

PROPOSED HIGHWAY PLANS

FAP ROUTE 318 (IL 29)
SECTION (4B,5B)BJR
CONTRACT MAINTENANCE
PEORIA COUNTY

C-94-060-17

FOR INDEX OF SHEETS, SEE SHEET NO. 2

HIGHWAY STANDARDS

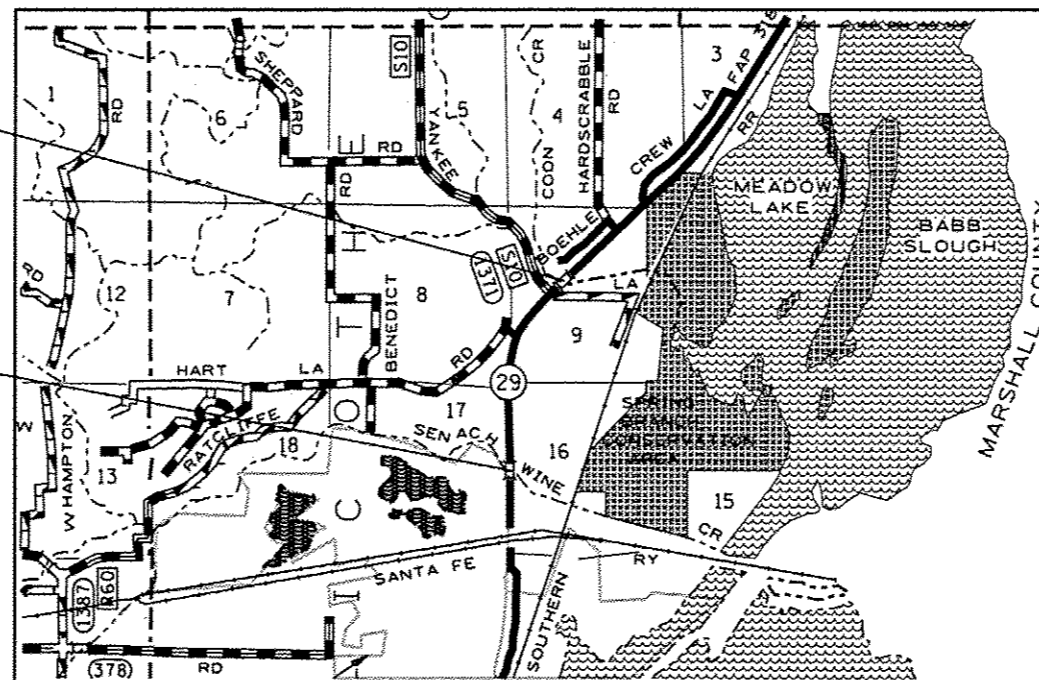
001001-02	701321-16
701006-05	701901-06
701201-04	704001-08
701301-04	780001-05
701311-03	782006

ADT = 9300 (2015)
SU = 2.69%
MU = 4.30%

IL 29 OVER COON CREEK (SN 072-0013)
STA. 1474 + 31.32 TO STA. 1475 + 52.68

OMMISSION STA. 1475 + 52.68 TO 1534 + 39.83

IL 29 OVER SENACHWINE CREEK (SN 072-0014)
STA. 1534 + 39.83 TO STA. 1536 + 60.17



J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER RICH DOTSON (309-671-3455)
PROJECT MANAGER CHAD LANGAN (309-671-3473)
CATALOG NO. 035484-00D
CONTRACT NO. 68D64

GROSS LENGTH = 6228.85 FT. = 1.180 MILE
NET LENGTH = 341.70 FT. = 0.065 MILE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	(4B,5B)BJR	PEORIA	25	1
		ILLINOIS	CONTRACT NO. 68D64	

D-94-031-17



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED April 7 20 17
Kenil O. Garnett
REGION THREE ENGINEER

May 12 20 17
Maureen M. Addis, PE, LE
ENGINEER OF DESIGN AND ENVIRONMENT

May 12 20 17
Paula J. ...
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

- 1. COVER SHEET
- 2. INDEX OF SHEETS AND GENERAL NOTES
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- 10-12. STAGING DETAILS FOR S.N. 072-0014
- 13-17. BRIDGE JOINT REPAIR DETAILS FOR S.N. 072-0013
- 18-22. BRIDGE JOINT REPAIR DETAILS FOR S.N. 072-0014
- 23-25. DISTRICT CADD STANDARDS

GENERAL NOTES

COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made. There are no commitments for this project.

PROPERTY OWNER ACCESS REQUIREMENTS

Access must be maintained to all existing properties during construction per Article 107.09 unless arrangements are made in writing by the Contractor with the property owners with a copy to the Engineer for short-term closures.

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

Prior to any waste materials being removed from the construction site the required environmental resource surveys will need to be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Environmental Survey Request)
- * BDE Form 2290 (Waste/Use Area Review)
- * A location map showing the size limits and location of the use area
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form-D4 PI0101

Please note that a minimum of four weeks shall be allowed for the District to obtain the required environmental clearances and six weeks for the required borrow site environmental clearances.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

Mixture Use(s):	Shoulders (Surface Lift 2")
AC/PC:	PG 64-22
Design Air Voids:	4.0% @ N=50
Mixture Composition: (Gradation Mixture)	IL 9.5
Friction Aggregate:	Mix C
Quality Management:	QCQA

Notes: Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal aggregate size.

FILE NAME : 68064 Cover Sheet ETC.dgn	USER NAME : aubreyg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS AND GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000 " / in.	DRAWN -	REVISED -			318	(48,58)BJR	PEORIA	25	2
Default	PLOT DATE = 4/7/2017	CHECKED -	REVISED -	SCALE:	SHEET OF SHEETS	STA.	TO STA.	[ILLINOIS] FED. AID PROJECT		
		DATE -	REVISED -							

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE ✓ PEORIA CO.	100% STATE ✓ PEORIA CO.
				S. N. 072-0013 0014 ✓	S. N. 072-0014 0014 ✓
42001300	PROTECTIVE COAT	SO YD	74.9	33	41.9
50102400	CONCRETE REMOVAL	CU YD	19.4	5.2	14.2
50300255	CONCRETE SUPERSTRUCTURE	CU YD	19.3	5.1	14.2
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2780	940	1840
50800515	BAR SPLICERS	EACH	34	10	24
52000110	PREFORMED JOINT STRIP SEAL	FOOT	244	122	122
67100100	MOBILIZATION	L SUM	1	0.5	0.5
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	2	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	0.5	0.5
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	2	1	1
70106700	TEMPORARY RUMBLE STRIP	EACH	12	6	6
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	4647	2207	2440
70300924	PAVEMENT MARKING TAPE, TYPE IV 24"	FOOT	100	76	24
70400100	TEMPORARY CONCRETE BARRIER	FOOT	975	400	575

FILE NAME :
68064 Cover Sheet ETC.dgn

USER NAME : aubreyggs
PLOT SCALE = 100.0000 ' / in.
PLOT DATE = 4/7/2017

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	(4B,SB)BJR	PEORIA	25	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68064	

REV

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE PEORIA CO.	100% STATE PEORIA CO.
				S.N. 072-0013 0014	S.N. 072-0014 0014
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	788	388	400
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	2	2
70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	2	2
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	4159	2994	1165
* 78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	94	94	
X0327980	PAVEMENT MARKING REMOVAL - WATER BLAST	SO FT	1480	1092	388
X0556100	PARTIAL DEPTH PATCHING (SPECIAL)	SO YD	680	485	195
X4421000	PARTIAL DEPTH PATCHING	TON	76	54	22
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	28	14	14
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	1749	888	861

⑩
*SPECIALTY ITEM

FILE NAME :
68064 Cover Sheet ETC.dgn

USER NAME : aubrags
PLOT SCALE : 100.0000 ' / in.
PLOT DATE : 4/27/2017

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REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	148.SB1BJR	PEORIA	25	4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 68064	

PATCHING SCHEDULE								
LOCATION				AREA	X0556100 PARTIAL DEPTH PATCHING (SPECIAL)	X4421000 PARTIAL DEPTH PATCHING		
IL 29		LT/RT	SQ FT	SQ YD	TON			
S.N. 072-0013								
PRE-STAGE								
STA	1472+85.43	TO	1474+49.81	LT/RT	2,366.1	262.9	29.4	
STA	1475+35.38	TO	1476+93.96	LT/RT	1,998.7	222.1	24.9	
S.N. 072-0014								
PRE-STAGE								
STA	1532+73.86	TO	1533+96.89	RT	750.2	83.4	9.3	
STA	1536+69.55	TO	1538+18.62	RT	1,000.0	111.1	12.4	
S.N. 072-0013 SUBTOTALS					485.0	54.3		
S.N. 072-0014 SUBTOTALS					194.5	21.8		
TOTALS					680	76		

TEMPORARY CONCRETE BARRIER SCHEDULE							
LOCATION				70400100 TEMPORARY CONCRETE BARRIER	70400200 RELOCATE TEMPORARY CONCRETE BARRIER		
IL 29		LT/RT	FT	FT			
S.N. 072-0013							
STAGE I							
STA	1472+94.94	TO	1476+95.93	RT/LT	400.0		
STAGE II							
STA	1472+94.56	TO	1476+83.82	RT/LT		387.5	
S.N. 072-0014							
STAGE I							
STA	1533+41.99	TO	1537+34.90	RT	400.0		
STAGE II							
STA	1532+66.36	TO	1538+35.65	LT/RT	175.0	400.0	
S.N. 072-0013 SUBTOTALS					400.0	387.5	
S.N. 072-0014 SUBTOTALS					575.0	400.0	
TOTALS					975	788	

TEMPORARY IMPACT ATTENUATOR SCHEDULE						
STATION			70600250 IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	70600350 IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3		
IL 29		LT/RT	EACH	EACH		
S.N. 072-0013						
STAGE I						
STA	1472+94.94	RT	1			
STA	1476+95.93	RT	1			
STAGE II						
STA	1472+94.56	LT		1		
STA	1476+83.82	LT		1		
S.N. 072-0014						
STAGE I						
STA	1533+41.99	RT	1			
STA	1537+34.90	RT	1			
STAGE II						
STA	1532+66.36	LT		1		
STA	1538+35.65	LT		1		
S.N. 072-0013 SUBTOTALS			2	2		
S.N. 072-0014 SUBTOTALS			2	2		
TOTALS			4	4		

70106500 TEMPORARY BRIDGE TRAFFIC SIGNALS	
LOCATION	EACH
S.N. 072-0013	1
S.N. 072-0014	1
TOTAL	2

70106700 TEMPORARY RUMBLE STRIPS	
LOCATION	EACH
S.N. 072-0013	6
S.N. 072-0014	6
TOTAL	12

X7015005 CHANGEABLE MESSAGE SIGN	
LOCATION	CAL DA
S.N. 072-0013 (2 SIGNS/ 7 DAYS EACH)	14
S.N. 072-0014 (2 SIGNS/ 7 DAYS EACH)	14
TOTAL	28

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		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

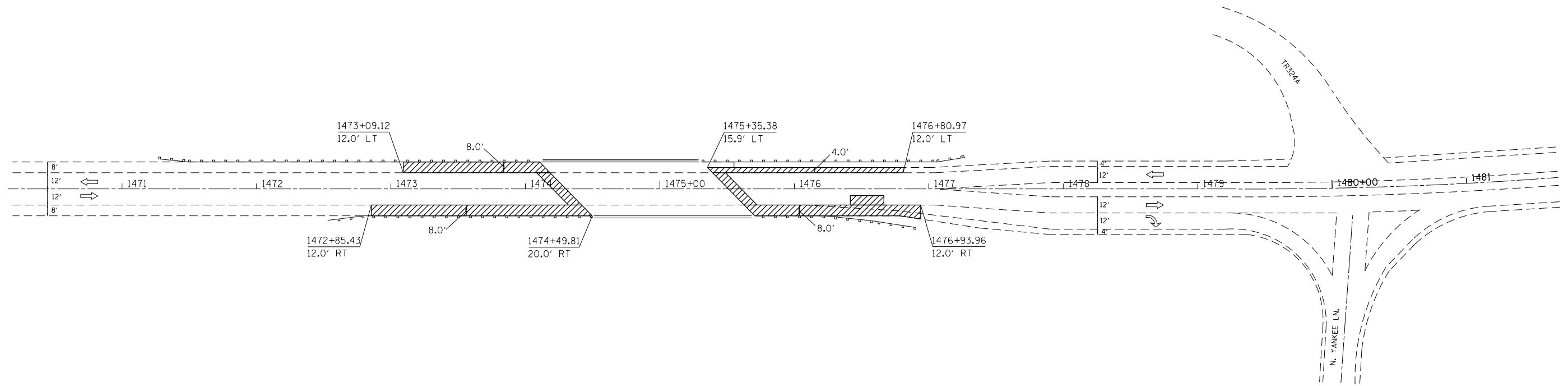
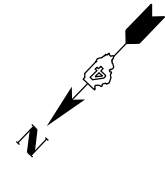
SCHEDULE OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 318	SECTION (4B,5B)BJR	COUNTY PEORIA	TOTAL SHEETS 25	SHEET NO. 5
CONTRACT NO. 68D64				ILLINOIS FED. AID PROJECT

TEMPORARY PAVEMENT MARKING SCHEDULE							
LOCATION			X0327980 PAVEMENT MARKING REMOVAL - WATER BLASTING	70300904 PAVEMENT MARKING TAPE, TYPE IV 4''	70300924 PAVEMENT MARKING TAPE, TYPE IV 24''	X7030005 TEMPORARY PAVEMENT MARKING REMOVAL	
IL 29		EDGE/CL	LT/RT	SQ FT	FT	FT	SQ FT
S.N. 072-0013						76.00	152.0
STAGE I							
STA	1471+25.00	TO	1477+04.69	CL			386.5
STA	1477+04.69	TO	1479+68.00	MEDIAN			445.1
STA	1473+09.00	TO	1476+81.00	EDGE	LT		124.0
STA	1472+85.00	TO	1476+94.00	EDGE	RT		136.3
STA	1473+09.00	TO	1476+81.00		LT	372.1	124.0
STA	1471+41.00	TO	1478+54.00		LT/RT	714.4	238.1
STAGE II							
STA	1471+85.00	TO	1478+95.00		LT/RT	711.3	237.1
STA	1472+85.00	TO	1476+94.00		RT	409.0	136.3
S.N. 072-0014						24.0	48.0
STAGE I							
STA	1530+82.00	TO	1539+98.00	CL			76.3
STA	1533+43.00	TO	1537+34.00	EDGE	LT		130.3
STA	1532+74.00	TO	1538+19.00	EDGE	RT		181.7
STA	1533+43.00	TO	1537+34.00		LT	391.0	130.3
STA	1531+87.00	TO	1538+42.00		LT/RT	656.0	218.7
STAGE II							
STA	1532+74.00	TO	1538+19.00		RT	545.3	181.8
STA	1531+42.00	TO	1539+88.00		LT/RT	848.0	282.7
S.N. 072-0013 SUBTOTALS						76.0	887.6
S.N. 072-0014 SUBTOTALS						24.0	861.4
TOTALS						100	1,749

PAVEMENT MARKING SCHEDULE						
LOCATION			78001110 PAINT PAVEMENT MARKING - LINE 4''	78001150 PAINT PAVEMENT MARKING - LINE 12''		
IL 29		EDGE/CL	LT/RT	FT	FT	
S.N. 072-0013						
STA	1471+25.00	TO	1477+04.69	CL	1,159.4	
STA	1477+04.69	TO	1479+68.00	MEDIAN	1,053.2	
STA	1473+09.00	TO	1476+81.00	EDGE	372.0	
STA	1472+85.00	TO	1476+94.00	EDGE	409.0	
S.N. 072-0014						
STA	1530+82.00	TO	1539+98.00	CL	229.0	
STA	1533+43.00	TO	1537+34.00	EDGE	391.0	
STA	1532+74.00	TO	1538+19.00	EDGE	545.0	
S.N. 072-0013 SUBTOTALS					2,993.6	
S.N. 072-0014 SUBTOTALS					1,165.0	
TOTALS					4,159	



LEGEND

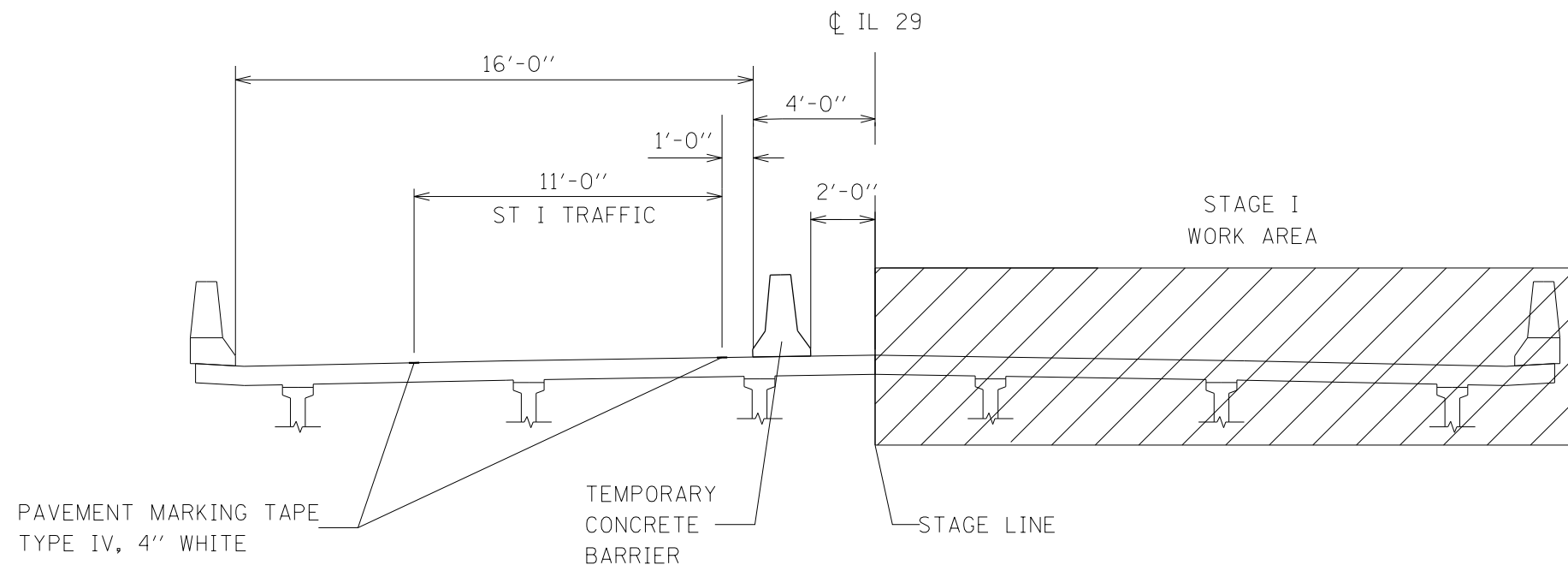
 PARTIAL DEPTH PATCHING (SPECIAL) & PARTIAL DEPTH PATCHING

NOTES

1. USE TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 TO COMPLETE THE PARTIAL DEPTH PATCHING.

PRE-STAGE

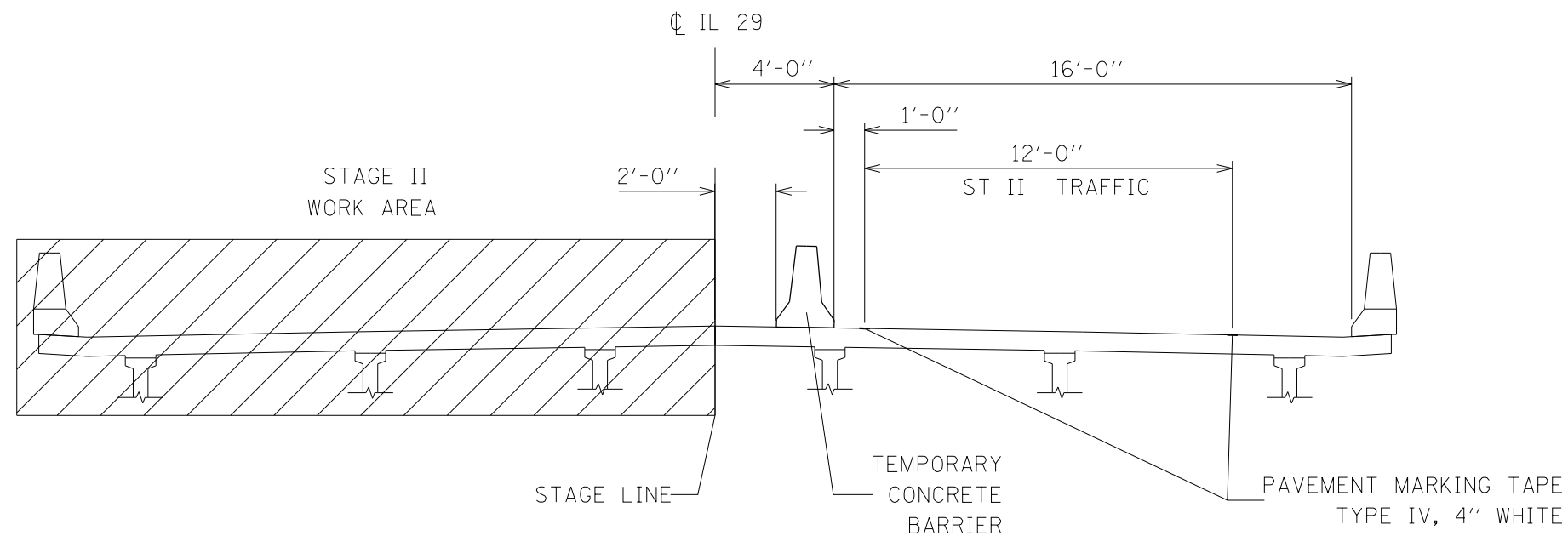
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\$FILEL\$		DRAWN - CML	REVISED -		SCALE:	SHEET 1	OF 3 SHEETS	STA.	TO STA.	318	(4B,5B)BJR	PEORIA	25	7
\$MODELNAME\$		CHECKED -	REVISED -		CONTRACT NO. 68D64									
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT									



TYPICAL SECTION

STAGE I

(looking South)

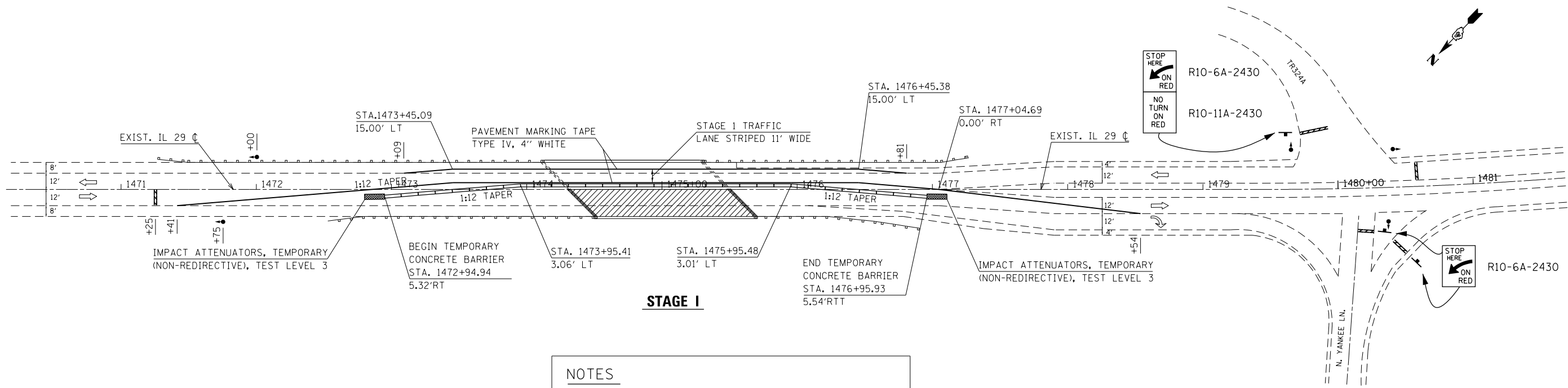


TYPICAL SECTION

STAGE II

(looking South)

FILE NAME =	USER NAME = \$USER*	DESIGNED - CML	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING DETAILS S.N. 072-0013			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
\$FILEL\$		DRAWN - CML	REVISED -		318	(4B,5B)BJR	PEORIA	25	8			
\$MODELNAME\$		CHECKED -	REVISED -		CONTRACT NO. 68D64							
		DATE -	REVISED -		SCALE:	SHEET 2 OF 3 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT			



STAGE I

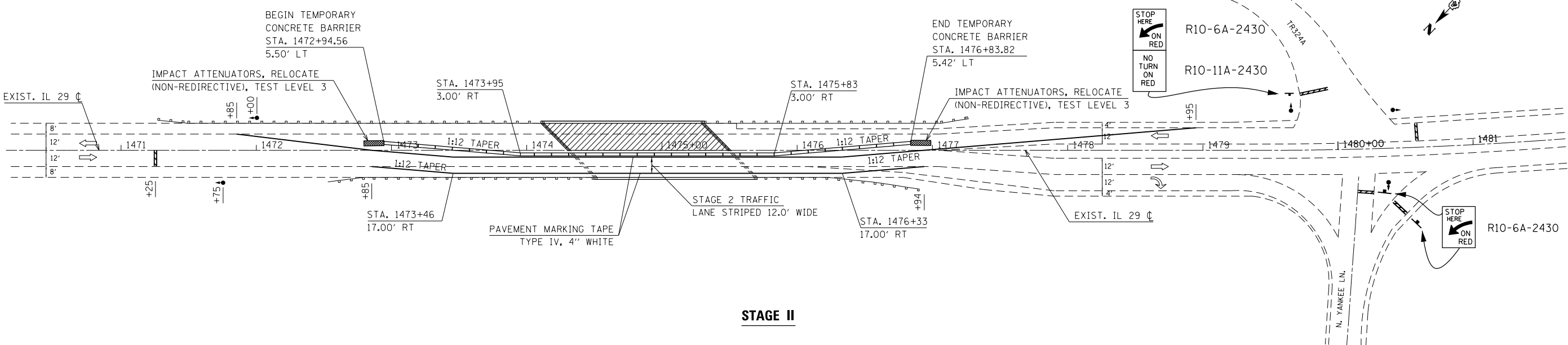
LEGEND

- TRAFFIC SIGNALS
- ▨ IMPACT ATTENUATORS
- ▨ PAVEMENT MARKING TAPE, TYPE IV 24"
- ▭ TEMPORARY CONCRETE BARRIER
- ▨ WORK AREA

NOTES

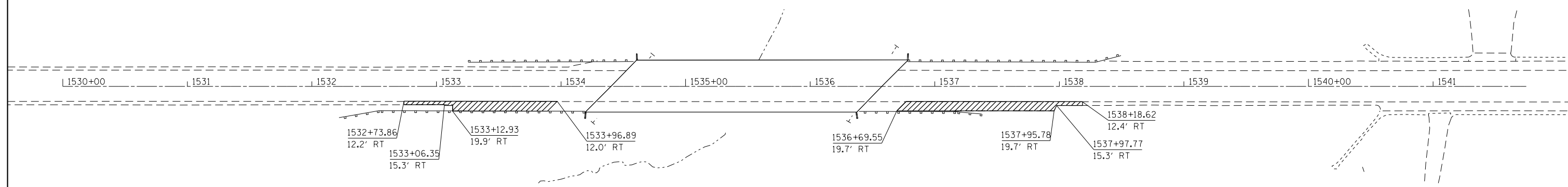
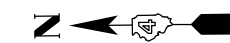
1. OFFSETS TO TEMPORARY CONCRETE BARRIER ARE MEASURED TO THE CENTERLINE OF THE BARRIER.
2. TRAFFIC CONTROL SHALL BE INSTALLED ACCORDING TO HIGHWAY STANDARD 701321 AND THIS DETAIL.
3. USE A BLACKOUT TAPE APPROVED BY THE ENGINEER TO COVER ANY CONFLICTING RAISED REFLECTIVE PAVEMENT MARKERS. USE A MATERIAL APPROVED BY THE ENGINEER TO COVER THE LENSES PRIOR TO THE PLACEMENT OF THE BLACKOUT TAPE IN ORDER TO PROTECT THE LENSES FROM THE BLACKOUT TAPE ADHESIVE. ALL COSTS ASSOCIATED WITH PROVIDING, INSTALLING, AND REMOVING THESE ITEMS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL PAY ITEMS PROVIDED IN THE PLANS.
4. THE SIGNS SHOWN ON THESE DETAILS ARE ADDITIONAL SIGNS THAT SHALL BE INCLUDED IN THE COST OF TEMPORARY BRIDGE TRAFFIC SIGNALS.

TEMPORARY BRIDGE TRAFFIC SIGNAL NOTES	
THE TEMPORARY TRAFFIC INSTALLATION SHALL CONFORM TO ALL APPLICABLE MUTCD REQUIREMENTS.	
THE TEMPORARY TRAFFIC SIGNALS SHALL CONSIST OF FOUR PHASE VEHICLE SERVICE (NB IL 29, SB IL 29, NORTH YANKEE LANE, AND TOWNSHIP ROAD 324A.)	
THE TEMPORARY TRAFFIC SIGNALS SHALL BE EQUIPPED WITH VEHICLE DETECTION FOR ALL APPROACHES (EITHER MICROWAVE OR DETECTOR LOOPS).	
ALL TRAFFIC SIGNAL HEADS SHALL BE EQUIPPED WITH RETROREFLECTIVE BACKPLATES.	
THE CONTRACTOR SHALL FURNISH AND INSTALL A "NO TURN ON RED" SIGN ON THE TOWNSHIP ROAD 324A APPROACH.	
THE CONTRACTOR SHALL NOTIFY ERIC HOWALD, THE IDOT TRAFFIC ENGINEER, AT (309) 671-4481 FORTY EIGHT HOURS PRIOR TO SIGNAL TURN ON.	



STAGE II

FILE NAME =	USER NAME = #USER*	DESIGNED - CML	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING DETAILS S.N. 072-0013			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
#FILE#		DRAWN - CML	REVISED -		SCALE:	SHEET 3	OF 3 SHEETS	STA.	TO STA.	318	(4B,5B)BJR	PEORIA	25	9
#MODELNAME#		CHECKED -	REVISED -											
		DATE -	REVISED -											



LEGEND

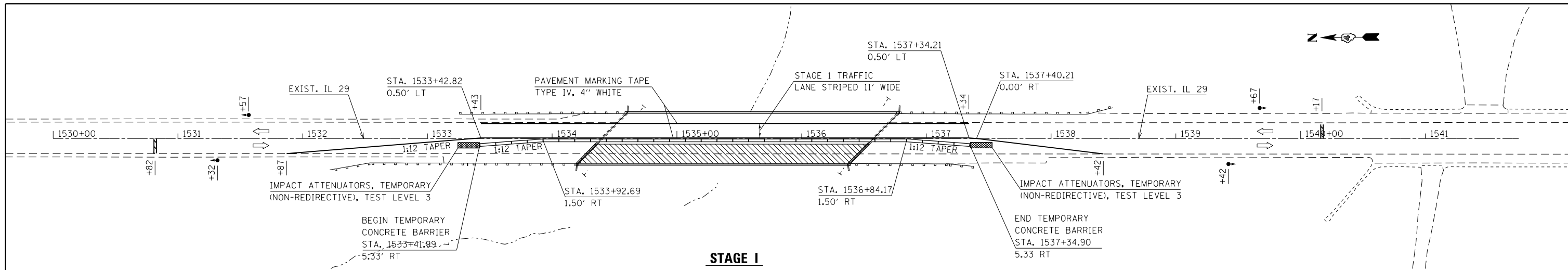
 PARTIAL DEPTH PATCHING (SPECIAL) & PARTIAL DEPTH PATCHING

NOTES

1. USE TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 TO COMPLETE THE PARTIAL DEPTH PATCHING.

PRE-STAGE

FILE NAME = Plan.dgn	USER NAME = aubreygs	DESIGNED - CML	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING DETAILS S.N. 072-0014			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - CML	REVISED -		318	(4B,5B)BJR	PEORIA	25	10			
		PLOT SCALE = 80.0000' / in.	CHECKED -		REVISED -	CONTRACT NO. 68D64						
Sheets		PLOT DATE = 4/7/2017	DATE -		REVISED -	SCALE:	SHEET 1 OF 3 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



STAGE I

LEGEND

- TRAFFIC SIGNALS
- ▨ IMPACT ATTENUATORS
- ▨ PAVEMENT MARKING TAPE, TYPE IV 24"
- ▭ TEMPORARY CONCRETE BARRIER
- ▨ WORK AREA

- NOTES**
- OFFSETS TO TEMPORARY CONCRETE BARRIER ARE MEASURED TO THE CENTERLINE OF THE BARRIER.
 - TRAFFIC CONTROL SHALL BE INSTALLED ACCORDING TO HIGHWAY STANDARD 701321 AND THIS DETAIL.
 - USE A BLACKOUT TAPE APPROVED BY THE ENGINEER TO COVER ANY CONFLICTING RAISED REFLECTIVE PAVEMENT MARKERS. USE A MATERIAL APPROVED BY THE ENGINEER TO COVER THE LENSES PRIOR TO THE PLACEMENT OF THE BLACKOUT TAPE IN ORDER TO PROTECT THE LENSES FROM THE BLACKOUT TAPE ADHESIVE. ALL COSTS ASSOCIATED WITH PROVIDING, INSTALLING, AND REMOVING THESE ITEMS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL PAY ITEMS PROVIDED IN THE PLANS.
 - THE SIGNS SHOWN ON THESE DETAILS ARE ADDITIONAL SIGNS THAT SHALL BE INCLUDED IN THE COST OF TEMPORARY BRIDGE TRAFFIC SIGNALS.

TEMPORARY BRIDGE TRAFFIC SIGNAL NOTES

THE TEMPORARY TRAFFIC INSTALLATION SHALL CONFORM TO ALL APPLICABLE MUTCD REQUIREMENTS.

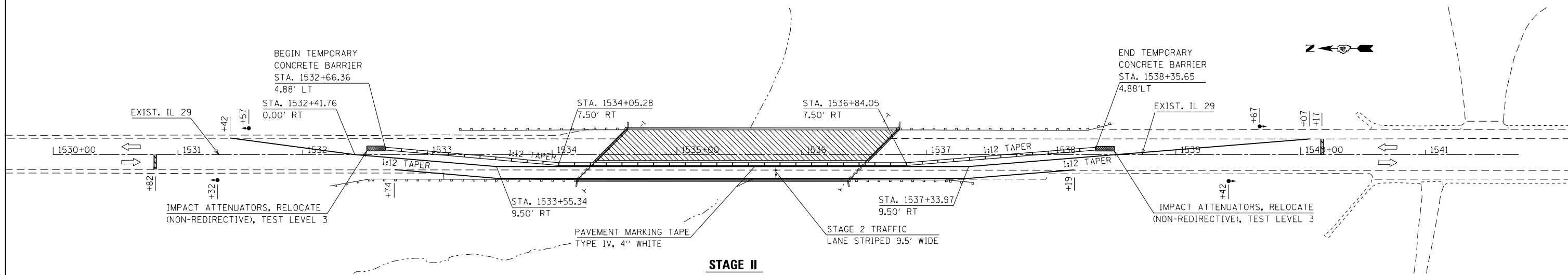
THE TEMPORARY TRAFFIC SIGNALS SHALL CONSIST OF FOUR PHASE VEHICLE SERVICE (NB IL 29, SB IL 29, NORTH YANKEE LANE, AND TOWNSHIP ROAD 324A.)

THE TEMPORARY TRAFFIC SIGNALS SHALL BE EQUIPPED WITH VEHICLE DETECTION FOR ALL APPROACHES (EITHER MICROWAVE OR DETECTOR LOOPS).

ALL TRAFFIC SIGNAL HEADS SHALL BE EQUIPPED WITH RETROREFLECTIVE BACKPLATES.

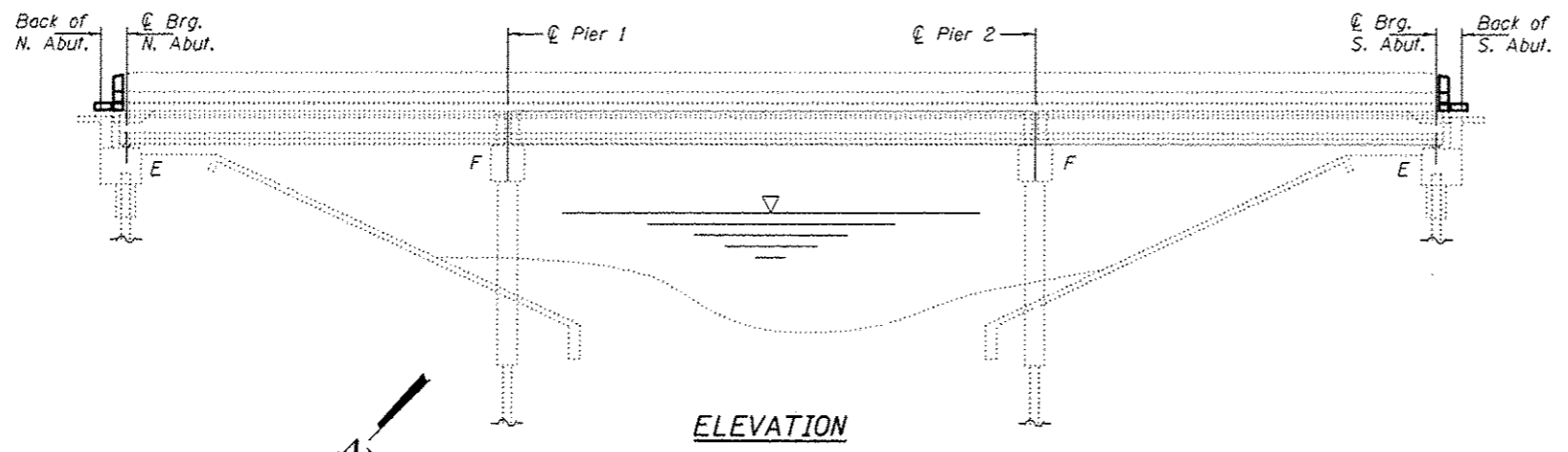
THE CONTRACTOR SHALL FURNISH AND INSTALL A "NO TURN ON RED" SIGN ON THE TOWNSHIP ROAD 324A APPROACH.

THE CONTRACTOR SHALL NOTIFY ERIC HOWALD, THE IDOT TRAFFIC ENGINEER, AT (309) 671-4481 FORTY EIGHT HOURS PRIOR TO SIGNAL TURN ON.

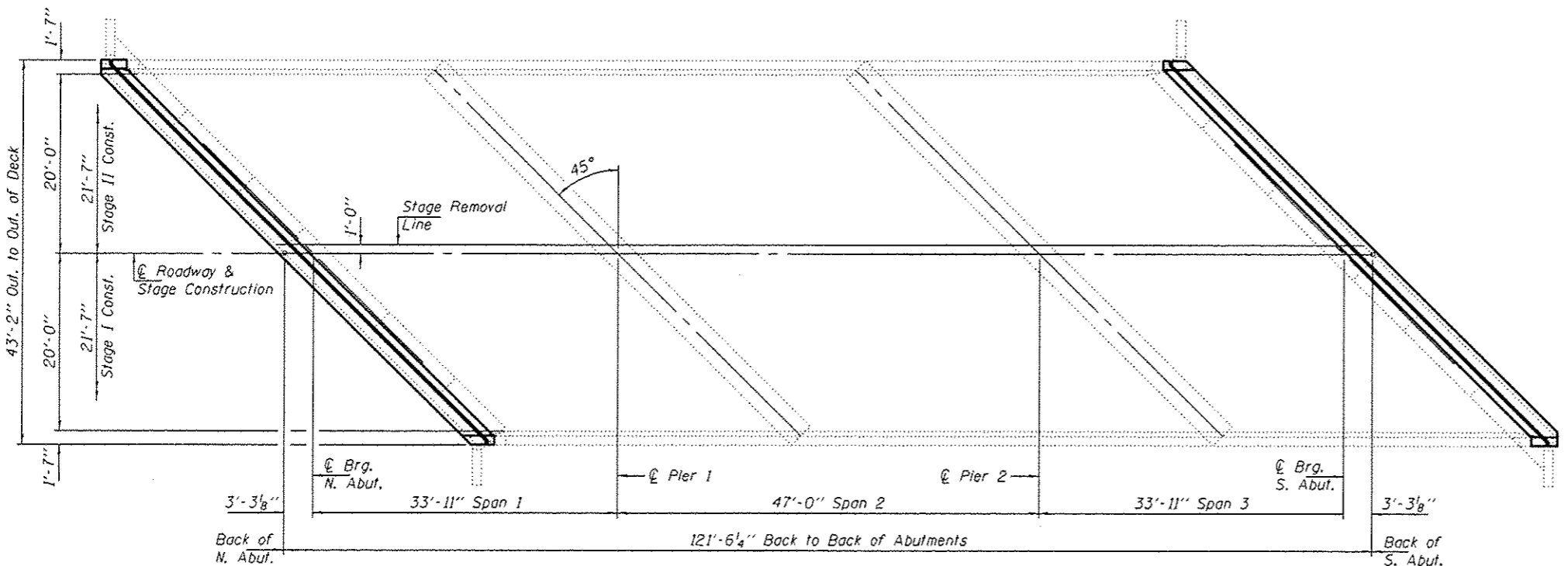


STAGE II

FILE NAME = Plan.dgn	USER NAME = aubreys	DESIGNED - CML	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGING DETAILS S.N. 072-0014	F.A. RTE. = 318	SECTION = (4B,5B)BJR	COUNTY = PEORIA	TOTAL SHEETS = 25	SHEET NO. = 12		
Sheets	PLOT SCALE = 80.0000' / in.	CHECKED -	REVISED -			SCALE:	SHEET 3 OF 3 SHEETS	STA.	TO STA.	CONTRACT NO. 68D64		
	PLOT DATE = 4/7/2017	DATE -	REVISED -			ILLINOIS FED. AID PROJECT						



ELEVATION



PLAN

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars designated (E) shall be epoxy coated.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC I-Beam.

Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

Cost of removal and re-installation of guardrail necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Concrete Removal.

The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	5.2
Concrete Superstructure	Cu. Yd.	5.1
Reinforcement Bars, Epoxy Coated	Pound	940
Bar Splicers	Each	10
Preformed Joint Strip Seal	Foot	122
Protective Coat	Sq. Yd.	33
--	--	--
--	--	--

* On new concrete only.



Expires: November 30, 2018

DESIGNED - *Stephan M. Ryan*
 CHECKED - *[Signature]*
 DRAWN - Kyle M. Steffan
 CHECKED - *SML, NRA*

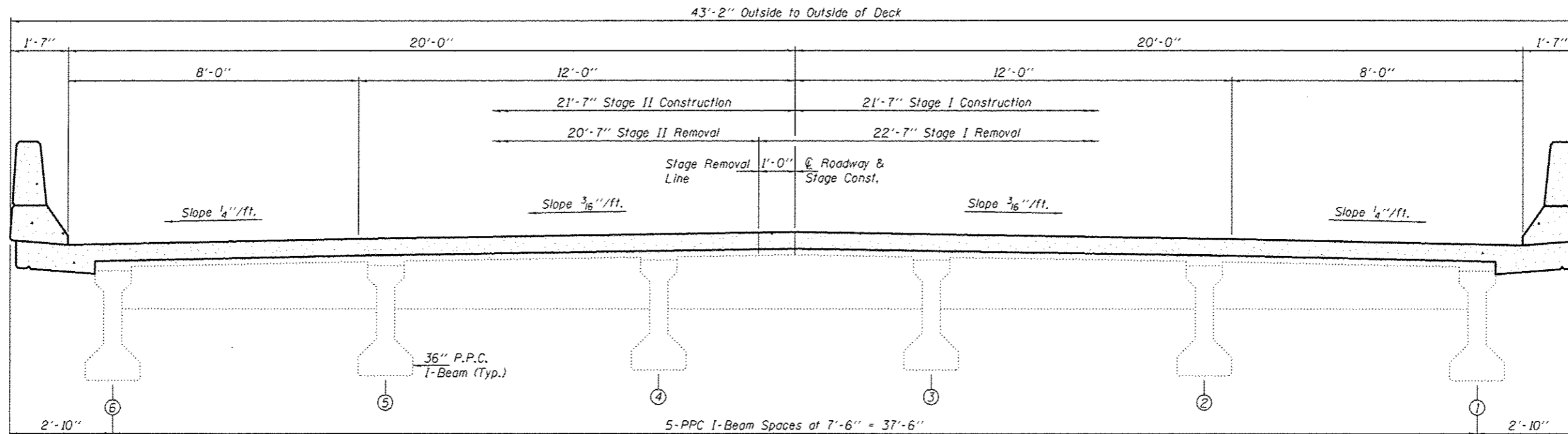
PASSED *[Signature]*
 ENGINEER OF BRIDGES AND STRUCTURES

DATE - MAY 5, 2017
 REVISED
 REVISED

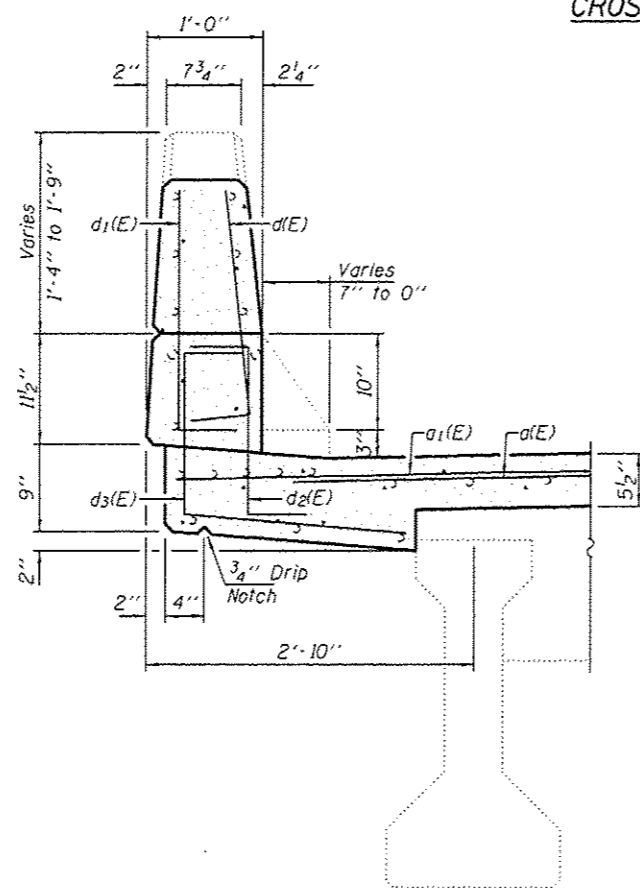
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
 ILLINOIS ROUTE 29 OVER COON CREEK
 SN 072-0013
 SHEET NO. 1 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	(48.5B)JR	PEORIA	25	13
CONTRACT NO. 68D64			ILLINOIS FED. AID PROJECT	




CROSS SECTION AT ABUTMENTS
(Looking South)



SECTION THRU PARAPET

DESIGNED - SMR
CHECKED - NRM
DRAWN - Kyle M. Steffen
CHECKED - SMR NRM

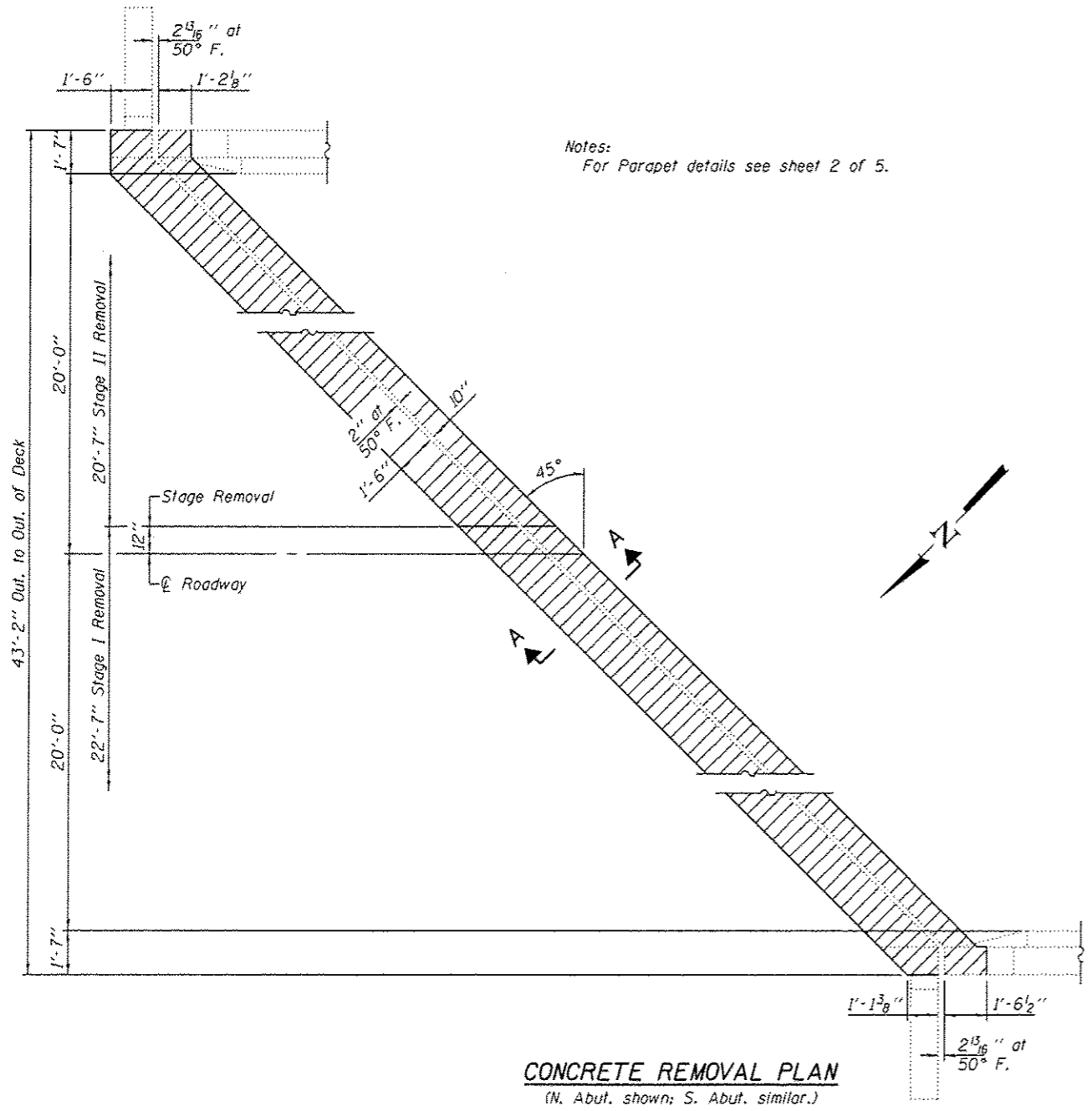
PASSED

 ENGINEER OF BRIDGES AND STRUCTURES

DATE - MAY 5, 2017
REVISED
REVISED

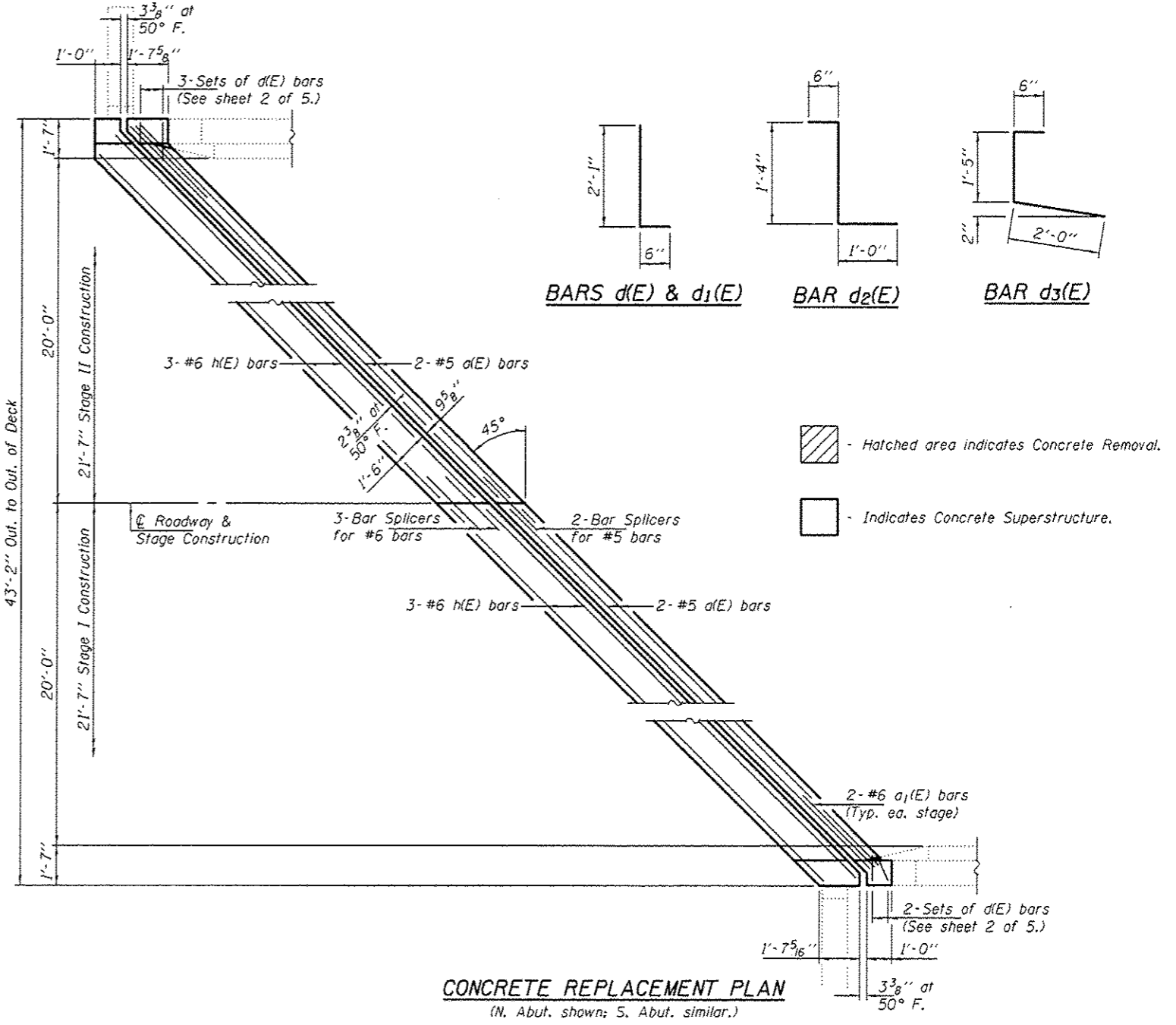
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REPAIR DETAILS
SN 072-0013
 SHEET NO. 2 OF 5 SHEETS

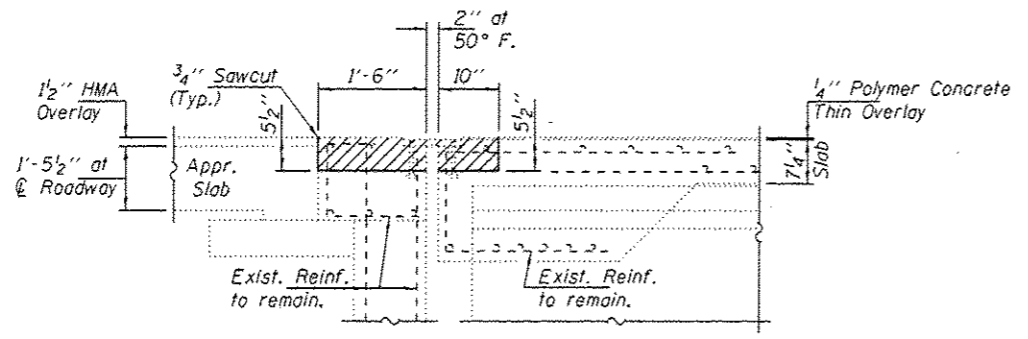
F.A.P. RTE. 318	SECTION 148.5B1B-JR	COUNTY PEORIA	TOTAL SHEETS 25	SHEET NO. 14
CONTRACT NO. 68D64				ILLINOIS FED. AID PROJECT



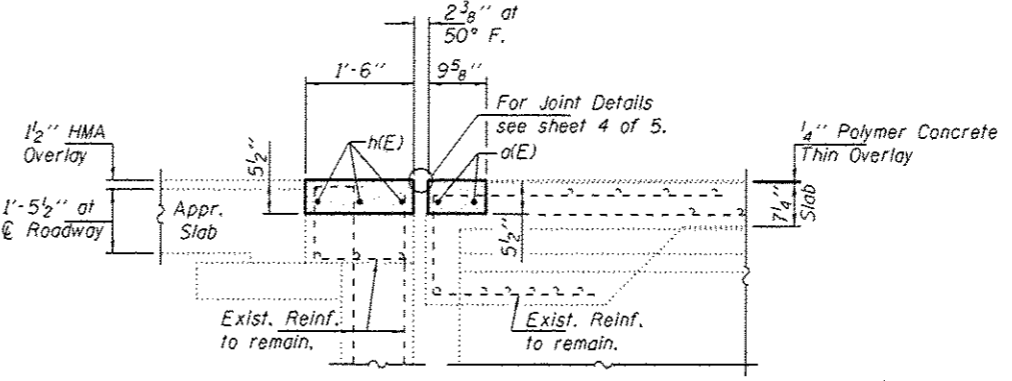
CONCRETE REMOVAL PLAN
(N. Abut. shown; S. Abut. similar.)



CONCRETE REPLACEMENT PLAN
(N. Abut. shown; S. Abut. similar.)



SECTION A-A
Dims. at Rt. L's



SECTION B-B
Dims. at Rt. L's

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	8	#5	30'-0"	—
a1(E)	8	#6	4'-0"	—
d(E)	10	#5	2'-7"	L
d1(E)	10	#4	2'-7"	L
d2(E)	10	#5	2'-10"	L
d3(E)	10	#4	3'-11"	L
h(E)	12	#6	30'-2"	—
Concrete Removal			Cu. Yd.	5.2
Concrete Superstructure			Cu. Yd.	5.1
Bar Splicers			Each	10
Reinforcement Bars, Epoxy Coated			Pound	940

DESIGNED - SMR
CHECKED - NRM
DRAWN - Kyle M. Steffen
CHECKED - SMR NRM

DATE - MAY 5, 2017
REVISOR
REVISOR

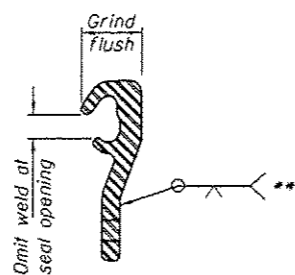
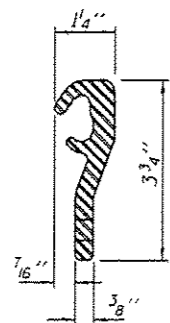
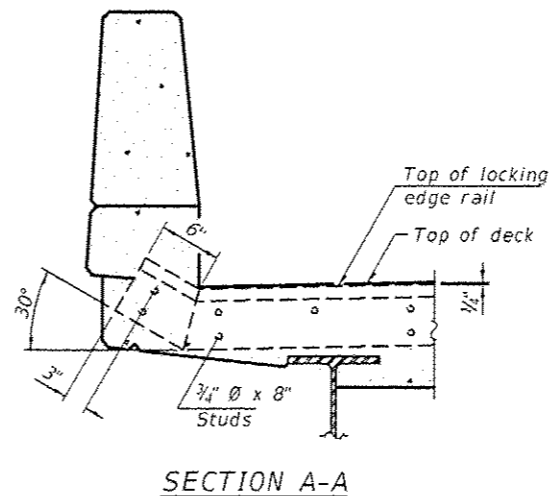
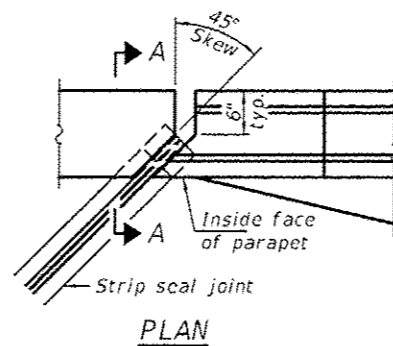
ENGINEER OF BRIDGES AND STRUCTURES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

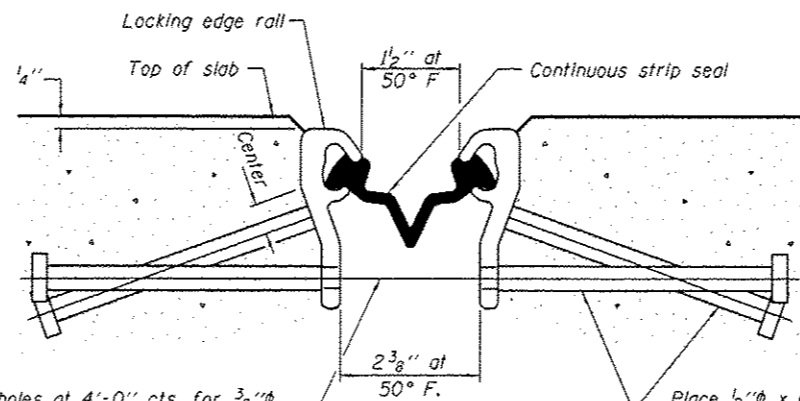
JOINT REMOVAL & REPLACEMENT DETAILS
SN 072-0013

SHEET NO. 3 OF 5 SHEETS

F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
318 14B.581BJR PEORIA 25 15
CONTRACT NO. 68D64
ILLINOIS FED. AID PROJECT



** Back gouge not required if complete joint penetration is verified by mock-up.



7/16" Ø holes at 4'-0" cts. for 3/8" Ø bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Place 1/2" Ø x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alf. cts.

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed.

The inside of the Locking Edge Rail groove shall be free of weld residue. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

The manufacturer's recommended installation methods shall be followed.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	122

DESIGNED - SMR	PASSED
CHECKED - NRM	
DRAWN - Kyle M. Steffen	
CHECKED - SMR NRM	

DATE - MAY 5, 2017

 ENGINEER OF BRIDGES AND STRUCTURES

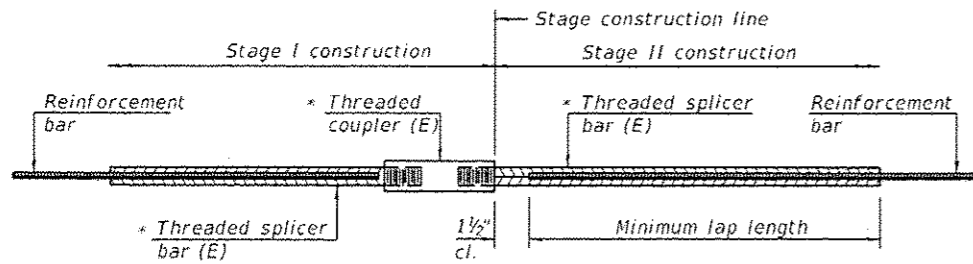
REVISIONS

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS
 SN 072-0013

SHEET NO. 4 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	14B,5B18JR	PEORIA	25	16
CONTRACT NO. 68D64				
ILLINOIS FED. AID PROJECT				

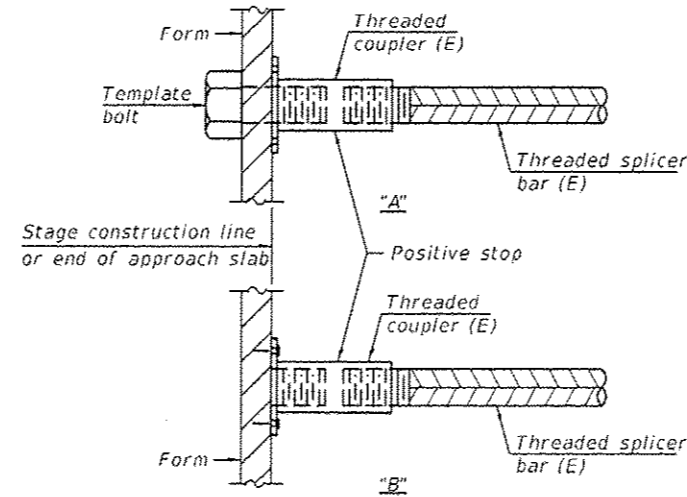


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Bridge Deck	#5	4	3'-6"
Abutment	#6	6	4'-0"

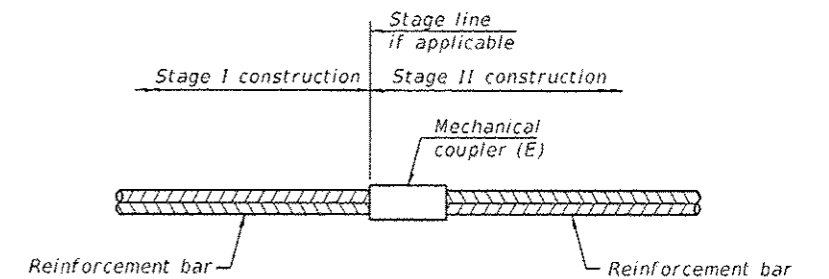


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

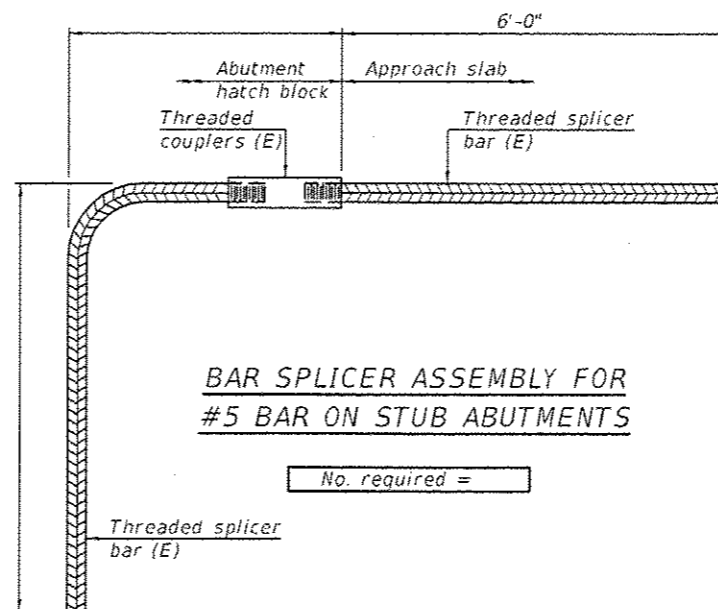
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 2-17-2017

DESIGNED - SMR	
CHECKED - NRM	
DRAWN - Kyle M. Steffen	
CHECKED - SMR NRM	

PASSED

Carl Perry
ENGINEER OF BRIDGES AND STRUCTURES

DATE - MAY 5, 2017

REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
SN 072-0013

SHEET NO. 5 OF 5 SHEETS

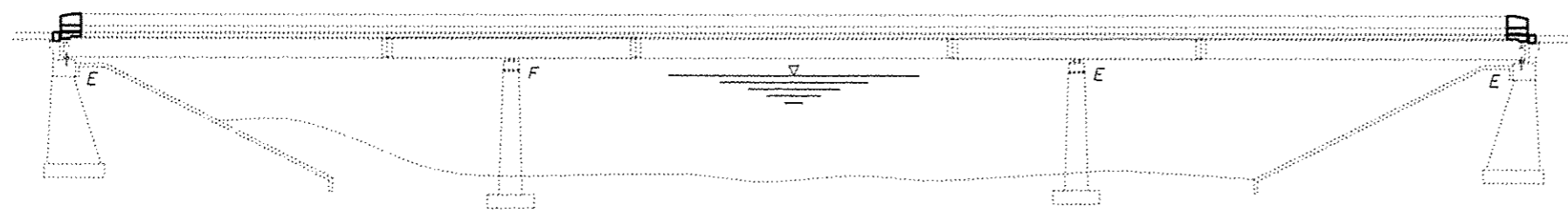
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	148.5B1BJR	PEORIA	25	17
CONTRACT NO. 68D64				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

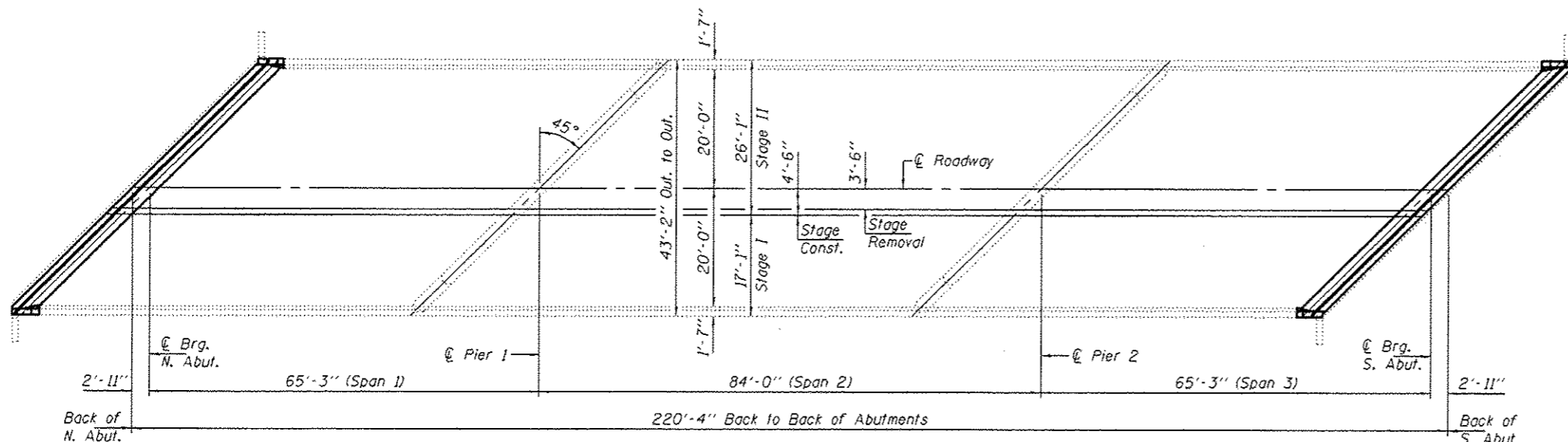
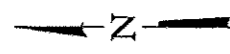
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars designated (E) shall be epoxy coated.
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
 Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.
 Cost of removal and re-installation of guardrail necessary to complete the work as detailed on the plans and as specified in the Special Provisions shall be included with Concrete Removal.

The deck surface shall have its final finish lined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.
 Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.



ELEVATION



PLAN

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	14.2
Concrete Superstructure	Cu. Yd.	14.2
Reinforcement Bars, Epoxy Coated	Pound	1840
Bar Splicers	Each	24
Preformed Joint Strip Seal	Foot	122
* Protective Coat	Sq. Yd.	41.9
--	--	--
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* On new concrete only.



Expires: November 30, 2018

DESIGNED - *[Signature]*
 CHECKED - *[Signature]*
 DRAWN - Kyle M. Steffen
 CHECKED - *[Signature]*

PASSED
[Signature]
 ENGINEER OF BRIDGES AND STRUCTURES

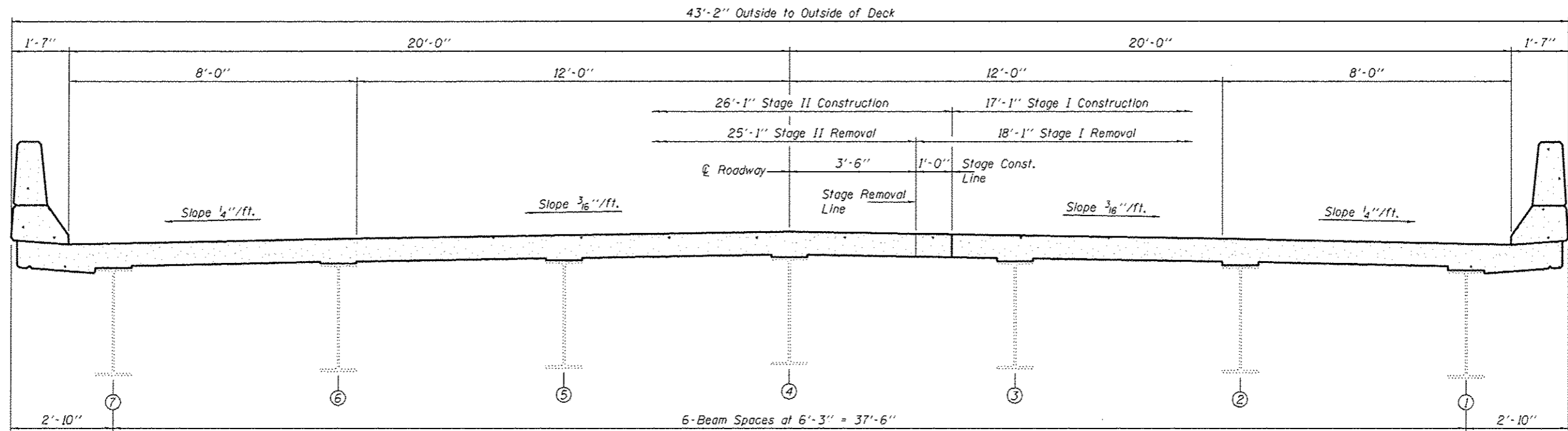
DATE - MAY 5, 2017
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

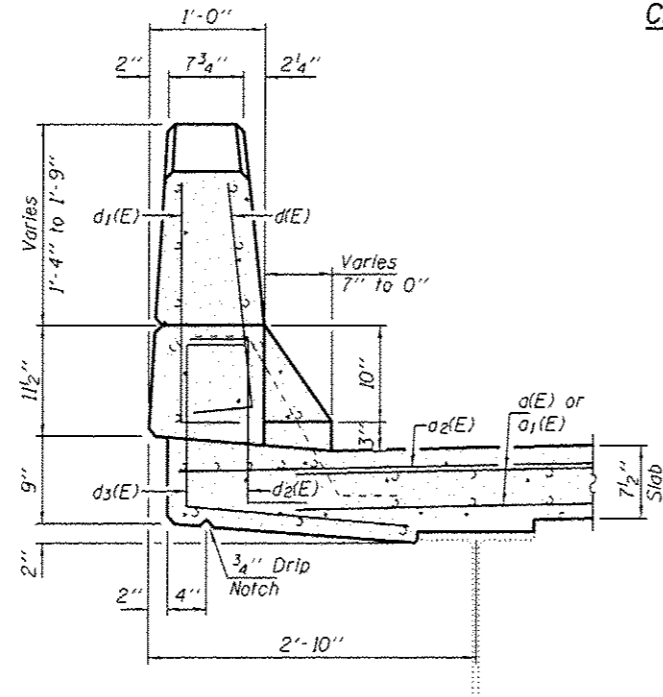
GENERAL PLAN & ELEVATION
 ILLINOIS ROUTE 29 OVER SENACHWINE CREEK
 SN 072-0014

SHEET NO. 1 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
31B	14B,5B1BJR	PEORIA	25	18
				CONTRACT NO. 68D64
ILLINOIS FED. AID PROJECT				

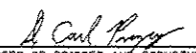


CROSS SECTION AT ABUTMENTS
(Looking South)



SECTION THRU PARAPET

DESIGNED - SMR
 CHECKED - NRM
 DRAWN - Kyle M. Steffen
 CHECKED - SMR NRM

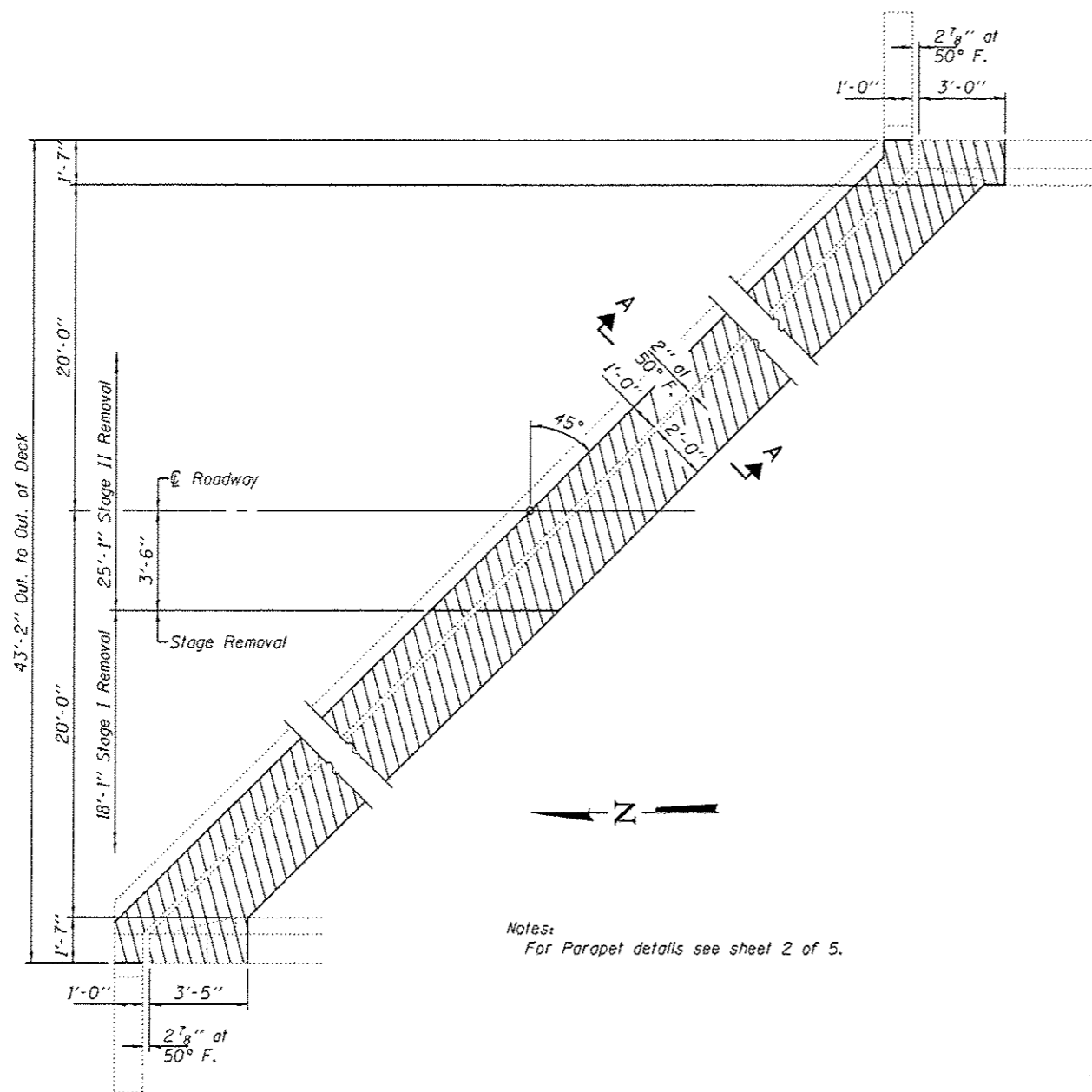
PASSED

 ENGINEER OF BRIDGES AND STRUCTURES
 DATE - MAY 5, 2017
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

STAGING AND REPAIR DETAILS
 SN 072-0014

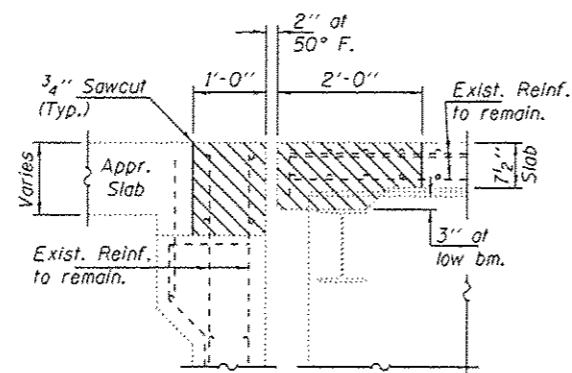
SHEET NO. 2 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	14B,581BJR	PEORIA	25	19
CONTRACT NO. 68064			ILLINOIS FED. AID PROJECT	

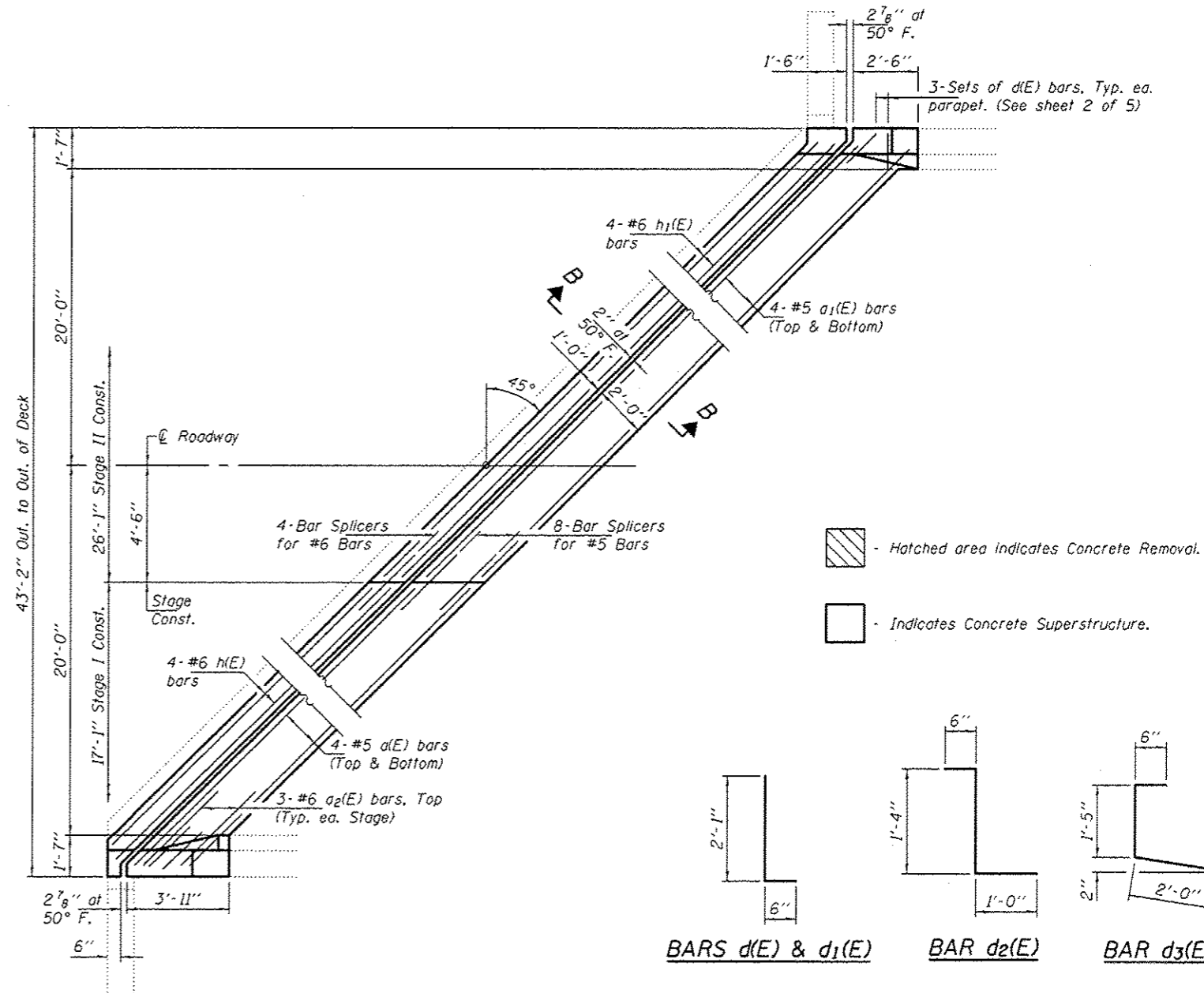


CONCRETE REMOVAL PLAN
(N. Abut. shown; S. Abut. similar.)

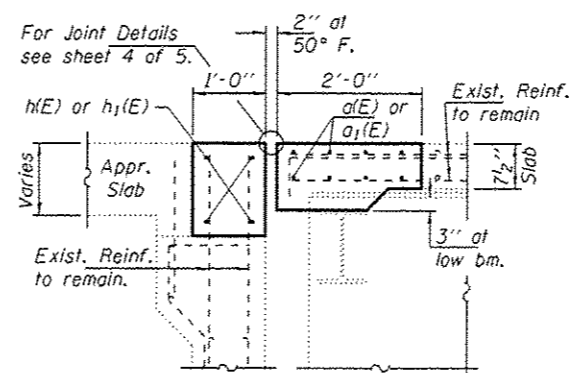
Notes:
For Parapet details see sheet 2 of 5.



SECTION A-A
Dims. at Rt. L's

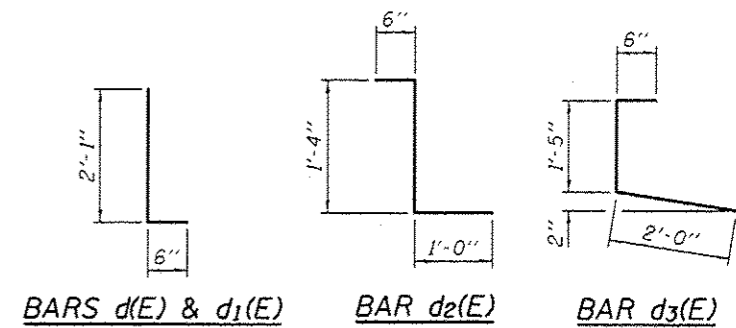


CONCRETE REPLACEMENT PLAN
(N. Abut. shown; S. Abut. similar.)



SECTION B-B
Dims. at Rt. L's

- Hatched area indicates Concrete Removal.
 - Indicates Concrete Superstructure.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	16	#5	22'-10"	—
a1(E)	16	#5	34'-8"	—
a2(E)	12	#6	4'-0"	—
d(E)	12	#5	2'-7"	L
d1(E)	12	#4	2'-7"	L
d2(E)	12	#5	2'-10"	L
d3(E)	12	#4	3'-11"	L
h(E)	8	#6	22'-10"	—
h1(E)	8	#6	34'-8"	—
Concrete Removal			Cu. Yd.	14.2
Concrete Superstructure			Cu. Yd.	14.2
Bar Splicers			Each	24
Reinforcement Bars, Epoxy Coated			Pound	1840

DESIGNED - SMR
CHECKED - NRM
DRAWN - Kyle M. Steffen
CHECKED - SMR NRM

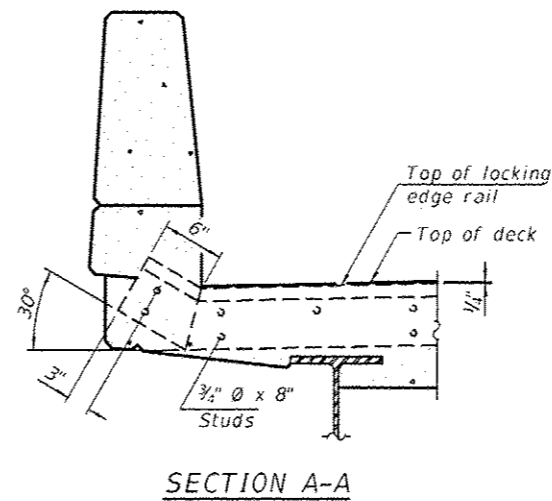
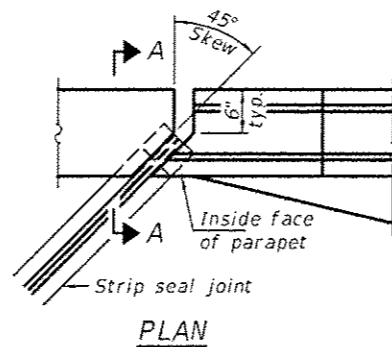
DATE - MAY 5, 2017
REVISOR
REVISOR
PASSED
Carl Perry
ENGINEER OF BRIDGES AND STRUCTURES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

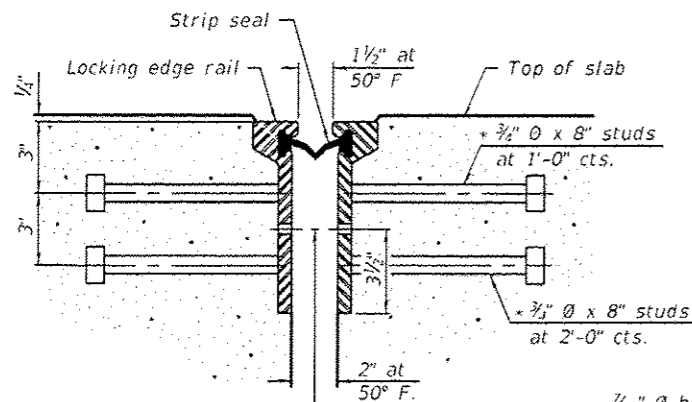
JOINT REMOVAL & REPLACEMENT DETAILS
SN 072-0014

SHEET NO. 3 OF 5 SHEETS

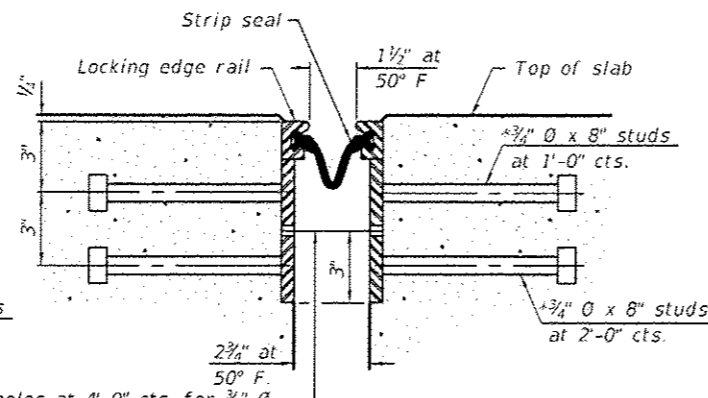
F.A.P. SECTION COUNTY TOTAL SHEET SHEET NO.
RTE. 318 14B,SB18JR PEORIA 25 20
CONTRACT NO. 68D64
ILLINOIS FED. AID PROJECT



Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
 The manufacturer's recommended installation methods shall be followed.
 The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

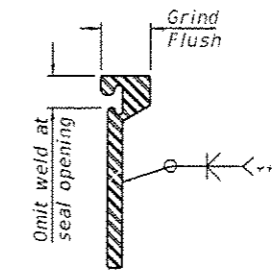
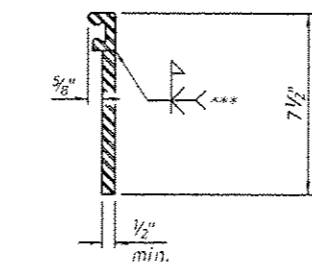
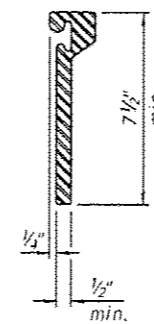


1/16" Ø holes at 4'-0" cts. for 3/8" Ø bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.



1/16" Ø holes at 4'-0" cts. for 3/8" Ø bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



*** Back gouge not required if complete joint penetration is verified by mock-up.

ROLLED EXTRUDED RAIL WELDED RAIL

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.
 Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	122

DESIGNED - SMR
 CHECKED - NRM
 DRAWN - Kyle M. Stoffan
 CHECKED - SMR NRM

PASSED

Carl Perry
 ENGINEER OF BRIDGES AND STRUCTURES

DATE - MAY 5, 2017

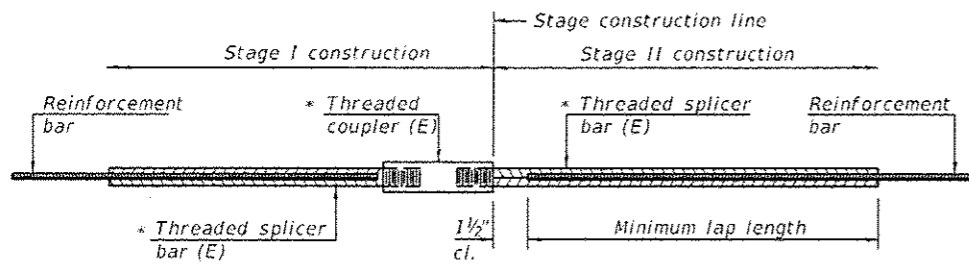
REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS
 SN 072-0014

SHEET NO. 4 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	14B,5B18JR	PEORIA	25	21
CONTRACT NO. 68D64			ILLINOIS FED. AID PROJECT	

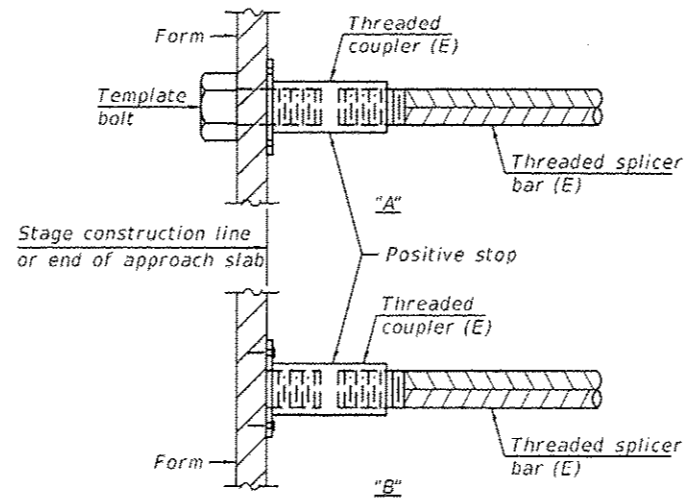


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

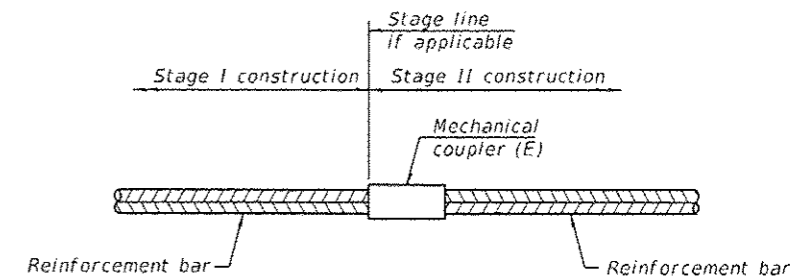
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Bridge Deck	#5	16	3'-6"
Abutment	#6	8	4'-0"



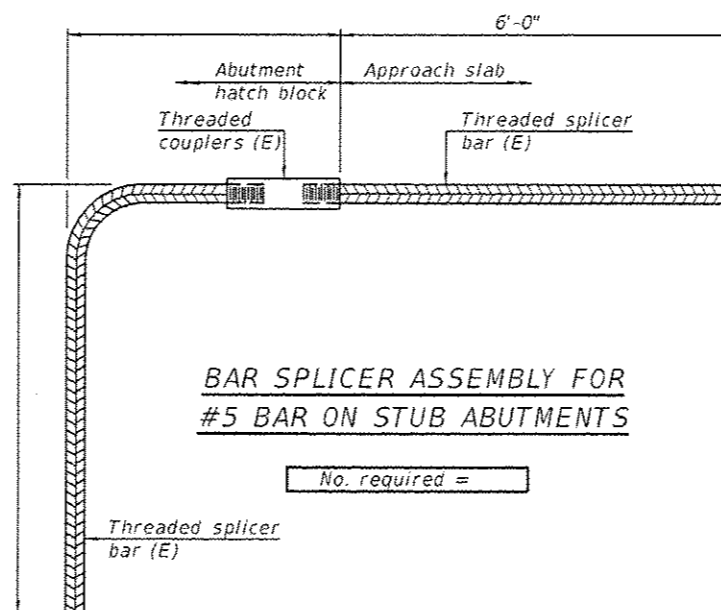
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

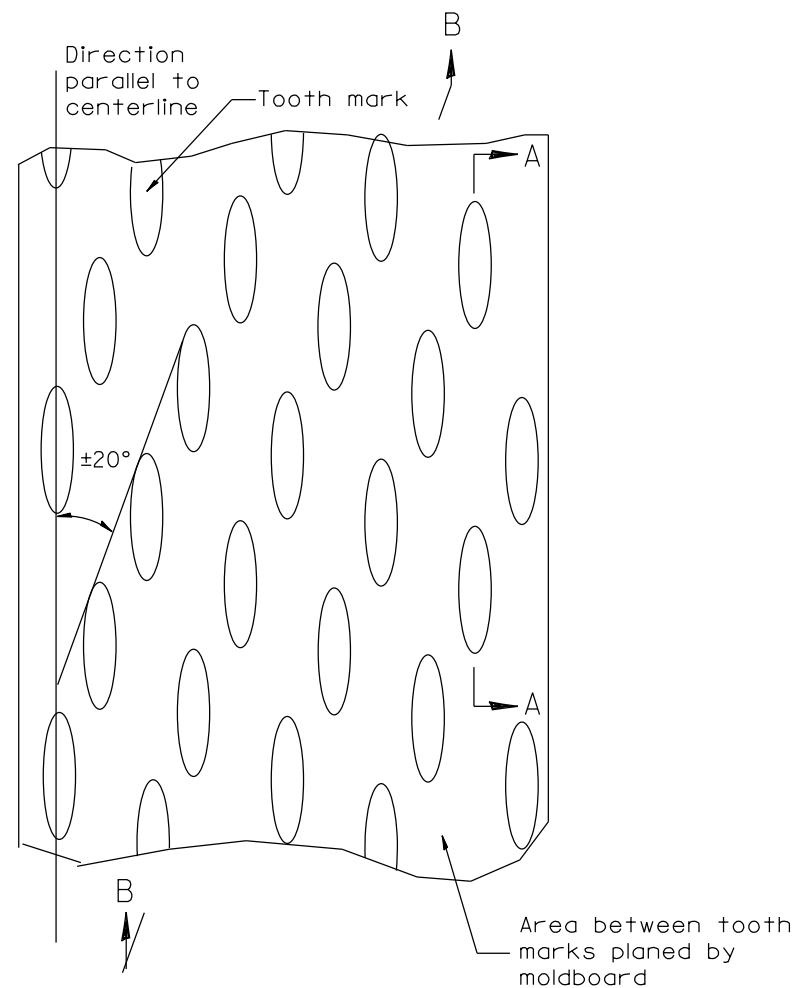
No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 2-17-2017

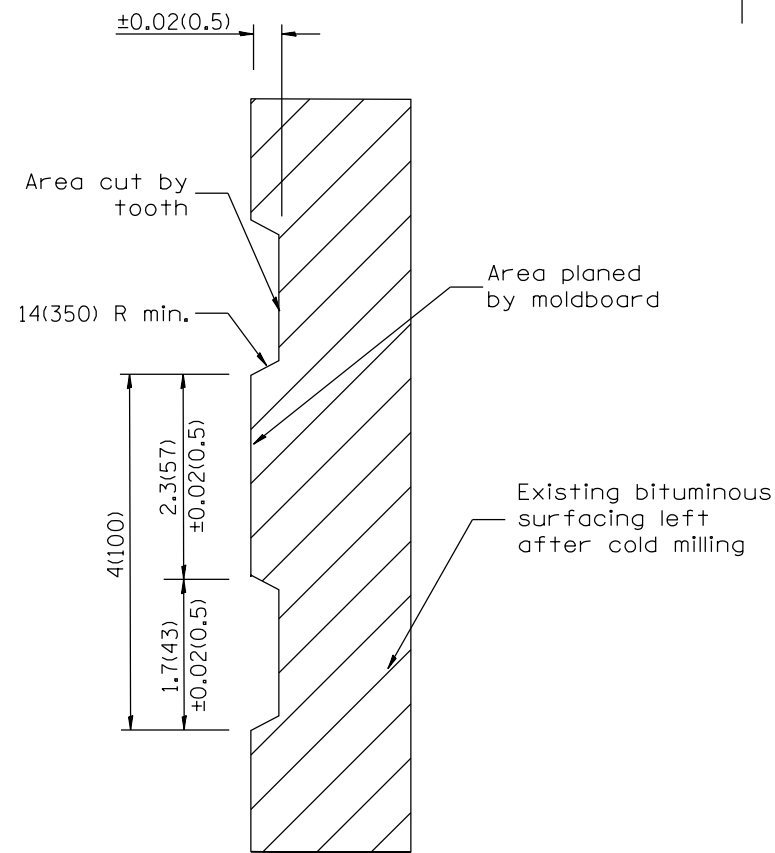
DESIGNED - SMR	DATE - MAY 5, 2017	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS SN 072-0014		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
CHECKED - NRM	REVISOR				318	14B.SBIBJR	PEORIA	25	22
DRAWN - Kyle M. Steffen	REVISOR				CONTRACT NO. 68D64				
CHECKED - SMR NRM	REVISOR				SHEET NO. 5 OF 5 SHEETS				
ENGINEER OF BRIDGES AND STRUCTURES					ILLINOIS FED. AID PROJECT				



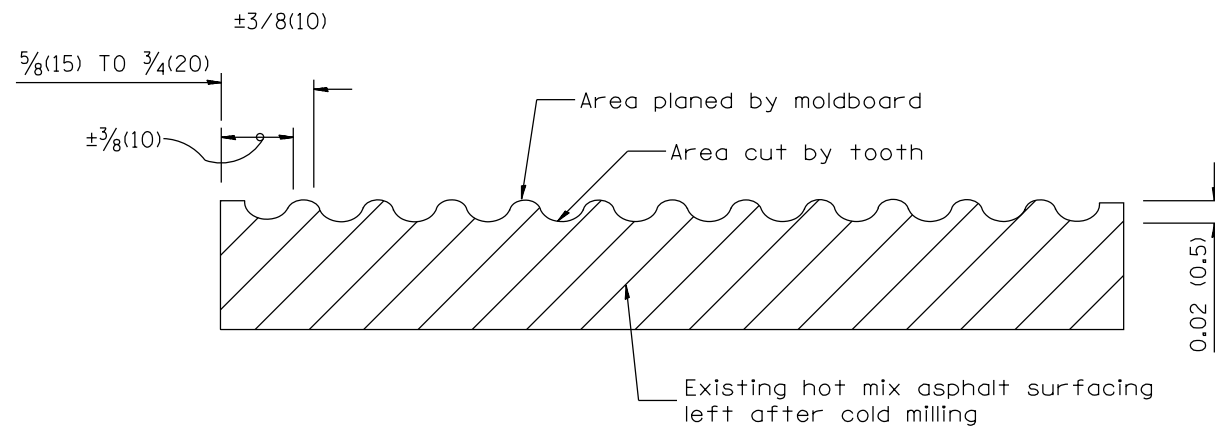
PLAN

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



SECTION A-A



SECTION B-B PROJECTED
PERPENDICULAR TO CENTERLINE

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-104.01, NEW REVISION BOX	T.P.
04-20-98	REMOVED MILLING DETAIL FROM STANDARD	J.A.
09-08-98	CORRECT NOTE LEADER PLACEMENT	R.W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

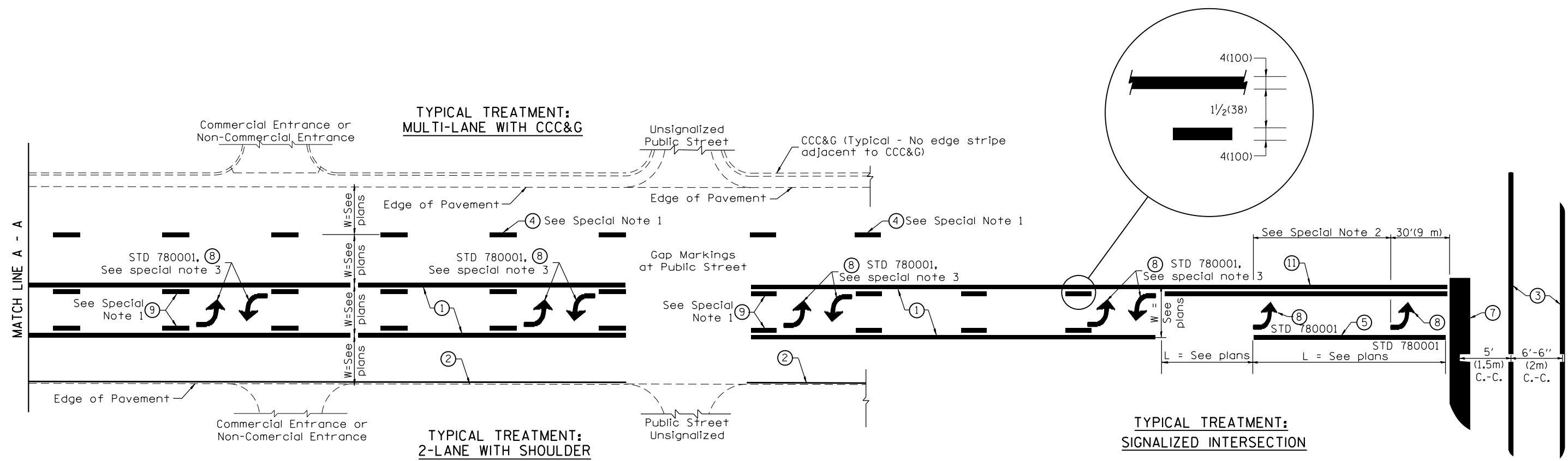
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

NOT TO SCALE

CADD STD. 440001-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
318	(4B,5B)BJR	PEORIA	25	23
CONTRACT NO. 68D64				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

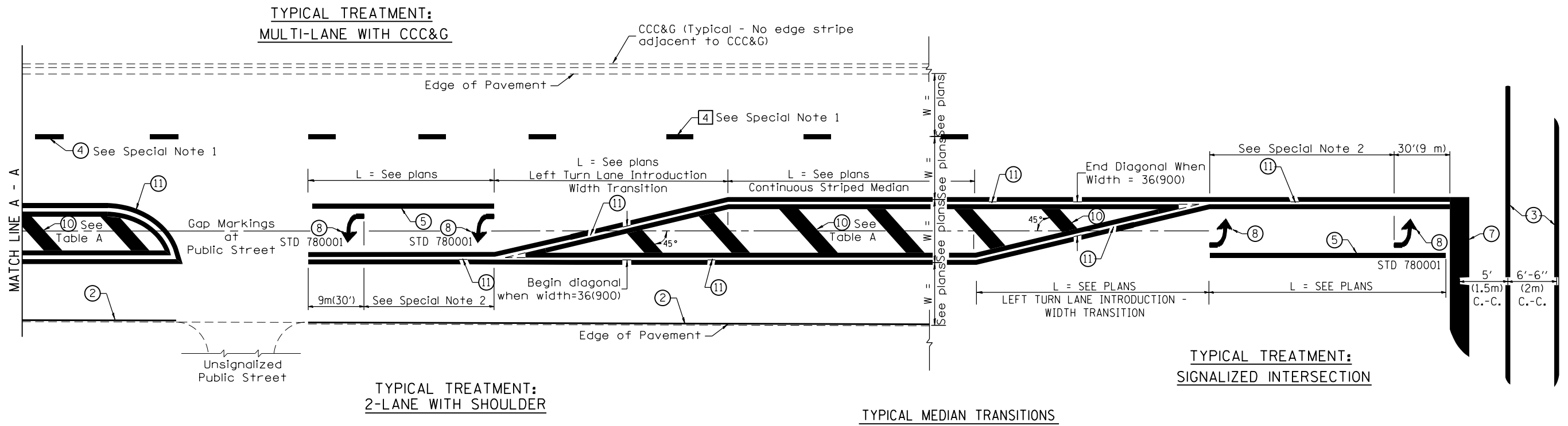
GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL PAVEMENT MARKINGS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.			318	(4B,5B)BJR	PEORIA	25	24	
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.						SHT. 1 OF 2 CADD STD. 780001-D4					
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.						CONTRACT NO. 68D64					

NOT TO SCALE

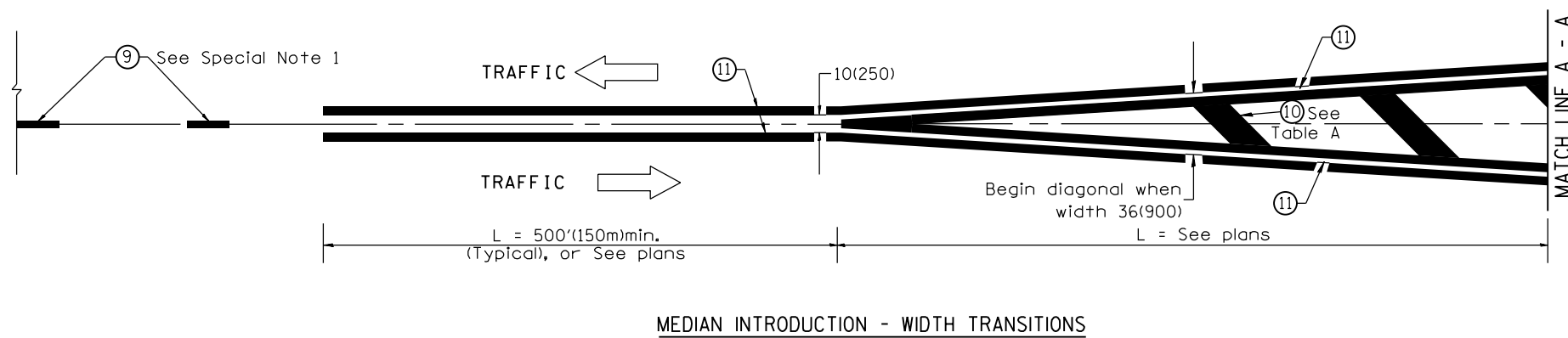
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.