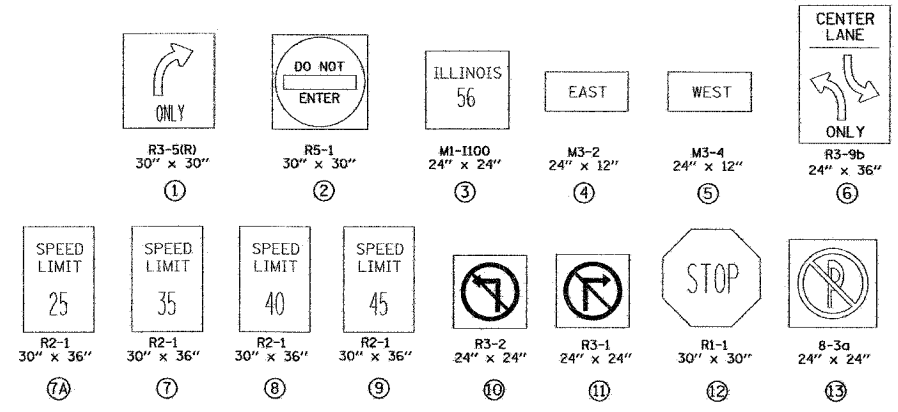
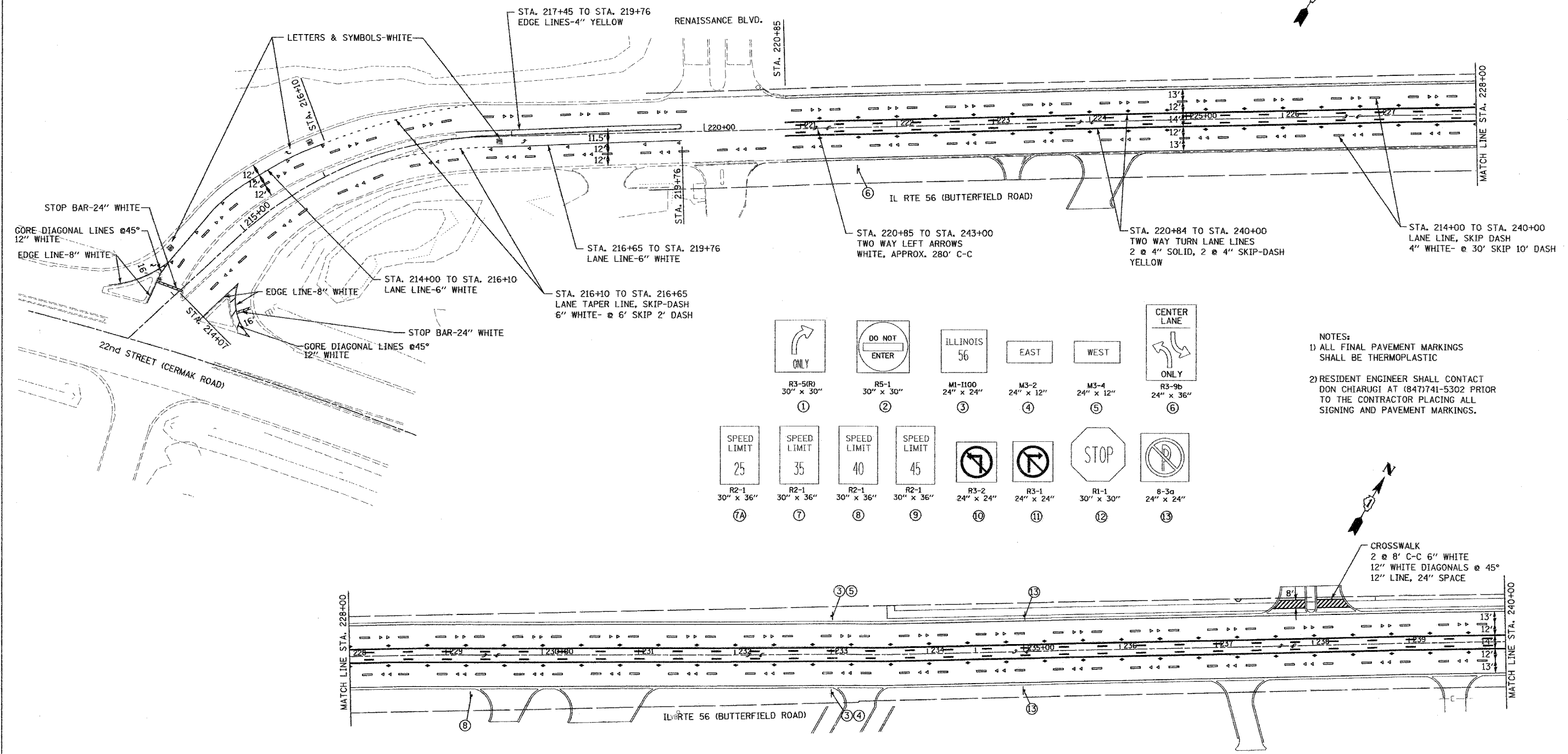
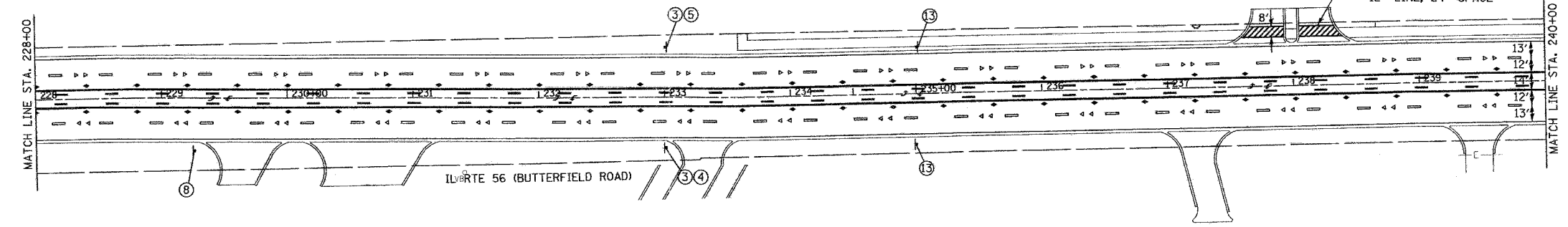


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	101
STA. 220		TO STA. 240+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				



NOTES:
 1) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC
 2) RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI AT (847)741-5302 PRIOR TO THE CONTRACTOR PLACING ALL SIGNING AND PAVEMENT MARKINGS.



REVISIONS	
NAME	DATE

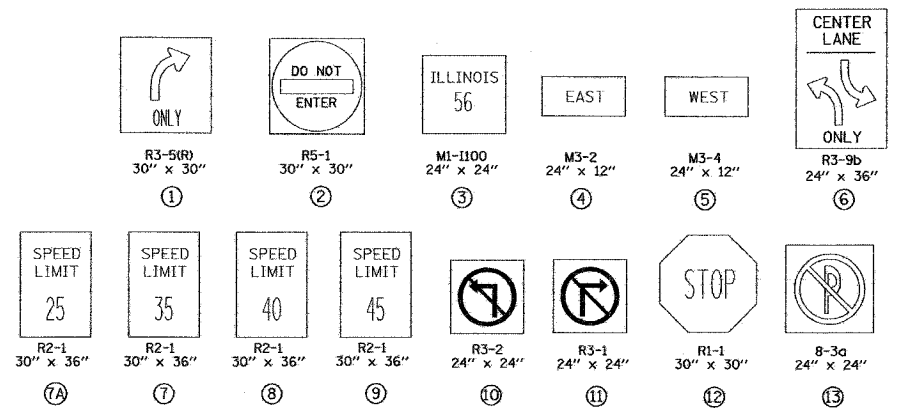
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 3545
 IL RTE 56 (BUTTERFIELD ROAD)

PAVEMENT MARKING PLAN
 AND SIGNING PLAN

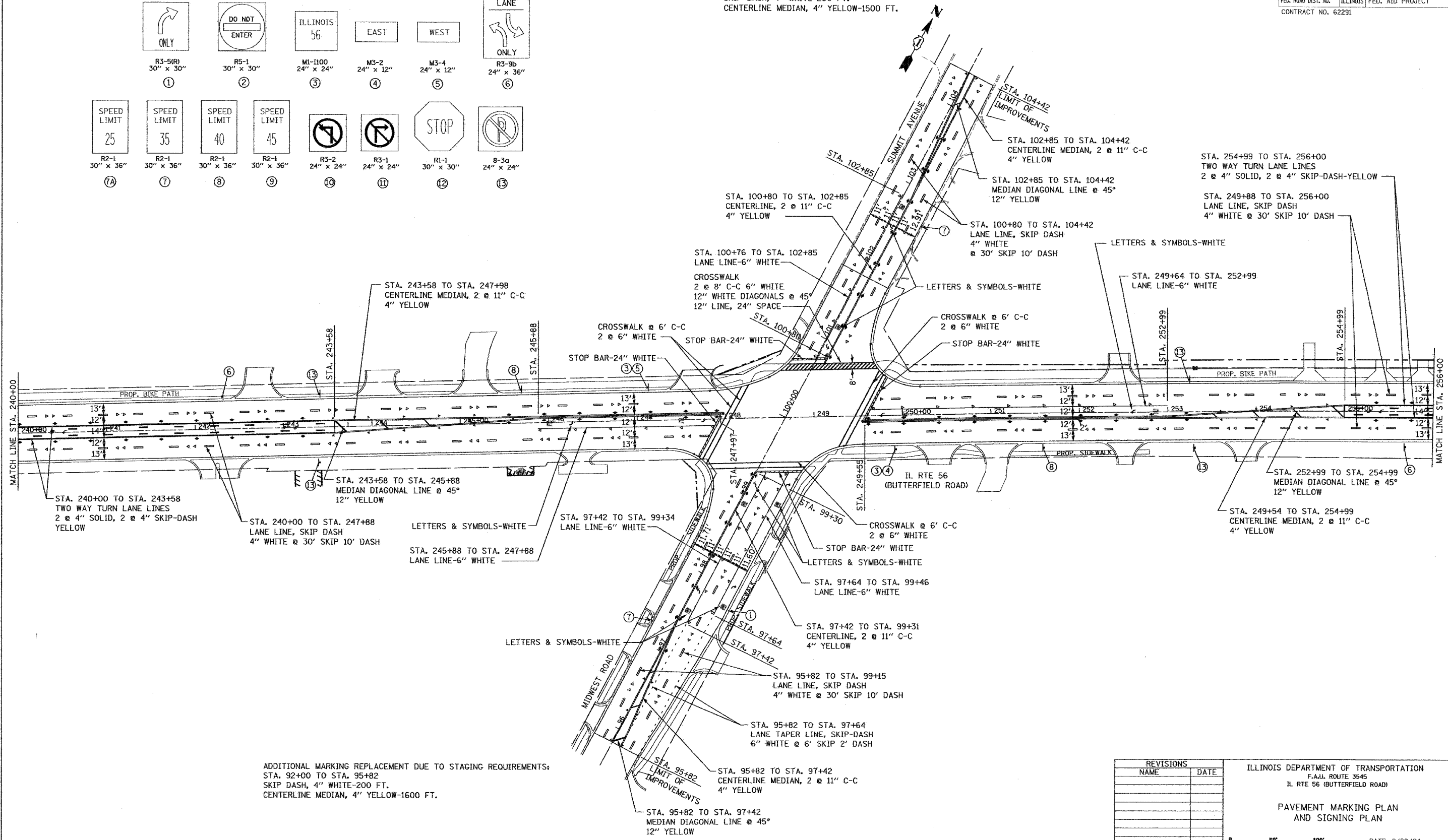
0 50' 100'
 1" = 50'

DATE: 2/20/04
 DRAWN BY: SMP
 CHECKED BY: SJG

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	102
STA. 240+00		TO STA. 256+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				



ADDITIONAL MARKING REPLACEMENT DUE TO STAGING REQUIREMENTS:
 STA. 104+42 TO STA. 108+00
 SKIP DASH, 4" WHITE-200 FT.
 CENTERLINE MEDIAN, 4" YELLOW-1500 FT.



ADDITIONAL MARKING REPLACEMENT DUE TO STAGING REQUIREMENTS:
 STA. 92+00 TO STA. 95+82
 SKIP DASH, 4" WHITE-200 FT.
 CENTERLINE MEDIAN, 4" YELLOW-1600 FT.

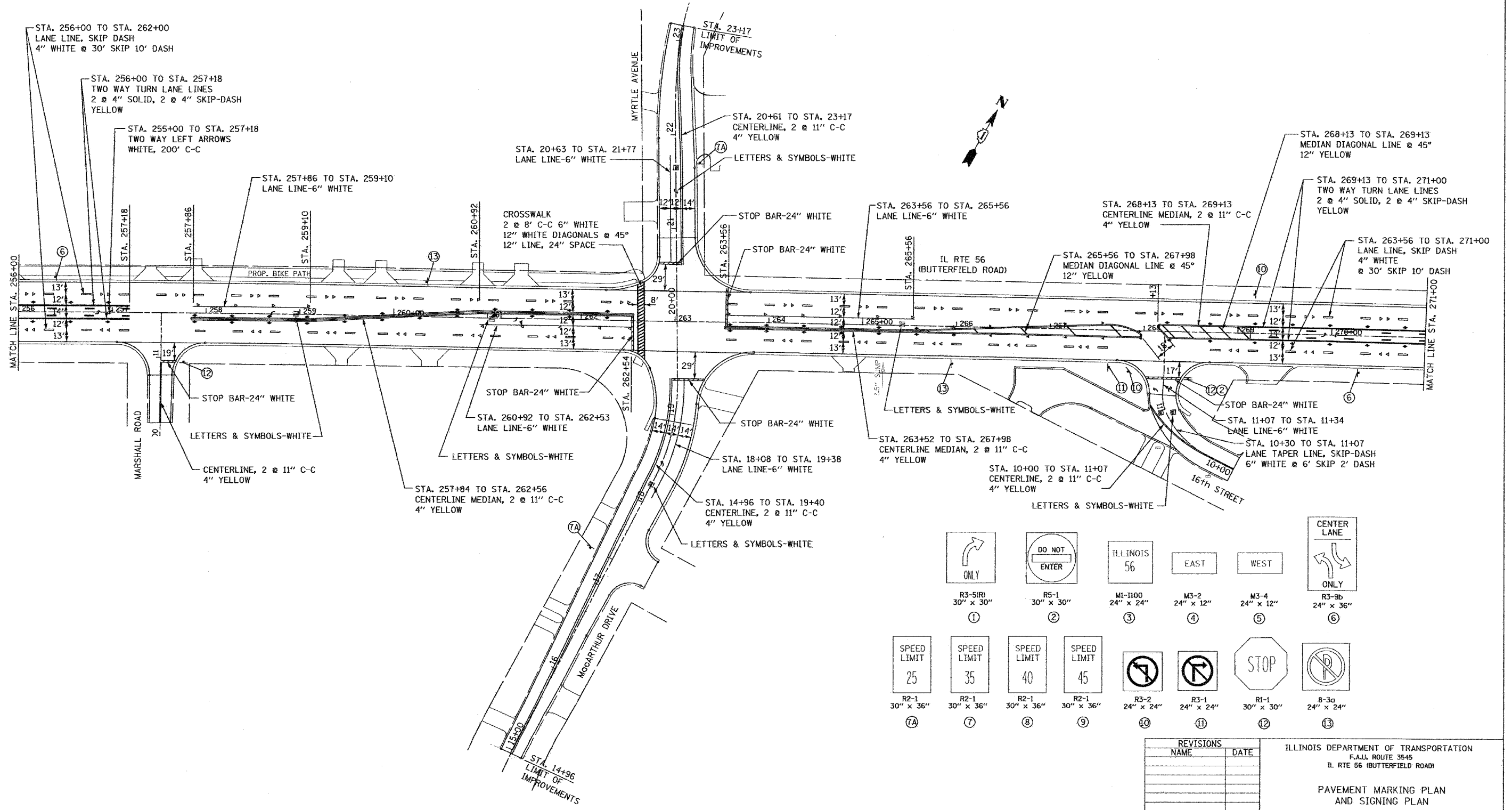
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.J. ROUTE 3545
 IL RTE 56 (BUTTERFIELD ROAD)

PAVEMENT MARKING PLAN
 AND SIGNING PLAN

DATE: 2/20/04
 DRAWN BY: SMP
 CHECKED BY: SJG

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	103
STA. 256+00		TO STA. 271+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				



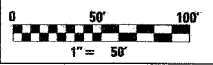
R3-5(R) 30" x 30"	R5-1 30" x 30"	M1-1100 24" x 24"	M3-2 24" x 12"	M3-4 24" x 12"	R3-9b 24" x 36"
①	②	③	④	⑤	⑥
R2-1 30" x 36"	R2-1 30" x 36"	R2-1 30" x 36"	R2-1 30" x 36"	R3-2 24" x 24"	R3-1 24" x 24"
⑦	⑧	⑨	⑩	⑪	⑫
R1-1 30" x 30"	8-3a 24" x 24"				
⑬					

REVISIONS	
NAME	DATE

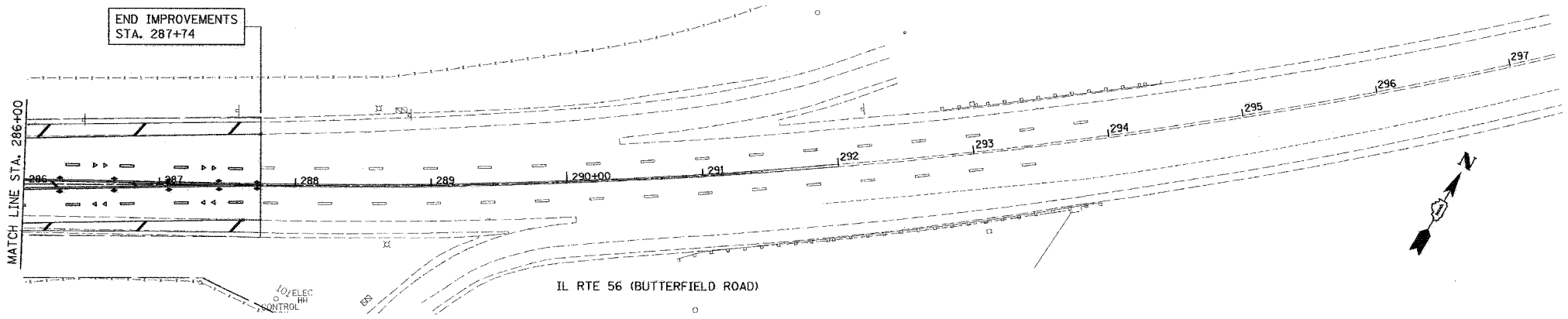
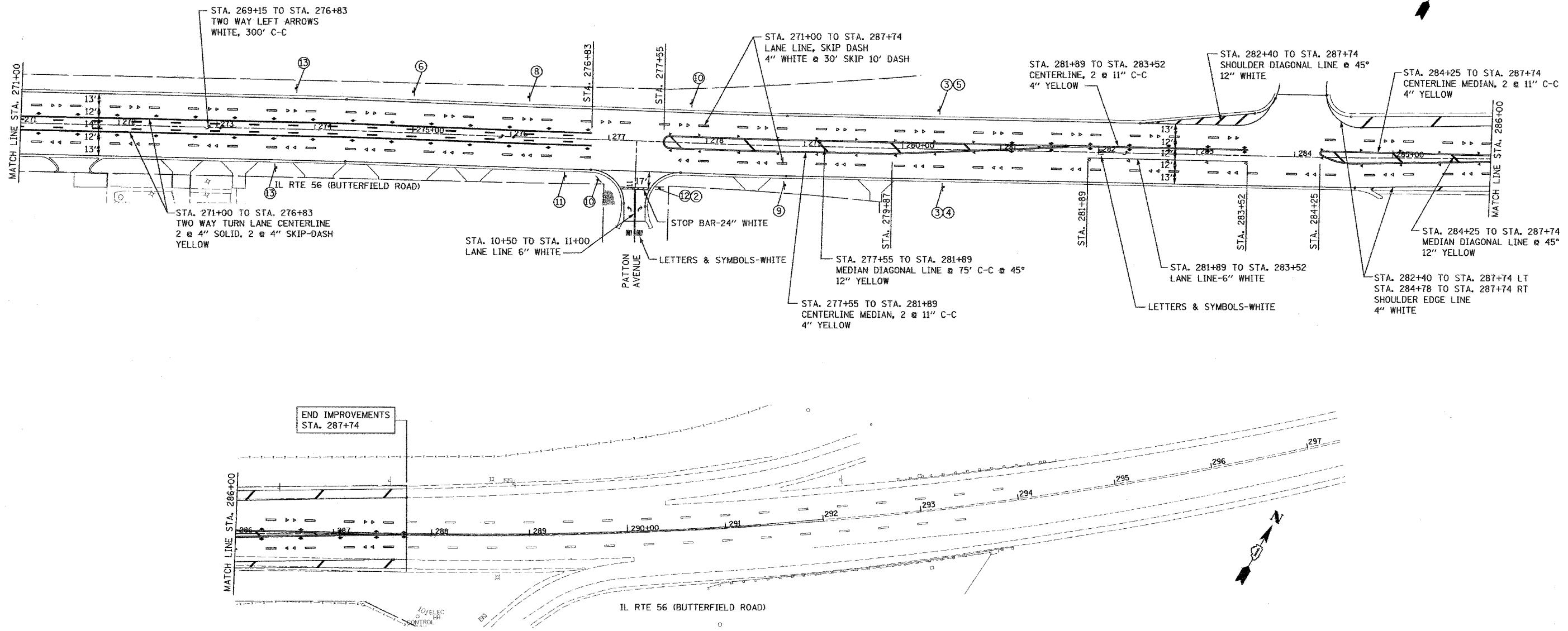
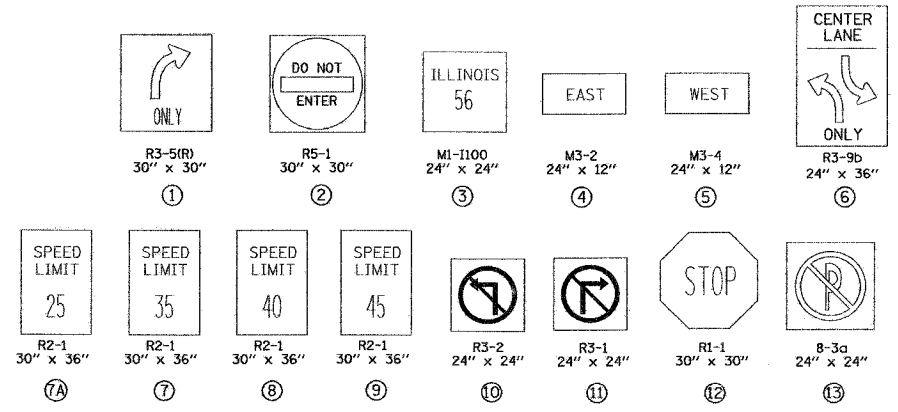
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. ROUTE 3545
IL RTE 56 (BUTTERFIELD ROAD)

PAVEMENT MARKING PLAN
AND SIGNING PLAN

DATE: 2/20/04
DRAWN BY: SMP
CHECKED BY: SJG



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WR5-7	DuPAGE	235	104
STA. 271+00		TO STA. 287+74		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				



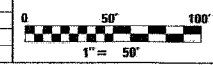
ADDITIONAL MARKING REPLACEMENT DUE TO STAGING REQUIREMENTS:
 STA. 287+74 TO STA. 295+00
 SKIP DASH, 4" WHITE-200 FT.
 EDGE LINE, 4" WHITE-400 FT.

REVISIONS	
NAME	DATE

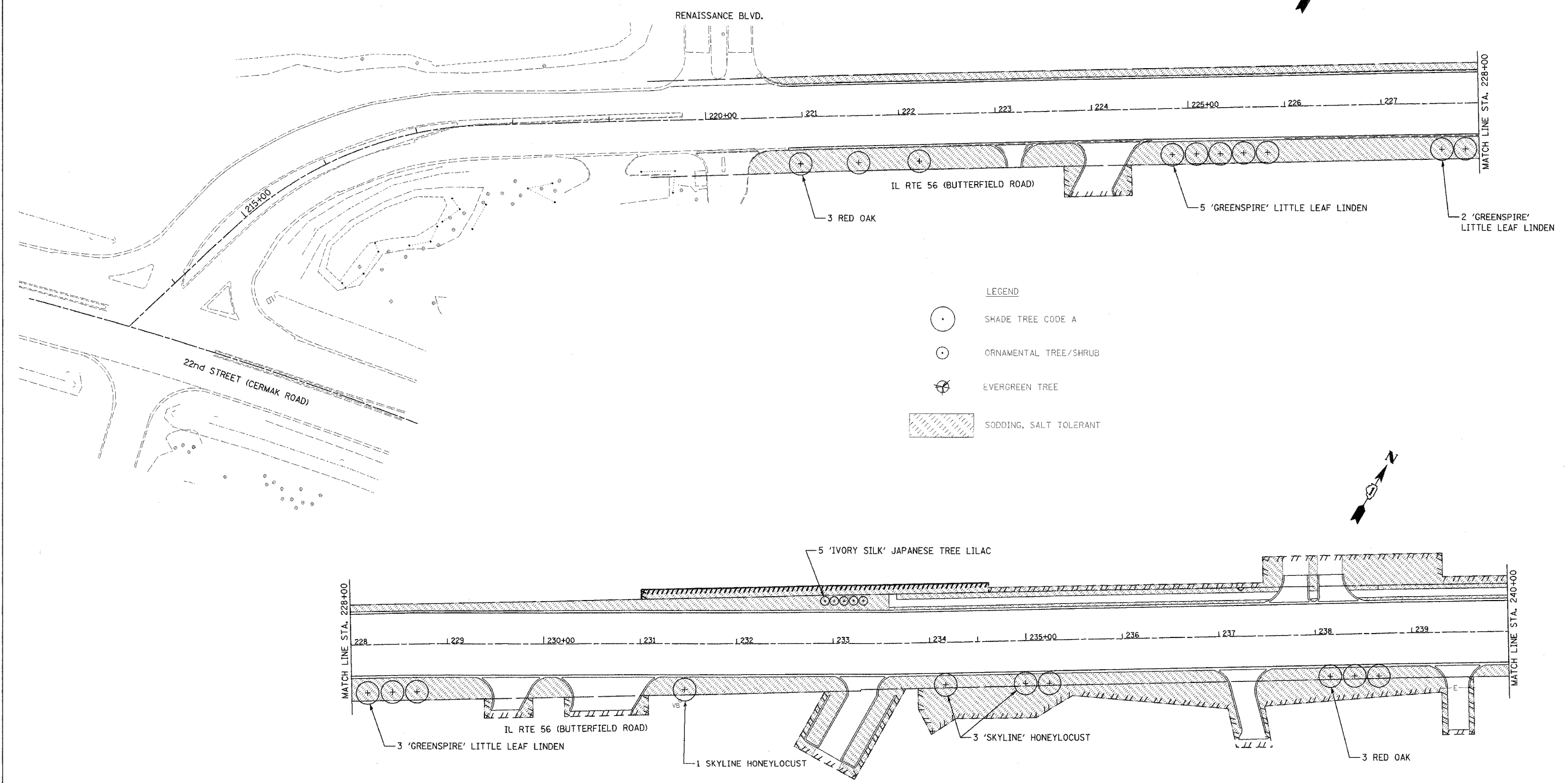
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 3545
 IL RTE 56 (BUTTERFIELD ROAD)

PAVEMENT MARKING PLAN
 AND SIGNING PLAN

DATE: 2/20/04
 DRAWN BY: SMP
 CHECKED BY: SJG



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	105
STA. 220		TO STA. 240+00		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
CONTRACT NO. 62291				



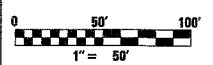
LEGEND

	SHADE TREE CODE A
	ORNAMENTAL TREE/SHRUB
	EVERGREEN TREE
	SODDING, SALT TOLERANT



REVISIONS	
NAME	DATE




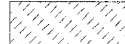
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 3545
 IL RTE 56 (BUTTERFIELD ROAD)
 LANDSCAPE PLAN

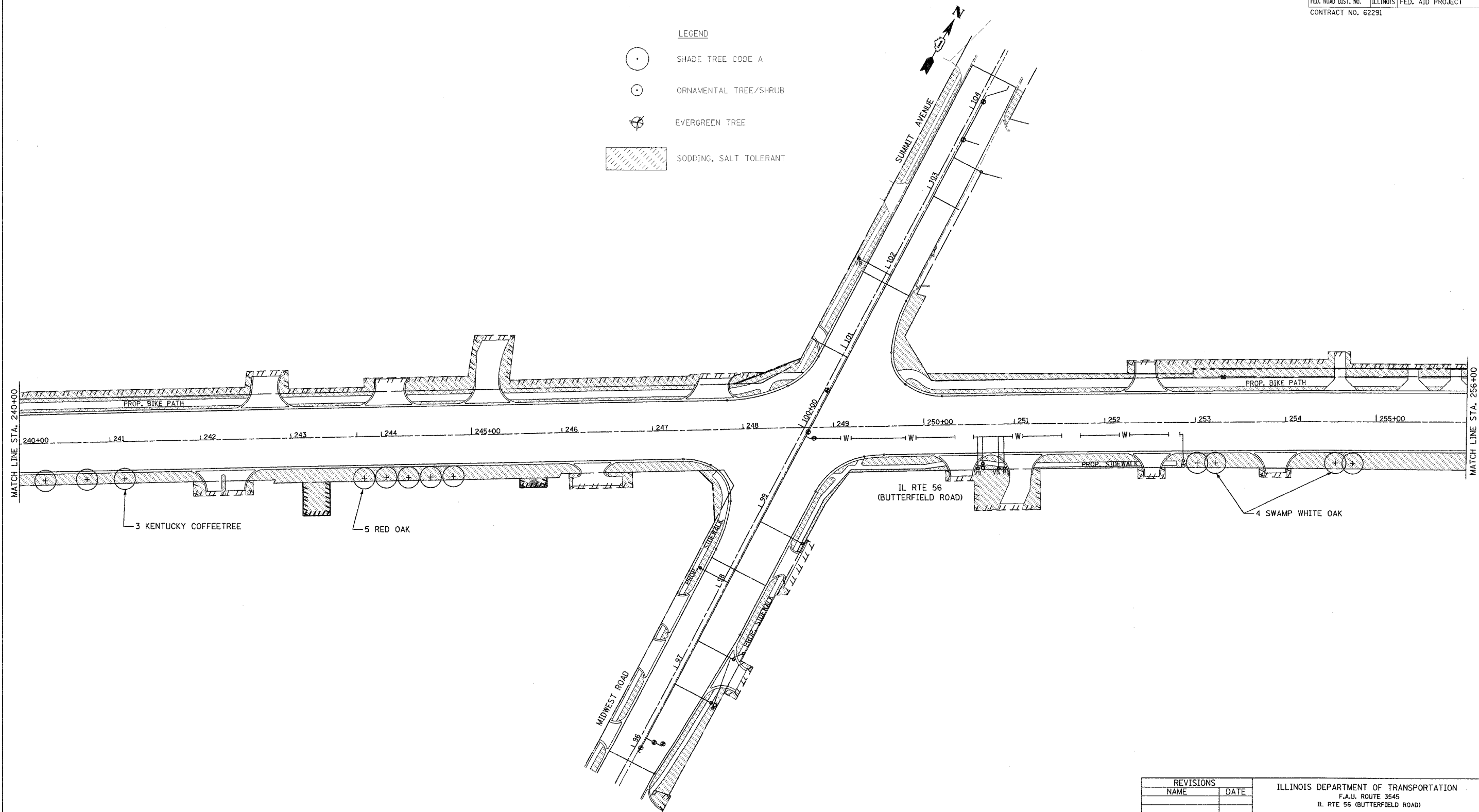


DATE: 2/20/04
 DRAWN BY: SMP
 CHECKED BY: BDH

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	106
STA. 240+00		TO STA. 256+00		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
CONTRACT NO. 62291				

LEGEND

-  SHADE TREE CODE A
-  ORNAMENTAL TREE/SHRUB
-  EVERGREEN TREE
-  SODDING, SALT TOLERANT



3 KENTUCKY COFFEETREE

5 RED OAK

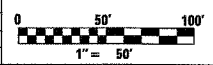
IL RTE 56
(BUTTERFIELD ROAD)

4 SWAMP WHITE OAK

REVISIONS	
NAME	DATE





ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. ROUTE 3545
IL RTE 56 (BUTTERFIELD ROAD)

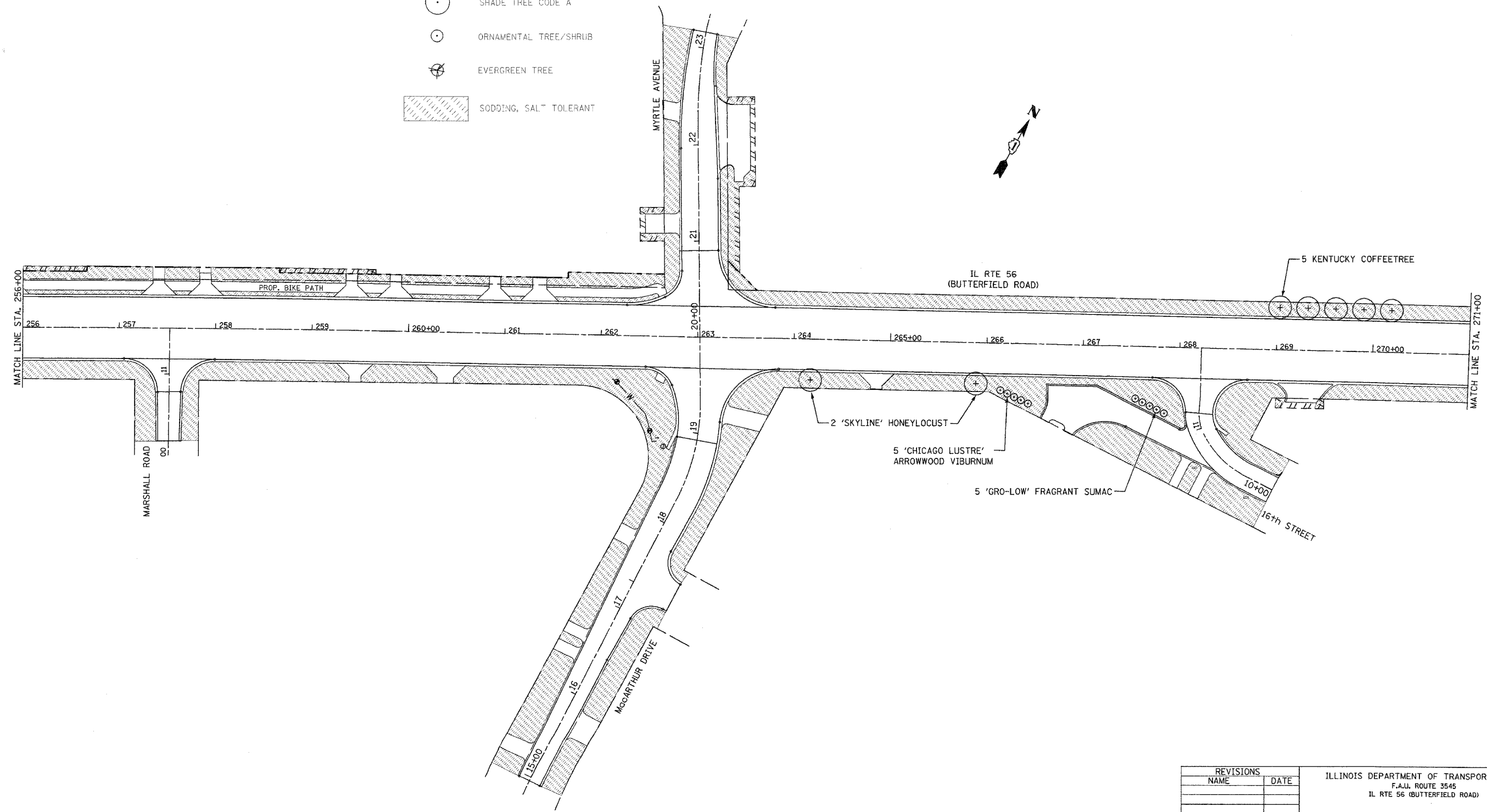
LANDSCAPE PLAN



DATE: 2/20/04
DRAWN BY: SMP
CHECKED BY: BDH

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	107
STA. 256+00		TO STA. 271+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				

- LEGEND
-  SHADE TREE CODE A
 -  ORNAMENTAL TREE/SHRUB
 -  EVERGREEN TREE
 -  SODDING, SALT TOLERANT



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 3545
 IL RTE 56 (BUTTERFIELD ROAD)




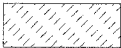
LANDSCAPE PLAN

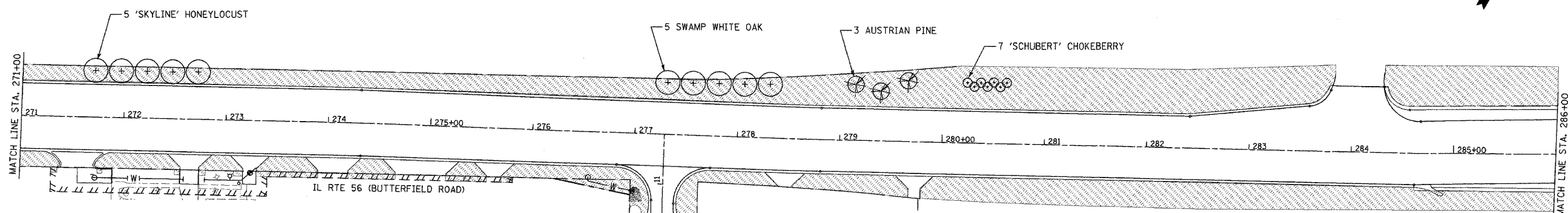
0 50' 100'
 1" = 50'

DATE: 2/20/04
 DRAWN BY: SMP
 CHECKED BY: BDH

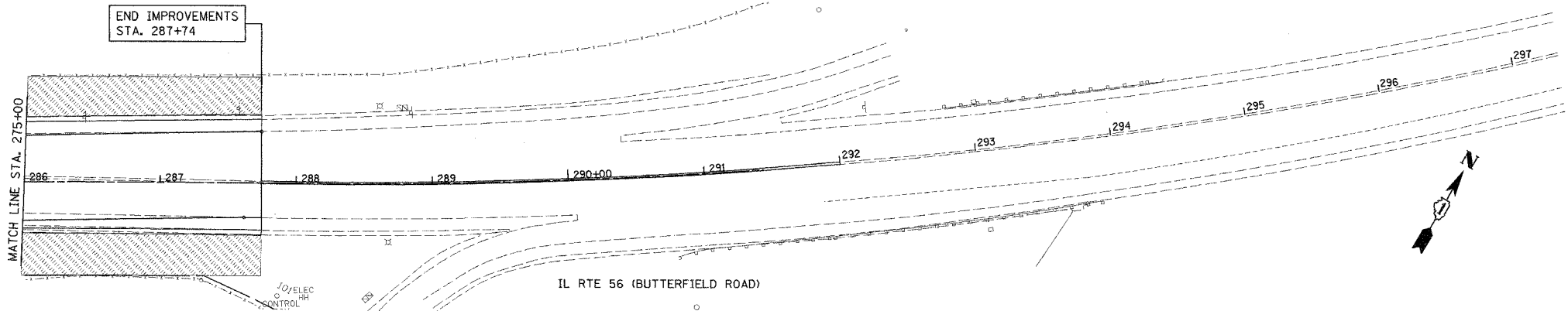
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	108
STA. 271+00		TO STA. 297+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				

LEGEND

-  SHADE TREE CODE A
-  ORNAMENTAL TREE/SHRUB
-  EVERGREEN TREE
-  SODDING, SALT TOLERANT



END IMPROVEMENTS
STA. 287+74

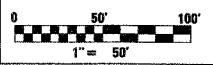


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. ROUTE 3545
IL RTE 56 (BUTTERFIELD ROAD)

LANDSCAPE PLAN

DATE: 2/20/04
DRAWN BY: SMP
CHECKED BY: BDH



NOTES FOR TEMPORARY TRAFFIC SIGNALS

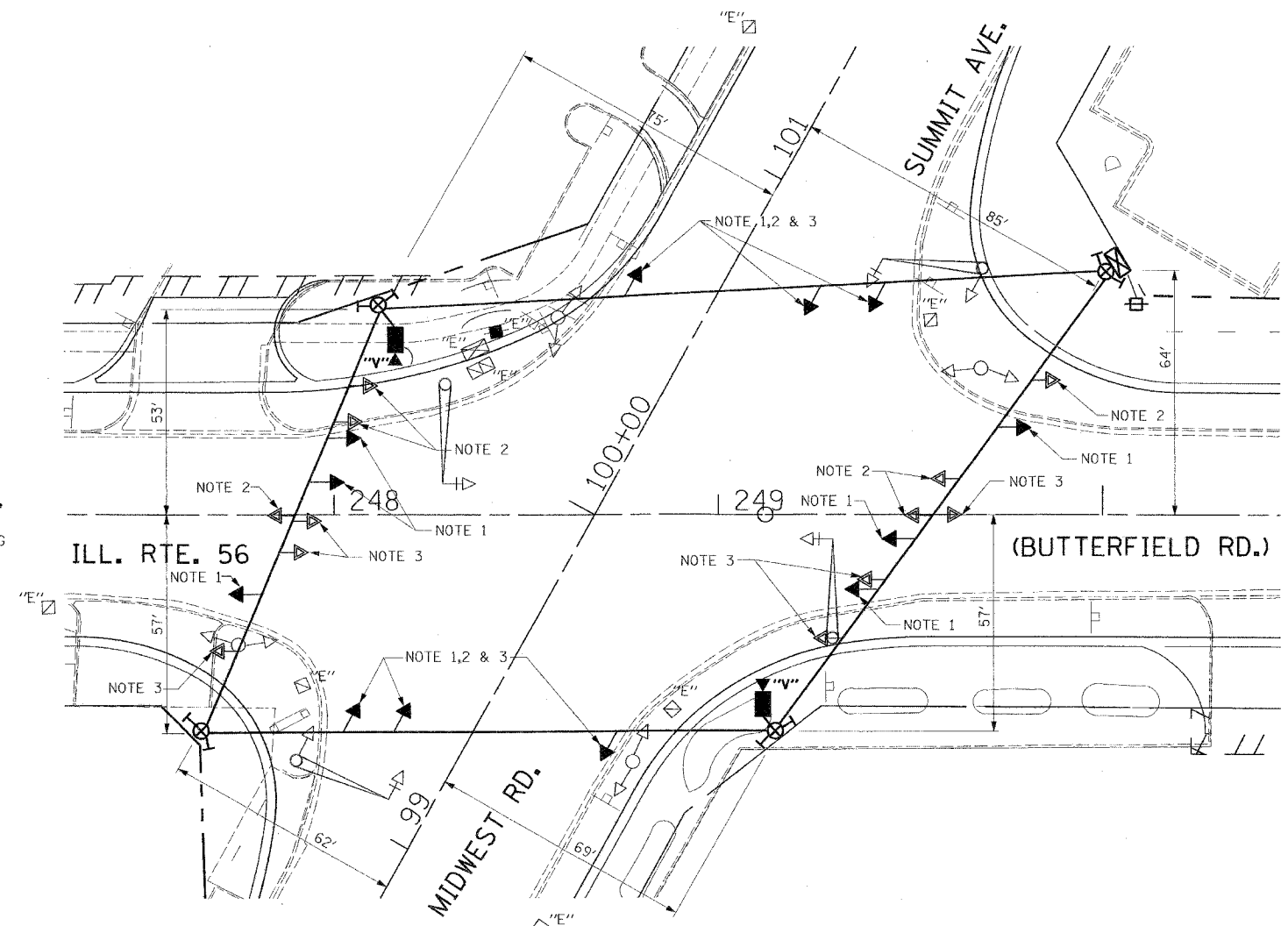
- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL. AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH CONTROLLER AND CABINET, COMPLETE

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGH-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED
 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION, MAST ARM MOUNTED
 4 EACH SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
 4 EACH TRAFFIC SIGNAL BACKPLATE
 4 EACH TRAFFIC SIGNAL POST, 16 FT.
 4 EACH ALUMINUM MAST ARM ASSEMBLY AND POLE
 1 EACH SERVICE INSTALLATION



TEMPORARY TRAFFIC SIGNAL LEGEND

- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ▲ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊕ TEMPORARY SERVICE INSTALLATION
- ⊔ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊖ VIDEO DETECTION
- ⊙ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊗ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊕ CONFIRMATION BEACON
- ⊠ VEHICLE DETECTOR, INDUCTION LOOP
- CT COMMON TRENCH
- UD UNIT DUCT
- G.S. CONDUIT IN GROUND
- ⊠ HANDHOLE
- ⊠ HEAVY DUTY HANDHOLE

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ▲ EXISTING SIGNAL TO BE REMOVED
- ⊖ "E" EXISTING SERVICE INSTALLATION TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ▲ EXISTING MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊠ "E" EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- ⊠ "E" EXISTING HANDHOLE TO BE REMOVED
- ⊠ "E" EXISTING DOUBLE HANDHOLE TO BE REMOVED
- ⊔ PEDESTRIAN SIGNAL TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN PUSH-BUTTON TO BE REMOVED
- ⊗ EXISTING EMERGENCY VEHICLE LIGHT DETECTOR TO BE REMOVED
- ⊕ EXISTING CONFIRMATION BEACON TO BE REMOVED
- ⊠ "E" EXISTING HEAVY DUTY HANDHOLE TO BE REMOVED
- EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED

STAGE CONSTRUCTION NOTES

- SIGNAL HEAD LOCATION FOR PRE CONSTRUCTION AND CONSTRUCTION STAGE 1
- SIGNAL HEAD LOCATION FOR CONSTRUCTION STAGE 2
- SIGNAL HEAD LOCATION FOR CONSTRUCTION STAGE 3

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

GO GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HIGHWAY
 SUITE 306
 CHICAGO, ILLINOIS 60631 TEL: (773) 774-590

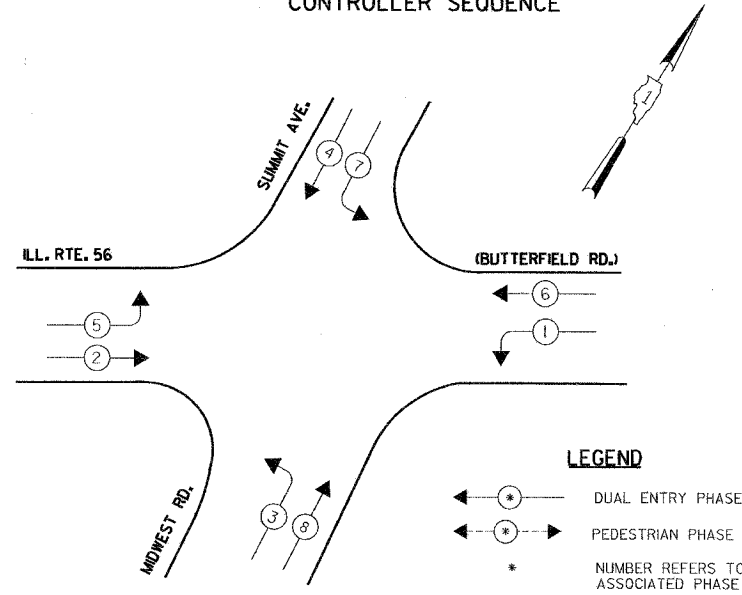
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN
 ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
 AT SUMMIT AVENUE-MIDWEST ROAD



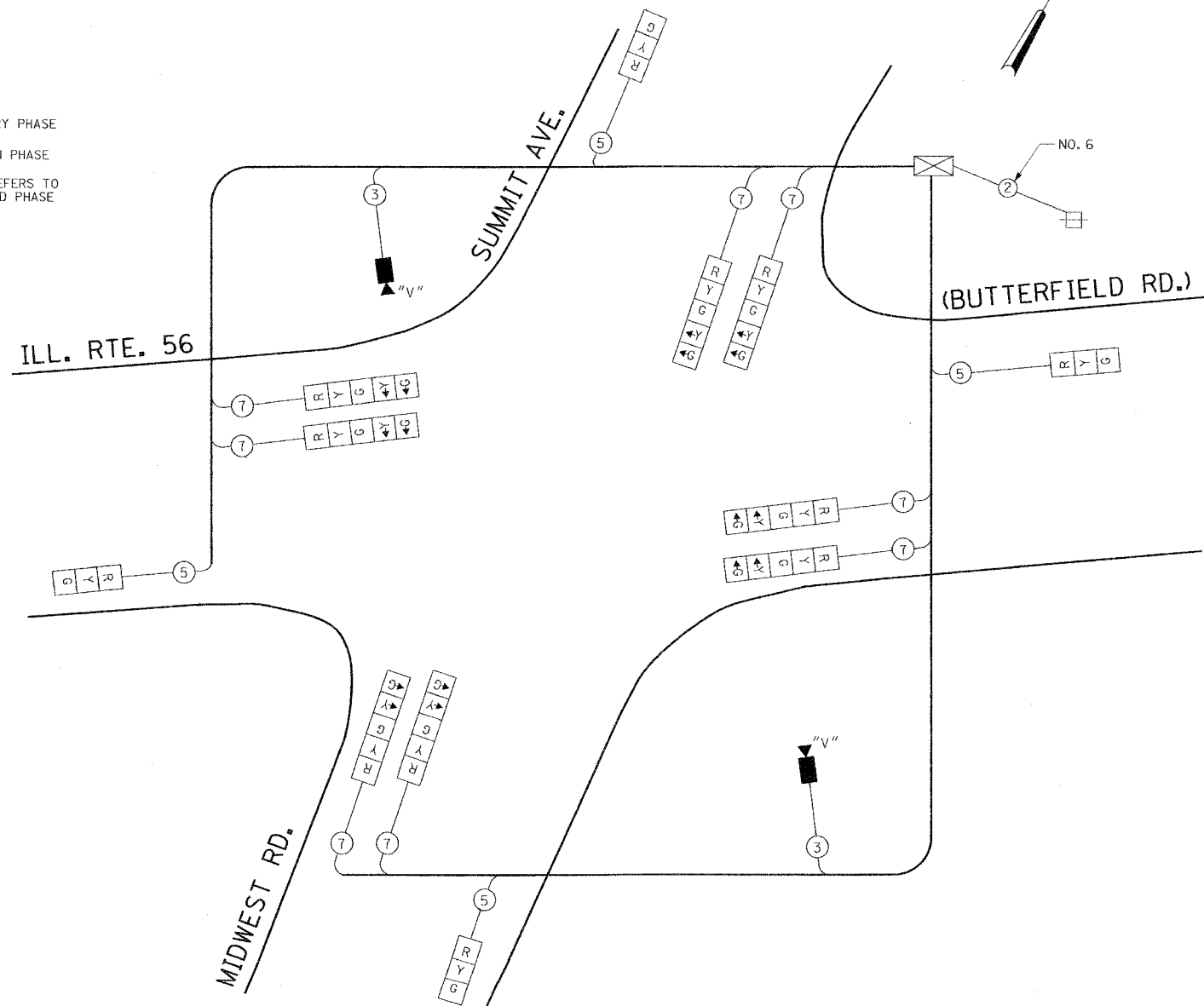
DATE: 9/03/04
 DRAWN BY: JEK
 DESIGNED BY: PKG
 CHECKED BY: PKG

CONTROLLER SEQUENCE



PHASE DESIGNATION DIAGRAM

FOR ALL CONSTRUCTION STAGES AND PRIOR TO PERMANENT SIGNAL OPERATION WHEN ALL NEW PAVEMENT IS OPENED TO TRAFFIC



TEMPORARY CABLE DIAGRAM LEGEND

- [R] TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- [] TEMPORARY CONTROLLER CABINET
- [] TEMPORARY SERVICE INSTALLATION
- (5) INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- ◄◄ EMERGENCY VEHICLE LIGHT DETECTOR
- ◄◄ CONFIRMATION BEACON
- [] VEHICLE DETECTOR, INDUCTION LOOP
- PEDESTRIAN PUSHBUTTON DETECTOR
- [] 12" (300mm) PEDESTRIAN SIGNAL SECTION
- ◄◄ VIDEO DETECTION

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE		OPERATION	
SIGNAL (RED)	12	135	17	0.50	810.00
(YELLOW)	12	135	25	0.25	405.00
(GREEN)	12	135	15	0.25	405.00
ARROW	16	135	12	0.10	216.00
PED. SIGNAL		90	25	1.00	100.00
CONTROLLER	1	100	100	1.00	100.00
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	1936.00

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

ENERGY SUPPLY CONTACT: Robert Rogers
PHONE: (630) 691-4861
COMPANY: ComEd-Glenbard

GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL: (773) 774-5900

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**TEMPORARY CABLE PLAN
PHASE DESIGNATION DIAGRAM
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT SUMMIT AVENUE-MIDWEST ROAD**

SCALE: N.T.S.

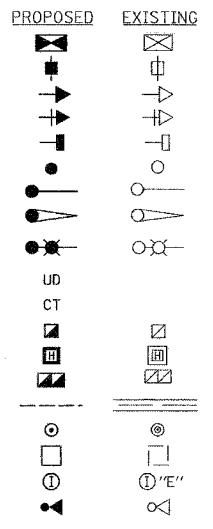
DATE: SEPTEMBER 03, 2004

DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG

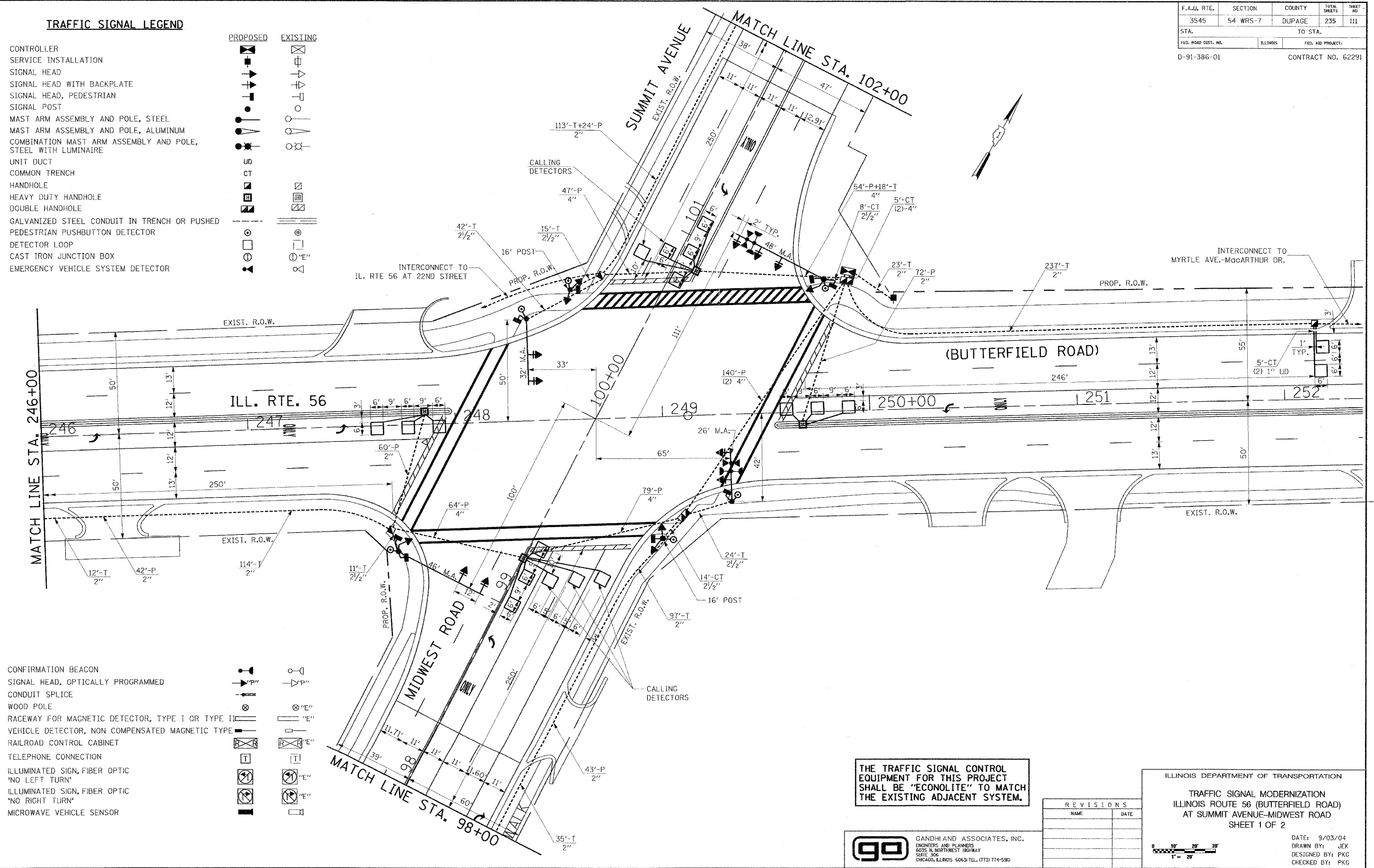
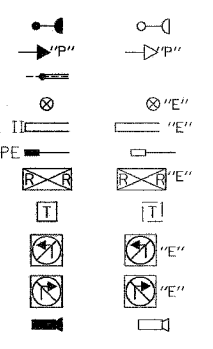
F.A.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	111
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT:	
D-91-386-01		CONTRACT NO. 62291		

TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE
- UNIT DUCT
- COMMON TRENCH
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP
- CAST IRON JUNCTION BOX
- EMERGENCY VEHICLE SYSTEM DETECTOR



- CONFIRMATION BEACON
- SIGNAL HEAD, OPTICALLY PROGRAMMED
- CONDUIT SPLICE
- WOOD POLE
- RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
- VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
- RAILROAD CONTROL CABINET
- TELEPHONE CONNECTION
- ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
- ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
- MICROWAVE VEHICLE SENSOR



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

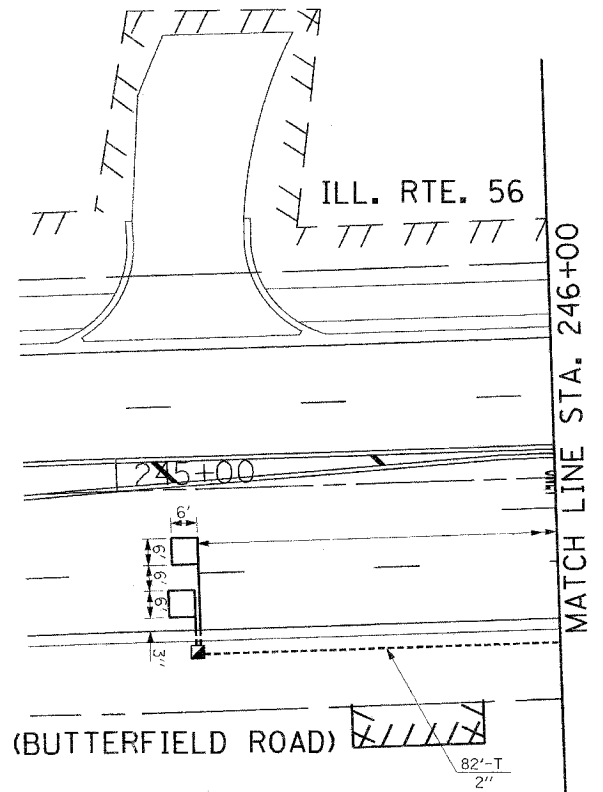
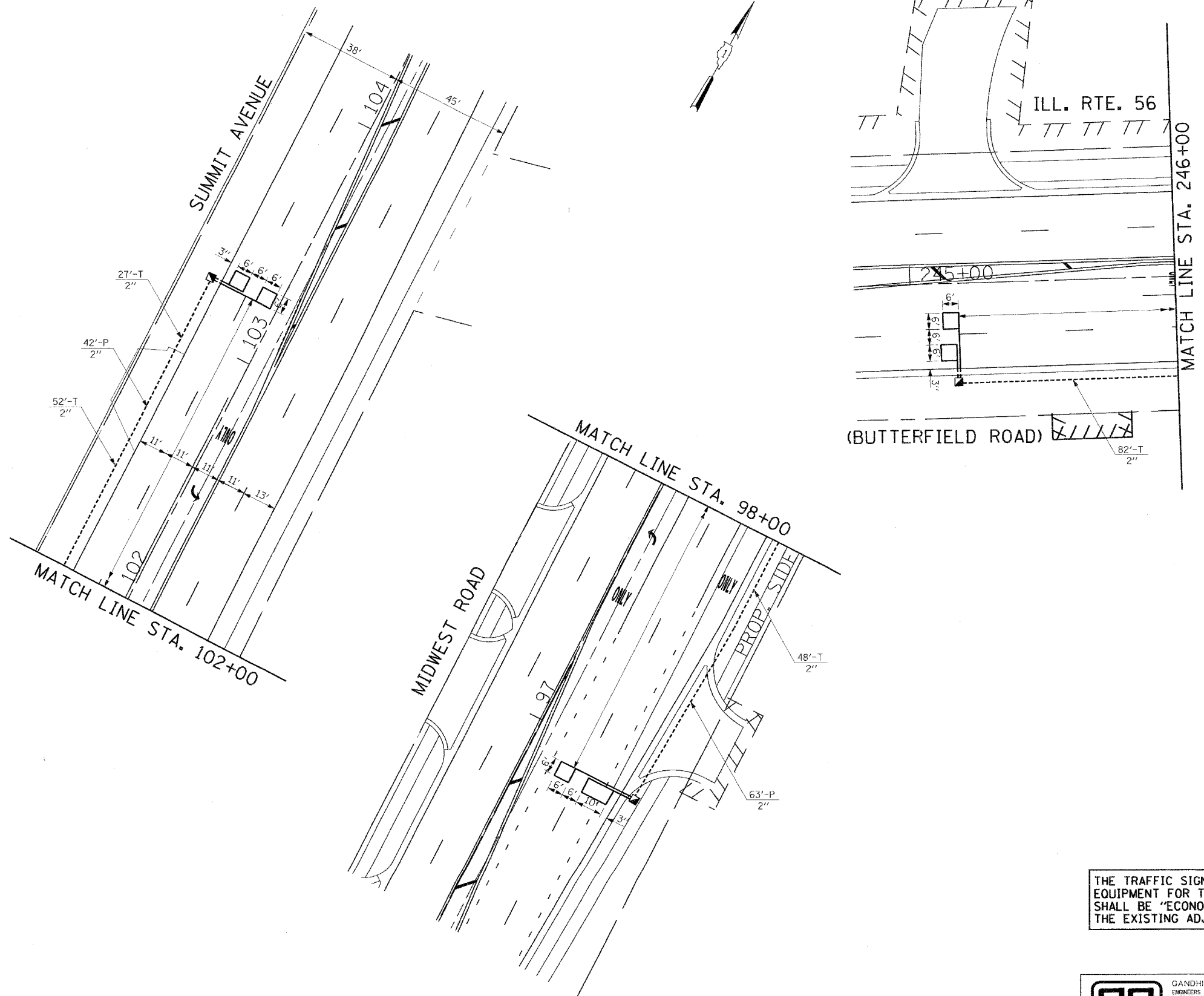
GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 305
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

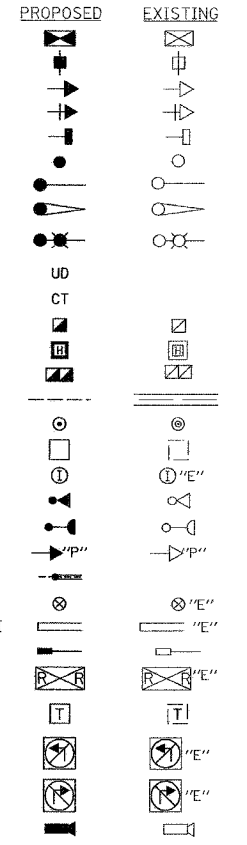
TRAFFIC SIGNAL MODERNIZATION
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT SUMMIT AVENUE-MIDWEST ROAD
SHEET 1 OF 2

DATE: 9/03/04
DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG



TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE
- UNIT DUCT
- COMMON TRENCH
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP
- CAST IRON JUNCTION BOX
- EMERGENCY VEHICLE SYSTEM DETECTOR
- CONFIRMATION BEACON
- SIGNAL HEAD, OPTICALLY PROGRAMMED
- CONDUIT SPLICE
- WOOD POLE
- RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
- VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
- RAILROAD CONTROL CABINET
- TELEPHONE CONNECTION
- ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
- ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
- MICROWAVE VEHICLE SENSOR



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6032 N. NORTHWEST HIGHWAY
SUITE 305
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL MODERNIZATION
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT SUMMIT AVENUE-MIDWEST ROAD
SHEET 2 OF 2

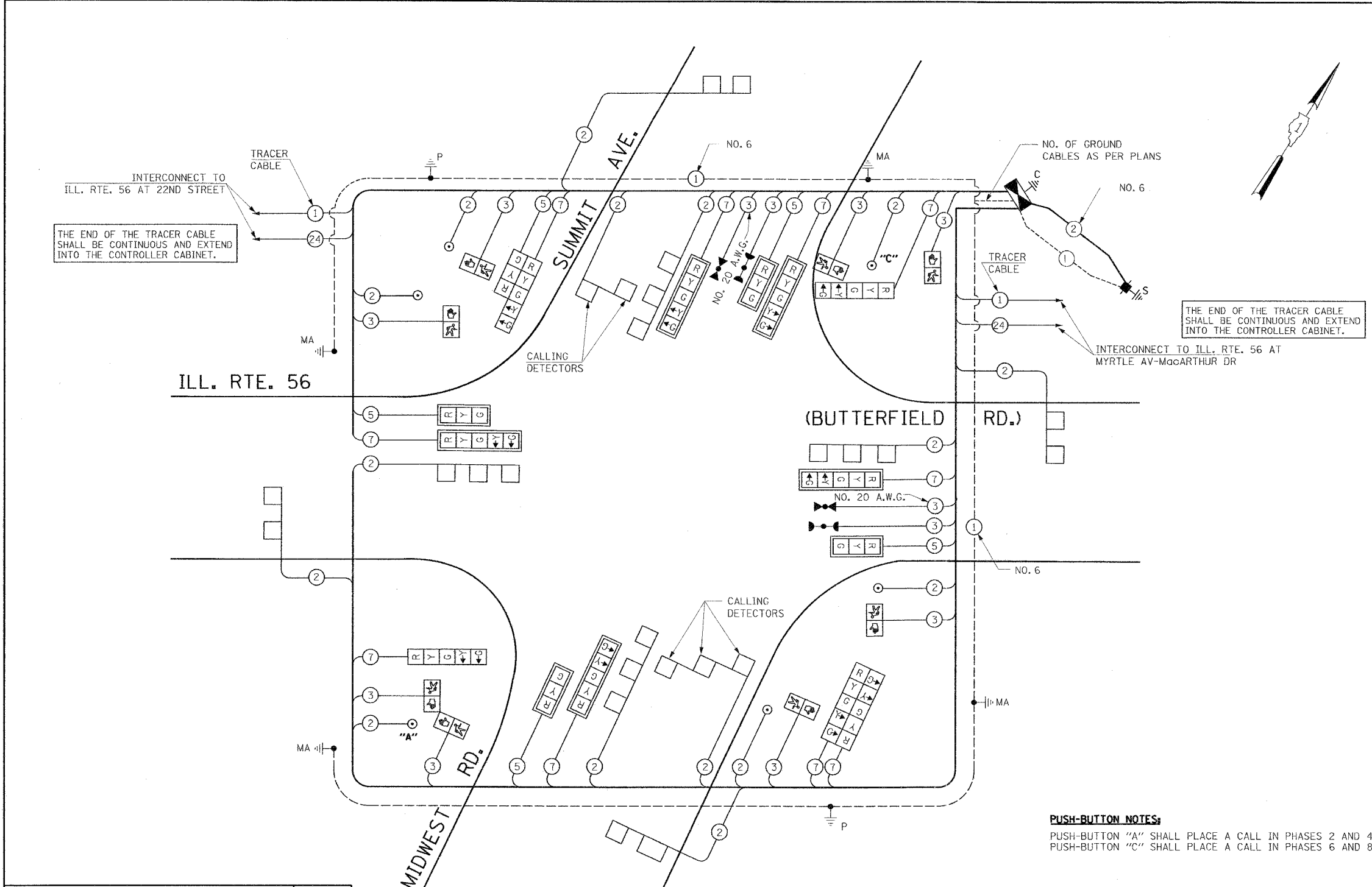
DATE: 9/03/04
DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG

CABLE PLAN LEGEND

- | EXISTING | PROPOSED |
|----------|---|
| | 8" (200mm) TRAFFIC SIGNAL SECTION |
| | 12" (300mm) TRAFFIC SIGNAL SECTION |
| | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | CONTROLLER CABINET |
| | SERVICE INSTALLATION |
| | TELEPHONE INSTALLATION |
| | VEHICLE DETECTOR, INDUCTION LOOP |
| | MAGNETIC DETECTOR |
| | EMERGENCY VEHICLE LIGHT DETECTOR |
| | CONFIRMATION BEACON |
| | PUSH-BUTTON DETECTOR |
| | DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) |
| | FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F & SM12F |
| | SIGNAL FACE WITH BACKPLATE. *P* INDICATES PROGRAMMED HEAD. |
| | RAILROAD CONTROL CABINET |
| | ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN" |
| | ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN" |
| | GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C) |
| | GROUND ROD AT POST (P) OR MAST ARM POLE (MA) |
| | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |
| | LOCAL AND MASTER CONTROLLER |
| | MICROWAVE VEHICLE SENSOR |

PUSH-BUTTON NOTES:
 PUSH-BUTTON "A" SHALL PLACE A CALL IN PHASES 2 AND 4.
 PUSH-BUTTON "C" SHALL PLACE A CALL IN PHASES 6 AND 8

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.



CABLE PLAN

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE		OPERATION	
		INCAND.	LED		
SIGNAL (RED)	15	135	17	0.50	127.50
(YELLOW)	15	135	25	0.25	93.75
(GREEN)	15	135	15	0.25	56.25
ARROW	20	135	12	0.10	24.00
PED. SIGNAL	8	90	25	1.00	200.00
CONTROLLER	1	100	100	1.00	100.00
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:					TOTAL =
ILLINOIS DEPARTMENT OF TRANSPORTATION 201 WEST CENTER COURT SCHAMBURG, ILLINOIS 60196-1096					601.50
ENERGY SUPPLY CONTACT: Robert Rogers PHONE: (630) 691-4861 COMPANY: ComEd-Glenbard					

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A-POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D-CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'-H-2'
E-MAST ARM POLE		SIGNAL POST	2 (1.0)	(6m+L-0.6m)=	
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)	BRACKET MOUNTED	13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
		ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CABLE PLAN
 ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
 AT SUMMIT AVENUE-MIDWEST ROAD
 SCALE: N.T.S.
 DATE: MAY 27, 2003
 DRAWN BY: JEK
 DESIGNED BY: PKG
 CHECKED BY: PKG

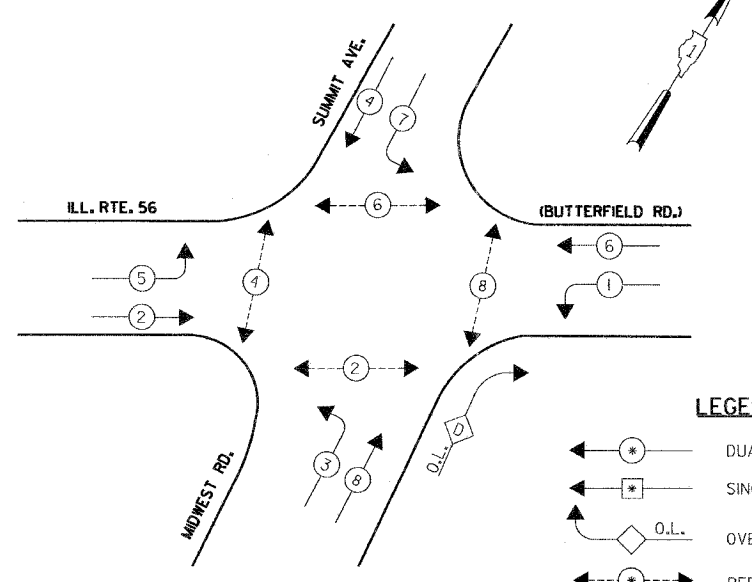
GO GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HWYWAY
 SUITE 306
 CHICAGO, ILLINOIS 60631 TEL. (773) 774-5980

SCHEDULE OF QUANTITIES

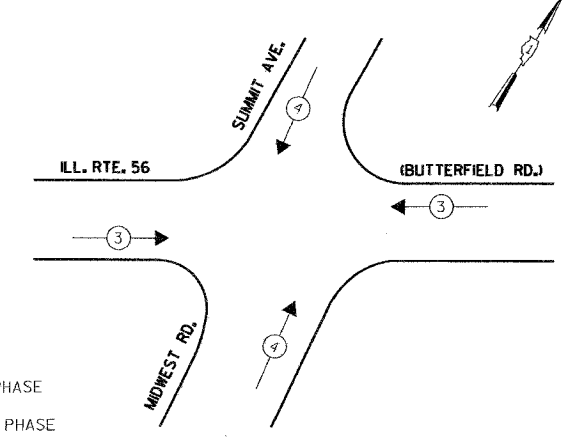
QUANTITY	UNIT	ITEM
24	SQ FT	SIGN PANEL - TYPE 1
25	SQ FT	SIGN PANEL - TYPE 2
835	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
114	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
18	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
10	FOOT	CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL
346	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
524	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
6	EACH	HANDHOLE
4	EACH	HEAVY-DUTY HANDHOLE
2	EACH	DOUBLE HANDHOLE
950	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	FOOT	TRANSCIVER-FIBER OPTIC
1532	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
1450	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
1152	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
2107	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2964	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
54	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
2	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 26 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 32 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 48 FT.
8	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE D
24	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
26	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
9	EACH	TRAFFIC SIGNAL BACKPLATE
10	EACH	INDUCTIVE LOOP DETECTOR
900	FOOT	DETECTOR LOOP, TYPE 1
2	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
6	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
8	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	EACH	SERVICE INSTALLATION, POLE MOUNT
708	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C (GREEN)
347	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED
4	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, BRACKET MOUNTED
5	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, L.E.D., 2-FACE, 5-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, L.E.D., 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, L.E.D., 1-FACE, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, L.E.D., 2-FACE, BRACKET MOUNTED

* 100% COST TO THE VILLAGE OF OAKBROOK TERRACE

CONTROLLER SEQUENCE



EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←	↑

LEGEND

- ← * → DUAL ENTRY PHASE
- ← * → SINGLE ENTRY PHASE
- ◊ O.L. OVERLAP
- ← * → PEDESTRIAN PHASE
- * NUMBER REFERS TO ASSOCIATED PHASE

PHASE DESIGNATION DIAGRAM

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
D	= 8	+ 1

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 305
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PHASE DESIGNATION DIAGRAM
EMERGENCY VEHICLE PREEMPTION SEQUENCE
AND SCHEDULE OF QUANTITIES
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT SUMMIT AVENUE-MIDWEST ROAD

SCALE: N.T.S.
DATE: SEPTEMBER 03, 2004

DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG

Rev.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	115
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT:		
D-91-386-01	CONTRACT NO. 62291			

NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12". HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON. IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

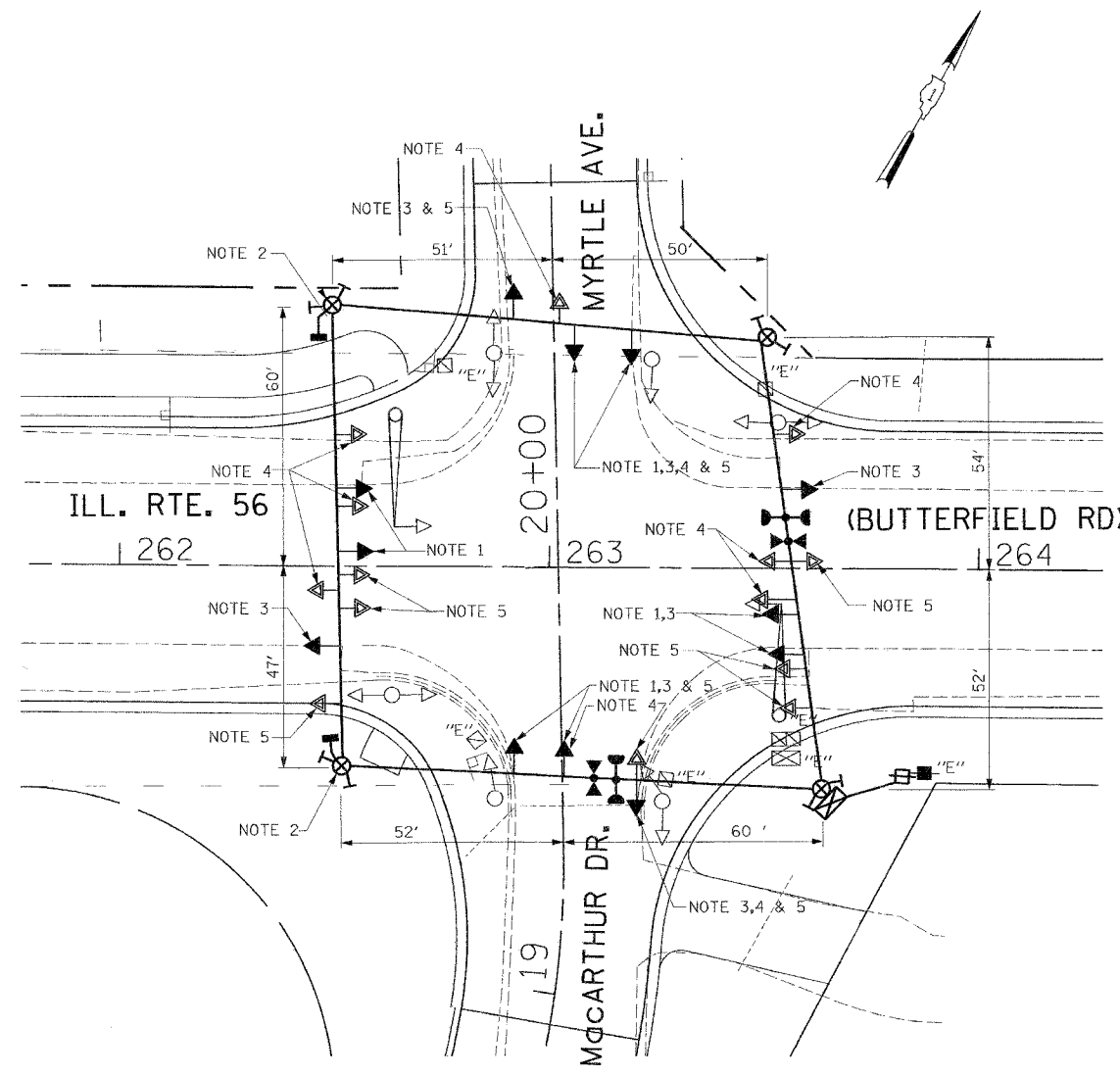
- 1 EACH CONTROLLER AND CABINET, COMPLETE

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED
- 2 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
- 4 EACH SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED
- 2 EACH TRAFFIC SIGNAL BACKPLATE
- 4 EACH TRAFFIC SIGNAL POST, 14 FT.
- 2 EACH ALUMINUM MAST ARM ASSEMBLY AND POLE
- 1 EACH SERVICE INSTALLATION

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE OAK BROOK TERRACE CITY AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- 1 EACH FIRE PRE-EMPTOR CP-100



TEMPORARY TRAFFIC SIGNAL LEGEND

- ← TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- ↖ TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- ⊗ TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- ⊠ TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- ⊕ TEMPORARY SERVICE INSTALLATION
- ⊔ TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- ⊙ MICROWAVE VEHICLE SENSOR
- ⊙ PEDESTRIAN PUSHBUTTON DETECTOR
- ⊙ EMERGENCY VEHICLE LIGHT DETECTOR
- ⊙ CONFIRMATION BEACON
- ⊙ VEHICLE DETECTOR, INDUCTION LOOP
- ⊔ COMMON TRENCH
- UD UNIT DUCT
- G.S. CONDUIT IN GROUND
- ⊔ HANDHOLE
- ⊔ HEAVY DUTY HANDHOLE

EXISTING EQUIPMENT TO BE REMOVED LEGEND

- ← EXISTING SIGNAL TO BE REMOVED
- ⊔ "E" EXISTING SERVICE INSTALLATION TO BE REMOVED
- ⊙ EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- ⊙ EXISTING MAST ARM POLE AND FOUNDATION TO BE REMOVED
- ⊔ "E" EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- ⊔ "E" EXISTING HANDHOLE TO BE REMOVED
- ⊔ "E" EXISTING DOUBLE HANDHOLE TO BE REMOVED
- ⊔ PEDESTRIAN SIGNAL TO BE REMOVED
- ⊙ EXISTING PEDESTRIAN PUSH-BUTTON TO BE REMOVED
- ⊙ EMERGENCY VEHICLE LIGHT DETECTOR TO BE REMOVED
- ⊙ CONFIRMATION BEACON TO BE REMOVED
- ⊔ "E" EXISTING HEAVY DUTY HANDHOLE TO BE REMOVED
- ⊙ EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED

STAGE CONSTRUCTION NOTES

- THE GREEN AND YELLOW LEFT TURN ARROW SECTIONS ARE TO BE BAGGED IN CONSTRUCTION STAGES 1, 2 & 3. IF THE ROADWAY IS OPENED TO TRAFFIC PRIOR TO THE PERMANENT SIGNAL BEING PLACED IN OPERATION THE GREEN AND YELLOW ARROW SECTION ARE TO BE UNBAGGED AND THE LEFT TURN PHASE ARE TO BE PLACED IN OPERATION AT THAT TIME.
- PEDESTRIAN SIGNAL HEADS ARE TO BE INSTALLED IF THE ROADWAY IS OPENED TO TRAFFIC AND ALL SIDEWALKS HAVE BEEN CONSTRUCTED PRIOR TO THE PERMANENT SIGNAL BEING PLACED IN OPERATION.
- SIGNAL HEAD LOCATION FOR CONSTRUCTION STAGE 1
- SIGNAL HEAD LOCATION FOR CONSTRUCTION STAGE 2. NORTH BOUND SIGNAL HEADS FOR MacARTHUR DR. ARE TO BE BAGGED AND DISCONNECTED AT THE CONTROLLER.
- SIGNAL HEAD LOCATIONS FOR CONSTRUCTION STAGE 3. NORTH BOUND SIGNAL HEADS ARE TO BE BAGGED AND DISCONNECTED AT THE CONTROLLER.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

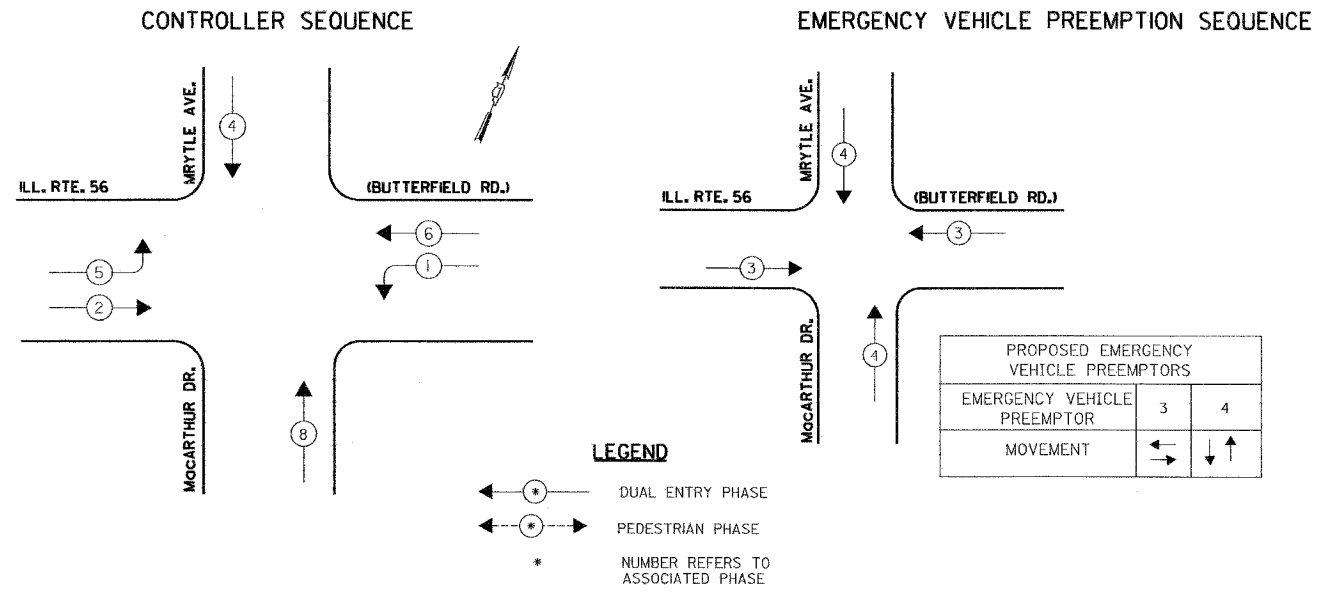
TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN

ILLINOIS ROUTE 56 (BUTTERFIELD ROAD) AT MYRTLE AVENUE-MacARTHUR DRIVE

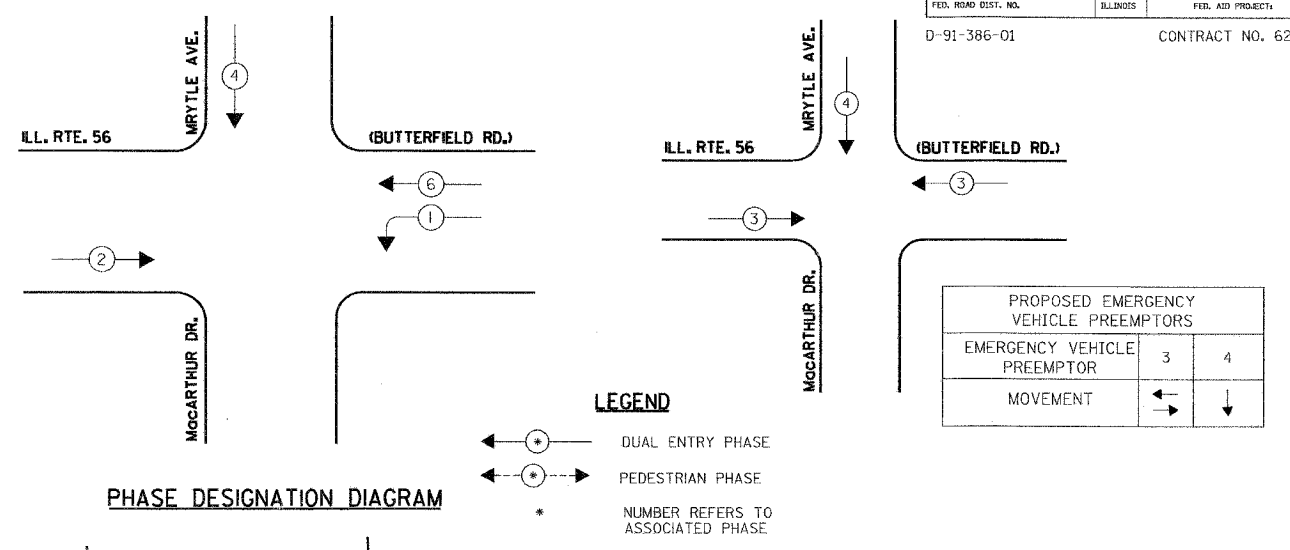
DATE: 9/03/04
DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
3545	54 WRS-7	DUPAGE	235	116
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
D-91-386-01		CONTRACT NO. 62291		

**CONSTRUCTION STAGE 1 AND PRIOR TO PERMANENT SIGNAL OPERATION
WHEN NEW PAVEMENT IS OPENED TO TRAFFIC**



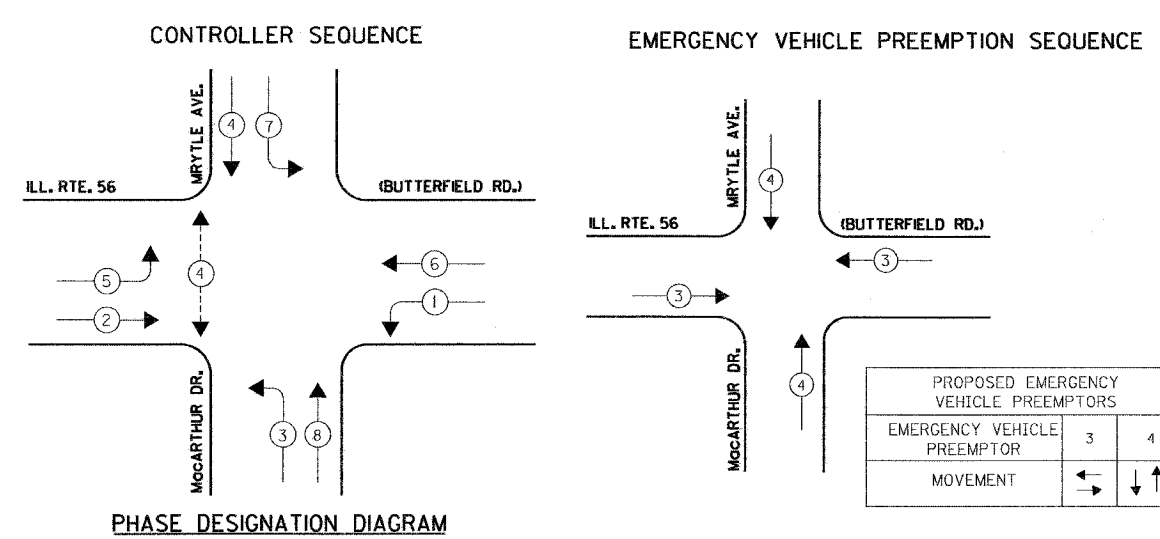
CONSTRUCTION STAGES 2 & 3



PROPOSED EMERGENCY VEHICLE PREEMPTORS	
EMERGENCY VEHICLE PREEMPTOR	3 4
MOVEMENT	←→ ↑↓

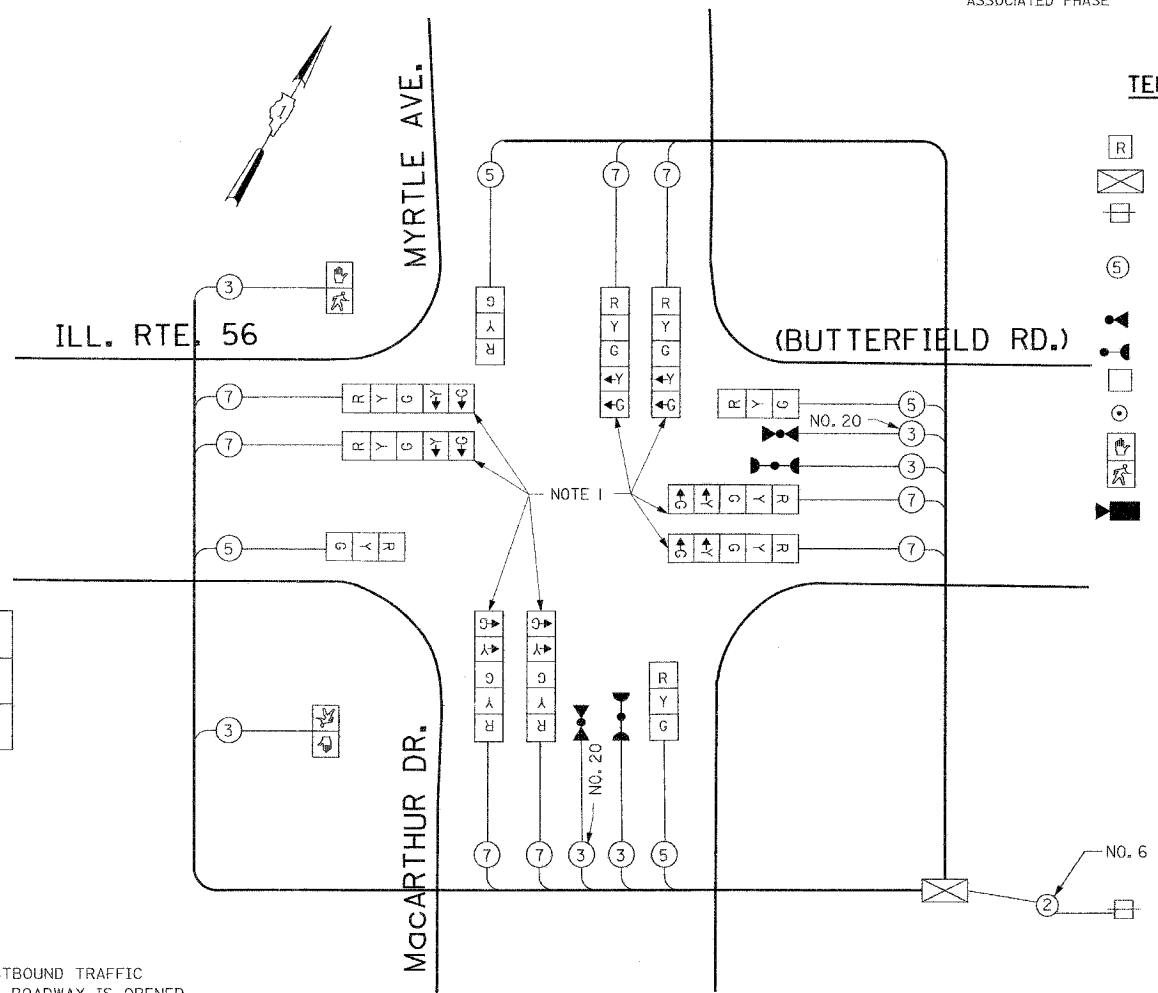
PHASE DESIGNATION DIAGRAM

TEMPORARY SIGNAL PRIOR TO PERMANENT SIGNAL



TEMPORARY CABLE DIAGRAM LEGEND

- TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- TEMPORARY CONTROLLER CABINET
- TEMPORARY SERVICE INSTALLATION
- INDICATES NUMBER OF CONDUCTORS IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- PEDESTRIAN PUSHBUTTON DETECTOR
- 12" (300mm) PEDESTRIAN SIGNAL SECTION
- MICROWAVE VEHICLE SENSOR



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

STAGE CONSTRUCTION NOTE

- THE GREEN AND YELLOW LEFT TURN ARROW SECTIONS FOR EASTBOUND TRAFFIC ARE TO BE BAGGED IN CONSTRUCTION STAGES 2 & 3. IF THE ROADWAY IS OPENED TO TRAFFIC PRIOR TO THE PERMANENT SIGNAL BEING PLACED IN OPERATION THE GREEN AND YELLOW ARROW SECTION ARE TO BE UNBAGGED AND THE LEFT TURN PHASE ARE TO BE PLACE IN OPERATION AT THAT TIME.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE		OPERATION	
		INCAND.	LED		
SIGNAL (RED)	12	135	17	0.50	810.00
(YELLOW)	12	135	25	0.25	405.00
(GREEN)	12	135	15	0.25	405.00
ARROW	16	135	12	0.10	216.00
PED. SIGNAL	2	90	25	1.00	180.00
CONTROLLER	1	100	100	1.00	100.00
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:					TOTAL = 2116.00

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

ENERGY SUPPLY CONTACT: Robert Rogers
PHONE: (630) 691-4861
COMPANY: ComEd-Glenbard

GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 300
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**TEMPORARY CABLE PLAN
PHASE DESIGNATION DIAGRAM
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT MYRTLE AVENUE-McARTHUR DRIVE**

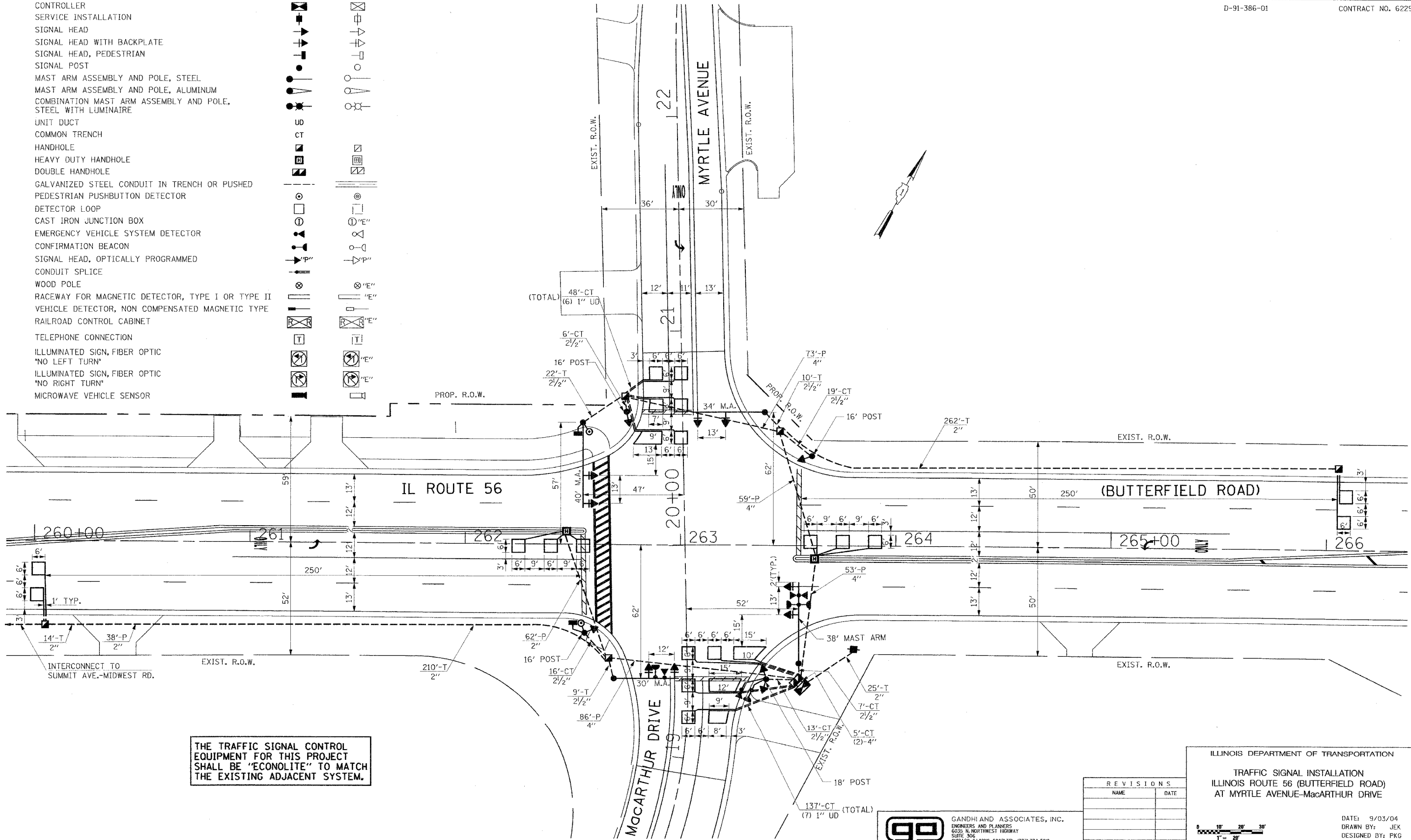
SCALE: N.T.S.
DATE: SEPTEMBER 03, 2004

DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	117
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT:		
D-91-386-01	CONTRACT NO. 62291			

TRAFFIC SIGNAL LEGEND

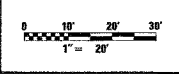
- | | | |
|--|-----------------|-----------------|
| | PROPOSED | EXISTING |
| CONTROLLER | | |
| SERVICE INSTALLATION | | |
| SIGNAL HEAD | | |
| SIGNAL HEAD WITH BACKPLATE | | |
| SIGNAL HEAD, PEDESTRIAN | | |
| SIGNAL POST | | |
| MAST ARM ASSEMBLY AND POLE, STEEL | | |
| MAST ARM ASSEMBLY AND POLE, ALUMINUM | | |
| COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE | | |
| UNIT DUCT | | |
| COMMON TRENCH | | |
| HANDHOLE | | |
| HEAVY DUTY HANDHOLE | | |
| DOUBLE HANDHOLE | | |
| GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED | | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | |
| DETECTOR LOOP | | |
| CAST IRON JUNCTION BOX | | |
| EMERGENCY VEHICLE SYSTEM DETECTOR | | |
| CONFIRMATION BEACON | | |
| SIGNAL HEAD, OPTICALLY PROGRAMMED | | |
| CONDUIT SPLICE | | |
| WOOD POLE | | |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II | | |
| VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE | | |
| RAILROAD CONTROL CABINET | | |
| TELEPHONE CONNECTION | | |
| ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN" | | |
| ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN" | | |
| MICROWAVE VEHICLE SENSOR | | |



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC SIGNAL INSTALLATION
 ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
 AT MYRTLE AVENUE-MacARTHUR DRIVE



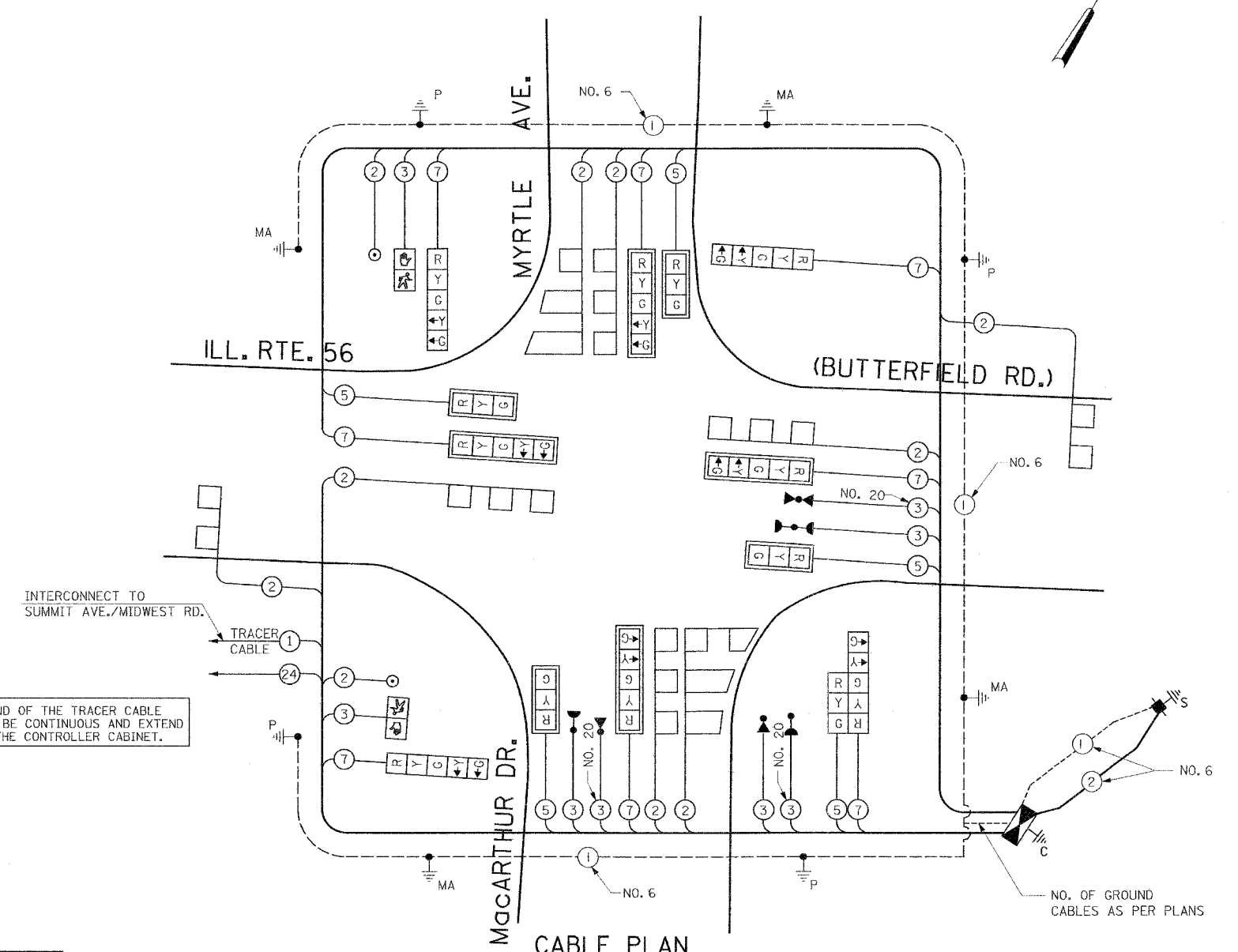
DATE: 9/03/04
 DRAWN BY: JEK
 DESIGNED BY: PKG
 CHECKED BY: PKG

ga GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 6035 N. NORTHWEST HIGHWAY
 SUITE 505
 CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	118
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
D-91-386-01		CONTRACT NO. 62291		

CABLE PLAN LEGEND

- | EXISTING | PROPOSED | |
|----------|----------|---|
| | | 8" (200mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) TRAFFIC SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION |
| | | CONTROLLER CABINET |
| | | SERVICE INSTALLATION |
| | | TELEPHONE INSTALLATION |
| | | VEHICLE DETECTOR, INDUCTION LOOP |
| | | MAGNETIC DETECTOR |
| | | EMERGENCY VEHICLE LIGHT DETECTOR |
| | | CONFIRMATION BEACON |
| | | PUSH-BUTTON DETECTOR |
| | | DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED, ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | | GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) |
| | | FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F & SM12F |
| | | SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD. |
| | | RAILROAD CONTROL CABINET |
| | | ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN" |
| | | ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN" |
| | | GROUND ROD AT HANDHOLE, DOUBLE HANDHOLE, OR CONTROLLER |
| | | GROUND ROD AT POST OR MAST ARM POLE |
| | | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |
| | | LOCAL AND MASTER CONTROLLER |
| | | MICROWAVE VEHICLE SENSOR |



THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO LAMPS	WATTAGE		%OPERATION	
SIGNAL (RED)	13	135	17	0.50	110.5
(YELLOW)	13	135	25	0.25	81.5
(GREEN)	13	135	15	0.25	48.75
ARROW	16	135	12	0.10	19.2
PED. SIGNAL	2	90	25	1.00	50.0
CONTROLLER	1	100	100	1.00	100.0
ILLUM. SIGN				0.05	
FLASHER				0.50	
ENERGY COSTS TO:					TOTAL = 409.95

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196-1096

ENERGY SUPPLY CONTACT: Robert Rogers
PHONE: (630) 691-4861
COMPANY: ComEd-Glenbard

FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A-POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
D-CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'-H-2'
F-M.ARM POLE		SIGNAL POST	2 (1.0)	BRACKET MOUNTED	(6m-HL-0.6m)=
24" (600mm)	10 (3.0)	CONTROLLER CAB.	1 (0.5)		13 (4.0)
30" (750mm)	15 (4.6)	FIBER OPTIC	13 (4.0)	PED. PUSHBUTTON	4 (1.2)
		ELECTRIC SERVICE	1 (0.5)	ELECTRIC SERVICE	13.5 (4.1)
		GROUND CABLE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
				POST MOUNTED	6 (1.8)

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ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 100
CHICAGO, ILLINOIS 60631 TEL. (773) 774-590

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

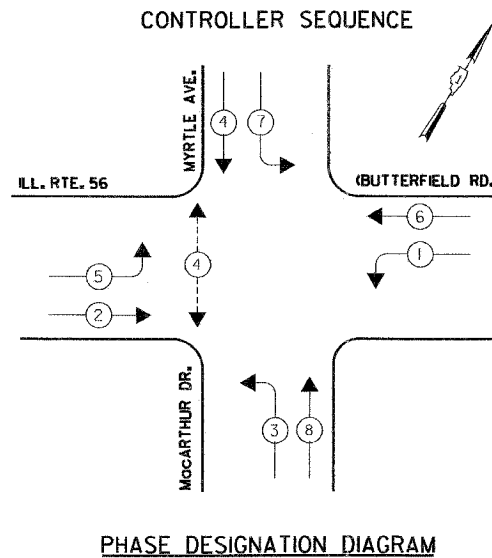
CABLE PLAN
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT MYRTLE AVENUE-McARTHUR DRIVE

SCALE: N.T.S. DRAWN BY: JEK
DATE: SEPTEMBER 03, 2004 DESIGNED BY: PKG
CHECKED BY: PKG

\$FILE\$
\$DATE\$
\$TIME\$

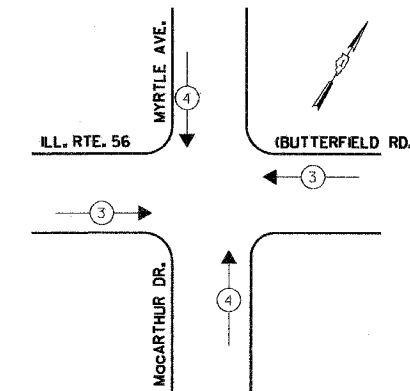
QUANTITY	UNIT	ITEM
24	SQ FT	SIGN PANEL - TYPE 1
25	SQ FT	SIGN PANEL - TYPE 2
511	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
102	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
10	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL
95	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
269	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL
5	EACH	HANDHOLE
2	EACH	HEAVY-DUTY HANDHOLE
1	EACH	DOUBLE HANDHOLE
557	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL
1	FOOT	TRANSCEIVER-FIBER OPTIC
259	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
723	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
946	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
1434	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1381	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
44	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C
3	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 18 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 30 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.
16	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE D
42	FOOT	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER
13	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
8	EACH	TRAFFIC SIGNAL BACKPLATE
8	EACH	INDUCTIVE LOOP DETECTOR
845	FOOT	DETECTOR LOOP, TYPE 1
3	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
2	EACH	PEDESTRIAN PUSH-BUTTON
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
7	EACH	REMOVE EXISTING HANDHOLE
9	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	EACH	SERVICE INSTALLATION, POLE MOUNT
522	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C (GREEN)
310	FOOT	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED
4	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED
3	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, BRACKET MOUNTED
4	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, L.E.D., 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, L.E.D., 1-FACE, BRACKET MOUNTED

* 100% COST TO THE CITY OF OAKBROOK TERRACE

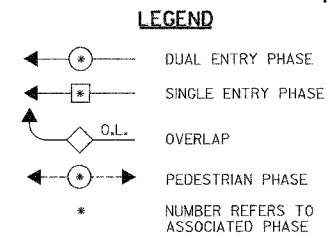


CONTROLLER SEQUENCE

EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↑ ↓



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

Ret. **GO** GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 300
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

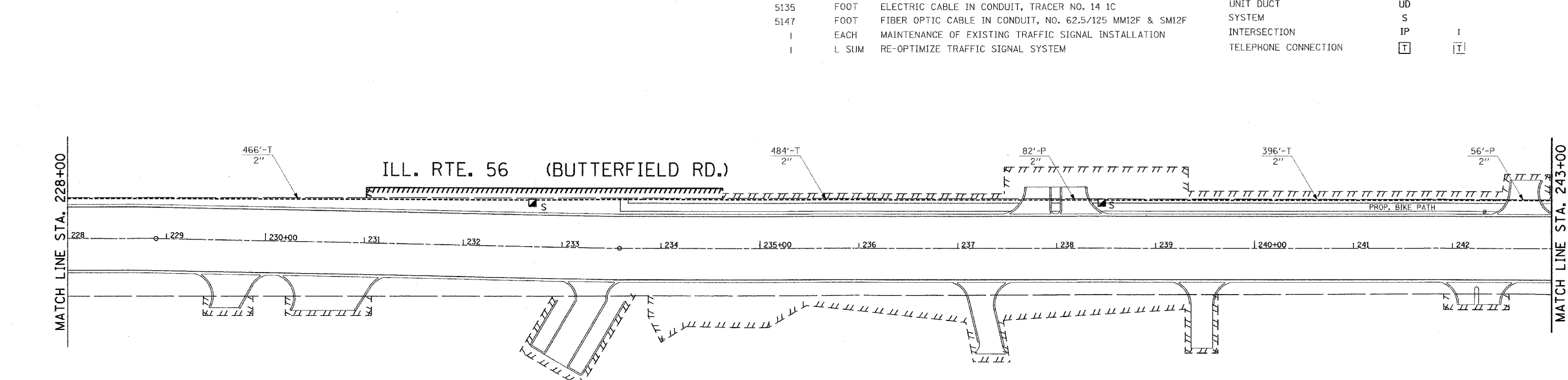
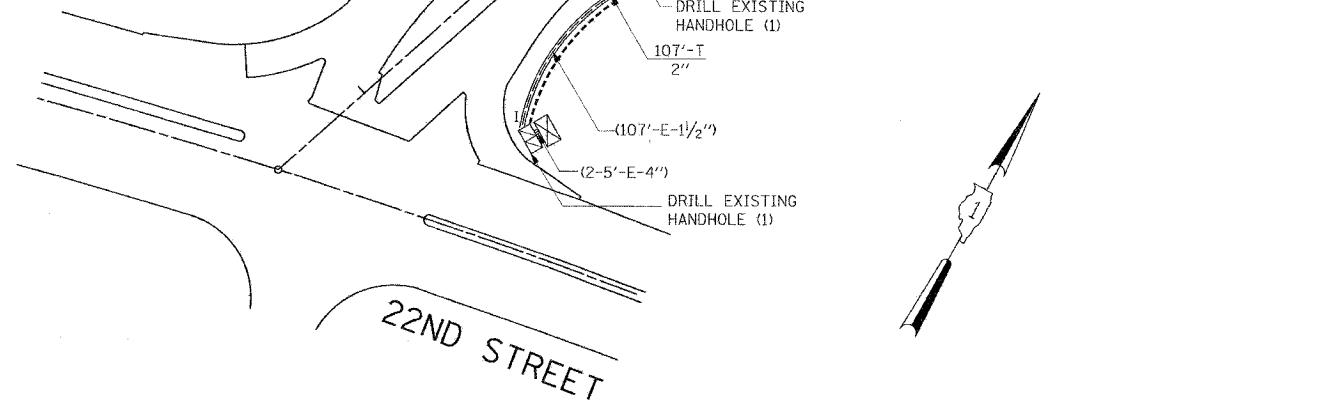
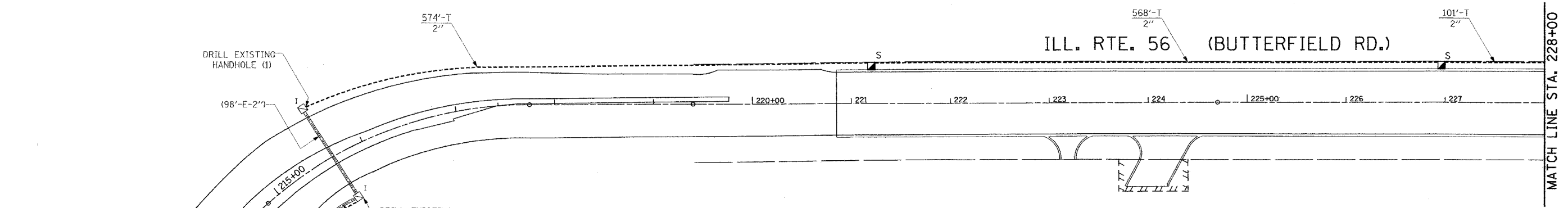
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
PHASE DESIGNATION DIAGRAM
EMERGENCY VEHICLE PREEMPTION SEQUENCE
AND SCHEDULE OF QUANTITIES
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT MYRTLE AVENUE-MacARTHUR DRIVE

SCALE: N.T.S.
DATE: SEPTEMBER 03, 2004

DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	120
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
D-91-386-01	CONTRACT NO. 62291			



SCHEDULE OF INTERCONNECT QUANTITIES

QUANTITY	UNIT	ITEM
3702	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
514	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
7	EACH	HANDHOLE
3702	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
3	EACH	DRILL EXISTING HANDHOLE
5135	FOOT	ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1C
5147	FOOT	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 MM12F & SM12F
1	EACH	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION
1	L SUM	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM

INTERCONNECT PLAN LEGEND

	PROPOSED	EXISTING
CONTROLLER		
HANDHOLE		
DOUBLE HANDHOLE		
HEAVY DUTY HANDHOLE		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
COMMON TRENCH	CT	
UNIT DUCT	UD	
SYSTEM	S	
INTERSECTION	IP	I
TELEPHONE CONNECTION		

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

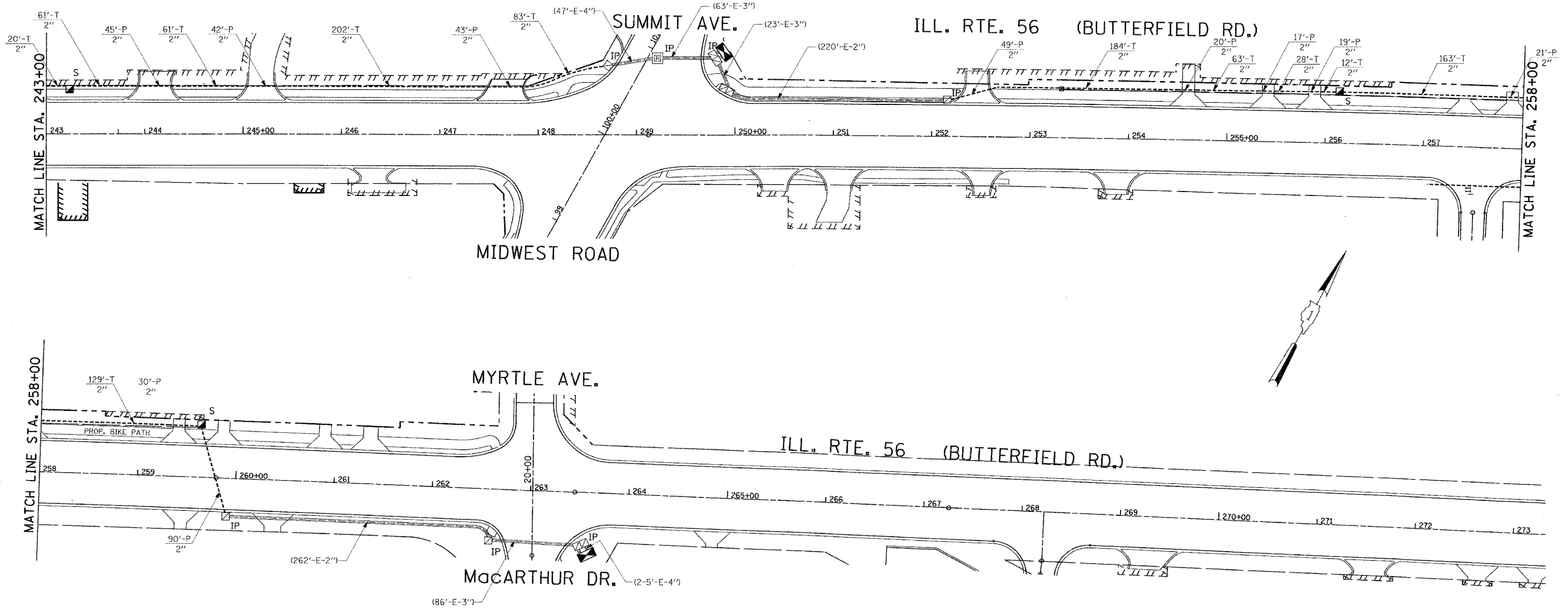
INTERCONNECT PLAN
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT SUMMIT AVENUE-MIDWEST ROAD
SHEET 1 OF 2

DATE: 9/03/04
DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG

0 25' 50' 75'
1" = 50'

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ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60639 TEL. (773) 774-5900

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	121
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
D-91-386-01		CONTRACT NO. 62291		



INTERCONNECT PLAN LEGEND

	PROPOSED	EXISTING
CONTROLLER		
HANDHOLE		
DOUBLE HANDHOLE		
HEAVY DUTY HANDHOLE		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
COMMON TRENCH	CT	
UNIT DUCT	UD	
SYSTEM	S	
INTERSECTION	IP	I
TELEPHONE CONNECTION	T	T

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

INTERCONNECT PLAN
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)
AT SUMMIT AVENUE-MIDWEST ROAD
SHEET 2 OF 2

DATE: 9/03/04
DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG





























GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 ILNORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5980

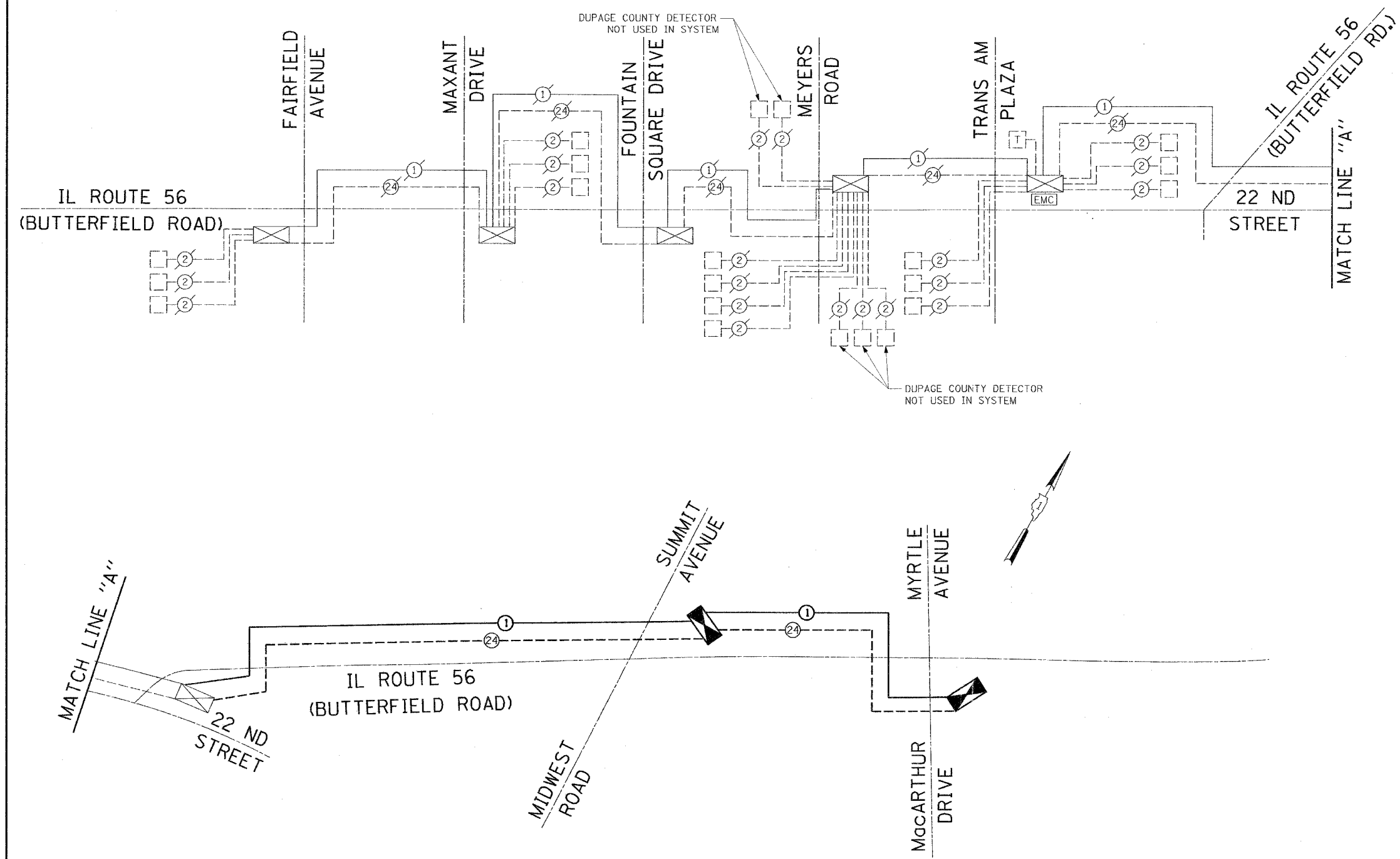
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\$TIME\$

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	122
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

D-91-386-01 CONTRACT NO. 62291

INTERCONNECT SCHEMATIC LEGEND

-  EXISTING INTERSECTION CONTROLLER
-  PROPOSED INTERSECTION CONTROLLER
-  EXISTING MASTER CONTROLLER
-  PROPOSED MASTER CONTROLLER
-  MASTER MASTER CONTROLLER
-  EXISTING INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  EXISTING INTERSECTION LOOP DETECTORS AND PROPOSED SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) DETECTORS
-  PROPOSED SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) DETECTORS, PROPOSED INTERSECTION & SAMPLING (SYSTEM) DETECTORS.
-  EXISTING SAMPLING (SYSTEM) DETECTORS, PROPOSED SAMPLING (SYSTEM) DETECTORS.
-  EXISTING PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  PROPOSED PREFORMED INTERSECTION & SAMPLING (SYSTEM) DETECTORS
-  EXISTING SAMPLING (SYSTEM) PREFORMED DETECTORS.
-  PROPOSED SAMPLING (SYSTEM) PREFORMED DETECTORS.
-  EXISTING FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F & SM12F
-  PROPOSED FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 MM12F & SM12F
-  EXISTING INTERCONNECT CABLE - NO. 62.5/125 12F. FIBER OPTIC CABLE
-  PROPOSED INTERCONNECT CABLE - NO. 62.5/125 12F. FIBER OPTIC CABLE
-  EXISTING INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
-  PROPOSED INTERCONNECT CABLE - NO. 18 3 PAIR TWISTED, SHIELDED
-  EXISTING LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
-  PROPOSED LOOP DETECTOR CABLE - 2/C TWISTED, SHIELDED
-  EXISTING ELECTRIC CABLE 1/C (AS SPECIFIED)
-  PROPOSED ELECTRIC CABLE, 1/C (AS SPECIFIED)
-  EXISTING TELEPHONE CONNECTION
-  PROPOSED TELEPHONE CONNECTION



THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

GO GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL: (773) 774-5910

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

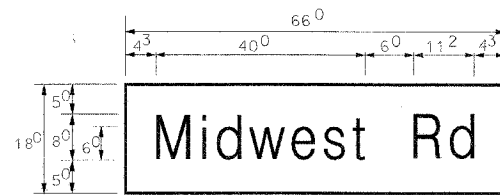
INTERCONNECT SCHEMATIC
ILLINOIS ROUTE 56 (BUTTERFIELD ROAD)

SCALE: N.T.S.

DATE: SEPTEMBER 03, 2004

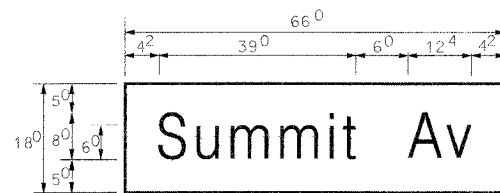
DRAWN BY: JEK
DESIGNED BY: PKG
CHECKED BY: PKG

PANEL SIGN DESIGN TYPE 1



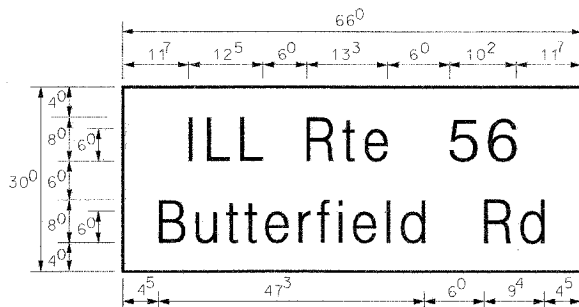
- Sq. M. each
6.0 Sq. Ft. each
2 Required
Design Series D

PANEL SIGN DESIGN TYPE 1



- Sq. M. each
6.0 Sq. Ft. each
2 Required
Design Series D

PANEL SIGN DESIGN TYPE 2



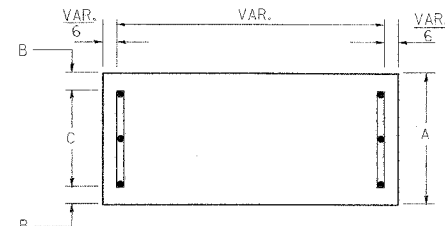
- Sq. M. each
12.5 Sq. Ft. each
4 Required
Design Series C

NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

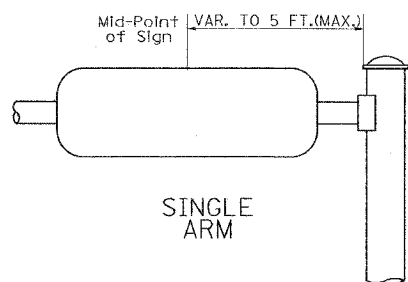
GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 834001, 834006 AND 834011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" X 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
 - ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
 - THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
 - ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
 - SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:
* A.K.T. CORPORATION (SCHAUMBURG, IL)
* TUCKER COMPANY, INC. (WAUWATOSA, WI)
* AMERICAN FABRICATION CO. (CHICAGO HEIGHTS, IL)
* WESTERN TRAFFIC CONTROL INC. (CICERO, IL)
- PARTS LISTING:
SIGN CHANNEL: PART #HPN053 (MED. CHANNEL)
SIGN SCREWS: 1/4" X 14 X 1" H.W.H. #3
BRACKETS: PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING
OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

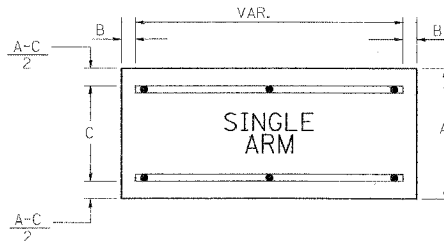
SUPPORTING CHANNELS



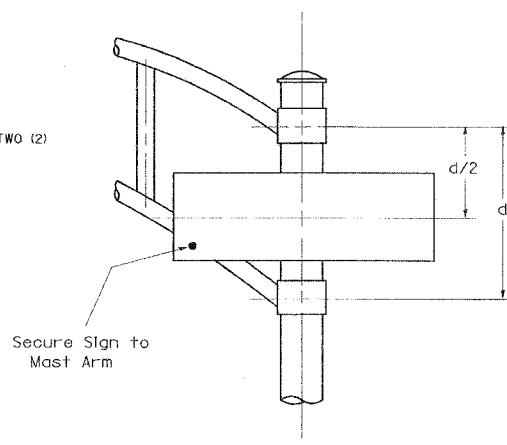
A	B	C
18"	2"	14"



SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"



DUAL ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM
Shall be used. See Note #5.

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

EXAMPLE, 2 DENOTES 3/B

SERIES	SECOND LETTER																	
	acde				bhikl				f w		j		s t		v y		x z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14		
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17		
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15		
D O Q R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15		
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12		
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21		
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21		
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14		
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14		
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14		
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14		
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14		
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12		
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21		

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER																	
	acde				bhikl				f w		j		s t		v y		x z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
ad h g i j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17		
l m n q u																		
bf k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14		
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14		
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10		
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14		
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12		
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14		
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14		

Number To Number
Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21
2 3 4	14	15	14	15	14	15	12	14	12	14	14	15	14	15	11	12	16	17	14	15
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15
6	16	17	14	15	14	15	12	14	14	15	14	15	14	15	11	12	14	15	14	15
7	12	14	12	14	14	15	12	14	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	14	14	15	16	17	12	14	14	15	16	17	14	15

UPPER AND LOWER CASE
LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS		8 INCH UPPER CASE LETTERS		LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES			SERIES	
	C	D	C	D		C	D
A	36	50	50	65	a	35	42
B	32	40	43	53	b	35	42
C	32	40	43	53	c	35	41
D	32	40	43	53	d	35	42
E	30	35	40	47	e	35	42
F	30	35	40	47	f	23	26
G	32	40	43	53	g	35	42
H	32	40	43	53	h	35	42
I	07	07	11	12	i	11	11
J	30	36	40	50	j	20	22
K	32	41	43	54	k	35	42
L	30	35	40	47	l	11	11
M	37	45	51	61	m	60	70
N	32	40	43	53	n	35	42
O	34	42	45	55	o	36	43
P	32	40	43	53	p	35	42
Q	34	42	45	55	q	35	42
R	32	40	43	53	r	26	32
S	32	40	43	53	s	36	42
T	30	35	40	47	t	27	32
U	32	40	43	53	u	35	42
V	35	44	47	60	v	42	47
W	44	52	60	70	w	55	64
X	34	40	45	53	x	44	51
Y	36	50	50	66	y	46	53
Z	32	40	43	53	z	36	43

NUMBER	6 INCH SERIES		8 INCH SERIES	
	SERIES		SERIES	
	C	D	C	D
1	12	14	15	20
2	32	40	43	53
3	32	40	43	53
4	35	43	47	57
5	32	40	43	53
6	32	40	43	53
7	32	40	43	53
8	32	40	43	53
9	32	40	43	53
0	34	42	45	55

REVISIONS	
NAME	DATE
D.A.Z./D.A.G.	11/90
	6/98
CADD	10/00

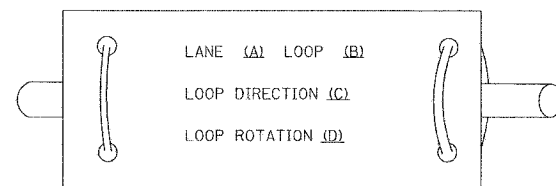
Illinois Department of Transportation
DISTRICT 1
MAST ARM MOUNTED STREET NAME SIGNS

SCALE: NONE
DATE: **DATE**
DRAWN BY: RBG
DESIGNED BY: JHE
CHECKED BY: JAD

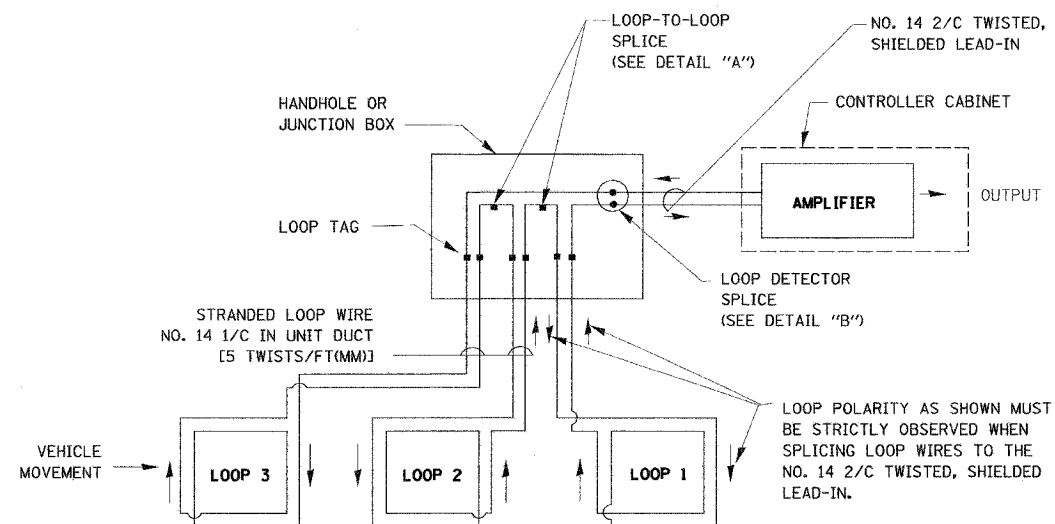
LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

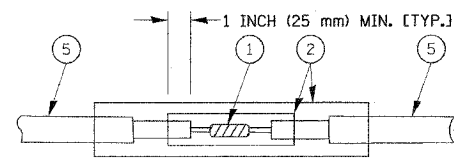


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

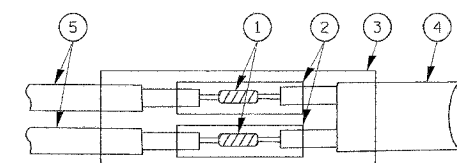


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 STANDARD TRAFFIC SIGNAL
 DESIGN DETAILS

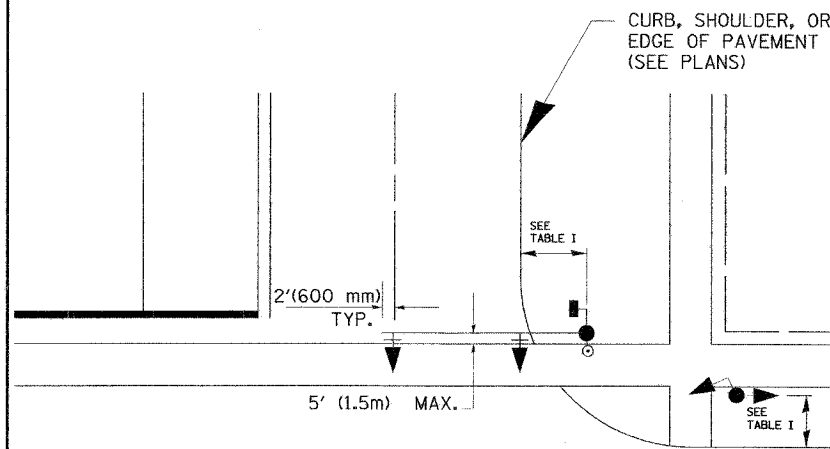
VERT. NONE
 SCALE: HORIZ.
 DATE 1-01-02

DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 1 OF 4

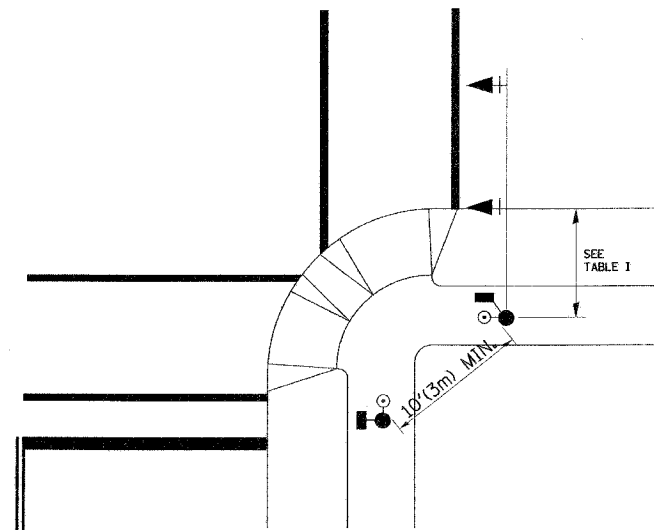
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	126
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
D-91-386-01		CONTRACT NO. 62291		

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.
AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.
PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK.
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

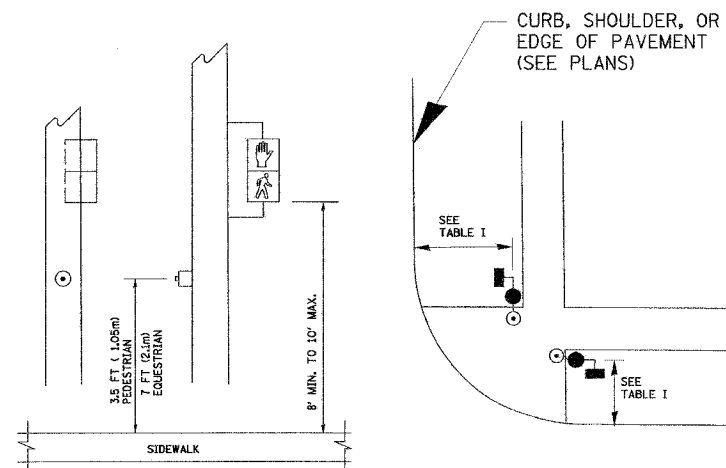


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

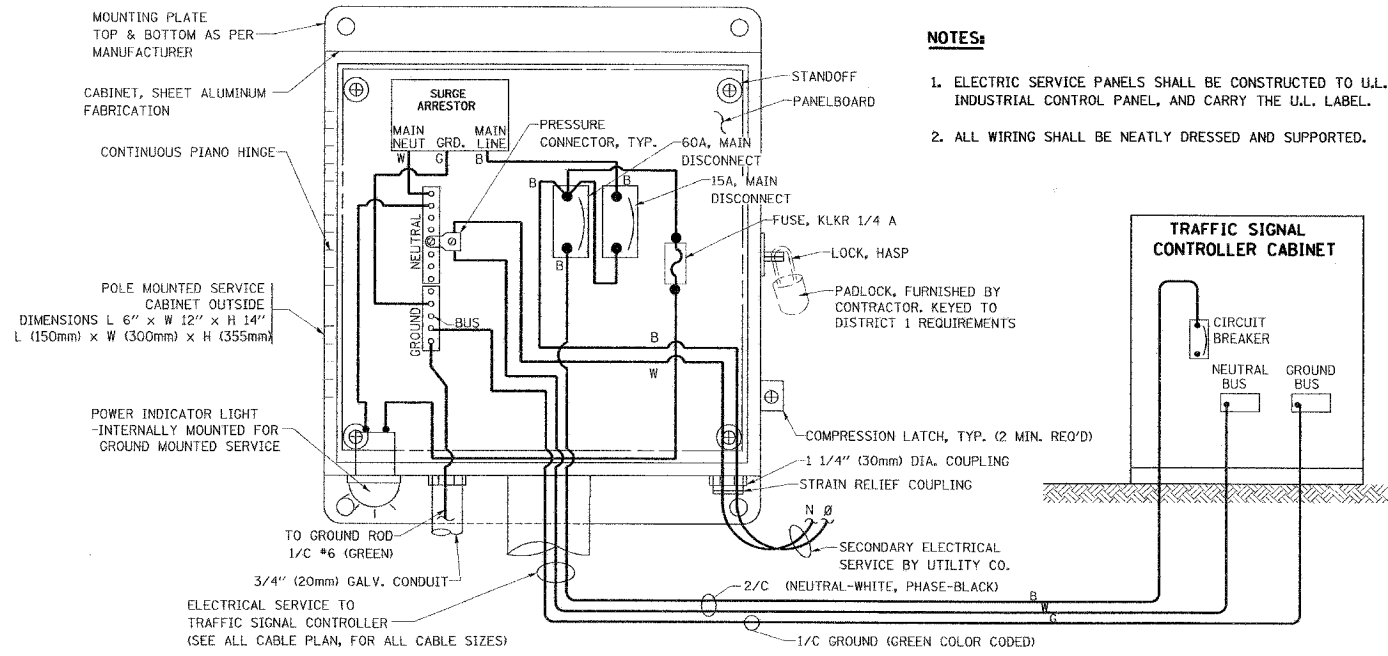
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

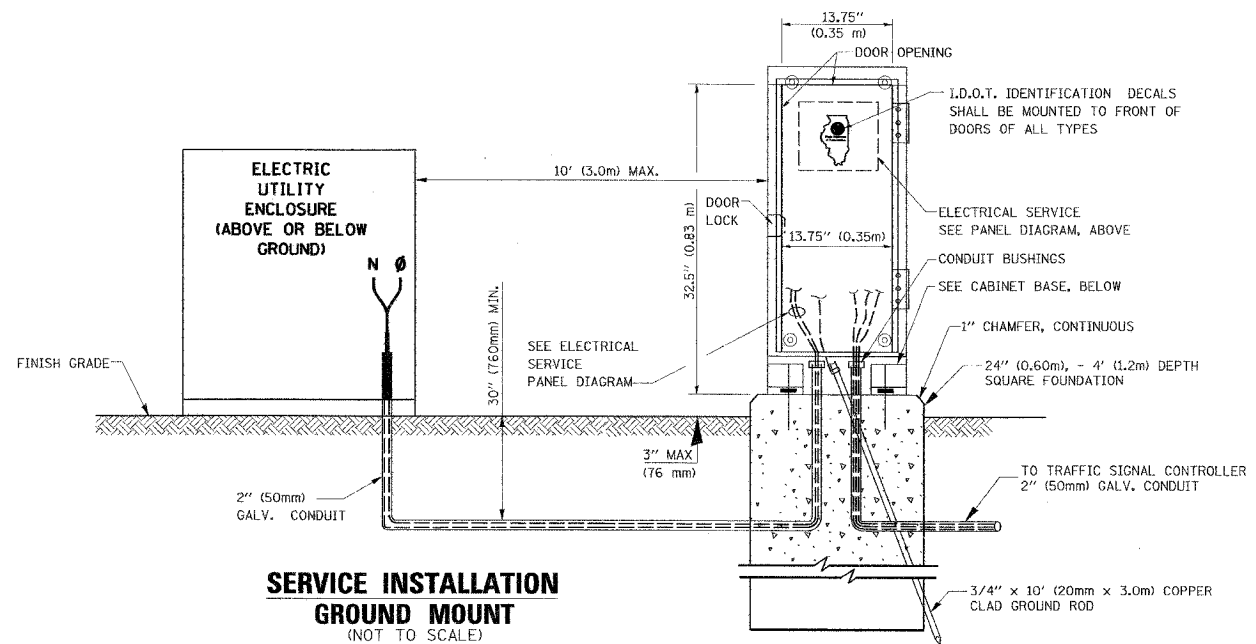
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HORIZ. NONE
DATE 1-01-02

DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 2 OF 4

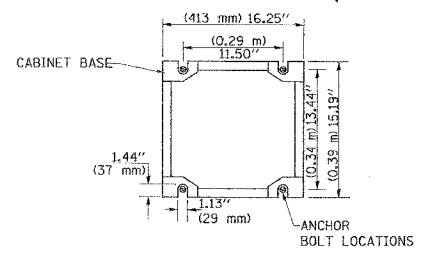
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STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
D-91-386-01		CONTRACT NO. 62291		



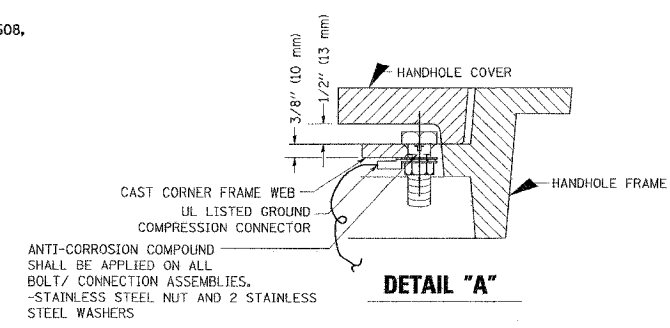
ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



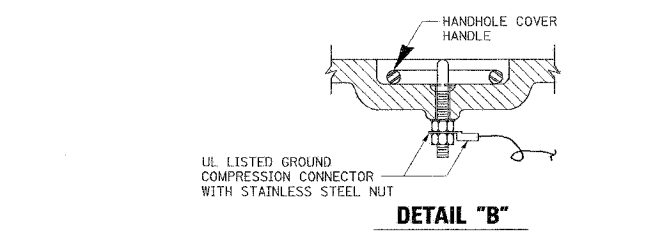
SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)



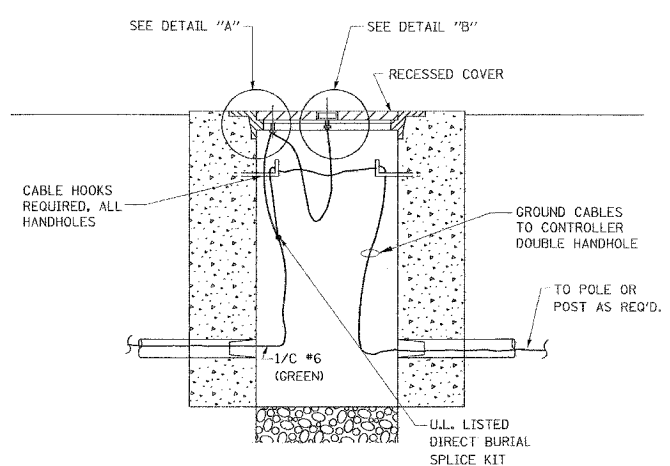
CABINET - BASE BOLT PATTERN
 (NOT TO SCALE)



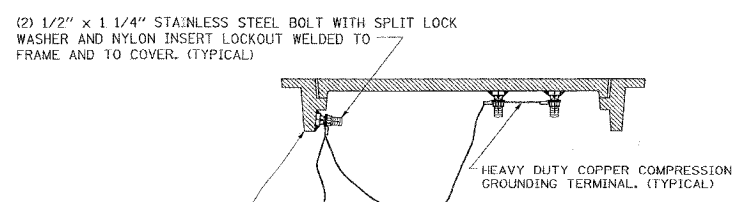
DETAIL "A"



DETAIL "B"



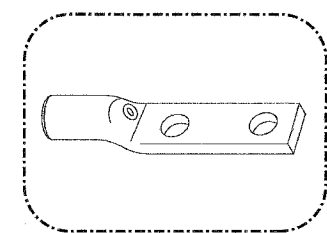
HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)



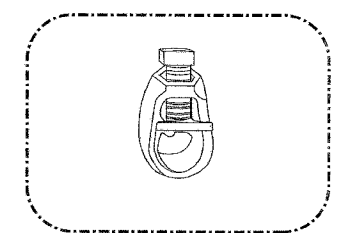
EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)

NOTES:
GROUNDING SYSTEM

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



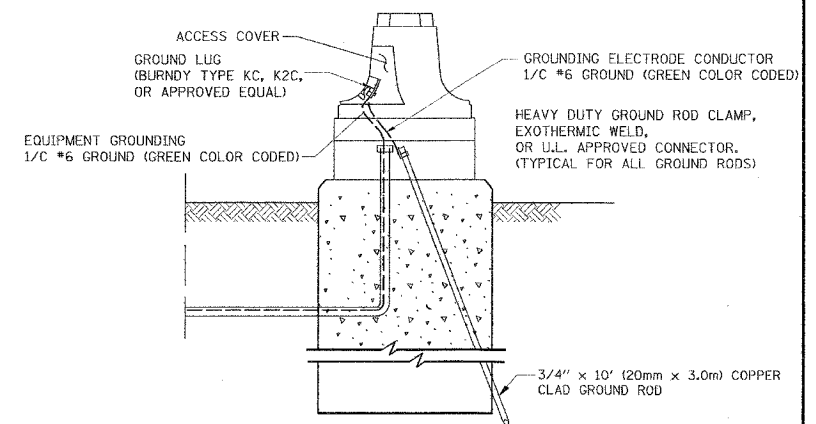
HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YCHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



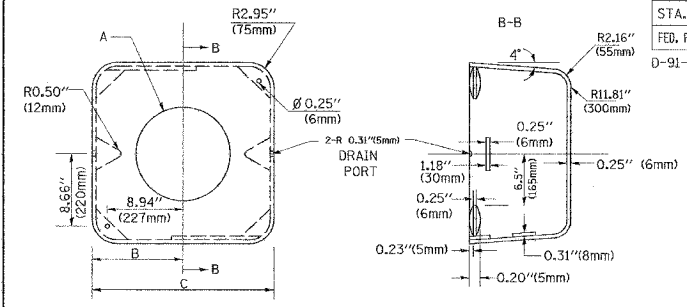
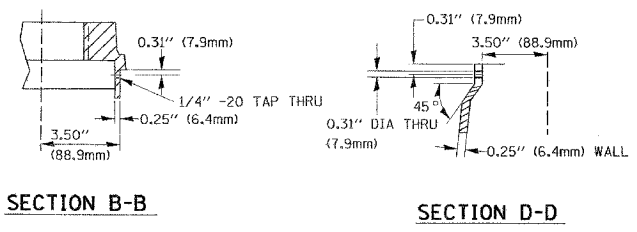
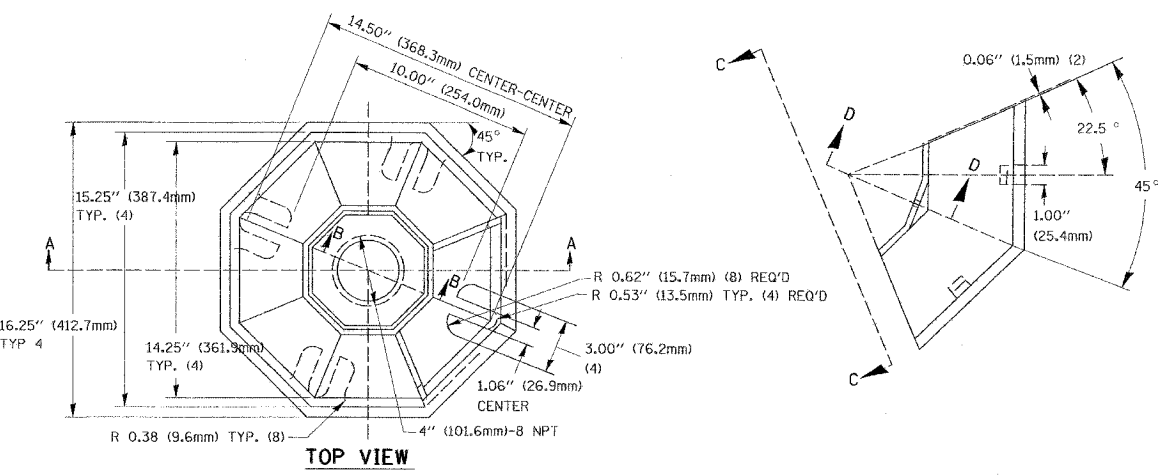
MAST ARM POLE / POST-GROUNDING DETAIL
 (NOT TO SCALE)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

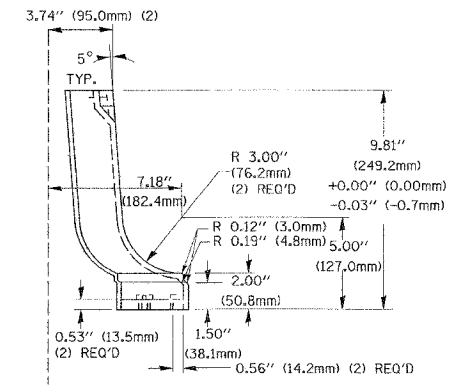
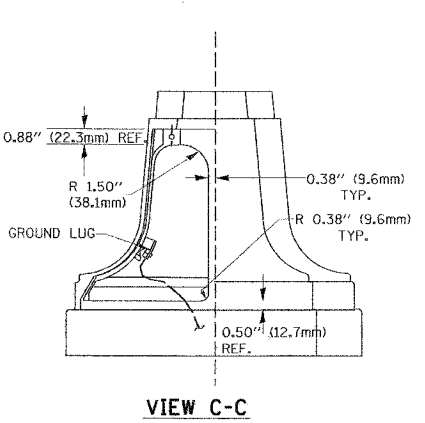
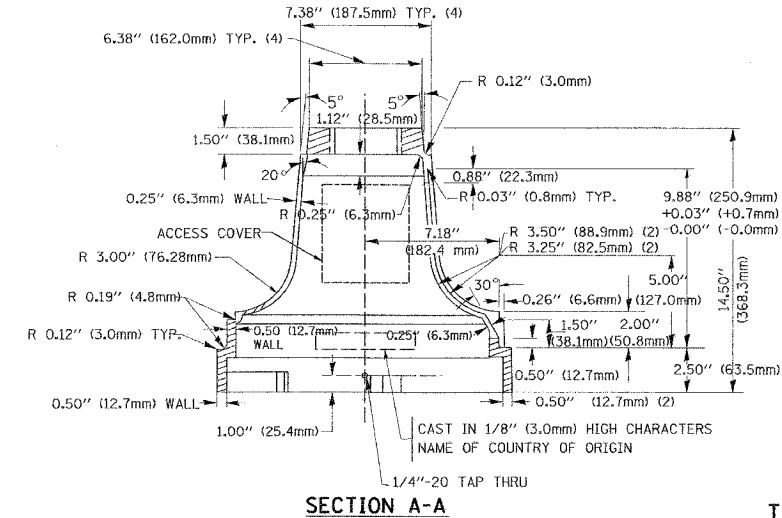
SCALE: VERT. NONE
 HORIZ. DATE 1-01-02
 DRAWN BY: RWP
 DESIGNED BY: DAD
 CHECKED BY: DAZ
 SHEET 3 OF 4

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54 WRS-7	DUPAGE	235	128
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT			
D-91-386-01	CONTRACT NO. 62291			

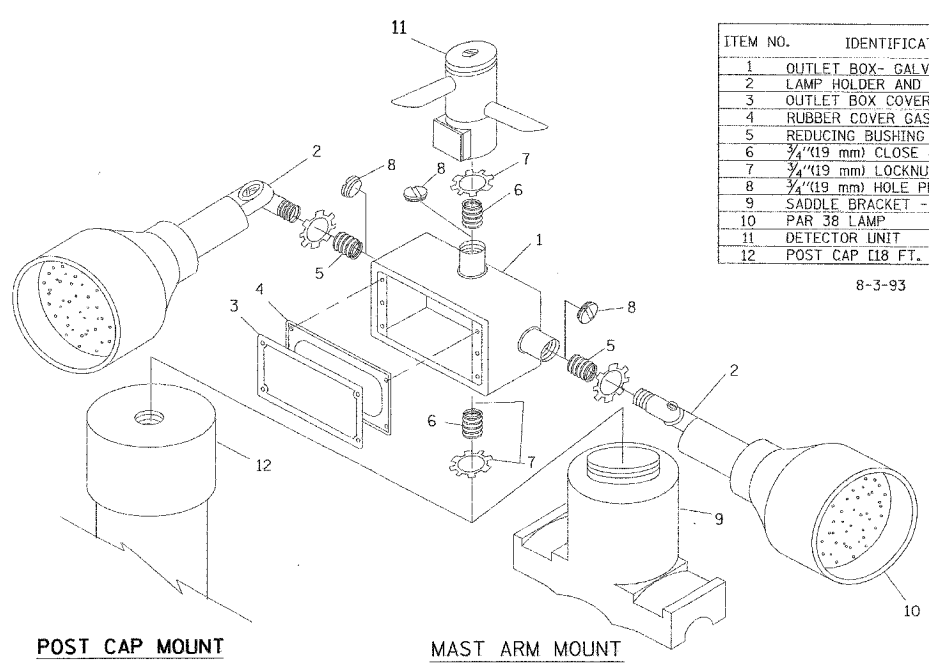
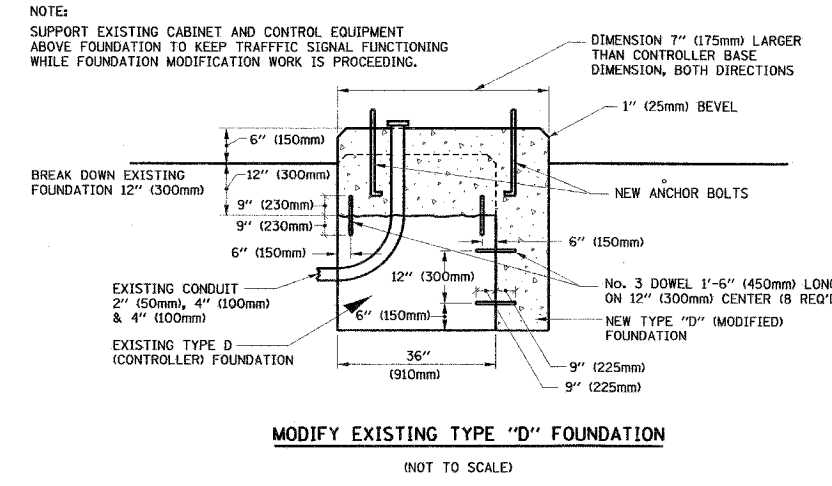


TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125\"(257mm)	9.5\"(241mm)	19\"(483mm)	12\"(300mm)	24kg
II	Ø 11.125\"(283mm)	10.75\"(273mm)	21.5\"(546mm)	12\"(300mm)	26kg

MATERIAL:
 - ASTM A48 CLASS 30 GREY IRON
 - ASTM A123 HOT DIPPED GALVANIZED

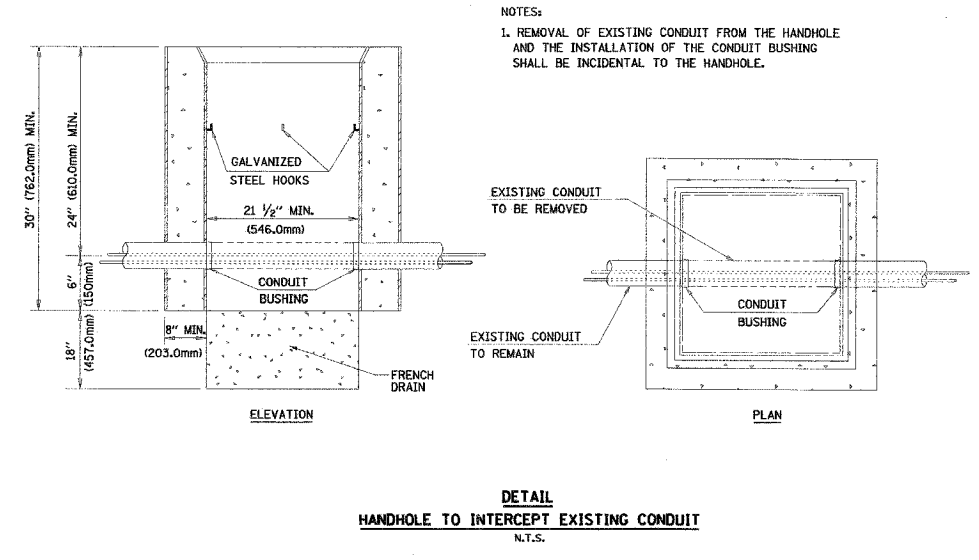
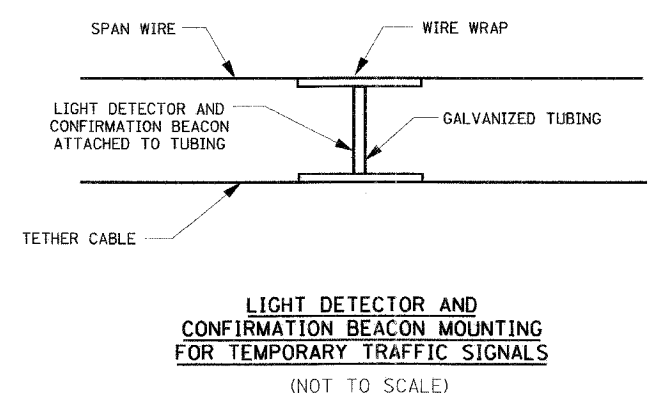


TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU. IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

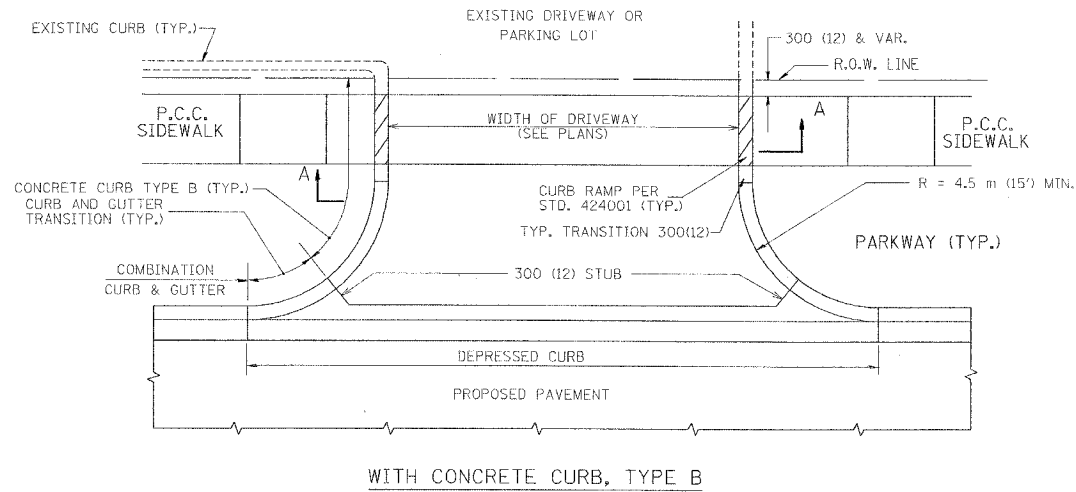
- NOTES:
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
 ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
 ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



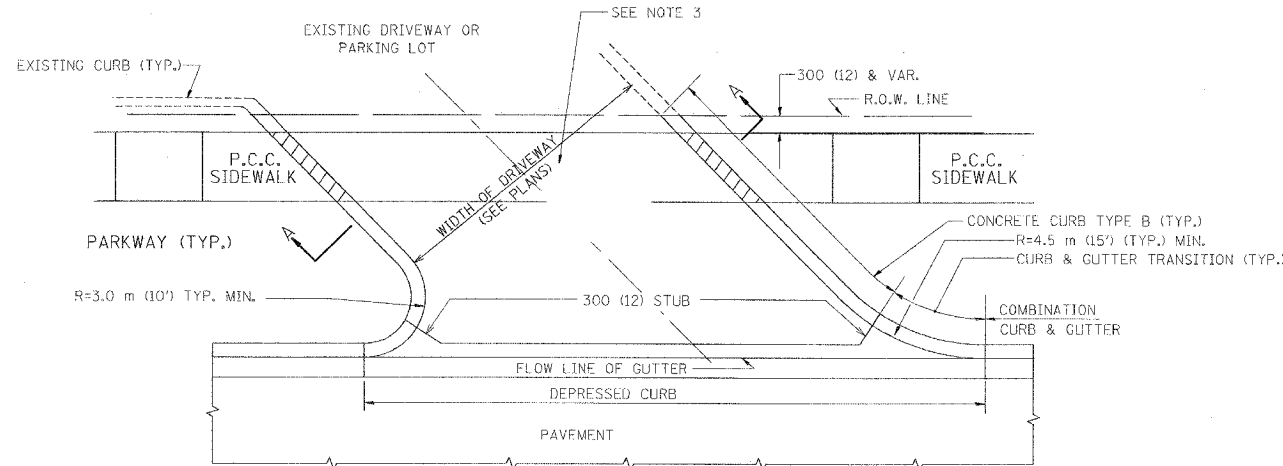
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE		
		DISTRICT 1	
		STANDARD TRAFFIC SIGNAL	
		DESIGN DETAILS	
		SCALE: VERT. NONE	DRAWN BY: RWP
		DATE 1-01-02	DESIGNED BY: DAD
			CHECKED BY: DAZ
			SHEET 4 OF 4

DATE-TIME:
 DGN-SPEC

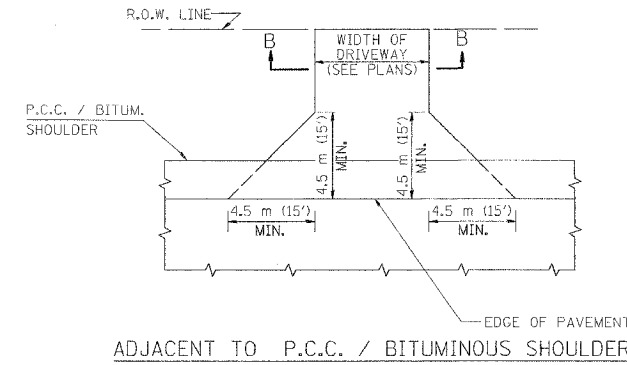
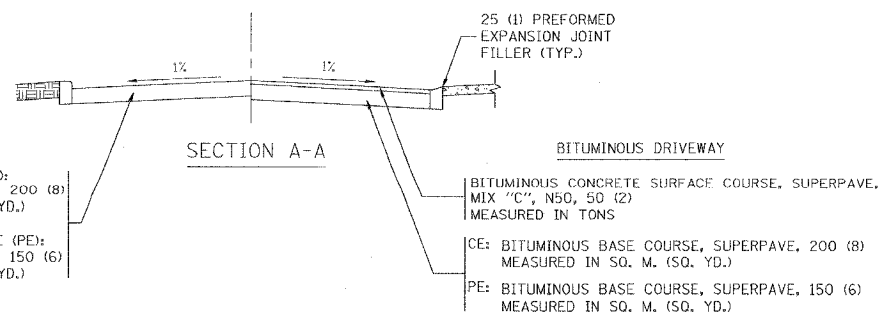
P.A. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	SA WRS-7	DuPage	235	129
STA.	TO STA.			
FED. ROAD DIST. NO. 1	STATE	FED. AID PROJECT		



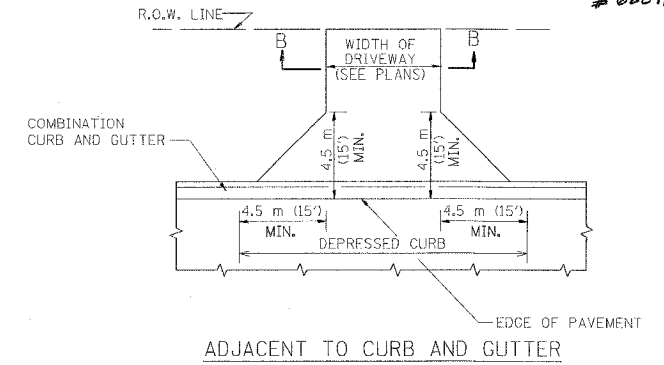
WITH CONCRETE CURB, TYPE B



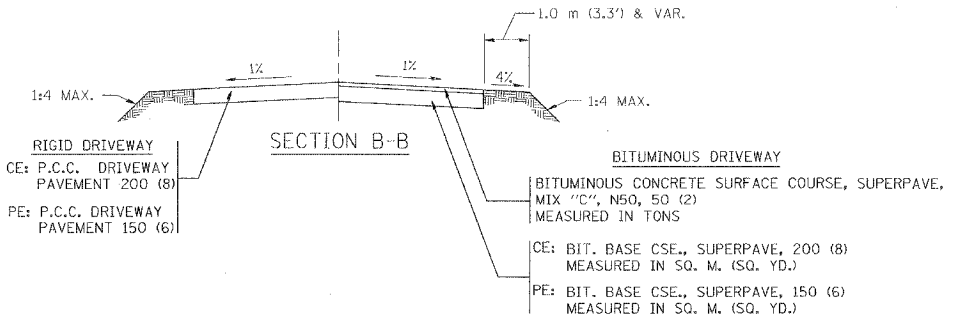
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / BITUMINOUS SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50, 50 (2) MEASURED IN TONS
 CE: BIT. BASE CSE., SUPERPAVE, 200 (8) MEASURED IN SQ. M. (SQ. YD.)
 PE: BIT. BASE CSE., SUPERPAVE, 150 (6) MEASURED IN SQ. M. (SQ. YD.)

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE 'HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS'. FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 1.2 METERS (4 FEET) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

25 (1) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED

ILLINOIS DEPARTMENT OF TRANSPORTATION

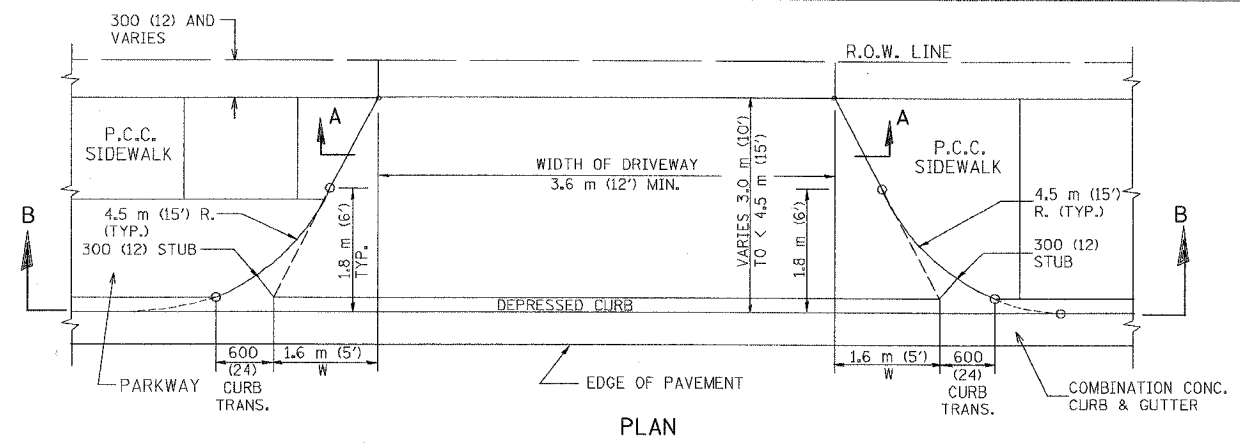
DRIVEWAY DETAILS
 DISTANCE BETWEEN R.O.W. AND
 FACE OF CURB / EDGE OF
 SHOULDER >= 4.5 m (15')

REVISIONS	
NAME	DATE
P. LOFLEUR	04-15-03
R. SHAH	11-04-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01

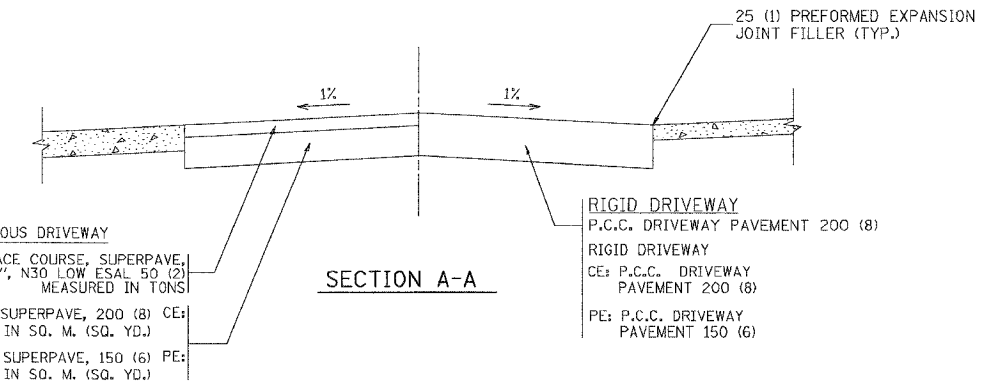
SCALE: NONE
 DATE PLOTTED: 04/17/2003
 DRAWN BY: SG
 CHECKED BY: JFP

F. & S. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA. TO STA.				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

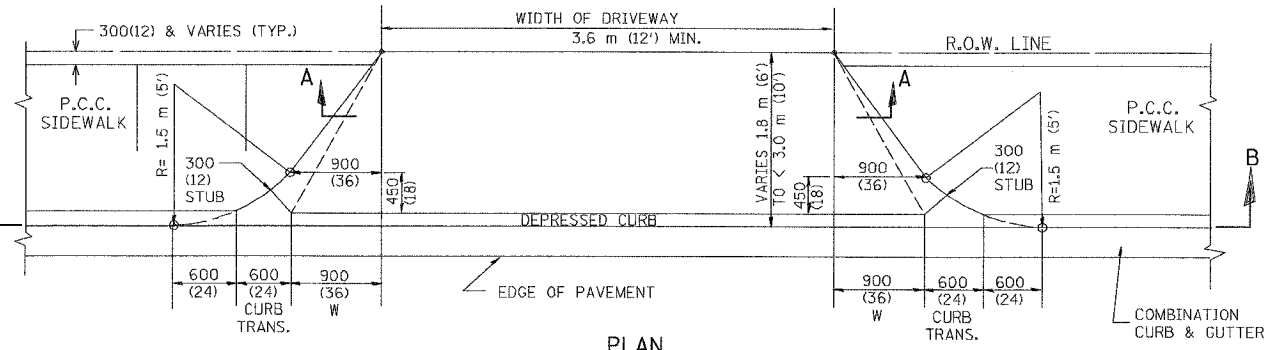
#62291



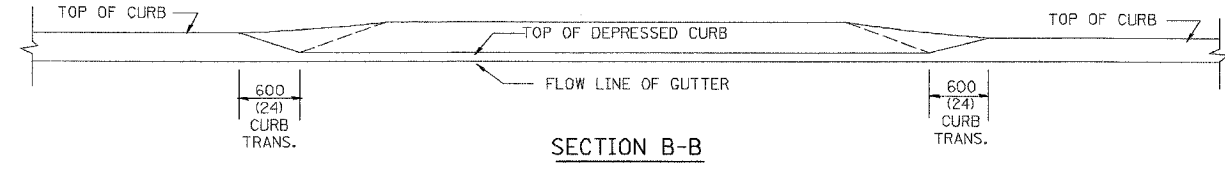
PLAN
3.0 m (10') TO < 4.5 m (15')



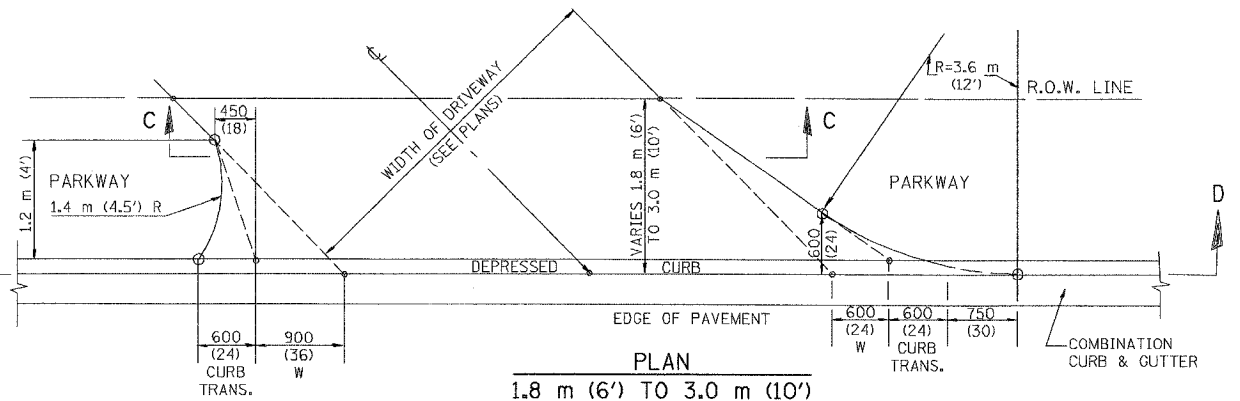
SECTION A-A



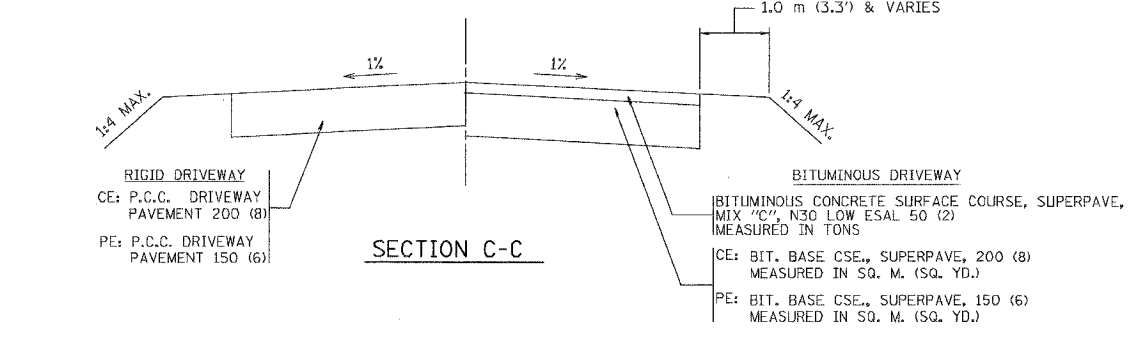
PLAN
1.8 m (6') < 3.0 m (10')



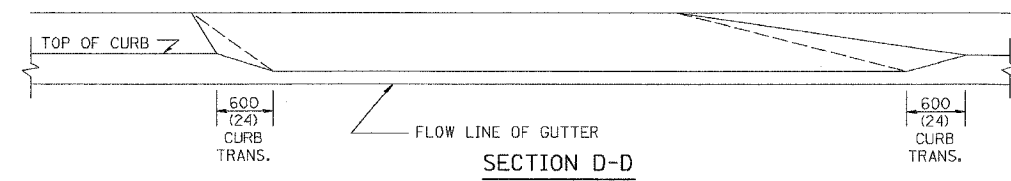
SECTION B-B



PLAN
1.8 m (6') TO < 3.0 m (10')



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 2.4 M (8'), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

25 (1) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 900 (36) TO 1.5 m (5 FT.) PROPORTIONAL TO THE LENGTH (L), FROM 1.8 m (6 FT.) TO 3 m (10 FT.).

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRIVEWAY DETAILS

DISTANCE BETWEEN ROW AND FACE OF CURB < 4.5 m (15')

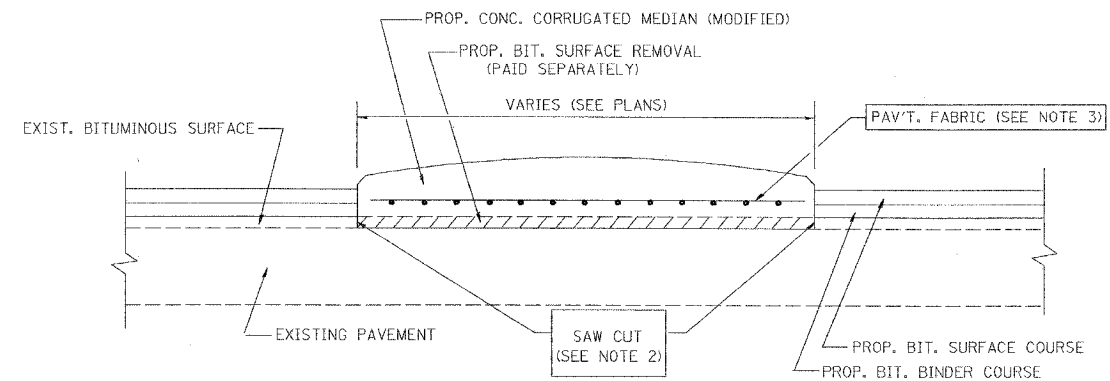
REVISIONS	
NAME	DATE
M. GOMEZ	04/06/01
P. LAFLEUR	06/18/02
R. SHAH	11/06/95
J. POLLASTRINI	08/12/96
J. POLLASTRINI	12/14/96
A. ABBAS	03/21/97
T. HOLTZ	04/08/97

SCALE: NONE
DATE PLOTTED: 10/18/2002
DRAWN BY: SG
CHECKED BY: JFP

BD400-02 (BD-02)
REVISION DATE: 04/06/01

F. A. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	SA WBS-7	DuPage	235	131
STA.		FO STA.		
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

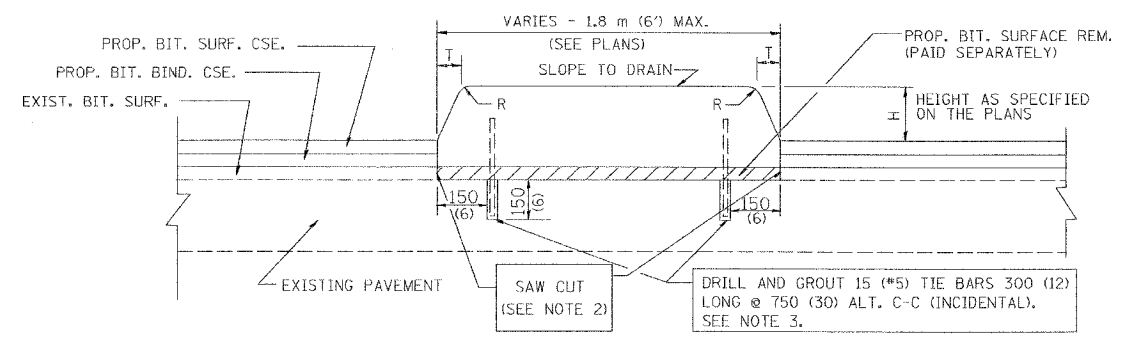
62291



- NOTES:
1. CORRUGATED MEDIAN (MODIFIED) SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 606 OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE PORTIONS OF STATE STANDARD 606306.
 2. WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY DELETE THE SAW CUT IF A NEAT JOINT CAN BE OBTAINED BY MILLING THE BIT. SURFACE TO BE REMOVED. SAW CUT WILL BE INCLUDED IN THE COST OF CORRUGATED MEDIAN (MODIFIED)
 3. PAVEMENT FABRIC WILL BE INCLUDED IN THE COST OF CORRUGATED MEDIAN (MODIFIED)

DETAILS FOR CORRUGATED MEDIAN (MODIFIED)

BASIS OF PAYMENT : "CORRUGATED MEDIAN (MODIFIED)"
MEASUREMENT: m² (sq. ft.)



- NOTES:
1. CONCRETE MEDIAN TYPE SB (DOWELLED) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STATE STANDARD 606301 AND SECTION 606 OF THE STANDARD SPECIFICATIONS.
 2. WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR MAY DELETE THE SAW CUT IF A NEAT JOINT CAN BE OBTAINED BY MILLING THE BIT. SURFACE TO BE REMOVED. SAW CUT WILL BE INCLUDED IN THE COST OF "CONCRETE MEDIAN TYPE SB (DOWELLED)"
 3. FOR MEDIAN WIDTH LESS THAN 1.2 m (4') USE ONE ROW OF 15 (#5) BARS @ 750 (30) C-C ALONG THE MEDIAN CENTERLINE. TIE BARS WILL BE INCLUDED IN THE COST OF "CONCRETE MEDIAN TYPE SB (DOWELLED)"

H	R	T
150 (6)	25 (1)	25 (1)
225 (9)	25 (1)	50 (2)

**DETAILS FOR CONCRETE MEDIAN
TYPE SB (DOWELLED)**

BASIS OF PAYMENT : "CONCRETE MEDIAN TYPE SB (DOWELLED)"
MEASUREMENT: m² (sq. ft.)

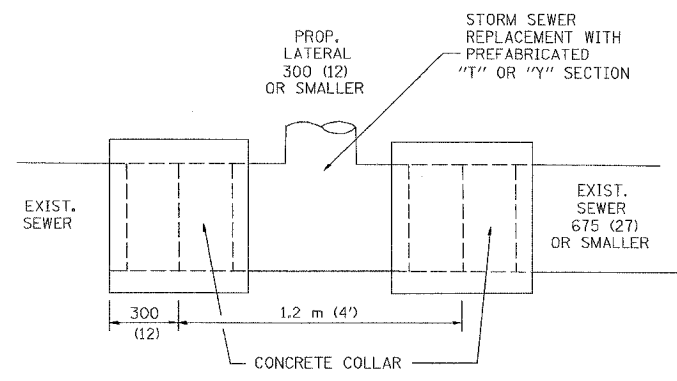
ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
M. DE YONG	05/14/90
M. DE YONG	06/13/90
M. DE YONG	06/14/90
M. DE YONG	06/20/90
M. DE YONG	09/21/90
M. DE YONG	10/12/90
R. SHAH	09/09/94
R. SHAH	10/25/94
E. GOMEZ	08/28/00

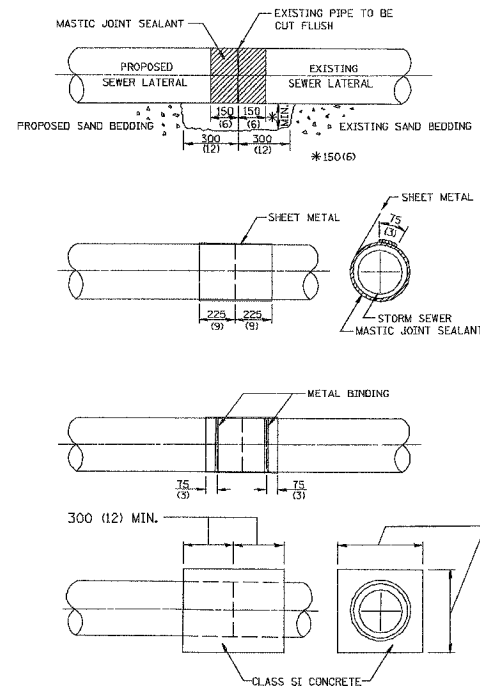
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR
CONCRETE MEDIAN TYPE SB (DOWELLED)
CORRUGATED MEDIAN (MODIFIED)

SCALE: NONE
DATE 10/18/2002
DRAWN BY: JLS
CHECKED BY: BD-51
REVISION DATE: 08/28/00

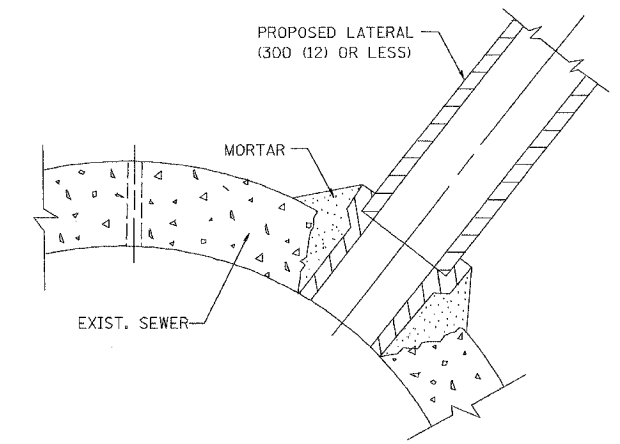
62291



DETAIL "A"
LATERAL CONNECTION TO EXISTING SEWER OF 675 (27) OR SMALLER



DETAIL "B"
CLASS SI CONCRETE COLLAR



DETAIL "C"
PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 750 (30) OR LARGER

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 150 (6) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 300 x 150 (12 x 6) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 450 (18) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 75 (3) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 225 (9) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 75 (3) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 675 (27) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 750 (30) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

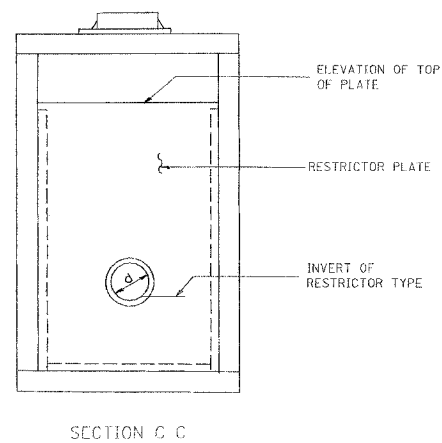
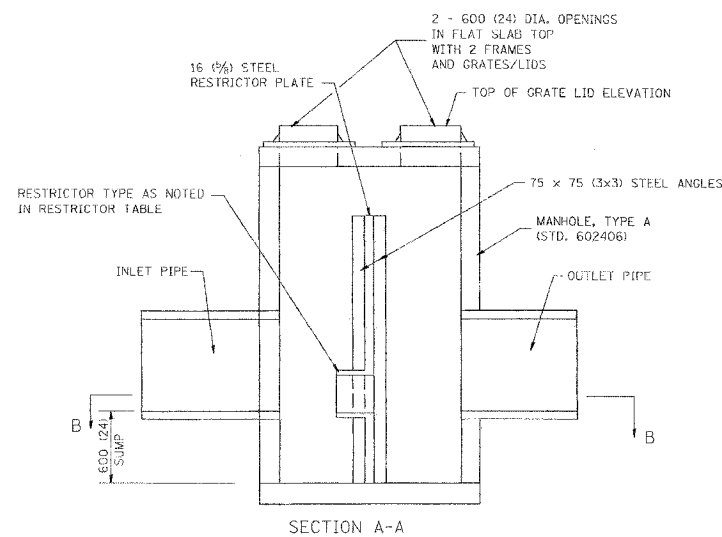
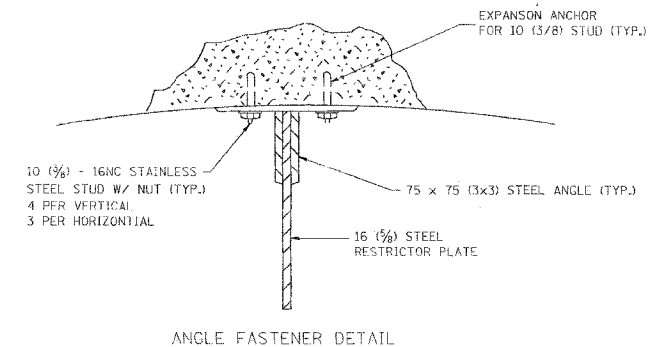
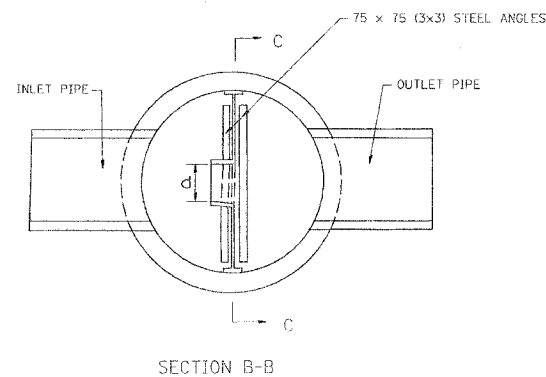
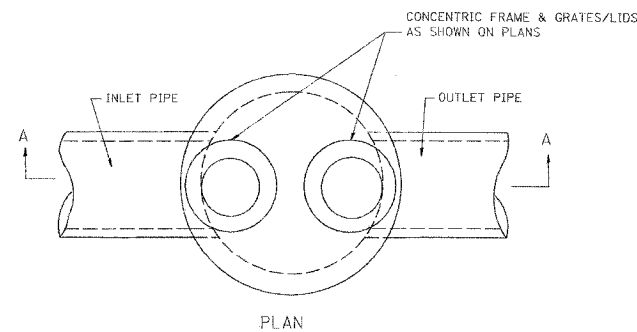
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER

REVISIONS	
NAME	DATE
M. DE YONG	07/25/90
M. DE YONG	07/05/92
M. DE YONG	05/08/92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	06/12/96

SCALE: NONE
DATE 10/18/2002

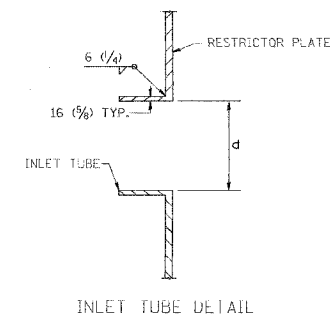
DRAWN BY CADD
CHECKED BY
BD500-01 (BD-7)
REVISION DATE: 06/12/96

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	133
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

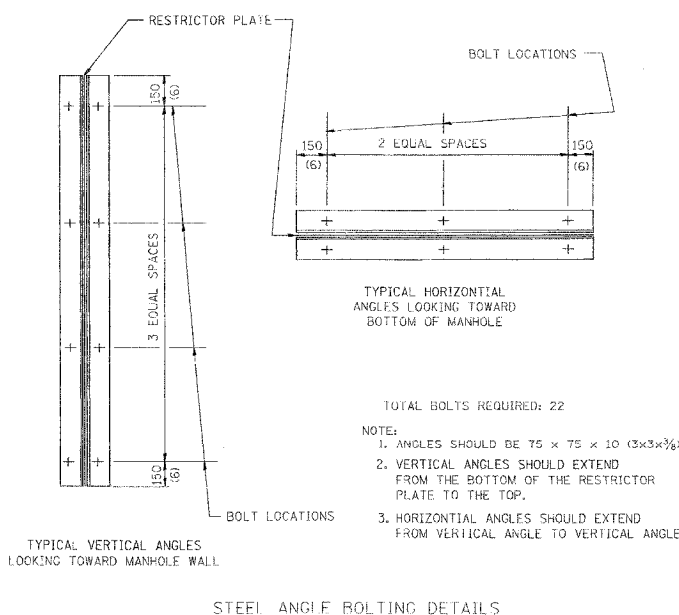


NOTES:

- ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
- ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
- BASIS OF PAYMENT: "MANHOLES, TYPE A, 1.8M (6FT.) DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER mm (in.) (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW
231+85	6	TIF CL	2	30.00	709.70	713.28
243+24	6	TIF CL	2	9.25	700.08	703.10



RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

MANHOLE WITH RESTRICTOR PLATE

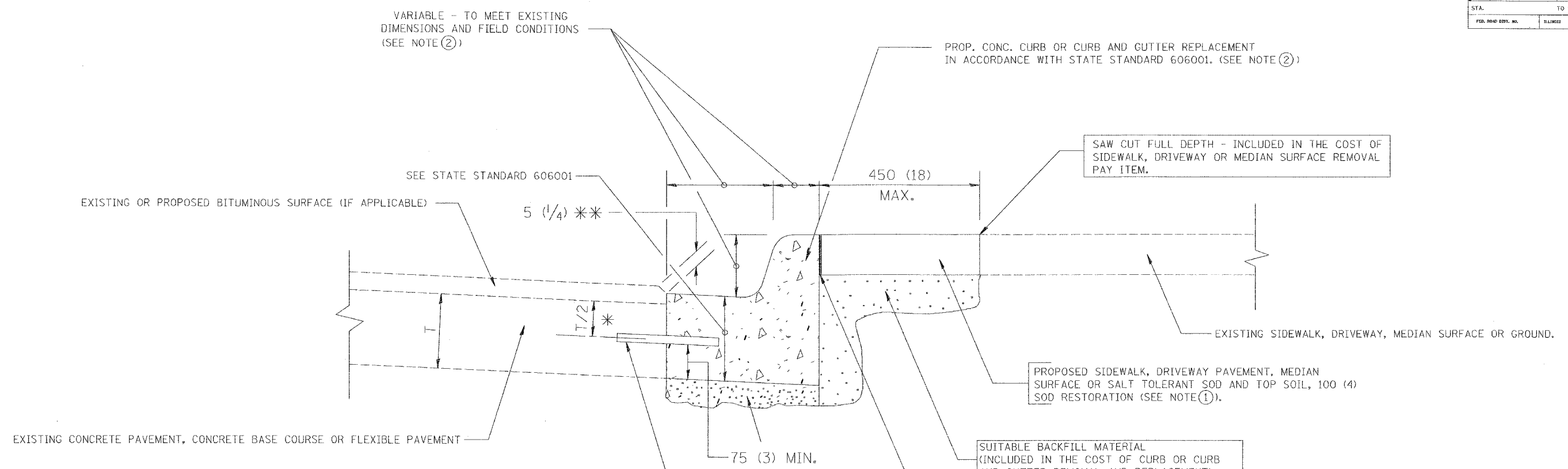
REVISIONS	
NAME	DATE
R. SHAH	09/09/94
R. SHAH	10/25/94
E. GOMEZ	08/28/00
M. GOMEZ	01/08/01

SCALE: NONE
DATE 10/18/2002

DRAWN BY
CHECKED BY
BD600-04 (BD-12)
REVISION DATE: 01/08/01

F. A. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3845	SA WBS-7	DuPage	235	134
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

62291



- * 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE:
- ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
 - ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING CUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 100 (4) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 100 (4) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED NO. 20 (NO. 6) EPOXY COATED TIE BARS 600 (24) LONG AT 600 (24) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

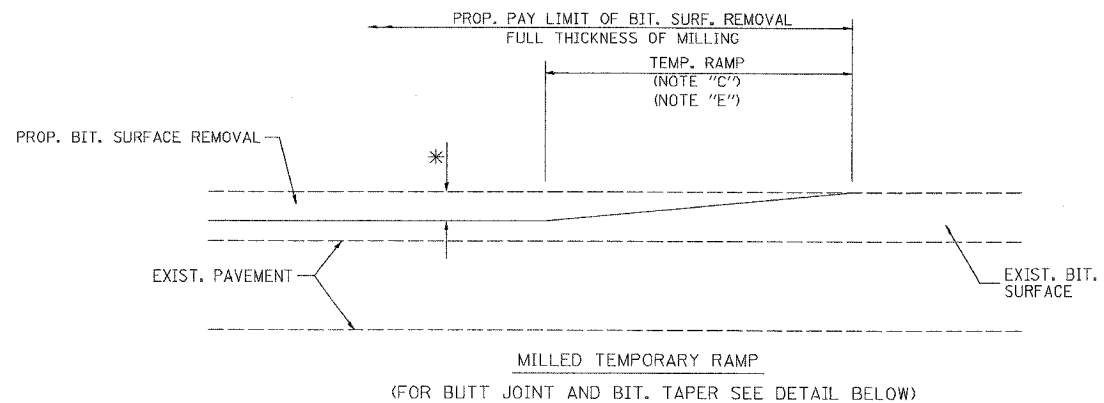
ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

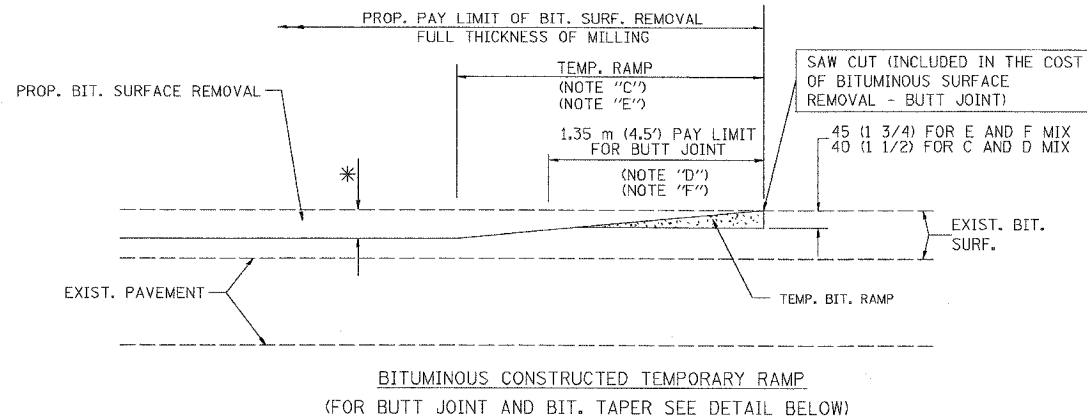
SCALE: NONE
 DATE: 10/18/2002
 DRAWN BY:
 CHECKED BY:
 BD600-06 (BD-24)

F. A. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
366	SA WRS-7	Du Page	235	135
STA.	TO STA.			
FED. ROAD DIST. NO.	LENGTH	FED. AID PROJECT		

62291

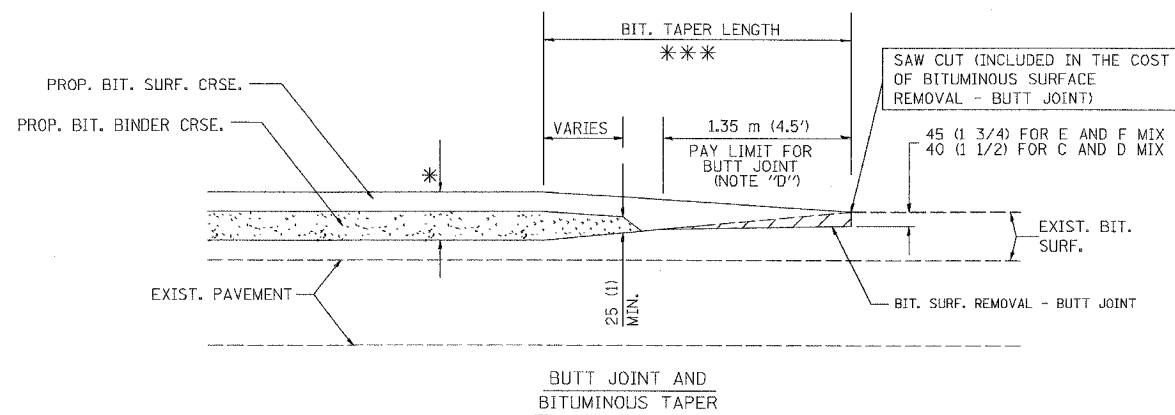


OPTION 1

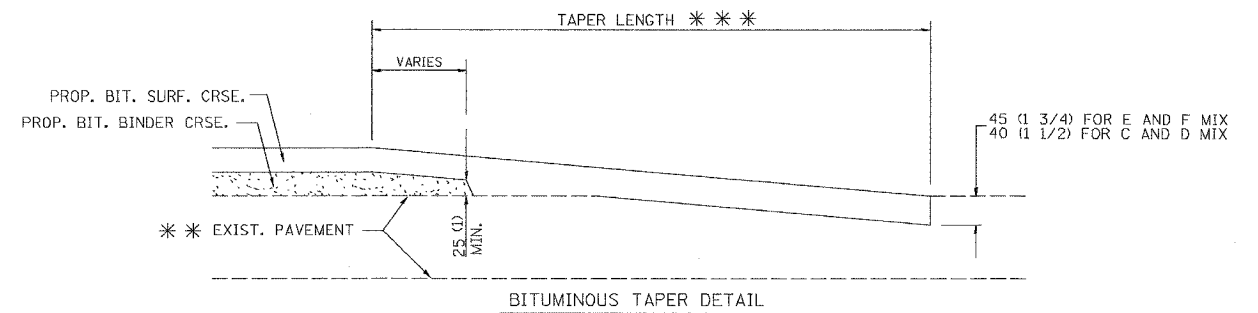
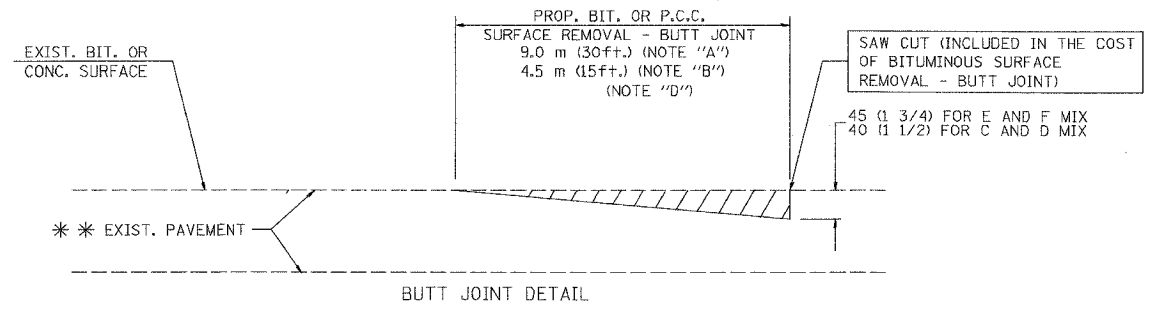


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR RESURFACING ONLY

*** PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 ft.) PER INCH OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".
- G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 6.1 m (20') PER 25 (1) RESURFACING (NOTE 'A')
3.0 m (10') PER 25 (1) RESURFACING (NOTE 'B')

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND BITUMINOUS TAPER DETAILS

BASIS OF PAYMENT:

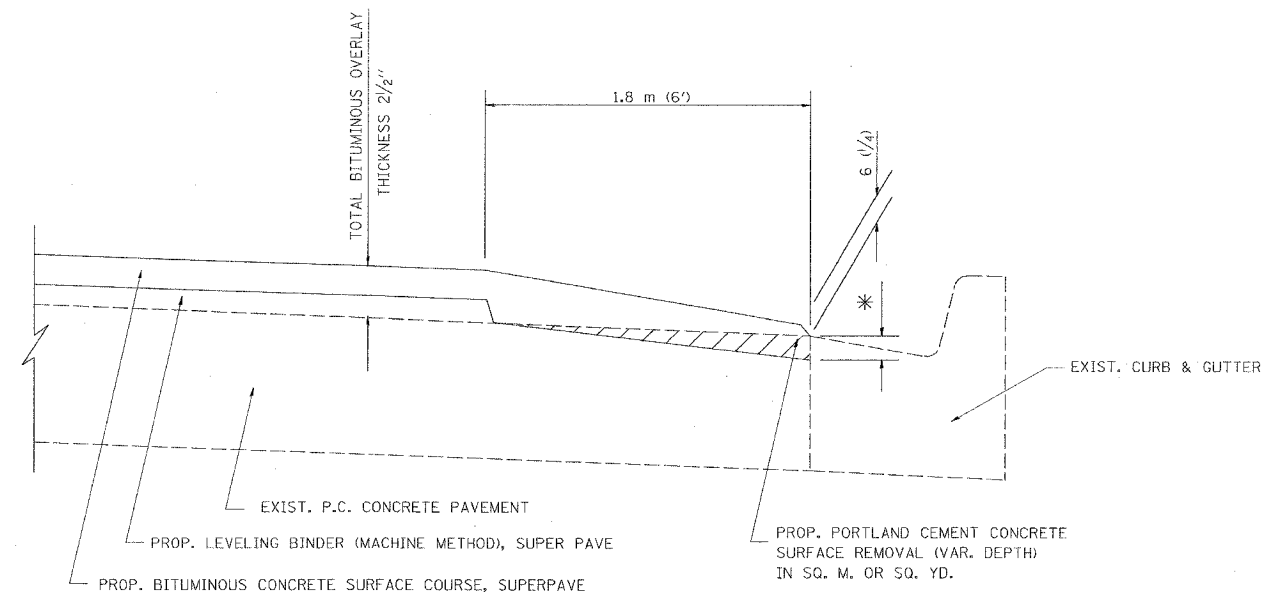
THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01

SCALE: NONE
DATE PLOTTED: 10/18/2002

DRAWN BY
CHECKED BY
BD400-05 (VI-BD32)

P.A. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	SA WRS-7	De Page	235	136
STA.		TO STA.		
FED. ROAD DIST. NO.	BLINDS	FED. AID PROJECT		
# 62291				



BITUMINOUS TAPER AT
EDGE OF P.C.C PAVEMENT

SUPERPAVE	SURFACE MIX	THICKNESS	LEVELING BINDER SUPERPAVE	* MILLING AT GUTTER FLAG
			THICKNESS	
C OR D	38 (1 1/2)	25 (1)	33 (1 1/4)	
E OR F	44 (1 3/4)	19 (3/4)	38 (1 1/2)	

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

ILLINOIS DEPARTMENT OF TRANSPORTATION

BITUMINOUS TAPER AT
EDGE OF P.C.C. PAVEMENT

REVISIONS	
NAME	DATE
R. SHAH	09/10/94
R. SHAH	10/25/94
A. ABBAS	05/05/99
E. GOMEZ	12/21/00

SCALE: NONE
DATE 10/18/2002
DRAWN BY Jjs
CHECKED BY A. ABBAS
BD400-06 (BD33)
REVISION DATE: 12/21/00

DATE-TIME
DGN-SPEC
VH-8033

F. A. REC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3548	SD WRS-7	DuPage	235	137
STA.	TO STA.		FED. AID PROJECT	
				# 62291

GENERAL NOTES

ALTERNATE MATERIAL FOR THE WALLS MAY BE CONCRETE MASONRY UNITS, PRECAST REINFORCED CONCRETE SECTIONS OR CAST-IN-PLACE CONCRETE. THE CAST IRON STEPS AS DETAILED HEREON ARE TYPICAL. STEPS OF OTHER DESIGN AND MATERIAL THAT CONFORM TO THE MINIMUM REQUIREMENTS OF THE STEPS SHOWN MAY BE USED WHEN APPROVED BY THE ENGINEER.

CAST IRON STEPS SHALL BE GRAY IRON CONFORMING TO THE REQUIREMENTS OF ARTICLE 1006.14 OF THE STANDARD SPECIFICATIONS.

STEPS SHALL BE EMBEDDED INTO THE WALL A MINIMUM OF THREE(3) INCHES. STEPS SHALL NOT BE EXTENDED ON THE OUTSIDE.

STEPS SHALL BE OMITTED FOR WORK IN COOK COUNTY WHEN THE DEPTH OF THE MANHOLE IS TEN(10') OR LESS.

IN ADDITION TO THE REQUIREMENTS OF ARTICLE 612.13 OF THE STANDARD SPECIFICATIONS, THE CONTRACT UNIT PRICE FOR MANHOLES, TYPE A, 7'-DIAMETER SHALL INCLUDE THE SAND CUSHION WHEN REQUIRED, FURNISHING AND INSTALLING STEPS WHEN REQUIRED, FURNISHING AND COMPACTING THE SPECIFIED BACKFILL MATERIAL, AND FURNISHING AND INSTALLING FLAT SLAB TOP.

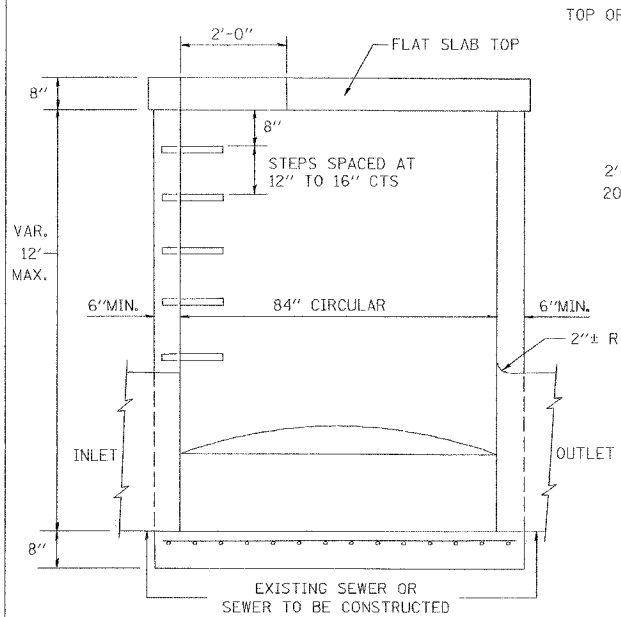
PRECAST FLAT SLAB TOP SHALL CONFORM TO ARTICLES 505.01 THRU 505.05 OF THE STANDARD SPECIFICATIONS EXCEPT THAT THE CONCRETE STRENGTH SHALL BE 4,000 PSI AFTER 28 DAYS. REINFORCEMENT BARS AND WELDED WIRE FABRIC SHALL CONFORM TO THE REQUIREMENTS OF ARTICLE 1006.10. ONLY GRADE 60 REINFORCEMENT BARS WILL BE PERMITTED.

BOTTOM SLAB SHALL BE REINFORCED BY EITHER REINFORCEMENT BARS OR WELDED WIRE FABRIC. THE MINIMUM REINFORCEMENT SHALL BE 0.46 SQUARE INCH PER LINEAR FOOT IN BOTH DIRECTIONS.

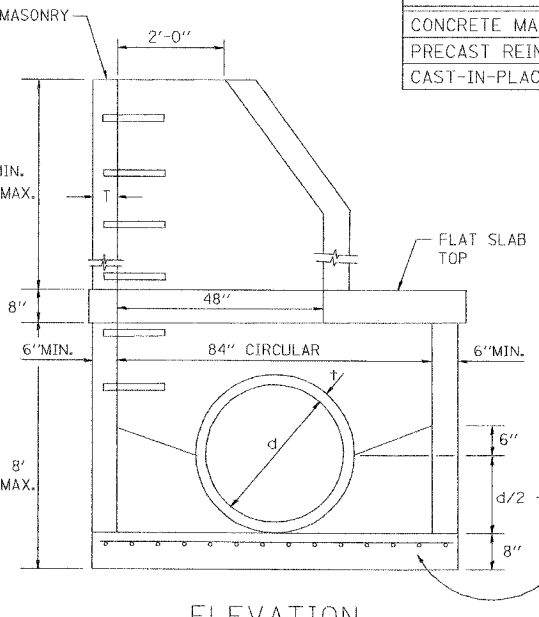
JOINT CONFIGURATION AND DIMENSIONS OF FLAT SLAB TOP SHALL MATCH AND FIT THE RISER JOINT DETAIL.

LIFTING DEVICES SHALL BE APPROVED BY THE ENGINEER.

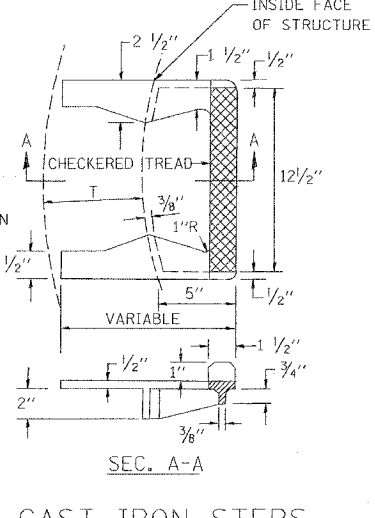
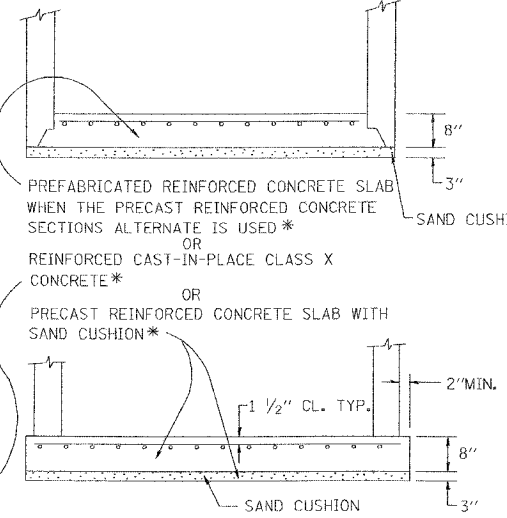
ALTERNATE MATERIALS FOR RISERS	T (MIN.)
CONCRETE MASONRY UNITS	5"
PRECAST REINFORCED CONCRETE SECTIONS	4"
CAST-IN-PLACE CONCRETE	6"



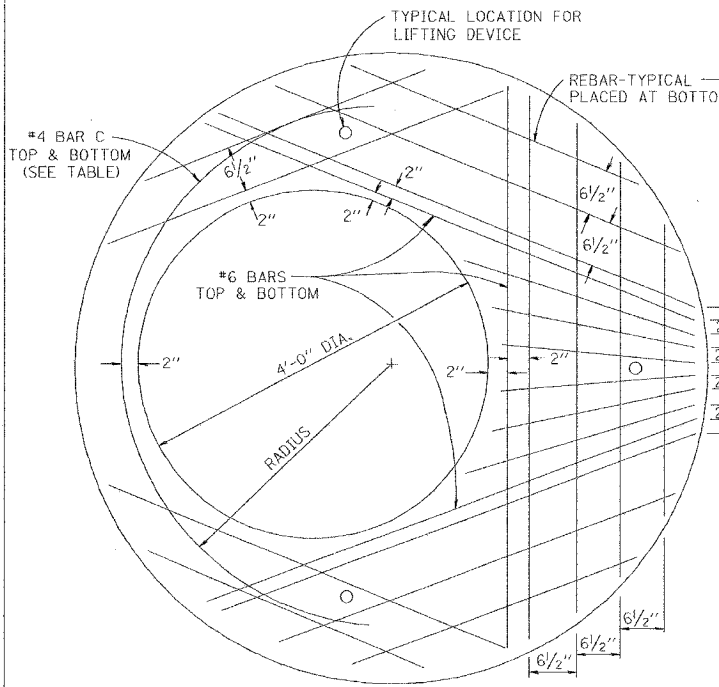
ELEVATION



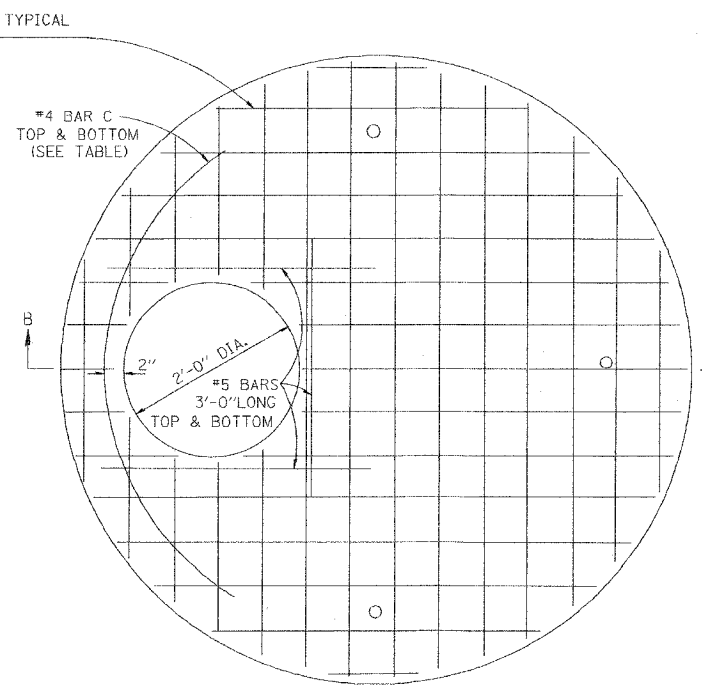
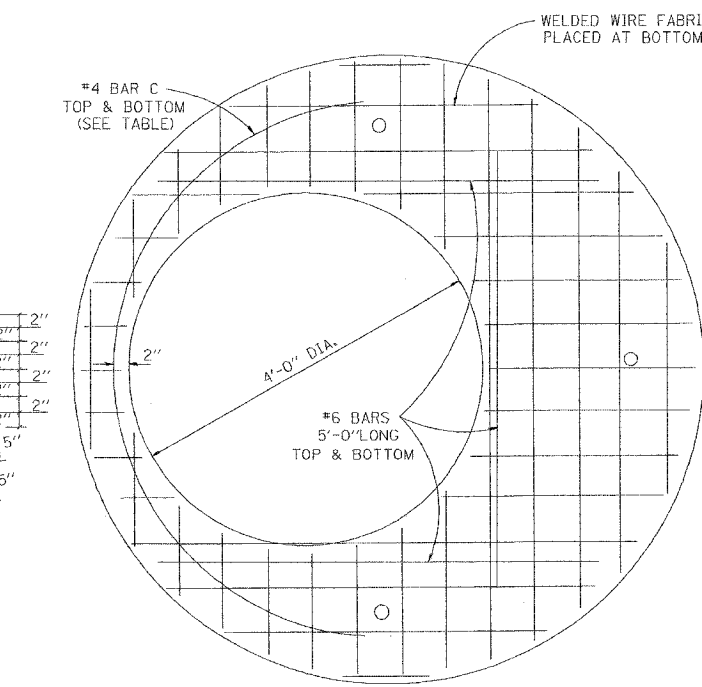
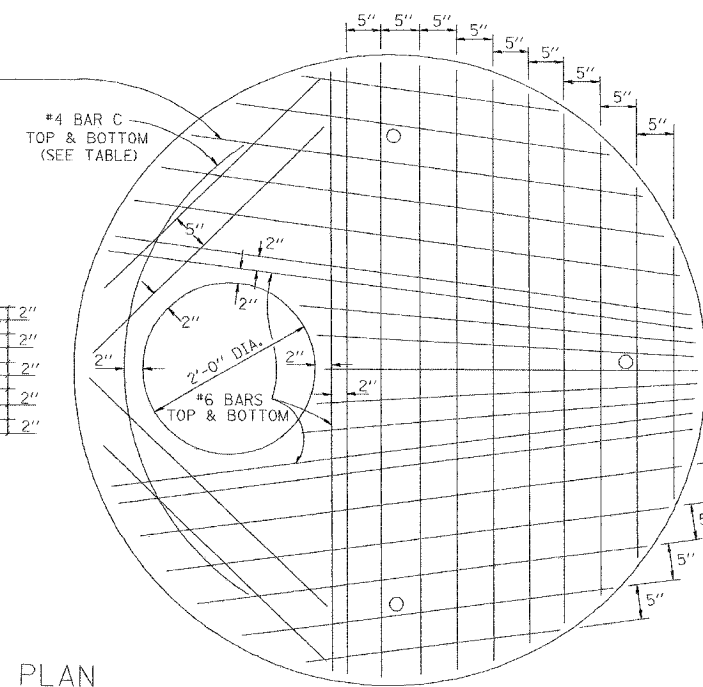
ELEVATION



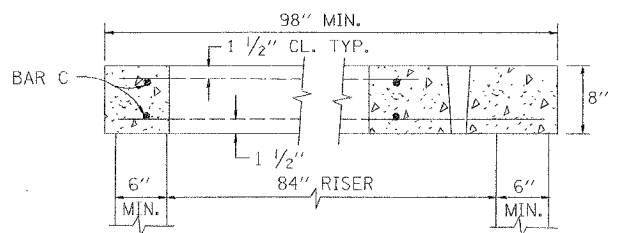
CAST IRON STEPS



PLAN
SHOWING REBAR REINFORCEMENT



PLAN
SHOWING WELDED WIRE FABRIC REINFORCEMENT



SECTION B-B

DIAMETER OF OPENING	REINFORCEMENT "A" WWF OR BAR SIZE EACH DIRECTION	BAR SIZE	SIZE	LENGTH	RADIUS
2'-0"	1.06 SQ.IN./LIN.FT.	#6	#4	6'-0"	38"
4'-0"	0.82 SQ.IN./LIN.FT.	#6	#4	9'-0"	38"

NOTE: THIS STRUCTURE SHOULD BE USED WITH PIPES SIZE 54" DIA. OR SMALLER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

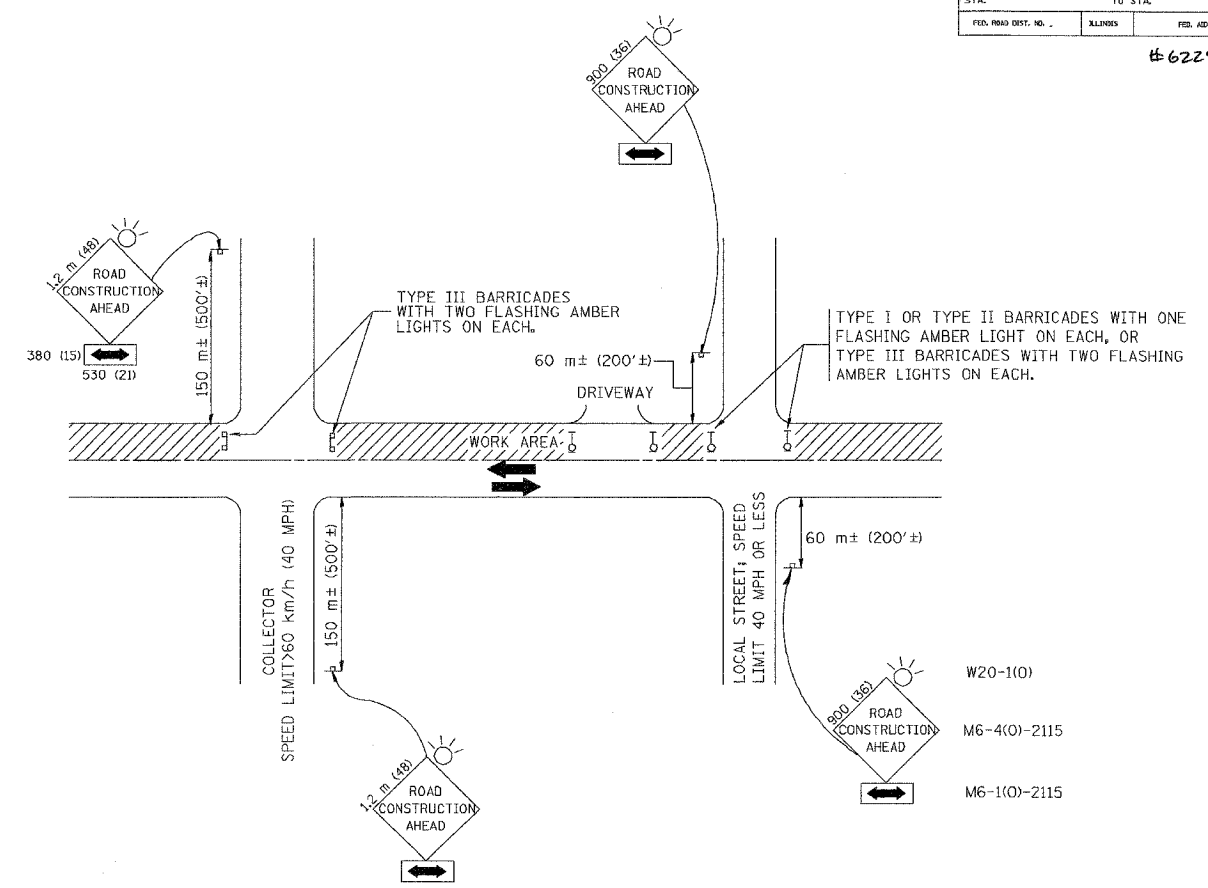
**MANHOLE TYPE A
7 FOOT DIAMETER**

SCALE: VERT. HORIZ.
DATE: 10/18/2002

DRAWN BY: _____
CHECKED BY: _____
BD600-11 (BD-37)
REVISION DATE: _____

DATE-TIME
DGN-SPEC
VI-BD37

F. A. EYE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	SA WRS-7	Du Page	235	138
STA.	TO STA.			
FED. ROAD DIST. NO.	KLINGS	FED. AID PROJECT		
#62291				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

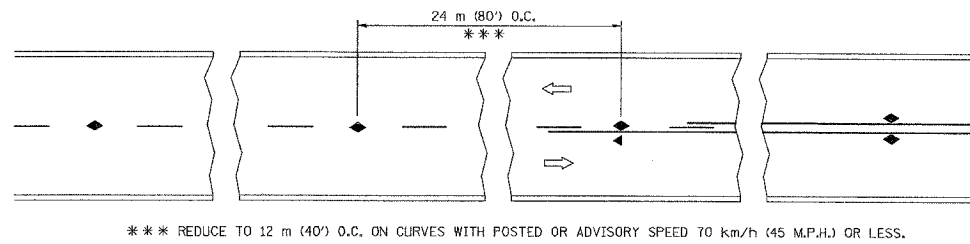
- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

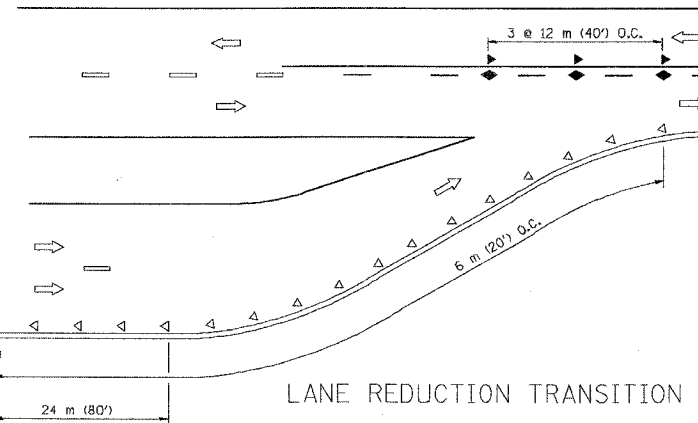
ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

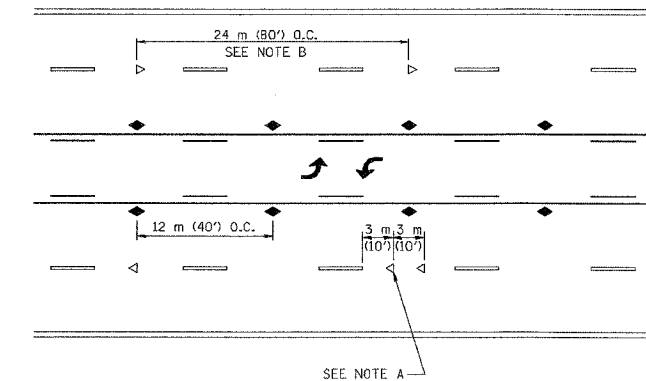
SCALE: NONE
 DATE: **DATE**
 DRAWN BY
 CHECKED BY



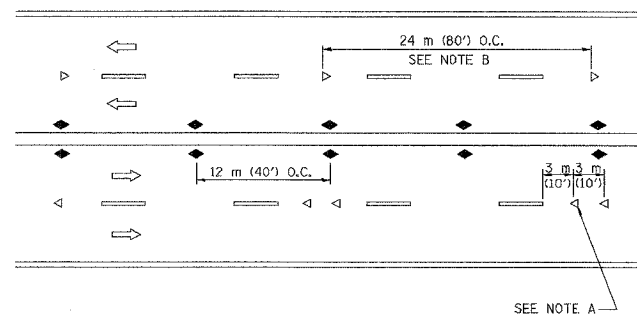
TWO-LANE/TWO-WAY



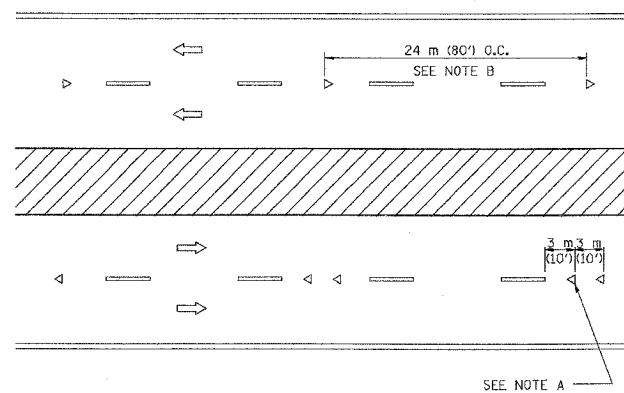
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 50 TO 75 (2 TO 3) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 150 m (500') IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

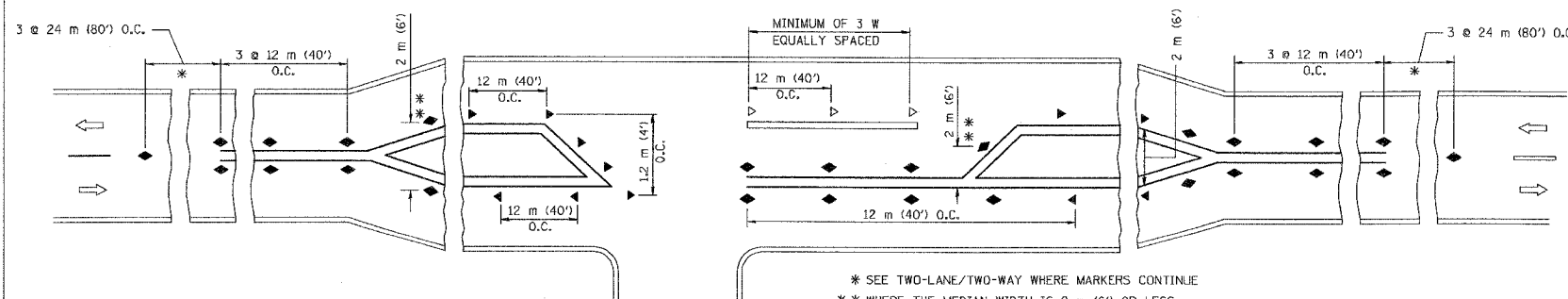
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 12 m (40') O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 20 km/h (10 M.P.H.) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

- * SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
- ** WHERE THE MEDIAN WIDTH IS 2 m (6') OR LESS USE TWO-WAY MARKERS.

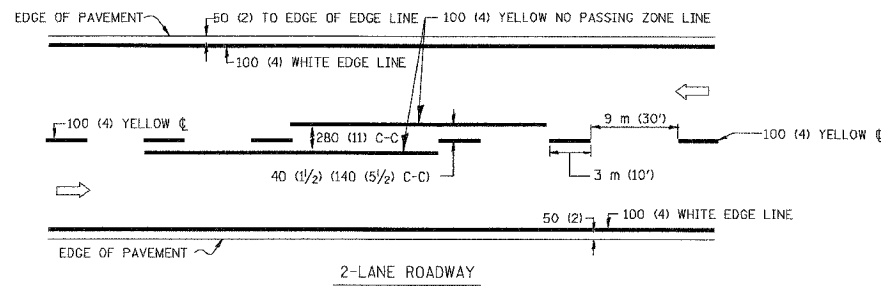
All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS
(SNOW-PLOW RESISTANT)

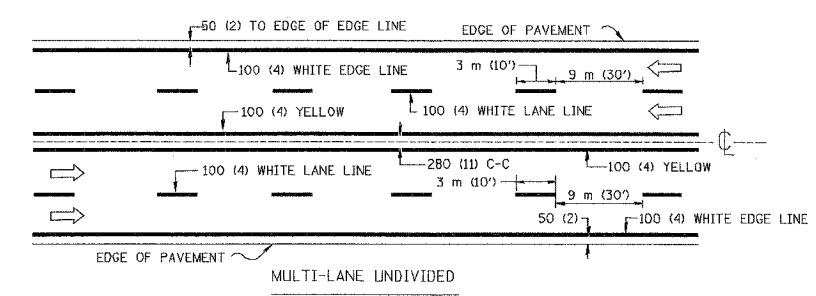
REVISIONS	
NAME	DATE
T. RAMMACHER	09-19-94
T. RAMMACHER	03-12-99
T. RAMMACHER	01-06-00

SCALE: NONE
DATE: 02/01/2002
DRAWN BY CADD
CHECKED BY

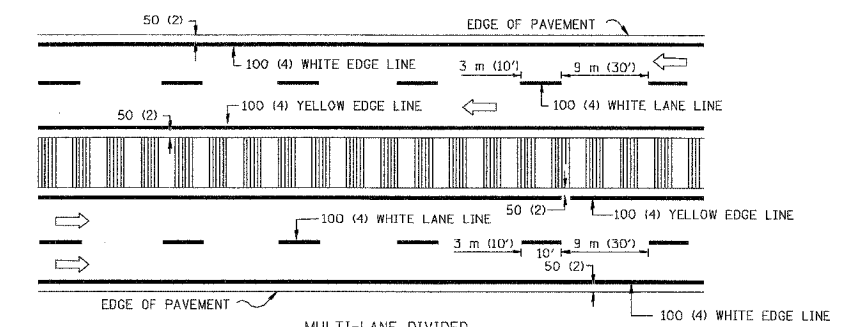
#62291



2-LANE ROADWAY



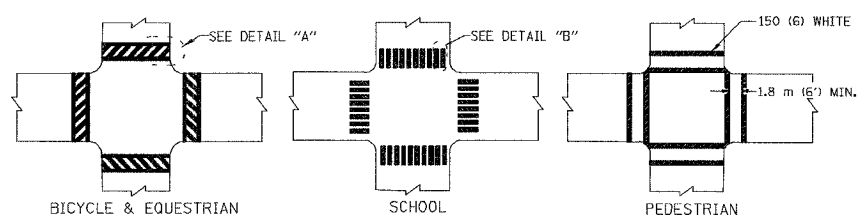
MULTI-LANE UNDIVIDED



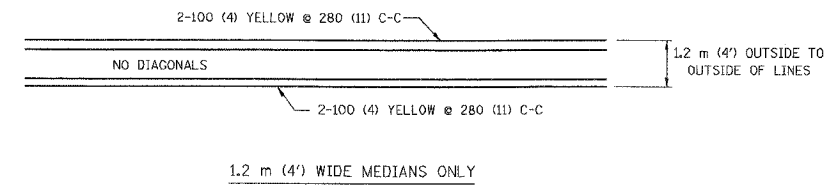
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

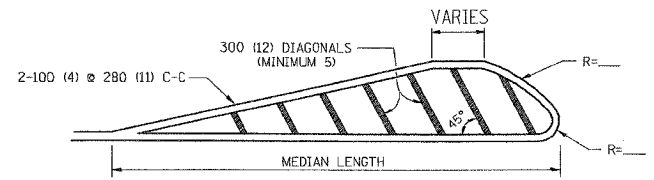
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



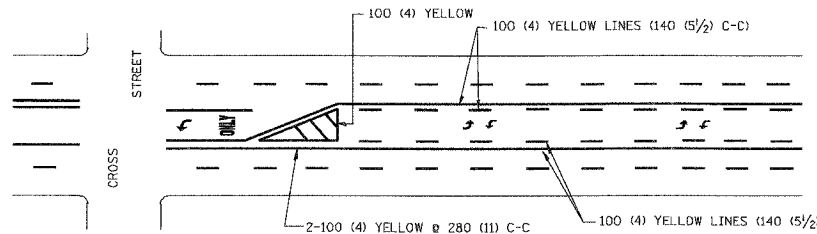
1.2 m (4') WIDE MEDIANS ONLY



MEDIAN LENGTH

FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

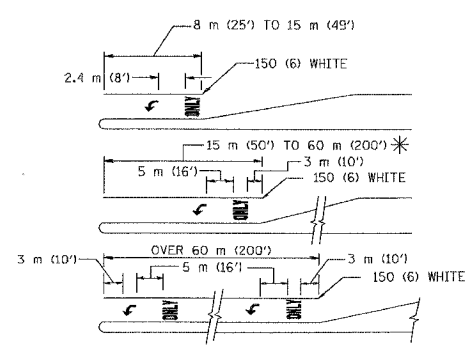
MEDIANS OVER 1.2 m (4') WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

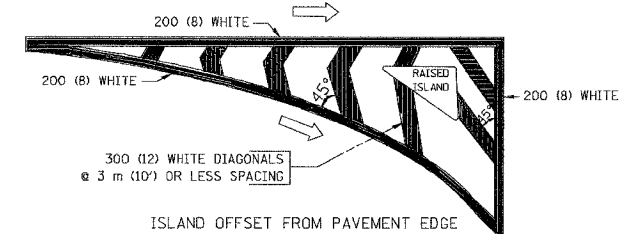
TYPICAL PAINTED MEDIAN MARKING



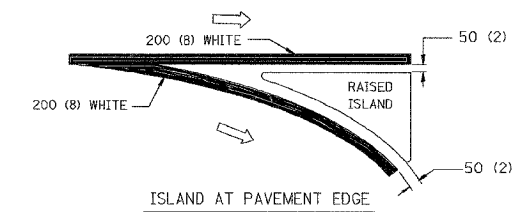
FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED. AREA = 1.5 m² (15.6 SQ. FT.) ONLY AREA = 1.9 m² (20.8 SQ. FT.)

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

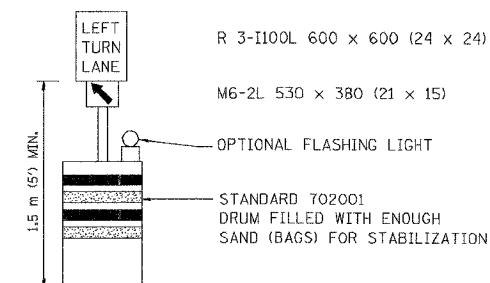
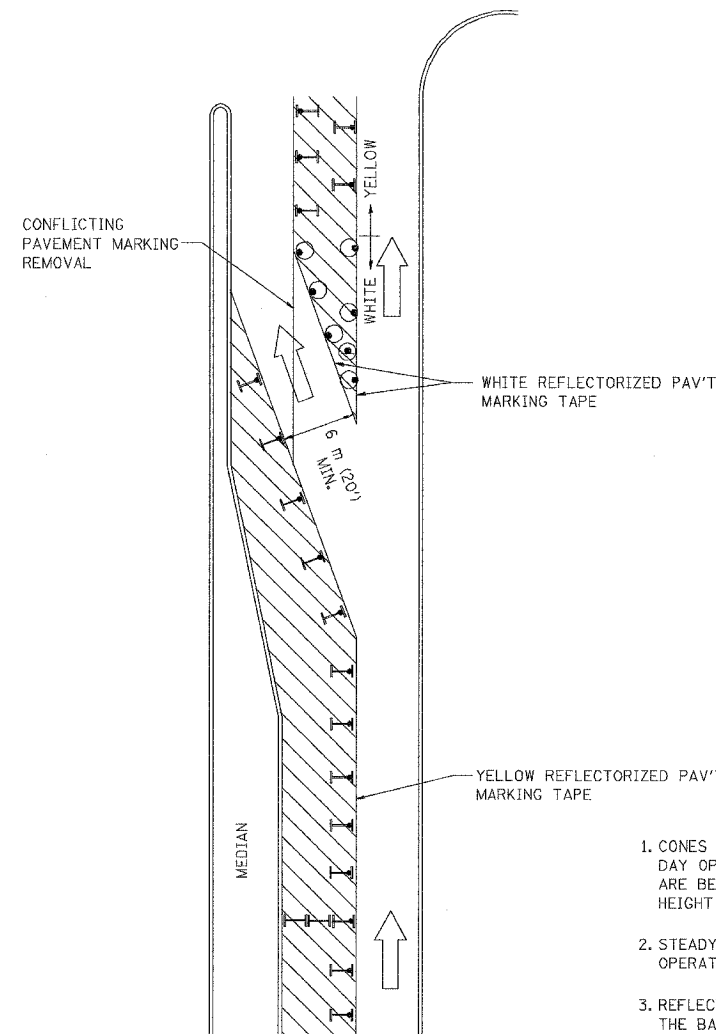
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

SCALE: NONE
DATE: **DATE**
DRAWN BY: CADD
CHECKED BY:

F. A. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
225	SA WPS-7	Do Page	235	141
STA.	TO STA.			
FED. ROAD DIST. NO.	BLKRES	FED. AID PROJECT		

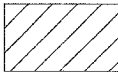





#62291



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 710 (28) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 1.5 m (5').
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 600 x 600 (24 x 24) AND M6-2R 530 x 380 (21 x 15) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in millimeters (inches) unless otherwise shown.

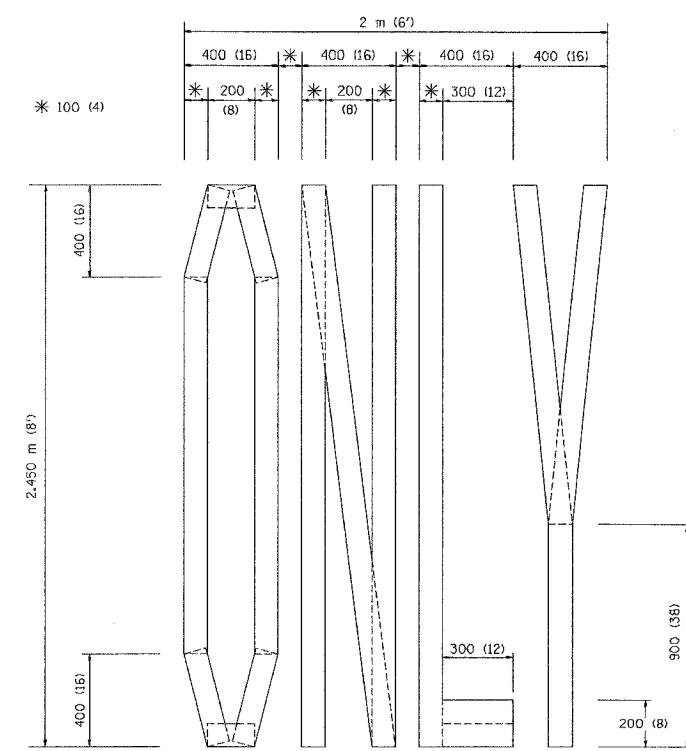
ILLINOIS DEPARTMENT OF TRANSPORTATION
**TRAFFIC CONTROL AND PROTECTION
 AT TURN BAYS
 (TO REMAIN OPEN TO TRAFFIC)**

REVISIONS	
NAME	DATE
T. RAMMACHER	09/08/94
A. HOUSEH	11/07/95
A. HOUSEH	10/12/96
T. RAMMACHER	01/06/00

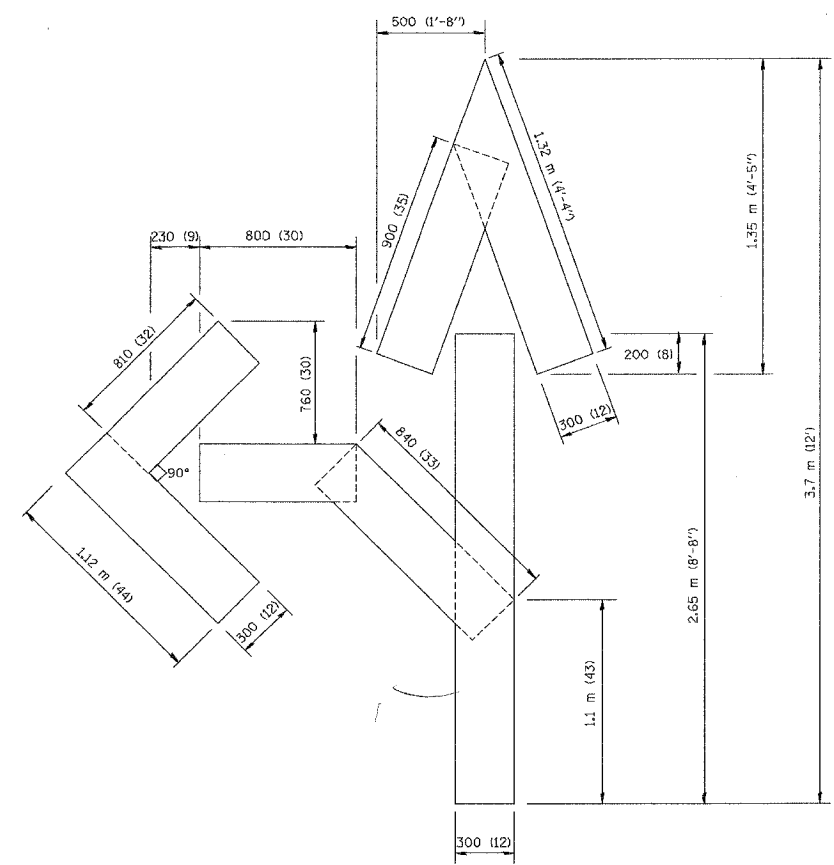
SCALE: NONE
 DATE: 10/18/2002

DRAWN BY
 CHECKED BY LHA
 TC-14

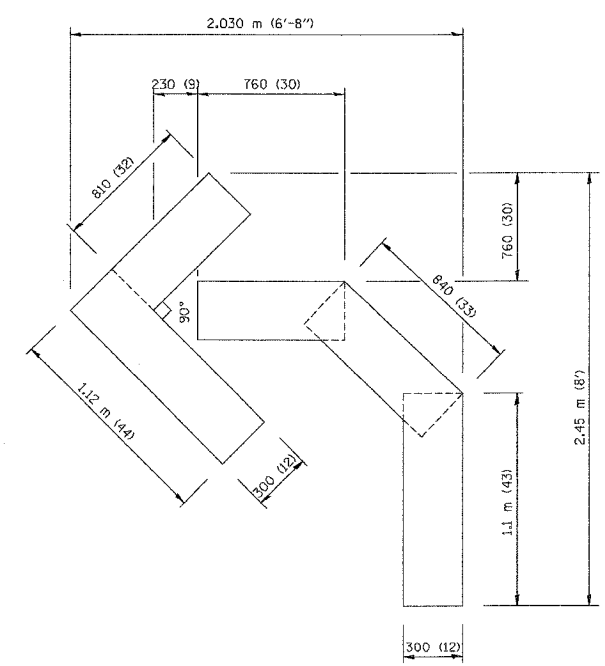
F.A. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	WES-7	DePue	235	142
STA.	TO STA.		FED. AID PROJECT	
			#62291	



QUANTITY
 100 (4) LINE = 19.7 m (64.1 ft.)
 1.97 sq. m (21.1 sq. ft.)



QUANTITY
 100 (4) LINE = 25.3 m (82.5 ft.)
 2.53 sq. m (27.5 sq. ft.)



QUANTITY
 100 (4) LINE = 13.9 m (45.5 ft.)
 1.39 sq. m (15.2 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

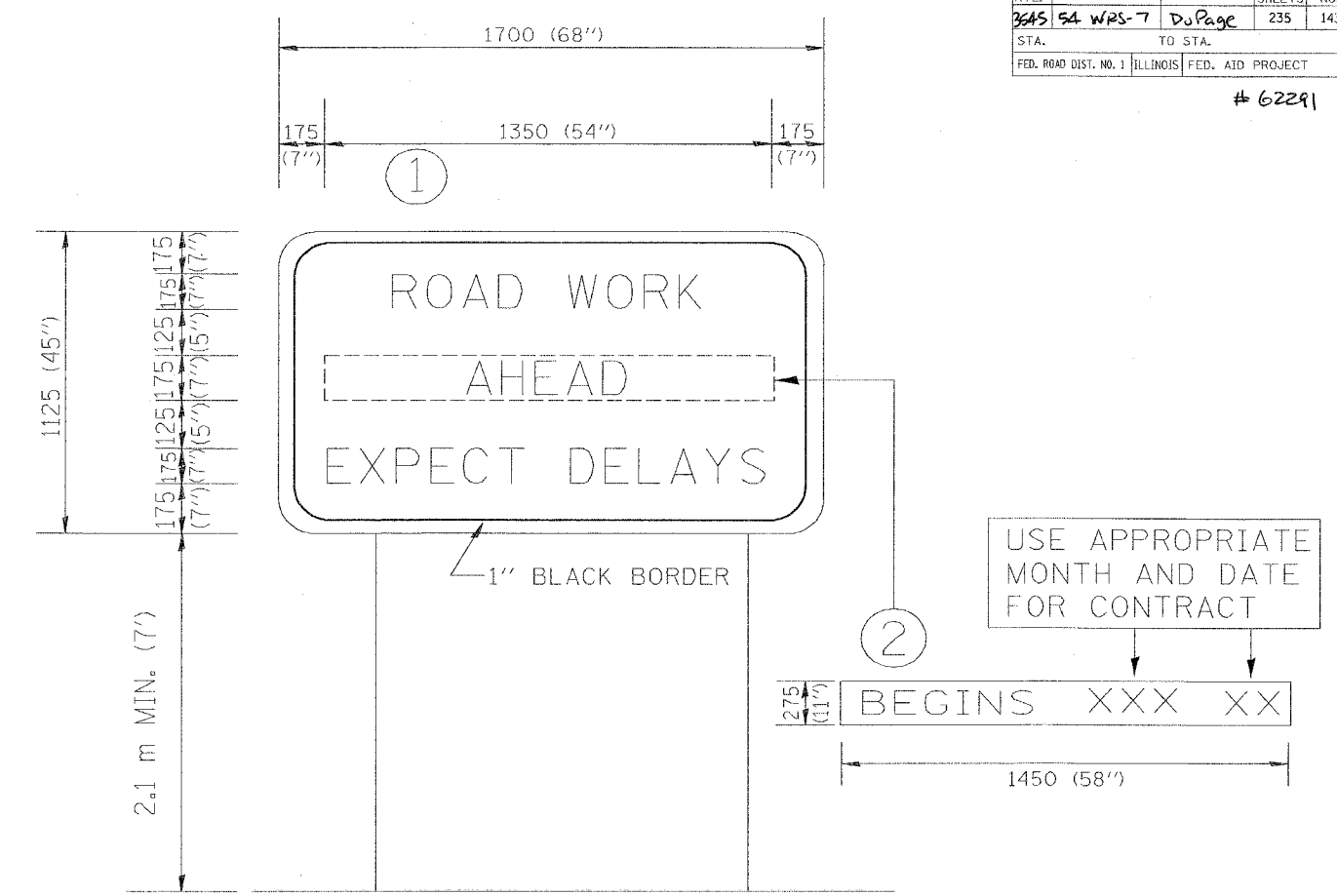
REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

SCALE: NONE
 DATE **DATE**

DRAWN BY CADD
 CHECKED BY

F.A. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3445	54 WPS-7	DoPage	235	143
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

62291



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 2.3 SQ. M. (25.70 SQ. FT.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
R. MIRS	9-15-97	TEMPORARY INFORMATION SIGNING
R. MIRS	12-11-97	
T. RAMMACHER	2-2-99	

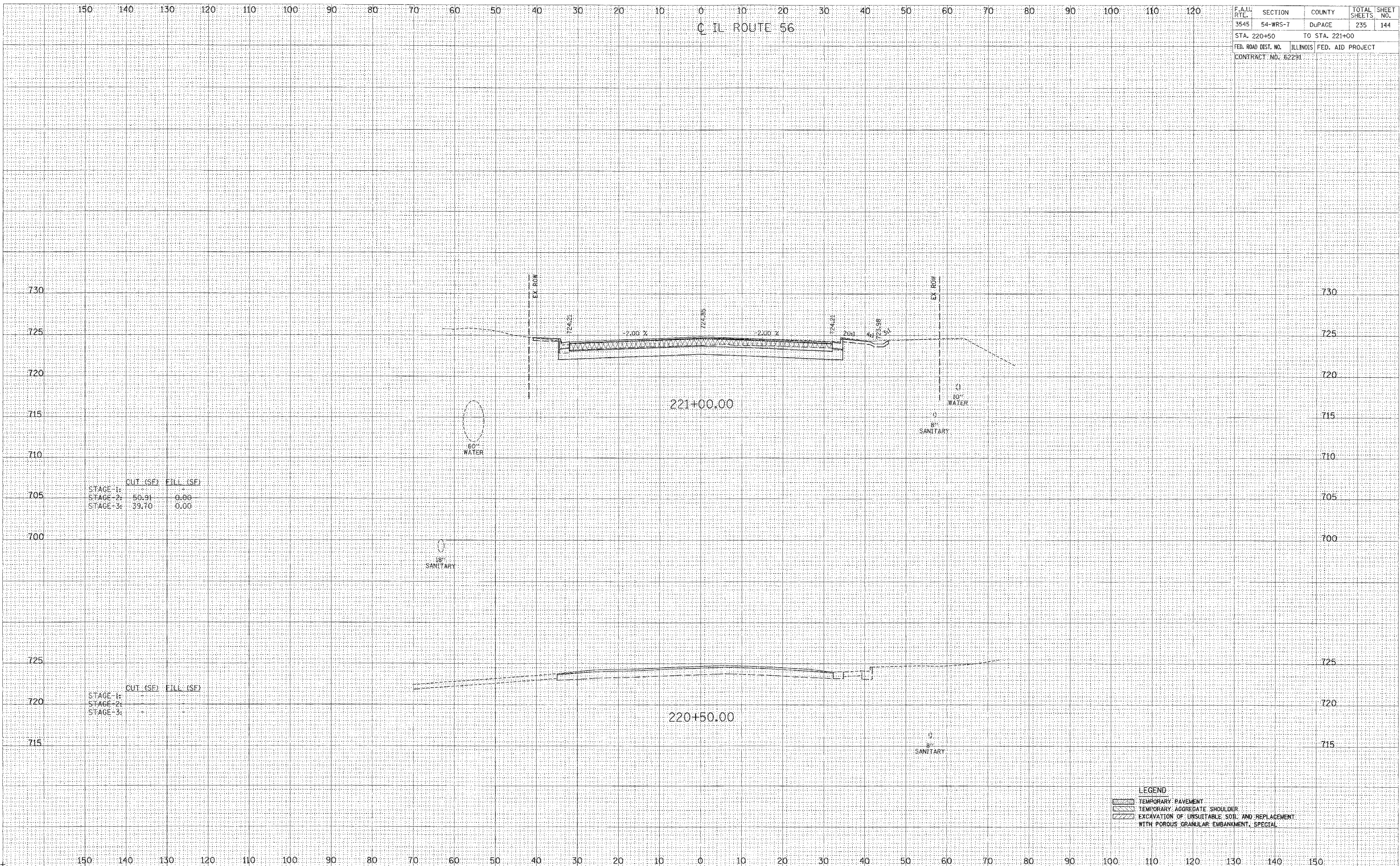
SCALE: DATE 10/18/2002 DRAWN BY: BUR. OF DESIGN CHECKED BY:

CL IL ROUTE 56

F.A.U. RTL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	144
STA. 220+50		TO STA. 221+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

DATE	BY
DATE	BY
DATE	BY
DATE	BY

DATE	BY
DATE	BY
DATE	BY
DATE	BY



	CUT (SF)	FILL (SF)
STAGE 1:		
STAGE 2:	50.91	0.00
STAGE 3:	39.70	0.00

	CUT (SF)	FILL (SF)
STAGE 1:		
STAGE 2:		
STAGE 3:		

LEGEND

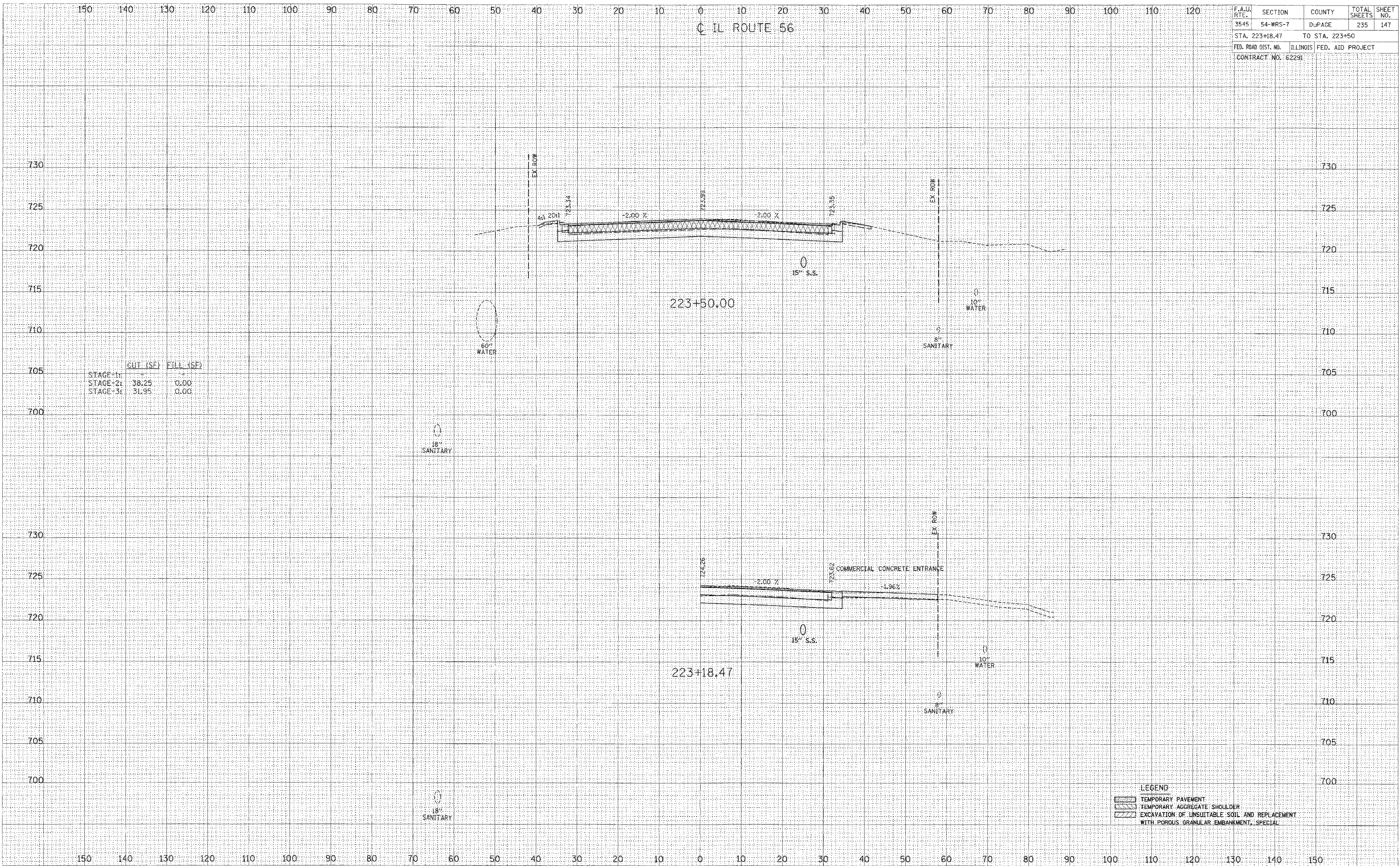
	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	147
STA. 223+18.47		TO STA. 223+50		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62291				

IL ROUTE 56

DATE _____
 BY _____
 SURVEYED _____
 PLOTTED _____
 FINAL SURVEY _____
 NOTE BOOK _____
 NO. _____
 AREAS CHECKED _____

DATE _____
 BY _____
 SURVEYED _____
 PLOTTED _____
 ORIGINAL SURVEY _____
 NOTE BOOK _____
 NO. _____
 AREAS CHECKED _____



	CUT (SF)	FILL (SF)
STAGE-1:		
STAGE-2:	38.25	0.00
STAGE-3:	31.95	0.00

LEGEND

[Pattern]	TEMPORARY PAVEMENT
[Pattern]	TEMPORARY AGGREGATE SHOULDER
[Pattern]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

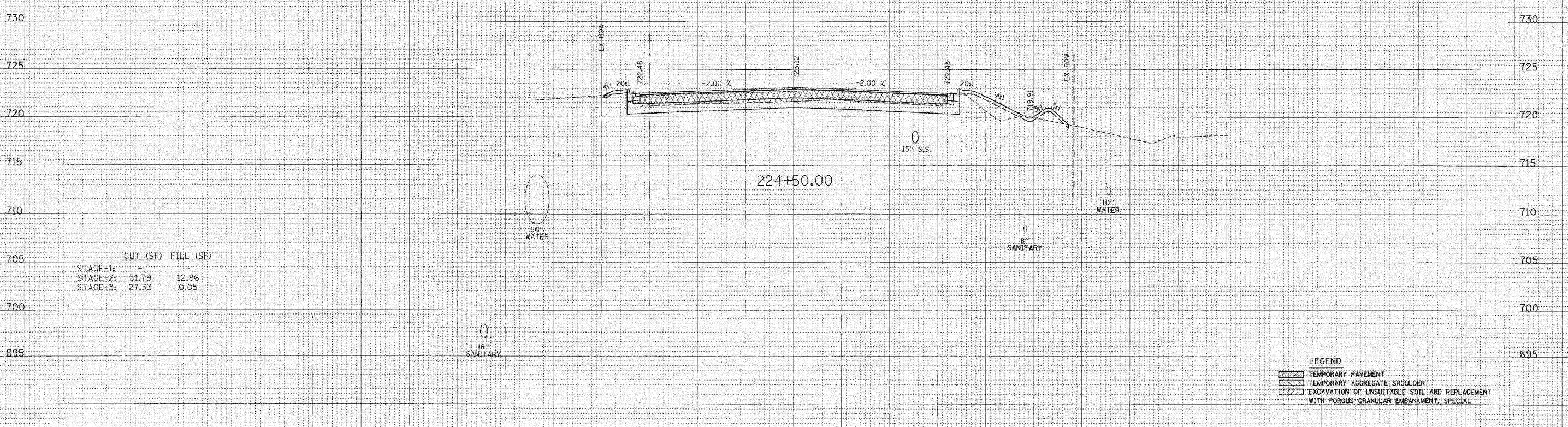
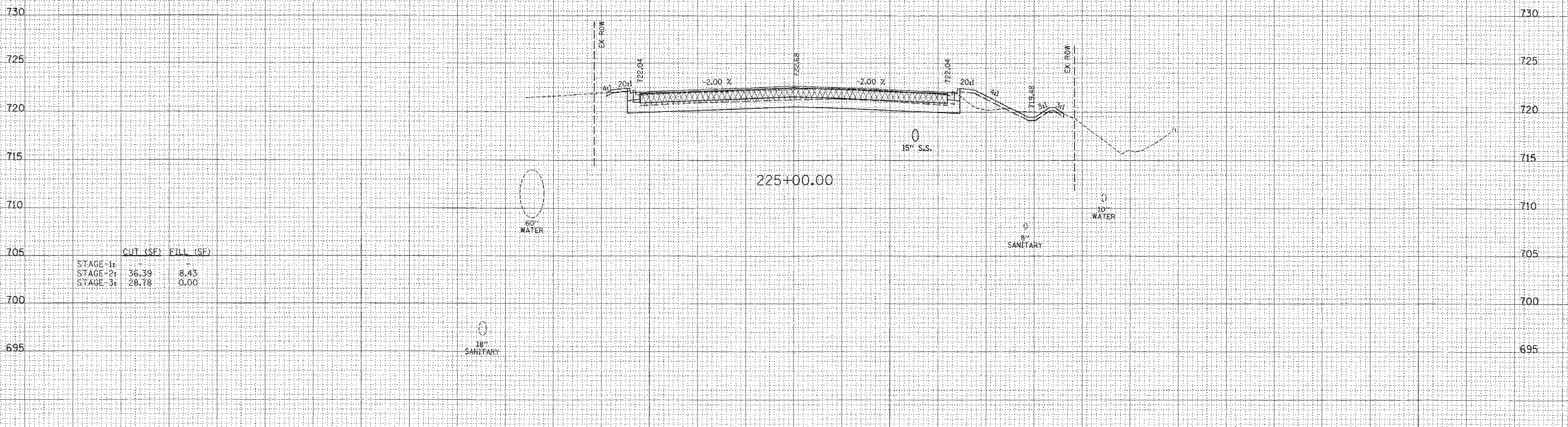
150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

IL ROUTE 56

F.A.U. RTE. 3545	SECTION 54-WRS-7	COUNTY DUPAGE	TOTAL SHEETS 235	SHEET NO. 149
STA. 224+50		TO STA. 225+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

DATE	
BY	
REVIEWED	
DESIGNED	
CHECKED	
DATE	
BY	
REVIEWED	
DESIGNED	
CHECKED	
DATE	
BY	
REVIEWED	
DESIGNED	
CHECKED	

DATE	
BY	
REVIEWED	
DESIGNED	
CHECKED	
DATE	
BY	
REVIEWED	
DESIGNED	
CHECKED	
DATE	
BY	
REVIEWED	
DESIGNED	
CHECKED	



	CUT (SF)	FILL (SF)
STAGE-1:		
STAGE-2:	36.39	8.43
STAGE-3:	28.78	0.00

	CUT (SF)	FILL (SF)
STAGE-1:		
STAGE-2:	31.79	12.86
STAGE-3:	27.33	0.05

- LEGEND
- TEMPORARY PAVEMENT
 - TEMPORARY AGGREGATE SHOULDER
 - EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

IL ROUTE 56

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	150
STA. 225+50		TO STA. 226+00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62291				

DATE	BY
DATE	BY
DATE	BY

NO.	AREAS CHECKED

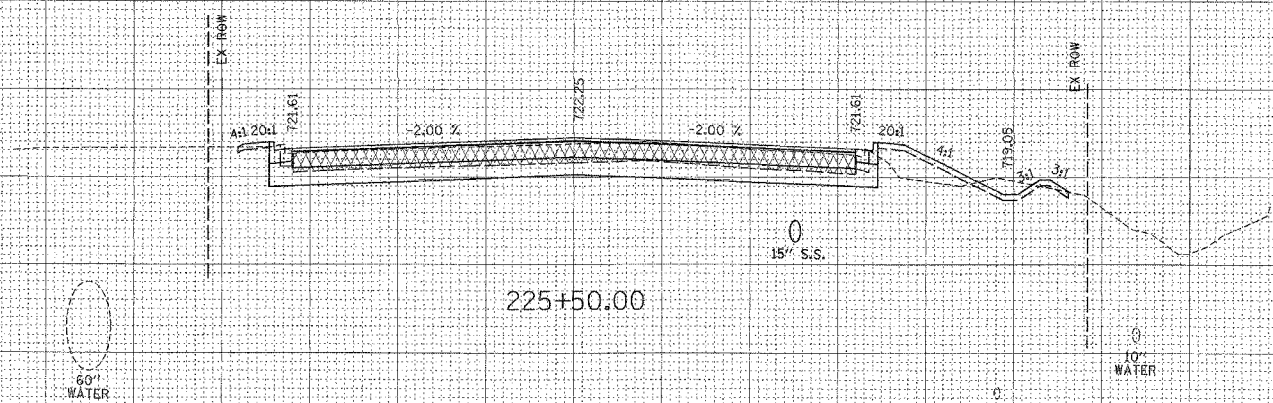
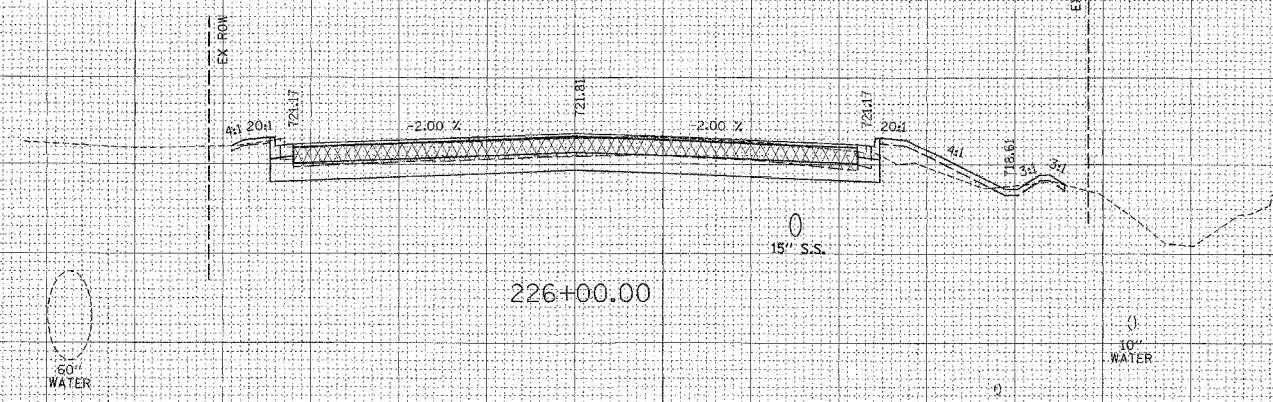
NO.	AREAS CHECKED

NO.	AREAS CHECKED

NO.	AREAS CHECKED

STAGE	CUT (SF)	FILL (SF)
STAGE-1:		
STAGE-2:	33.35	7.63
STAGE-3:	31.05	0.00

STAGE	CUT (SF)	FILL (SF)
STAGE-1:		
STAGE-2:	35.18	9.70
STAGE-3:	29.00	0.00



LEGEND

[Symbol]	TEMPORARY PAVEMENT
[Symbol]	TEMPORARY AGGREGATE SHOULDER
[Symbol]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

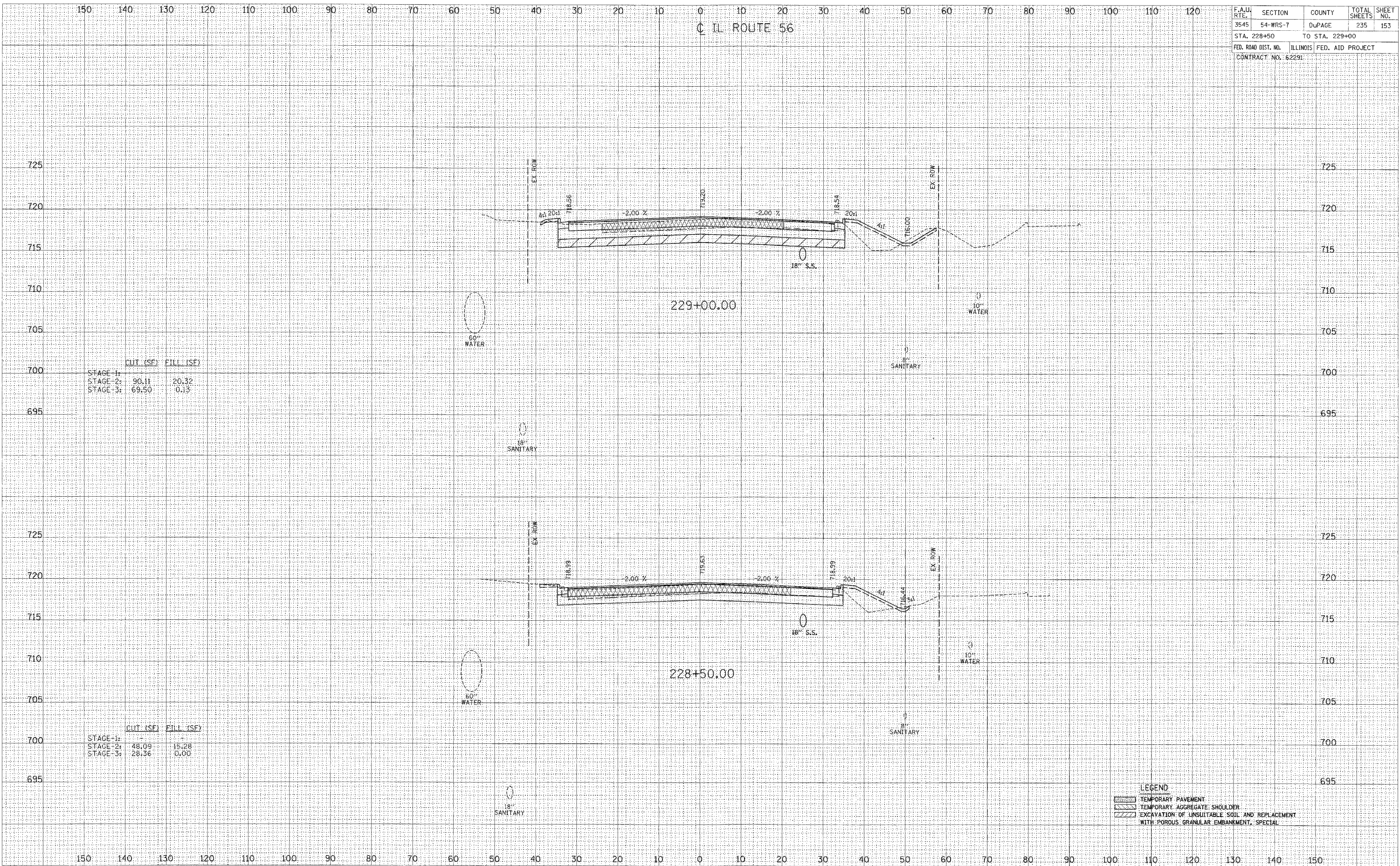
150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

IL ROUTE 56

F.A.U. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	153
STA. 228+50		TO STA. 229+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

DATE	
BY	
FINAL SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	



	CUT (SF)	FILL (SF)
STAGE 1:	-	-
STAGE 2:	90.11	20.32
STAGE 3:	69.50	0.13

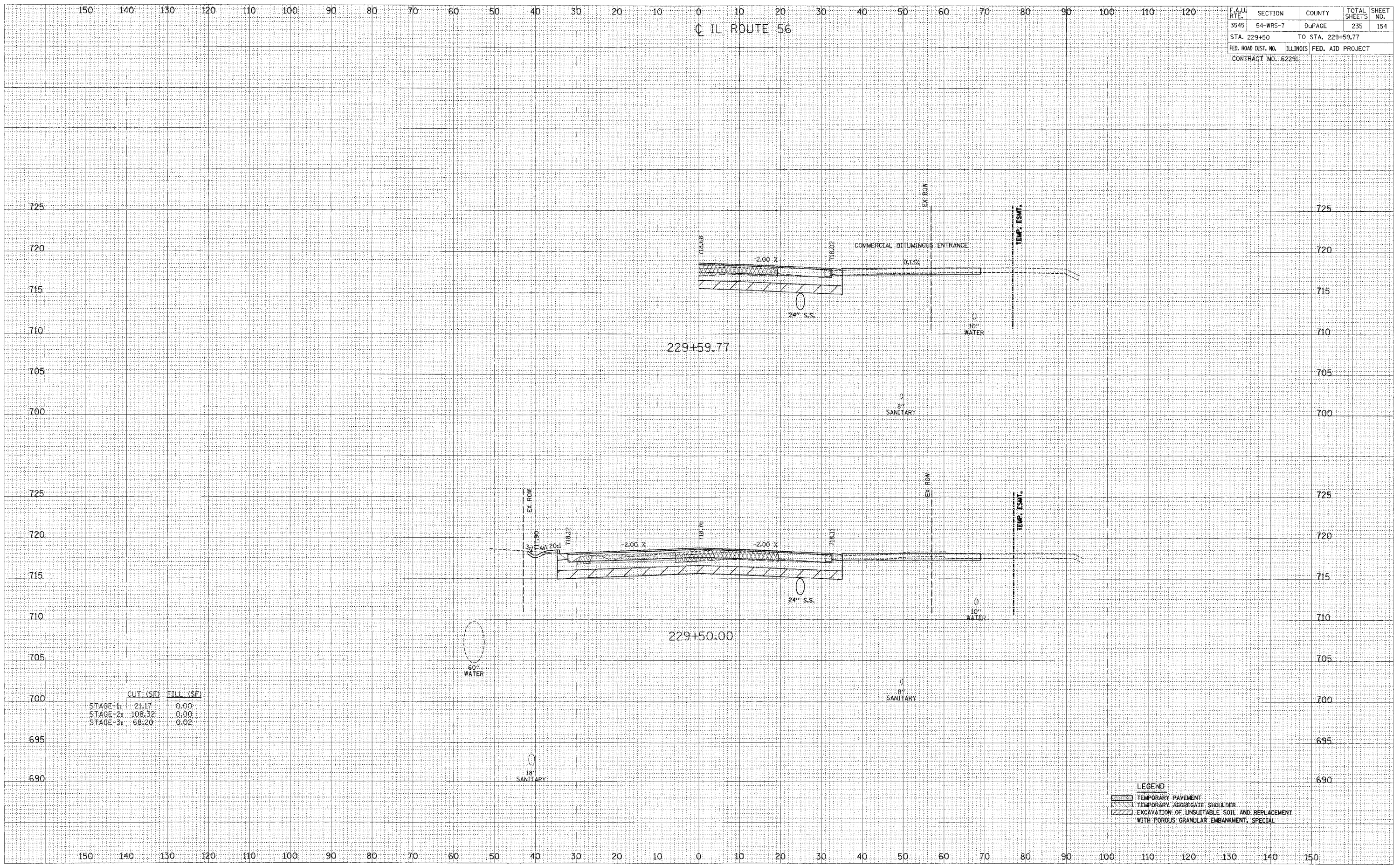
	CUT (SF)	FILL (SF)
STAGE 1:	-	-
STAGE 2:	48.09	15.28
STAGE 3:	28.36	0.00

LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

FINAL SURVEY	DATE
BY	
DATE	
NO.	
NO.	
NO.	
NO.	
NO.	

ORIGINAL SURVEY	DATE
BY	
DATE	
NO.	
NO.	
NO.	
NO.	
NO.	



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	154
STA. 229+50	TO STA. 229+59.77			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				

	CUT (SF)	FILL (SF)
STAGE-1:	21.17	0.00
STAGE-2:	108.32	0.00
STAGE-3:	68.20	0.02

LEGEND	
[Hatched Box]	TEMPORARY PAVEMENT
[Dotted Box]	TEMPORARY AGGREGATE SHOULDER
[Diagonal Lines Box]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

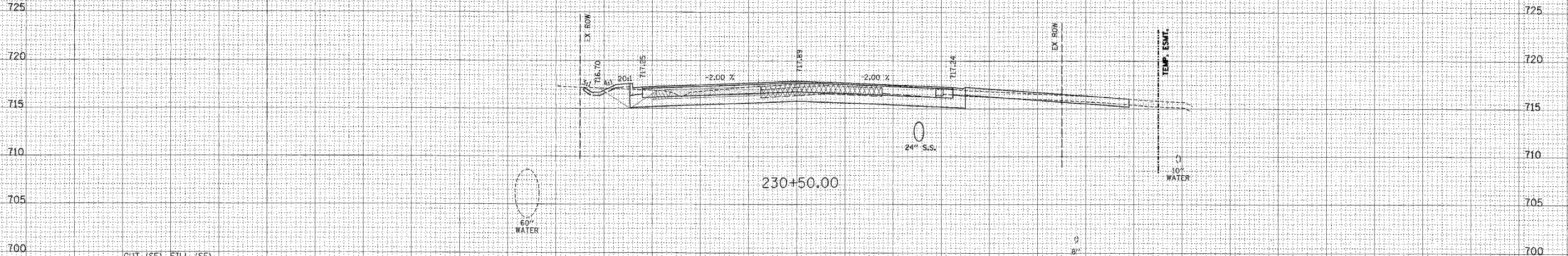
150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

IL ROUTE 56

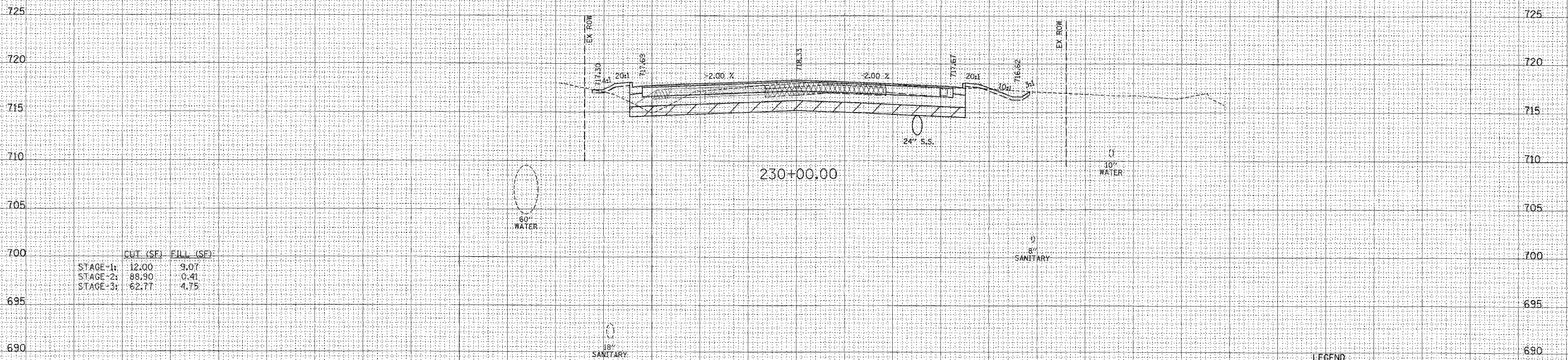
F.A.U. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	155
STA. 230+00		TO STA. 230+50		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

DATE _____ BY _____
 SURVEYED _____
 PLOTTED _____
 NOTE BOOK _____
 TEMPLATE _____
 AREAS CHECKED _____
 NO. _____

DATE _____ BY _____
 SURVEYED _____
 PLOTTED _____
 NOTE BOOK _____
 TEMPLATE _____
 AREAS CHECKED _____
 NO. _____



	CUT (SF)	FILL (SF)
STAGE-1:	28.70	0.00
STAGE-2:	72.04	0.00
STAGE-3:	30.36	6.41



	CUT (SF)	FILL (SF)
STAGE-1:	12.00	9.07
STAGE-2:	88.90	0.41
STAGE-3:	62.77	4.75

LEGEND

[Pattern]	TEMPORARY PAVEMENT
[Pattern]	TEMPORARY AGGREGATE SHOULDER
[Pattern]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

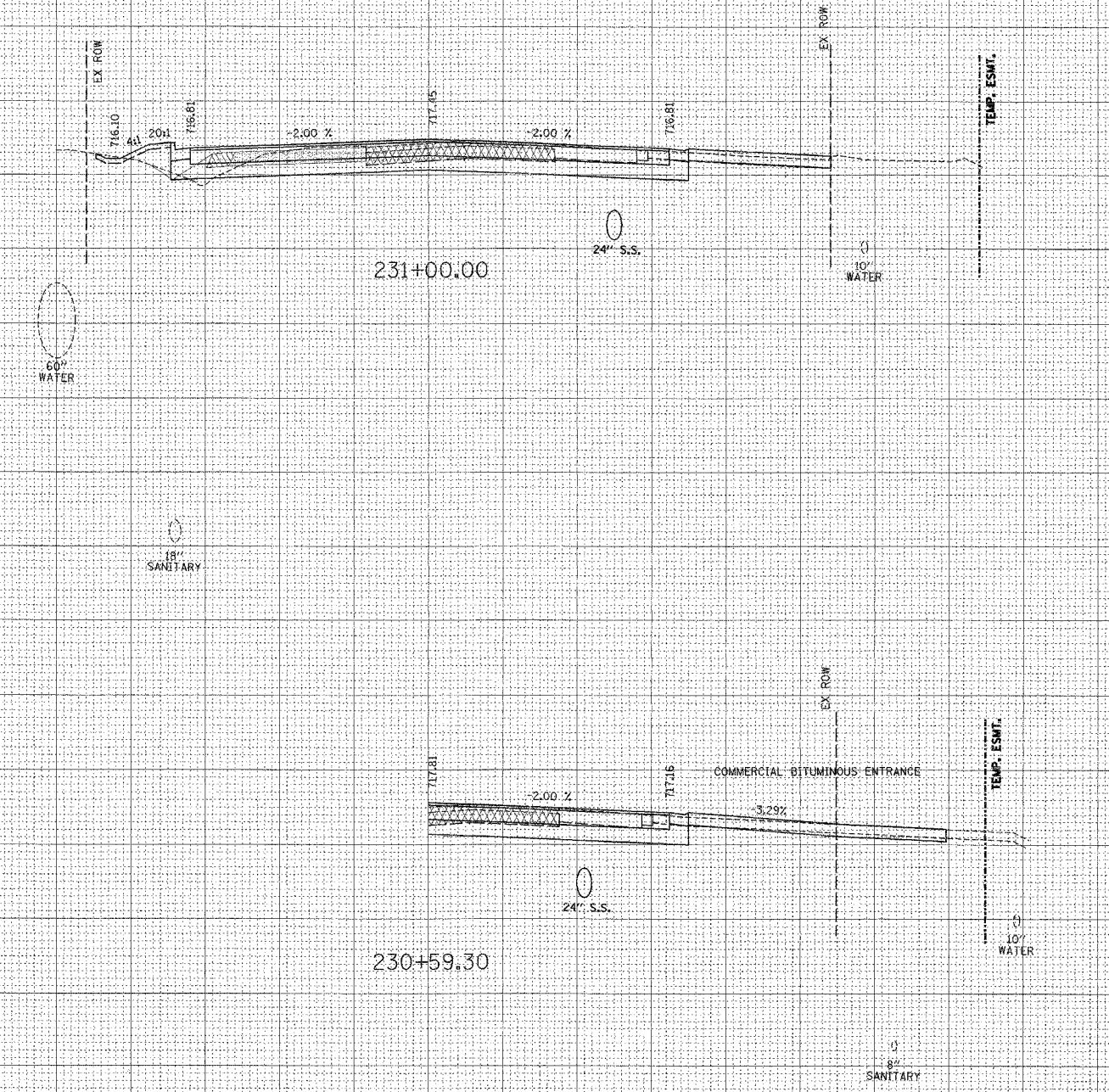
IL ROUTE 56

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	156
STA. 230+59.30		TO STA. 231+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

DATE
BY
SUPERVISOR
PLOTTED
TEMPLATE
NOTE BOOK
AREAS CHECKED
NO.

DATE
BY
SUPERVISOR
PLOTTED
TEMPLATE
NOTE BOOK
AREAS CHECKED
NO.

	CUT (SF)	FILL (SF)
STAGE-1:	15.73	5.98
STAGE-2:	59.95	0.00
STAGE-3:	29.39	5.93



LEGEND

[Hatched Box]	TEMPORARY PAVEMENT
[Dotted Box]	TEMPORARY AGGREGATE SHOULDER
[Cross-hatched Box]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

DATE

BY

FINAL SURVEY SHEET PLOTTED TEMPLATE AREAS CHECKED

NO.

DATE

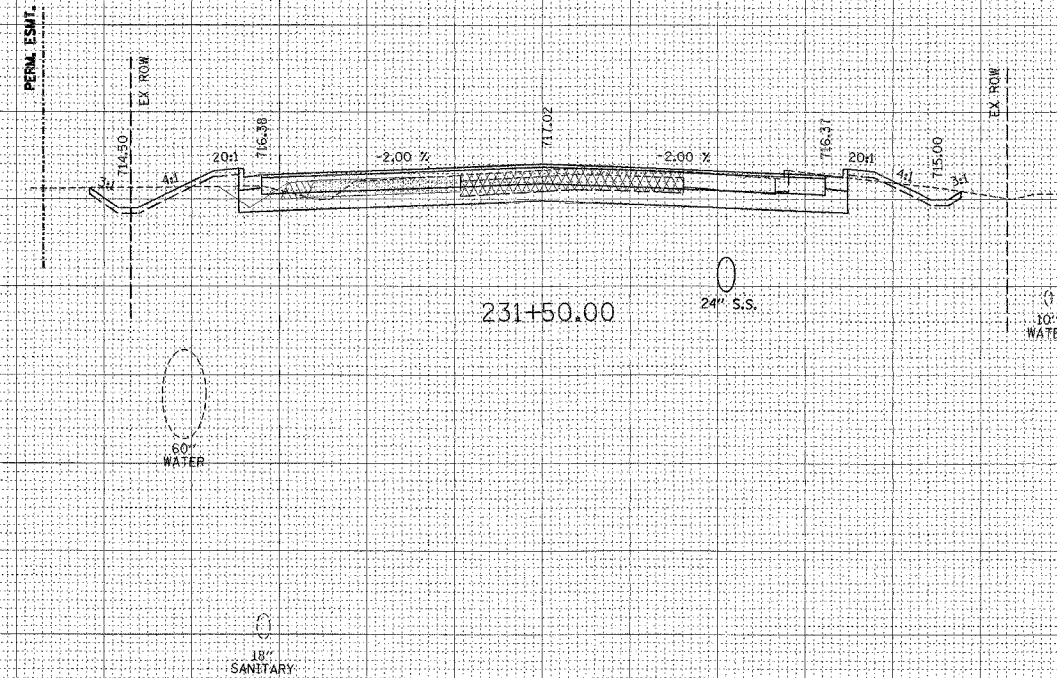
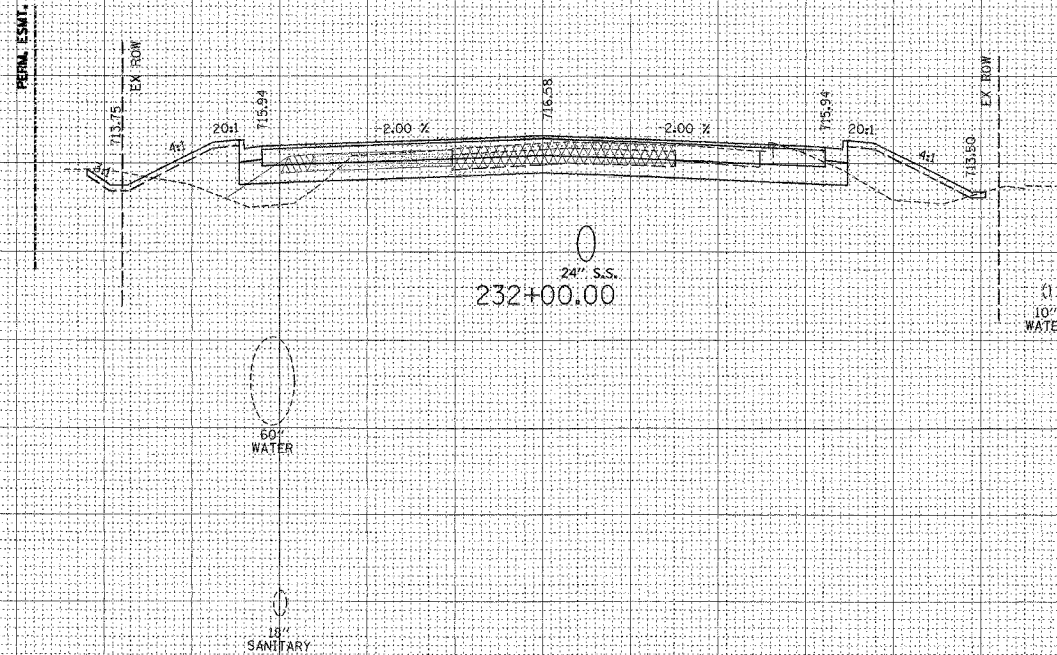
BY

ORIGINAL SURVEY SHEET PLOTTED TEMPLATE AREAS CHECKED

NO.

STAGE-1:	10.73	16.07
STAGE-2:	41.96	22.20
STAGE-3:	27.24	16.34

	CUT (SF)	FILL (SF)
STAGE-1:	16.79	0.37
STAGE-2:	54.67	0.59
STAGE-3:	37.30	3.51

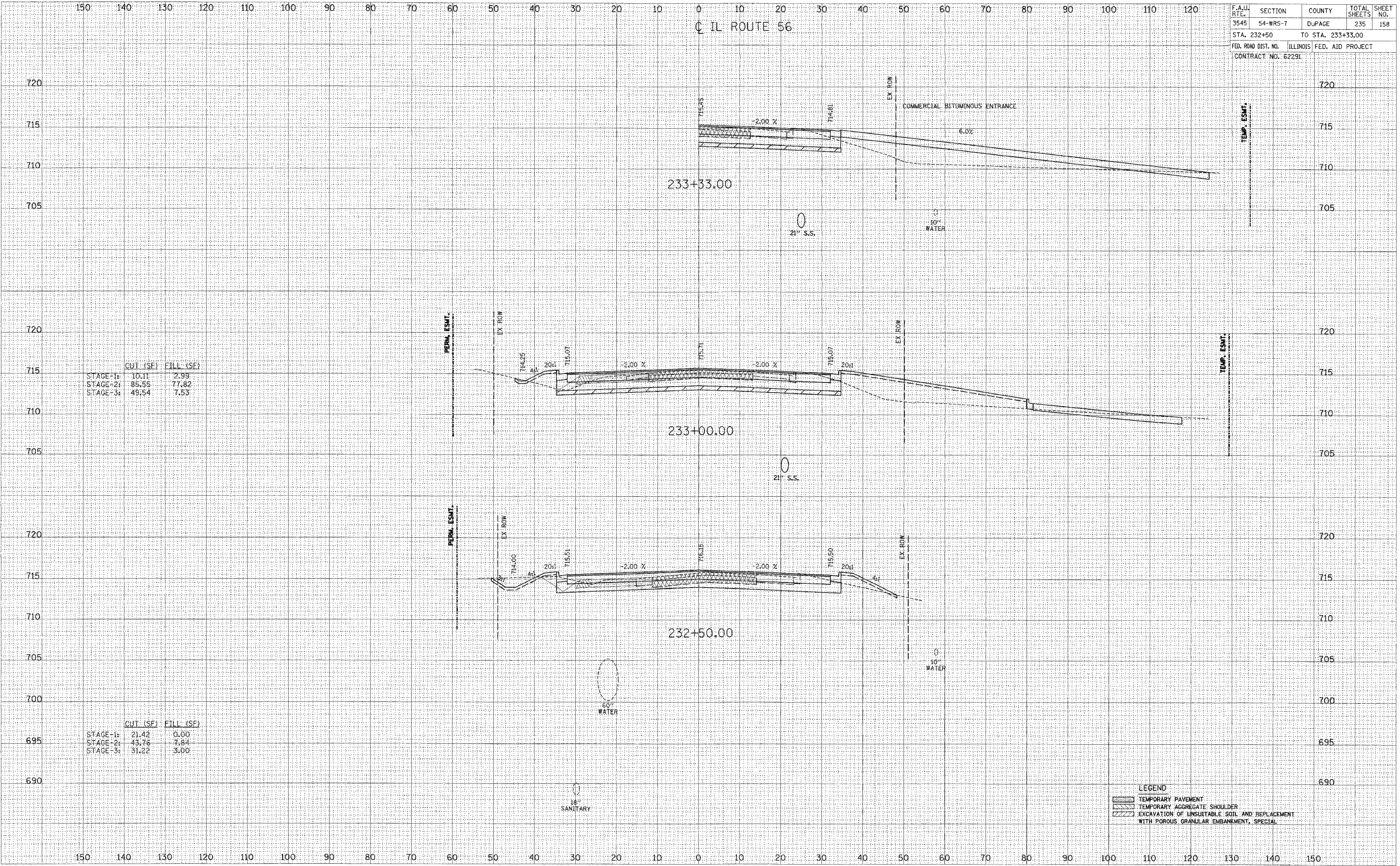


- LEGEND**
- [Hatched Pattern] TEMPORARY PAVEMENT
 - [Dotted Pattern] TEMPORARY AGGREGATE SHOULDER
 - [Cross-hatched Pattern] EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

DATE	
BY	
SURVEYED	
FINAL SURVEY	
NOTE BOOK	
NO.	
AREAS CHECKED	

DATE	
BY	
SURVEYED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	
AREAS CHECKED	

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	158
STA. 232+50		TO STA. 233+33.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				



	CUT (SF)	FILL (SF)
STAGE-1:	10.11	2.99
STAGE-2:	85.55	77.82
STAGE-3:	49.54	7.53

	CUT (SF)	FILL (SF)
STAGE-1:	21.42	0.00
STAGE-2:	43.76	7.84
STAGE-3:	31.22	3.00

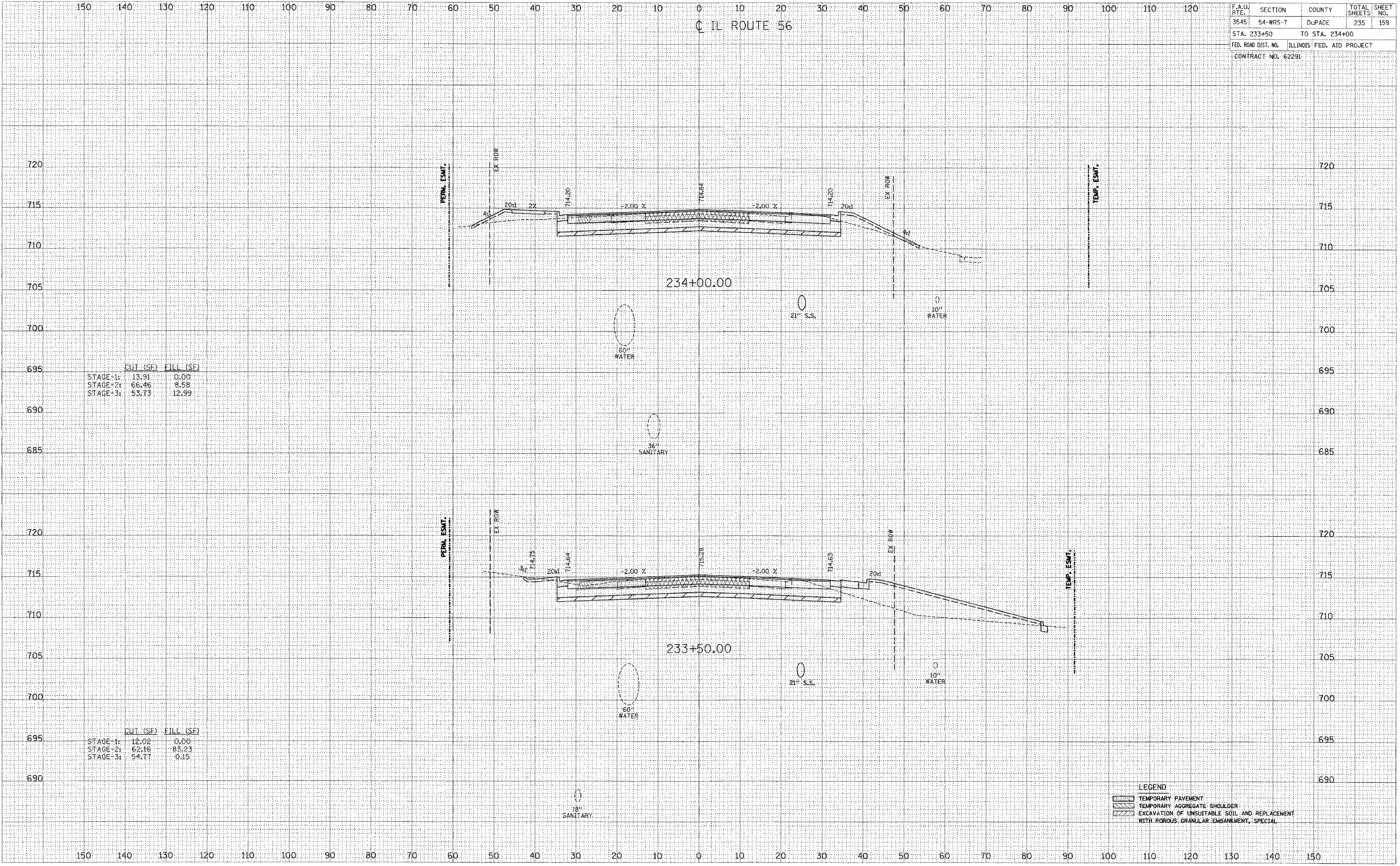
LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

IL ROUTE 56

DATE	
BY	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
FINAL SURVEY	
NOTE BOOK	
NO.	



	CUT (SF)	FILL (SF)
STAGE-1:	13.91	0.00
STAGE-2:	66.46	8.58
STAGE-3:	53.73	12.99

	CUT (SF)	FILL (SF)
STAGE-1:	12.02	0.00
STAGE-2:	62.16	85.23
STAGE-3:	54.77	0.15

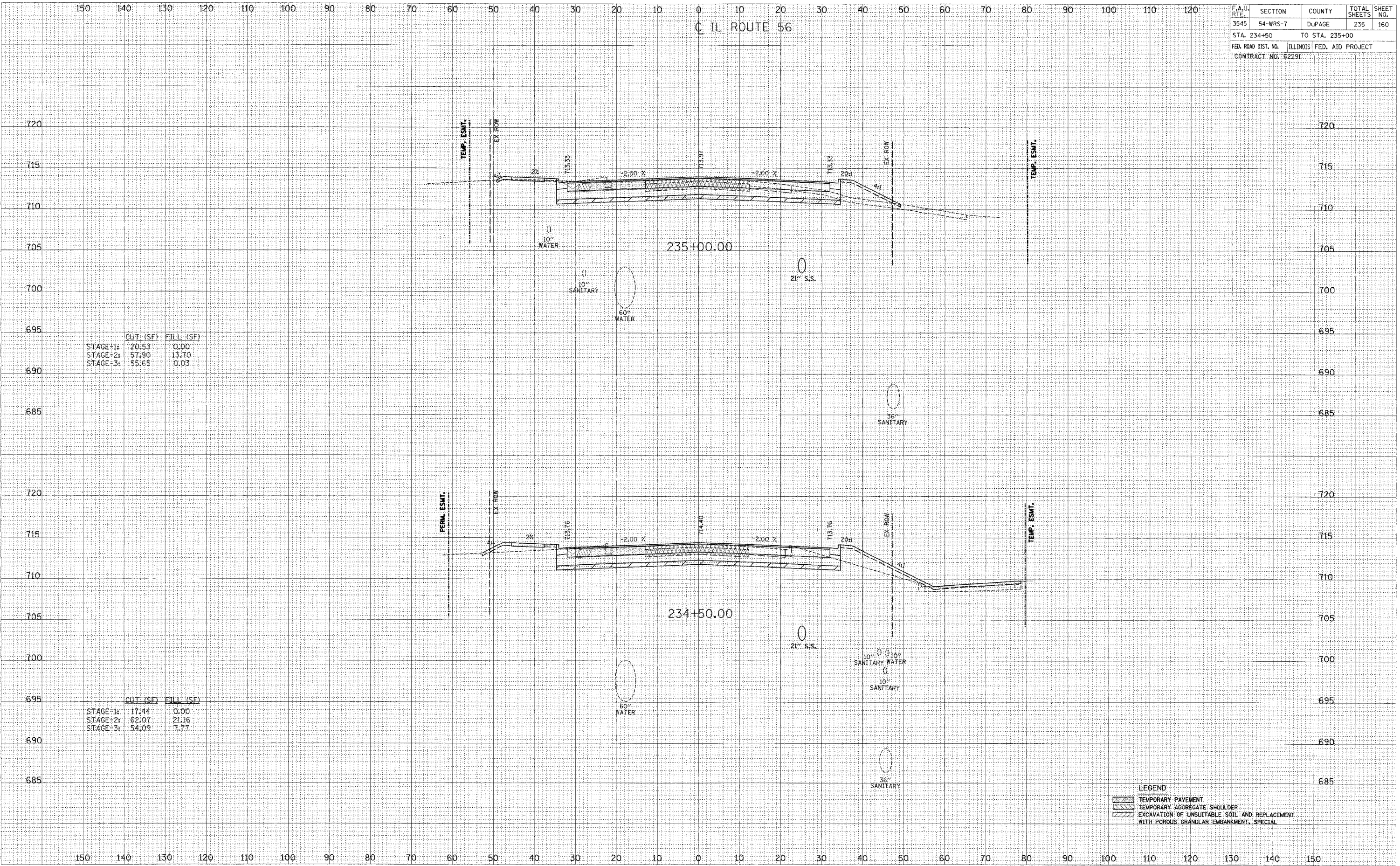
- LEGEND
- TEMPORARY PAVEMENT
 - TEMPORARY AGGREGATE SHOULDER
 - EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	160
STA. 234+50		TO STA. 235+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

C IL ROUTE 56

DATE _____
 BY _____
 SURVEYED _____
 CHECKED _____
 NOTE BOOK _____
 AREAS CHECKED _____
 NO. _____

DATE _____
 BY _____
 SURVEYED _____
 CHECKED _____
 NOTE BOOK _____
 AREAS CHECKED _____
 NO. _____



	CUT (SF)	FILL (SF)
STAGE-1:	20.53	0.00
STAGE-2:	57.90	13.70
STAGE-3:	55.65	0.03

	CUT (SF)	FILL (SF)
STAGE-1:	17.44	0.00
STAGE-2:	62.07	21.16
STAGE-3:	54.09	7.77

- LEGEND
- TEMPORARY PAVEMENT
 - TEMPORARY AGGREGATE SHOULDER
 - EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

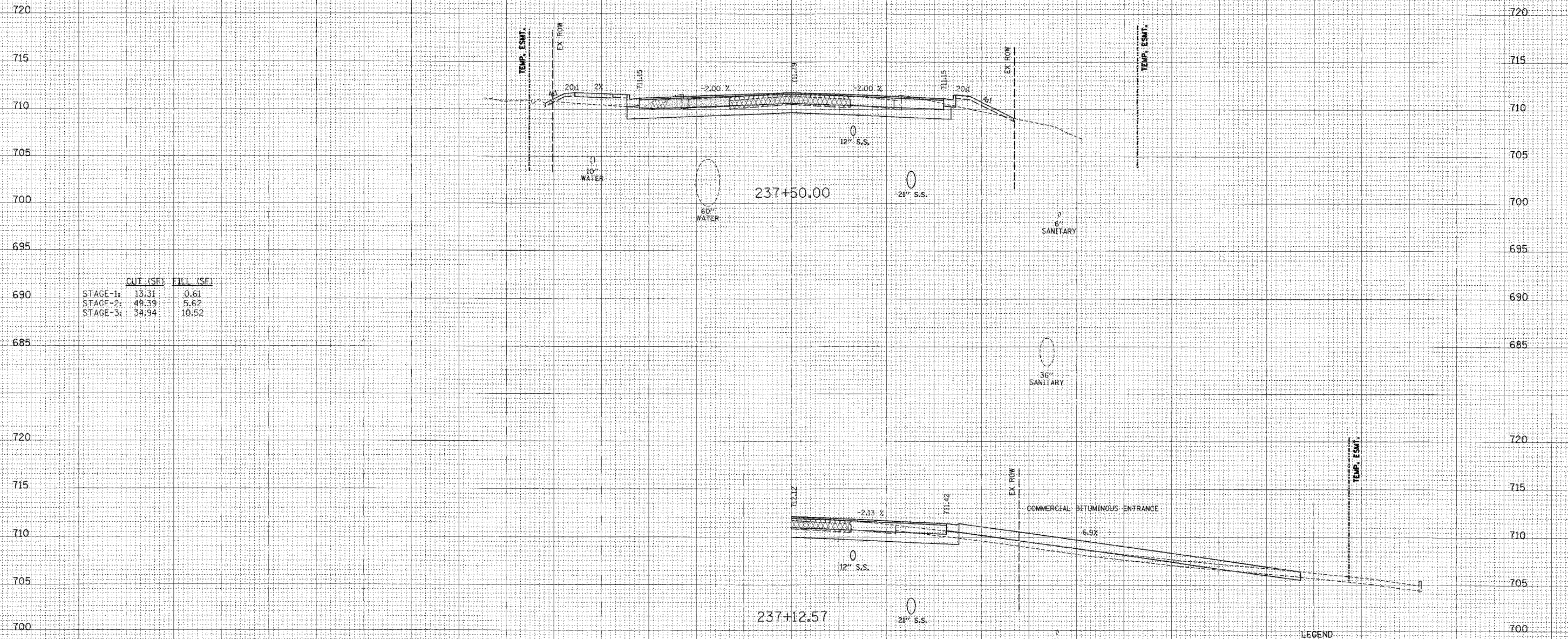
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	163
STA. 237+12.57		TO STA. 237+50		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 6229				

IL ROUTE 56

DATE	BY	FINAL SURVEY	SURVEYED
		REVISION	DATE
		NOTE BOOK	NO.
		AREAS CHECKED	

DATE	BY	ORIGINAL SURVEY	SURVEYED
		REVISION	DATE
		NOTE BOOK	NO.
		AREAS CHECKED	

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120



	CUT (SF)	FILL (SF)
STAGE-1:	13.31	0.61
STAGE-2:	49.39	5.62
STAGE-3:	34.94	10.52

LEGEND

[Symbol]	TEMPORARY PAVEMENT
[Symbol]	TEMPORARY AGGREGATE SHOULDER
[Symbol]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

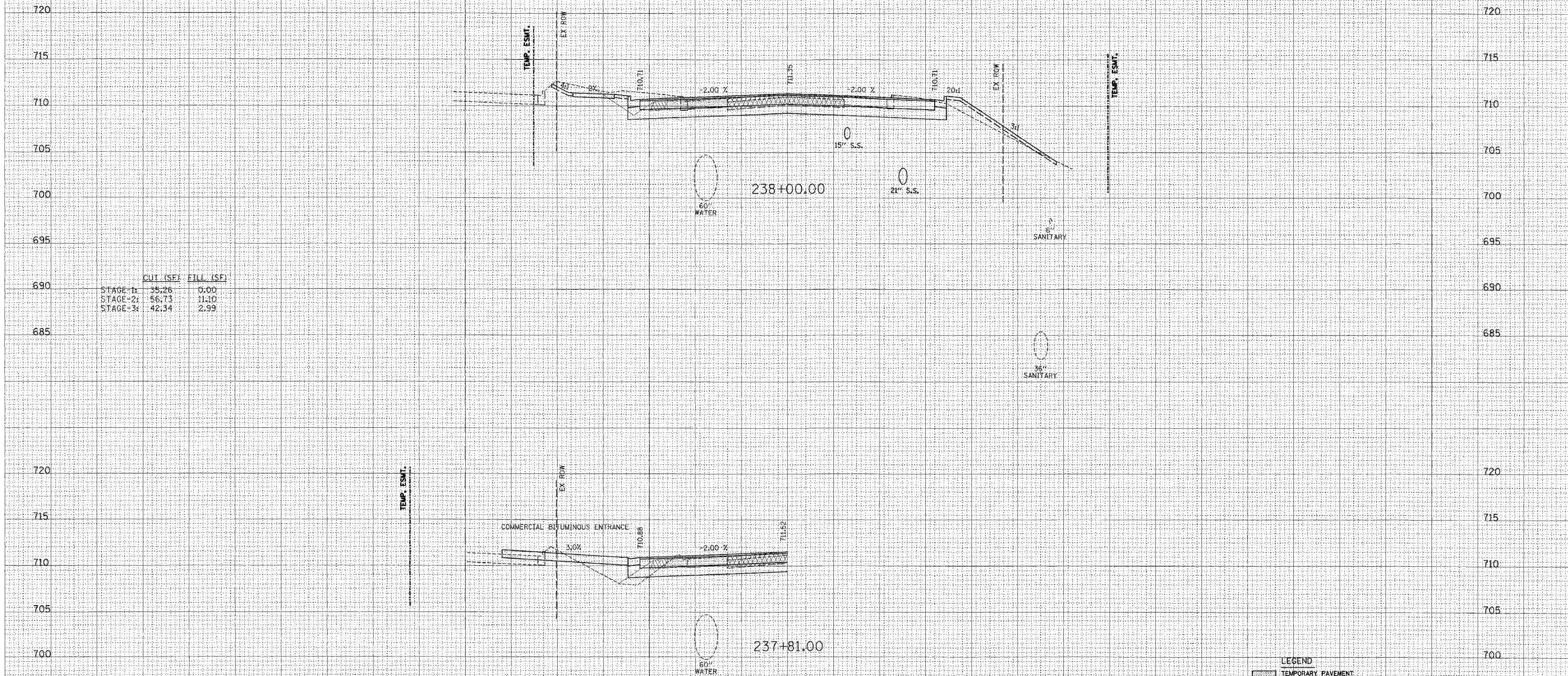
150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

IL ROUTE 56

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	164
STA. 237+81.00		TO STA. 238+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

DATE	BY

DATE	BY



	CUT (SF)	FILL (SF)
STAGE-1:	35.26	0.00
STAGE-2:	56.73	11.10
STAGE-3:	42.34	2.99

LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

F.A.I.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	168
STA. 240+50		TO STA. 241+00		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
CONTRACT NO. 62291				

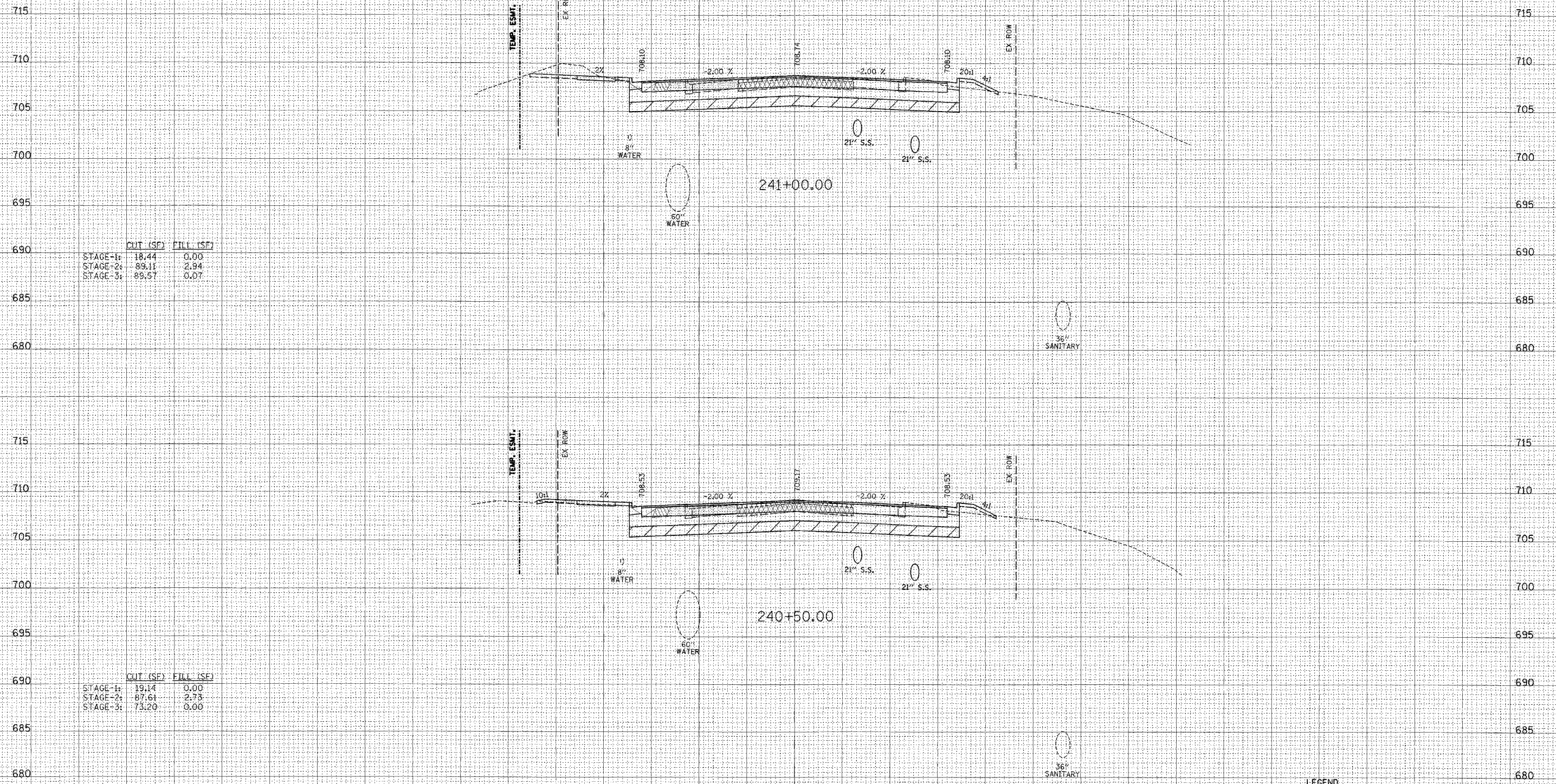
IL ROUTE 56

DATE _____
BY _____
SURVEYED _____
CURVE _____
NOTE BOOK _____
NO. _____
TEMP. DATE _____
AREAS CHECKED _____

DATE _____
BY _____
SURVEYED _____
CURVE _____
NOTE BOOK _____
NO. _____
TEMP. DATE _____
AREAS CHECKED _____

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



	CUT (SF)	FILL (SF)
STAGE-1:	18.44	0.00
STAGE-2:	89.11	2.94
STAGE-3:	89.57	0.07

	CUT (SF)	FILL (SF)
STAGE-1:	19.14	0.00
STAGE-2:	87.61	2.73
STAGE-3:	73.20	0.00

LEGEND

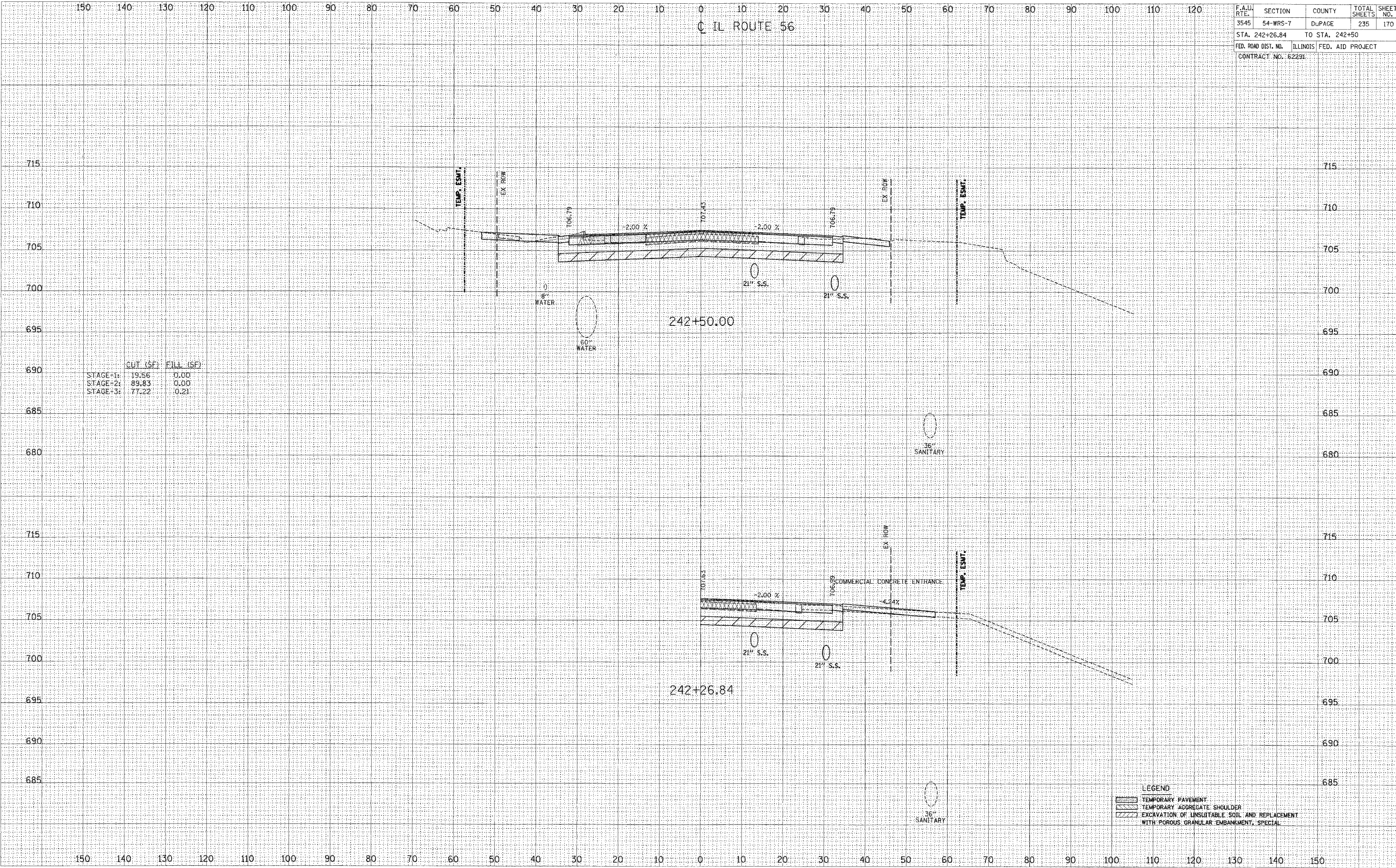
	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	170
STA. 242+26.84		TO STA. 242+50		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

IL ROUTE 56

BY _____ DATE _____
 SURVEYED _____
 SURVEY _____
 NOTE BOOK _____
 TEMPLATE _____
 AREAS CHECKED _____
 NO. _____

BY _____ DATE _____
 SURVEYED _____
 SURVEY _____
 NOTE BOOK _____
 TEMPLATE _____
 AREAS CHECKED _____
 NO. _____



	CUT (SF)	FILL (SF)
STAGE+1:	19.56	0.00
STAGE+2:	89.83	0.00
STAGE+3:	77.22	0.21

LEGEND

[Hatched Box]	TEMPORARY PAVEMENT
[Hatched Box]	TEMPORARY AGGREGATE SHOULDER
[Hatched Box]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	171
STA. 242+74.73		TO STA. 243+00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 6229				

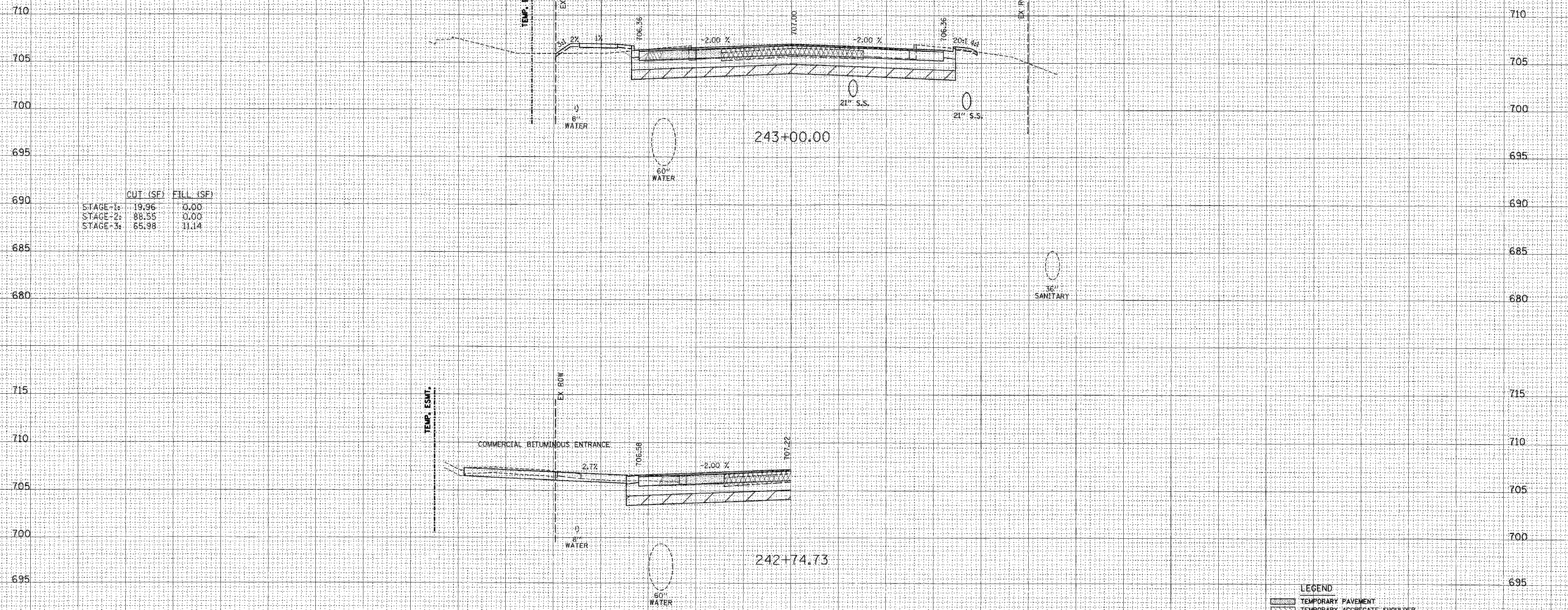
IL ROUTE 56

DATE: _____
 BY: _____
 SURVEYED BY: _____
 CHECKED BY: _____
 FINAL SURVEY: _____
 NOTE BOOK NO.: _____
 AREAS CHECKED: _____

DATE: _____
 BY: _____
 SURVEYED BY: _____
 CHECKED BY: _____
 ORIGINAL SURVEY: _____
 NOTE BOOK NO.: _____
 AREAS CHECKED: _____

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



	CUT (SF)	FILL (SF)
STAGE-1:	19.96	0.00
STAGE-2:	88.55	0.00
STAGE-3:	65.38	11.14

LEGEND

[Pattern]	TEMPORARY PAVEMENT
[Pattern]	TEMPORARY AGGREGATE SHOULDER
[Pattern]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	172
STA. 243+27.03		TO STA. 243+50		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				

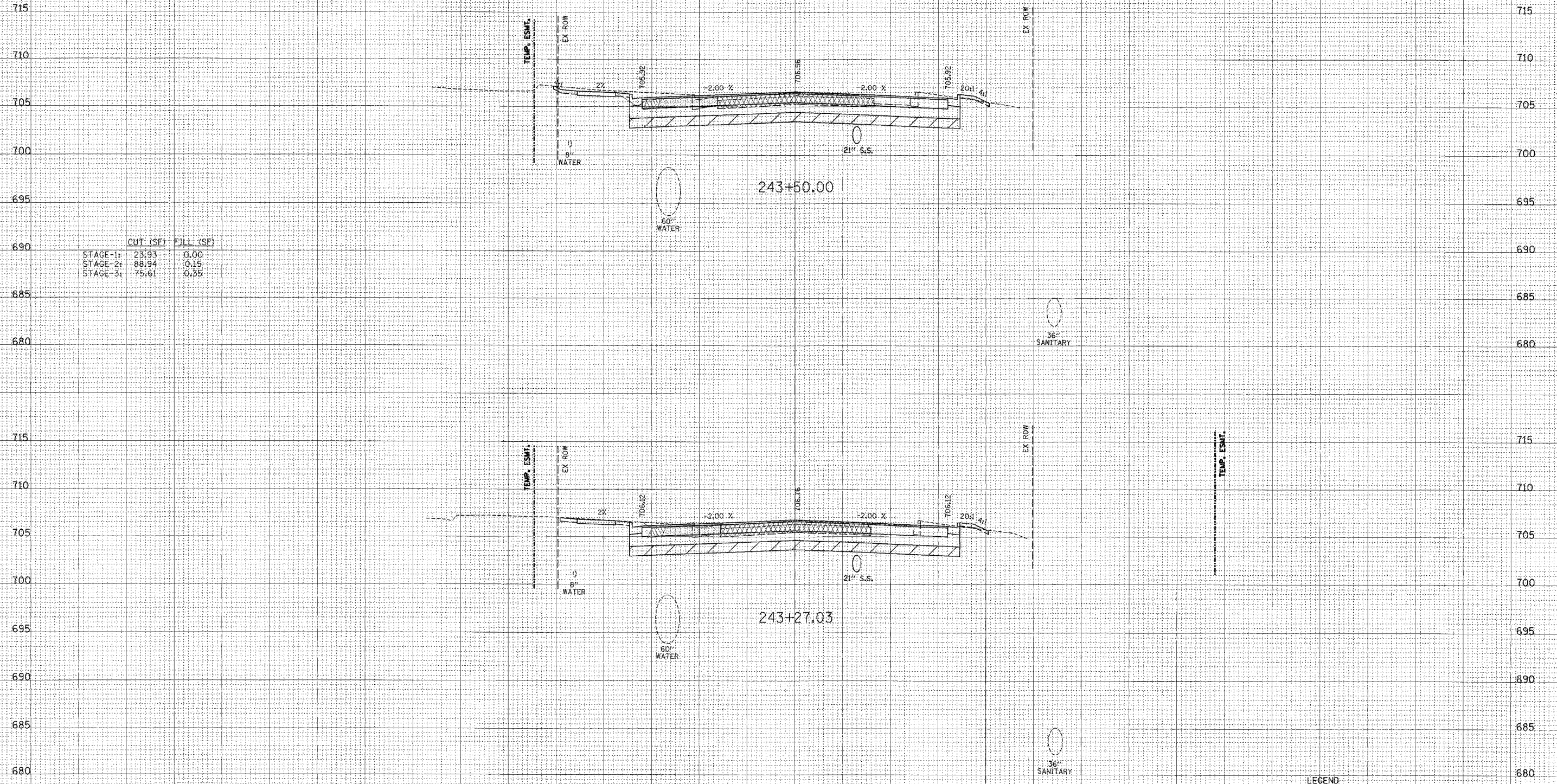
IL ROUTE 56

DATE _____
 BY _____
 SURVEYED _____
 TABLE _____
 NOTE BOOK _____
 AREAS CHECKED _____

DATE _____
 BY _____
 SURVEYED _____
 TABLE _____
 NOTE BOOK _____
 AREAS CHECKED _____

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



	CUT (SF)	FILL (SF)
STAGE-1	23.93	0.00
STAGE-2	88.94	0.15
STAGE-3	75.61	0.35

LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

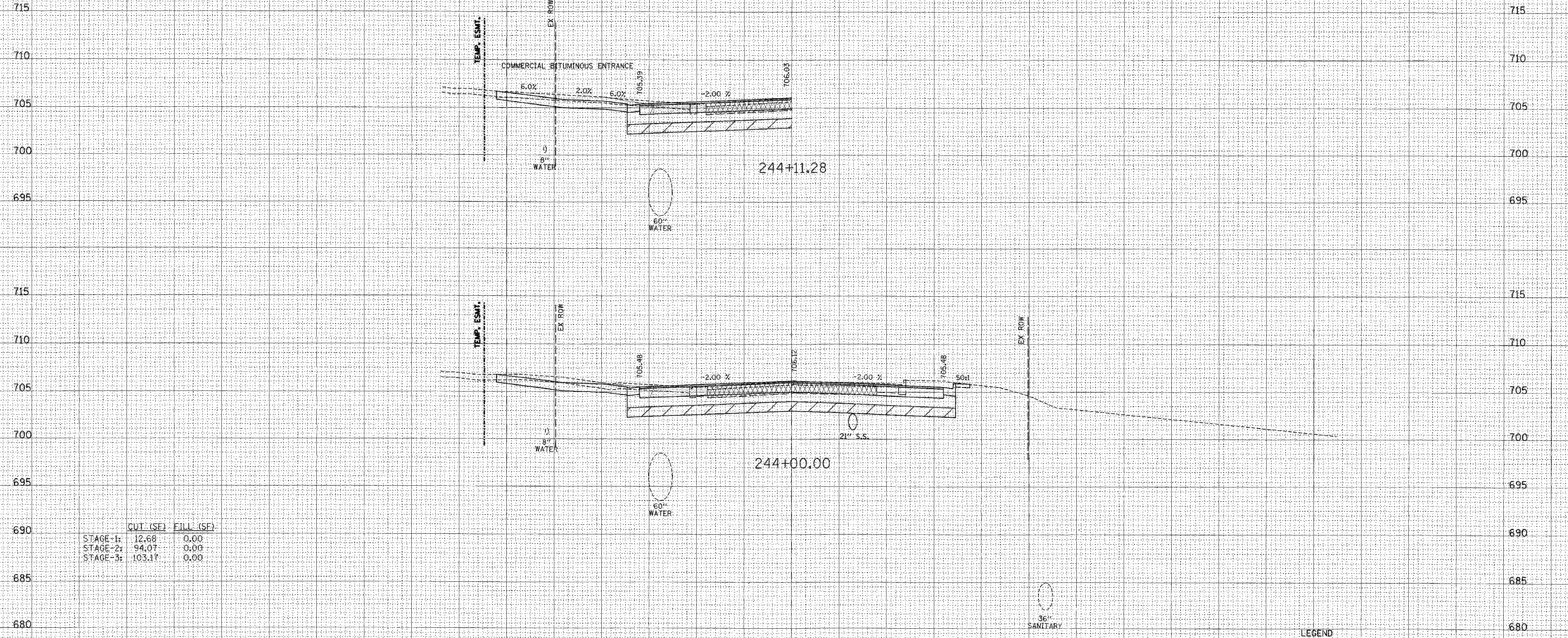
150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

IL ROUTE 56

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	173
STA. 244+00		TO STA. 244+11.28		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62291				

DATE _____
BY _____
SURVEYED _____
PLOTTED _____
NOTE BOOK _____
AREAS CHECKED _____

DATE _____
BY _____
SURVEYED _____
PLOTTED _____
NOTE BOOK _____
AREAS CHECKED _____



	CUT (SF)	FILL (SF)
STAGE-1:	12.68	0.00
STAGE-2:	94.07	0.00
STAGE-3:	103.17	0.00

LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	175
STA. 245+28.70		TO STA. 245+50		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 62291				

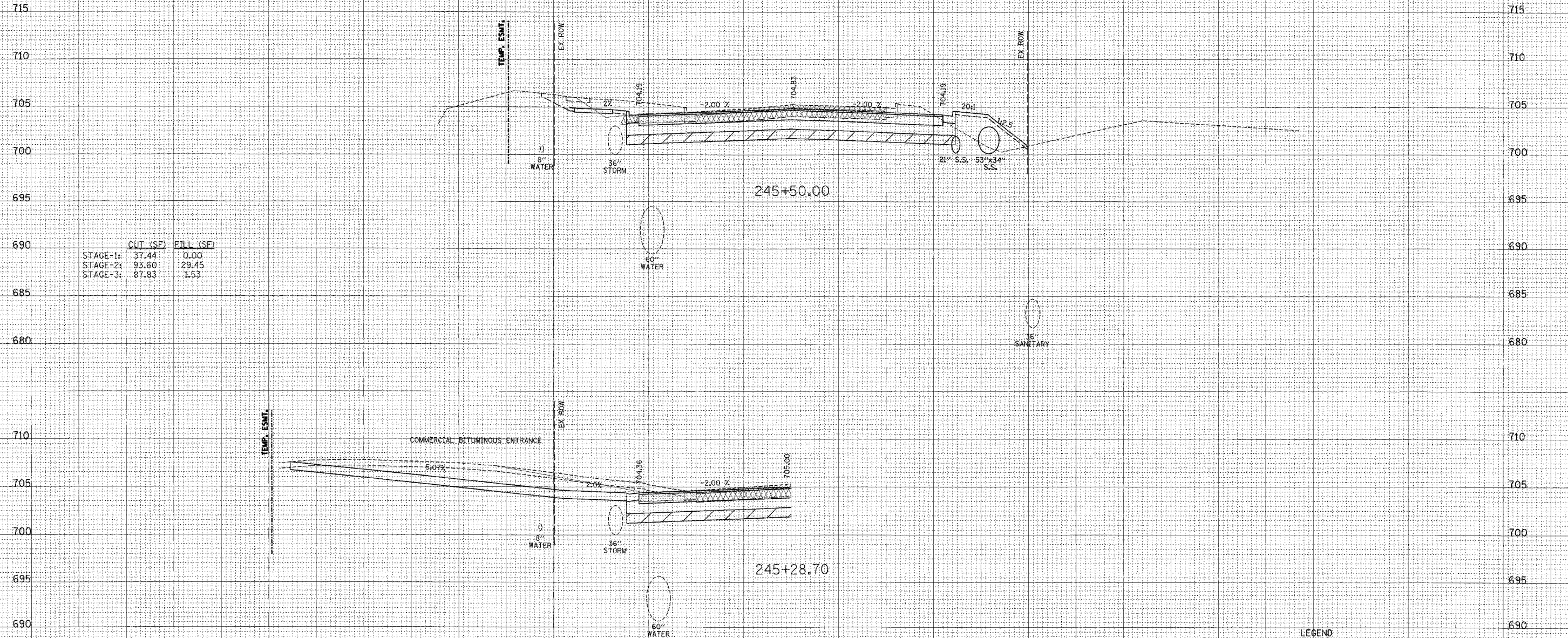
IL ROUTE 56

DATE
BY
SURVEYED
PLOTTED
NOTED
AREAS CHECKED

DATE
BY
SURVEYED
PLOTTED
NOTED
AREAS CHECKED

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



	CUT (SF)	FILL (SF)
STAGE-1:	37.44	0.00
STAGE-2:	93.60	29.45
STAGE-3:	87.83	1.53

LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

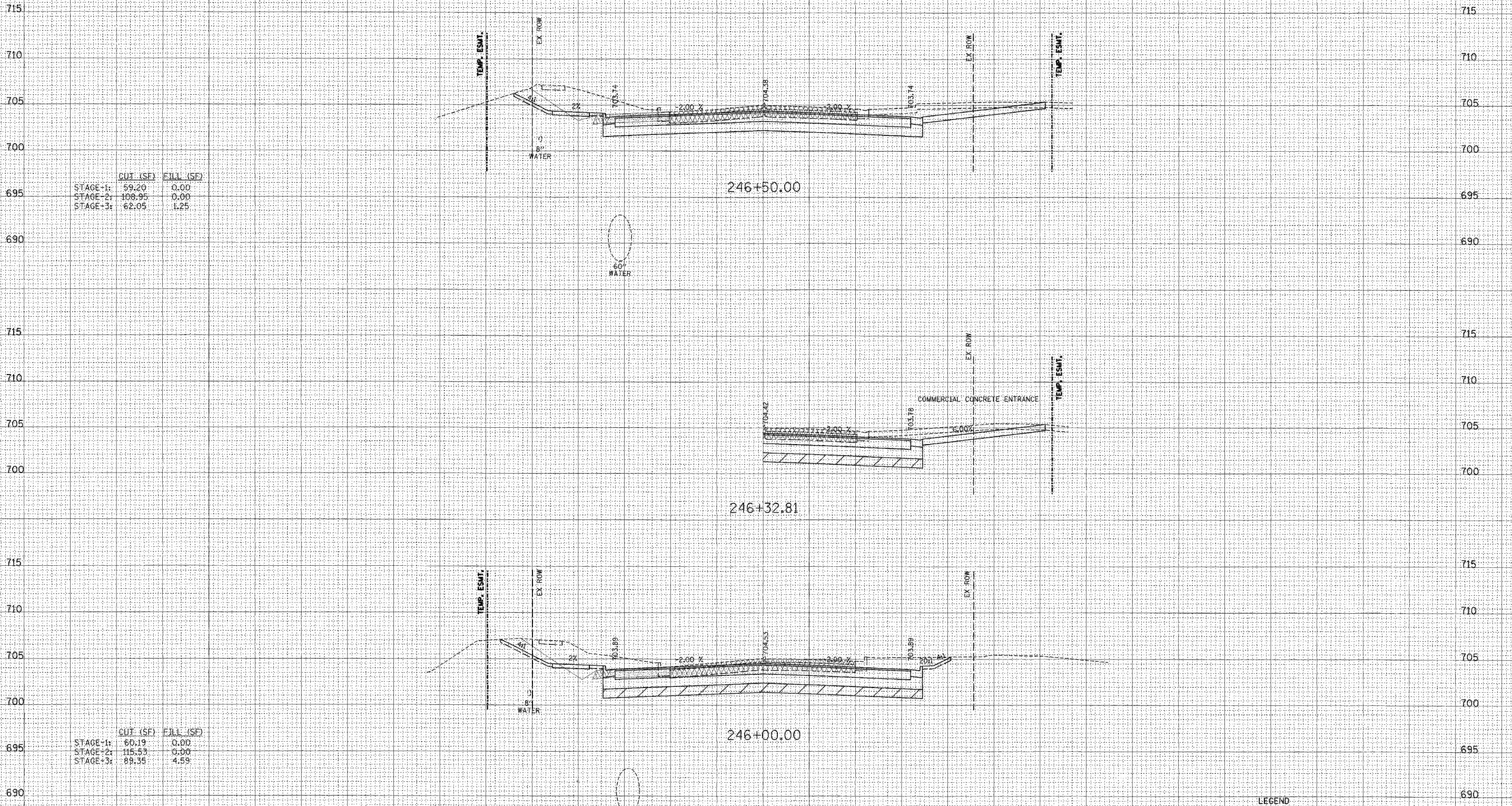
IL ROUTE 56

BY _____ DATE _____
 SURVEYED _____
 PLOTTED _____
 TEMPLATE _____
 NOTE BOOK _____
 AREAS CHECKED _____

BY _____ DATE _____
 SURVEYED _____
 PLOTTED _____
 TEMPLATE _____
 NOTE BOOK _____
 AREAS CHECKED _____

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



	CUT (SF)	FILL (SF)
STAGE-1:	59.20	0.00
STAGE-2:	108.95	0.00
STAGE-3:	62.05	1.25

	CUT (SF)	FILL (SF)
STAGE-1:	60.19	0.00
STAGE-2:	115.53	0.00
STAGE-3:	89.35	4.59

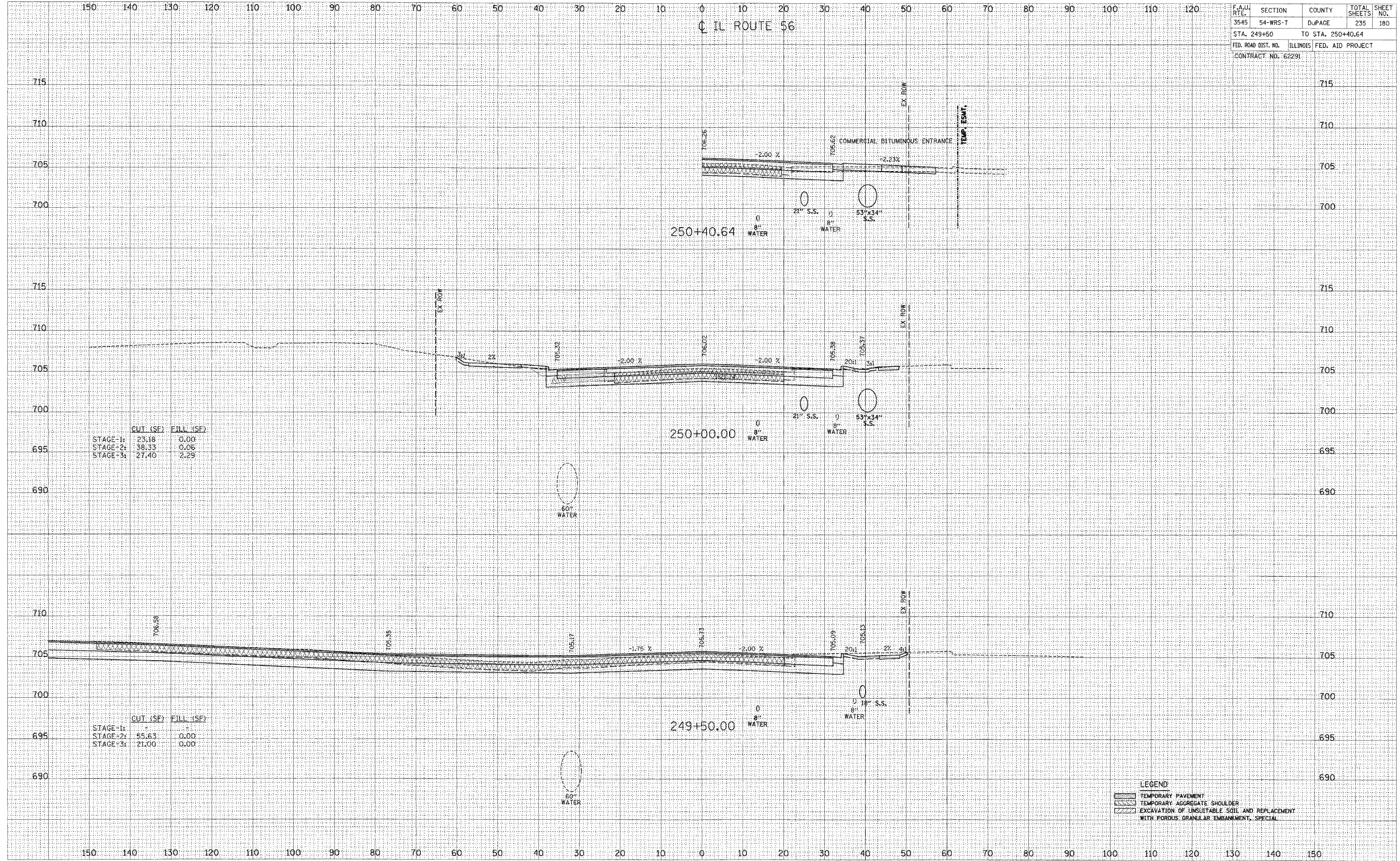
LEGEND
 [Symbol] TEMPORARY PAVEMENT
 [Symbol] TEMPORARY AGGREGATE SHOULDER
 [Symbol] EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	180
STA. 249+50		TO STA. 250+40.64		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

IL ROUTE 56

DATE
BY
SUPERVISOR
SURVEY
NOTE BOOK
NO.

DATE
BY
SUPERVISOR
SURVEY
NOTE BOOK
NO.



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

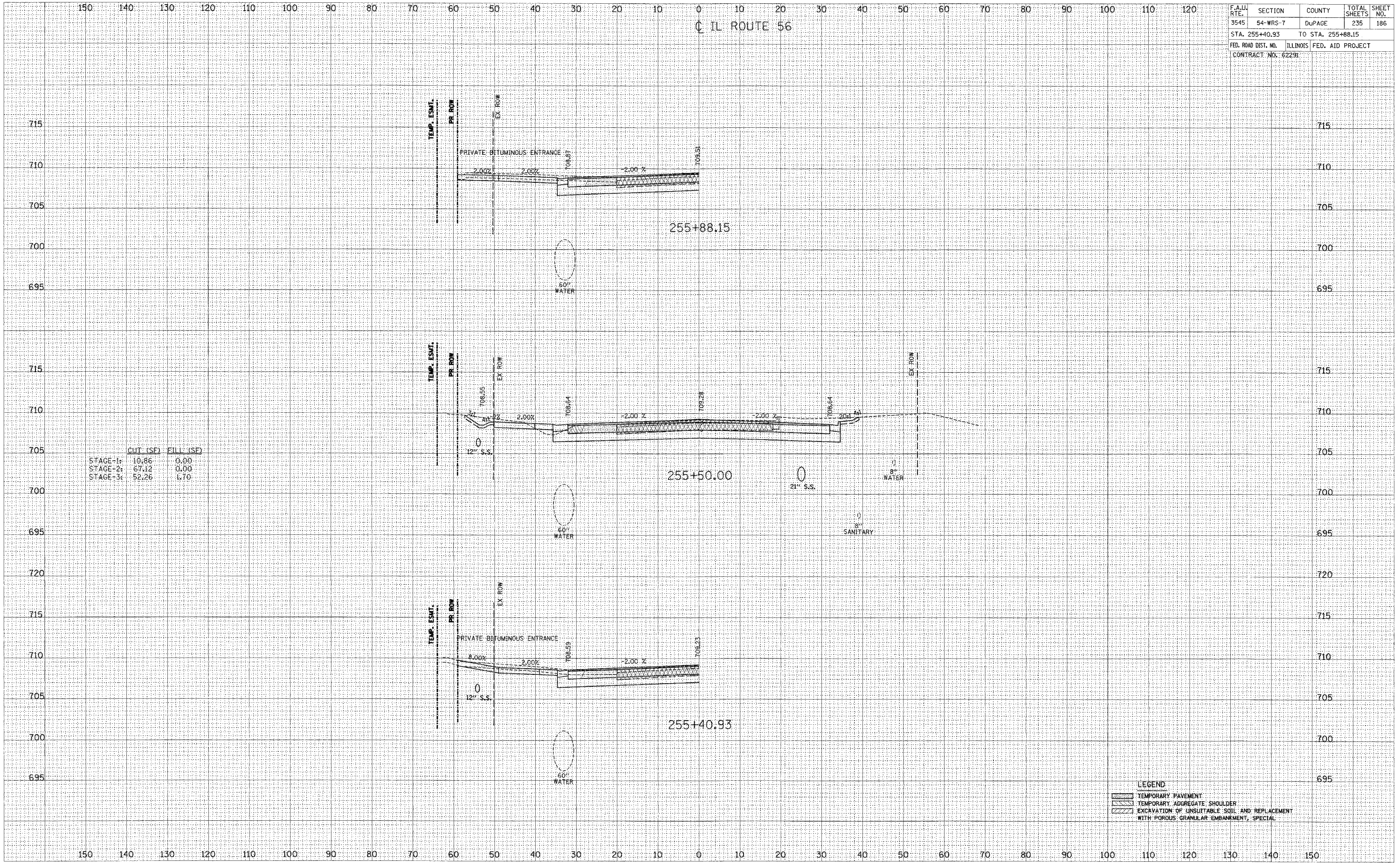
	CUT (SF)	FILL (SF)
STAGE-1:	23.18	0.00
STAGE-2:	38.33	0.06
STAGE-3:	27.40	2.29

	CUT (SF)	FILL (SF)
STAGE-1:	-	0.00
STAGE-2:	55.63	0.00
STAGE-3:	21.00	0.00

IL ROUTE 56

DATE _____ BY _____
 SURVEYED PLOTTED
 SURVEY TEMPLATE
 NOTE BOOK AREAS CHECKED
 NO. _____

DATE _____ BY _____
 SURVEYED PLOTTED
 SURVEY TEMPLATE
 NOTE BOOK AREAS CHECKED
 NO. _____



	CUT (SF)	FILL (SF)
STAGE-1*	10.86	0.00
STAGE-2*	67.12	0.00
STAGE-3*	52.26	1.70

LEGEND

- TEMPORARY PAVEMENT
- TEMPORARY AGGREGATE SHOULDER
- EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

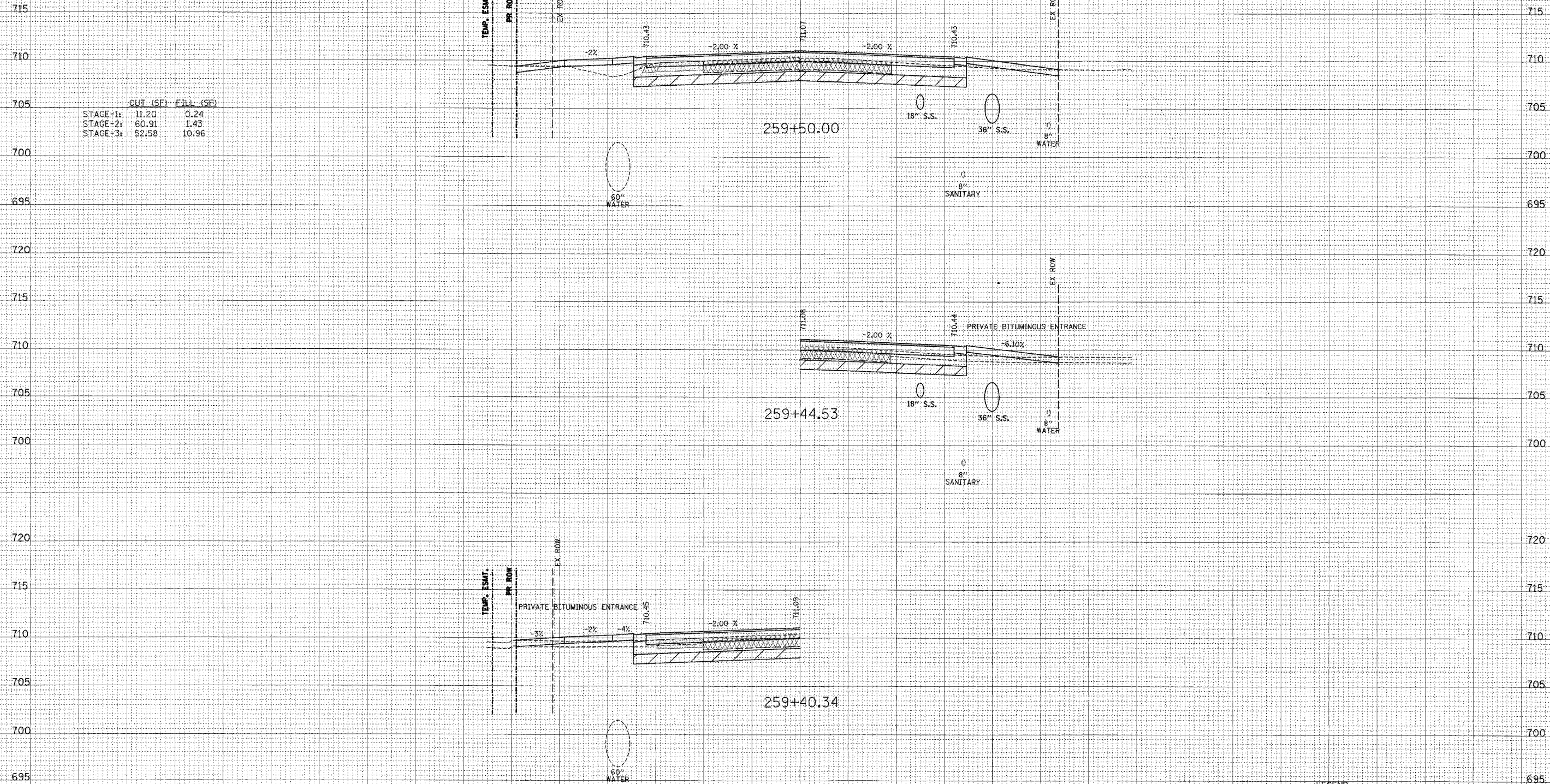
IL ROUTE 56

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	191
STA. 259+40.34		TO STA. 259+50		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 6229				

DATE _____
 BY _____
 SURVEYED _____
 CHECKED _____
 FINAL SURVEY _____
 NOTE BOOK _____
 NO. _____

DATE _____
 BY _____
 SURVEYED _____
 CHECKED _____
 ORIGINAL SURVEY _____
 NOTE BOOK _____
 NO. _____

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

	CUT (SF)	FILL (SF)
STAGE-1:	11.20	0.24
STAGE-2:	60.91	1.43
STAGE-3:	52.58	10.96

LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	192
STA. 259+85.78		TO STA. 260+38.91		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

IL ROUTE 56

FINAL SURVEY SHEET
 SURVEYED _____
 TEMPLATE _____
 NOTE BOOK _____
 AREAS CHECKED _____
 NO. _____

ORIGINAL SURVEY SHEET
 SURVEYED _____
 TEMPLATE _____
 NOTE BOOK _____
 AREAS CHECKED _____
 NO. _____

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

720 715 710 705 700 695

715 710 705 700 695

715 710 705 700 695

715 710 705 700 695

715 710 705 700 695

715 710 705 700 695

715 710 705 700 695

715 710 705 700 695

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715 710 705 700 695

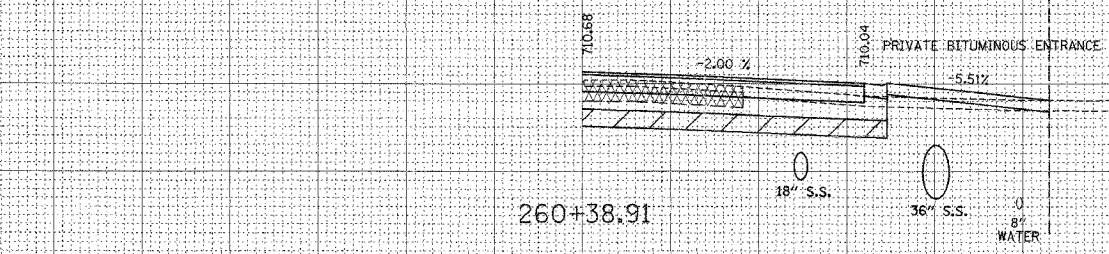
715 710 705 700 695

715 710 705 700 695

715 710 705 700 695

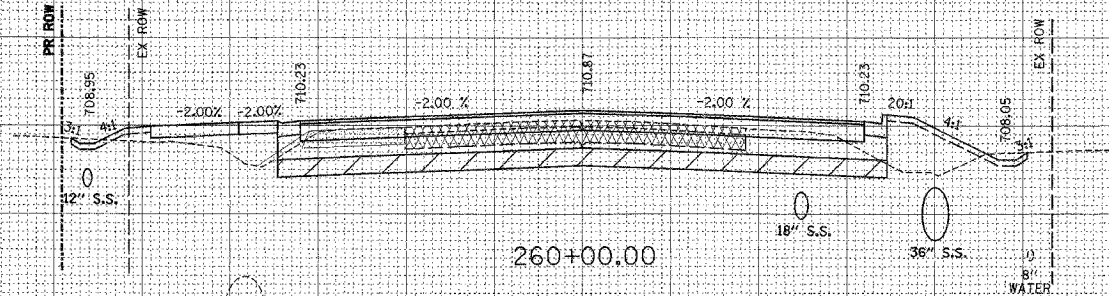
715 710 705 700 695

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

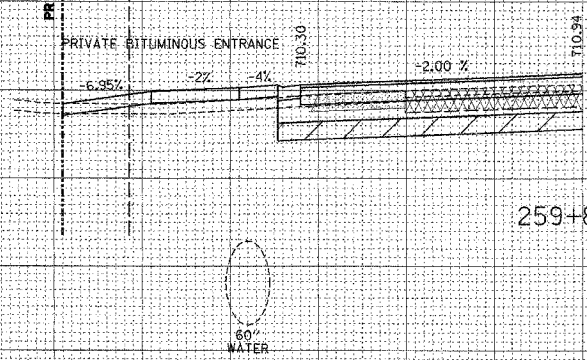


260+38.91

	CUT (SF)	FILL (SF)
STAGE-1:	10.42	1.73
STAGE-2:	60.76	20.32
STAGE-3:	53.32	13.77



260+00.00



259+85.78

LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.U. RTEL	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	193
STA. 260+50		TO STA. 261+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

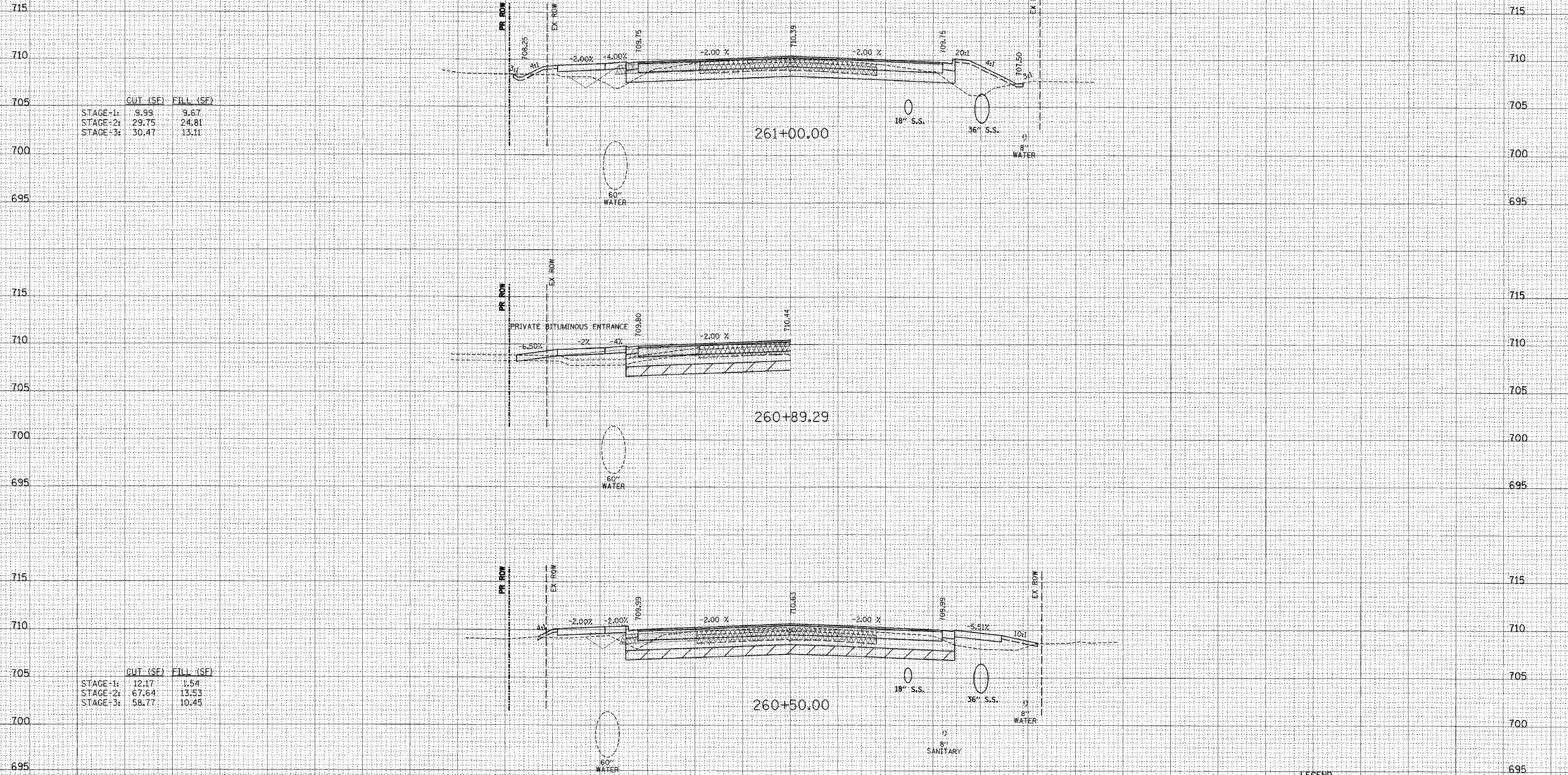
IL ROUTE 56

DATE: _____
 SHEET NO.: _____
 SURVEYED _____
 CHECKED _____
 ORIGINAL SURVEY _____
 NOTE BOOK _____
 NO. _____

DATE: _____
 SHEET NO.: _____
 SURVEYED _____
 CHECKED _____
 ORIGINAL SURVEY _____
 NOTE BOOK _____
 NO. _____

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150



	CUT (SF)	FILL (SF)
STAGE-1:	9.99	9.67
STAGE-2:	29.75	24.81
STAGE-3:	30.47	13.11

	CUT (SF)	FILL (SF)
STAGE-1:	12.17	1.54
STAGE-2:	67.64	13.53
STAGE-3:	58.77	10.45

LEGEND

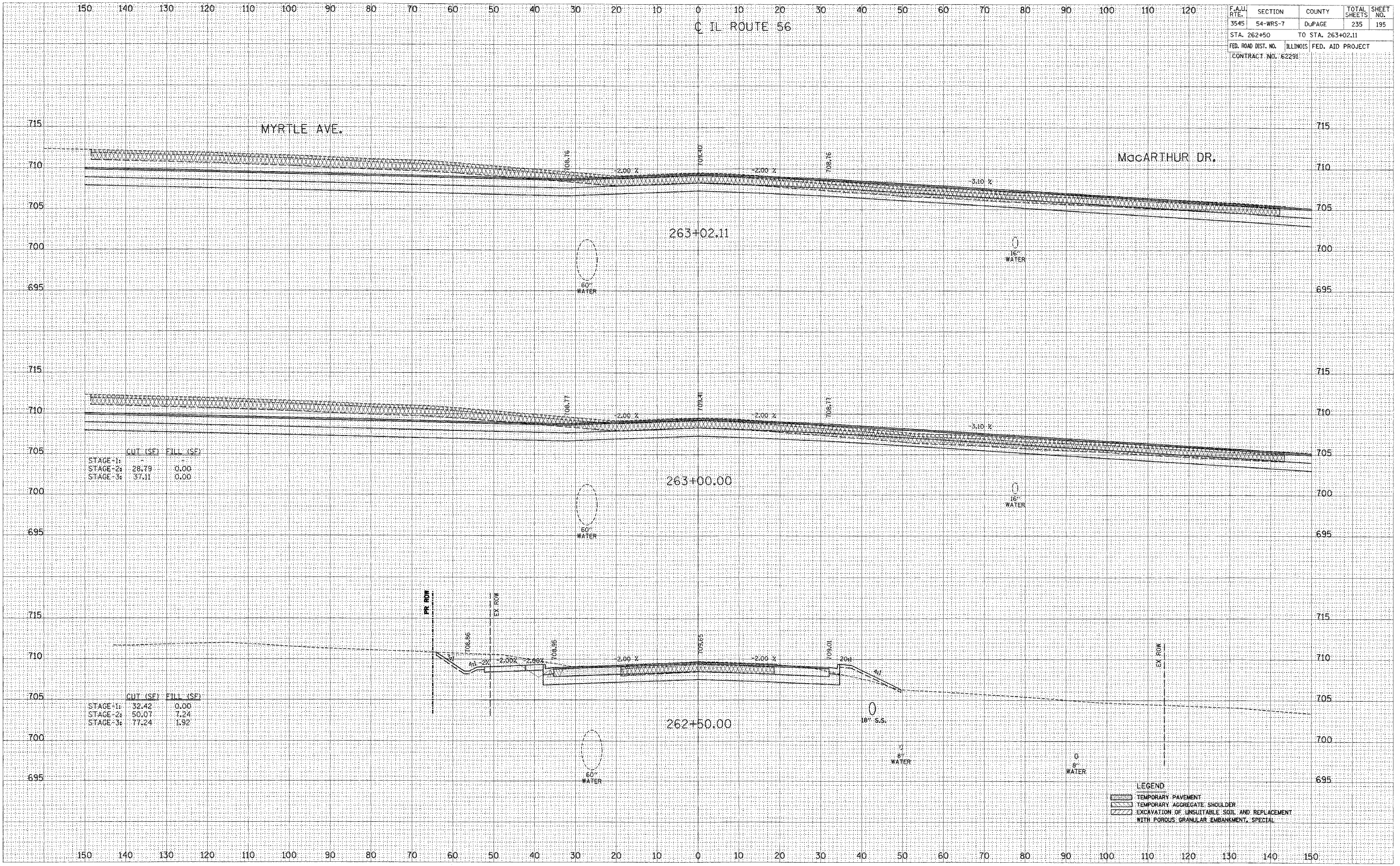
[Hatched Box]	TEMPORARY PAVEMENT
[Hatched Box]	TEMPORARY AGGREGATE SHOULDER
[Hatched Box]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	195
STA. 262+50		TO STA. 263+02.11		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62291				

C IL ROUTE 56

FINAL SURVEY PLOTTED FROM BOOK NO. AREA CHECKED

ORIGINAL SURVEY PLOTTED FROM BOOK NO. AREA CHECKED



	CUT (SF)	FILL (SF)
STAGE-1:	-	0.00
STAGE-2:	28.79	0.00
STAGE-3:	37.11	0.00

	CUT (SF)	FILL (SF)
STAGE-1:	32.42	0.00
STAGE-2:	50.07	7.24
STAGE-3:	77.24	1.92

LEGEND
 [Symbol] TEMPORARY PAVEMENT
 [Symbol] TEMPORARY AGGREGATE SHOULDER
 [Symbol] EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

IL ROUTE 56

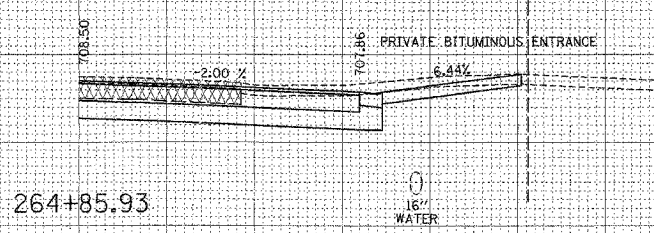
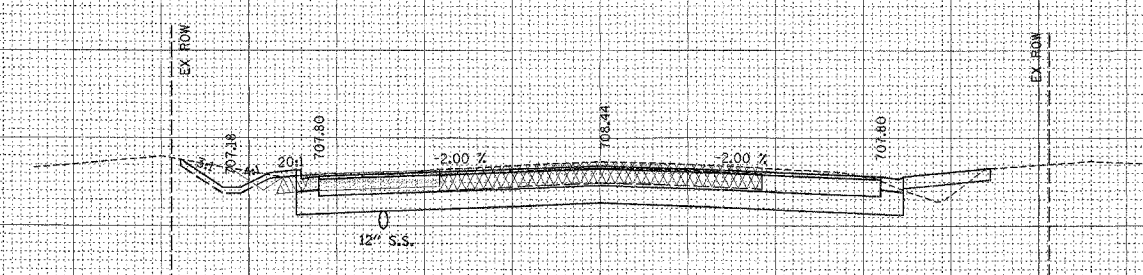
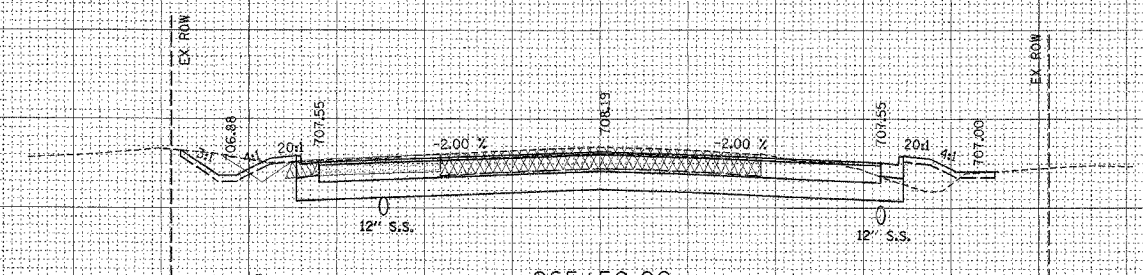
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	197
STA. 264+85.93		TO STA. 265+50		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

ORIGINAL SURVEY PLOTTED FROM ASKAS AREA CHECKED

ORIGINAL SURVEY PLOTTED FROM ASKAS AREA CHECKED

	CUT (SF)	FILL (SF)
STAGE-1:	20.23	0.00
STAGE-2:	51.95	7.06
STAGE-3:	52.58	2.05

	CUT (SF)	FILL (SF)
STAGE-1:	18.99	0.00
STAGE-2:	57.95	4.02
STAGE-3:	50.72	0.88



LEGEND

	TEMPORARY PAVEMENT
	TEMPORARY AGGREGATE SHOULDER
	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

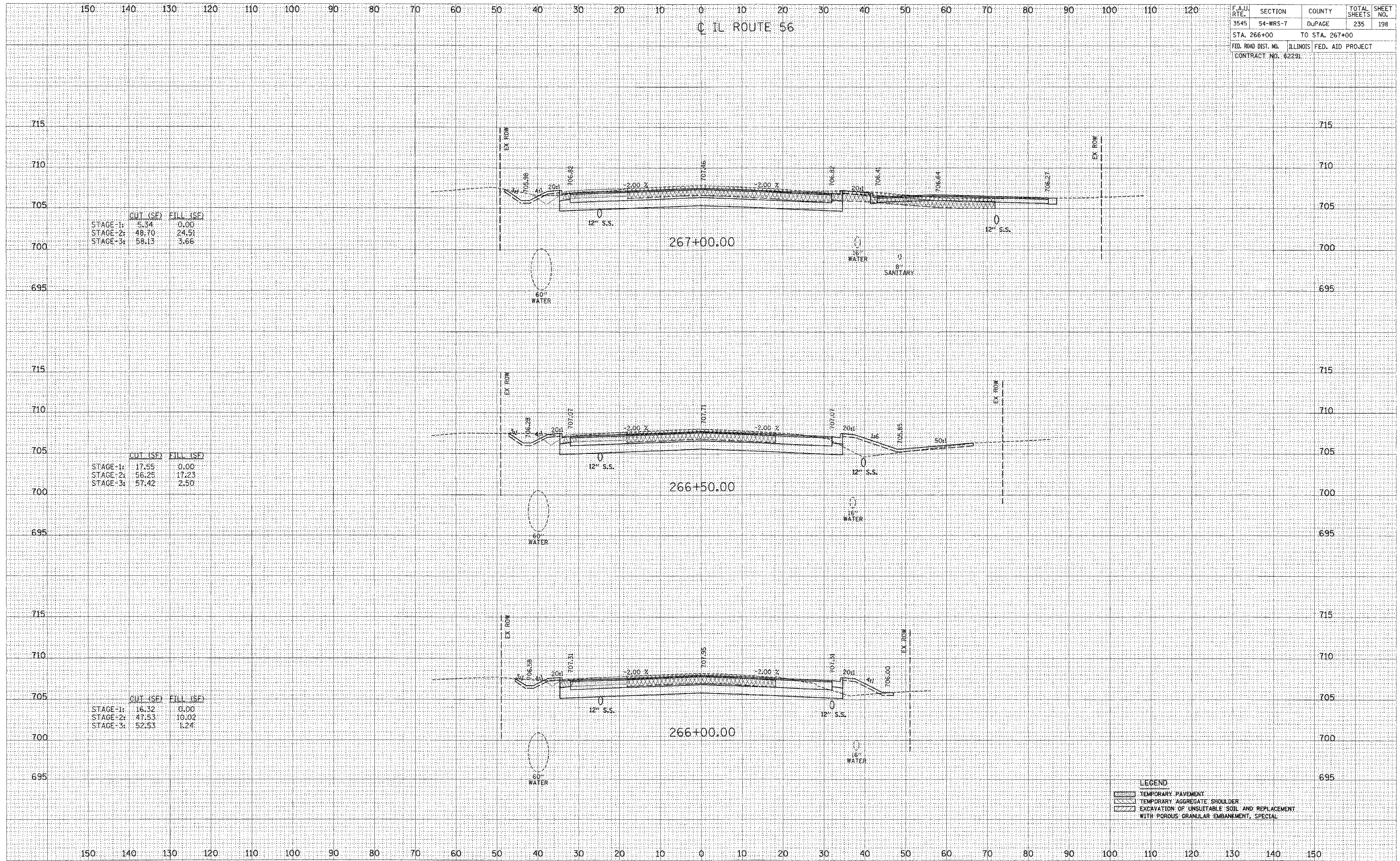
150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DUPAGE	235	198
STA. 266+00		TO STA. 267+00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

IL ROUTE 56

FINAL SURVEY
 SURVEYED
 TEMPLATE
 NOTE BOOK
 AREAS CHECKED

ORIGINAL SURVEY
 SURVEYED
 TEMPLATE
 NOTE BOOK
 AREAS CHECKED



LEGEND

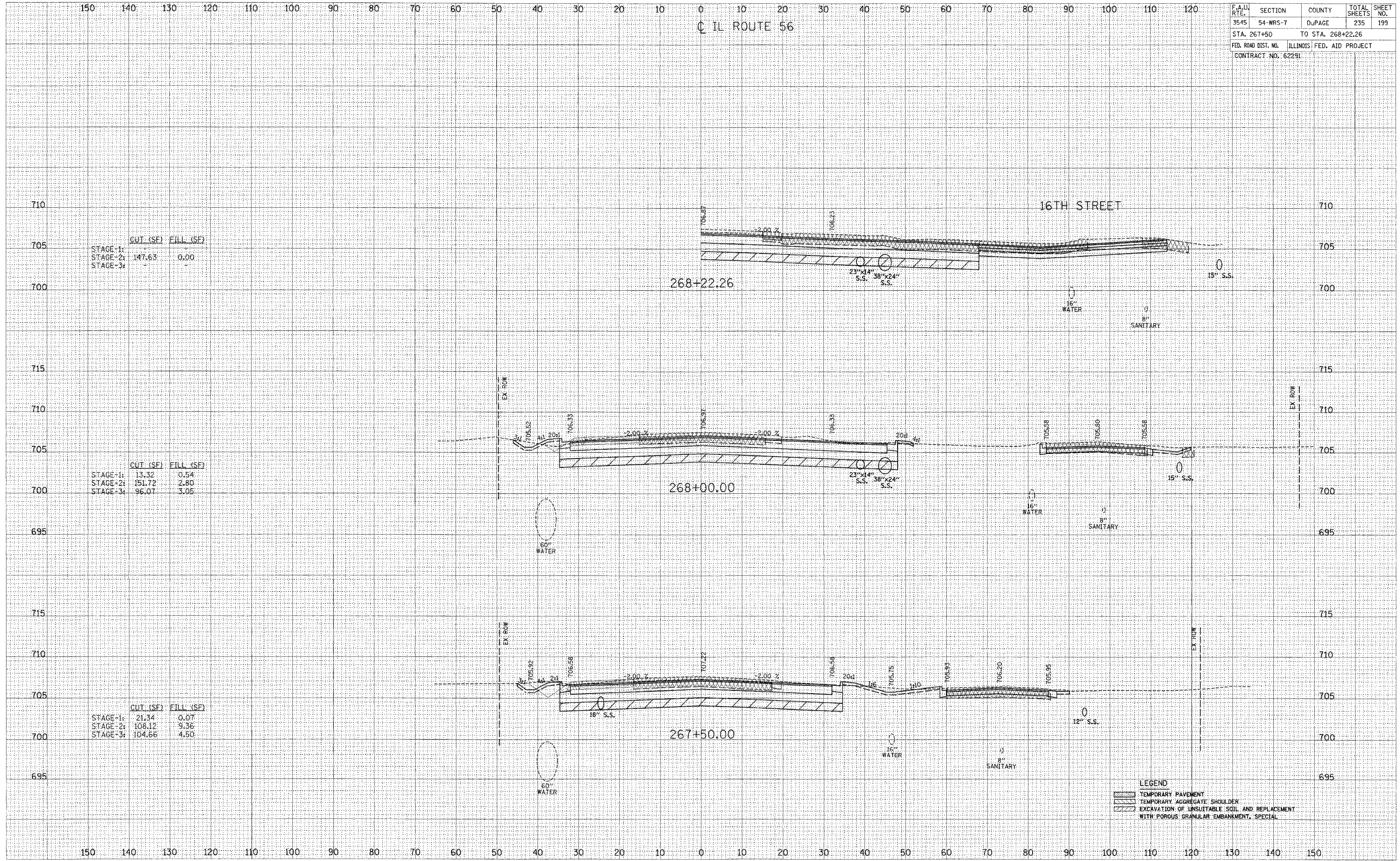
- TEMPORARY PAVEMENT
- TEMPORARY AGGREGATE SHOULDER
- EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	199
STA. 267+50		TO STA. 268+22.26		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62291				

IL ROUTE 56

FINAL SURVEY
 DRAWING
 NOTE BOOK
 NO.

ORIGINAL SURVEY
 DRAWING
 NOTE BOOK
 NO.

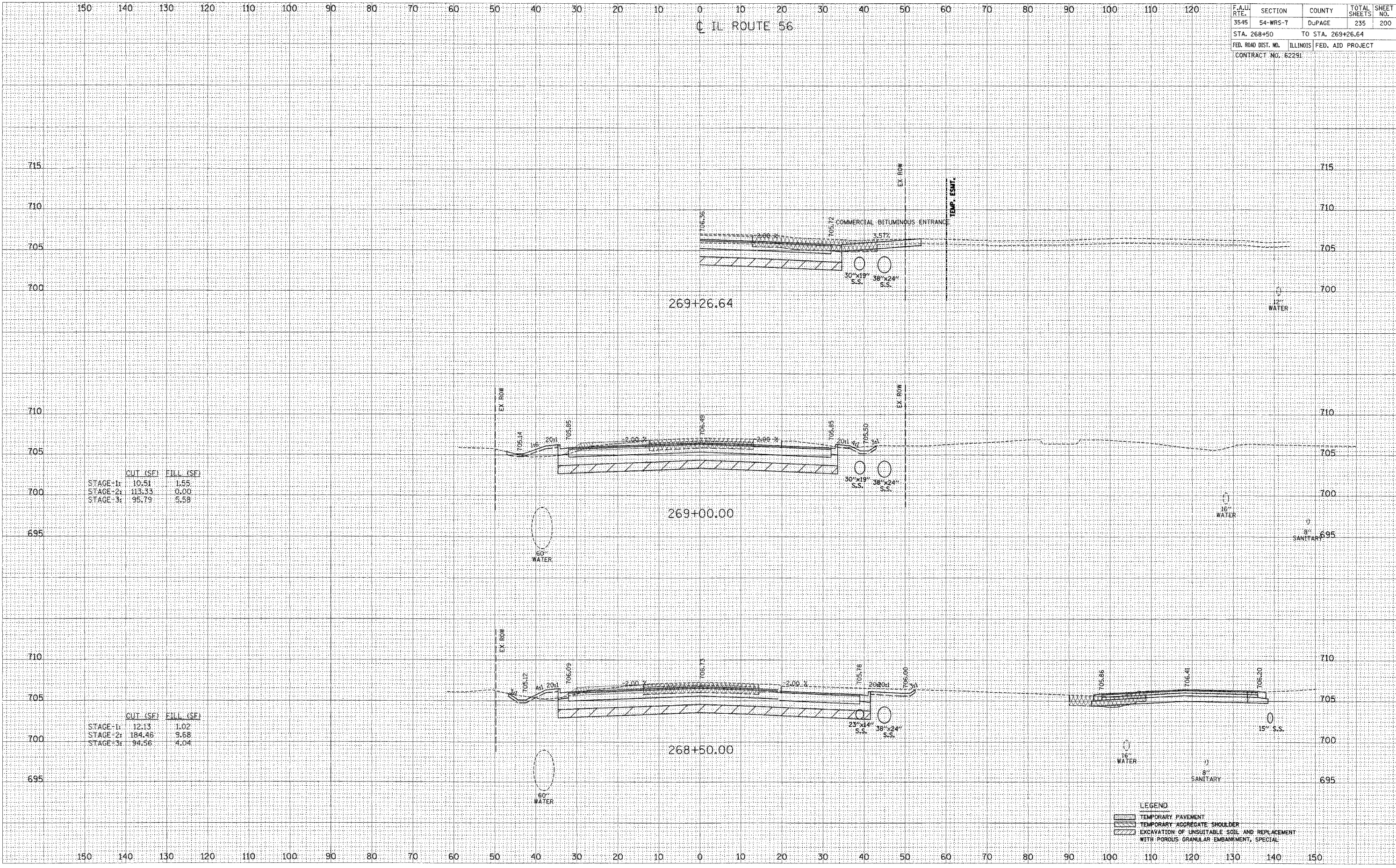


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3545	54-WRS-7	DuPAGE	235	200
STA. 268+50		TO STA. 269+26.64		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62291				

C. IL ROUTE 56

DATE: _____
 SURVEYED: _____
 PLOTTED: _____
 NOTE BOOK: _____
 AREAS CHECKED: _____
 NO. _____

DATE: _____
 SURVEYED: _____
 PLOTTED: _____
 NOTE BOOK: _____
 AREAS CHECKED: _____
 NO. _____



LEGEND

[Hatched Box]	TEMPORARY PAVEMENT
[Hatched Box]	TEMPORARY AGGREGATE SHOULDER
[Hatched Box]	EXCAVATION OF UNSUITABLE SOIL AND REPLACEMENT WITH POROUS GRANULAR EMBANKMENT, SPECIAL