

**CURVE C4701**  
P.I. STA= 8068+34.90  
N= 1,872,302.10  
E= 1,175,769.16  
Δ= 2° 04' 05"  
D= 1° 08' 45"  
R= 5000.00'  
T= 90.24'  
L= 180.47'  
E= 0.81'  
e= N/A  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 8067+44.66  
N= 1,872,211.86  
E= 1,175,768.33  
P.T. STA= 8069+25.12  
N= 1,872,392.31  
E= 1,175,766.72

**CURVE D4702**  
P.I. STA= 7071+15.85  
N= 1,872,344.72  
E= 1,175,380.81  
Δ= 3° 54' 33"  
D= 0° 57' 38"  
R= 5964.00'  
T= 203.53'  
L= 406.91'  
E= 3.47'  
e= N/A  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 7069+12.32  
N= 1,872,548.03  
E= 1,175,371.29  
P.T. STA= 7073+19.22  
N= 1,872,142.54  
E= 1,175,404.17

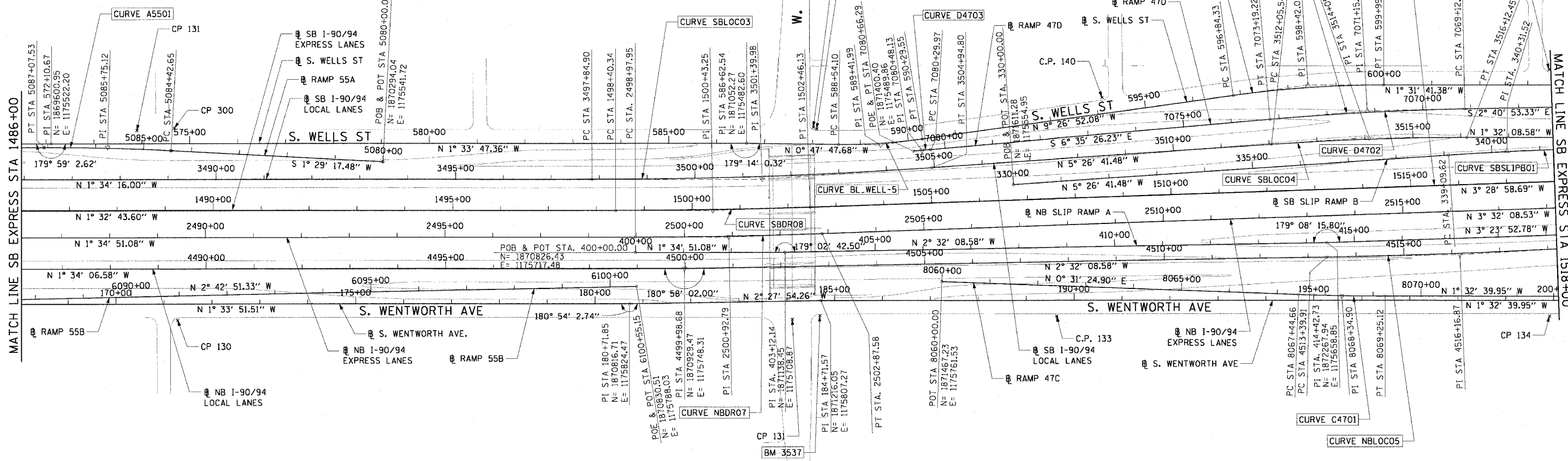
**CURVE D4703**  
P.I. STA= 7080+48.13  
N= 1,871,418.45  
E= 1,175,487.83  
Δ= 0° 11' 56"  
D= 0° 32' 51"  
R= 10464.00'  
T= 18.16'  
L= 36.32'  
E= 0.02'  
e= N/A  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 7080+29.97  
N= 1,871,436.49  
E= 1,175,485.75  
P.T. STA= 7080+66.29  
N= 1,871,400.40  
E= 1,175,489.86

**CURVE A5501**  
P.I. STA= 5085+75.12  
N= 1,869,719.11  
E= 1,175,526.79  
Δ= 3° 02' 07"  
D= 1° 08' 45"  
R= 5000.00'  
T= 132.48'  
L= 264.89'  
E= 1.75'  
e= N/A  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 5084+42.65  
N= 1,869,851.55  
E= 1,175,530.23  
P.T. STA= 5087+07.53  
N= 1,869,586.69  
E= 1,175,530.36

**CURVE BL\_WELL-5**  
P.I. STA= 589+41.99  
N= 1,871,331.70  
E= 1,175,478.71  
Δ= 8° 39' 04"  
D= 4° 55' 51"  
R= 1162.00'  
T= 87.89'  
L= 175.45'  
E= 3.32'  
e= N/A  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 588+54.10  
N= 1,871,243.82  
E= 1,175,479.94  
P.T. STA= 590+29.55  
N= 1,871,418.40  
E= 1,175,464.29

**CURVE BL\_WELL-6**  
P.I. STA= 598+42.02  
N= 1,872,219.86  
E= 1,175,330.92  
Δ= 7° 55' 11"  
D= 2° 30' 55"  
R= 2278.00'  
T= 157.69'  
L= 314.87'  
E= 5.45'  
e= N/A  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 596+84.33  
N= 1,872,064.31  
E= 1,175,356.80  
P.T. STA= 599+99.21  
N= 1,872,377.49  
E= 1,175,326.72

**CURVE SBSLIPB01**  
P.I. STA= 340+31.52  
N= 1,872,638.15  
E= 1,175,457.07  
Δ= 2° 00' 00"  
D= 0° 49' 13"  
R= 6984.00'  
T= 121.91'  
L= 243.79'  
E= 1.06'  
e= N/A  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 339+09.62  
N= 1,872,516.79  
E= 1,175,468.64  
P.T. STA= 341+53.40  
N= 1,872,759.83  
E= 1,175,449.75



**CURVE SBLOC03**  
P.I. STA= 3501+39.98  
N= 1,871,086.59  
E= 1,175,557.00  
Δ= 3° 52' 25"  
D= 0° 32' 44"  
R= 10500.00'  
T= 355.09'  
L= 709.90'  
E= 6.00'  
e= RC  
T.R.= 96' (96')  
S.E. RUN= 62' (62')  
P.C. STA= 3497+84.90  
N= 1,870,731.64  
E= 1,175,566.73  
P.T. STA= 3504+94.80  
N= 1,871,440.08  
E= 1,175,523.31

**CURVE SBLOC04**  
P.I. STA= 3514+09.07  
N= 1,872,350.23  
E= 1,175,436.55  
Δ= 3° 54' 33"  
D= 0° 57' 39"  
R= 5964.00'  
T= 203.53'  
L= 406.91'  
E= 3.47'  
e= 2.8%  
T.R.= 41' (41')  
S.E. RUN= 115' (115')  
P.C. STA= 3512+05.54  
N= 1,872,147.61  
E= 1,175,455.86  
P.T. STA= 3516+12.45  
N= 1,872,553.69  
E= 1,175,431.10

**CURVE SBDR08**  
P.I. STA= 1500+43.25  
N= 1,870,985.63  
E= 1,175,625.10  
Δ= 1° 56' 15"  
D= 0° 28' 39"  
R= 12000.00'  
T= 202.92'  
L= 405.79'  
E= 1.72'  
e= NC  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 1498+40.34  
N= 1,870,782.79  
E= 1,175,630.57  
P.T. STA= 1502+46.13  
N= 1,871,188.17  
E= 1,175,612.77

**CURVE NBDRO7**  
P.I. STA= 2500+92.79  
N= 1,871,020.31  
E= 1,175,680.12  
Δ= 1° 57' 17"  
D= 0° 30' 06"  
R= 11420.00'  
T= 194.84'  
L= 389.63'  
E= 1.66'  
e= NC  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 2498+97.95  
N= 1,870,825.55  
E= 1,175,685.50  
P.T. STA= 2502+87.58  
N= 1,871,214.78  
E= 1,175,668.10

**CURVE NBLOC05**  
P.I. STA= 4516+16.87  
N= 1,872,546.08  
E= 1,175,676.71  
Δ= 2° 26' 28"  
D= 0° 26' 27"  
R= 13000.00'  
T= 276.96'  
L= 553.84'  
E= 2.95'  
e= NC  
T.R.= N/A  
S.E. RUN= N/A  
P.C. STA= 4513+39.91  
N= 1,872,269.39  
E= 1,175,688.97  
P.T. STA= 4518+93.75  
N= 1,872,822.00  
E= 1,175,652.69

**NOTES:**

1. AVERAGE GRID TO GROUND CONVERSION FACTOR = 1.000010988654360
2. FOR BENCH MARK INFORMATION, SEE SHEET 1 OF ALIGNMENT PLAN SHEETS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
F.A.I. 90/94 (DAN RYAN EXPRESSWAY)  
63rd ST TO 59th ST (PAVEMENT MARKINGS)

**ALIGNMENT PLAN**

SCALE: 1"=100'  
DATE: March 4, 2005  
DRAWN BY: NJH/AMM  
CHECKED BY: JAL/MS



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