILL BE PLACED OVER THE NUMERALS AND PLACED IN A MANNER APPROVED BY THE AIRPORT DIRECTOR. THE PROPOSED CROSSES WILL BE PLACED EACH DAY WHEN THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES.

THE COST OF SUPPLYING, PLACING, MAINTAINING, AND REMOVING LIGHTED RUNWAY CLOSURE MARKERS WILL BE PAID FOR UNDER ITEM AR150530 "TRAFFIC MAINTENANCE" PER LUMP SUM. THE DESIGN OF THE LIGHTED RUNWAY CLOSURE MARKERS WILL COMPLY WITH FAA ADVISORY CIRCULAR AC 150/5345-55 AND THE SPECIAL PROVISIONS. THE LIGHTED RUNWAY CLOSURE MARKERS WILL BE PLACED OVER THE NUMERALS AS SHOWN IN THE DETAIL BELOW, AT A LOCATION APPROVED BY THE AIRPORT DIRECTOR. THE PROPOSED LIGHTED RUNWAY CLOSURE MARKERS WILL BE PLACED EACH DAY WHEN THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE LIGHTED RUNWAY CLOSURE MARKERS.

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123.

**UTILITY NOTE**

THE CONTRACTOR SHALL CONTACT ALL UTILITY COMPANIES AND AGENCIES WHICH HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR WILL CALL J.U.L.I.E. (1-800-892-0123) TO ACCOMPLISH THE ABOVE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO IDENTIFY ALL UNDERGROUND NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UNDERGROUND IMPROVEMENTS WILL BE LOCATED AT THE CONTRACTOR'S OWN EXPENSE PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.

**ENGINEER'S FIELD OFFICE**

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE FURNISHED, MAINTAINED, AND REMOVED IN ACCORDANCE WITH ITEM AR150510 "ENGINEER'S FIELD OFFICE" AS STATED ON PAGE 168 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE LOCATION OF THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT THE PRE-CONSTRUCTION MEETING.

THE ENGINEERING FIRM WILL MAKE PAYMENT FOR ALL LONG DISTANCE TELEPHONE CALLS IN EXCESS OF ONE HUNDRED DOLLARS (\$100.00) PER MONTH.

THE PROPOSED ENGINEER'S FIELD OFFICE WILL BE PAID FOR UNDER ITEMS: AR150510 ENGINEER'S FIELD OFFICE \_\_\_\_\_ 1 LS.

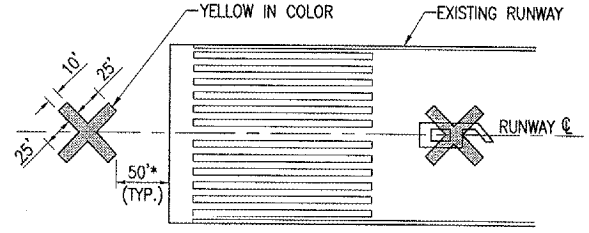
**AIRCRAFT OPERATION LINE**

THE CONTRACTOR WILL LOCATE THIS LINE AT THE START OF CONSTRUCTION AND WILL PLACE FLAGGED LATHE EVERY 150' ALONG IT. THIS LINE WILL BE THE LIMITS THAT ALL CONTRACTOR PERSONNEL MAY VENTURE WHEN A RUNWAY IS NOT CLOSED. THE CONTRACTOR WILL MAINTAIN THE LATHE LINE FOR RUNWAYS.

**EROSION CONTROL**

THIS PROJECT WILL DISTURB MORE THAN 1 ACRE OF LAND, THEREFORE A N.P.D.E.S. PERMIT WILL BE REQUIRED AND APPLIED FOR BY HANSON PROFESSIONAL SERVICES, INC.

CONTROL POINT DATA				
NO.	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	END OF RUNWAY 05	1,151,385.236	832,252.084	N/A
2	END OF RUNWAY 36	1,151,315.601	832,373.780	N/A
3	END OF RUNWAY 18	1,156,614.214	832,324.427	N/A
4	CB#12 CHSLD "X" ON N. RIM OF INLET, 80' LT OF RWY 18/36			676.01
5	CB#15 CHSLD SQUARE ON N. CORNER OF CONC. HEADWALL W. OF TXY F			667.81
6	CB#21 CHSLD "X" ON N. RIM OF A MANHOLE ON E. SIDE OF AIRPORT RD			672.63



\* UNLESS OTHERWISE NOTED  
**DETAIL OF RUNWAY CLOSURE MARKER**  
"NOT TO SCALE"

APR 27, 2006 10:57 PM RAW  
I:\AIRPORTS\DECATUR\802-061XY\AIRPORT SHEETS\R-0035FY.DWG - Safety

REVISION BY DATE

DATE

DECATUR, ILLINOIS

ALP. PROJ.: 3-17-00

IL. PROJ.: DEC-3588

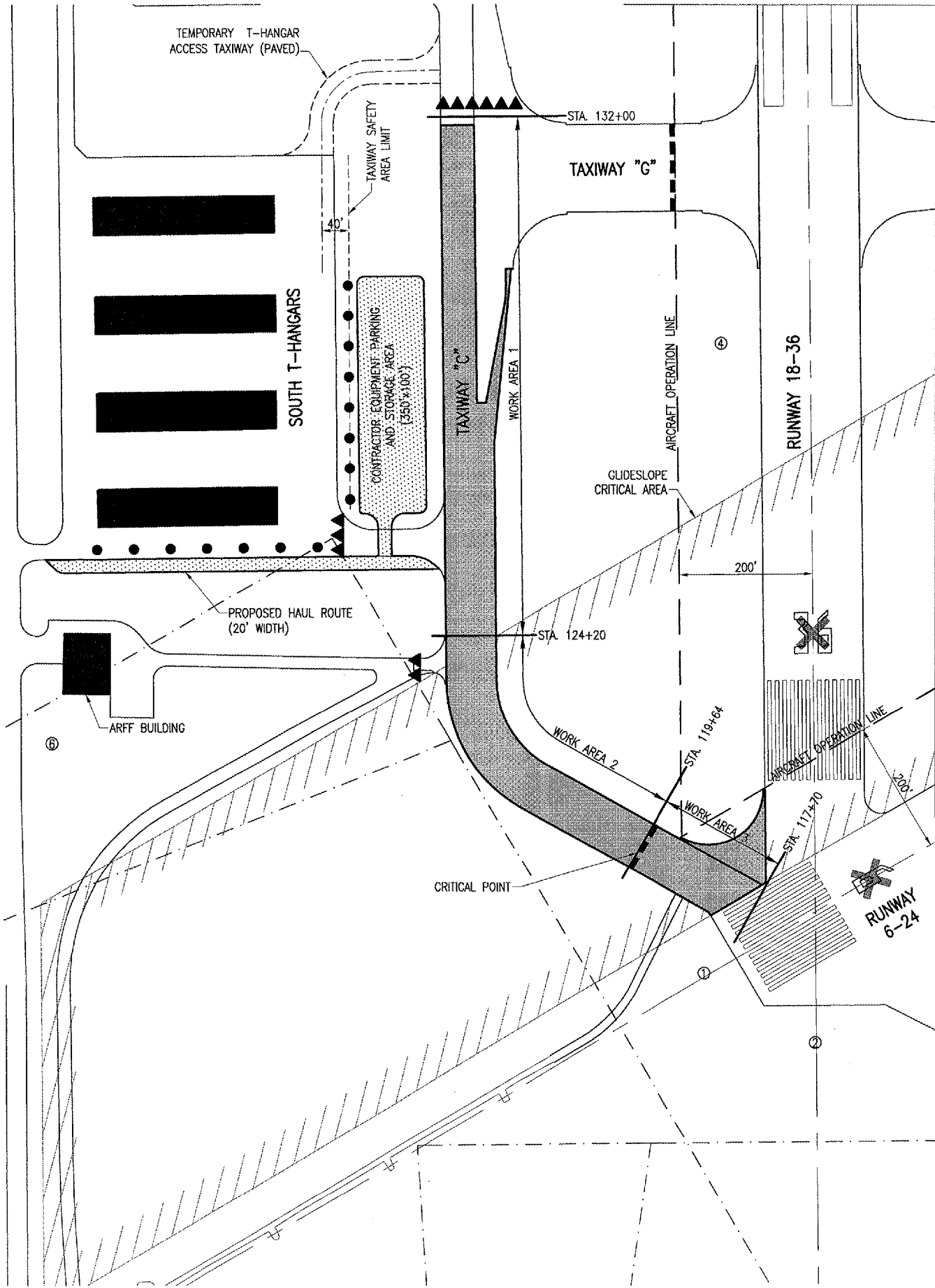
Project No. 802-061XYD 0800  
Name R-0035FY.DWG  
Scale 1" = 500'  
Date 01/24/06

LAYOUT	RAW	03/02/06
DRAWN	CCC	03/07/06
REVIEWED	WJM	03/31/06

**HANSON**  
Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62705-2886  
Offices Nationwide

TAXIWAY "C" RECONSTRUCTION  
PROPOSED SAFETY PLAN

**3**  
3 of 58 sheets



**RUNWAY AND TAXIWAY OPENING AND CLOSING NOTES**

ANY TIME CONSTRUCTION WORK IS BEING CONDUCTED WITHIN 200 OF THE CENTERLINE OF A RUNWAY AND 85 FEET OF THE CENTERLINE OF THE TAXIWAY, THE RUNWAY/TAXIWAY WILL BE CLOSED FOR AIR OPERATIONS. CONSTRUCTION WORK INCLUDES THE PRESENCE OF VEHICLES, PERSONNEL, EXCAVATION, HOLES AND MOUNDS, EQUIPMENT PARKED, DROP-OFFS EXCEEDING 3 INCHES FROM THE EDGE OF PAVEMENT AND OTHER HAZARDS TO AIR TRAFFIC.

RUNWAY 6-24 WILL OPEN AND THE INSTRUMENT LANDING SYSTEM (ILS) UTILIZED DURING INSTRUMENT WEATHER CONDITIONS. THEREFORE, NO WORK WILL BE COMPLETED IN AREA 2 AND 3 DURING IFR CONDITIONS.

THE AIRPORT DIRECTOR, OR HIS REPRESENTATIVE, WILL BE CONSULTED EACH MORNING PRIOR TO THE START OF WORK TO CONFIRM WEATHER AND FLIGHT CONDITIONS PRESENT AT THE AIRPORT, TO CONFIRM THE STATUS OF THE GLIDESLOPE USE.

THE TAXIWAY SAFETY AREA LIMIT FOR THE TEMPORARY T-HANGAR ACCESS TAXIWAY, AS LABELED, IS BASED ON DESIGN GROUP I AIRCRAFT AND LIMITS VEHICLE PARKING AND MATERIAL STORAGE A DISTANCE OF 40' FROM THE TEMPORARY ACCESS TAXIWAY CENTERLINE.

PRIOR NOTICE AND APPROVED SCHEDULE BY THE AIRPORT DIRECTOR WILL BE REQUIRED PRIOR TO CLOSING A RUNWAY AND OR TAXIWAY. THE RUNWAY WILL BE CLOSED BY CROSSES PLACED OVER THE RUNWAY NUMERALS OR PLACEMENT OF LIGHTED RUNWAY CLOSURE MARKERS, AND TAXIWAYS WILL BE CLOSED WITH LIGHTED AND FLAGGED BARRICADES AND/OR BARRIERS.

OPENING A RUNWAY OR TAXIWAY WILL REQUIRE THE FOLLOWING ACTIONS:

- 1) THE PAVEMENT WILL BE SWEEPED BY A POWER BROOM TO REMOVE ANY AND ALL DEBRIS FROM THE PAVEMENT.
- 2) THE AREA WITHIN 200 FEET OF A RUNWAY CENTERLINE AND 85 FEET OF A TAXIWAY CENTERLINE MUST BE SMOOTH GRADED WITH A 3% - 5% SLOPE AWAY FROM THE PAVEMENT EDGE. THE PAVEMENT EDGE DROP-OFF CAN NOT EXCEED 3 INCHES. NO HOLES OR MOUNDS WILL BE PERMITTED WITHIN THE AREA.
- 3) ALL PERSONNEL AND EQUIPMENT WILL BE OUT OF THE 200 FEET AND 85 FEET LIMITS.
- 4) ALL RUNWAY AND TAXIWAY LIGHTING CIRCUITS WILL BE OPERATIONAL.
- 5) RUNWAY AND TAXIWAY CENTERLINE MARKING, IF OBLITERATED, WILL BE REMARKED.
- 6) THE CLOSED PAVEMENTS WILL BE VISUALLY INSPECTED BY AIRPORT PERSONNEL PRIOR TO OPENING. THE CONTRACTOR WILL MAKE ANY CORRECTIONS REQUIRED AS A RESULT OF THE INSPECTION.

WORK AREA 3 CONTAINS SPECIAL REQUIREMENTS TO BE FOLLOWED BY THE CONTRACTOR DURING THE CONSTRUCTION OF WORK ITEMS. THE ITEMS OF WORK WILL BE CONSTRUCTED IN SUCH A MANNER TO ALLOW FOR RUNWAY 6-24 TO BE OPENED FOR DEFINED PERIODS DURING THE CONSTRUCTION SEQUENCES. THE ALLOWABLE CLOSURE TIMES ARE STATED IN THE WORK AREA 3 OPENING AND CLOSING SCHEDULE. PAYMENT FOR THE SPECIAL REQUIREMENTS TO OPEN RUNWAY 6-24 FOR THE DEFINED PERIODS WILL BE MADE BY ITEM AR800501 OFF-PEAK CONSTRUCTION PER LUMP SUM.

**ROAD AND ACCESS CLOSURE NOTES**

ACCESS TO THE SOUTH T-HANGAR AREA WILL BE BY A TEMPORARY ACCESS CONSTRUCTED AS SHOWN.

ACCESS TO TAXIWAY "C" FROM THE ARFF BUILDING AND THE PERIMETER ROAD WILL BE CLOSED FOR THE DURATION OF THE PROJECT. ARFF EQUIPMENT WILL BE TEMPORARILY RELOCATED TO AN ALTERNATE FACILITY FOR THE DURATION OF THE PROJECT.

**RUNWAY AND TAXIWAY OPENING AND CLOSING SCHEDULE**

**WORK AREA 1**

LOCATION: TAXIWAY "C" (STA. 124+20 TO STA. 132+00)  
 UNAFFECTED: RUNWAYS 6-24, 18-36, AND 12-30  
 TAXIWAYS A, B, F, AND G (EAST OF RWY 18-36)  
 AFFECTED: TAXIWAY "C" (CLOSED FOR DURATION OF PROJECT, SOUTH OF TAXIWAY "G") STA. 132+00  
 TAXIWAY "G" (CLOSED FOR DURATION OF PROJECT, WEST OF RUNWAY 18-36)

**WORK AREA 2**

LOCATION: TAXIWAY "C" (STA. 119+64 TO STA. 124+20)  
 UNAFFECTED: RUNWAYS 6-24 (NON-IFR CONDITIONS), 18-36, AND 12-30  
 TAXIWAYS A, B, F, AND G (EAST OF RWY 18-36)  
 AFFECTED: TAXIWAY "C" (CLOSED FOR DURATION OF PROJECT) SOUTH OF TAXIWAY "G") STA. 132+00  
 TAXIWAY "G" (CLOSED FOR DURATION OF PROJECT, WEST OF RUNWAY 18-36)

**WORK AREA 3**

LOCATIONS: TAXIWAY "C" (STA. 117+70 TO STA. 119+64) AREAS WITHIN 200' OF RUNWAY 6-24  
 UNAFFECTED: RUNWAY 12-30  
 TAXIWAYS A, B, F, AND G (EAST OF RWY 18-36)  
 AFFECTED: TAXIWAY "C" (CLOSED FOR DURATION OF PROJECT) SOUTH OF TAXIWAY "G") STA. 132+00  
 TAXIWAY "G" (CLOSED FOR DURATION OF PROJECT, WEST OF RUNWAY 18-36)  
 RUNWAY 6-24 (CLOSED FOR THE CONSTRUCTION DAY WHEN WEATHER AND CLOSURE SCHEDULE PERMITS)

THE PERMITTED RUNWAY CLOSURE TIME IS AS FOLLOWS:

DAY	CLOSURE TIME
SUNDAY	12:00am - 12:00am (24 hrs.)
MONDAY	12:00am - 9:00pm (21 hrs.)
TUESDAY	6:00am - 9:00pm (15 hrs.)
WEDNESDAY	6:00am - 9:00pm (15 hrs.)
THURSDAY	6:00am - 9:00pm (15 hrs.)
FRIDAY	6:00am - 12:00am (18 hrs.)
SATURDAY	12:00am - 12:00am (24 hrs.)

RUNWAY 18-36 (CLOSED FOR THE CONSTRUCTION DAY WHEN RUNWAY 6-24 IS CLOSED DUE TO AIRCRAFT OPERATIONAL LIMIT OVERLAP.)

**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA
- PROPOSED BENCHMARK
- PROPOSED BARRICADES
- PROPOSED BARRIERS
- PROPOSED TRAFFIC CONES

0' 50' 100' 200'  
 FULL SIZE SCALE: 1" = 100'  
 HALF SIZE SCALE: 1" = 200'

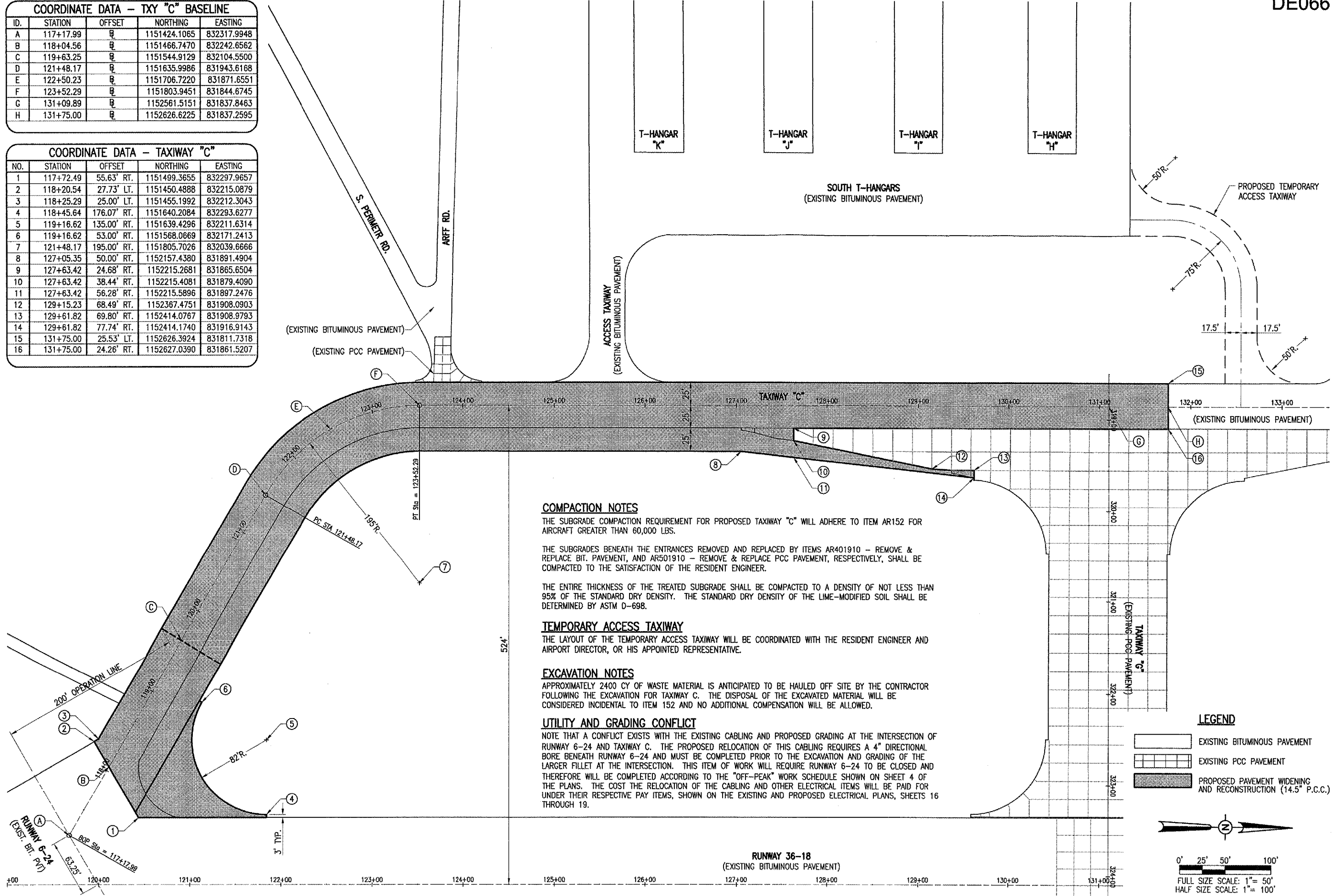
DATE	REVISION	BY			
<p><b>HANSON</b>                  Hanson Professional Services Inc.                  625 S. Springfield, Illinois 62703-2888                  Offices Nationwide</p>					
<p><b>TAXIWAY "C" RECONSTRUCTION PROPOSED STAGING PLAN</b></p>					
<p>4</p>					

ILL. Project No. 802-06TXD-0800  
 Filename: R-0035FY.DWG  
 Scale: 1" = 100'  
 Date: 03/02/06  
 LAYOUT: RAW  
 DRAWN: CCC  
 REVIEWED: WJM  
 Date: 03/02/06  
 Date: 03/07/06  
 Date: 03/31/06  
 ILL. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

APR 27, 2006 10:56 PM RAW  
 I:\AIRPORTS\DECATUR\802-06TXD\AIRPORT SHEETS\R-0035FY.DWG - Staging

COORDINATE DATA - TXY "C" BASELINE				
ID.	STATION	OFFSET	NORTHING	EASTING
A	117+17.99	0	1151424.1065	832317.9948
B	118+04.56	0	1151466.7470	832242.6562
C	119+63.25	0	1151544.9129	832104.5500
D	121+48.17	0	1151635.9986	831943.6168
E	122+50.23	0	1151706.7220	831871.6551
F	123+52.29	0	1151803.9451	831844.6745
G	131+09.89	0	1152561.5151	831837.8463
H	131+75.00	0	1152626.6225	831837.2595

COORDINATE DATA - TAXIWAY "C"				
NO.	STATION	OFFSET	NORTHING	EASTING
1	117+72.49	55.63' RT.	1151499.3655	832297.9657
2	118+20.54	27.73' LT.	1151450.4888	832215.0879
3	118+25.29	25.00' LT.	1151455.1992	832212.3043
4	118+45.64	176.07' RT.	1151640.2084	832293.6277
5	119+16.62	135.00' RT.	1151639.4296	832211.6314
6	119+16.62	53.00' RT.	1151568.0669	832171.2413
7	121+48.17	195.00' RT.	1151805.7026	832039.6666
8	127+05.35	50.00' RT.	1152157.4380	831891.4904
9	127+63.42	24.68' RT.	1152215.2681	831865.6504
10	127+63.42	38.44' RT.	1152215.4081	831879.4090
11	127+63.42	56.28' RT.	1152215.5896	831897.2476
12	129+15.23	68.49' RT.	1152367.4751	831908.0903
13	129+61.82	69.80' RT.	1152414.0767	831908.9793
14	129+61.82	77.74' RT.	1152414.1740	831916.9143
15	131+75.00	25.53' LT.	1152626.3924	831811.7318
16	131+75.00	24.26' RT.	1152627.0390	831861.5207



**COMPACTION NOTES**

THE SUBGRADE COMPACTION REQUIREMENT FOR PROPOSED TAXIWAY "C" WILL ADHERE TO ITEM AR152 FOR AIRCRAFT GREATER THAN 60,000 LBS.

THE SUBGRADES BENEATH THE ENTRANCES REMOVED AND REPLACED BY ITEMS AR401910 - REMOVE & REPLACE BIT. PAVEMENT, AND AR501910 - REMOVE & REPLACE PCC PAVEMENT, RESPECTIVELY, SHALL BE COMPACTED TO THE SATISFACTION OF THE RESIDENT ENGINEER.

THE ENTIRE THICKNESS OF THE TREATED SUBGRADE SHALL BE COMPACTED TO A DENSITY OF NOT LESS THAN 95% OF THE STANDARD DRY DENSITY. THE STANDARD DRY DENSITY OF THE LIME-MODIFIED SOIL SHALL BE DETERMINED BY ASTM D-698.

**TEMPORARY ACCESS TAXIWAY**

THE LAYOUT OF THE TEMPORARY ACCESS TAXIWAY WILL BE COORDINATED WITH THE RESIDENT ENGINEER AND AIRPORT DIRECTOR, OR HIS APPOINTED REPRESENTATIVE.

**EXCAVATION NOTES**

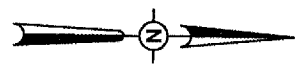
APPROXIMATELY 2400 CY OF WASTE MATERIAL IS ANTICIPATED TO BE HAULED OFF SITE BY THE CONTRACTOR FOLLOWING THE EXCAVATION FOR TAXIWAY C. THE DISPOSAL OF THE EXCAVATED MATERIAL WILL BE CONSIDERED INCIDENTAL TO ITEM 152 AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**UTILITY AND GRADING CONFLICT**

NOTE THAT A CONFLICT EXISTS WITH THE EXISTING CABLING AND PROPOSED GRADING AT THE INTERSECTION OF RUNWAY 6-24 AND TAXIWAY C. THE PROPOSED RELOCATION OF THIS CABLING REQUIRES A 4" DIRECTIONAL BORE BENEATH RUNWAY 6-24 AND MUST BE COMPLETED PRIOR TO THE EXCAVATION AND GRADING OF THE LARGER FILLET AT THE INTERSECTION. THIS ITEM OF WORK WILL REQUIRE RUNWAY 6-24 TO BE CLOSED AND THEREFORE WILL BE COMPLETED ACCORDING TO THE "OFF-PEAK" WORK SCHEDULE SHOWN ON SHEET 4 OF THE PLANS. THE COST THE RELOCATION OF THE CABLING AND OTHER ELECTRICAL ITEMS WILL BE PAID FOR UNDER THEIR RESPECTIVE PAY ITEMS, SHOWN ON THE EXISTING AND PROPOSED ELECTRICAL PLANS, SHEETS 16 THROUGH 19.

**LEGEND**

- [Pattern] EXISTING BITUMINOUS PAVEMENT
- [Pattern] EXISTING PCC PAVEMENT
- [Pattern] PROPOSED PAVEMENT WIDENING AND RECONSTRUCTION (14.5" P.C.C.)



0' 25' 50' 100'  
 FULL SIZE SCALE: 1" = 50'  
 HALF SIZE SCALE: 1" = 100'

DATE	REVISION	BY

DECATUR AIRPORT  
**DECATUR, ILLINOIS**  
 A.I.P. PROJ.: 3-17-00  
 I.L. PROJ.: DEC-3586

Project No.	802-06TXYD.0800
Electronic	R-121GEO.DWG
Scale	1"=50'
Date	02/08/06
LAYOUT	CCC 02/08/06
DRAWN	CCC 02/08/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2886  
 Offices Nationwide

TAXIWAY "C"  
 RECONSTRUCTION  
 PROPOSED GEOMETRIC PLAN

MAY 02, 2006 11:06 AM RAW  
 I:\AIRPORTS\DECATUR\802-06TXYD\AIRPORT\ SHEETS\R-121GEO.DWG - Geometrics

LEGEND

- ① PROPOSED AR501524: 14.5" PCC PAVEMENT
- ② PROPOSED AR201: 4" BITUMINOUS BASE COURSE
- ③ PROPOSED AR603: BITUMINOUS TACK COAT BETWEEN 2" BITUMINOUS LIFTS
- ④ PROPOSED AR155: SOIL PROCESSING - 16"
- ⑤ PROPOSED AR209: CRUSHED AGGREGATE BASE COURSE - 4"
- ⑥ PROPOSED AR208: OVERSIZE AGGREGATE - 8"
- ⑦ PROPOSED AR705: POROUS BACKFILL NO. 1
- ⑧ PROPOSED AR705: 6" PERFORATED UNDERDRAIN W/SOCK
- ⑨ EXISTING GROUND

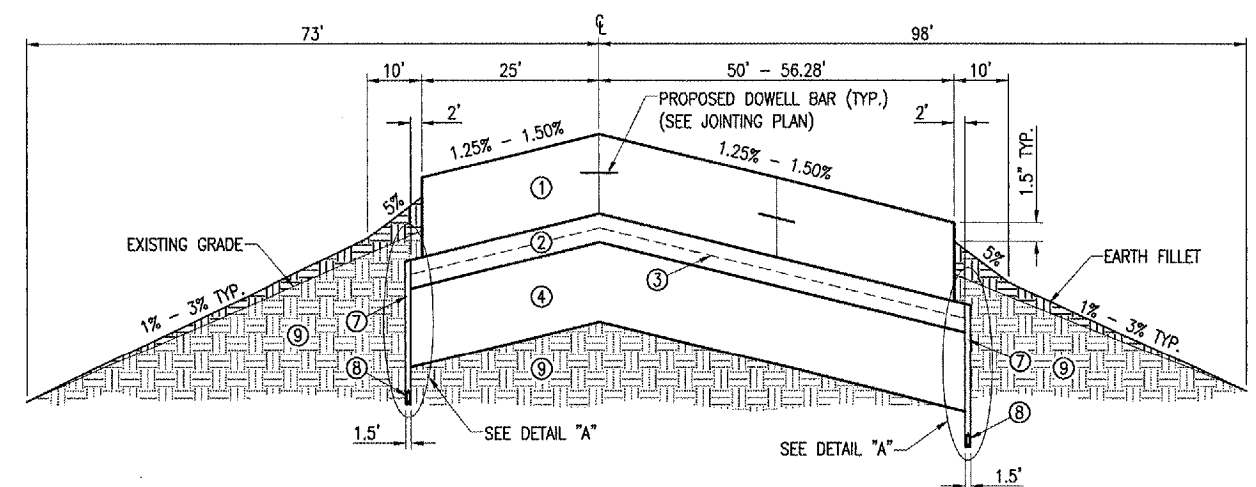
BY	
REVISION	
DATE	

DECATUR, ILLINOIS  
 A.I.P. PROJ.: 3-17-00  
 I.L. PROJ.: DEC-3588

HEL Project No.	802-06TXXD-0800
Drawings	R-521TYP.DWG
Scale	N/A
Date	02/08/06
LAYOUT	CCC 02/08/06
DRAWN	CCC 02/08/06
REVIEWED	WJM 03/31/06

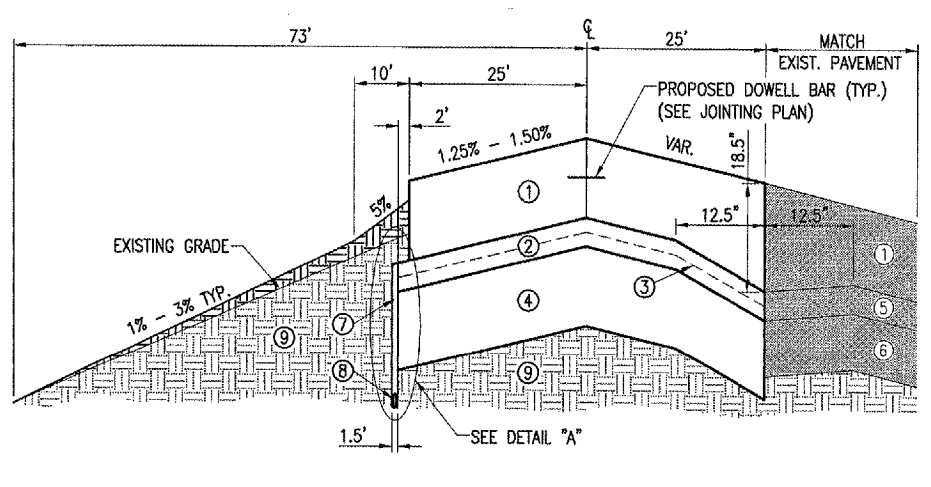
**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62705-2886  
 Offices Nationwide

TAXIWAY "C"  
 RECONSTRUCTION  
 TYPICAL SECTIONS



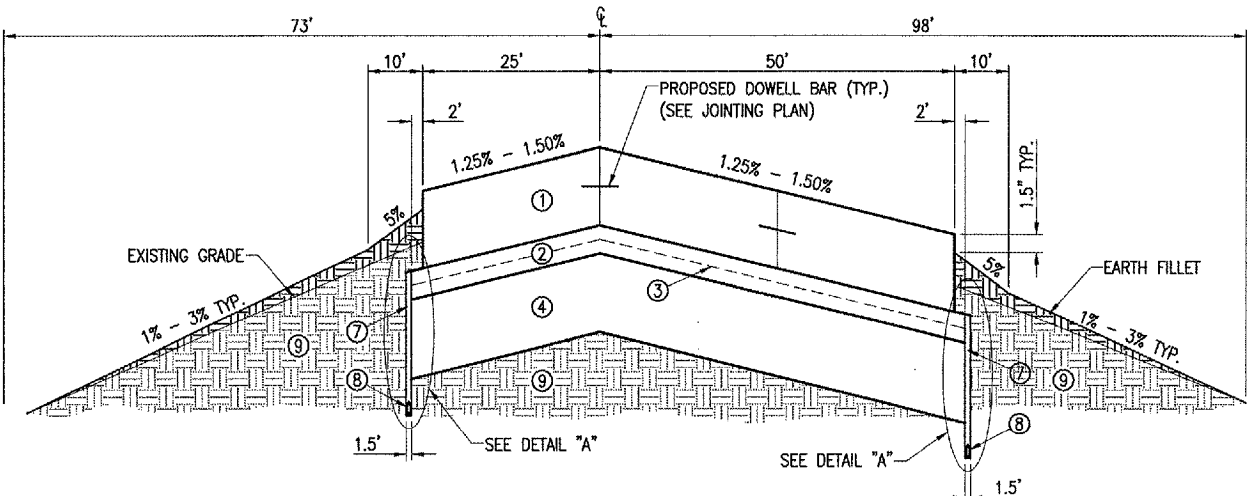
PROPOSED TAXIWAY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"  
 STA. 127+05.35 TO STA. 127+63.42



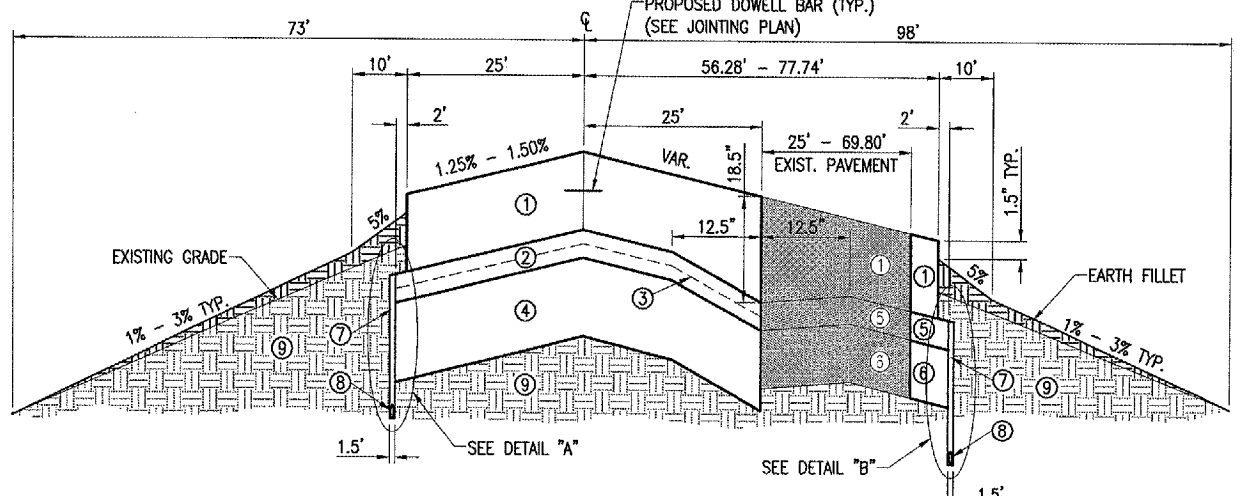
PROPOSED TAXIWAY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"  
 STA. 129+61.82 TO STA. 131+75



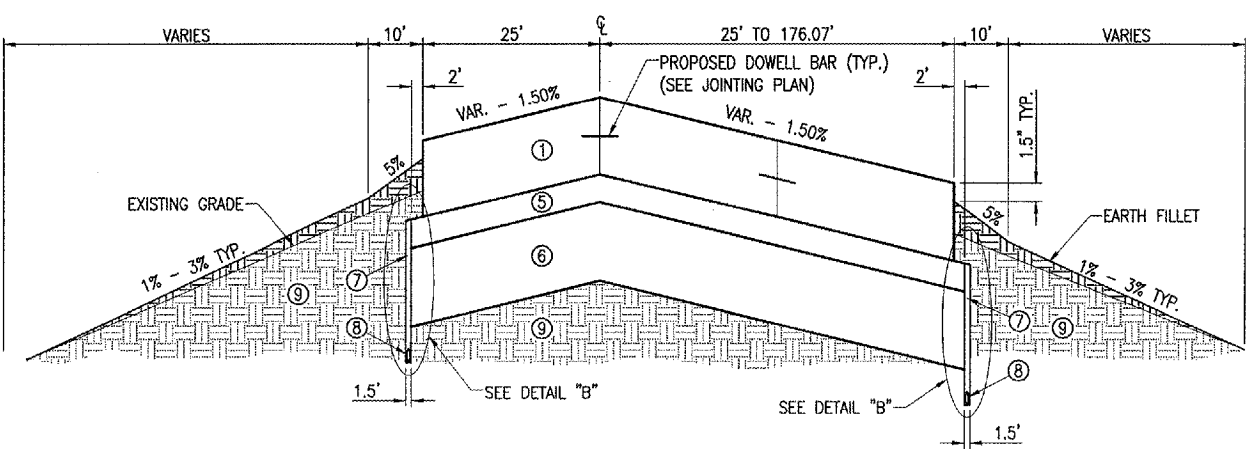
PROPOSED TAXIWAY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"  
 STA. 119+63.25 TO STA. 127+05.35



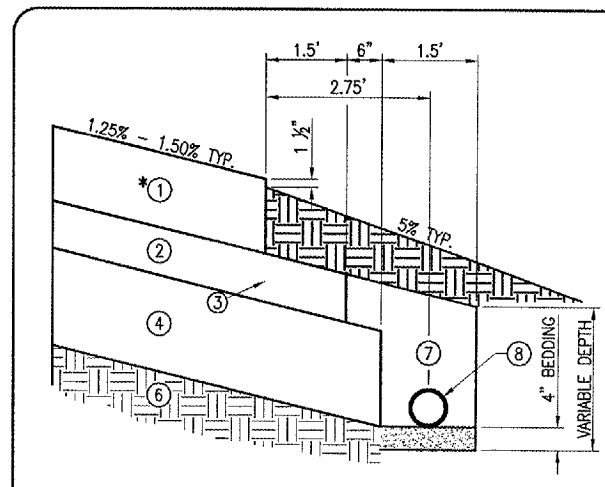
PROPOSED TAXIWAY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"  
 STA. 127+63.42 TO STA. 129+61.82



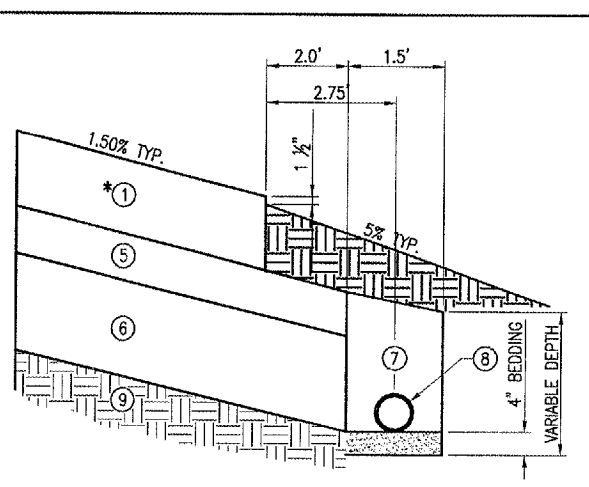
PROPOSED TAXIWAY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"  
 6/24 RUNWAY EDGE (STA. 118+04.56 SKEW) TO STA. 119+63.25



PROPOSED TAXIWAY PAVEMENT - DETAIL "A"

"NOT TO SCALE"



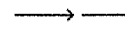

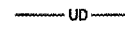
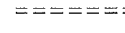

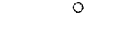






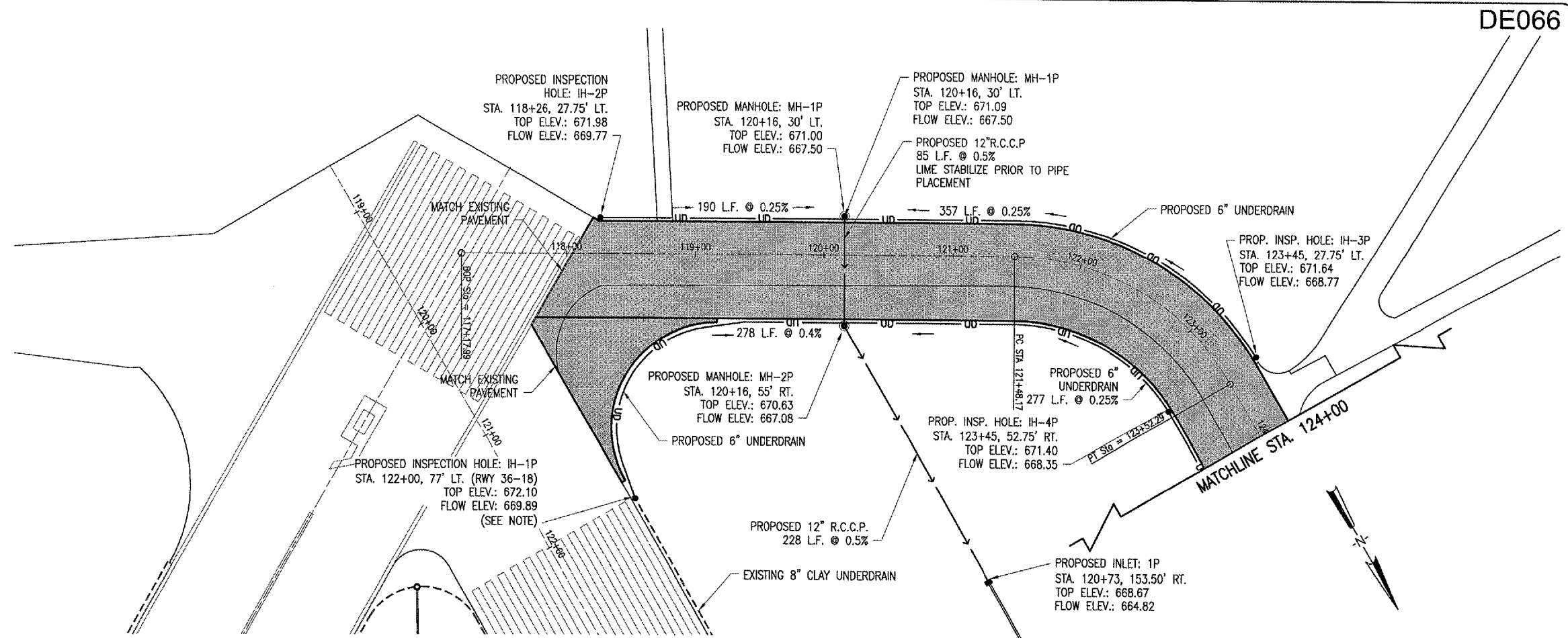
PROPOSED TAXIWAY PAVEMENT - DETAIL "B"

"NOT TO SCALE"

APR. 28, 2006 11:51 AM RAW  
 I:\AIRPORTS\DECATUR\B02-06TXX\AIRPORT\ SHEETS\R-521TYP.DWG - SHEET 06

**LEGEND**

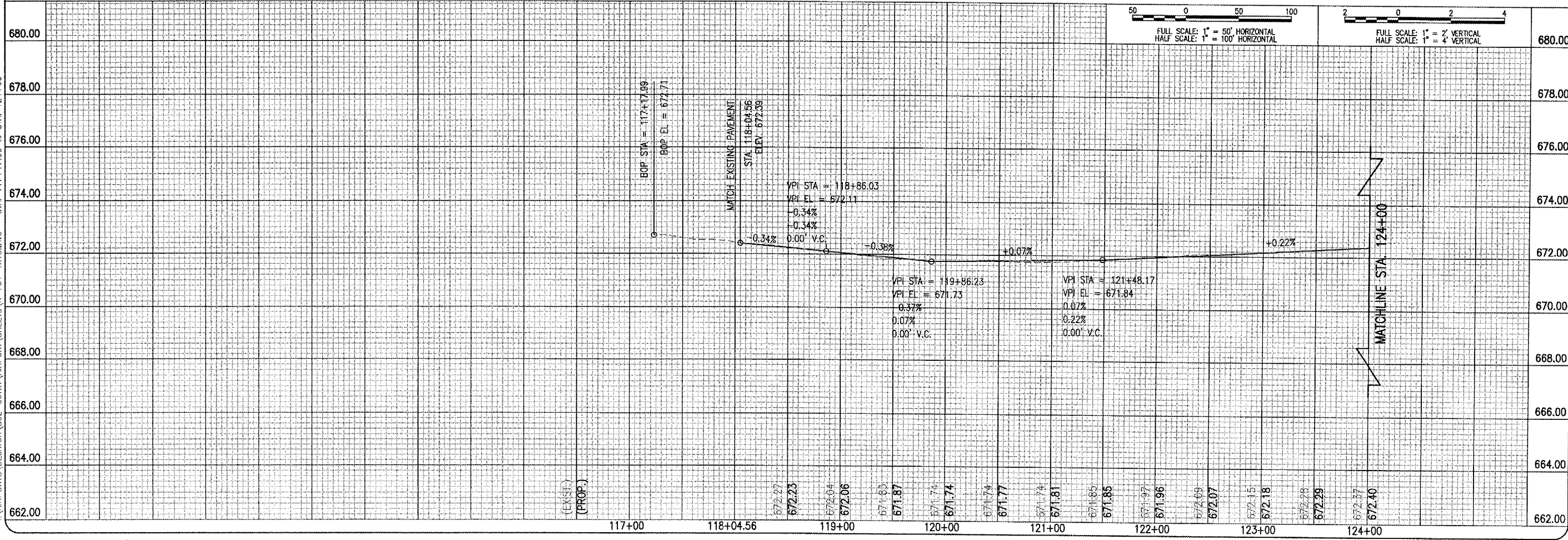
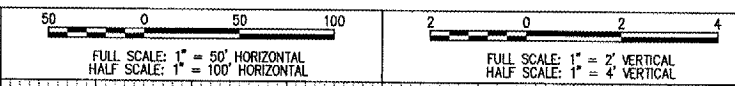
-  EXISTING PAVEMENT
-  PROPOSED BITUMINOUS PAVEMENT
-  PROPOSED STORM SEWER
-  EXISTING STORM SEWER
-  PROPOSED UNDERDRAIN
-  EXISTING UNDERDRAIN
-  PROPOSED INSPECTION HOLE
-  EXISTING INSPECTION HOLE
-  PROPOSED INLET
-  EXISTING INLET
-  PROPOSED MANHOLE
-  EXISTING MANHOLE



DE066

REVISION	DATE	BY

**DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 ILL. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00






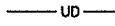
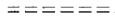




LEG. Project No. 802-061XVD_0800	DATE 03/17/06
R-701-TXV.DWG	SCALE 1"=50'H, 2"
DATE 03/17/06	BY BKB
BY BKB	DATE 03/09/06
DATE 03/17/06	BY CCC
BY CCC	DATE 3/16/06
DATE 03/17/06	BY WJM
BY WJM	DATE 03/31/06

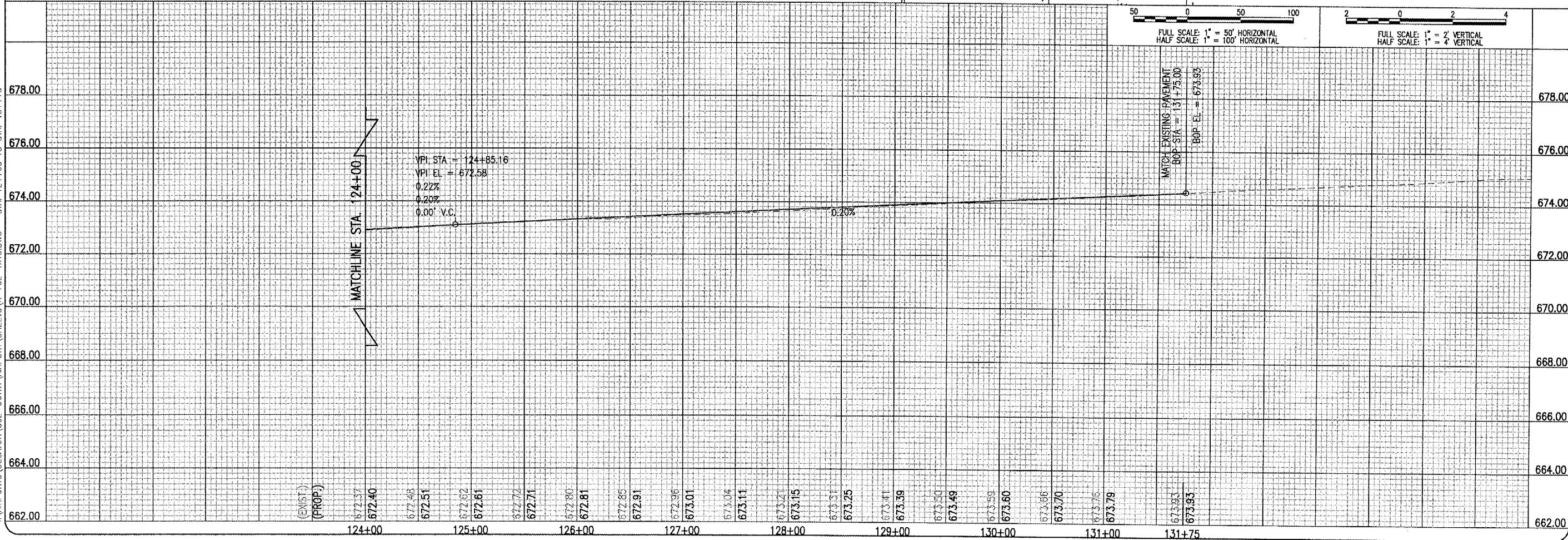
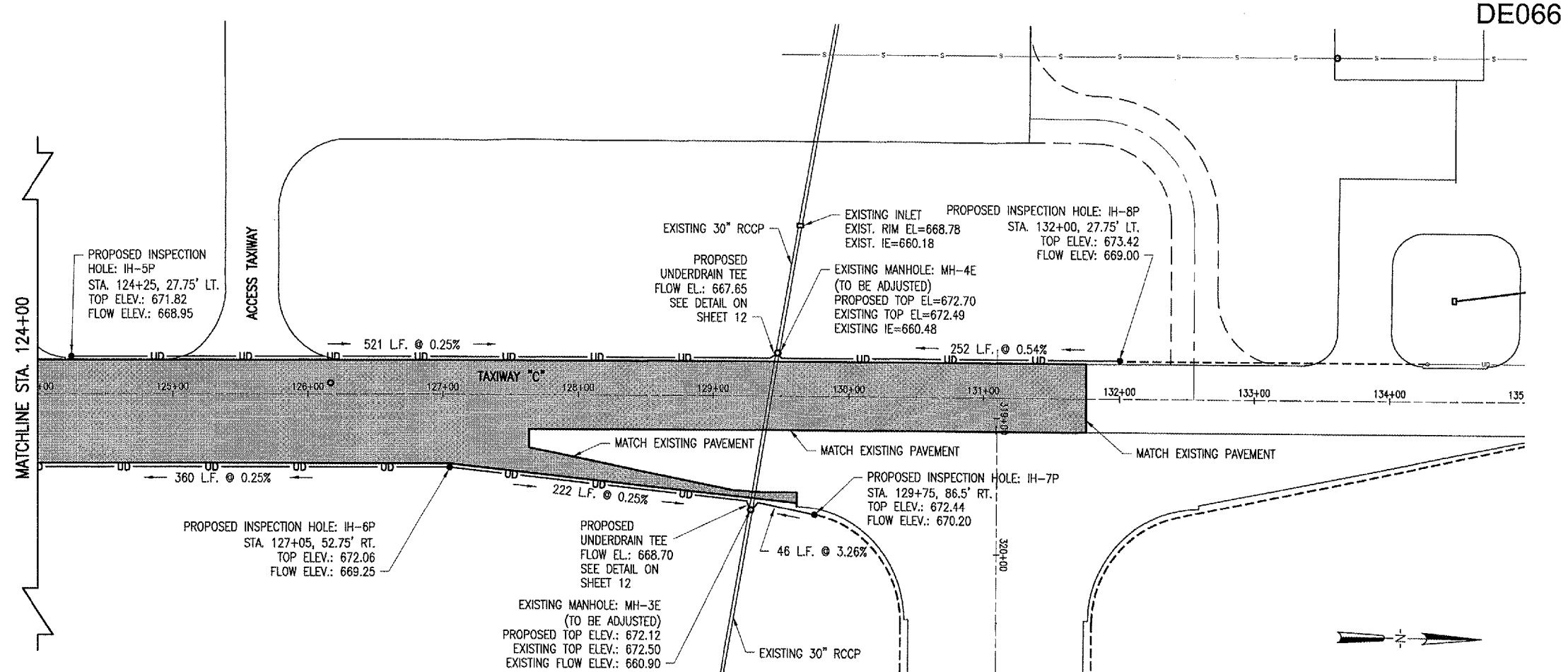
**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2895  
 Offices Nationwide

**TAXIWAY "C"**  
**RECONSTRUCTION**  
 PROPOSED  
 PLAN AND PROFILE  
 STA. 118+04.56 TO STA. 124+00

APR 27, 2006 10:58 PM RAW I:\AIRPORTS\DECATUR\B02-061XVD\AIRPORT\SHEETS\R-701-TXV.DWG - STA. 117+17.99 TO STA. 124+00

LEGEND

-  EXISTING PAVEMENT
-  PROPOSED BITUMINOUS PAVEMENT
-  EXISTING STORM SEWER
-  PROPOSED UNDERDRAIN
-  EXISTING UNDERDRAIN
-  PROPOSED INSPECTION HOLE
-  EXISTING INSPECTION HOLE
-  EXISTING INLET
-  EXISTING MANHOLE



APR 27, 2006 10:59 PM RAW  
I:\AIRPORTS\DECATUR\B02-TXIC.DWG - STA. 124+00 TO STA. 131+75

REVISION	DATE	BY

DECATUR, ILLINOIS

DECATUR AIRPORT

ILL. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

FILE PROJECT No.	802-061X1D 0800
FILE NAME	R-702-TXIC.DWG
SCALE	1"=50'H, 2"
DATE	03/17/06
LAYOUT	BKB 03/09/06
DRAWN	CCC 3/16/06
REVIEWED	WJM 03/31/06

**HANSON**

Hanson Professional Services Inc.  
1525 South State Street  
Springfield, Illinois 62703-2886  
Offices Nationwide

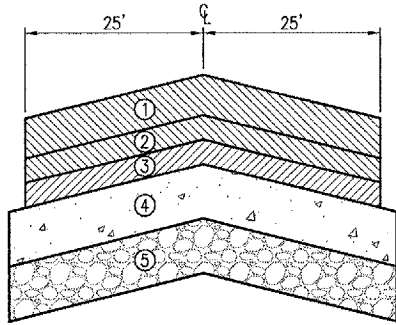
**TAXIWAY "C"**  
**RECONSTRUCTION**

PROPOSED  
PLAN AND PROFILE  
STA. 124+00 TO STA. 131+75

8

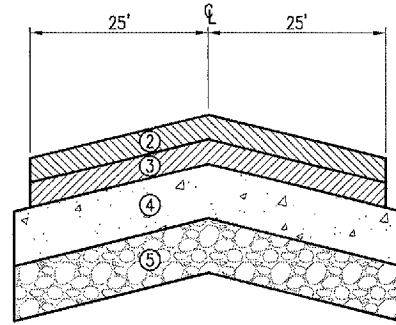
8 of 56 sheets





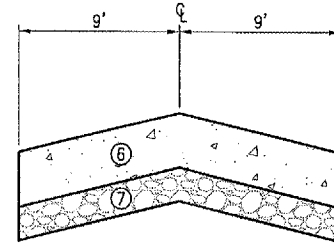
EXISTING TAXIWAY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"  
6/24 RUNWAY EDGE (STA. 118+04.56 SKEW)  
TAPER TO SECTION @ STA. 120+00±



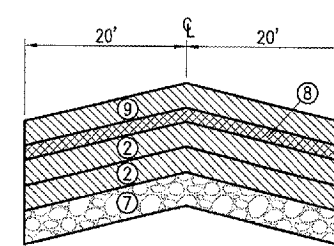
EXISTING TAXIWAY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"  
STA. 120+00± TO STA. 131+75



EXISTING A.R.F.F. PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"



EXISTING ACCESS TXY PAVEMENT - TYPICAL SECTION

"NOT TO SCALE"

LEGEND

- ① EXISTING 401 - 11" BIT. SURFACE COURSE
- ② EXISTING 401 - 2" BIT. SURFACE COURSE
- ③ EXISTING 201 - 2"± BIT. BASE COURSE
- ④ EXISTING 501 - 9" P.C.C.
- ⑤ EXISTING 209 - 7" CRUSHED AGGREGATE
- ⑥ EXISTING 501 - 10" P.C.C.
- ⑦ EXISTING 209 - 6" CRUSHED AGGREGATE
- ⑧ EXISTING 402 - 3/4" P.F.C.
- ⑨ EXISTING 401 - 1 1/2" BIT. SURFACE COURSE

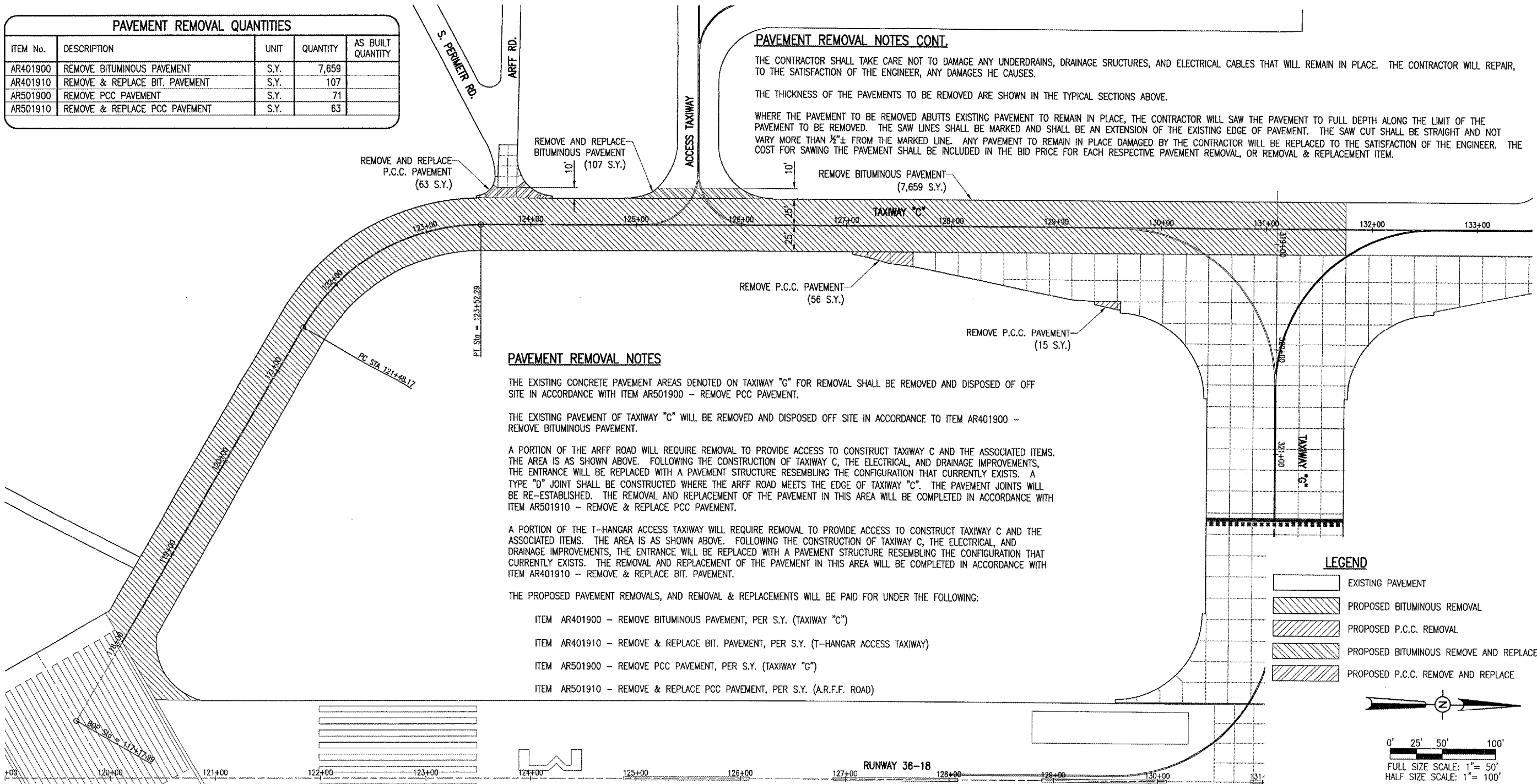
PAVEMENT REMOVAL QUANTITIES				
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	7,659	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	107	
AR501900	REMOVE PCC PAVEMENT	S.Y.	71	
AR501910	REMOVE & REPLACE PCC PAVEMENT	S.Y.	63	

PAVEMENT REMOVAL NOTES CONT.

THE CONTRACTOR SHALL TAKE CARE NOT TO DAMAGE ANY UNDERDRAINS, DRAINAGE STRUCTURES, AND ELECTRICAL CABLES THAT WILL REMAIN IN PLACE. THE CONTRACTOR WILL REPAIR, TO THE SATISFACTION OF THE ENGINEER, ANY DAMAGES HE CAUSES.

THE THICKNESS OF THE PAVEMENTS TO BE REMOVED ARE SHOWN IN THE TYPICAL SECTIONS ABOVE.

WHERE THE PAVEMENT TO BE REMOVED ABUTTS EXISTING PAVEMENT TO REMAIN IN PLACE, THE CONTRACTOR WILL SAW THE PAVEMENT TO FULL DEPTH ALONG THE LIMIT OF THE PAVEMENT TO BE REMOVED. THE SAW LINES SHALL BE MARKED AND SHALL BE AN EXTENSION OF THE EXISTING EDGE OF PAVEMENT. THE SAW CUT SHALL BE STRAIGHT AND NOT VARY MORE THAN 1/2"± FROM THE MARKED LINE. ANY PAVEMENT TO REMAIN IN PLACE DAMAGED BY THE CONTRACTOR WILL BE REPLACED TO THE SATISFACTION OF THE ENGINEER. THE COST FOR SAWING THE PAVEMENT SHALL BE INCLUDED IN THE BID PRICE FOR EACH RESPECTIVE PAVEMENT REMOVAL, OR REMOVAL & REPLACEMENT ITEM.



PAVEMENT REMOVAL NOTES

THE EXISTING CONCRETE PAVEMENT AREAS DENOTED ON TAXIWAY "G" FOR REMOVAL SHALL BE REMOVED AND DISPOSED OF OFF SITE IN ACCORDANCE WITH ITEM AR501900 - REMOVE PCC PAVEMENT.

THE EXISTING PAVEMENT OF TAXIWAY "C" WILL BE REMOVED AND DISPOSED OFF SITE IN ACCORDANCE TO ITEM AR401900 - REMOVE BITUMINOUS PAVEMENT.

A PORTION OF THE ARFF ROAD WILL REQUIRE REMOVAL TO PROVIDE ACCESS TO CONSTRUCT TAXIWAY C AND THE ASSOCIATED ITEMS. THE AREA IS AS SHOWN ABOVE. FOLLOWING THE CONSTRUCTION OF TAXIWAY C, THE ELECTRICAL, AND DRAINAGE IMPROVEMENTS, THE ENTRANCE WILL BE REPLACED WITH A PAVEMENT STRUCTURE RESEMBLING THE CONFIGURATION THAT CURRENTLY EXISTS. A TYPE "D" JOINT SHALL BE CONSTRUCTED WHERE THE ARFF ROAD MEETS THE EDGE OF TAXIWAY "C". THE PAVEMENT JOINTS WILL BE RE-ESTABLISHED. THE REMOVAL AND REPLACEMENT OF THE PAVEMENT IN THIS AREA WILL BE COMPLETED IN ACCORDANCE WITH ITEM AR501910 - REMOVE & REPLACE PCC PAVEMENT.

A PORTION OF THE T-HANGAR ACCESS TAXIWAY WILL REQUIRE REMOVAL TO PROVIDE ACCESS TO CONSTRUCT TAXIWAY C AND THE ASSOCIATED ITEMS. THE AREA IS AS SHOWN ABOVE. FOLLOWING THE CONSTRUCTION OF TAXIWAY C, THE ELECTRICAL, AND DRAINAGE IMPROVEMENTS, THE ENTRANCE WILL BE REPLACED WITH A PAVEMENT STRUCTURE RESEMBLING THE CONFIGURATION THAT CURRENTLY EXISTS. THE REMOVAL AND REPLACEMENT OF THE PAVEMENT IN THIS AREA WILL BE COMPLETED IN ACCORDANCE WITH ITEM AR401910 - REMOVE & REPLACE BIT. PAVEMENT.

THE PROPOSED PAVEMENT REMOVALS, AND REMOVAL & REPLACEMENTS WILL BE PAID FOR UNDER THE FOLLOWING:

- ITEM AR401900 - REMOVE BITUMINOUS PAVEMENT, PER S.Y. (TAXIWAY "C")
- ITEM AR401910 - REMOVE & REPLACE BIT. PAVEMENT, PER S.Y. (T-HANGAR ACCESS TAXIWAY)
- ITEM AR501900 - REMOVE PCC PAVEMENT, PER S.Y. (TAXIWAY "G")
- ITEM AR501910 - REMOVE & REPLACE PCC PAVEMENT, PER S.Y. (A.R.F.F. ROAD)

LEGEND

- [Hatched Box] EXISTING PAVEMENT
- [Diagonal Hatched Box] PROPOSED BITUMINOUS REMOVAL
- [Cross-hatched Box] PROPOSED P.C.C. REMOVAL
- [Diagonal Hatched Box] PROPOSED BITUMINOUS REMOVE AND REPLACE
- [Cross-hatched Box] PROPOSED P.C.C. REMOVE AND REPLACE

REVISION	DATE	BY

DECATUR, ILLINOIS  
A.I.P. PROJ.: 3-17-00  
I.L. PROJ.: DEC-3588

LEG. Project No.	802-06TXD-0800
Drawn	R-111PRP.DWG
Scale	1"=50'
Date	02/08/06
LAYOUT	CCC 02/08/06
DRAWN	CCC 02/08/06
REVIEWED	WJM 03/31/06

**HANSON**  
Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62703-2888  
Offices Nationwide

TAXIWAY "C"  
RECONSTRUCTION  
PROPOSED PAVEMENT  
PREPARATION PLAN  
**9**  
9 of 56 sheets

**INSPECTION HOLES AND UNDERDRAIN REMOVAL**

EXISTING UNDERDRAINS THAT ARE LOCATED IN THE PROPOSED PAVEMENT SECTION OR THE 16" SOIL PROCESSING AREA WILL BE REMOVED.

EXISTING UNDERDRAINS THAT ARE LOCATED BENEATH THE 16" SOIL PROCESS AREA WILL BE EXCAVATED.

HOLES THAT REMAIN FROM REMOVING UNDERDRAIN PIPE AT MANHOLES OR PIPE SECTIONS WILL BE REPAIRED. THE REPAIR WILL RESTORE REINFORCING AND FULL WALL THICKNESS WITH AR610 STRUCTURAL CONCRETE.

ALONG THE LINE OF UNDERDRAIN REMOVAL, INSPECTION HOLES WILL BE REMOVED TO A DEPTH BENEATH THE SOIL PROCESS ELEVATION.

WHERE THE UNDERDRAIN PIPE IS EXCAVATED, P.E. TUBING OR BROKEN CLAY PIPE WILL NOT BE PERMITTED IN THE FILL USED FOR EMBANKMENTS.

WHERE THE UNDERDRAIN PIPE IS EXCAVATED BELOW THE SOIL PROCESSING DEPTH, THE EXCAVATION WILL BE BACKFILLED WITH A GRANULAR MATERIAL COMPLYING TO AR209 SPECIFICATIONS. LOOSE EARTH MATERIAL WILL BE REMOVED FROM THE EXCAVATION PRIOR TO PLACING THE AR209 GRANULAR MATERIAL.

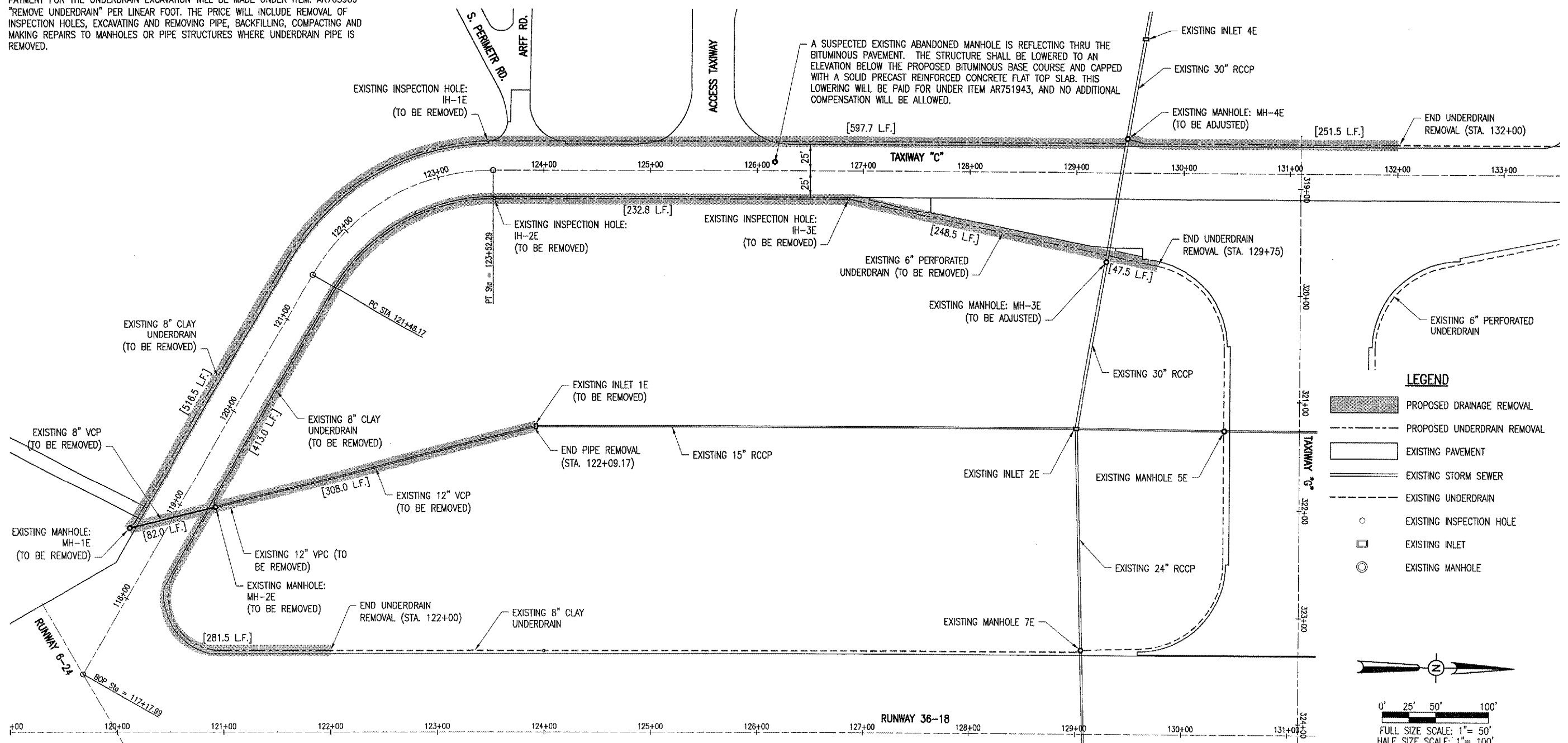
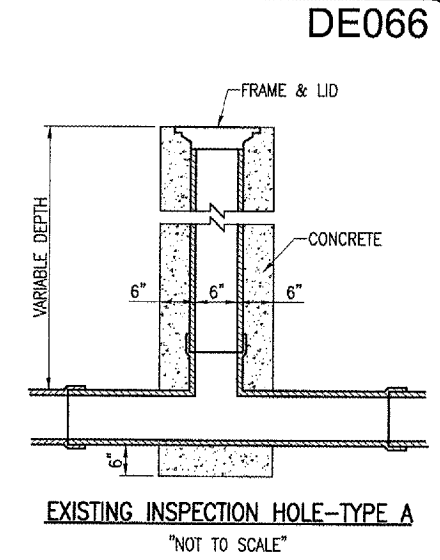
THE REMOVED INSPECTION HOLES AND PIPE MATERIAL WILL BE DISPOSED OFF AIRPORT SITE BY CONTRACTOR.

PAYMENT FOR THE UNDERDRAIN EXCAVATION WILL BE MADE UNDER ITEM: AR705900 "REMOVE UNDERDRAIN" PER LINEAR FOOT. THE PRICE WILL INCLUDE REMOVAL OF INSPECTION HOLES, EXCAVATING AND REMOVING PIPE, BACKFILLING, COMPACTING AND MAKING REPAIRS TO MANHOLES OR PIPE STRUCTURES WHERE UNDERDRAIN PIPE IS REMOVED.

DRAINAGE REMOVAL AND ADJUSTMENT QUANTITIES				
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR701900	REMOVE PIPE	L.F.	390	
AR705900	REMOVE UNDERDRAIN	L.F.	2,689	
AR751900	REMOVE INLET	EACH	1	
AR751903	REMOVE MANHOLE	EACH	2	
AR751943	ADJUST MANHOLE	EACH	3	

AR701900 "REMOVE PIPE"		
STRUCTURES	LOCATION	DISTANCE (L.F.)
MH-1E TO MH-2E	TXY "C" STA. 118+58 LT. TO TXY "C" STA. 119+15 RT.	82
MH-2E TO INLET-1E	TXY "C" STA. 119+15 RT. TO TXY "C" STA. 122+09.17 RT.	308

AR705900 "REMOVE UNDERDRAIN"		
STRUCTURES	LOCATION	DISTANCE (L.F.)
MH-1E TO IH-1E	TXY "C" STA. 118+58 LT. TO TXY "C" STA. 123+48 LT.	516.5
IH-1E TO MH-4E	TXY "C" STA. 123+48 LT. TO TXY "C" STA. 129+47 LT.	597.7
MH-4E TO 132+00	TXY "C" STA. 129+47 LT. TO TXY "C" STA. 132+00 LT.	251.5
122+00 TO MH-2E	RWY 36-18 STA. 122+00 RT. TO TXY "C" STA. 119+15 RT.	281.5
MH-2E TO IH-2E	TXY "C" STA. 119+15 RT. TO TXY "C" STA. 123+53 RT.	413.0
IH-2E TO IH-3E	TXY "C" STA. 123+53 RT. TO TXY "C" STA. 126+86 RT.	332.8
IH-3E TO MH-3E	TXY "C" STA. 126+86 RT. TO TXY "C" STA. 129+28 RT.	248.5
MH-3E TO 129+75	TXY "C" STA. 129+28 RT. TO TXY "C" STA. 129+75 RT.	47.5



**LEGEND**

- PROPOSED DRAINAGE REMOVAL
- PROPOSED UNDERDRAIN REMOVAL
- EXISTING PAVEMENT
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING INSPECTION HOLE
- EXISTING INLET
- EXISTING MANHOLE

0' 25' 50' 100'

FULL SIZE SCALE: 1" = 50'  
HALF SIZE SCALE: 1" = 100'

DECATUR AIRPORT <b>DECATUR, ILLINOIS</b>	DATE: _____ REVISION: _____ BY: _____	ILL. PROJECT NO.: 802-06TYXD-0800 DRAWING NO.: R-131DRN.DWG SCALE: 1"=50' DATE: 02/08/06 LAYOUT: CCC 02/08/06 DRAWN: CCC 02/08/06 REVIEWED: WJM 03/31/06
<p><b>HANSON</b>                  Hanson Professional Services Inc.                  1525 South Sixth Street                  Springfield, Illinois 62703-2886                  Offices Nationwide</p>	<p><b>TAXIWAY "C" RECONSTRUCTION</b></p> <p>DRAINAGE REMOVAL PLAN</p>	<p><b>10</b></p> <p>10 of 66 sheets</p>

ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR701512	12" RCP, CLASS IV	L.F.	313	
AR705411	POROUS BACKFILL No. 1	C.Y.	448	
AR705528	6" PERFORATED UNDERDRAIN W/SOCK	L.F.	2,503	
AR705630	UNDERDRAIN INSPECTION HOLE	EACH	8	
AR751410	INLET	EACH	1	
AR751540	MANHOLE 4'	EACH	2	
AR751943	ADJUST MANHOLE	EACH	3	

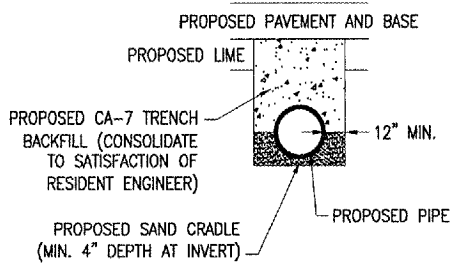
STRUCTURE	TOP ELEV.	FLOW ELEV.	NORTHING	EASTING	STATION	OFFSET	TYPE	WORK AREA
IH-1P	672.10	669.89	1143858.9706	842977.4946	*122+00	77' LT.	A	1
IH-2P	671.98	669.77	1143656.8200	842892.3316	118+26	27.75' LT.	A	1
IH-3P	671.64	668.77	1144002.6790	842502.3627	123+45	27.75' LT.	A	2
IH-4P	671.40	668.35	1144005.6637	842582.8074	123+45	52.75' RT.	A	2
IH-5P	671.82	668.95	1144083.6966	842502.1843	124+25	27.75' LT.	A	3
IH-6P	672.06	669.25	1144363.6906	842582.8138	127+05	52.75' RT.	A	3
IH-7P	672.44	670.20	1144633.6806	842616.6770	129+75	86.5' RT.	A	3
IH-8P	673.42	670.50	1144858.6118	842501.9312	132+00	27.75' LT.	A	3

\* DENOTES STATIONING AND OFFSET BASED ON RUNWAY 18/36

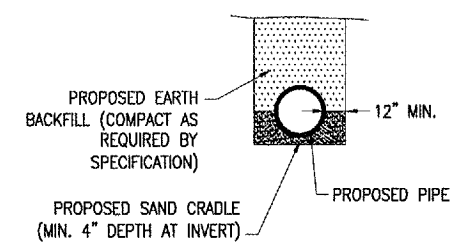
STRUCTURE	TOP ELEV.	FLOW ELEV.	NORTHING	EASTING	STATION	OFFSET	TYPE	WORK AREA
MH-1P	671.09	667.50	1143749.9943	842726.7309	120+16	30' LT.	4'	1
MH-2P	670.71	667.08	1143823.5749	842769.2855	120+16	55' RT.	4'	1

STRUCTURE	PROPOSED TOP ELEV.	EXISTING TOP ELEV.	EXISTING FLOW ELEV.	NORTHING	EASTING	STATION	OFFSET	WORK AREA
MH-4E	672.70	672.49	660.48	1152398.7190	831807.3726	129+47.83	31.94' LT.	3

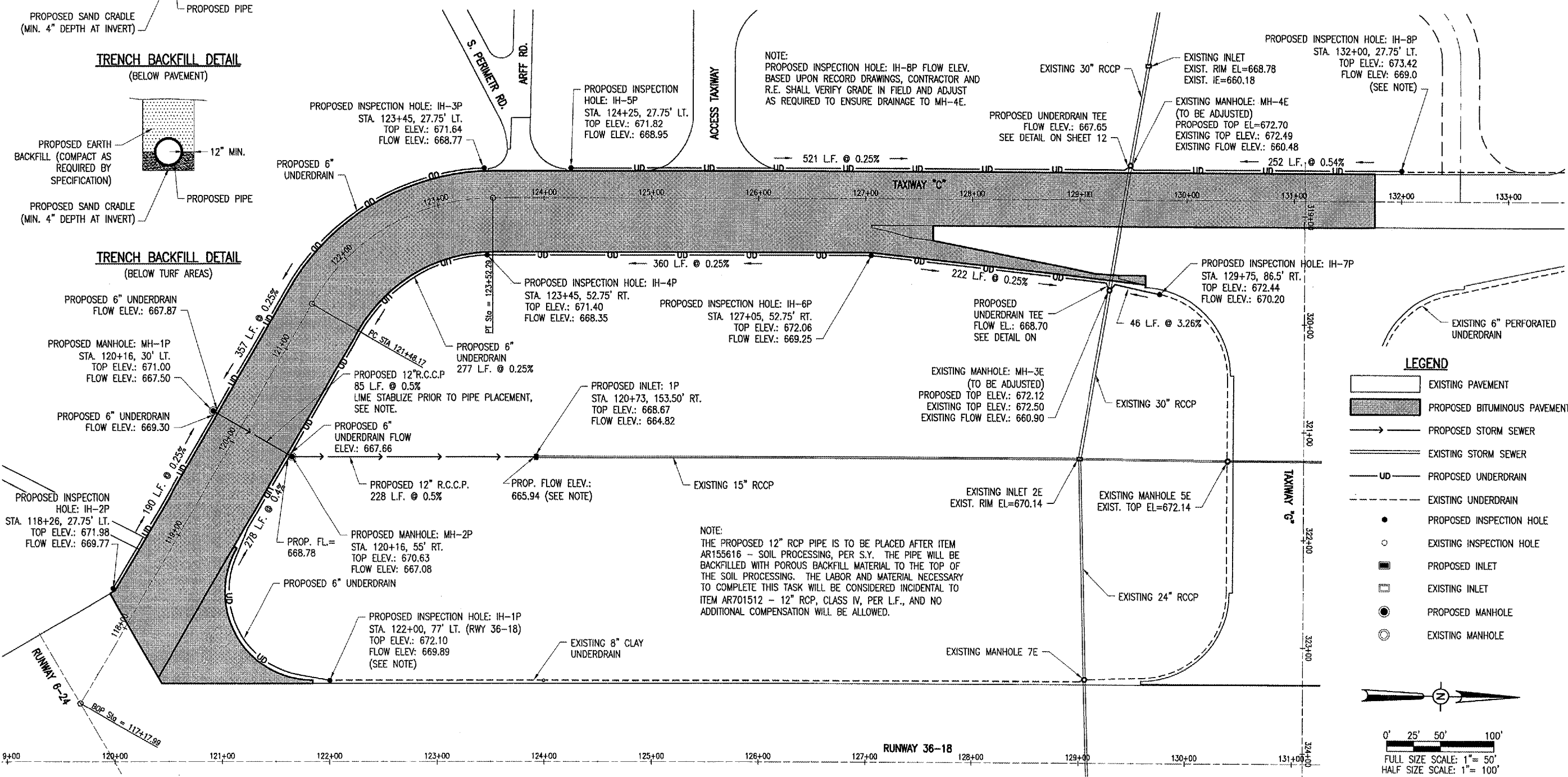
STRUCTURE	TOP ELEV.	FLOW ELEV.	NORTHING	EASTING	STATION	OFFSET	TYPE	WORK AREA
1P	668.67	664.82	1151846.3511	832083.9211	123+92.54	239.62' RT.	A	1



TRENCH BACKFILL DETAIL (BELOW PAVEMENT)



TRENCH BACKFILL DETAIL (BELOW TURF AREAS)



DATE	REVISION

**DECATUR, ILLINOIS**

DECATUR AIRPORT

PROJECT: 3-17-00

Project No.	802-06199D.0800
Drawn	R-132DRN.DWG
Scale	1"=50'
Date	02/08/06
LAYOUT	CCC
DRAWN	CCC
REVIEWED	WJM
DATE	02/08/06
DATE	02/08/06
DATE	03/31/06

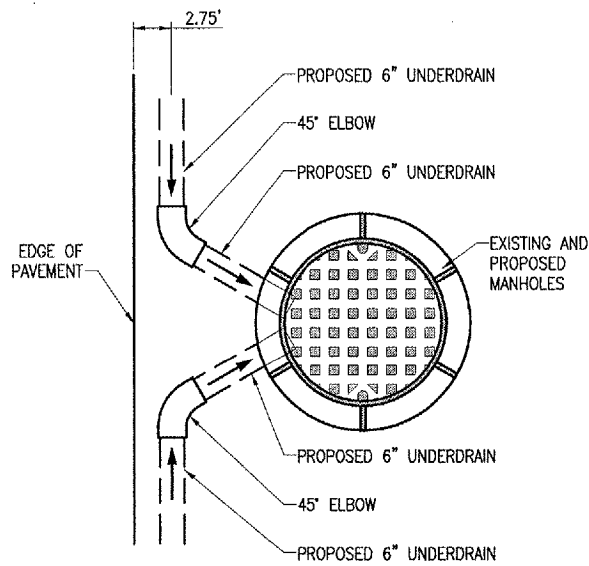
**HANSON**

Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62703-2886  
Offices Nationwide

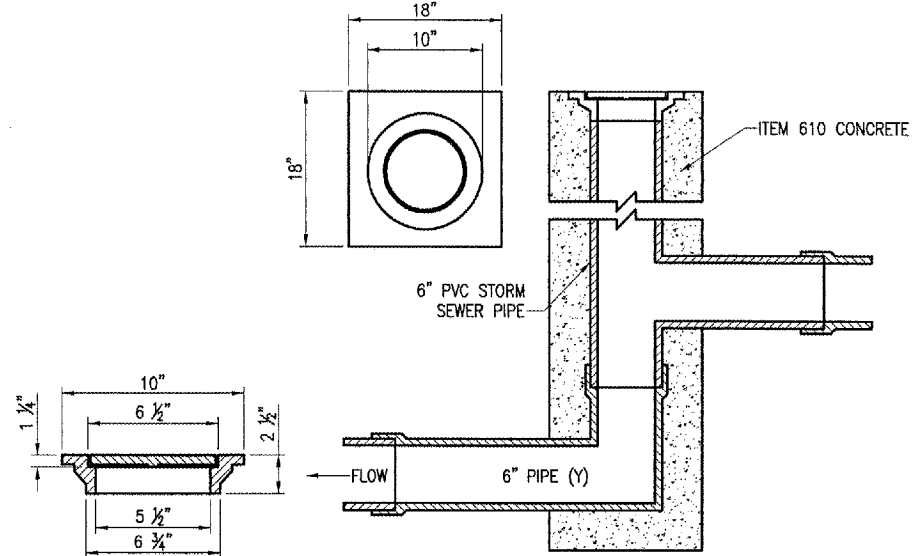
**TAXIWAY "C" RECONSTRUCTION**

PROPOSED DRAINAGE PLAN

MAY 01, 2006 2:03 PM RAW  
I:\AIRPORTS\DECATUR\802-06199D\PROJECT\DRG\802-06199D.DWG - PROPOSED



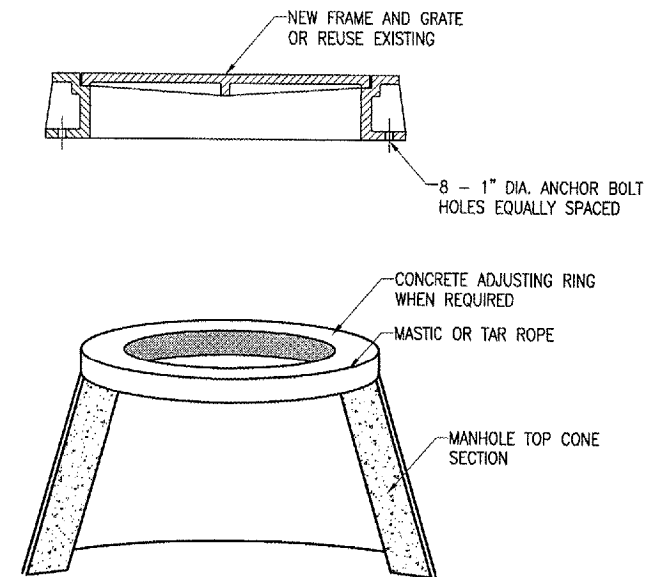
**UNDERDRAIN CONNECTION DETAIL**  
"NOT TO SCALE"



**CAST IRON FRAME AND COVER**

NEENAH R-6013, DEETER 1810,  
EAST JORDAN 2790-6 OR  
APPROVED EQUAL

**INSPECTION HOLE**  
TYPE A



**ADJUST MANHOLE DETAIL**  
"NOT TO SCALE"

**INSPECTION HOLE NOTES**

DIAMETER OF PIPE AS SPECIFIED.  
1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF INSPECTION HOLES.  
THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE (NON-REINFORCED).  
THE INSPECTION FRAME AND LID SHALL BE EAST JORDAN 2790-6, NEENAH R-6013 OR DEETER 1810.  
THE PROPOSED INSPECTION HOLE WILL BE PAID FOR UNDER THE FOLLOWING:  
ITEM AR705630 "UNDERDRAIN INSPECTION HOLE"

**INLET NOTES**

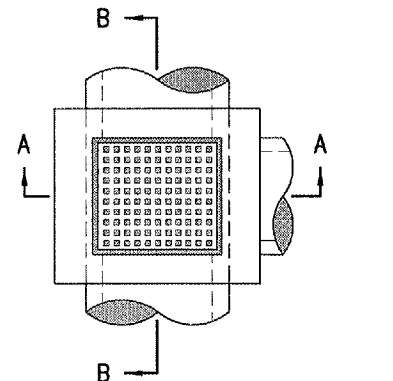
INLET TO BE CONSTRUCTED OF STRUCTURAL P.C. CONCRETE. THE CONTRACT UNIT PRICE PER INLET SHALL INCLUDE THE FRAME, GRATE AND STEPS IN PLACE AND COMPLETE PER UNIT.  
1/2" CHAMFER TO BE USED ON ALL EXPOSED CORNERS OF INLETS. BARS TO BE INSTALLED 2" FROM FACE OF WALLS.  
THE FRAME AND GRATE SHALL BE NEENAH R-3475-E, DEETER 2425-E.  
INLET STEPS SHALL BE NEENAH R-1980-1. 12" TO 15" C.C. STEPS TO BE INSTALLED WHEN Y IS GREATER THAN 5'. THE COST OF THE STEPS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EACH INLET.

**MANHOLE NOTES**

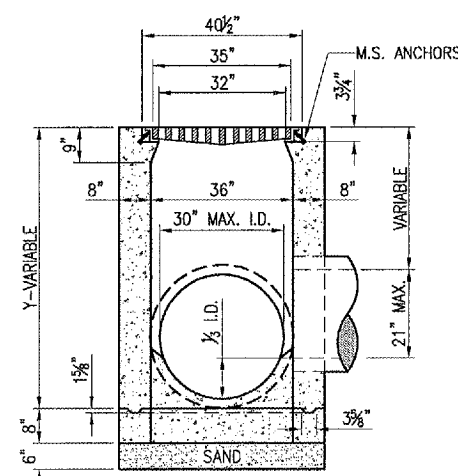
MANHOLE TO BE CONSTRUCTED OF STRUCTURAL P.C. CONCRETE. THE CONTRACT UNIT PRICE PER MANHOLE SHALL INCLUDE THE FRAME, LID AND STEPS PER UNIT.  
THE FRAME AND LID SHALL BE NEENAH R-3492 OR EQUAL, ANCHORED TO THE MANHOLE STRUCTURE.  
MANHOLE STEPS SHALL BE NEENAH R-1980-1. 12" TO 15" C.C. STEPS TO BE INSTALLED WHEN Y IS GREATER THAN 5'. THE COST OF THE STEPS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EACH MANHOLE.  
THE PROPOSED MANHOLES WILL BE PAID FOR UNDER THE FOLLOWING:  
ITEM AR751540 "MANHOLE 4"

**ADJUSTED MANHOLE NOTES**

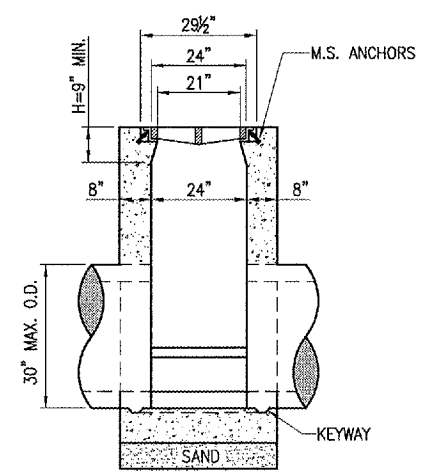
THE ADJUSTMENTS NECESSARY FOR THE CHANGE IN MANHOLE TOP ELEVATIONS VARIES BY LOCATION. TO RAISE THE ELEVATION A CONCRETE MANHOLE ADJUSTING RING CAN BE USED TO OBTAIN THE PROPER ELEVATION FOR THE TOP OF THE LID ELEVATION. EXISTING CONCRETE MANHOLE ADJUSTING RINGS EXIST ON THE MANHOLE AT STA. 129+28 RT, TO BE LOWERED, AND THE ADJUSTMENT WILL BE ACCOMPLISHED BY REMOVING THE EXISTING RINGS. THE SUSPECTED EXISTING ABANDONED MANHOLE AT STA. 126+20 SHALL BE LOWERED BY REMOVING THE MANHOLE WALL AND CAPPING IT WITH A SOLID PRECAST REINFORCED CONCRETE FLAT TOP SLAB, AT AN ELEVATION BENEATH THE PROPOSED BITUMINOUS BASE COURSE.  
THE PROPOSED CONCRETE ADJUSTING RING SHALL CONFORM TO THE REQUIREMENTS OF ASTM C478. THE RING DIAMETER WILL MATCH THE TOP OF THE CONE SECTION OF THE MANHOLE, AND BE OF EQUAL WALL THICKNESS TO THE STRUCTURE. THE RING WILL BE SECURED WITH MASTIC OR TAR ROPE.  
IF CASTING LEVELING IS NECESSARY, MORTAR LEVELING BETWEEN RING AND CASTING WILL BE ALLOWED. THE MORTAR SHALL CONSIST OF ONE PART PORTLAND CEMENT AND TWO PARTS SAND. THE PORTLAND CEMENT SHALL CONFORM TO THE REQUIREMENTS OF ASTM C150, TYPE 1. THE SAND SHALL CONFORM TO THE REQUIREMENTS OF ASTM C144.  
THE PROPOSED MANHOLE ADJUSTMENTS WILL BE PAID FOR UNDER THE FOLLOWING:  
ITEM AR751943 - ADJUST MANHOLE, PER EACH.



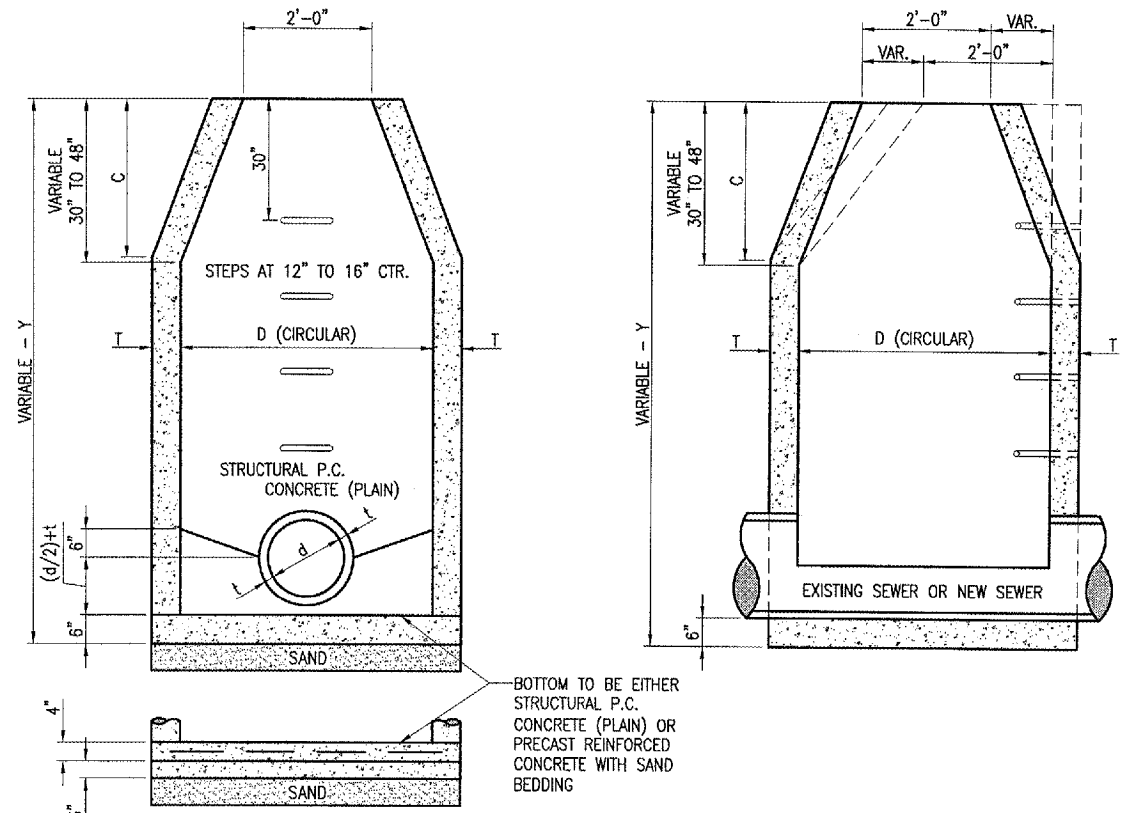
**REINFORCING STEEL BAR TYPE**



**SECTION A-A**



**SECTION B-B**



**REINFORCING BARS SCHEDULE**

ALT. MATERIAL FOR WALLS	D	C	T	D	C	T
PRECAST REINFORCED CONC. RINGS	4'	2'-6"	5"	5'	4'-0"	6"
MONOLITHIC CONCRETE	4'	2'-6"	6"	5'	4'-0"	8"

**MANHOLE DETAILS**  
"NOT TO SCALE"

**REINFORCING BARS SCHEDULE**

TYPE	QUANTITY PER INLET	DIMENSIONS		SIZE	APPROX. WT. OF BARS IN INLET
		A	B		
A	2	3'-4"	2'-4"	#5	16.7

**INLET DETAILS - TYPE "A"**  
"NOT TO SCALE"

APR 27, 2006 11:02 PM RAW  
I:\AIRPORTS\DECATUR\802-067X\AIRPORT\_SHEETS\A-R-031DRN.DWG - Drainage Details

BY: \_\_\_\_\_  
REVISION: \_\_\_\_\_  
DATE: \_\_\_\_\_

PROJECT NO. 802-067X/D 0800  
SHEET NO. R-53/DRN.DWG  
SCALE: N/A  
DATE: 02/15/06

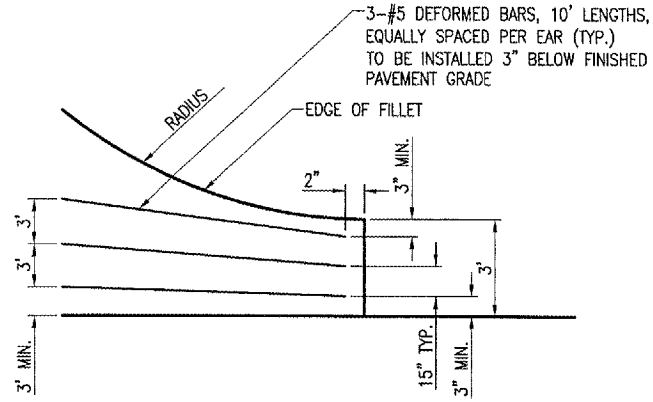
LAYOUT: CCC 02/16/06  
DRAWN: CCC 02/16/06  
REVIEWED: WJM 03/31/06

**HANSON**  
Hanson Professional Services Inc.  
1528 South Sixth Street  
Springfield, Illinois 62703-2886  
Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**

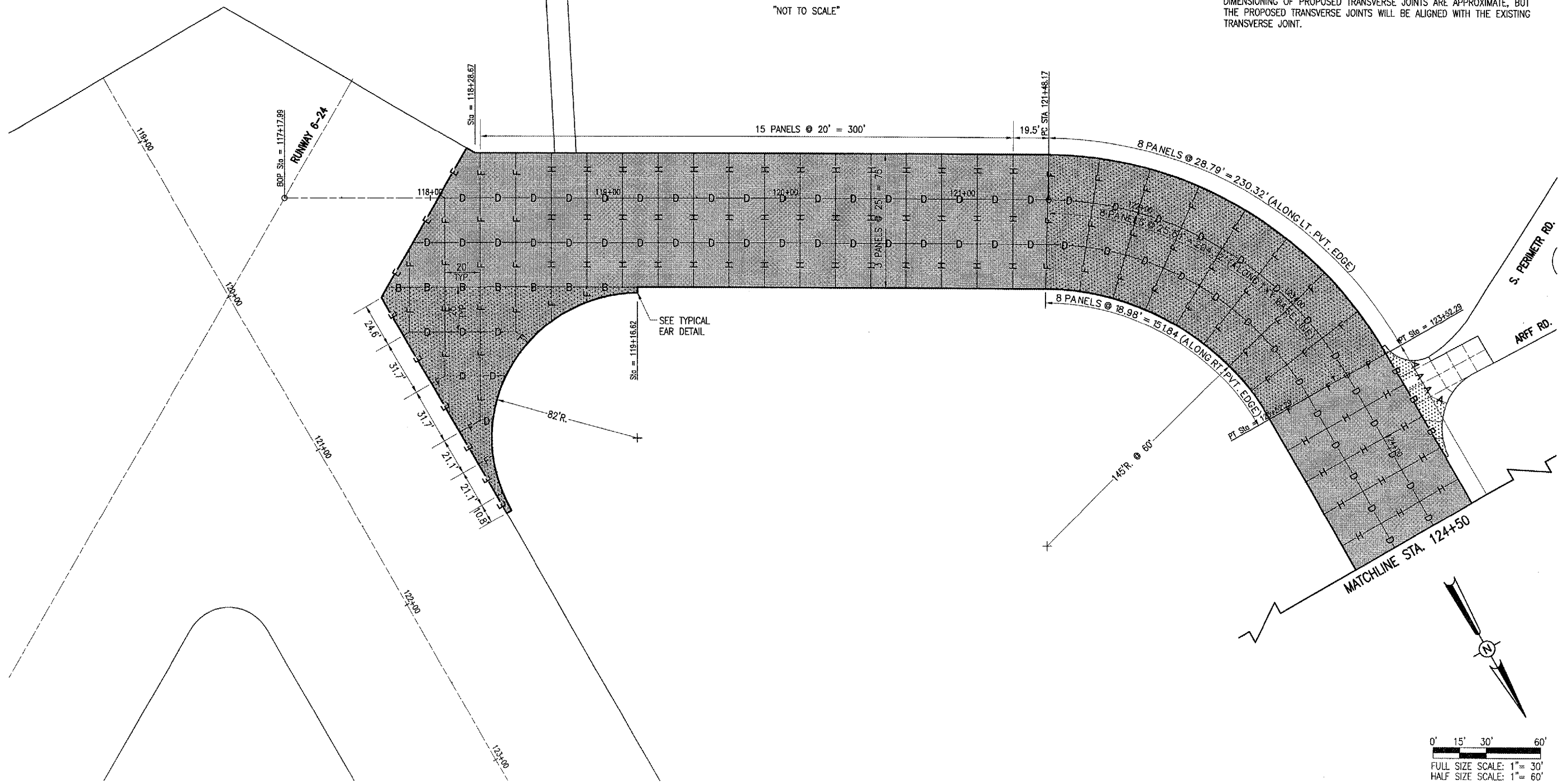
**DRAINAGE DETAILS**

12  
12 of 66 sheets



TYPICAL EAR DETAIL  
"NOT TO SCALE"

- LEGEND**
- PROPOSED CONCRETE PAVEMENT
  - EXISTING PAVEMENT
  - PAVEMENT AREA TO BE REINFORCED WITH WIRE MESH 6"x6" (W5xW5)
  - CONSTRUCTION JOINT WITH DOWELS - TYPE D
  - LONGITUDINAL CONTRACTION JOINT WITH TIE BARS - TYPE G HINGED
  - TRANSVERSE CONTRACTION JOINT WITH DOWELS - TYPE F
  - TRANSVERSE CONTRACTION JOINT WITHOUT DOWELS - TYPE H
  - THICKENED EDGE EXPANSION JOINT - TYPE B
  - THICKENED EDGE EXPANSION JOINT - TYPE E
  - DOWELED EXPANSION JOINT - TYPE A
- NOTE:**  
DIMENSIONING OF PROPOSED TRANSVERSE JOINTS ARE APPROXIMATE, BUT THE PROPOSED TRANSVERSE JOINTS WILL BE ALIGNED WITH THE EXISTING TRANSVERSE JOINT.



APR 27, 2006 11:02 PM RAW  
I:\AIRPORTS\DECATUR\802-06TAXI\AIRPORT\SHEETS\R-161JUNT.DWG - Sheet 13

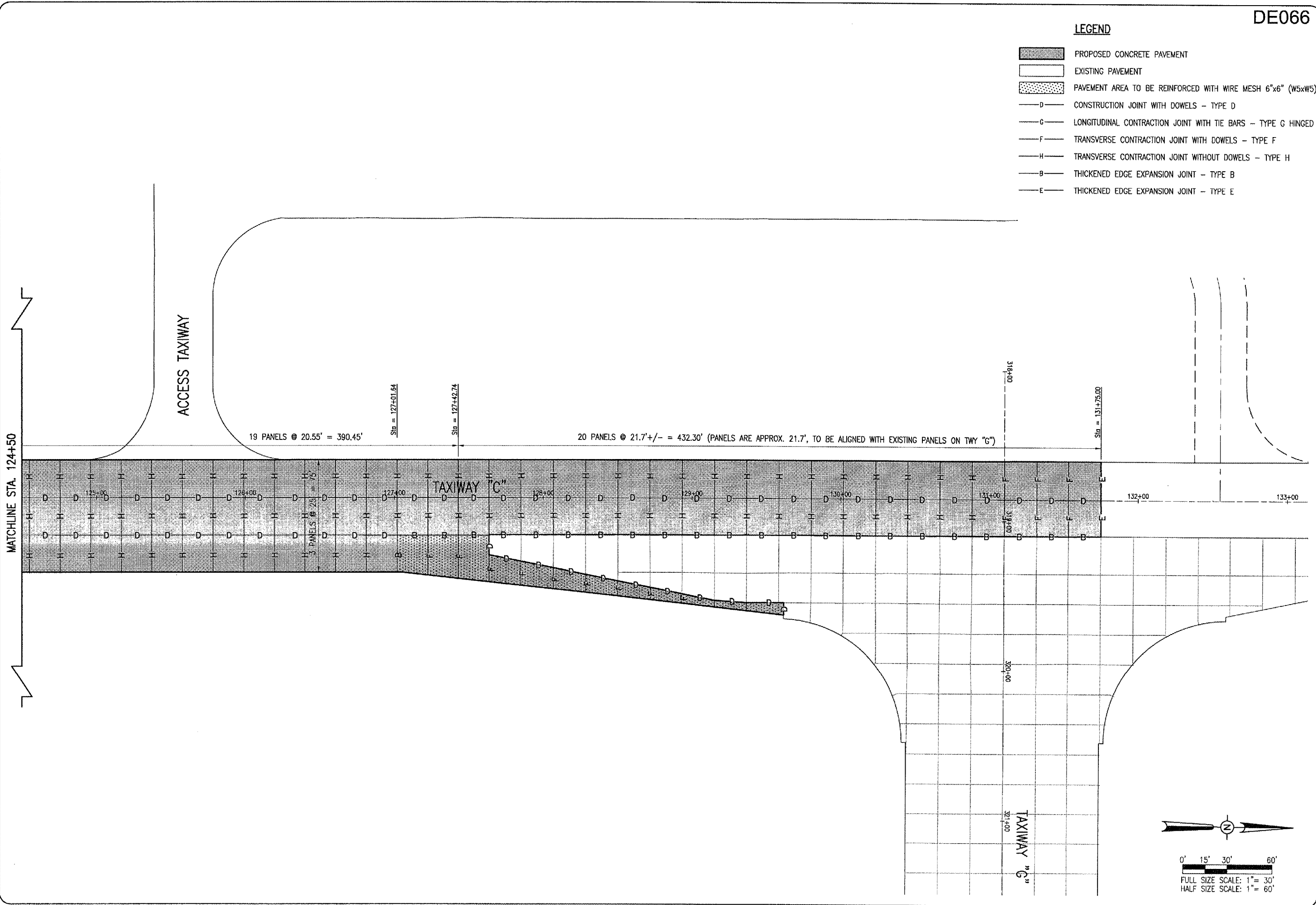
DATE	REVISION	BY

**DECATUR, ILLINOIS**  
A.I.P. PROJ.: 3-17-00  
IL PROJ.: DEC-3588

802-06TAXI.DWG	03/08/06
R-161JUNT.DWG	03/08/06
1"=30'	03/08/06
03/08/06	03/31/06

**HANSON**  
Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62705-2886  
Offices Nationwide

TAXIWAY "C"  
RECONSTRUCTION  
PROPOSED JOINTING PLAN  
STA. 118+04.56 TO STA. 124+50



**LEGEND**

- PROPOSED CONCRETE PAVEMENT
- EXISTING PAVEMENT
- PAVEMENT AREA TO BE REINFORCED WITH WIRE MESH 6"x6" (W5xW5)
- CONSTRUCTION JOINT WITH DOWELS - TYPE D
- LONGITUDINAL CONTRACTION JOINT WITH TIE BARS - TYPE G HINGED
- TRANSVERSE CONTRACTION JOINT WITH DOWELS - TYPE F
- TRANSVERSE CONTRACTION JOINT WITHOUT DOWELS - TYPE H
- THICKENED EDGE EXPANSION JOINT - TYPE B
- THICKENED EDGE EXPANSION JOINT - TYPE E

DE066

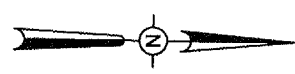
DATE	REVISION	BY

**DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 IL. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

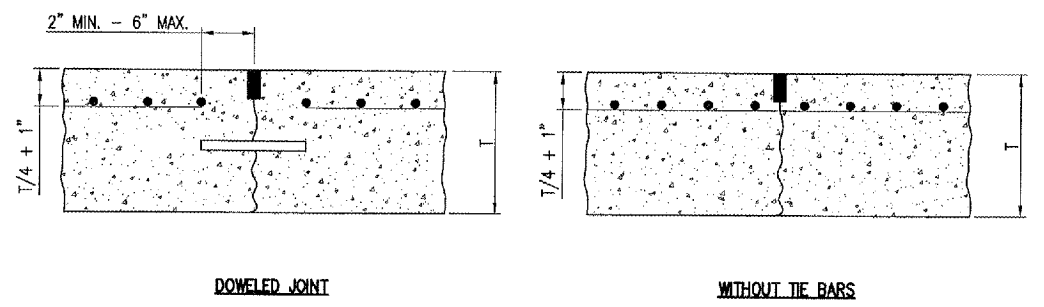
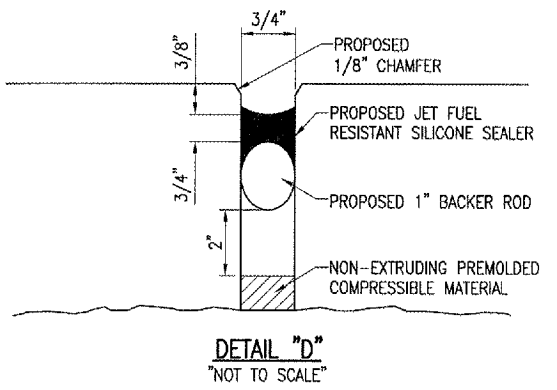
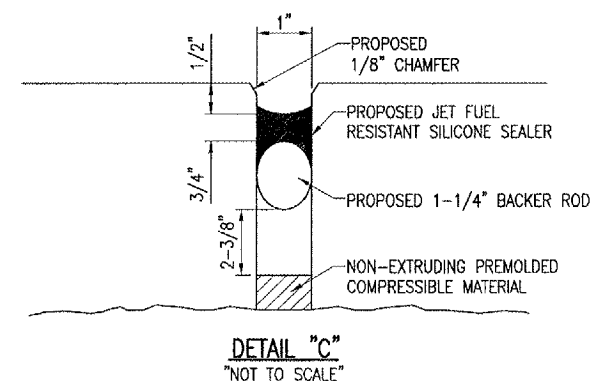
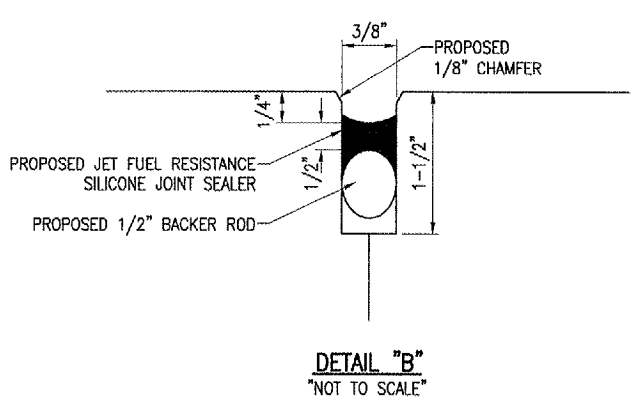
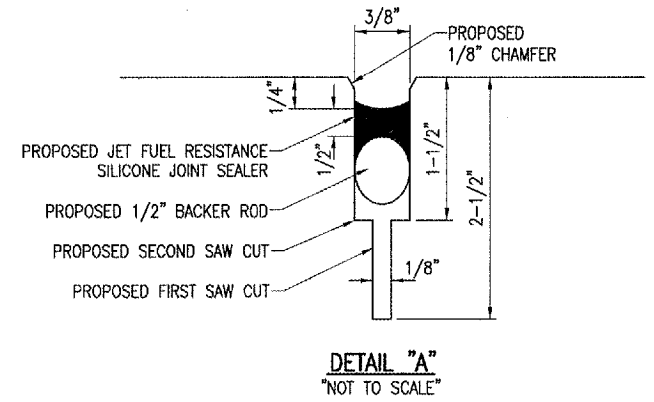
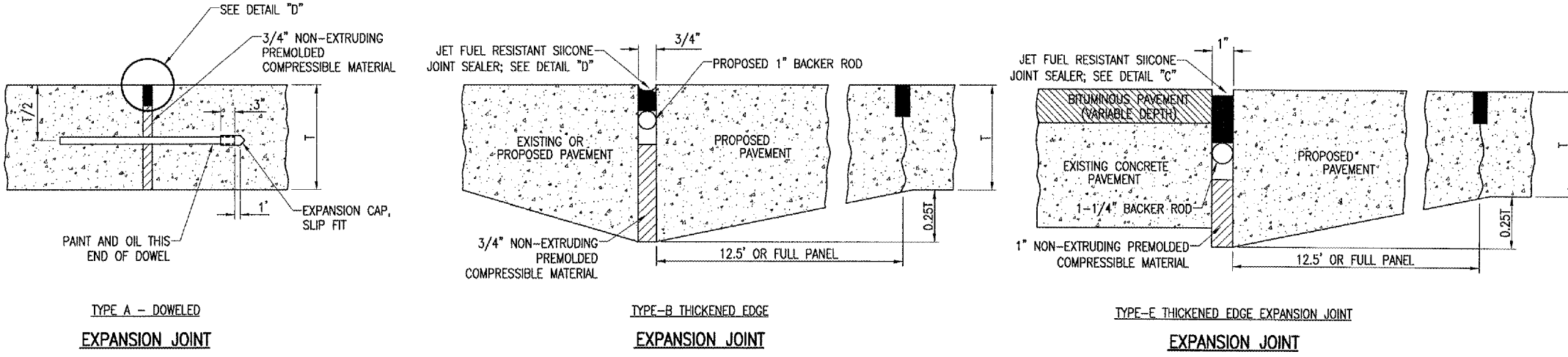
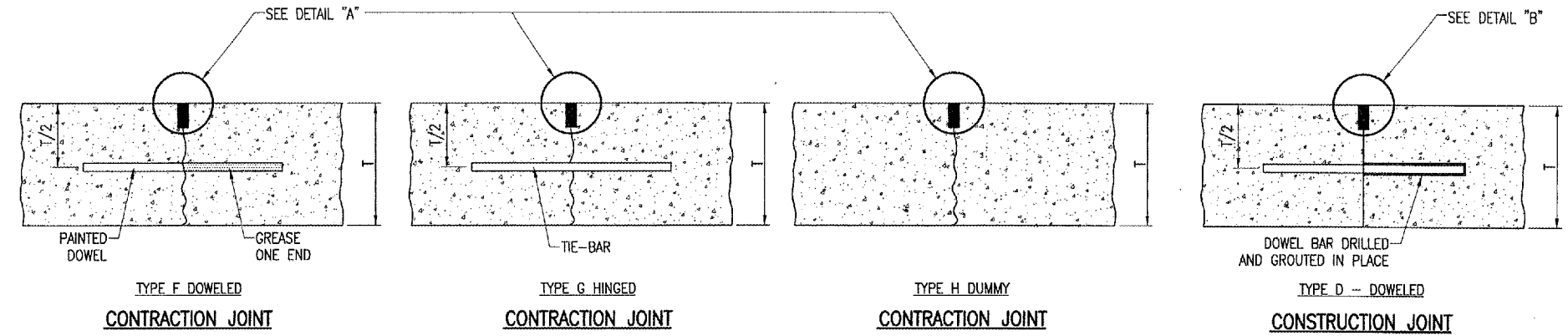
Proj. No. 802-06TXXD.0800	Scale 1"=30'	Date 03/08/06
Drawn by R-161JUNT.DWG	Checked by CCC	Reviewed by WJM
Drawn Date 03/08/06	Drawn by BKB	Reviewed Date 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62705-2886  
 Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**  
 PROPOSED JOINTING PLAN  
 STA. 124+50 TO STA. 131+75



0' 15' 30' 60'  
 FULL SIZE SCALE: 1" = 30'  
 HALF SIZE SCALE: 1" = 60'



**JOINTING NOTES**

ALL EXPOSED JOINT EDGES SHALL BE CHAMFERED ACCORDING TO THE JOINT DETAILS ON THIS SHEET.

ALL LONGITUDINAL AND TRANSVERSE CONTRACTION AND CONSTRUCTION JOINTS SHALL BE SAWEED.

ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. DOWELS SHALL BE 1-1/4" DIA., 20" LENGTH AND 15" SPACING.

ALL DOWELS ARE TO BE PLACED AT A POINT NOT TO EXCEED 7-1/2" INCHES OR CLOSER THAN 6" FROM A LONGITUDINAL OR TRANSVERSE JOINT.

ALL TIE-BARS SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.

ALL TIE-BARS SHALL BE PLACED AT A POINT NOT EXCEEDING 15" OR CLOSER THAN 6" FROM A TRANSVERSE, CONTRACTION, EXPANSION, OR CONSTRUCTION JOINT AND SPACED 30" ON CENTERS AND SHALL BE NO. 5 DEFORMED BARS, 30" IN LENGTH.

DOWELS IN TRANSVERSE, CONTRACTION, AND CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.

DOWELS LOCATED IN THE CONSTRUCTION JOINTS WILL BE DRILLED AND GROUTED IN PLACE.

ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE +1/8" FOR CONSTRUCTION JOINTS AND +1/4" FOR CONTRACTION JOINTS.

THE COST OF ALL DOWEL BARS, TIE-BARS, PAVING MESH, SAWING, NEOPRENS AND SEALING SHALL BE INCLUDED IN THE COST OF THE P.C.C. PAVEMENT.

PRIOR TO PLACING ADJACENT PAVEMENT SECTIONS, THE VERTICAL EDGE SHALL BE CHECKED FOR TRUENESS, IF THE FACE IS BURRED OR IRREGULAR, THE CONTRACTOR SHALL GRIND, STONE OR SAW THE FACE TO PRODUCE A SMOOTH AND STRAIGHT EDGE.

WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL ROPE THE TRANSVERSE JOINTS.

JOINT SEALANT SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS ITEM 501-2.5.

CURING COMPOUND WILL BE AS SPECIFIED IN THE SPECIFICATIONS, ITEM 501-3.17 AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE ENGINEER.

ALL NON-ALIGNED EDGES WILL BE SAWEED FULL DEPTH.

WIRE MESH IS TO BE SECURELY HELD IN PLACE BY MEANS OF SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING OR SAGGING DURING AND AFTER THE CONCRETE PLACEMENT. SUPPORT PINS ARE TO BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.

PRIOR TO PLACING MATERIAL HAULING CONSTRUCTION EQUIPMENT ON THE NEW PAVEMENT, THE PSI OF THE PAVEMENT IS TO REACH 3000 POUNDS STRENGTH.

BY	
REVISION	
DATE	

DECATUR, ILLINOIS

DECATUR AIRPORT

IL PROJ.: DEC-3588

A.I.P. PROJ.: 3-17-00

HEL Project No.	802-06TXD 0800
Filename	R-561.JNT.DWG
Scale	N/A
Date	03/18/05
LAYOUT	CCC 03/18/05
DRAWN	CCC 03/18/05
REVIEWED	WJM 03/31/06

**HANSON**

Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62703-2886  
Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**

PROPOSED PCC PAVEMENT JOINTING DETAILS

APR 27 11:04 PM RAW  
I:\AIRPORTS\DECATUR\802-06TXD\AIRPORT SHEETS\R-561.JNT.DWG - Sheet 15

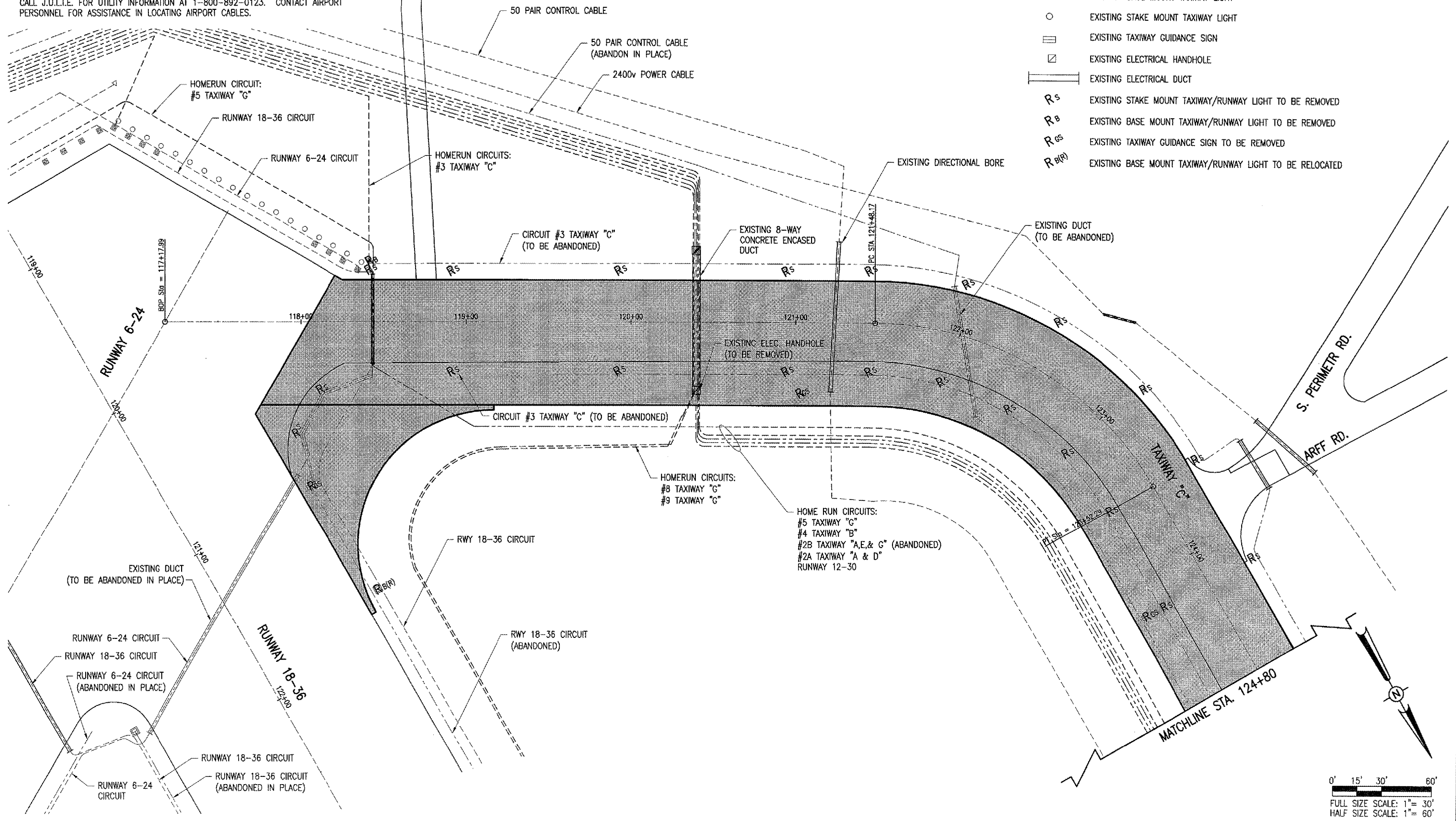
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES.

QUANTITIES - REMOVAL				
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR110906	REMOVE ELECTRICAL HANDHOLE	EACH	1	
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	36	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	4	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	3	
AR125961	RELOCATE STAKE MOUNTED LIGHT	EACH	4	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	2	
AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	1	

**LEGEND**

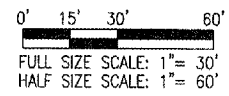
- PROPOSED CONCRETE PAVEMENT
- EXISTING PAVEMENT
- EXISTING ELECTRICAL CABLE
- EXISTING ELECTRICAL CABLE TO BE ABANDONED/CURRENTLY ABANDONED
- EXISTING BASE MOUNT RUNWAY LIGHT
- EXISTING BASE MOUNT TAXIWAY LIGHT
- EXISTING STAKE MOUNT TAXIWAY LIGHT
- EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL DUCT
- EXISTING STAKE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
- EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
- EXISTING TAXIWAY GUIDANCE SIGN TO BE REMOVED
- EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE RELOCATED



<p>DATE</p> <p>REVISION</p>	<p>BY</p> <p>DATE</p>
<p><b>HANSON</b> Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide</p>	
<p>PROJECT No. 802-061X10_0800                  ENGINEER R-141ELE.DWG                  SCALE 1"=30'                  DATE 02/24/06</p>	
<p>LAYOUT CCC 02/24/06                  DRAWN CCC 02/24/06                  REVIEWED WJM 03/31/06</p>	
<p><b>TAXIWAY "C" RECONSTRUCTION</b></p> <p>EXISTING ELECTRICAL PLAN                  STA. 117+99 TO STA. 124+80</p>	
<p><b>16</b></p> <p>16 of 58 sheets</p>	


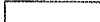
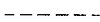







APR 27, 2005 11:05 PM RAW  
 I:\AIRPORTS\DECATUR\802-061X10\AIRPORT\SHEETS\141ELE.DWG - Sta. 117+99 to Sta. 124+80

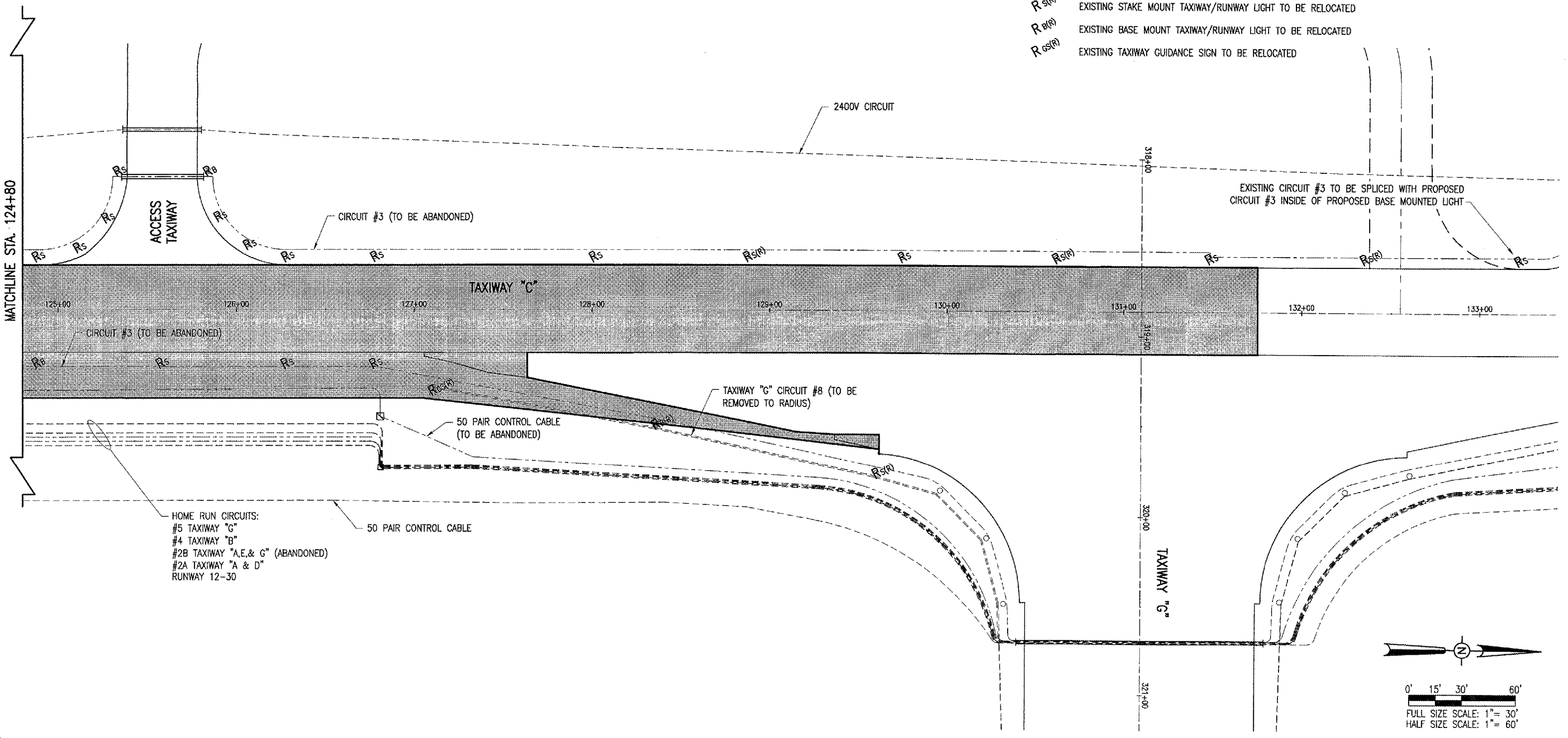
DECATUR AIRPORT  
 DECATUR, ILLINOIS  
 A.I.P. PROJ.: 3-17-00  
 I.L. PROJ.: DEC-3588







LEGEND

-  PROPOSED CONCRETE PAVEMENT
-  EXISTING PAVEMENT
-  EXISTING ELECTRICAL CABLE
-  EXISTING ELECTRICAL CABLE TO BE ABANDONED/CURRENTLY ABANDONED
-  EXISTING BASE MOUNT RUNWAY LIGHT
-  EXISTING BASE MOUNT TAXIWAY LIGHT
-  EXISTING STAKE MOUNT TAXIWAY LIGHT
-  EXISTING TAXIWAY GUIDANCE SIGN
-  EXISTING ELECTRICAL HANDHOLE
-  EXISTING ELECTRICAL DUCT
- $R_s$  EXISTING STAKE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
- $R_B$  EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE REMOVED
- $R_{s(r)}$  EXISTING STAKE MOUNT TAXIWAY/RUNWAY LIGHT TO BE RELOCATED
- $R_{B(r)}$  EXISTING BASE MOUNT TAXIWAY/RUNWAY LIGHT TO BE RELOCATED
- $R_{GS(r)}$  EXISTING TAXIWAY GUIDANCE SIGN TO BE RELOCATED



APR 27, 2006 11:04 PM RAW  
 I:\AIRPORTS\DECATUR\802-06TAXI\AIRPORT\SHEETS\R-141ELE.DWG - Sta. 124+80 to Sta. 133+21

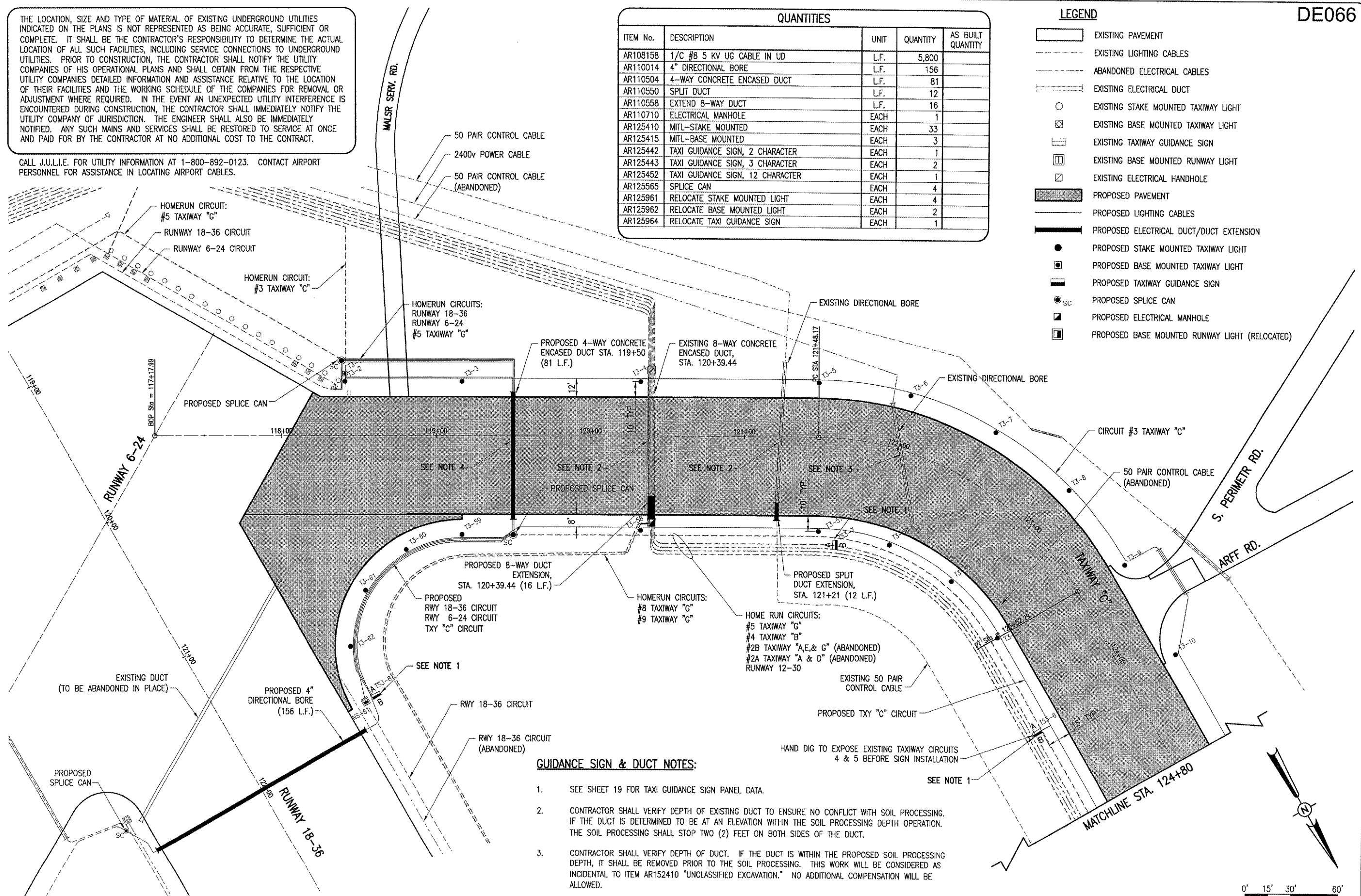
	BY							
	REVISION							
	DATE							
 <b>DECATUR, ILLINOIS</b>		I.L. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00						
I.S.I. Project No. 802-06TAXI.DWG Drawing R-141ELE.DWG Scale 1"=30' Date 02/24/06		LAYOUT CCC 02/24/06 DRAWN CCC 02/24/06 REVIEWED WJM 03/31/06						
 <b>HANSON</b> Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2988 Offices Nationwide		<b>TAXIWAY "C"</b> <b>RECONSTRUCTION</b> EXISTING ELECTRICAL PLAN STA. 124+80 TO STA. 133+21						
<b>17</b> <small>17 of 58 sheets</small>								

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

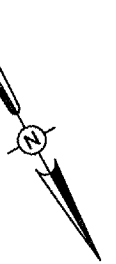
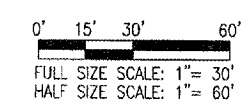
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES.

QUANTITIES				
ITEM No.	DESCRIPTION	UNIT	QUANTITY	AS BUILT QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	L.F.	5,800	
AR110014	4" DIRECTIONAL BORE	L.F.	156	
AR110504	4-WAY CONCRETE ENCASED DUCT	L.F.	81	
AR110550	SPLIT DUCT	L.F.	12	
AR110558	EXTEND 8-WAY DUCT	L.F.	16	
AR110710	ELECTRICAL MANHOLE	EACH	1	
AR125410	MITL-STAKE MOUNTED	EACH	33	
AR125415	MITL-BASE MOUNTED	EACH	3	
AR125442	TAXI GUIDANCE SIGN, 2 CHARACTER	EACH	1	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	2	
AR125452	TAXI GUIDANCE SIGN, 12 CHARACTER	EACH	1	
AR125565	SPLICE CAN	EACH	4	
AR125961	RELOCATE STAKE MOUNTED LIGHT	EACH	4	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	2	
AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	1	

- LEGEND**
- EXISTING PAVEMENT
  - EXISTING LIGHTING CABLES
  - ABANDONED ELECTRICAL CABLES
  - EXISTING ELECTRICAL DUCT
  - EXISTING STAKE MOUNTED TAXIWAY LIGHT
  - EXISTING BASE MOUNTED TAXIWAY LIGHT
  - EXISTING TAXIWAY GUIDANCE SIGN
  - EXISTING BASE MOUNTED RUNWAY LIGHT
  - EXISTING ELECTRICAL HANDHOLE
  - PROPOSED PAVEMENT
  - PROPOSED LIGHTING CABLES
  - PROPOSED ELECTRICAL DUCT/DUCT EXTENSION
  - PROPOSED STAKE MOUNTED TAXIWAY LIGHT
  - PROPOSED BASE MOUNTED TAXIWAY LIGHT
  - PROPOSED TAXIWAY GUIDANCE SIGN
  - PROPOSED SPLICE CAN
  - PROPOSED ELECTRICAL MANHOLE
  - PROPOSED BASE MOUNTED RUNWAY LIGHT (RELOCATED)



- GUIDANCE SIGN & DUCT NOTES:**
- SEE SHEET 19 FOR TAXI GUIDANCE SIGN PANEL DATA.
  - CONTRACTOR SHALL VERIFY DEPTH OF EXISTING DUCT TO ENSURE NO CONFLICT WITH SOIL PROCESSING. IF THE DUCT IS DETERMINED TO BE AT AN ELEVATION WITHIN THE SOIL PROCESSING DEPTH OPERATION. THE SOIL PROCESSING SHALL STOP TWO (2) FEET ON BOTH SIDES OF THE DUCT.
  - CONTRACTOR SHALL VERIFY DEPTH OF DUCT. IF THE DUCT IS WITHIN THE PROPOSED SOIL PROCESSING DEPTH, IT SHALL BE REMOVED PRIOR TO THE SOIL PROCESSING. THIS WORK WILL BE CONSIDERED AS INCIDENTAL TO ITEM AR152410 "UNCLASSIFIED EXCAVATION." NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
  - DUCT SHALL BE CONSTRUCTED AT A DEPTH BELOW THE PROPOSED SOIL PROCESSING.



DE066

APR 27, 2006 11:06 PM RAW I:\AIRPORTS\DECATUR\802-06TXY\AIRPORT SHEETS\R-142ELE.DWG - Layout1

<b>HANSON</b>	Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide
<b>TAXIWAY "C" RECONSTRUCTION</b>	<b>PROPOSED ELECTRICAL PLAN</b> STA. 117+99 TO STA. 124+80
<b>18</b>	18 of 56 sheets

H&E Project No. 802-06TXYD-0800  
 Drawn By R-142ELE.DWG  
 Scale 1"=30'  
 Date 02/24/06
 

LAYOUT	CCC	02/24/06
DRAWN	CCC	02/24/06
REVIEWED	WJM	03/31/06

DECATUR AIRPORT  
**DECATUR, ILLINOIS**

 A.I.P. PROJ.: 3-17-00  
 I.L. PROJ.: DEC-3588

NAVIGATIONAL AID COORDINATES (TAXIWAY LIGHTS)					
LIGHT No.	BASELINE	STATION	OFFSET	NORTHING	EASTING
T3-1*	TXY "C"	118+41.00	40.00' LT.	1151449.8856	832191.2396
T3-2	TXY "C"	118+41.00	35.00' LT.	1151454.2370	832193.7024
T3-3	TXY "C"	119+16.62	35.00' LT.	1151491.4825	832127.8957
T3-4	TXY "C"	120+32.39	35.00' LT.	1151548.5107	832027.1364
T3-5	TXY "C"	121+48.17	35.00' LT.	1151605.5389	831926.3771
T3-6	TXY "C"	121+99.22	35.00' LT.	1151641.6808	831878.4311
T3-7	TXY "C"	122+50.23	35.00' LT.	1151688.9563	831841.4992
T3-8	TXY "C"	123+01.24	35.00' LT.	1151744.1743	831818.0492
T3-9	TXY "C"	123+52.29	35.00' LT.	1151803.6296	831809.6759
T3-10	TXY "C"	124+18.57	35.00' LT.	1151869.9046	831809.0786
T3-11	TXY "C"	124+88.90	35.00' LT.	1151940.2330	831808.4447
T3-12	TXY "C"	125+09.00	40.00' LT.	1151960.2890	831803.2637
T3-13	TXY "C"	125+23.75	55.00' LT.	1151974.9032	831788.1314
T3-14	TXY "C"	125+29.25	75.00' LT.	1151980.2227	831768.0826
T3-15*	TXY "C"	125+88.00	75.00' LT.	1152038.9704	831767.5531
T3-16	TXY "C"	125+93.25	55.00' LT.	1152044.4004	831787.5050
T3-17	TXY "C"	126+08.00	40.00' LT.	1152059.2850	831802.3714
T3-18	TXY "C"	126+29.00	35.00' LT.	1152080.3292	831807.1819
T3-19	TXY "C"	123+33.50	35.00' LT.	1152284.8209	831805.3388
T3-20	TXY "C"	129+60.00	35.00' LT.	1152411.3158	831804.1987

NAVIGATIONAL AID COORDINATES (TAXIWAY LIGHTS)					
LIGHT No.	BASELINE	STATION	OFFSET	NORTHING	EASTING
T3-21	TXY "C"	130+10.00	35.00' LT.	1152461.3137	831803.7480
T3-22	TXY "C"	130+60.00	35.00' LT.	1152511.3117	831803.2974
T3-23	TXY "C"	131+10.00	35.00' LT.	1152561.3097	831802.8467
T3-24	TXY "C"	131+60.00	35.00' LT.	1152611.3076	831802.3961
T3-25	TXY "C"	132+10.00	35.00' LT.	1152661.3056	831801.9455
T3-26	TXY "C"	132+60.00	35.00' LT.	1152711.3036	831801.4948
T3-27*	TXY "C"	133+21.00	35.00' LT.	1152772.3011	831800.9450
T8-8	TXY "C"	129+60.00	91.00' RT.	1152412.4514	831930.1936
T8-9*	TXY "C"	128+33.50	74.00' RT.	1152285.8033	831914.3344
T3-51	TXY "C"	126+29.00	60.00' RT.	1152081.1854	831902.1781
T3-52	TXY "C"	125+59.09	60.00' RT.	1152011.2815	831902.8082
T3-53	TXY "C"	124+88.90	60.00' RT.	1151941.0893	831903.4408
T3-54	TXY "C"	123+52.29	60.00' RT.	1151804.4859	831904.6721
T3-55	TXY "C"	122+84.22	60.00' RT.	1151758.3884	831913.2294
T3-56	TXY "C"	122+16.24	60.00' RT.	1151718.0436	831936.9977
T3-57	TXY "C"	121+48.17	60.00' RT.	1151688.2152	831973.1706
T3-58	TXY "C"	120+32.40	60.00' RT.	1151631.1898	832073.9249
T3-59	TXY "C"	119+16.62	63.00' RT.	1151576.7697	832176.1669
T3-60	TXY "C"	118+80.62	72.50' RT.	1151567.3050	832212.1762
T3-61	TXY "C"	118+54.26	99.00' RT.	1151577.3864	832248.1643
T3-62	TXY "C"	118+44.62	135.00' RT.	1151603.9650	832274.2914

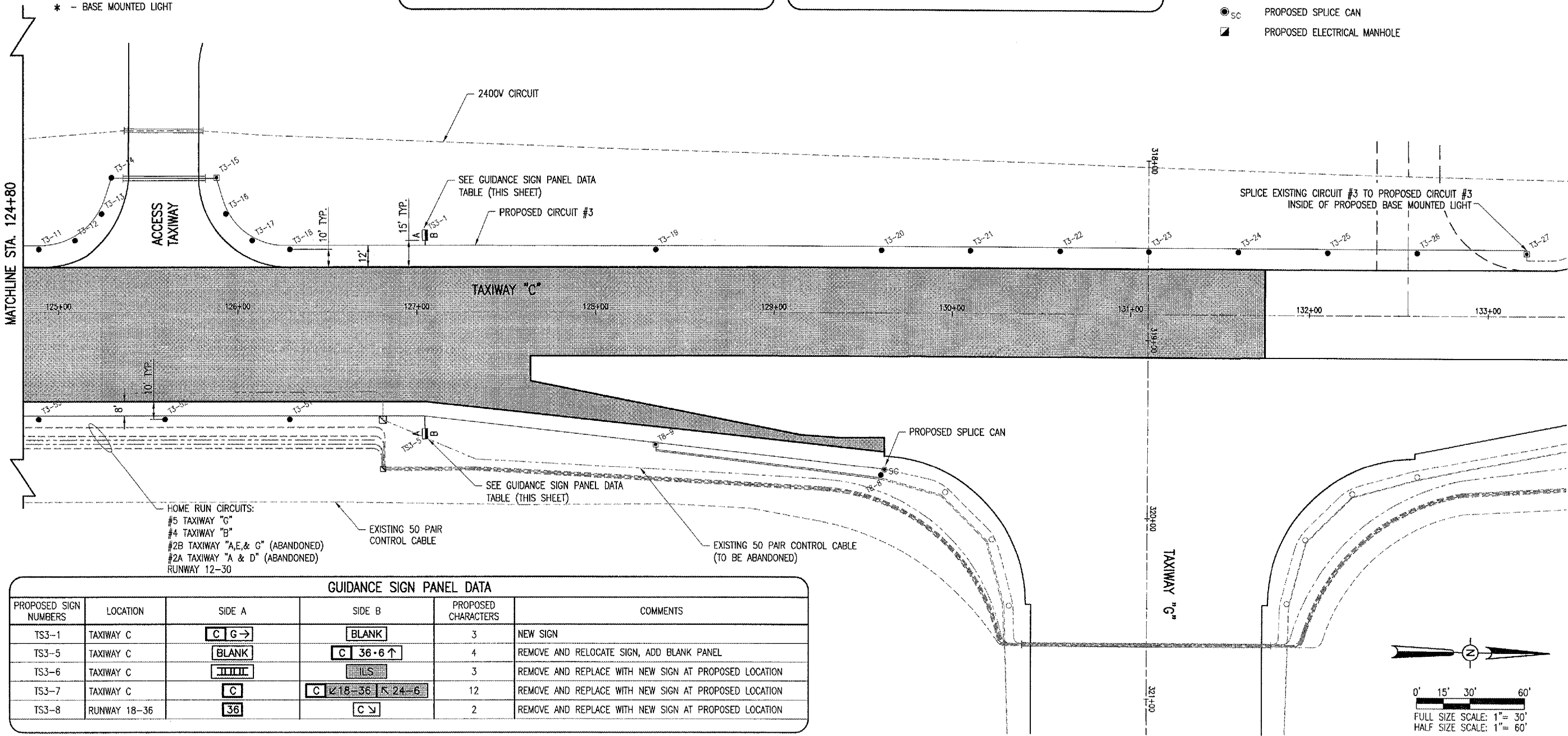
NAVIGATIONAL AID COORDINATES (GUIDANCE SIGNS)					
LIGHT No.	BASELINE	STATION	OFFSET	NORTHING	EASTING
TS3-1	TXY "C"	127+04.50	40.00' LT.	1152155.7811	831801.5017
TS3-5	TXY "C"	127+04.50	65.00' RT.	1152156.7274	831906.4974
TS3-6	TXY "C"	124+18.57	65.00' RT.	1151870.8075	831909.0745
TS3-7	TXY "C"	121+64.51	65.00' RT.	1151698.3219	831966.3905
TS3-9B	RWY 18-36	121+84.12	92.13' LT.	1151640.0660	832278.6284

NAVIGATIONAL AID COORDINATES (RUNWAY LIGHTS)					
LIGHT No.	BASELINE	STATION	OFFSET	NORTHING	EASTING
NS61*	RWY 18-36	121+84.12	87.13' LT.	1151640.1125	832283.6282

ELECTRICAL EQUIPMENT COORDINATES					
STRUCTURE	BASELINE	STATION	OFFSET	NORTHING	EASTING
SPLICE CAN	TXY "C"	118+38.75	49.00' LT.	1151440.9448	832188.7646
SPLICE CAN	TXY "C"	119+50.00	63.00' RT.	1151593.2135	832147.1134
MANHOLE	TXY "C"	120+39.44	55.00' RT.	1151630.3056	832065.3363
SPLICE CAN	TXY "C"	129+62.00	88.00' RT.	1152414.4243	831927.1757
SPLICE CAN	RWY 18-36	122+00.00	81.00' RT.	1151657.5607	832451.5985

**LEGEND**

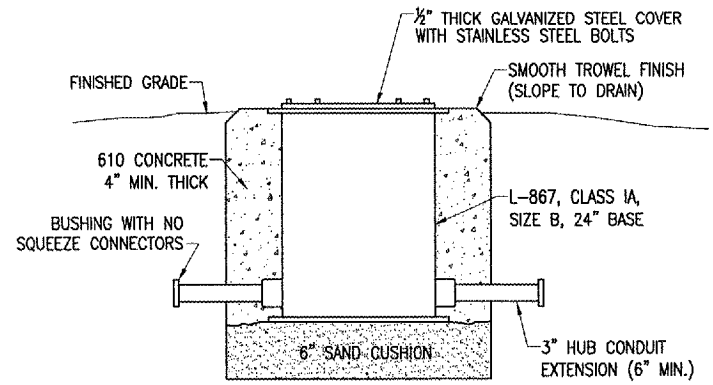
- EXISTING PAVEMENT
- EXISTING LIGHTING CABLES
- ABANDONED ELECTRICAL CABLES
- EXISTING ELECTRICAL DUCT
- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING ELECTRICAL HANDHOLE
- PROPOSED PAVEMENT
- PROPOSED LIGHTING CABLES
- PROPOSED ELECTRICAL DUCT/DUCT EXTENSION
- PROPOSED STAKE MOUNTED TAXIWAY LIGHT
- PROPOSED BASE MOUNTED TAXIWAY LIGHT
- PROPOSED TAXIWAY GUIDANCE SIGN
- PROPOSED SPLICE CAN
- PROPOSED ELECTRICAL MANHOLE



GUIDANCE SIGN PANEL DATA					
PROPOSED SIGN NUMBERS	LOCATION	SIDE A	SIDE B	PROPOSED CHARACTERS	COMMENTS
TS3-1	TAXIWAY C			3	NEW SIGN
TS3-5	TAXIWAY C			4	REMOVE AND RELOCATE SIGN, ADD BLANK PANEL
TS3-6	TAXIWAY C			3	REMOVE AND REPLACE WITH NEW SIGN AT PROPOSED LOCATION
TS3-7	TAXIWAY C			12	REMOVE AND REPLACE WITH NEW SIGN AT PROPOSED LOCATION
TS3-8	RUNWAY 18-36			2	REMOVE AND REPLACE WITH NEW SIGN AT PROPOSED LOCATION

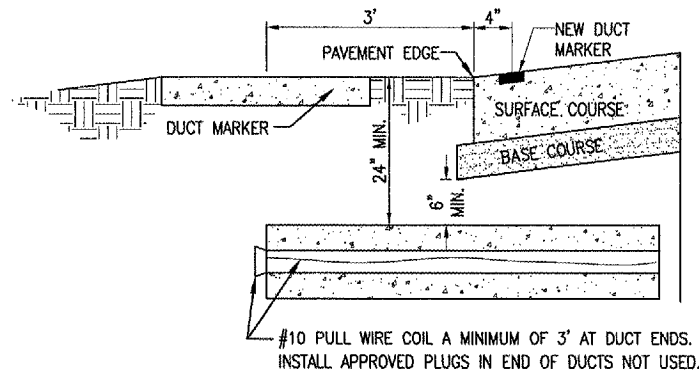
BY	
REVISION	
DATE	
 <b>HANSON</b> Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2898 Offices Nationwide	
ILS Project No. 802-06TXD-0800 File Name R-142ELE.DWG Scale 1"=30' Date 02/24/06	LAYOUT CCC 02/24/06 DRAWN CCC 02/24/06 REVIEWED WJM 03/31/06
<b>TAXIWAY "C"</b> <b>RECONSTRUCTION</b> PROPOSED ELECTRICAL PLAN STA. 124+80 TO STA. 133+21	
<b>19</b> 19 of 56 sheets	

APR 27, 2006 11:05 PM RAW  
 L:\AIRPORTS\DECATUR\802-06TXD\AIRPORT\SHEETS\R-142ELE.DWG - Layout2

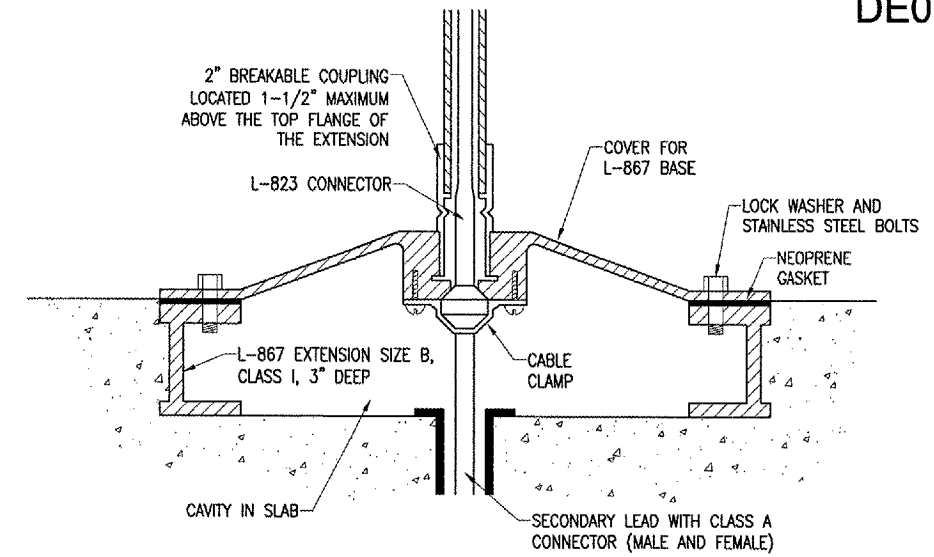


**SPLICE CAN DETAIL**  
(NOT TO SCALE)

INCLUDE INTERNAL AND EXTERNAL GROUND LUGS



**UNDERGROUND ELECTRICAL DUCT**  
(NOT TO SCALE)



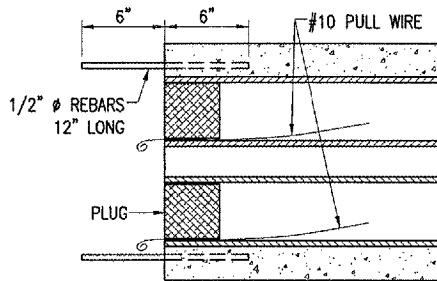
**DETAIL "A"**  
(NOT TO SCALE)

**GENERAL NOTES**

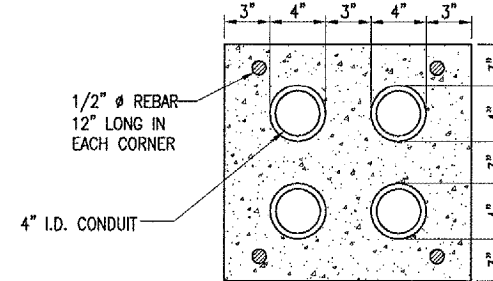
SEE LIGHTING LAYOUT SHEET 19 FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND CLASS.

SEE SHEETS 23 THROUGH 24 FOR ELECTRICAL NOTES.

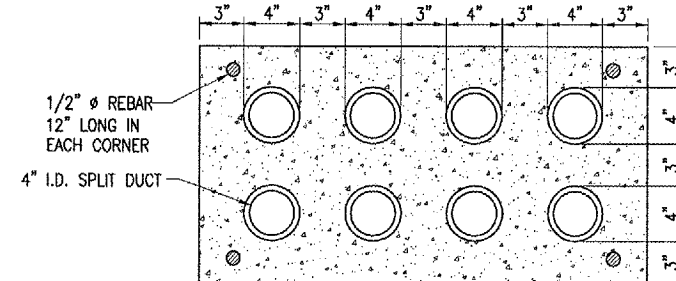
NOTE:  
FLEXIBLE CONDUIT MAY BE CONNECTED TO THE BASE EITHER THROUGH A HUB OR GROMMET.



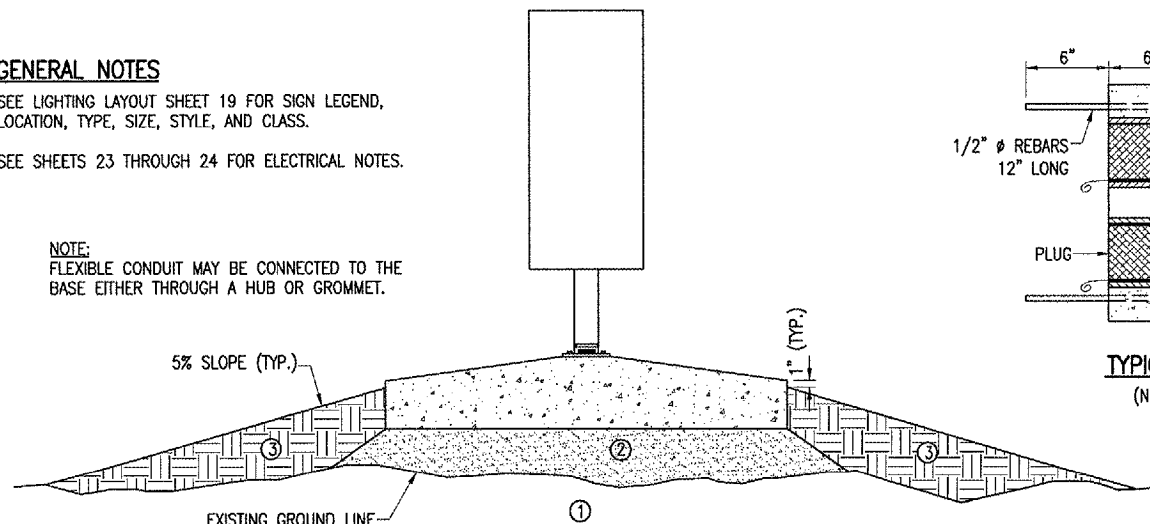
**TYPICAL SECTION**  
(NOT TO SCALE)



**4-DUCT BANK**  
(NOT TO SCALE)

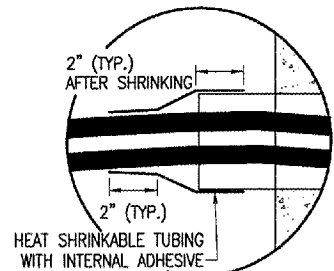


**8-DUCT BANK**  
(NOT TO SCALE)



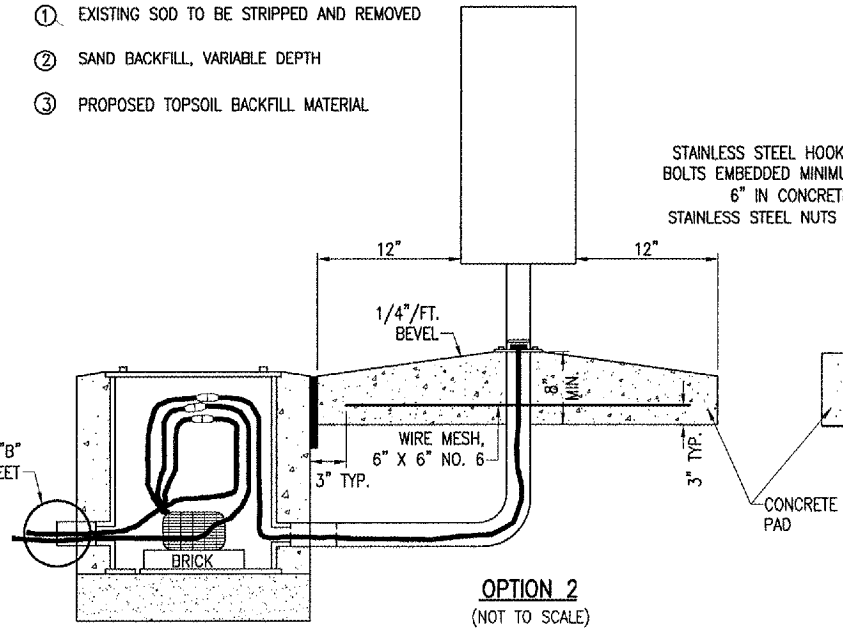
**SIGN DETAIL**  
(NOT TO SCALE)

- 1 EXISTING SOD TO BE STRIPPED AND REMOVED
- 2 SAND BACKFILL, VARIABLE DEPTH
- 3 PROPOSED TOPSOIL BACKFILL MATERIAL

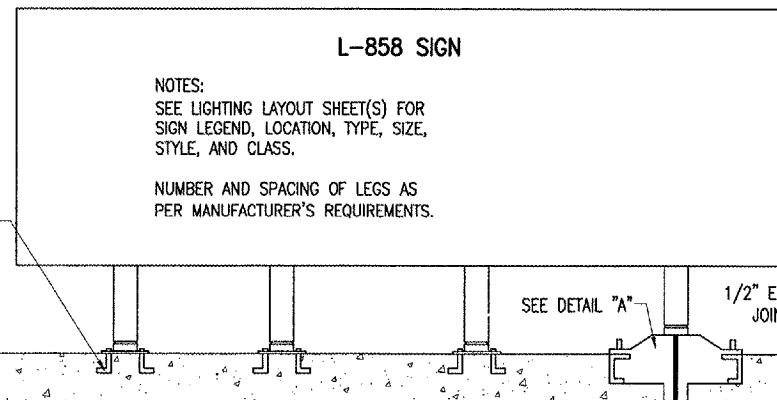


**DETAIL "B"**  
(NOT TO SCALE)

SEE DETAIL "B" ON THIS SHEET



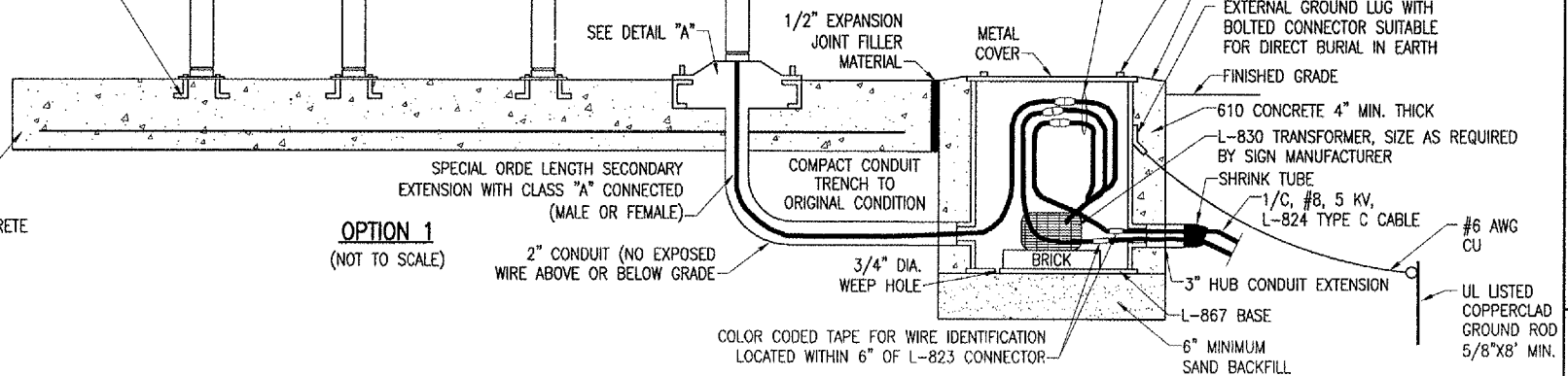
**OPTION 2**  
(NOT TO SCALE)



**L-858 SIGN**

NOTES:  
SEE LIGHTING LAYOUT SHEET(S) FOR SIGN LEGEND, LOCATION, TYPE, SIZE, STYLE, AND CLASS.  
NUMBER AND SPACING OF LEGS AS PER MANUFACTURER'S REQUIREMENTS.

**OPTION 1**  
(NOT TO SCALE)



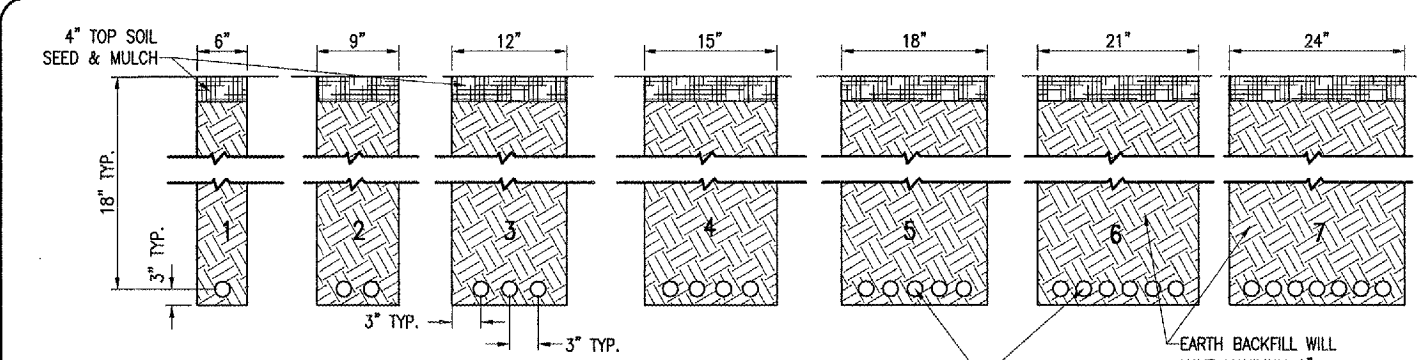
BY	
REVISION	
DATE	

DECATUR AIRPORT  
DECATUR, ILLINOIS  
A.I.P. PROJ.: 3-17-00

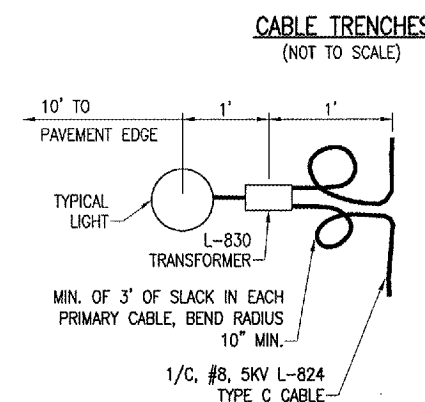
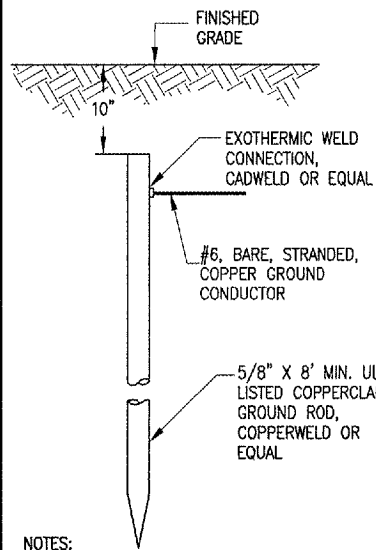
HL Project No.	802-0811XD 0800
Rev	R-541ELE.DWG
Scale	N/A
Date	03/08/05
LAYOUT	CCC 03/18/05
DRAWN	CCC 03/18/05
REVIEWED	WJM 03/31/05

**HANSON**  
Hanson Professional Services Inc.  
1025 South Sixth Street  
Springfield, IL 62703-2886  
Office: Natick, MA

TAXIWAY "C"  
RECONSTRUCTION  
ELECTRICAL DETAILS



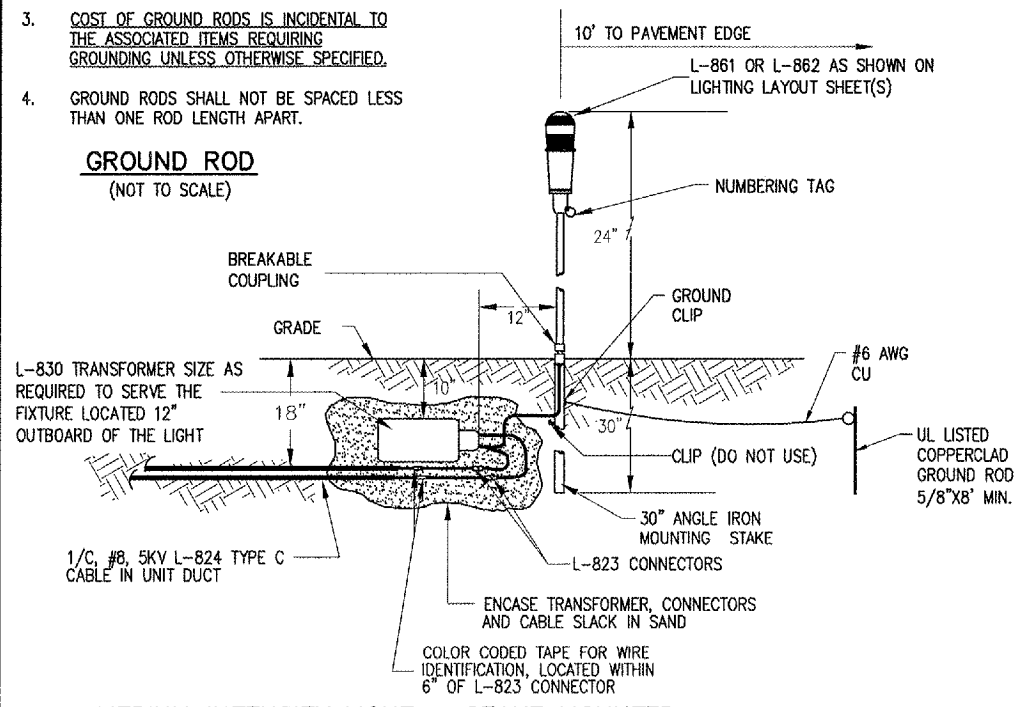
**NOTES:**  
 DETAIL NUMBERS INDICATE NO. OF CABLES.  
 TRENCHES WITH MORE THAN SEVEN CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.  
 DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.  
 ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH.



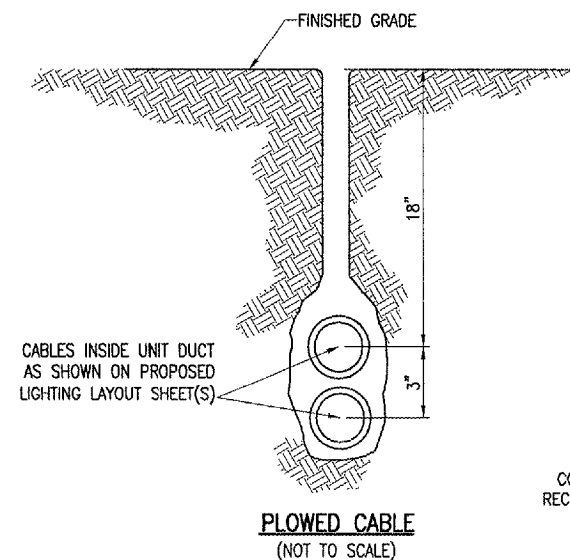
**CABLE TRENCHES**  
(NOT TO SCALE)

- NOTES:**
- TYPE AND MINIMUM NUMBER OF GROUND RODS SHALL BE AS SPECIFIED ON THE PLAN.
  - THE RESISTANCE TO GROUND OF THE GROUNDING SYSTEM SHALL NOT EXCEED 25 OHMS.
  - COST OF GROUND RODS IS INCIDENTAL TO THE ASSOCIATED ITEMS REQUIRING GROUNDING UNLESS OTHERWISE SPECIFIED.
  - GROUND RODS SHALL NOT BE SPACED LESS THAN ONE ROD LENGTH APART.

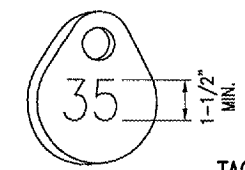
**GROUND ROD**  
(NOT TO SCALE)



**MEDIUM INTENSITY LIGHT - STAKE MOUNTED**  
(NOT TO SCALE)

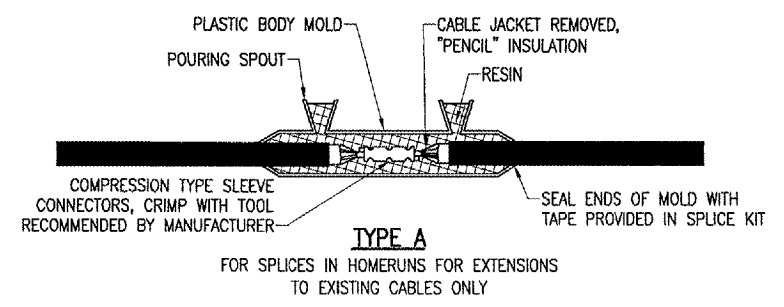


**PLOVED CABLE**  
(NOT TO SCALE)



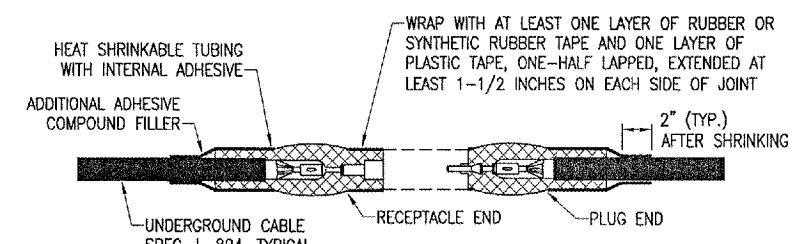
**TAG DETAIL**  
(NOT TO SCALE)

**NOTE:**  
 AFFIX NON-CORROSIVE TAG TO FIXTURE FACING RUNWAY WITH SET SCREW, WIRE TIE, OR METAL BAND. NUMERALS SHALL BE ENGRAVED FOR PERMANENT READABILITY.



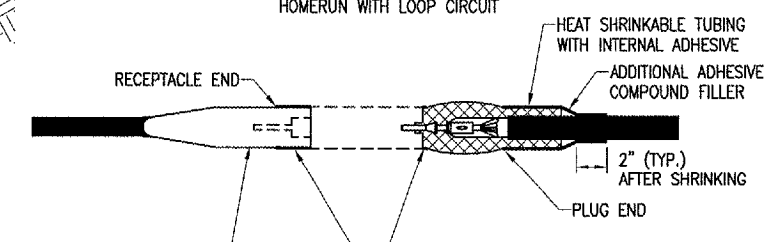
**TYPE A**

FOR SPLICES IN HOMERUNS FOR EXTENSIONS TO EXISTING CABLES ONLY



**TYPE B**

FOR SPLICES AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT



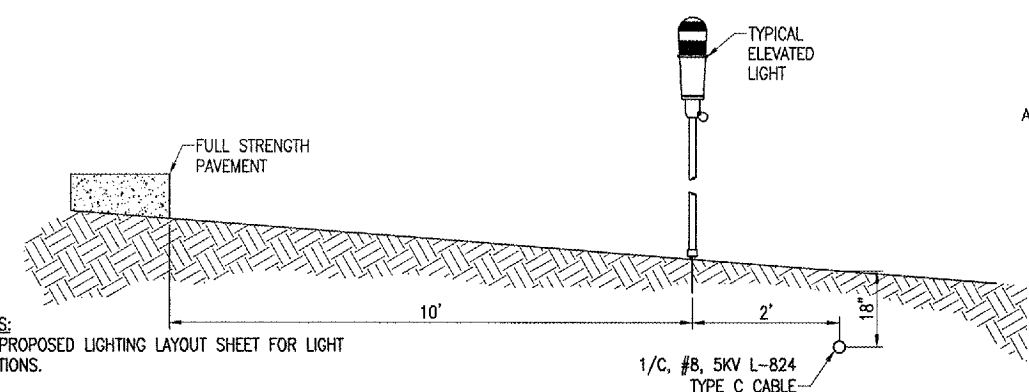
**TYPE C**

FOR SPLICES AT RUNWAY LIGHTS

**NOTES:**  
 SEE PROPOSED LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE.  
 INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.

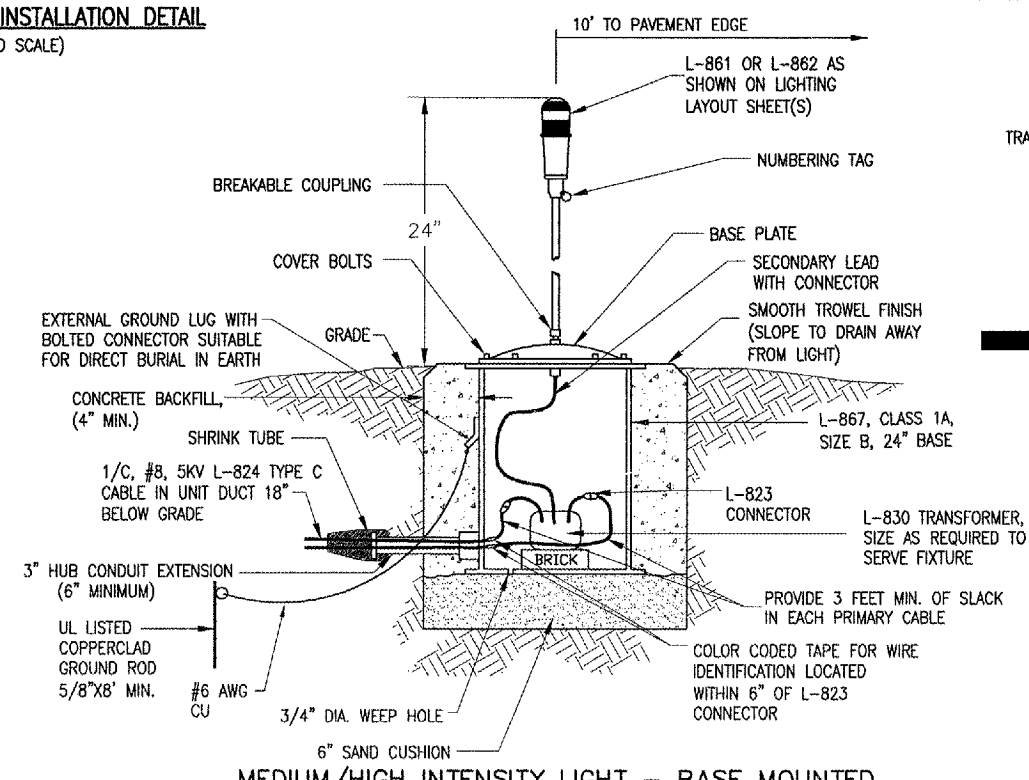
**CABLE SPLICES**  
(NOT TO SCALE)

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER, IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.



**PROFILE VIEW**

**LIGHT AND CABLE INSTALLATION DETAIL**  
(NOT TO SCALE)



**MEDIUM/HIGH INTENSITY LIGHT - BASE MOUNTED**  
(NOT TO SCALE)

APR 28, 2006 11:32 AM DMV I:\AIRPORTS\DECATUR\802-06TXX\AIRPORT SHEETS\R-541ELE.DWG - ELEC. DETAILS 2

BY	
REVISION	
DATE	

802-06TXXD-0800  
 R-541ELE.DWG  
 N/A  
 03/08/05

CCC  
 CCC  
 WJM

03/18/05  
 03/18/05

DECATUR AIRPORT  
 DECATUR, ILLINOIS

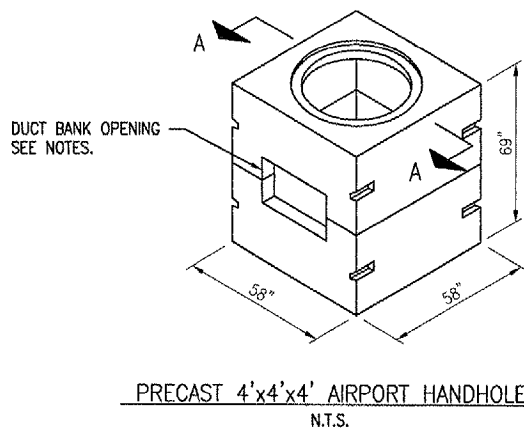
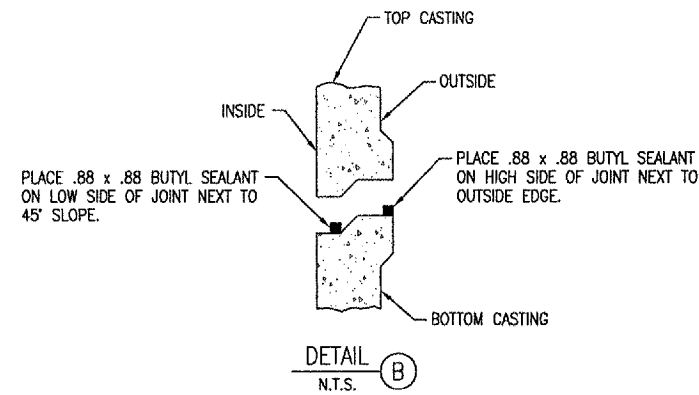
A.I.P. PROJ.: 3-17-00  
 I.L. PROJ.: DEC-3588

**HANSON**

Hanson Professional Services Inc.  
 1325 South State Street  
 Springfield, Illinois 62766  
 Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**

ELECTRICAL DETAILS



PRECAST 4'x4'x4' AIRPORT HANDHOLE NOTES

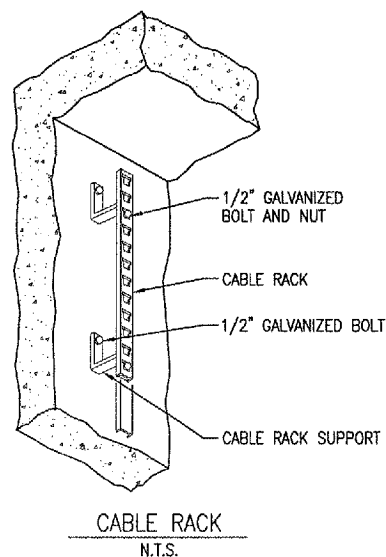
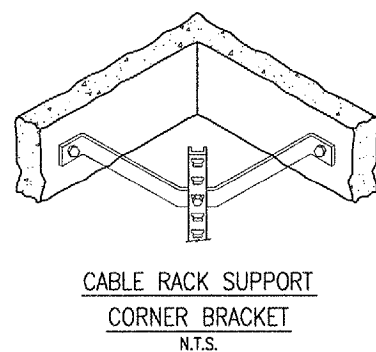
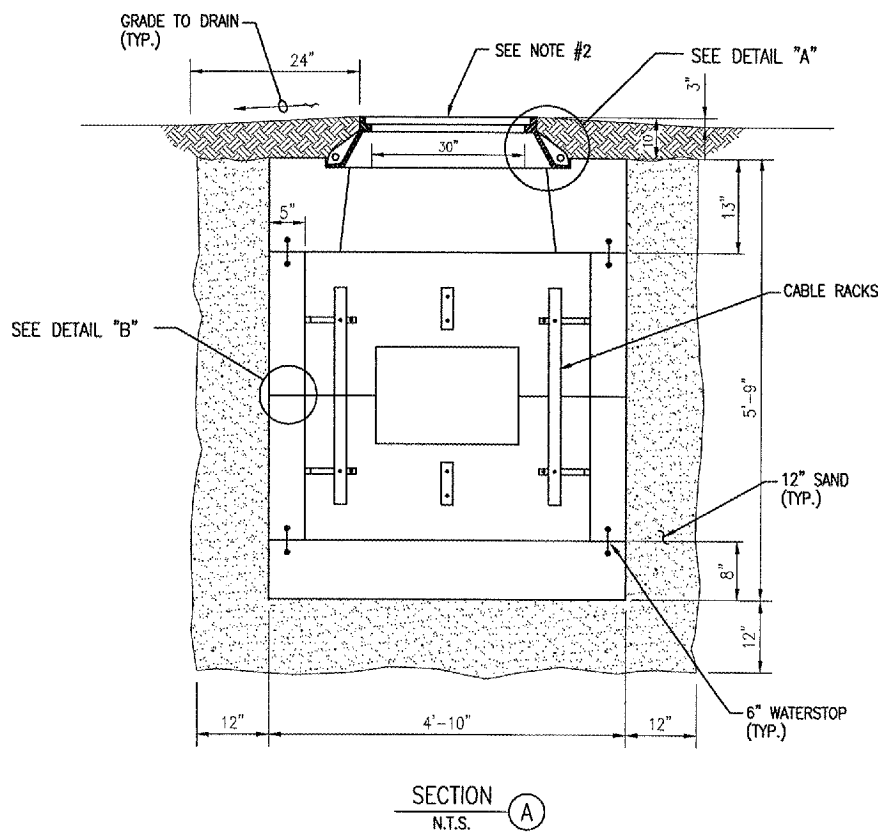
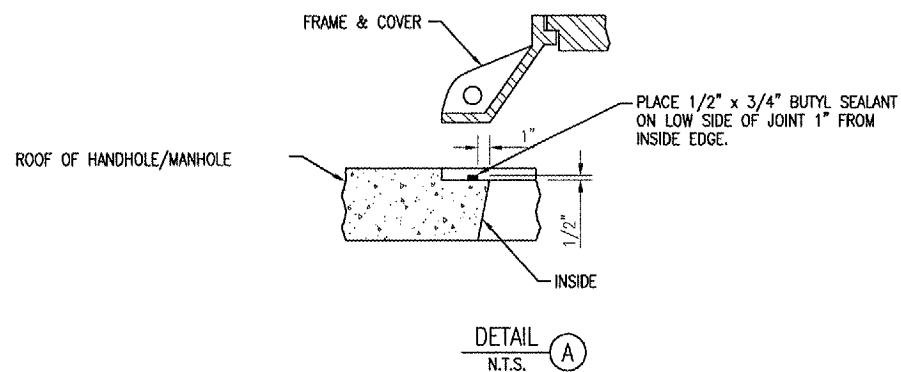
1. 4'x4'x4' AIRPORT HANDHOLE SHALL BE CONSTRUCTED TO MEET THE FOLLOWING:

DESIGN CRITERIA:

- 1) DESIGN SPECIFICATIONS: ACI 318, ASTM C858, FAA AC 150/5320-6D
- 2) DESIGN LOADING:  
8727-200 (210,000 LB. TAXI WEIGHT, 97,600 LB. MAX. GEAR)  
8777-200/300 (752,000 LB. TAXI WEIGHT, 352,000 LB. MAX. GEAR)
- 3) LIVE LOAD SURCHARGE: 24.5% OF THE WHEEL LOAD SOIL PRESSURE
- 4) CONCRETE COMPRESSIVE STRENGTH:  $f'_c = 5,000$  PSI
- 5) REINFORCING STEEL: ASTM A706,  $F_y = 60,000$  PSI

DESIGN ASSUMPTIONS:

- 1) GROUND WATER LEVEL: 3'-6" BELOW GRADE
  - 2) EARTH COVER: 0'-8" - 2'-0"
  - 3) LIVE LOAD IMPACT:  $I = 20\%$
  - 4) COEFFICIENT OF ACTIVE EARTH PRESSURE:  $K_a = 0.3$
  - 5) SPECIFIC WEIGHT OF STD. AGGREGATE CONCRETE: 150 PCF
  - 6) SPECIFIC WEIGHT OF DRY EARTH: 100 PCF
  - 7) SPECIFIC WEIGHT OF SATURATED EARTH: 120 PCF
  - 8) EQUIVALENT FLUID PRESSURE OF DRY EARTH: 30 PSF
  - 9) EQUIVALENT FLUID PRESSURE OF SATURATED EARTH: 80 PSF
2. AIRPORT HANDHOLE FRAME & LID SHALL BE CAPABLE OF WITHSTANDING MINIMUM 100,000 POUND LOADS AS CALLED FOR IN FAA ADVISORY CIRCULAR AC 150/5320-6D APPENDIX 3 ITEM 2.d. (1). AIRPORT HANDHOLE FRAME & LID SHALL BE NEENAH CATALOG NO. R-3492-A OR APPROVED EQUAL. LID SHALL BE LABELED "ELECTRIC".
  3. COORDINATE DUCT BANK INTERFACE & OPENINGS WITH THE HANDHOLE MFR. CONTRACTOR SHALL SLOPE DUCT BANK TO PRECAST HANDHOLE OPENINGS. ALL OPENINGS SHALL BE SEALED WATERTIGHT AFTER DUCT BANK INSTALLATION.
  4. 4'x4'x4' AIRPORT HANDHOLE SHALL BE MANUFACTURED BY HARTFORD CONCRETE PRODUCTS, INC., 1400 N. WABASH AVE., P.O. BOX 660, HARTFORD CITY, IN. 47348, PHONE 765-348-3506 OR 1-800-428-8110, OR APPROVED EQUIVALENT.
  5. 4'x4'x4' AIRPORT HANDHOLE SHALL BE PAID FOR UNDER ITEM AR110710 ELECTRICAL MANHOLE PER EACH.



4'x4'x4' AIRPORT HANDHOLE DETAILS  
N.T.S.

REVISION	DATE	BY

**DECATUR, ILLINOIS**  
 ILL. PROJ.: DEC-3588  
 A.I.P. PROJ.: 3-17-00

HBI Project No. 802-06TXD.0800 Element R-541ELE.DWG Scale N/A Date 03/31/06	LAYOUT KNL 03/30/06 DRAWN MV 03/30/06 REVIEWED WJM 03/30/06
--	---

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2888  
 Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**  
 ELECTRICAL DETAILS



**GENERAL**

1. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE (LATEST RECOGNIZED VERSION) AND LOCAL REGULATIONS.
2. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM, INCLUDING FAA APPROVED EQUIPMENT, ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
3. IN CASE THE CONTRACTOR ELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT REQUIRING ADDITIONAL WIRING, TRANSFORMERS, ADAPTORS, MOUNTINGS, ETC., TO THOSE SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATION, ANY COST FOR THESE ITEMS SHALL BE INCIDENTAL TO THE EQUIPMENT COST.
4. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
5. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES STYLES, CLASSES, ETC. MAY BE APPROVED.
6. ANY AND ALL INSTRUCTIONS FROM THE ENGINEER TO THE CONTRACTOR REGARDING CHANGES IN OR DEVIATIONS FROM THE PLANS AND SPECIFICATIONS SHALL BE IN WRITING WITH COPIES SENT TO THE AIRPORT SPONSOR AND THE FAA FIELD OFFICE (ADO/AFO). THE CONTRACTOR SHALL NOT ACCEPT ANY VERBAL INSTRUCTIONS FROM THE RESIDENT ENGINEER REGARDING ANY CHANGES FROM THE PLANS AND SPECIFICATIONS.
7. A MINIMUM OF THREE COPIES OF THE INSTRUCTION BOOK SHALL BE SUPPLIED WITH EACH DIFFERENT TYPE OF EQUIPMENT. THE BOOKS DESCRIBING A MORE SOPHISTICATED TYPE OF EQUIPMENT, SUCH AS REGULATORS, PAPI, REIL, ETC. AS A MINIMUM SHALL CONTAIN THE FOLLOWING:
  - A. A DETAILED DESCRIPTION OF THE OVERALL EQUIPMENT AND ITS INDIVIDUAL COMPONENTS.
  - B. THEORY OF OPERATION INCLUDING THE FUNCTION OF EACH COMPONENT.
  - C. INSTALLATION INSTRUCTIONS.
  - D. START-UP INSTRUCTIONS.
  - E. PREVENTATIVE MAINTENANCE REQUIREMENTS.
  - F. CHART FOR TROUBLE-SHOOTING.
  - G. COMPLETE POWER AND CONTROL DETAILED WIRING DIAGRAM(S), SHOWING EACH CONDUCTOR/CONNECTION/COMPONENT - "BLACK" BOXES ARE NOT ACCEPTABLE. THE DIAGRAM OF THE NARRATIVE SHALL SHOW VOLTAGE/CURRENTS/WAVE SHAPES AT STRATEGIC LOCATIONS TO BE USED WHEN CHECKING AND/OR TROUBLE-SHOOTING THE EQUIPMENT. WHEN THE EQUIPMENT HAS SEVERAL MODES OF OPERATION, SUCH AS SEVERAL BRIGHTNESS STEPS, THESE PARAMETERS SHALL BE INDICATED FOR ALL DIFFERENT MODES.
  - H. PARTS LIST WHICH WILL INCLUDE ALL MAJOR AND MINOR COMPONENTS SUCH AS RESISTORS, DIODES, ETC. IT SHALL INCLUDE A COMPLETE NOMENCLATURE OF EACH COMPONENT AND, IF APPLICABLE, THE NAME OF ITS MANUFACTURER AND THE CATALOG NUMBER.
  - I. SAFETY INSTRUCTIONS.

**POWER AND CONTROL**

1. STENCIL ALL ELECTRICAL EQUIPMENT TO IDENTIFY FUNCTION, CIRCUIT VOLTAGE AND PHASE. WHERE THE EQUIPMENT CONTAINS FUSES, ALSO STENCIL THE FUSE OR FUSE LINK AMPERE RATING. WHERE THE EQUIPMENT DOES NOT HAVE SUFFICIENT STENCILING AREA, THE STENCILING SHALL BE DONE ON THE WALL NEXT TO THE UNIT. THE LETTERS SHALL BE ONE INCH HIGH AND PAINTED IN WHITE OR BLACK TO PROVIDE THE HIGHEST CONTRAST WITH THE BACKGROUND.
2. COLOR CODE ALL PHASE WIRING BY THE USE OF COLORED WIRE INSULATION AND/OR COLORED TAPE. WHERE TAPE IS USED, THE WIRE INSULATION SHALL BE BLACK, BLACK AND RED SHALL BE USED FOR SINGLE-PHASE, THREE WIRE SYSTEMS AND BLACK, RED AND BLUE SHALL BE USED FOR THREE-PHASE SYSTEMS. NEUTRAL CONDUCTORS, SIZE NO. 6 AWG OR SMALLER, SHALL BE IDENTIFIED BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH. NEUTRAL CONDUCTORS LARGER THAN NO. 6 AWG SHALL BE IDENTIFIED EITHER BY A CONTINUOUS WHITE OR NATURAL GRAY OUTER FINISH ALONG ITS ENTIRE LENGTH OR BY THE USE OF WHITE TAPE AT ITS TERMINATIONS AND INSIDE ACCESSIBLE WIREWAYS.
3. ALL BRANCH CIRCUIT CONDUCTORS CONNECTED TO A PARTICULAR PHASE SHALL BE IDENTIFIED WITH THE SAME COLOR. THE COLOR CODING SHALL BE EXTENDED TO THE POINT OF UTILIZATION.
4. IN CONTROL WIRING THE SAME COLOR SHALL BE USED THROUGHOUT THE SYSTEM FOR THE SAME FUNCTION, SUCH AS 10%, 30%, 100% BRIGHTNESS CONTROL, ETC.
5. LOW VOLTAGE (600 V.) AND HIGH VOLTAGE (5000 V.) CONDUCTORS SHALL BE INSTALLED IN SEPARATE WIREWAYS.
6. NEATLY LACE WIRING IN DISTRIBUTION PANELS, WIREWAYS, SWITCHES AND JUNCTION/PULL BOXES.
7. THE MINIMUM SIZE OF PULL/JUNCTION BOXES, REGARDLESS OF THE QUANTITY AND SIZE OF THE CONDUCTORS SHOWN, SHALL BE AS FOLLOWS:
  - A. IN STRAIGHT PULLS THE LENGTH OF THE BOX SHALL NOT BE LESS THAN EIGHT TIMES THE TRADE DIAMETER OF THE LARGER CONDUIT. THE TOTAL AREA (INCLUDING THE CONDUIT CROSS-SECTIONAL AREA) OF A BOX END SHALL BE AT LEAST 3 TIMES GREATER THAN THE TOTAL TRADE CROSS-SECTIONAL AREA OF THE CONDUITS TERMINATING AT THE END.
  - B. IN ANGLE PULLS OR 'U' PULLS THE DISTANCE BETWEEN EACH CONDUIT ENTRY INSIDE THE BOX AND THE OPPOSITE WALL OF THE BOX SHALL NOT BE LESS THAN SIX (6) TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT. THIS DISTANCE SHALL BE INCREASED FOR ADDITIONAL ENTRIES BY THE AMOUNT OF THE SUM OF THE DIAMETERS OF ALL OTHER CONDUIT ENTRIES ON THE SAME WALL AS THE BOX. THE DISTANCE BETWEEN CONDUIT ENTRIES ENCLOSING THE SAME CONDUCTOR SHALL NOT BE LESS THAN SIX TIMES THE TRADE DIAMETER OF THE LARGEST CONDUIT.
8. A RUN OF CONDUIT BETWEEN TERMINATIONS AT EQUIPMENT ENCLOSURES, SQUARE DUCTS AND PULL/JUNCTION BOXES, SHALL NOT CONTAIN MORE THAN THE EQUIVALENT OF FOUR QUARTER BENDS (360 DEGREES TOTAL), INCLUDING THOSE BENDS LOCATED IMMEDIATELY AT THE TERMINATIONS, CAST, CONDUIT TYPE OUTLETS SHALL NOT BE TREATED AS PULL/JUNCTION BOXES.
9. EQUIPMENT CABINETS SHALL NOT BE USED AS PULL/JUNCTION BOXES. ONLY WIRING TERMINATING AT THE EQUIPMENT SHALL BE BROUGHT INTO THESE ENCLOSURES.
10. SPLICES AND JUNCTION POINTS SHALL BE PERMITTED ONLY IN JUNCTION BOXES, DUCTS EQUIPPED WITH REMOVABLE COVERS, AND AT EASILY ACCESSIBLE LOCATIONS.
11. CIRCUIT BREAKERS IN POWER DISTRIBUTION PANEL(S) SHALL BE THERMAL-MAGNETIC MOLDED CASE, PERMANENT TRIP WITH 100 AMPERE, MINIMUM FRAME.
12. DUAL LUGS SHALL BE USED WHERE TWO (2) WIRES, SIZE NO. 6 OR LARGER, ARE TO BE CONNECTED TO THE SAME TERMINAL.
13. ALL WALL MOUNTED EQUIPMENT ENCLOSURES SHALL BE MOUNTED ON WOODEN MOUNTING BOARDS.
14. WOODEN EQUIPMENT MOUNTING BOARDS SHALL BE PLYWOOD, EXTERIOR TYPE, 3/4 INCH, MINIMUM, THICKNESS, BOTH SIDES PAINTED WITH ONE COAT OF PRIMER AND TWO COATS OF GRAY OIL-BASED PAINT.
15. RIGID STEEL CONDUIT SHALL BE USED THROUGHOUT THE INSTALLATION UNLESS OTHERWISE SPECIFIED. THE MINIMUM TRADE SIZE SHALL BE 3/4 INCH.
16. ALL RIGID CONDUIT SHALL BE TERMINATED AT CONSTANT CURRENT REGULATORS WITH A SECTION (10" MINIMUM) OF FLEXIBLE CONDUIT.
17. UNLESS OTHERWISE SHOWN, ALL EXPOSED CONDUITS SHALL BE RUN PARALLEL TO OR AT RIGHT ANGLES WITH THE LINES OF THE STRUCTURE.
18. ALL STEEL CONDUITS, FITTINGS, NUTS, BOLTS, ETC. SHALL BE GALVANIZED.
19. USE CONDUIT BUSHINGS AT EACH CONDUIT TERMINATION. WHERE NO. 4 AWG OR LARGER UNDERGROUND WIRE IS INSTALLED, USE INSULATED BUSHINGS.
20. USE DOUBLE LOCK NUTS AT EACH CONDUIT TERMINATION.
21. WRAP ALL PRIMARY AND SECONDARY POWER TRANSFORMER CONNECTIONS WITH SUFFICIENT LAYERS OF INSULATING TAPE AND COVER WITH INSULATING VARNISH FOR FULL VALUE OF CABLE INSULATION VOLTAGE.
22. UNLESS OTHERWISE NOTED, ALL INDOOR SINGLE CONDUCTOR CONTROL WIRING SHALL BE NO. 12 AWG. MINIMUM.
23. THE FOLLOWING SHALL APPLY TO RELAY/CONTACTOR PANELS/ENCLOSURES:
  - A. ALL COMPONENTS SHALL BE MOUNTED IN DUST PROOF ENCLOSURE(S) WITH VERTICALLY HINGED COVERS.
  - B. THE ENCLOSURE(S) SHALL HAVE AMPLE SPACE FOR THE CIRCUIT COMPONENTS, TERMINAL BLOCKS AND INCOMING AND INTERNAL WIRING.
  - C. ALL CONTROL CONDUCTOR TERMINATIONS SHALL BE OF THE OPEN-EYE CONNECTOR/SCREW TYPE. SOLDERED CLOSED-EYE TERMINATIONS, OR TERMINATIONS WITHOUT CONNECTORS ARE NOT ACCEPTABLE.
  - D. WHEN THE ENCLOSURE COVER IS OPENED, ALL CIRCUIT COMPONENTS, WIRING AND TERMINALS SHALL BE EXPOSED AND ACCESSIBLE WITHOUT REMOVAL OF ANY PANELS, COVERS, ETC., EXCEPT THOSE COVERING HIGH VOLTAGE COMPONENTS.
  - E. ACCESS TO, OR REMOVAL OF A CIRCUIT COMPONENT OR TERMINAL BLOCK WILL NOT REQUIRE THE REMOVAL OF ANY OTHER CIRCUIT COMPONENT OR TERMINAL BLOCK.
  - F. EACH CIRCUIT COMPONENT SHALL BE CLEARLY IDENTIFIED INDICATING ITS CORRESPONDING NUMBER SHOWN ON THE DRAWINGS AND ITS FUNCTION.
  - G. A COMPLETE WIRING DIAGRAM (NOT A SCHEMATIC DIAGRAM) SHALL BE MOUNTED ON THE INSIDE OF THE COVER. THE DIAGRAM SHALL REPRESENT EACH CONDUCTOR BY A SEPARATE LINE.
  - H. THE DIAGRAM SHALL IDENTIFY EACH CIRCUIT COMPONENT AN NUMBERING AND COLOR OF EACH TERMINAL CONDUCTOR AND TERMINAL.
  - I. ALL WIRING SHALL BE NEATLY TRAINED AND LACED.
  - J. MINIMUM WIRE SIZE SHALL BE NO. 12 AWG.

APR 26, 2006 3:53 PM RAW I:\AIRPORTS\DECATUR\02-06TXX\AIRPORT\SHEETS\R-641ELE.DWG - ELEC. NOTES 1

DATE	REVISION	BY	DATE	REVISION	BY	DATE	REVISION
 <p><b>DECATUR, ILLINOIS</b></p> <p style="font-size: small;">IL PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00</p>							
I.E. Project No. 802-06TXXD-0800 Revision R-541ELE.DWG Scale N/A Date 03/08/05		LAYOUT CCC 03/18/05 DRAWN CCC 03/18/05 REVIEWED WJM		 <p style="font-size: x-small;">Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62709-2886 Offices Nationwide</p>			
TAXIWAY "C" RECONSTRUCTION				ELECTRICAL NOTES			
23							
23 of 56 sheets							

**FIELD LIGHTING NOTES**



1. UNLESS OTHERWISE NOTED, ALL UNDERGROUND FIELD POWER MULTIPLE AND SERIES CIRCUIT CONDUCTORS WHETHER DEB OR L-867 DUCT/CONDUIT SHALL BE FAA APPROVED L-824 TYPE. INSULATION VOLTAGE AND SIZE SHALL BE AS SPECIFIED.
2. NO COMPONENTS OF PRIMARY CIRCUIT SUCH AS CABLE, CONNECTORS AND TRANSFORMERS SHALL BE BROUGHT ABOVE GROUND AT EDGE LIGHTS, SIGNS, REIL, PAPI, ETC.
3. THERE SHALL BE NO EXPOSED POWER/CONTROL CABLES BETWEEN THE POINT WHERE THEY LEAVE THE UNDERGROUND (DEB OR L-867 BASES) AND WHERE THEY ENTER THE EQUIPMENT (SUCH AS TAXIWAY SIGNS, PAPI, REIL, ETC.) ENCLOSURES. THESE CABLES SHALL BE ENCLOSED IN RIGID CONDUIT OR IN FLEXIBLE, WATERTIGHT CONDUIT WITH BREAKABLE COUPLING(S) AT THE GRADE OR THE HOUSING COVER, AS SHOWN IN APPLICABLE DETAILS.
4. THE JOINTS OF THE L-823 PRIMARY CONNECTORS SHALL BE WRAPPED WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF THE JOINT, AS SHOWN ON SHEET NO. 21.
5. THE CABLE ENTRANCE INTO THE FIELD-ATTACHED L-823 CONNECTORS SHALL BE ENCLOSED BY A HEAT-SHRINKABLE TUBING WITH CONTINUOUS INTERNAL ADHESIVE, AS SHOWN ON SHEET NO. 21.
6. L-823 TYPE II, TWO-CONDUCTOR SECONDARY CONNECTORS SHALL BE CLASS 'A' (FACTORY MOLDED).
7. THERE SHALL BE NO SPLICES IN THE SECONDARY CABLE(S) WITHIN THE STEMS OF A RUNWAY/TAXIWAY EDGE/THRESHOLD LIGHTING FIXTURE AND THE WIREWAYS LEADING TO TAXIWAY SIGNS AND PAPI/REIL EQUIPMENT.
8. ELECTRICAL INSULATING GREASE SHALL BE APPLIED WITHIN THE L-823, SECONDARY, TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THESE CONNECTORS SHALL NOT BE TAPED.
9. DEB ISOLATION TRANSFORMERS SHALL BE BURIED AT A DEPTH OF TEN (10") INCHES ON A LINE CROSSING THE LIGHT AND PERPENDICULAR TO THE RUNWAY/TAXIWAY CENTERLINE AT A LOCATION TWELVE (12") INCHES FROM THE LIGHT OPPOSITE FROM THE RUNWAY/TAXIWAY.
10. A SLACK OF THREE (3') FEET, MINIMUM, SHALL BE PROVIDED IN THE PRIMARY CABLE AT EACH TRANSFORMER/CONNECTOR TERMINATION. AT STAKE-MOUNTED LIGHTS, THE SLACK SHALL BE LOOSELY COILED IMMEDIATELY BELOW THE ISOLATION TRANSFORMER.
11. DIRECTION OF PRIMARY CABLES SHALL BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK TO PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO RIGHT IS CODED BLUE. THIS APPLIES TO STAKE MOUNTED LIGHTS AND BASE MOUNTED LIGHTS WHERE THE BASE HAS ONLY ONE ENTRANCE.
12. L-867 BASES SHALL BE SIZE B, 24" DEEP, CLASS I, UNLESS OTHERWISE NOTED.
13. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL BE A 1/4" DIAMETER, MINIMUM, OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE SECONDARY CONNECTOR INTO THE L-867 BASE.
14. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1-1/2" ABOVE THE EDGE OF THE COVER IN CASE OF BASE MOUNTED COUPLINGS, OR THE TOP OF THE STAKE IN CASE OF STAKE MOUNTED COUPLINGS.

15. WHERE THE BREAKABLE COUPLING IS NOT AN INTEGRAL PART OF THE LIGHT FIXTURE STEM OR MOUNTING LEG, A BEAD OF SILICON SEAL SHALL BE APPLIED COMPLETELY AROUND LIGHT STEM OR WIREWAY AT BREAKABLE COUPLING TO PROVIDE A WATERTIGHT SEAL.
16. TOPS OF THE STAKES SUPPORTING LIGHT FIXTURES SHALL BE FLUSH WITH THE SURROUNDING GRADE.
17. PLASTIC LIGHTING FIXTURE COMPONENTS, SUCH AS LAMP HEADS, STEMS, BREAKABLE COUPLINGS, BASE COVERS, BRACKETS, STAKES, SHALL NOT BE ACCEPTABLE.
18. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE: ONE (1) INCH. IN CASE OF STAKE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE STAKE AND THE TOP OF THE LENS. IN CASE OF BASE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE BASE FLANGE AND THE TOP OF THE LENS, THUS INCLUDING THE BASE COVER, THE FRANGIBLE COUPLING, THE STEM, THE LAMP HOUSING AND THE LENS.
19. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE ONE (1) INCH. THIS ALSO APPLIES AT INTERSECTIONS TO LATERAL SPACING BETWEEN LIGHTS OF A RUNWAY/TAXIWAY AND THE INTERSECTING RUNWAY/TAXIWAY.
20. ENTRANCES INTO L-867 BASES SHALL BE SEALED WITH HEAT SHRINK AS SHOWN IN DETAIL "B" ON SHEET NO. 20.
21. GALVANIZED/PAINTED EQUIPMENT/COMPONENT SURFACES SHALL NOT BE DAMAGED BY DRILLING, FILING, ETC. DRAIN HOLES IN METAL TRANSFORMER HOUSINGS SHALL BE MADE BEFORE GALVANIZING.
22. EDGE LIGHT NUMBERING TAGS SHALL BE FACING THE PAVEMENT.
23. CABLE/SPLICE/DUCT MARKERS SHALL BE PRECAST CONCRETE OF THE SIZE SHOWN. LETTERS/NUMBERS/ARROWS FOR THE LEGEND TO BE IMPRESSED INTO THE TOPS OF THE MARKERS SHALL BE PRE-ASSEMBLED AND SECURED IN THE MOLD BEFORE THE CONCRETE IS POURED. LEGEND INSCRIBED BY HAND IN WET CONCRETE SHALL NOT BE ACCEPTABLE.
24. ALL UNDERGROUND CABLE RUNS SHALL BE IDENTIFIED BY CABLE MARKERS AT 200 FEET MAXIMUM SPACING, WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS SHALL BE INSTALLED IMMEDIATELY ABOVE THE CABLES.
25. THERE SHALL BE NO SPLICES BETWEEN THE ISOLATION TRANSFORMERS. L-823 CONNECTORS ARE ALLOWED AT TRANSFORMER CONNECTIONS ONLY, UNLESS OTHERWISE SHOWN.
26. APPLY AN OXIDE INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS AND BREAKAGE COUPLING THREADS.
27. LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS SHALL BE IDENTIFIED BY DUCT MARKERS.
28. WHERE A PARALLEL, CONSTANT VOLTAGE PAPI SYSTEM IS PROVIDED, THE "I" SPLICES SHALL BE OF THE CAST TYPE.
29. CONCRETE USED FOR SLABS, FOOTINGS, BACKFILL AROUND TRANSFORMER HOUSINGS, MARKINGS, ETC. SHALL BE 3000 PSI, AIR-ENTRAINED.
30. ALL POWER AND CONTROL CABLES IN MAN/HAND HOLES SHALL BE TAGGED. USE EMBOSSED COPPER STRIPS TO BE ATTACHED AT BOTH ENDS TO THE CABLE BY THE USE OF PLASTIC STRAPS. MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A MAN/HAND HOLE-ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT.

**GROUNDING NOTES**

1. ALL GROUND CONNECTIONS TO GROUND RODS, BUSES, PANELS, ETC. SHALL BE MADE WITH PRESSURE TYPE SOLDERLESS LUGS AND GROUND CLAMPS SOLDERED OR BOLT AND WASHER TYPE CONNECTIONS ARE NOT ACCEPTABLE. CLEAN ALL METAL SURFACES BEFORE MAKING GROUND CONNECTIONS. CONNECTIONS TO GROUND RODS SHALL BE EXOTHERMIC WELD WHERE SPECIFIED HEREIN.
2. TOP OF GROUND RODS SHALL BE TEN (10) INCHES BELOW GRADE, UNLESS SPECIFIED OTHERWISE HEREIN, FOR RESPECTIVE APPLICATIONS.
3. THE RESISTANCE TO GROUND OF THE VAULT GROUNDING SYSTEM WITH THE COMMERCIAL POWER LINE NEUTRAL DISCONNECTED SHALL NOT EXCEED 10 OHMS.

APR 28, 2006 7:25 AM DWG  
I:\AIRPORTS\DECATUR\B02-06TAXY\AIRPORT\SHEETS\R-541ELE.DWG - ELEC. NOTES 2

DATE	REVISION	BY			
 <b>DECATUR, ILLINOIS</b> I.L. PROJ.: DEC-3568      A.I.P. PROJ.: 3-17-00					
HET Project No. 802-06TAXY-0800 Filename R-541ELE.DWG Scale N/A Date 03/08/05	LAYOUT CCC 03/18/05 DRAWN CCC 03/18/05 REVIEWED WJM	 <b>HANSON</b> Hanson Professional Services Inc. 1625 South Sixth Street Springfield, Illinois 62703-2866 Offices Nationwide			
<b>TAXIWAY "C" RECONSTRUCTION</b>		<b>ELECTRICAL NOTES</b>			
24					
24 of 56 sheets					



**MARKING NOTES**

ALL RUNWAY MARKING WILL BE SOLID AND WHITE IN COLOR.

THE TAXIWAY CENTERLINE STRIPE, HOLDING POSITION, AND I.L.S. HOLDING POSITION MARKING WILL BE SOLID AND YELLOW IN COLOR. THE HOLDING POSITION MARKINGS WILL BE AS SPECIFIED IN THE DETAILS SHOWN ON THE FOLLOWING SHEET.

ALL PROPOSED MARKING WILL BE LOCATED AS SHOWN ON THIS SHEET.

ALL YELLOW & WHITE PROPOSED MARKING WILL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIFICATIONS.

CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

ALL MARKING ON EXISTING AND PROPOSED PCC PAVEMENT AND HOLDING LINES SHALL HAVE A 6 INCH WIDE (MIN.) BLACK OUTLINE.

ALL PROPOSED MARKING ITEMS WILL BE PAID FOR AS FOLLOWS:  
AR620520 "PAVEMENT MARKING-WATERBORNE" \_\_\_\_\_ PER SQ. FT.

WHERE CALLED FOR, THE CENTERLINE TAXIWAY WILL BE REMOVED BY SAND BLASTING OR WATER BLAST METHOD.

ALL PERMANENT MARKING REMOVAL ITEMS WILL BE PAID FOR AS FOLLOWS:  
AR620900 "PAVEMENT MARKING REMOVAL" \_\_\_\_\_ PER SQ. FT.

MARKING QUANTITIES	
DESCRIPTION	TOTAL (S.F.)
TAXIWAY CENTERLINE STRIPING - YELLOW	2,327
TAXIWAY HOLDING POSITION - YELLOW	228
I.L.S. HOLDING POSITION - YELLOW	178
<b>TOTAL YELLOW:</b>	<b>2,733</b>
RUNWAY EDGE STRIPE - WHITE	240
<b>TOTAL WHITE:</b>	<b>240</b>
TAXIWAY CENTERLINE OUTLINE - BLACK	2,260
TAXIWAY HOLDING POSITION OUTLINE - BLACK	600
I.L.S. HOLDING POSITION OUTLINE - BLACK	375
RUNWAY EDGE STRIPE - BLACK	40
<b>TOTAL BLACK:</b>	<b>3,275</b>
<b>TOTAL MARKING:</b>	<b>6,248</b>

MARKING REMOVAL QUANTITIES	
DESCRIPTION	TOTAL (S.F.)
TAXIWAY CENTERLINE STRIPING - YELLOW	421

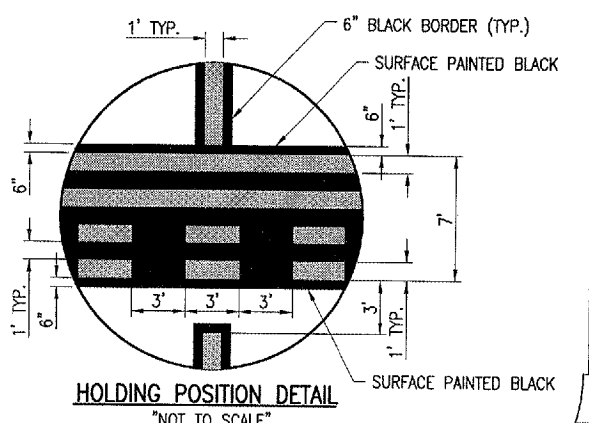
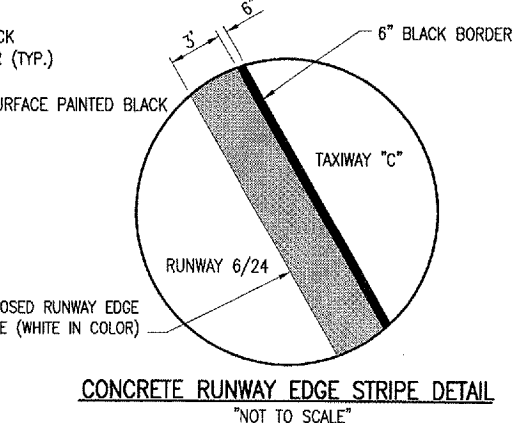
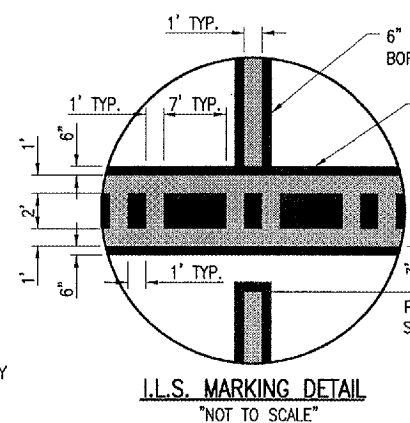
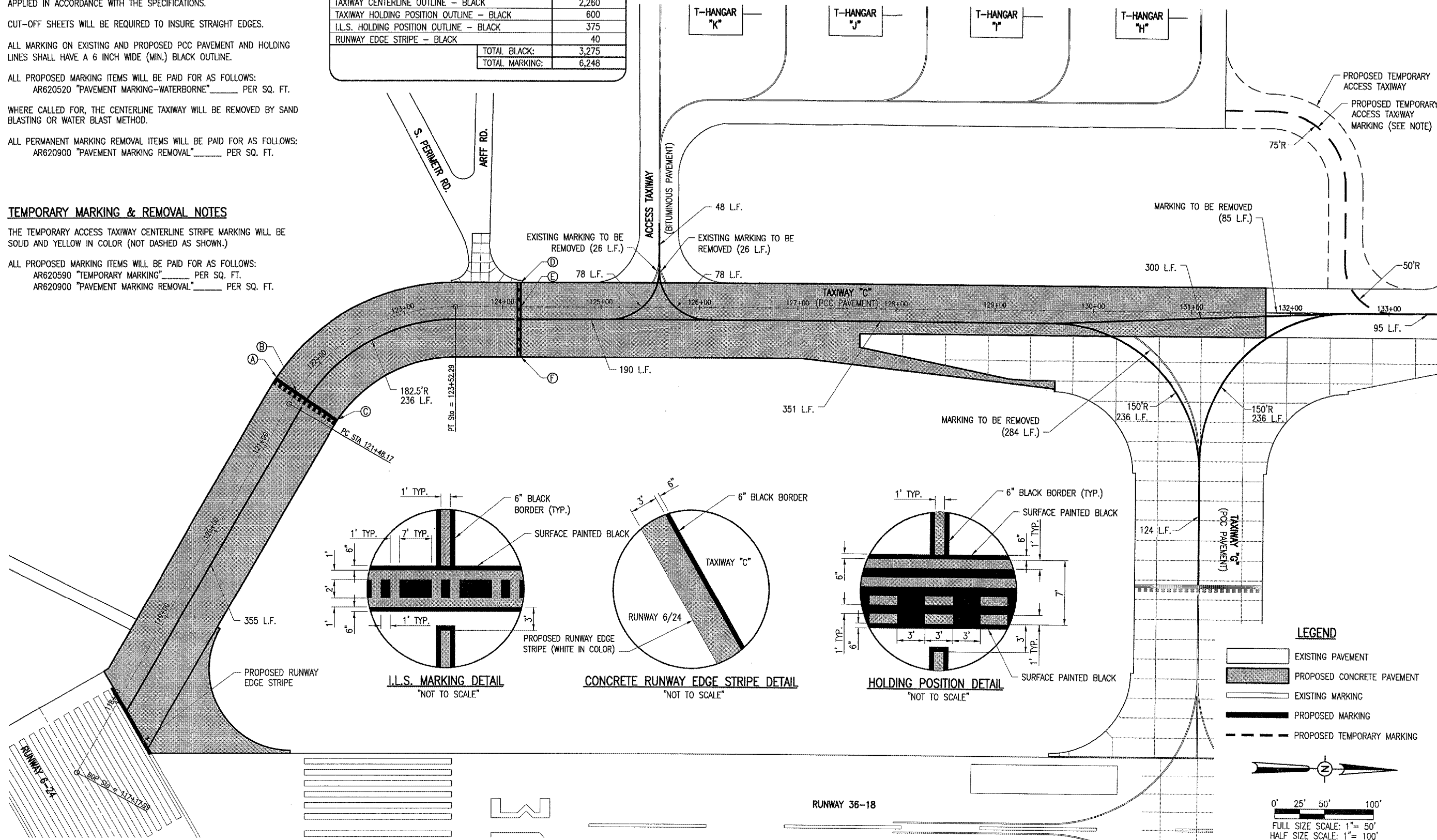
TEMPORARY MARKING & REMOVAL QUANTITIES	
DESCRIPTION	TOTAL (S.F.)
ACCESS TAXIWAY CENTERLINE STRIPING - YELLOW	125

COORDINATE DATA				
ID.	STATION	OFFSET	NORTHING	EASTING
A	121+62.99	25' LT.	1151623.0230	831917.0772
B	121+62.99	0	1151643.7820	831931.0078
C	121+62.99	50' RT.	1151685.3001	831958.8690
D	124+19.07	25' LT.	1151869.9947	831819.0782
E	124+19.07	0	1151870.2200	831844.0771
F	124+19.07	50' RT.	1151870.6707	831894.0751

**TEMPORARY MARKING & REMOVAL NOTES**

THE TEMPORARY ACCESS TAXIWAY CENTERLINE STRIPE MARKING WILL BE SOLID AND YELLOW IN COLOR (NOT DASHED AS SHOWN.)

ALL PROPOSED MARKING ITEMS WILL BE PAID FOR AS FOLLOWS:  
AR620590 "TEMPORARY MARKING" \_\_\_\_\_ PER SQ. FT.  
AR620900 "PAVEMENT MARKING REMOVAL" \_\_\_\_\_ PER SQ. FT.



**LEGEND**

- EXISTING PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- EXISTING MARKING
- PROPOSED MARKING
- PROPOSED TEMPORARY MARKING

**LEGEND**

0' 25' 50' 100'

FULL SIZE SCALE: 1" = 50'  
HALF SIZE SCALE: 1" = 100'

BY	
REVISION	
DATE	
<p>DECATUR, ILLINOIS</p>	
I.L. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00	
I.S. Project No. R02-06TXD-0800 Elements R-151MRK.DWG Scale 1" = 50' Date 02/15/06	LAYOUT CCC 02/15/06 DRAWN CCC 02/15/06 REVIEWED WJM 03/31/06
<p>Hanson Professional Services Inc. 1625 South State Street Springfield, IL 62761-2886 Offices Nationwide</p>	
TAXIWAY "C" RECONSTRUCTION	PROPOSED MARKING PLAN
<p><b>25</b></p> 25 of 56 sheets	

APR 27, 2006 10:54 PM RAW  
I:\AIRPORTS\DECATUR\802-06TXD\AIRPORT SHEETS\R-151MRK.DWG - Sheet 25

**SEEDING DATA**

THE GRADING LIMITS ARE SHOWN ON THIS SHEET BY A HEAVY DASHED LINE (---). ALL AREAS WITHIN THESE LIMITS, EXCEPT THE PROPOSED PAVEMENT, SHALL BE LIMED, FERTILIZED AND SEEDED IN ACCORDANCE WITH THE FOLLOWING FORMULA AND RATES OF APPLICATION.

LIME (TONS PER ACRE) \_\_\_\_\_ 2  
 FERTILIZER (MINIMUM POUNDS OF AVAILABLE PLANT FOOD PER ACRE)

N	90
P <sub>2</sub> O <sub>5</sub>	350
K <sub>2</sub> O	240
<b>TOTAL</b>	<b>680</b>

SEEDING (MINIMUM POUNDS OF PURE LIVE SEED PER ACRE)

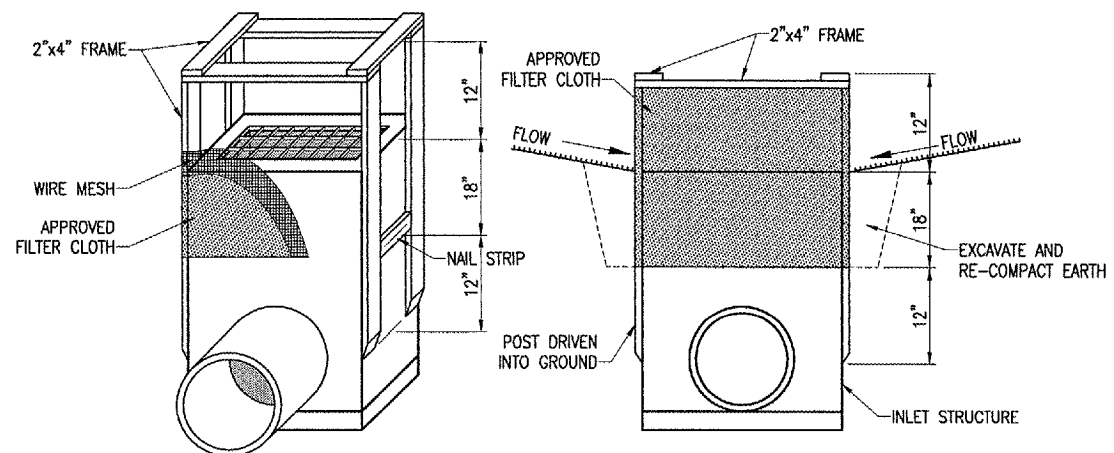
ALTA FESCUE	80
PERENNIAL RYEGRASS	30
CREeping RED FESCUE	20
SPRING OATS	48
AREA TO BE SEEDED	4.3 ACRES
DATE SEEDING COMPLETED	

**MULCHING DATA**

ALL EARTHEN AREAS WITHIN THE GRADING LIMITS WILL BE MULCHED IN ACCORDANCE WITH THE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS AT THE RATE OF 2 TONS PER ACRE.

HYDRAULICALLY APPLIED HYDROSTRAW™, OR AN APPROVED EQUAL, WILL BE USED, APPLIED AS DEFINED IN THE SPECIAL PROVISIONS.

AREA TO BE MULCHED \_\_\_\_\_ 4.3 ACRES  
 DATE MULCHING COMPLETED \_\_\_\_\_



**INLET PROTECTION DETAIL**  
 "NOT TO SCALE"

**INLET PROTECTION NOTES**

THE PROPOSED INLET PROTECTION WILL BE PLACED AT THE LOCATIONS SHOWN ON THIS SHEET AND THE FOLLOWING SHEET.

INSPECTION OF THE INLET PROTECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.

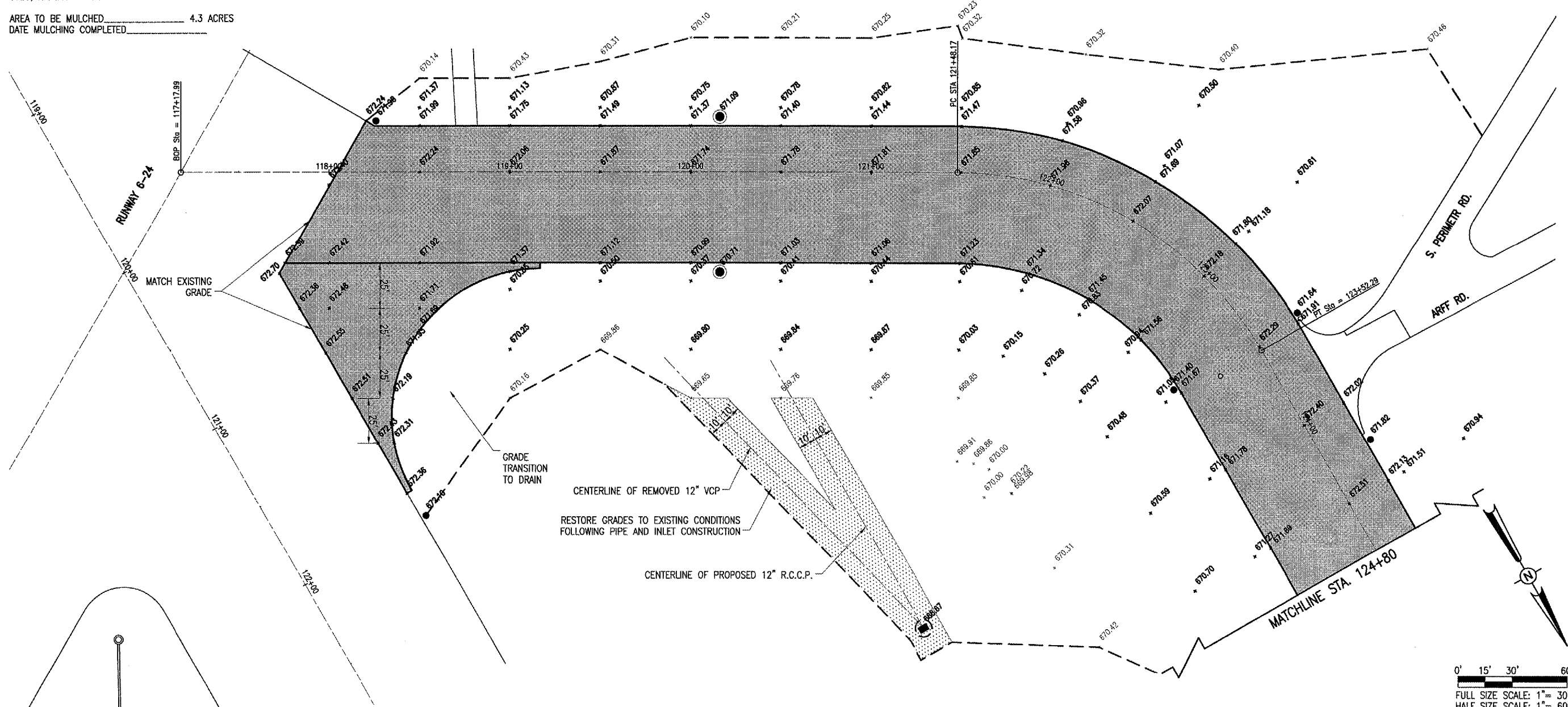
THE INLET PROTECTION SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM WATER DRAINAGE.

THE INLET PROTECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSIONS AND SEDIMENT CONTROL.

ALL THE LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO CONSTRUCT AND MAINTAIN THE INLET PROTECTION AS SHOWN IN THE DETAIL AND AT THE LOCATIONS SHOWN WILL BE PAID FOR UNDER ITEM AR156520 "INLET PROTECTION" \_\_\_\_\_ 2 EACH.

**LEGEND**

- EXISTING PAVEMENT
- PROPOSED IMPROVEMENTS
- PROPOSED GRADE
- EXISTING GRADE
- PROPOSED INSPECTION HOLE
- EXISTING INSPECTION HOLE
- PROPOSED INLET
- EXISTING INLET
- PROPOSED MANHOLE
- EXISTING MANHOLE
- PROPOSED SEEDING LIMITS
- PROPOSED INLET PROTECTION

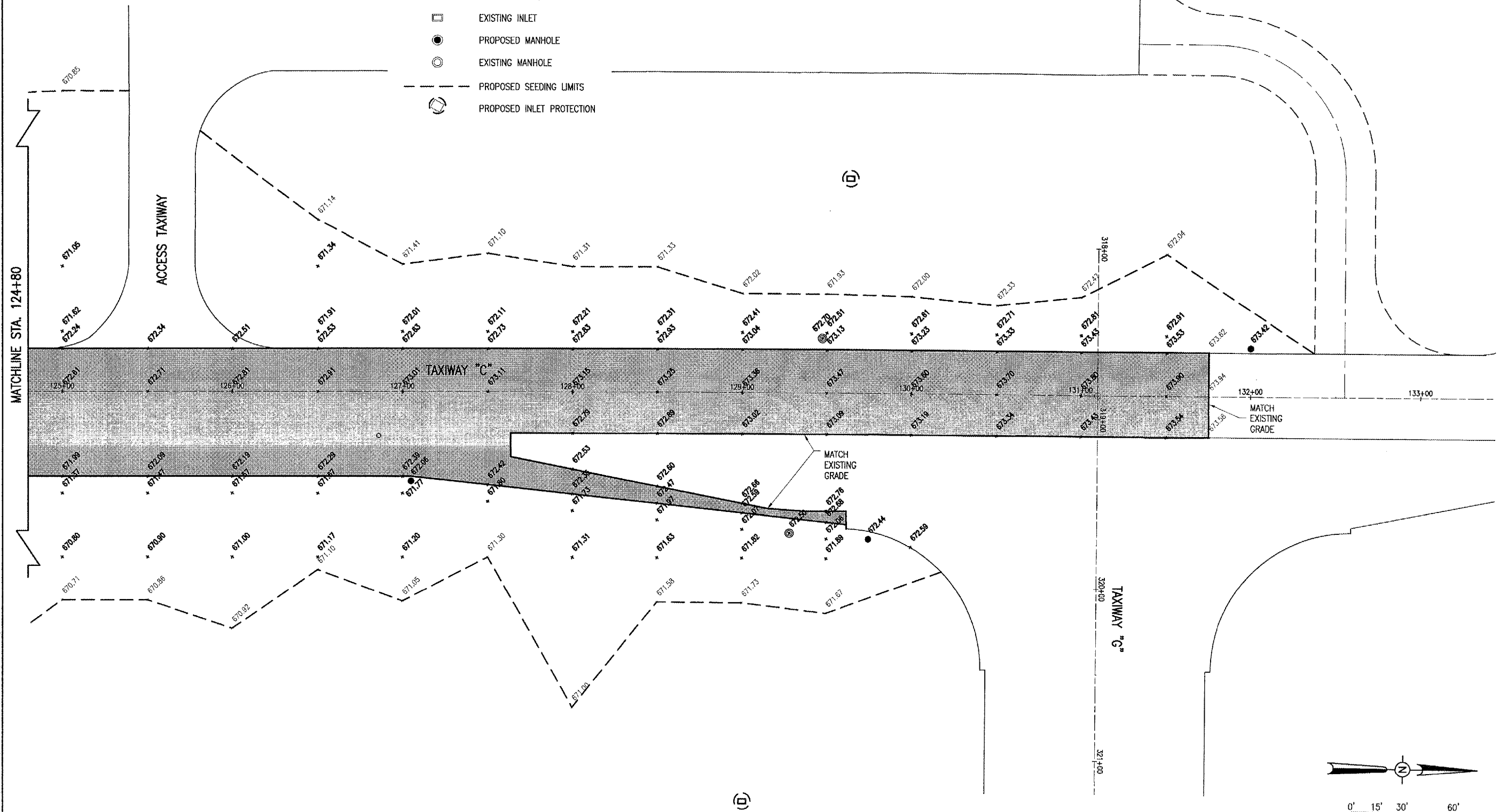


MAY 01, 2006 2:09 PM RAW  
 I:\AIRPORTS\DECATUR\B02-06TAXI\AIRPORT\SHEETS\19-181STK.DWG - STA. 117+99 TO STA. 124+80

REVISION DATE	DECATUR, ILLINOIS A.I.P. PROJ.: 3-17-00 IL PROJ.: DEC-3568
HEI Project No. B02-06TAXI.DWG R-191STK.DWG Scale 1" = 30' Date 02/24/06	LAYOUT CCC 02/24/06 DRAWN CCC 02/24/06 REVIEWED WJM 03/31/06
HANSON Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62705-2886 Offices Nationwide	
<b>TAXIWAY "C" RECONSTRUCTION</b> PROPOSED STAKING, GRADING, SEEDING AND MULCHING PLAN STA. 117+99 TO STA. 124+80	
<span style="font-size: 2em; font-weight: bold;">26</span> 26 of 56 sheets	

SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AR156520	INLET PROTECTION	EACH	3
AR901510	SEEDING	ACRE	4.3
AR908510	MULCHING	ACRE	4.3

- LEGEND**
- EXISTING PAVEMENT
  - PROPOSED IMPROVEMENTS
  - PROPOSED GRADE
  - EXISTING GRADE
  - PROPOSED INSPECTION HOLE
  - EXISTING INSPECTION HOLE
  - PROPOSED INLET
  - EXISTING INLET
  - PROPOSED MANHOLE
  - EXISTING MANHOLE
  - PROPOSED SEEDING LIMITS
  - PROPOSED INLET PROTECTION



REVISION	DATE	BY

**DECATUR, ILLINOIS**

IL PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

FILE PROJECT NO.	802-061XKD_0800
FILENAME	R-191STK.DWG
SCALE	1" = 30'
DATE	02/24/06
LAYOUT	CCC 02/24/06
DRAWN	CCC 02/24/06
REVIEWED	WJM 03/31/06

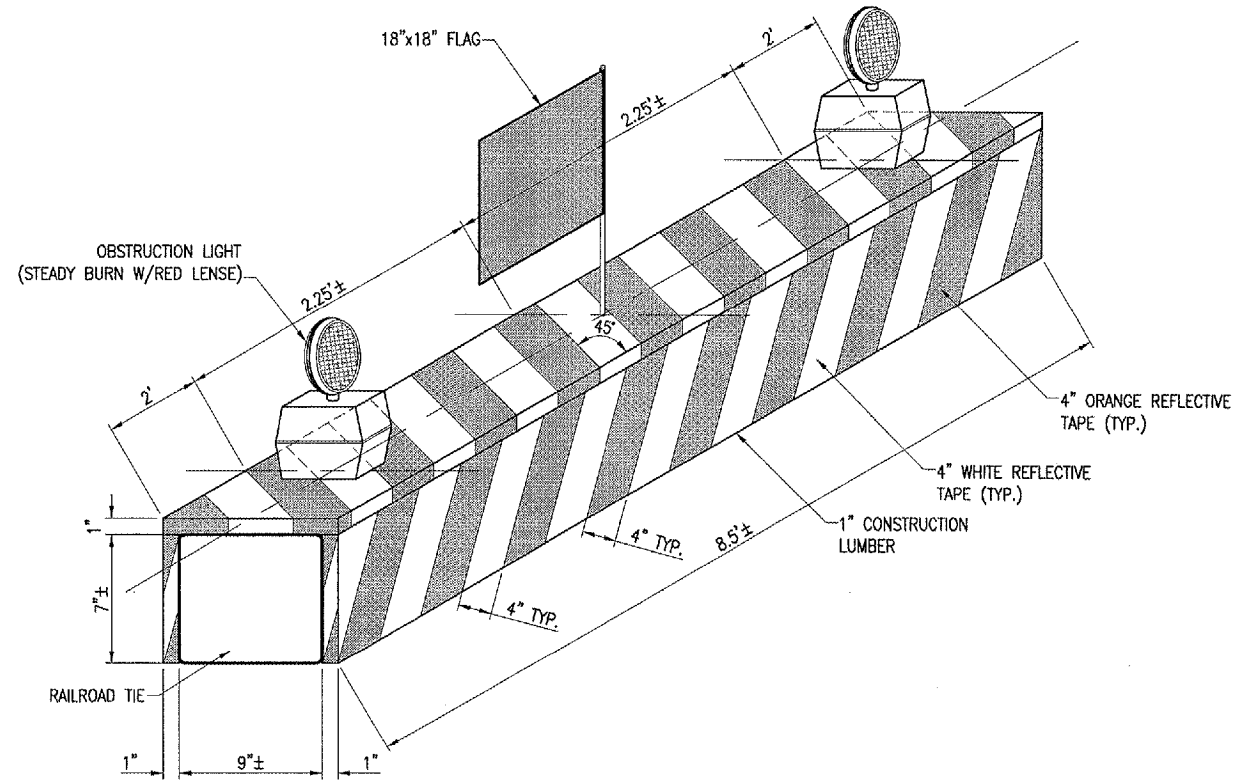
**HANSON**

Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62703-2886  
Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**

PROPOSED STAKING, GRADING, SEEDING AND MULCHING PLAN  
STA. 124+80 TO STA. 132+50

APR 27, 2006 11:09 PM RAW  
I:\AIRPORTS\DECATUR\802-061XKD\AIRPORT\SHEETS\R-191STK.DWG - STA. 124+80 TO STA. 132+50



**LOW PROFILE BARRIER NOTES**

THE BARRIERS WILL BE CONSTRUCTED OF NEW OR SALVAGED RAILROAD TIES (7"±H x 9"±W x 8.5"±L) CAPPED WITH 1" CONSTRUCTION GRADE LUMBER ON THREE SIDES, 4" WHITE AND 4" ORANGE REFLECTIVE TAPE ON THREE SIDES, (2) OBSTRUCTION LIGHTS, AND (1) 18"x18" FLAG.

BARRIERS THAT ARE IN PLACE AND CONFLICT WITH CONSTRUCTION OPERATIONS CAN BE TEMPORARILY RELOCATED AND REPLACED AS SOON AS THE CONSTRUCTION SEQUENCE IS COMPLETED, BUT NO LATER THAN THE END OF THE CONSTRUCTION DAY.

THE SEPARATION DISTANCE BETWEEN EACH BARRIER WILL BE A MAXIMUM OF 8 FEET.

THE OBSERVATION LIGHTS WILL BE THE SAME FIXTURES AS USED ON A BARRICADE WITH A RED STEADY BURN LIGHT. THE FIXTURES WILL BE MOUNTED 2 FEET FROM EITHER END OF THE BARRIER AND CENTERED ON THE BARRIER.

ONE 18" SQUARE ORANGE FLAG, EXTENDED, WITH MOUNTING DOWEL AND BRACKET. MOUNT THE FLAG CENTERED WITH THE LENGTH AND WIDTH OF THE BARRIER. THE HEIGHT OF THE FLAG WILL BE 30"±.

11 BARRIERS WITH OBSTRUCTION LIGHTS AND FLAG ARE REQUIRED.

THE BARRIERS WILL BE LOCATED AS SHOWN ON SHEET 4.

PAYMENT FOR PROVIDING AND MAINTAINING THE BARRIERS, INCLUDING THE LIGHTS, FLAGS, AND SIGN SHALL BE PAID FOR UNDER ITEM AR150530 - TRAFFIC MAINTENANCE, PER L.S.

REVISION	DATE	BY

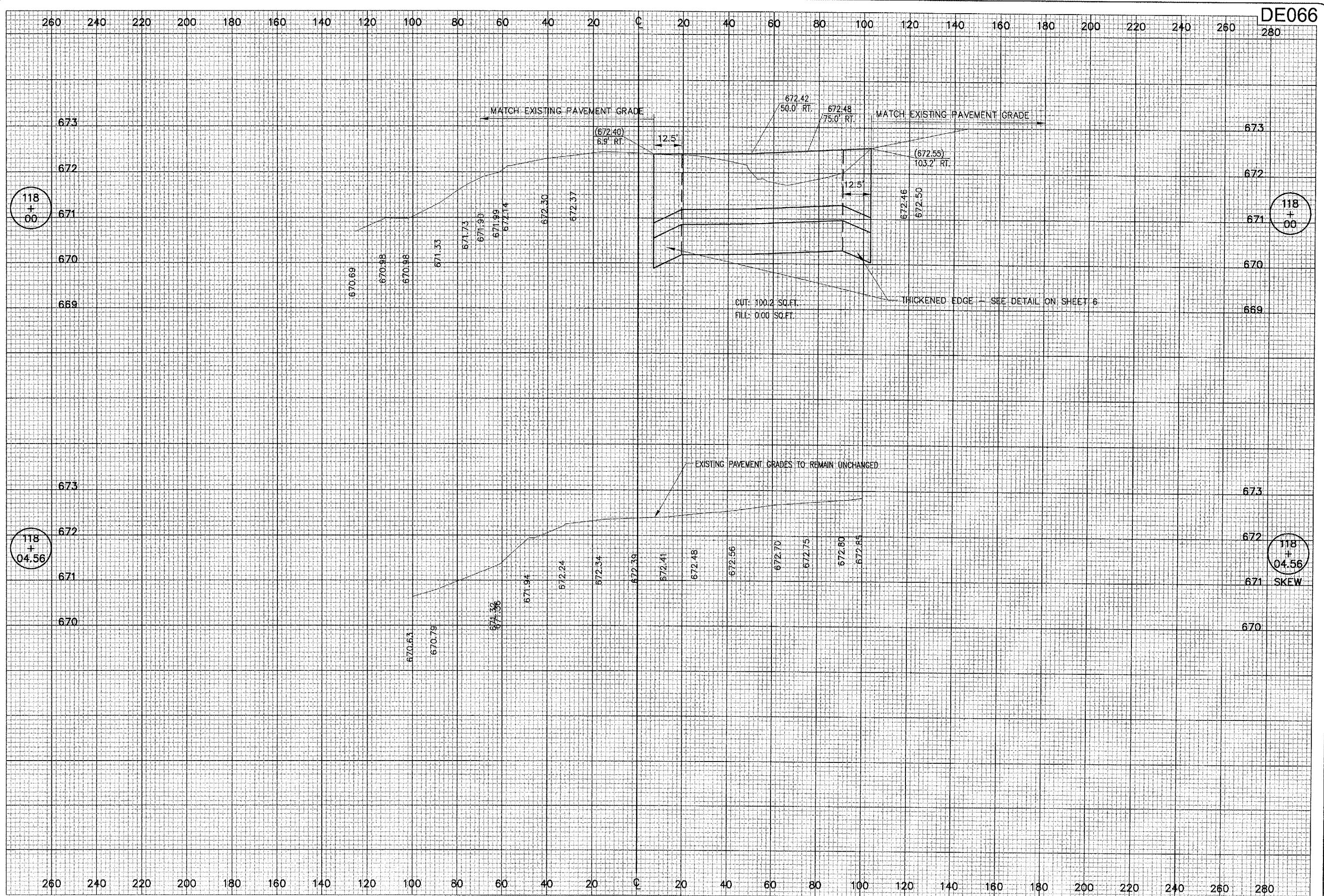
**DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 I.L. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

I.E. Project No. 802-06TXVD 0800	
Revision	R-501BAR.DWG
Scale	N/A
Date	03/02/06
LAYOUT	RAW 03/02/06
DRAWN	CCC 03/02/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1529 South Sixth Street  
 Springfield, Illinois 62703-2886  
 Offices Nationwide

TAXIWAY "C"  
 RECONSTRUCTION  
 LOW PROFILE BARRIER  
 DETAIL AND NOTES

APR 27, 2006 11:11 PM RAW  
I:\AIRPORTS\DECATUR\802-06TXY\AIRPORT\ SHEET'S\R-301-XS.DWG - Layout1



DE066  
280

118 + 00

118 + 00

118 + 04.56

118 + 04.56

DATE	REVISION

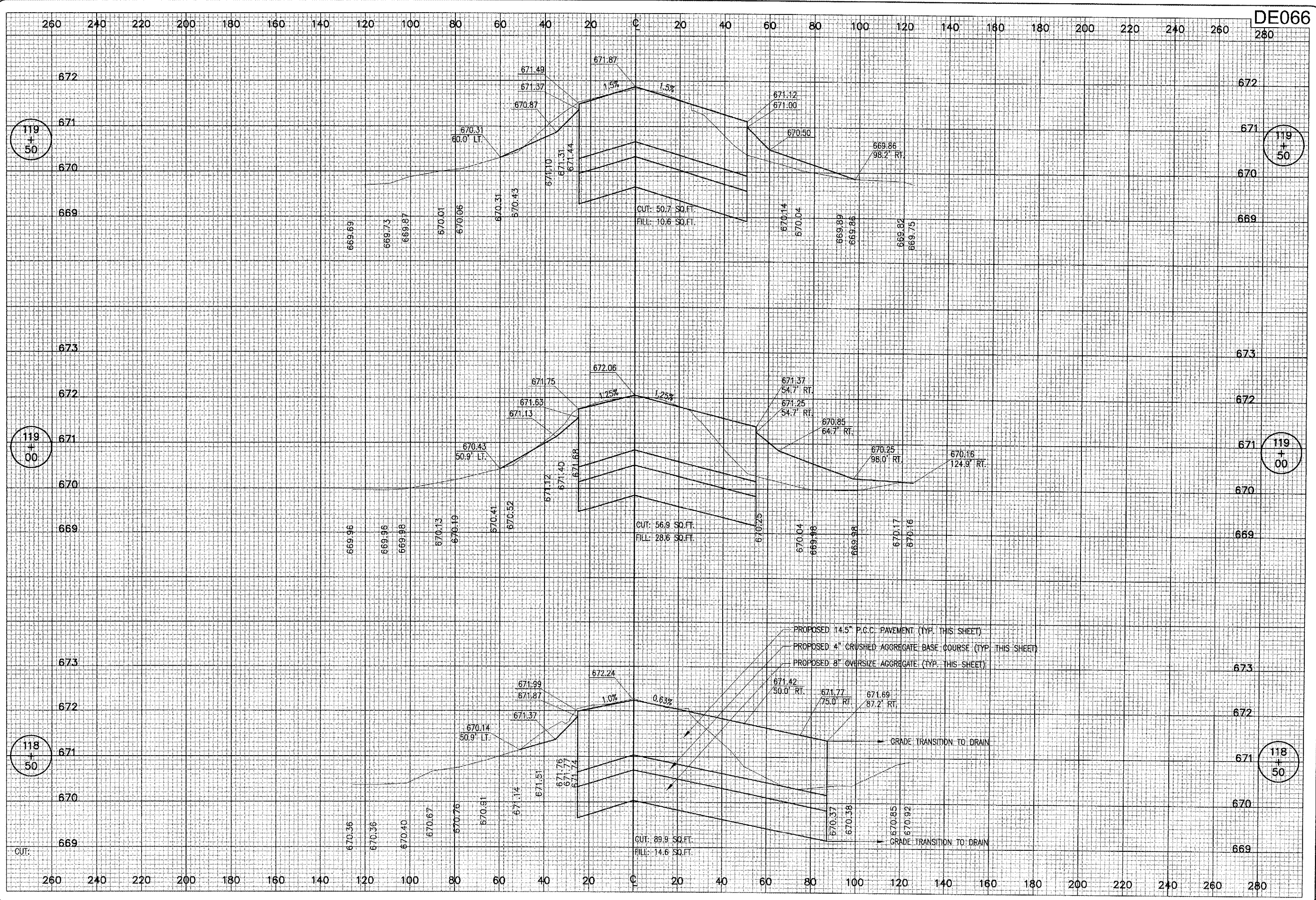
DECATUR, ILLINOIS  
A.I.P. PROJ.: 3-17-00

IEB Project No. 802-06TXYD 0800	
Drawings R-301-XS.DWG	
Scale	
Date 02/17/06	
LAYOUT BKG 02/17/06	
DRAWN BKG 02/17/06	
REVIEWED WJM 03/31/06	

HANSON  
Hanson Professional Services Inc.  
1525 South Sixth Street  
Springfield, Illinois 62703-2885  
Offices Nationwide

TAXIWAY "C"  
RECONSTRUCTION  
PROPOSED  
CROSS-SECTIONS  
STA. 117+50 TO STA. 118+00

APR 27, 2006 11:11 PM RAW  
 I:\AIRPORTS\DECATUR\B02-061XX\AIRPORT SHEETS\R-302-XS.DWG - Layout1



DE066

DATE	REVISION

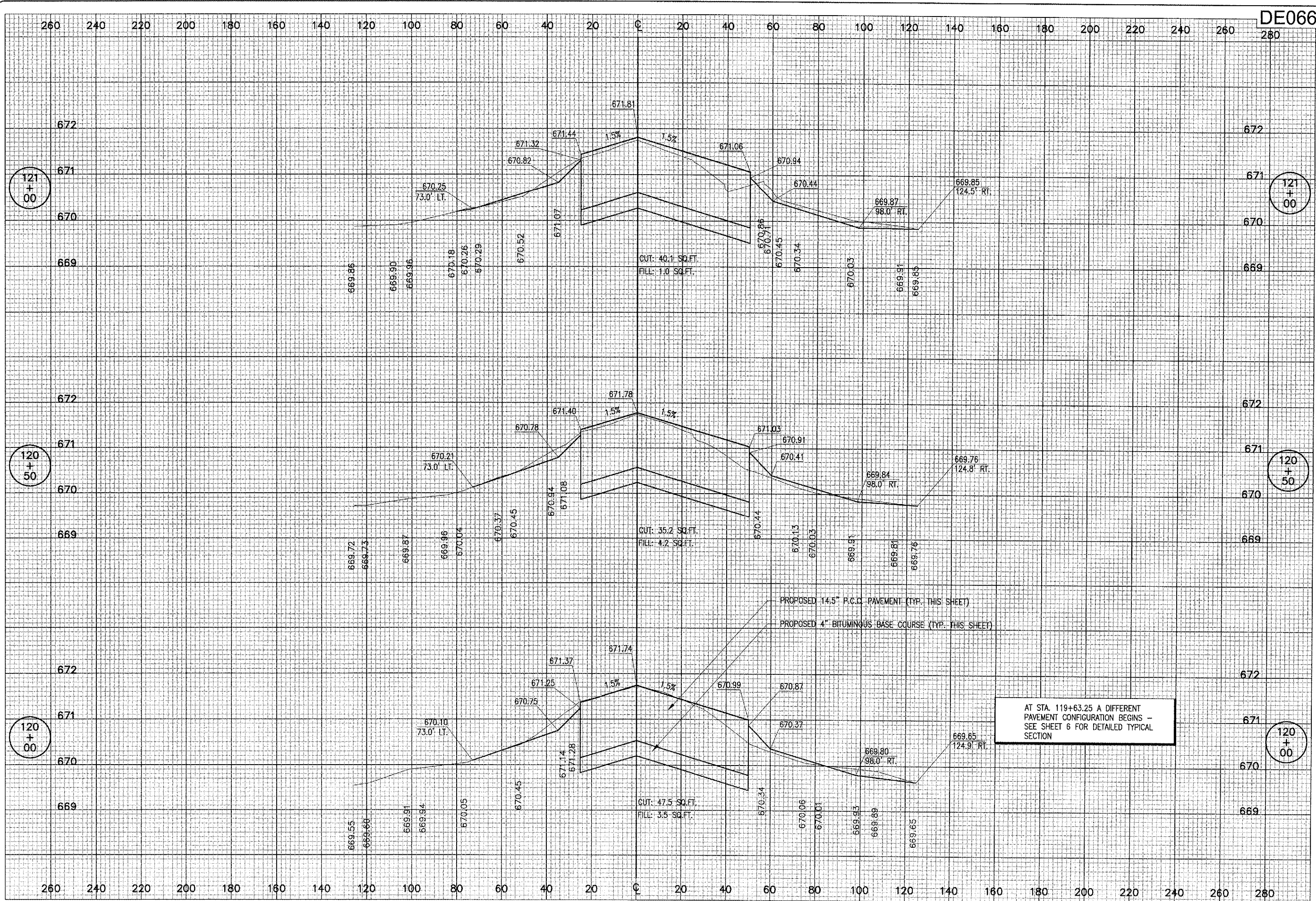
**HA** **DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 ILL. PROJ.: DEC-3588  
 A.I.P. PROJ.: 3-17-00

HE Project No.	802-061XXD.0800
Filename	R-302-XS.DWG
Scale	
Date	02/17/06
LAYOUT	BKB 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2888  
 Offices Nationwide

**TAXIWAY "C"**  
**RECONSTRUCTION**  
 PROPOSED  
 CROSS-SECTIONS  
 STA. 118+50 TO STA. 119+50

APR 27, 2006 11:12 PM RAW  
 I:\AIRPORTS\DECATUR\B02-06TXY\AIRPORT\SHEETS\R-303-XS.DWG - Layout1



AT STA. 119+63.25 A DIFFERENT PAVEMENT CONFIGURATION BEGINS - SEE SHEET 6 FOR DETAILED TYPICAL SECTION

PROPOSED 14.5" P.C.C. PAVEMENT (TYP. THIS SHEET)  
 PROPOSED 4" BITUMINOUS BASE COURSE (TYP. THIS SHEET)

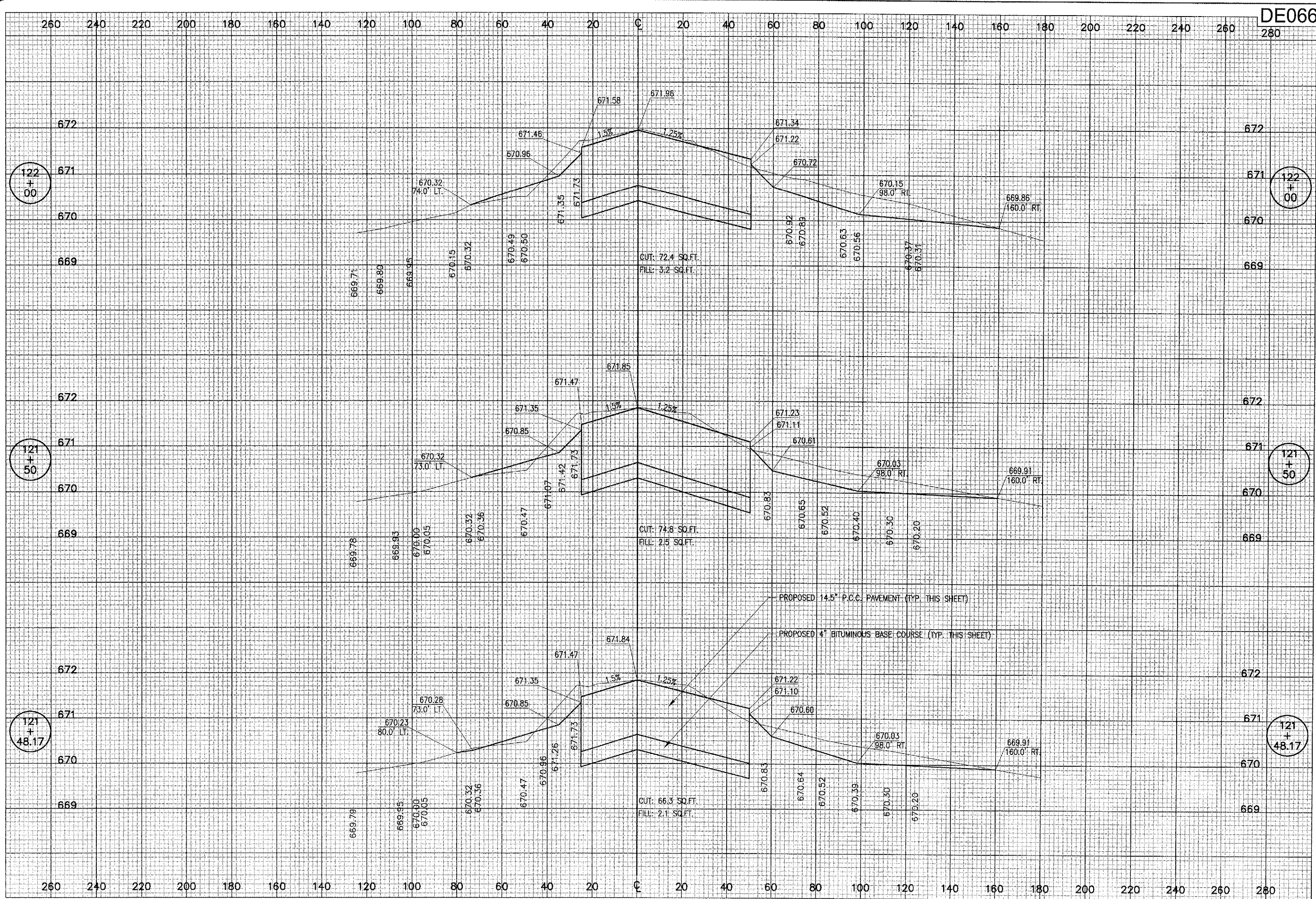
DATE	REVISION	BY

**DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 I.L. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

HE Project No.	802-06TXYD 0800
Filename	R-303-XS.DWG
Scale	
Date	02/17/06
LAYOUT	BKG 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62709-2886  
 Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**  
 PROPOSED CROSS-SECTIONS  
 STA. 120+00 TO STA. 121+00



DE066

BY	
REVISION	
DATE	

**DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 A.I.P. PROJ.: 3-17-00  
 ILL. PROJ.: DEC-3588

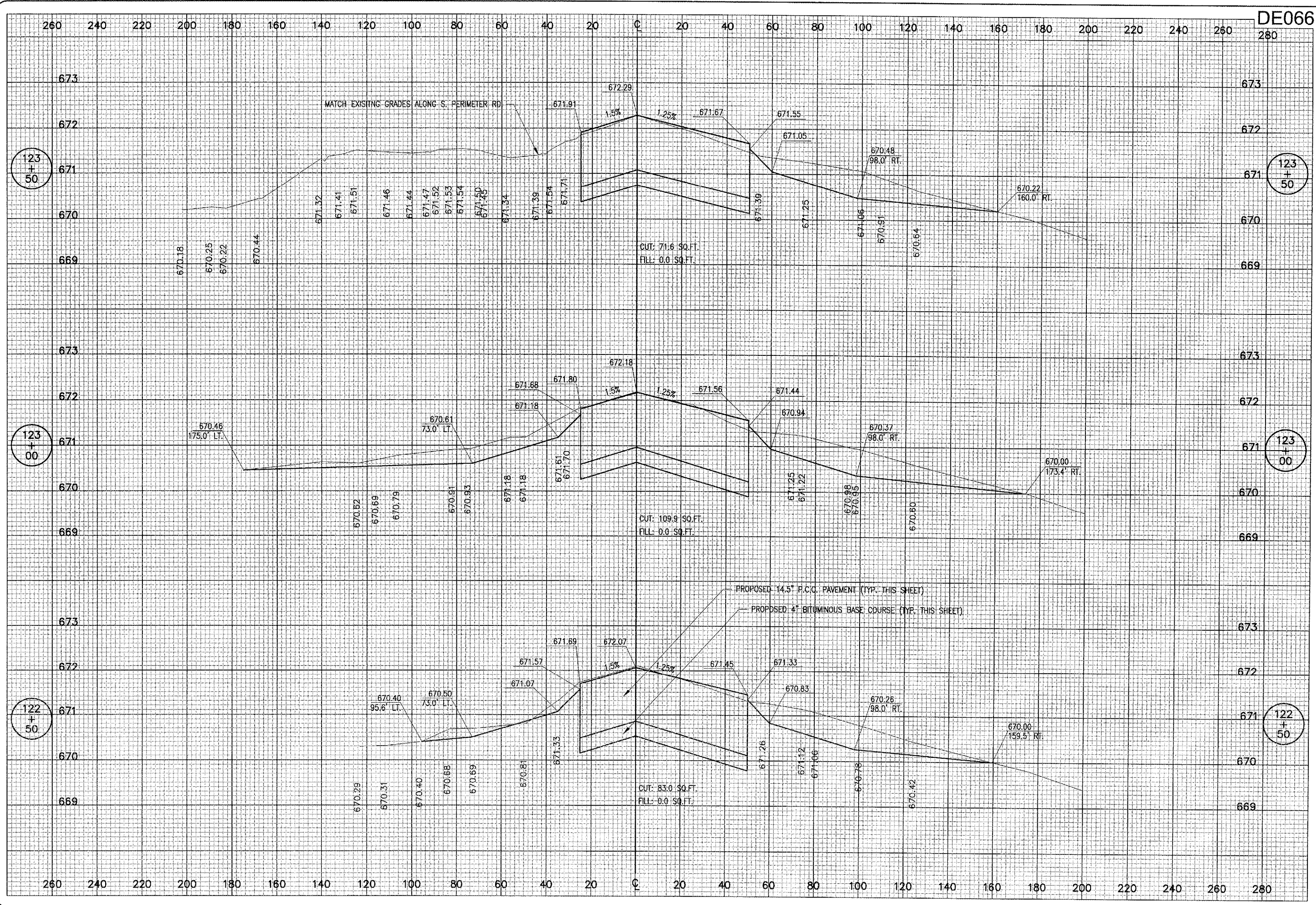
File Project No.	802-06TXYD 0800
Element	R-304-XS.DWG
Scale	
Date	02/17/06
LAYOUT	BKB 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2886  
 Offices Nationwide

**TAXIWAY "C"**  
**RECONSTRUCTION**  
 PROPOSED  
 CROSS-SECTIONS  
 STA. 121+48.17 TO STA. 122+00



APR 27, 2006 11:14 PM RAW  
 I:\AIRPORTS\DECATUR\802-06TX\AIRPORT SHEETS\R-305-XS.DWG - Layout1



DE066

123  
+  
50

123  
+  
50

123  
+  
00

123  
+  
00

122  
+  
50

122  
+  
50

BY	
REVISION	
DATE	

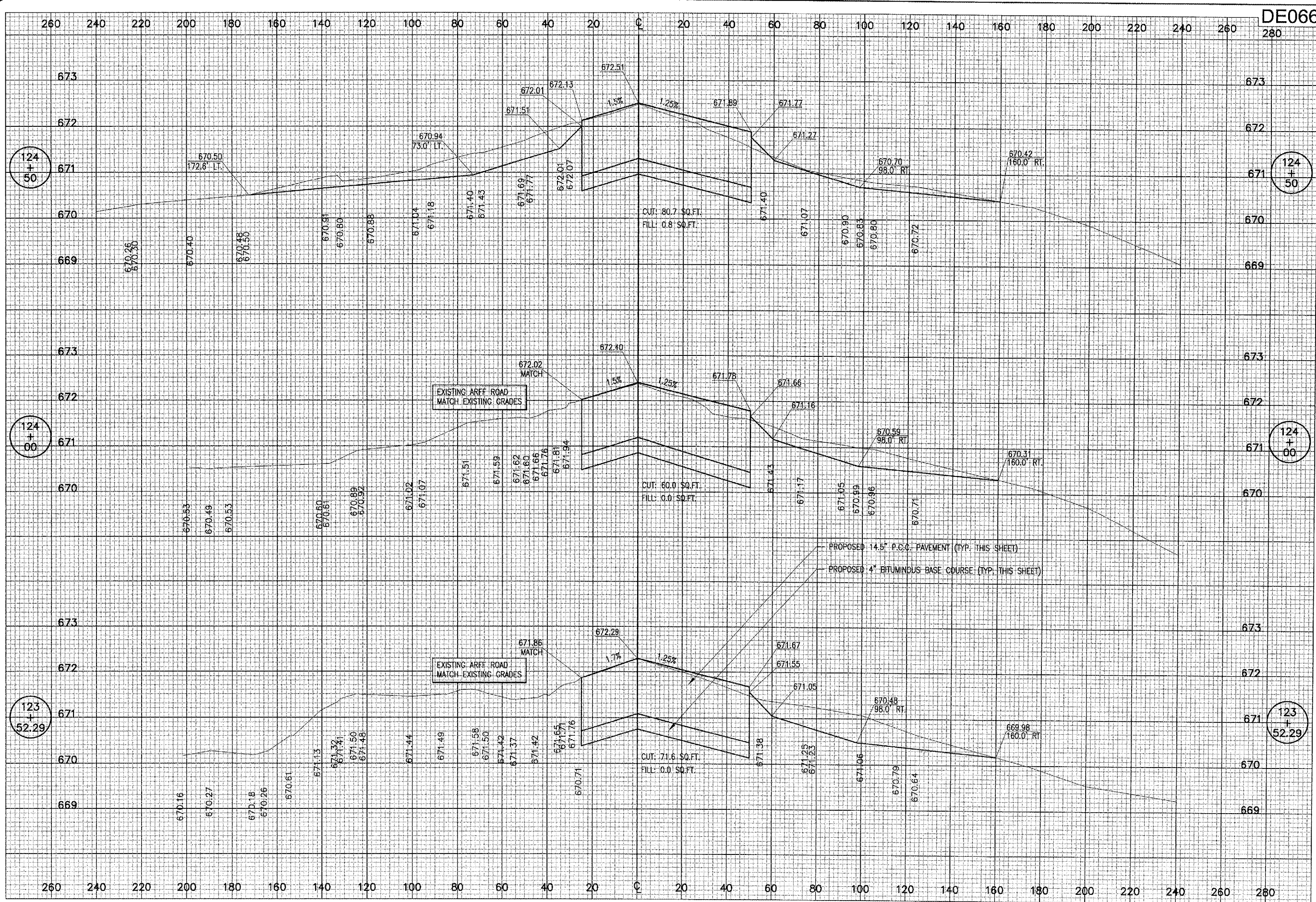
**TA** *DECATUR AIRPORT*  
**DECATUR, ILLINOIS**  
 IL PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

HE Project No.	802-06TXD 0800
Filename	R-305-XS.DWG
Scale	
Date	02/17/06
LAYOUT	BKB 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1626 South Sixth Street  
 Springfield, Illinois 62703-2886  
 Offices Nationwide

**TAXIWAY "C"  
 RECONSTRUCTION**  
 PROPOSED  
 CROSS-SECTIONS  
 STA. 122+50 TO STA. 123+50

APR 27, 2006 11:15 PM RAW  
 I:\AIRPORTS\DECATUR\802-061XY\AIRPORT SHEETS\R-306-XS.DWG - Layout1



DE066

DATE	REVISION	BY

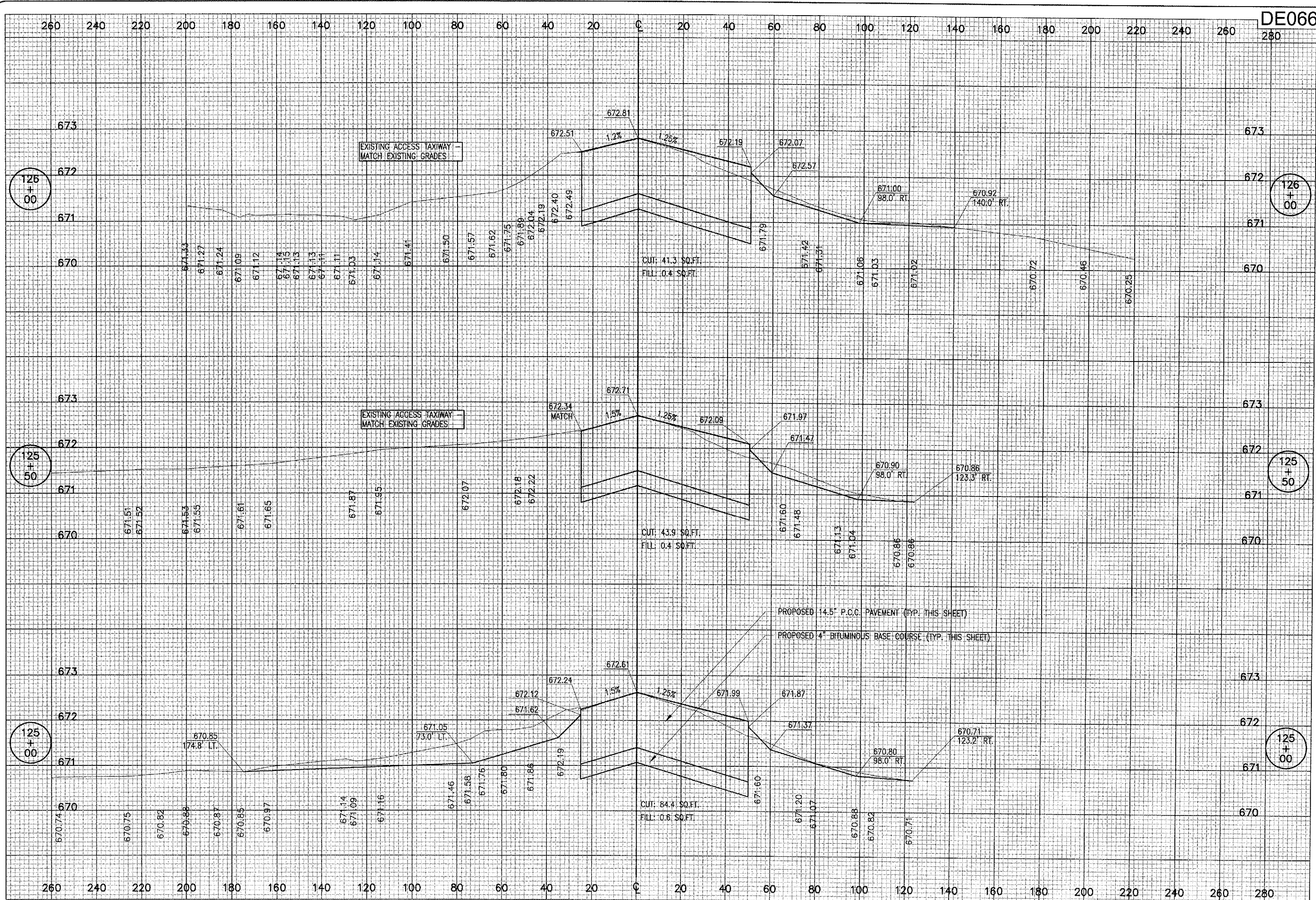
**HA**  
**DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 A.I.P. PROJ.: 3-17-00  
 I.L. PROJ.: DEC-3588

Project No.	802-061XYD-0800
Station	R-306-XS.DWG
Scale	
Date	02/17/06
LAYOUT	BKB 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62705-2886  
 Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**  
 PROPOSED CROSS-SECTIONS  
 STA. 123+52.29 TO STA. 124+50

APR 27, 2006 11:15 PM RAW  
 I:\AIRPORTS\DECATUR\802-06TAXY\AIRPORT\ SHEETS\R-307-XS.DWG - Layout1



DE066  
280

126  
+  
00

126  
+  
00

125  
+  
50

125  
+  
50

125  
+  
00

125  
+  
00

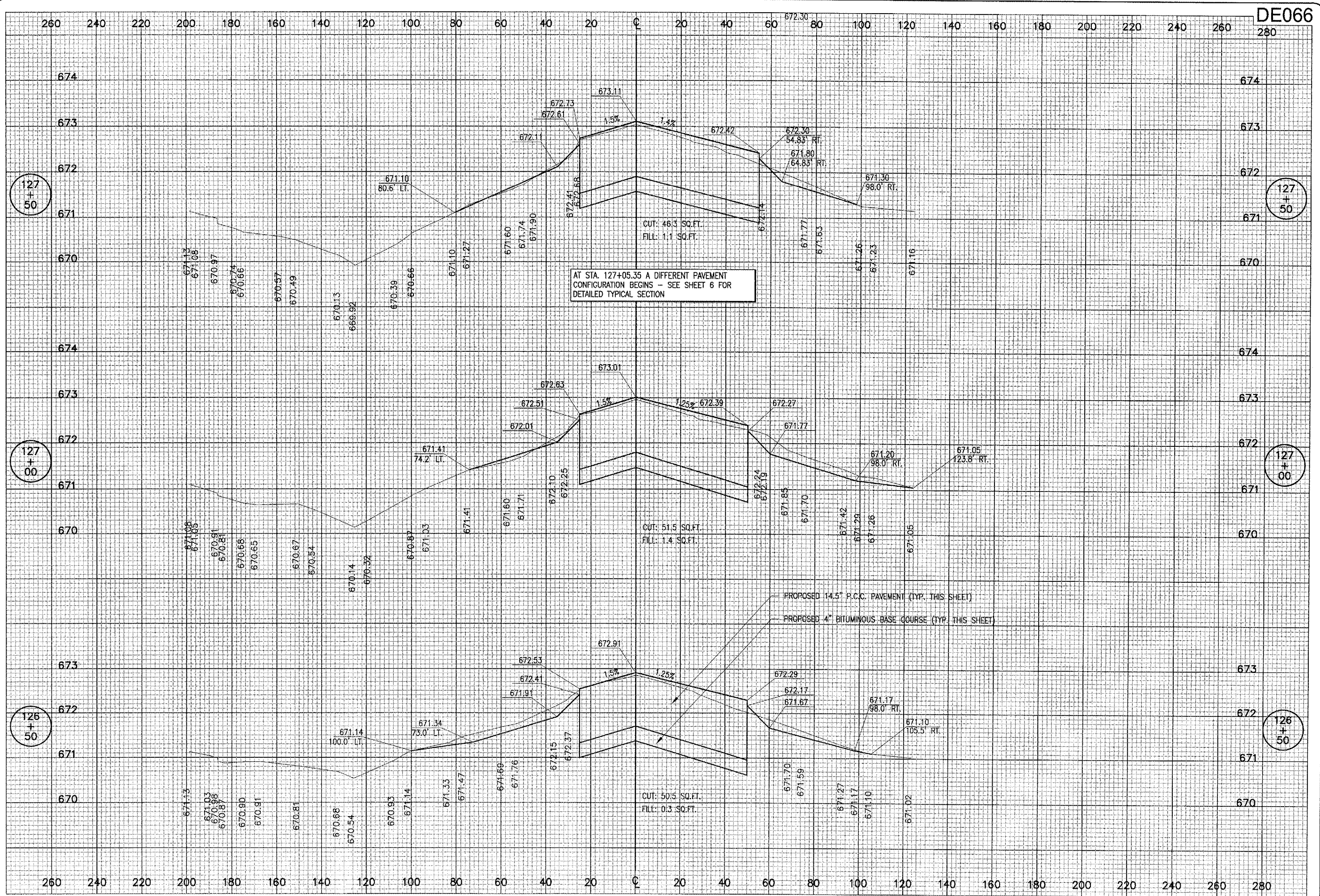
DATE	REVISION	BY

**DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 ILL. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

Project No. 802-06TAXY 0800	
Drawings R-307-XS.DWG	
Scale	
Date 02/17/06	
LAYOUT BKB 02/17/06	
DRAWN BKB 02/17/06	
REVIEWED WJM 03/31/06	

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2986  
 Offices Nationwide

**TAXIWAY "C"  
 RECONSTRUCTION**  
 PROPOSED  
 CROSS-SECTIONS  
 STA. 125+00 TO STA. 126+00



DE066

DATE	REVISION	BY

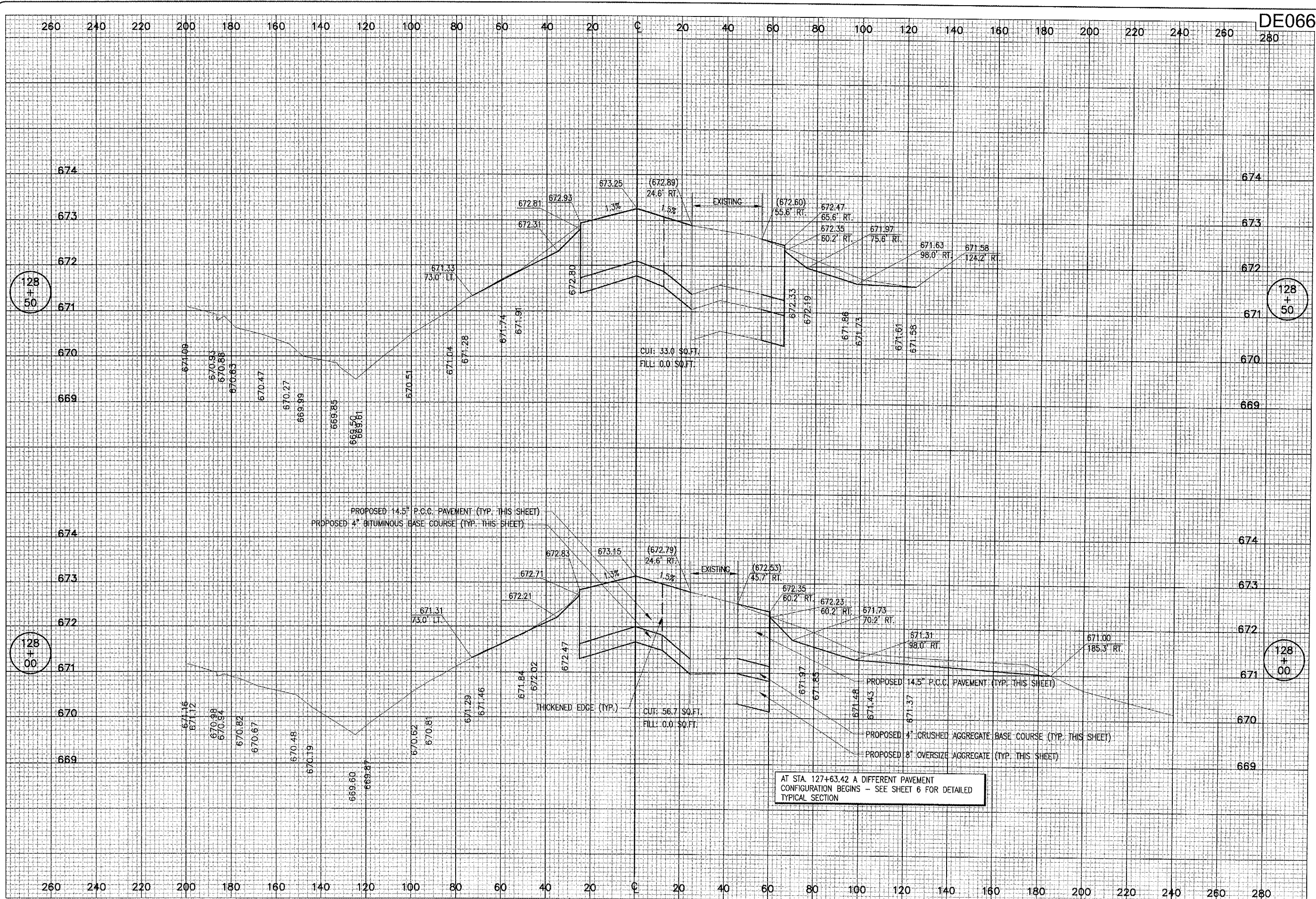
**HA** *DECATUR AIRPORT*  
**DECATUR, ILLINOIS**  
 ILL. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

HS Project No.	802-06TXYD 0800
Planets	R-308-XS.DWG
Scale	
Date	02/17/06
LAYOUT	BKB 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62705-2886  
 Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**  
 PROPOSED CROSS-SECTIONS  
 STA. 126+50 TO STA. 127+50

APR 27, 2006 11:16 PM RAW  
 I:\AIRPORTS\DECATUR\802-06TX\AIRPORT\ SHEETS\R-309-XS.DWG - Layout1



DE066

BY	
REVISION	
DATE	

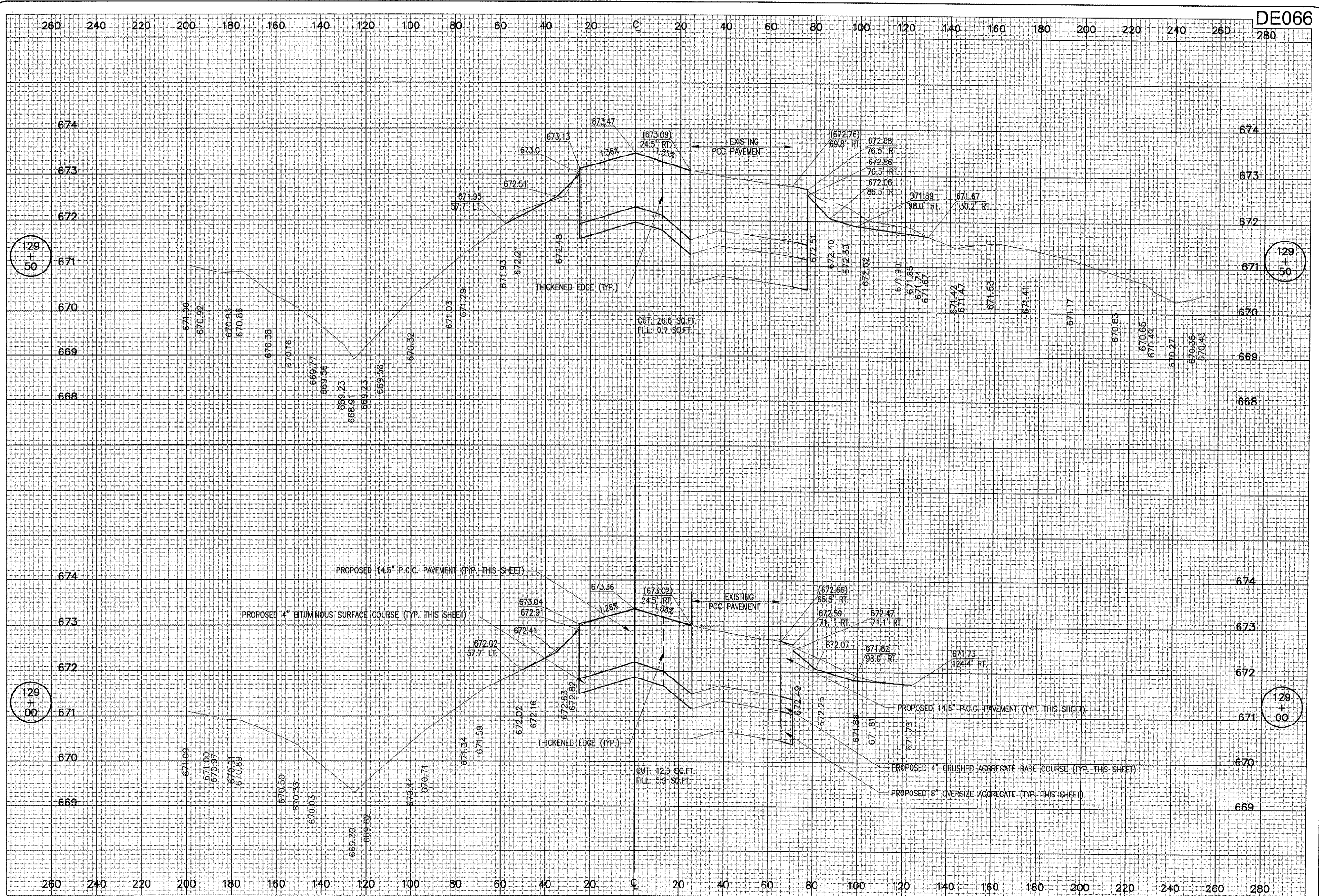
**HA** **DECATUR AIRPORT**  
**DECATUR, ILLINOIS**  
 ILL. PROJ.: DEC-3568 A.I.P. PROJ.: 3-17-00

152 Project No.	802-06TXVD 0800
Electronic	R-309-XS.DWG
Scale	
Date	02/17/06
LAYOUT	BKG 02/17/06
DRAWN	BKG 02/17/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services Inc.  
 1526 South Sixth Street  
 Springfield, Illinois 62705-2695  
 Offices Nationwide

**TAXIWAY "C"**  
**RECONSTRUCTION**  
 PROPOSED  
 CROSS-SECTIONS  
 STA. 128+00 TO STA. 128+50

APR 27, 2006 11:17 PM RAW  
 I:\AIRPORTS\DECATUR\802-06TX\AIRPORT SHEETS\R-310-XS.DWG - Layout1



DE066  
 280

129  
 +  
 50

129  
 +  
 50

129  
 +  
 00

129  
 +  
 00

DATE	REVISION

BY



**DECATUR, ILLINOIS**

IL PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

Project No.	802-06TXD-0800
Revision	R-310-XS.DWG
Date	02/17/06
LAYOUT	BKB 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

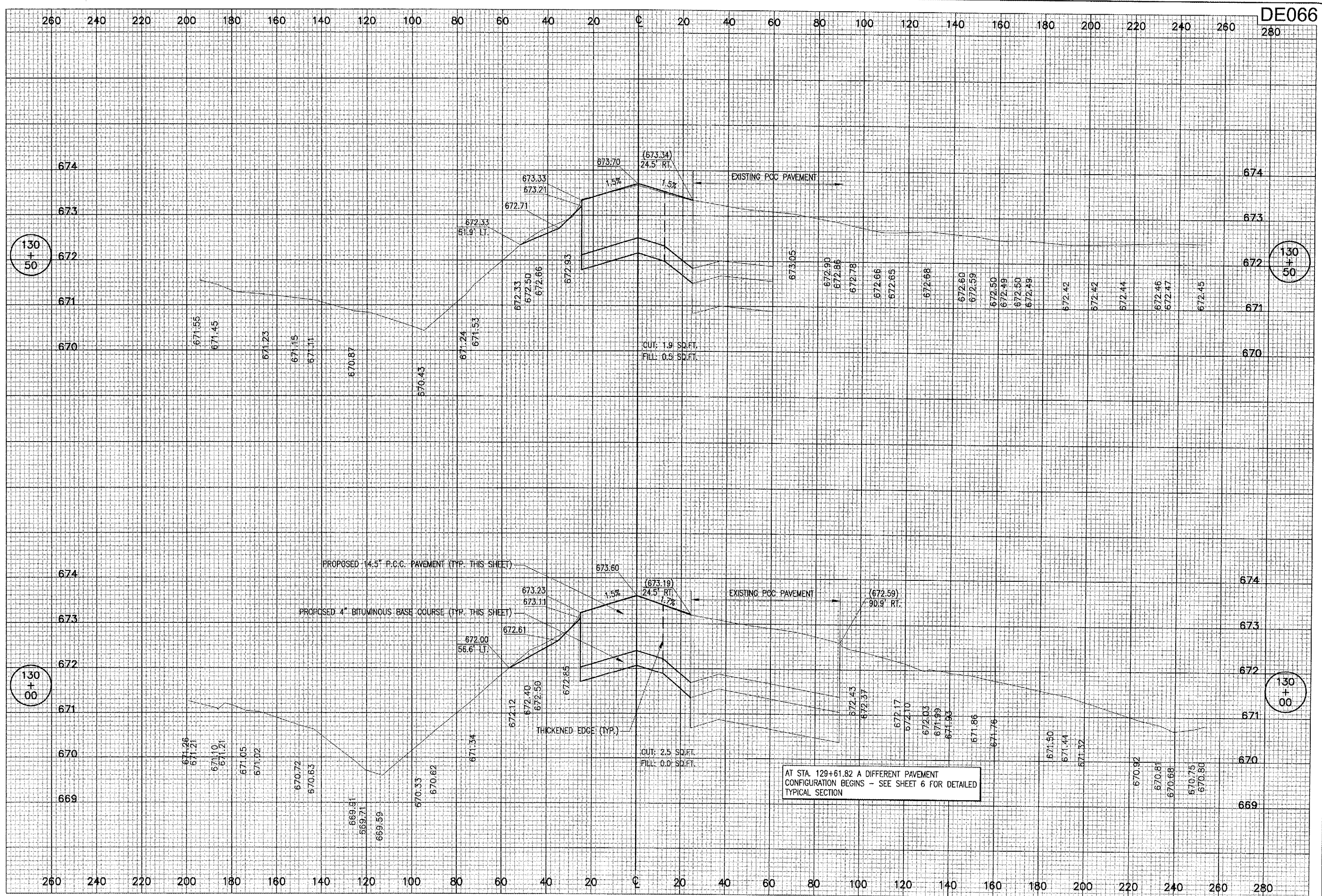


**HANSON**

Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62705-2886  
 Offices Nationwide

**TAXIWAY "C"  
 RECONSTRUCTION**

PROPOSED  
 CROSS-SECTIONS  
 STA. 129+00 TO STA. 129+50



DE066

DATE	REVISION

**HA** **DECATUR AIRPORT**  
**DECATUR, ILLINOIS**

IL PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

HE Project No.	802-061XYD_0800
Drawings	R-311-XS.DWG
Scale	
Date	02/17/06
LAYOUT	BKB 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

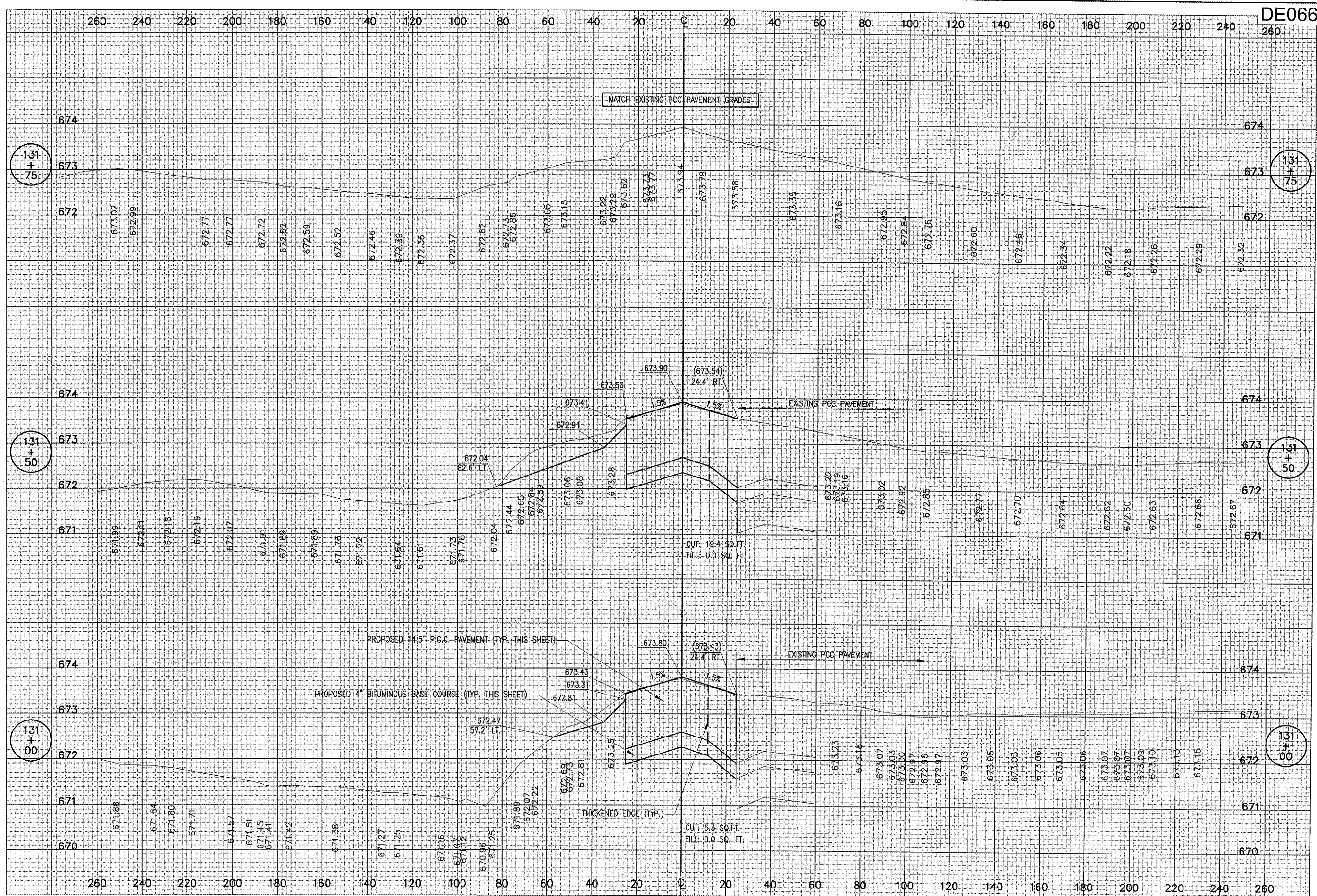
**HANSON**

Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2868  
 Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**

PROPOSED CROSS-SECTIONS  
 STA. 130+00 TO STA. 130+50

APR 27, 2006 11:19 PM RAW  
 I:\AIRPORTS\DECATUR\802-06TXY\AIRPORT\ SHEETS\R-312-XS.DWG - Layout1



DE066

BY	
DATE	
REVISION	

**HA**  
 DECATUR AIRPORT  
 DECATUR, ILLINOIS  
 I.L. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

IE Project No.	802-06TXYD 0800
File Name	R-312-XS.DWG
Scale	02/17/06
Date	
LAYOUT	BKB 02/17/06
DRAWN	BKB 02/17/06
REVIEWED	WJM 03/31/06

**HANSON**  
 Hanson Professional Services, Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2885  
 Offices Nationwide

**TAXIWAY "C"  
 RECONSTRUCTION**  
 PROPOSED  
 CROSS-SECTIONS  
 STA. 131+00 TO STA. 131+75



ELECTRICAL LEGEND - ONE-LINE DIAGRAM	
	CABLE TERMINATOR/LUG
	TRANSFORMER
	DISCONNECT SWITCH
	FUSIBLE DISCONNECT SWITCH
	CIRCUIT BREAKER
	THERMAL MAGNETIC CIRCUIT BREAKER
	FUSE
	TRANSIENT VOLTAGE SURGE SUPPRESSOR OR SURGE PROTECTOR DEVICE
	GROUND - GROUND ROD, GROUNDING ELECTRODE, OR AT EARTH POTENTIAL
	INDICATING LIGHT
	MOTOR
	LOAD, MOTOR, # = HORSEPOWER
	ELECTRIC UTILITY METER BASE
	JUNCTION BOX WITH SPLICE
	EQUIPMENT, XXX = DEVICE DESCRIPTION
	GROUND BUS OR TERMINAL
	NEUTRAL BUS
	PANELBOARD WITH MAIN LUGS
	PANELBOARD WITH MAIN BREAKER
	FUSE PANEL WITH MAIN FUSE PULLOUT
	DUPLEX RECEPTACLE 120V SINGLE PHASE GROUNDING TYPE
	CONTROL STATION
	TRANSFER SWITCH
	ENGINE GENERATOR SET

ELECTRICAL LEGEND - SCHEMATIC	
	NORMALLY OPEN (N.O.) CONTACT
	NORMALLY CLOSED (N.C.) CONTACT
	STARTER COIL, * = STARTER NUMBER
	OVERLOAD RELAY CONTACT
	CONTROL RELAY, * = CONTROL RELAY NUMBER
	RELAY, * = RELAY NUMBER
	TOGGLE SWITCH / 2 POSITION SWITCH
	2-POSITION SELECTOR SWITCH
	3-POSITION SELECTOR SWITCH (H-O-A SHOWN)
	2 POLE DISCONNECT SWITCH
	3 POLE DISCONNECT SWITCH
	PHOTOCELL
	TERMINAL BLOCK, * = TERMINAL NUMBER
	DEVICE TERMINAL, * = DEVICE TERMINAL NUMBER
	INTERNAL PANEL WIRING
	FIELD WIRING
	FUSE
	GROUND BUS OR TERMINAL
	NEUTRAL BUS
	GROUND, GROUND ROD
	INDUSTRIAL CONTROL RELAY OR LIGHTING CONTACTOR
	S1 CUTOUT HANDLE REMOVED
	S1 CUTOUT HANDLE INSERTED
	N.O. THERMAL SWITCH
	N.C. THERMAL SWITCH

ELECTRICAL ABBREVIATIONS	
A.F.F.	ABOVE FINISHED FLOOR
A. AMP	AMPERES
ATS	AUTOMATIC TRANSFER SWITCH
AWG	AMERICAN WIRE GAUGE
BKR	BREAKER
C	CONDUIT
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CR	CONTROL RELAY
CU	COPPER
DPDT	DOUBLE POLE DOUBLE THROW
DPST	DOUBLE POLE SINGLE THROW
EM	EMERGENCY
EMT	ELECTRICAL METALLIC TUBING
ENCL	ENCLOSURE
EP	EXPLOSION PROOF
ES	EMERGENCY STOP
ETM	ELAPSE TIME METER
GFCI	GROUND FAULT CIRCUIT INTERRUPTER
GFI	GROUND FAULT INTERRUPTER
GND	GROUND
GRSC	GALVANIZED RIGID STEEL CONDUIT
HID	HIGH INTENSITY DISCHARGE
HOA	HAND OFF AUTOMATIC
HP	HORSEPOWER
HPS	HIGH PRESSURE SODIUM
J	JUNCTION BOX
KVA	KILOVOLT AMPERE(S)
KW	KILOWATTS
LC	LIGHTING CONTACTOR
LTFMC	LIQUID TIGHT FLEXIBLE METAL CONDUIT (UL LISTED)
LP	LIGHTING PANEL
MAX	MAXIMUM
MCB	MAIN CIRCUIT BREAKER
MCM	THOUSAND CIRCULAR MIL
MDP	MAIN DISTRIBUTION PANEL
MH	METAL HALIDE
MIN	MINIMUM
MLO	MAIN LUGS ONLY
NC	NORMALLY CLOSED
NO	NORMALLY OPEN
NTS	NOT TO SCALE
OHE	OVERHEAD ELECTRIC
OL	OVERLOAD
PB	PULL BOX
PC	PHOTO CELL
PDB	POWER DISTRIBUTION BLOCK
PNL	PANEL

ELECTRICAL ABBREVIATIONS (CONTINUED)	
RCPT	RECEPTACLE
R	RELAY
S	STARTER
SPD	SURGE PROTECTION DEVICE
SPST	SINGLE POLE SINGLE THROW
TVSS	TRANSIENT VOLTAGE SURGE SUPPRESSOR
TYP	TYPICAL
UG	UNDERGROUND
UGE	UNDERGROUND ELECTRIC
V	VOLTS
W/	WITH
W/O	WITHOUT
WP	WEATHER PROOF
XFER	TRANSFER
XFMR	TRANSFORMER

AIRPORT EQUIPMENT ABBREVIATIONS	
CCR	CONSTANT CURRENT REGULATOR
MIRL	MEDIUM INTENSITY RUNWAY LIGHT
MIL	MEDIUM INTENSITY TAXIWAY LIGHT
NDB	NON-DIRECTIONAL BEACON
PAPI	PRECISION APPROACH PATH INDICATOR
PLASI	PULSE LIGHT APPROACH SLOPE INDICATOR
REIL	RUNWAY END IDENTIFIER LIGHT
VASI	VISUAL APPROACH SLOPE INDICATOR
WC	WIND CONE

ELECTRICAL LEGEND - PLANS	
	CONDUIT (EXPOSED)
	CONDUIT OR UNIT DUCT (CONCEALED OR BURIED)
	DUCT
	BURIED/UNDERGROUND ELECTRIC
	UNDERGROUND ELECTRIC
	OVERHEAD ELECTRIC
	POLE MOUNTED HID FIXTURE
	DUPLEX CONVENIENCE RECEPTACLE, 120V, SINGLE PHASE, GROUNDING TYPE, 48" A.F.F. EXCEPT AS NOTED
	WALL OR CEILING MTD. JUNCTION BOX. CONFIGURATION VARIES WITH USE
	SINGLE THROW DISCONNECT SWITCH
	SINGLE THROW, FUSIBLE DISCONNECT SWITCH
	ENCLOSED CIRCUIT BREAKER
	CONTROL PANEL
	MOTOR. ESTIMATED H.P. AS INDICATED.
	MOTOR
	TRANSFORMER
	ELECTRIC UTILITY METER
	ENCLOSURE
	CIRCUIT BREAKER PANEL-SEE SCHEDULES
	GROUND ROD
	LONG SLASHES INDICATE NEUTRAL. SHORT SLASHES INDICATE HOT OR SWITCHED LEG. G = SEPARATE GROUND WIRE.
	HOMERUN TO PANEL. PNL A INDICATES PANEL. 1,3,5 INDICATES CIRCUIT NUMBERS
	SINGLE POLE SWITCH
	FRACTIONAL HP STARTER
	CONTACTOR
	SURFACE MOUNTED OR CHAIN HUNG FLUORESCENT FIXTURE
	WALL OR CEILING MTD. INCANDESCENT OR HID FIXTURE.

- NOTES:**
- ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 (MOST CURRENT ISSUE IN FORCE), THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE SHALL NOT BE PERMITTED.
  - CONTRACTOR SHALL COORDINATE WORK AND ANY POWER OUTAGES WITH THE RESPECTIVE FACILITY OWNER PERSONNEL AND THE AIRPORT DIRECTOR.
  - COLOR CODE PHASE AND NEUTRAL CONDUCTOR INSULATION FOR NO. 6 AWG OR SMALLER. PROVIDE COLORED INSULATION OR COLORED MARKING TAPE FOR PHASE AND NEUTRAL CONDUCTORS FOR NO. 4 AWG AND LARGER. INSULATED GROUND CONDUCTORS SHALL HAVE GREEN COLORED INSULATION FOR ALL CONDUCTOR AWG AND/OR KCMIL. STANDARD COLORS FOR POWER WIRING AND BRANCH CIRCUITS SHALL BE AS FOLLOWS:  
  

120/240 VAC, 1 PHASE, 3 WIRE	
PHASE A	BLACK
PHASE B	RED
NEUTRAL	WHITE
GROUND	GREEN

DATE	REVISION
 <b>DECATUR, ILLINOIS</b>	
IEL Project No.: 802-06TXYD-0800 File Name: E-141.DWG Sheet: NONE Date: 03/31/06	LAYOUT: KNL 04/22/05 DRAWN: WY 04/25/05 REVIEWED: LDP 03/30/06
<b>HANSON</b> Hanson Professional Services Inc. 1525 South State Street Springfield, Illinois 62766 Offices Nationwide	
<b>TAXIWAY "C" RECONSTRUCTION</b> ELECTRICAL LEGEND AND ABBREVIATIONS	
<b>41</b> 41 of 56 sheets	
ILL. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00	

BY	
REVISION	
DATE	

DECATUR AIRPORT  
**DECATUR, ILLINOIS**  
 ILL. PROJ.: DEC-3588  
 A.I.P. PROJ.: 3-17-00

NET Project No.	802-06TXD_0800
Employee	E-142.DWG
Scale	AS SHOWN
Date	03/31/06
LAYOUT	KNL 04/22/05
DRAWN	MV 04/25/05
REVIEWED	LDP 03/30/06

**HANSON**  
 Hanson Professional Services Inc.  
 1525 South Sixth Street  
 Springfield, Illinois 62703-2886  
 Offices Nationwide

TAXIWAY "C"  
 RECONSTRUCTION  
 ELECTRICAL SITE PLAN  
 RUNWAY 12 & RUNWAY 18  
 WIND CONES

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES. CONTACT FAA FOR ASSISTANCE IN LOCATING FAA CABLES

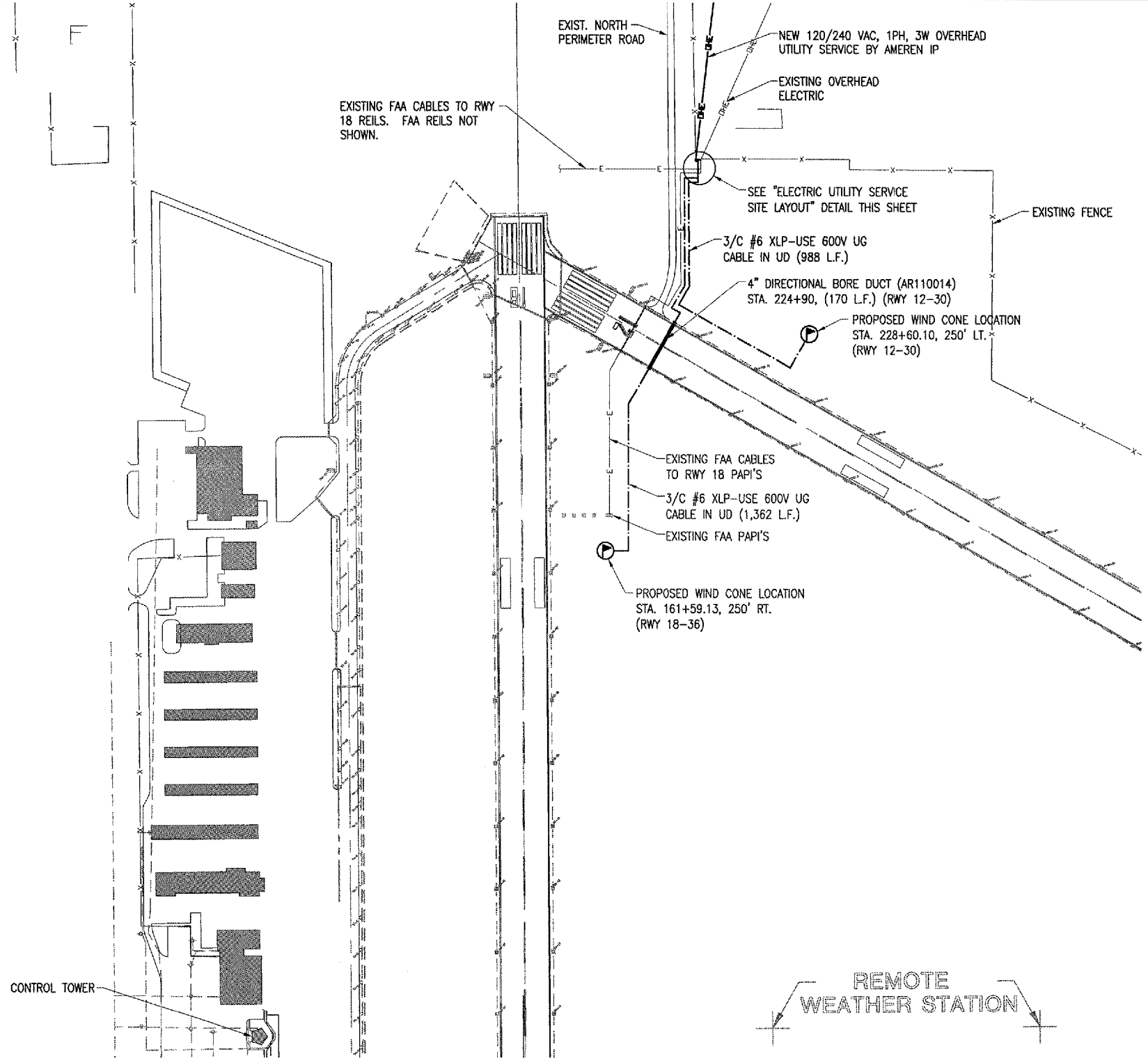
**SUMMARY OF PROPOSED QUANTITIES (THIS SHEET)**

ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
AS107408	L-806 WIND CONE 8' LIGHTED	EA	2
AS108656	3/C #6 600V UG CABLE IN UD	L.F.	2,350
AS110014	4" DIRECTIONAL BORE	L.F.	170
AS800579	ELECTRIC SERVICE FOR RWY 12 AND RWY 18 WIND CONES	L.S.	1

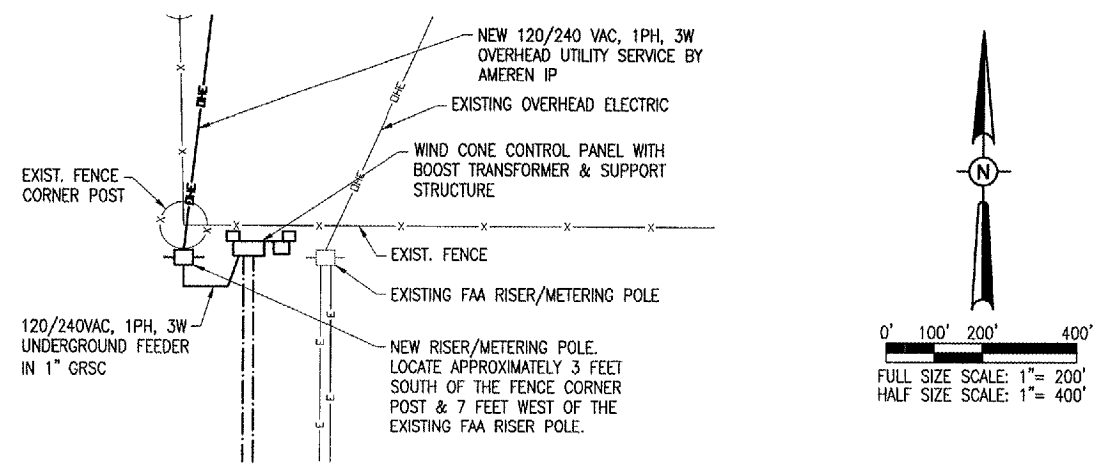
**WIND CONE NOTES**  
 THE PROPOSED WIND CONES SHALL BE INSTALLED AS SHOWN ON THIS SHEET & THE "WIND CONE ELEVATION DETAIL" SHEET, AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.  
 COORDINATE NEW ELECTRIC SERVICE WITH THE SERVING ELECTRIC UTILITY COMPANY (AMEREN IP, 2460 N. JASPER ST. MC K-30, DECATUR, IL 62526, ATTN MS. TAMMIE HAUGERUD, SENIOR ENGINEERING REP, PHONE: 217-425-6032, CELL PHONE: 217-412-6455, FAX: 217-475-8445)

**LEGEND**

	EXISTING PAVEMENT
	PROPOSED DUCT
	PROPOSED 3/C #6 XLP-USE 600V UG CABLE IN UD -- AR108656
	PROPOSED SUPPLEMENTAL WIND CONE
	EXISTING ELECTRICAL CABLES
	EXISTING UNDERGROUND ELECTRICAL CABLES (FAA, UTILITY, ETC.)



**ELECTRICAL SITE PLAN FOR  
 RWY 12 & RWY 18 WIND CONES**



**ELECTRIC UTILITY SERVICE SITE LAYOUT**  
 N.T.S.

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES. CONTACT FAA FOR ASSISTANCE IN LOCATING FAA CABLES



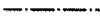


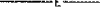

SUMMARY OF PROPOSED QUANTITIES (THIS SHEET)			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
AS107408	L-806 WIND CONE 8' LIGHTED	EA	2
AS108656	3/C #6 600V UG CABLE IN UD	L.F.	3,550
AS110014	4" DIRECTIONAL BORE	L.F.	170
AS800580	ELECTRIC SERVICE FOR RWY 24 AND RWY 30 WIND CONES	L.S.	1

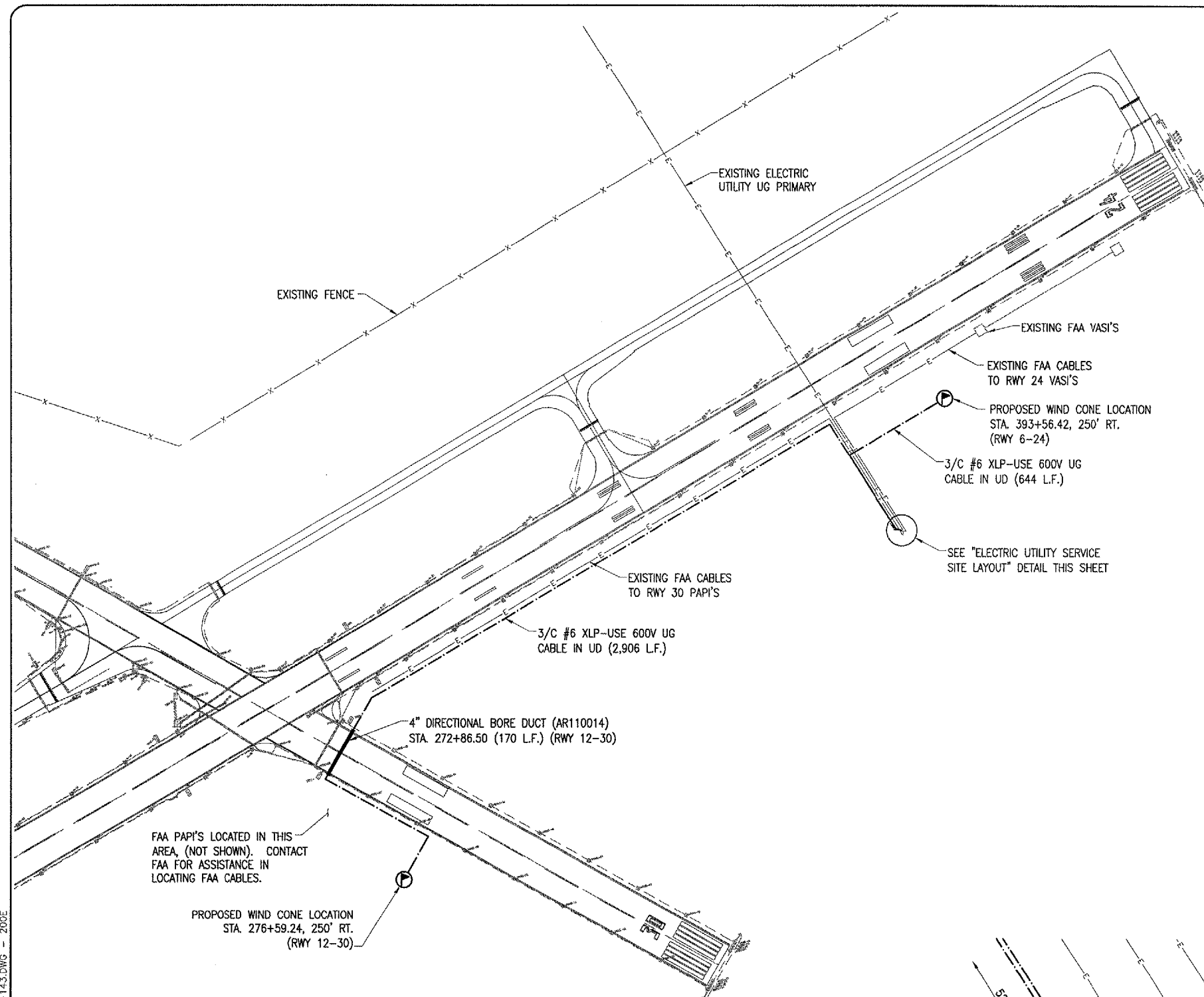
**WIND CONE NOTES**

THE PROPOSED WIND CONES SHALL BE INSTALLED AS SHOWN ON THIS SHEET & THE "WIND CONE ELEVATION DETAIL" SHEET, AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

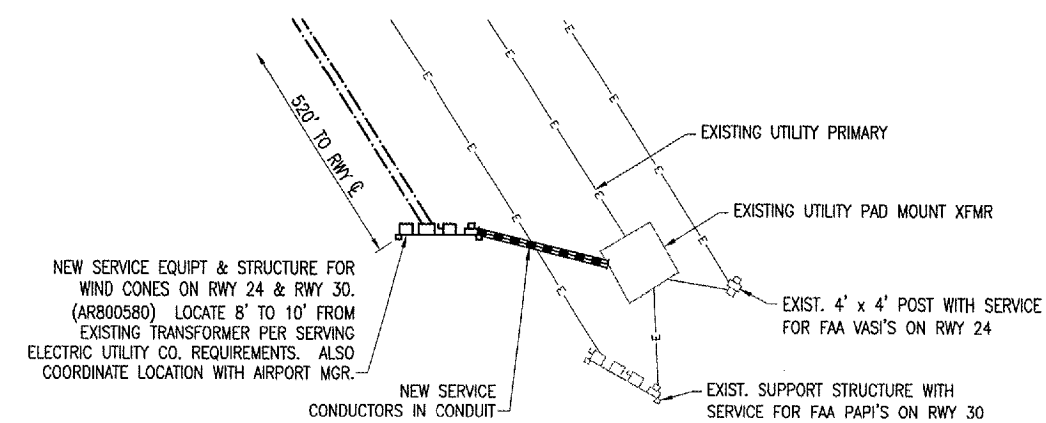
COORDINATE NEW ELECTRIC SERVICE WITH THE SERVING ELECTRIC UTILITY COMPANY (AMEREN IP, 2460 N. JASPER ST. MC K-30, DECATUR, IL 62526, ATTN MS. TAMMIE HAUGERUD, SENIOR ENGINEERING REP, PHONE: 217-425-6032, CELL PHONE: 217-412-6455, FAX: 217-475-8445)

**LEGEND**

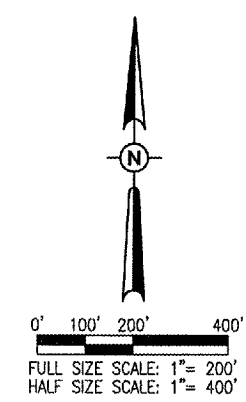
-  EXISTING PAVEMENT
-  PROPOSED DUCT
-  PROPOSED 3/C #6 XLP-USE 600V UG CABLE IN UD - AR108656
-  PROPOSED SUPPLEMENTAL WIND CONE
-  EXISTING ELECTRICAL CABLES
-  EXISTING UNDERGROUND ELECTRICAL CABLES (FAA, UTILITY, ETC.)
-  EXISTING WIND CONE




**ELECTRICAL SITE PLAN FOR RWY 24 & RWY 30 WIND CONES**



**ELECTRIC UTILITY SERVICE SITE LAYOUT**  
N.T.S.



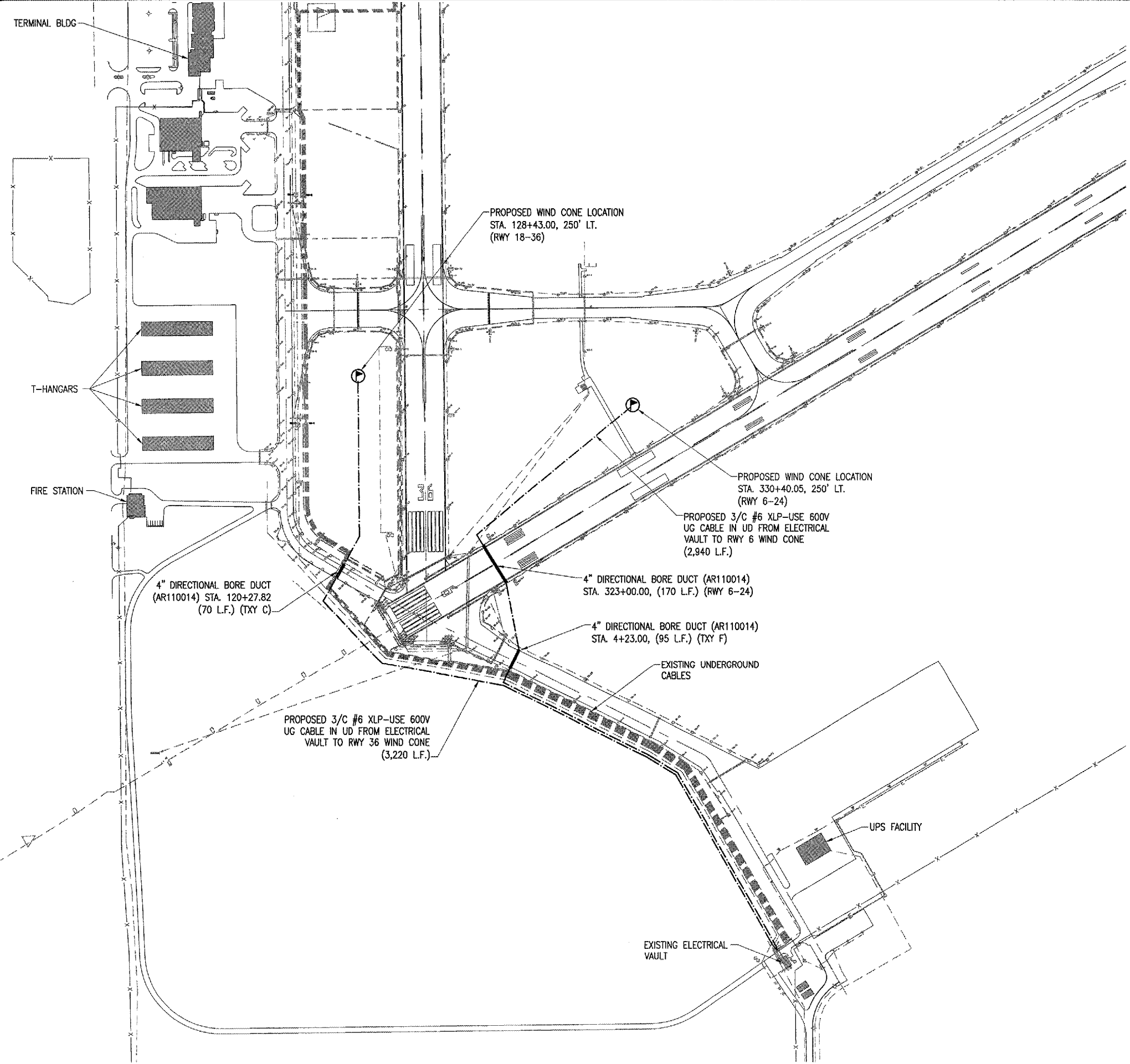
DATE	REVISION	BY


**DECATUR, ILLINOIS**  
 I.L. PROJ.: DEC-3588  
 A.I.P. PROJ.: 3-17-00

HANSON Hanson Professional Services Inc. 1525 South Sixth Street Springfield, Illinois 62703-2886 Offices Nationwide	HET Project No. 802-061X1D_0800 Filename E-143.DWG Scale AS SHOWN Date 03/31/06	LAYOUT KNL 04/22/05 DRAWN MV 04/25/05 REVIEWED LDP 03/30/06
--	--	---

**TAXIWAY "C" RECONSTRUCTION**  
**ELECTRICAL SITE PLAN**  
**RUNWAY 24 & RUNWAY 30 WIND CONES**

**43**  
 43 of 56 sheets



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES. CONTACT FAA FOR ASSISTANCE IN LOCATING FAA CABLES

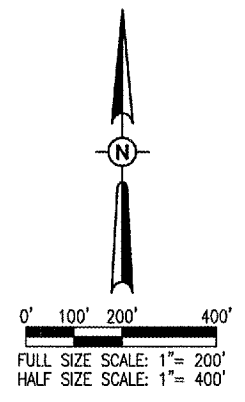
**SUMMARY OF PROPOSED QUANTITIES (THIS SHEET)**

ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
AS107408	L-806 WIND CONE 8' LIGHTED	EA	2
AS108656	3/C #6 600V UG CABLE IN UD	L.F.	6,160
AS110014	4" DIRECTIONAL BORE	L.F.	335
AS800581	ELECTRICAL POWER FOR RWY 6 AND RWY 36 WIND CONES	L.S.	1

**WIND CONE NOTES**  
 THE PROPOSED WIND CONES SHALL BE INSTALLED AS SHOWN ON THIS SHEET & THE "WIND CONE ELEVATION DETAIL" SHEET, AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

**LEGEND**

- EXISTING PAVEMENT
- PROPOSED DUCT
- PROPOSED 3/C #6 XLP-USE 600V UG CABLE IN UD - AR108656
- PROPOSED SUPPLEMENTAL WIND CONE
- EXISTING ELECTRICAL CABLES
- EXISTING WIND CONE



DATE	REVISION	BY

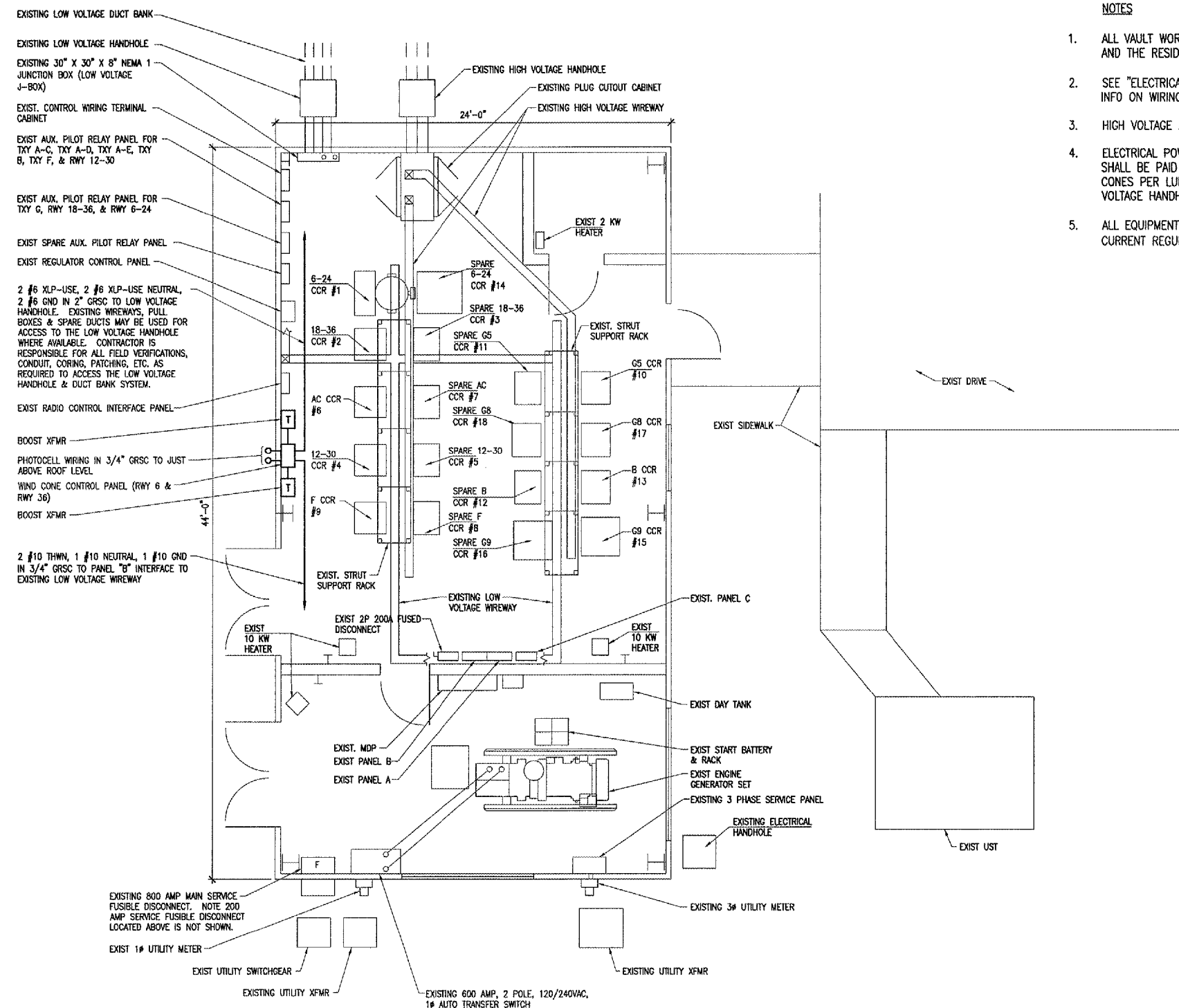


FILE NUMBER: 802-08TXD_0800	DATE: 04/22/05
ELECTION: E-144.DWG	SCALE: AS SHOWN
DATE: 03/31/06	DATE: 04/25/05
LAYOUT: KNL	REVIEWED: LDP
DRAWN: MV	DATE: 03/30/06



**TAXIWAY "C" RECONSTRUCTION**  
 ELECTRICAL SITE PLAN  
 RUNWAY 6 & RUNWAY 36  
 WIND CONES

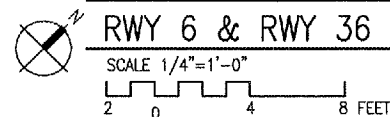
APR 28, 2006 11:32 AM DWG  
 I:\AIRPORTS\DECATUR\802-08TXD\AIRPORT\SHEETS\E-144.DWG - 2006



NOTES

1. ALL VAULT WORK AND/OR POWER OUTAGES SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR AND THE RESIDENT ENGINEER.
2. SEE "ELECTRICAL ONE LINE DIAGRAM FOR RUNWAY 6 & RUNWAY 36 WIND CONES" SHEET FOR INFO ON WIRING REQUIREMENTS.
3. HIGH VOLTAGE AND LOW VOLTAGE CIRCUITS SHALL NOT BE INSTALLED IN THE SAME RACEWAY.
4. ELECTRICAL POWER & WIND CONE CONTROL WORK FOR RUNWAY 6 & RUNWAY 36 WIND CONES SHALL BE PAID FOR UNDER AR800581 ELECTRIC POWER FOR RUNWAY 6 & RUNWAY 36 WIND CONES PER LUMP SUM. ALL POWER & CONTROL CABLE AT THE VAULT AND INTO THE LOW VOLTAGE HANDHOLE ADJACENT TO THE VAULT SHALL BE PAID FOR UNDER AR800581.
5. ALL EQUIPMENT NOT LABELED OR IDENTIFIED AS EXISTING IS NEW. ALL CCR'S (CONSTANT CURRENT REGULATORS) ARE EXISTING.

ELECTRICAL VAULT PLAN FOR  
RWY 6 & RWY 36 WIND CONES



DATE	REVISION	BY

A.I.P. PROJ.: 3-17-00

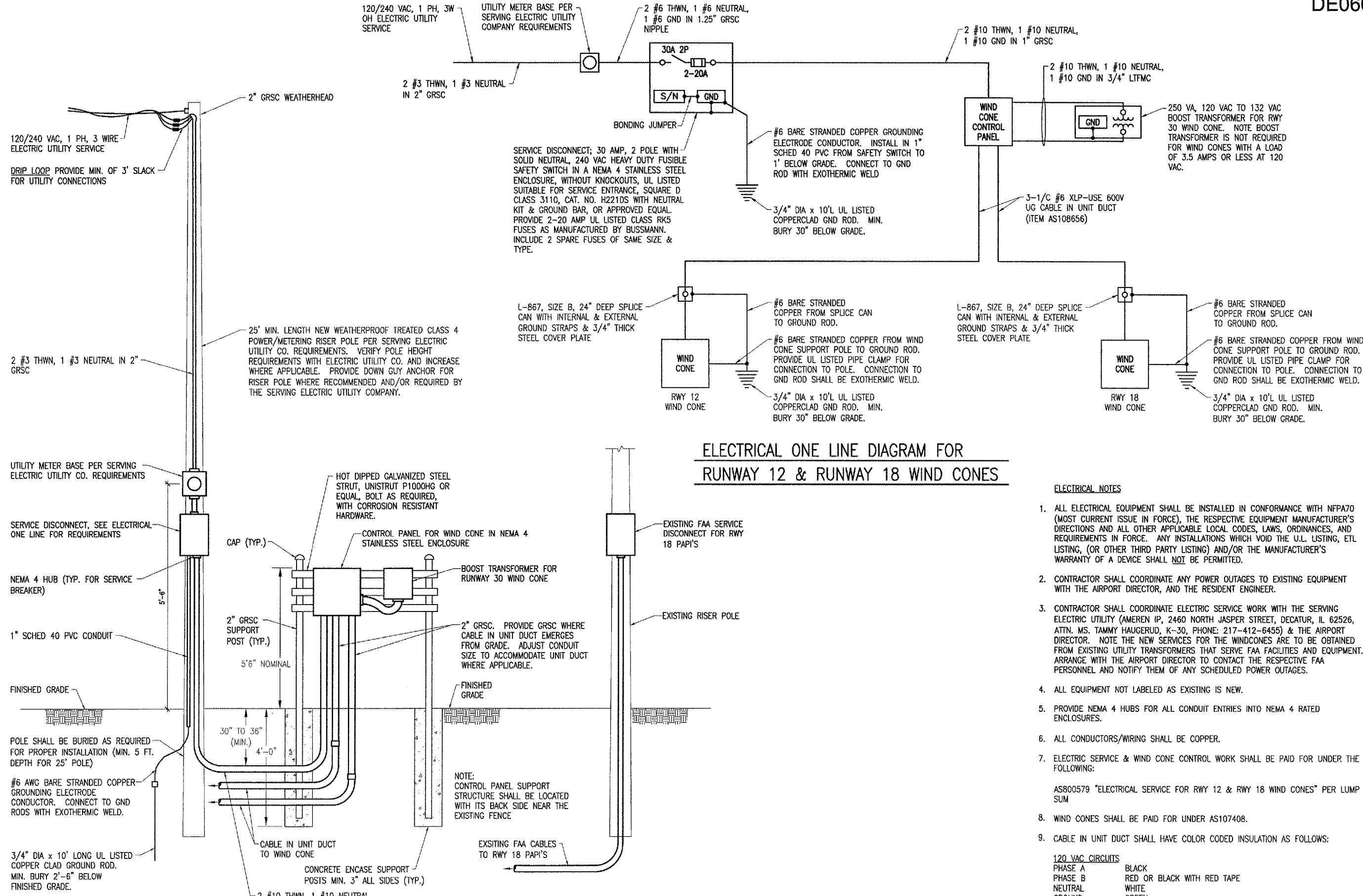
**DECATUR, ILLINOIS**

Project No.	802-06TXND_0800
Filename	E-145.DWG
Scale	1/4" = 1'-0"
Date	03/31/06
LAYOUT	KNL 04/22/05
DRAWN	MV 04/25/05
REVIEWED	LDP 03/30/06

**HANSON**

Hanson Professional Services, Inc.  
1600 N. 30th St.  
Springfield, Illinois 62718-2886  
Offices Nationwide

TAXIWAY "C"  
RECONSTRUCTION  
ELECTRICAL VAULT PLAN  
RUNWAY 6 & RUNWAY 36  
WIND CONES



**ELECTRICAL ONE LINE DIAGRAM FOR  
RUNWAY 12 & RUNWAY 18 WIND CONES**

**ELECTRICAL NOTES**

- ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA70 (MOST CURRENT ISSUE IN FORCE), THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING, (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE SHALL NOT BE PERMITTED.
- CONTRACTOR SHALL COORDINATE ANY POWER OUTAGES TO EXISTING EQUIPMENT WITH THE AIRPORT DIRECTOR, AND THE RESIDENT ENGINEER.
- CONTRACTOR SHALL COORDINATE ELECTRIC SERVICE WORK WITH THE SERVING ELECTRIC UTILITY (AMEREN IP, 2460 NORTH JASPER STREET, DECATUR, IL 62526, ATTN. MS. TAMMY HAUGERUD, K-30, PHONE: 217-412-6455) & THE AIRPORT DIRECTOR. NOTE THE NEW SERVICES FOR THE WINDCONES ARE TO BE OBTAINED FROM EXISTING UTILITY TRANSFORMERS THAT SERVE FAA FACILITIES AND EQUIPMENT. ARRANGE WITH THE AIRPORT DIRECTOR TO CONTACT THE RESPECTIVE FAA PERSONNEL AND NOTIFY THEM OF ANY SCHEDULED POWER OUTAGES.
- ALL EQUIPMENT NOT LABELED AS EXISTING IS NEW.
- PROVIDE NEMA 4 HUBS FOR ALL CONDUIT ENTRIES INTO NEMA 4 RATED ENCLOSURES.
- ALL CONDUCTORS/WIRING SHALL BE COPPER.
- ELECTRIC SERVICE & WIND CONE CONTROL WORK SHALL BE PAID FOR UNDER THE FOLLOWING:  
AS800579 "ELECTRICAL SERVICE FOR RWY 12 & RWY 18 WIND CONES" PER LUMP SUM
- WIND CONES SHALL BE PAID FOR UNDER AS107408.
- CABLE IN UNIT DUCT SHALL HAVE COLOR CODED INSULATION AS FOLLOWS:  
**120 VAC CIRCUITS**  
PHASE A      BLACK  
PHASE B      RED OR BLACK WITH RED TAPE  
NEUTRAL      WHITE  
GROUND      GREEN

**SERVICE ENTRANCE ELEVATION**  
NOT TO SCALE

APR 28, 2006 11:32 AM DWG  
I:\AIRPORTS\DECATUR\802-067X\AIRPORT\SHEETS\E-147.DWG - Work-FLR

DATE	REVISION	BY							
<b>DECATUR, ILLINOIS</b> <small>ILL. PROJ.: DEC-3588      A.I.P. PROJ.: 3-17-00</small>									
REL. Project No.	802-067X/D 0800	E-147.DWG	Scale	NONE	Date	03/31/06	LAYOUT	KML	04/22/05
DRAWN	MVP	REVIEWED	LDP	04/25/05	03/30/06				
<b>HANSON</b> <small>Hanson Professional Services Inc. 1655 S. Main Street Springfield, Illinois 62703-2886 Offices Nationwide</small>									
<b>TAXIWAY "C" RECONSTRUCTION</b>					<b>ELECTRICAL ONE LINE DIAGRAM FOR RUNWAY 12 &amp; RUNWAY 18 WIND CONES</b>				
46									
46 of 56 sheets									

120/240 VAC, 1 PH, 3W  
ELECTRIC UTILITY SERVICE  
FROM EXISTING UTILITY  
PAD MOUNT XFMR

UTILITYMETER BASE PER  
SERVING ELECTRIC UTILITY  
COMPANY REQUIREMENTS

2 #6 THWN, 1 #6 NEUTRAL,  
1 #6 GND IN 1.25" GRSC  
NIPPLE

2 #10 THWN, 1 #10 NEUTRAL,  
1 #10 GND IN 1" GRSC NIPPLE

2 #10 THWN, 1 #10 NEUTRAL,  
1 #10 GND IN 3/4" LTFMC

CONTRACTOR SHALL FURNISH & INSTALL  
2" SCHED 40 PVC CONDUIT OR 2"  
GRSC WITH GRSC ELBOWS FROM METER  
BASE TO PAD MOUNT TRANSFORMER.  
SERVICE CONDUCTORS WILL BE  
FURNISHED & INSTALLED BY THE  
ELECTRIC UTILITY COMPANY.  
COORDINATE CONDUIT ENTRY TO PAD  
MOUNT TRANSFORMER WITH THE  
SERVING ELECTRIC UTILITY COMPANY.

SERVICE DISCONNECT; 30 AMP, 2 POLE  
WITH SOLID NEUTRAL, 240 VAC HEAVY  
DUTY FUSIBLE SAFETY SWITCH IN A NEMA  
4 STAINLESS STEEL ENCLOSURE, WITHOUT  
KNOCKOUTS, UL LISTED SUITABLE FOR  
SERVICE ENTRANCE, SQUARE D CLASS  
3110, CAT. NO. H221DS WITH NEUTRAL KIT  
& GROUND BAR, OR APPROVED EQUAL.  
PROVIDE 2-20 AMP UL LISTED CLASS RK5  
FUSES AS MANUFACTURED BY BUSSMANN.  
INCLUDE 2 SPARE FUSES OF SAME SIZE &  
TYPE.

BONDING JUMPER

#6 BARE STRANDED COPPER GROUNDING  
ELECTRODE CONDUCTOR. INSTALL IN 1"  
SCHED 40 PVC FROM SAFETY SWITCH TO  
1' BELOW GRADE. CONNECT TO GND  
ROD WITH EXOTHERMIC WELD

3/4" DIA x 10'L UL LISTED  
COPPERCLAD GND ROD. MIN.  
BURY 30" BELOW GRADE.

3-1/C #6 XLP-USE 600V  
UG CABLE IN UNIT DUCT  
(ITEM AS108656)

250 VA, 120 VAC TO 132 VAC  
BOOST TRANSFORMER FOR RWY  
30 WIND CONE

L-867, SIZE B, 24" DEEP SPLICE  
CAN WITH INTERNAL & EXTERNAL  
GROUND STRAPS & 3/4" THICK  
STEEL COVER PLATE

#6 BARE STRANDED  
COPPER FROM SPLICE CAN  
TO GROUND ROD.

#6 BARE STRANDED COPPER FROM WIND  
CONE SUPPORT POLE TO GROUND ROD.  
PROVIDE UL LISTED PIPE CLAMP FOR  
CONNECTION TO POLE. CONNECTION TO  
GND ROD SHALL BE EXOTHERMIC WELD.

3/4" DIA x 10'L UL LISTED  
COPPERCLAD GND ROD. MIN.  
BURY 30" BELOW GRADE.

L-867, SIZE B, 24" DEEP SPLICE  
CAN WITH INTERNAL & EXTERNAL  
GROUND STRAPS & 3/4" THICK  
STEEL COVER PLATE

#6 BARE STRANDED  
COPPER FROM SPLICE CAN  
TO GROUND ROD.

#6 BARE STRANDED COPPER FROM WIND  
CONE SUPPORT POLE TO GROUND ROD.  
PROVIDE UL LISTED PIPE CLAMP FOR  
CONNECTION TO POLE. CONNECTION TO  
GND ROD SHALL BE EXOTHERMIC WELD.

3/4" DIA x 10'L UL LISTED  
COPPERCLAD GND ROD. MIN.  
BURY 30" BELOW GRADE.

### ELECTRICAL ONE LINE DIAGRAM FOR RUNWAY 24 & RUNWAY 30 WIND CONES

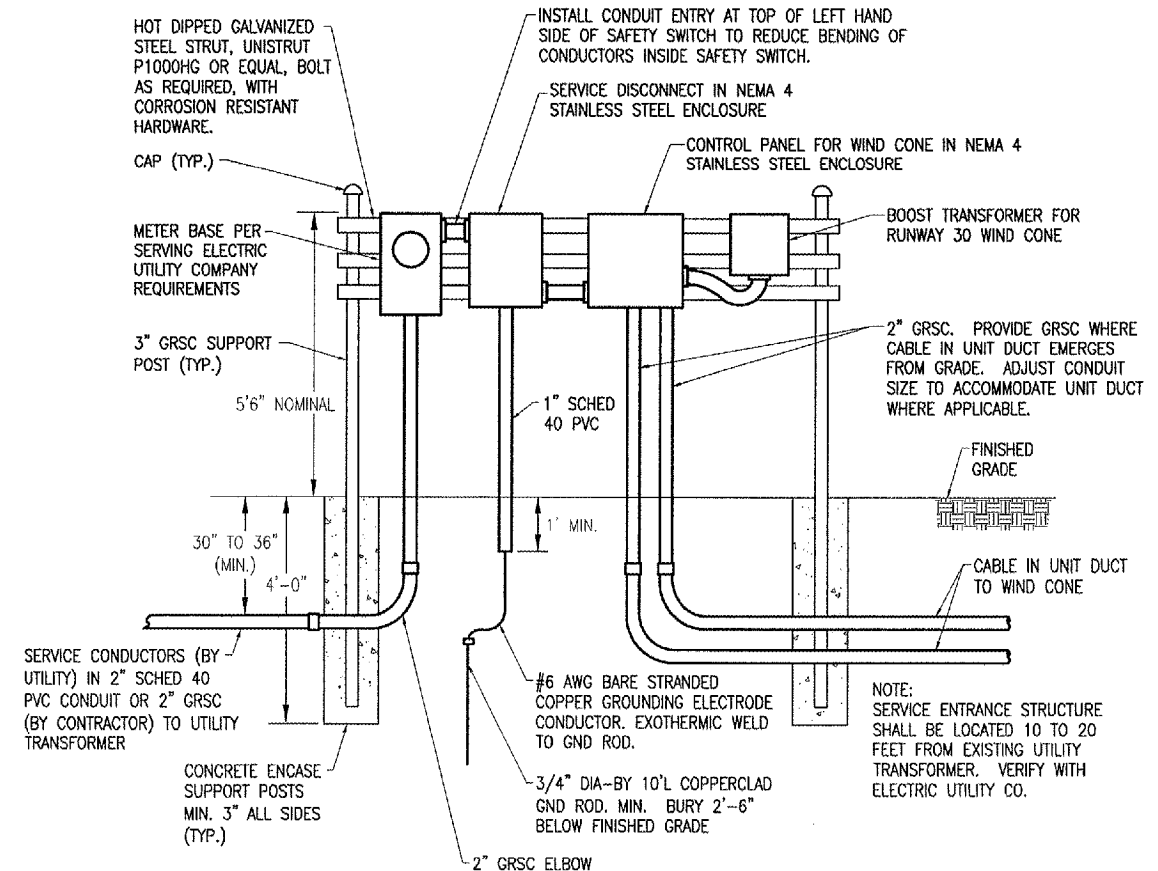
#### ELECTRICAL NOTES

DE066

- ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA70 (MOST CURRENT ISSUE IN FORCE), THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING, (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE SHALL NOT BE PERMITTED.
- CONTRACTOR SHALL COORDINATE ANY POWER OUTAGES TO EXISTING EQUIPMENT WITH THE AIRPORT DIRECTOR, AND THE RESIDENT ENGINEER.
- CONTRACTOR SHALL COORDINATE ELECTRIC SERVICE WORK WITH THE SERVING ELECTRIC UTILITY (AMEREN IP, 2460 NORTH JASPER STREET, DECATUR, IL 62526, ATTN. MS. TAMMY HAUGERUD, K-30, PHONE: 217-412-6455) & THE AIRPORT DIRECTOR. NOTE THE NEW SERVICES FOR THE WINDCONES ARE TO BE OBTAINED FROM EXISTING UTILITY TRANSFORMERS THAT SERVE FAA FACILITIES AND EQUIPMENT. ARRANGE WITH THE AIRPORT DIRECTOR TO CONTACT THE RESPECTIVE FAA PERSONNEL AND NOTIFY THEM OF ANY SCHEDULED POWER OUTAGES.
- ALL EQUIPMENT NOT LABELED AS EXISTING IS NEW.
- PROVIDE NEMA 4 HUBS FOR ALL CONDUIT ENTRIES INTO NEMA 4 RATED ENCLOSURES.
- ALL CONDUCTORS/WIRING SHALL BE COPPER.
- ELECTRIC SERVICE & WIND CONE CONTROL WORK SHALL BE PAID FOR UNDER THE FOLLOWING:  
AS800580 "ELECTRICAL SERVICE FOR RWY 24 & RWY 30 WIND CONES" PER LUMP SUM
- WIND CONES SHALL BE PAID FOR UNDER AS107408.
- CABLE IN UNIT DUCT SHALL HAVE COLOR CODED INSULATION AS FOLLOWS:

#### 120 VAC CIRCUITS

PHASE A BLACK  
PHASE B RED OR BLACK WITH RED TAPE  
NEUTRAL WHITE  
GROUND GREEN



### SERVICE ENTRANCE ELEVATION

NOT TO SCALE

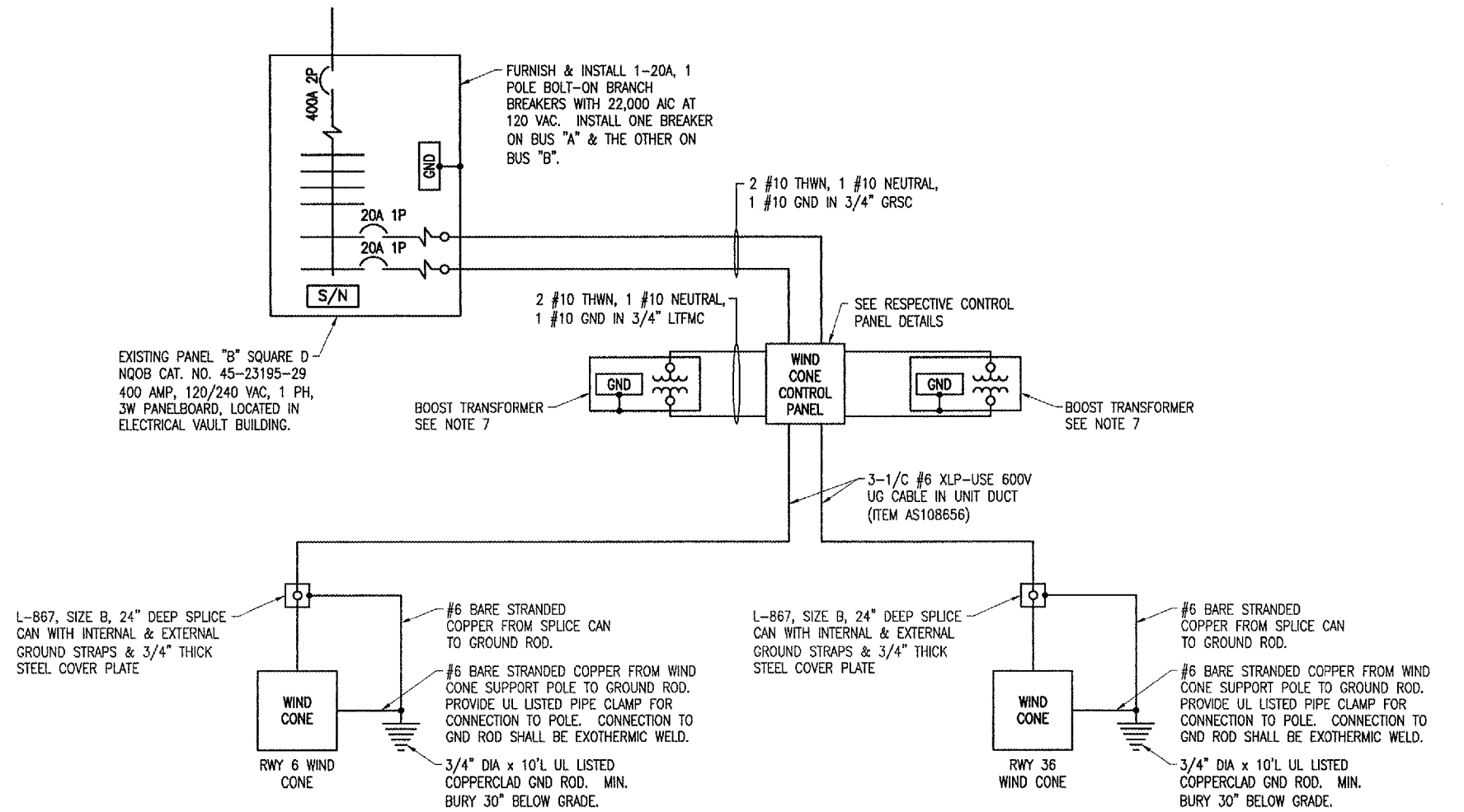
BY	
REVISION	
DATE	

**DECATUR, ILLINOIS**  
 I.L. PROJ.: DEC-3588  
 A.I.P. PROJ.: 3-17-00

HEL Project No.	802-06TXD-0800
Revision	E-148.DWG
Scale	AS SHOWN
Date	03/31/06
LAYOUT	KNL 04/22/05
DRAWN	MV 04/25/05
REVIEWED	LDP 03/30/06

**HANSON**  
 Hanson Professional Services, Inc.  
 625 S. Springfield, Illinois 62703-2886  
 Offices Nationwide

**TAXIWAY "C"  
RECONSTRUCTION**  
 ELECTRICAL ONE LINE  
 DIAGRAM FOR RUNWAY 24  
 & RUNWAY 30 WIND CONES



**ELECTRICAL ONE LINE DIAGRAM FOR  
 RUNWAY 6 & RUNWAY 36 WIND CONES**

**ELECTRICAL NOTES**

**DE066**

1. ALL ELECTRICAL EQUIPMENT SHALL BE INSTALLED IN CONFORMANCE WITH NFPA70 (MOST CURRENT ISSUE IN FORCE), THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, ETL LISTING, (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE SHALL NOT BE PERMITTED.
2. CONTRACTOR SHALL COORDINATE ANY POWER OUTAGES TO EXISTING EQUIPMENT WITH THE AIRPORT DIRECTOR, AND THE RESIDENT ENGINEER.
3. ALL EQUIPMENT NOT LABELED AS EXISTING IS NEW.
4. ALL CONDUCTORS/WIRING SHALL BE COPPER.
5. ELECTRICAL POWER & WIND CONE CONTROL WORK FOR RUNWAY 6 & RUNWAY 36 WIND CONES SHALL BE PAID FOR UNDER AS800581 "ELECTRIC POWER FOR RWY 6 & RWY 36 WIND CONES" PER LUMP SUM.
6. WIND CONES SHALL BE PAID FOR UNDER AS107408.
7. BOOST TRANSFORMER FOR WIND CONE WITH A LOAD OF APPROXIMATELY 3.5 AMPS SHALL BE 250 VA 120 VAC TO 132 VAC UNIT, SQUARE D CAT. NO. 250S43B OR APPROVED EQUAL. BOOST TRANSFORMER FOR A WIND CONE WITH A LOAD OF APPROXIMATELY 7 AMPS SHALL BE 250 VA 120 VAC, 136 VAC UNIT, SQUARE D CAT. NO. 250S46B OR APPROVED UQUAL.
8. CABLE IN UNIT DUCT SHALL HAVE COLOR CODED INSULATION AS FOLLOWS:

**120 VAC CIRCUITS**

PHASE A	BLACK
PHASE B	RED OR BLACK WITH RED TAPE
NEUTRAL	WHITE
GROUND	GREEN

REVISION	DATE	BY

**DECATUR, ILLINOIS**

IL PROJ.: DEC-3588  
 A.I.P. PROJ.: 3-17-00

FILE NO.:	B02-061X.DWG
FILE NAME:	E-149.DWG
SCALE:	NONE
DATE:	03/31/06
LAYOUT:	KNL 04/22/05
DRAWN:	MV 04/25/05
REVIEWED:	LDP 03/30/06

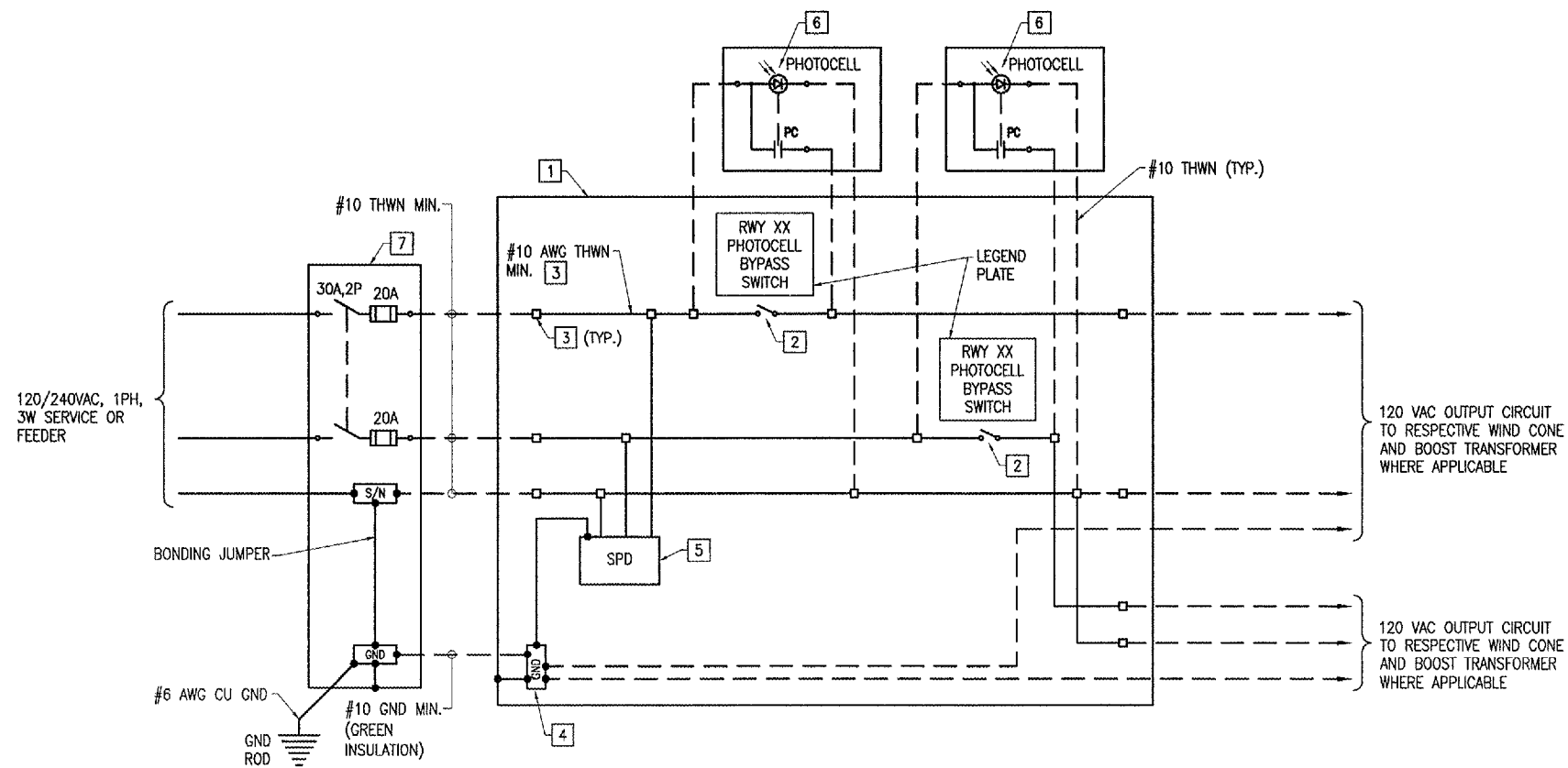
**HANSON**

Hanson Professional Services Inc.  
 12525 S. Illinois  
 Springfield, Illinois 62703-2888  
 Offices: Nationwide

**TAXIWAY "C"  
 RECONSTRUCTION**

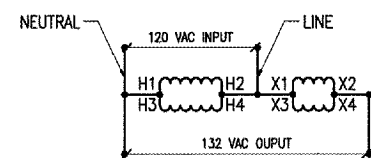
ELECTRICAL ONE LINE  
 DIAGRAM FOR RUNWAY 6  
 & RUNWAY 36 WIND CONES





**CONTROL PANEL FOR WIND CONES**

(TYPICAL FOR 3)



NOTE:  
CONFIRM WIRING WITH RESPECTIVE  
TRANSFORMER MFR.

**120 VAC TO 132 VAC BOOST TRANSFORMER  
CONNECTION DIAGRAM FOR SQUARE D  
CAT. NO. 250SV43B OR CAT. NO. 500SV43B TRANSFORMER**

**KEYED NOTES**

- 1 CONTROL PANEL ENCLOSURE ADEQUATELY SIZED TO HOLD THE RESPECTIVE COMPONENTS AND EQUIPMENT. PANEL ENCLOSURE SHALL BE UL LISTED NEMA 4X STAINLESS STEEL WITH HINGED COVER FOR OUTDOOR APPLICATIONS. PANEL ENCLOSURE SHALL BE UL LISTED NEMA 12 PAINTED STEEL WITH HINGED COVER FOR THE UNIT LOCATED IN THE AIRPORT ELECTRICAL VAULT. PROVIDE INNER DOOR TO MOUNT PHOTOCELL BYPASS SWITCHES.
- 2 20 AMP, 120 VAC, 1 POLE SPECIFICATION GRADE TOGGLE SWITCH. MOUNT ON PANEL INNER DOOR PROVIDE LEGEND PLATES LABELED "RUNWAY XX PHOTOCELL BYPASS SWITCH" WHERE "XX" IS THE RESPECTIVE RUNWAY NUMBER. PROVIDE ADDITIONAL LEGEND PLATES LABELED "ON" & "PHOTOCELL", TO INDICATE SWITCH FUNCTION & POSITIONS.
- 3 POWER CONTROL WIRING SHALL BE SIZED AS REQUIRED PER NEC MINIMUM #10 AWG TYPE MTW, THW, OR THWN, COPPER. TERMINAL BLOCKS FOR POWER & CONTROL WIRING SHALL BE 600 VOLT, WITH AMPERAGE RATINGS IN CONFORMANCE WITH NEC TABLE 310-16 USING 75 DEGREE C WIRE FOR THE RESPECTIVE WIRE LUG RANGE, BOX LUG TYPE, SQUARE D CLASS 9080, TYPE GC6, OR APPROVED EQUAL. PROVIDE A MINIMUM OF 8 SPARE TERMINAL BLOCKS TO ACCOMMODATE CONNECTIONS TO BOOST TRANSFORMERS.
- 4 EQUIPMENT GROUNDING BAR: PROVIDE A GROUNDING BAR MOUNTED AND BONDED INSIDE THE PANEL ENCLOSURE, ADEQUATELY SIZED TO ACCOMMODATE ALL GROUND CONDUCTORS TO OR FROM THE CONTROL PANEL. TERMINATE ONE GROUND WIRE PER LUG/TERMINAL.
- 5 U.L. LISTED PER UL1449, AC SURGE PROTECTOR SUITABLE FOR 120/240 VAC, 1 PH, 3W PLUS GROUND SYSTEM, WITH SURGE CURRENT RATING OF 40 KA (MIN.), 8x20 MICROSECOND WAVE, PER MODE, AND STATUS INDICATION LIGHTS, JOSLYN MODEL 1265-21, SQUARE D CAT. NO. TVS120XR40S OR APPROVED EQUAL. MAINTAIN LEADS AS SHORT & AS STRAIGHT AS POSSIBLE. INCLUDE MOUNTING BRACKET.
- 6 PHOTOCELL RATED 2000 WATTS AT 120 VAC, WITH OFF DELAY, AND -40 DEGREE C TO 60 DEGREE C OPERATING TEMPERATURE RANGE, TORK MODEL NO. 2101, OR APPROVED EQUAL. WHERE CONTROL PANEL IS LOCATED INSIDE A BUILDING THE PHOTOCELL SHALL BE MOUNTED JUST ABOVE ROOF LEVEL OF RESPECTIVE BUILDING WHERE CONTROL PANEL IS INSTALLED. PHOTOCELL SHALL FACE NORTH.
- 7 30 AMP, 2 POLE, 240 VAC, U.L. LISTED, HEAVY DUTY FUSIBLE SAFETY SWITCH IN A NEMA 4X STAINLESS STEEL ENCLOSURE, WITHOUT KNOCKOUTS, SQUARE D CLASS 3110, CAT. NO. H221DS OR APPROVED EQUAL. PROVIDE TWO 20 AMP, U.L. LISTED CLASS RK5 FUSES AS MANUFACTURED BY BUSSMANN. INCLUDE 2 SPARE FUSES OF THE SAME SIZE AND TYPE. INCLUDE LEGEND PLATE "LABELED SERVICE DISCONNECT 120/240 VAC, 1 PH, 3W MAX FUSE SIZE: 20 AMP." SEE "ELECTRICAL ONE LINE DIAGRAM FOR WIND CONES" FOR ADDITIONAL DETAILS.

**NOTES**

1. FURNISH & INSTALL A WEATHERPROOF WARNING LABEL FOR EACH SERVICE DISCONNECT & CONTROL PANEL TO WARN PERSONS OF POTENTIAL ELECTRIC ARC FLASH HAZARDS, PER THE REQUIREMENTS OF NEC 110.16 "FLASH PROTECTION".
2. ELECTRIC SERVICE/POWER & WIND CONE CONTROL WORK SHALL BE PAID FOR UNDER THE FOLLOWING:  
 AS800579 ELECTRIC SERVICE FOR RUNWAY 12 AND RUNWAY 18 WIND CONES - PER LUMP SUM  
 AS800580 ELECTRIC SERVICE FOR RUNWAY 24 AND RUNWAY 30 WIND CONES - PER LUMP SUM  
 AS800581 ELECTRIC POWER FOR RUNWAY 6 AND RUNWAY 36 WIND CONES - PER LUMP SUM
3. PROVIDE LEGEND PLATES FOR THE RESPECTIVE WIND CONE CONTROL PANELS AS FOLLOWS:  
 "CONTROL PANEL RWY 6 WIND CONE & RWY 36 WIND CONE"  
 "CONTROL PANEL RWY 24 WIND CONE & RWY 30 WIND CONE"  
 "CONTROL PANEL RWY 12 WIND CONE & RWY 18 WIND CONE"

BY	REVISION	DATE



DECATUR, ILLINOIS

ILL. PROJ.: DEC-3588 A.I.P. PROJ.: 3-17-00

Project No.	802-06TXVD_0800
Drawn	E-150.DWG
Scale	NONE
Date	03/31/06
LAYOUT	KNL 04/22/05
DRAWN	WV 04/25/05
REVIEWED	LDP 03/30/06



Hanson Professional Services Inc.  
1026 South Sixth Street  
Springfield, IL 62761  
Offices Nationwide

TAXIWAY "C"  
RECONSTRUCTION

CONTROL PANEL  
FOR WIND CONES

BY	
REVISION	
DATE	



DECATUR, ILLINOIS

A.I.P. PROJ.: 3-17-00

ILL. PROJ.: DEC-3588

Project No.	802-06TX10_0800
Filename	E-146.DWG
Scale	NONE
Date	03/31/06
LAYOUT	KNL 04/22/05
DRAWN	MDR 03/11/05
REVIEWED	LDP 03/30/06



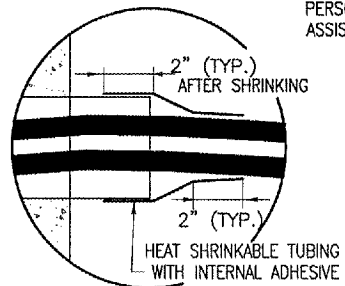
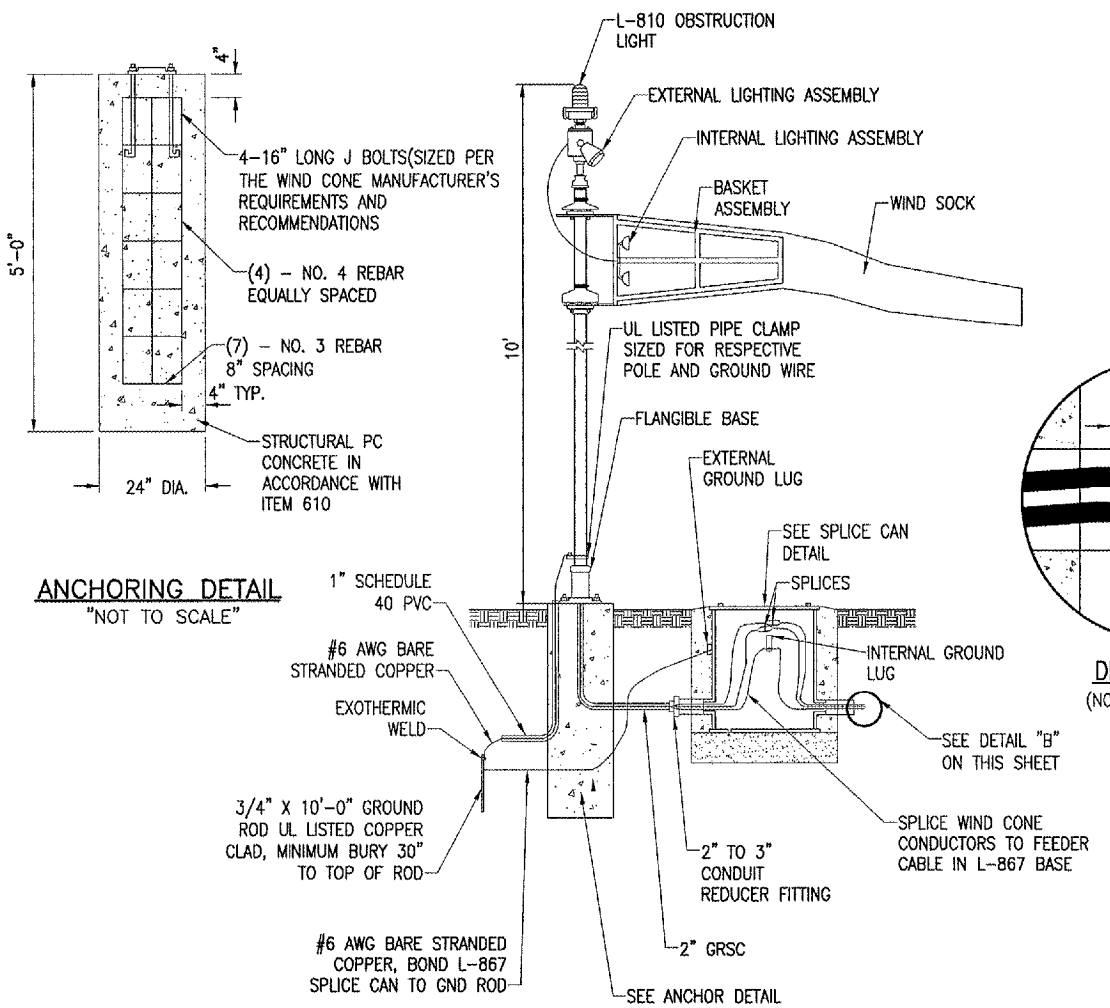
Hanson Professional Services Inc.  
1155 N. State Street  
Springfield, Illinois 62703-2886  
Offices Nationwide

TAXIWAY "C" RECONSTRUCTION

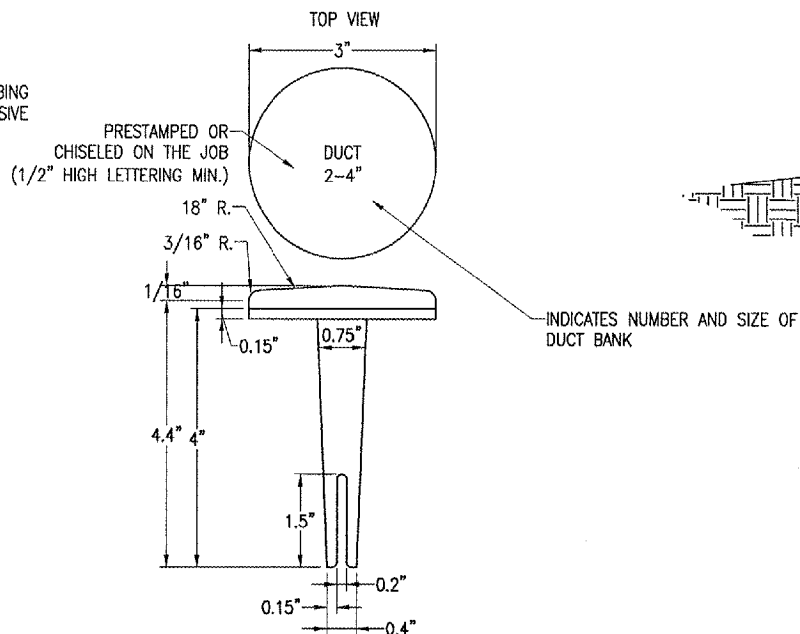
WIND CONE ELEVATION DETAIL

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. CONTACT AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING AIRPORT CABLES. CONTACT FAA FOR ASSISTANCE IN LOCATING FAA CABLES

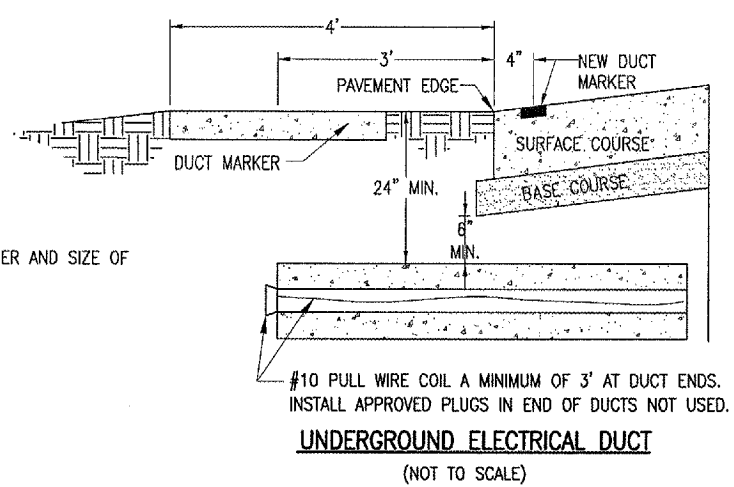


DETAIL "B" (NOT TO SCALE)



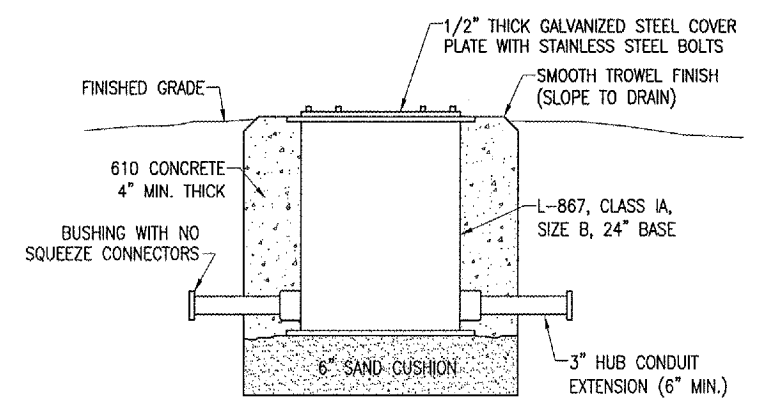
BITUMINOUS PAVEMENT DUCT MARKERS (NOT TO SCALE)

NOTE: TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE. MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH EPOXY GLUE.



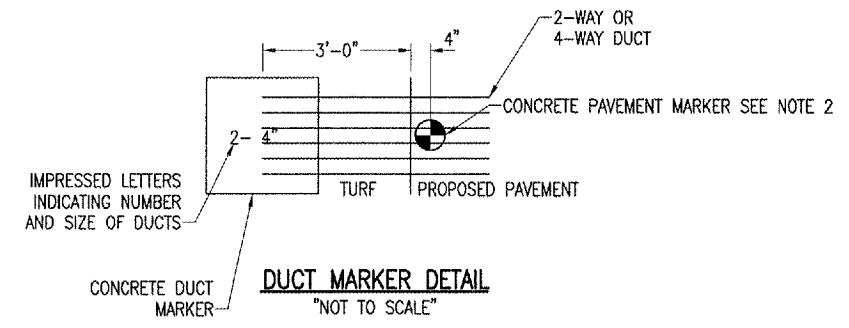
UNDERGROUND ELECTRICAL DUCT (NOT TO SCALE)

#10 PULL WIRE COIL A MINIMUM OF 3' AT DUCT ENDS. INSTALL APPROVED PLUGS IN END OF DUCTS NOT USED.

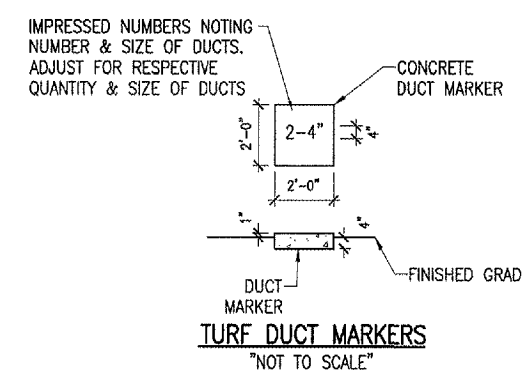


SPLICE CAN DETAIL (NOT TO SCALE)

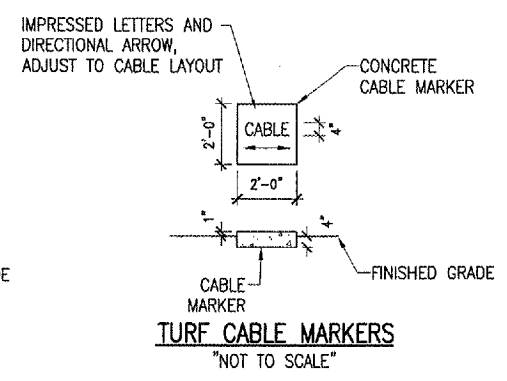
INCLUDE INTERNAL AND EXTERNAL GROUND LUGS



DUCT MARKER DETAIL (NOT TO SCALE)



TURF DUCT MARKERS (NOT TO SCALE)



TURF CABLE MARKERS (NOT TO SCALE)

APR 28, 2006 11:32 AM DMV  
I:\AIRPORTS\DECATUR\802-06TX10\AIRPORT SHEETS\E-146.DWG - ELEC DETAILS

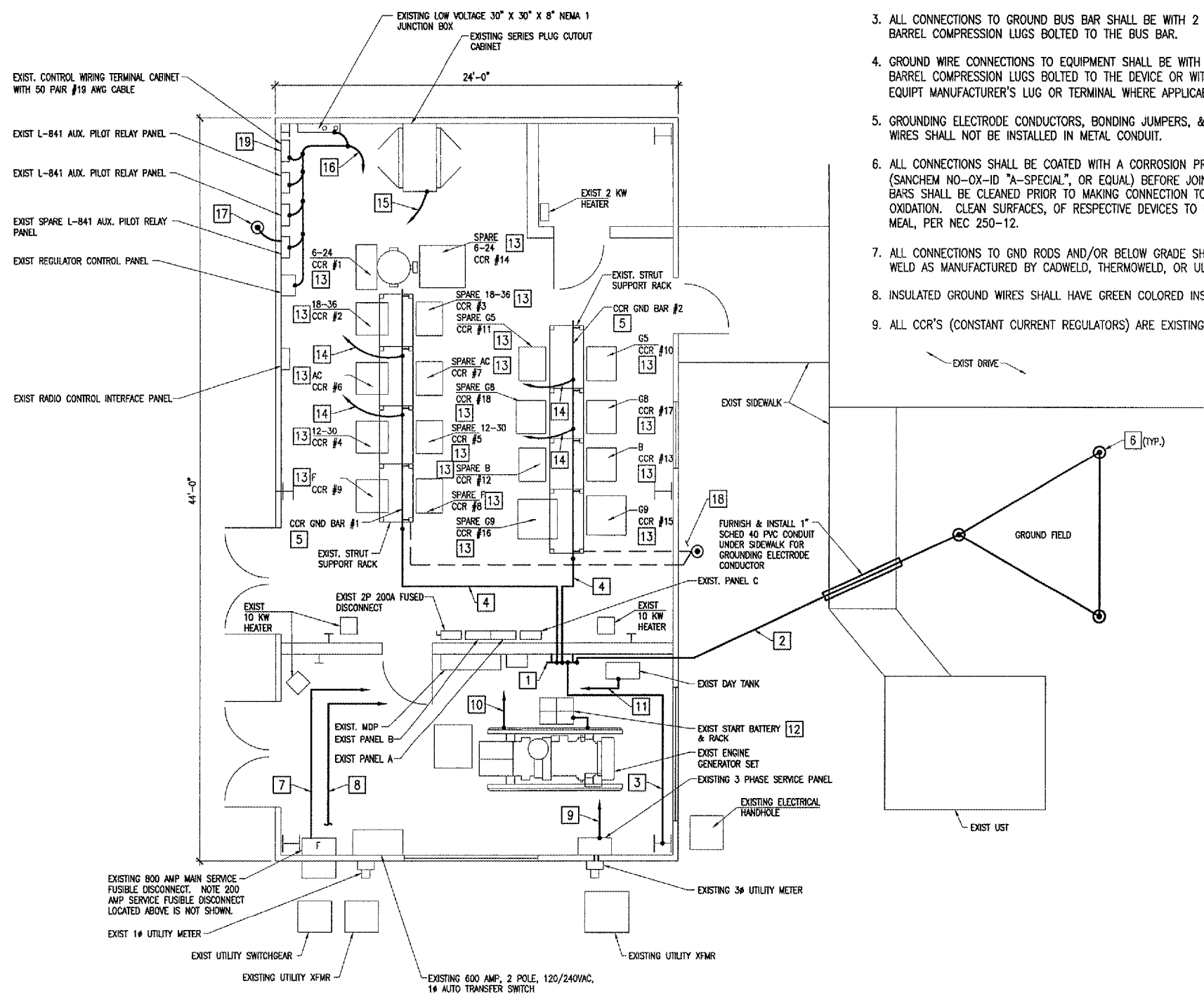
NOTES

1. SEE "VAULT GROUND RISER" AND "GROUNDING DETAILS" SHEETS FOR ADDITIONAL GROUNDING REQUIREMENTS & DETAILS.
2. GROUND RODS SHALL NOT BE SPACED LESS THAN 10 FEET APART.
3. ALL CONNECTIONS TO GROUND BUS BAR SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE BUS BAR.
4. GROUND WIRE CONNECTIONS TO EQUIPMENT SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE DEVICE OR WITH THE RESPECTIVE EQUIPT MANUFACTURER'S LUG OR TERMINAL WHERE APPLICABLE.
5. GROUNDING ELECTRODE CONDUCTORS, BONDING JUMPERS, & INDIVIDUAL GROUND WIRES SHALL NOT BE INSTALLED IN METAL CONDUIT.
6. ALL CONNECTIONS SHALL BE COATED WITH A CORROSION PREVENTATIVE COMPOUND (SANCHEM NO-OX-ID "A-SPECIAL", OR EQUAL) BEFORE JOINING. ALL COPPER BUS BARS SHALL BE CLEANED PRIOR TO MAKING CONNECTION TO REMOVE SURFACE OXIDATION. CLEAN SURFACES, OF RESPECTIVE DEVICES TO BE BONDED, TO BARE MEAL, PER NEC 250-12.
7. ALL CONNECTIONS TO GND RODS AND/OR BELOW GRADE SHALL BE EXOTHERMIC WELD AS MANUFACTURED BY CADWELD, THERMOWELD, OR ULTRAWELD.
8. INSULATED GROUND WIRES SHALL HAVE GREEN COLORED INSULATION.
9. ALL CCR'S (CONSTANT CURRENT REGULATORS) ARE EXISTING.

KEYED NOTES

- 1 MAIN GROUND BAR; 1/4" THICK BY 4" WIDE BY 18" LONG COPPER BUS BAR WITH WALL MOUNTING BRACKETS & INSULATORS, HARGER LIGHTNING PROTECTION INC. CAT. NO. GBI4418N, OR APPROVED EQUAL. LOCATE 4' ABOVE FINISHED FLOOR.
- 2 #3/0 AWG BARE STRANDED COPPER GROUNDING ELECTRODE CONDUCTOR FROM MAIN GROUND BAR TO GROUND FIELD. PROVIDE 1" PVC SLEEVE AT BLDG EXIT. CONNECT TO GROUND BAR WITH 2-HOLE TONGUE LONG BARREL DOUBLE COMPRESSION CRIMP LUG & APPROPRIATE BOLTS, NUTS, & WASHERS. CONNECT TO GROUND FIELD WITH EXOTHERMIC WELD.
- 3 #3/0 AWG COPPER GROUNDING ELECTRODE CONDUCTOR FROM MAIN GROUND BAR TO BUILDING STEEL COLUMN. CONNECTIONS SHALL BE WITH 2-HOLE TONGUE LONG BARREL DOUBLE COMPRESSION CRIMP LUG BOLTED TO RESPECTIVE DEVICE.
- 4 #3/0 AWG COPPER IN 1" SCHED 40 PVC FROM MAIN GROUND BAR TO RESPECTIVE CCR GROUND BUS.
- 5 1/4" THICK BY 2" WIDE COPPER BUS BAR TO REPLACE EXISTING GROUND BUS. MOUNT TO EXISTING STRUT SUPPORT RACK. GROUND BUS BAR LENGTH SHALL SPAN SUPPORT RACK. INCLUDE MOUNTING HARDWARE.
- 6 3/4" x 10 FT. LONG UL LISTED COPPERCLAD GROUND ROD. CONNECT GROUND RODS WITH #3/0 BARE STRANDED COPPER TO FORM A GROUND FIELD. GROUND RODS SHALL NOT BE SPACED LESS THAN 10 FEET APART. ADJUST LOCATION TO AVOID INTERFERENCE WITH EXISTING SIDEWALKS OR PAVEMENT. CONNECTIONS TO GND RODS SHALL BE EXOTHERMIC WELD.
- 7 #3/0 AWG COPPER IN 1" SCHED 40 PVC FROM 800 AMP SERVICE DISCONNECT GROUND BUS TO MAIN GROUND BAR.
- 8 #2 AWG COPPER IN 1" SCHED 40 PVC FROM 200 AMP SERVICE DISCONNECT GROUND BUS TO MAIN GROUND BAR.
- 9 #2 AWG COPPER IN 1" SCHED 40 PVC FROM 200 AMP, 3 PHASE SERVICE PANEL GROUND BUS TO MAIN GROUND BAR.
- 10 #1/0 AWG COPPER FROM ENGINE GENERATOR FRAME TO MAIN GROUND BAR.
- 11 #6 AWG COPPER FROM DAY TANK FRAME/SUPPORT TO MAIN GROUND BAR.
- 12 BOND BATTERY RACK FRAME TO ENGINE GENERATOR FRAME WITH #6 AWG COPPER BONDING JUMPER.
- 13 FOR EACH CONSTANT CURRENT REGULATOR DISCONNECT EXISTING #6 AWG COPPER BONDING JUMPER FROM EXISTING VAULT GROUND BUS AND RECONNECT TO NEW CCR GND BUS. SEE NOTE 3.
- 14 BOND HIGH VOLTAGE & LOW VOLTAGE WIREWAYS TO RESPECTIVE CCR GROUND BAR WITH #6 AWG COPPER BONDING JUMPER (WIREWAYS NOT SHOWN FOR CLARITY). SEE NOTES 3 & 4.
- 15 BOND CUTOUT ENCLOSURE TO RESPECTIVE CCR GROUND BAR WITH #6 AWG COPPER BONDING JUMPER. SEE NOTES 3 & 4.
- 16 BOND L-841 PANELS, TERMINAL PANELS, PULL BOX & REGULATOR CONTROL PANEL TO CCR GND BAR #1 WITH #6 AWG COPPER BONDING JUMPER.
- 17 EXISTING GROUND ROD CONNECTED TO L-841 PANELS, TERMINAL PANEL, & CONTROL PANELS SHALL BE DISCONNECTED & REMOVED.
- 18 EXISTING GROUND RODS CONNECTED TO THE EXISTING CCR GROUND BARS SHALL BE DISCONNECTED & REMOVED.
- 19 EXISTING CONTROL WIRING FOR RUNWAY, TAXIWAY, & OTHER AIRFIELD LIGHTING THAT RUNS FROM THE CONTROL TOWER TO THE BAGGAGE ROOM IN THE TERMINAL BUILDING AND ON TO THE VAULT SHALL BE DISCONNECTED & RECONNECTED TO AVAILABLE SPARE CONDUCTORS INSTALLED PREVIOUSLY IN 2005. SEE SPECIAL PROVISION SPEC SECTION AR109430 FOR DETAILS.

ALL GROUNDING WORK SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ITEM AR109200 INSTALL ELECTRICAL EQUIPMENT PER LUMP SUM



**ELECTRICAL VAULT GROUNDING PLAN**  
 SCALE 1/4"=1'-0"  
 2 0 4 8 FEET

APR 26, 2006 11:33 AM DWG  
 I:\AIRPORTS\DECATUR\802-06TXX\AIRPORT\SHEETS\E-151.DWG -- Work-FLR

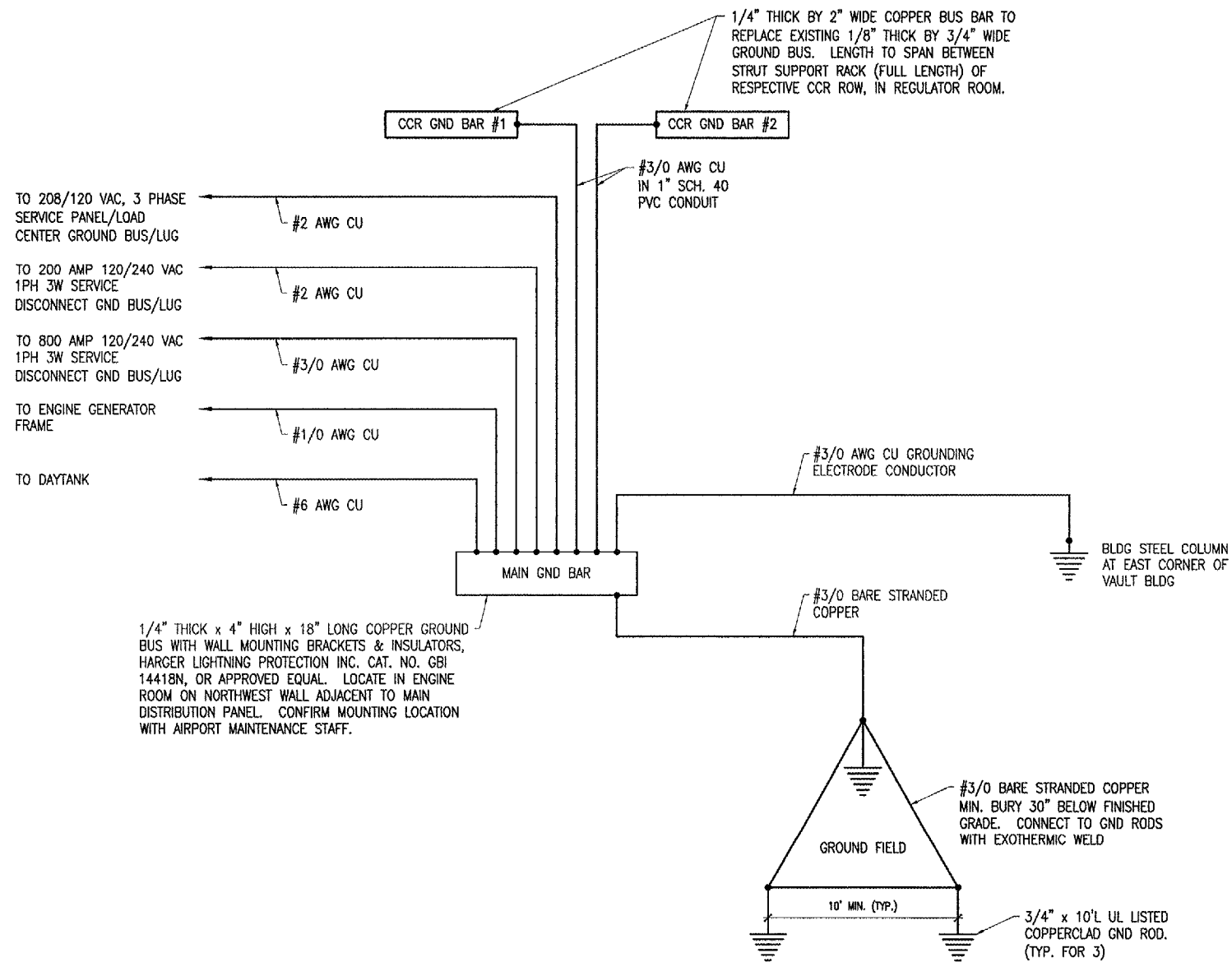
BY	REVISION	DATE



Proj. No. 802-06TXXD 0800	Scale 1/4" = 1'-0"	Date 03/31/06
Drawn KNL	Checked MW	Reviewed LDP
Date 04/22/05	Date 04/25/05	Date 03/30/06



TAXIWAY "C"  
 RECONSTRUCTION  
 ELECTRICAL VAULT  
 GROUNDING PLAN

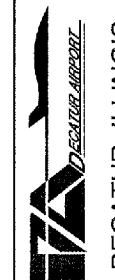


VAULT GROUND RISER

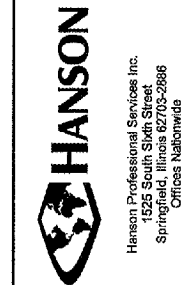
- NOTES
1. CONNECTIONS TO GROUND BUS BARS SHALL BE WITH 2-HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE BUS BAR.
  2. INSULATED GROUND WIRES SHALL HAVE GREEN COLORED INSULATION FOR ALL CONDUCTOR AWG AND KCMIL.
  3. PROVIDE LEGEND PLATE TO IDENTIFY THE VAULT MAIN GROUND BUS. LETTERING SHALL BE 1/2" HIGH, WHITE ON A GREEN BACKGROUND. LEGEND PLATE SHALL BE LABELED "MAIN GROUND BUS".
  4. FURNISH AND INSTALL TAGS IDENTIFYING THE ORIGIN AND/OR FAR END TERMINATION POINT FOR EACH GROUND WIRE LANDED ON THE MAIN GROUND BUS. TAGS FOR GROUND CONNECTIONS SHALL BE GREEN LAMINATED PLASTIC, WITH ENGRAVED WHITE CORES AND LACED TO CONDUCTOR WITH NYLON TIES. LETTERING SHALL BE 3/16" MINIMUM IN HEIGHT. INCLUDE IDENTIFICATION TAGS AS DETAILED ON THE GROUND WIRE IDENTIFICATION SCHEDULE.
  5. FURNISH AND INSTALL BRASS TAGS WITH THE WORDS "DO NOT DISCONNECT" EMBOSSED FOR ALL GROUND WIRES TERMINATED ON THE MAIN GROUND BUS.
  6. ALL WORK SHOWN ON THIS SHEET SHALL BE PAID FOR UNDER ITEM AR109200 "INSTALL ELECTRICAL EQUIPMENT" PER LUMP SUM.

GROUND WIRE IDENTIFICATION SCHEDULE	
GROUND WIRE	IDENTIFICATION LABEL
GROUNDING ELECTRODE CONDUCTOR TO GROUND FIELD	GROUND FIELD
GROUNDING ELECTRODE CONDUCTOR TO BUILDING STEEL	BUILDING STEEL
GROUNDING ELECTRODE CONDUCTOR FROM 120/240 VAC, 800 AMP SERVICE DISCONNECT	800 AMP SERVICE DISCONNECT
GROUNDING ELECTRODE CONDUCTOR FROM 120/240 VAC, 200 AMP SERVICE DISCONNECT	200 AMP SERVICE DISCONNECT
GROUNDING ELECTRODE CONDUCTOR FROM 208/120 V SERVICE PANEL CENTER	208/120 V SERVICE PANEL
ENGINE GENERATOR FRAME	ENGINE GENERATOR
DAY TANK	DAY TANK
CONSTANT CURRENT REGULATOR RACK #1 GROUND BAR	CCR GND BAR #1
CONSTANT CURRENT REGULATOR RACK #2 GROUND BAR	CCR GND BAR #2

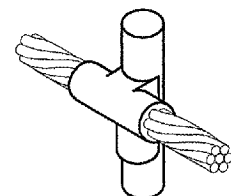
DATE	REVISION	BY



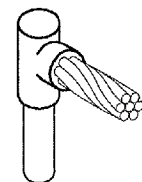
Proj. No. 807-06TXD.0800	04/22/05
Drawn E-152.DWG	04/25/05
Scale NONE	03/31/06
Date	
LAYOUT KNL	LDP
DRAWN MW	
REVIEWED LDP	



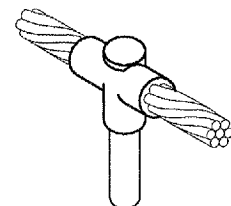
TAXIWAY "C" RECONSTRUCTION  
VAULT GROUND RISER



CABLE TO GROUND ROD

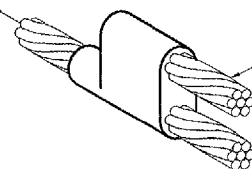


CABLE TO GROUND ROD



CABLE TO GROUND ROD

TO NEAREST GND ROD



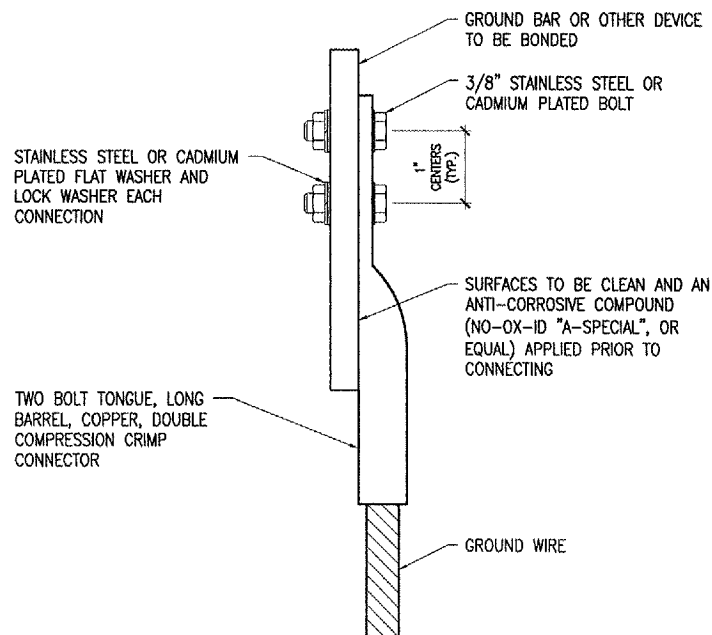
CABLE TO CABLE  
HORIZONTAL PARALLEL TAP

TAP CONDUCTOR SHALL BE ROUTED IN THE DIRECTION TOWARDS THE NEAREST GROUND ROD

DETAIL NOTES

- EXOTHERMIC WELDS SHALL BE CADWELD AS MANUFACTURED BY ERICO PRODUCTS, SOLON, OHIO, ULTRAWELD AS MANUFACTURED BY HARGER LIGHTNING PROTECTION & GROUNDING EQUIPMENT, GRAYSLAKE, IL, OR THERMOWELD AS MANUFACTURED BY CONTINENTAL INDUSTRIES, TULSA, OKLAHOMA. VERIFY PROPER SIZES, MOLDS, TYPES, AND REQUIREMENTS FOR THE RESPECTIVE APPLICATION WITH THE MANUFACTURER, AND INSTALL PER THEIR DIRECTIONS.
- FOR APPLICATIONS TO GALVANIZED STEEL OR PAINTED STEEL, REMOVE GALVANIZING AND/OR PAINT & CLEAN THE SURFACE TO EXPOSE BARE STEEL BEFORE MAKING EXOTHERMIC WELD CONNECTION.
- INDIVIDUAL GROUNDING ELECTRODE CONDUCTORS SHALL NOT BE INSTALLED IN METAL CONDUIT. INSTALL GROUNDING ELECTRODE CONDUCTORS IN SCHED 40 PVC CONDUIT AS REQUIRED IN FOUNDATIONS, FOR PROTECTION, WHERE ENTERING ENCLOSURES, ETC.

EXOTHERMIC WELD DETAILS



STAINLESS STEEL OR CADMIUM PLATED FLAT WASHER AND LOCK WASHER EACH CONNECTION

GROUND BAR OR OTHER DEVICE TO BE BONDED

3/8" STAINLESS STEEL OR CADMIUM PLATED BOLT

1" CENTERS (TYP.)

SURFACES TO BE CLEAN AND AN ANTI-CORROSIVE COMPOUND (NO-OX-ID "A-SPECIAL", OR EQUAL) APPLIED PRIOR TO CONNECTING

TWO BOLT TONGUE, LONG BARREL, COPPER, DOUBLE COMPRESSION CRIMP CONNECTOR

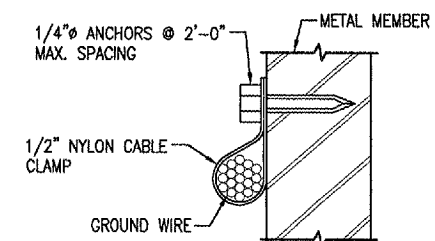
GROUND WIRE

2 HOLE LONG BARREL COMPRESSION LUG TABLE		
WIRE SIZE	BURNDY CAT. NO.	THOMAS & BETTS CAT. NO.
#6 AWG STRANDED	YA6C-2TC38	256-30695-1158
#4 AWG STRANDED	YA4C-2TC38	256-30695-1159
#2 AWG STRANDED	YA2C-2TC38	256-30695-1160
#2 AWG SOLID	YA3C-2TC38	256-30695-1160
#1/0 AWG STRANDED	YA25-2TC38	256-30695-1162
#2/0 AWG STRANDED	YA26-2TC38	256-30695-1116
#3/0 AWG STRANDED	YA27-2TC38	54816BE

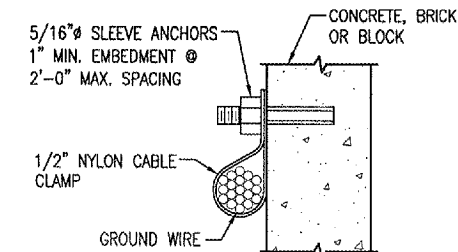
NOTES

- ALL CONNECTIONS TO GROUND BUS BAR SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE BUS BAR.
- GROUND WIRE CONNECTIONS TO EQUIPMENT SHALL BE WITH 2 HOLE TONGUE LONG BARREL COMPRESSION LUGS BOLTED TO THE DEVICE OR WITH THE RESPECTIVE EQUIPT MANUFACTURER'S LUG OR TERMINAL WHERE APPLICABLE.
- GROUNDING ELECTRODE CONDUCTORS, BONDING JUMPERS, & INDIVIDUAL GROUND WIRES SHALL NOT BE INSTALLED IN METAL CONDUIT.
- ALL CONNECTIONS SHALL BE COATED WITH A CORROSION PREVENTATIVE COMPOUND (SANICHEM INC. NO-OX-ID "A-SPECIAL", BURNDY PENETROX E, OR EQUAL) BEFORE JOINING. ALL COPPER BUS BARS SHALL BE CLEANED PRIOR TO MAKING CONNECTIONS TO REMOVE SURFACE OXIDATION. CLEAN SURFACES, OF RESPECTIVE DEVICES TO BE BONDED, TO BARE METAL, PER NEC 250-12.

GROUNDING LUG CONNECTION DETAIL



SUPPORT FOR BUS WIRE FROM METAL BAR, ANGLE, OR SIMILAR METAL MEMBER

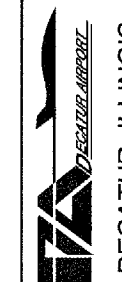


SUPPORT FOR BUS WIRE FROM CONCRETE WALL, BRICK, OR BLOCK

NOTE: WHERE NOTED ON THE PLANS GROUND WIRES SHALL BE RUN IN SCHED 40 PVC CONDUIT (SIZE AS DETAILED). PVC CONDUIT CONTAINING GROUND CONDUCTORS SHALL NOT HAVE METAL SUPPORTS THAT COMPLETELY ENCIRCLE THE CONDUIT. USE NYLON BOLTS, NUTS, WASHERS, AND/OR REINFORCED FIBERGLASS STRUT SUPPORT. DO NOT USE CABLE TIES OR THE WRAPS.

TYPICAL GROUND WIRE SUPPORT DETAIL

BY	REVISION	DATE



DECATUR, ILLINOIS

A.I.P. PROJ.: 3-17-00

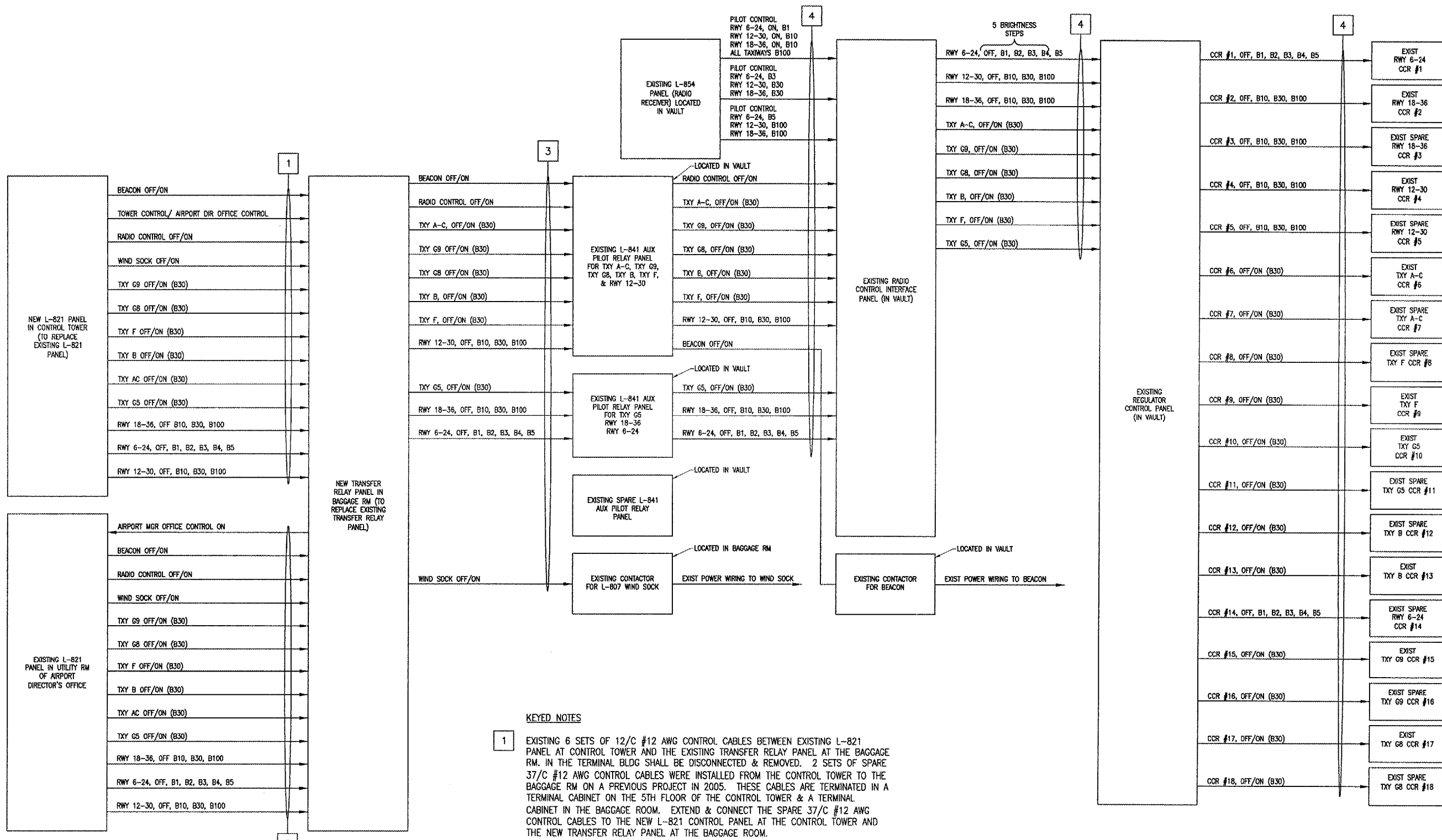
IL PROJ.: DEC-3588

PH: Project No. 802-06TXD 0800	Scale NONE	Date 03/31/06
File Name E-153.DWG	Scale NONE	Date 03/31/06
LAYOUT	KNL	04/22/05
DRAWN	MV	04/25/05
REVIEWED	LDP	03/30/06



Hanson Professional Services Inc.  
1525 S. Springfield, Illinois 62703-2886  
Offices Nationwide

TAXIWAY "C"  
RECONSTRUCTION  
GROUNDING DETAILS



- KEYED NOTES**
- 1 EXISTING 6 SETS OF 12/C #12 AWG CONTROL CABLES BETWEEN EXISTING L-821 PANEL AT CONTROL TOWER AND THE EXISTING TRANSFER RELAY PANEL AT THE BAGGAGE RM. IN THE TERMINAL BLDG SHALL BE DISCONNECTED & REMOVED. 2 SETS OF SPARE 37/C #12 AWG CONTROL CABLES WERE INSTALLED FROM THE CONTROL TOWER TO THE BAGGAGE RM ON A PREVIOUS PROJECT IN 2005. THESE CABLES ARE TERMINATED IN A TERMINAL CABINET ON THE 5TH FLOOR OF THE CONTROL TOWER & A TERMINAL CABINET IN THE BAGGAGE ROOM. EXTEND & CONNECT THE SPARE 37/C #12 AWG CONTROL CABLES TO THE NEW L-821 CONTROL PANEL AT THE CONTROL TOWER AND THE NEW TRANSFER RELAY PANEL AT THE BAGGAGE ROOM.
  - 2 IDENTIFY EXISTING CONDUCTORS BETWEEN THE EXISTING L-821 PANEL AT THE AIRPORT DIRECTOR'S OFFICE & THE EXISTING TRANSFER RELAY PANEL. DISCONNECT EXISTING CONTROL WIRES FROM EXISTING TRANSFER RELAY PANEL & RECONNECT TO NEW TRANSFER RELAY PANEL.
  - 3 EXISTING 50 PAIR #19 AWG CONTROL CABLE BETWEEN THE EXISTING RELAY TRANSFER PANEL & THE VAULT SHALL BE DISCONNECTED & REMOVED. A SPARE 50 PAIR #19 AWG CONTROL CABLE WAS INSTALLED FROM THE BAGGAGE RM IN THE TERMINAL BUILDING TO THE VAULT ON A PREVIOUS PROJECT IN 2005. THIS CABLE IS TERMINATED IN A TERMINAL CABINET IN THE BAGGAGE ROOM & A TERMINAL CABINET IN THE VAULT. EXTEND & CONNECT THE SPARE 50 PAIR #19 AWG CONTROL CABLES TO THE NEW TRANSFER RELAY PANEL AT THE BAGGAGE ROOM & THE RESPECTIVE L-841 AUX. PILOT RELAY PANELS IN THE VAULT.
  - 4 EXISTING CONTROL WIRING.

- NOTES**
1. CONTROL BLOCK DIAGRAM FOR EXISTING AIRFIELD LIGHTING SYSTEM IS BASED ON FIELD SURVEY DATA, AS-BUILT DATA & INFORMATION PROVIDED BY OTHERS AND IS FOR INFORMATION ONLY. CONTRACTOR SHALL VERIFY EXISTING WIRING & CONDITIONS BEFORE COMMENCING WORK.
  2. ALL WORK SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR, AIRPORT MAINTENANCE STAFF, THE CONTROL TOWER PERSONNEL, & THE RESIDENT ENGINEER.
  3. NEW L-821 PANEL & NEW TRANSFER RELAY PANEL SHALL BE PAID FOR UNDER ITEM AR109600 L-821 CONTROL PANEL PER EACH. CONTROL WIRING MODIFICATIONS SHALL BE PAID FOR UNDER ITEM AR109430 POWER AND CONTROL WIRING PER LUMP SUM.

ITEM AR109430 POWER AND CONTROL WIRING SHALL INCLUDE FURNISHING UPDATED WIRING SCHEMATIC DRAWINGS FOR EACH AIRFIELD LIGHTING CONTROL PANEL ASSOCIATED WITH THE RUNWAY AND TAXIWAY LIGHTING (NEW AND EXISTING) THAT ACCURATELY REPRESENTS THE EXISTING CIRCUITRY WITH THE RESPECTIVE MODIFICATIONS AND UPDATES. THE PANEL MANUFACTURER OF THE EXISTING AIRFIELD LIGHTING CONTROL PANELS IS UNIVERSE INC., 1833 WEST HOVEY AVENUE, NORMAL, ILLINOIS 61761, ATTN. MR. JOHN HOUSOUR, PHONE: 309-454-5665, FAX: 309-452-2521. CONTRACTOR IS RESPONSIBLE TO PROVIDE UPDATED DRAWINGS FOR THE AIRFIELD LIGHTING CONTROL SYSTEM AND INCLUDE THIS WORK WITH HIS BID. DRAWINGS SHALL BE IN AUTOCAD OR COMPATIBLE COMPUTER AIDED DESIGN SYSTEM FORMAT. HAND SKETCHES WILL NOT MEET THIS REQUIREMENT.

DATE	REVISION	BY

**DECATUR, ILLINOIS**

A.I.P. PROJ.: 3-17-00  
IL PROJ.: DEC-3588

HEL Project No. 802-067XYD 0800	Element E-154.DWG	Scale NONE	Date 03/31/06
LAYOUT	KNL	03/28/06	03/28/06
DRAWN	MV	03/28/06	03/28/06
REVIEWED	LDP	03/30/06	03/30/06

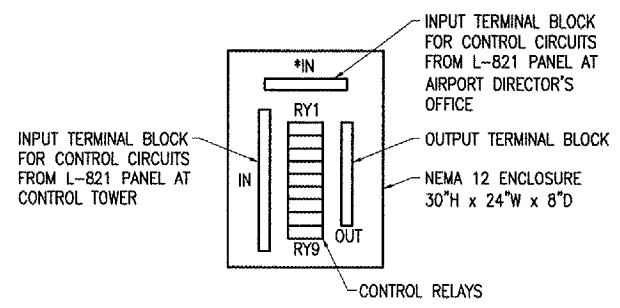
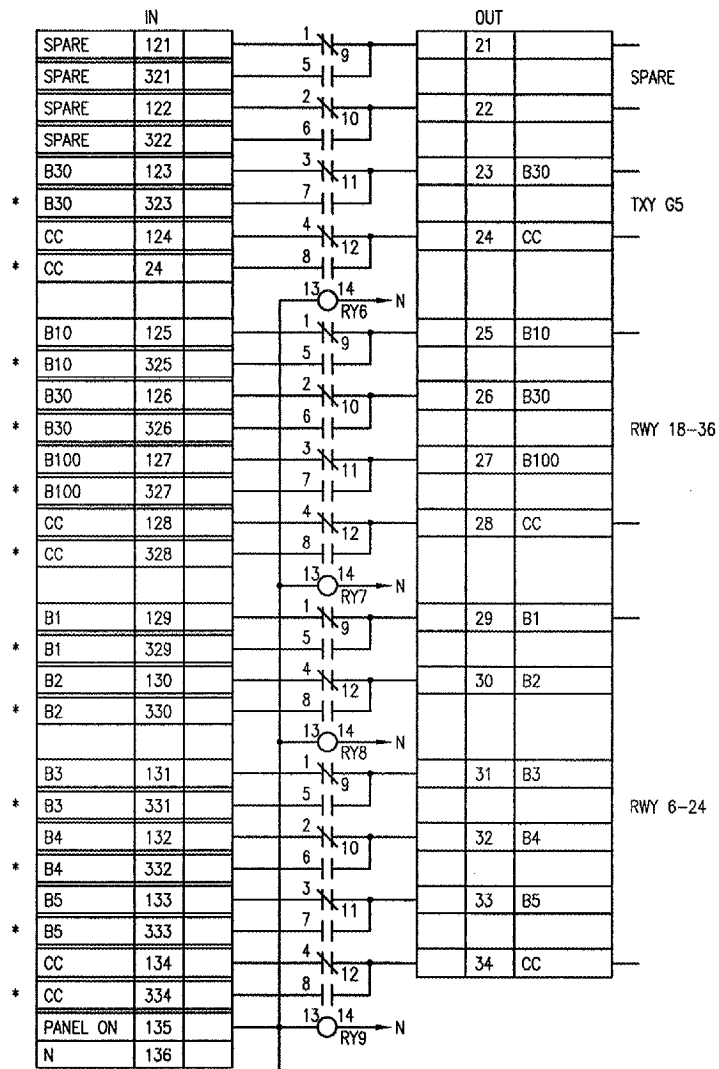
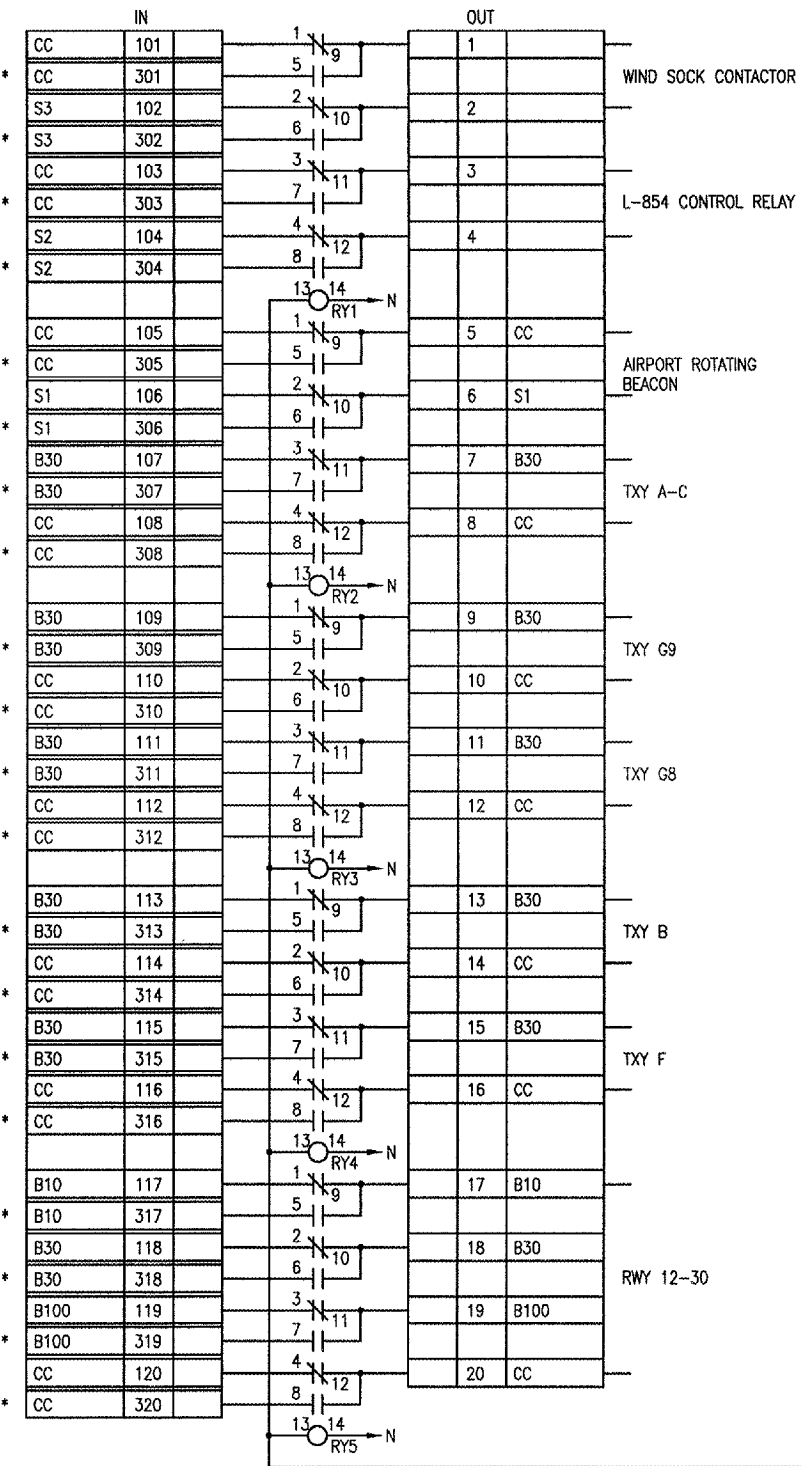
**HANSON**

Hanson Professional Services Inc.  
1000 S. Main Street  
Springfield, IL 62703-2806  
Offices Nationwide

**TAXIWAY "C" RECONSTRUCTION**

**CONTROL BLOCK DIAGRAM FOR RWY & TYX LIGHTING**

APR 26, 2006 11:33 AM DWG  
I:\AIRPORTS\DECATUR\802-067XYD\AIRPORT\SHEETS\E-154.DWG - Work-FLR



PANEL LAYOUT  
NOT TO SCALE

NOTES

1. NEW TRANSFER RELAY PANEL SHALL REPLACE EXISTING TRANSFER RELAY PANEL LOCATED IN THE BAGGAGE ROOM OF THE TERMINAL BUILDING. NEW TRANSFER RELAY PANEL SHALL BE MANUFACTURED BY UNIVERSE INC., 1833 WEST HOVEY AVE., NORMAL, ILLINOIS 61761, ATTN. MR. JOHN HOUSOUR, PHONE: 309-454-5665, FAX: 309-452-2521, OR EQUIVALENT FAA APPROVED L-821 PANEL MFR.
2. NORMAL CONTROL INPUT SHALL BE FROM THE L-821 PANEL LOCATED AT THE CONTROL TOWER. CONTRACTOR SHALL COORDINATE TERMINAL & WIRING NUMBERING BETWEEN THE NEW L-821 PANEL AT THE CONTROL TOWER & THE NEW TRANSFER RELAY PANEL.
3. ALTERNATE CONTROL INPUT SHALL BE FROM THE EXISTING L-821 PANEL LOCATED AT THE AIRPORT DIRECTOR'S OFFICE. CONTRACTOR SHALL FIELD VERIFY EXISTING WIRING, AND COORDINATE TERMINAL & WIRING NUMBERING BETWEEN THE EXISTING L-821 PANEL AT THE AIRPORT DIRECTOR'S OFFICE & THE NEW TRANSFER RELAY PANEL.
4. OUTPUT CONTROL FOR RUNWAY LIGHTING, TAXIWAY LIGHTING, BEACON, & L-854 RADIO CONTROL SHALL INTERFACE TO THE EXISTING L-841 AUX. PILOT RELAY PANELS LOCATED AT THE VAULT. OUTPUT CONTROL WIRING FOR THE WIND SOCK SHALL INTERFACE TO THE WIND SOCK CONTACTOR IN THE BAGGAGE ROOM. CONTRACTOR SHALL FIELD VERIFY EXISTING WIRING AND COORDINATE TERMINAL & WIRING NUMBERING BETWEEN THE NEW TRANSFER RELAY PANEL & THE L-841 PANELS OR OTHER RESPECTIVE DEVICES.
5. EACH CONTROL WIRE SHALL HAVE A UNIQUE NUMBER DEDICATED TO THAT RESPECTIVE WIRE & SHALL BE LABELED AT EVERY SPICE AND TERMINATION.
6. INPUT CONTROL WIRING FROM THE NEW L-821 PANEL AT THE CONTROL TOWER TO THE NEW TRANSFER RELAY PANEL SHALL BE NUMBERED 101 THROUGH 137 AND 201 THROUGH 237 TO CORRESPOND TO THE TWO 37/C #12 AWG CONTROL CABLES THAT RUN BETWEEN THE CONTROL TOWER & THE BAGGAGE ROOM AT THE TERMINAL BLDG. RENUMBER EXISTING TERMINAL PANELS AT EACH LOCATION.
7. INPUT CONTROL WIRING FROM THE EXISTING L-821 PANEL AT THE AIRPORT DIRECTOR'S OFFICE TO THE NEW TRANSFER RELAY PANEL SHALL BE RENUMBERED STARTING AT 301 THROUGH 3XX.
8. OUTPUT CONTROL WIRING FROM THE NEW RELAY TRANSFER PANEL TO THE VAULT SHALL BE NUMBERED 1 THROUGH 100 TO MATCH THE EXISTING 50 PAIR #19 AWG CONTROL CABLE TERMINAL PANELS AT EACH LOCATION.
9. CONTROL RELAYS SHALL BE 10 AMP, 4PDT WITH 120 VAC COIL, IDEC RH4BU, OR APPROVED EQUAL FOR RELAYS RY1 THROUGH RY7 & RY9. CONTROL RELAY FOR RY8 SHALL BE 10 AMP DPDT WITH 120 VAC COIL, IDEC RH2BU, OR APPROVED EQUAL. INCLUDE SOCKET FOR EACH RELAY. INCLUDE 1 SPARE FOR EACH TYPE OF RELAY.
10. TERMINAL BLOCKS SHALL BE SUITABLE FOR #18 AWG THROUGH #12 AWG CONDUCTORS. PANEL INTERNAL CONTROL WIRING SHALL NOT BE SMALLER THAN #16 AWG COPPER, 105°C, MIL SPEC 16878B16, OR EQUAL.
11. TRANSFER RELAY PANEL SHALL BE INCIDENTAL TO (INCLUDED WITH) PAY ITEM AR109600 L-821 CONTROL PANEL PER EACH.

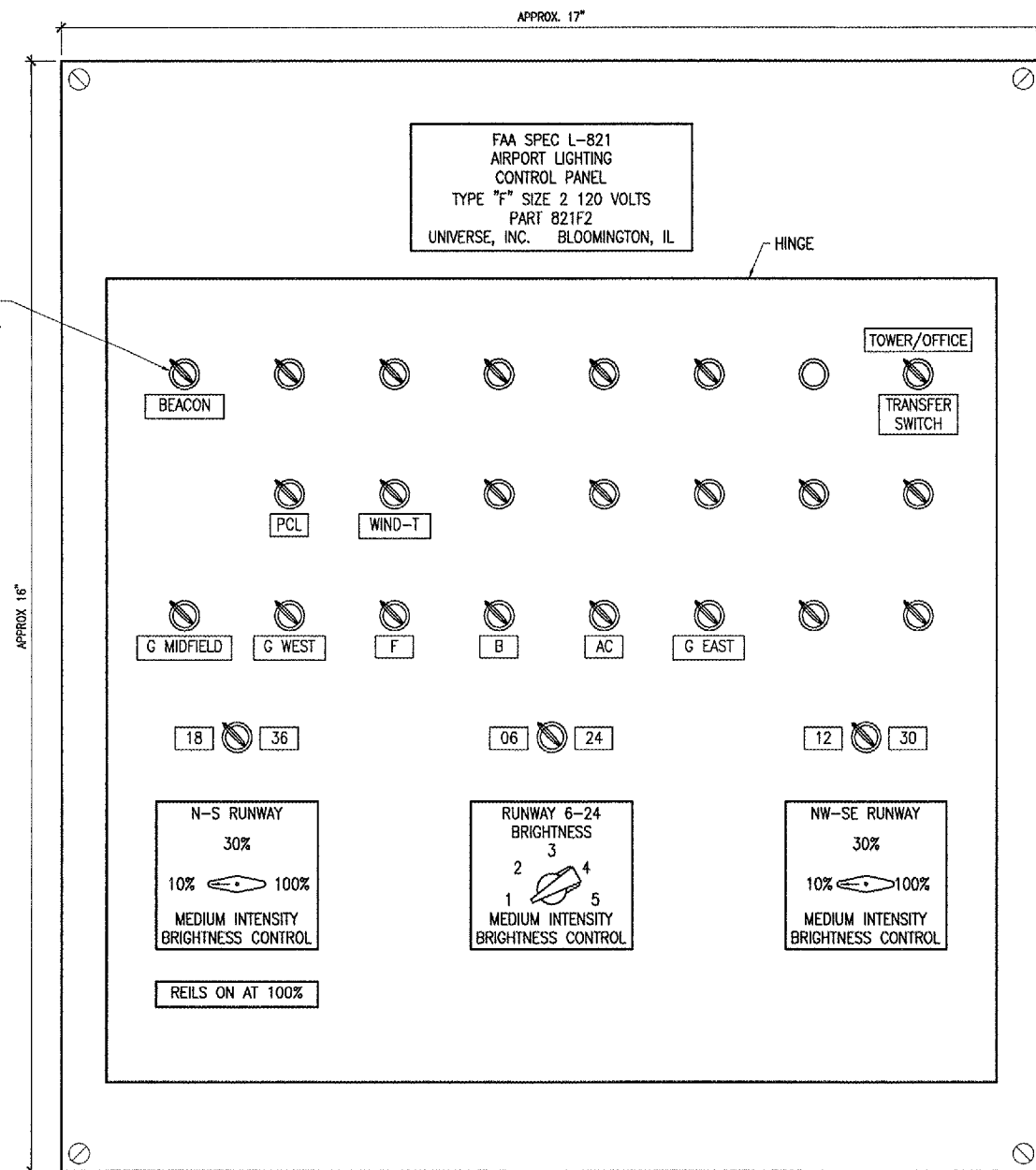
\* INTERFACE TO L-821 PANEL AT AIRPORT DIRECTOR'S OFFICE

TRANSFER RELAY PANEL SCHEMATIC

APR 28, 2006 11:33 AM DWV  
I:\AIRPORTS\DECATUR\B02-06TX\AIRPORT\SHEETS\E-155.DWG - Work-FLR

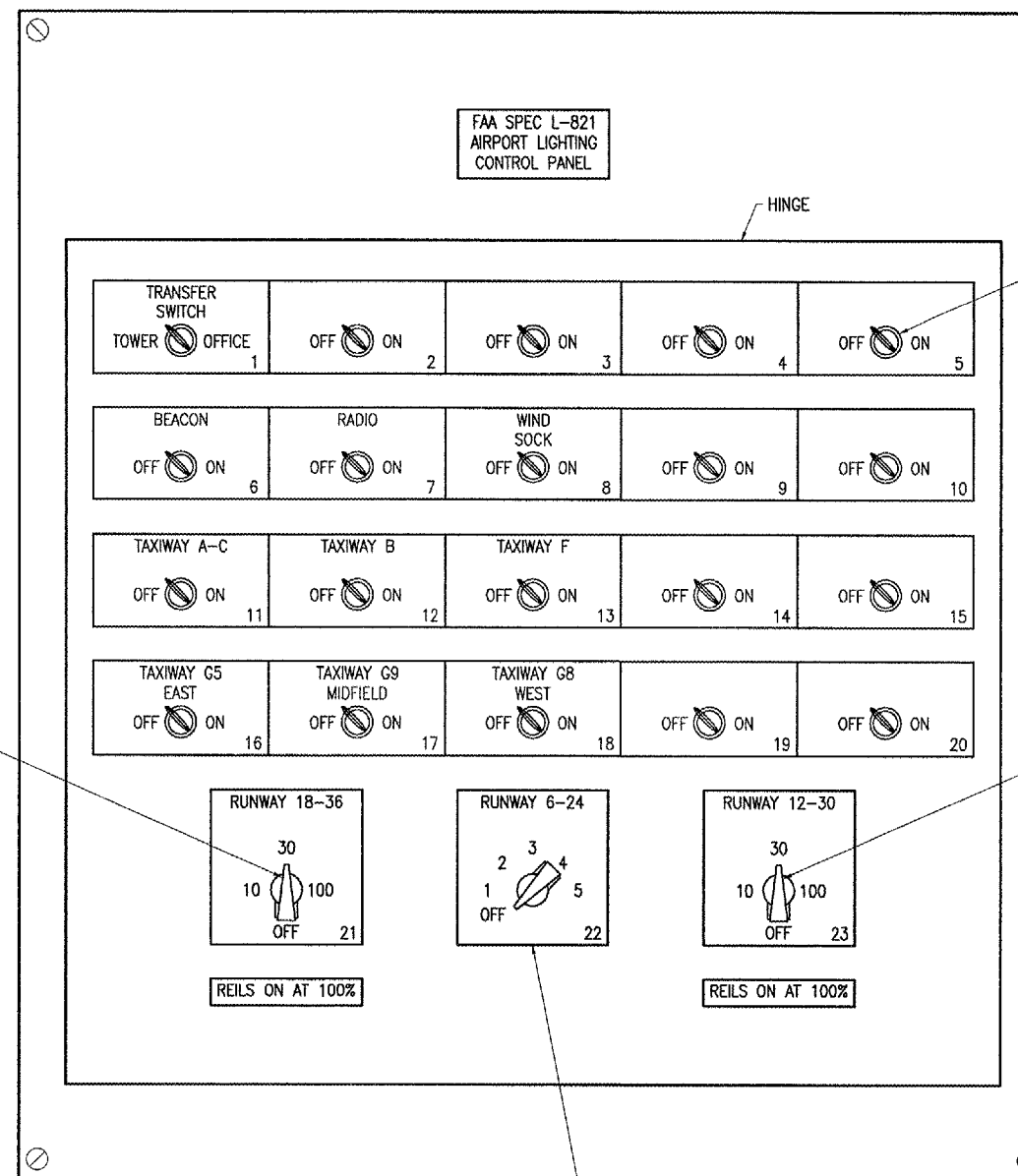
BY	
REVISION	
DATE	
 <b>DECATUR, ILLINOIS</b>	
A.I.P. PROJ.: 3-17-00 IL PROJ.: DEC-3588	
H&E Project No. B02-06TX\00_0800 File Name E-155.DWG Scale NONE Date 03/31/06	LAYOUT 03/28/06 DRAWN MW 03/28/06 REVIEWED LDP 03/30/06
 <b>HANSON</b> Hanson Professional Services Inc. 1828 South State Street Springfield, Illinois 62703-2886 Offices Nationwide	
<b>TAXIWAY "C" RECONSTRUCTION</b> <b>TRANSFER RELAY PANEL DETAILS</b>	
<b>55</b> 55 of 56 sheets	

EXISTING 2-POSITION TOGGLE SWITCH (TYP. FOR 25)



EXISTING L-821 CONTROL PANEL

4 POSITION ROTARY SELECTOR SWITCH



NEW L-821 CONTROL PANEL

2 POSITION TOGGLE SWITCH (TYP. FOR 20)

4 POSITION ROTARY SELECTOR SWITCH

6 POSITION ROTARY SELECTOR SWITCH

NOTES

1. NEW L-821 CONTROL PANEL SHALL REPLACE THE EXISTING L-821 CONTROL PANEL LOCATED IN THE CONTROL TOWER. THE EXISTING L-821 PANEL IS AN FAA SPEC TYPE I (CONVENTIONAL PANEL), CLASS F (FLUSH MOUNT IN A CONSOLE), STYLE 1 (UNLIGHTED), MODE 1. THE TOP PLATE DIMENSIONS ARE APPROXIMATELY 17" WIDE BY 16" HIGH. THE TUB/ENCLOSURE DIMENSIONS ARE APPROXIMATELY 15" WIDE BY 13.75" HIGH BY 12.5" DEEP. CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS TO CONFIRM.
2. THE NEW L-821 CONTROL PANEL SHALL BE COMPATIBLE WITH THE EXISTING CONSOLE AT THE CONTROL TOWER. THE NEW L-821 CONTROL PANEL SHALL BE TYPE I, CLASS F, STYLE 1, MODE 1 CONFORMING TO FAA A/C 150/5345-3E, AS DETAILED ON THIS SHEET, AND PER THE SPECIAL PROVISION SPECIFICATION. THE NEW L-821 CONTROL PANEL SHALL BE MANUFACTURED BY UNIVERSE INC., 1833 WEST HOVEY AVE., NORMAL, ILLINOIS 61761, PHONE 309-454-5665, FAX: 309-452-2521, ATTN. MR. JOHN HOUSOUR, OR AN EQUIVALENT FAA-APPROVED L-821 CONTROL PANEL MANUFACTURER.
3. NEW L-821 PANEL SHALL INCLUDE TERMINALS NUMBERED 101 THROUGH 137 AND 201 THROUGH 237 (WHERE APPLICABLE) TO BE COMPATIBLE WITH TERMINAL PANEL FOR THE TWO 37 CONDUCTOR CABLES LOCATED ON THE 5TH FLOOR CABLE JUNCTION ROOM OF THE CONTROL TOWER. COORDINATE TERMINAL NUMBERING WITH THE REPLACEMENT RELAY TRANSFER PANEL AT THE BAGGAGE ROOM OF THE TERMINAL BUILDING. SHOP DRAWINGS FOR L-821 PANEL SHALL INCLUDE PANEL LAYOUT AND WIRING DIAGRAM WITH TERMINAL BLOCK NUMBER DESIGNATIONS.
4. L-821 CONTROL PANEL SHALL BE PAID FOR UNDER ITEM AR109600 L-821 CONTROL PANEL PER EACH.

BY	REVISION	DATE



DECATUR, ILLINOIS

A.I.P. PROJ.: 3-17-00

ILL. PROJ.: DEC-3588

ILL. Project No.	802-06TXD_0800
Filename	E-156.DWG
Scale	NONE
Date	03/30/06
LAYOUT	KNL 03/29/06
DRAWN	MV 03/29/06
REVIEWED	LDP 03/30/06



Hanson Professional Services Inc.  
1529 South Sixth Street  
Springfield, IL 62766  
Offices Nationwide

TAXIWAY "C"  
RECONSTRUCTION

L-821 CONTROL PANEL  
DETAILS