

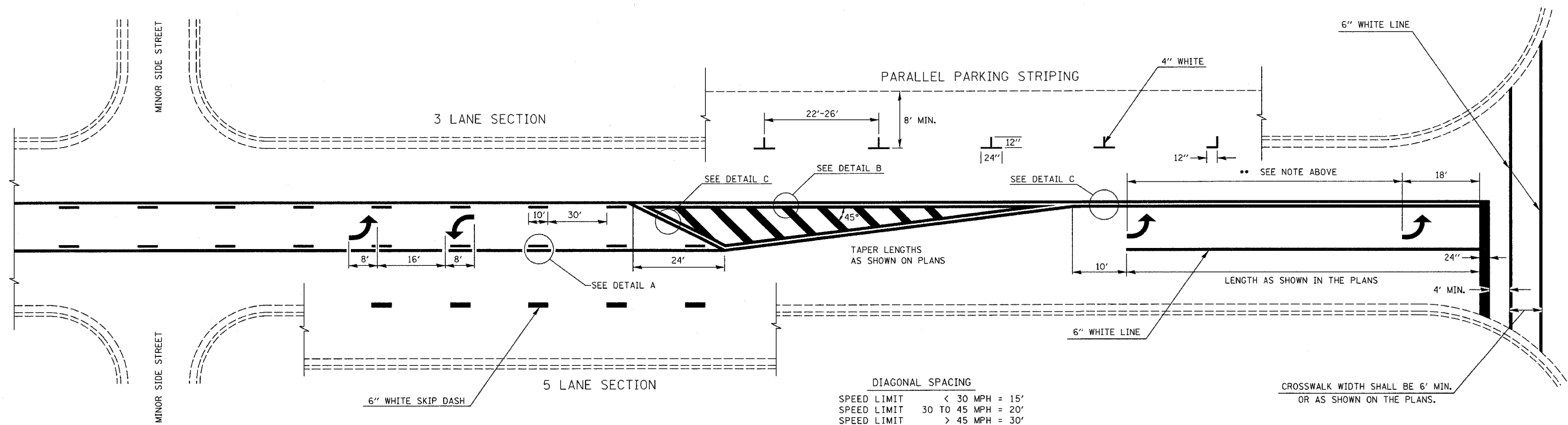
3/25/2005
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 MMD REV: 05-06-96
 EPMD1

c:\projects\94652\02101pa.dgn
 stefennk\ DATE

FAP #	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
332	110121W,RS	WHITE/EDWARDS	198	67
FED. ROAD DIST. NO. 1		BLINDS	FED. AID PROJECT NO.	

CONTRACT No. 94652

** PLACE THE FIRST ARROW EVEN WITH THE BEGINNING OF THE TURN BAY.
 PLACE ANOTHER ARROW 18' BACK FROM THE STOP BAR, IF REQUIRED.
 ADD ADDITIONAL ARROWS TO MAINTAIN A MAXIMUM OF 80' SPACING.



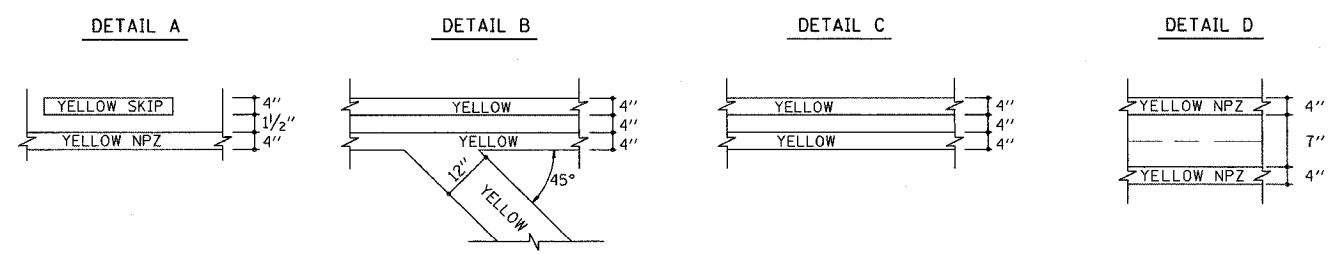
DIAGONAL SPACING

SPEED LIMIT	< 30 MPH	= 15'
SPEED LIMIT	30 TO 45 MPH	= 20'
SPEED LIMIT	> 45 MPH	= 30'

CROSSWALK WIDTH SHALL BE 6' MIN.
 OR AS SHOWN ON THE PLANS.

TYPICAL BI-DIRECTIONAL TURN LANE MARKINGS
 (SHOWING TRANSITION TO EXCLUSIVE LEFT TURN LANE)

- NOTES:
1. NO LESS THAN 2 ARROWS SHALL BE USED.
 2. TAPER RATES, LANE WIDTHS, AND STORAGE LENGTHS AS SHOWN ON THE PLANS.
 3. AVOID PLACING BI-DIRECTIONAL ARROWS IN INTERSECTIONS.
 4. BI-DIRECTIONAL ARROWS SHALL BE SPACED A MAXIMUM OF 250'.



NOT TO SCALE

REVISIONS	
NAME	DATE
MMD	12-99
DRM	08-04

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL APPLICATION OF PAVEMENT MARKINGS
SHEET 2 OF 2

SCALE: VERT. _____
 HORIZ. _____
 DATE _____
 DRAWN BY _____
 CHECKED BY _____

Revised 3/31/05