

OFFSETS TO FACE OF RAIL

POST	DISTANCE X (m) (ft.)	OFFSET Y (m) (ft.)
POINT 0	0 (0)	0 (0)
(4)	7.60 (24.93)	0.56 (1.83)
(8)	15.14 (49.68)	1.60 (5.28)
(12)	22.60 (74.16)	3.16 (10.37)
(16)	29.94 (98.23)	5.20 (17.05)
(20)	37.13 (121.83)	7.71 (25.3)
(24)	44.14 (144.83)	10.70 (35.09)

Offsets (Y) are measured between the face of rail and the offset baseline, which is parallel to the pavement edge and passes through point 0.

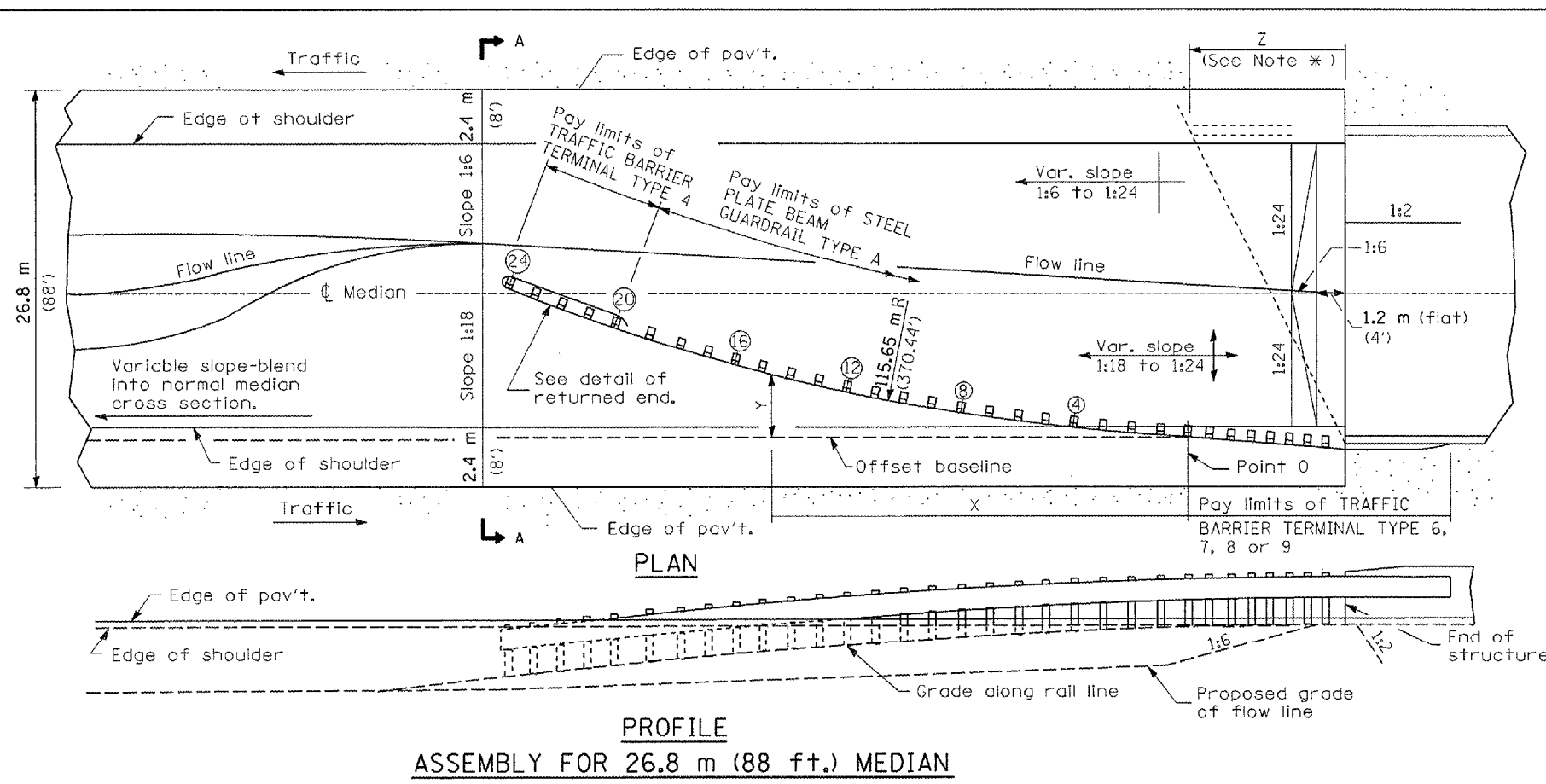
The location of point 0 will vary, being dependent on structure details and the type of traffic barrier terminal utilized.

GENERAL NOTES

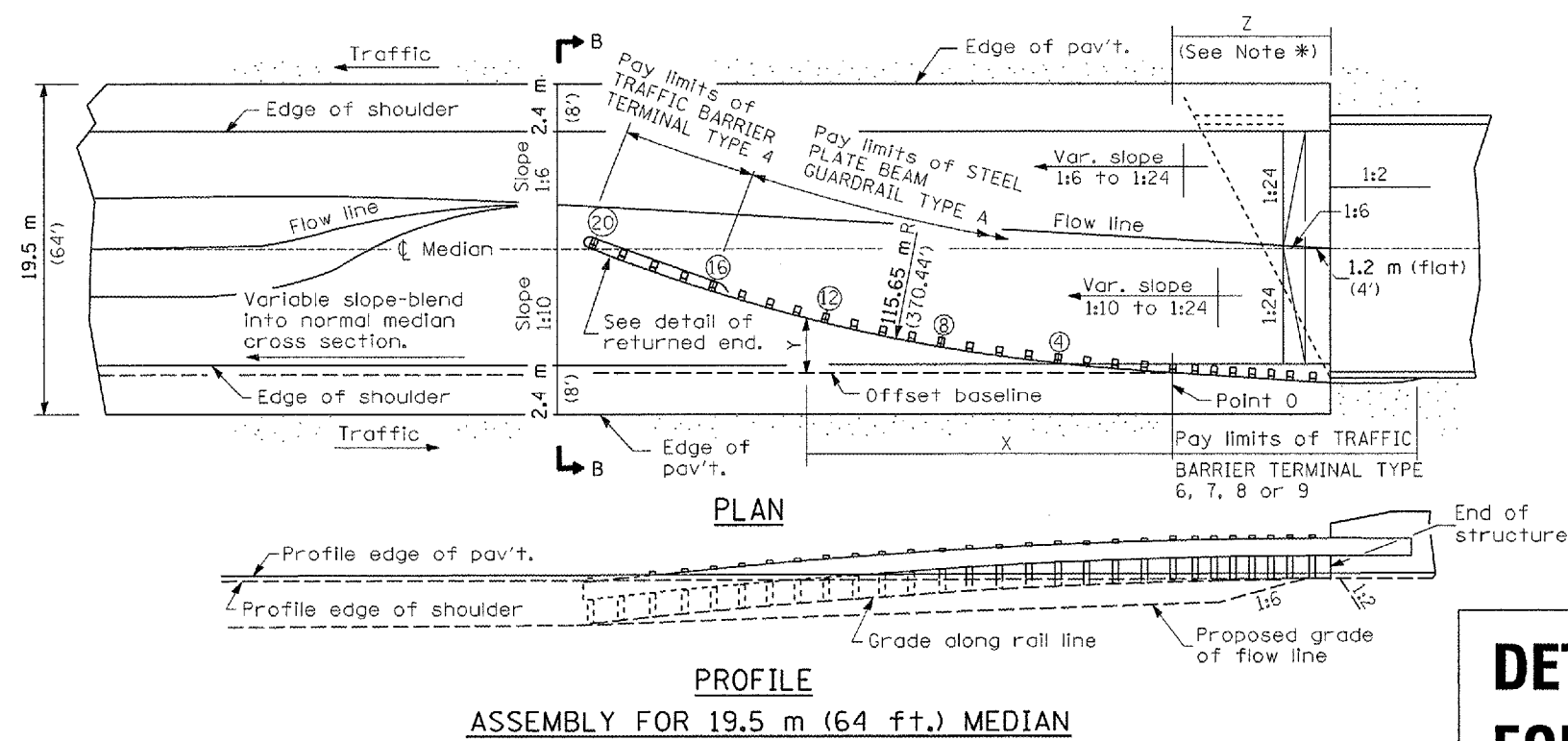
- See Standard 630001 for details of guard-rail not shown.
- * For dual structures skewed right forward, the nose of the Type 4 terminal shall be positioned longitudinally away from the structure for a distance equal to dimension Z. Appropriate adjustments to the length of the Type A guardrail and its offsets (Y) shall be calculated and used. All additional lengths of guardrail shall be in increments of 3.87 m (12'-6").
- For dual structures that are 90°, or skewed left forward, the length of guardrail is appropriate.
- The bearing plate K shall be held in position by (2) two eight penny nails driven into the post and bent over the top of the plate.
- All slopes are expressed as units of vertical displacement to units of horizontal displacement (V:H).

All dimensions are in millimeters (inches) unless otherwise shown.

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PROFILE
ASSEMBLY FOR 26.8 m (88 ft.) MEDIAN



PROFILE
ASSEMBLY FOR 19.5 m (64 ft.) MEDIAN

DETAIL INCLUDED FOR REPAIR ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC BARRIER TERMINAL TYPE 4