

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAI ROUTE 155 (I-155)
SECTION 90-[(107-1)HB]BJR,B-R
CONTRACT MAINTENANCE
TAZEWELL COUNTY

C-94-061-17

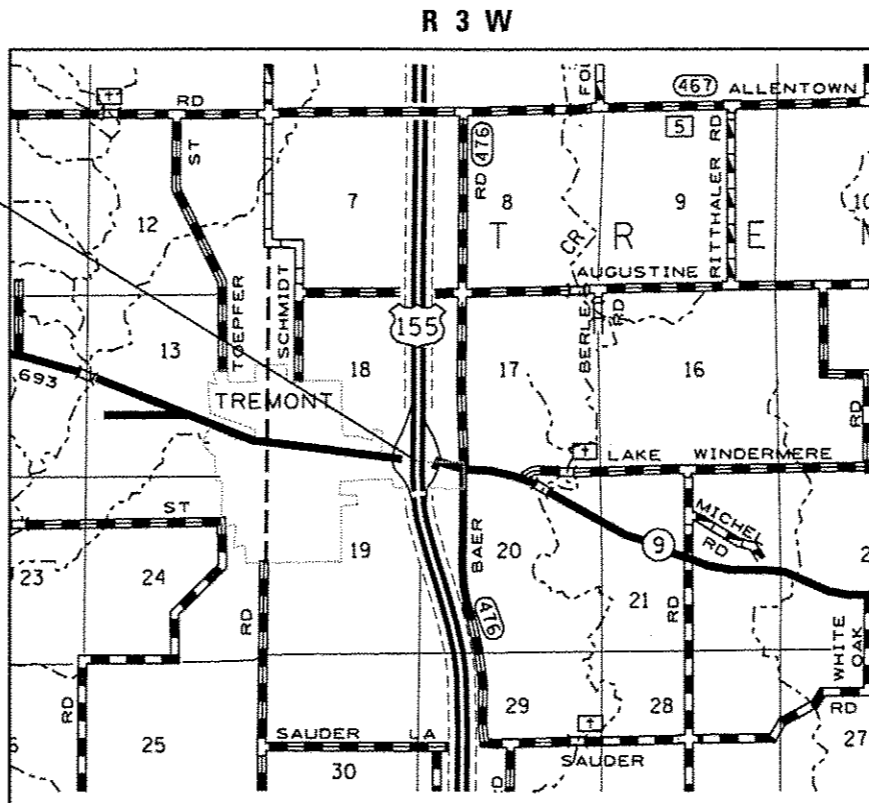
FOR INDEX OF SHEETS, SEE SHEET NO. 2

HIGHWAY STANDARDS

001001-02	701426-09
642001-02	701601-09
701101-05	701901-06
701400-09	704001-08
701401-10	780001-05
701402-12	782006
701411-09	

ADT = 13100 (2015)
SU = 4.39%
MU = 11.83%

SN 090-0134 (SB I-155 STA. 345 + 24.45
TO STA. 346 + 50.40) AND
SN 090-0135 (NB I-155 STA. 345 + 35.65
TO STA. 346 + 61.69)



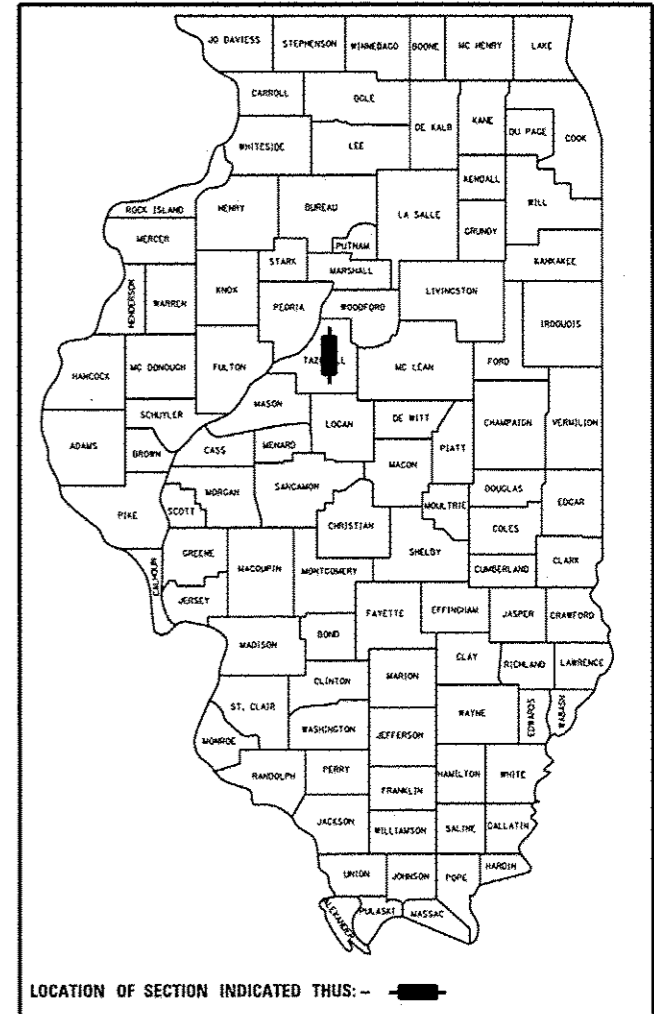
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER RICH DOTSON (309-671-3455)
PROJECT MANAGER NICOLE FAYANT (309-671-3473)
CATALOG NO. 035485-00D
CONTRACT NO. 68D65

GROSS LENGTH = 137.24 FT. = 0.026 MILE
NET LENGTH = 137.24 FT. = 0.026 MILE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-[(107-1)HB]BJR,B-R	TAZEWELL	22	1
		ILLINOIS	CONTRACT NO. 68D65	

D-94-032-17



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED April 7 2017
Rensil A. Garnett
REGION THREE ENGINEER

May 12 2017
Mahveen M. Adis
ENGINEER OF DESIGN AND ENVIRONMENT

May 12 2017
Amelle
DIRECTOR OF PROGRAM DEVELOPMENT

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

- 1. COVER SHEET
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- 4-5. SUMMARY OF QUANTITIES
- 6-7. SCHEDULE OF QUANTITIES
- 8-12. STAGING DETAILS
- 13-19. BRIDGE JOINT REPAIR DETAILS
- 20-22. DISTRICT CADD STANDARDS

GENERAL NOTES

COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made. There are no commitments for this project.

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

Prior to any waste materials being removed from the construction site the required environmental resource surveys will need to be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Environmental Survey Request)
- * BDE Form 2290 (Waste/Use Area Review)
- * A location map showing the size limits and location of the use area
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form-D4 PI0101

Please note that a minimum of four weeks shall be allowed for the District to obtain the required environmental clearances and six weeks for the required borrow site environmental clearances.

POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

Surface Type	Residual Rate
Milled (HMA or PCC)	0.08 lbs./sq.ft.
Existing Pavement	0.04 lbs./sq.ft.
Fog Coat (between lifts)	0.04 lbs./sq.ft.

FILE NAME : 60065 Cover Sheet.dgn	USER NAME : ksthr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS AND GENERAL NOTES	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default	PLOT SCALE * 100.0000 * / in.	DRAWN -	REVISED -			155	90-(1107-1)HB)BJR,B-R	TAZEWELL	22	2	
	PLOT DATE * 4/7/2017	CHECKED -	REVISED -			SCALE:		SHEET OF SHEETS		STA. TO STA.	
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
						CONTRACT NO. 68D65					

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE TAZEWELL CO.	100% STATE TAZEWELL CO.
				SN 090-0134 0014 ✓	SN 090-0135 0014 ✓
40600295	POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	POUND	607	303	304
40603310	HOT - MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	94	47	47
42001300	PROTECTIVE COAT	SO YD	60	27.5	32.5
44000157	HOT - MIX ASPHALT SURFACE REMOVAL, 2 "	SO YD	842	420	422
50102400	CONCRETE REMOVAL	CU YD	17.6	8.8	8.8
50300255	CONCRETE SUPERSTRUCTURE	CU YD	17.6	8.8	8.8
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2320	1160	1160
50800515	BAR SPLICERS	EACH	40	20	20
52000110	PREFORMED JOINT STRIP SEAL	FOOT	179	89.5	89.5
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	3033	1513	1520
67100100	MOBILIZATION	L SUM	1	0.5	0.5
70100205	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	EACH	2	1	1
70100207	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	EACH	2	1	1
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	2	1	1

14

FILE NAME : 68065 Cover Sheet.dgn	USER NAME : hschbr	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-(1107-1)HBJR,B-R	TAZEWELL	22	4
CONTRACT NO. 68065				
ILLINOIS FED. AID PROJECT				

Rev

7/1/2017

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				100% STATE TAZEWELL CO.	100% STATE TAZEWELL CO.
				SN 090-0134 0014	SN 090-0135 0014
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	0.5	0.5
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	12065	6025	6040
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1025	512.5	512.5
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	925	462.5	462.5
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	1	1
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	1	1
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	3033	1513	1520
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	644	321	323
X0327980	PAVEMENT MARKING REMOVAL - WATER BLAST	SO FT	1333	665	668
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	14	7	7
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	4022	2009	2013
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SO FT	255	107	148

12
* SPECIALTY ITEM

FILE NAME : 68065 Cover Sheet.dgn

USER NAME : h010b-
PLOT SCALE * 100.0000 ' / in.
PLOT DATE * 4/7/2017

DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
156	90-1107-11HBJR,B-R	TAZEWELL	22	5
			CONTRACT NO. 68065	
ILLINOIS FED. AIG PROJECT				

SHOULDER SCHEDULE													
LOCATION							WIDTH OF EXISTING SHOULDER RUMBLE STRIP REMOVAL (30")	LENGTH	AREA	44000157 HOT-MIX ASPHALT SURFACE REMOVAL, 2"	40800031 POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)	40603310 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	64200116 SHOULDER RUMBLE STRIPS, 16"
S.N.	NB/SB	I-155				LT/RT	FT	FT	SQ FT	SQ YD	POUND	TON	FT
STAGE I													
090-0135	NB	STA	342+37.50	TO	350+29.24	LT	2.5	791.7	1,979.3	219.9	158.3	24.6	791.7
090-0134	SB	STA	341+54.93	TO	349+39.60	RT	2.5	784.7	1,961.7	218.0	156.9	24.4	784.7
STAGE II													
090-0135	NB	STA	342+35.97	TO	349+64.07	RT	2.5	728.1	1,820.3	202.3	145.6	22.7	728.1
090-0134	SB	STA	342+11.46	TO	349+39.60	LT	2.5	728.1	1,820.4	202.3	145.6	22.7	728.1
S.N. 090-0134 SUBTOTALS									420	303	47	1,513	
S.N. 090-0135 SUBTOTALS									422	304	47	1,520	
TOTALS									842	607	94	3,033	

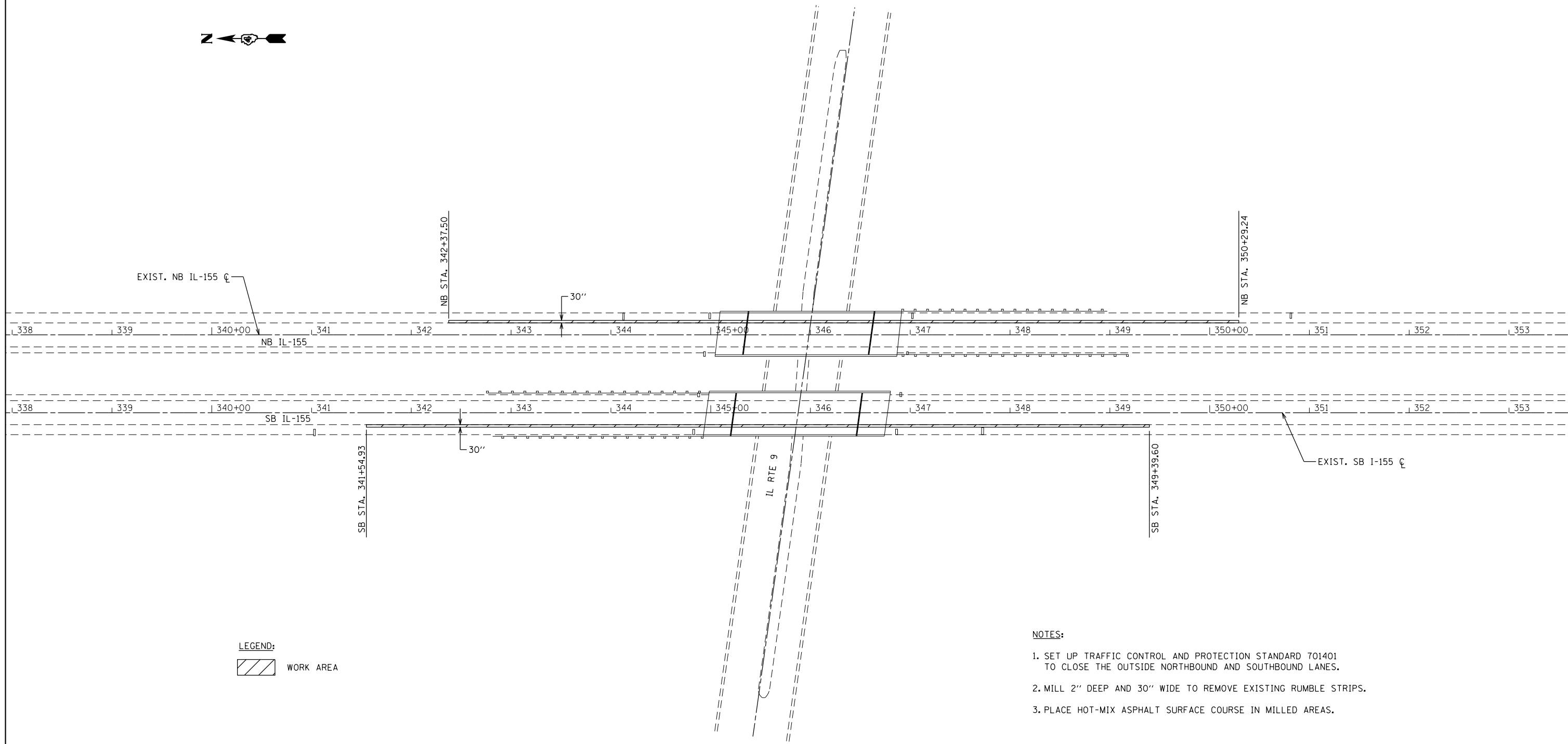
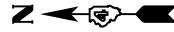
TEMPORARY CONCRETE BARRIER SCHEDULE									
LOCATION							70400100 TEMPORARY CONCRETE BARRIER	70400200 RELOCATE TEMPORARY CONCRETE BARRIER	
S.N.	NB/SB	I-155				LT/RT	FT	FT	
STAGE I									
090-0135	NB	STA	344+62.50	TO	349+80.75	LT	512.5		
090-0134	SB	STA	342+09.11	TO	347+14.60	RT	512.5		
STAGE II									
090-0135	NB	STA	344+60.97	TO	349+15.78	RT		462.5	
090-0134	SB	STA	342+59.82	TO	347+14.60	LT		462.5	
S.N. 090-0134 SUBTOTALS							513	463	
S.N. 090-0135 SUBTOTALS							513	463	
TOTALS							1,025	925	


TEMPORARY IMPACT ATTENUATOR SCHEDULE									
LOCATION					70600250 IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	70600350 IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3			
S.N.	NB/SB	I-155		LT/RT	EACH	EACH			
STAGE I									
090-0135	NB	STA	349+87.00	RT	1				
090-0134	SB	STA	342+02.00	LT	1				
STAGE II									
090-0135	NB	STA	349+22.00	LT		1			
090-0134	SB	STA	342+54.00	RT		1			
S.N. 090-0134 SUBTOTALS					1	1			
S.N. 090-0135 SUBTOTALS					1	1			
TOTALS					2	2			

TEMPORARY PAVEMENT MARKING SCHEDULE										
LOCATION								X0327980 PAVEMENT MARKING REMOVAL - WATER BLASTING	70300904 PAVEMENT MARKING TAPE, TYPE IV 4"	X7030005 TEMPORARY PAVEMENT MARKING REMOVAL
S.N.	NB/SB	I-155				EDGE/CL	LT/RT	SQ FT	FT	SQ FT
STAGE I										
090-0135	NB	STA	342+35.97	TO	355+29.07	CL		161.6		
090-0135	NB	STA	342+37.50	TO	350+29.24	EDGE	LT	263.9	791.7	263.9
090-0135	NB	STA	342+37.50	TO	365+29.07	CL	LT		2291.6	763.9
090-0134	SB	STA	336+55.10	TO	349+39.60	CL		160.6		
090-0134	SB	STA	341+54.93	TO	349+39.60	EDGE	RT	261.6	784.7	261.6
090-0134	SB	STA	326+55.10	TO	349+39.60	CL	RT		2284.5	761.5
STAGE II										
090-0135	NB	STA	342+35.97	TO	349+64.07	EDGE	RT	242.7	728.1	242.7
090-0135	NB	STA	342+35.97	TO	364+64.07	CL	RT		2228.1	742.7
090-0134	SB	STA	342+11.46	TO	349+39.60	EDGE	LT	242.7	728.1	242.7
090-0134	SB	STA	327+11.46	TO	349+39.60	CL	LT		2228.1	742.7
S.N. 090-0134 SUBTOTALS								665	6025	2008
S.N. 090-0135 SUBTOTALS								668	6040	2013
TOTALS								1,333	12,065	4,022

PAVEMENT MARKING SCHEDULE										
LOCATION								78009004 MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	78009006 MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	
S.N.	NB/SB	I-155				EDGE/CL	LT/RT	(WHITE) FT	(YELLOW) FT	(WHITE) FT
090-0135	NB	STA	342+35.97	TO	355+29.07	CL				323.3
090-0135	NB	STA	342+37.50	TO	350+29.24	EDGE	LT		791.7	
090-0135	NB	STA	342+35.97	TO	349+64.07	EDGE	RT	728.1		
090-0134	SB	STA	336+55.10	TO	349+39.60	CL				321.1
090-0134	SB	STA	342+11.46	TO	349+39.60	EDGE	LT		728.1	
090-0134	SB	STA	341+54.93	TO	349+39.60	EDGE	RT	784.7		
S.N. 090-0134 SUBTOTALS								785	728	321
S.N. 090-0135 SUBTOTALS								728	792	323
SUBTOTAL								1513	1520	
TOTALS								3033		644

X7015005 CHANGEABLE MESSAGE SIGN	
LOCATION	CAL DA
SB I-155 (S.N. 090-0134, 1 SIGN FOR 7	7
NB I-155 (S.N. 090-0135, 1 SIGN FOR 7	7
TOTAL	14



LEGEND:
 WORK AREA

NOTES:

1. SET UP TRAFFIC CONTROL AND PROTECTION STANDARD 701401 TO CLOSE THE OUTSIDE NORTHBOUND AND SOUTHBOUND LANES.
2. MILL 2" DEEP AND 30" WIDE TO REMOVE EXISTING RUMBLE STRIPS.
3. PLACE HOT-MIX ASPHALT SURFACE COURSE IN MILLED AREAS.

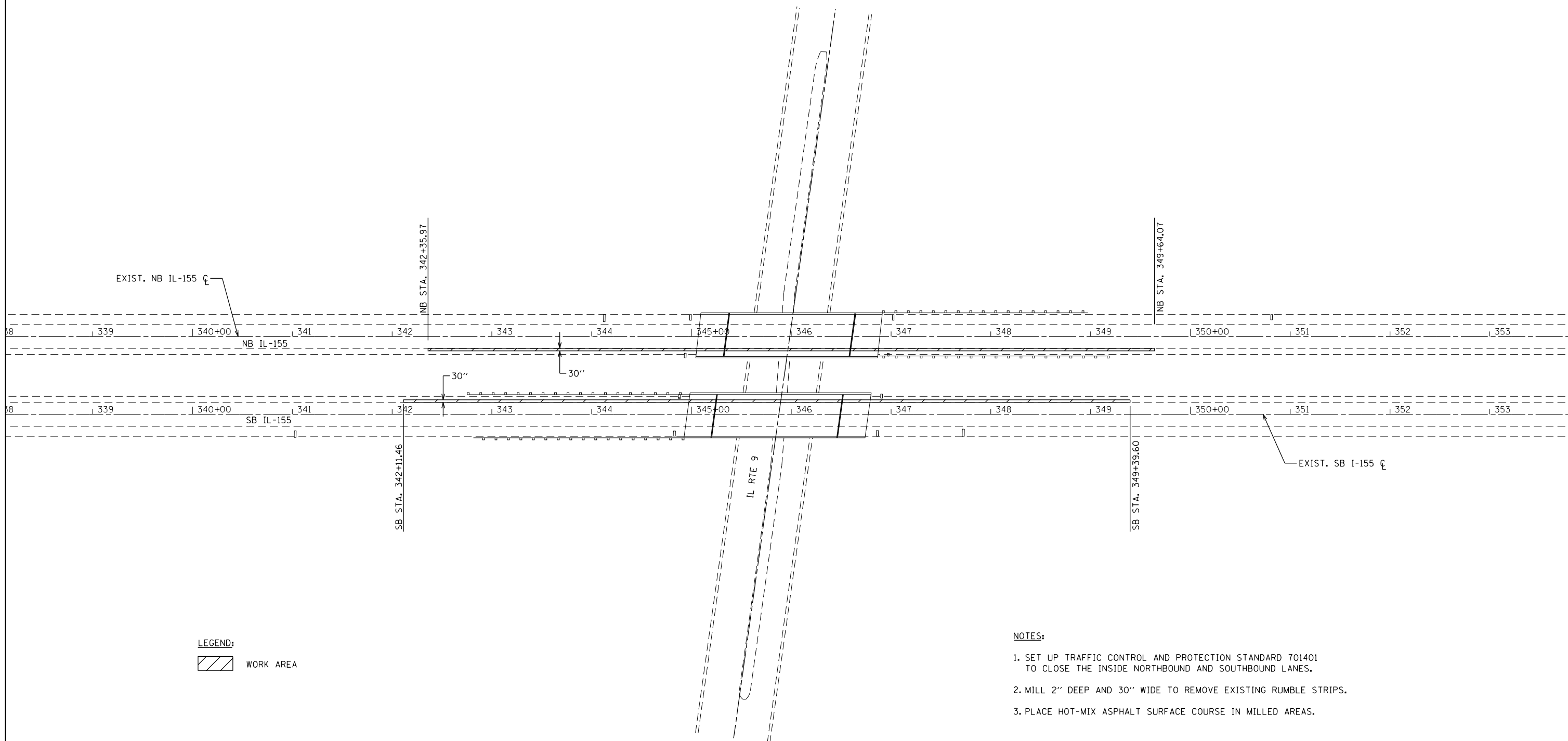
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	PLOT DATE = 4/7/2017	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**STAGING DETAILS
 PRE-STAGE A**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-((107-1)HB)BJR,B-R	TAZEWELL	22	8
CONTRACT NO. 68D65			ILLINOIS FED. AID PROJECT	



LEGEND:
 WORK AREA

NOTES:

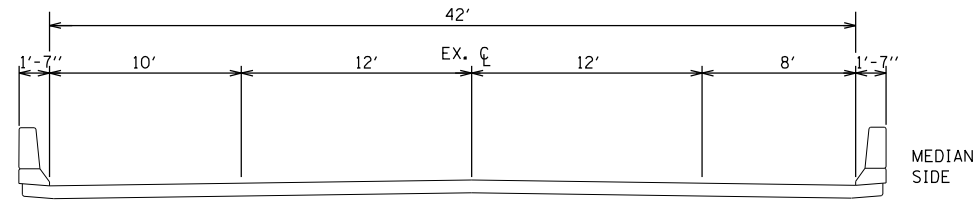
1. SET UP TRAFFIC CONTROL AND PROTECTION STANDARD 701401 TO CLOSE THE INSIDE NORTHBOUND AND SOUTHBOUND LANES.
2. MILL 2" DEEP AND 30" WIDE TO REMOVE EXISTING RUMBLE STRIPS.
3. PLACE HOT-MIX ASPHALT SURFACE COURSE IN MILLED AREAS.

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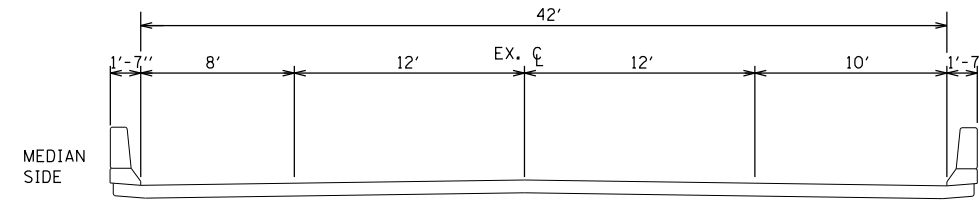
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING DETAILS PRE-STAGE B			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

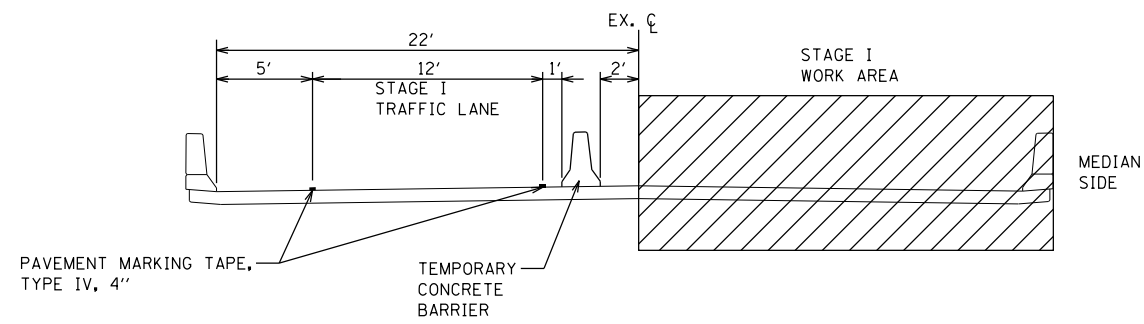
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 68D65				
ILLINOIS FED. AID PROJECT				



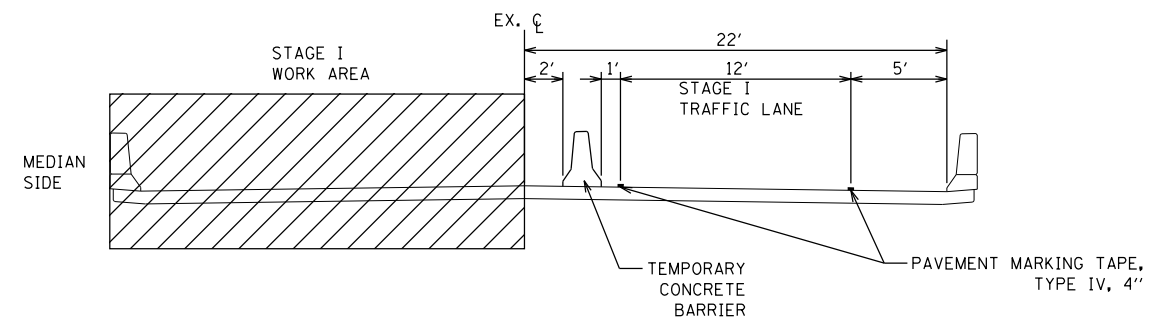
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(S.N. 090-135)



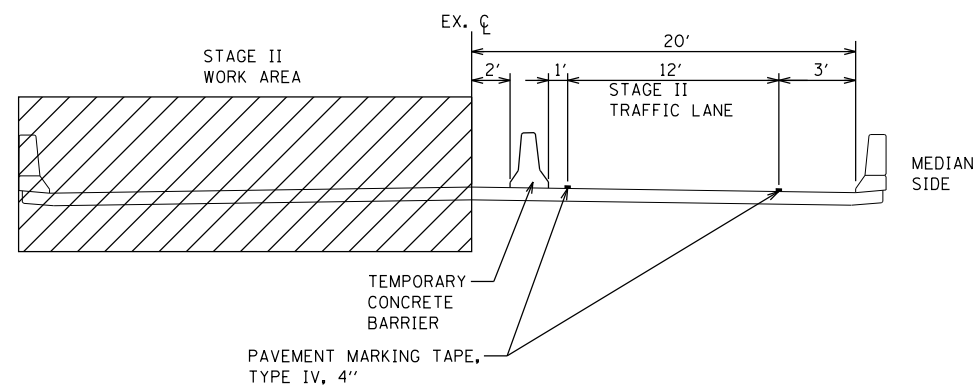
EXISTING SB IL-155
(S.N. 090-134)



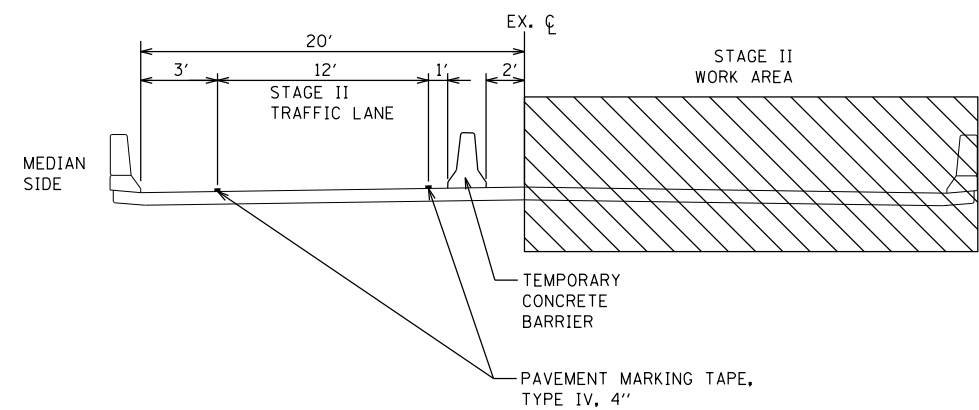
STAGE I TYPICAL SECTION
NB IL-155
(S.N. 090-135)



STAGE I TYPICAL SECTION
SB IL-155
(S.N. 090-134)



STAGE II TYPICAL SECTION
NB IL-155
(S.N. 090-135)



STAGE II TYPICAL SECTION
SB IL-155
(S.N. 090-134)

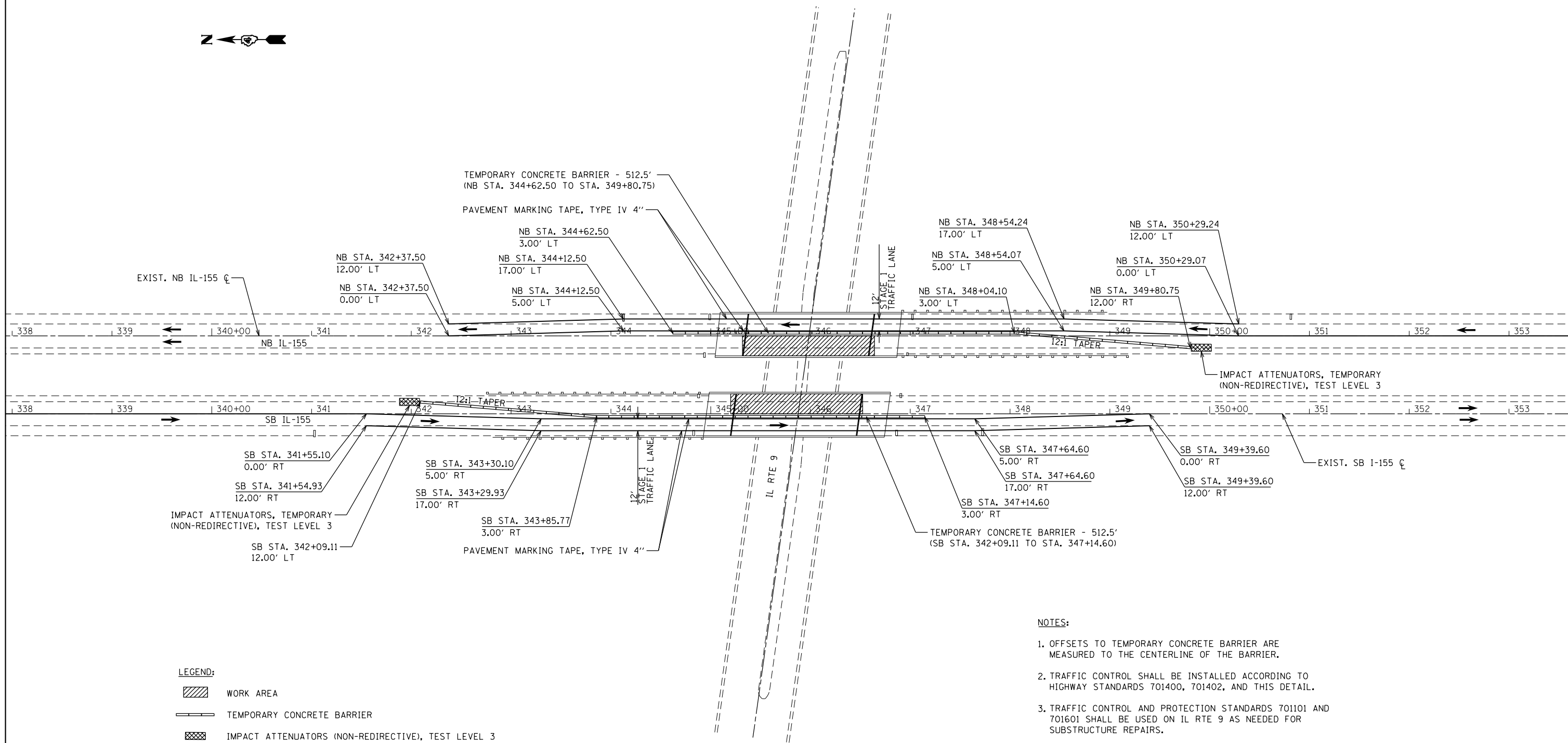
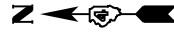
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TYPICAL SECTIONS -	PLOT DATE = 4/7/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGING DETAILS
STAGE I AND II TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-[(107-1)HBJBJR,B-R	TAZEWELL	22	10
CONTRACT NO. 68D65			ILLINOIS FED. AID PROJECT	



NOTES:

1. OFFSETS TO TEMPORARY CONCRETE BARRIER ARE MEASURED TO THE CENTERLINE OF THE BARRIER.
2. TRAFFIC CONTROL SHALL BE INSTALLED ACCORDING TO HIGHWAY STANDARDS 701400, 701402, AND THIS DETAIL.
3. TRAFFIC CONTROL AND PROTECTION STANDARDS 701101 AND 701601 SHALL BE USED ON IL RTE 9 AS NEEDED FOR SUBSTRUCTURE REPAIRS.
4. TEMPORARY CONCRETE BARRIER SHALL NOT BE PLACED DIRECTLY ON THE EXISTING RAISED REFLECTIVE PAVEMENT MARKERS. USE A BLACKOUT TAPE APPROVED BY THE ENGINEER TO COVER ANY CONFLICTING RAISED REFLECTIVE PAVEMENT MARKERS. USE A MATERIAL APPROVED BY THE ENGINEER TO COVER THE LENSES PRIOR TO PLACEMENT OF THE BLACKOUT TAPE IN ORDER TO PROTECT THE LENSES FROM THE BLACKOUT TAPE ADHESIVE. ALL COSTS ASSOCIATED WITH PROVIDING, INSTALLING, AND REMOVING THESE ITEMS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL PAY ITEMS PROVIDED IN THE PLANS.

LEGEND:

- WORK AREA
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3

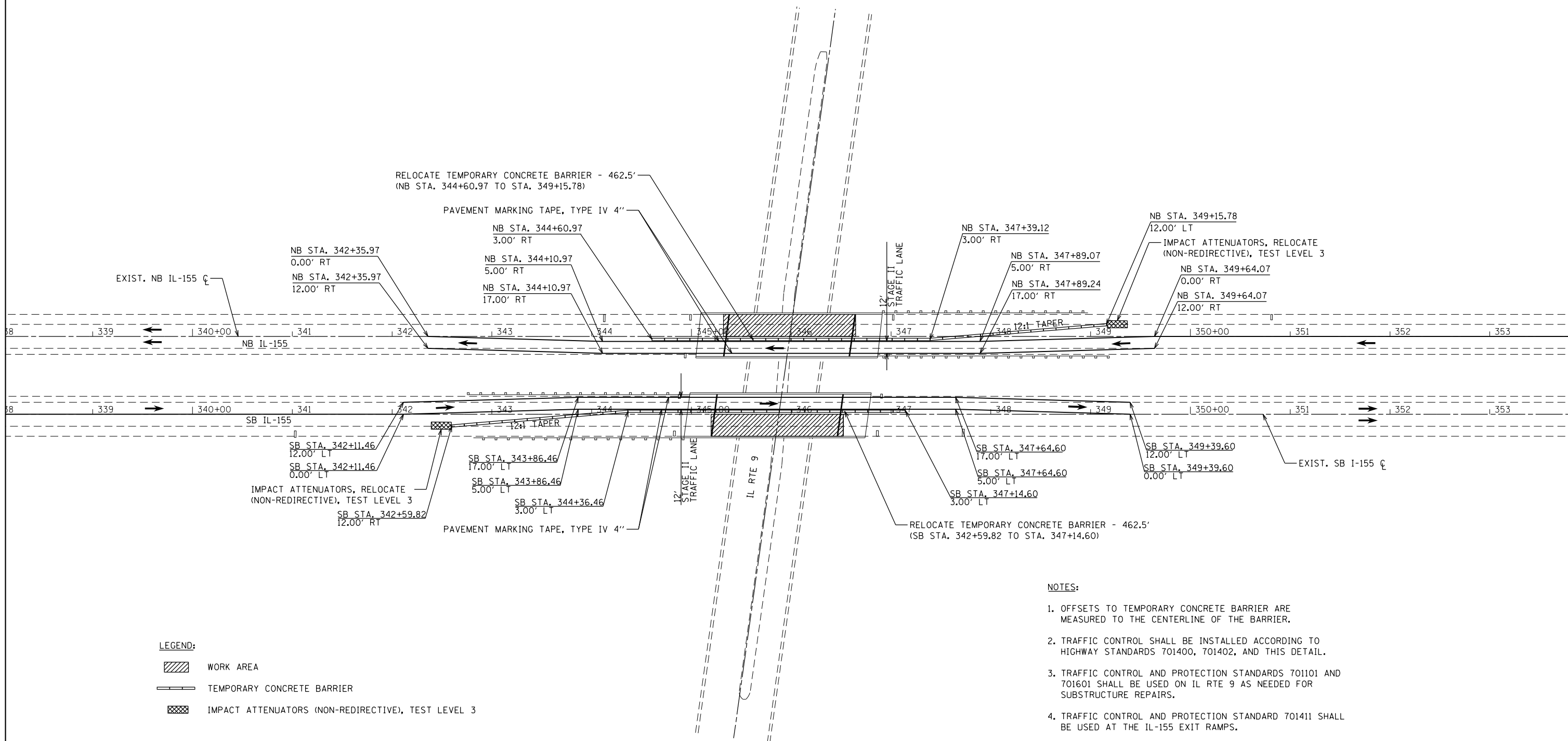
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	PLOT DATE = 4/7/2017	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STAGING DETAILS
STAGE I**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-[(107-1)HB]BJR,B-R	TAZEWELL	22	11
CONTRACT NO. 68D65				
ILLINOIS FED. AID PROJECT				



LEGEND:

- WORK AREA
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3

NOTES:

1. OFFSETS TO TEMPORARY CONCRETE BARRIER ARE MEASURED TO THE CENTERLINE OF THE BARRIER.
2. TRAFFIC CONTROL SHALL BE INSTALLED ACCORDING TO HIGHWAY STANDARDS 701400, 701402, AND THIS DETAIL.
3. TRAFFIC CONTROL AND PROTECTION STANDARDS 701101 AND 701601 SHALL BE USED ON IL RTE 9 AS NEEDED FOR SUBSTRUCTURE REPAIRS.
4. TRAFFIC CONTROL AND PROTECTION STANDARD 701411 SHALL BE USED AT THE IL-155 EXIT RAMP.
5. TEMPORARY CONCRETE BARRIER SHALL NOT BE PLACED DIRECTLY ON THE EXISTING RAISED REFLECTIVE PAVEMENT MARKERS. USE A BLACKOUT TAPE APPROVED BY THE ENGINEER TO COVER ANY CONFLICTING RAISED REFLECTIVE PAVEMENT MARKERS. USE A MATERIAL APPROVED BY THE ENGINEER TO COVER THE LENSES PRIOR TO PLACEMENT OF THE BLACKOUT TAPE IN ORDER TO PROTECT THE LENSES FROM THE BLACKOUT TAPE ADHESIVE. ALL COSTS ASSOCIATED WITH PROVIDING, INSTALLING, AND REMOVING THESE ITEMS SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL PAY ITEMS PROVIDED IN THE PLANS.

FILE NAME = Plan.dgn	USER NAME = keathbr	DESIGNED -	REVISED -
Stage 2	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
	PLOT DATE = 4/7/2017	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

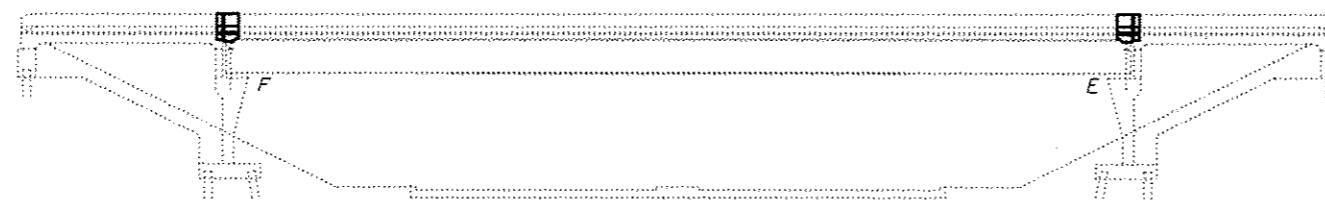
**STAGING DETAILS
STAGE II**

SCALE: SHEET OF SHEETS STA. TO STA.

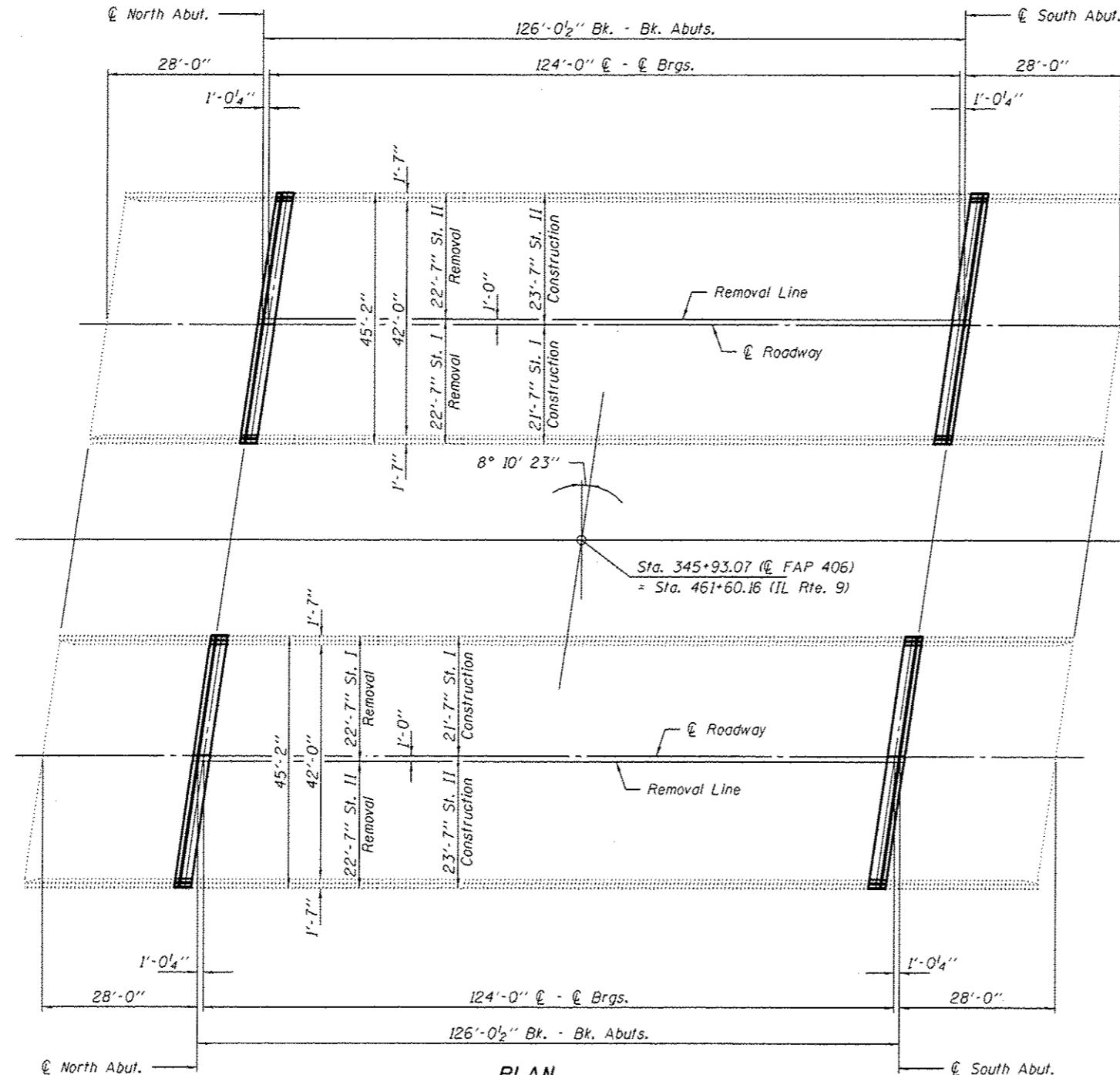
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-[(107-1)HBJBJR,B-R	TAZEWELL	22	12
CONTRACT NO. 68D65				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
 Reinforcement bars designated (E) shall be epoxy coated.
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
 Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.
 The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructure.



ELEVATION



PLAN

Remove & replace expansion joints at all abutments with Joint Strip Seal.

Concrete repair on abutments.

S.N. 090-0135 (NB)

S.N. 090-0134 (SB)

Sta. 345+93.07 (@ FAP 406)
 = Sta. 461+60.16 (IL Rte. 9)



EXPIRES 11-30-2018

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	17.6
Concrete Superstructure	Cu. Yd.	17.6
Reinforcement Bars, Epoxy Coated	Pound	2320
Bar Splicers	Each	40
Preformed Joint Strip Seal	Foot	179
Protective Coat	Sq. Yd.	60
Structural Repair of Concrete, Depth ≤ 5"	Sq. Ft.	255

* To be applied to new concrete areas only

DESIGNED VHV *Victor H. Valis*
 CHECKED DAB *DAB*
 DRAWN daburdell
 CHECKED VHV DAB VHV *VHV*

PASSED

David Carl Puzey
 ENGINEER OF BRIDGES AND STRUCTURES

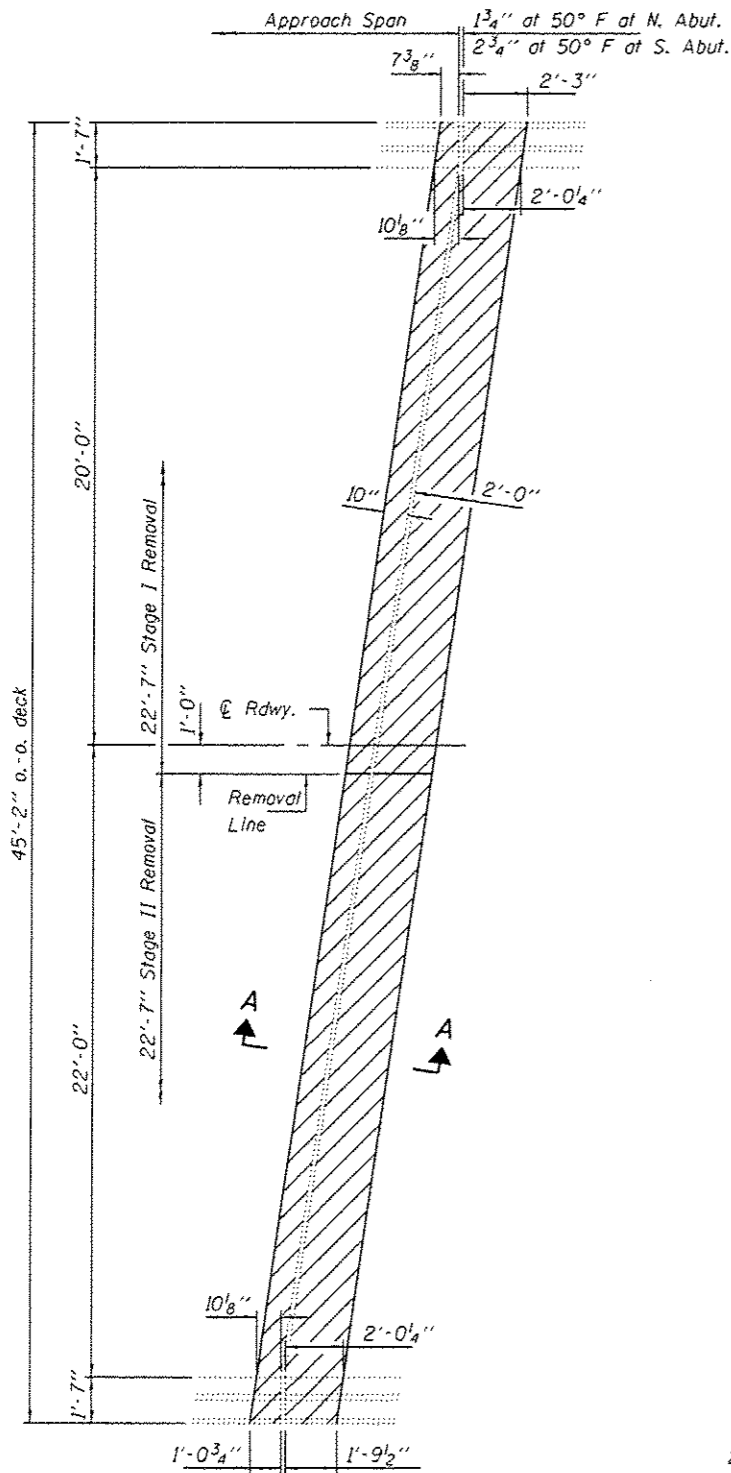
DATE MAY 5, 2017

REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PLAN AND ELEVATION
 FAI 155 OVER IL 9
 SN 090-0134 (SB) & 0135 (NB)
 SHEET NO. 1 OF 1 SHEETS

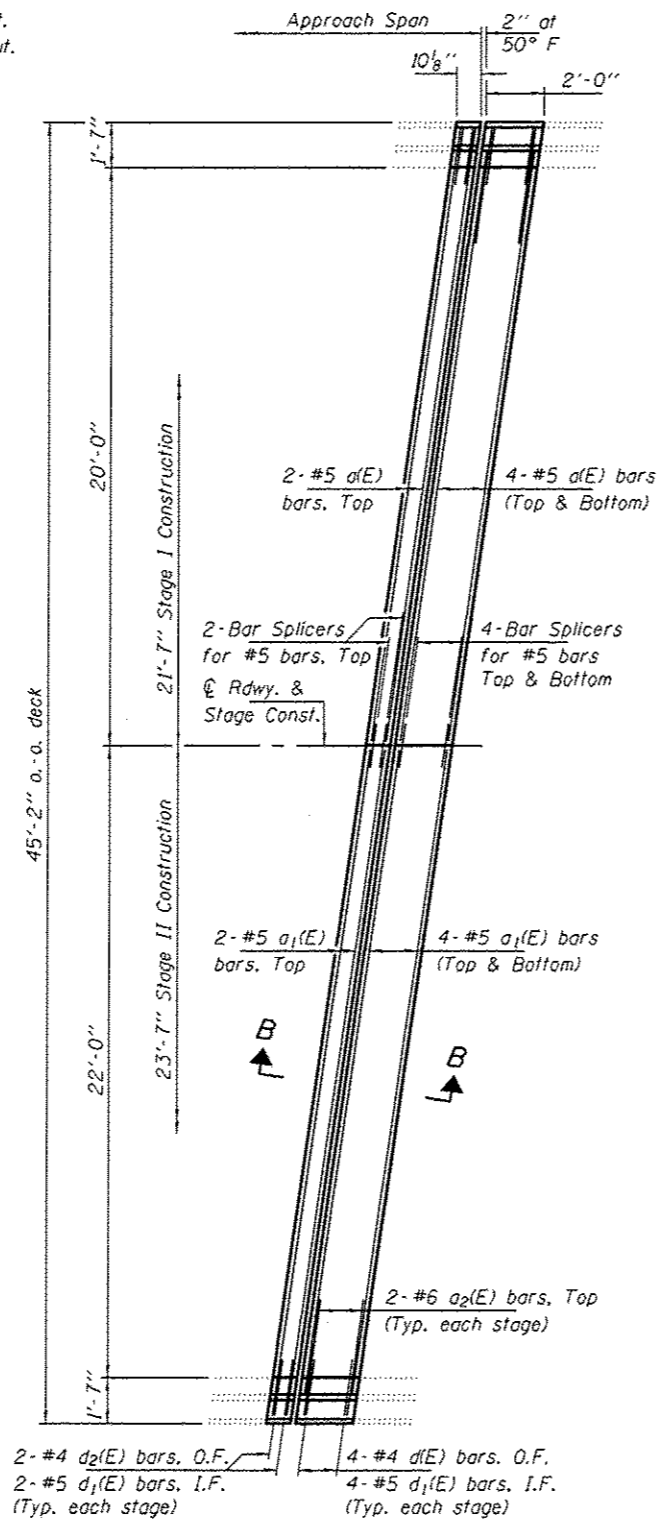
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-0107-14B03JULB-R	TAZEWELL	22	13
CONTRACT NO. 68065				ILLINOIS FED. AID PROJECT



CONCRETE REMOVAL DETAILS

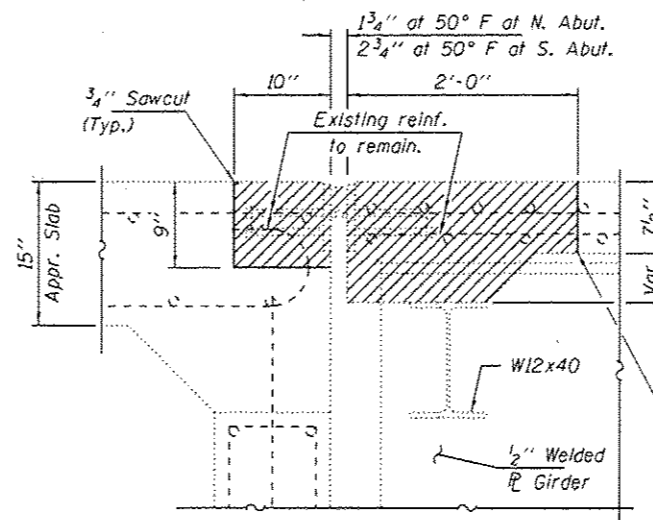
North Abutment shown,
 South Abutment similar.

Note:
 Hatched areas indicate
 Concrete Removal.

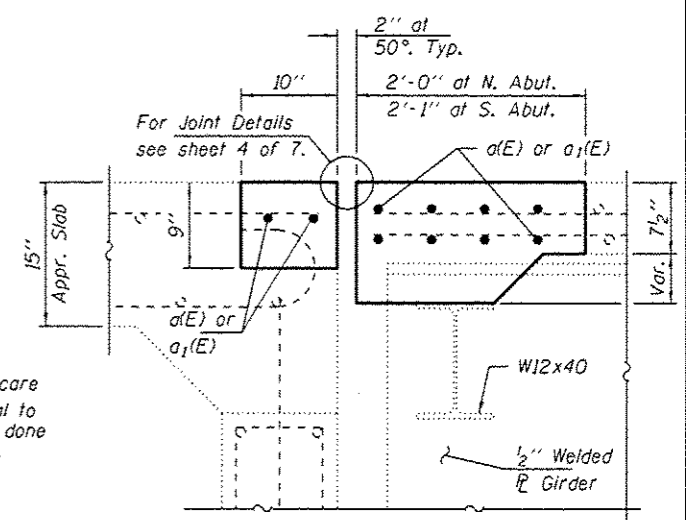


CONCRETE REPLACEMENT DETAILS

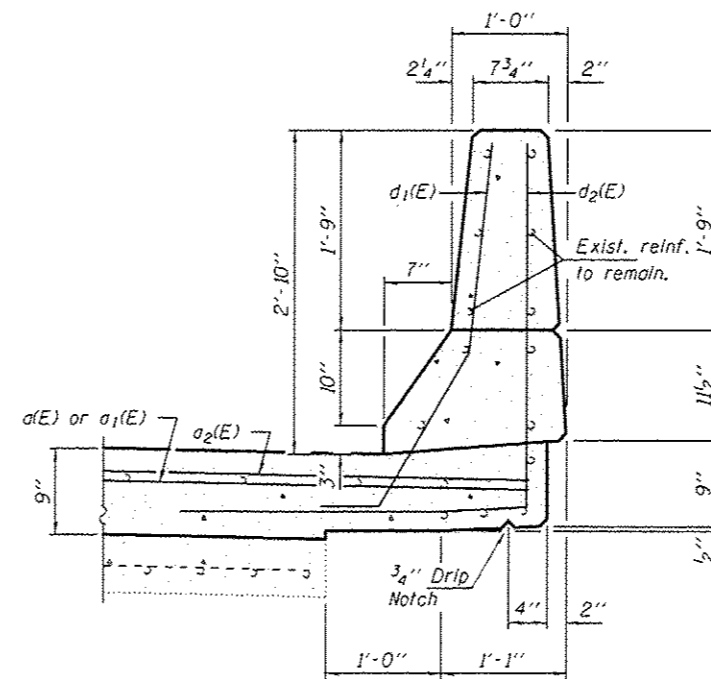
North Abutment shown,
 South Abutment similar.



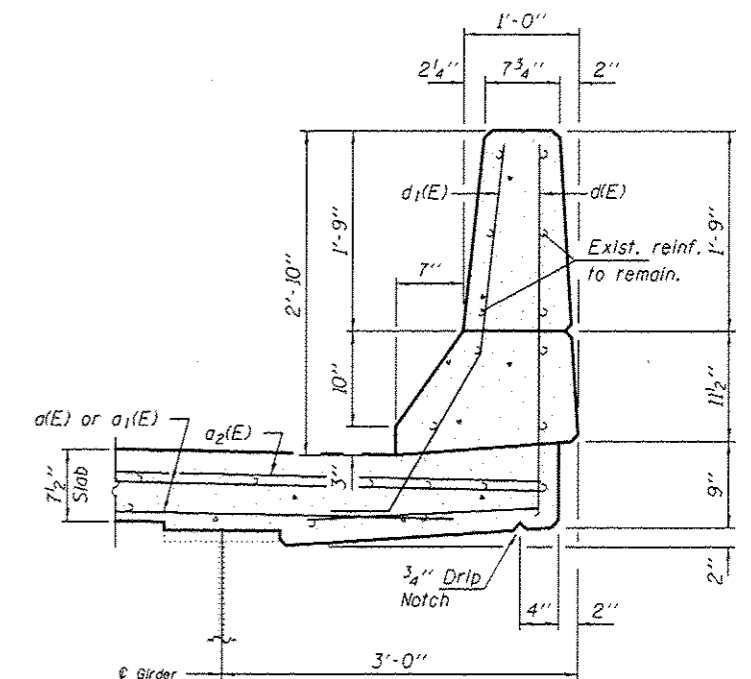
SECTION A-A



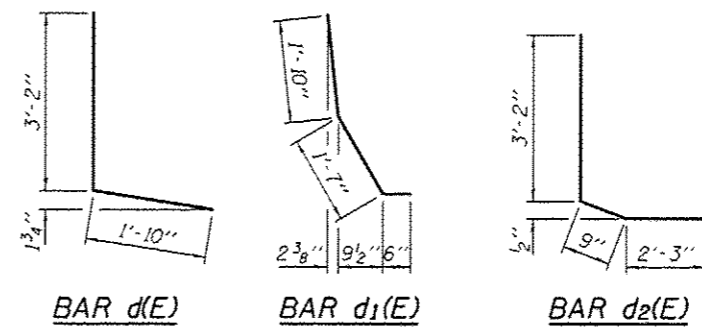
SECTION B-B



TYPICAL SECTION THRU APPROACH PARAPET



TYPICAL SECTION THRU BRIDGE PARAPET



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	20	#5	21'-3"	—
a ₁ (E)	20	#5	23'-3"	—
a ₂ (E)	8	#6	4'-0"	—
d(E)	16	#4	5'-0"	L
d ₁ (E)	24	#5	3'-11"	L
d ₂ (E)	8	#4	6'-2"	L
Concrete Removal			Cu. Yd.	8.8
Concrete Superstructure			Cu. Yd.	8.8
Bar Splicers			Each	20
Reinforcement Bars, Epoxy Coated			Pound	1160

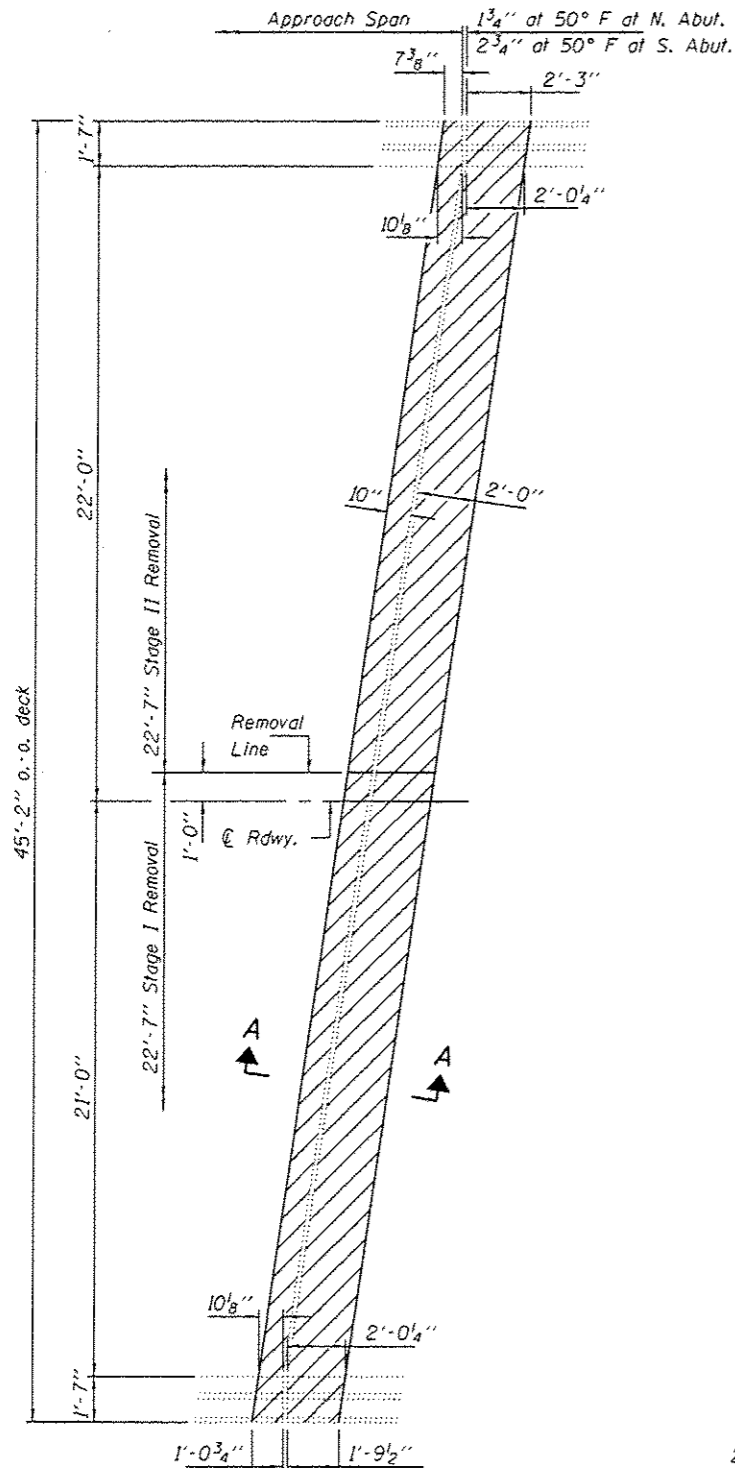
DESIGNED VHV
 CHECKED DAB
 DRAWN daburdell
 CHECKED VHV DAB

DATE MAY 5, 2017
 PASSED
 ENGINEER OF BRIDGES AND STRUCTURES

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

JOINT REMOVAL AND REPLACEMENT DETAILS
 SN 090-0134 (SB)

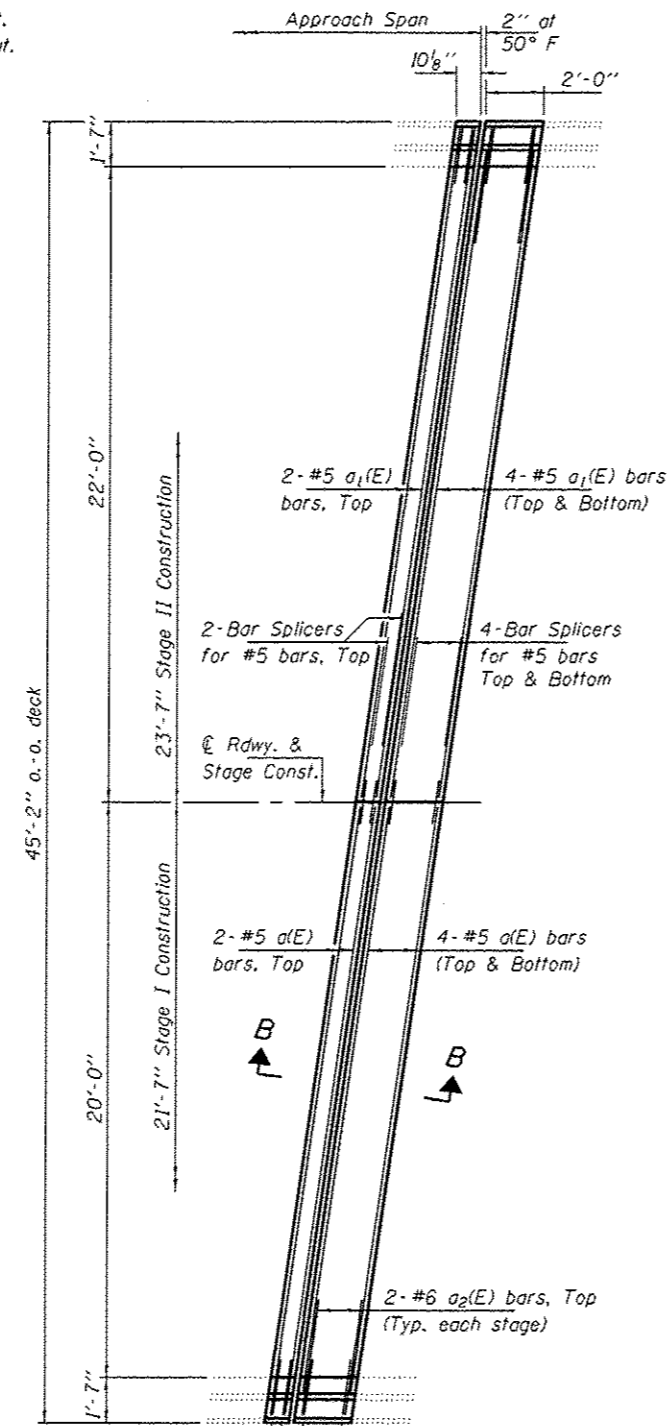
F.A.I. SECTION COUNTY TOTAL SHEETS SHEET NO.
 155 90-0107-11463B-14-R TAZEWELL 22 14
 CONTRACT NO. 68065
 SHEET NO. 2 OF 7 SHEETS
 [ILLINOIS] FED. AID PROJECT



CONCRETE REMOVAL DETAILS

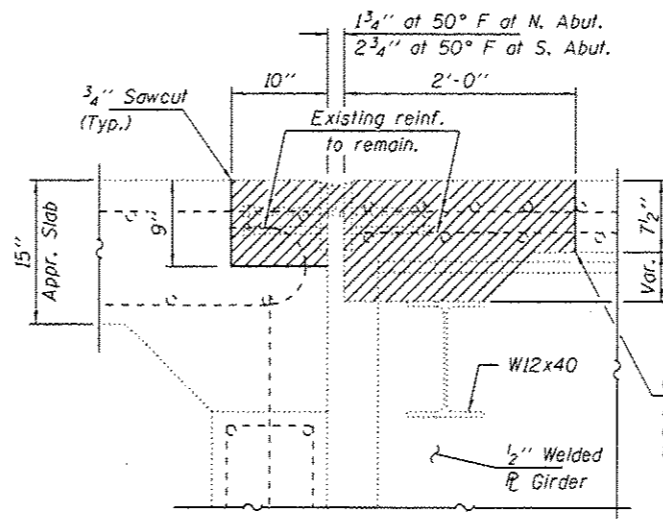
North Abutment shown,
South Abutment similar.

Note:
Hatched areas indicate
Concrete Removal.

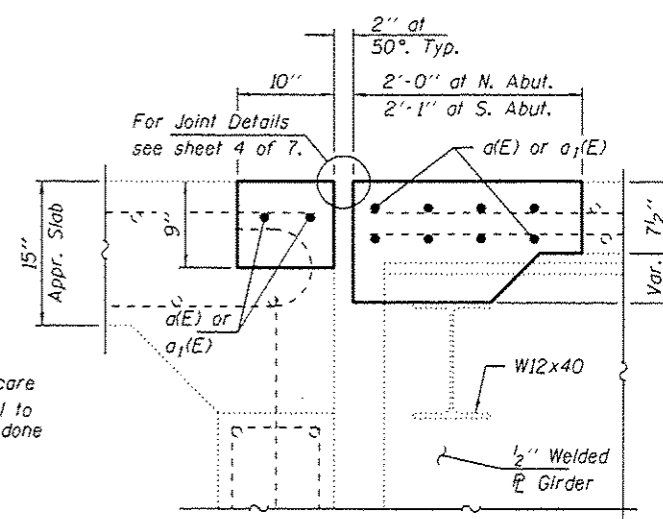


CONCRETE REPLACEMENT DETAILS

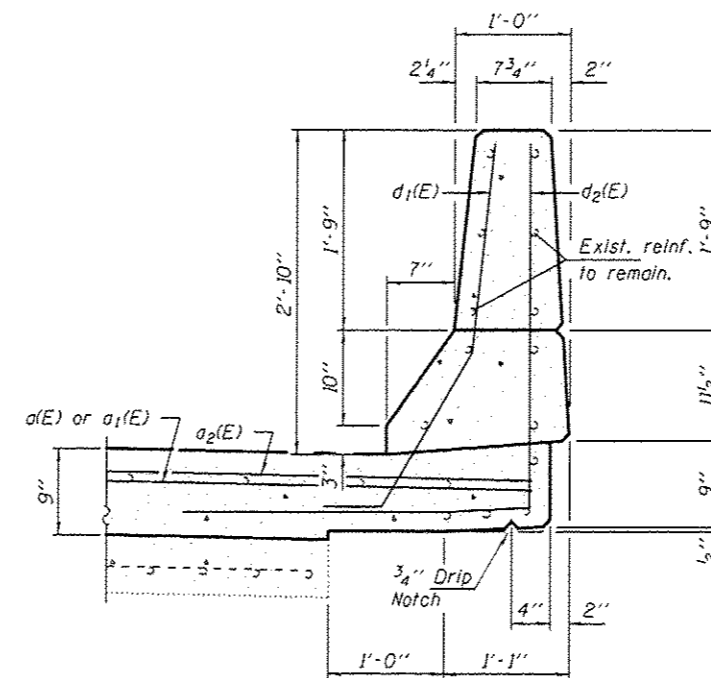
North Abutment shown,
South Abutment similar.



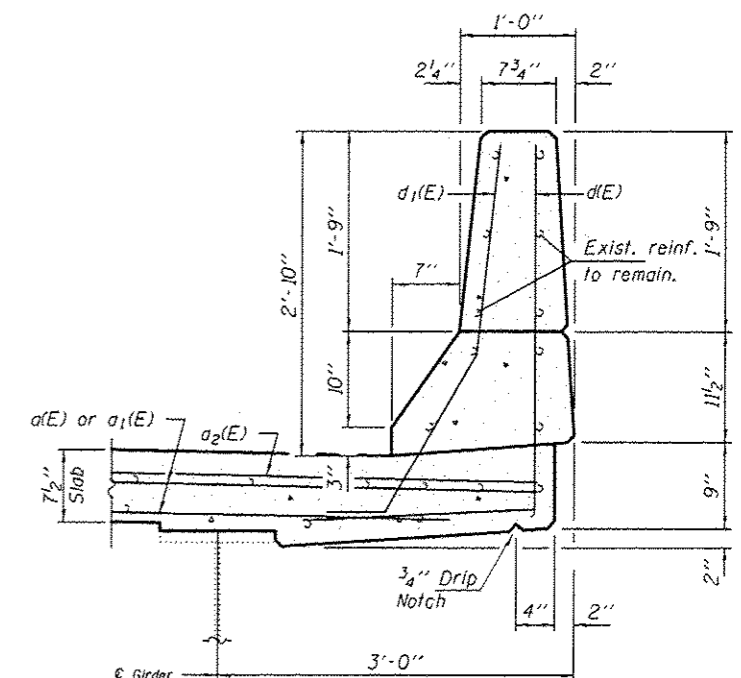
SECTION A-A



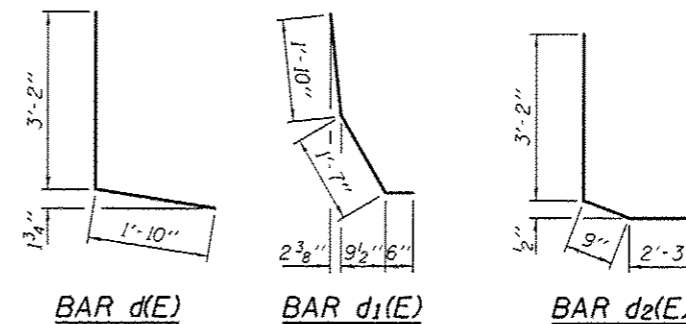
SECTION B-B



**TYPICAL SECTION THRU
APPROACH PARAPET**



**TYPICAL SECTION THRU
BRIDGE PARAPET**



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	20	#5	21'-3"	—
a1(E)	20	#5	23'-3"	—
a2(E)	8	#6	4'-0"	—
d(E)	16	#4	5'-0"	L
d1(E)	24	#5	3'-11"	L
d2(E)	8	#4	6'-2"	L
Concrete Removal			Cu. Yd.	8.8
Concrete Superstructure			Cu. Yd.	8.8
Bar Splicers			Each	20
Reinforcement Bars, Epoxy Coated			Pound	1160

DESIGNED VHV
CHECKED DAB
DRAWN daburdell
CHECKED VHV DAB

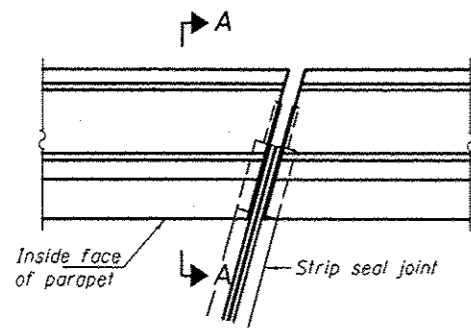
DATE MAY 5, 2017
PASSED
ENGINEER OF BRIDGES AND STRUCTURES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

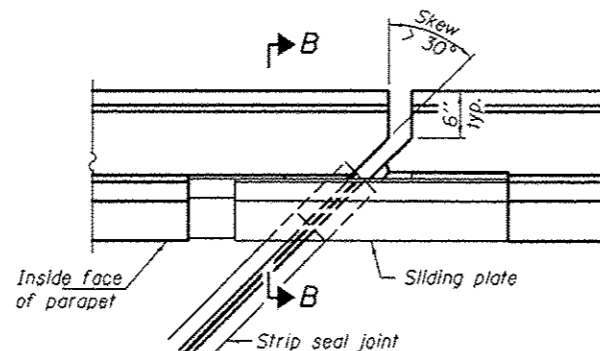
JOINT REMOVAL AND REPLACEMENT DETAILS
SN 090-0135 (NB)

SHEET NO. 3 OF 7 SHEETS

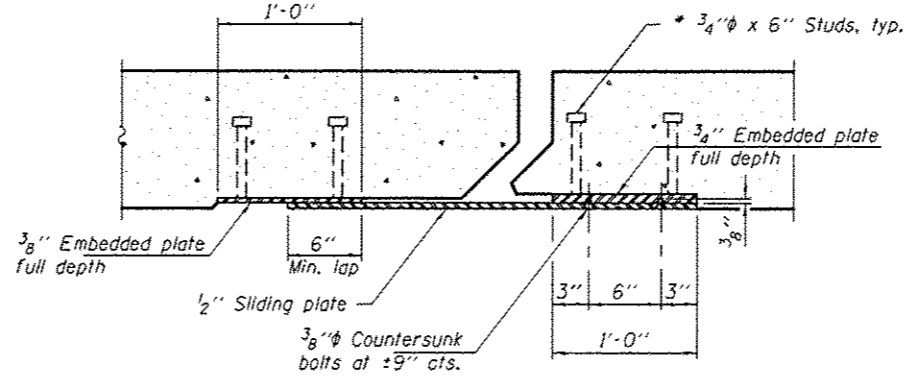
F.A.I. RTE. 155
SECTION 90-(107-1)HB3B,JB,R
COUNTY TAZEWELL
TOTAL SHEETS 22
SHEET NO. 15
CONTRACT NO. 68D65
ILLINOIS FED. AID PROJECT



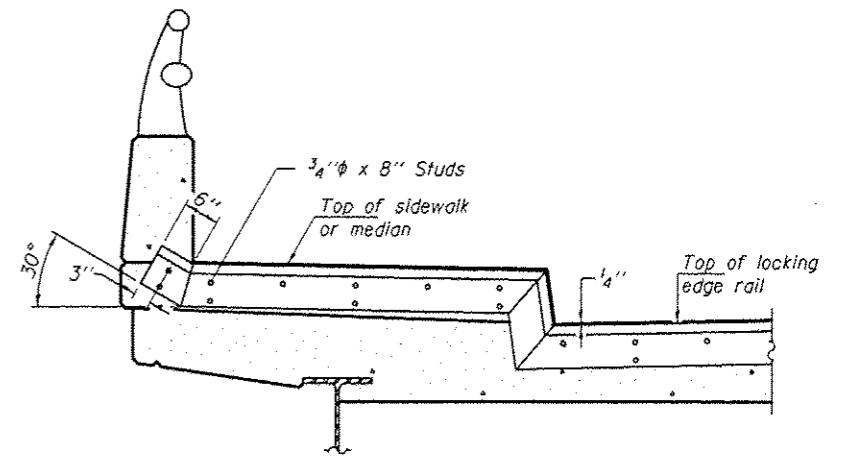
PLAN
(For skews $\leq 30^\circ$)



PLAN
(For skews $> 30^\circ$)
Showing point block

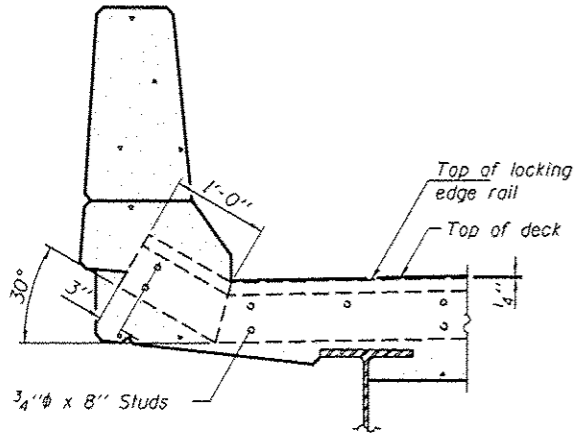


SECTION C-C

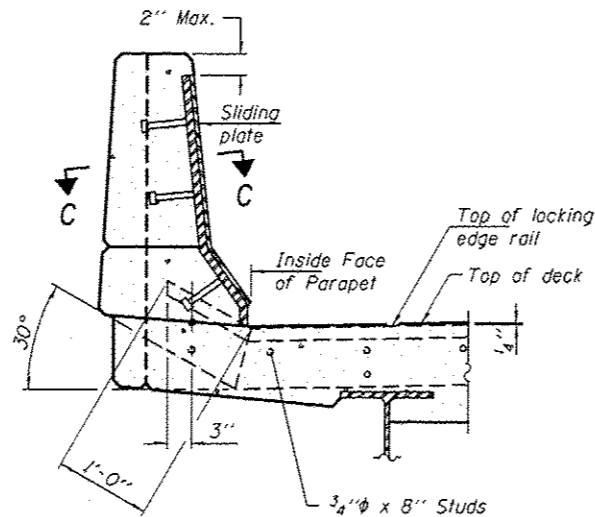


**TYPICAL END TREATMENT
AT SIDEWALK OR MEDIAN**

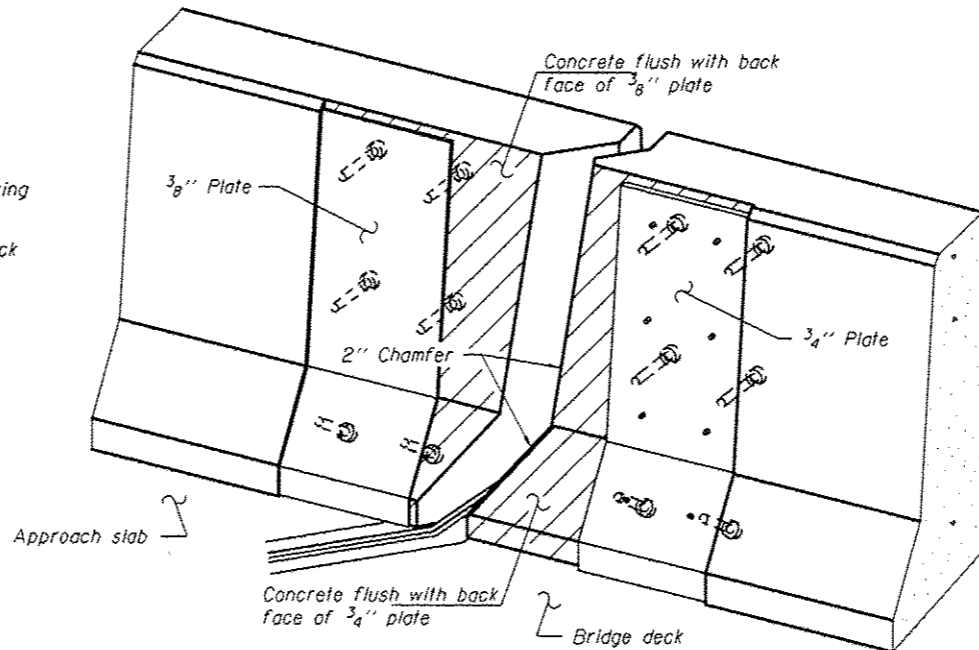
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



SECTION A-A



SECTION B-B



TRIMETRIC VIEW
(Showing back plates only)

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

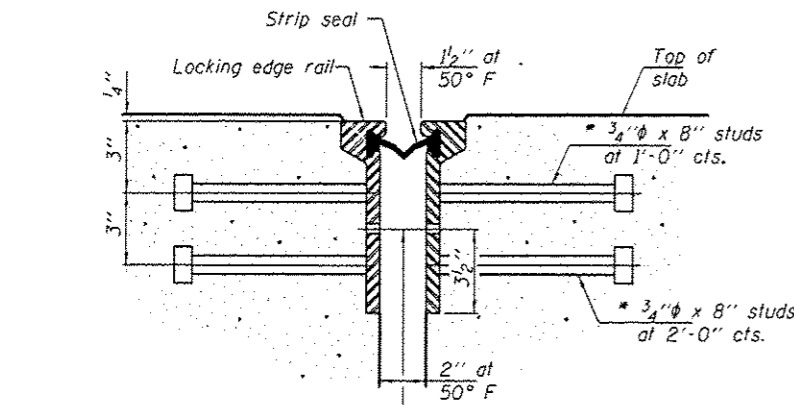
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

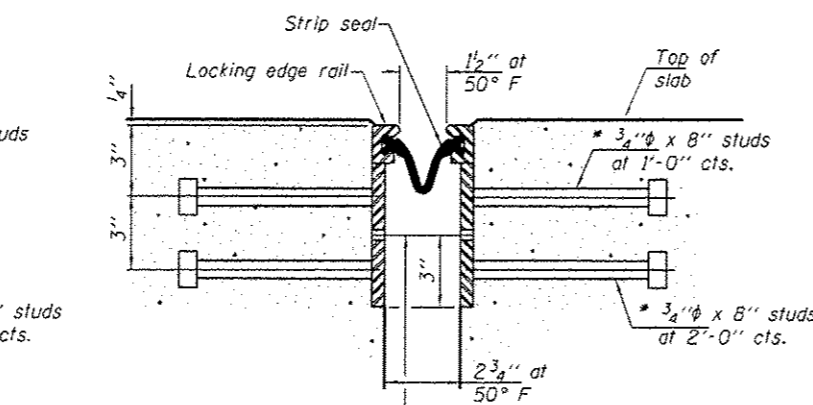
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

Parapet plates and anchorage studs for skews $> 30^\circ$ included in the cost of Preformed Joint Strip Seal.



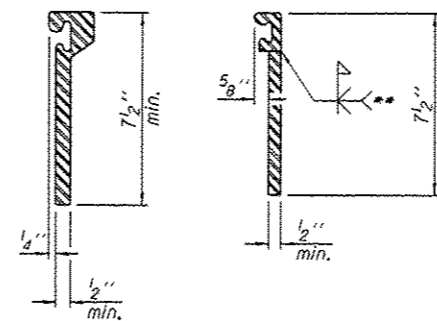
**SECTION THRU
ROLLED RAIL JOINT**

7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

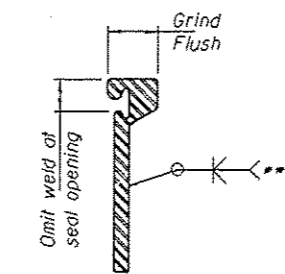


**SECTION THRU
WELDED RAIL JOINT**

7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.



**ROLLED
EXTRUDED RAIL WELDED RAIL**



** Back gouge not required if complete joint penetration is verified by mock-up.

The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	179

EJ-SSJ

11-22-2016

DESIGNED VHV
CHECKED DAB
DRAWN daburdell
CHECKED VHV DAB

PASSED

Carl Perry
ENGINEER OF BRIDGES AND STRUCTURES

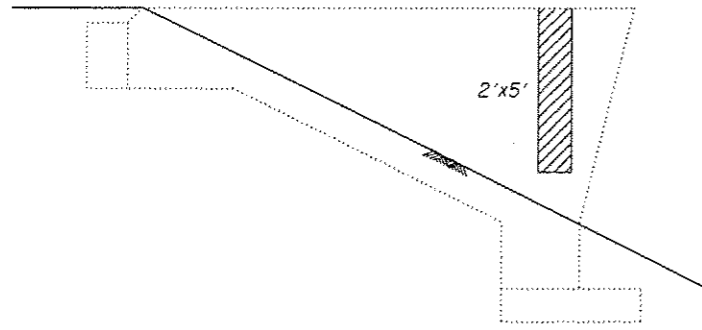
DATE MAY 5, 2017
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

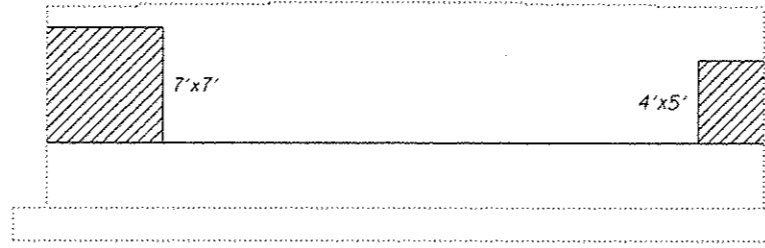
PREFORMED JOINT STRIP SEAL
SN 090-0134 (SB) & 0135 (NB)

SHEET NO. 4 OF 1 SHEETS

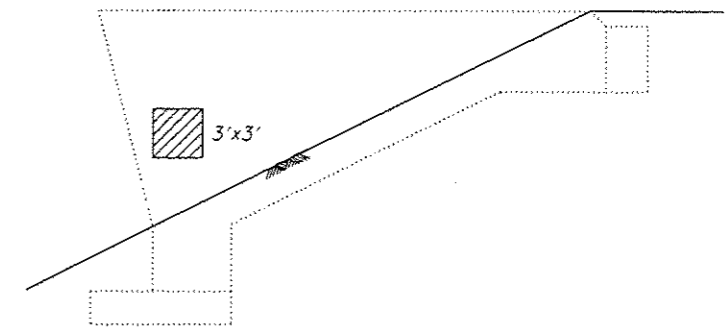
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-1107-1WB3B,R,B-R	TAZEWELL	22	16
CONTRACT NO. 68D65			ILLINOIS FED. AID PROJECT	



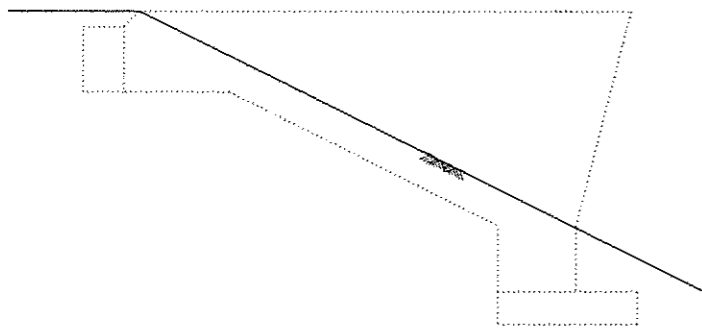
SIDE ELEVATION
(Looking West)



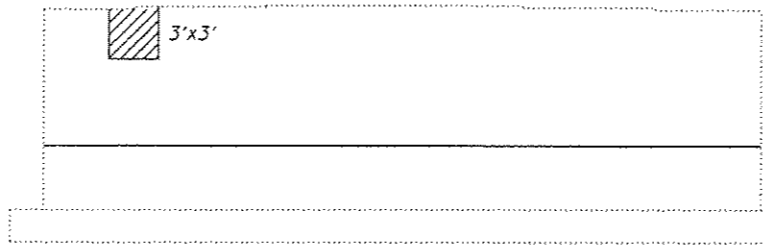
ELEVATION - SOUTH ABUTMENT
(Looking South)



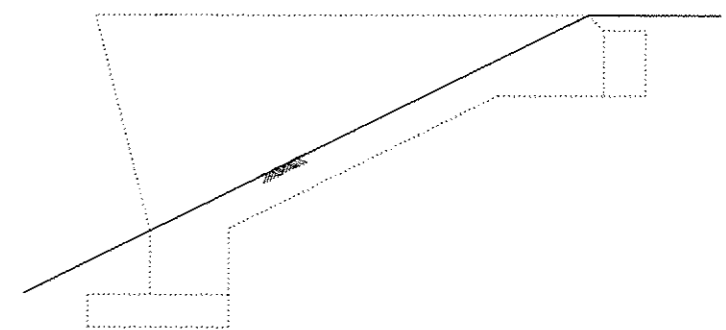
SIDE ELEVATION
(Looking East)




SIDE ELEVATION
(Looking East)



ELEVATION - NORTH ABUTMENT
(Looking North)




SIDE ELEVATION
(Looking West)

 Structural Repair of Concrete, Depth ≤ 5"

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete, Depth ≤ 5"	Sq. Ft.	107

DESIGNED *VHV*
 CHECKED *DAB*
 DRAWN *daburdell*
 CHECKED *VHV DAB*

PASSED

 ENGINEER OF BRIDGES AND STRUCTURES

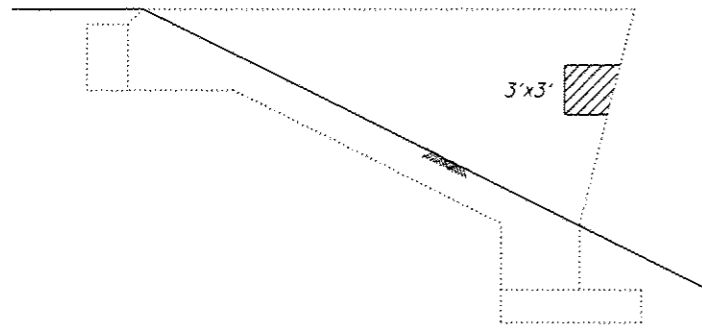
DATE MAY 5, 2017
 REVISED
 REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

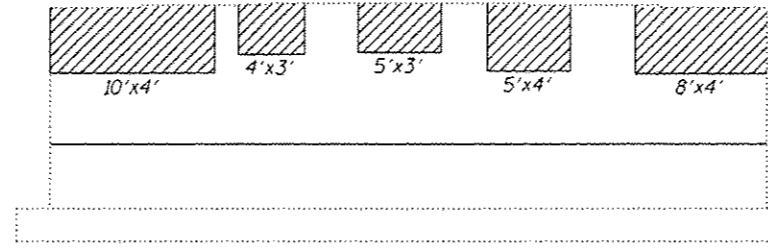
NORTH AND SOUTH ABUTMENT REPAIR DETAILS
SN 090-0134 (SB)

SHEET NO. 5 OF 7 SHEETS

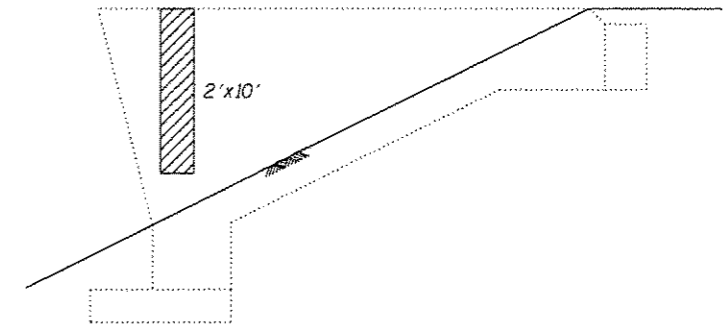
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-(1107-1)MBJBJR,B-R	TAZEWELL	22	17
CONTRACT NO. 68D65				
ILLINOIS FED. AID PROJECT				



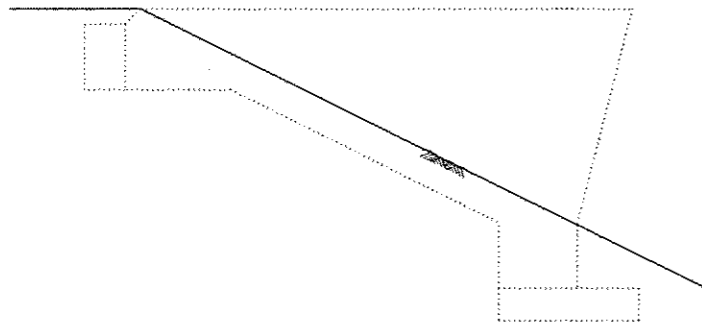
SIDE ELEVATION
(Looking West)



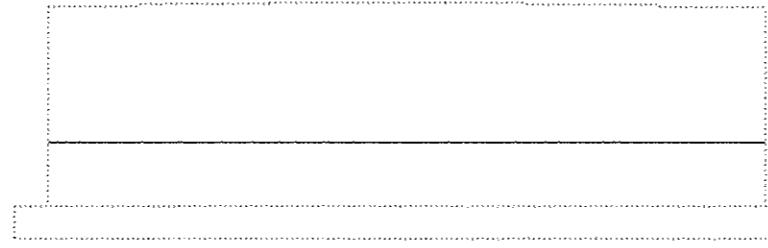
ELEVATION - SOUTH ABUTMENT
(Looking South)



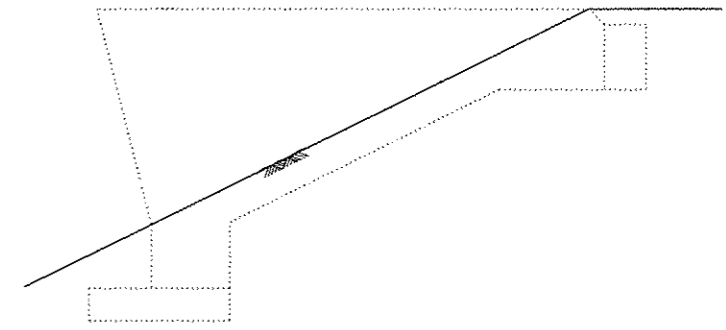
SIDE ELEVATION
(Looking East)




SIDE ELEVATION
(Looking East)



ELEVATION - NORTH ABUTMENT
(Looking North)



SIDE ELEVATION
(Looking West)

 Structural Repair of Concrete, Depth ≤ 5"

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete, Depth ≤ 5"	Sq. Ft.	148

DESIGNED *VHV*
 CHECKED *DAB*
 DRAWN *daburdell*
 CHECKED *VHV DAB*

PASSED

 ENGINEER OF BRIDGES AND STRUCTURES

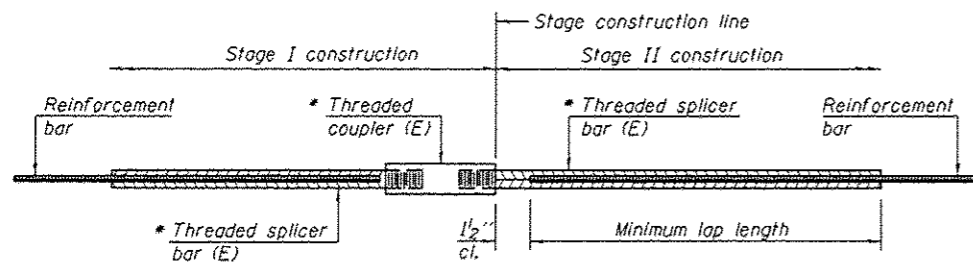
DATE MAY 5, 2017
 REVISED
 REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

NORTH AND SOUTH ABUTMENT REPAIR DETAILS
 SN 090-0135 (NB)

SHEET NO. 6 OF 7 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-(107-1)NB10R,B-R	TAZEWELL	22	18
CONTRACT NO. 68D65			ILLINOIS FED. AID PROJECT	

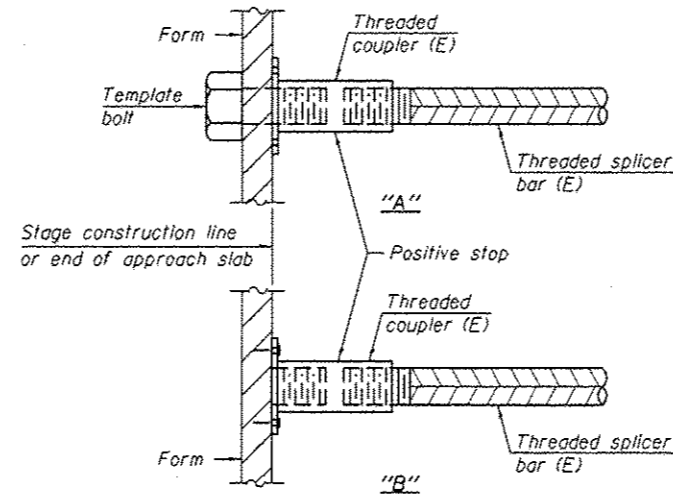


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1/2" + thread length

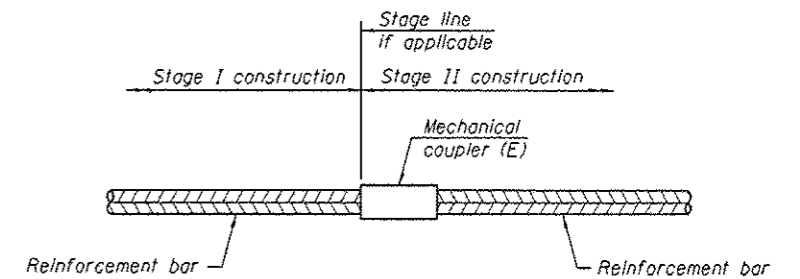
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Top of slab	#5	16	3'-6"
Bottom of slab	#5	16	3'-6"
Top of backwall	#5	8	4'-0"



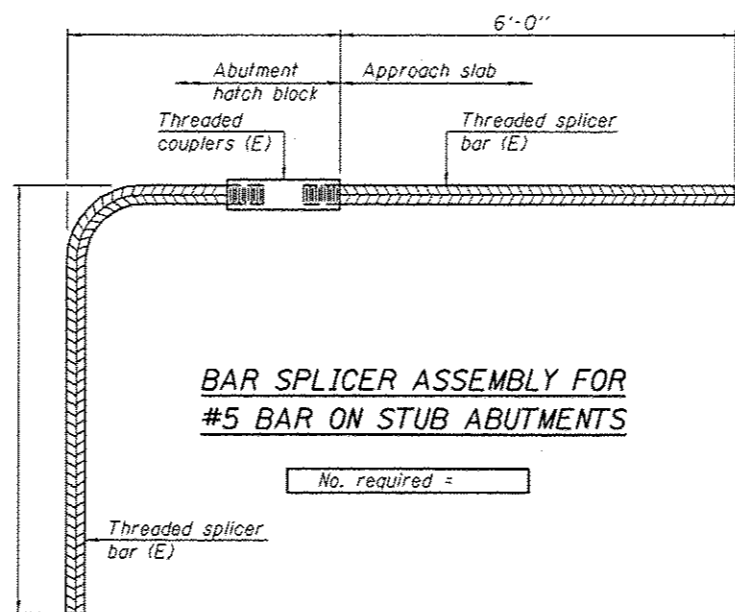
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



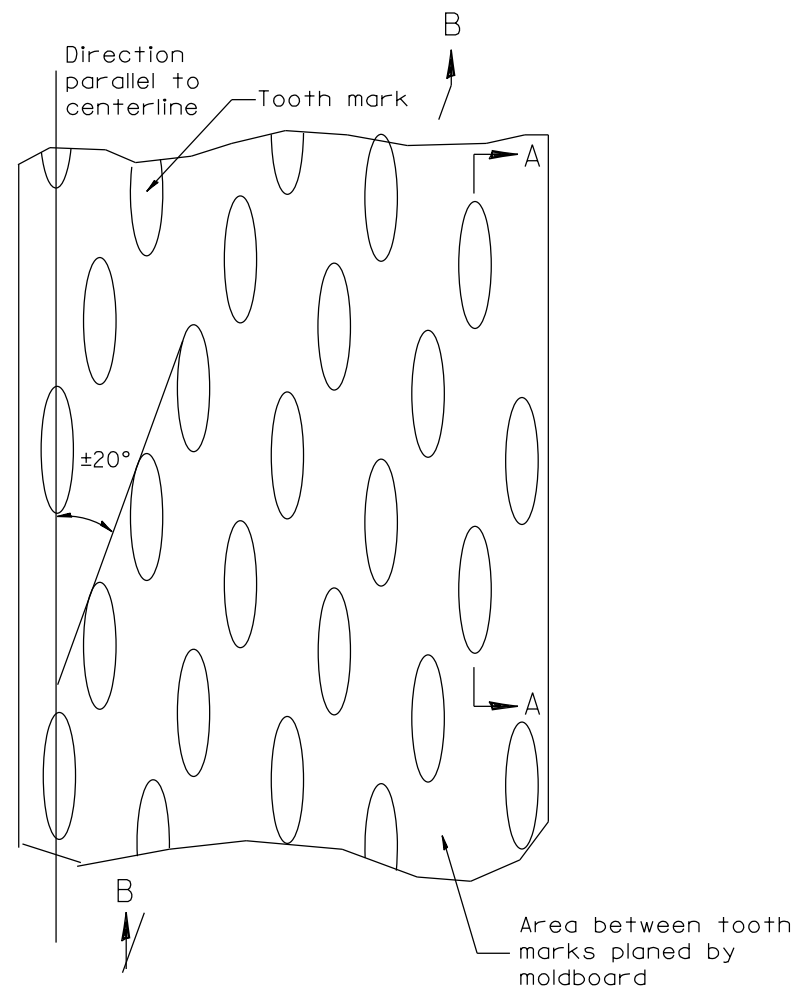
BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

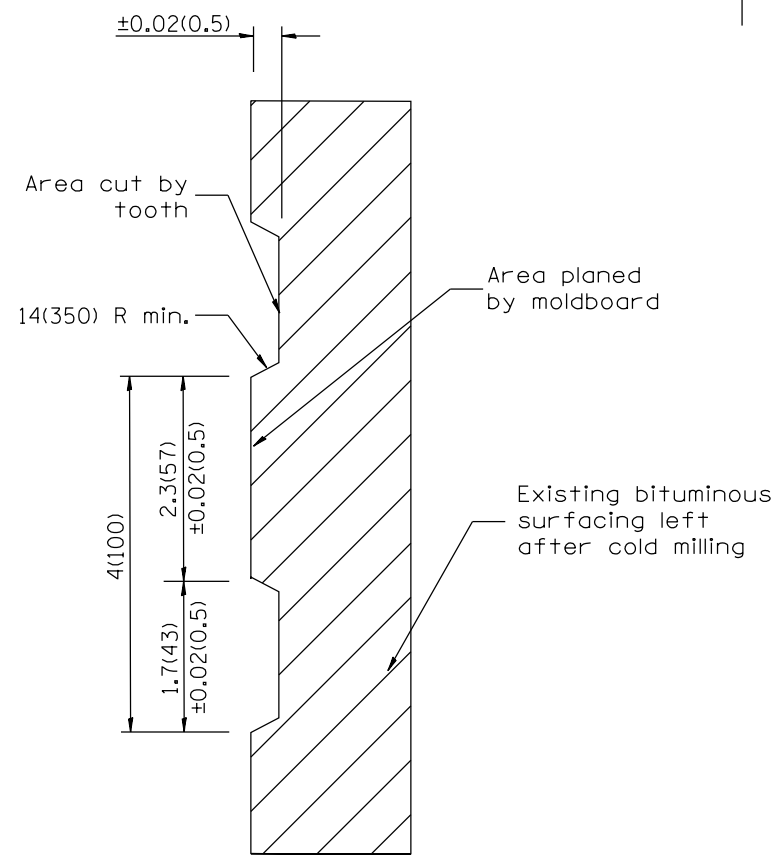
BSD-1 11-22-2016



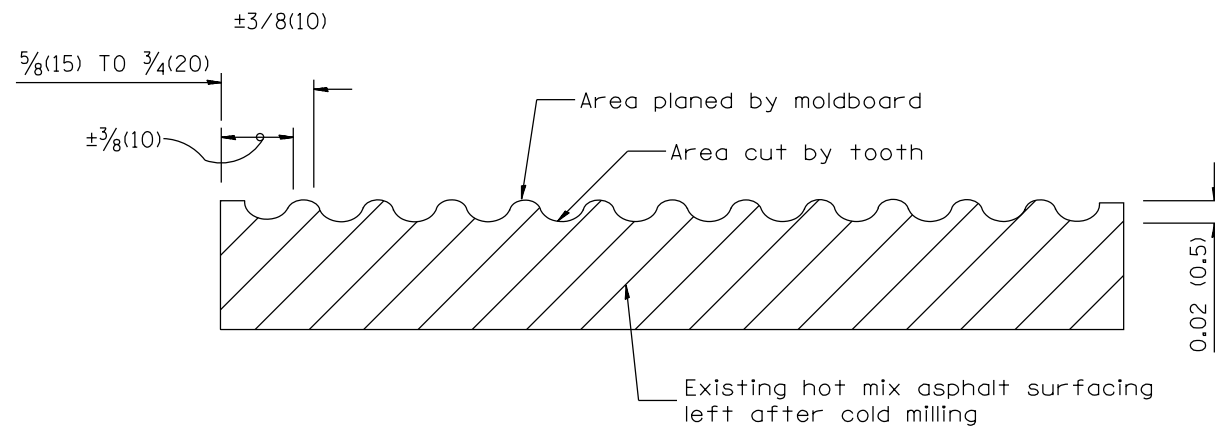
PLAN

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



SECTION A-A



SECTION B-B PROJECTED PERPENDICULAR TO CENTERLINE

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-104.01, NEW REVISION BOX	T.P.
04-20-98	REMOVED MILLING DETAIL FROM STANDARD	J.A.
09-08-98	CORRECT NOTE LEADER PLACEMENT	R.W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

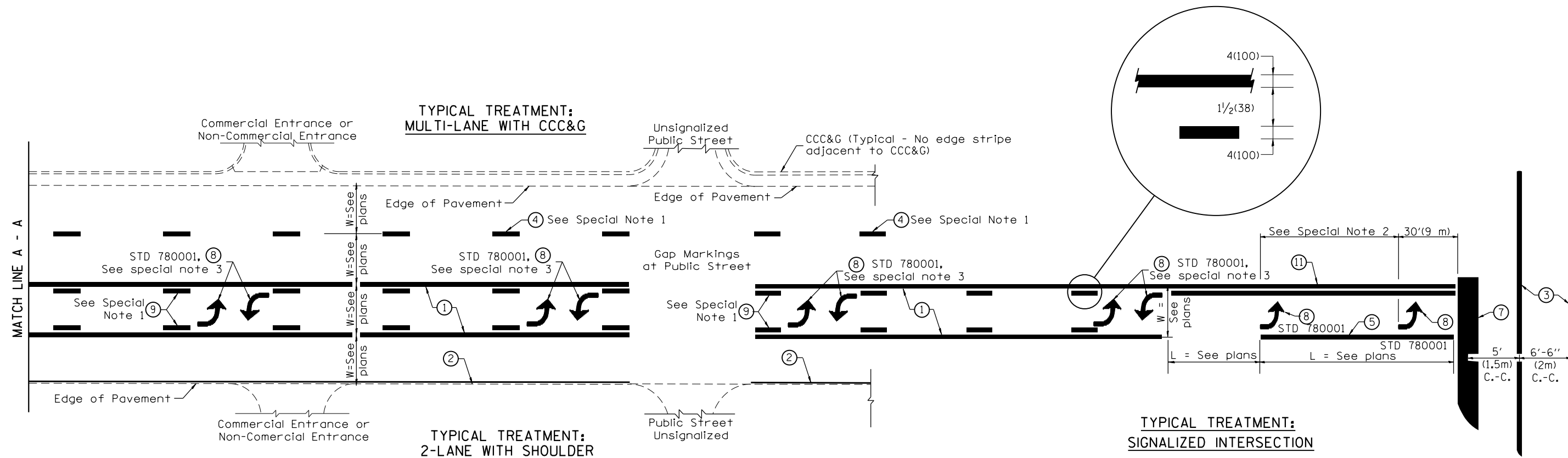
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

NOT TO SCALE

CADD STD. 440001-D4

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-[(107-1)HB]BJR,B-R	TAZEWELL	22	20
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68D65	



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.			
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.			

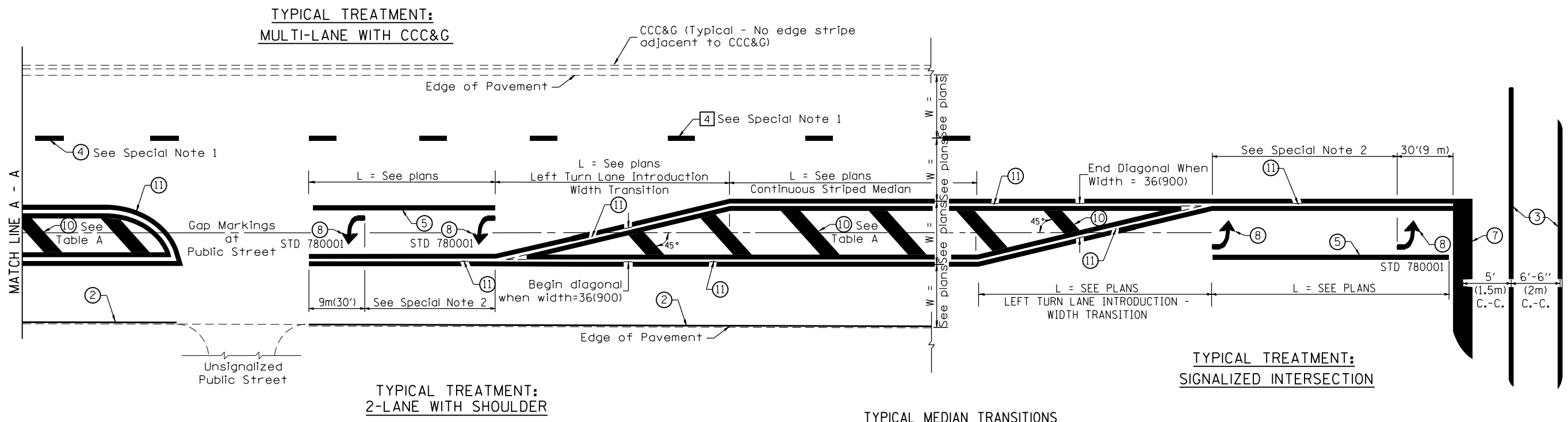
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

TYPICAL PAVEMENT MARKINGS

SHT. 1 OF 2
CADD STD. 780001-D4

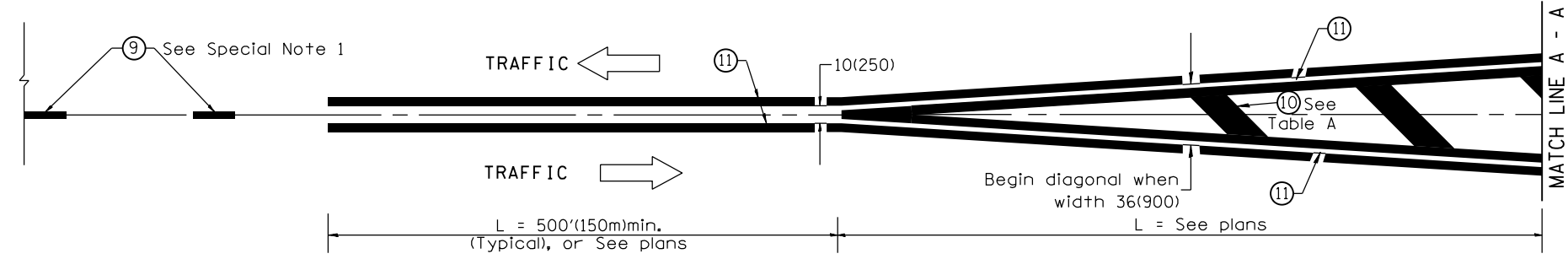
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
155	90-[(107-1)HB]B,R,B-R	TAZEWELL	22	21
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68D65	



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



MEDIAN INTRODUCTION - WIDTH TRANSITIONS

All dimensions are in inches (millimeters) unless otherwise noted.