06-17-11 LETTING ITEM 289

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	*77	1
	ILLINOIS	CONTRACT NO. 60K65		

*77+1=78

FOR INDEX OF SHEETS, SEE SHEET NO. 2

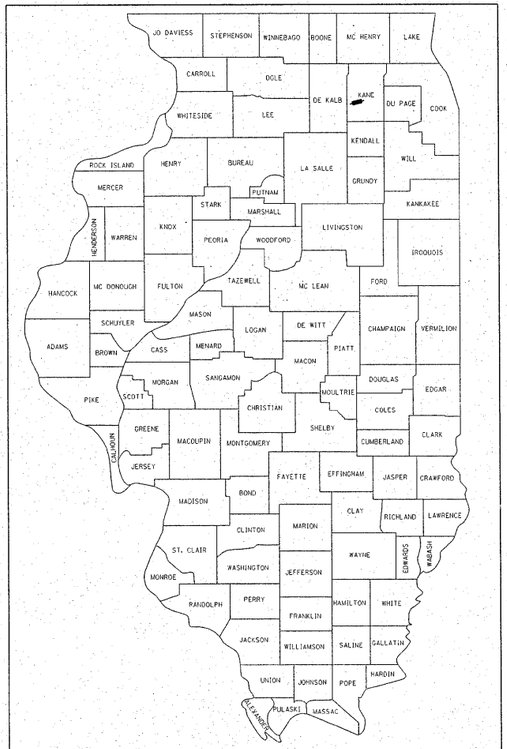
PROJECT IS LOCATED IN
UNINCORPORATED VIRGIL TOWNSHIP

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

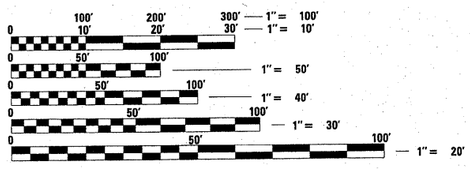
PROPOSED
HIGHWAY PLANS

FAP ROUTE 567 / ILL 38
AT MEREDITH ROAD
SECTION: 5 R–N–1
PROJECT: HSIP-0567(127)
INTERSECTION IMPROVEMENT &
TRAFFIC SIGNAL INSTALLATION
KANE COUNTY
C–91–542–10

D–91–542–10

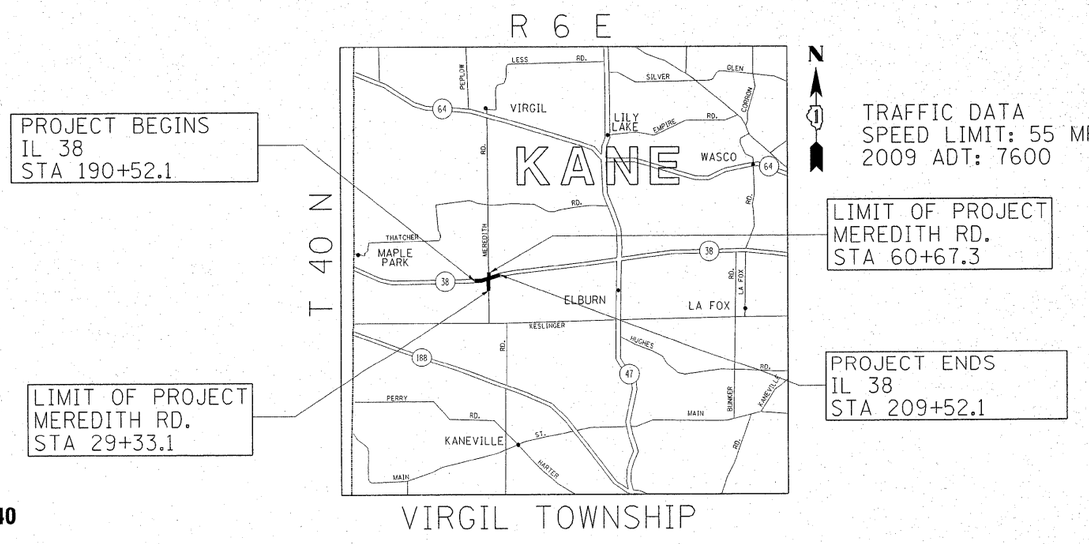


LOCATION OF SECTION INDICATED THUS: – ■ –



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811



GROSS AND NET LENGTH OF PROJECT = 5,034.2 FEET (0.95 MILES)

PROJECT ENGINEER: Dan Wilgreen (847) 705–4240
PROJECT MANAGER: Ken Eng

CONTRACT NO. 60K65

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 1, 20 11

Diane M. O'Keefe PE
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 25 20 11

Scott E. Stitt P.E. /bc
acting ENGINEER OF DESIGN AND ENVIRONMENT

March 25 20 11

Christine M. Reed /bc
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
542301-03	GRATING FOR CONCRETE FLARED END SECTION FOR 600MM (24") THRU 1300MM (54") PIPE
542311-02	PRECAST REINFORCED CONCRETE FLARED END SECTIONS
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS $\geq$ 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L,2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS $\geq$ 45 MPH
701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS $\geq$ 45 MPH
701901-01	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
814001-02	CONCRETE HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
877001-04	STEEL MAST ARM ASSEMBLY AND POLE
878001-08	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTOR LOOPS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND KANE COUNTY.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER AT (847) 741-9857 (OFFICE) OR (847) 715-8416 (CELL), A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE STOP SIGNS SHALL REMAIN THE PROPERTY OF THE STATE. THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI AT (847) 741-9857 (OFFICE) TO NOTIFY HIM THAT THE STOPS SIGNS HAVE BEEN TAKEN DOWN AND ARE READY TO BE PICKED UP.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED ALONG IL 38 ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

PRIOR TO EMBANKMENT PLACEMENT, ALL VEGETATION, LOOSE MATERIAL, AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL. ANY EMBANKMENT WIDENING ON EXISTING SLOPES SHOULD BE BENCHED IN ACCORDANCE WITH ARTICLE 205.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATING PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURE.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470, A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK.

KANE COUNTY NOTES

KANE COUNTY WILL REMOVE ALL EXISTING AND INSTALL ALL NEW SIGNING FOR MEREDITH ROAD. RAY JOHNSON (630) 406-7356 SHALL BE CONTACTED A MINIMUM OF 72 HOURS IN ADVANCE OF ANY NECESSARY REMOVAL AND A MINIMUM OF THREE WEEKS FOR THE INSTALLATION OF NEW SIGNING.

RECESSED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED ON MEREDITH ROAD ONLY.

URETHANE PAVEMENT MARKINGS SHALL BE PLACED ON MEREDITH ROAD ONLY.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILL 38 AT MEREDITH ROAD
INDEX OF SHEETS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	2
		CONTRACT NO. 60K65		
		ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN 90% FED. 10% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0004 80% FED 20% STATE ROADWAY	0021 80% FED 10% STATE 10% COUNTY T. SIGNAL	0021 100% FPD** EVP
20200100	EARTH EXCAVATION	CU YD	2602	2602		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	8578	8578		
20400800	FURNISHED EXCAVATION	CU YD	6628	6628		
20800150	TRENCH BACKFILL	CU YD	67.1	67.1		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	19525	19525		
25000210	SEEDING, CLASS 2A	ACRE	4	4		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	363	363		
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	363	363		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	363	363		
25100630	EROSION CONTROL BLANKET	SQ YD	19525	19525		
25200200	SUPPLEMENTAL WATERING	UNIT	195	195		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	403	403		
28000305	TEMPORARY DITCH CHECKS	FOOT	606	606		
28000315	AGGREGATE DITCH CHECKS	TON	128	128		
28000400	PERIMETER EROSION BARRIER	FOOT	7870	7870		
28100105	STONE RIPRAP, CLASS A3	SQ YD	167	167		
28200200	FILTER FABRIC	SQ YD	167	167		
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	85	85		
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	183	183		
35501312	HOT-MIX ASPHALT BASE COURSE, 7"	SQ YD	1586	1586		
35501326	HOT-MIX ASPHALT BASE COURSE, 10 1/2"	SQ YD	2907	2907		
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	15	15		
40600300	AGGREGATE (PRIME COAT)	TON	76	76		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	17	17		
40600895	CONSTRUCTING TEST STRIP	EACH	1	1		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	62	62		
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	30	30		
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	1768	1768		
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	248	248		
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SQ YD	192	192		
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	1866	1866		
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	3079	3079		
48203037	HOT-MIX ASPHALT SHOULDERS, 10"	SQ YD	94	94		
50104400	CONCRETE HEADWALL REMOVAL	EACH	4	4		
50105220	PIPE CULVERT REMOVAL	FOOT	112	112		

SUMMARY OF QUANTITIES

	CODE NO	ITEM	UNIT	URBAN 90% FED. 10% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0004 80% FED 20% STATE ROADWAY	0021 80% FED 10% STATE 10% COUNTY T. SIGNAL	0021 100% FPD** EVP
	54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	4	4		
	54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	6	6		
	54247100	GRATING FOR CONCRETE FLARED END SECTION 15"	EACH	4	4		
	54247130	GRATING FOR CONCRETE FLARED END SECTION 24"	EACH	10	10		
	542A0215	PIPE CULVERTS, CLASS A, TYPE 1 10"	FOOT	80	80		
	542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	75	75		
	550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	287	287		
	55101200	STORM SEWER REMOVAL 24"	FOOT	155	155		
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6		
	67100100	MOBILIZATION	L SUM	1	1		
	70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	0.5	0.5	
	70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1		
	70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1		
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2		2	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	3512	3512		
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	291.2	291.2		
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	19650	19650		
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1061	1061		
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	144	144		
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	151	151		
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	390	390		
*	72000100	SIGN PANEL - TYPE 1	SQ FT	31.5		31.5	
*	72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	40.5	40.5		
*	73700100	REMOVE GROUND MOUNTED SIGN SUPPORT	EACH	6	6		
•	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	145.6	145.6		
•	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9667	9667		
•	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	526	526		
•	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	72	72		
•	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	90	90		

• SPECIALTY ITEMS •• ELBURN & COUNTRYSIDE FIRE PROTECTION DISTRICT

Rev.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 38 AT MEREDITH ROAD
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	3
		CONTRACT NO. 60K65		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT

SUMMARY OF QUANTITIES

	CODE NO	ITEM	UNIT	URBAN 90% FED. 10% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0004 80% FED 20% STATE ROADWAY	0021 80% FED 10% STATE 10% COUNTY	0021 100% FPD**
•	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	110	110		
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	184	184		
•	81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	485		485	
•	81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	40		40	
•	81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	158		158	
•	81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	83		83	
•	81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	253		253	
•	81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	318		318	
•	81400100	HANDHOLE	EACH	4		4	
•	81400200	HEAVY-DUTY HANDHOLE	EACH	6		6	
•	81400300	DOUBLE HANDHOLE	EACH	1		1	
•	81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	766		766	
•	85000500	MAINTENANCE OF EXISTING FLASHING BEACON INSTALLATION	EACH	1		1	
•	85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1		1	
•	87200400	SPAN WIRE	FOOT	147		147	
•	87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	432			432
•	87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1082		1082	
•	87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1962		1962	
•	87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1977		1977	
•	87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	138		138	
•	87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4		4	
•	87700240	STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	2		2	
•	87700220	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	1		1	
•	87700260	STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.	EACH	1		1	
•	87800100	CONCRETE FOUNDATION, TYPE A	FOOT	16		16	
•	87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4		4	
•	87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	46		46	
•	88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4		4	

SUMMARY OF QUANTITIES

	CODE NO	ITEM	UNIT	URBAN 90% FED. 10% STATE TOTAL QUANTITIES	CONSTRUCTION TYPE CODE 0004 80% FED 20% STATE ROADWAY	0021 80% FED 10% STATE 10% COUNTY T. SIGNAL	0021 100% FPD** EVP
•	88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4		4	
•	88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4		4	
•	88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	8		8	
•	88500100	INDUCTIVE LOOP DETECTOR	EACH	8		8	
•	88600100	DETECTOR LOOP, TYPE I	FOOT	772		772	
•	88700200	LIGHT DETECTOR	EACH	2			2
•	88700300	LIGHT DETECTOR AMPLIFIER	EACH	1			1
•	89501510	RELOCATE EXISTING FLASHING BEACON	EACH	1		1	
•	89502400	REMOVE EXISTING FLASHING BEACON INSTALLATION COMPLETE	EACH	1		1	
•	X0322118	REMOVE CONCRETE FLARED END SECTIONS	EACH	2	2		
	X0326133	TEMPORARY WOOD POLE, 45 FEET, CLASS 5	EACH	1		1	
	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	3	3		
	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	1	1		
	X4060826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	632	632		
	X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	11290	11290		
•	X7800605	URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	145.6	145.6		
•	X7800610	URETHANE PAVEMENT MARKING - LINE 4"	FOOT	9984	9984		
•	X7800630	URETHANE PAVEMENT MARKING - LINE 6"	FOOT	535	535		
•	X7800650	URETHANE PAVEMENT MARKING - LINE 12"	FOOT	72	72		
•	X7800680	URETHANE PAVEMENT MARKING - LINE 24"	FOOT	62	62		
•	X7810300	RECESSED REFLECTIVE PAVMENT MARKER	EACH	120	120		
•	80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1		1	
•	86200120	UNINTERRUPTIBLE POWER SUPPLY	EACH	1		1	
•	87301900	ELECTRIC CABLE IN CONDUIT, (EQUIPMENT GROUNDING CONDUCTOR) NO. 6 1C	FOOT	658		658	
•	X8730250	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	432			432
	Z0001050	AGGREGATE SUBGRADE 12"	SQ YD	7667	7667		
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
△	Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	2	2		
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	102.8	102.8		
	31101250	SUBBASE GRANULAR MATERIAL, TYPE B 4 1/2"	SQ YD	3173	3173		
	Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CUYD	117	117		
	60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	400	400		
	60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	11	11		

•• ELBURN & COUNTRYSIDE FIRE PROTECTION DISTRICT • SPECIALTY ITEMS △ = Non-participating

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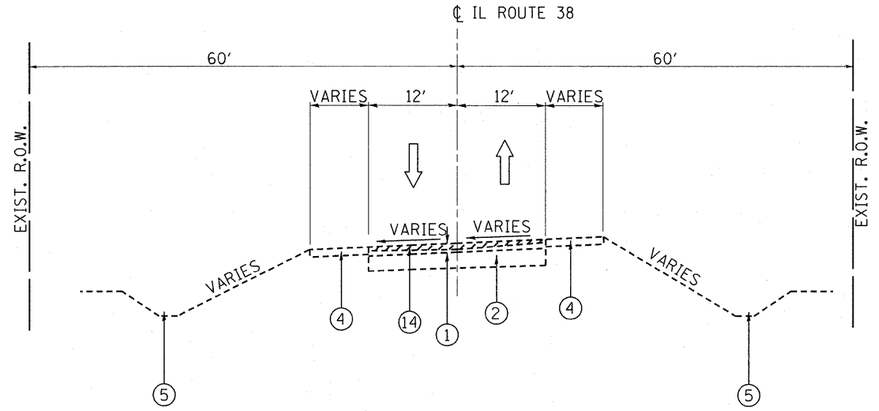
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 38 AT MEREDITH ROAD
SUMMARY OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

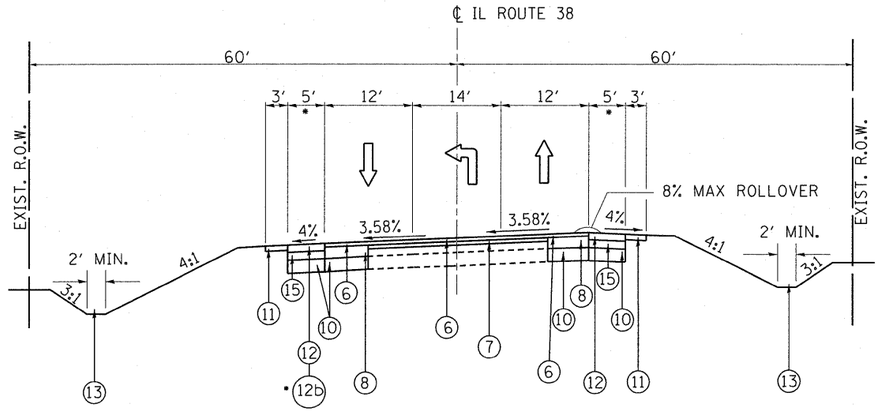
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	4
		CONTRACT NO. 60K65		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



EXISTING TYPICAL SECTION
IL ROUTE 38

SECTION SUPERELEVATED FROM
STA. 190+52 TO STA. 200+00



PROPOSED TYPICAL SECTION
IL ROUTE 38

SECTION SUPERELEVATED FROM
STA. 190+52 TO STA. 200+00

• NOTE THE HMA SHOULDER BETWEEN STA. 193+10 TO STA. 194+96 WILL RANGE FROM 3.98' TO 4.7' WIDE, AND WILL BE 10" THICK, ADJACENT TO WINERY DRIVEWAY.

LEGEND

(1) EXISTING HOT-MIX ASPHALT OVERLAY VARIES ±5 1/2" TO ±8 1/4" (IL 38)
(2) EXISTING PCC BASE COURSE ±6" (IL 38)
(3) EXISTING HMA BASE ±11 3/4" (MEREDITH RD.)
(4) EXISTING AGGREGATE SHOULDER
(5) EXISTING SWALE/DITCH
(6) PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90; 2"
(7) PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 1"
(8) PROPOSED HOT-MIX ASPALT BASE COURSE (HMA BINDER IL-19mm, N70); 10 1/2" (IN 3 LIFTS)
(9) PROPOSED HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm, N70); 7" (IN 2 LIFTS)
(10) PROPOSED AGGEGATE SUBGRADE, 12"
(11) PROPOSED AGGREGATE SHOULDER TYPE B, 6"
(12) PROPOSED HOT-MIX ASPHALT SHOULDER, 8" (IN 2 LIFTS)
• (12b) PROPOSED HOT-MIX ASPHALT SHOULDER, 10" (IN 3 LIFTS)
(13) PROPOSED SWALE/DITCH
•• (14) PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
(15) PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B, 4 1/2"

•• REFER TO SHEET 7A FOR THE EDGE OF PAVEMENT ELEVATION AFTER MILLING.

CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS
SHOULDERS	
HOT-MIX ASPHALT SHOULDERS, (HMA BINDER IL-19 mm); 8" & 10"	2% @ 30 GYR
RESURFACING	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm); 2"	4% @ 90 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 1"	4% @ 50 GYR
PAVEMENT WIDENING	
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL 9.5 mm); 2"	4% @ 90 GYR
HMA BASE COURSE (HMA BINDER IL-19 mm), N70; 7" & 10.5"	4% @ 70 GYR
DRIVEWAYS	
HMA SURFACE COURSE, MIX C, N50 (IL 9.5 mm); 2", FE, PE	4% @ 50 GYR
HMA BASE COURSE (HMA BINDER IL-19 mm); PE - 6"	4% @ 50 GYR
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LB/ SQ YD/IN"

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

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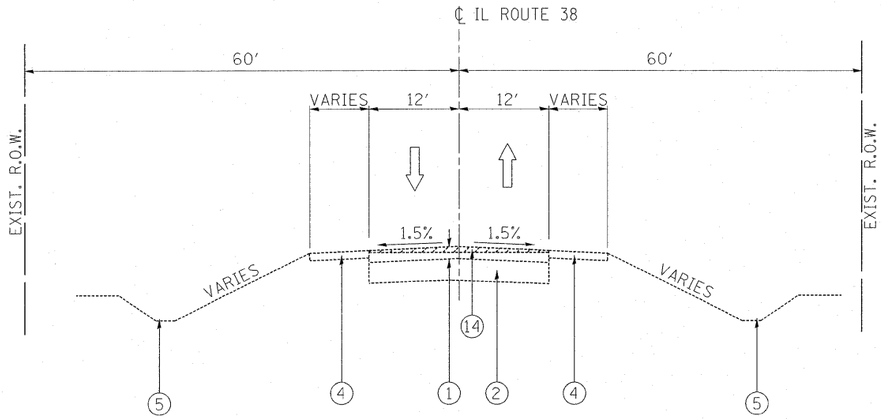
	PLOT DATE = 3/1/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILL 38 AT MEREDITH ROAD
TYPICAL SECTIONS

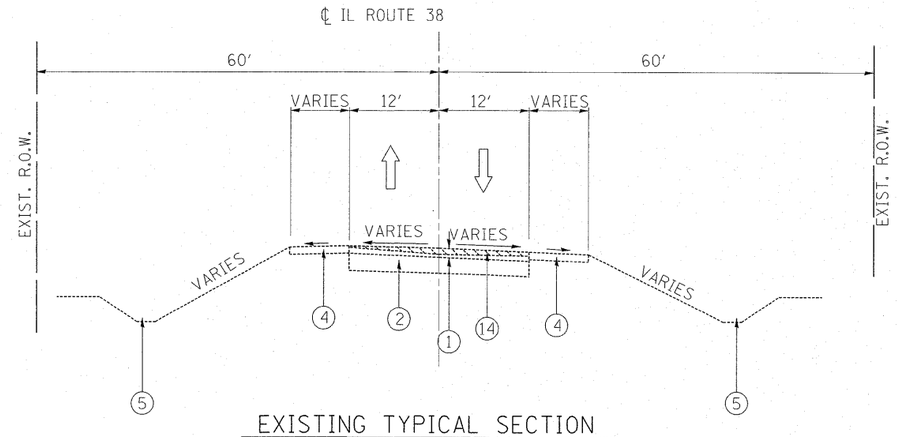
SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	5
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			



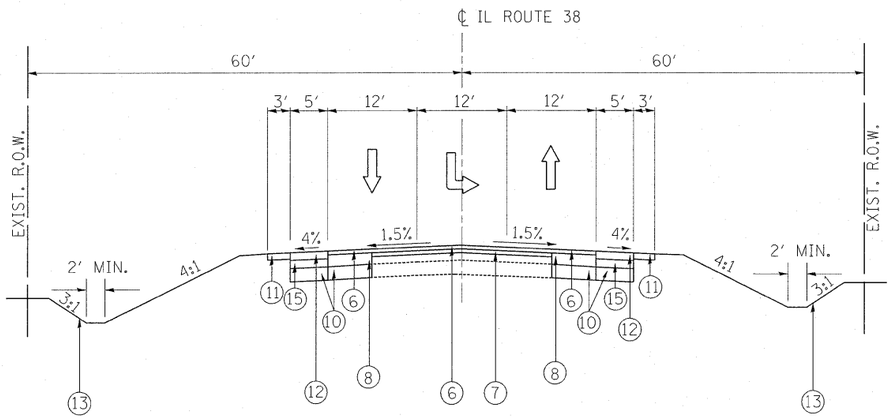
STA. 200+00 TO STA. 208+00

EXISTING TYPICAL SECTION
IL ROUTE 38



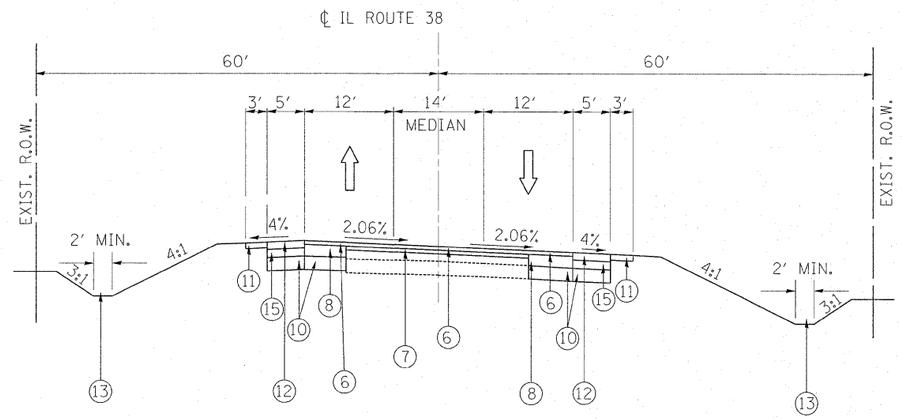
EXISTING TYPICAL SECTION
IL ROUTE 38

SECTION SUPERELEVATED STA. 208+00 TO STA. 209+50



PROPOSED TYPICAL SECTION
IL ROUTE 38

STA. 200+00 TO STA. 208+00



PROPOSED TYPICAL SECTION
IL ROUTE 38

SECTION SUPERELEVATED STA. 208+00 TO STA. 209+50

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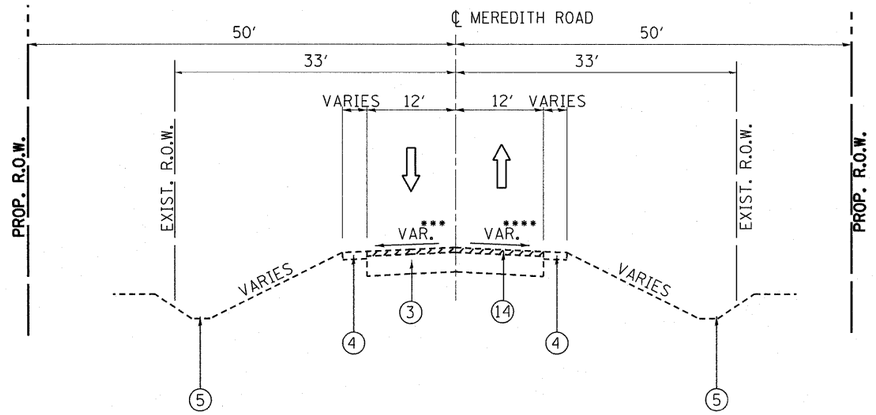
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
IL. RTE. 38 AT MEREDITH RD.

SCALE: NONE | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	6
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			



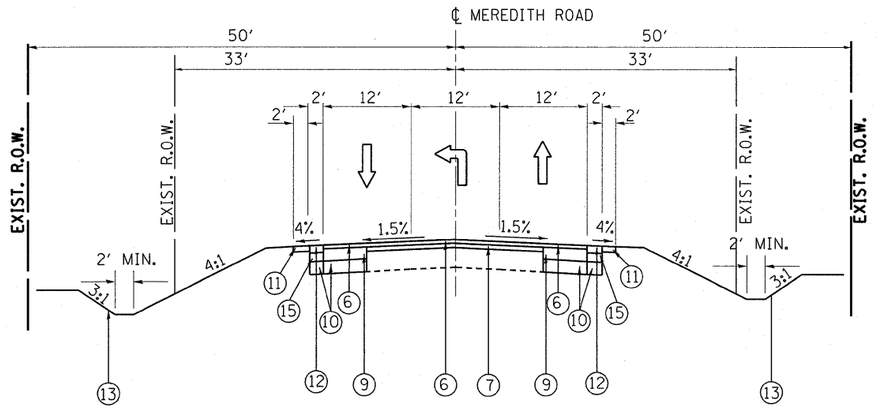
EXISTING TYPICAL SECTION
MEREDITH ROAD

LOOKING NORTH
SECTION SUPERELEVATED STA. 29+33.1 TO STA. 60+67.3

LEGEND

1. EXISTING HOT-MIX ASPHALT OVERLAY VARIES ±5 1/2" TO ±8 1/4" (IL 38)
2. EXISTING PCC BASE COURSE ±6" (IL 38)
3. EXISTING HMA BASE ±11 3/4" (MEREDITH RD.)
4. EXISTING AGGREGATE SHOULDER
5. EXISTING SWALE/DITCH
6. PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90; 2"
7. PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 1"
8. PROPOSED HOT-MIX ASPALT BASE COURSE (HMA BINDER IL-19mm, N70); 10 1/2" (IN 3 LIFTS)
9. PROPOSED HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm, N70); 7" (IN 2 LIFTS)
10. PROPOSED AGGEGATE SUBGRADE, 12"
11. PROPOSED AGGREGATE SHOULDER TYPE B, 6"
12. PROPOSED HOT-MIX ASPHALT SHOULDER, 8" (IN 2 LIFTS)
12b. PROPOSED HOT-MIX ASPHALT SHOULDER, 10" (IN 3 LIFTS)
13. PROPOSED SWALE/DITCH
•• 14. PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
15. PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B, 4 1/2"

•• REFER TO SHEET 7A FOR THE EDGE OF PAVEMENT ELEVATION AFTER MILLING.



PROPOSED TYPICAL SECTION
MEREDITH ROAD

LOOKING NORTH
SECTION SUPERELEVATED STA. 29+33.1 TO STA. 60+67.3

••• CROSS SLOPE VARIES FROM 2.2% TO 4%
•••• CROSS SLOPE VARIES FROM 1.0% TO 3.7%

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	PLOT DATE = 3/1/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
IL. RTE. 38 AT MEREDITH RD.

SCALE: NONE | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	7
		CONTRACT NO. 60K65		
		ILLINOIS FED. AID PROJECT		

EOP ELEVATIONS AFTER VAR. DEPTH MILLING			
LOCATION	LEFT EOP -	CENTER EOP	RIGHT EOP +
ILL 38 (STA. 190+52.10)	880.09	880.49	880.92
ILL 38 (STA. 191+00)	879.58	879.99	880.44
ILL 38 (STA. 191+50)	879.03	879.44	879.88
ILL 38 (STA. 192+00)	878.45	878.88	879.31
ILL 38 (STA. 192+50)	877.91	878.33	878.77
ILL 38 (STA. 192+65)	878.00	878.17	878.61
ILL 38 (STA. 193+00)	877.36	877.78	878.22
ILL 38 (STA. 193+50)	876.82	877.22	877.67
ILL 38 (STA. 194+00)	876.26	876.67	877.12
ILL 38 (STA. 194+50)	875.71	876.12	876.58
ILL 38 (STA. 195+00)	875.18	875.59	876.04
ILL 38 (STA. 195+50)	874.65	875.07	875.52
ILL 38 (STA. 196+00)	874.11	874.52	874.96
ILL 38 (STA. 196+50)	873.53	873.94	874.39
ILL 38 (STA. 197+00)	872.94	873.34	873.80
ILL 38 (STA. 197+50)	872.44	872.83	873.29
ILL 38 (STA. 198+00)	871.97	872.36	872.82
ILL 38 (STA. 198+39)	871.61	872.03	872.49
ILL 38 (STA. 198+50)	871.52	872.40	871.93
ILL 38 (STA. 199+00)	871.31	871.56	871.90
ILL 38 (STA. 199+50)	871.15	871.34	N/A
ILL 38 (STA. 200+00)	N/A	N/A	N/A
ILL 38 (STA. 200+50)	N/A	871.10	870.89
ILL 38 (STA. 201+00)	870.9	871.09	870.90
ILL 38 (STA. 201+50)	871.13	871.32	871.13
ILL 38 (STA. 202+00)	871.37	871.54	871.36
ILL 38 (STA. 202+50)	871.67	871.85	871.67
ILL 38 (STA. 203+00)	871.97	872.15	871.97
ILL 38 (STA. 203+50)	872.29	872.47	872.29
ILL 38 (STA. 204+00)	872.60	872.78	872.60
ILL 38 (STA. 204+50)	872.86	873.04	872.86
ILL 38 (STA. 205+00)	873.12	873.30	873.12

EOP ELEVATIONS AFTER VAR. DEPTH MILLING			
LOCATION	LEFT EOP -	CENTER EOP	RIGHT EOP +
ILL 38 (STA. 205+50)	873.52	873.69	873.51
ILL 38 (STA. 205+85)	874.04	873.97	873.79
ILL 38 (STA. 206+00)	873.91	874.09	873.91
ILL 38 (STA. 206+50)	874.23	874.41	874.23
ILL 38 (STA. 206+70)	874.36	874.54	874.36
ILL 38 (STA. 207+00)	874.56	874.73	874.55
ILL 38 (STA. 207+50)	874.96	875.14	874.95
ILL 38 (STA. 208+00)	875.47	875.54	875.35
ILL 38 (STA. 208+50)	876.17	876.01	875.83
ILL 38 (STA. 209+00)	876.72	876.48	876.23
ILL 38 (STA. 209+50)	877.29	877.06	876.81

NOTE:

THE DEPTH OF HOT-MIX ASPHALT SURFACE REMOVAL SHALL EQUAL 3 INCHES BELOW THE PROPOSED PAVEMENT ELEVATIONS (AS SHOWN ON THE CROSS-SECTIONS). THE EXISTING AND PROPOSED CENTERLINE ELEVATIONS SHALL REMAIN THE SAME.

EOP ELEVATIONS AFTER VAR. DEPTH MILLING			
LOCATION	LEFT EOP -	CENTER EOP	RIGHT EOP +
Meredith Rd. (STA. 29+33.10)	867.62	867.79	867.59
Meredith Rd. (STA. 29+50)	868.26	868.43	868.24
Meredith Rd. (STA. 30+00)	870.17	870.33	870.14
Meredith Rd. (STA. 30+50)	871.20	871.36	871.17
Meredith Rd. (STA. 31+00)	872.23	872.39	872.20
Meredith Rd. (STA. 31+50)	872.23	872.40	872.21
Meredith Rd. (STA. 32+00)	872.23	872.40	872.21
Meredith Rd. (STA. 32+50)	871.84	872.01	871.82
Meredith Rd. (STA. 33+00)	871.49	871.66	871.46
Meredith Rd. (STA. 33+50)	871.44	871.60	871.40
Meredith Rd. (STA. 34+00)	871.42	871.37	871.37
Meredith Rd. (STA. 34+50)	871.61	871.77	871.57
Meredith Rd. (STA. 35+00)	871.80	871.76	871.96
Meredith Rd. (STA. 35+50)	871.80	871.97	871.77
Meredith Rd. (STA. 36+00)	871.80	871.98	871.79
Meredith Rd. (STA. 36+50)	871.92	872.09	871.90
Meredith Rd. (STA. 37+00)	872.03	872.20	872.01
Meredith Rd. (STA. 37+50)	871.68	871.85	871.67
Meredith Rd. (STA. 38+00)	871.31	871.49	871.31
Meredith Rd. (STA. 38+40.34)	871.08	871.27	871.09
Meredith Rd. (STA. 38+50)	871.02	871.21	871.04
Meredith Rd. (STA. 39+00)	870.73	870.94	870.76
Meredith Rd. (STA. 39+50)		870.73	870.55

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILL 38 AT MEREDITH ROAD
VARIABLE DEPTH MILLING CHART

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	7A
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

					EARTHWORK							
1	2		3		4		5		6		7	
	CUT		UNSUITABLE		FILL		ADJUSTMENT		EARTHWORK		TOPSOIL	
	EARTH		MATERIAL		EMBANKMENT		FOR		BALANCE		FURNISH &	
ILL 38 AT MEREDITH ROAD	EXCAVATION						SHRINKAGE		WASTE (+)		PLACE	
									SHORTAGE (-)			
	(CU YD)		(CU YD)		(CU YD)		(CU YD)		(CU YD)		(SQ YD)	
	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II
ILL 38 (STA. 190+52.1 TO STA. 191+00)	11	28	57	75	36	26	9.4	23.8	-26.7	-2.2		
ILL 38 (STA. 191+00 TO STA. 191+50)	12	24	54	81	25	28	10.2	20.4	-14.8	-7.6		
ILL 38 (STA. 191+50 TO STA. 192+00)	23	31	73	77	24	29	19.6	26.4	-4.5	-2.7		
ILL 38 (STA. 192+00 TO STA. 192+50)	24	40	59	73	19	34	20.4	34.0	1.4	0.0		
ILL 38 (STA. 192+50 TO STA. 192+65)	4	18	15	22	8	13	3.4	15.3	-4.6	2.3		
ILL 38 (STA. 192+65 TO STA. 193+00)	11	40	36	51	20	35	9.4	34.0	-10.7	-1.0		
ILL 38 (STA. 193+00 TO STA. 193+50)	20	34	18	74	10	63	17.0	28.9	7.0	-34.1		
ILL 38 (STA. 193+50 TO STA. 194+00)	26	29	0	74	0	80	22.1	24.7	22.1	-55.4		
ILL 38 (STA. 194+00 TO STA. 194+50)	28	21	0	72	0	86	23.8	17.9	23.8	-68.2		
ILL 38 (STA. 194+50 TO STA. 195+00)	26	20	22	70	28	82	22.1	17.0	-5.9	-65.0		
ILL 38 (STA. 195+00 TO STA. 195+50)	21	18	53	68	60	83	17.9	15.3	-42.2	-67.7		
ILL 38 (STA. 195+50 TO STA. 196+00)	27	16	71	67	67	87	23.0	13.6	-44.1	-73.4		
ILL 38 (STA. 196+00 TO STA. 196+50)	32	16	79	68	70	92	27.2	13.6	-42.8	-78.4		
ILL 38 (STA. 196+50 TO STA. 197+00)	28	15	79	67	70	99	23.8	12.8	-46.2	-86.3		
ILL 38 (STA. 197+00 TO STA. 197+50)	23	16	70	69	70	95	19.6	13.6	-50.5	-81.4		
ILL 38 (STA. 197+50 TO STA. 198+00)	20	19	62	72	73	83	17.0	16.2	-56.0	-66.9		
ILL 38 (STA. 198+00 TO STA. 198+39)	16	39	49	57	58	34	13.6	33.2	-44.4	-0.9		
ILL 38 (STA. 198+39 TO STA. 198+50)	5	19	16	18	18	4	4.3	16.2	-13.8	12.2		
ILL 38 (STA. 198+50 TO STA. 199+00)	20	68	79	85	105	58	17.0	57.8	-88.0	-0.2		
ILL 38 (STA. 199+00 TO STA. 199+50)	17	23	78	40	123	47	14.5	19.6	-108.6	-27.5		
ILL 38 (STA. 199+50 TO STA. 200+00)	16	11	71	28	91	12	13.6	9.4	-77.4	-2.7		
ILL 38 (STA. 200+00 TO STA. 200+50)	8	17	33	63	29	43	6.8	14.5	-22.2	-28.6		
ILL 38 (STA. 200+50 TO STA. 201+00)	16	17	38	72	32	72	13.6	14.5	-18.4	-57.6		
ILL 38 (STA. 201+00 TO STA. 201+50)	43	18	77	75	53	105	36.6	15.3	-16.5	-89.7		
SUB-TOTAL	477	597	1189	1518	1089	1390	405.5	507.5	-683.6	-882.6	2690.5	3824.6

COLUMN 1: LOCATION FROM PLANS
COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL
COLUMN 3: CUT MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT, ASSUME 10'' OF UNSUITABLE MATERIAL
COLUMN 4: QUANTITIES FROM CROSS SECTIONS (FILL)

COLUMN 5: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR WAS DETERMINED TO BE 15%
COLUMN 6: COLUMN 5 - COLUMN 4, POSITIVE QUANTITY= EXTRA EXCAVATION, NEGETIVE QUANTITY= FURNISHED EXCAVATION NEEDED
COLUMN 7: TOPSOIL FURNISH AND PLACE= AREA OF SEEDING AND

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	PLOT SCALE = 50.7684 ' / IN.	DRAWN -	REVISED -			CONTRACT NO. 60K65
	PLOT DATE = 2/24/2011	CHECKED -	REVISED -		SCALE: / SHEET NO. OF SHEETS / STA. / TO STA.	ILLINOIS FED. AID PROJECT
		DATE -	REVISED -			

EARTHWORK

1	2		3		4		5		6		7	
	CUT EARTH EXCAVATION		UNSUITABLE MATERIAL		FILL EMBANKMENT		ADJUSTMENT FOR SHRINKAGE		EARTHWORK BALANCE WASTE (+) SHORTAGE (-)		TOPSOIL FURNISH & PLACE	
ILL 38 AT MEREDITH ROAD	(CU YD)		(CU YD)		(CU YD)		(CU YD)		(CU YD)		(SQ YD)	
	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II
ILL 38 (STA. 201+50 TO STA. 202+00)	61	16	78	71	40	123	51.9	13.6	11.9	-109.4		
ILL 38 (STA. 202+00 TO STA. 202+50)	70	18	79	67	37	112	59.5	15.3	22.5	-96.7		
ILL 38 (STA. 202+50 TO STA. 203+00)	44	19	64	64	50	94	37.4	16.2	-12.6	-77.9		
ILL 38 (STA. 203+00 TO STA. 203+50)	17	21	54	63	79	55	14.5	17.9	-64.6	-37.2		
ILL 38 (STA. 203+50 TO STA. 204+00)	16	23	58	42	90	13	13.6	19.6	-76.4	6.6		
ILL 38 (STA. 204+00 TO STA. 204+50)	17	19	66	43	94	15	14.5	16.2	-79.6	1.2		
ILL 38 (STA. 204+50 TO STA. 205+00)	19	16	77	67	110	29	16.2	13.6	-93.9	-15.4		
ILL 38 (STA. 205+00 TO STA. 205+50)	19	38	79	76	129	30	16.2	32.3	-112.9	2.3		
ILL 38 (STA. 205+50 TO STA. 205+85)	13	37	45	55	53	24	11.1	31.5	-42.0	7.5		
ILL 38 (STA. 205+85 TO STA. 206+00)	6	17	18	22	14	11	5.1	14.5	-8.9	3.5		
ILL 38 (STA. 206+00 TO STA. 206+50)	18	48	70	69	101	42	15.3	40.8	-85.7	-1.2		
ILL 38 (STA. 206+50 TO STA. 206+70)	7	10	26	26	38	14	6.0	8.5	-32.1	-5.5		
ILL 38 (STA. 206+70 TO STA. 207+00)	10	25	38	44	42	20	8.5	21.3	-33.5	1.3		
ILL 38 (STA. 207+00 TO STA. 207+50)	15	49	69	78	70	53	12.8	41.7	-57.3	-11.4		
ILL 38 (STA. 207+50 TO STA. 208+00)	13	22	75	73	71	73	11.1	18.7	-60.0	-54.3		
ILL 38 (STA. 208+00 TO STA. 208+50)	10	11	75	69	71	91	8.5	9.4	-62.5	-81.7		
ILL 38 (STA. 208+50 TO STA. 209+00)	9	11	75	70	66	89	7.7	9.4	-58.4	-79.7		
ILL 38 (STA. 209+00 TO STA. 209+50)	9	11	46	53	34	46	7.7	9.4	-26.4	-36.7		
SUB-TOTAL	373	411	1092	1052	1189	934	317.05	349.4	-872.0	-584.7	2608.7	2681.4

COLUMN 1: LOCATION FROM PLANS
COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL
COLUMN 3: CUT MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT, ASSUME 10" OF UNSUITABLE MATERIAL
COLUMN 4: QUANTITIES FROM CROSS SECTIONS (FILL)

COLUMN 5: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR WAS DETERMINED TO BE 15%
COLUMN 6: COLUMN 5 - COLUMN 4, POSITIVE QUANTITY= EXTRA EXCAVATION, NEGETIVE QUANTITY= FURNISHED EXCAVATION NEEDED
COLUMN 7: TOPSOIL FURNISH AND PLACE= AREA OF SEEDING AND

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	PLOT SCALE = 50.7684 ' / IN.	DRAWN -	REVISED -			
	PLOT DATE = 2/24/2011	CHECKED -	REVISED -		SCALE:	SHEET NO. OF SHEETS STA. TO STA.
		DATE -	REVISED -			

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	9
		CONTRACT NO. 60K65		
		ILLINOIS FED. AID PROJECT		

EARTHWORK

1	2		3		4		5		6		7	
	CUT EARTH EXCAVATION		UNSUITABLE MATERIAL		FILL EMBANKMENT		ADJUSTMENT FOR SHRINKAGE		EARTHWORK BALANCE WASTE (+) SHORTAGE (-)		TOPSOIL FURNISH & PLACE	
MEREDITH ROAD	(CU YD)		(CU YD)		(CU YD)		(CU YD)		(CU YD)		(SQ YD)	
	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II
STA. 29+33.1 TO STA. 29+50	6	8	16	11	3	3	5.1	6.8	2.1	3.8		
STA. 29+50 TO STA. 30+00	3	55	27	43	11	9	2.6	46.8	-8.5	37.8		
STA. 30+00 TO STA. 30+50	3	103	15	55	11	10	2.6	87.6	-8.5	77.6		
STA. 30+50 TO STA. 31+00	4	107	18	57	14	9	3.4	91.0	-10.6	82.0		
STA. 31+00 TO STA. 31+50	3	47	23	38	19	13	2.6	40.0	-16.5	27.0		
STA. 31+50 TO STA. 32+00	2	4	30	28	31	34	1.7	3.4	-29.3	-30.6		
STA. 32+00 TO STA. 32+50	2	4	47	46	85	81	1.7	3.4	-83.3	-77.6		
STA. 32+50 TO STA. 33+00	1	4	58	58	147	137	0.9	3.4	-146.2	-133.6		
STA. 33+00 TO STA. 33+50	1	3	60	59	177	148	0.9	2.6	-176.2	-145.5		
STA. 33+50 TO STA. 34+00	1	3	64	56	201	140	0.9	2.6	-200.2	-137.5		
STA. 34+00 TO STA. 34+50	3	5	50	47	180	139	2.6	4.3	-177.5	-134.8		
STA. 34+50 TO STA. 35+00	5	7	30	35	110	111	4.3	6.0	-105.8	-105.1		
STA. 35+00 TO STA. 35+50	10	9	27	28	53	65	8.5	7.7	-44.5	-57.4		
STA. 35+50 TO STA. 36+00	11	20	24	24	27	27	9.4	17.0	-17.7	-10.0		
STA. 36+00 TO STA. 36+50	9	32	21	25	19	12	7.7	27.2	-11.4	15.2		
STA. 36+50 TO STA. 37+00	9	20	27	29	17	14	7.7	17.0	-9.4	3.0		
STA. 37+00 TO STA. 37+50	6	7	34	35	16	21	5.1	6.0	-10.9	-15.1		
STA. 37+50 TO STA. 38+00	6	7	32	39	16	26	5.1	6.0	-10.9	-20.1		
STA. 38+00 TO STA. 38+40.34	4	8	24	34	13	23	3.4	6.8	-9.6	-16.2		
STA. 38+40.34 TO STA. 38+50	1	3	5	8	3	6	0.9	2.6	-2.2	-3.5		
STA. 38+50 TO STA. 39+00	7	14	38	44	31	32	6.0	11.9	-25.1	-20.1		
STA. 39+00 TO STA. 39+50	5	16	25	50	24	30	4.3	13.6	-19.8	-16.4		
STA. 39+50 TO STA. 40+00	0	9	0	28	0	13	0.0	7.7	0.0	-5.4		
SUB-TOTAL	102	495	695	877	1208	1103	86.7	420.8	-1121.3	-682.3	1949.5	2300.7

COLUMN 1: LOCATION FROM PLANS
COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL
COLUMN 3: CUT MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT, ASSUME 10" OF UNSUITABLE MATERIAL
COLUMN 4: QUANTITIES FROM CROSS SECTIONS (FILL)

COLUMN 5: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR WAS DETERMINED TO BE 15%
COLUMN 6: COLUMN 5 - COLUMN 4, POSITIVE QUANTITY= EXTRA EXCAVATION, NEGETIVE QUANTITY= FURNISHED EXCAVATION NEEDED
COLUMN 7: TOPSOIL FURNISH AND PLACE= AREA OF SEEDING AND

FILE NAME = c:\pw_work\pwidot\abreuah\d0139632\P115308-Design.dgn	USER NAME = abreuah	DESIGNED -	REVISED - AHA 2/25/11
	PLOT SCALE = 50.7684 ' / IN.	DRAWN -	REVISED -
	PLOT DATE = 2/24/2011	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILL 38 AT MEREDITH ROAD
SCHEDULE OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	10
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

EARTHWORK

1	2		3		4		5		6		7	
	CUT EARTH EXCAVATION (CU YD)		UNSUITABLE * MATERIAL (CU YD)		FILL EMBANKMENT (CU YD)		ADJUSTMENT FOR SHRINKAGE (CU YD)		EARTHWORK BALANCE WASTE (+) SHORTAGE (-) (CU YD)		TOPSOIL FURNISH & PLACE (SQ YD)	
MEREDITH ROAD (north leg)	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II	LEFT STAGE I	RIGHT STAGE II
STA. 51+00 TO STA. 51+50	0	14	124	83	98	71	0.0	11.9	-98.0	-59.1		
STA. 51+50 TO STA. 52+00	0	0	25	14	19	11	0.0	0.0	-19.0	-11.0		
STA. 52+00 TO STA. 52+50	7	4	72	53	50	35	6.0	3.4	-44.1	-31.6		
STA. 52+50 TO STA. 53+00	13	11	61	68	53	39	11.1	9.4	-42.0	-29.7		
STA. 53+00 TO STA. 53+50	4	11	65	68	99	47	3.4	9.4	-95.6	-37.7		
STA. 53+50 TO STA. 54+00	0	7	65	66	113	52	0.0	6.0	-113.0	-46.1		
STA. 54+00 TO STA. 54+50	10	3	62	61	80	54	8.5	2.6	-71.5	-51.5		
STA. 54+50 TO STA. 55+00	10	3	66	60	69	53	8.5	2.6	-60.5	-50.5		
STA. 55+00 TO STA. 55+50	4	7	67	60	75	49	3.4	6.0	-71.6	-43.1		
STA. 55+50 TO STA. 56+00	4	6	68	60	77	51	3.4	5.1	-73.6	-45.9		
STA. 56+00 TO STA. 56+50	5	1	67	59	72	57	4.3	0.9	-67.8	-56.2		
STA. 56+50 TO STA. 57+00	8	0	63	53	67	59	6.8	0.0	-60.2	-59.0		
STA. 57+00 TO STA. 57+50	3	0	53	47	59	51	2.6	0.0	-56.5	-51.0		
STA. 57+50 TO STA. 58+00	6	0	56	39	48	37	5.1	0.0	-42.9	-37.0		
STA. 58+00 TO STA. 58+50	6	0	61	31	44	23	5.1	0.0	-38.9	-23.0		
STA. 58+50 TO STA. 59+00	0	0	49	33	33	18	0.0	0.0	-33.0	-18.0		
STA. 59+00 TO STA. 59+50	0	0	44	34	28	18	0.0	0.0	-28.0	-18.0		
STA. 59+50 TO STA. 59+50	0	0	39	28	23	15	0.0	0.0	-23.0	-15.0		
STA. 59+50 TO STA. 60+00	0	0	33	21	20	12	0.0	0.0	-20.0	-12.0		
STA. 60+00 TO STA. 60+50	0	0	28	19	17	11	0.0	0.0	-17.0	-11.0		
STA. 60+50 TO STA. 60+67.3	0	0	9	7	6	4	0.0	0.0	-6.0	-4.0		
STA. 60+67.3 TO STA. 61+00	0	0	8	6	5	4	0.0	0.0	-5.0	-4.0		
SUB-TOTAL	80	67	1185	970	1155	771	68.0	57.0	-1087.0	-714.1	1957.7	1511.4

COLUMN 1: LOCATION FROM PLANS
COLUMN 2: CUT QUANTITIES FROM CROSS SECTIONS, WHICH DOES NOT INCLUDE UNSUITABLE MATERIAL
COLUMN 3: CUT MATERIAL THAT IS DETERMINED TO BE EITHER UNSTABLE OR UNSUITABLE FOR USE IN EMBANKMENT, ASSUME 10" OF UNSUITABLE MATERIAL
COLUMN 4: QUANTITIES FROM CROSS SECTIONS (FILL)

COLUMN 5: EARTH EXCAVATION THAT IS TO BE USED AS FILL MATERIAL IN THE EMBANKMENT, SHRINKAGE FACTOR WAS DETERMINED TO BE 15%
COLUMN 6: COLUMN 5 - COLUMN 4, POSITIVE QUANTITY= EXTRA EXCAVATION, NEGETIVE QUANTITY= FURNISHED EXCAVATION NEEDED
COLUMN 7: TOPSOIL FURNISH AND PLACE= AREA OF SEEDING AND

* BETWEEN STA. 50+50 TO STA. 52+00 (NORTH MEREDITH) UNDERCUT THE WEST EMBANKMENT AN ADDITIONAL 14" BEYOND THE 10" UNSUITABLE REMOVAL, FOR A TOTAL OF 24". THE WIDTH OF THIS UNDERCUT SHALL BE FROM THE BOTTOM OF THE EXISTING DITCH TO THE PROPOSED DITCH BOTTOM. THIS UNDERCUT SHALL BE FILLED WITH POROUS GRANULAR EMBANKMENT SUBGRADE AND PAID PER CUYD. SEE TEMPLATED CROSS SECTIONS.

* BETWEEN STA. 50+50 TO STA. 60+67 (NORTH MEREDITH) UNDER CUT THE WEST EMBANKMENT AN ADDITIONAL 18" BELOW THE PROPOSED AGGREGATE SUBGRADE. THIS UNDERCUT SHALL BE BENEATH THE PROPOSED WIDENING. THIS AREA CAN BE FILLED WITH NEW EMBANKMENT MATERIAL. SEE TEMPLATED CROSS SECTIONS.

* BETWEEN STA. 51+00 TO STA. 60+67 (NORTH MEREDITH) UNDER CUT THE EAST EMBANKMENT AN ADDITIONAL 18" BELOW THE PROPOSED AGGREGATE SUBGRADE. THIS UNDERCUT SHALL BE BENEATH THE PROPOSED WIDENING. THIS AREA CAN BE FILLED WITH NEW EMBANKMENT MATERIAL. SEE TEMPLATED CROSS SECTIONS.

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	PLOT DATE = 2/24/2011	CHECKED -	REVISED -
		DATE -	REVISED -

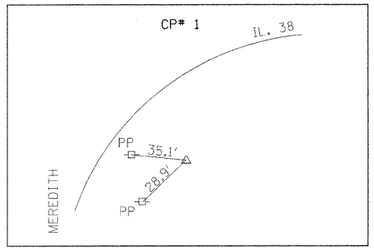
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILL 38 AT MEREDITH ROAD
SCHEDULE OF QUANTITIES

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	11
		CONTRACT NO. 60K65		
		ILLINOIS FED. AID PROJECT		

CP# 1



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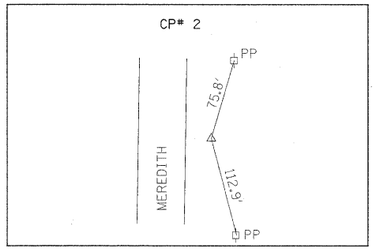

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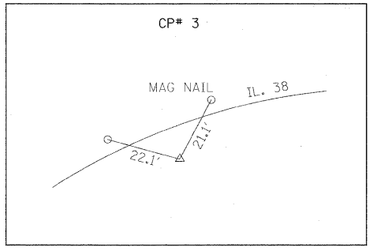

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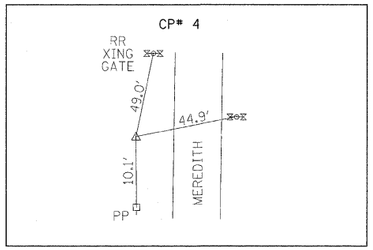

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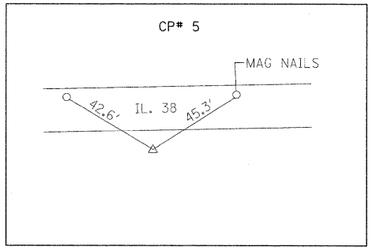
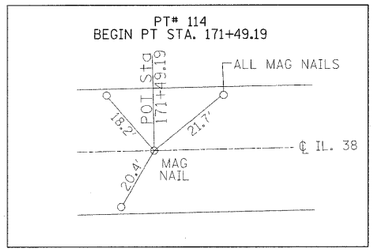
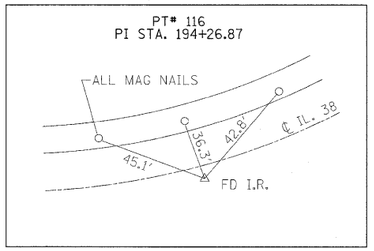


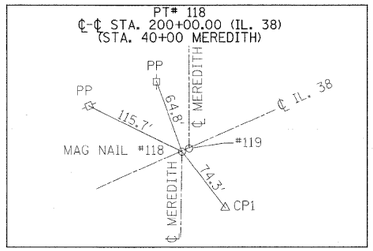
PT# 114
BEGIN PT STA. 171+49.19



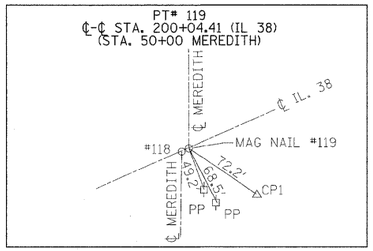
PT# 116
PI STA. 194+26.87



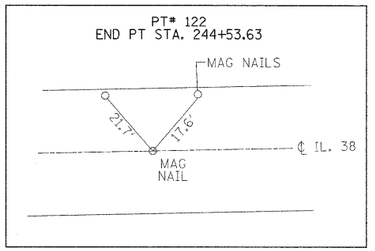
PT# 118
℄-℄ STA. 200+00.00 (IL. 38)
(STA. 40+00 MEREDITH)



PT# 119
℄-℄ STA. 200+04.41 (IL. 38)
(STA. 50+00 MEREDITH)



PT# 122
END PT STA. 244+53.63



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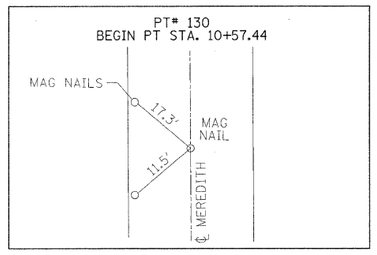
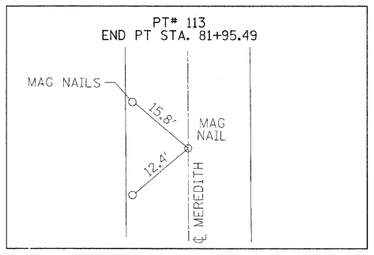


PT# 113
END PT STA. 81+95.49



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	PLOT DATE = 8/18/2010	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=200' | SHEET NO. 2 OF 2 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	12
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

BENCHMARKS

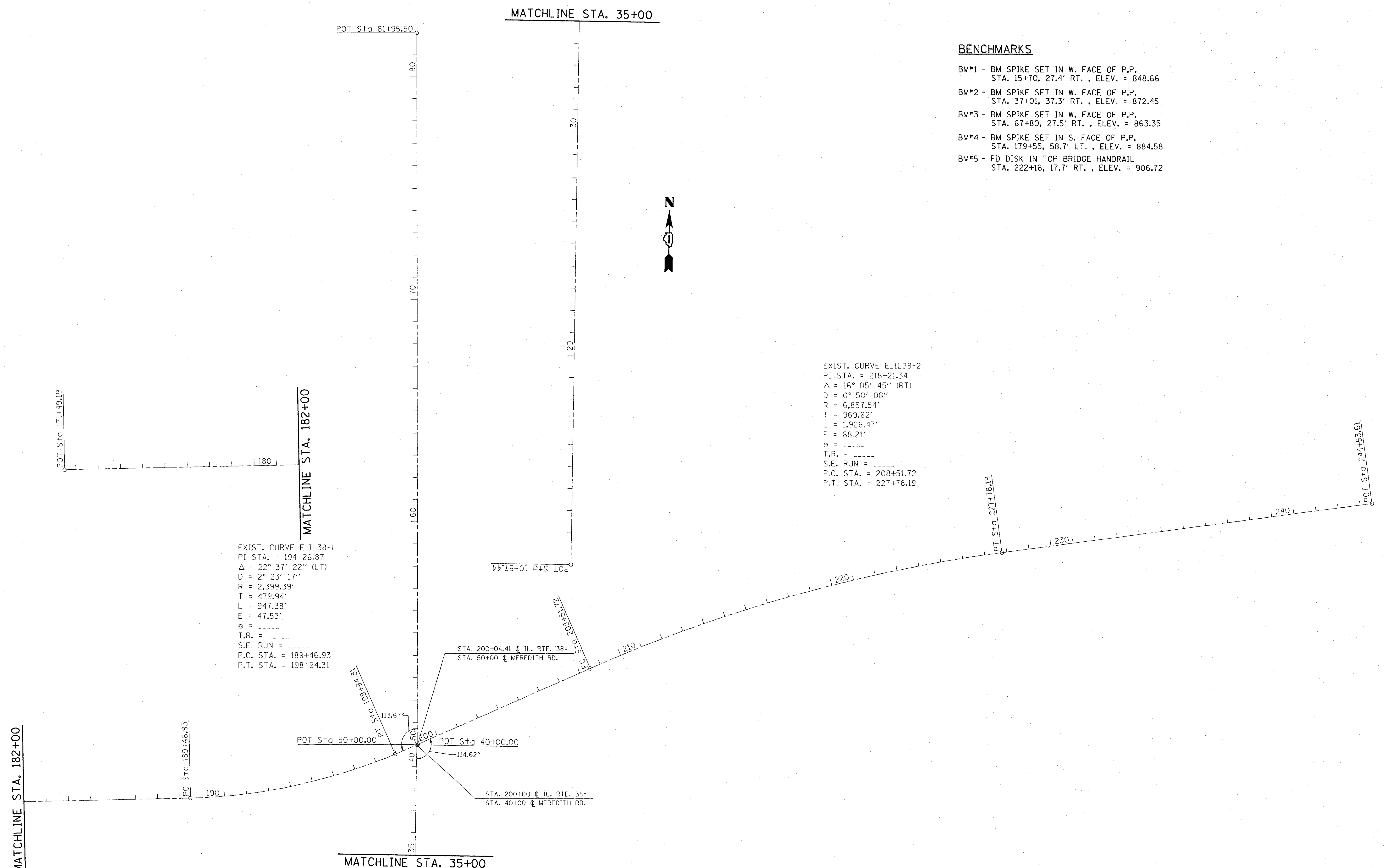
BM#1 - BM SPIKE SET IN W. FACE OF P.P.
STA. 15+70, 27.4' RT. , ELEV. = 848.66

BM#2 - BM SPIKE SET IN W. FACE OF P.P.
STA. 37+01, 37.3' RT. , ELEV. = 872.45

BM#3 - BM SPIKE SET IN W. FACE OF P.P.
STA. 67+80, 27.5' RT. , ELEV. = 863.35

BM#4 - BM SPIKE SET IN S. FACE OF P.P.
STA. 179+55, 58.7' LT. , ELEV. = 884.58

BM#5 - FD DISK IN TOP BRIDGE HANDRAIL
STA. 222+16, 17.7' RT. , ELEV. = 906.72



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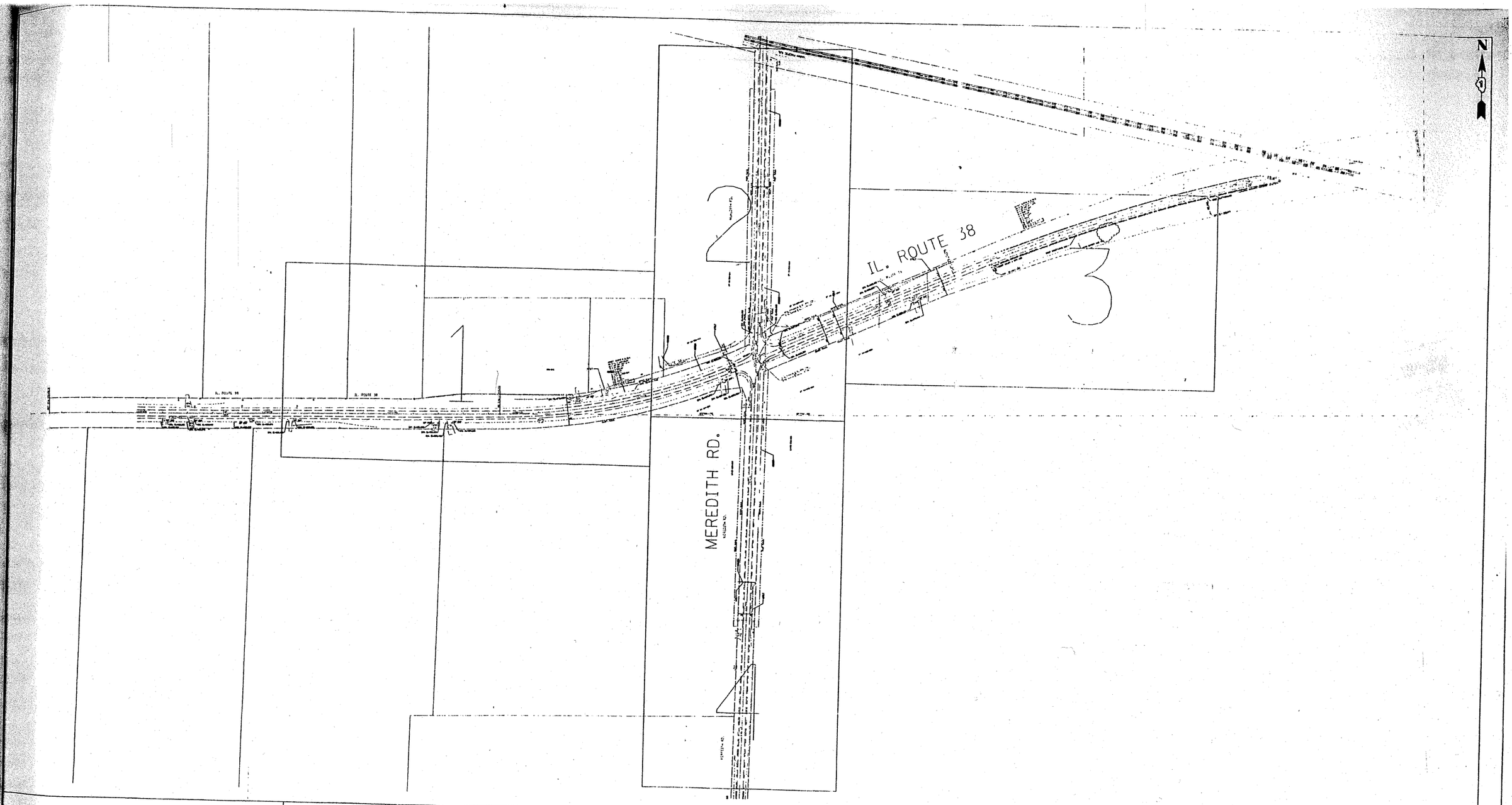
	PLOT DATE = 2/3/2011	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=200' | SHEET NO. 1 OF 2 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	13
		CONTRACT NO. 60K65		
		ILLINOIS FED. AID PROJECT		



A	AERIAL UTILITY
	UNKNOWN
CTV	CABLE TV
T	TELEPHONE
	GAS
E	ELECTRIC
W	WATER
FO	FIBER OPTIC
	SEWER
	TBE TEST HOLE

Utility Quality Level "A" : Test Hole
Utility Quality Level "B" : Designating
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

Utilities shown in color on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. TBE's SUE field investigation was finished on 10/01/10. Changes to utilities after this date may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN IN COLOR QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.

DESIGNED	EG	REVISED
DRAWN	KLC	REVISED
CHECKED		REVISED
DATE	10/27/10	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

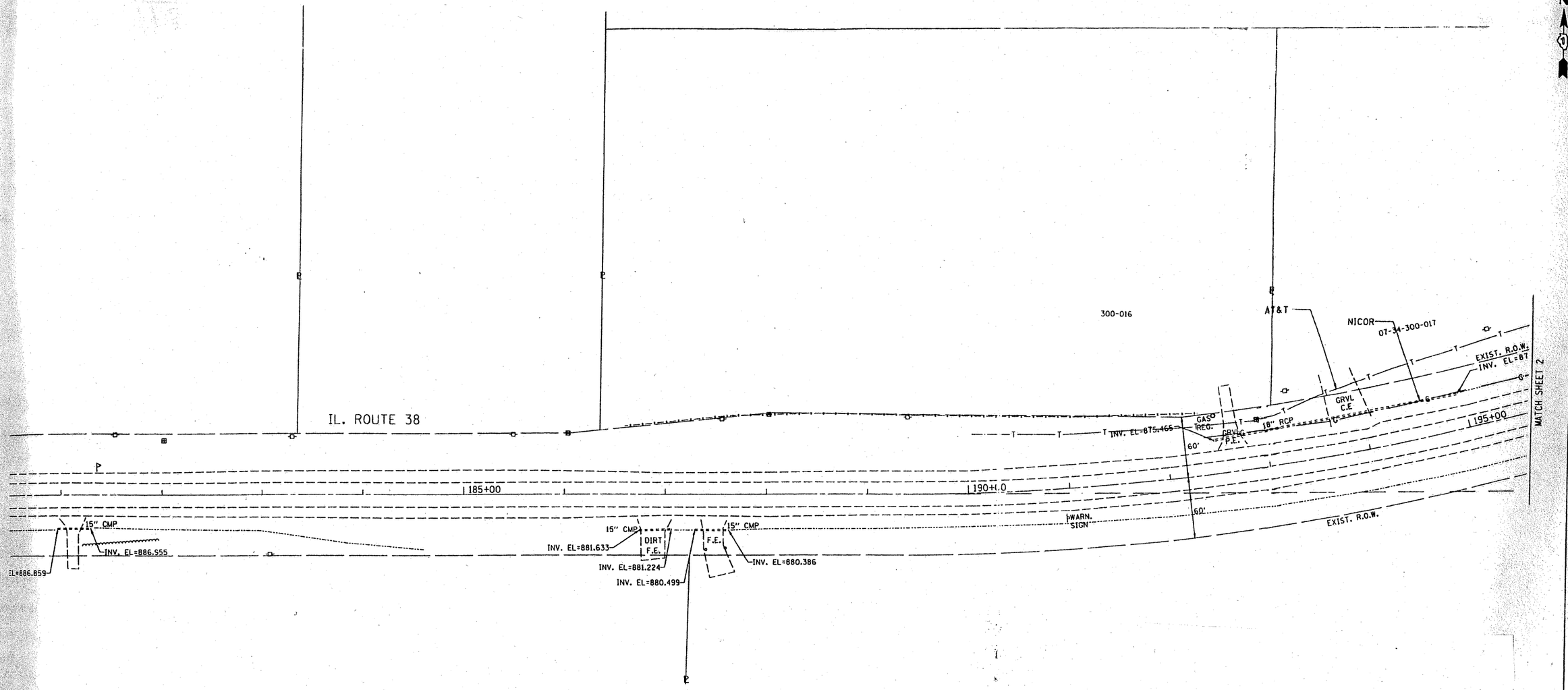
LICENSED PROFESSIONAL ENGINEER
KENNETH F SLANINKA, Jr.
062-055488
STATE OF ILLINOIS
Signature
10/28/10
Date
license expires 11-30-11

Cardno TBE
CIVIL ENGINEERING * TRANSPORTATION * ENVIRONMENTAL * PLANNING * UTILITY ENGINEERING/LOCATING

TBE Job No. IL09510420
SUE Plan Page: Cover

IL RT. 38 at Meredith Road
Maple Park, IL.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	5R-N-1	Kane	77	19
		Contract No. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	IDOT Project No.		



A ——— A ——— AERIAL UTILITY
- - - - - - - - - - - - UNKNOWN
CTV ——— CTV ——— CABLE TV
T ——— T ——— TELEPHONE
G ——— G ——— GAS
E ——— E ——— ELECTRIC
W ——— W ——— WATER
FO ——— FO ——— FIBER OPTIC
SEWER
TBE TEST HOLE

Utility Quality Level "A" : Test Hole
Utility Quality Level "B" : Designating
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

Utilities shown in color on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. TBE's SUE field investigation was finished on 10/01/10. Changes to utilities after this date may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN IN COLOR
QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.

20 0 METERS 20 40
50 0 FEET 50 100

KENNETH F. SLANINKA, Jr. 062-055488 LICENSED PROFESSIONAL ENGINEER STATE OF ILLINOIS
10/28/10
date
license expires 11-30-11

Cardno TBE
CIVIL ENGINEERING * TRANSPORTATION * ENVIRONMENTAL * PLANNING * UTILITY ENGINEERING/LOCATING

TBE Job No. IL09510420
SUE Plan Page: 1 of 4

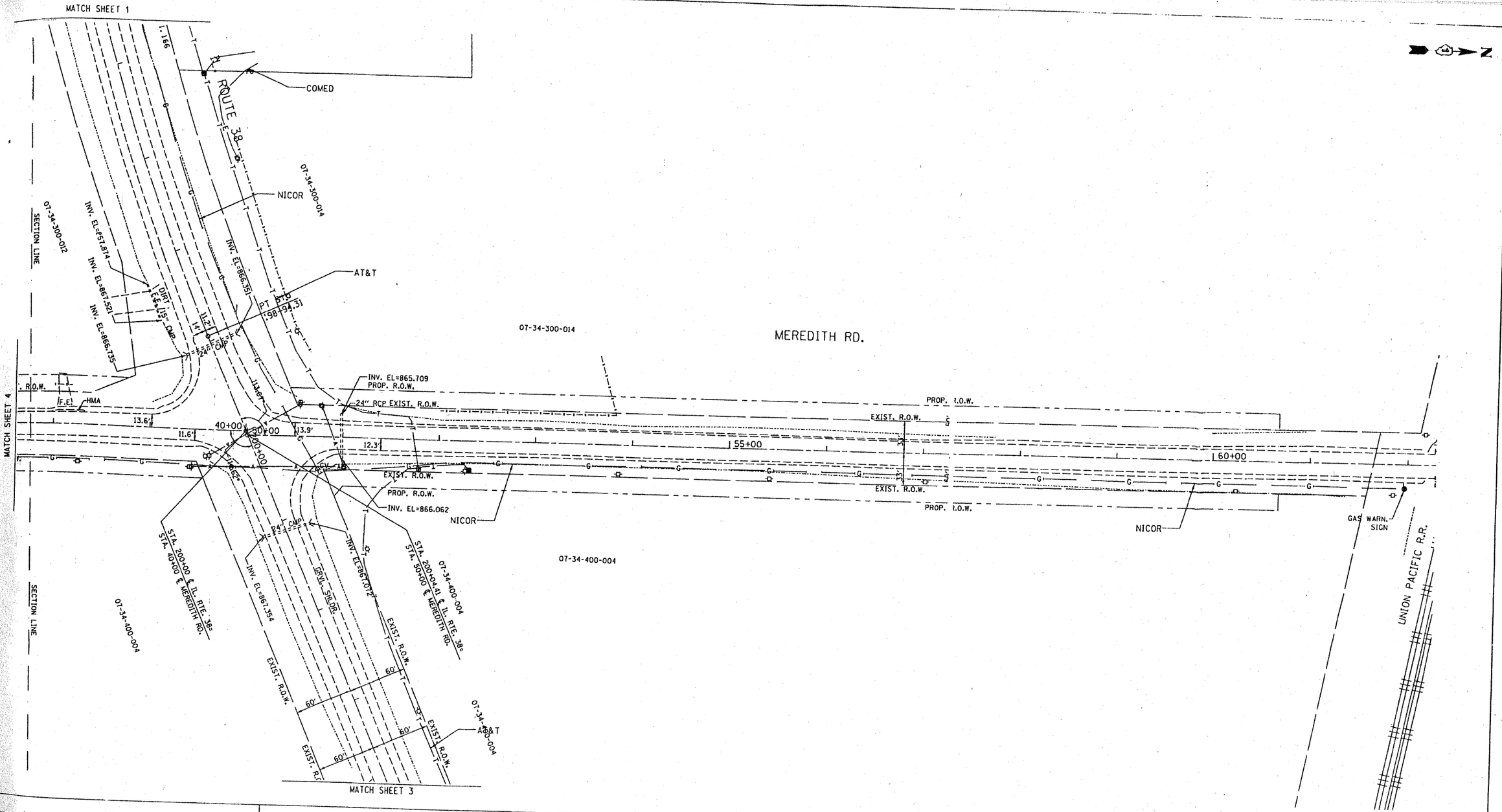
DESIGNED	EG
DRAWN	KLC
CHECKED	
DATE	10/27/10

REVISED
REVISED
REVISED
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL RT. 38 at Meredith Road
Maple Park, IL.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	5R-N-1	Kane	77	15
		Contract No. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	IDOT Project No.		



A — A	AERIAL UTILITY
- - - - - -	UNKNOWN
CTV — CTV	CABLE TV
T — T	TELEPHONE
G	GAS
E — E	ELECTRIC
W	WATER
FO — FO	FIBER OPTIC
	SEWER
	TBE TEST HOLE

Utility Quality Level "A" : Test Hole
Utility Quality Level "B" : Designating
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

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ALL UTILITIES SHOWN IN COLOR QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.

20 0 METERS 20 40
50 0 FEET 50 100

LICENSED PROFESSIONAL ENGINEER
KENNETH F SLANINKA, Jr.
062-055488
STATE OF ILLINOIS
signature
10/28/10
date
license expires 11-30-11

Cardno TBE
CIVIL ENGINEERING * TRANSPORTATION * ENVIRONMENTAL * PLANNING * UTILITY ENGINEERING/LOCATING

TBE Job No. IL09510420
SUE Plan Page: 2 of 4

DESIGNED	EG
DRAWN	KLC
CHECKED	
DATE	10/27/10

REVISED
REVISED
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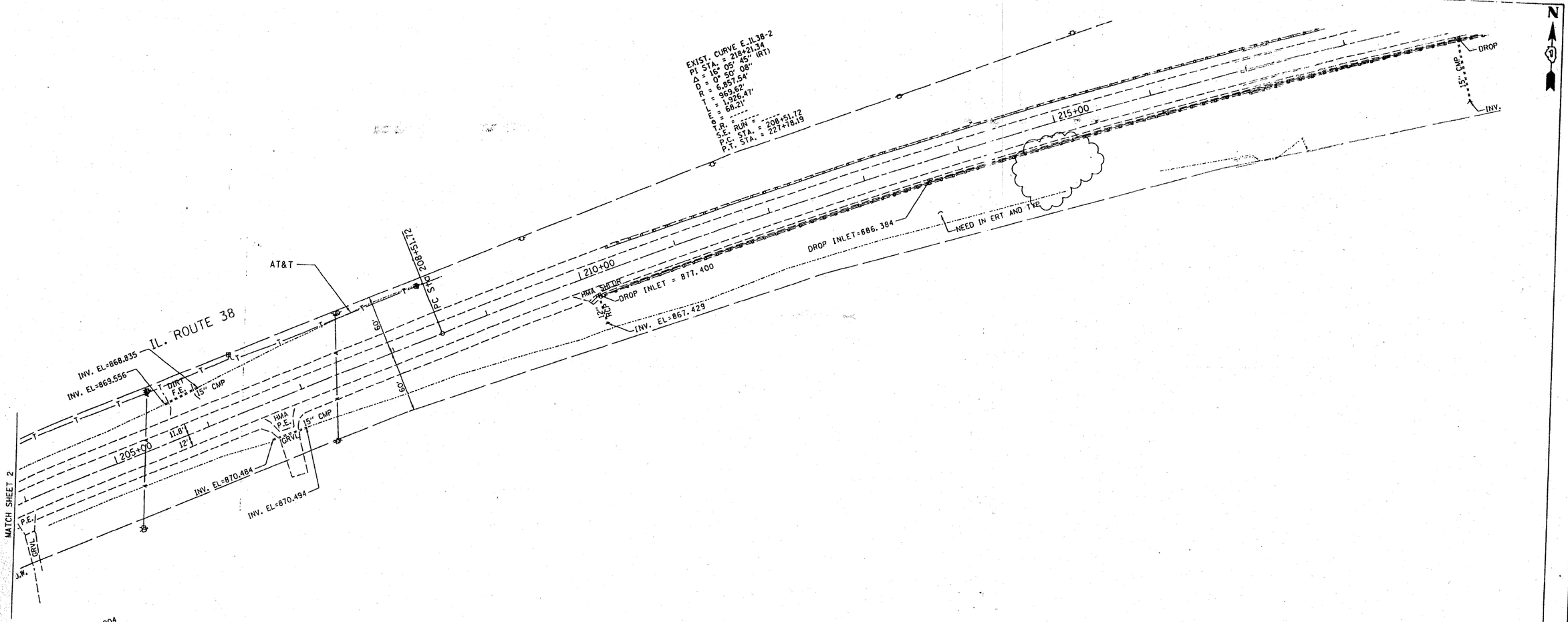
REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL RT. 38 at Meredith Road
Maple Park, IL.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	5R-N-1	Kane	77	16
		Contract No. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	IDOT Project No.		



A — A	AERIAL UTILITY
	UNKNOWN
CTV — CTV	CABLE TV
T — T	TELEPHONE
G —	GAS
E — E	ELECTRIC
W —	WATER
FO — FO	FIBER OPTIC
	SEWER
	TBE TEST HOLE

Utility Quality Level "A" : Test Hole
Utility Quality Level "B" : Designating
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

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20 0 METERS 20 40
50 0 FEET 50 100

DESIGNED	EG	REVISED
DRAWN	KLC	REVISED
CHECKED		REVISED
DATE	10/27/10	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL RT. 38 at Meredith Road
Maple Park, IL.

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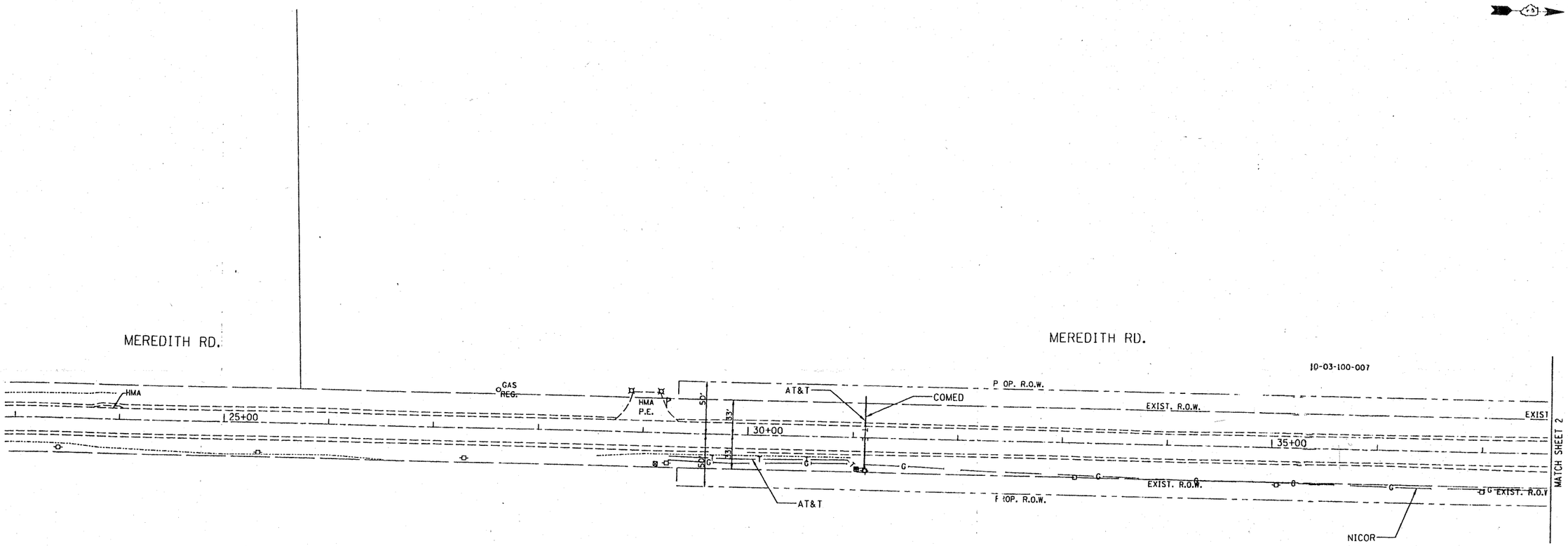
Cardno TBE
CIVIL ENGINEERING * TRANSPORTATION * ENVIRONMENTAL * PLANNING * UTILITY ENGINEERING/LOCATING

TBE Job No. IL09510420
SUE Plan Page: 3 of 4

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	5R-N-1	Kane	77	17
		Contract No. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	IDOT Project No.		



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A ——— A ——— AERIAL UTILITY
- - - - - - - - - - - - UNKNOWN
CTV ——— CTV ——— CABLE TV
T ——— T ——— TELEPHONE
G ——— G ——— GAS
E ——— E ——— ELECTRIC
W ——— W ——— WATER
FO ——— FO ——— FIBER OPTIC
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KENNETH F SLANINKA, Jr 062-055488
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STATE OF ILLINOIS
signature
10/20/10
date
license expires 11-30-11

Cardno TBE
CIVIL ENGINEERING * TRANSPORTATION * ENVIRONMENTAL * PLANNING * UTILITY ENGINEERING/LOCATING

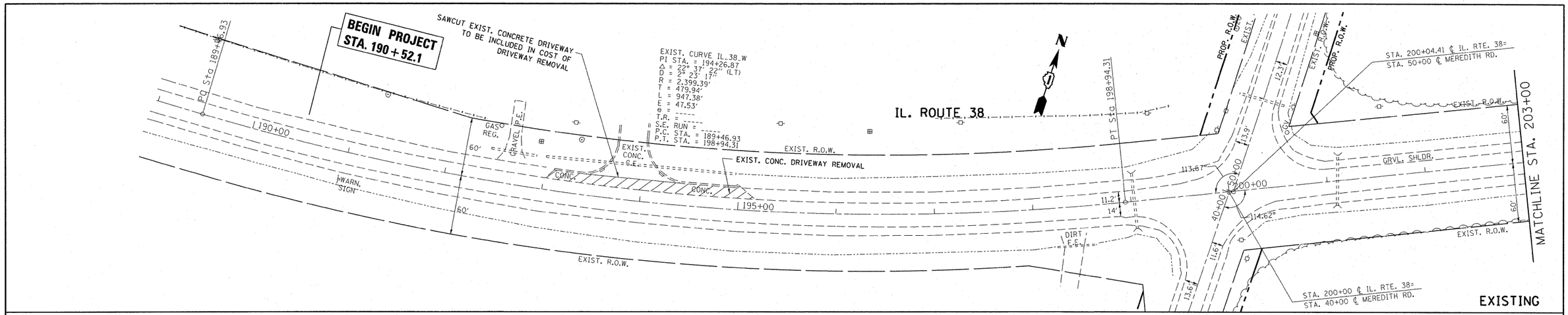
TBE Job No. IL09510420
SUE Plan Page: 4 of 4

DESIGNED	EG	REVISED
DRAWN	KLC	REVISED
CHECKED		REVISED
DATE	10/27/10	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL RT. 38 at Meredith Road
Maple Park, IL.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	5R-N-1	Kane	77	18
		Contract No. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	IDOT Project No.		



LEGEND

① (IL. 38 WIDENING AREA)
-PROPOSED AGGREGATE SUBGRADE, 12"
-PROPOSED HOT-MIX ASPHALT BASE COURSE, 10 1/2 "
-PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 2"

② (RESURFACING AREA)
-PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
-PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 ; 1"
-PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 2"

③ (HMA P.E.) PRIVATE ENTRANCE
-PROPOSED HOT-MIX ASPHALT BASE COURSE, 6"
-PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"

④ (HMA F.E.) FIELD ENTRANCE
-PROPOSED AGGREGATE BASE COURSE, TYPE B, 8"
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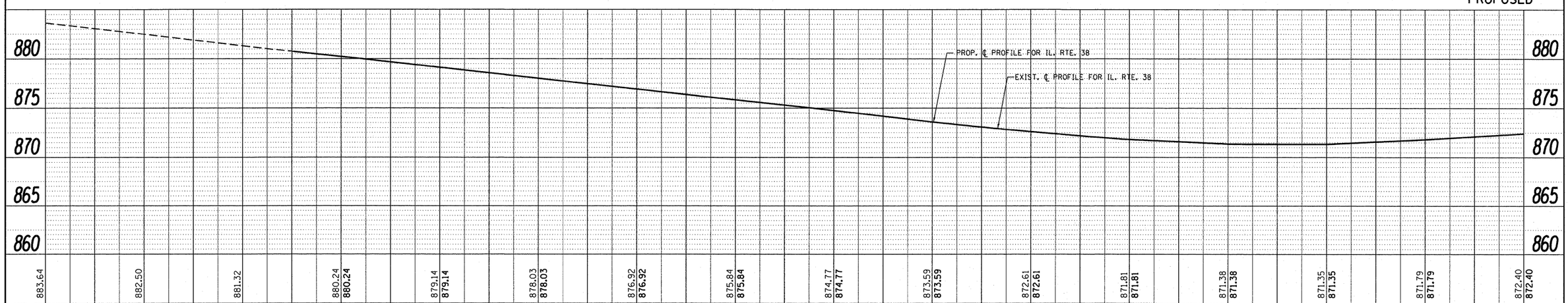

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN & PROFILE
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	19

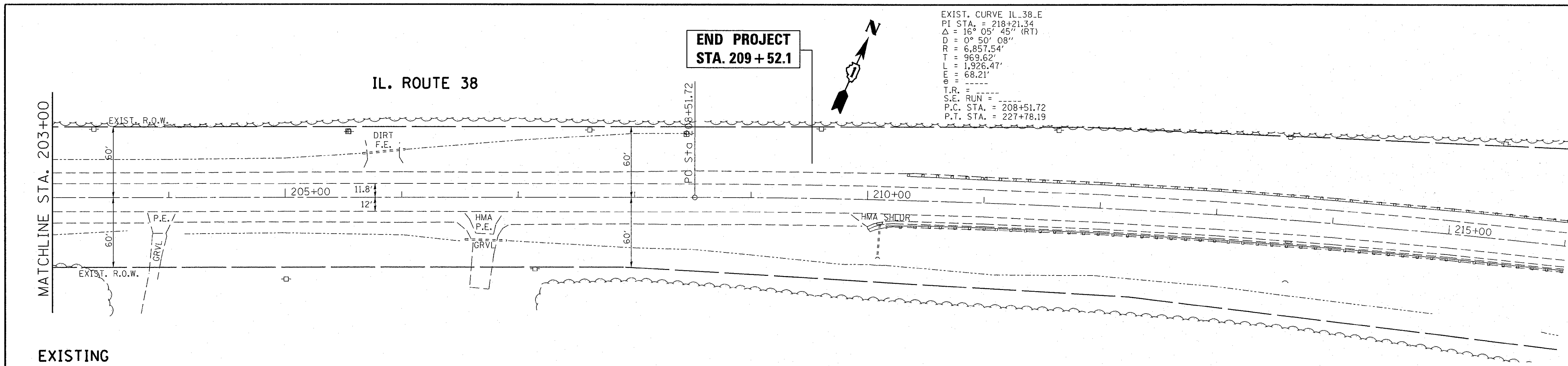
CONTRACT NO. 60K65

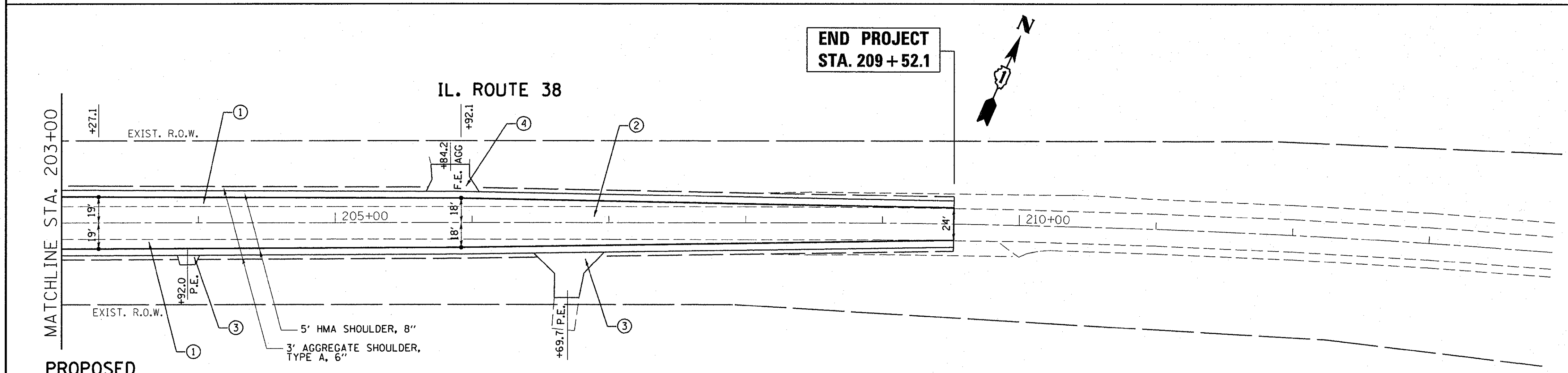
ILLINOIS FED. AID PROJECT

PLAN — SURVEYED, DRAWN, ALIGNMENT CHECKED, RT. OF WAY CHECKED, NOTE BOOK, NO. ____, CADD FILE NAME ____, BY, DATE

PROFILE — SURVEYED, DRAWN, GRADES CHECKED, R.M. NOTED, STRUCTURE NO(S) ____, BY, DATE

EXIST. CURVE IL_38_E
PI STA. = 218+21.34
Δ = 16° 05' 45" (RT)
D = 0° 50' 08"
R = 6,857.54'
T = 969.62'
L = 1,926.47'
E = 68.21'
e = -----
T.R. = -----
S.E. RUN = -----
P.C. STA. = 208+51.72
P.T. STA. = 227+78.19





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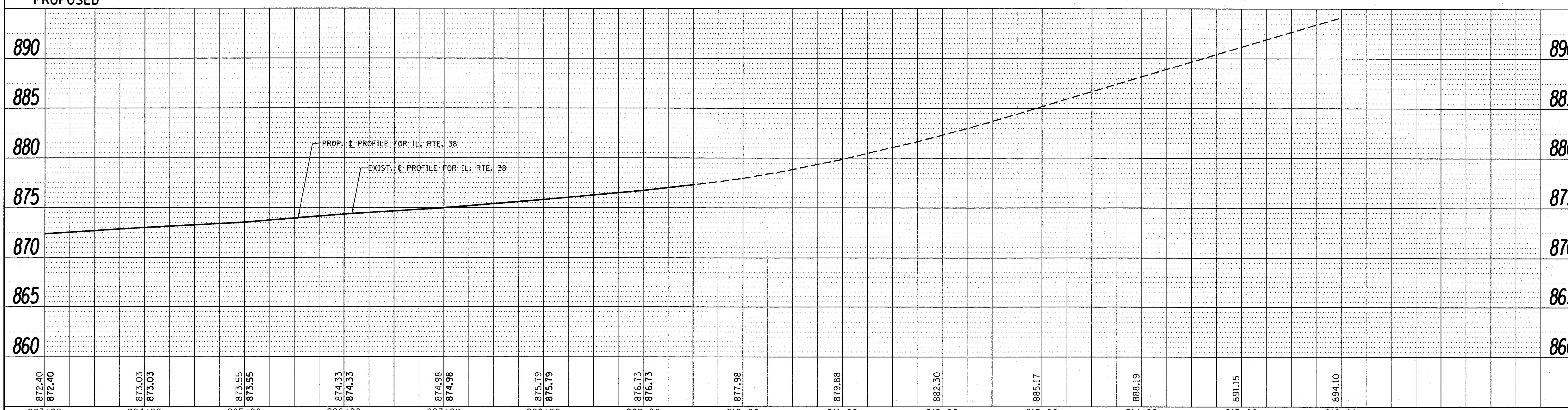

PLAN		
NOTE BOOK NO.	SURVEYED / PLOTTED / ALIGNMENT CHECKED / R.T. OF WAY CHECKED / CADD FILE NAME	BY / DATE

PROFILE		
NOTE BOOK NO.	SURVEYED / PLOTTED / GRADES CHECKED / B.M. NOTED / STRUCTURE NOTAT'NS CH'KD	BY / DATE

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

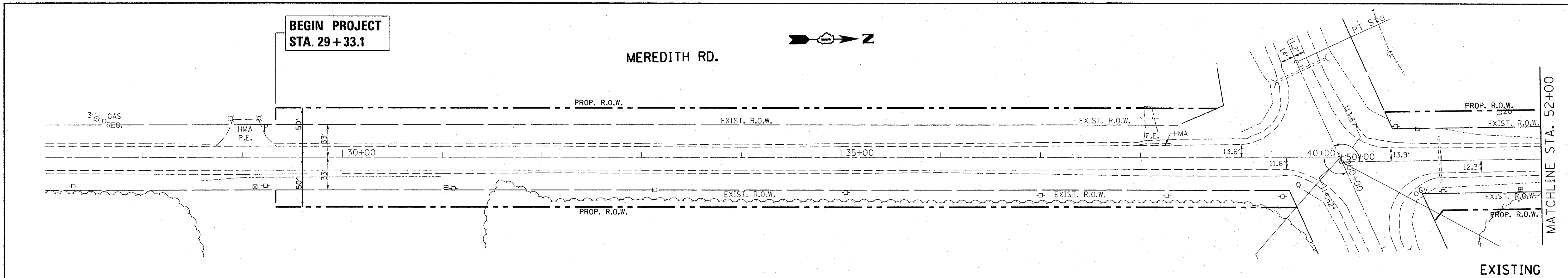
ROADWAY PLAN & PROFILE
IL. RTE. 38 AT MEREDITH RD.

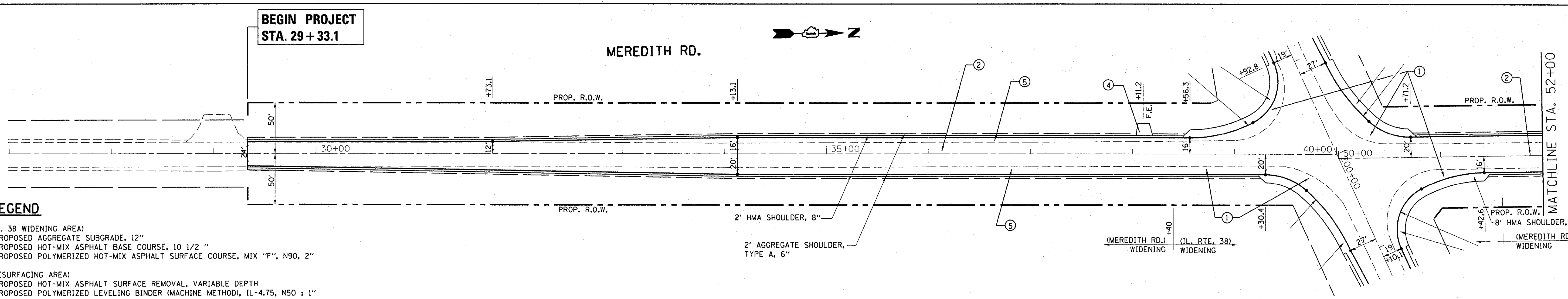
SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	20

CONTRACT NO. 60K65

ILLINOIS | FED. AID PROJECT





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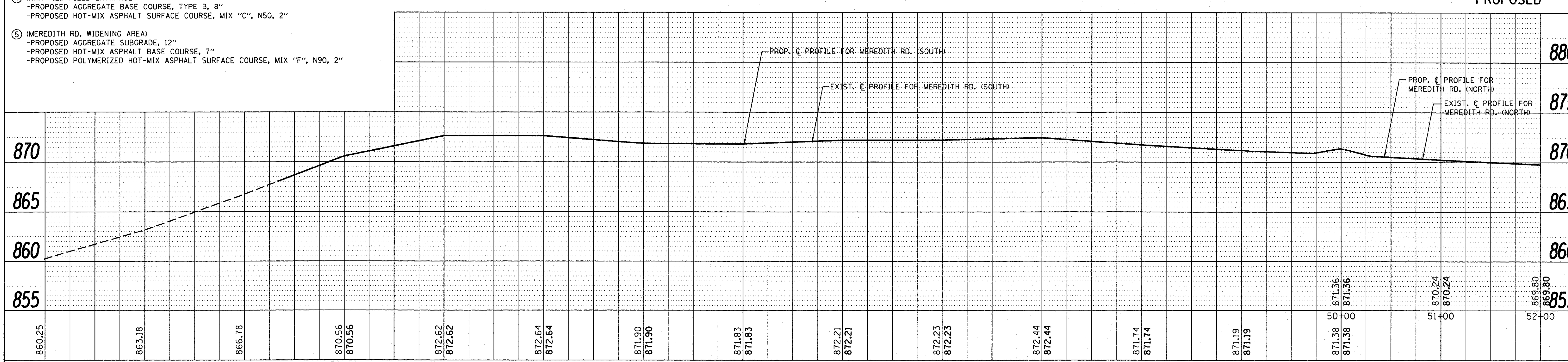

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

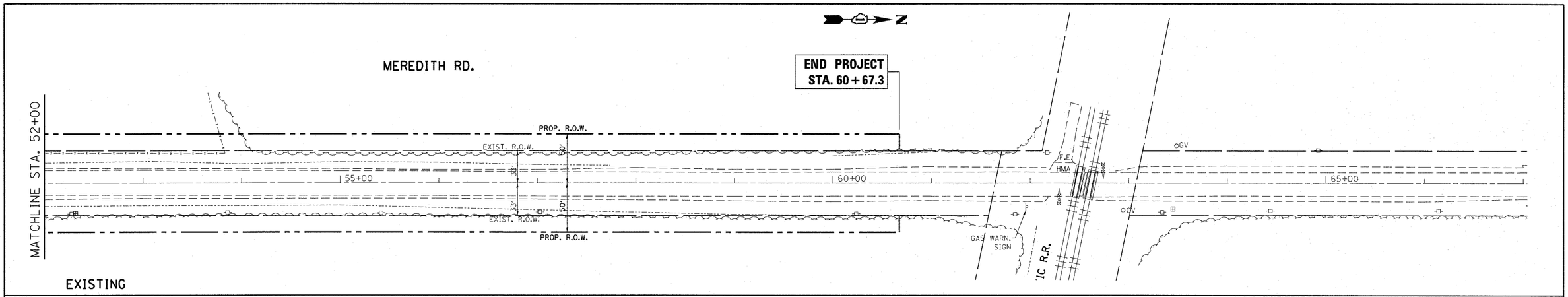
ROADWAY PLAN & PROFILE
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	21

CONTRACT NO. 60K65

ILLINOIS FED. AID PROJECT



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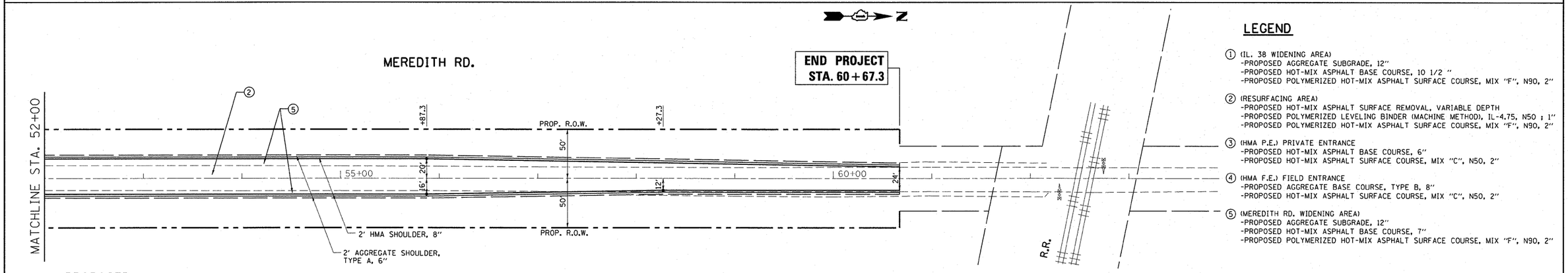

PROPOSED

LEGEND

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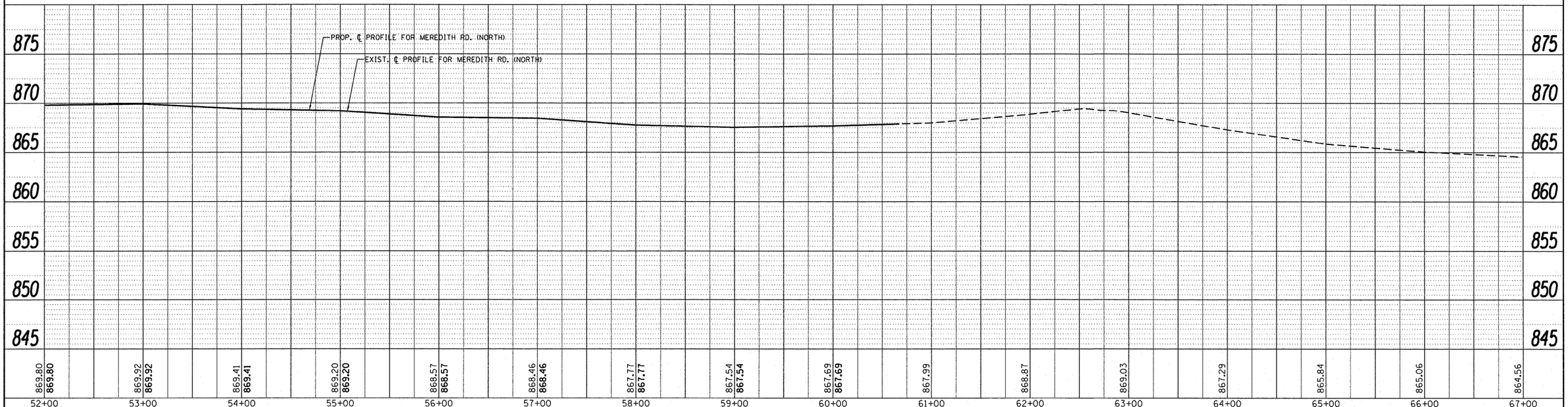

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PROFILE: SURVEYED, PLOTTED, GRADES CHECKED, B.M. NOTED, STRUCTURE NOTATING CHKD, NOTE BOOK, NO. ____, BY, DATE

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN & PROFILE
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	22
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

EROSION CONTROL NOTES

THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN EROSION CONTROL MEASURES IMMEDIATELY AFTER STRIPPING OF EXISTING VEGETATION.

NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE SITE OTHER THAN THROUGH SEDIMENTATION/STILLING BASINS. THE CONTRACTOR WILL ADJUST HIS OPERATIONS AND IMPLEMENT EROSION CONTROL MEASURES ACCORDINGLY.

THE QUANTITIES SHOWN FOR TEMPORARY DITCH CHECKS ARE MEASURED PER FOOT, REGARDLESS OF TYPE OR CONFIGURATION USED.

THE CONTRACTOR SHALL SURROUND ALL EARTH STOCKPILES WITH SILT FENCE AND SHALL BE PAID FOR AS PERIMETER EROSION BARRIER, EROSION CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR AND ENGINEER WITHIN 24 HOURS OR ANY STORM EXCEEDING 0.5 INCH OF PRECIPITATION.

STOCKPILES OF SOIL AND OTHER BUILDING MATERIALS TO REMAIN IN PLACE MORE THAN THREE (3) DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES (I.E. PERIMETER SILT FENCE). STOCKPILES TO REMAIN IN PLACE FOR 14 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.

ALL CONSTRUCTION ACTIVITIES SHALL BE IN ACCORDANCE WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM STORM WATER PERMIT.

THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PREVENT POLLUTION OF STORM WATER AND SHALL FOLLOW IEPA & IDOT CONSTRUCTION MEMORANDUM NO. 95-60.

EROSION CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH SEQUENCE OF STAGE CONSTRUCTION.

STABILIZATION MEASURES SHALL BE INTIATED AS SOON AS PRACTICAL, BUT IN NO CASE EXCEED 7 DAYS AFTER CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY WILL NOT OCCUR FOR A PERIOD OF 14 OR MORE CALENDER DAYS.

THE CONTRACTOR SHALL APPLY TEMPORARY EROSION CONTROL SEEDING TO ALL ERODIBLE BARE EARTH AREAS WITHIN THE CONTRACT LIMITS EACH WEEK, REGARDLESS OF WEATHER CONDITIONS OR PROGRESS OF THE WORK. UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ERODIBLE EMBANKMEN AND EXCAVATION AREAS WHERE WORK IS IN PROGRESS SHALL BE INCLUDED ON THE AREAS TO BE SEEDED. SEE SPECIAL PROVISION FOR TEMPORARY EROSION CONTROL SEEDING.

ALL PERIMETER EROSION BARRIER SHALL BE PLACED IN STAGE I, IF REQUIRED IN STAGE II IT SHALL BE LEFT IN PLACE. IT SHALL BE REPLACED IF DAMAGED, AT THE DIRECTION OF THE ENGINEER.

REFER TO LANDSCAPING PLAN FOR AREA TO BE SEEDED

UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SHALL MEET IDOT STANDARDS AND SPECIFICATIONS AND SPECIAL PROVISIONS.

A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.

DURING DEWATERING OPERATIONS, WATER WILL BE PUMPED INTO SEDIMENT BASINS OR SILT TRAPS. DEWATERING INTO DRAIN TILES IS STRICTLY PROHIBITED. COMPROMISED DRAIN TILES SHOULD BE IMMEDIATELY REPAIRED OR INCORPORATED INTO STORM WATER FACILITIES.

ALL DROP INLETS ON AND ADJACENT TO THE SITE MUST HAVE A SEDIMENT TRAPPING OR CONTAINMENT DEVICE INSTALLED DURING CONSTRUCTION ACTIVITIES.

ALL ADJACENT STREETS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY AND CLEANED WHEN NECESSARY.

ALL EROSION CONTROL MEASURES MUST BE INSPECTED WEEKLY AND AFTER EACH 1/2 " RAIN EVENT OR EQUIVALENT RAINFALL.

EROSION CONTROL BLANKET AND/OR STRAW MULCH WITH NETTING (DEPENDING ON SLOPE, SLOPE LENGTH, AND FLOW RATES) SHALL BE INSTALLED ON ALL SLOPES AND IN CRITICAL AREAS (I.E. DETENTION BASIN PERIMETERS, BERMS ETC.) IMMEDIATELY UPON FINAL GRADING.

THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER FOR PROPER EROSION AND SEDIMENT CONTROL. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.

BERMS MUST BE STABILIZED IMMEDIATELY UPON RECEIVING FINAL GRADING. STRAW MULCH WITH NETTING OR EROSION CONTROL BLANKET SHALL BE USED ON SIDE SLOPES AND SUMMIT.

IN AREAS WHERE WORK IS COMPLETE, PERMANENT STABILIZATION SHALL OCCUR WITHIN 7 DAYS OF COMPLETION, AND IN AREAS WHERE WORK HAS TEMPORARILY CEASED FOR 14 DAYS OR MORE, TEMPORARY STABILIZATION SHALL OCCUR BY THE 7TH DAY AFTER WORK HAS CEASED.

COMPLETED SLOPES SHALL BE SEEDED AND MULCHED (OR BLANKETED, IF APPLICABLE) AS THE EXCAVATION PROCEEDS TO THE EXTENT CONSIDERED DESIRABLE AND PRACTICAL. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WATER IN AND NEAR THE CRITICAL AREAS SHOULD BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOW. THE STREAM BANKS SHOULD BE STABILIZED AT THE END OF EACH DAY. ONCE WORK IN THIS AREA BEGINS, PRIORITY SHALL BE GIVEN TO THE COMPLETION OF THE WORK AND FINAL STABILIZATION OF ALL DISTURBED AREAS.

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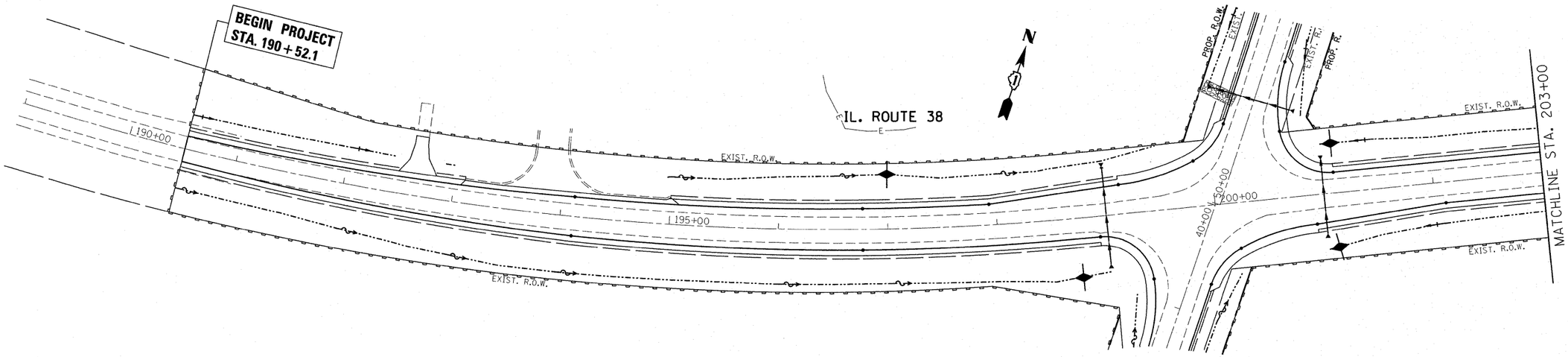
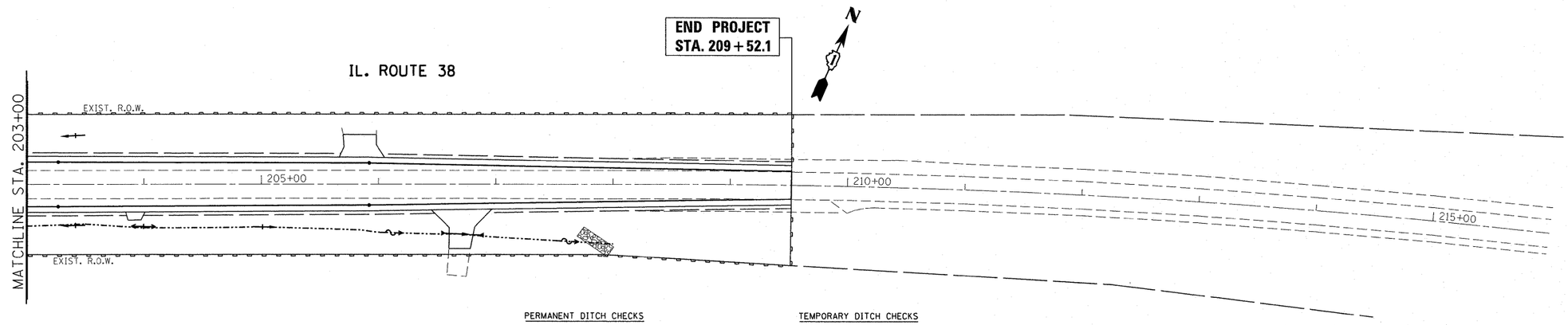
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ILL 38 AT MEREDITH ROAD
EROSION CONTROL NOTES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	23
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			





TEMPORARY EROSION CONTROL
SEQUENCE OF CONSTRUCTION

1. ESTABLISH TEMPORARY EROSION CONTROL MEASURES AND ERECT SILT FILTER FENCE ALONG SITE BOUUNDRIES PRIOR TO MASS EARTHWORK.
2. INSTALL DITCH CHECKS IMMEDIATELY AFTER DITCH DISTURBANCE STARTS
3. INSTALL PERMANENT SEEDING BEFORE BEGINNING NEXT CONSTRUCTION STAGE.

PERMANENT DITCH CHECKS

197+00 LT.
198+75 RT.
201+10 RT.
201+10 LT.

NOTE:
PERMANENT DITCH CHECKS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR AGGREGATE DITCH CHECKS.

INSTALL PERIMETER EROSION BARRIER ALL ALONG R.O.W. & TEMPORARY EASEMENTS.

TEMPORARY DITCH CHECKS

(LEFT O/S)	(RIGHT O/S)	(LEFT O/S)	(RIGHT O/S)
191+50	192+00	29+30	29+30
192+43	192+42	38+80	30+30
192+58	192+74	51+73	30+90
192+80	193+30	54+50	38+40
193+19	194+16	60+65	53+00
194+05	195+95		56+00
194+80	196+66		60+65
195+50	197+70		
197+12	198+60		
198+00	201+27		
198+75	201+54		
201+25	202+54		
201+50	203+46		
202+50	203+75		
	204+00		
	205+50		
	206+50		
	207+50		

LEGEND

SYMBOL	DESCRIPTION
	PERMANENT DITCH CHECK
	PERIMETER EROSION BARRIER (SILT FENCE, SEE STANDARD 280001)
	EXISTING FLOW LINE
	EXIST. R.O.W.
	PROP. R.O.W.

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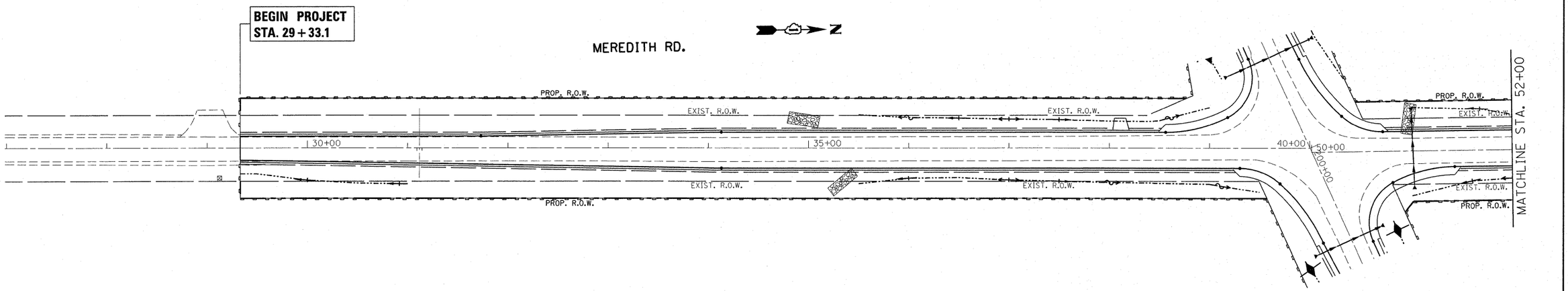
	PLOT DATE = 3/2/2011	DATE -	REVISED -

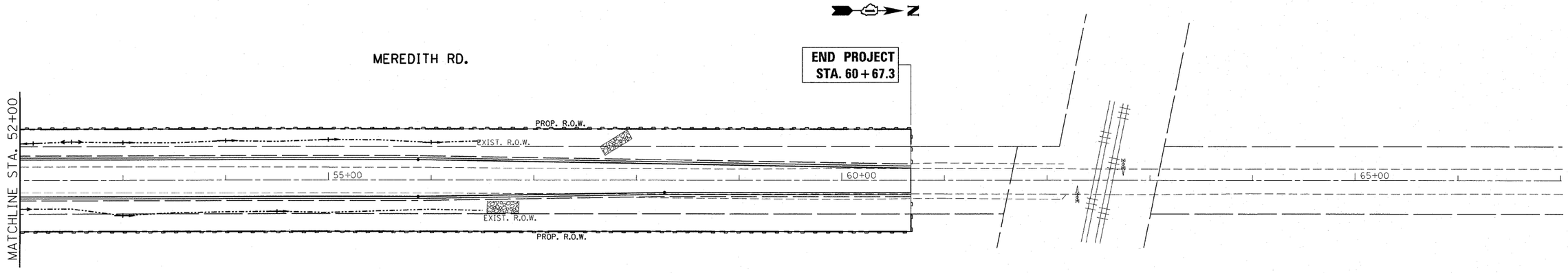
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	24
		CONTRACT NO. 60K65		
		ILLINOIS FED. AID PROJECT		





TEMPORARY EROSION CONTROL
SEQUENCE OF CONSTRUCTION

1. ESTABLISH TEMPORARY EROSION CONTROL MEASURES AND ERECT SILT FILTER FENCE ALONG SITE BOUUNDRIES PRIOR TO MASS EARTHWORK.
2. INSTALL DITCH CHECKS IMMEDIATELY AFTER DITCH DISTURBANCE STARTS
3. INSTALL PERMANENT SEEDING BEFORE BEGINNING NEXT CONSTRUCTION STAGE.

PERMANENT DITCH CHECKS

197+00 LT.
198+75 RT.
201+10 RT.
201+10 LT.

NOTE:

PERMANENT DITCH CHECKS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR AGGREGATE DITCH CHECKS.

INSTALL PERIMETER EROSION BARRIER ALL ALONG R.O.W. & TEMPORARY EASEMENTS.

TEMPORARY DITCH CHECKS

(LEFT O/S)	(RIGHT O/S)	(LEFT O/S)	(RIGHT O/S)
191+50	192+00	29+30	29+30
192+43	192+42	38+80	30+30
192+58	192+74	51+73	30+90
192+80	193+30	54+50	38+40
193+19	194+16	60+65	53+00
194+05	195+95		56+00
194+80	196+66		60+65
195+50	197+70		
197+12	198+60		
198+00	201+27		
198+75	201+54		
201+25	202+54		
201+50	203+46		
202+50	203+75		
	204+00		
	205+50		
	206+50		
	207+50		

LEGEND

SYMBOL	DESCRIPTION
	PERMANENT DITCH CHECK
	PERIMETER EROSION BARRIER (SILT FENCE, SEE STANDARD 280001)
	EXISTING FLOW LINE
	EXIST. R.O.W.
	PROP. R.O.W.

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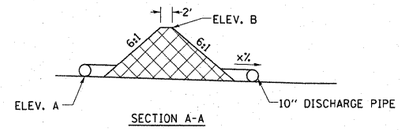
	PLOT DATE = 3/2/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION CONTROL PLAN
IL. RTE. 38 AT MEREDITH RD.

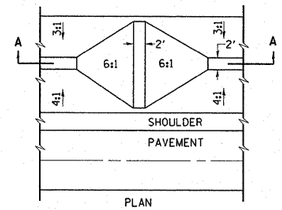
SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	25
		CONTRACT NO. 60K65		
		ILLINOIS FED. AID PROJECT		



SECTION A-A

DITCH CHECK RESTRICTOR				
CONTROL STRUCTURE LOCATION	RESTRICTOR #	DISCHARGE PIPE DIAMETER (INCH)	ELEV. A ORIFICE INVERT ELEVATION (FT)	ELEV. B OVERFLOW ELEVATION (FT)
LEFT OF IL 38 AT STA. 197+00	3-1	10	868.65	870.16
RIGHT OF IL 38 AT STA. 198+75	3-2	10	866.78	868.78
RIGHT OF IL 38 AT STA. 201+10	3-3	10	867.07	869.50
LEFT OF IL 38 AT STA. 201+10	3-4	10	865.96	869.07



PLAN

DITCH CHECK RESTRICTOR
(SEE TABLE FOR LOCATIONS)

DETAIL "A"

NOTE:
PERMANENT DITCH CHECK RESTRICTOR WILL BE PAID FOR AT CONTRACTOR UNIT PRICE TON FOR "AGGREGATE DITCH CHECKS",

PIPE UNDERDRAIN LOCATION CHART			
LOCATION	STATIONS	OFFSET	TYPE
IL 38	STA. 195+00 TO STA. 195+50	LEFT & RIGHT WIDENING	LONGITUDINAL
IL 38	STA. 198+50 TO STA. 199+00	LEFT & RIGHT WIDENING	LONGITUDINAL
IL 38	STA. 200+50 TO STA. 201+50	LEFT & RIGHT WIDENING	LONGITUDINAL
IL 38	STA. 204+50 TO STA. 205+00	LEFT & RIGHT WIDENING	LONGITUDINAL
MEREDITH RD.	STA. 38+50 TO STA. 39+00	LEFT & RIGHT WIDENING	LONGITUDINAL
MEREDITH RD.	STA. 56+50 TO STA. 57+50	LEFT WIDENING	LONGITUDINAL

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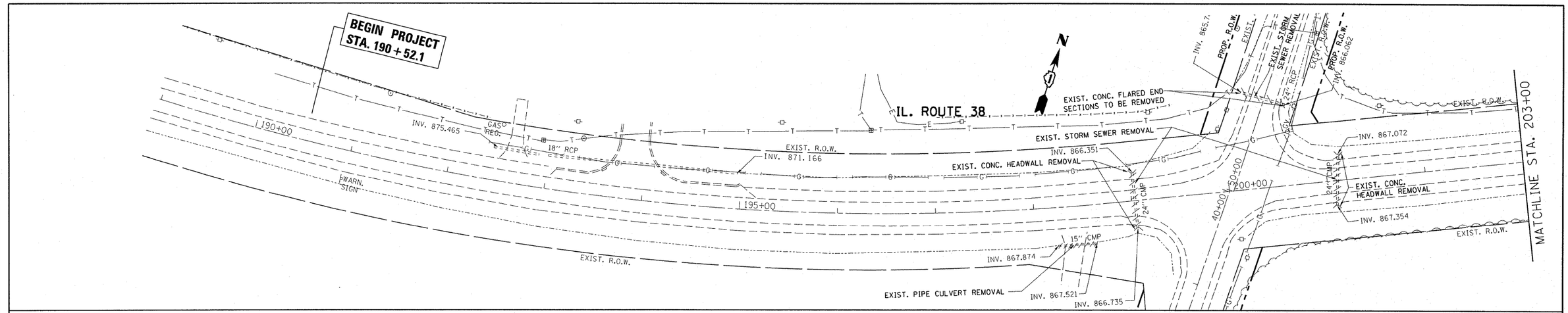
	PLOT DATE = 2/22/2011	DATE -	REVISED -

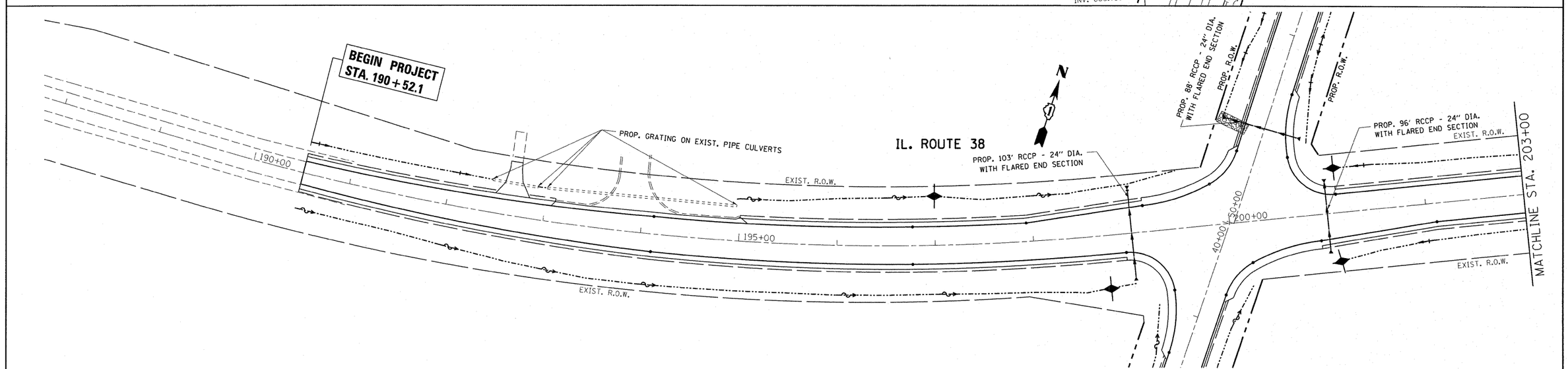
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

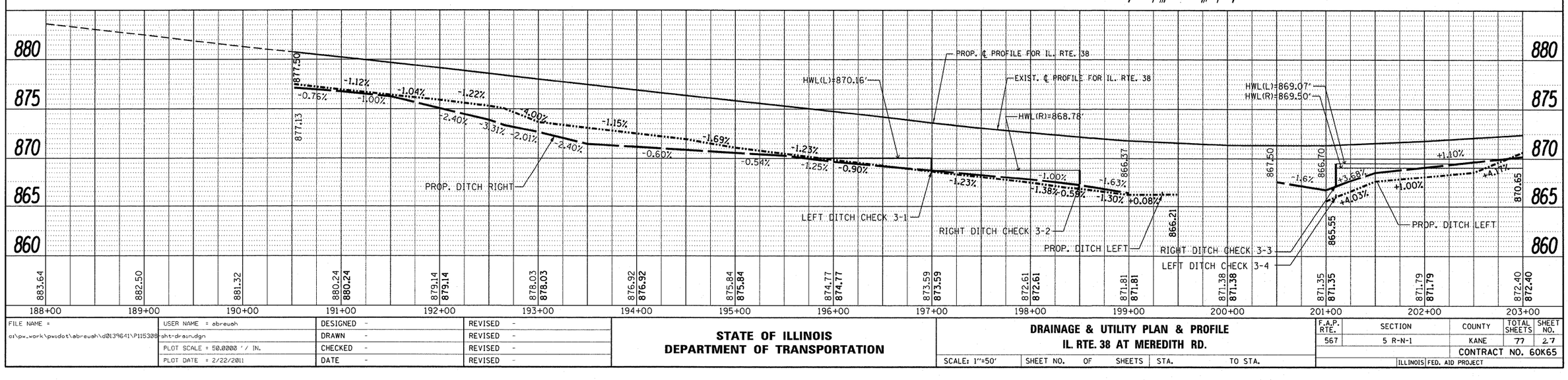
ILL 38 AT MEREDITH ROAD
DRAINAGE DETAILS/TABLES

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	26
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			







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	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

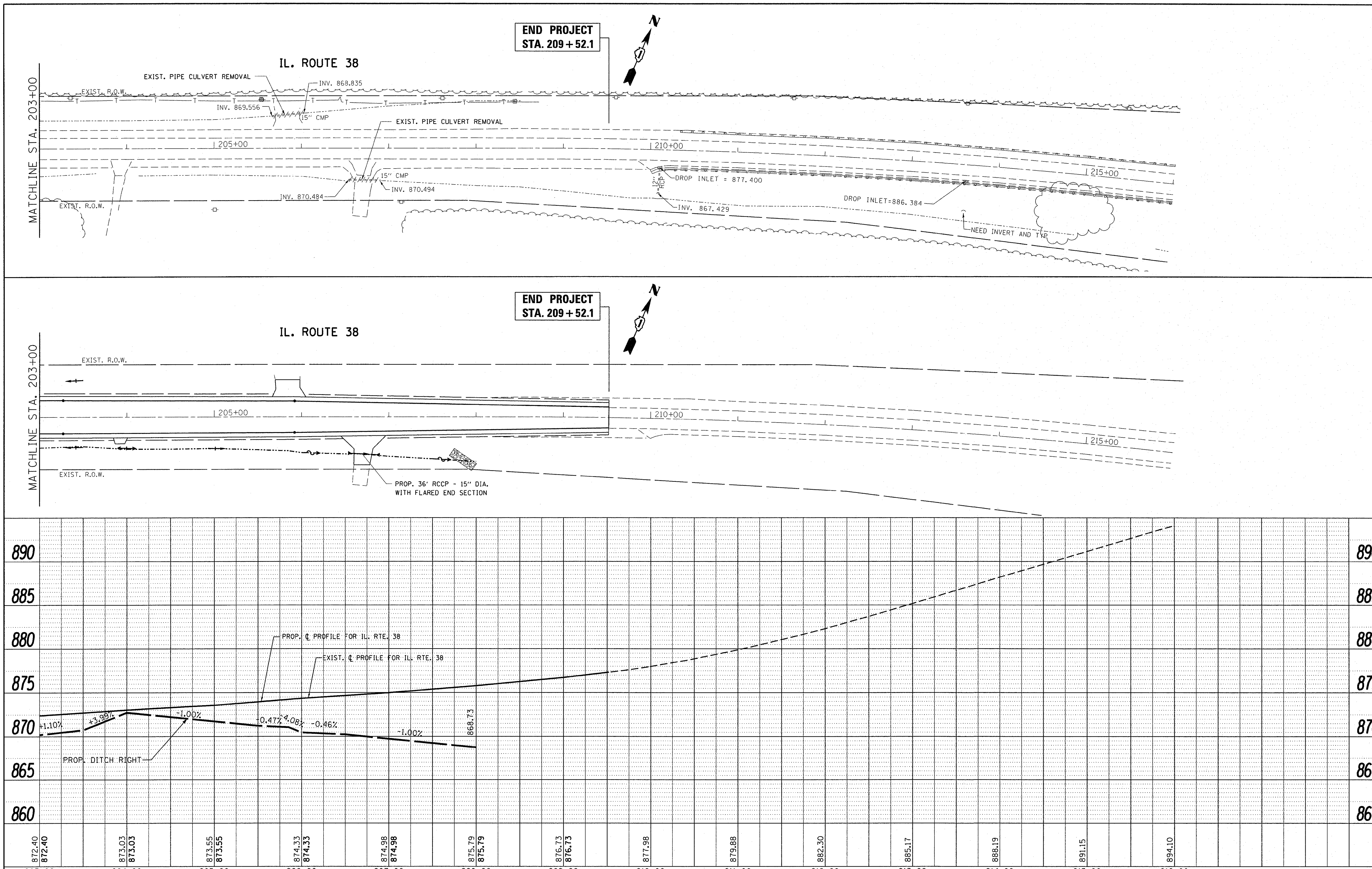
DRAINAGE & UTILITY PLAN & PROFILE
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	27
		CONTRACT NO. 60K65		
	ILLINOIS	FED. AID PROJECT		

PLAN: SURVEYED, PLOTTED, ALIGNMENT CHECKED, GRADES CHECKED, CADD FILE NAME, NOTE BOOK, NO., BY, DATE

PROFILE: SURVEYED, PLOTTED, GRADES CHECKED, CADD FILE NAME, NOTE BOOK, NO., STRUCTURE NOTATIONS CHKD, BY, DATE



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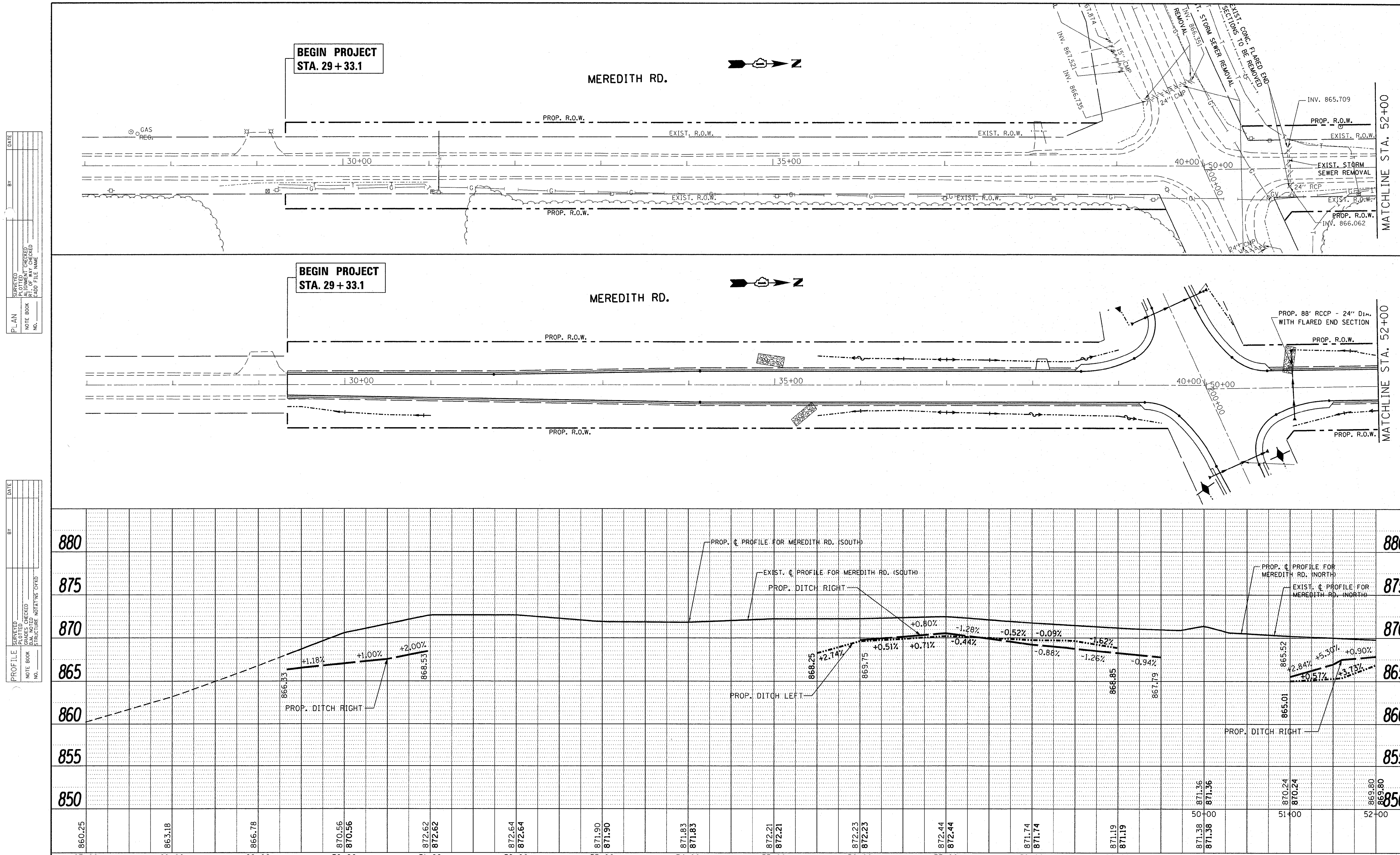
	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE & UTILITY PLAN & PROFILE
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	29
		CONTRACT NO. 60K65		
		ILLINOIS FED. AID PROJECT		



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	PLOT DATE = 2/22/2011	CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

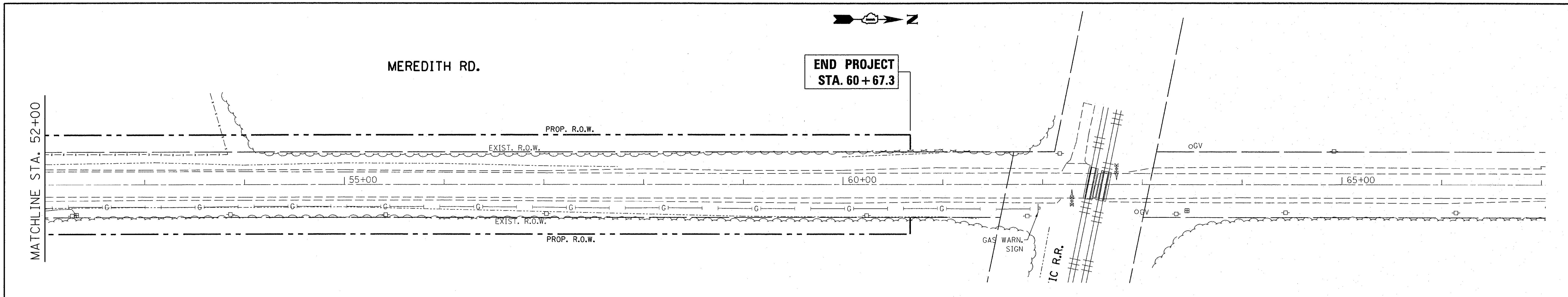
DRAINAGE & UTILITY PLAN & PROFILE
IL. RTE. 38 AT MEREDITH RD.

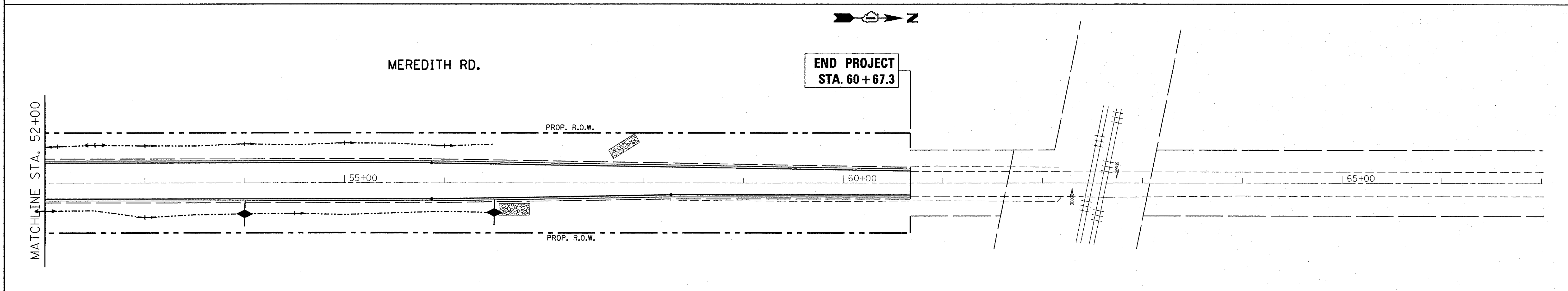
SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

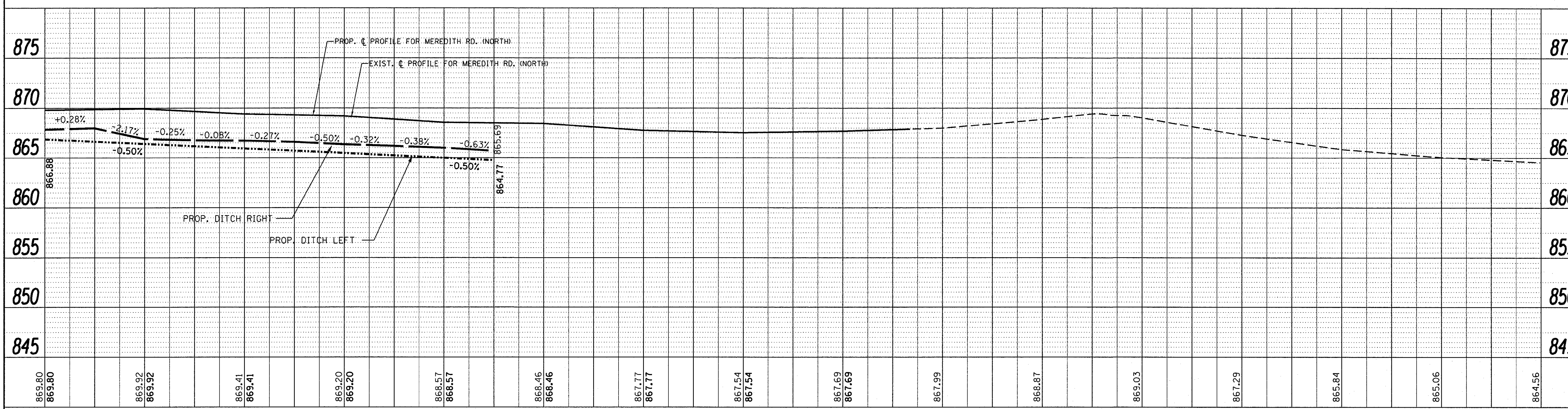
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	29

CONTRACT NO. 60K65

ILLINOIS FED. AID PROJECT







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	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE & UTILITY PLAN & PROFILE
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	30

CONTRACT NO. 60K65

ILLINOIS FED. AID PROJECT

PLAN	
NOTE BOOK NO.	SURVEYED / PLOTTED / ALIGNMENT CHECKED / RT. OF WAY CHECKED / CADD FILE NAME
BY	DATE

PROFILE	
NOTE BOOK NO.	SURVEYED / PLOTTED / GRADES CHECKED / B.M. NOTED / STRUCTURE NOTATN'S CHKD
BY	DATE

PART OF THE NORTH FRACTIONAL 1/2 OF SEC. 3, TWP. 39 N., R. 6 E. AND PART OF THE SOUTH 1/2 OF SEC. 34, TWP. 40 N., R. 6 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1HB0001	957184	August 25, 1961
1HB0002	279728	September 1, 1926
1HB0002	62-594	*February 19, 1963
----	62-594	*February 19, 1963

*Date Filed

Schedule of Ties		
Point Number □	Tie to point	Tie Distance (feet)
1	T1	30.00
	T2	25.89
	T3	28.27
2	T1	18.49
	T2	7.85
	T3	9.42
3	T1	21.64
	T2	17.73
	T3	20.82
4	T1	16.25
	T2	11.82
	T3	14.23
5	T1	15.59
	T2	12.37
	T3	21.46
6	T1	33.53
	T2	30.51
	T3	34.59

See Sheet 4 for Total Holdings Parcel 1HB0001

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
37+72.70	1.01' Rt.	1,904,342.857	928,873.732
37+73.25	17.61' Rt.	1,904,343.170	928,890.339
37+73.70	31.01' Rt.	1,904,343.421	928,903.743
38+00.12	17.12' Rt.	1,904,370.048	928,890.239
38+00.14	28.98' Lt.	1,904,370.736	928,844.145
38+76.04	50.00' Lt.	1,904,446.931	928,824.228
38+87.85	31.06' Rt.	1,904,457.565	928,905.448
50+73.52	31.23' Lt.	1,904,645.373	928,848.650
196+37.34	95.23' Lt.	1,904,527.335	928,510.034
198+04.11	60.32' Rt.	1,904,436.565	928,718.183
198+94.69	0.34' Rt.	1,904,527.292	928,779.628
199+04.14	85.33' Rt.	1,904,453.335	928,822.554
200+05.77	0.36' Rt.	1,904,572.083	928,881.286
200+14.73	0.36' Rt.	1,904,575.692	928,889.479
200+21.71	60.36' Rt.	1,904,523.606	928,920.071
200+27.52	60.36' Rt.	1,904,525.947	928,925.384
200+64.81	59.64' Lt.	1,904,650.788	928,911.108

LEGEND

- SECTION CORNER (9|10 / 16|15)
- QUARTER SECTION CORNER (16 — 15)
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- ℄ PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 120.32' / 129.32'(Comp.) / ( ) MEASURED DIMENSION / COMPUTED DIMENSION / RECORD DATA
- EXISTING BUILDING

0 40' 80' N

SCALE: 1"=40'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- ○ IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- \+ CUT CROSS FOUND OR SET
- ● 5/8" REBAR SET
- ● T1 T2 T3 ***THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.*** SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ● BT1 BT2 BT3 ***THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.*** BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ■ ***STAKING OF PROPOSED RIGHT OF WAY.*** SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- ■ M ***STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.*** BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

SEE SHEET 3

See Sheet 3 for Parcel 1HB0002

See Sheet 3 for Parcel 1HB0004

See Sheet 5 for Total Holdings Parcel 1HB0003

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1HB0001	Itasca Bank & Trust Co., as Trustee under Trust Agreement dated January 31, 2000 known as Trust No. 11779	38.389	1.041	0.608	37.348	N/A	N/A	07-34-300-012 10-03-100-007	
1HB0003	Gordon Stade as Trustee under a Trust Agreement dated November 15, 2005 known as the Stade Farm Trust #11-05	264.309	1.017	0.579	263.292	N/A	N/A	07-34-400-004(pt) 07-35-300-005 07-35-300-020 10-03-200-001 10-02-100-001 10-02-100-007	

Measured ℄ R.O.W. Illinois Route 38 Curve Data

Δ = 22°38'01"
R = 2404.19'
T = 481.14'
L = 949.73'
E = 47.67'

Record ℄ R.O.W. Illinois Route 38 Curve Data

Δ = 22°40'
R = 2404.19'
T = 481.86'
L = 951.05'
E = 47.80'

Existing & Proposed ℄ Pavement Illinois Route 38 Curve #1

P.I. = Sta. 194+26.87
Δ = 22°37'22"
R = 2399.39'
T = 479.94'
L = 947.38'
E = 47.53'
P.C. = Sta. 189+46.93
P.T. = Sta. 198+94.31

M-1 "Monument Record"

South 1/4 Corner of Section 34-40-6
N.1,904,343.170-E.928,890.339
North 1/4 Corner of Fractional Section 3-39-6
N.1,904,342.857-E.928,873.732
Recorded June 9, 2010
Document No. 2010K036888

STATE OF ILLINOIS } SS
COUNTY OF LAKE }

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 3, TOWNSHIP 39N., RANGE 6E. AND SECTION 34, TOWNSHIP 40N., RANGE 6E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 9th DAY OF June 2010 A.D.

Christian H. Jorgensen PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2010
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
Note: Surface Coordinates are Shown

CHRISTIAN H. JORGENSEN 2797 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS LAKE VILLA, ILLINOIS

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
29+32.62	0.64' Rt.	1,903,502.878	928,861.186
29+32.64	29.36' Lt.	1,903,503.326	928,831.190
29+32.64	30.64' Rt.	1,903,502.456	928,891.183
29+32.64	50.00' Rt.	1,903,502.176	928,910.543
29+32.64	50.00' Lt.	1,903,503.625	928,810.553
29+32.65	0.64' Rt.	1,903,502.904	928,861.187

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.P. 567 (ILLINOIS ROUTE 38)

SECTION KANE COUNTY
PROJECT JOB NO. R-91-033-09
STATION 29+00 TO STATION 40+00.00 (MEREDITH ROAD)
STATION 50+00.00 TO STATION 51+00 (MEREDITH ROAD)
STATION 196+00 TO STATION 203+00 (ILLINOIS ROUTE 38)
SCALE: 1"=40' SHEET 2 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

RECEIVED JUL 15 2010 PLATS & LEGALS

MADE BY

REVISION DATE REVISION

ROUTE F.A.P. 567 (ILLINOIS ROUTE 38) SECTION COUNTY KANE JOB NO. R-91-033-09

RECORDING: RECORDED ON AS DOCUMENT NO.

PART OF THE SOUTH 1/2 OF SEC. 34, TWP. 40 N., R. 6 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
1HB0002	Red Bird Partners, L.P., a limited partnership	36.126	1.329	0.850	34.797	N/A	N/A	07-34-300-014	
1HB0004	Gordon Stade as Trustee under a Trust Agreement dated November 15, 2005 known as the Stade Farm Trust #11-05	22.431	0.944	0.515	21.487	N/A	N/A	07-34-400-004(pt)	

Measured ℄ R.O.W. Illinois Route 38 Curve Data
- Δ = 22°38'01"
- R = 2404.19'
- T = 481.14'
- L = 949.73'
- E = 47.67'

Record ℄ R.O.W. Illinois Route 38 Curve Data
- Δ = 22°40'
- R = 2404.19'
- T = 481.86'
- L = 951.05'
- E = 47.80'

Existing & Proposed ℄ Pavement Illinois Route 38 Curve #1
- P.I. = Sta. 194+26.87
- Δ = 22°37'22"
- R = 2399.39'
- T = 479.94'
- L = 947.38'
- E = 47.53'
- P.C. = Sta. 189+46.93
- P.T. = Sta. 198+94.31

EXISTING R.O.W. RECORDED INFORMATION		
Parcel	Document No.	Date Recorded
1HB0004	62-594	*February 19, 1963
-----	279728	September 1, 1926
-----	957184	August 25, 1961
-----	62-594	*February 19, 1963

*Date Filed

See Sheet 4 for Total Holdings Parcel 1HB0002

See Sheet 4 for Total Holdings Parcel 1HB0004

See Sheet 2 for Parcel 1HB0001

See Sheet 2 for Parcel 1HB0003

SEE SHEET 2

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
50+69.22	9.35' Rt.	1,904,641.152	928,889.237
50+73.52	31.23' Lt.	1,904,645.373	928,848.650
50+87.56	9.32' Rt.	1,904,659.498	928,889.169
51+00.59	50.00' Rt.	1,904,672.610	928,929.822
52+15.45	28.79' Lt.	1,904,787.307	928,850.799
60+66.89	7.65' Rt.	1,905,638.821	928,885.547
60+66.93	29.03' Lt.	1,905,638.787	928,848.870
60+66.93	30.97' Rt.	1,905,638.907	928,908.870
60+66.93	50.00' Rt.	1,905,638.945	928,927.897
60+66.93	50.00' Lt.	1,905,638.746	928,827.898
60+66.99	7.65' Rt.	1,905,638.923	928,885.546
196+37.34	95.23' Lt.	1,904,527.335	928,510.034
198+04.11	60.32' Rt.	1,904,436.565	928,718.183
198+94.69	0.34' Rt.	1,904,527.292	928,779.628
199+04.14	85.33' Rt.	1,904,453.335	928,822.554
200+05.77	0.36' Rt.	1,904,572.083	928,881.286
200+14.73	0.36' Rt.	1,904,575.692	928,889.479
200+21.71	60.36' Rt.	1,904,523.606	928,920.071
200+64.81	59.64' Lt.	1,904,650.788	928,911.108
200+97.20	59.63' Lt.	1,904,663.850	928,940.755

Schedule of Ties		
Point Number ☐	Tie to point	Tie Distance (feet)
1	T1	24.48
	T2	16.69
	T3	13.67
2	T1	15.45
	T2	14.40
	T3	22.20
3	BT1	16.54
	BT2	14.77
	BT3	21.31
4	BT1	34.23
	BT2	33.50
	BT3	36.89
5	BT1	38.70
	BT2	35.40
	BT3	36.94
6	BT1	21.18
	BT2	14.71
	BT3	17.44
7	T1	10.53
	T2	7.21
	T3	8.16

LEGEND

SECTION CORNER — QUARTER SECTION CORNER

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- ℄ PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 120.32' MEASURED DIMENSION
- 129.32'(Comp.) COMPUTED DIMENSION
- ( ) RECORD DATA
- EXISTING BUILDING

SCALE: 1"=40'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- ○ IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- \+ CUT CROSS FOUND OR SET
- ● 5/8" REBAR SET
- ● T1, T2, T3 ***THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.*** SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ● BT1, BT2, BT3 ***THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.*** BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ■ ***STAKING OF PROPOSED RIGHT OF WAY.*** SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- ■ M ***STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.*** BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊕ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)SS
COUNTY OF LAKE)

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 34, TOWNSHIP 40N., RANGE 6E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 12TH DAY OF February 2010 A.D.

Christian H. Jorgensen — PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2010
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

CHRISTIAN H. JORGENSEN 2797 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS LAKE VILLA, ILLINOIS

Note: Surface Coordinates are Shown

COORDINATE TABLE			
STATION	OFFSET	NORTH	EAST
37+72.70	1.01' Rt.	1,904,342.857	928,873.732
37+73.25	17.61' Rt.	1,904,343.170	928,890.339
37+73.70	31.01' Rt.	1,904,343.421	928,903.743
38+00.14	28.98' Lt.	1,904,370.736	928,844.145
38+87.85	31.06' Rt.	1,904,457.565	928,905.448
50+67.02	50.00' Lt.	1,904,638.834	928,829.890

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.P. 567 (ILLINOIS ROUTE 38)

SECTION KANE COUNTY
PROJECT JOB NO. R-91-033-09
STATION 37+00 TO STATION 40+00.00 (MEREDITH ROAD)
STATION 50+00.00 TO STATION 61+00 (MEREDITH ROAD)
STATION 196+00 TO STATION 203+00 (ILLINOIS ROUTE 38)
SCALE: 1"=40' SHEET 3 OF 6

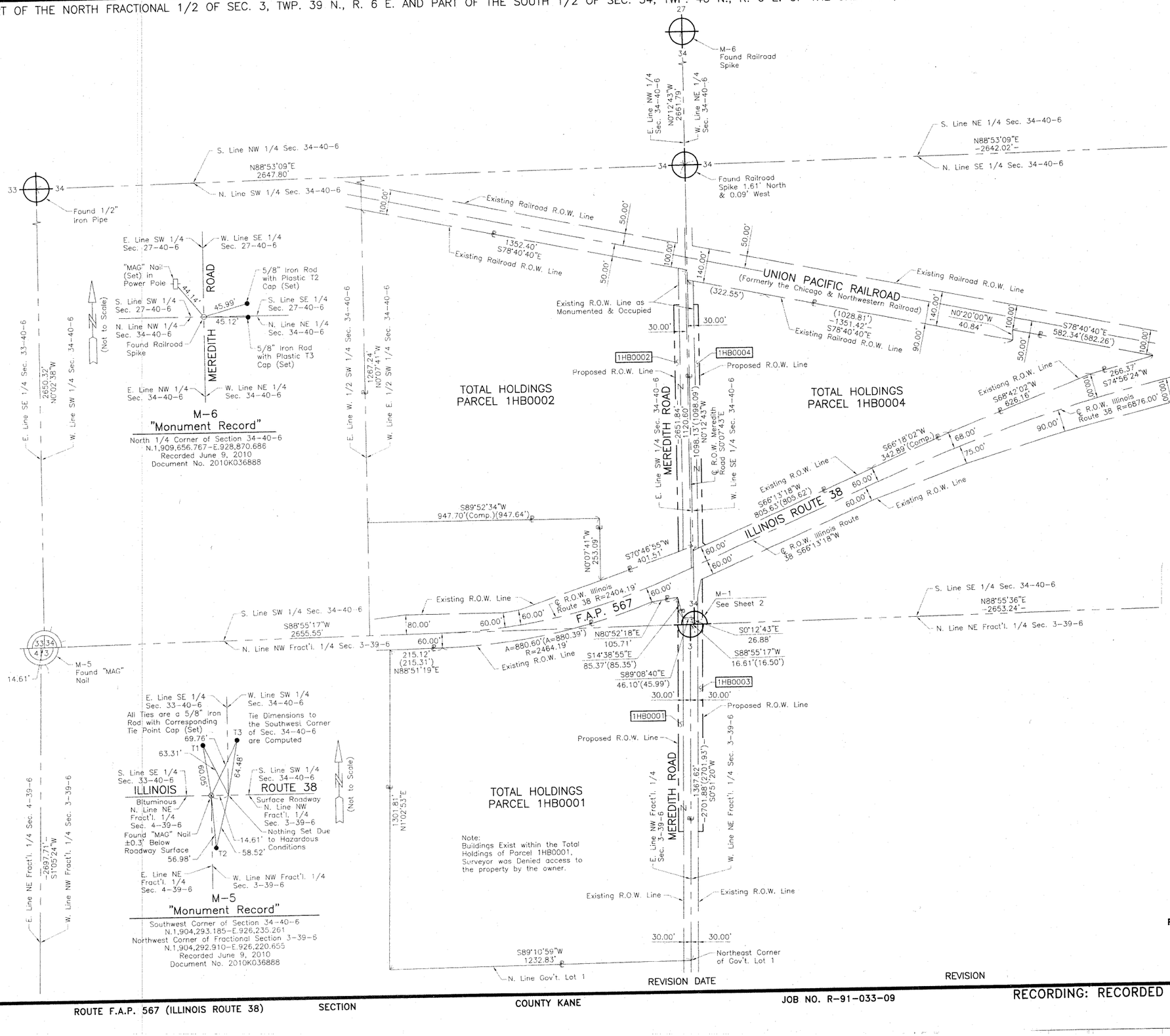
BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

RECEIVED JUL 15 2010 PLL
PLATS & LEGALS

REVISION DATE | REVISION | MADE BY

ROUTE F.A.P. 567 (ILLINOIS ROUTE 38) | SECTION | COUNTY KANE | JOB NO. R-91-033-09 | RECORDING: RECORDED ON | AS DOCUMENT NO.

PART OF THE NORTH FRACTIONAL 1/2 OF SEC. 3, TWP. 39 N., R. 6 E. AND PART OF THE SOUTH 1/2 OF SEC. 34, TWP. 40 N., R. 6 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.



LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APL — APPARENT PROPERTY LINE
- CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 120.32' — MEASURED DIMENSION
- (129.32'(Comp.)) — COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING

N

0 200' 400'

SCALE: 1"=200'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- ○ IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- ⊕ "MAG" NAIL SET
- ● 5/8" REBAR SET
- ● T1 T2 T3 ***THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.*** SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ● BT1 BT2 BT3 ***THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.*** BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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- ◓ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- □ RIGHT OF WAY STAKING PROPOSED TO BE SET.

STATE OF ILLINOIS)
)SS
COUNTY OF LAKE)

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 3, TOWNSHIP 39N., RANGE 6E. AND SECTION 34, TOWNSHIP 40N., RANGE 6E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 9th DAY OF June 2010 A.D.

CHRISTIAN H. JORGENSEN 2797 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS LAKE VILLA, ILLINOIS

Christian H. Jorgensen PRESIDENT

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2010
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
Note: Surface Coordinates are Shown

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.P. 567 (ILLINOIS ROUTE 38)

SECTION KANE COUNTY
PROJECT JOB NO. R-91-033-09
STATION NONE TO STATION
SCALE: 1"=200' SHEET 4 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

RECEIVED
JUL 15 2010
PLATS & LEGALS

MADE BY

REVISION

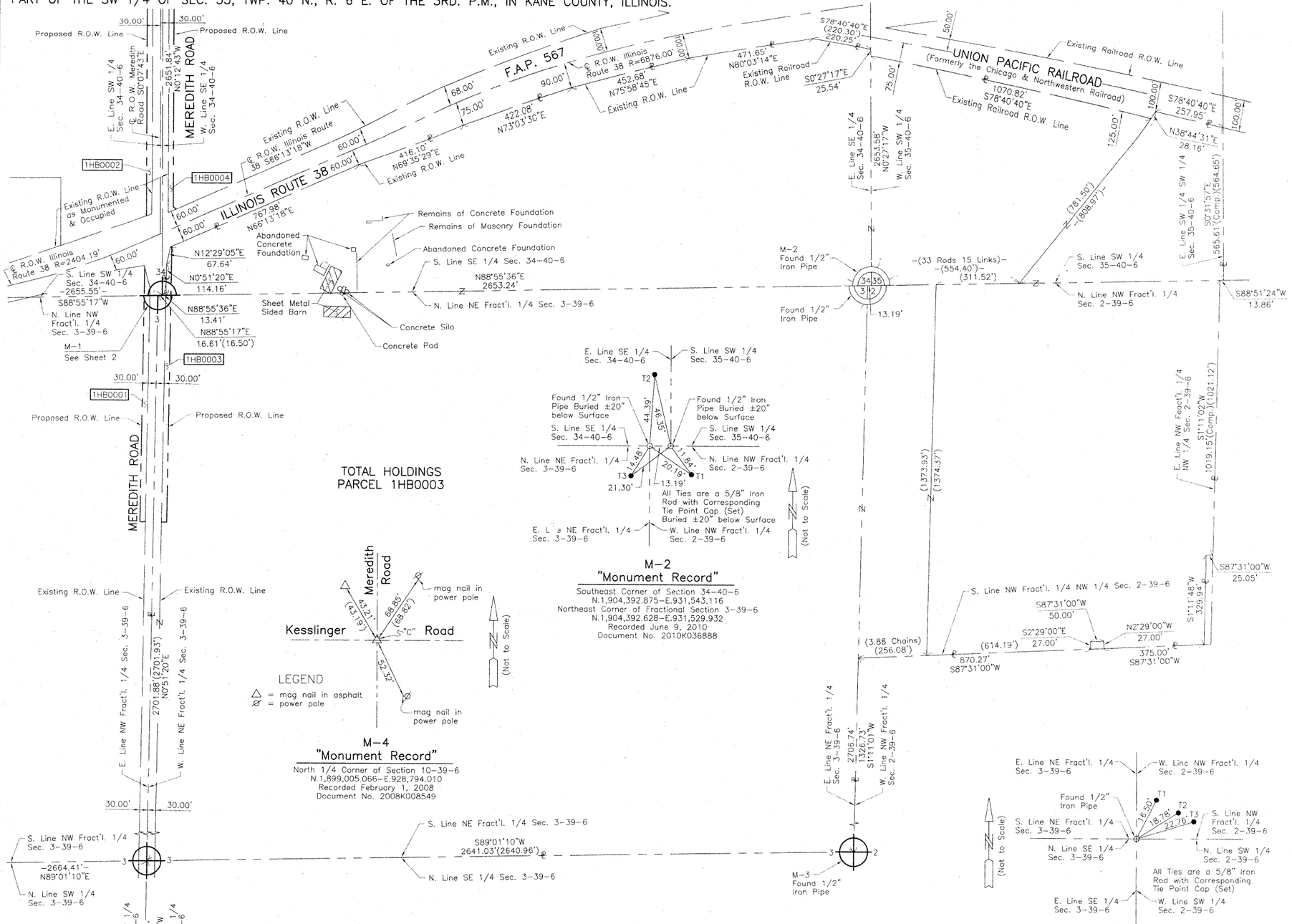
REVISION DATE

RECORDING: RECORDED ON

AS DOCUMENT NO.

ROUTE F.A.P. 567 (ILLINOIS ROUTE 38) SECTION COUNTY KANE JOB NO. R-91-033-09

PART OF THE NW FRACTIONAL 1/4 OF SEC. 2 AND PART OF THE NORTH FRACTIONAL 1/2 OF SEC. 3, TWP. 39 N., R. 6 E. AND PART OF THE SOUTH 1/2 OF SEC. 34 AND PART OF THE SW 1/4 OF SEC. 35, TWP. 40 N., R. 6 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.



M-2
"Monument Record"

Southeast Corner of Section 34-40-6
N.1,904,392.875-E.931,543.116
Northeast Corner of Fractional Section 3-39-6
N.1,904,392.628-E.931,529.932
Recorded June 9, 2010
Document No. 2010K036888

M-4
"Monument Record"

North 1/4 Corner of Section 10-39-6
N.1,899,005.066-E.928,794.010
Recorded February 1, 2008
Document No. 2008K008549

M-3
"Monument Record"

East 1/4 Corner of Section 3-39-6
N.1,901,686.470-E.931,474.025
Recorded June 9, 2010
Document No. 2010K036887

LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- CENTER LINE
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- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- 120.32' MEASURED DIMENSION
- 129.32'(Comp.) COMPUTED DIMENSION
- ( ) RECORD DATA
- EXISTING BUILDING

N

SCALE: 1"=200'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- ○ IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- \+ CUT CROSS FOUND OR SET
- ● 5/8" REBAR SET
- ● T1, T2, T3 ***THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.*** SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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STATE OF ILLINOIS)
)SS
COUNTY OF LAKE)

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 2, TOWNSHIP 39N., RANGE 6E. AND SECTION 34, TOWNSHIP 40N., RANGE 6E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 9th DAY OF June 20 10 A.D.

Christian H. Jorgensen PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2010
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
Note: Surface Coordinates are Shown

CHRISTIAN H. JORGENSEN 2797 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS LAKE VILLA, ILLINOIS

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.P. 567 (ILLINOIS ROUTE 38)

SECTION KANE COUNTY
PROJECT JOB NO. R-91-033-09
STATION NONE TO STATION
SCALE: 1"=200' SHEET 5 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

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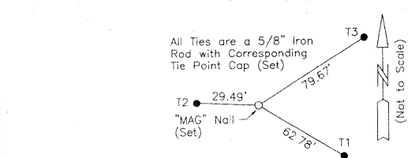
PLATS & LEGALS

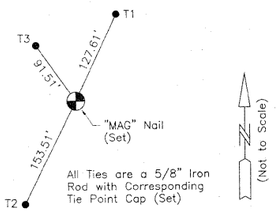
REVISION DATE | REVISION | MADE BY

ROUTE F.A.P. 567 (ILLINOIS ROUTE 38) | SECTION | COUNTY KANE | JOB NO. R-91-033-09 | RECORDING: RECORDED ON | AS DOCUMENT NO.

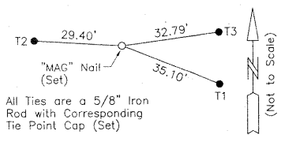
PART OF THE SOUTH 1/2 AND THE NORTH FRACTIONAL 1/2 OF SEC. 3, TWP. 39 N., R. 6 E., PART OF THE SOUTH 1/2 OF SEC. 34, AND PART OF THE SW 1/4 OF SEC. 35, TWP. 40 N., R. 6 E. OF THE 3RD. P.M., IN KANE COUNTY, ILLINOIS.



Existing P.O.T.
Meredith Road
Sta. 81+95.10
N.1,907,767.007–E.928,873.658



Existing & Proposed ℄ Illinois Route 38
Existing & Proposed ℄ Meredith Road
Sta. 200+04.55 ℄ Illinois Route 38
Sta. 50+00.00 ℄ Meredith Road
N.1,904,571.916–E.928,880.022



Existing P.O.T.
Meredith Road
Sta. 10+56.98
N.1,901,627.437–E.928,833.360

Existing P.O.T.
Sta. 81+95.10

MEREDITH ROAD

80+00
60+00
55+00
35+00
30+00
25+00
15+00

3195.10'
N0°06'51"W
Exist. & Prop. ℄ Pavement
Meredith Road N0°49'51"E

Exist. & Prop. ℄ Pavement
Meredith Road N0°49'50"E
2943.02'

Existing & Proposed ℄ Pavement
Illinois Route 38
Curve #1

P.I. = Sta. 194+26.87
△ = 22°37'22"
R = 2399.39'
T = 479.94'
L = 947.38'
E = 47.53'
P.C. = Sta. 189+46.93
P.T. = Sta. 198+94.31

Existing & Proposed ℄ Pavement
Illinois Route 38
Curve #2

P.I. = Sta. 218+21.34
△ = 16°05'45"
R = 6857.54'
T = 969.62'
L = 1926.47'
E = 68.21'
P.C. = Sta. 208+51.72
P.T. = Sta. 227+78.19

ILLINOIS ROUTE 38

Exist. & Prop. P.I. #2
Sta. 218+21.34

Exist. & Prop. P.T. #2
Sta. 227+78.19

Existing P.O.T.
Sta. 244+53.61

220+00
225+00
230+00
1675.42'
N82°18'36"E
Exist. & Prop. ℄ Pavement
Illinois Route 38 N82°18'36"E

215+00
A=1926.47'
R=6857.54'
Exist. & Prop. ℄ Pavement
Illinois Route 38 R=6857.54'

210+00
205+00
Exist. & Prop. P.C. #2
Sta. 208+51.72

Exist. & Prop. ℄ Pavement
Illinois Route 38 N66°12'51"E
957.41'

Sta. 200+00.18 Exist. & Prop. ℄ Illinois Route 38
Sta. 40+00.00 Exist. & Prop. ℄ Meredith Road

Exist. & Prop. P.T. #1
Sta. 198+94.31

Exist. & Prop. ℄ Pavement
Illinois Route 38 R=2399.39'
A=947.38'

195+00 F.A.P. 567

Exist. & Prop. P.C. #1
Sta. 189+46.93

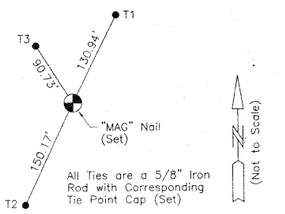
Existing P.O.T.
Sta. 171+49.19

1797.74'
N88°50'13"E
Exist. & Prop. ℄ Pavement
Illinois Route 38 N88°50'13"E

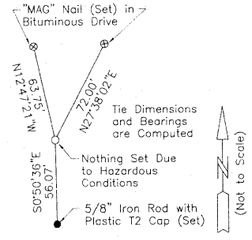
Exist. & Prop. P.I. #1
Sta. 194+26.87

Sta. 200+04.55 Exist. & Prop. ℄ Illinois Route 38
Sta. 50+00.00 Exist. & Prop. ℄ Meredith Road

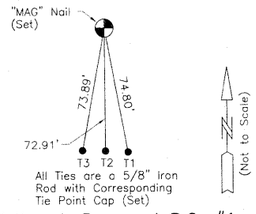
Existing P.O.T.
Sta. 10+56.98



Existing & Proposed ℄ Illinois Route 38
Existing & Proposed ℄ Meredith Road
Sta. 200+00.18 ℄ Illinois Route 38
Sta. 40+00.00 ℄ Meredith Road
N.1,904,570.152–E.928,875.020



Existing P.O.T.
Illinois Route 38
Sta. 171+49.19
N.1,904,287.649–E.926,062.759



Existing & Proposed P.C. #1
Illinois Route 38
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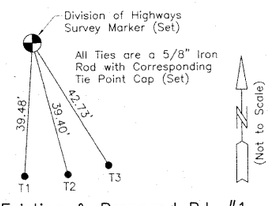
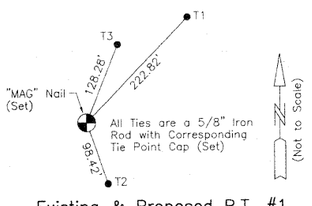
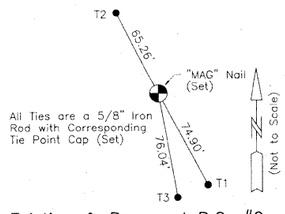
N.1,904,324.141–E.927,860.128



Existing & Proposed P.I. #1
Illinois Route 38
Sta. 194+26.87
N.1,904,333.883–E.928,339.969



Existing & Proposed P.T. #1
Illinois Route 38
Sta. 198+94.31
N.1,904,527.452–E.928,779.142



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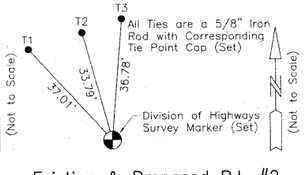
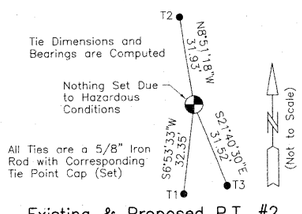
Illinois Route 38
Sta. 208+51.72
N.1,904,913.594–E.929,655.230



Existing & Proposed P.I. #2
Illinois Route 38
Sta. 218+21.34
N.1,905,304.660–E.930,542.490



Existing & Proposed P.T. #2
Illinois Route 38
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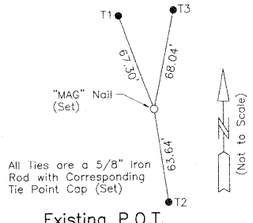


Existing P.O.T.
Illinois Route 38
Sta. 244+53.61
N.1,905,658.596–E.933,163.747

LEGEND

9 10 16 15	SECTION CORNER
16 15	QUARTER SECTION CORNER
	SECTION LINE
	QUARTER SECTION LINE
	QUARTER, QUARTER SECTION LINE
	PLATTED LOT LINE
℄	PROPERTY (DEED) LINE
APL	APPARENT PROPERTY LINE
	CENTER LINE
	EXISTING RIGHT OF WAY LINE
	PROPOSED RIGHT OF WAY LINE
	PROPOSED EASEMENT
120.32'	MEASURED DIMENSION
129.32'(Comp.)	COMPUTED DIMENSION
( )	RECORD DATA
	EXISTING BUILDING

N

1

0 200' 400'

SCALE: 1"=200'

Bearings and Coordinates are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

O	IRON PIPE OR ROD FOUND	⊕	"MAG" NAIL SET
+	CUT CROSS FOUND OR SET	●	5/8" REBAR SET
● T1 T2 T3	***THESE STAKES REFERENCE FOUND OR SET MONUMENTATION.*** SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.		
● BT1 BT2 B13	***THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.*** BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.		
■	***STAKING OF PROPOSED RIGHT OF WAY.*** SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.		
■ M	***STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS.*** BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.		
◒	PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)		
□	RIGHT OF WAY STAKING PROPOSED TO BE SET.		

STATE OF ILLINOIS)
)SS
COUNTY OF LAKE)

THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184–2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON BETWEEN SECTION 3, TOWNSHIP 39N., RANGE 6E. AND SECTION 35, TOWNSHIP 40N., RANGE 6E., OF THE THIRD PRINCIPAL MERIDIAN, KANE COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT LAKE VILLA, ILLINOIS THIS 19th DAY OF February 2010 A.D.

CHRISTIAN H. JORGENSEN 2797 PROFESSIONAL LAND SURVEYOR STATE OF ILLINOIS LAKE VILLA, ILLINOIS

Christian H. Jorgensen PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35–2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2010
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
Note: Surface Coordinates are Shown

RECEIVED
JUL 15 2010
PLATS & LEGALS

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356–3371

SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

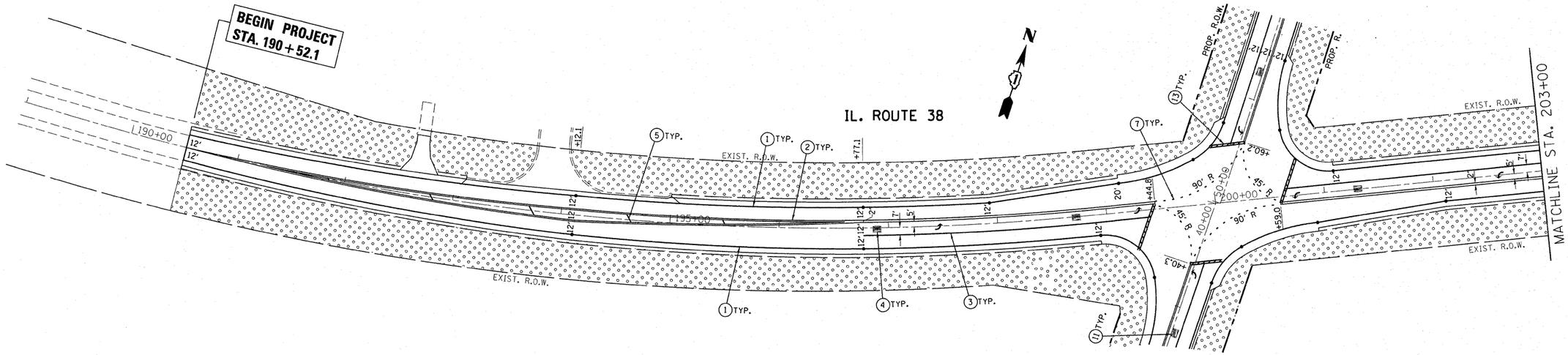
PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
F.A.P. 567 (ILLINOIS ROUTE 38)

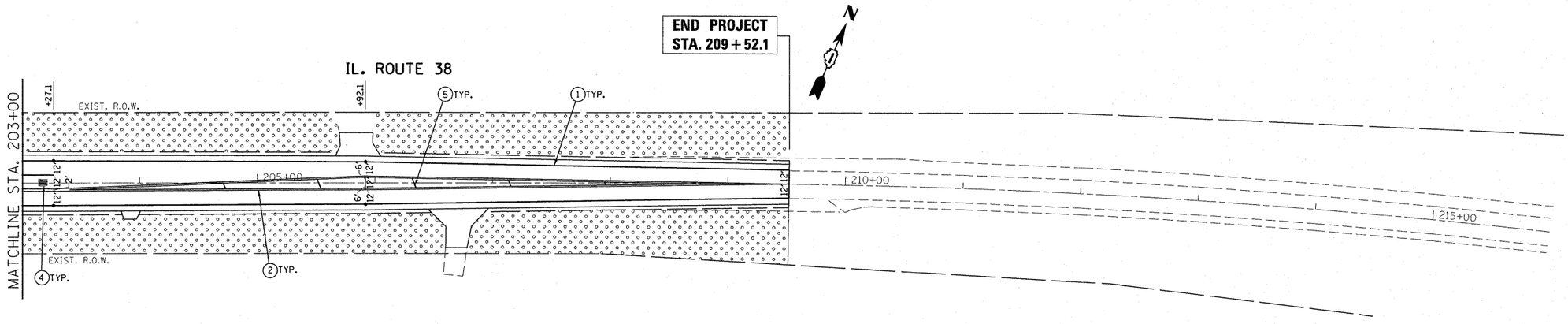
SECTION KANE COUNTY
PROJECT JOB NO. R–91–033–09
STATION 10+56.98 TO STATION 40+00.00 (MEREDITH ROAD)
STATION 50+00.00 TO STATION 81+95.10 (MEREDITH ROAD)
STATION 171+49.19 TO STATION 244+53.61 (ILLINOIS ROUTE 38)
SCALE: 1"=200' SHEET 6 OF 6

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

REVISION DATE | REVISION | MADE BY

ROUTE F.A.P. 567 (ILLINOIS ROUTE 38) | SECTION | COUNTY KANE | JOB NO. R–91–033–09 | RECORDING: RECORDED ON | AS DOCUMENT NO.





NOTES:

1. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL STANDARD TC-13.
2. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)" STANDARD TC-11.
3. THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

THERMOPLASTIC PAVEMENT MARKING LEGEND

1. 4" EDGE LINE - SOLID WHITE
2. 4" DOUBLE YELLOW - SOLID LINE
3. 6" TURN LANE - WHITE
4. LETTERS & SYMBOLS - WHITE
5. 12" DIAGONALS - YELLOW @ 45°
6. 24" STOP BAR - WHITE
7. 4" YELLOW DOTTED EXTENSION (2' LINE - 6' SPACE)

LANDSCAPING NOTES

1. TOPSOIL, 4" SHALL BE PROVIDED IN ALL AREAS OF SEEDING
2. SUPPLEMENTARY WATERING SHALL BE PLACED ON ALL SEEDED AREAS
3. EROSION CONTROL BLANKET SHALL BE PLACED ON ALL SEEDED AREAS

LEGEND

TEMPORARY AND PERMANENT SEEDING, CLASS 2A AREA
APPLY NITROGEN FERTILIZER NUTRIENT, PHOSPHOROUS FERTILIZER NUTRIENT AND POTASSIUM FERTILIZER NUTRIENT TO ALL SEEDING CLASS 2A AREAS.

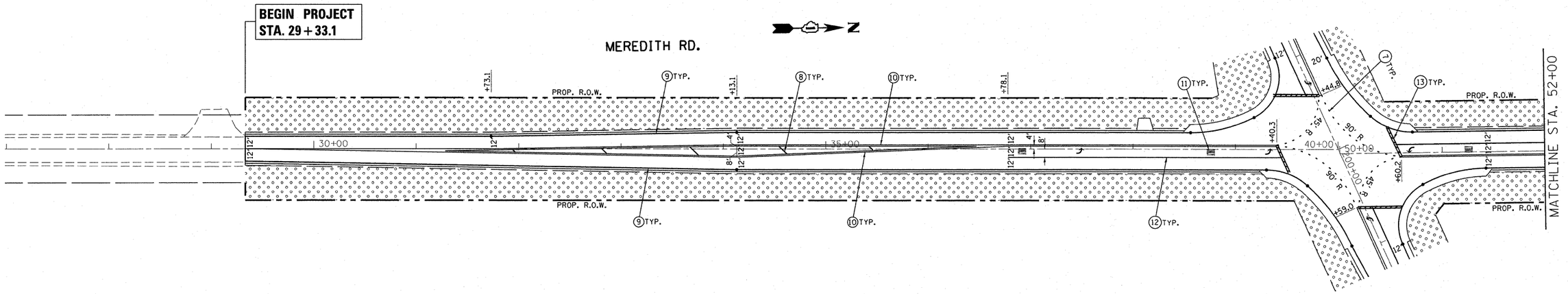
FILE NAME = c:\pw_work\pwidot\abreuch\d0139641\P115308-sht-pmk.dgn	USER NAME = abreuch	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/3/2011	DATE -	REVISED -

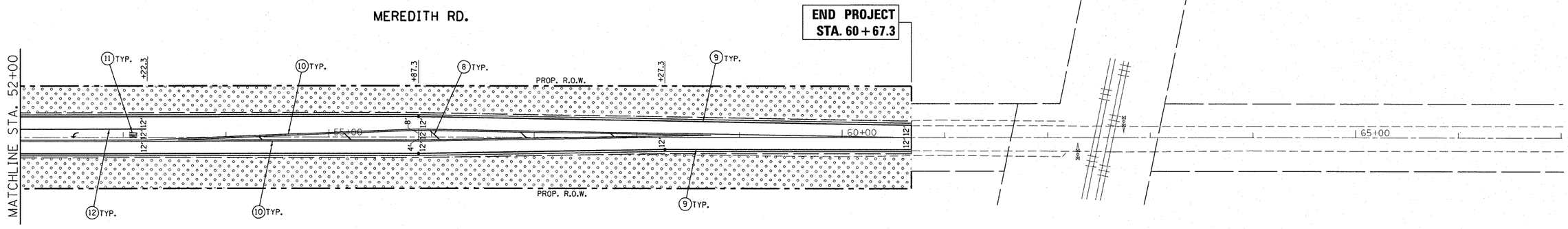
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PAVEMENT MARKING & LANDSCAPING PLAN
IL. RTE. 38 AT MEREDITH RD.

SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	34
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			





NOTES:

1. THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

URETHANE PAVEMENT MARKING LEGEND (MEREDITH RD. ONLY)

(8) 12" DIAGONALS - YELLOW @ 45°
(9) 4" EDGE LINE - SOLID WHITE
(10) 4" DOUBLE YELLOW - SOLID LINE
(11) LETTERS & SYMBOLS - WHITE
(12) 6" TURN LANE - WHITE
(13) 24" STOP BAR - WHITE

LANDSCAPING NOTES

(1) TOPSOIL, 4" SHALL BE PROVIDED IN ALL AREAS OF SEEDING
(2) SUPPLEMENTARY WATERING SHALL BE PLACED ON ALL SEEDED AREAS
(3) EROSION CONTROL BLANKET SHALL BE PLACED ON ALL SEEDED AREAS

LEGEND

TEMPORARY AND PERMANENT SEEDING, CLASS 2A AREA
APPLY NITROGEN FERTILIZER NUTRIENT, PHOSPHOROUS FERTILIZER NUTRIENT AND POTASSIUM FERTILIZER NUTRIENT TO ALL SEEDING CLASS 2A AREAS.

FILE NAME = c:\pw_work\pwidot\abreuah\d0139641\P115308-sht-pmk.dgn	USER NAME = abreuah	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/3/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED PAVEMENT MARKING & LANDSCAPING PLAN
IL. RTE. 38 AT MEREDITH RD.

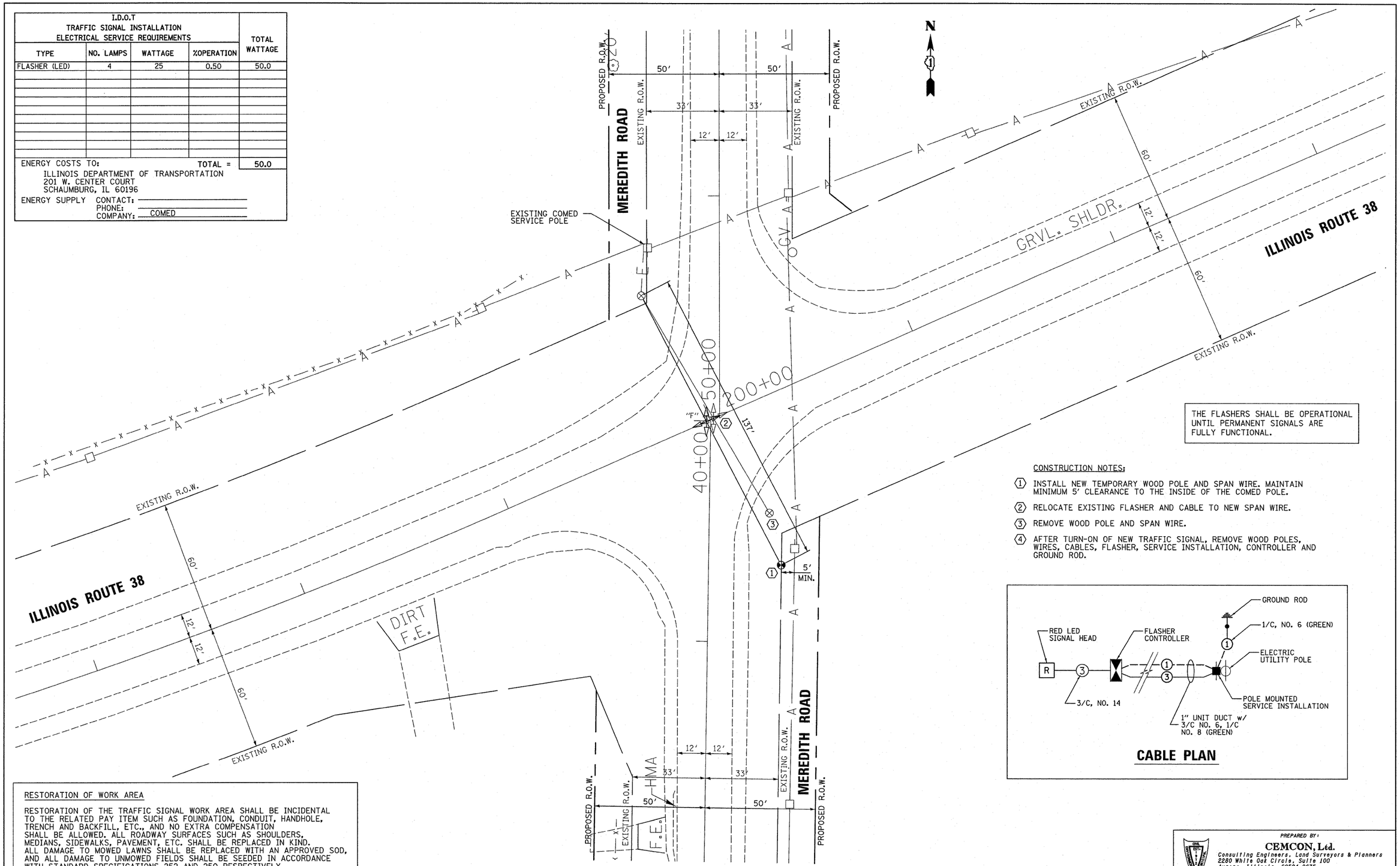
SCALE: 1"=50' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	37
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE	%OPERATION	
FLASHER (LED)	4	25	0.50	50.0
ENERGY COSTS TO:			TOTAL =	50.0

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W. CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY CONTACT: _____
PHONE: _____
COMPANY: COMED



THE FLASHERS SHALL BE OPERATIONAL UNTIL PERMANENT SIGNALS ARE FULLY FUNCTIONAL.

CONSTRUCTION NOTES:

1. INSTALL NEW TEMPORARY WOOD POLE AND SPAN WIRE. MAINTAIN MINIMUM 5' CLEARANCE TO THE INSIDE OF THE COMED POLE.
2. RELOCATE EXISTING FLASHER AND CABLE TO NEW SPAN WIRE.
3. REMOVE WOOD POLE AND SPAN WIRE.
4. AFTER TURN-ON OF NEW TRAFFIC SIGNAL, REMOVE WOOD POLES, WIRES, CABLES, FLASHER, SERVICE INSTALLATION, CONTROLLER AND GROUND ROD.

CABLE PLAN

RESTORATION OF WORK AREA

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

PREPARED BY:
CEMCON, Ltd.
Consulting Engineers, Land Surveyors & Planners
2280 White Oak Circle, Suite 100
Aurora, Illinois 60504-9675
Ph: 630.862.2100 Fax: 630.862.2199
E-Mail: cadd@cemcon.com Website: www.cemcon.com

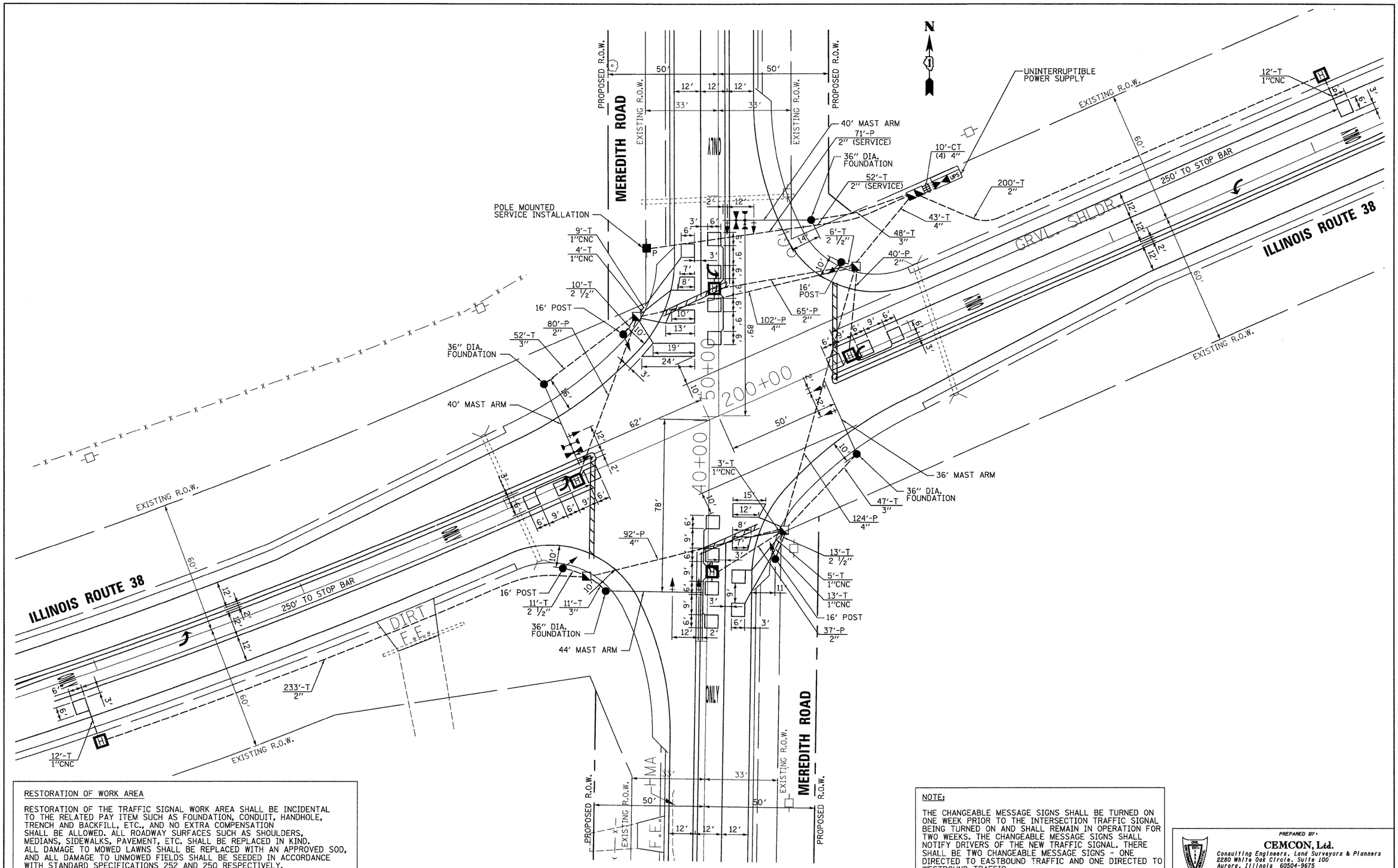
FILE NAME =	USER NAME = RDS	DESIGNED - KK	REVISED -
\MICROST\352092\TEMP RTE 38 SIG.DGN		DRAWN - RDS	REVISED -
	PLOT SCALE = 1'=20'	CHECKED - BPT	REVISED -
	PLOT DATE = 7-XX-10	DATE - 7-XX-10	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF ILLINOIS

EXISTING FLASHER MODIFICATION AND REMOVAL PLAN
ILLINOIS ROUTE 38 AT MEREDITH ROAD

SCALE: N.T.S. | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	38
		CONTRACT NO. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



RESTORATION OF WORK AREA

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE:

THE CHANGEABLE MESSAGE SIGNS SHALL BE TURNED ON ONE WEEK PRIOR TO THE INTERSECTION TRAFFIC SIGNAL BEING TURNED ON AND SHALL REMAIN IN OPERATION FOR TWO WEEKS. THE CHANGEABLE MESSAGE SIGNS SHALL NOTIFY DRIVERS OF THE NEW TRAFFIC SIGNAL. THERE SHALL BE TWO CHANGEABLE MESSAGE SIGNS - ONE DIRECTED TO EASTBOUND TRAFFIC AND ONE DIRECTED TO WESTBOUND TRAFFIC.

PREPARED BY:
CEMCON, Ltd.
Consulting Engineers, Land Surveyors & Planners
2280 White Oak Circle, Suite 100
Aurora, Illinois 60504-9675
Ph: 630.862.2100 Fax: 630.862.2199
E-Mail: cadd@cemcon.com Website: www.cemcon.com

FILE NAME =	USER NAME = RDS	DESIGNED - KK	REVISED -
\MICROST\352892\RTE 38 SIG.DGN		DRAWN - RDS	REVISED -
	PLOT SCALE = 1"=20'	CHECKED - BPT	REVISED -
	PLOT DATE = 1-27-11	DATE - 1-27-11	REVISED -

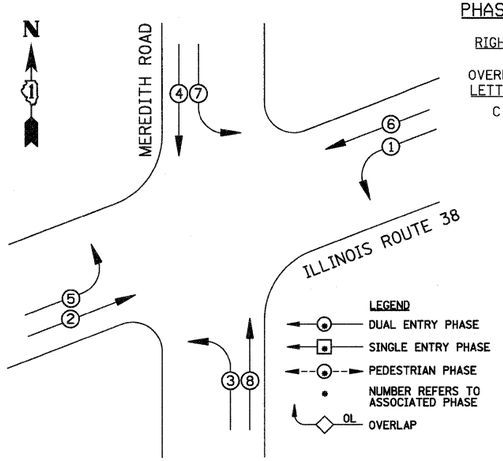
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL INSTALLATION PLAN
ILLINOIS ROUTE 38 AT MEREDITH ROAD

SCALE: N.T.S. | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	39
		CONTRACT NO. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTROLLER SEQUENCE



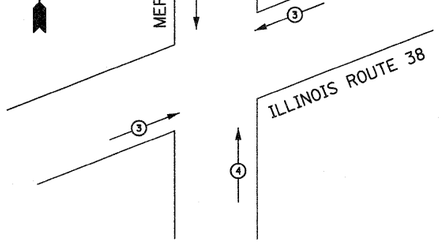
PHASE DESIGNATION DIAGRAM

RIGHT TURN OVERLAP DESIGNATION

OVERLAP LETTER		PERMISSIVE PHASE		PROTECTED PHASE
C	=	6	+	7

PHASE DESIGNATION DIAGRAM

EMERGENCY VEHICLE PREEMPTION SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTOR		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↓ ↑

SCHEDULE OF QUANTITIES

	SIGN PANEL, TYPE 1	SQ FT	32
	CONDUIT IN TRENCH, 2 " DIA., GALVANIZED STEEL	FOOT	485
	CONDUIT IN TRENCH, 2½" DIA., GALVANIZED STEEL	FOOT	40
	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	158
	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	83
	CONDUIT PUSHED, 2 " DIA., GALVANIZED STEEL	FOOT	253
	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	318
	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	766
	MAINTENANCE OF EXISTING FLASHING BEACON INSTALLATION	EACH	1
	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1
•	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	432
	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1082
	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1962
	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	138
	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1977
	HANDHOLE	EACH	4
	HEAVY DUTY HANDHOLE	EACH	6
	DOUBLE HANDHOLE	EACH	1
	SPAN WIRE	FOOT	147
	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4
	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	1
	STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	2
	STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.	EACH	1
	CONCRETE FOUNDATION, TYPE A	FOOT	16
	CONCRETE FOUNDATION, TYPE C	FOOT	4
	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	46
	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	4
	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	4
	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	8
	INDUCTIVE LOOP DETECTOR	EACH	8
	DETECTOR LOOP, TYPE 1	FOOT	772
•	LIGHT DETECTOR	EACH	2
•	LIGHT DETECTOR AMPLIFIER	EACH	1
	RELOCATE EXISTING FLASHING BEACON	EACH	1
	REMOVE EXISTING FLASHING BEACON INSTALLATION COMPLETE	EACH	1
	TEMPORARY WOOD POLE, 45 FEET, CLASS 5	EACH	1
	SERVICE INSTALLATION, POLE MOUNT	EACH	1
	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C	FOOT	658
•	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED & SHIELDED	FOOT	432
	UNINTERRUPTIBLE POWER SUPPLY	EACH	1

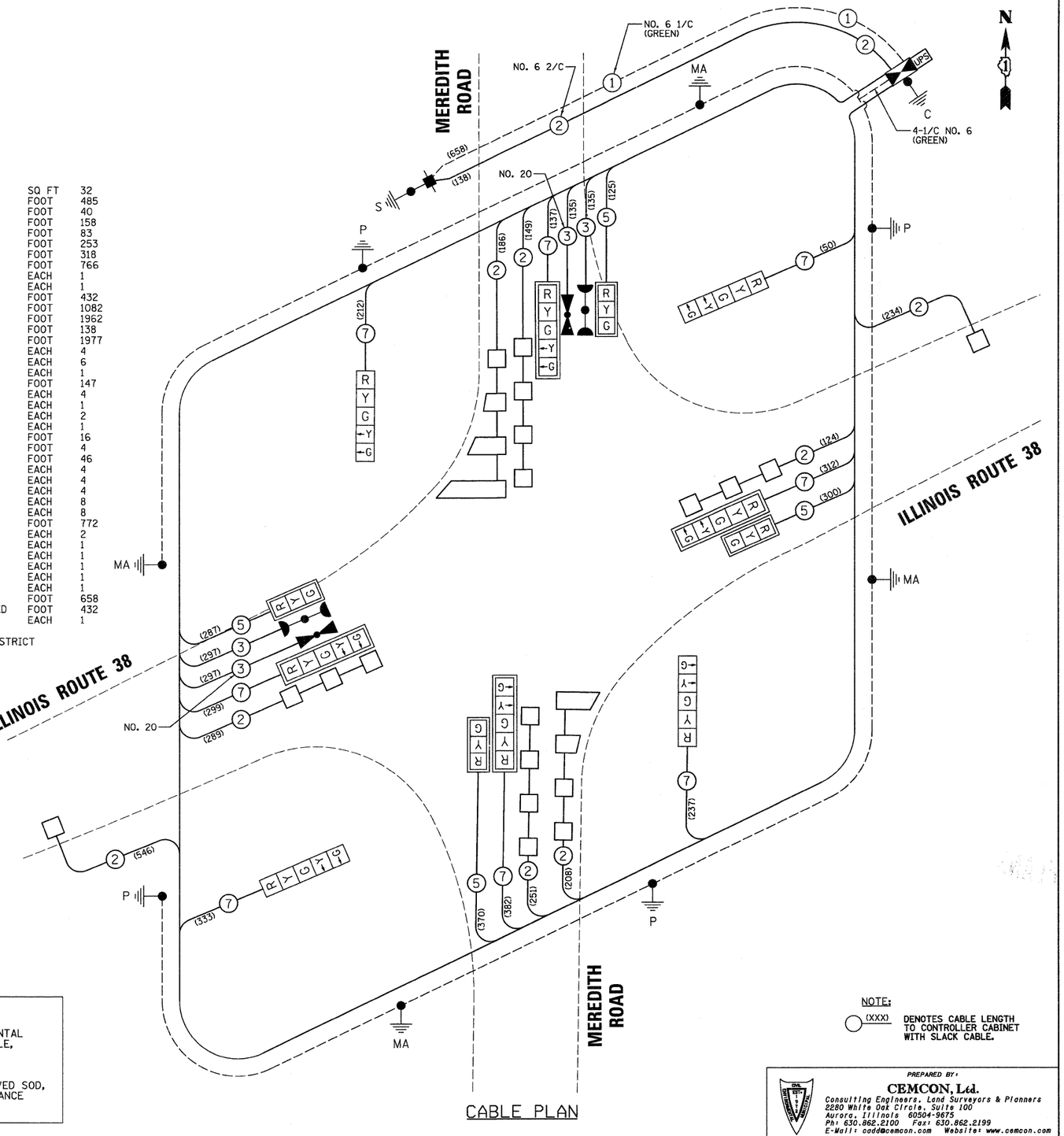
•100% COST TO ELBURN AND COUNTRYSIDE FIRE PROTECTION DISTRICT

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE INCAND.	WATTAGE LED	%OPERATION	
SIGNAL (RED)	12	135	17	0.50	102
(YELLOW)	12	135	25	0.25	75
(GREEN)	12	135	15	0.25	45
ARROW	16	135	12	0.10	19.2
PED. SIGNAL	-	90	25	1.00	200
CONTROLLER	1	100	100	1.00	100
ILLUM. SIGN		84		0.05	
FLASHER				0.50	
ENERGY COSTS TO: ILLINOIS DEPARTMENT OF TRANSPORTATION 201 W. CENTER COURT SCHAUMBURG, IL 60196				TOTAL =	341.2

ENERGY SUPPLY CONTACT: _____
PHONE: _____
COMPANY: COMED

RESTORATION OF WORK AREA

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



CABLE PLAN

NOTE:
(XXX) DENOTES CABLE LENGTH TO CONTROLLER CABINET WITH SLACK CABLE.

PREPARED BY:
CEMCON, Ltd.
Consulting Engineers, Land Surveyors & Planners
2280 White Oak Circle, Suite 100
Aurora, Illinois 60504-9675
Ph: 630.862.2100 Fax: 630.862.2199
E-Mail: cadd@cemcon.com Website: www.cemcon.com

FILE NAME = \MICROST\352892\RTE 38 CAB.DGN	USER NAME = RDS	DESIGNED - KK	REVISED -
		DRAWN - RDS	REVISED -
	PLOT SCALE = 1"=20'	CHECKED - BPT	REVISED -
	PLOT DATE = 1-27-11	DATE - 1-27-11	REVISED -

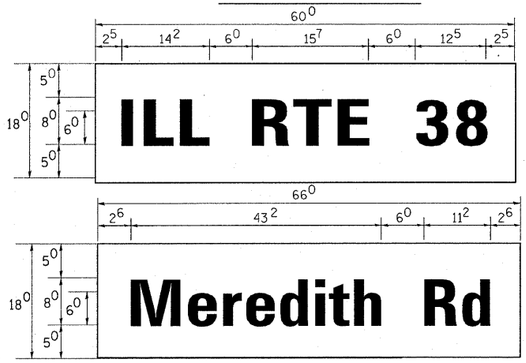
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN
ILLINOIS ROUTE 38 AT MEREDITH ROAD

SCALE: N.T.S. | SHEET NO. OF SHEETS | STA. TO STA.

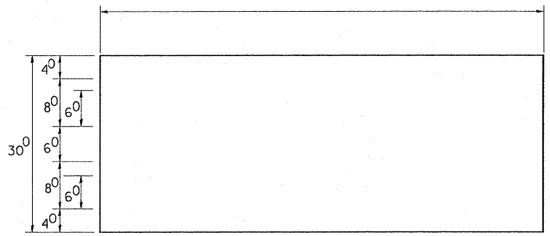
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	40
		CONTRACT NO. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

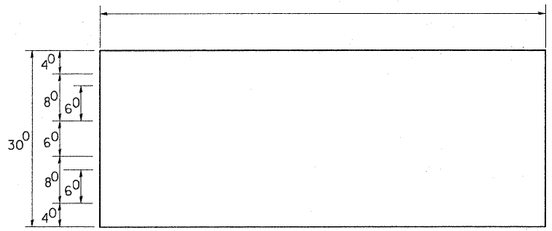
PANEL SIGN DESIGN TYPE 1



NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

PANEL SIGN DESIGN TYPE 2



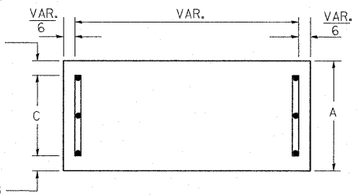


___ Sq. M. each
7.5 Sq. Ft. each
2 Required
Design Series D

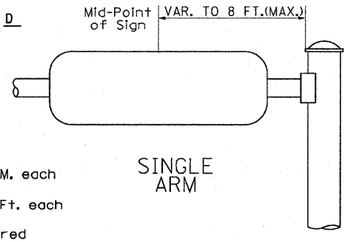
___ Sq. M. each
8.25 Sq. Ft. each
2 Required
Design Series D

___ Sq. M. each
___ Sq. Ft. each
___ Required
Design Series ___

SUPPORTING CHANNELS

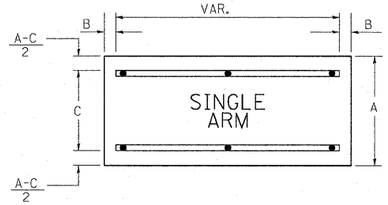


A	B	C
18″	2″	14″

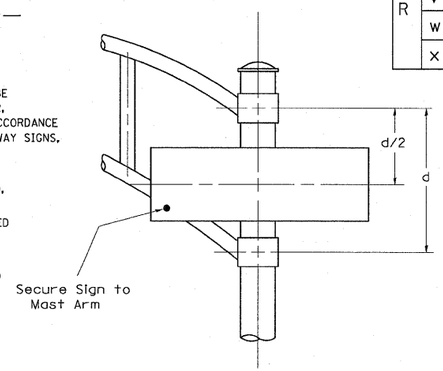


SINGLE ARM

SUPPORTING CHANNELS



A	B	C
18″	2″	12″
30″	2″	22″



DUAL ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM
shall be used. See Note *5.

GENERAL NOTES

1. WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2′-6″ x 8′-0″ MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
2. ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
3. THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8′-0″.
4. ALL BORDERS SHALL BE ¾″ WIDE AND CORNER RADIUS SHALL BE 2-1/4 ″.
5. SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

* J.O. HERBERT CO.
MIDLOTHIAN, VA.

* WESTERN REMAC INC.
WOODRIDGE, IL.

PARTS LISTING:

SIGN CHANNEL	PART *HPN053 (MED. CHANNEL)
SIGN SCREWS	¼″ x 14 x 1″ H.W.H. *3 SELF TAPPING WITH NEOPRENE WASHER
BRACKETS	PART *HPN034 (UNIVERSAL) CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER a c d e g o q		SECOND LETTER b h i k l m n p r u		SECOND LETTER f w		SECOND LETTER j		SECOND LETTER s t		SECOND LETTER v y		SECOND LETTER x		SECOND LETTER z	
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	1^{2}	1^{4}	1^{4}	1^{5}	1^{2}	1^{4}	0^{6}	1^{0}	1^{1}	1^{4}	0^{6}	1^{0}	1^{1}	1^{2}	1^{2}	1^{4}
B	1^{4}	1^{5}	2^{0}	2^{1}	1^{4}	1^{5}	1^{1}	1^{2}	1^{4}	1^{5}	1^{2}	1^{4}	1^{2}	1^{4}	1^{6}	1^{7}
C E G	1^{4}	1^{5}	2^{0}	2^{1}	1^{2}	1^{4}	0^{6}	1^{0}	1^{2}	1^{4}	1^{2}	1^{4}	1^{4}	1^{5}	1^{4}	1^{5}
D O Q R	1^{4}	1^{5}	2^{0}	2^{1}	1^{4}	1^{5}	0^{6}	1^{0}	1^{2}	1^{4}	1^{2}	1^{4}	1^{4}	1^{5}	1^{4}	1^{5}
F	0^{5}	0^{6}	1^{4}	1^{5}	0^{6}	1^{0}	0^{5}	0^{6}	0^{6}	1^{0}	0^{6}	1^{0}	0^{6}	1^{0}	1^{1}	1^{2}
H I M N	2^{0}	2^{1}	2^{2}	2^{4}	2^{0}	2^{1}	1^{4}	1^{5}	1^{6}	1^{7}	1^{6}	1^{7}	2^{0}	2^{1}	2^{0}	2^{1}
J U	2^{0}	2^{1}	2^{0}	2^{1}	1^{6}	1^{7}	1^{4}	1^{5}	1^{6}	1^{7}	1^{6}	1^{7}	1^{6}	1^{7}	2^{0}	2^{1}
K L	1^{1}	1^{2}	1^{6}	1^{7}	1^{1}	1^{2}	0^{5}	0^{6}	1^{1}	1^{2}	1^{1}	1^{2}	1^{1}	1^{2}	1^{2}	1^{4}
P	1^{2}	1^{4}	1^{4}	1^{5}	1^{2}	1^{4}	0^{5}	0^{6}	1^{1}	1^{2}	1^{1}	1^{2}	1^{2}	1^{4}	1^{2}	1^{4}
S	1^{2}	1^{4}	1^{6}	1^{7}	1^{2}	1^{4}	0^{6}	1^{0}	1^{2}	1^{4}	1^{2}	1^{4}	1^{2}	1^{4}	1^{2}	1^{4}
T	1^{1}	1^{2}	1^{6}	1^{7}	0^{6}	1^{0}	0^{6}	1^{0}	1^{1}	1^{2}	1^{1}	1^{2}	1^{1}	1^{2}	1^{2}	1^{4}
V	0^{6}	1^{0}	1^{4}	1^{5}	1^{1}	1^{2}	0^{6}	1^{0}	1^{2}	1^{4}	1^{2}	1^{4}	1^{2}	1^{4}	1^{2}	1^{4}
Y	0^{5}	0^{6}	1^{4}	1^{5}	0^{6}	1^{0}	0^{5}	0^{6}	0^{5}	0^{7}	0^{5}	0^{6}	0^{6}	1^{0}	1^{1}	1^{2}
Z	1^{6}	1^{7}	2^{2}	2^{4}	1^{6}	1^{7}	1^{2}	1^{4}	1^{6}	1^{7}	1^{6}	1^{7}	1^{6}	1^{7}	2^{0}	2^{1}

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

FIRST LETTER	SECOND LETTER a c d e g o q		SECOND LETTER b h i k l m n p r u		SECOND LETTER f w		SECOND LETTER j		SECOND LETTER s t		SECOND LETTER v y		SECOND LETTER x		SECOND LETTER z	
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
a d h g i j l m n q u	1^{6}	1^{7}	2^{2}	2^{4}	1^{6}	1^{7}	1^{2}	1^{4}	1^{4}	1^{5}	1^{4}	1^{5}	1^{6}	1^{7}	1^{6}	1^{7}
b f k o p s	1^{2}	1^{4}	1^{6}	1^{7}	1^{1}	1^{2}	0^{5}	0^{6}	1^{1}	1^{2}	1^{1}	1^{2}	1^{2}	1^{4}	1^{2}	1^{4}
c e	1^{2}	1^{4}	1^{6}	1^{7}	1^{2}	1^{4}	0^{6}	1^{0}	1^{2}	1^{4}	1^{2}	1^{4}	1^{2}	1^{4}	1^{2}	1^{4}
r	0^{6}	1^{0}	1^{2}	1^{4}	0^{6}	1^{0}	0^{3}	0^{3}	0^{5}	0^{6}	0^{5}	0^{6}	0^{6}	1^{0}	0^{6}	1^{0}
t z	1^{2}	1^{4}	1^{6}	1^{7}	1^{2}	1^{4}	0^{6}	1^{0}	1^{1}	1^{2}	1^{1}	1^{2}	1^{2}	1^{4}	1^{2}	1^{4}
v y	1^{1}	1^{2}	1^{4}	1^{5}	1^{1}	1^{2}	0^{5}	0^{6}	0^{6}	1^{0}	0^{6}	1^{0}	1^{1}	1^{2}	1^{1}	1^{2}
w	1^{1}	1^{2}	1^{4}	1^{5}	1^{1}	1^{2}	0^{5}	0^{6}	1^{1}	1^{2}	1^{1}	1^{2}	1^{1}	1^{2}	1^{2}	1^{4}
x	1^{2}	1^{4}	1^{6}	1^{7}	1^{1}	1^{2}	0^{5}	0^{6}	1^{1}	1^{2}	1^{1}	1^{2}	1^{1}	1^{2}	1^{2}	1^{4}

Number To Number
Spacing Chart 8 Inch Series "C & D"

FIRST NUMBER	SECOND NUMBER 0		1		2		3		4		5		6		7		8		9	
SERIES	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	1^{6}	1^{7}	1^{6}	1^{7}	1^{4}	1^{5}	1^{2}	1^{4}	1^{4}	1^{5}	1^{4}	1^{5}	1^{6}	1^{7}	1^{2}	1^{4}	1^{6}	1^{7}	1^{6}	1^{7}
1	2^{0}	2^{1}	2^{0}	2^{1}	2^{0}	2^{1}	1^{6}	1^{7}	1^{4}	1^{5}	2^{0}	2^{1}	2^{0}	2^{1}	1^{4}	1^{5}	2^{0}	2^{1}	2^{0}	2^{1}
2 3 4	1^{4}	1^{5}	1^{4}	1^{5}	1^{4}	1^{5}	1^{2}	1^{4}	1^{2}	1^{4}	1^{4}	1^{5}	1^{4}	1^{5}	1^{1}	1^{2}	1^{6}	1^{7}	1^{4}	1^{5}
5	1^{4}	1^{5}	1^{4}	1^{5}	1^{4}	1^{5}	1^{1}	1^{2}	1^{1}	1^{2}	1^{4}	1^{5}	1^{4}	1^{5}	1^{1}	1^{2}	1^{4}	1^{5}	1^{4}	1^{5}
6	1^{6}	1^{7}	1^{4}	1^{5}	1^{4}	1^{5}	1^{2}	1^{5}	1^{2}	1^{4}	1^{4}	1^{5}	1^{4}	1^{5}	1^{1}	1^{2}	1^{4}	1^{5}	1^{4}	1^{5}
7	1^{2}	1^{4}	1^{2}	1^{4}	1^{4}	1^{5}	1^{2}	1^{5}	0^{5}	0^{6}	1^{2}	1^{4}	1^{4}	1^{5}	1^{1}	1^{2}	1^{4}	1^{5}	1^{2}	1^{4}
8	1^{6}	1^{7}	1^{6}	1^{7}	1^{4}	1^{5}	1^{2}	1^{5}	1^{2}	1^{4}	1^{4}	1^{5}	1^{6}	1^{7}	1^{2}	1^{4}	1^{6}	1^{7}	1^{4}	1^{5}

EXAMPLE, $2^{(3)}$ DENOTES $\frac{3}{8}$″

UPPER AND LOWER CASE
LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS SERIES C	D	8 INCH UPPER CASE LETTERS SERIES C	D	LETTERS	6 INCH LOWER CASE LETTERS SERIES C	D
A	3^{6}	5^{0}	5^{0}	6^{5}	a	3^{5}	4^{2}
B	3^{2}	4^{0}	4^{3}	5^{3}	b	3^{5}	4^{2}
C	3^{2}	4^{0}	4^{3}	5^{3}	c	3^{5}	4^{1}
D	3^{2}	4^{0}	4^{3}	5^{3}	d	3^{5}	4^{2}
E	3^{0}	3^{5}	4^{0}	4^{7}	e	3^{5}	4^{2}
F	3^{0}	3^{5}	4^{0}	4^{7}	f	2^{3}	2^{6}
G	3^{2}	4^{0}	4^{3}	5^{3}	g	3^{5}	4^{2}
H	3^{2}	4^{0}	4^{3}	5^{3}	h	3^{5}	4^{2}
I	0^{7}	0^{7}	1^{1}	1^{2}	i	1^{1}	1^{1}
J	3^{0}	3^{6}	4^{0}	5^{0}	j	2^{0}	2^{2}
K	3^{2}	4^{1}	4^{3}	5^{4}	k	3^{5}	4^{2}
L	3^{0}	3^{5}	4^{0}	4^{7}	l	1^{1}	1^{1}
M	3^{7}	4^{5}	5^{1}	6^{1}	m	6^{0}	7^{0}
N	3^{2}	4^{0}	4^{3}	5^{3}	n	3^{5}	4^{2}
O	3^{4}	4^{2}	4^{5}	5^{5}	o	3^{6}	4^{3}
P	3^{2}	4^{0}	4^{3}	5^{3}	p	3^{5}	4^{2}
Q	3^{4}	4^{2}	4^{5}	5^{5}	q	3^{5}	4^{2}
R	3^{2}	4^{0}	4^{3}	5^{3}	r	2^{6}	3^{2}
S	3^{2}	4^{0}	4^{3}	5^{3}	s	3^{6}	4^{2}
T	3^{0}	3^{5}	4^{0}	4^{7}	t	2^{7}	3^{2}
U	3^{2}	4^{0}	4^{3}	5^{3}	u	3^{5}	4^{2}
V	3^{5}	4^{4}	4^{7}	6^{0}	v	4^{2}	4^{7}
W	4^{4}	5^{2}	6^{0}	7^{0}	w	5^{5}	6^{4}
X	3^{4}	4^{0}	4^{5}	5^{3}	x	4^{4}	5^{1}
Y	3^{6}	5^{0}	5^{0}	6^{6}	y	4^{6}	5^{3}
Z	3^{2}	4^{0}	4^{3}	5^{3}	z	3^{6}	4^{3}

NUMBER	6 INCH SERIES C	D	8 INCH SERIES C	D
1	1^{2}	1^{4}	1^{5}	2^{0}
2	3^{2}	4^{0}	4^{3}	5^{3}
3	3^{2}	4^{0}	4^{3}	5^{3}
4	3^{5}	4^{3}	4^{7}	5^{7}
5	3^{2}	4^{0}	4^{3}	5^{3}
6	3^{2}	4^{0}	4^{3}	5^{3}
7	3^{2}	4^{0}	4^{3}	5^{3}
8	3^{2}	4^{0}	4^{3}	5^{3}
9	3^{2}	4^{0}	4^{3}	5^{3}
0	3^{4}	4^{2}	4^{5}	5^{5}

FILE NAME = c:\pw_work\PWIDOT\KANTHAPHIXAYBC\d0112614\traffic_legend_v7.dgn	USER NAME = kanthaphixaybc	DESIGNED - DAG./BCK.	REVISED - DAG-10/28/09
	PLOT SCALE = 20.0000 ′ / IN.	DRAWN - BCK.	REVISED -
	PLOT DATE = 10/6/2009	CHECKED - DAG/DAD	REVISED -
		DATE - 03/15/09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1
MAST ARM MOUNTED STREET NAME SIGNS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	41
		CONTRACT NO. 60K65		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

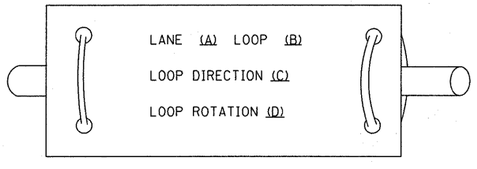
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

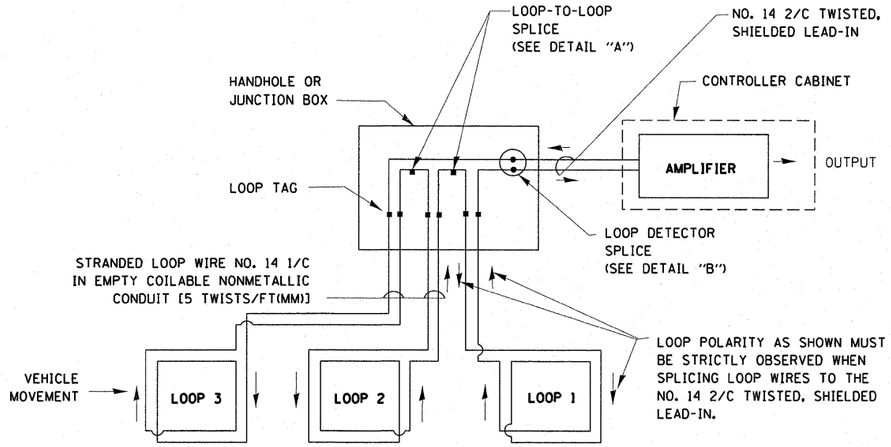


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

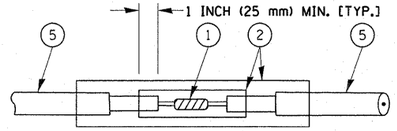
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

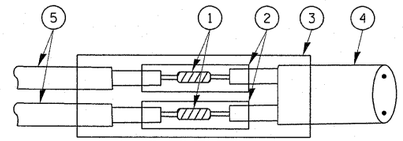


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

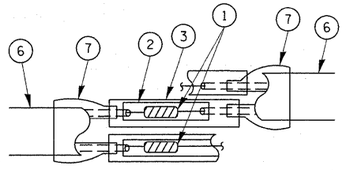


DETAIL "A"
LOOP-TO-LOOP SPLICE



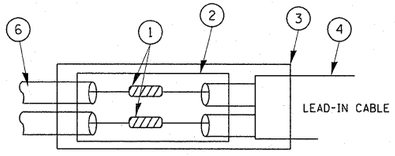
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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	PLOT DATE = 2/3/2011	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

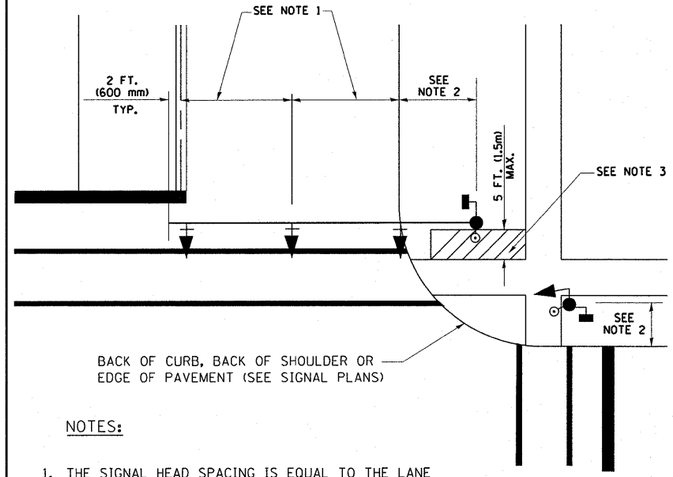
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 1 OF 6 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	42
	TS-05	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

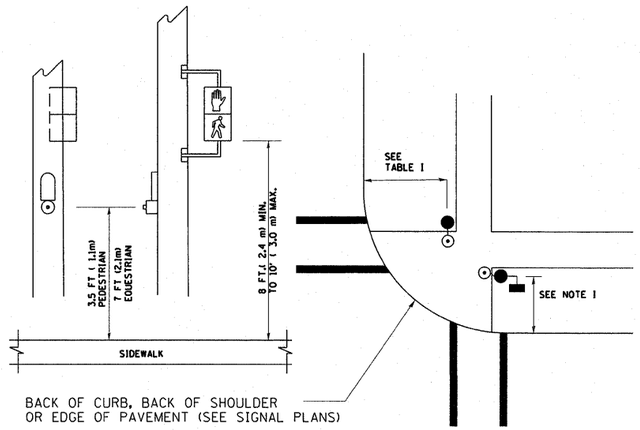
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

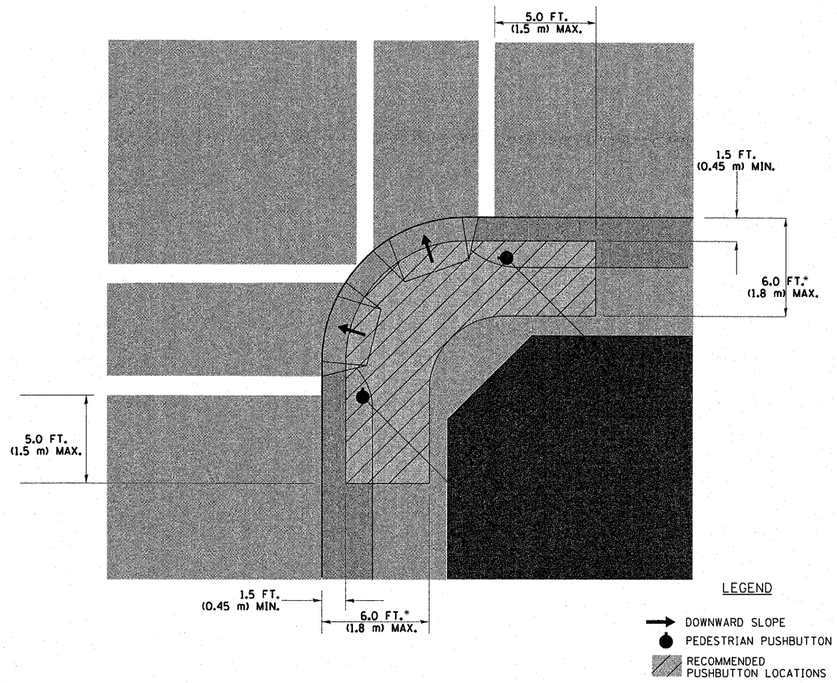
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- •• WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 2 OF 6 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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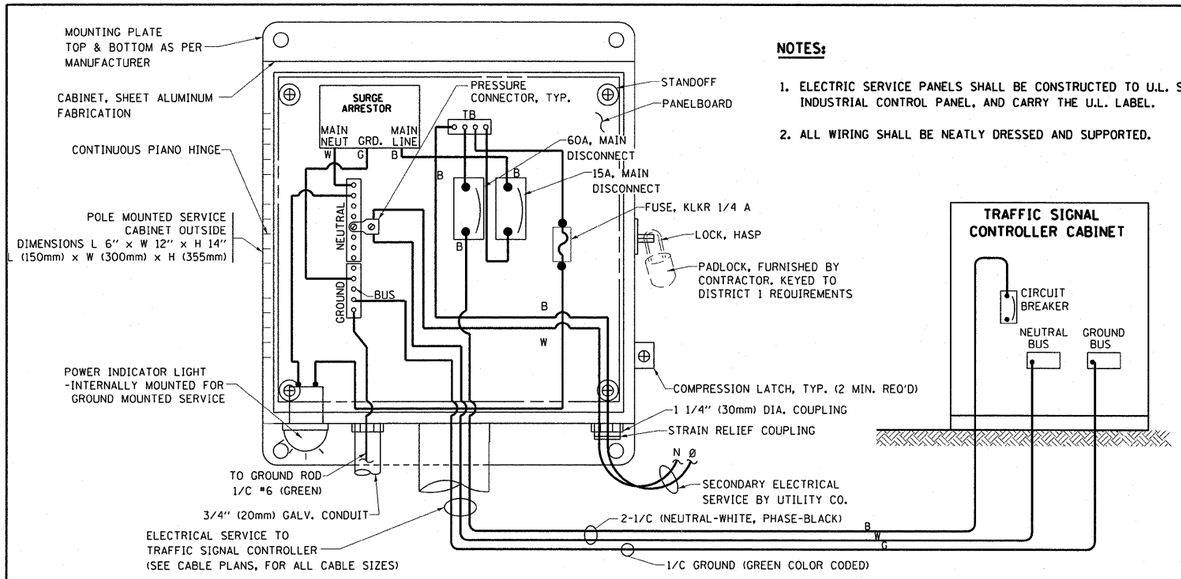
	TS-05	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



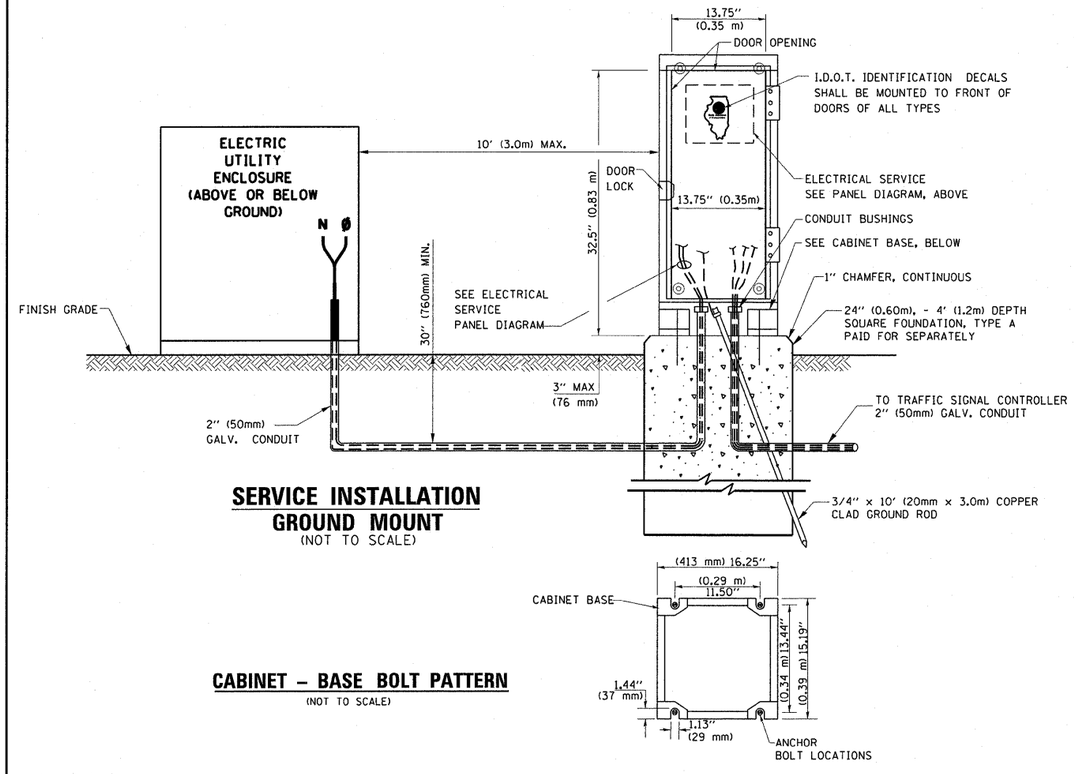
NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)

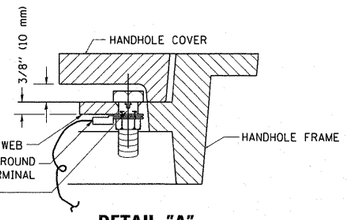


SERVICE INSTALLATION GROUND MOUNT

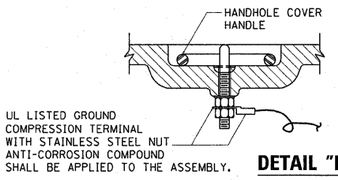
(NOT TO SCALE)

CABINET – BASE BOLT PATTERN

(NOT TO SCALE)



DETAIL "A"



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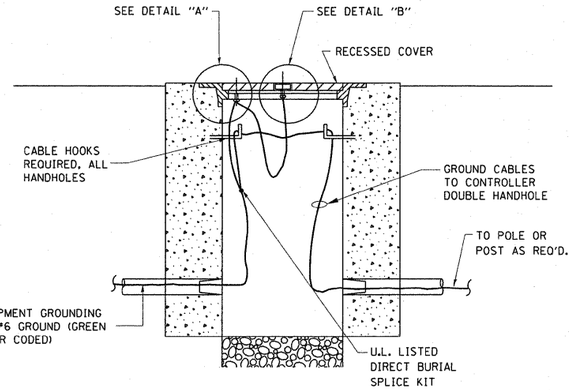
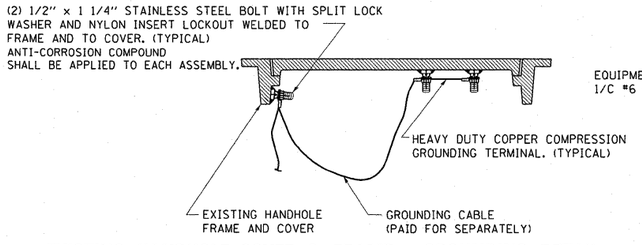


HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



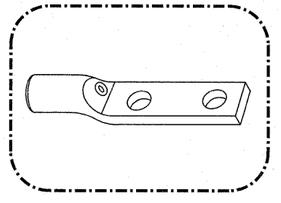
EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)

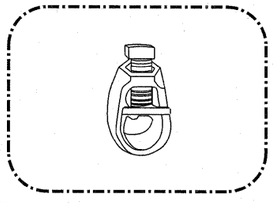
NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



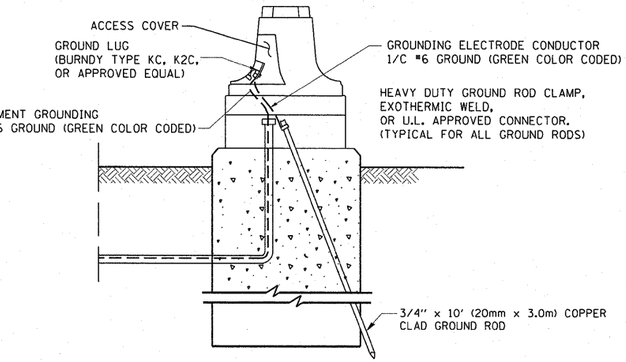
HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST–GROUNDING DETAIL

(NOT TO SCALE)

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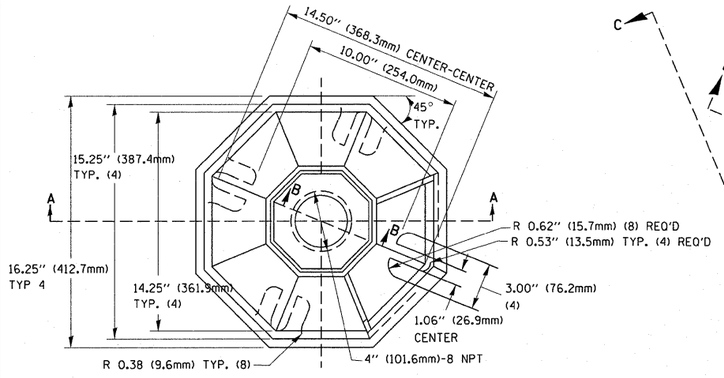
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

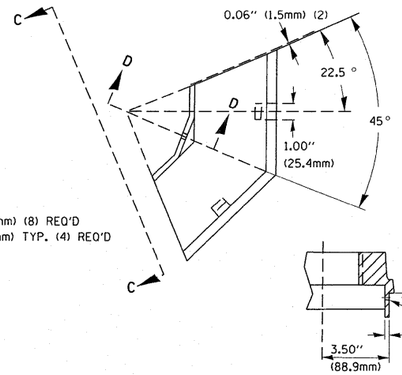
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

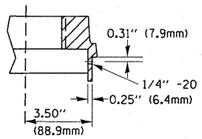
SCALE: NONE | SHEET NO. 3 OF 6 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	44
	TS-05	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

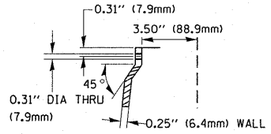


TOP VIEW

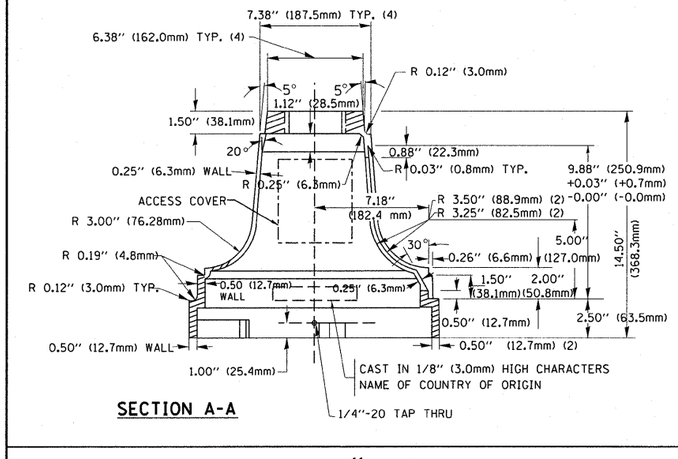




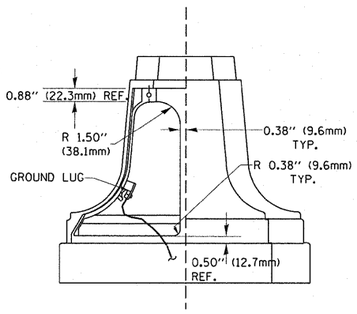
SECTION B-B



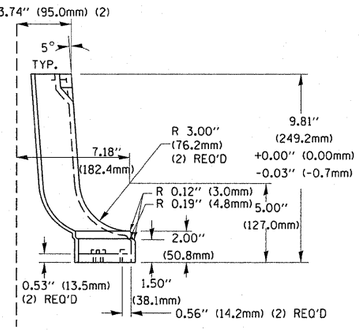
SECTION D-D



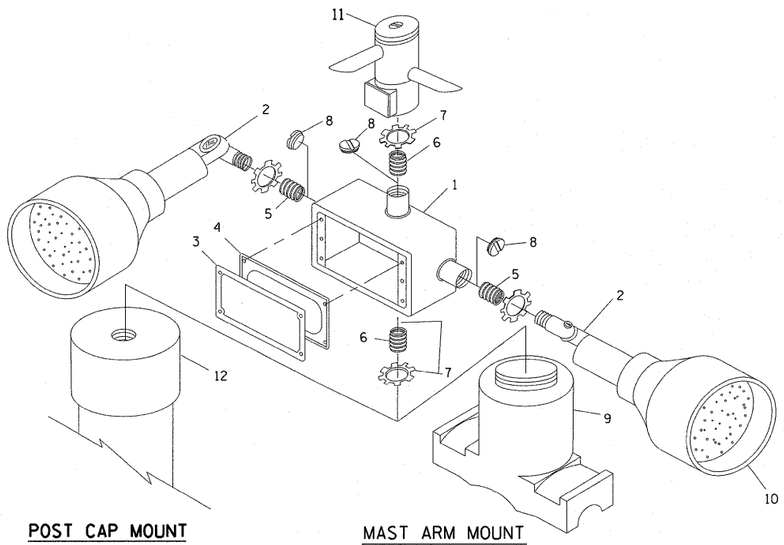
SECTION A-A



VIEW C-C



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



POST CAP MOUNT

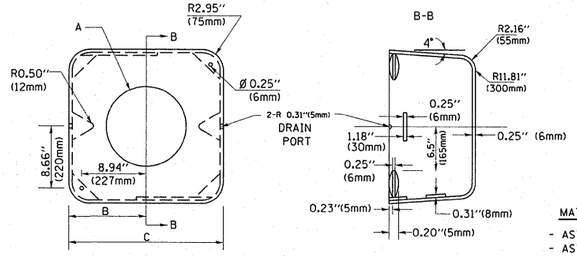
MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A ¾"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

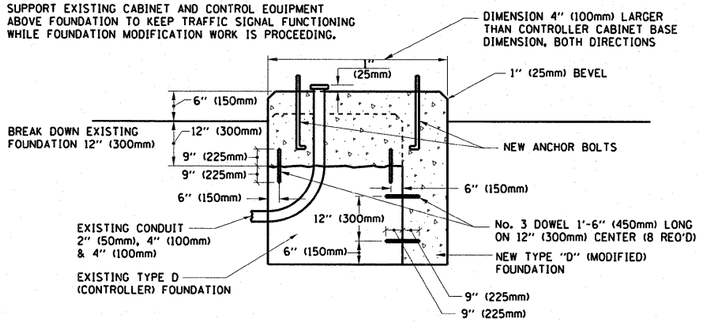
A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

SHROUD

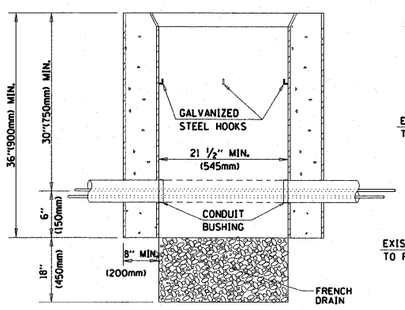
NOTES:

1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



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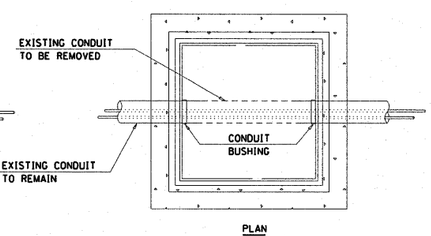

PLAN

NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

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		DRAWN - BCK	REVISED -
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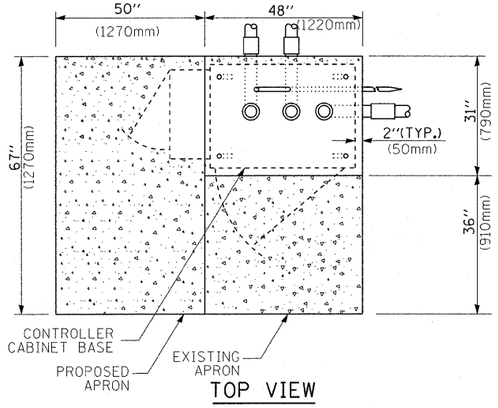
	PLOT DATE = 2/3/2011	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

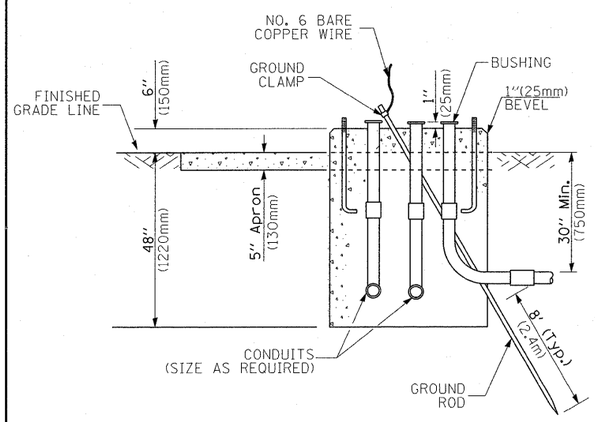
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 4 OF 6 SHEETS | STA. | TO STA.

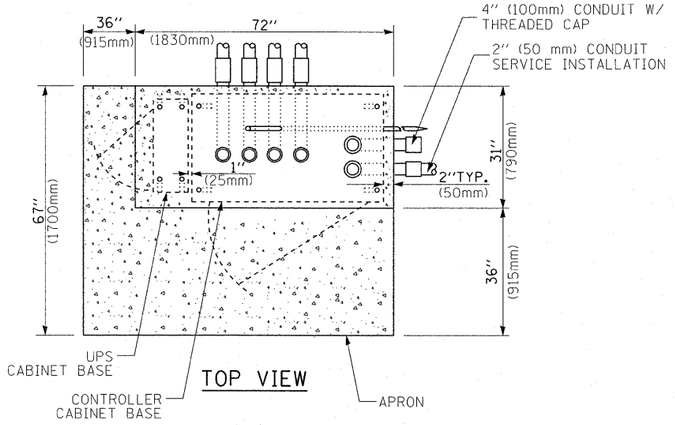
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	45
	TS-05	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



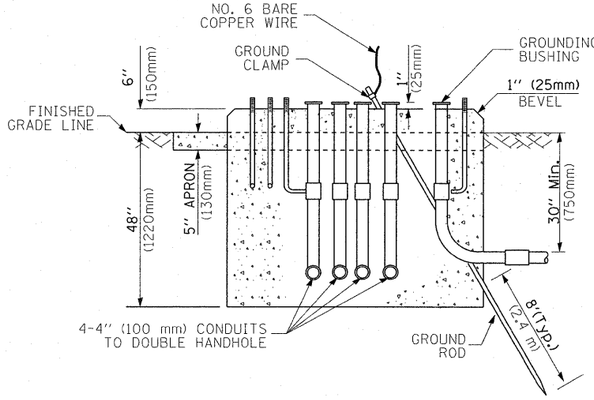
TOP VIEW



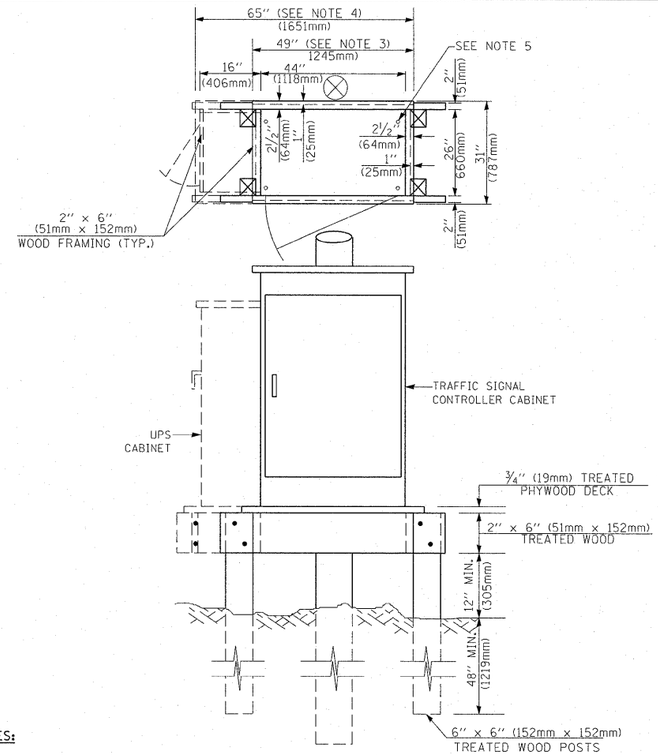
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



TOP VIEW



TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 5 OF 6 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	46
	TS-05	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET	R		
RAILROAD CONTROL CABINET		R	
COMMUNICATIONS CABINET	CC R	ECC	CC
MASTER CONTROLLER		EMC	MC
MASTER MASTER CONTROLLER		EMMC	MMC
UNINTERRUPTIBLE POWER SUPPLY	UPS R	EUPS	UPS
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT	R	P	P
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT	R T	P T	P T
STEEL MAST ARM ASSEMBLY AND POLE	R		
ALUMINUM MAST ARM ASSEMBLY AND POLE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE	R		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA	R PTZ	PTZ	PTZ
SIGNAL POST	R		
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM	R		
GUY WIRE	R		
SIGNAL HEAD	R		
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)			2
SIGNAL HEAD WITH BACKPLATE	R		
SIGNAL HEAD OPTICALLY PROGRAMMED	R "P"	"P"	"P"
FLASHER INSTALLATION (S DENOTES SOLAR POWER)	R "F"	"F"	"F"
PEDESTRIAN SIGNAL HEAD	R		
PEDESTRIAN PUSHBUTTON DETECTOR	R		
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR	R APS	APS	APS
ILLUMINATED SIGN "NO LEFT TURN"	R		
ILLUMINATED SIGN "NO RIGHT TURN"	R		
DETECTOR LOOP, TYPE I			
PREFORMED DETECTOR LOOP		P	P
MICROWAVE VEHICLE SENSOR	R M	M	M
VIDEO DETECTION CAMERA	R V	V	V
VIDEO DETECTION ZONE			
PAN, TILT, ZOOM CAMERA	R PTZ	PTZ	PTZ
WIRELESS DETECTOR SENSOR	R W	W	W
WIRELESS ACCESS POINT	R		

ITEM	REMOVAL	EXISTING	PROPOSED
EMERGENCY VEHICLE LIGHT DETECTOR	R		
CONFIRMATION BEACON	R		
HANDHOLE	R		
HEAVY DUTY HANDHOLE	R H	H	H
DOUBLE HANDHOLE	R		
JUNCTION BOX	R		
GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)			
TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE	R		
COMMON TRENCH			CT
COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC
SYSTEM ITEM		S	S
INTERSECTION ITEM		I	IP
REMOVE ITEM	R		
RELOCATE ITEM	RL		
ABANDON ITEM	A		
12" (300mm) TRAFFIC SIGNAL SECTION		R	R
12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE		R / Y / G	
SIGNAL FACE		R Y G Y G	R Y G Y G
SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD		R Y G Y G "P"	R Y G Y G "P"
12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL		DW / W	
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED			
12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID			
PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER		C D	C D
RADIO INTERCONNECT	R		
RADIO REPEATER	R ERR	ERR	RR
DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED		5	5
GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)		1	1

ITEM	REMOVAL	EXISTING	PROPOSED
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE		1	1
COAXIAL CABLE		C	C
VENDOR CABLE FOR CAMERA		V	V
COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED		6	6
FIBER OPTIC CABLE NO. 62.5/125, MM12F		12F	
FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F		24F	24F
FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE		C	C
CONTROLLER CABINET AND FOUNDATION TO BE REMOVED	RCF		
STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED	RMF		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED	RMF		
SIGNAL POST AND FOUNDATION TO BE REMOVED	RMF		
INTERSECTION & SAMPLING (SYSTEM) DETECTOR		IS	IS
SAMPLING (SYSTEM) DETECTOR		S	S
EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		P	
EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PP	
PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR		PIS	PIS
PREFORMED SAMPLING (SYSTEM) DETECTOR		PS	PS

RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET	R	R
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 6 OF 6 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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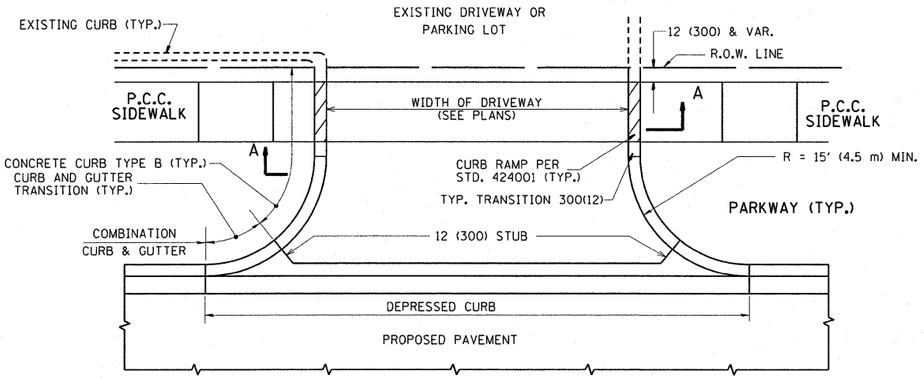
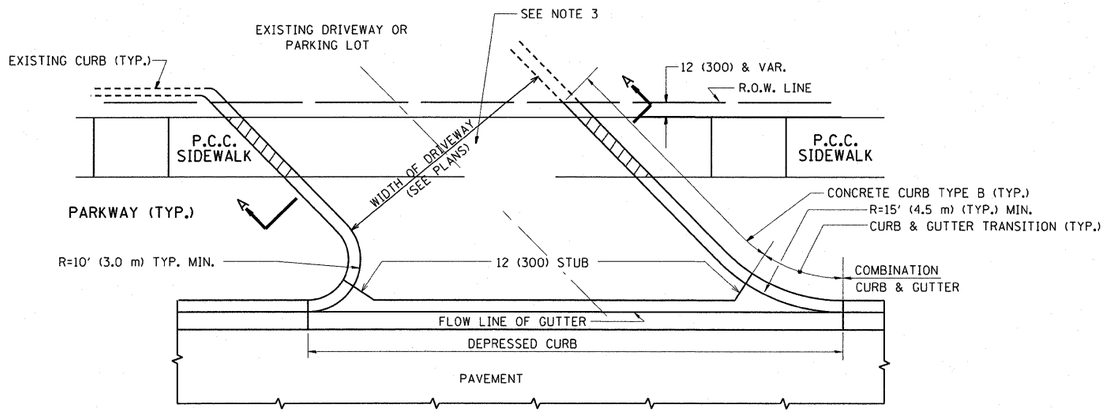
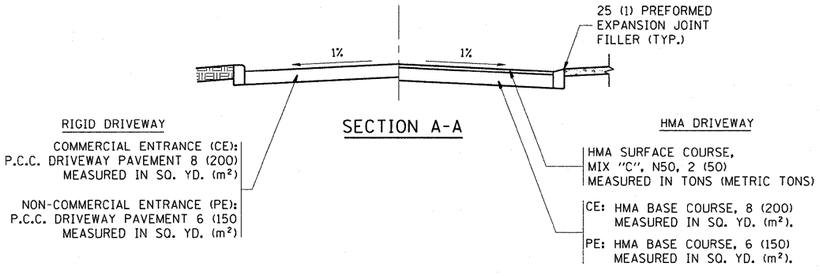
	TS-05	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

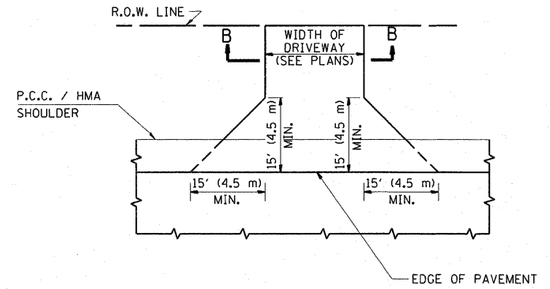


WITH CONCRETE CURB, TYPE B

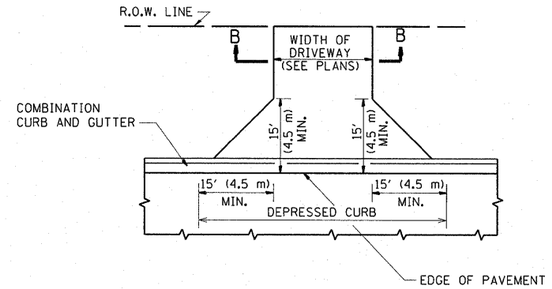


WITH CONCRETE CURB, TYPE B

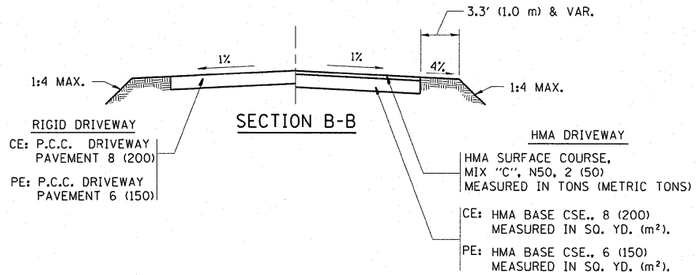




ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE,
MIX "C", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200)
MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

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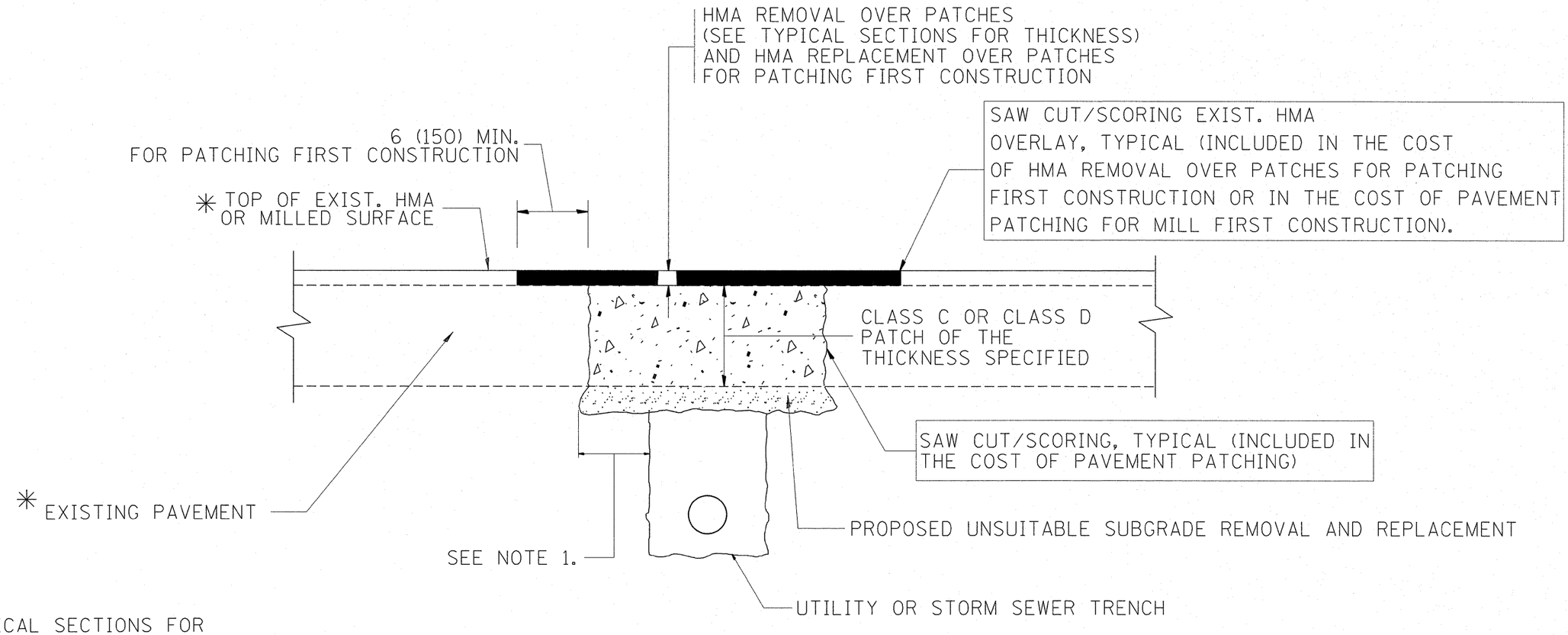
	PLOT SCALE = 50.0000 ' / IN.	DRAWN -	REVISED - P. LaFLUER 04-15-03
	PLOT DATE = 2/3/2011	CHECKED -	REVISED - R. BORO 01-01-07
		DATE - 11-04-95	REVISED - R. BORO 06-11-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER > = 15' (4.5 m)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	48
	BD0156-07 (BD-01)	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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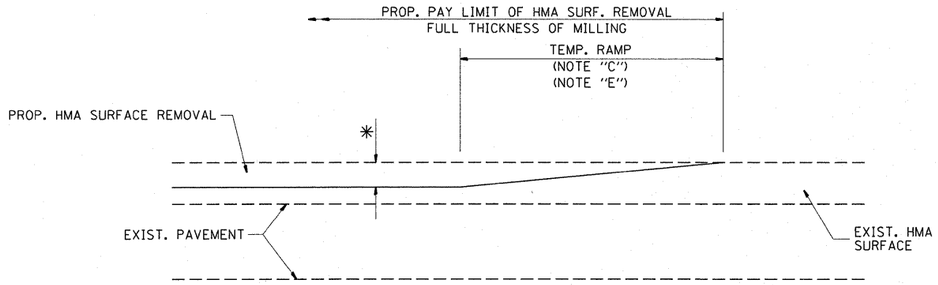
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 2/3/2011	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

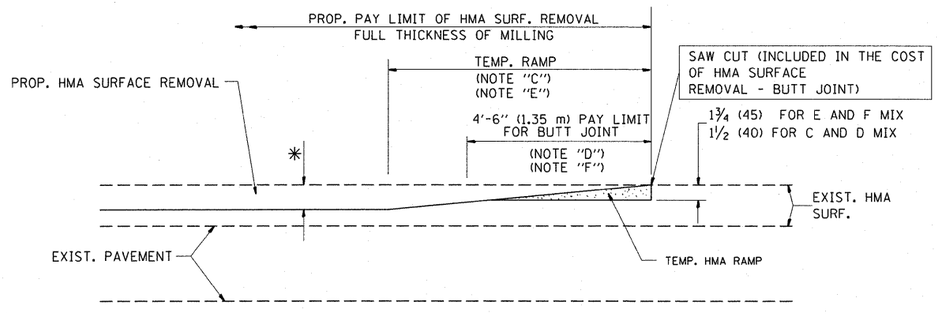
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	49
	BD400-04 (BD-22)	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

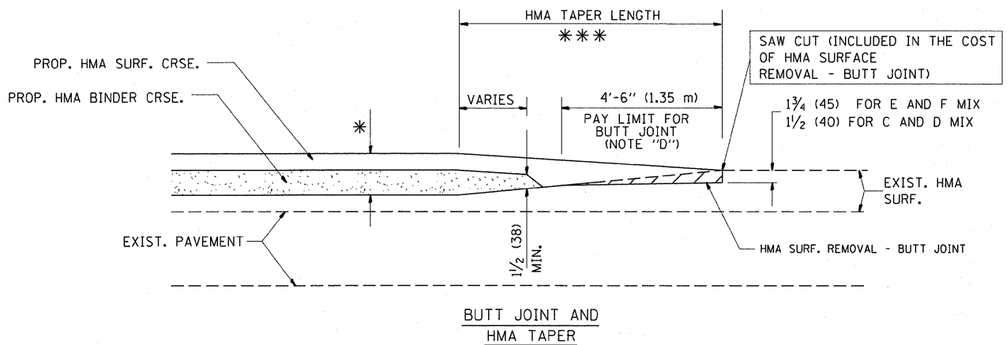


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

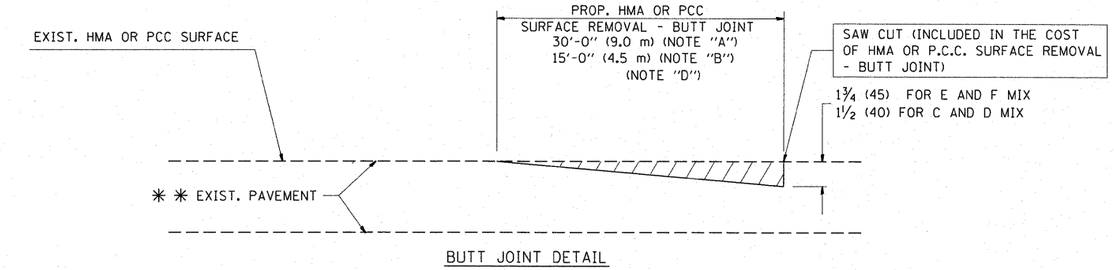
OPTION 2

TYPICAL TEMPORARY RAMP

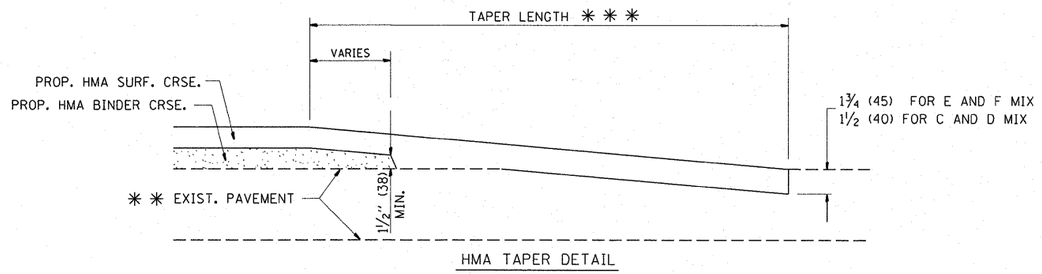


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

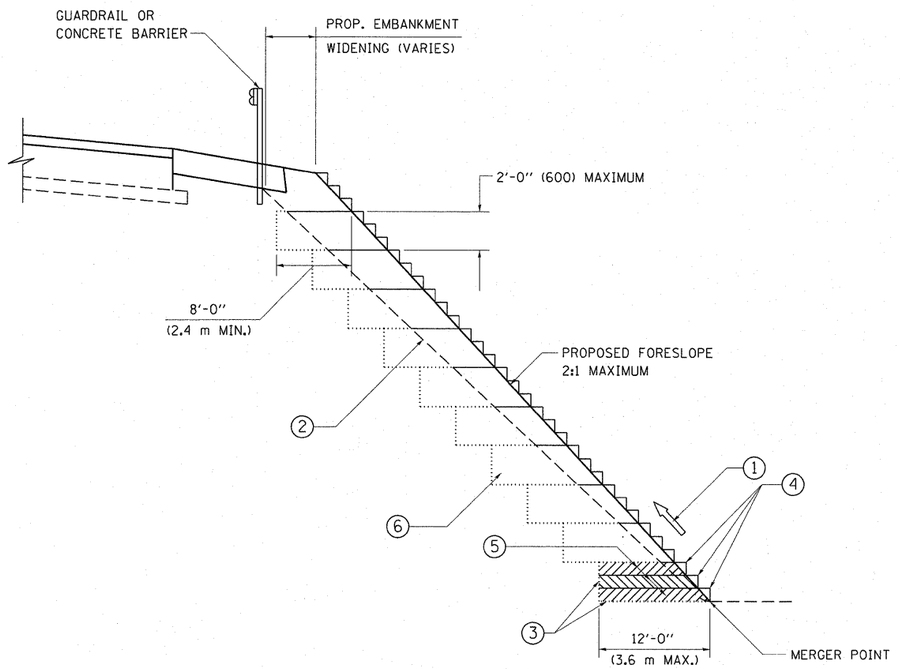
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\pwidot\abreuah\d0139632\DistStd.dgn	USER NAME = abreuah	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**
		DRAWN -	REVISED - A. ABBAS 03-21-97	
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01	
	PLOT DATE = 2/3/2011	DATE - 06-13-90	REVISED - R. BORO 01-01-07	

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	50
	BD400-05 BD32	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TYPICAL BENCHING DETAIL FOR EMBANKMENT

NOTES:

1. CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
2. EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
3. BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
4. TRIM TO FINAL SLOPE.
5. EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
6. EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
7. SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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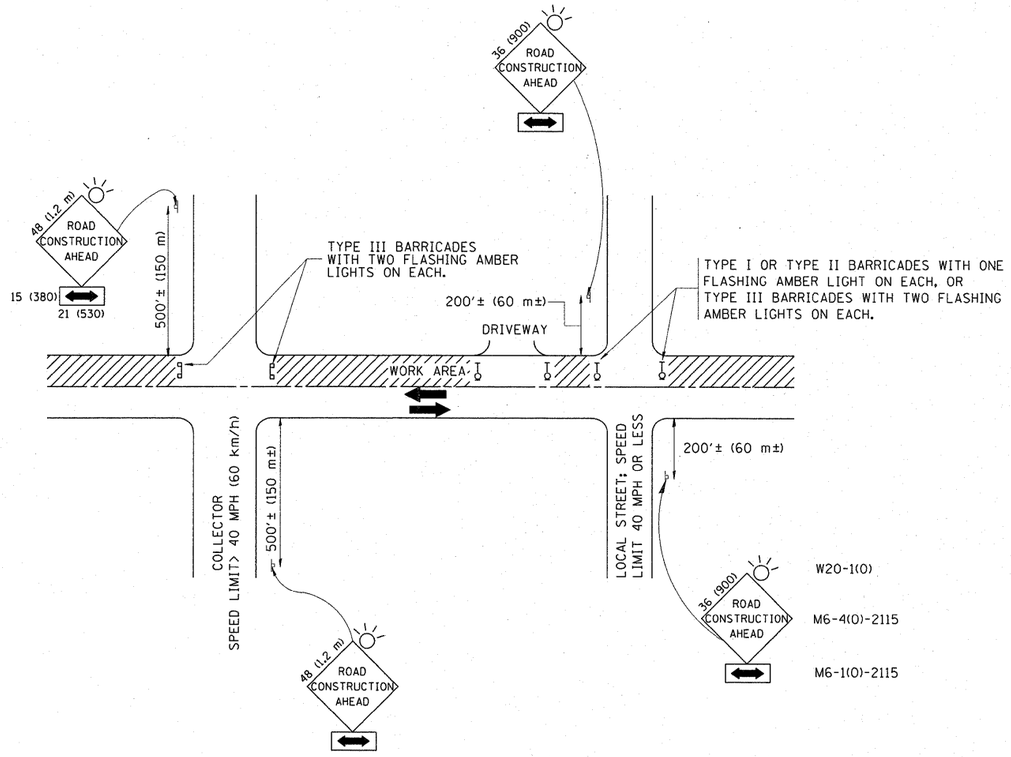
		DRAWN - CADD	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - S.E.B.	REVISED -
	PLOT DATE = 2/3/2011	DATE - 06-16-04	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BENCHING DETAIL
FOR EMBANKMENT WIDENING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	51
	BD-51	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

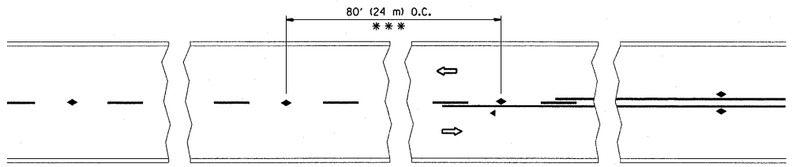
FILE NAME = c:\pw_work\pwidot\abreuah\d0139632\DistStd.dgn	USER NAME = abreuah	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 2/3/2011	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

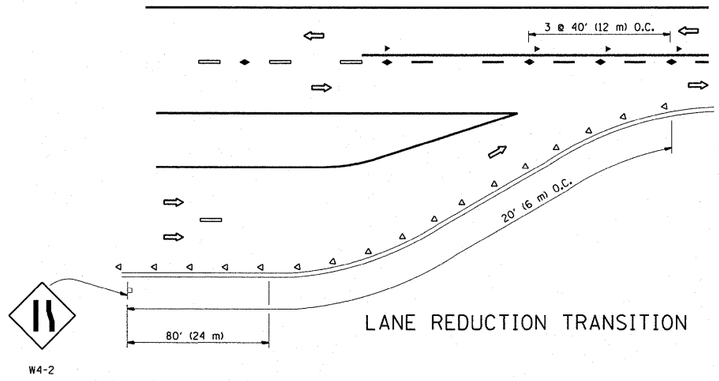
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	52
	TC-10	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

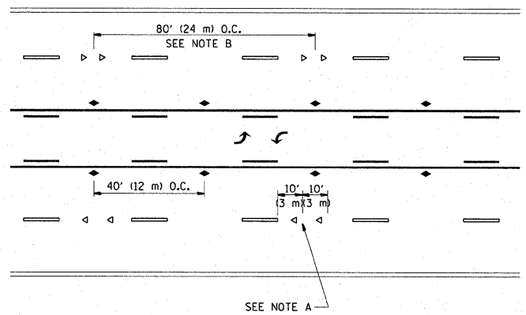


* * * REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

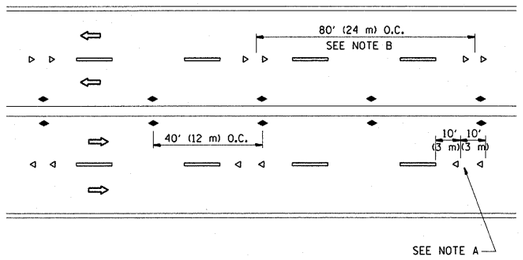
TWO-LANE/TWO-WAY



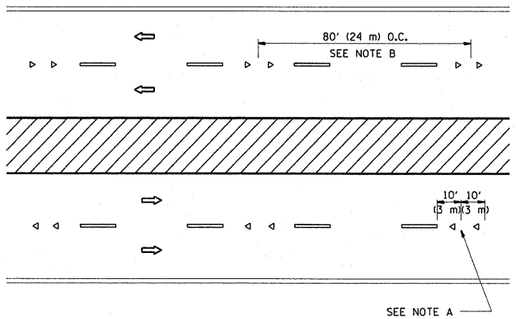
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

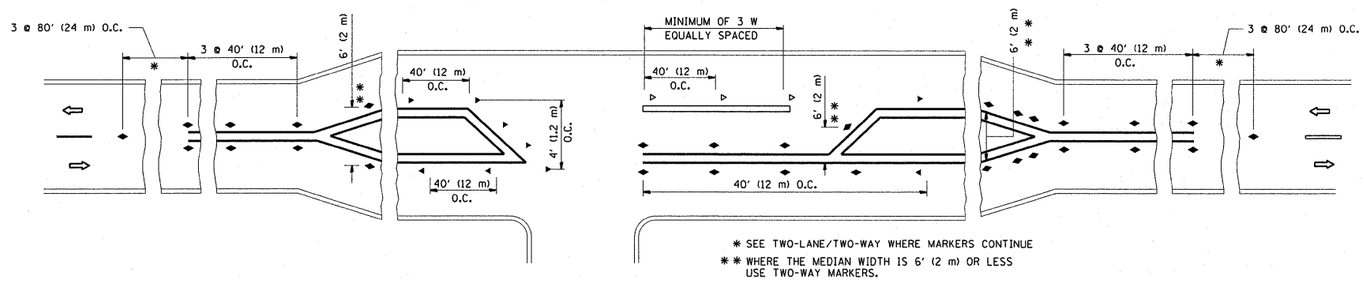
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

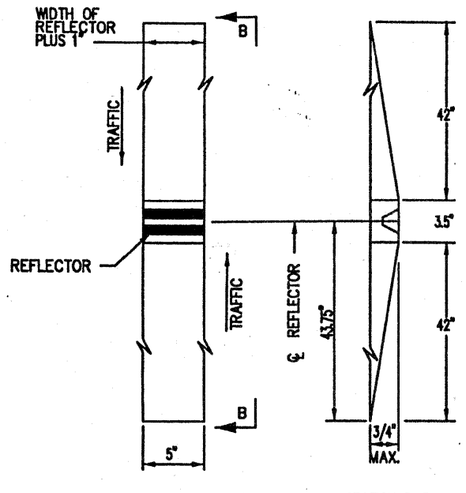
FILE NAME = c:\pw_work\pwidot\abreuah\d0139632\DistStd.dgn	USER NAME = abreuah	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 2/3/2011	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

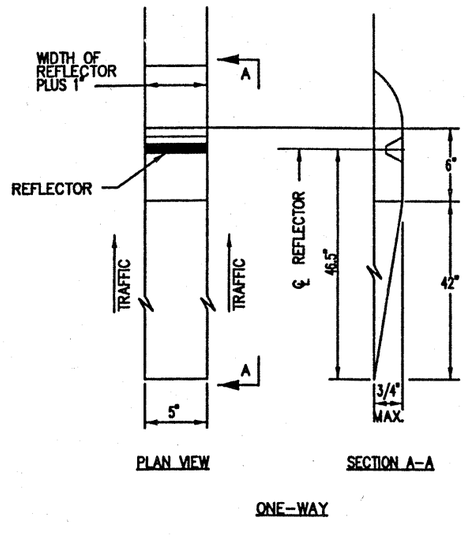
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	53
	TC-11	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TWO-WAY



ONE-WAY

GENERAL NOTES:

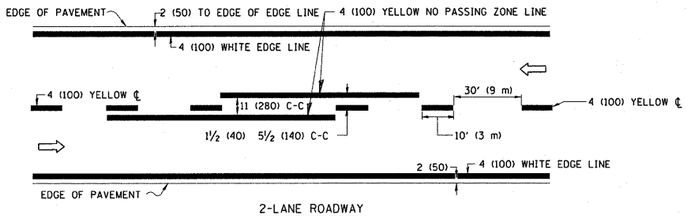
1. Installation shall conform to IDOT Highway Standard 781001-02 (or latest) for marker placement.
2. IDOT Standard 781001-02 shall be modified to reflect recessed pavement markers instead of raised pavement markers.

RECESSED REFLECTIVE PAVEMENT MARKERS

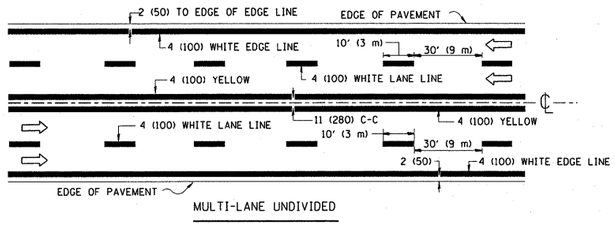
INSTALLATION NOTES:

1. Saw cut to dimensions shown.
2. Sawcut areas to be dry and free or material that adversely affects the adhesive bond.
3. Install the reflector with an approved two-component epoxy adhesive. Epoxy should not obscure or block the lens.
4. Install top of reflector +/- 3/8 inch below the pavement surface.
5. Reflector shall be 3M Series 190 or approved equivalent.

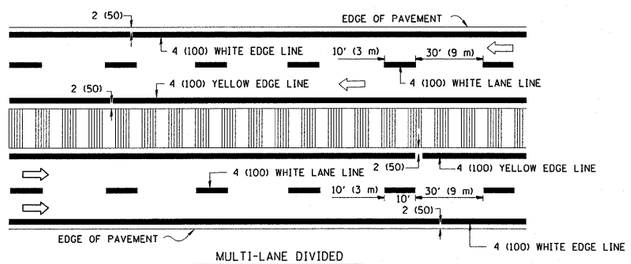
	TYPICAL RECESSED REFLECTIVE PAVEMENT MARKERS
	STANDARD KC781001-03



2-LANE ROADWAY



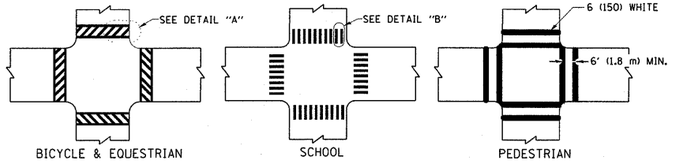
MULTI-LANE UNDIVIDED

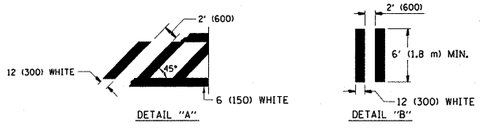


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

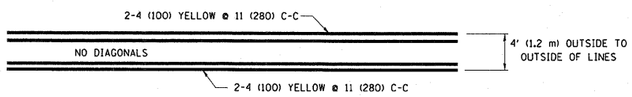
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

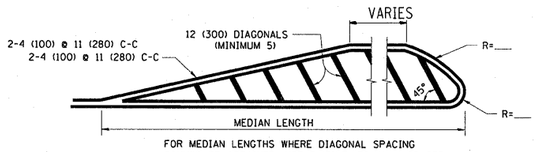




TYPICAL CROSSWALK MARKING



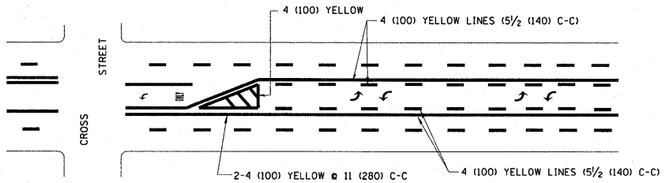
4' (1.2 m) WIDE MEDIANS ONLY



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

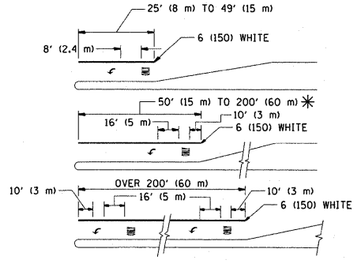
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

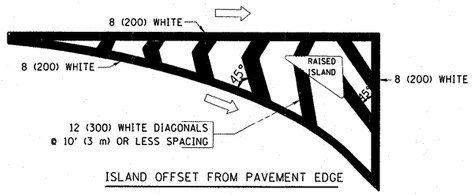


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

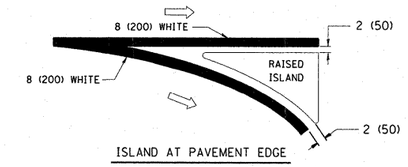
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⊄ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⊄ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⊄ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⊄ 6 (150) 12 (300) ⊄ 45° 12 (300) ⊄ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⊄ 4 (100) WITH 12 (300) DIAGONALS ⊄ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⊄ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS, 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS	12 (300) ⊄ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

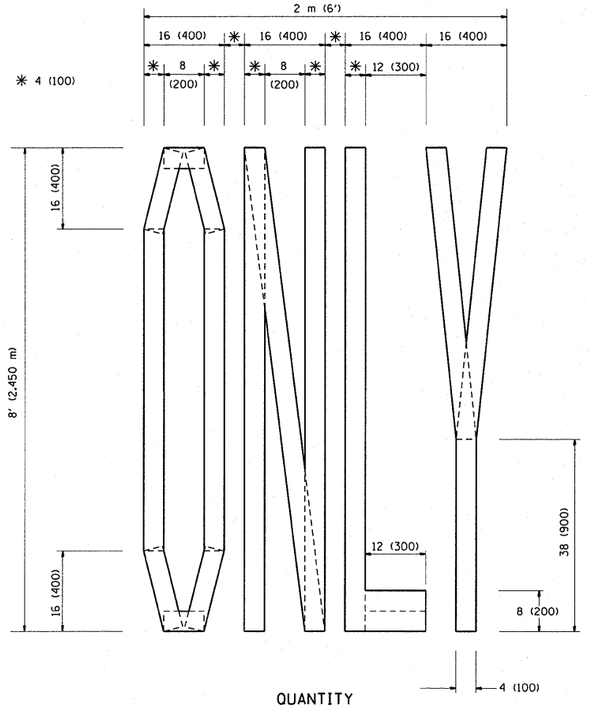
FILE NAME = c:\pw_work\pwidot\abreuah\d0139632\DistStd.dgn	USER NAME = abreuah	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
		DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/3/2011	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

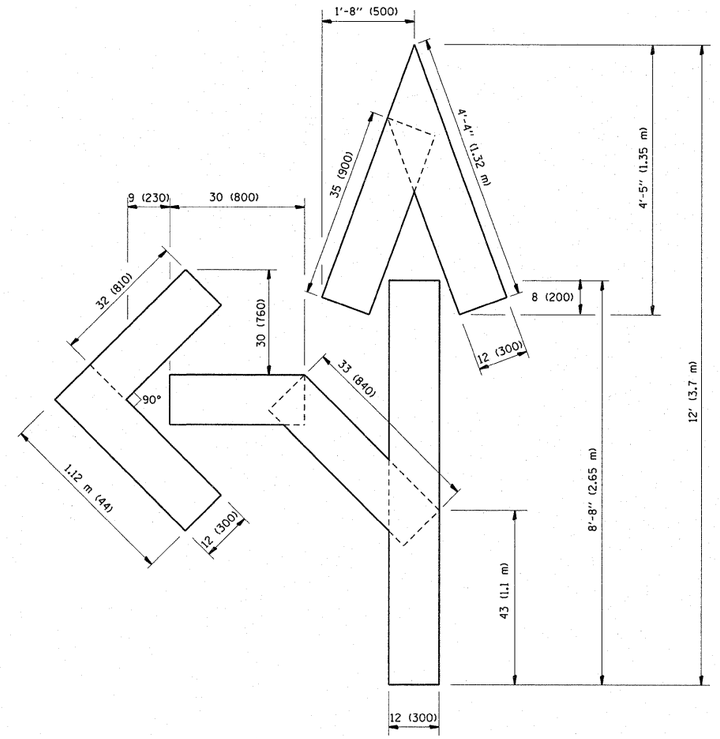
DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

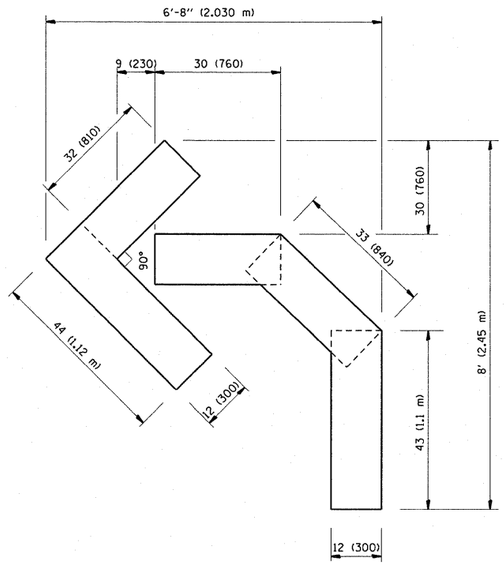
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	55
	TC-13	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

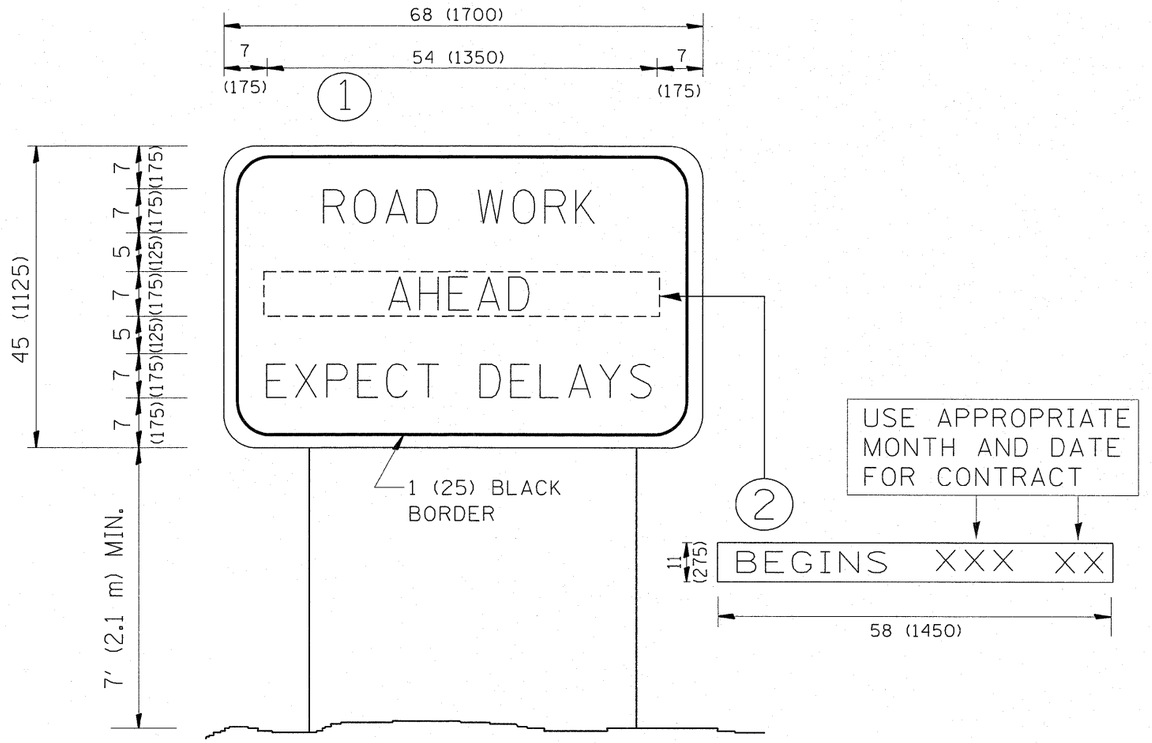
FILE NAME = c:\pw_work\pwidot\abreuah\d0139632\DistStd.dgn	USER NAME = abreuah	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 2/3/2011	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	56
	TC-16	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\pwidot\abreuch\d0139632\DistStd.dgn	USER NAME = abreuch	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 2/3/2011	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

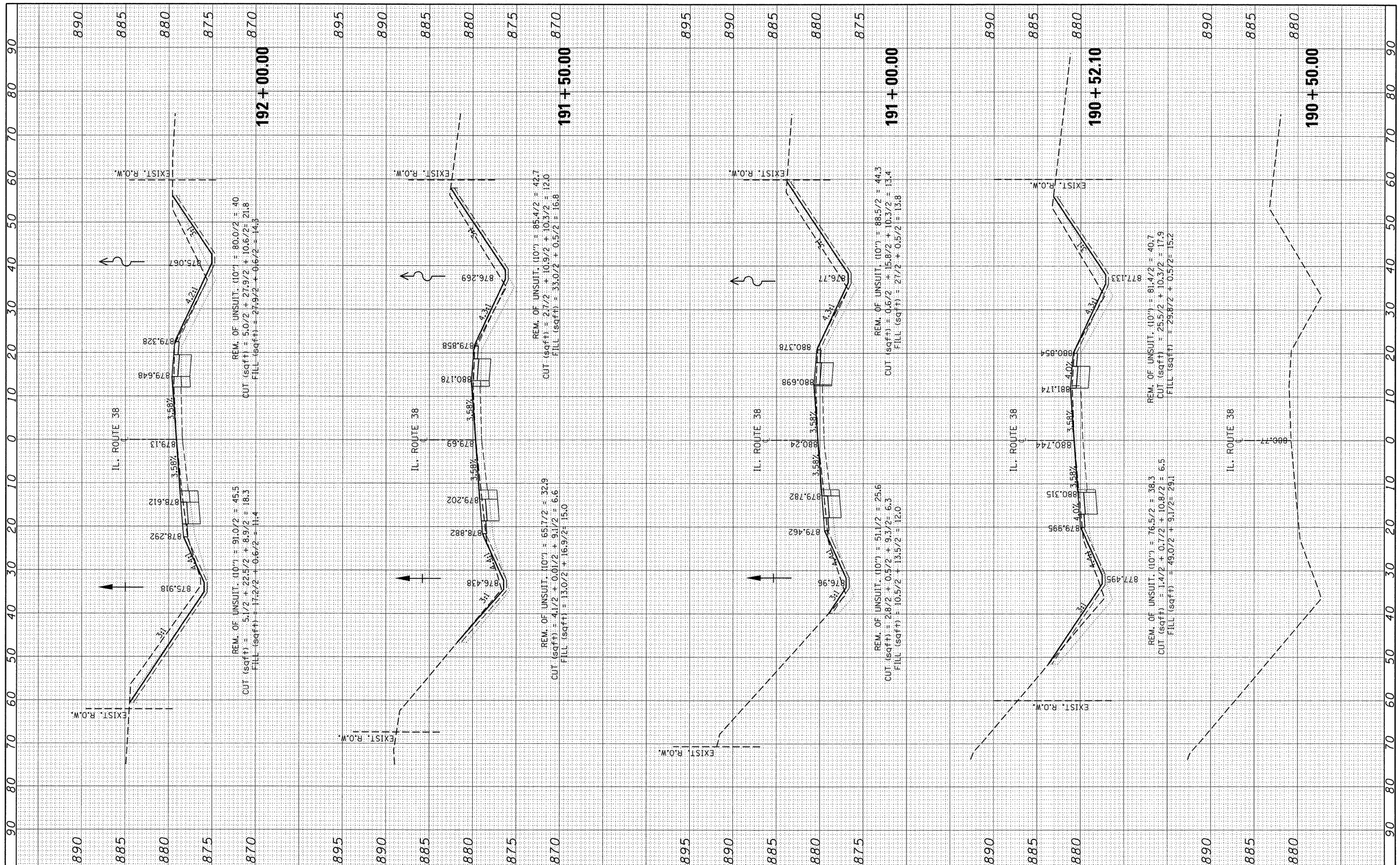
ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	71	57
	TC-22	CONTRACT NO. 60K65		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	
NO.	PLOTTED	
	TOPOGRAPHY	
	AREAS	
	AREAS CHECKED	

ORIGINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	
NO.	PLOTTED	
	TOPOGRAPHY	
	AREAS	
	AREAS CHECKED	



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		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

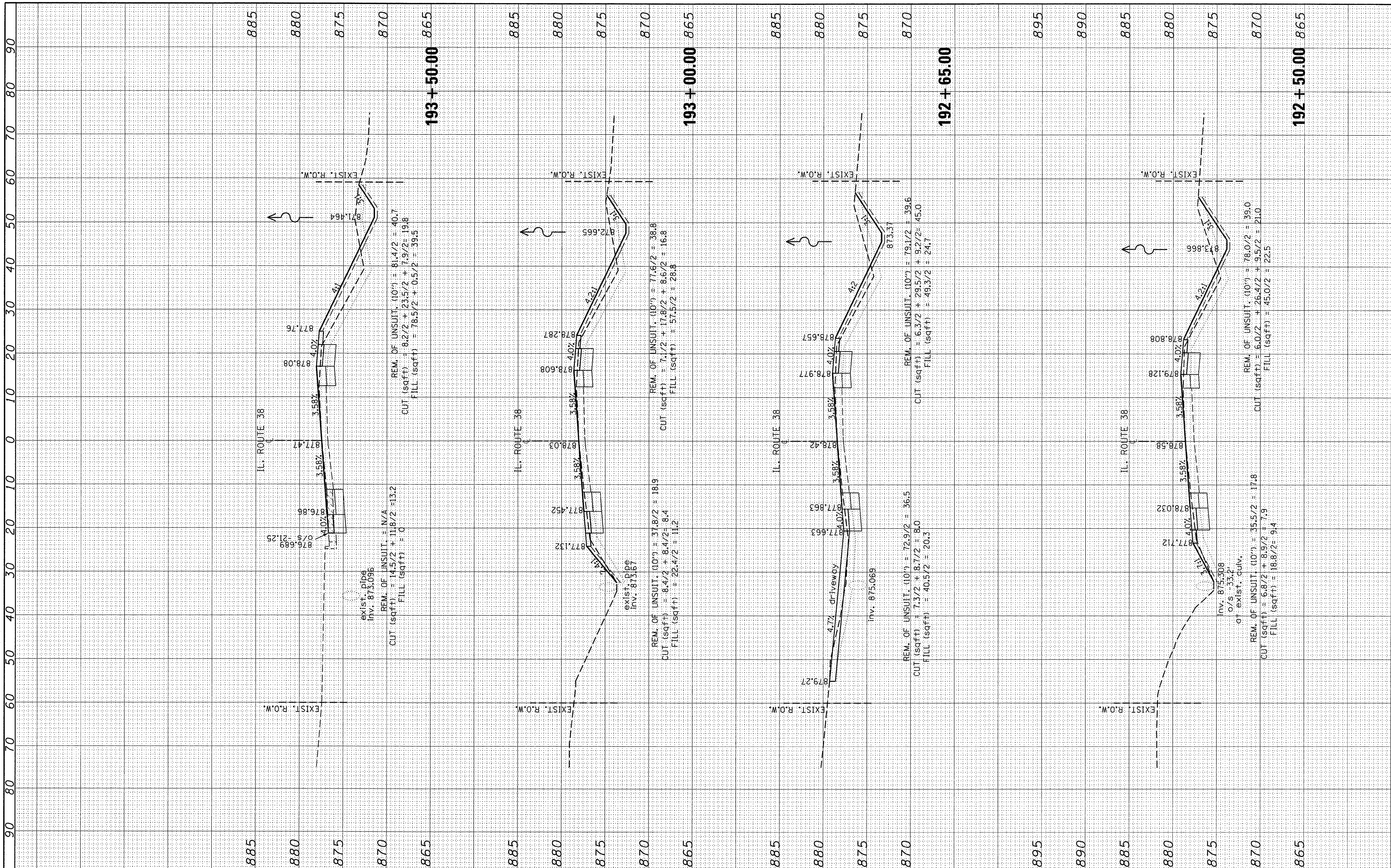
IL. ROUTE 38
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 190+50.00 TO STA. 192+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	58
		CONTRACT NO. 60K65		
	ILLINOIS	FED. AID PROJECT		

FINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		



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		DRAWN -	REVISED -
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	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

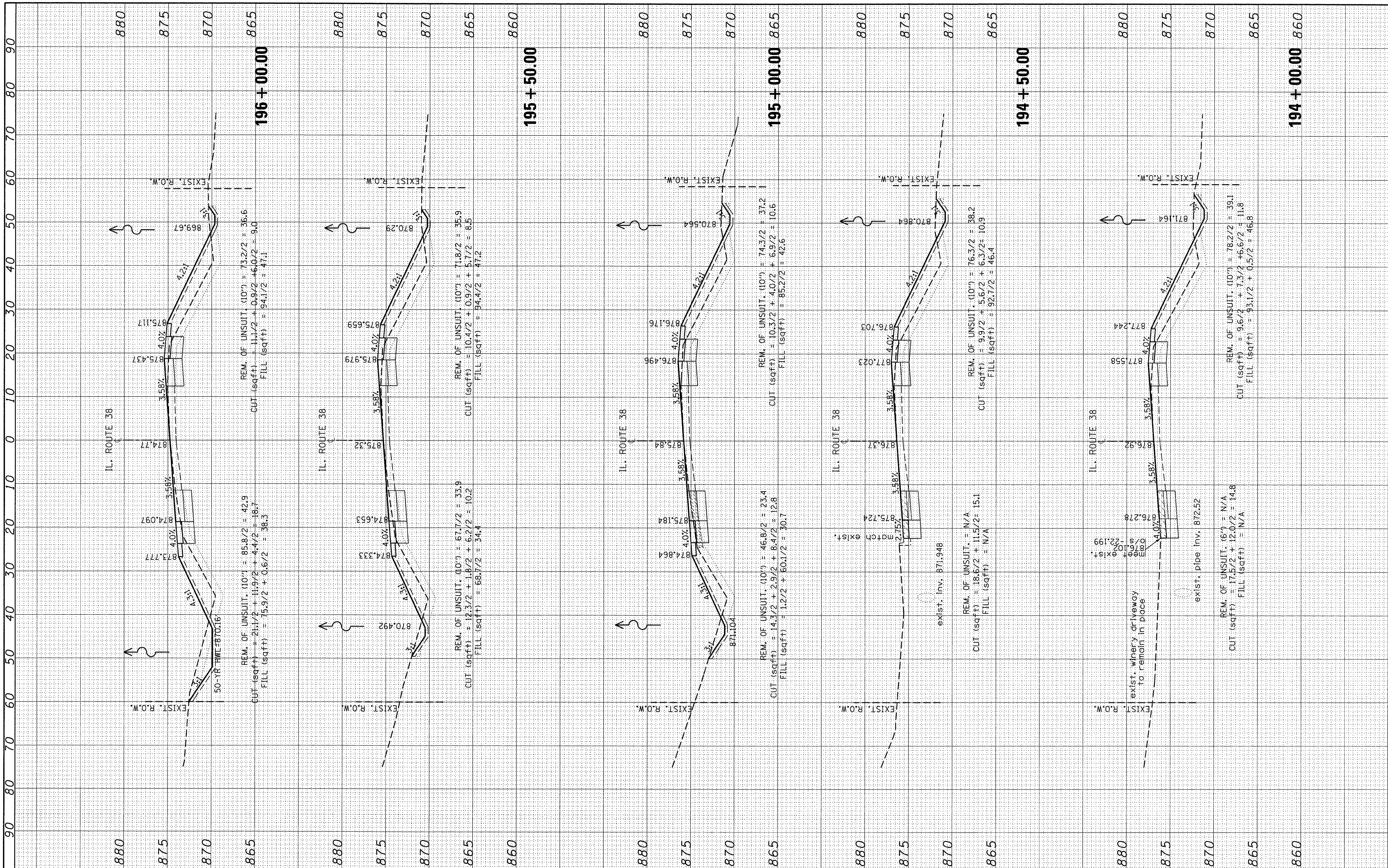
IL. ROUTE 38
CROSS SECTIONS

SCALE: | SHEET NO. | OF | SHEETS | STA. 192+50.00 TO STA. 193+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	59

CONTRACT NO. 60K65

ILLINOIS | FED. AID PROJECT



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		DRAWN -	REVISED -		
	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -		
	PLOT DATE = 2/22/2011	DATE -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. 194+00.00 TO STA. 196+00.00

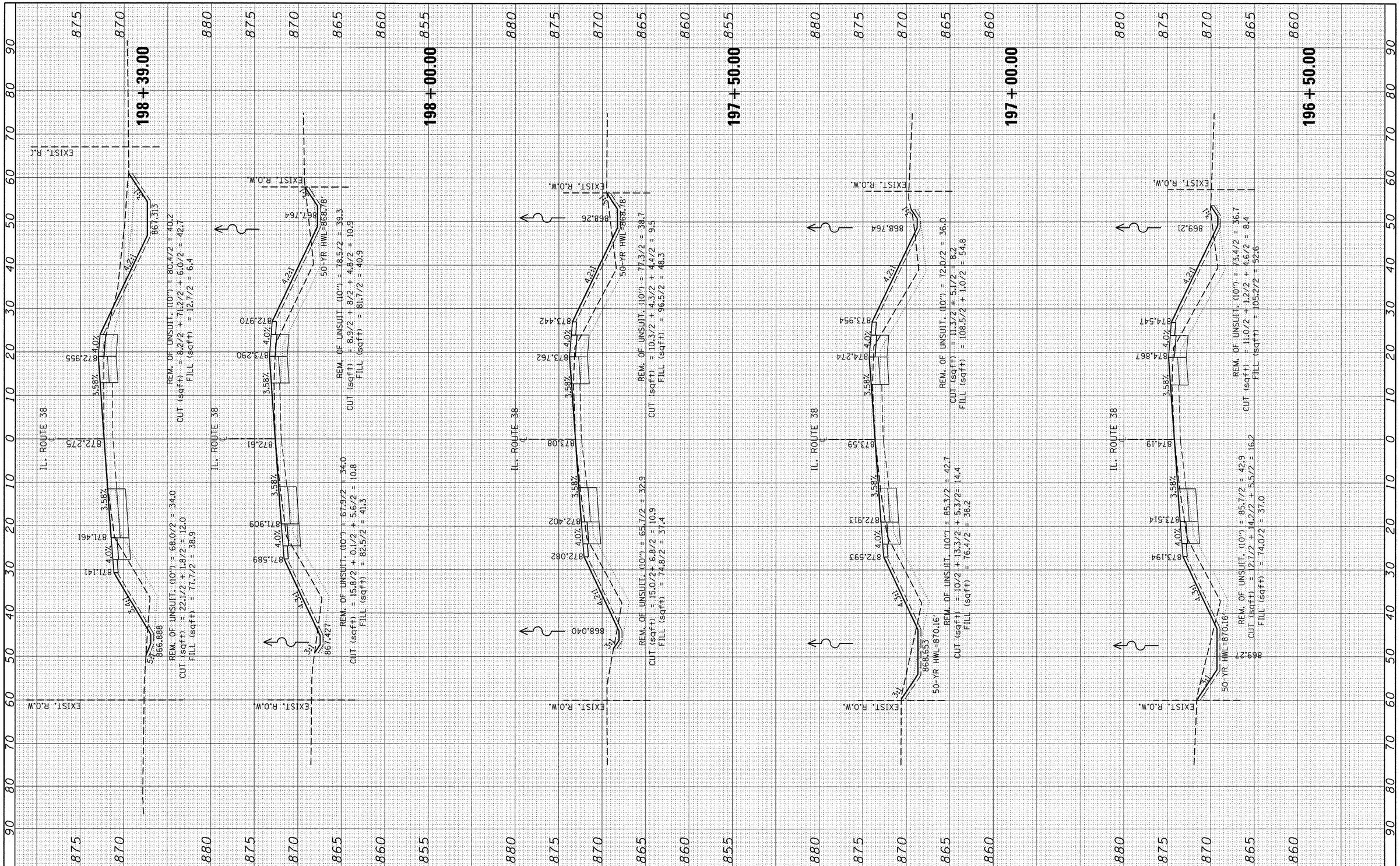
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	60
		CONTRACT NO. 60K65		
		ILLINOIS	FED. AID PROJECT	

FINAL SURVEY — NOTE BOOK NO. — SURVEYED / PLOTTED / TEMPLATE / AREAS / AREAS CHECKED — BY — DATE

ORIGINAL SURVEY — NOTE BOOK NO. — SURVEYED / PLOTTED / TEMPLATE / AREAS / AREAS CHECKED — BY — DATE

FINAL SURVEY	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK	TOPO UPDATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK	TOPO UPDATE		
	AREAS		
NO.	AREAS CHECKED		



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P115308-sht-xsht-IL38.dgn		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

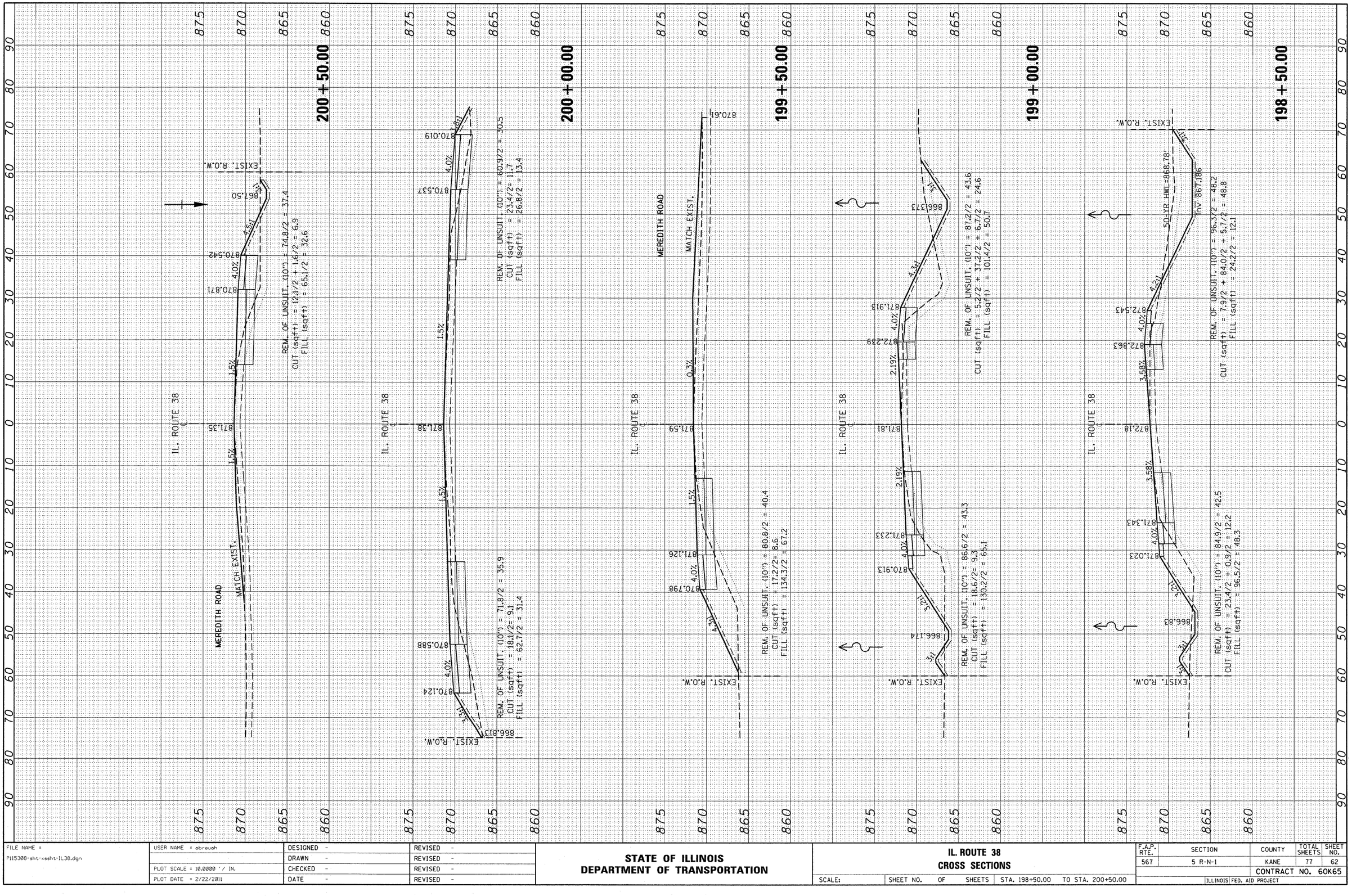
IL. ROUTE 38
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 196+50.00 TO STA. 198+39.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	61
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	ISSUE RATE		
	AREAS		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	ISSUE RATE		
	AREAS		
NO.	AREAS CHECKED		



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		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 38
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 198+50.00 TO STA. 200+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	62
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

FILE NAME = P115308-dot-xsect-IL38.dgn

USER NAME = dbr-kush

PLOT SCALE = 10.0000 ' / IN.

PLOT DATE = 8/22/2011

DESIGNED -

DRAWN -

CHECKED -

DATE -

REVISED -

REVISED -

REVISED -

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 38
CROSS SECTIONS

SCALE:

SHEET NO. OF SHEETS STA. 201+00.00 TO STA. 203+00.00

F.A.P. RTE. 567

SECTION 5 R-N-1

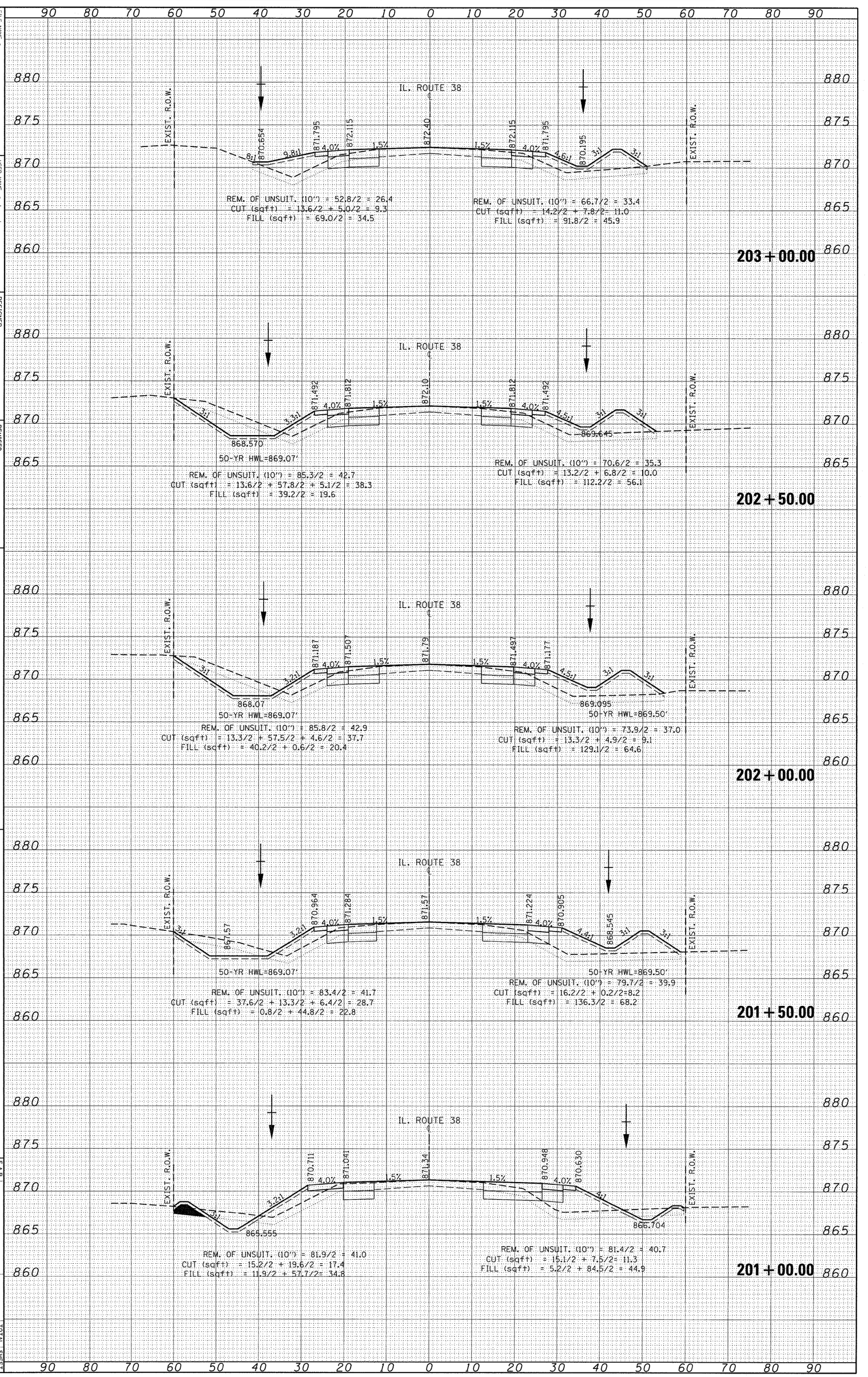
COUNTY KANE

CONTRACT NO. 60K65

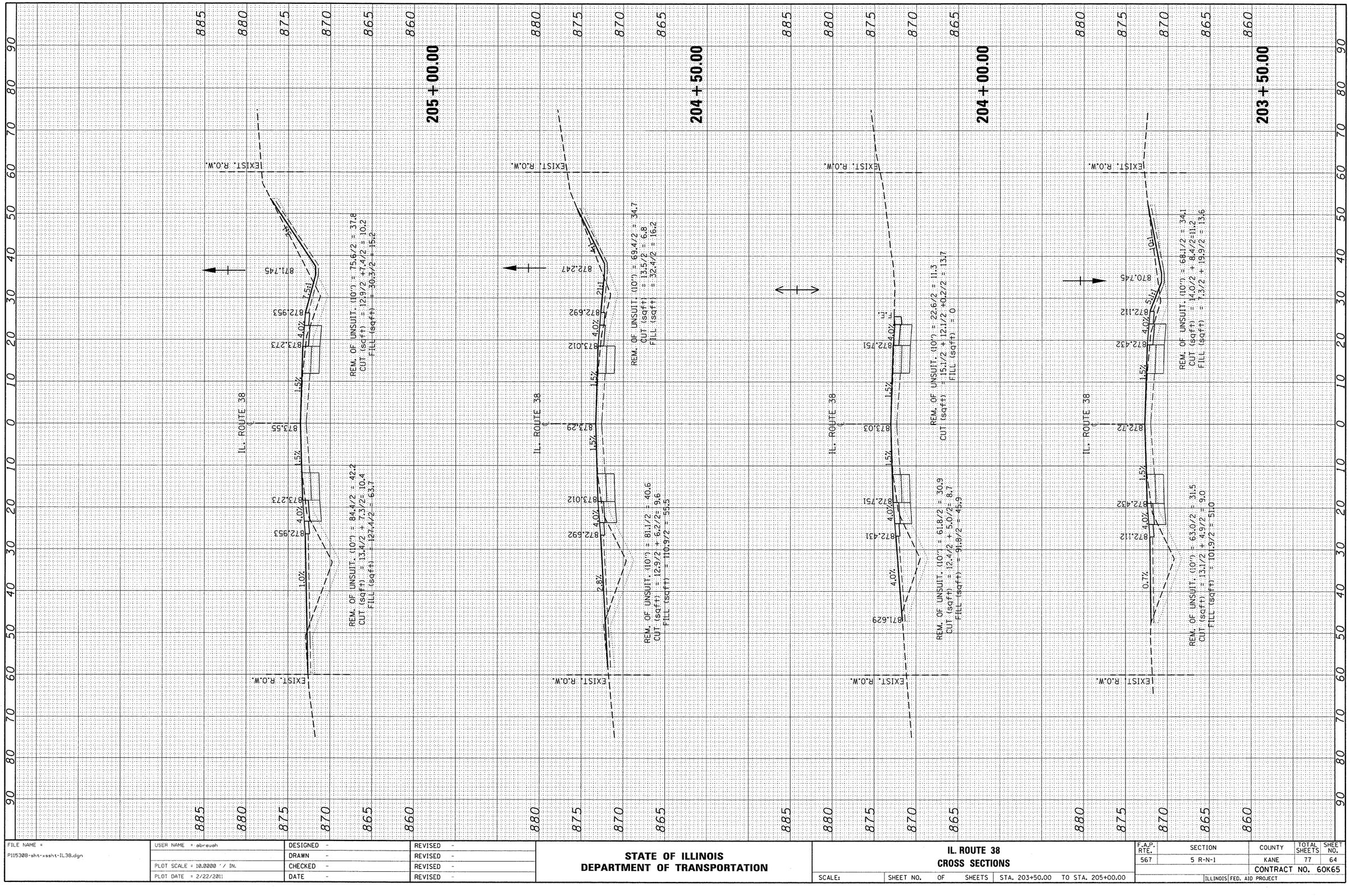
ILLINOIS FED. AID PROJECT

TOTAL SHEETS 77

SHEET NO. 63



FINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TOPSOIL SITE		
	AREAS		
NO.	AREAS CHECKED		



ORIGINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TOPSOIL SITE		
	AREAS		
NO.	AREAS CHECKED		

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		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 38
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 203+50.00 TO STA. 205+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	64
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

FILE NAME = P115308-sht-xssht-IL38.dgn

USER NAME = abrueah

PLOT SCALE = 10.0000 ' / IN.

PLOT DATE = 2/22/2011

DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 38
CROSS SECTIONS

SCALE:

SHEET NO. OF SHEETS STA. 205+50.00 TO STA. 206+70.00

F.A.P. RTE. 567

SECTION 5 R-N-1

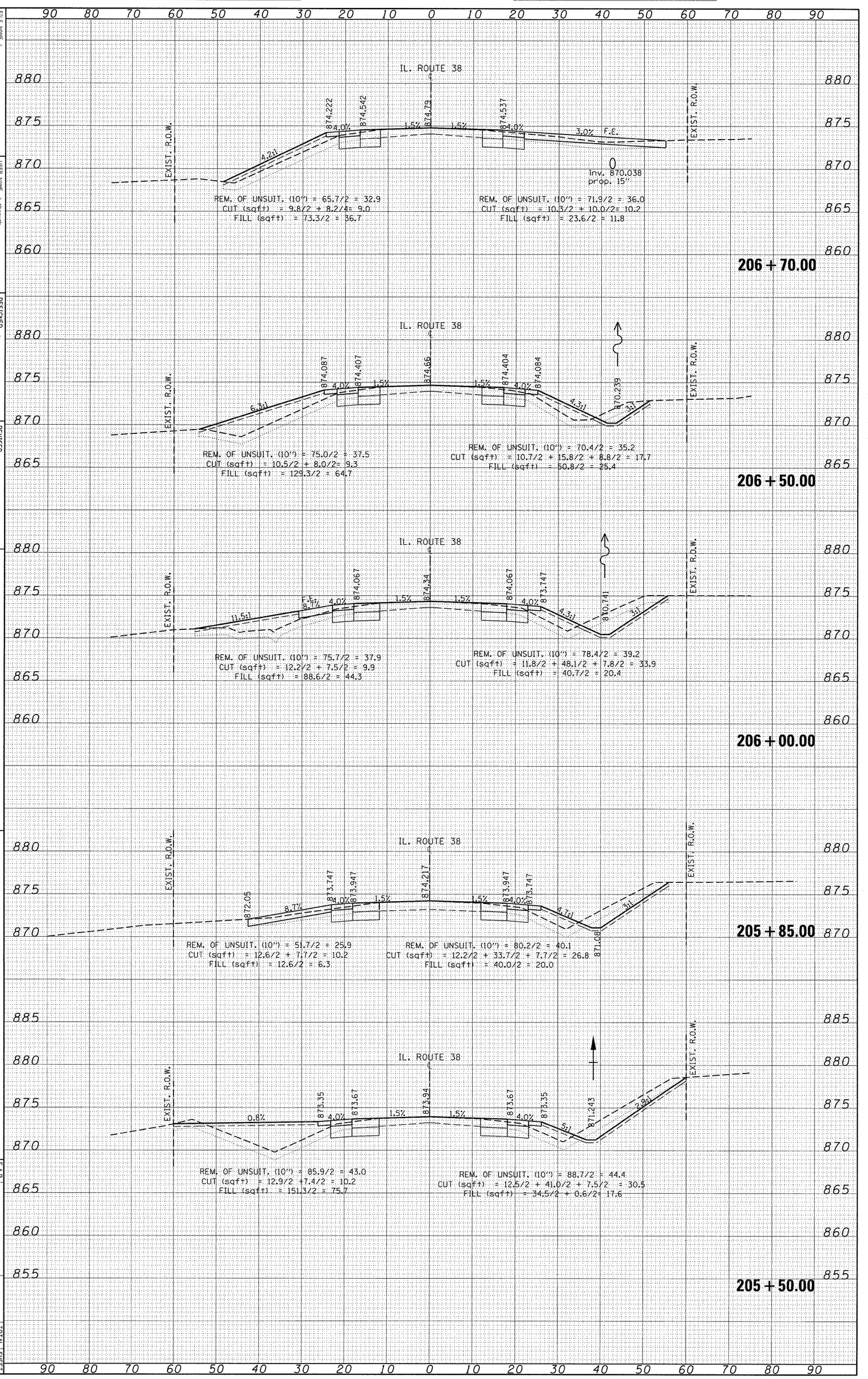
COUNTY KANE

CONTRACT NO. 60X65

TOTAL SHEETS 77

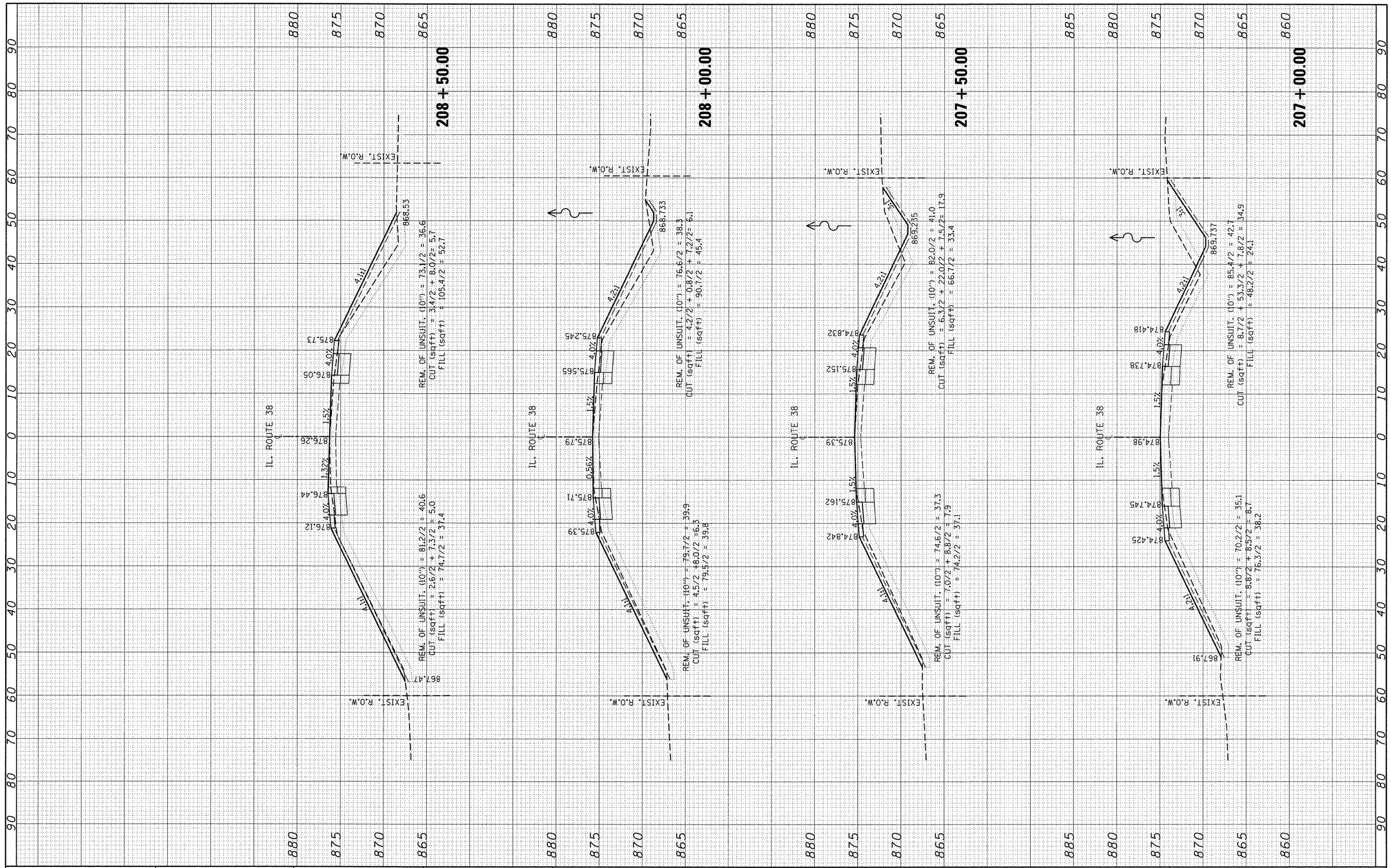
SHEET NO. 65

ILLINOIS FED. AID PROJECT



FINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	
NO.	PLOTTED	
	TERRAIN SITE	
	AREAS	
	AREAS CHECKED	

ORIGINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	
NO.	PLOTTED	
	TERRAIN SITE	
	AREAS	
	AREAS CHECKED	



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	PLOT DATE = 2/22/2011	DATE -	REVISED -

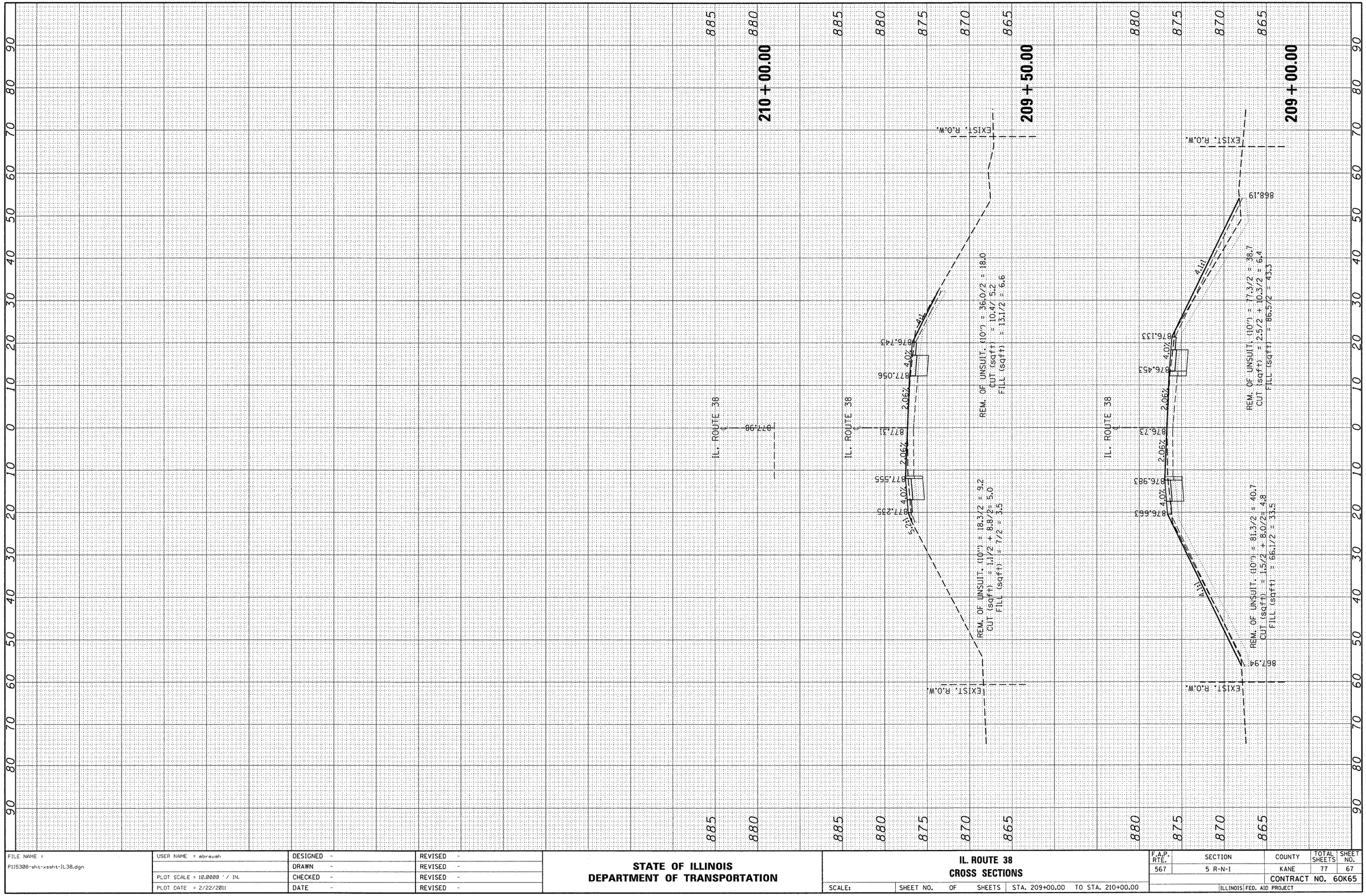
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. ROUTE 38
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 207+00.00 TO STA. 208+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	66
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	VOLUMES		
	AREAS		
	AREAS CHECKED		



ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	VOLUMES		
	AREAS		
	AREAS CHECKED		

FILE NAME = P115308-sht-xssht-IL38.dgn	USER NAME = abraush	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/22/2011	DATE -	REVISED -

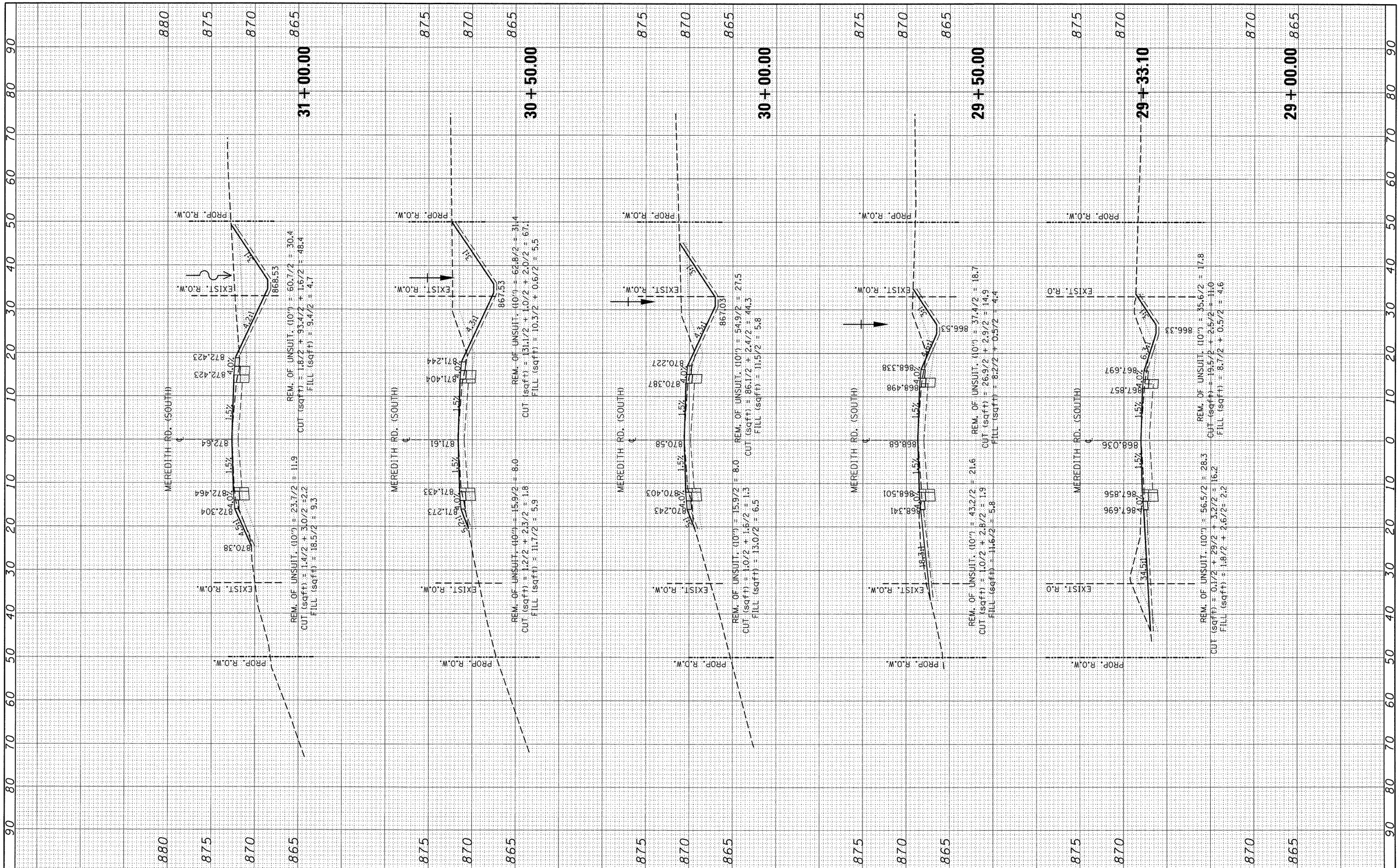
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL ROUTE 38
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 209+00.00 TO STA. 210+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	67
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	AREAS CHECKED		



ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	AREAS CHECKED		

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		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH MEREDITH ROAD
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 29+00.00 TO STA. 31+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	68
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

33 + 50.00

MEREDITH RD. (SOUTH) ℄
871.85
871.623
871.463
4.0%
1.5%
4:1
EXIST. R.O.W.
PROP. R.O.W.
864.75
REM. OF UNSUIT. (10") = 66.8/2 = 33.4
CUT (sqft) = 1.6/2 = 0.8
FILL (sqft) = 203.9/2 = 102.0

871.91
871.697
871.537
4.0%
1.5%
4:1
EXIST. R.O.W.
PROP. R.O.W.
864.873
REM. OF UNSUIT. (10") = 63.8/2 = 31.9
CUT (sqft) = 1.4/2 = 0.7
FILL (sqft) = 178.6/2 = 89.3

872.26
871.903
871.743
4.0%
1.5%
4:1
EXIST. R.O.W.
PROP. R.O.W.
865.246
REM. OF UNSUIT. (10") = 60.8/2 = 30.4
CUT (sqft) = 1.5/2 + 0.2/2 = 0.9
FILL (sqft) = 139.1/2 = 69.6

872.65
872.463
872.303
4.0%
1.5%
4.5:1
EXIST. R.O.W.
PROP. R.O.W.
REM. OF UNSUIT. (10") = 40.2/2 = 20.1
CUT (sqft) = 0.9/2 + 1.0/2 =
FILL (sqft) = 45.1/2 = 22.6

872.65
872.472
872.312
4.0%
1.5%
4.5:1
EXIST. R.O.W.
PROP. R.O.W.
870.114
REM. OF UNSUIT. (10") = 25.3/2 = 12.7
CUT (sqft) = 0.9/2 + 2.1/2 = 1.5
FILL (sqft) = 22.2/2 = 11.1

32 + 00.00

MEREDITH RD. (SOUTH) ℄
871.91
871.697
871.537
4.0%
1.5%
4:1
EXIST. R.O.W.
PROP. R.O.W.
864.873
REM. OF UNSUIT. (10") = 63.8/2 = 31.9
CUT (sqft) = 1.4/2 = 0.7
FILL (sqft) = 178.6/2 = 89.3

872.26
871.844
872.004
4.0%
1.5%
4:1
EXIST. R.O.W.
PROP. R.O.W.
866.016
REM. OF UNSUIT. (10") = 56.1/2 = 29.1
CUT (sqft) = 3.9/2 = 2.0
FILL (sqft) = 119.4/2 = 59.7

872.65
872.243
872.403
4.0%
1.5%
4:1
EXIST. R.O.W.
PROP. R.O.W.
REM. OF UNSUIT. (10") = 41.1/2 = 20.6
CUT (sqft) = 3.7/2 + 0.1/2 = 1.9
FILL (sqft) = 56.1/2 = 28.1

872.65
872.257
872.417
4.0%
1.5%
4:1
EXIST. R.O.W.
PROP. R.O.W.
871.214
REM. OF UNSUIT. (10") = 20.2/2 = 10.1
CUT (sqft) = 2.9/2 +1.3/4 = 2.1
FILL (sqft) = 18.0/2 = 9.0

31 + 50.00

FILE NAME = P115308-sht-xssht-MerS.dgn	USER NAME = abreuah	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

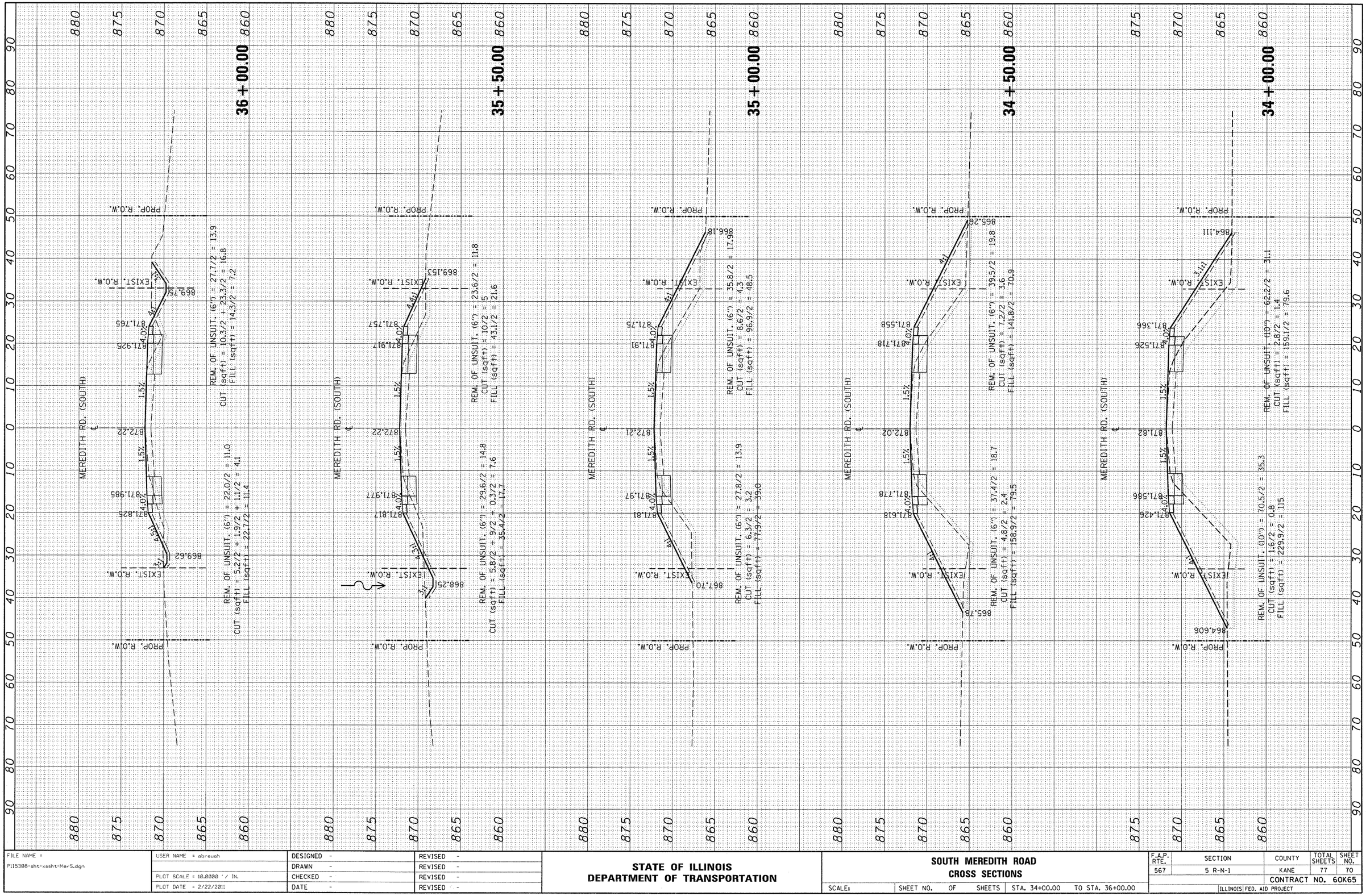
SOUTH MEREDITH ROAD
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 31+50.00 | TO STA. 33+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	69
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		



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	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

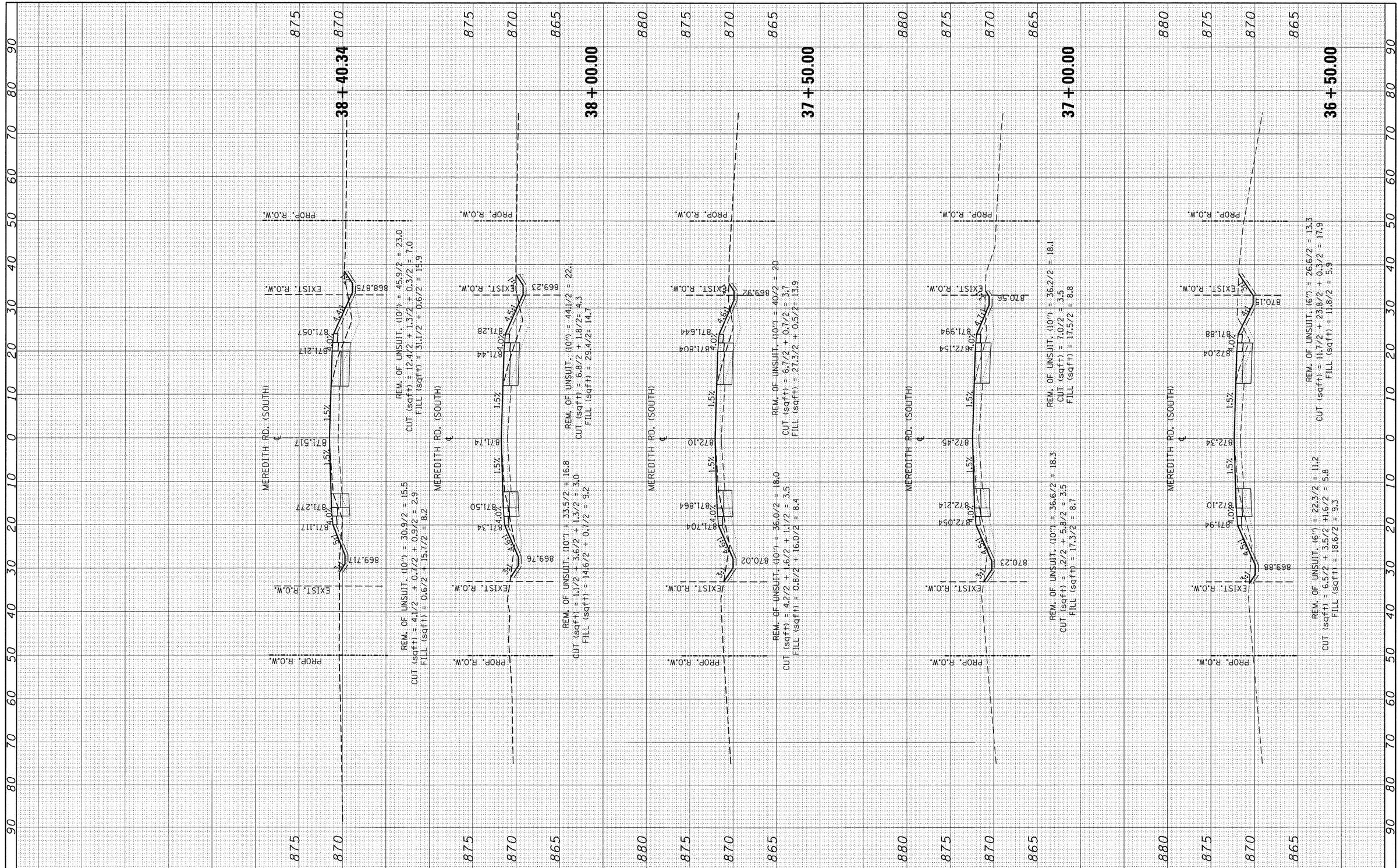
SOUTH MEREDITH ROAD
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 34+00.00 TO STA. 36+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	70
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TOPOGRAPHY		
NO.	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TOPOGRAPHY		
NO.	AREAS CHECKED		



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	PLOT SCALE = 10.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

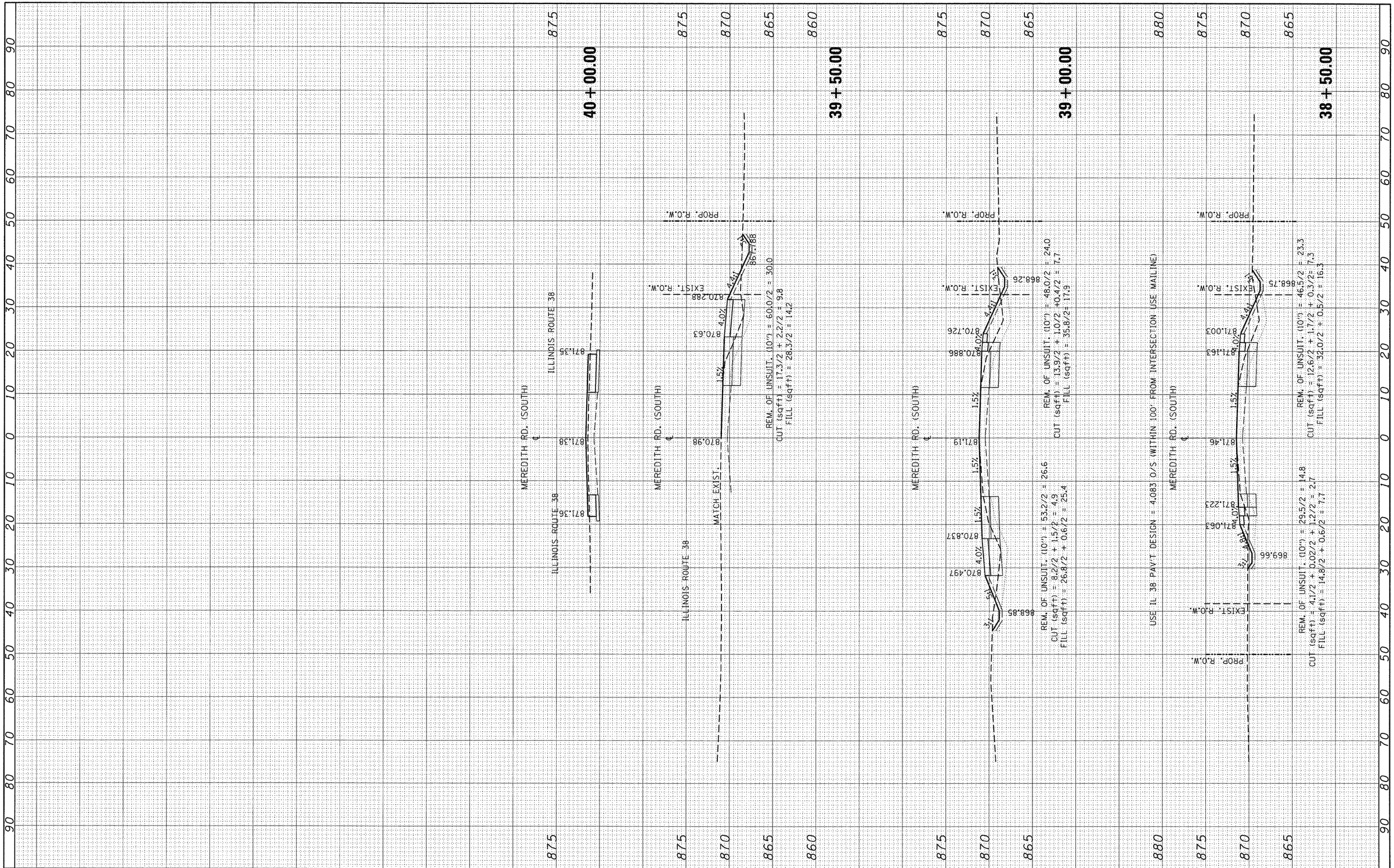
SOUTH MEREDITH ROAD
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 36+50.00 | TO STA. 38+40.34

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	71
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
NO.	PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		



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	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

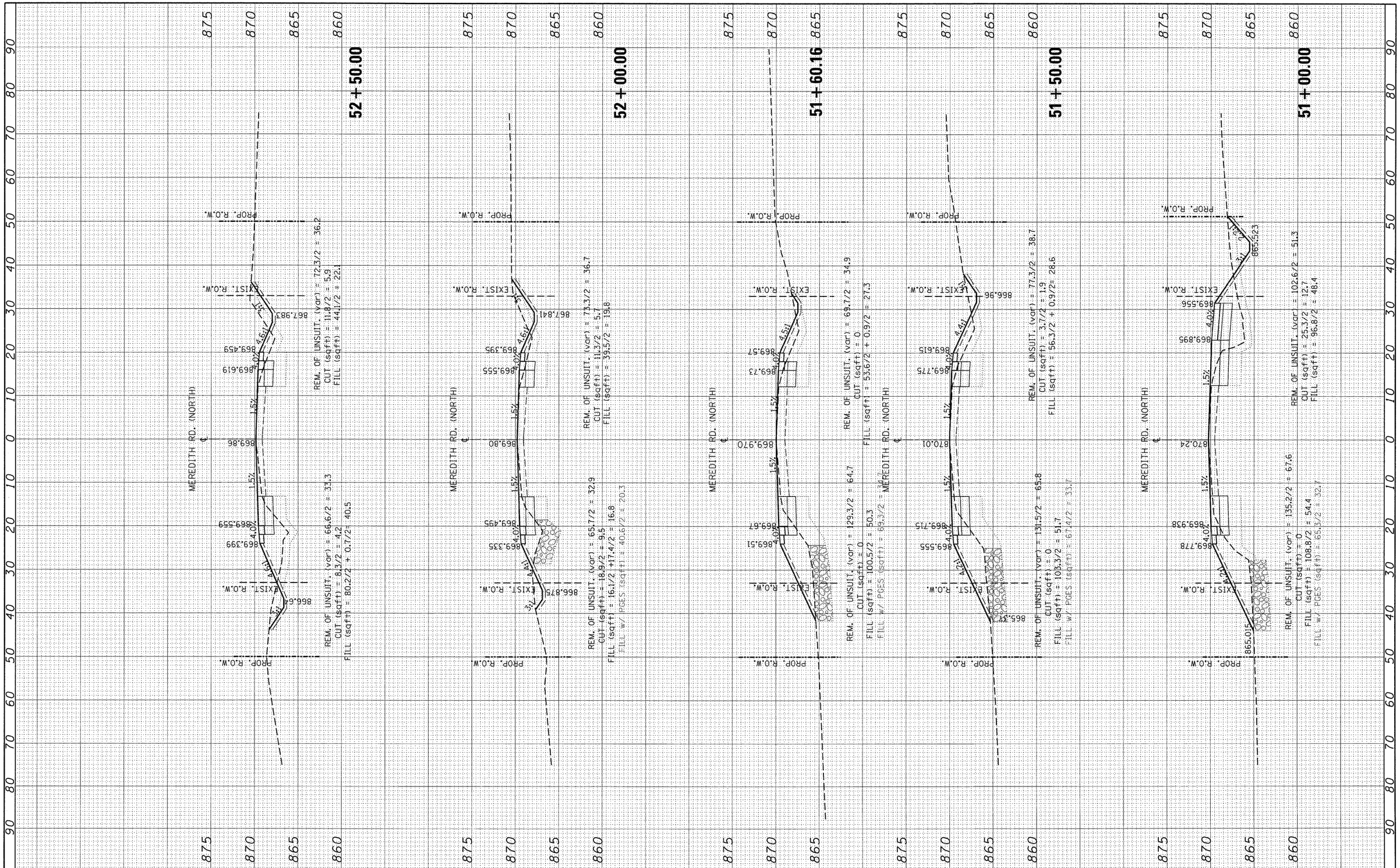
SOUTH MEREDITH ROAD
CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 38+50.00 TO STA. 40+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5 R-N-1	KANE	77	72
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

FINAL SURVEY	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		



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	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH MERIDETH ROAD CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 51+00.00 TO STA. 52+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5R-N-1	KANE	77	73
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

FILE NAME = P115380-sht-xssht-MerRdSgn

USER NAME = abraun

PLOT SCALE = 10.0000 ' / IN.

PLOT DATE = 2/22/2021

DESIGNED	-	REVISED -
DRAWN	-	REVISED -
CHECKED	-	REVISED -
DATE	-	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH MERIDETH ROAD CROSS SECTIONS

SCALE:

SHEET NO. OF SHEETS STA. 53+00.00 TO STA. 55+00.00

F.A.P. RTE. 567

SECTION 5R-N-1

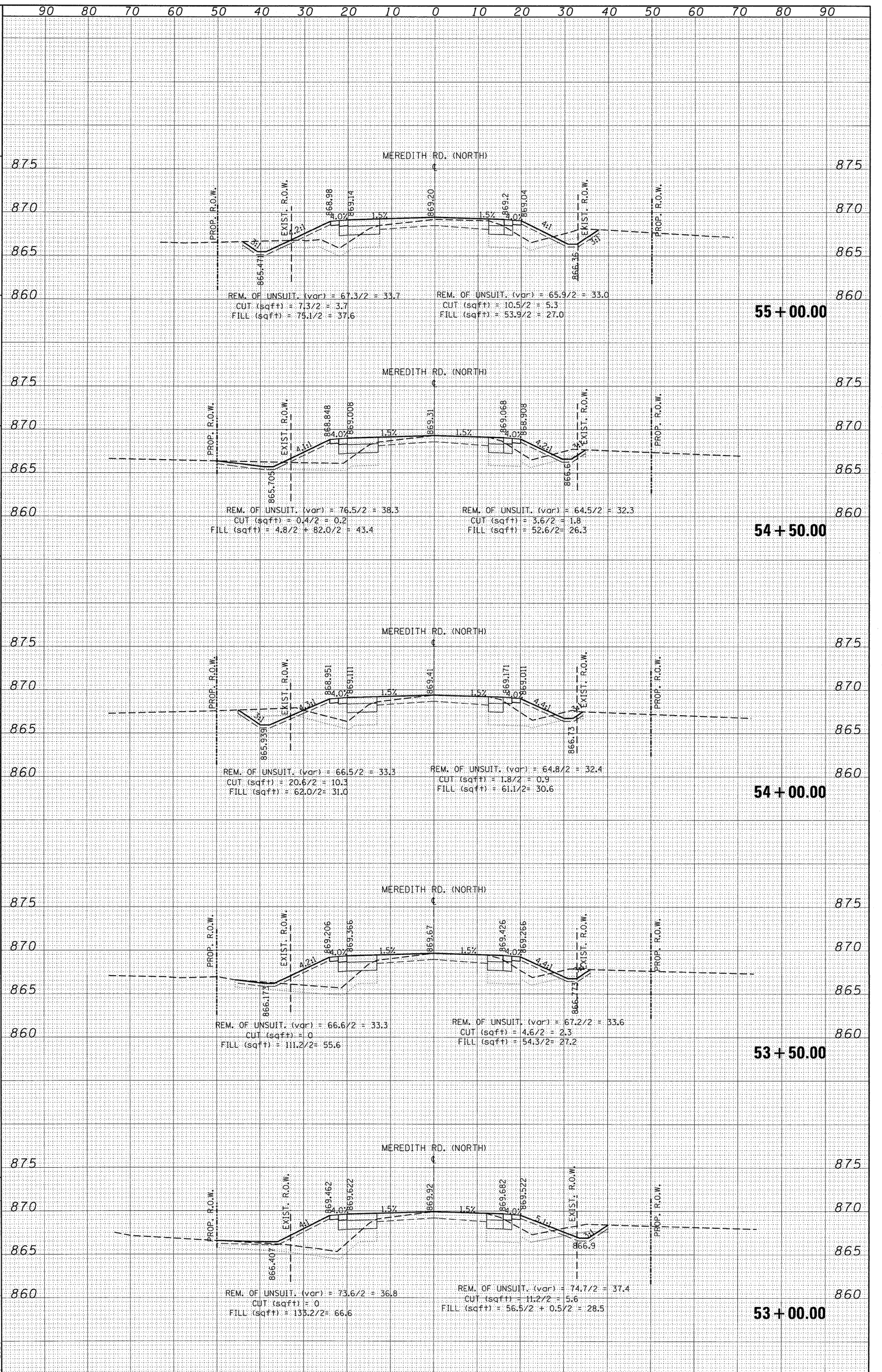
COUNTY KANE

TOTAL SHEETS 77

SHEET NO. 74

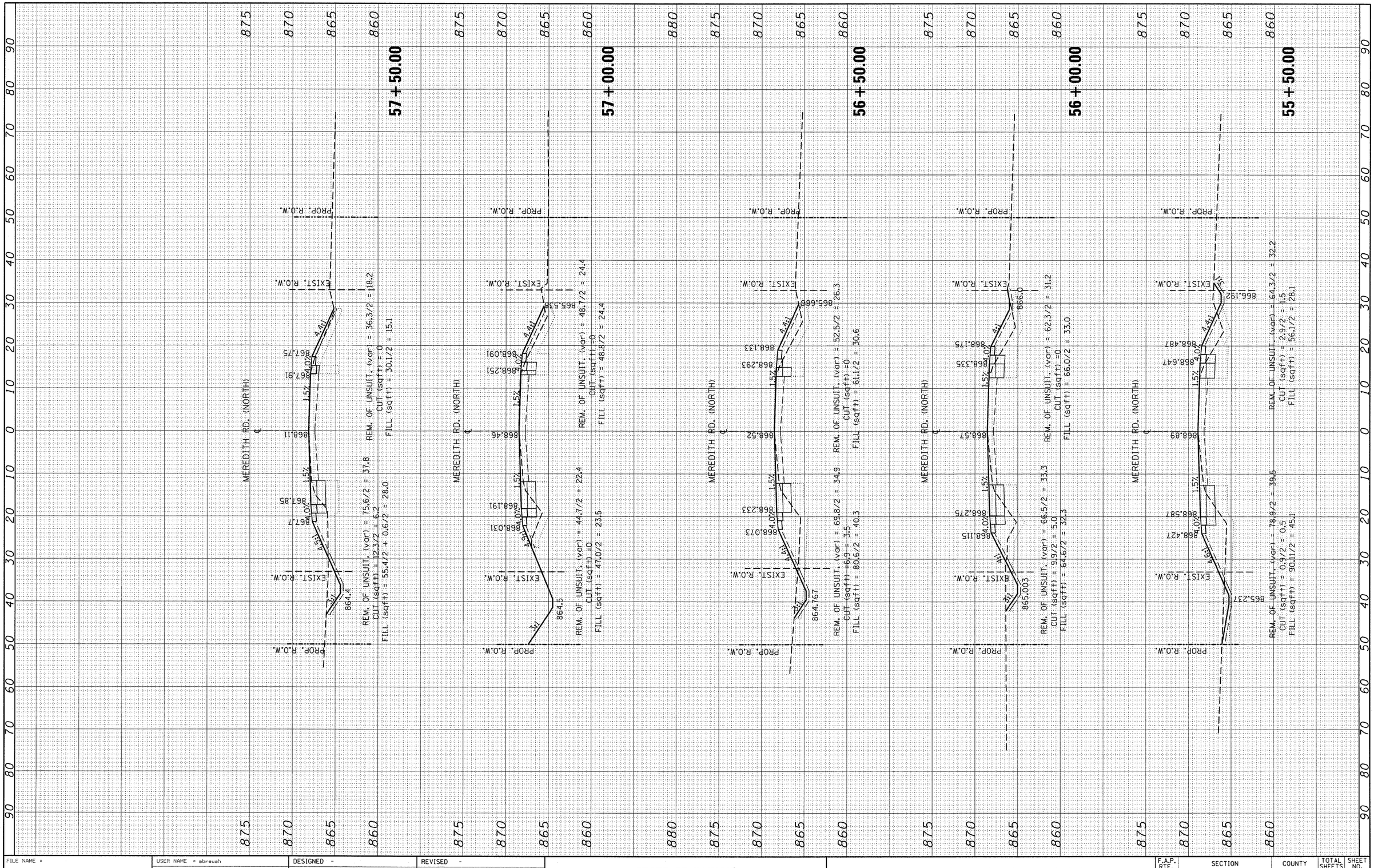
ILLINOIS FED. AID PROJECT

CONTRACT NO. 60K65



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TOPSOIL SITE		
	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TOPSOIL SITE		
	AREAS		
	AREAS CHECKED		



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	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH MERIDETH ROAD CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 55+50.00 TO STA. 57+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5R-N-1	KANE	77	75
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			

ORIGINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

FINAL SURVEY		BY	DATE
NOTE BOOK	SURVEYED		
	PLOTTED		
	TEMPLATE		
NO.	AREAS		
	AREAS CHECKED		

FILE NAME = P115308-sht-xsshts-MerRdSgn

USER NAME = ebrauch

PLOT SCALE = 10.0000 ' / IN.

PLOT DATE = 2/22/2021

DESIGNED	-	REVISED -
DRAWN	-	REVISED -
CHECKED	-	REVISED -
DATE	-	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

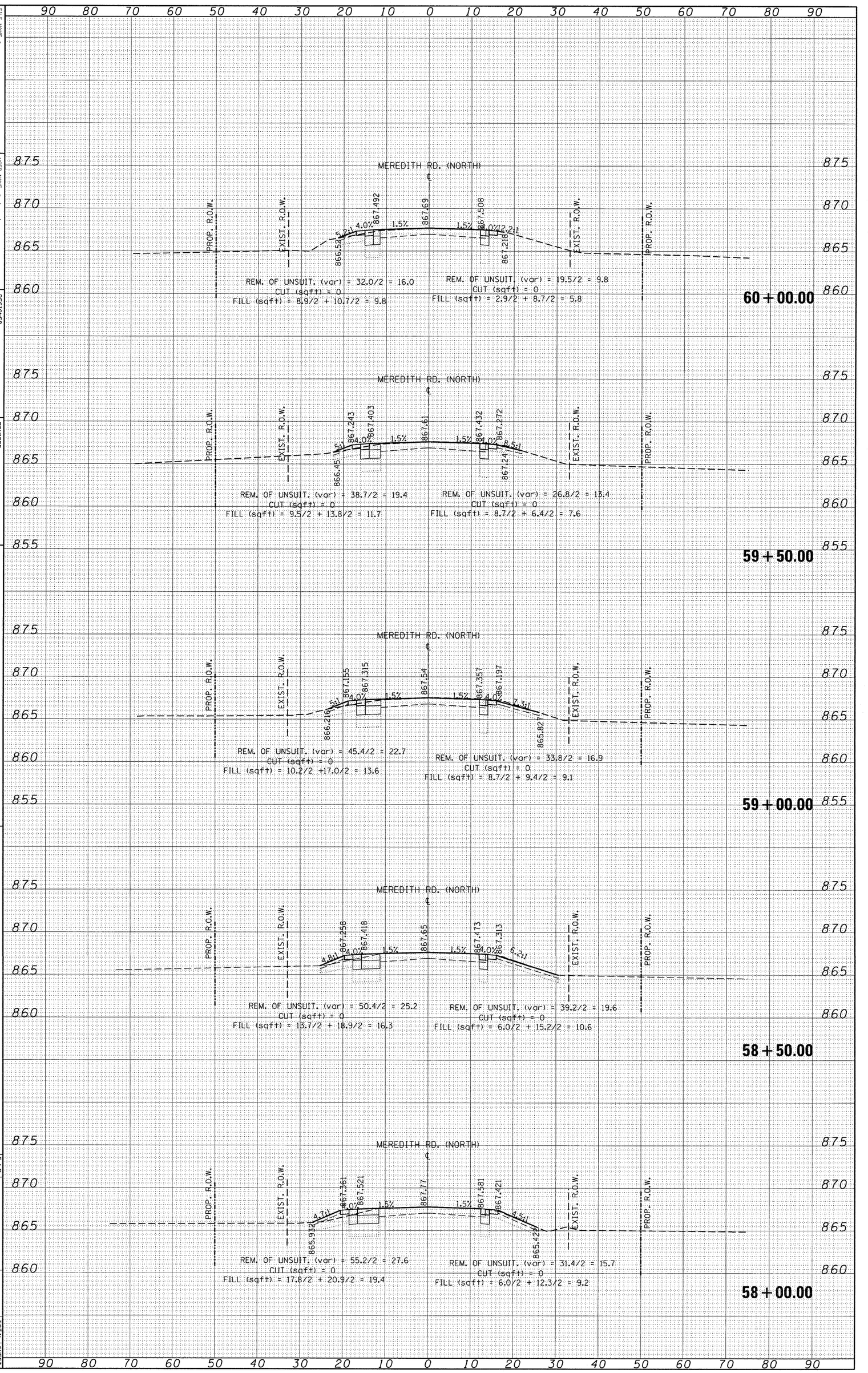
NORTH MERIDETH ROAD CROSS SECTIONS

SCALE:

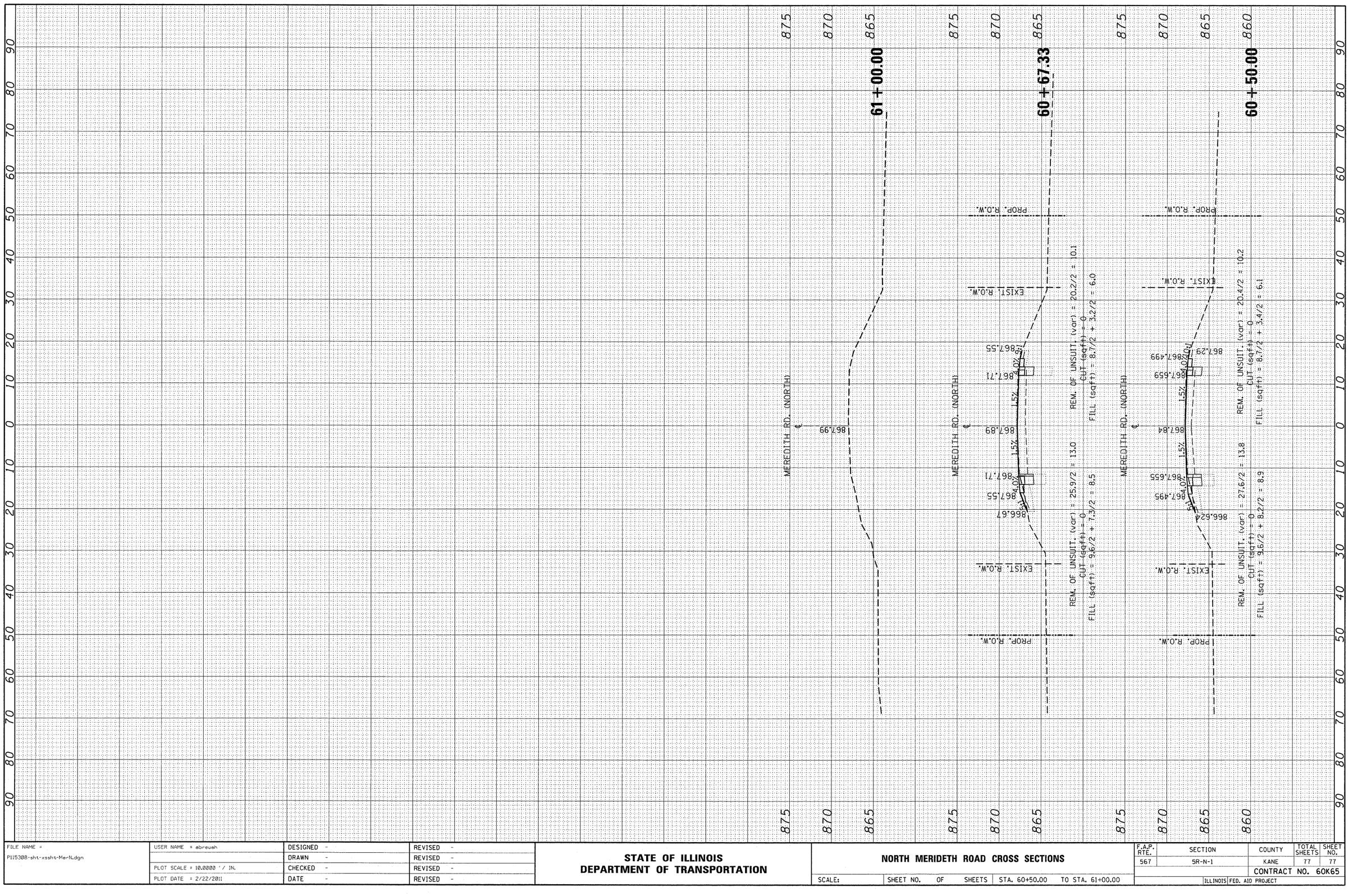
SHEET NO. OF SHEETS STA. 58+00.00 TO STA. 60+00.00

F.A.P. RTE. 567 | SECTION 5R-N-1 | COUNTY KANE | TOTAL SHEETS 77 | SHEET NO. 76

ILLINOIS FED. AID PROJECT | CONTRACT NO. 60K65



FINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		



ORIGINAL SURVEY		BY	DATE
	SURVEYED		
	PLOTTED		
NOTE BOOK	TEMPLATE		
	AREAS		
NO.	AREAS CHECKED		

FILE NAME = P115308-sht-xssht-MerN.dgn	USER NAME = abreueh	DESIGNED -	REVISED -
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	PLOT DATE = 2/22/2011	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH MERIDETH ROAD CROSS SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. 60+50.00 TO STA. 61+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
567	5R-N-1	KANE	77	77
		CONTRACT NO. 60K65		
	ILLINOIS FED. AID PROJECT			