

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44 R-N	COOK/WILL	68	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60K66		

*68+2 = 70

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE
VILLAGE OF ORLAND PARK

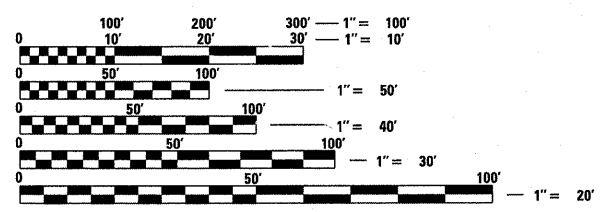
PROPOSED HIGHWAY PLANS

FAU 2688: WOLF ROAD
AT 183RD STREET/ORLAND PARKWAY
SECTION: 44 R-N

INTERSECTION IMPROVEMENT AND
NEW TRAFFIC SIGNAL INSTALLATION

PROJECT NO.: HSIP-2688(002)

COUNTY: COOK & WILL
C-91-543-10

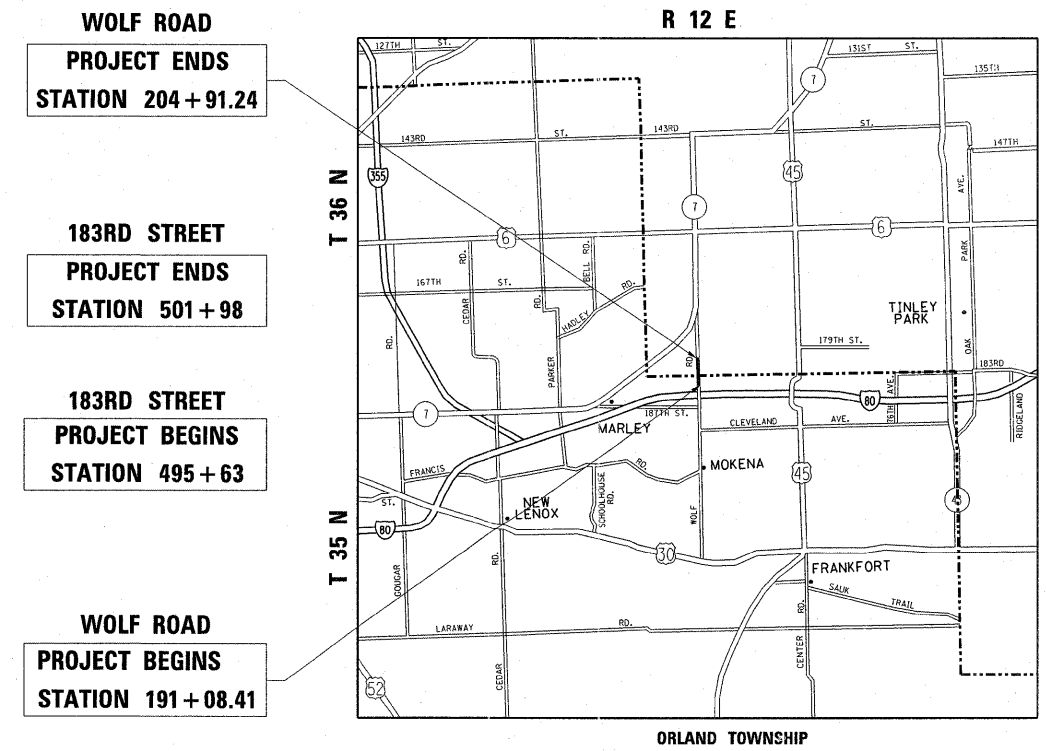


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 (OR 811)

PROJECT ENGINEER: JENPAI P. CHANG (847) 705 - 4432
PROJECT MANAGER: KEN ENG

CONTRACT NO. 60K66



WOLF ROAD
PROJECT ENDS
STATION 204 + 91.24

183RD STREET
PROJECT ENDS
STATION 501 + 98

183RD STREET
PROJECT BEGINS
STATION 495 + 63

WOLF ROAD
PROJECT BEGINS
STATION 191 + 08.41

WOLF ROAD: GROSS AND NET LENGTH OF PROJECT = 1382.83 FEET = 0.26 MILE
183RD STREET: GROSS AND NET LENGTH OF PROJECT = 635 FEET = 0.12 MILE

TRAFFIC DATA:

WOLF ROAD:
2008 ADT = 15,600
SPEED LIMIT = 45 MPH

183RD STREET:
2008 ADT = 1,600
SPEED LIMIT = 20 MPH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED MARCH 24, 2011

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

May 13 2011
Scott E. Stitt P.E. J.E.
ENGINEER OF DESIGN AND ENVIRONMENT

May 13 2011
Christine M. Reed J.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS**

LIST OF STATE STANDARDS:

INDEX OF SHEETS

STANDARD NO. DESCRIPTION

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280001-05	TEMPORARY EROSION CONTROL SYSTEM
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-03	HMA SHLD, STRIPS / SHLDS, WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602001-02	CATCH BASIN TYPE A
602401-03	MANHOLE TYPE A
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604091-02	FRAME AND GRATE TYPE 24
482011-03	CONCRETE CURB TYPE B AND COMBINATION CONCRTE CURB AND GUTTER
606006-02	OUTLETS FOR CONCRETE CURB AND GUTTER TYPE B-15.60 (B-6.24)
701006-03	OFF-RD OPERATIONS, 2L, 2W, 4.5 m (15') TO 600 mm (24") FROM PAVEMENT EDGE
701011-02	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEED > 45 MPH
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS > 45 MPH
701336-06	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEED > 45 MPH
701502-04	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h), WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER, WITH THE TYPE SPECIFIED ON THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAMES AND LIDS UNLESS OTHERWISE SPECIFIED IN THE PLANS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE PROJECT LIMITS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT

SIDEWALK REMOVAL AND P.C.C. SIDEWALK, 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKING ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT LAWRENCE HILL, AREA TRAFFIC FIELD ENGINEER (OR TECHNICIAN), AT (815) 485-6475 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

" POROUS GRANULAR EMBANKMENT, SUBGRADE (P.G.E.S.) HAS BEEN PROVIDED FOR USE FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIAL UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS AND GUIDLINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH PGES OR EMBANKMENT AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR".

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT 800-892-0123 (OR 811) FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF ORLAND PARK.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WOLF ROAD @ 183RD STREET				F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\galbanjr\d0150417\PI4309-Design.dgn		DRAWN -	REVISED -		INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES				2688	44 R-N	COOK/WILL	68	2
		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.				CONTRACT NO. 60K66				
		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	90% FED. 10% STATE	90% FED. 10% STATE	FED (90%) STATE (5%) VILLAGE (5%) COOK CO.	ORLAND FPD 100% COOK CO.	UNIT	CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	90% FED. 10% STATE	90% FED. 10% STATE	FED (90%) STATE (5%) VILLAGE (5%) COOK CO.	ORLAND FPD 100% COOK CO.
				ROADWAY 0004	ROADWAY 0004	TRAFFIC SIGNAL 0021							ROADWAY 0004	ROADWAY 0004	TRAFFIC SIGNAL 0021	
87702718	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 46 FT. AND 30 FT. AND	EACH	1			1			40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1098	463	635		
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	32		32				42001300	PROTECTIVE COAT	SO YD	767	442	325		
20101200	TREE ROOT PRUNING	EACH	8		8				42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1774	1276	498		
20200100	EARTH EXCAVATION	CU YD	3794	890	2904				42400800	DETECTABLE WARNINGS	SO FT	154	60	94		
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	454	202	252				44000100	PAVEMENT REMOVAL	SO YD	3478	698	2780		
20800150	TRENCH BACKFILL	CU YD	38	20.2	17.8				44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	6857	3496	3361		
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SO YD	450	350	100				44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	104		104		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	4295	1650	2645				44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1692	758	934		
21400100	GRADING AND SHAPING DITCHES	FOOT	2341	833	1508				44002208	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2"	SO YD	345	176	169		
25000210	SEEDING, CLASS 2A	ACRE	0.5		0.5				44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	91	40	51		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	69	21	48				44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	104	53	51		
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	69	21	48				44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	240	120	120		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	69	21	48				44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	3150	1850	1300		
25100630	EROSION CONTROL BLANKET	SO YD	4295	1650	2645				48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SO YD	1471	415	1056		
25200110	SODDING, SALT TOLERANT	SO YD	1875	1650	225				54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	4	2	2		
25200200	SUPPLEMENTAL WATERING	UNIT	44	17	27				54248510	CONCRETE COLLAR	CU YD	0.5	0.5			
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	90	35	55				550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	122.12	69.22	52.9		
28000305	TEMPORARY DITCH CHECKS	FOOT	209	30	179				550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	11.81	11.81			
28000400	PERIMETER EROSION BARRIER	FOOT	1385	200	1185				550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	172.97	75.06	97.91		
28000500	INLET AND PIPE PROTECTION	EACH	9	6	3				550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	80.2	80.2			
28100105	STONE RIPRAP, CLASS A3	SO YD	375	141	234				55100300	STORM SEWER REMOVAL 8"	FOOT	59	59			
28200200	FILTER FABRIC	SO YD	375	141	234				55100700	STORM SEWER REMOVAL 15"	FOOT	167	89	78		
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	60		60				55100900	STORM SEWER REMOVAL 18"	FOOT	6	6			
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	26		26				60107600	PIPE UNDERDRAINS 4"	FOOT	160		160		
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	6	3	3				60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1	1			
40600300	AGGREGATE (PRIME COAT)	TON	28	14	14				60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	4	2	2		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	12	6	6				60219000	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1	1			
40600895	CONSTRUCTING TEST STRIP	EACH	2	1	1				60250200	CATCH BASINS TO BE ADJUSTED	EACH	2		2		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	72	31	41				60500050	REMOVING CATCH BASINS	EACH	4	3	1		
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	52	26	26				60600095	CLASS SI CONCRETE (OUTLET)	CU YD	4	4			
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	1570	303	1267				60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	157		157		
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	62		62				60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1562	858	704		

■ SPECIALTY ITEMS

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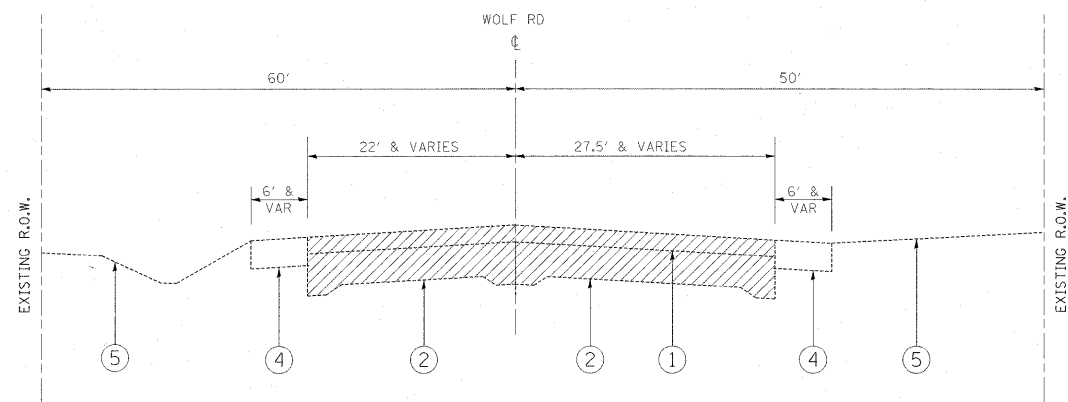
SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE							SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	90% FED. 10% STATE	90% FED. 10% STATE	FED (90%) STATE (5%) VILLAGE (5%) COOK CO.	ORLAND FPD 100% COOK CO.			CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	90% FED. 10% STATE	90% FED. 10% STATE	FED (90%) STATE (5%) VILLAGE (5%) COOK CO.	ORLAND FPD 100% COOK CO.		
				COOK COUNTY ROADWAY 0004	WILL COUNTY ROADWAY 0004	TRAFFIC SIGNAL 0021			COOK COUNTY ROADWAY 0004					WILL COUNTY ROADWAY 0004	TRAFFIC SIGNAL 0021			COOK COUNTY ROADWAY 0004	WILL COUNTY ROADWAY 0004
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	2	4					81400200	HEAVY-DUTY HANDHOLE	EACH	6			6			
67100100	MOBILIZATION	L SUM	1	0.25	0.75					81400300	DOUBLE HANDHOLE	EACH	1			1			
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	5	2	3					81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1044			1044			
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	1	0.25	0.75					85700200	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	EACH	1			1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2011	809	1202					87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1545			1545			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	364	218.4	145.6					87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2015			1620	395		
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	7069	2317	4752					87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1975			1975			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2046	968	1078					87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	920			920			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	90	38	52					87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1795			1795			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	147	77	70					87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	65			65			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	13465	5949	7516					87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	2			2			
70300570	PAVEMENT MARKING TAPE, TYPE III 24"	FOOT	173	97	76					87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	1			1			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	9624	4104	5520					87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2			2			
72000100	SIGN PANEL - TYPE 1	SO FT	25.5			25.5				87700230	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.	EACH	1			1			
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	364	218.4	145.6					87700260	STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.	EACH	1			1			
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	7069	2317	4752					87700280	STEEL MAST ARM ASSEMBLY AND POLE, 48 FT.	EACH	1			1			
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2046	968	1078					87800100	CONCRETE FOUNDATION, TYPE A	FOOT	20			20			
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	90	38	52					87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4			4			
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	147	77	70					87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	51			51			
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	121	53	68					88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	6			6			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	107	47	60					88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	2			2			
81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	562			562				88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2			2			
81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	46			46				88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2			2			
81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	129			129				88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8			8			
81001000	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	FOOT	94			94				88200210	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM	EACH	8			8			
81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	183			183				88500100	INDUCTIVE LOOP DETECTOR	EACH	8			8			
81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	289			289													
81400100	HANDHOLE	EACH	3			3													

▪ SPECIALTY ITEMS

▪ SPECIALTY ITEMS

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE							SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	URBAN TOTAL QUANTITIES	90% FED. 10% STATE COOK COUNTY ROADWAY 0004	90% FED. 10% STATE WILL COUNTY ROADWAY 0004	FED 100% STATE (5%) VILLAGE (5%) COOK CO. TRAFFIC SIGNAL 0021	ORLAND FPD 100% COOK CO. 0021			CODE NO	ITEM	UNIT	TOTAL QUANTITIES						
88600100	DETECTOR LOOP, TYPE I	FOOT	1249			1249													
88700200	LIGHT DETECTOR	EACH	2				2												
88700300	LIGHT DETECTOR AMPLIFIER	EACH	1				1												
88800100	PEDESTRIAN PUSH-BUTTON	EACH	8			8													
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	1		1														
X5539700	STORM SEWERS TO BE CLEANED	FOOT	92	56	36														
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	0.25	0.75														
80500020	SERVICE INSTALLATION - POLE MOUNTED	EACH	1			1													
86200120	UNINTERRUPTIBLE POWER SUPPLY	EACH	1			1													
87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1775			1775													
X8730250	ELECTRIC CABLE IN CONDUIT NO. 20 3/C, TWISTED, SHIELDED	FOOT	395				395												
Z0001050	AGGREGATE SUBGRADE 12"	SO YD	4125	927	3198														
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.5	0.5														
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	2		2														
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	102.8	51.4	51.4														
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	155	120	35														
Z0062456	TEMPORARY PAVEMENT	SO YD	1801	698	1103														
Z0064800	SELECTIVE CLEARING	UNIT	3		3														

▲ NON-PARTICIPATING ITEMS - 100% STATE
 ■ SPECIALTY ITEMS

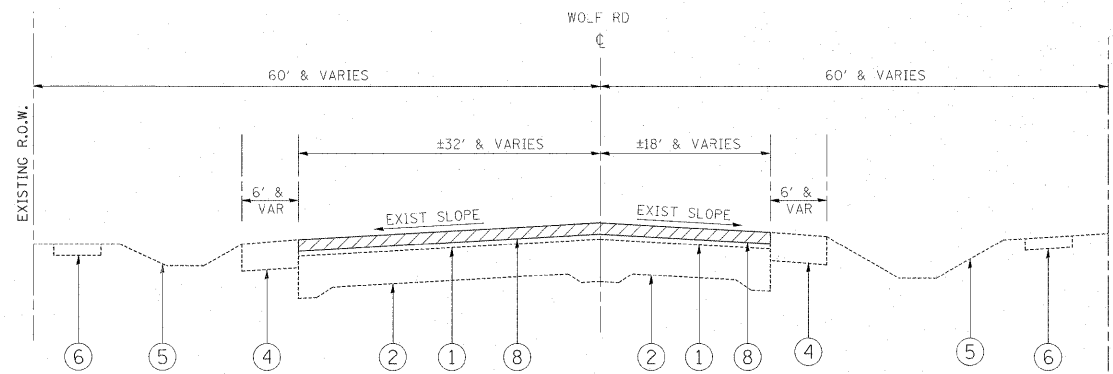


**WOLF ROAD
EXISTING TYPICAL SECTION (RECONSTRUCTION)
STA. 195+41.79 TO STA. 198+76.88**

EXISTING AGGREGATE SHOULDER:
STA. 195+41.79 TO STA. 198+76.88

EXISTING AGGREGATE SHOULDER:
STA. 195+41.79 TO STA. 197+43

EXISTING CONC C&G, B-6.12
STA. 197+43 TO STA. 198+76.88



**WOLF ROAD
EXISTING TYPICAL SECTION
WIDENING AND RESURFACING
STA. 191+08.41 TO STA. 195+41.79
STA. 198+76.88 TO STA. 204+91.24**

EXISTING AGGREGATE SHOULDER:
STA. 195+41.79 TO STA. 199+53
STA. 202+04 TO STA. 204+91.24

EXISTING CONC C & G, B-6.12
STA. 199+53 TO STA. 202+04

EXISTING AGGREGATE SHOULDER:
STA. 195+41.79 TO STA. 197+43
STA. 200+50 TO STA. 204+91.24

EXISTING CONC C&G, B-6.12
STA. 197+43 TO STA. 200+50

LEGEND:

- ① EXISTING HMA SURFACING, ±3-1/4" & VARIES
- ② EXISTING PCC PAVEMENT, 7"-9"-7"
- ③ EXISTING CONC C&G, B-6.12
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING GROUND LINE
- ⑥ EXISTING PCC SIDEWALK, 5"
- ⑦ EXISTING AGGREGATE SUBGRADE
- ⑧ PROPOSED HMA SURFACE REMOVAL, 2"
- ⑨ PROPOSED PAVEMENT REMOVAL
- ⑩ PROPOSED HMA SURFACE COURSE MIX "D", N70, 2"
- ⑪ PROPOSED HMA BINDER COURSE IL-19, N70, 10-1/4" (3 LIFTS)
- ⑫ PROPOSED AGGREGATE SUBGRADE, 12"
- ⑬ PROPOSED COMBINATION CONCRTE CURB & GUTTER, TYPE B-6.24
- ⑭ PROPOSED HMA SHOULDER, 8"
- ⑮ PROPOSED TOPSOIL FURNISH AND PLACE, 4"
- ⑯ PROPOSED SEEDING CLASS 2A

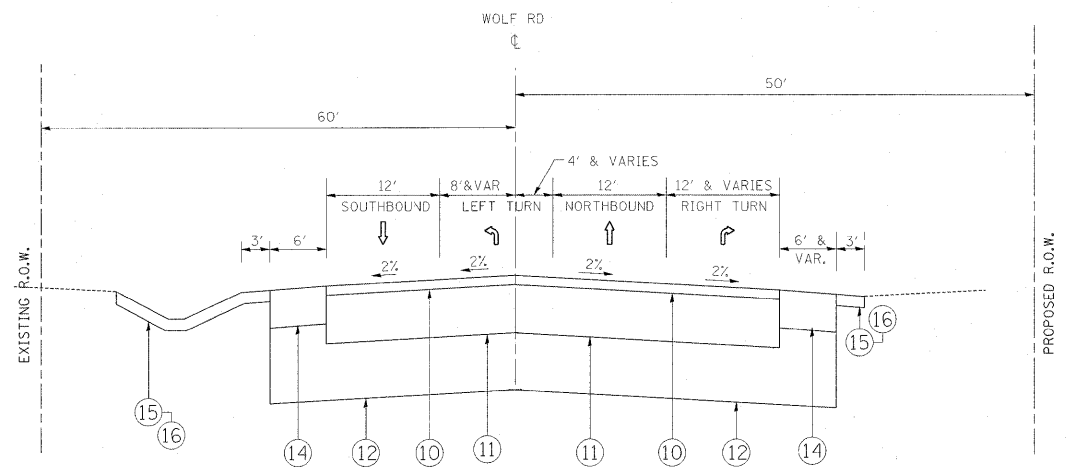
NOTE:

ADDITIONAL AGGREGATE SUBGRADE UNDER PROPOSED CURB AND GUTTER SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF AGGREGATE SUBGRADE, 12".

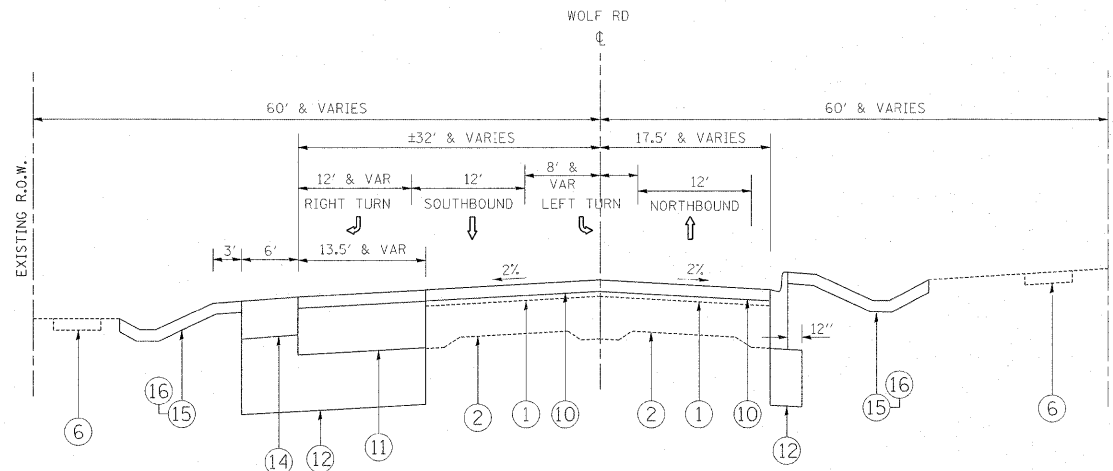
"THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING".

TEMPORARY PAVEMENT NOTE:

IF THE CONTRACTOR CHOOSES TO USE CONCRETE THE THICKNESS SHALL BE 10".



**WOLF ROAD
PROPOSED TYPICAL SECTION (RECONSTRUCTION)
STA. 195+41.79 TO STA. 198+76.88**



**WOLF ROAD
PROPOSED TYPICAL SECTION
WIDENING AND RESURFACING
STA. 191+08.41 TO STA. 195+41.79
STA. 198+76.88 TO STA. 204+91.24**

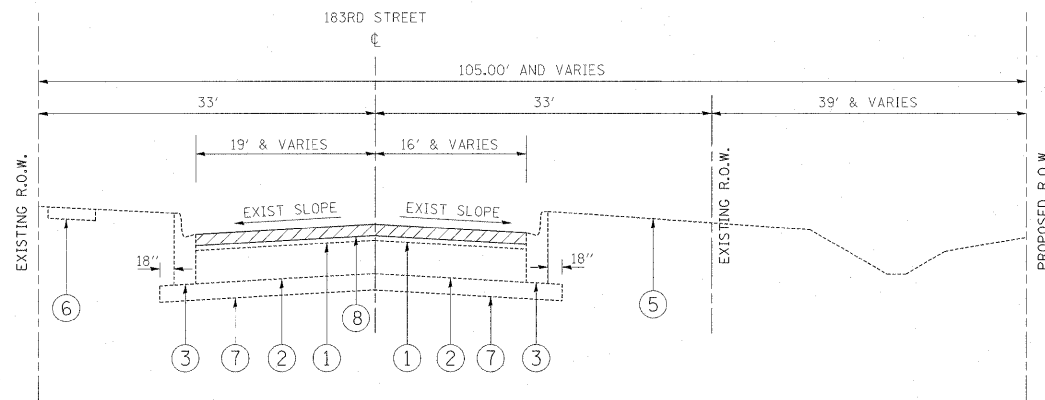
PROPOSED HMA SHOULDER:
STA. 191+08.41 TO STA. 198+71.33
STA. 202+71.24 TO STA. 204+91.24

PROPOSED COMB. CONC C&G, TYPE B-6.24
STA. 198+71.33 TO STA. 202+71.24

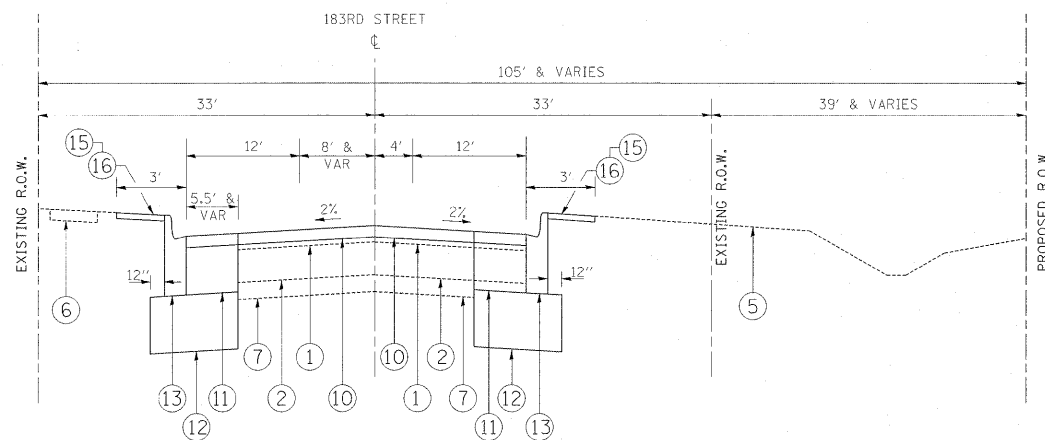
PROPOSED HMA SHOULDER:
STA. 191+08.41 TO STA. 199+29.15
STA. 200+84.74 TO STA. 204+91.24

PROPOSED COMB. CONC C&G, TYPE B-6.24
STA. 199+29.15 TO STA. 200+84.74

FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WOLF ROAD @ 183RD STREET EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.U. R/E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw\work\p\idot\galbanjr\d0150417\14309-Design.dgn		DRAWN -	REVISED -		2688	44 R-N	COOK/WILL	68	6			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -		CONTRACT NO. 60K66							
PLOT DATE = 3/26/2011		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							
				SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.				



183RD STREET
EXISTING TYPICAL SECTION
(WIDENING & RESURFACING)
STA. 495+98 TO STA. 501+98



183RD STREET
PROPOSED TYPICAL SECTION
(WIDENING & RESURFACING)
STA. 495+98 TO STA. 501+98

HOT-MIX ASPHALT MIXTURE REQUIREMENTS
THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT

MIXTURE TYPE	AIR VOIDS (%) @ NDES
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE MIX "D", N70, (IL-9.5 mm), 2"	4% @ 70 GYR.
WIDENING	
HOT-MIX ASPHALT SURFACE COURSE MIX "D", N70, (IL-9.5 mm), 2"	4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE IL-19.0, N70, 10-1/4"	4% @ 70 GYR.
SHOULDER	
HOT-MIX ASPHALT SHOULDER (HMA BINDER IL-19 mm), 8"	2% @ 30 GYR.
TEMPORARY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE MIX "D", N50 (IL 9.5mm), 1-1/2"	4% @ 50 GYR.
TEMPORARY PAVEMENT (HMA BINDER IL-19 mm), 8-1/2"	4% @ 50 GYR.
HOT-MIX ASPHALT PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm), 9"	4% @ 70 GYR.
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm), 2"	4% @ 70 GYR.
HMA DRIVEWAY (C.E.)	
HOT-MIX ASPHALT SURFACE COURSE MIX "C", N50, 2"	4% @ 50 GYR.
HOT-MIX-ASPHALT BASE COURSE, 8"	4% @ 50 GYR.

NOTE:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 POUND PER SQUARE YARD-INCH

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

LEGEND:

- ① EXISTING HMA SURFACING, ±3-1/4" & VARIES
- ② EXISTING PCC PAVEMENT, 7"-9"-7"
- ③ EXISTING CONC C&G, B-6.12
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING GROUND LINE
- ⑥ EXISTING PCC SIDEWALK, 5"
- ⑦ EXISTING AGGREGATE SUBGRADE
- ⑧ PROPOSED HMA SURFACE REMOVAL, 2"
- ⑨ PROPOSED PAVEMENT REMOVAL
- ⑩ PROPOSED HMA SURFACE COURSE MIX "D", N70, 2"
- ⑪ PROPOSED HMA BINDER COURSE IL-19, N70, 10-1/4" (3 LIFTS)
- ⑫ PROPOSED AGGREGATE SUBGRADE, 12"
- ⑬ PROPOSED COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
- ⑭ PROPOSED HMA SHOULDER, 8"
- ⑮ PROPOSED TOPSOIL FURNISH AND PLACE, 4"
- ⑯ PROPOSED SEEDING CLASS 2A

NOTE:

ADDITIONAL AGGREGATE SUBGRADE UNDER PROPOSED CURB AND GUTTER SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF AGGREGATE SUBGRADE, 12".

"THE CONTRACTOR SHALL PATCH FIRST BEFORE MILLING".

TEMPORARY PAVEMENT NOTE:

"IF THE CONTRACTOR CHOOSES TO USE CONCRETE THE THICKNESS SHALL BE 10".

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WOLF ROAD @ 183RD STREET EXISTING AND PROPOSED TYPICAL SECTIONS				F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 7
os:\pw_work\p\p\dot\galbenjr\d0150417\143009-Design.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	CONTRACT NO. 60K66			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT								
PLOT DATE = 3/26/2011		DATE -	REVISED -										

EARTHWORK SCHEDULE (WILL COUNTY)

① LOCATION	② EARTH EXCAVATION	③ EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	④ EMBANKMENT	⑤ EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STATION	CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD

WOLF ROAD

STA. 191+56.62 TO STA. 192+00	77.12	65.55	7.63	+57.92
STA. 192+00 TO STA. 193+00	163.89	139.31	22.22	+117.09
STA. 193+00 TO STA. 193+11.85	15.69	13.34	2.41	+10.93
STA. 193+11.85 TO STA. 194+00	99.58	84.64	9.79	+74.85
STA. 194+00 TO STA. 194+40.22	64.05	54.44	0.74	+53.60
STA. 194+40.22 TO STA. 195+00	105.17	89.40	1.11	+88.29
STA. 195+00 TO STA. 196+00	302.78	257.36	1.85	+255.75
STA. 196+00 TO STA. 197+00	665.74	565.85	0.93	+564.92
STA. 197+00 TO STA. 198+00	721.30	613.10	0.00	+613.10
STA. 198+00 TO STA. 199+00	428.70	364.40	5.56	+358.84
STA. 199+00 TO STA. 199+48.90	128.59	109.30	20.38	+88.92
STA. 199+48.90 TO STA. 199+61.68	39.76	33.80	6.27	+27.53
TOTAL	2742.37	2390.49	78.89	+1653.31

183RD STREET

STA. 496+00 TO STA. 496+24.94	0.69	0.59	0.00	+0.59
STA. 496+24.94 TO STA. 497+00	11.82	10.05	2.78	+7.27
STA. 497+00 TO STA. 498+00	34.26	29.12	6.48	+22.64
STA. 498+00 TO STA. 499+00	34.26	29.12	3.70	+25.42
STA. 499+00 TO STA. 500+00	12.96	11.02	0.93	+10.09
STA. 500+00 TO STA. 501+00	67.59	57.45	0.00	+57.45
TOTAL	161.58	137.35	13.89	+123.46

COLUMN 1: LOCATION FROM PLAN

COLUMN 2: QUANTITY OF EARTH EXCAVATION (CUT) FROM CROSS SECTIONS

COLUMN 3: QUANTITY OF EARTH EXCAVATION (CUT) ADJUSTED FOR A SHRINKAGE FACTOR OF 15%

COLUMN 4: QUANTITY OF EMBANKMENT (FILL) FROM CROSS SECTIONS

COLUMN 5: (COLUMN 3 - COLUMN 4)
(+) = QUANTITY OF FURNISHED EXCAVATION NEEDED
(-) = QUANTITY TO BE WASTED

EARTHWORK SCHEDULE (COOK COUNTY)

① LOCATION	② EARTH EXCAVATION	③ EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	④ EMBANKMENT	⑤ EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STATION	CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD

WOLF ROAD

STA. 200+50.24 TO STA. 201+00	94.91	80.67	23.04	+57.63
STA. 201+00 TO STA. 202+00	146.30	124.36	10.18	+114.18
STA. 202+00 TO STA. 203+00	141.67	120.42	12.04	+108.38
STA. 203+00 TO STA. 204+00	127.78	108.61	21.30	87.31
TOTAL	510.66	434.06	66.56	+367.50

183RD STREET

STA. 496+00 TO STA. 496+24.94	6.47	5.50	0.00	+5.50
STA. 496+24.94 TO STA. 497+00	27.80	23.63	1.00	+22.63
STA. 497+00 TO STA. 498+00	112.04	95.23	0.00	+95.23
STA. 498+00 TO STA. 499+00	140.74	119.63	0.00	+119.63
STA. 499+00 TO STA. 500+00	87.96	74.77	0.00	+74.77
STA. 500+00 TO STA. 501+00	16.67	14.17	0.00	+14.17
TOTAL	385.21	332.93	1.00	+331.93

COLUMN 1: LOCATION FROM PLAN

COLUMN 2: QUANTITY OF EARTH EXCAVATION (CUT) FROM CROSS SECTIONS

COLUMN 3: QUANTITY OF EARTH EXCAVATION (CUT) ADJUSTED FOR A SHRINKAGE FACTOR OF 15%

COLUMN 4: QUANTITY OF EMBANKMENT (FILL) FROM CROSS SECTIONS

COLUMN 5: (COLUMN 3 - COLUMN 4)
(+) = QUANTITY OF FURNISHED EXCAVATION NEEDED
(-) = QUANTITY TO BE WASTED

TREE REMOVAL SCHEDULE (WILL COUNTY)
183RD STREET

LOCATION STATION	OFFSTET (FEET)	DESCRIPTION			REASON FOR REMOVAL
		CIRCUMFERENCE (INCH)	6 TO 15 UNITS	OVER 15 UNITS	
497+35	39.31 (RT)	18	6		EMBANKMENT
498+02	30.00 (RT)	20	6.50		EMBANKMENT
498+26	30.03 (RT)	20	6.50		EMBANKMENT
498+26	30.03 (RT)	20	6.50		EMBANKMENT
498+26	30.03 (RT)	20	6.50		EMBANKMENT
TOTAL			32		

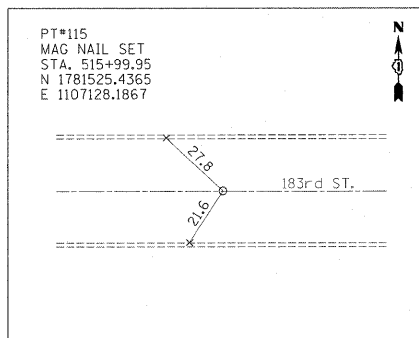
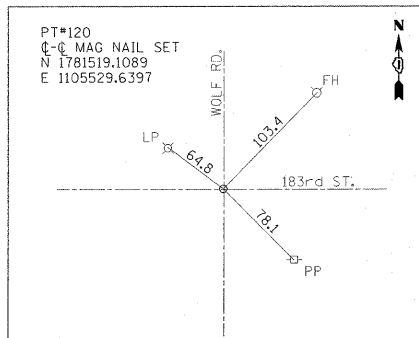
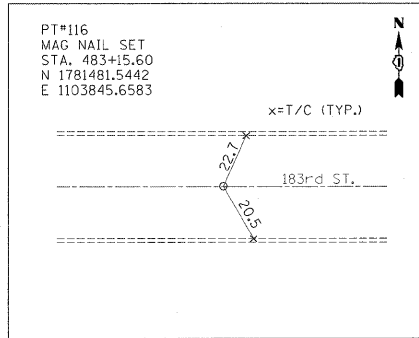
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WOLF ROAD @ IL 183RD STREET
QUANTITY SCHEDULES

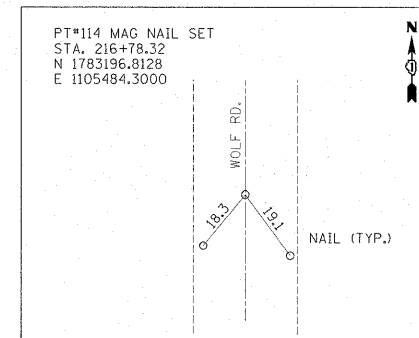
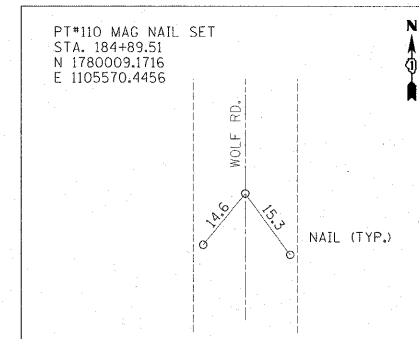
F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 8
CONTRACT NO. 60K66			ILLINOIS FED. AID PROJECT	

SCALE: SHEET NO. OF SHEETS STA. TO STA.



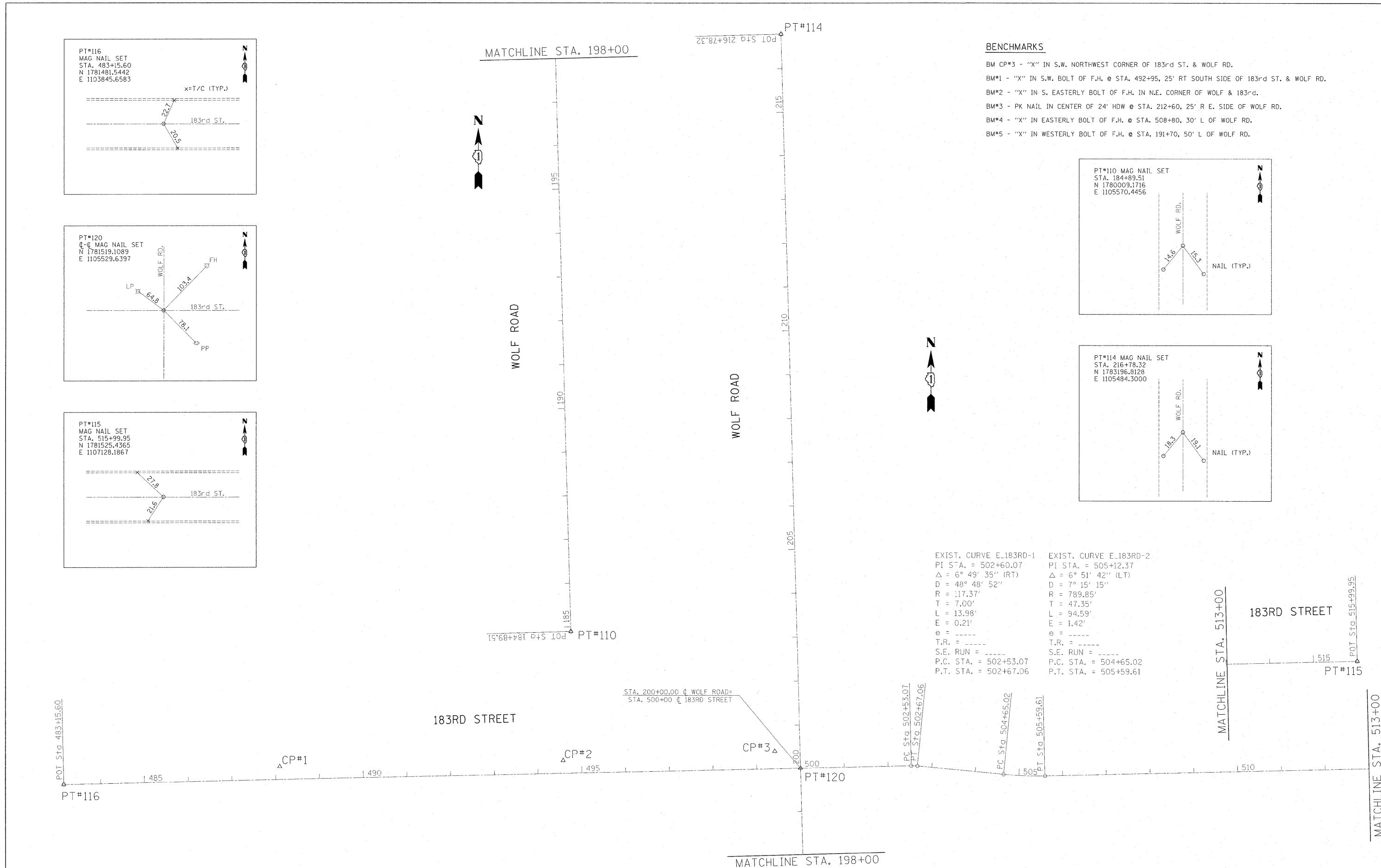
BENCHMARKS

- BM CP#3 - "X" IN S.W. NORTHWEST CORNER OF 183rd ST. & WOLF RD.
- BM#1 - "X" IN S.W. BOLT OF F.H. @ STA. 492+95, 25' RT SOUTH SIDE OF 183rd ST. & WOLF RD.
- BM#2 - "X" IN S. EASTERLY BOLT OF F.H. IN N.E. CORNER OF WOLF & 183rd.
- BM#3 - PK NAIL IN CENTER OF 24' HDW @ STA. 212+60, 25' R E. SIDE OF WOLF RD.
- BM#4 - "X" IN EASTERLY BOLT OF F.H. @ STA. 508+80, 30' L OF WOLF RD.
- BM#5 - "X" IN WESTERLY BOLT OF F.H. @ STA. 191+70, 50' L OF WOLF RD.



EXIST. CURVE E_183RD-1
PI STA. = 502+60.07
 Δ = 6° 49' 35" (RT)
D = 48° 48' 52"
R = 117.37'
T = 7.00'
L = 13.98'
E = 0.21'
e = -----
T.R. = -----
S.E. RUN = -----
P.C. STA. = 502+53.07
P.T. STA. = 502+67.06

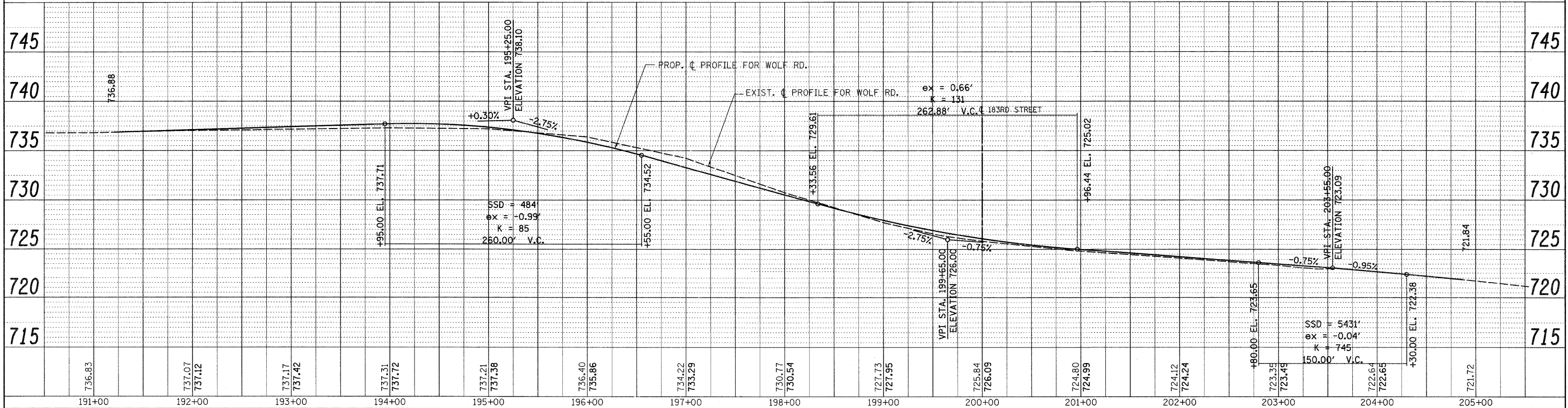
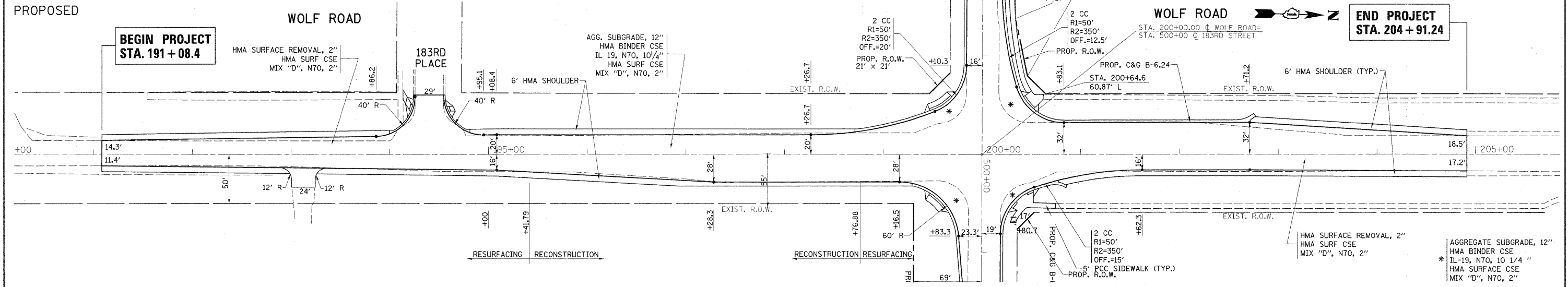
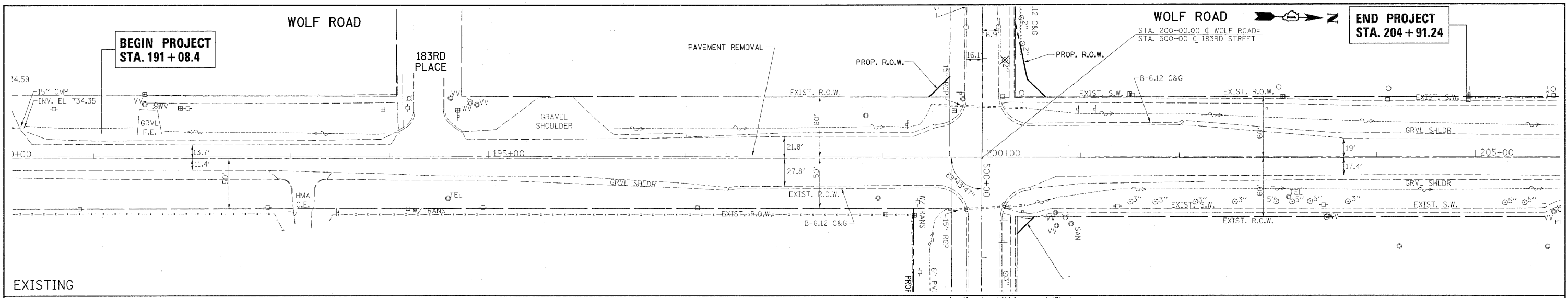
EXIST. CURVE E_183RD-2
PI STA. = 505+12.37
 Δ = 6° 51' 42" (LT)
D = 7° 15' 15"
R = 789.85'
T = 47.35'
L = 94.59'
E = 1.42'
e = -----
T.R. = -----
S.E. RUN = -----
P.C. STA. = 504+65.02
P.T. STA. = 505+59.61



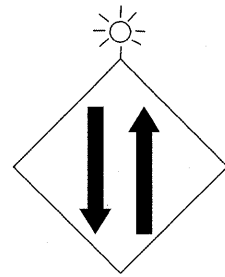
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	PLOT DATE = 3/8/2011	CHECKED -	REVISED -										CONTRACT NO. 60K66		
		DATE	REVISED -										ILLINOIS FED. AID PROJECT		

DATE	
BY	
SURVEYED	
ALIGNED	
CHECKED	
NOTE BOOK	
NO. OF WAY CHECKED	
NO. OF FILL NAME	
NO.	

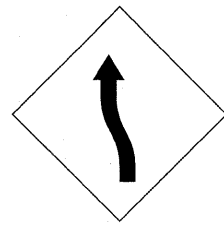
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BY	
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GRADES CHECKED	
NOTES CHECKED	
NO. OF STRUCTURE NOTATIONS CHD	
NO.	



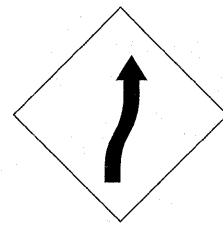
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PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -	HOR SCALE = 1" = 50'			VER SCALE = 1" = 5'	SHEET NO. OF SHEETS =	STA. 184+89.51 TO STA. 198+00.00	ILLINOIS FED. AID PROJECT			
PLOT DATE = 3/2/2011	DATE -	REVISED -										
CONTRACT NO. 60K66												



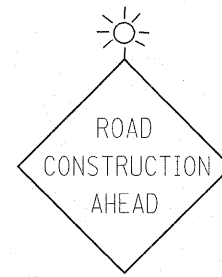
①
W 6-3 (0)
(48 X 48 in)



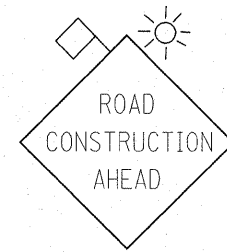
②
W1-4L
(48 X 48 in)



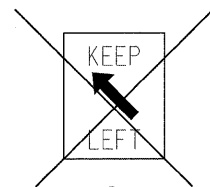
③
W1-4R
(48 X 48 in)



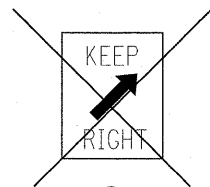
④
W20-1 (0)
(36 X 36 in)



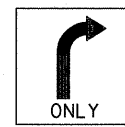
⑤
W20-1 (0)
(48 X 48 in)



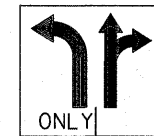
⑥
R3-5
(30 X 36 in)
RIGHT TURN ONLY



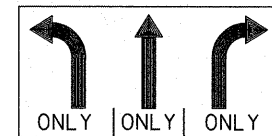
⑦
R3-5
(30 X 36 in)
RIGHT TURN ONLY



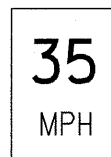
⑧
R3-5
(30 X 36 in)
RIGHT TURN ONLY



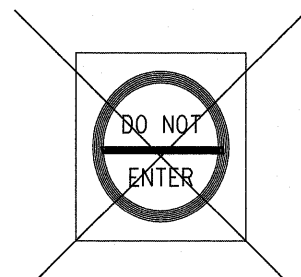
⑨
R3-8 (SPL)
(30 x 36 in)



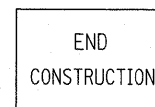
⑩
R3-8b
(30 x 48 in)
LEFT & RIGHT TURN ONLY



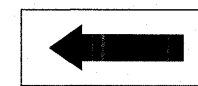
⑪
W13-1
(24 X 24 in)



⑫
W 6-3
(48 X 48 in)

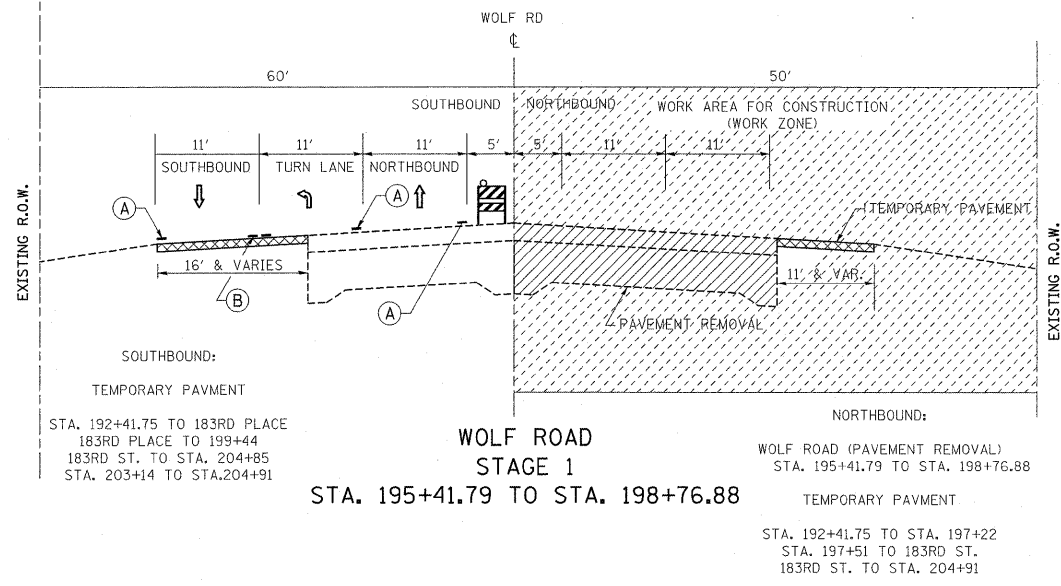


⑬
G 20-2a (0)
(60 X 24 in)



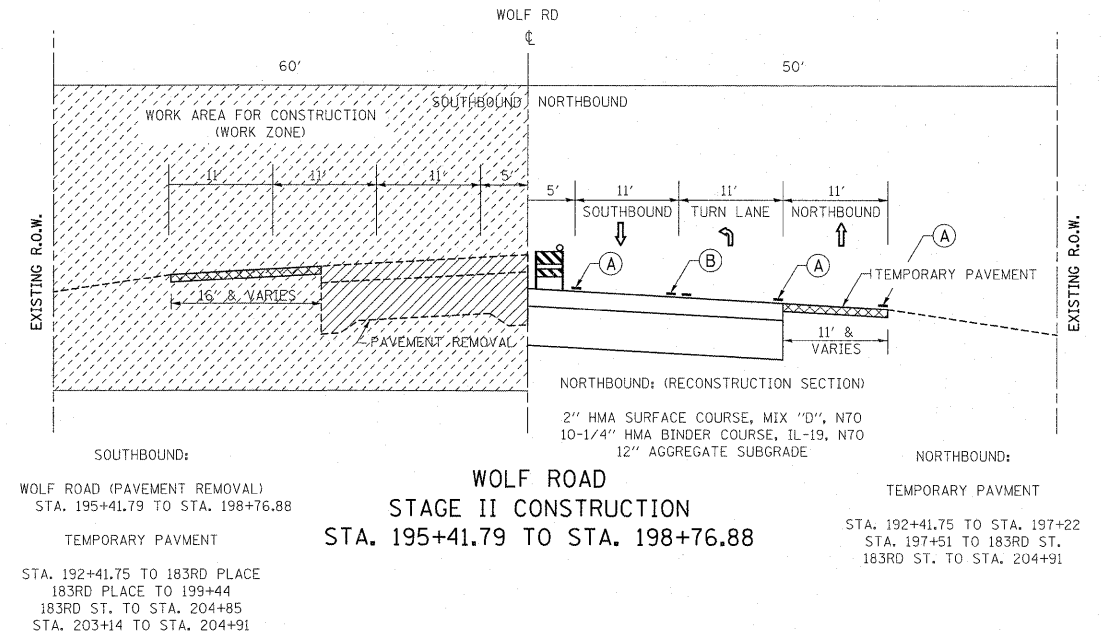
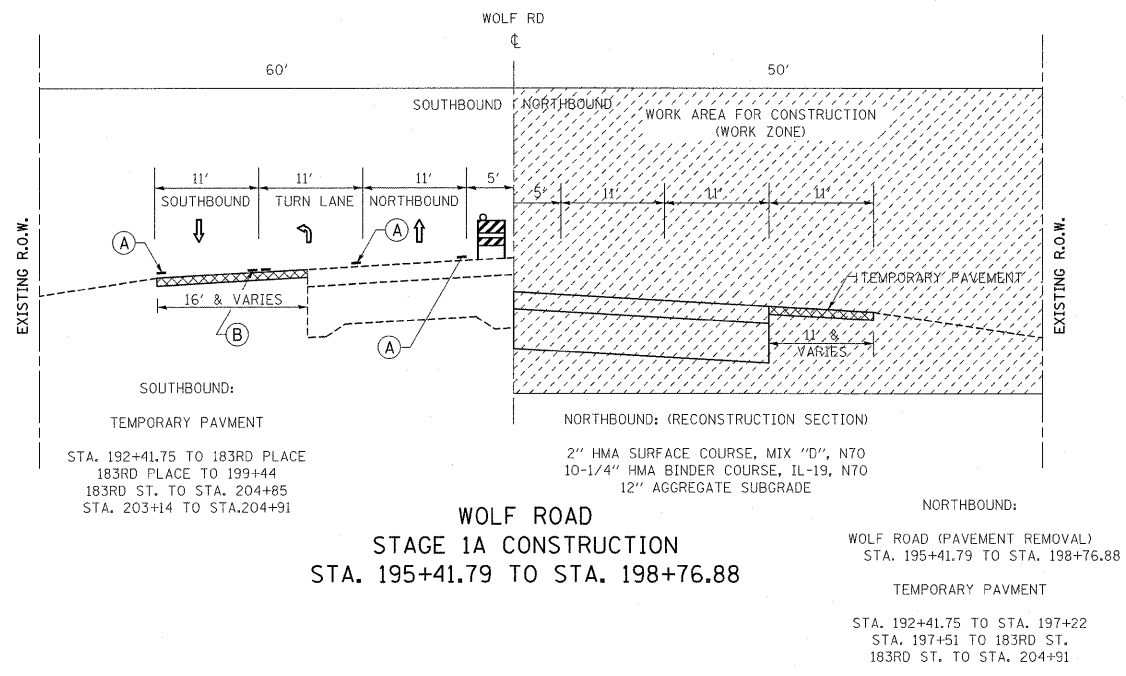
⑭
W1-6
(48X 24 in)

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WOLF ROAD @ IL 183RD STREET TRAFFIC CONTROL SIGNS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\pwsdot\galbenjr\d0150417\PI4329-Design.dgn	PLLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -					2688	44 R-N	COOK/WILL	68	12
	PLLOT DATE = 5/3/2011	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60K66				
		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				



LEGEND:

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
- (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
- (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
- (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
- PAVEMENT REMOVAL
- TEMPORARY PAVEMENT, 10"
- WORK ZONE

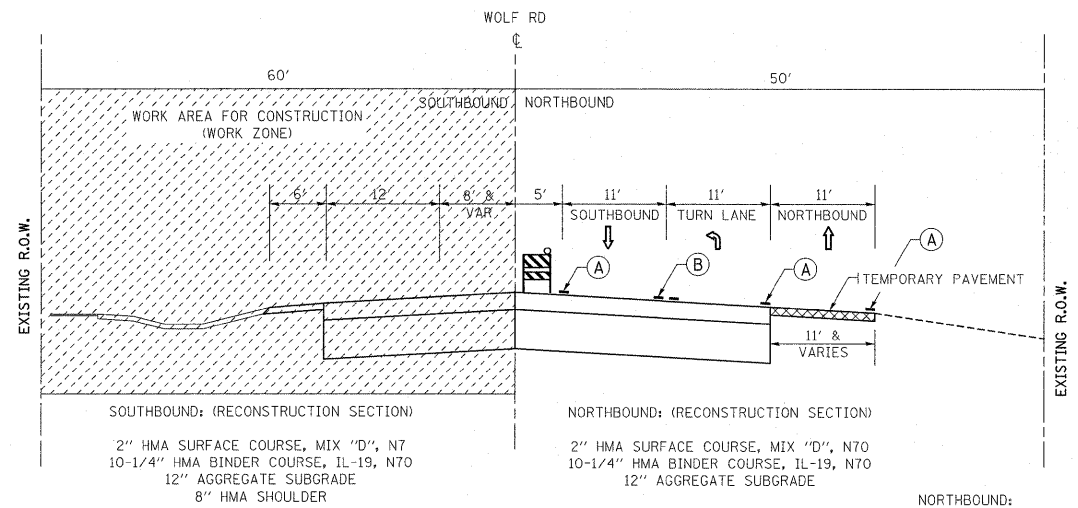


FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED -
ct:\pw_work\p\p\dot\galbanjr\d0150417\PI14309-Design.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS FOR MAINTENANCE OF TRAFFIC			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

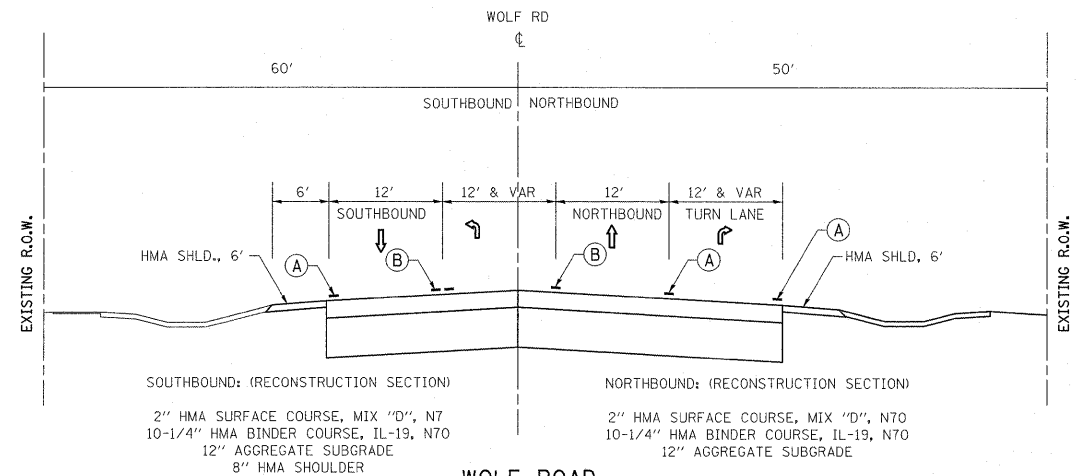
F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 13
CONTRACT NO. 60K66				
ILLINOIS FED. AID PROJECT				



**WOLF ROAD
STAGE IIA CONSTRUCTION
STA. 195+41.79 TO STA. 198+76.88**

NORTHBOUND:
TEMPORARY PAVMENT
STA. 192+41.75 TO STA. 197+22
STA. 197+51 TO 183RD ST.
183RD ST. TO STA. 204+91

- LEGEND:**
- Ⓐ TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - Ⓑ TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - Ⓒ TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - Ⓓ TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - PAVEMENT REMOVAL
 - ▨ TEMPORARY PAVEMENT, 10"
 - ▩ WORK ZONE



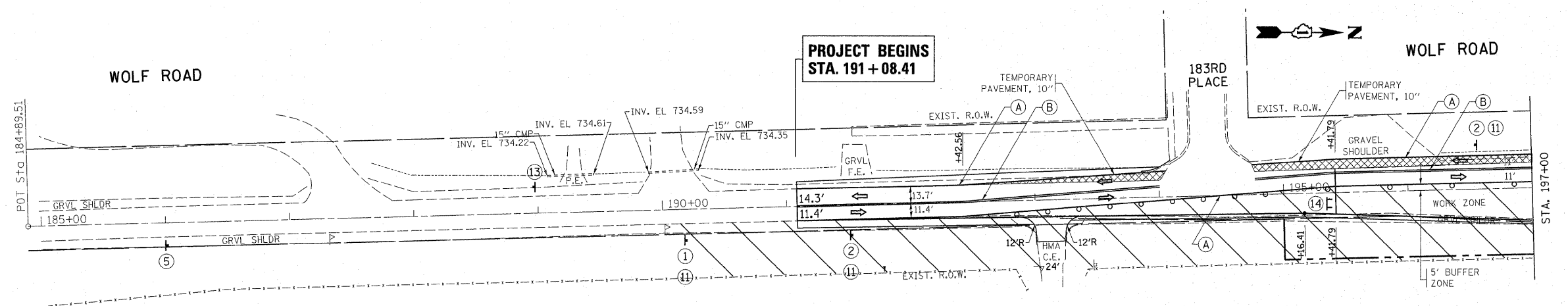
**WOLF ROAD
STAGE IIB CONSTRUCTION
STA. 195+41.79 TO STA. 198+76.88**

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -
ct:\pw_work\pwidet\galbenjr\d0150417\PI14309-Design.dgn		DRAWN -	REVISED -
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 5/3/2011		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS FOR MAINTENANCE OF TRAFFIC			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 14
CONTRACT NO. 60K66				
ILLINOIS FED. AID PROJECT				

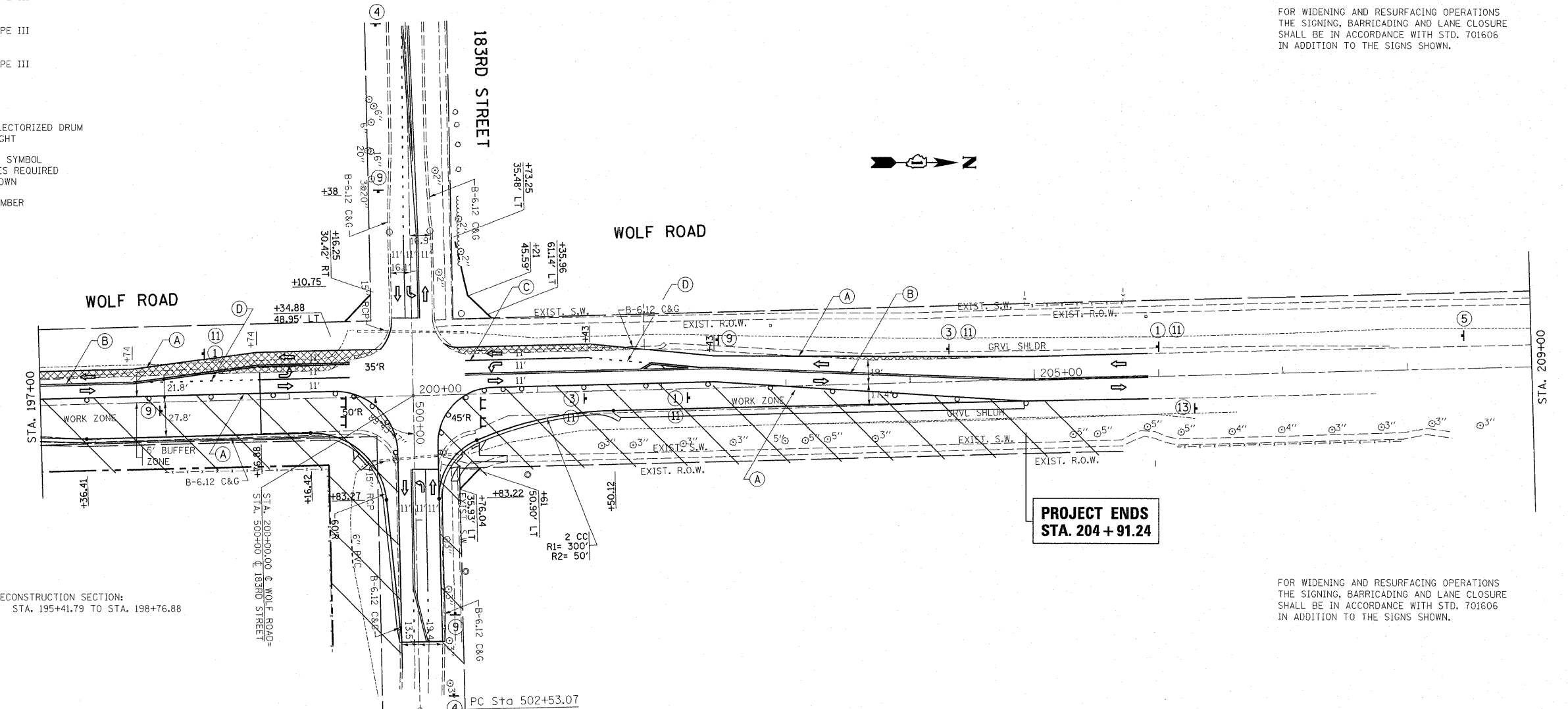


LEGEND:

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - (E) TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
-
- BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM
OR VERTICAL PANEL WITH ONE BURN LIGHT
 - ⊥ TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL
REPRESENTS ANY NUMBER OF BARRICADES REQUIRED
TO ADEQUATELY PROTECT THE AREA SHOWN
 - ⊥ CONSTRUCTION SIGN AND REFERENCE NUMBER
(REFER TO SIGN DETAIL LEGEND SHEET)
 - ↑ TRAFFIC LANE

• RECONSTRUCTION SECTION:
STA. 195+41.79 TO STA. 198+76.88

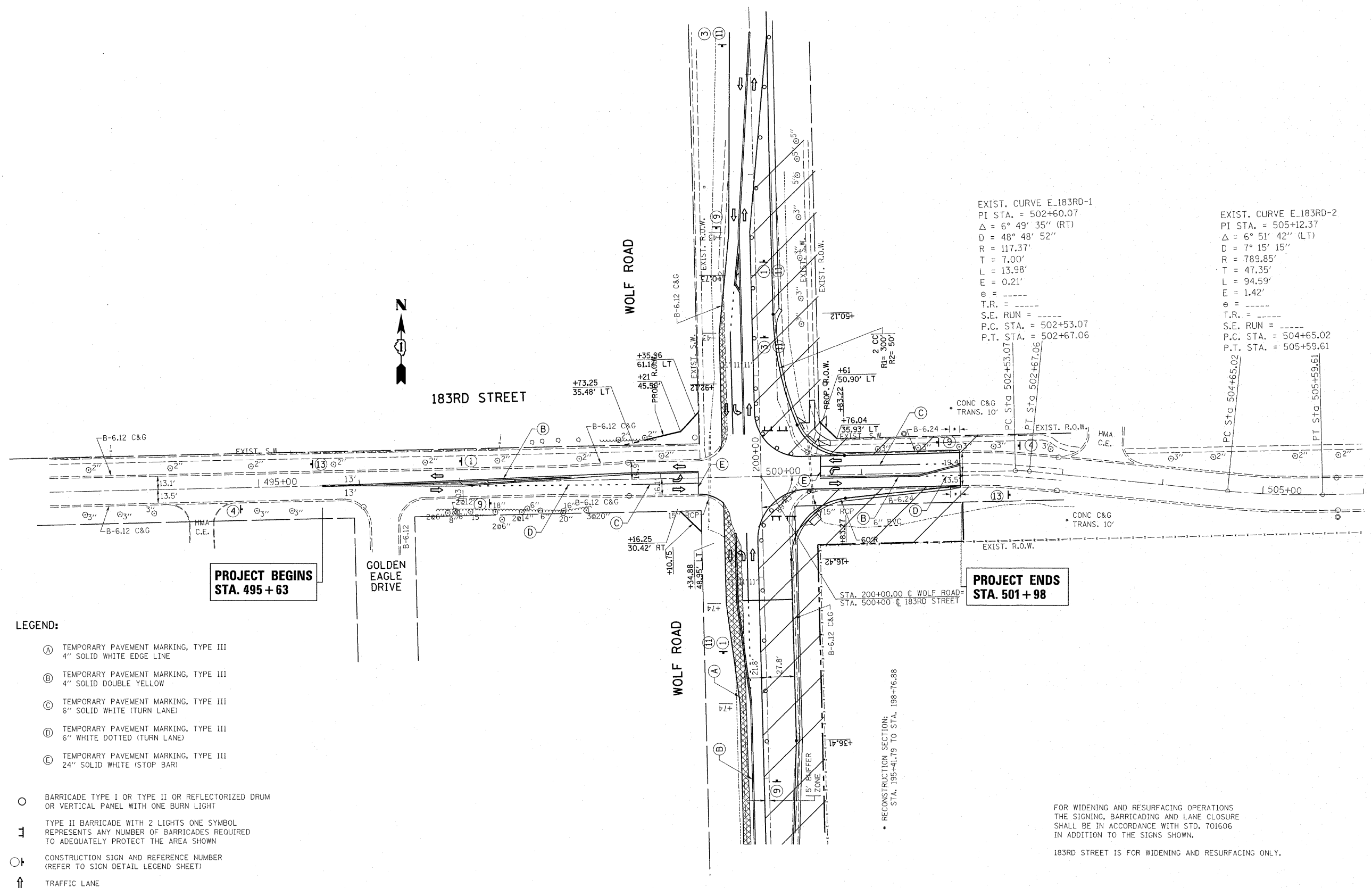
FOR WIDENING AND RESURFACING OPERATIONS
THE SIGNING, BARRICADING AND LANE CLOSURE
SHALL BE IN ACCORDANCE WITH STD. 701606
IN ADDITION TO THE SIGNS SHOWN.



• RECONSTRUCTION SECTION:
STA. 195+41.79 TO STA. 198+76.88

FOR WIDENING AND RESURFACING OPERATIONS
THE SIGNING, BARRICADING AND LANE CLOSURE
SHALL BE IN ACCORDANCE WITH STD. 701606
IN ADDITION TO THE SIGNS SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WOLF ROAD AT 183RD STREET SUGGESTED STAGING & TRAFFIC CONTROL PLAN, STAGE I				F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 16
es:\pw_work\pwidet\galbenjr\d0150417\14309-Design.dgn	PLOT SCALE = 58.0000' / IN.	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		
PLOT DATE = 5/3/2011	DATE -	CHECKED -	REVISED -								CONTRACT NO. 60K66		
		DATE -	REVISED -								CONTRACT NO. 60K66		



EXIST. CURVE E_183RD-1
 PI STA. = 502+60.07
 $\Delta = 6^\circ 49' 35''$ (RT)
 $D = 48^\circ 48' 52''$
 $R = 117.37'$
 $T = 7.00'$
 $L = 13.98'$
 $E = 0.21'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 502+53.07
 P.T. STA. = 502+67.06

EXIST. CURVE E_183RD-2
 PI STA. = 505+12.37
 $\Delta = 6^\circ 51' 42''$ (LT)
 $D = 7^\circ 15' 15''$
 $R = 789.85'$
 $T = 47.35'$
 $L = 94.59'$
 $E = 1.42'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 504+65.02
 P.T. STA. = 505+59.61

**PROJECT BEGINS
 STA. 495 + 63**

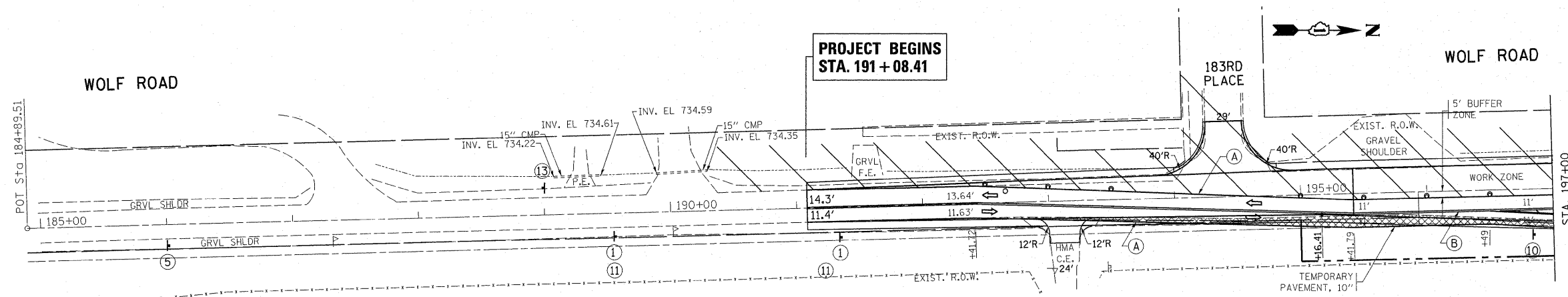
**PROJECT ENDS
 STA. 501 + 98**

- LEGEND:**
- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - (E) TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
 - BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM
OR VERTICAL PANEL WITH ONE BURN LIGHT
 - ⌋ TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL
REPRESENTS ANY NUMBER OF BARRICADES REQUIRED
TO ADEQUATELY PROTECT THE AREA SHOWN
 - + CONSTRUCTION SIGN AND REFERENCE NUMBER
(REFER TO SIGN DETAIL LEGEND SHEET)
 - ↑ TRAFFIC LANE

• RECONSTRUCTION SECTION:
 STA. 195+41.79 TO STA. 198+76.88

FOR WIDENING AND RESURFACING OPERATIONS
 THE SIGNING, BARRICADING AND LANE CLOSURE
 SHALL BE IN ACCORDANCE WITH STD. 701606
 IN ADDITION TO THE SIGNS SHOWN.
 183RD STREET IS FOR WIDENING AND RESURFACING ONLY.

FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	183RD STREET @ WOLF ROAD SUGGESTED STAGING & TRAFFIC CONTROL PLAN, STAGE IA			F.A. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 17
es:\pw_work\pwidot\galbanjr\d0150417\14309-Design.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60K66	
	PLOT DATE = 5/3/2011	CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



**PROJECT BEGINS
STA. 191 + 08.41**

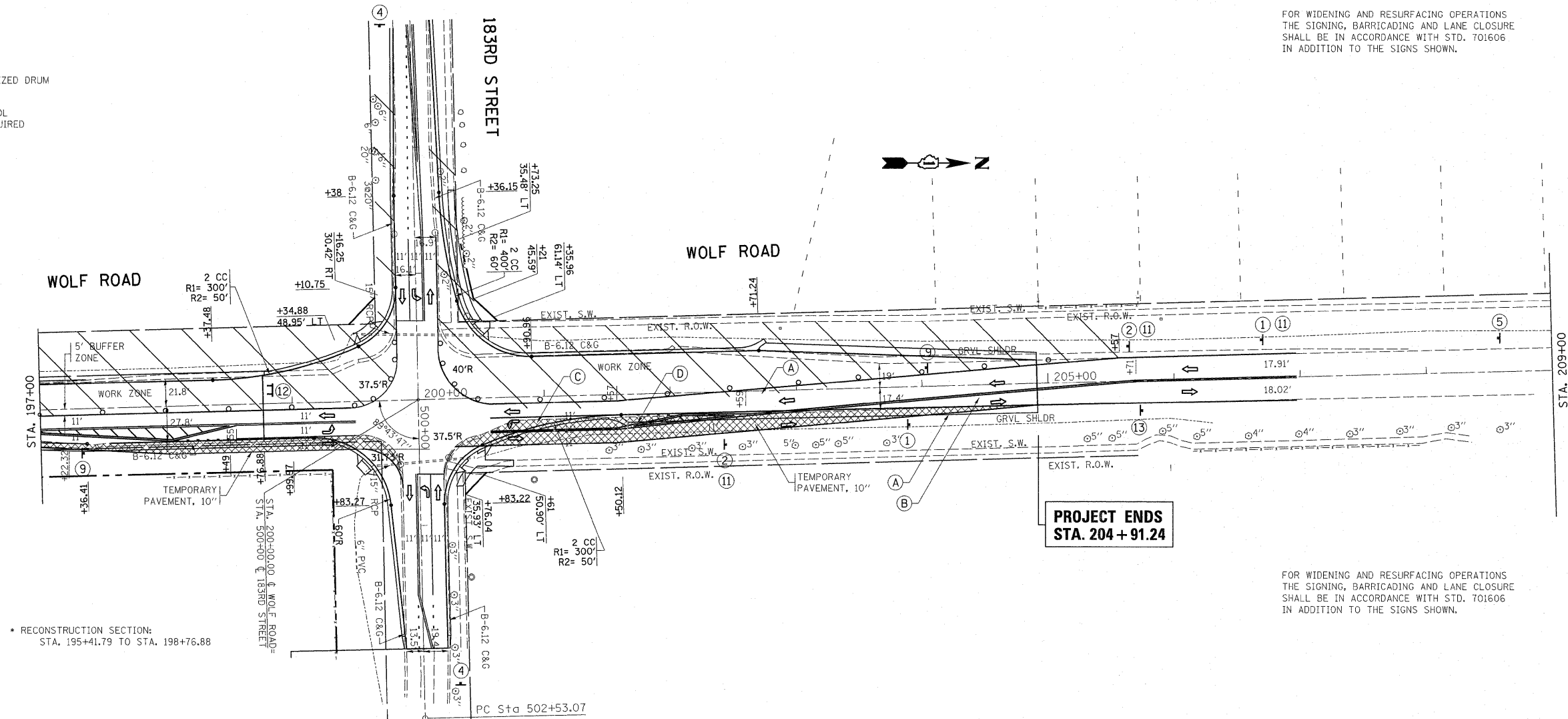
WOLF ROAD

LEGEND:

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - (E) TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)
- BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM
OR VERTICAL PANEL WITH ONE BURN LIGHT
 - ⊥ TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL
REPRESENTS ANY NUMBER OF BARRICADES REQUIRED
TO ADEQUATELY PROTECT THE AREA SHOWN
 - ⊕ CONSTRUCTION SIGN AND REFERENCE NUMBER
(REFER TO SIGN DETAIL LEGEND SHEET)
 - ↑ TRAFFIC LANE

• RECONSTRUCTION SECTION:
STA. 195+41.79 TO STA. 198+76.88

FOR WIDENING AND RESURFACING OPERATIONS
THE SIGNING, BARRICADING AND LANE CLOSURE
SHALL BE IN ACCORDANCE WITH STD. 701606
IN ADDITION TO THE SIGNS SHOWN.

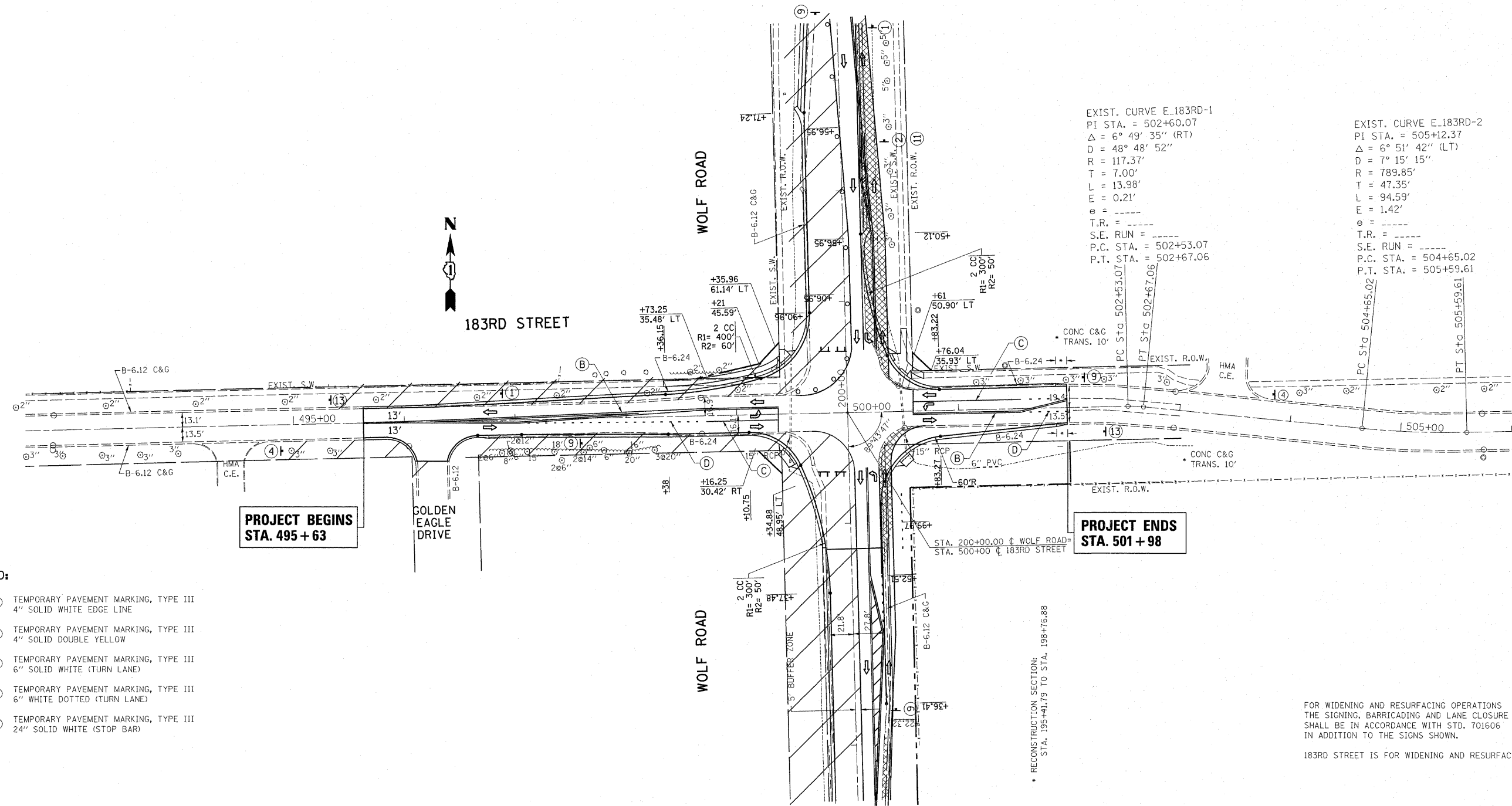


**PROJECT ENDS
STA. 204 + 91.24**

• RECONSTRUCTION SECTION:
STA. 195+41.79 TO STA. 198+76.88

FOR WIDENING AND RESURFACING OPERATIONS
THE SIGNING, BARRICADING AND LANE CLOSURE
SHALL BE IN ACCORDANCE WITH STD. 701606
IN ADDITION TO THE SIGNS SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WOLF ROAD AT 183RD STREET SUGGESTED STAGING & TRAFFIC CONTROL PLAN, STAGE II			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw_work\p\dot\galbenjr\d0150417\143	09-Design.dgn	DRAWN -	REVISED -		2688	44 R-N	COOK/WILL	68	18			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 60K66				
PLOT DATE = 5/3/2011		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



EXIST. CURVE E_183RD-1
 PI STA. = 502+60.07
 Δ = 6° 49' 35" (RT)
 D = 48° 48' 52"
 R = 117.37'
 T = 7.00'
 L = 13.98'
 E = 0.21'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 502+53.07
 P.T. STA. = 502+67.06

EXIST. CURVE E_183RD-2
 PI STA. = 505+12.37
 Δ = 6° 51' 42" (LT)
 D = 7° 15' 15"
 R = 789.85'
 T = 47.35'
 L = 94.59'
 E = 1.42'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA. = 504+65.02
 P.T. STA. = 505+59.61

**PROJECT BEGINS
 STA. 495 + 63**

**PROJECT ENDS
 STA. 501 + 98**

- LEGEND:**
- Ⓐ TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
 - Ⓑ TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
 - Ⓒ TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
 - Ⓓ TEMPORARY PAVEMENT MARKING, TYPE III
6" WHITE DOTTED (TURN LANE)
 - Ⓔ TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)

- BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT
- ⌚ TYPE II BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
- Ⓢ CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)
- ↑ TRAFFIC LANE

RECONSTRUCTION SECTION:
 STA. 195+41.75 TO STA. 198+76.88

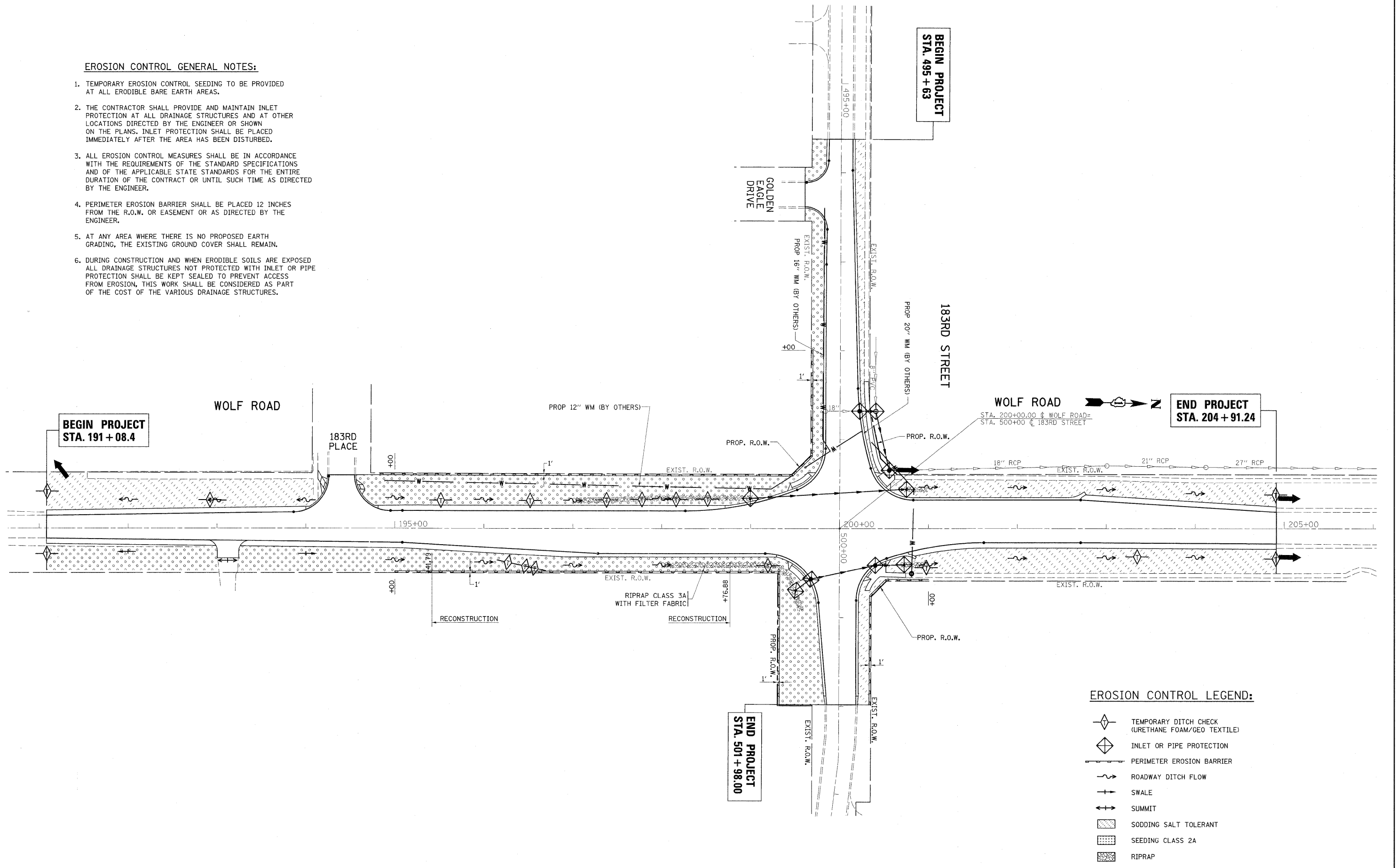
FOR WIDENING AND RESURFACING OPERATIONS THE SIGNING, BARRICADING AND LANE CLOSURE SHALL BE IN ACCORDANCE WITH STD. 701606 IN ADDITION TO THE SIGNS SHOWN.

183RD STREET IS FOR WIDENING AND RESURFACING ONLY.

FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	183RD STREET @ WOLF ROAD SUGGESTED STAGING & TRAFFIC CONTROL PLAN, STAGE IIA			F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 19
ct:\pw_work\pwidot\galbanjr\d0150417\143809-Design.dgn		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60K66	
		CHECKED -	REVISED -		ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

EROSION CONTROL GENERAL NOTES:

1. TEMPORARY EROSION CONTROL SEEDING TO BE PROVIDED AT ALL ERODIBLE BARE EARTH AREAS.
2. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET PROTECTION AT ALL DRAINAGE STRUCTURES AND AT OTHER LOCATIONS DIRECTED BY THE ENGINEER OR SHOWN ON THE PLANS. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE AREA HAS BEEN DISTURBED.
3. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND OF THE APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.
4. PERIMETER EROSION BARRIER SHALL BE PLACED 12 INCHES FROM THE R.O.W. OR EASEMENT OR AS DIRECTED BY THE ENGINEER.
5. AT ANY AREA WHERE THERE IS NO PROPOSED EARTH GRADING, THE EXISTING GROUND COVER SHALL REMAIN.
6. DURING CONSTRUCTION AND WHEN ERODIBLE SOILS ARE EXPOSED ALL DRAINAGE STRUCTURES NOT PROTECTED WITH INLET OR PIPE PROTECTION SHALL BE KEPT SEALED TO PREVENT ACCESS FROM EROSION, THIS WORK SHALL BE CONSIDERED AS PART OF THE COST OF THE VARIOUS DRAINAGE STRUCTURES.



EROSION CONTROL LEGEND:

- TEMPORARY DITCH CHECK (URETHANE FOAM/GEO TEXTILE)
- INLET OR PIPE PROTECTION
- PERIMETER EROSION BARRIER
- ROADWAY DITCH FLOW
- SWALE
- SUMMIT
- SODDING SALT TOLERANT
- SEEDING CLASS 2A
- RIPRAP

FILE NAME = P143409-sht-eros.dgn

USER NAME = ulrshkd
 DESIGNED -
 DRAWN -
 CHECKED -
 DATE = 3/2/2011

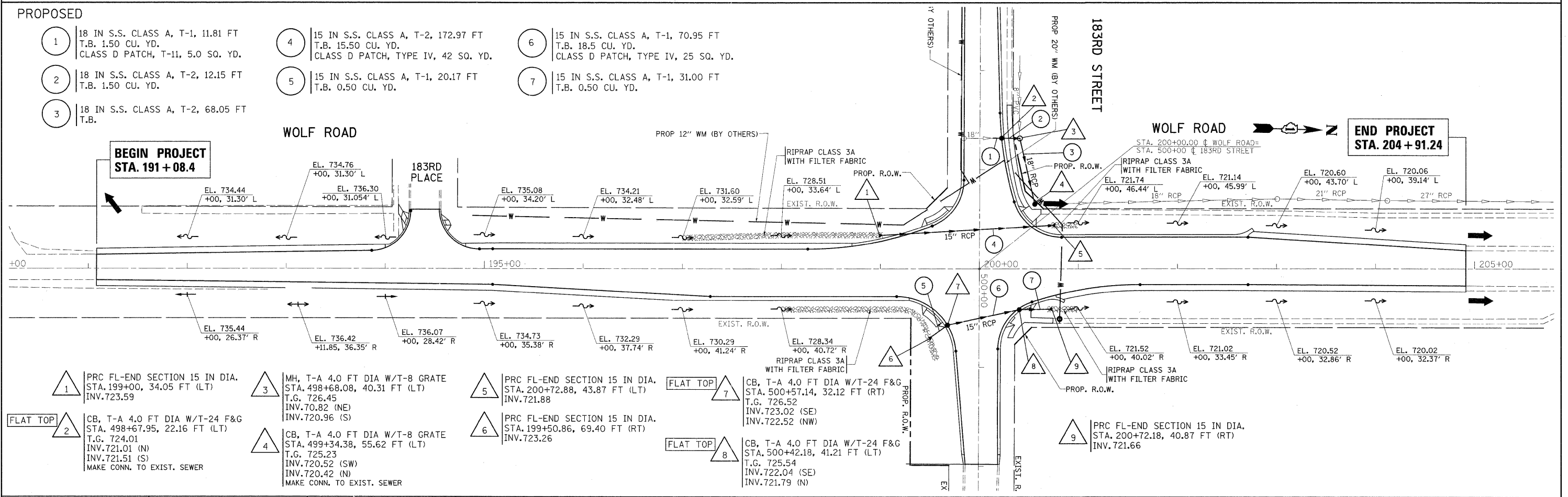
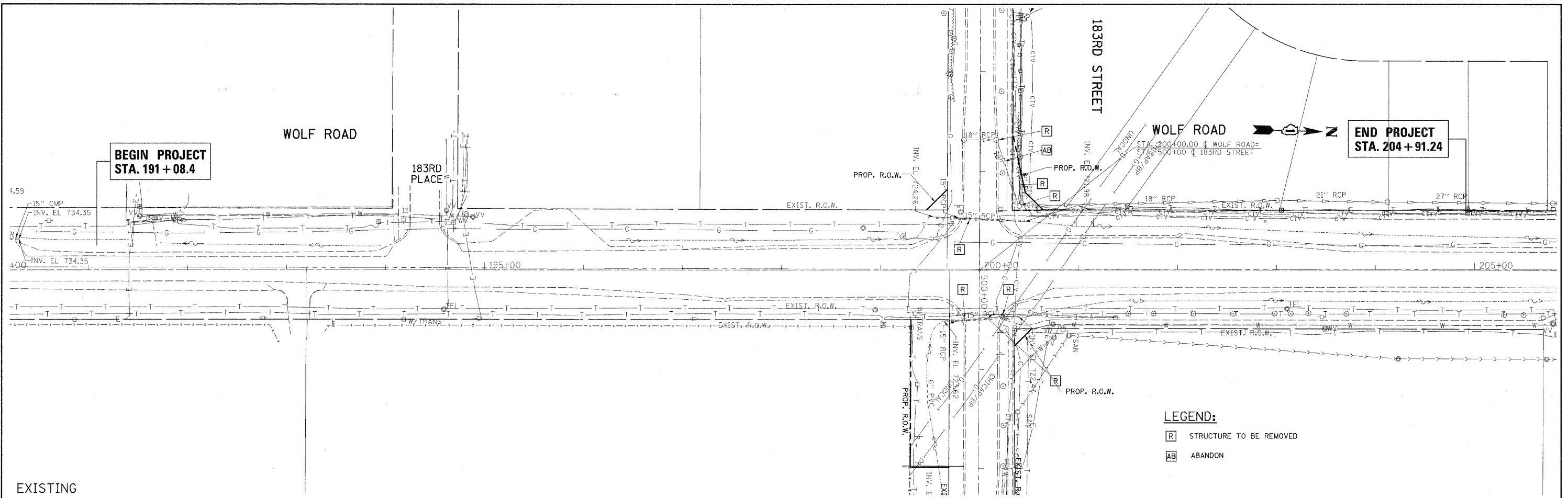
DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

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 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

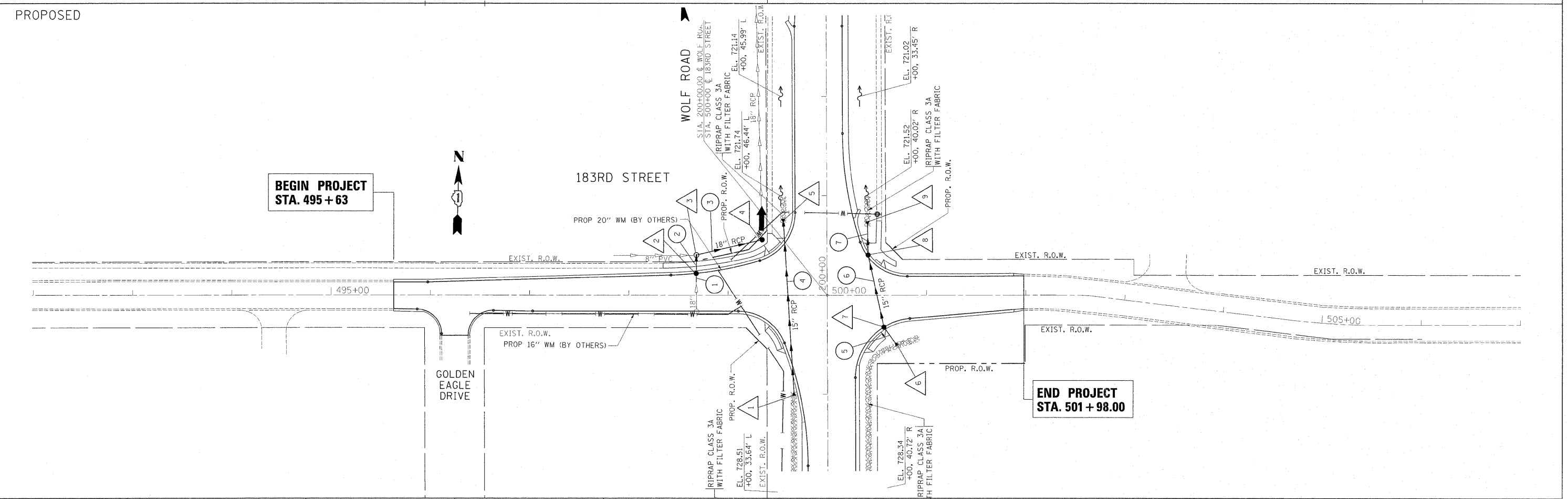
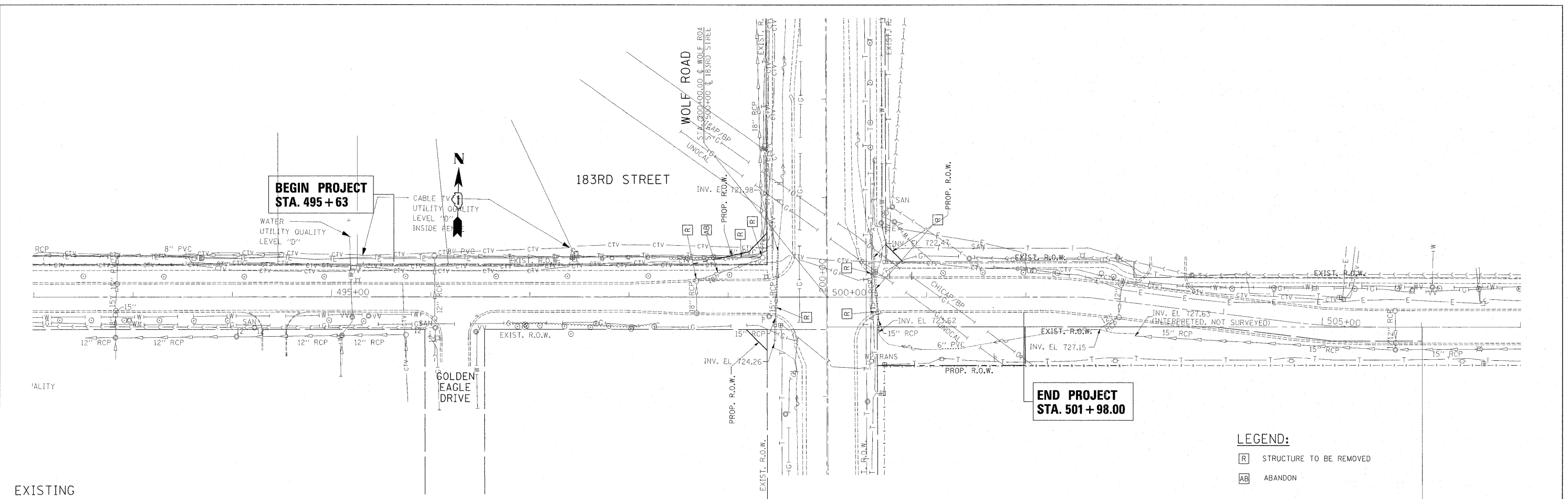
**WOLF ROAD @ 183RD STREET
 PROPOSED EROSION CONTROL PLAN**
 SCALE: 1"= 50' SHEET NO. OF SHEETS STA. 184+89.51 TO STA. 213+00.00

F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK	TOTAL SHEETS 68	SHEET NO. 20
CONTRACT NO. 60K66				ILLINOIS FED. AID PROJECT



- PROPOSED**
- 1 18 IN S.S. CLASS A, T-1, 11.81 FT T.B. 1.50 CU. YD. CLASS D PATCH, T-11, 5.0 SQ. YD.
 - 2 18 IN S.S. CLASS A, T-2, 12.15 FT T.B. 1.50 CU. YD.
 - 3 18 IN S.S. CLASS A, T-2, 68.05 FT T.B.
 - 4 15 IN S.S. CLASS A, T-2, 172.97 FT T.B. 15.50 CU. YD. CLASS D PATCH, TYPE IV, 42 SQ. YD.
 - 5 15 IN S.S. CLASS A, T-1, 20.17 FT T.B. 0.50 CU. YD.
 - 6 15 IN S.S. CLASS A, T-1, 70.95 FT T.B. 18.5 CU. YD. CLASS D PATCH, TYPE IV, 25 SQ. YD.
 - 7 15 IN S.S. CLASS A, T-1, 31.00 FT T.B. 0.50 CU. YD.

- 1 PRC FL-END SECTION 15 IN DIA. STA. 199+00, 34.05 FT (LT) INV. 723.59
- 2 CB, T-A 4.0 FT DIA W/T-24 F&G STA. 498+67.95, 22.16 FT (LT) T.G. 724.01 INV. 721.01 (N) INV. 721.51 (S) MAKE CONN. TO EXIST. SEWER
- 3 MH, T-A 4.0 FT DIA W/T-8 GRATE STA. 498+68.08, 40.31 FT (LT) T.G. 726.45 INV. 70.82 (NE) INV. 720.96 (S)
- 4 CB, T-A 4.0 FT DIA W/T-8 GRATE STA. 499+34.38, 55.62 FT (LT) T.G. 725.23 INV. 720.52 (SW) INV. 720.42 (N) MAKE CONN. TO EXIST. SEWER
- 5 PRC FL-END SECTION 15 IN DIA. STA. 200+72.88, 43.87 FT (LT) INV. 721.88
- 6 PRC FL-END SECTION 15 IN DIA. STA. 199+50.86, 69.40 FT (RT) INV. 723.26
- 7 CB, T-A 4.0 FT DIA W/T-24 F&G STA. 500+57.14, 32.12 FT (RT) T.G. 726.52 INV. 723.02 (SE) INV. 722.52 (NW)
- 8 CB, T-A 4.0 FT DIA W/T-24 F&G STA. 500+42.18, 41.21 FT (LT) T.G. 725.54 INV. 722.04 (SE) INV. 721.79 (N)
- 9 PRC FL-END SECTION 15 IN DIA. STA. 200+72.18, 40.87 FT (RT) INV. 721.66



FILE NAME = P143409-shd-drain.dgn

USER NAME = galbanjr
 PLOT SCALE = 50.0000 "/p>

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

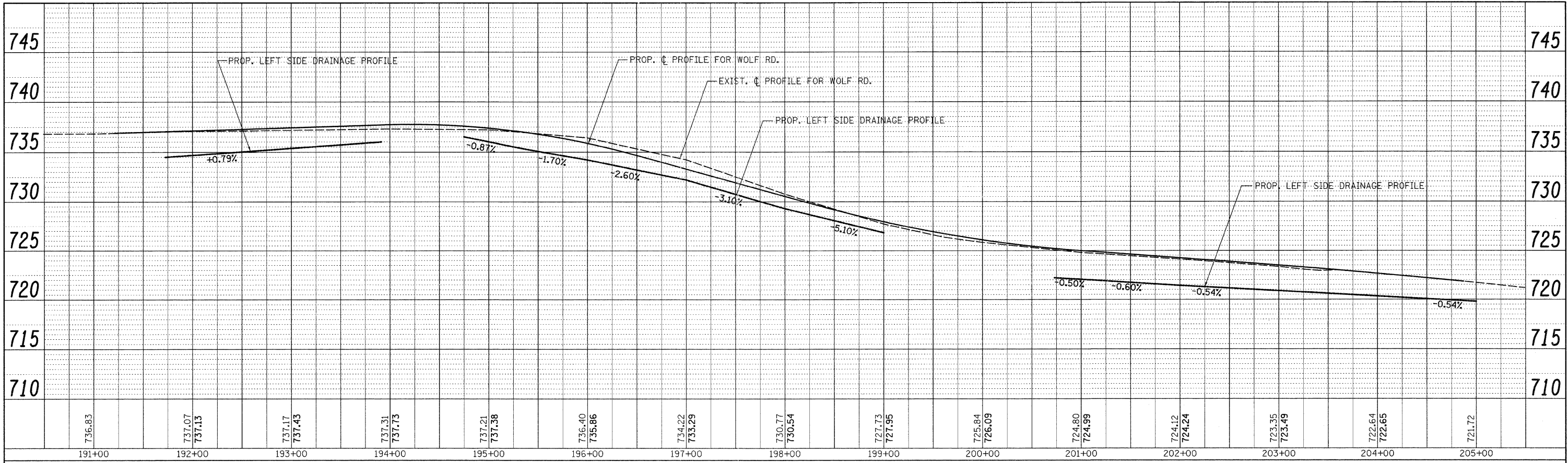
**183RD STREET @ WOLF ROAD
 EXISTING & PROPOSED DRAINAGE PLAN**

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. 492+00.00 TO STA. 507+00.0

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44 R-N	COOK	68	22
CONTRACT NO. 60K66				
ILLINOIS FED. AID PROJECT				

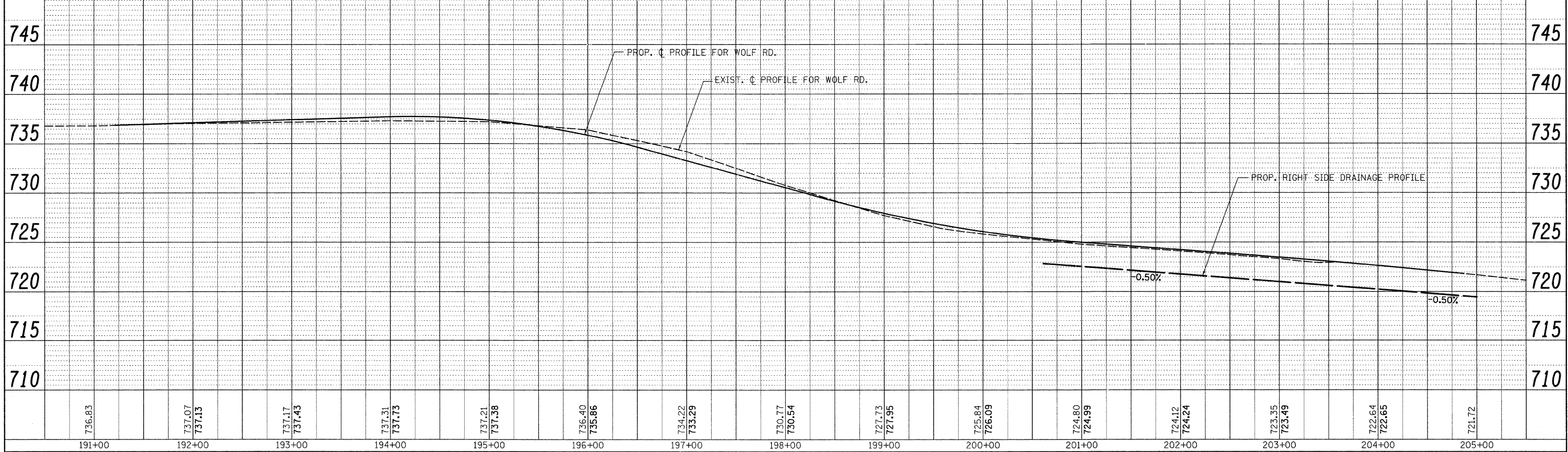
PLAN SURVEYED ALIGNED CHECKED
 NOTE BOOK NO. OF WAY CHECKED
 NO. ROAD FILE NAME

PROFILE SURVEYED GRADES CHECKED
 NOTE BOOK NO. NOTED
 NO. STRUCTURE NOTATIONS OK'D



LEFTSIDE

RIGHTSIDE



P143409-shd-drain.dgn
 USER NAME = ulriahkd

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

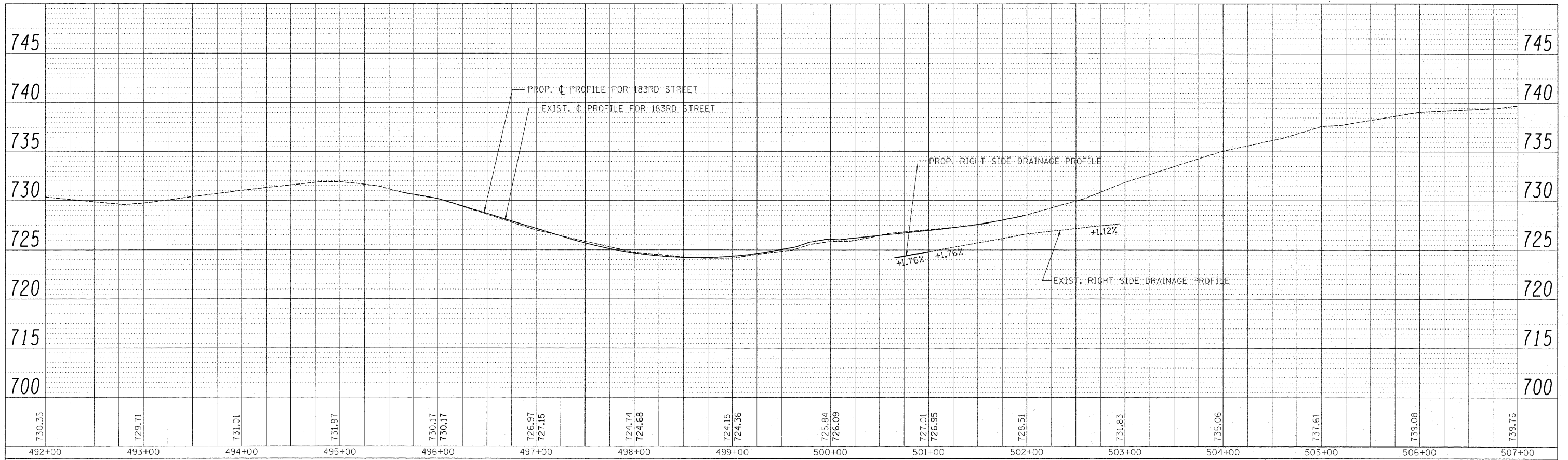
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

WOLF ROAD
 EXISTING & PROPOSED DRAINAGE PROFILE
 HOR SCALE: 1" = 50'
 VER SCALE: 1" = 5'

F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK	TOTAL SHEETS 68	SHEET NO. 23
CONTRACT NO. 60K66				ILLINOIS FED. AID PROJECT

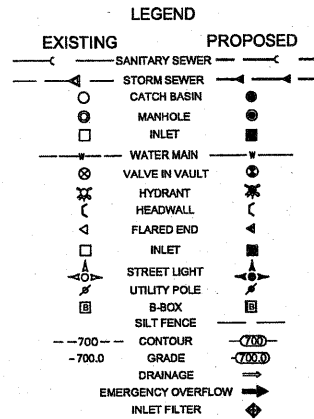
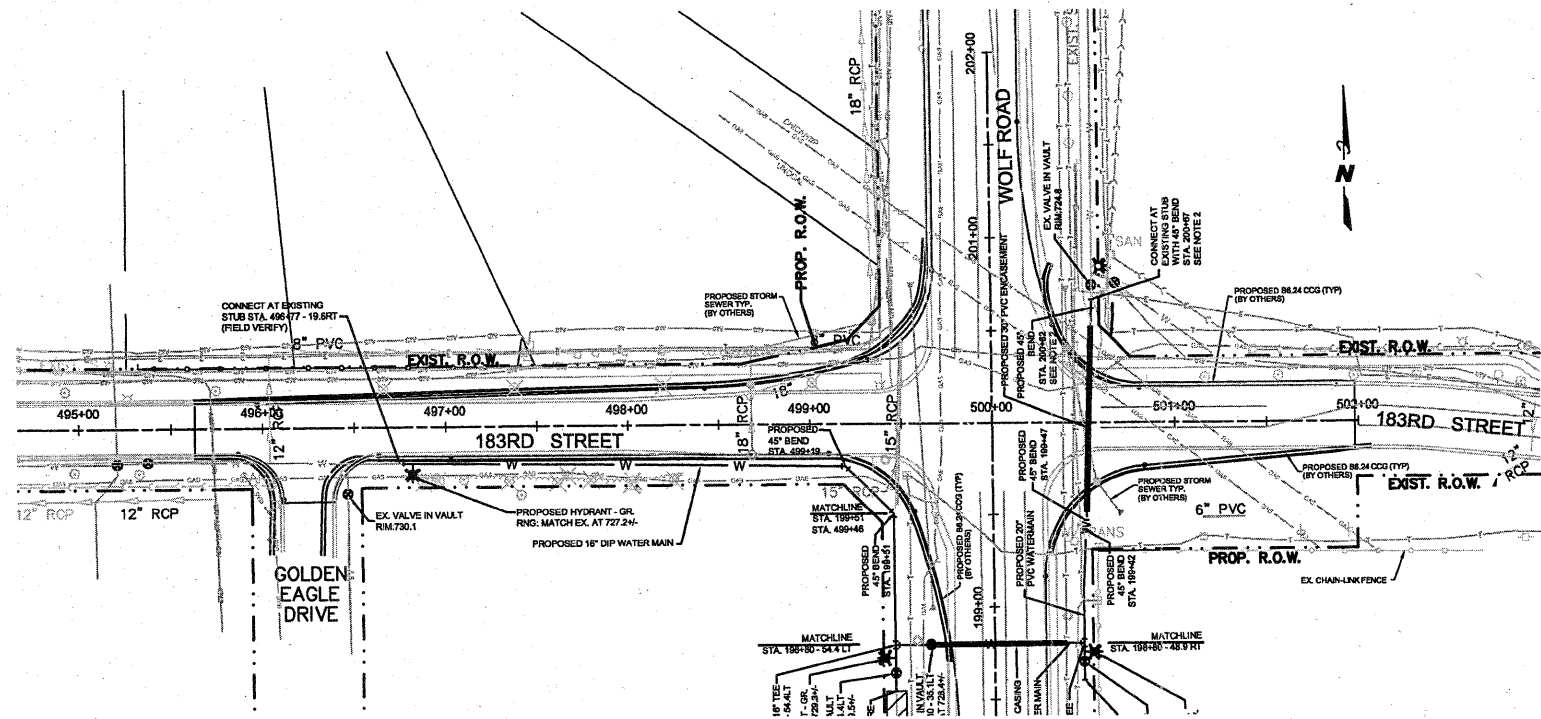
PLAN	REVISIONS	DATE
NO. _____	PLOTTED _____	BY _____
NOTE BOOK _____	ALIGNMENT CHECKED _____	
NO. _____	GRADES CHECKED _____	
	STRUCTURE NOTATIONS CHECKED _____	
	CADD FILE NAME _____	

PROFILE	REVISIONS	DATE
NO. _____	PLOTTED _____	BY _____
NOTE BOOK _____	GRADES CHECKED _____	
NO. _____	STRUCTURE NOTATIONS CHECKED _____	



P143409-shr-drain.dgn	USER NAME = galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	183RD STREET EXISTING & PROPOSED DRAINAGE PROFILE	HOR SCALE: 1" = 50' VER SCALE: 1" = 5'	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -				2688	44 R-N	COOK	68	24
PLOT DATE = 3/8/2011	CHECKED -	REVISED -	REVISED -	SHEET NO. OF SHEETS STA. 492+00.00 TO STA. 507+00.00		CONTRACT NO. 60K66					
	DATE -	REVISED -	REVISED -	ILLINOIS FED. AID PROJECT							

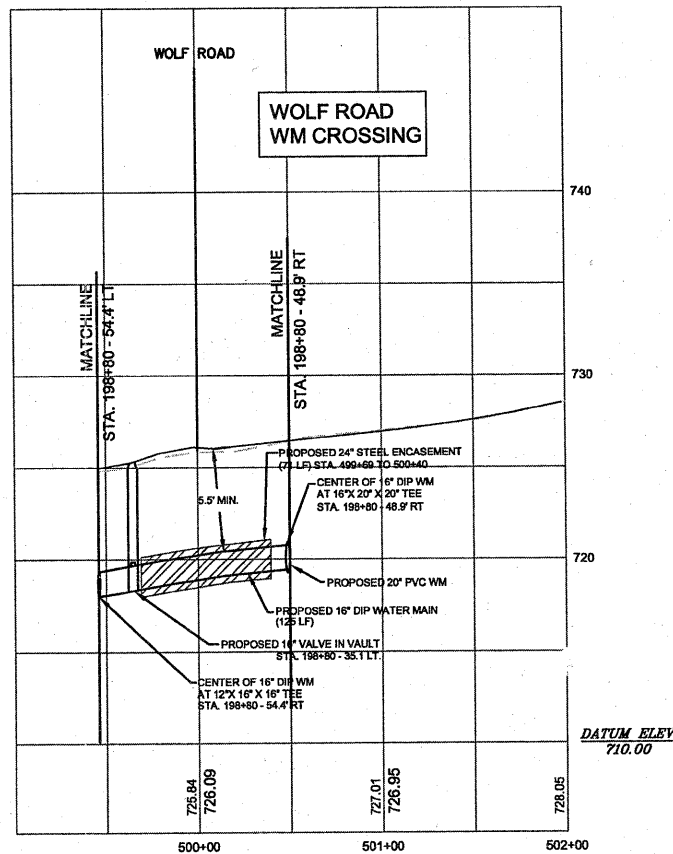
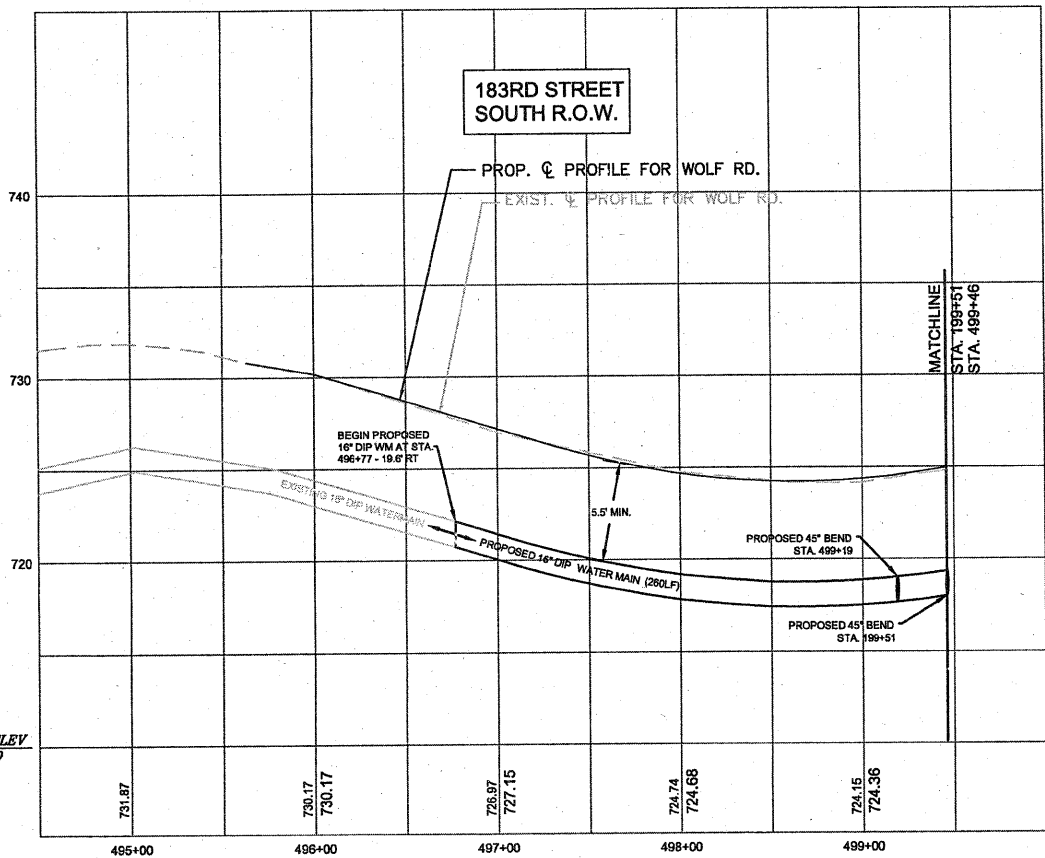
H:\2010.0034 - Orland Park 183rd and Wolf Watermain.dwg\Plan&Profile.dwg, 5/9/2011 12:51:53 PM



RECEIVED
MAY 10 2011

DIST. ONE - DESIGN

- PLAN NOTES:
1. TOPOGRAPHIC DATA WAS PROVIDED BY DOT.
 2. 45 DEGREE BENDS AT STA. 200+42 & 200+47 MAY BE PVC OR EPOXY COATED FITTINGS WITH STAINLESS STEEL NUTS AND BOLTS.
 3. REFER TO PLAN VIEW FOR FIREHYDRANT LOCATIONS.



SUMMARY OF QUANTITIES

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE
				1000-2A
1	DUCTILE IRON WATER MAIN, 12"	LF	25	
2	DUCTILE IRON WATER MAIN, 16"	LF	454	
3	PVC C900 WATER MAIN, 20"	LF	207	
4	STEEL ENCASEMENT PIPE, 24"	LF	71	
5	PVC ENCASEMENT, 30"	LF	100	
6	48" VAULT WITH 12" VALVE	EA	1	
7	60" VAULT WITH 16" VALVE	EA	1	
8	60" VAULT WITH 20" VALVE	EA	1	
9	45" DUCTILE IRON BEND, 16"	EACH	2	
10	45" PVC BEND, 20"	EACH	4	
11	12" X 16" X 16" DUCTILE IRON TEE	EACH	1	
12	20" X 20" X 16" DUCTILE IRON TEE	EACH	1	
13	PIPE HYDRANT	EA	3	
14	REMOVE & REPLACE TOPSOIL, 3"	L.SUM	1	
15	SEEDING - CORNER CROP	L.SUM	1	
16	EROSION & SEDIMENTATION CONTROL	L.SUM	1	
17	PAVEMENT REMOVAL AND REPLACEMENT	L.SUM	1	
18	MOBILIZATION	L.SUM	1	
19	TRAFFIC CONTROL	L.SUM	1	

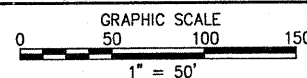
* DENOTES SPECIALTY ITEMS

FOR INFORMATION ONLY

TOTAL SHEETS 68
SHEET NO. 24A

DESIGN BY:	DLJ	NO.	DATE	REVISIONS	DES. DRW. CHK. NO.	DATE	REVISIONS	DES. DRW. CHK.
DRAWN BY:	DLJ	1	01-27-11	DIVIDE PLAN & PROFILE VIEWS PER TDW				
CHECKED BY:	MDB	2	02/16/11	PER VILLAGE REVIEW				
APPROVED BY:	TDW							
REVISION #								

McDONOUGH ASSOCIATES INC.
ENGINEERS ARCHITECTS
16634 S. 107TH COURT
ORLAND PARK, ILLINOIS 60467-8898
(708) 226-0261



14700 Ravinia Avenue
Orland Park, IL 60462
Ph. (708)403-6100

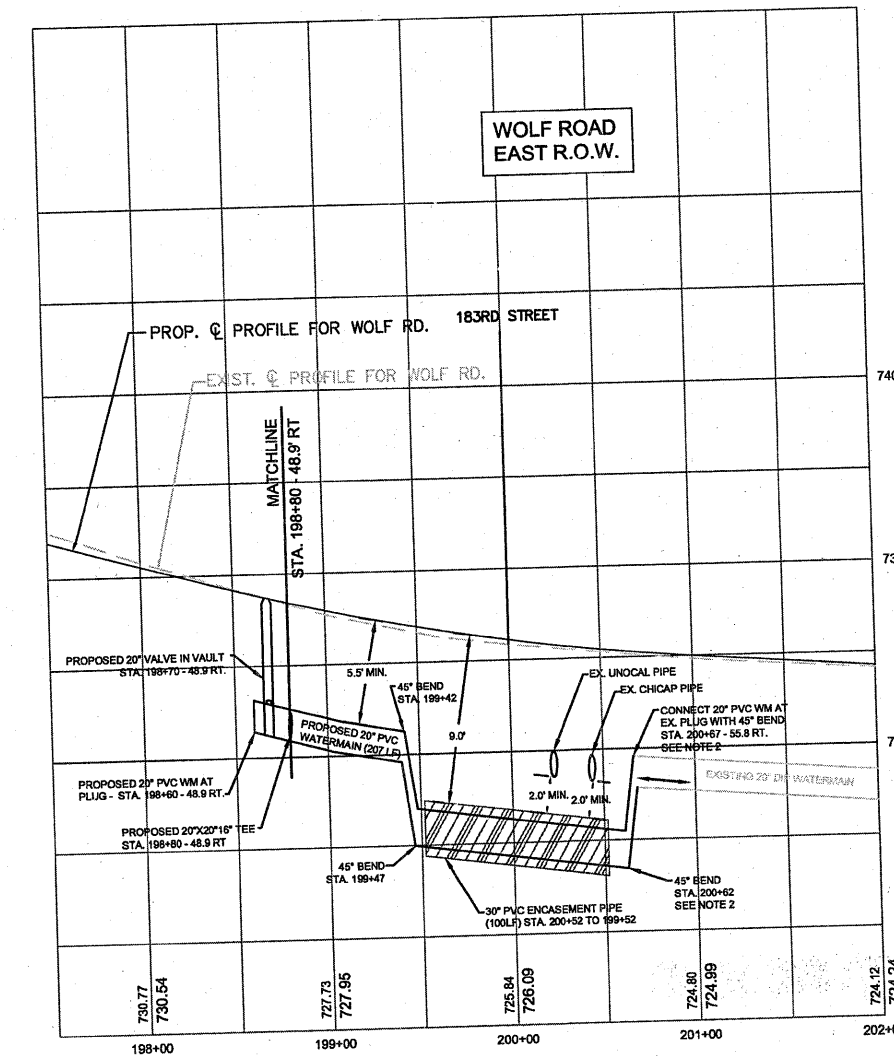
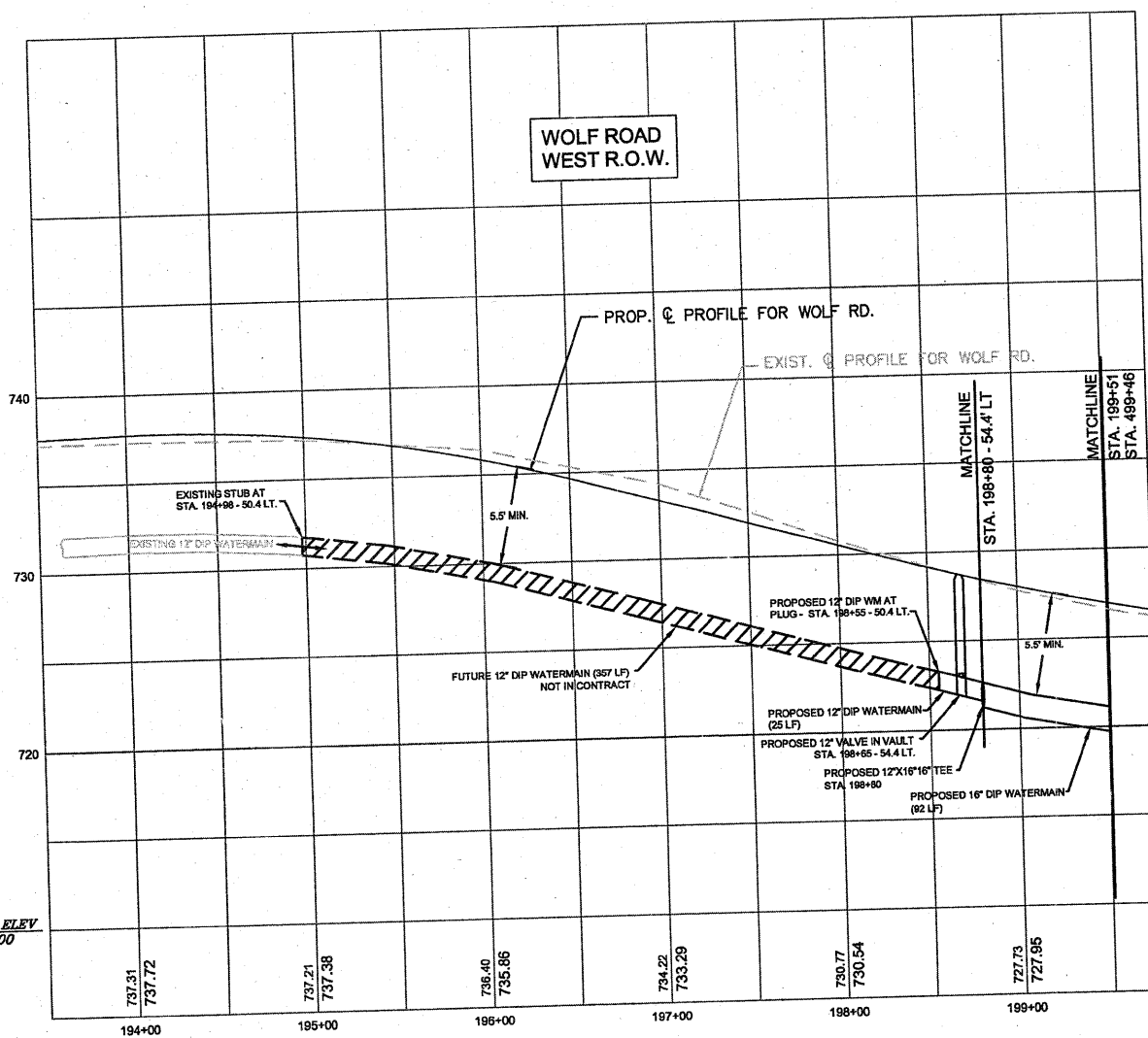
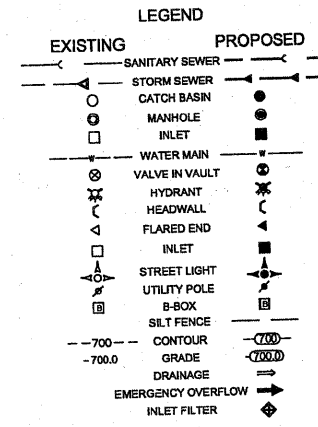
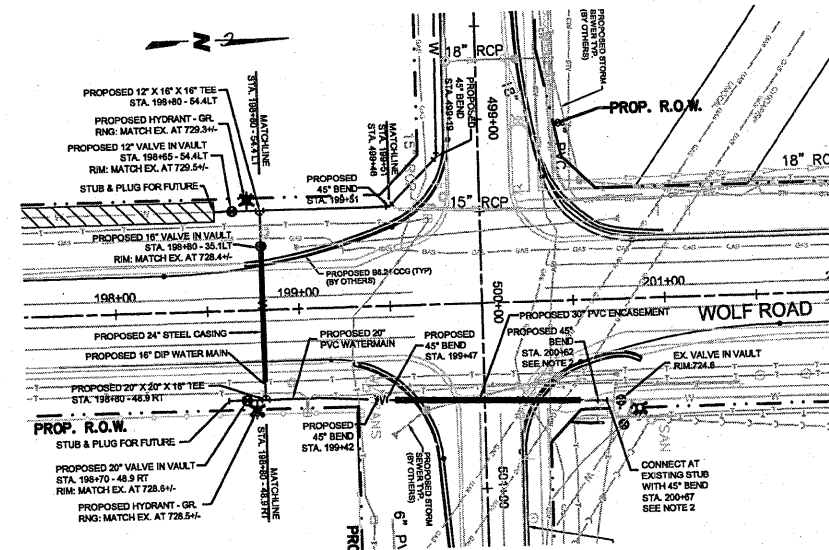
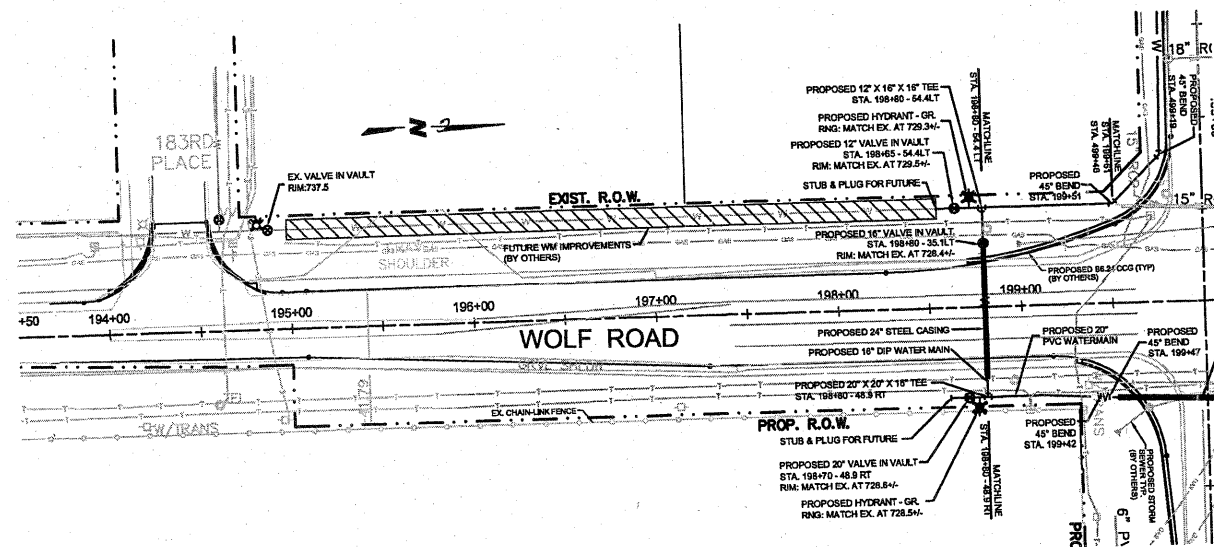
WATER MAIN IMPROVEMENTS
WOLF RD and 183RD ST

PLAN & PROFILE
183RD STREET

DATE: 05/09/11 SCALE: 1"=50' HORIZ
1"=5' VERT

MAI JOB NO.
2010.0034
DRAWING NO.
3
OF 11 SHEETS

H:\2010.0034 - Orland Park 183rd and Wolf Watermain\dwg\Plan&Profile.dwg, 5/9/2011 12:52:36 PM



- PLAN NOTES:**
1. TOPOGRAPHIC DATA WAS PROVIDED BY IDOT.
 2. 45 DEGREE BENDS AT STA. 200+62 & 200+67 MAY BE PVC OR EPOXY COATED FITTINGS WITH STAINLESS STEEL NUTS AND BOLTS.
 3. REFER TO PLAN VIEW FOR FIREHYDRANT LOCATIONS.

RECEIVED
MAY 10 2011
DIST. ONE - DESIGN

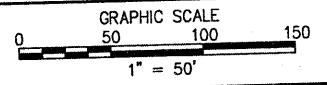
FOR INFORMATION ONLY

DATUM ELEV
710.00

TOTAL SHEETS 68
SHEET NO. 248

DESIGN BY:	DLJ	NO.	DATE	REVISIONS	DES.	DRW.	CHK.	NO.	DATE	REVISIONS	DES.	DRW.	CHK.
DRAWN BY:	DLJ	1	01-27-11	DIVIDE PLAN & PROFILE VIEWS PER TDW									
CHECKED BY:	MDB	2	02/16/11	PER VILLAGE REVIEW									
APPROVED BY:	TDW												
REVISION #													

McDONOUGH ASSOCIATES INC.
ENGINEERS ARCHITECTS
16834 S. 107TH COURT
ORLAND PARK, ILLINOIS 60467-8898
(708) 226-9261

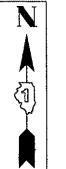


Orland Park, Illinois
14700 Ravinia Avenue
Orland Park, IL 60462
Ph. (708)403-6100

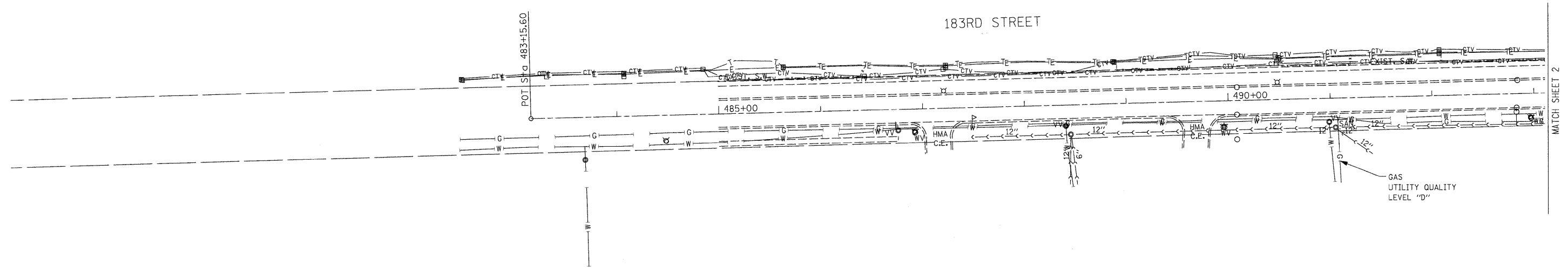
WATER MAIN IMPROVEMENTS
WOLF RD and 183RD ST
PLAN & PROFILE
WOLF ROAD

DATE: 05/09/11 SCALE: 1"=50' HORIZ
1"=5' VERT

MAI JOB NO. 2010.0034
DRAWING NO. 4
OF 11 SHEETS



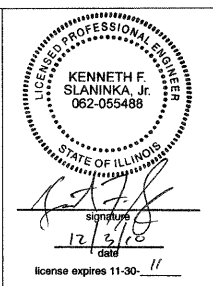
UTILITIES OWNERS	
COMED	= POWER
COMCAST	= CABLE TV
AT&T	= TELEPHONE
VILLAGE OF ORLAND PARK	= WATER
NICOR	= GAS
CHICAP/BP PIPELINE	= PIPELINE
UNOCAL PIPELINE	= PIPELINE
ENBRIDGE PIPELINE	= PIPELINE



— A — A —	AERIAL UTILITY
- - - - -	UNKNOWN
— CTV — CTV —	CABLE TV
— T — T —	TELEPHONE
— G — G —	GAS
— E — E —	ELECTRIC
— W — W —	WATER
— FO — FO —	FIBER OPTIC
— S — S —	SEWER
⊙	TBE TEST HOLE

Utilities shown on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. TBE's SUE field investigation was performed 11/16/10 through 11/19/10. Changes to utilities after this date may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510419, 10430
SUE Plan Page: 1 of 5

Utility Quality Level "A" : Test Hole	DESIGNED <i>JP</i>	REVISED
Utility Quality Level "B" : Designating	DRAWN <i>KLC</i>	REVISED
Utility Quality Level "C" : Research with Survey	CHECKED	REVISED
Utility Quality Level "D" : Records Research	DATE <i>12/03/10</i>	REVISED

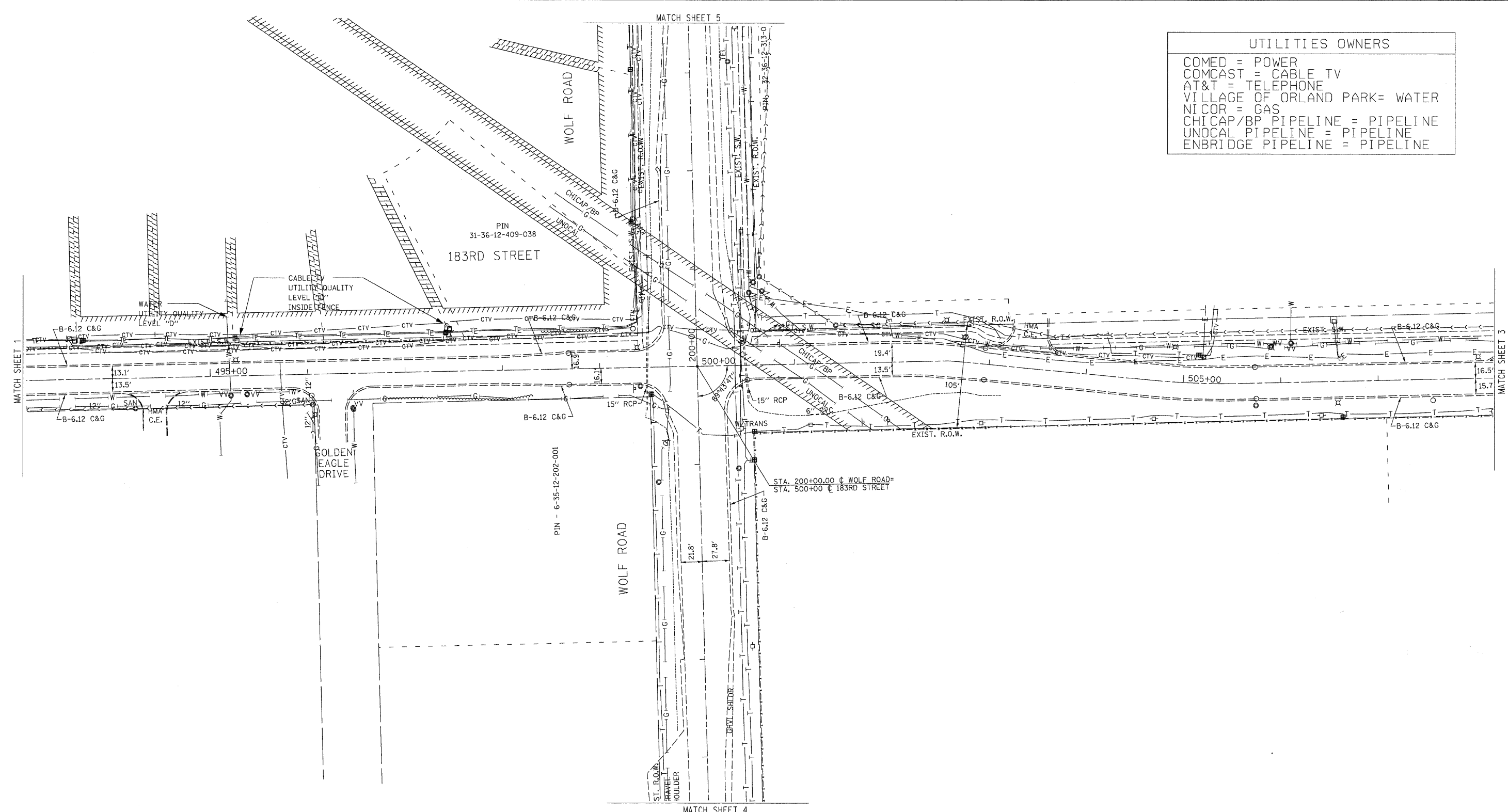
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Wolf Road at 183rd Street to Orland Parkway
Orland Park, IL

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
268B	44R-N	Cook	88	26
FED. ROAD DIST. NO. ILLINOIS IDOT Project No.			Contract No. 60K66	



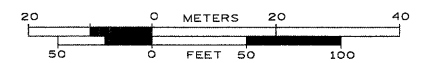
UTILITIES OWNERS	
COMED	= POWER
COMCAST	= CABLE TV
AT&T	= TELEPHONE
VILLAGE OF ORLAND PARK	= WATER
NICOR	= GAS
CHICAP/BP PIPELINE	= PIPELINE
UNOCAL PIPELINE	= PIPELINE
ENBRIDGE PIPELINE	= PIPELINE



— A — A —	AERIAL UTILITY UNKNOWN
— CTV — CTV —	CABLE TV
— T — T —	TELEPHONE
— G — G —	GAS
— E — E —	ELECTRIC
— W — W —	WATER
— FO — FO —	FIBER OPTIC
— S — S —	SEWER
— T — T —	TBE TEST HOLE

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TBE Job No. IL09510419, 10430
SUE Plan Page: 2 of 5

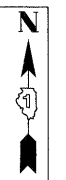
Utility Quality Level "A" : Test Hole
Utility Quality Level "B" : Designating
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

DESIGNED <i>IP</i>	REVISED
DRAWN <i>KLC</i>	REVISED
CHECKED	REVISED
DATE <i>12/03/10</i>	REVISED

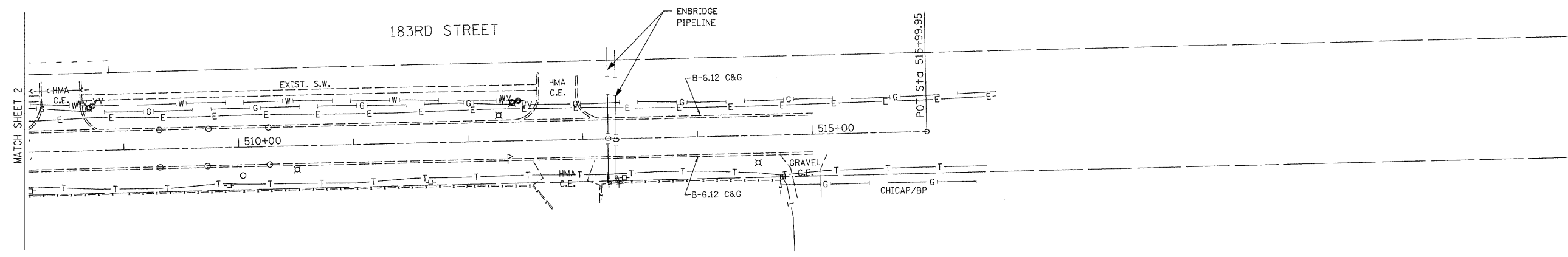
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**Wolf Road at 183rd Street to Orland Parkway
Orland Park, IL**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2G88	44R-N	Cook	68	27
Contract No. 60K66				
FED. ROAD DIST. NO. [ILLINOIS] IDOT Project No.				



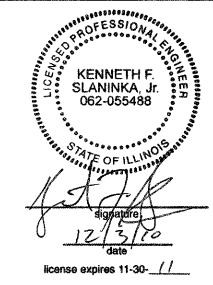
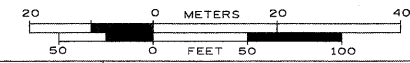
UTILITIES OWNERS	
COMED	= POWER
COMCAST	= CABLE TV
AT&T	= TELEPHONE
VILLAGE OF ORLAND PARK	= WATER
NICOR	= GAS
CHICAP/BP PIPELINE	= PIPELINE
UNOCAL PIPELINE	= PIPELINE
ENBRIDGE PIPELINE	= PIPELINE



— A — A —	AERIAL UTILITY
- - - - -	UNKNOWN
— CTV — CTV —	CABLE TV
— T — T —	TELEPHONE
— G — G —	GAS
— E — E —	ELECTRIC
— W — W —	WATER
— FO — FO —	FIBER OPTIC
— S — S —	SEWER
⊙	TBE TEST HOLE

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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510419, 10430
SUE Plan Page: 3 of 5

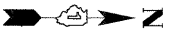
Utility Quality Level "A" : Test Hole
Utility Quality Level "B" : Designating
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

DESIGNED <i>IP</i>	REVISED
DRAWN <i>KLC</i>	REVISED
CHECKED	REVISED
DATE <i>12/03/10</i>	REVISED

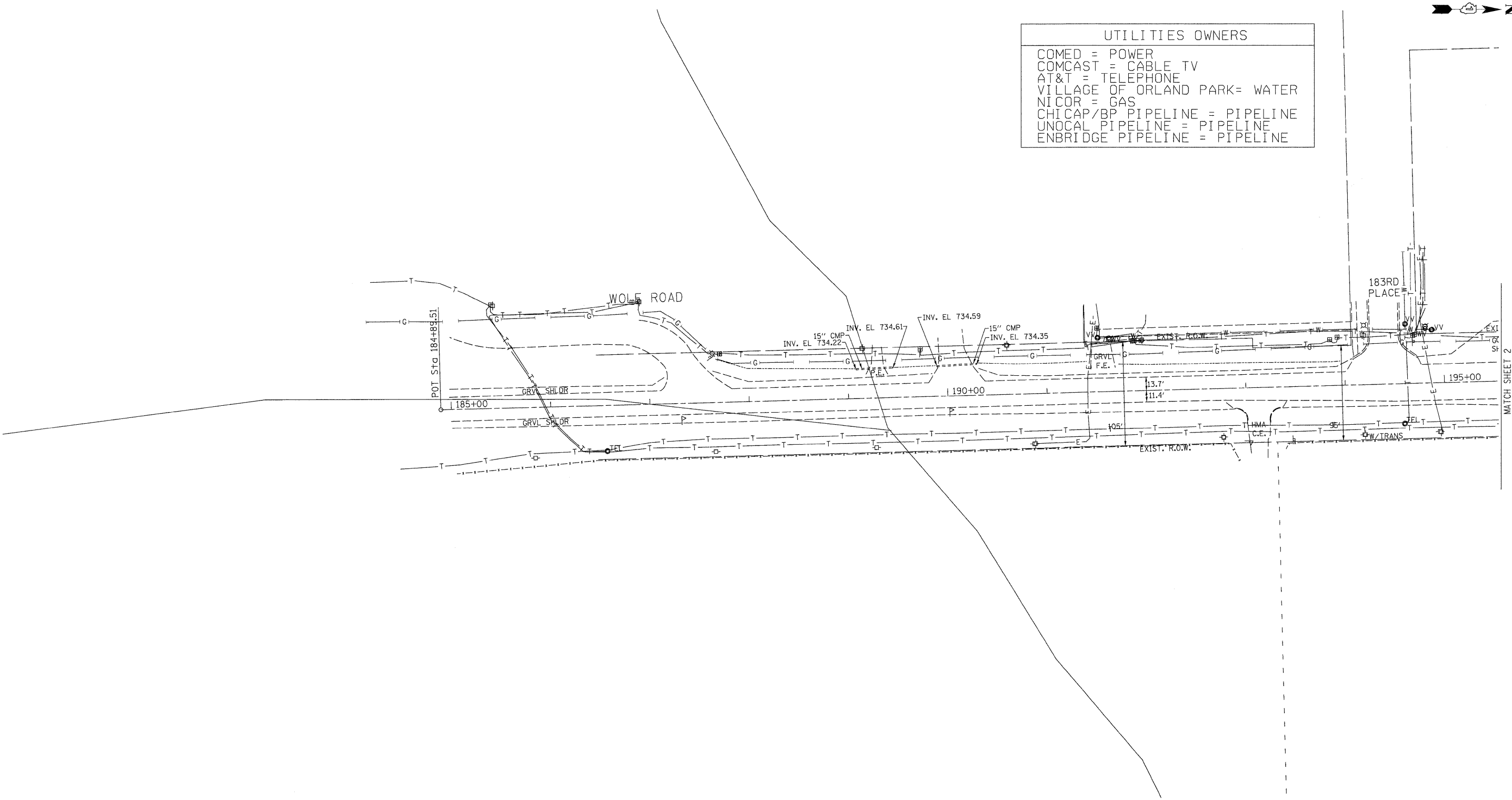
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Wolf Road at 183rd Street to Orland Parkway
Orland Park, IL

F.A.M. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
268B	44R-N	Cook	68	28
FED. ROAD DIST. NO. ILLINOIS IDOT Project No.			Contract No. 60K66	



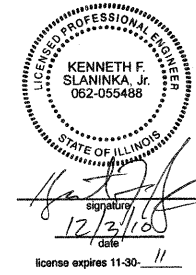
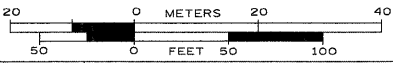
UTILITIES OWNERS	
COMED	= POWER
COMCAST	= CABLE TV
AT&T	= TELEPHONE
VILLAGE OF ORLAND PARK	= WATER
NICOR	= GAS
CHICAP/BP PIPELINE	= PIPELINE
UNOCAL PIPELINE	= PIPELINE
ENBRIDGE PIPELINE	= PIPELINE



— A — A —	AERIAL UTILITY
- - - - -	UNKNOWN
— CTV — CTV —	CABLE TV
— T — T —	TELEPHONE
— G — G —	GAS
— E — E —	ELECTRIC
— W — W —	WATER
— FO — FO —	FIBER OPTIC
—) —) —	SEWER
⊙	TBE TEST HOLE

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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. 1L09510419, 10430
SUE Plan Page: 4 of 5

Utility Quality Level "A" : Test Hole
Utility Quality Level "B" : Designating
Utility Quality Level "C" : Research with Survey
Utility Quality Level "D" : Records Research

DESIGNED	IP	REVISED	
DRAWN	KLC	REVISED	
CHECKED		REVISED	
DATE	12/03/10	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

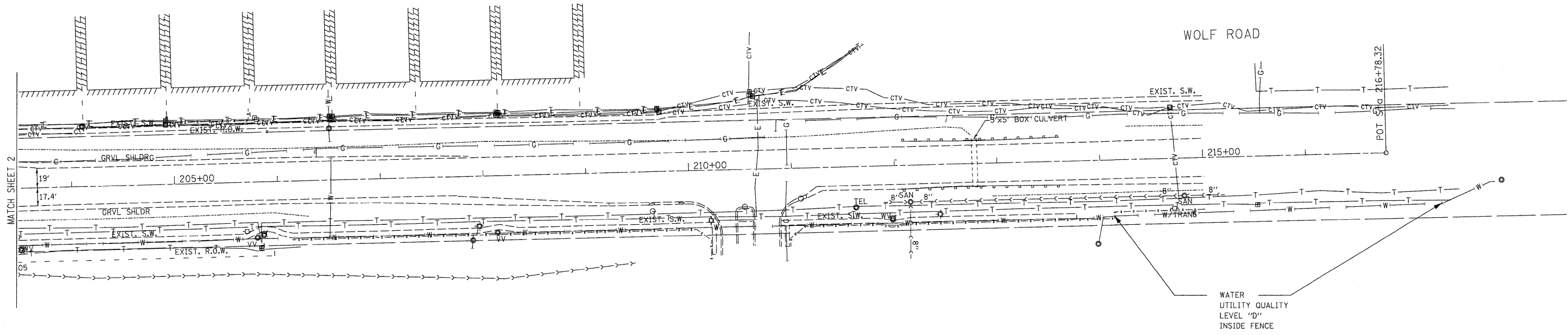
**Wolf Road at 183rd Street to Orland Parkway
Orland Park, IL**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2088	44R-N	Cook	68	29
Contract No. 60K66				
FED. ROAD DIST. NO.		ILLINOIS	IDOT Project No.	



UTILITIES OWNERS

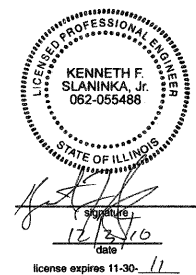
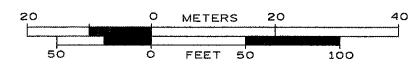
COMED = POWER
 COMCAST = CABLE TV
 AT&T = TELEPHONE
 VILLAGE OF ORLAND PARK = WATER
 NICOR = GAS
 CHICAP/BP PIPELINE = PIPELINE
 UNOCAL PIPELINE = PIPELINE
 ENBRIDGE PIPELINE = PIPELINE



- A — A — AERIAL UTILITY
- - - - - UNKNOWN
- CTV — CTV — CABLE TV
- T — T — TELEPHONE
- G — G — GAS
- E — E — ELECTRIC
- W — W — WATER
- FO — FO — FIBER OPTIC
- > -> -> SEWER
- ⊙ TBE TEST HOLE

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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510419, 10430
SUE Plan Page: 5 of 5

Utility Quality Level "A" : Test Hole
 Utility Quality Level "B" : Designating
 Utility Quality Level "C" : Research with Survey
 Utility Quality Level "D" : Records Research

DESIGNED <i>IP</i>	REVISED
DRAWN <i>KLC</i>	REVISED
CHECKED	REVISED
DATE <i>12/03/10</i>	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**Wolf Road at 183rd Street to Orland Parkway
Orland Park, IL**

F.A.U. RTE. <i>268B</i>	SECTION 44R-N	COUNTY Cook	TOTAL SHEETS 68	SHEET NO. 30
FED. ROAD DIST. No.		ILLINOIS IDOT Project No.		
Contract No. 60K66				

PART OF SECTION 31 & 32, TOWNSHIP 36 NORTH, RANGE 12 AND PART OF SECTION 6, TOWNSHIP 35 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK/WILL COUNTY, ILLINOIS

PARCEL NO.	OWNER	TOTAL HOLDING	PART TAKEN	REMAINDER	PREVIOUS DEDICATION	EASEMENT	EASEMENT PURPOSE	TAX NUMBER(S)	ACQUIRED BY
		(ACRES) SQ. FEET	(ACRES) SQ. FEET	(ACRES) SQ. FEET	(ACRES) SQ. FEET	(ACRES) SQ. FEET			
OHA0001	Kenneth Surma and Susan Surma, as husband and wife, as Tenants by the Entirety	(0.710)	(0.017)	(0.693)	N/A	N/A	N/A	27-31-409-038	
OHA0002	Parkview Christian Church of Orland Park, formerly known as Tinley Park Christian Church, an Illinois not-for-profit corporation	(15.987)	(0.003) 144	(15.984)	N/A	N/A	N/A	27-32-313-005	
OHA0003	Orland Parkway, LLC	(1.635)	(0.005) 220	(1.630)	N/A	N/A	N/A	09-06-202-001	

PROJECT COORDINATES FOR PROPOSED R.O.W.				
POINT NUMBER	STATION	OFFSET	NORTHING	EASTING
5100	STA.=498+62.95	33.29' LEFT	1781549.3331	1105391.8821
5101	STA.=499+20.90	45.80' LEFT	1781563.1378	1105449.5382
5102	STA.=499+38.83	64.31' LEFT	1781582.0375	1105467.0469
5102	STA.=200+64.60 *	60.87' LEFT	1781582.0375	1105467.0469
5103	STA.=500+58.87	53.00' LEFT	1781573.4118	1105587.3156
5103	STA.=200+52.72 *	59.12' RIGHT	1781573.4118	1105587.3156
5104	STA.=500+75.94	36.01' LEFT	1781556.8021	1105604.7586
5104	STA.=200+35.65 *	76.11' RIGHT	1781556.8021	1105604.7586
5105	STA.=499+18.21	32.70' RIGHT	1781484.5966	1105448.6012
5105	STA.=199+67.69 *	81.94' LEFT	1781484.5966	1105448.6012
5106	STA.=499+39.26	53.69' RIGHT	1781464.0769	1105470.1153
5106	STA.=199+46.59 *	60.99' LEFT	1781464.0769	1105470.1153

PROJECT COORDINATES FOR PROPOSED CENTERLINE				
POINT NUMBER	STATION	OFFSET	NORTHING	EASTING
5000	STA.=483+15.60	0.00' RIGHT	1781481.5442	1103845.6583
5001	STA.=493+00.00	0.00' RIGHT	1781503.4978	1104829.8135
5002	STA.=500+00.00	0.00' RIGHT	1781519.1089	1105529.6397
5002	STA.=200+00.00 *	0.00' LEFT	1781519.1089	1105529.6397
5003	STA.=502+53.07	0.00' RIGHT	1781524.7528	1105782.6483
5004	STA.=502+67.06	0.00' RIGHT	1781524.2321	1105796.6142
5005	STA.=504+65.02	0.00' RIGHT	1781505.0912	1105993.6498
5006	STA.=505+59.61	0.00' RIGHT	1781501.5978	1106088.1186
5007	STA.=511+00.00	0.00' RIGHT	1781513.9805	1106628.3667
5008	STA.=515+99.95	0.00' RIGHT	1781525.4365	1107128.1867
5009	STA.=184+89.51 *	0.00' LEFT	1780009.1716	1105570.4456
5010	STA.=193+00.00 *	0.00' LEFT	1780819.3644	1105548.5502
5011	STA.=207+00.00 *	0.00' LEFT	1782218.8534	1105510.7292
5012	STA.=216+78.32 *	0.00' LEFT	1783196.8128	1105484.3000

LEGEND

SECTION CORNER (0910/1615) QUARTER SECTION CORNER (16/15)

SECTION LINE
 QUARTER SECTION LINE
 QUARTER, QUARTER SECTION LINE
 PLATTED LOT LINE
 PROPERTY (DEED) LINE
 APPARENT PROPERTY LINE
 CENTERLINE
 EXISTING RIGHT OF WAY LINE
 PROPOSED CENTERLINE
 PROPOSED RIGHT OF WAY LINE
 PROPOSED EASEMENT
 MEASURED DIMENSION
 COMPUTED DIMENSION
 RECORD DATA

EXISTING BUILDING

3/4" IRON PIPE SET OR FOUND
 "MAG" NAIL SET OR FOUND
 5/8" REBAR SET OR FOUND
 "P.K" NAIL SET OR FOUND

T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY T3 COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

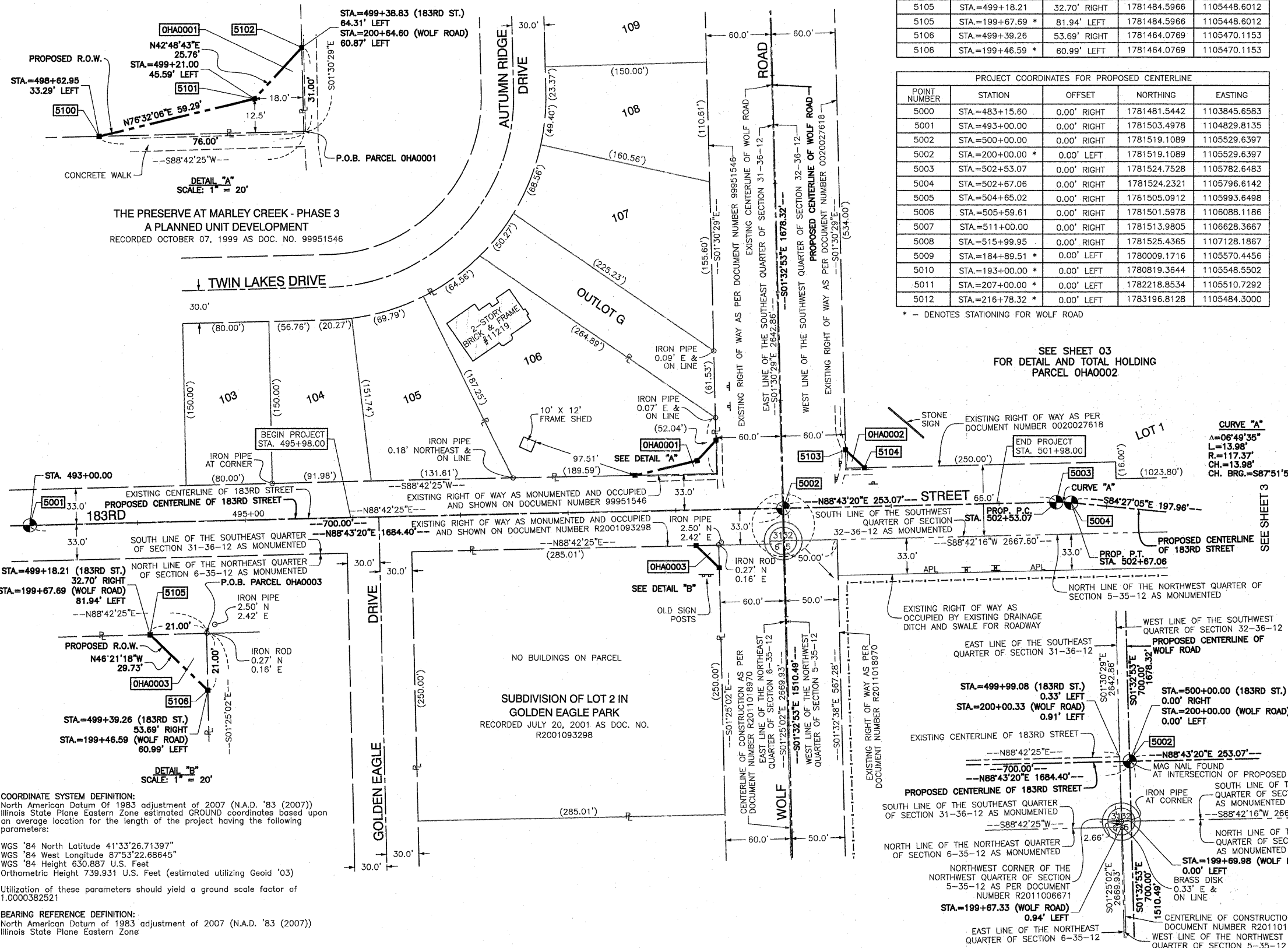
STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS)

RIGHT OF WAY STAKING PROPOSED TO BE SET.

SCALE: 1"=50'



SEE SHEET 03 FOR DETAIL AND TOTAL HOLDING PARCEL OHA0002

CURVE "A"
 Δ=06°49'35"
 L=13.98'
 R=117.37'
 CH.=13.98'
 CH. BRG.=S87°51'53"E

CURVE "B"
 Δ=06°49'35"
 L=13.98'
 R=117.37'
 CH.=13.98'
 CH. BRG.=S87°51'53"E



STATE OF ILLINOIS }
 COUNTY OF COOK } SS

THIS IS TO CERTIFY THAT I, RANDALL E. GANN, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 31 AND SECTION 32, TOWNSHIP 36 NORTH, RANGE 12 AND SECTION 6 AND SECTION 5, TOWNSHIP 35 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK/WILL COUNTY, ILLINOIS, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SOUTH HOLLAND, ILLINOIS THIS 10TH DAY OF AUGUST, 2010 A.D.

ILLINOIS REGISTERED LAND SURVEYOR NO. 035-003241
 EXPIRATION DATE: NOVEMBER 30, 2010

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

RECEIVED
 MAR 16 2011
 PLATS & LEGALS

Robinson ENGINEERING
 17000 SOUTH PARK AVENUE
 SOUTH HOLLAND, IL 60473
 (708) 331-6700

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 WOLF ROAD
 AT 183RD STREET

SECTION PROJECT: COOK/WILL COUNTY JOB NO. R-90-013-10
 SCALE: 1"=50' SHEET 2 OF 5

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAMBURG, ILLINOIS 60196

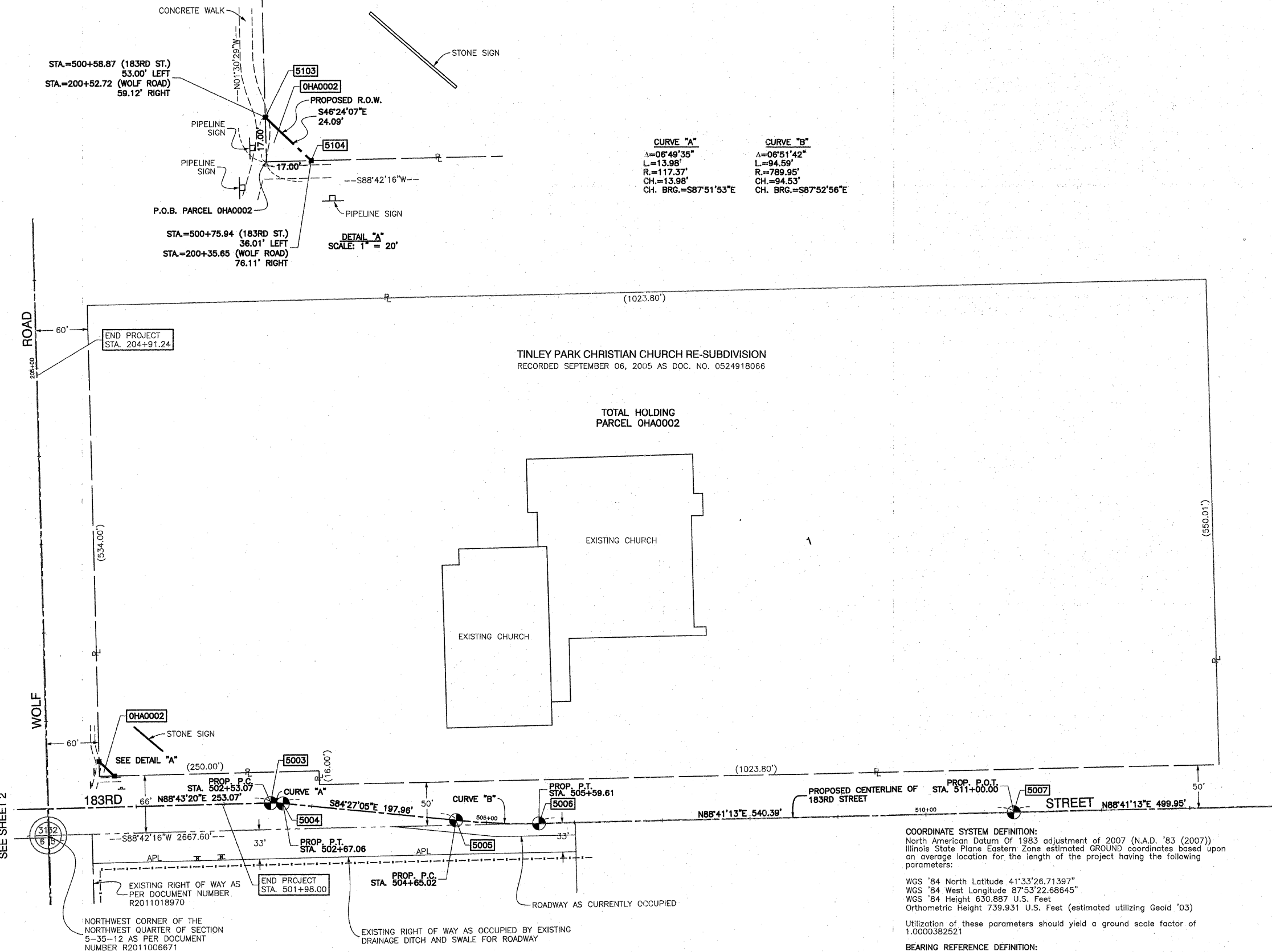
DATE	BY	R.O.W. PLAT	NOTEBOOK NO.

COORDINATE SYSTEM DEFINITION:
 North American Datum of 1983 adjustment of 2007 (N.A.D. '83 (2007))
 Illinois State Plane Eastern Zone estimated GROUND coordinates based upon an average location for the length of the project having the following parameters:
 WGS '84 North Latitude 41°33'26.71397"
 WGS '84 West Longitude 87°53'22.68645"
 WGS '84 Height 630.887 U.S. Feet
 Orthometric Height 739.931 U.S. Feet (estimated utilizing Geoid '03)
 Utilization of these parameters should yield a ground scale factor of 1.0000382521

BEARING REFERENCE DEFINITION:
 North American Datum of 1983 adjustment of 2007 (N.A.D. '83 (2007))
 Illinois State Plane Eastern Zone

PART OF SECTION 31 & 32, TOWNSHIP 36 NORTH, RANGE 12 AND PART OF SECTION 6, TOWNSHIP 35 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK/WILL COUNTY, ILLINOIS

PARCEL NO.	OWNER	TOTAL HOLDING	PART TAKEN	REMAINDER	PREVIOUS DEDICATION	EASEMENT	EASEMENT PURPOSE	TAX NUMBER(S)	ACQUIRED BY
		(ACRES) SQ. FEET	(ACRES) SQ. FEET	(ACRES) SQ. FEET	(ACRES) SQ. FEET	(ACRES) SQ. FEET			
OHA0002	Parkview Christian Church of Orland Park, formerly known as Tinley Park Christian Church, an Illinois not-for-profit corporation	(15.987)	(0.003) 144	(15.984)	N/A	N/A	N/A	27-32-313-005	



CURVE "A"
 $\Delta = 06^{\circ}49'35''$
 $L = 13.98'$
 $R = 117.37'$
 $CH = 13.98'$
 $CH. BRG. = S87^{\circ}51'53''E$

CURVE "B"
 $\Delta = 06^{\circ}51'42''$
 $L = 94.59'$
 $R = 789.95'$
 $CH = 94.53'$
 $CH. BRG. = S87^{\circ}52'56''E$

DETAIL "A"
 SCALE: 1" = 20'

LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED CENTERLINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING
- IRON PIPE OR ROD SET OR FOUND
- CUT CROSS SET OR FOUND
- CONCRETE R.O.W. MARKER FOUND
- 3/4" IRON PIPE SET OR FOUND
- "MAG" NAIL SET OR FOUND
- 5/8" REBAR SET OR FOUND
- "P.K." NAIL SET OR FOUND
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.



STATE OF ILLINOIS }
 COUNTY OF COOK } SS

THIS IS TO CERTIFY THAT I, RANDELL E. GANN, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 31 AND SECTION 32, TOWNSHIP 36 NORTH, RANGE 12 AND SECTION 6 AND SECTION 5, TOWNSHIP 35 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK/WILL COUNTY, ILLINOIS, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

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 17000 SOUTH PARK AVENUE
 SOUTH HOLLAND, IL 60473
 (708) 331-6700

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 WOLF ROAD
 AT 183RD STREET

SECTION PROJECT: COOK/WILL COUNTY JOB NO. R-90-013-10
 SCALE: 1"=60' SHEET 3 OF 5

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

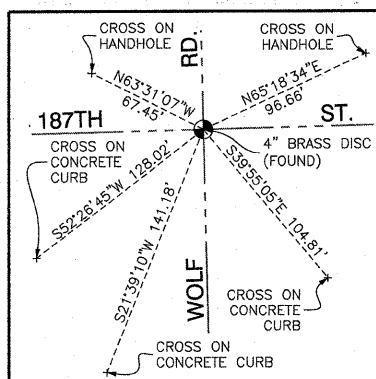
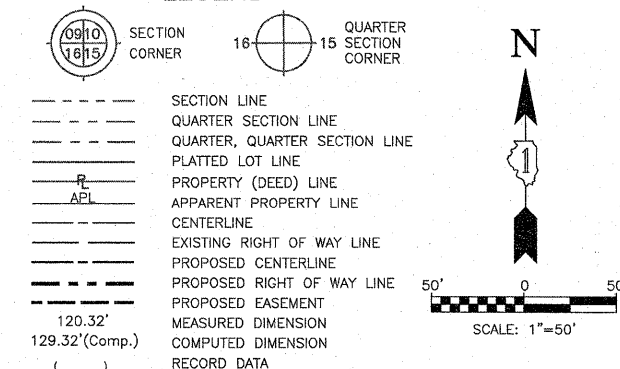
AS DOCUMENT NO. 68 32

COORDINATE SYSTEM DEFINITION:
 North American Datum of 1983 adjustment of 2007 (N.A.D. '83 (2007))
 Illinois State Plane Eastern Zone estimated GROUND coordinates based upon an average location for the length of the project having the following parameters:
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 WGS '84 West Longitude 87°53'22.68645"
 WGS '84 Height 630.887 U.S. Feet
 Orthometric Height 739.931 U.S. Feet (estimated utilizing Geoid '03)
 Utilization of these parameters should yield a ground scale factor of 1.0000382521

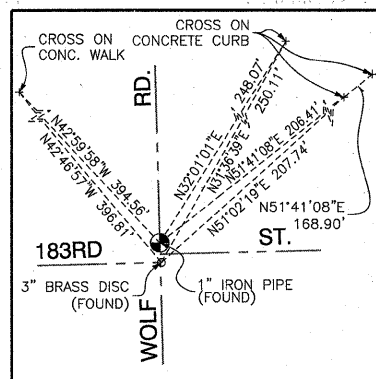
BEARING REFERENCE DEFINITION:
 North American Datum of 1983 adjustment of 2007 (N.A.D. '83 (2007))
 Illinois State Plane Eastern Zone

DATE	BY	R.O.W. PLAT	NOTEBOOK NO.

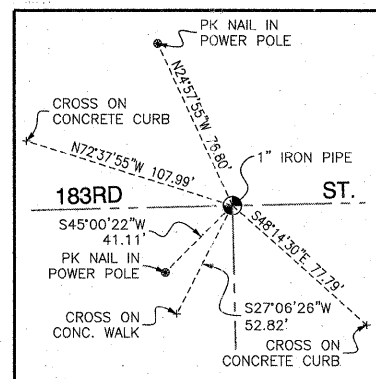
LEGEND



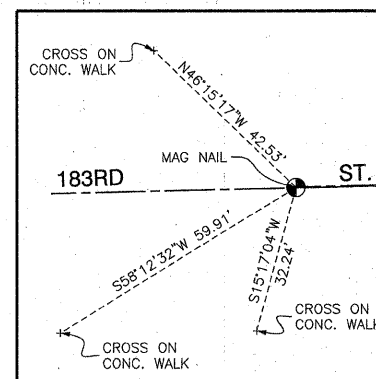
WEST QUARTER CORNER OF SECTION 5-35-12
RECORDED 01/14/11 AS DOC. NO. 2011006670
NORTHING: 1778819.9665 EASTING: 1105595.5464



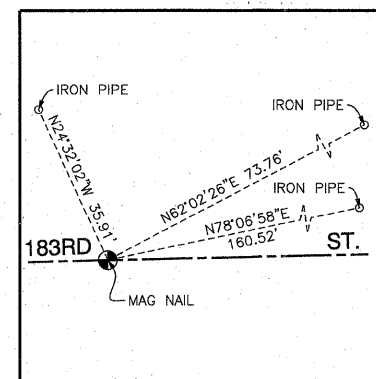
NORTHWEST CORNER OF SECTION 5-35-12
RECORDED 01/14/11 AS DOC. NO. 2011006671
NORTHING: 1781489.0946 EASTING: 1105529.4884



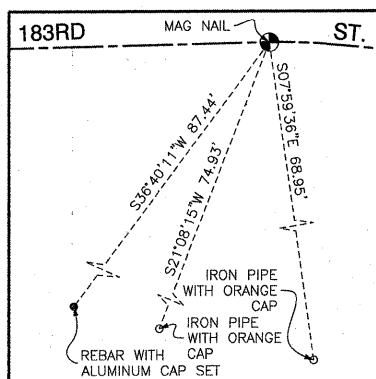
NORTH QUARTER CORNER OF SECTION 5-35-12
RECORDED 01/14/11 AS DOC. NO. 2011006672
NORTHING: 1781549.4020 EASTING: 1108196.4317



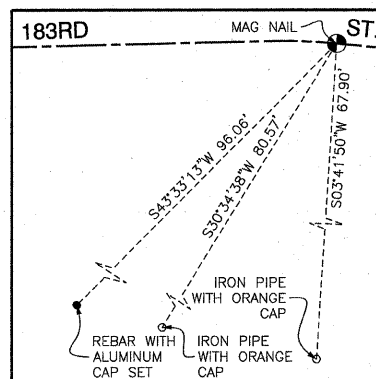
POINT NUMBER 5000
STATION: 483+15.60 OFFSET: 0.00' RIGHT
NORTHING: 1781481.5442 EASTING: 1103845.6583



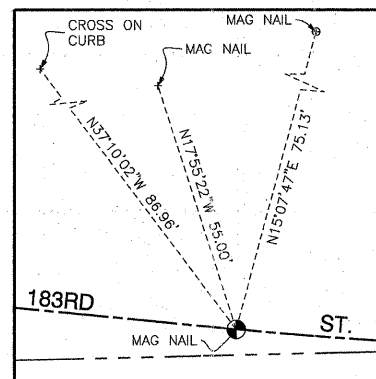
POINT NUMBER 5001
STATION: 493+00.00 OFFSET: 0.00' RIGHT
NORTHING: 1781503.4978 EASTING: 1104829.8135



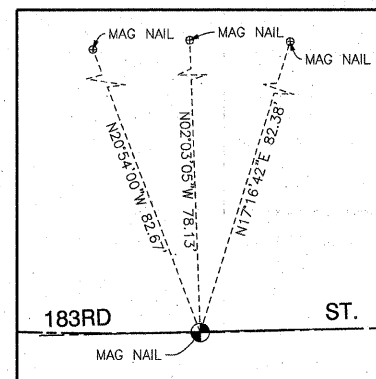
POINT NUMBER 5003
STATION: 502+53.07 OFFSET: 0.00' RIGHT
NORTHING: 1781524.7528 EASTING: 1105782.6483



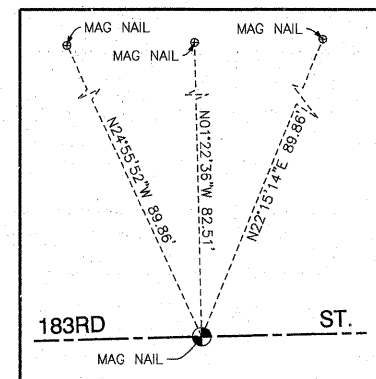
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NORTHING: 1781524.2321 EASTING: 1105796.6142



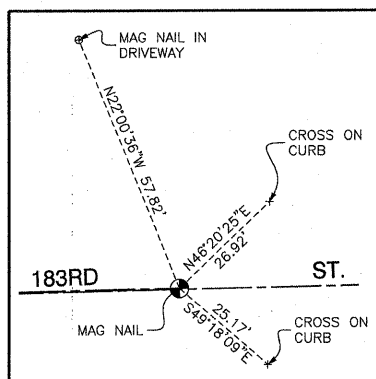
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NORTHING: 1781505.0912 EASTING: 1105993.6498



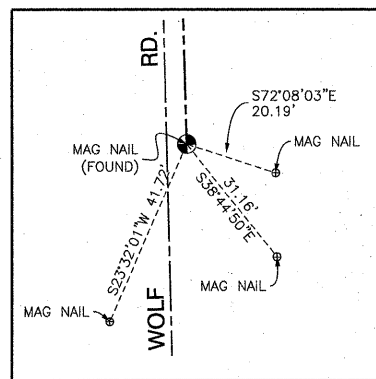
POINT NUMBER 5006
STATION: 505+59.61 OFFSET: 0.00' RIGHT
NORTHING: 1781501.5978 EASTING: 1106088.1186



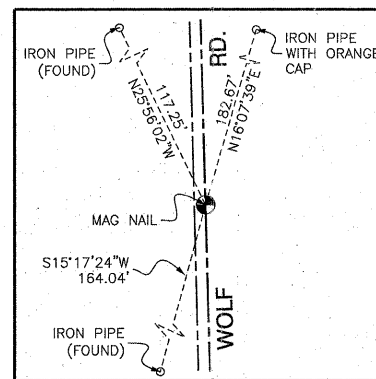
POINT NUMBER 5007
STATION: 511+00.00 OFFSET: 0.00' RIGHT
NORTHING: 1781513.9805 EASTING: 1106628.3667



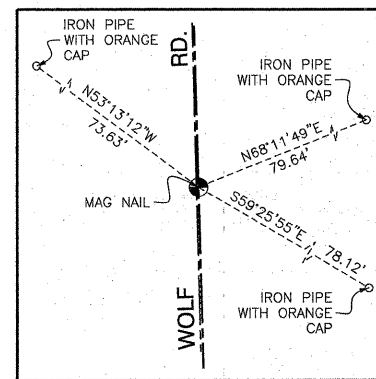
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STATION: 515+99.95 OFFSET: 0.00' RIGHT
NORTHING: 1781525.4365 EASTING: 1107128.1867



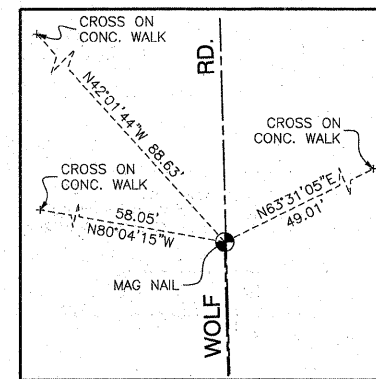
POINT NUMBER 5009
STATION: 184+89.51 OFFSET: 0.00' RIGHT
NORTHING: 1780009.1716 EASTING: 1105570.4456



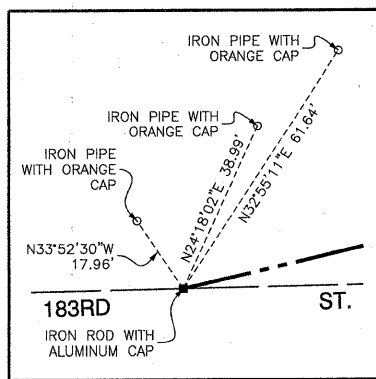
POINT NUMBER 5010
STATION: 193+00.00 OFFSET: 0.00' RIGHT
NORTHING: 1780819.3644 EASTING: 1105548.5502



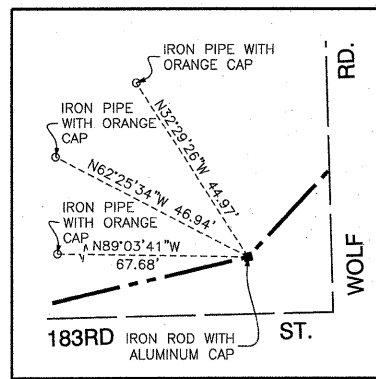
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STATION: 207+00.00 OFFSET: 0.00' RIGHT
NORTHING: 1782218.8534 EASTING: 1105510.7292



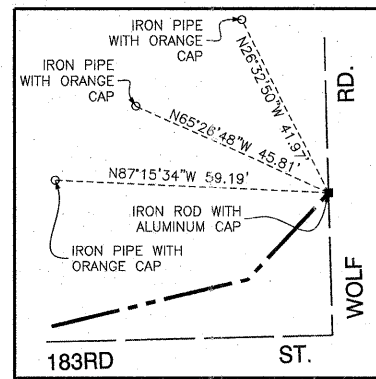
POINT NUMBER 5012
STATION: 218+78.32 OFFSET: 0.00' RIGHT
NORTHING: 1783196.8128 EASTING: 1105484.3000



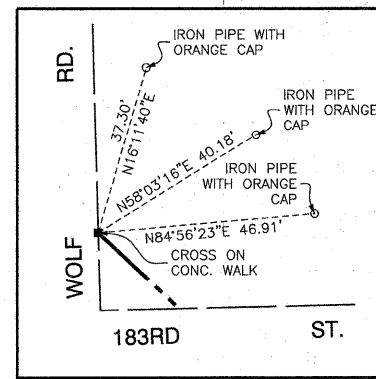
POINT NUMBER 5100
STATION: 498+62.95 OFFSET: 33.29' LEFT
NORTHING: 1781549.3331 EASTING: 1105391.8821



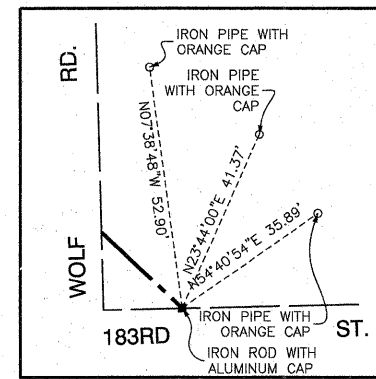
POINT NUMBER 5101
STATION: 499+38.83 OFFSET: 45.80' LEFT (183RD)
STATION: 200+46.18 OFFSET: 78.88' LEFT (WOLF)
NORTHING: 1781563.1378 EASTING: 1105449.5382



POINT NUMBER 5102
STATION: 499+38.83 OFFSET: 64.31' LEFT (183RD)
STATION: 200+64.60 OFFSET: 60.87' LEFT (WOLF)
NORTHING: 1781582.0375 EASTING: 1105487.0469



POINT NUMBER 5103
STATION: 500+58.87 OFFSET: 53.00' LEFT (183RD)
STATION: 200+52.72 OFFSET: 59.12' RIGHT (WOLF)
NORTHING: 1781573.4118 EASTING: 1105587.3156



POINT NUMBER 5104
STATION: 500+75.94 OFFSET: 36.01' LEFT (183RD)
STATION: 200+35.65 OFFSET: 76.11' RIGHT (WOLF)
NORTHING: 1781556.8021 EASTING: 1105604.7586

DATE	BY	R.O.W. PLAT	NOTEBOOK NO.



STATE OF ILLINOIS }
COUNTY OF COOK } SS

THIS IS TO CERTIFY THAT I, RANDELL E. GANN, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 31 AND SECTION 32, TOWNSHIP 36 NORTH, RANGE 12 AND SECTION 6 AND SECTION 5, TOWNSHIP 35 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK/WILL COUNTY, ILLINOIS, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SOUTH HOLLAND, ILLINOIS THIS 10TH DAY OF AUGUST, 2010 A.D.

ILLINOIS REGISTERED LAND SURVEYOR NO. 035-003241
EXPIRATION DATE: NOVEMBER 30, 2010

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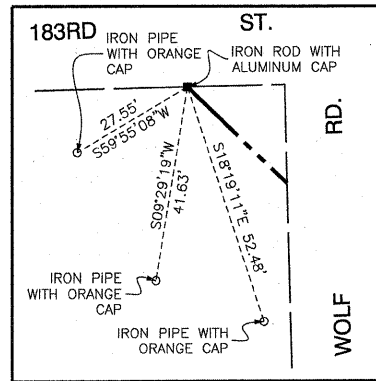
Robinson ENGINEERING
17000 SOUTH PARK AVENUE
SOUTH HOLLAND, IL 60473
(708) 331-6700

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
WOLF ROAD
AT 183RD STREET

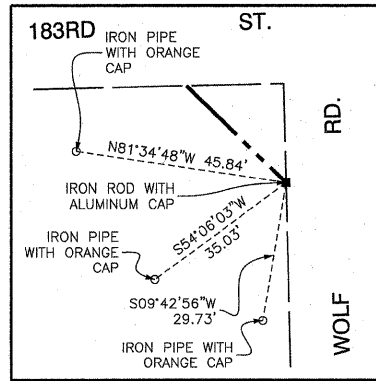
SECTION PROJECT: COOK/WILL COUNTY JOB NO. R-90-013-10
SCALE: 1"=50' SHEET 4 OF 5

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PART OF SECTION 31 & 32, TOWNSHIP 36 NORTH, RANGE 12 AND PART OF SECTION 5 & 6, TOWNSHIP 35 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK/WILL COUNTY, ILLINOIS



POINT NUMBER 5105
 STATION: 499+18.21 OFFSET: 32.70' RIGHT (183RD)
 STATION: 199+67.69 OFFSET: 81.94' LEFT (WOLF)
 NORTHING: 1781484.5966 EASTING: 1105448.6012



POINT NUMBER 5106
 STATION: 499+39.26 OFFSET: 53.69' RIGHT (183RD)
 STATION: 199+46.59 OFFSET: 60.99' LEFT (WOLF)
 NORTHING: 1781464.0769 EASTING: 1105470.1153

LEGEND

SECTION CORNER 16 15 QUARTER SECTION CORNER

SECTION LINE
 QUARTER SECTION LINE
 QUARTER, QUARTER SECTION LINE
 PLATTED LOT LINE
 PROPERTY (DEED) LINE
 APPARENT PROPERTY LINE
 CENTERLINE
 EXISTING RIGHT OF WAY LINE
 PROPOSED CENTERLINE
 PROPOSED RIGHT OF WAY LINE
 PROPOSED EASEMENT
 MEASURED DIMENSION
 COMPUTED DIMENSION
 RECORD DATA

EXISTING BUILDING

3/4" IRON PIPE SET OR FOUND
 "MAG" NAIL SET OR FOUND
 5/8" REBAR SET OR FOUND
 "P.K." NAIL SET OR FOUND

T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
 T2
 T3

BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
 BT2
 BT3

STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

PERMANENT SURVEY MARKER, I.D.O.T. STD 2135 (TO BE SET BY OTHERS)

RIGHT OF WAY STAKING PROPOSED TO BE SET.



STATE OF ILLINOIS)
 COUNTY OF COOK) SS

THIS IS TO CERTIFY THAT I, RANDELL E. GANN, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 31 AND SECTION 32, TOWNSHIP 36 NORTH, RANGE 12 AND SECTION 6 AND SECTION 5, TOWNSHIP 35 NORTH, RANGE 12 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK/WILL COUNTY, ILLINOIS, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT SOUTH HOLLAND, ILLINOIS THIS 10TH DAY OF AUGUST, 2010 A.D.

ILLINOIS REGISTERED LAND SURVEYOR NO. 035-003241
 EXPIRATION DATE: NOVEMBER 30, 2010

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

RECEIVED
 MAR 16 2011
 PLATS & LEGALS

Robinson
 ENGINEERING
 17000 SOUTH PARK AVENUE
 SOUTH HOLLAND, IL 60473
 (708) 331-6700

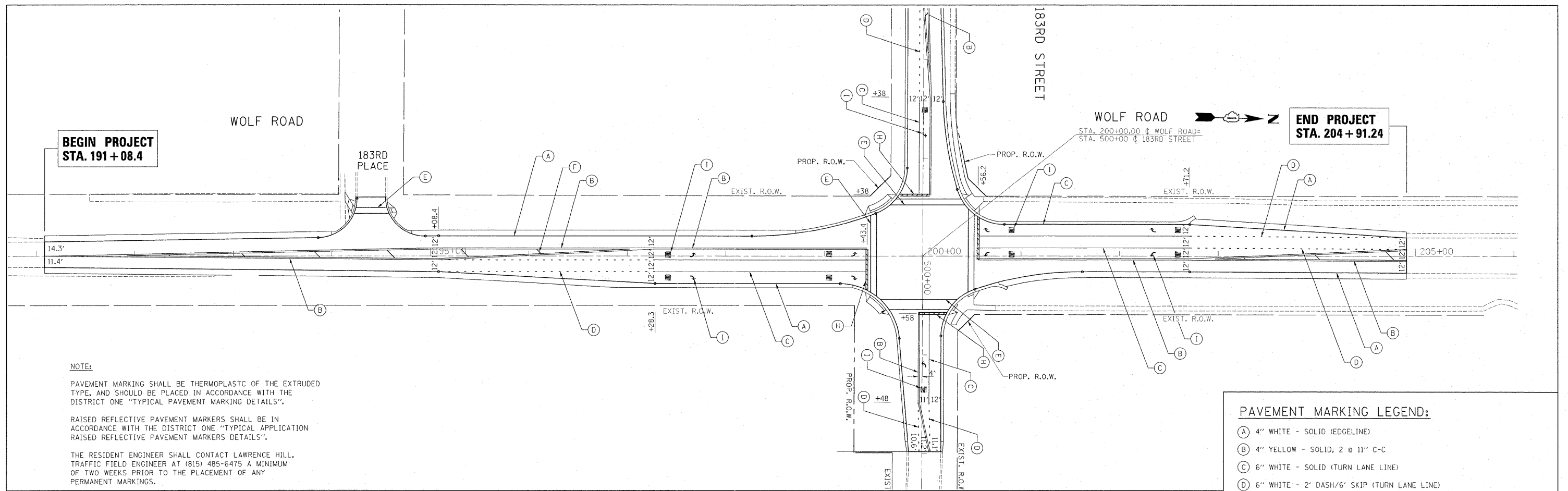
PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 WOLF ROAD
 AT 183RD STREET

SECTION PROJECT COOK/WILL COUNTY
 JOB NO. R-90-013-10

SCALE: 1"=50' SHEET 5 OF 5
 68 34

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

DATE	BY	R.O.W. PLAT	NOTEBOOK NO.



NOTE:

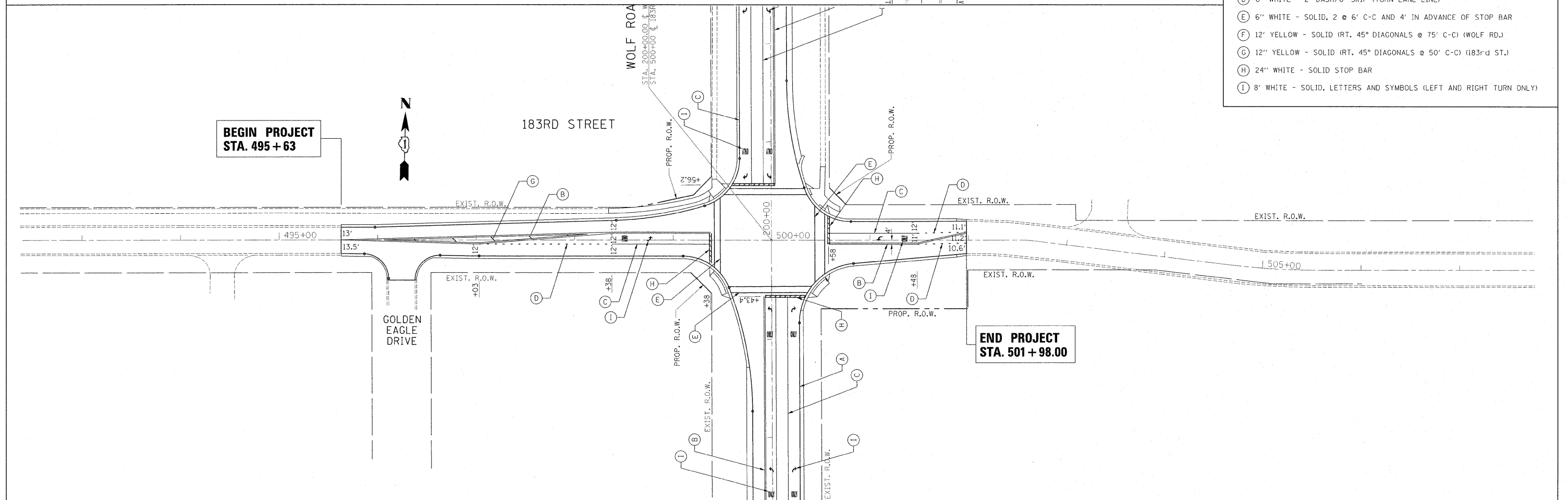
PAVEMENT MARKING SHALL BE THERMOPLASTIC OF THE EXTRUDED TYPE, AND SHOULD BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL PAVEMENT MARKING DETAILS".

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS".

THE RESIDENT ENGINEER SHALL CONTACT LAWRENCE HILL, TRAFFIC FIELD ENGINEER AT (815) 485-6475 A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF ANY PERMANENT MARKINGS.

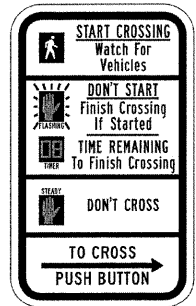
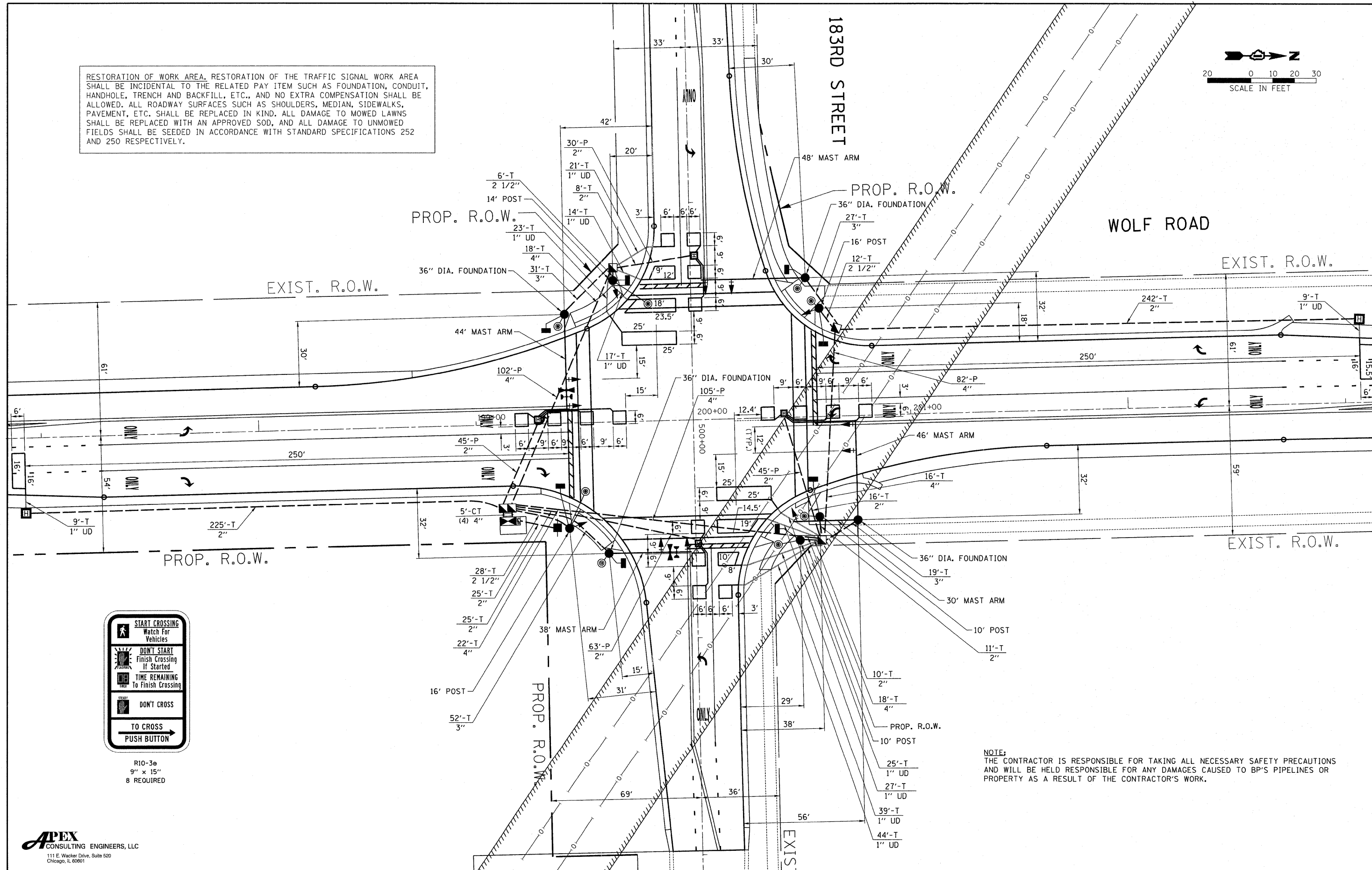
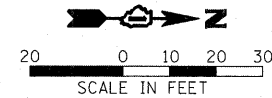
PAVEMENT MARKING LEGEND:

- (A) 4" WHITE - SOLID (EDGE LINE)
- (B) 4" YELLOW - SOLID, 2 @ 11" C-C
- (C) 6" WHITE - SOLID (TURN LANE LINE)
- (D) 6" WHITE - 2' DASH/6' SKIP (TURN LANE LINE)
- (E) 6" WHITE - SOLID, 2 @ 6' C-C AND 4' IN ADVANCE OF STOP BAR
- (F) 12" YELLOW - SOLID (RT. 45° DIAGONALS @ 75' C-C) (WOLF RD.)
- (G) 12" YELLOW - SOLID (RT. 45° DIAGONALS @ 50' C-C) (183rd ST.)
- (H) 24" WHITE - SOLID STOP BAR
- (I) 8" WHITE - SOLID, LETTERS AND SYMBOLS (LEFT AND RIGHT TURN ONLY)



FILE NAME = P143409-shr-pmk.dgn	USER NAME = galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WOLF ROAD @ 183RD STREET PROPOSED PAVEMENT MARKING PLAN	F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK	TOTAL SHEETS 68	SHEET NO. 35	
PLOT SCALE = 50,0000 1/4 IN.	CHECKED -	REVISED -	SCALE: 1" = 50'			SHEET NO. OF SHEETS	STA. 184+89.51 TO STA. 213+00.00	ILLINOIS FED. AID PROJECT			
PLOT DATE = 3/8/2011	DATE -	REVISED -									
CONTRACT NO. 60K66											

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

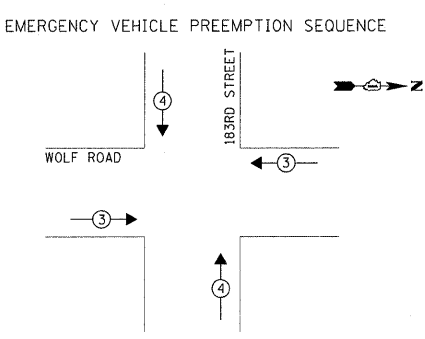
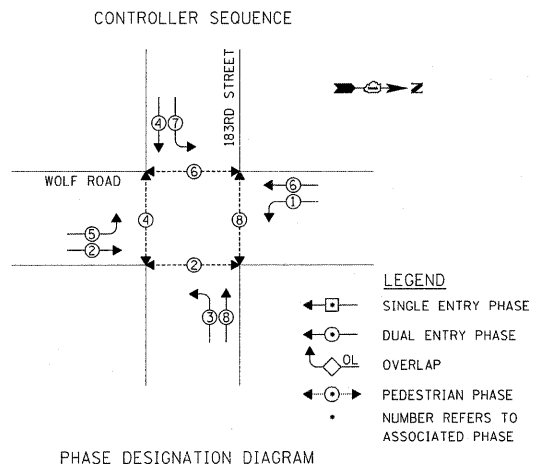


R10-3e
9" x 15"
8 REQUIRED

NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR TAKING ALL NECESSARY SAFETY PRECAUTIONS AND WILL BE HELD RESPONSIBLE FOR ANY DAMAGES CAUSED TO BP'S PIPELINES OR PROPERTY AS A RESULT OF THE CONTRACTOR'S WORK.

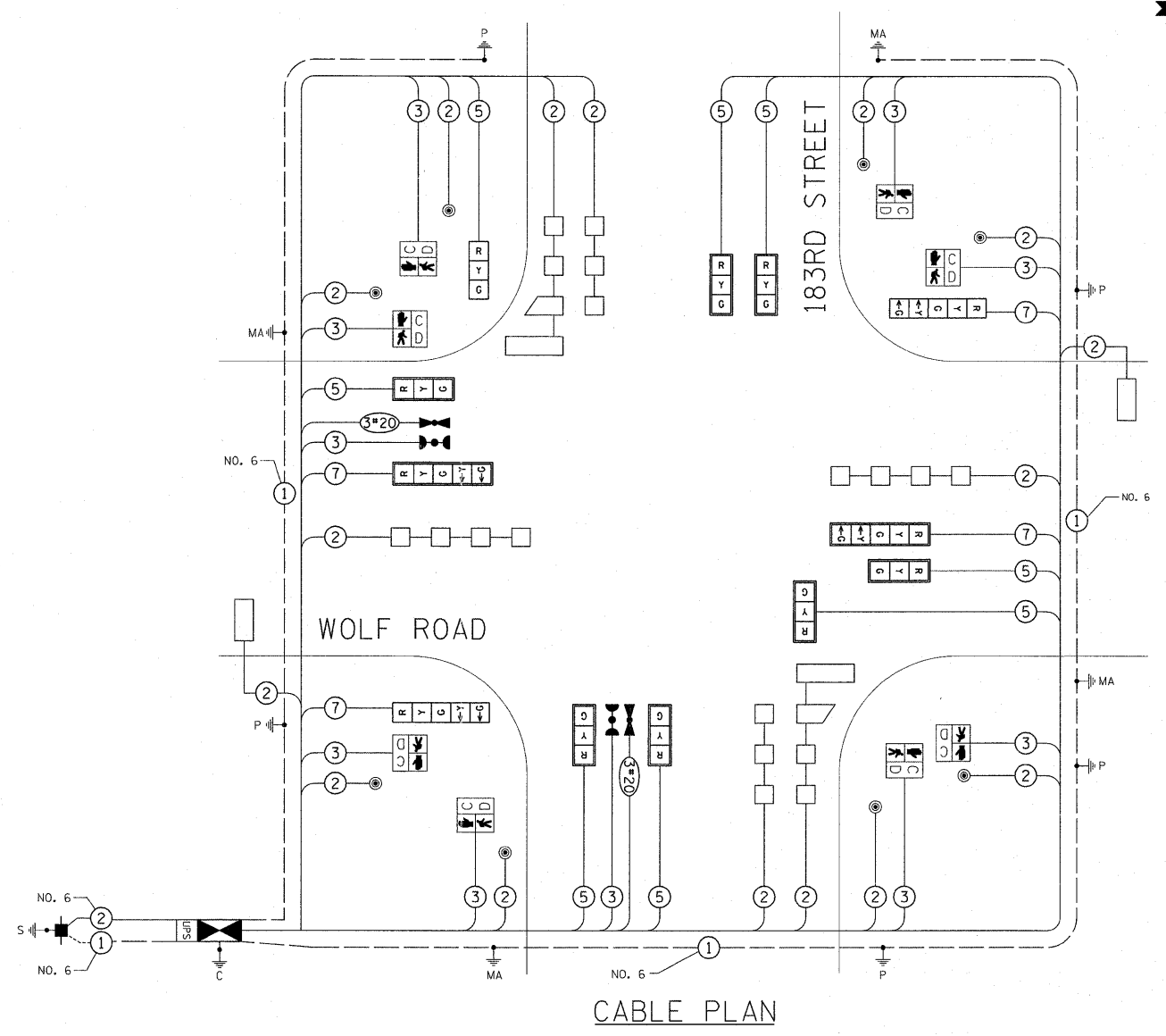
APEX
CONSULTING ENGINEERS, LLC
111 E. Wacker Drive, Suite 520
Chicago, IL 60601

FILE NAME =	USER NAME = wigram	DESIGNED - WHI	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	WOLF ROAD @ 183RD STREET PROPOSED TRAFFIC SIGNAL PLAN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
#FILEL\$		DRAWN - WHI	REVISED -			2688	44 R-N	COOK	68	35	
	PLOT SCALE = #SCALE\$	CHECKED - DEB	REVISED -			CONTRACT NO. 60K66					
	PLOT DATE = 3/24/2011	DATE - 2/25/2011	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 1"= 20'	SHEET NO. OF SHEETS STA. TO STA.					



PROPOSED EMERGENCY VEHICLE PREEMPTOR

EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↑ ↓



RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

SCHEDULE OF QUANTITIES

QUANTITY	UNIT	ITEM	QUANTITY	UNIT	ITEM
25.5	SO FT	SIGN PANEL - TYPE 1	1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 48 FT.
562	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	20	FOOT	CONCRETE FOUNDATION, TYPE A
46	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	4	FOOT	CONCRETE FOUNDATION, TYPE C
129	FOOT	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	51	FOOT	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER
94	FOOT	CONDUIT IN TRENCH, 4" DIA., GALVANIZED STEEL	6	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED
183	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	2	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, BRACKET MOUNTED
289	FOOT	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	2	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, MAST ARM MOUNTED
3	EACH	HANDHOLE	2	EACH	SIGNAL HEAD, L.E.D., 1-FACE, 5-SECTION, BRACKET MOUNTED
6	EACH	HEAVY-DUTY HANDHOLE	8	EACH	PEDESTRIAN SIGNAL HEAD, L.E.D., 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER
1	EACH	DOUBLE HANDHOLE	8	EACH	TRAFFIC SIGNAL BACKPLATE, LOUVERED, ALUMINUM
1044	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK	8	EACH	INDUCTIVE LOOP DETECTOR
1	EACH	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET	8	EACH	DETECTOR LOOP, TYPE I
1545	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	1249	FOOT	DETECTOR LOOP, TYPE I
2015	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	* 2	EACH	LIGHT DETECTOR
1975	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	* 1	EACH	LIGHT DETECTOR AMPLIFIER
920	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	8	EACH	PEDESTRIAN PUSH-BUTTON
1795	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1-PAIR	1	EACH	SERVICE INSTALLATION, POLE MOUNT
65	FOOT	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	1	EACH	UNINTERRUPTIBLE POWER SUPPLY
2	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	1775	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1C
1	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	* 395	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED
2	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	1	EACH	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 30 FT. AND 46 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 38 FT.			
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.			

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS

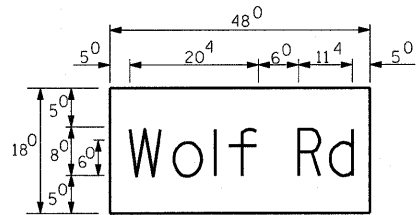
TYPE	NO. LAMPS	WATTAGE INCAND.	LED	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	12		17	0.50	102.00
(YELLOW)	12		25	0.25	75.00
(GREEN)	12		15	0.25	45.00
ARROW	8		12	0.10	9.60
PED. SIGNAL	8		25.29	1.00	232.00
CONTROLLER	1		100	1.00	100.00
ILLUM. SIGN	-		25	0.05	-
FLASHER					0.05
ENERGY COSTS TO:					TOTAL = 563.60

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAY/DISTRICT 1
 201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096
 ENERGY SUPPLY CONTACT: KATHERINE SUGRUE
 PHONE: (708) 235-2337
 COMPANY: COMMONWEALTH EDISON

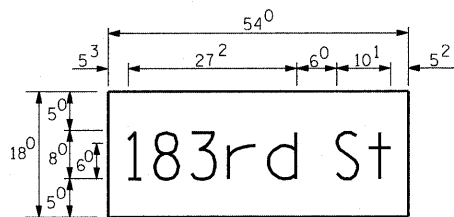
* 100% COST TO THE ORLAND FIRE PROTECTION DISTRICT

APEX CONSULTING ENGINEERS, LLC
 111 E. Wacker Drive, Suite 520
 Chicago, IL 60601

PANEL SIGN DESIGN TYPE 1

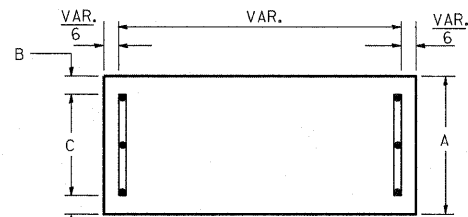


— Sq. M. each
6.00 Sq. Ft. each
— 2 Required
Design Series D

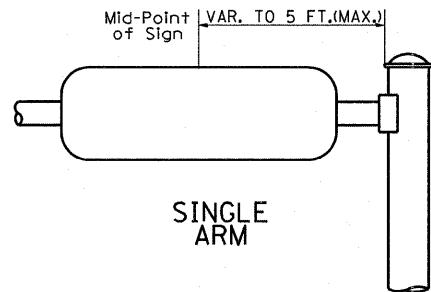


— Sq. M. each
6.75 Sq. Ft. each
— 2 Required
Design Series D

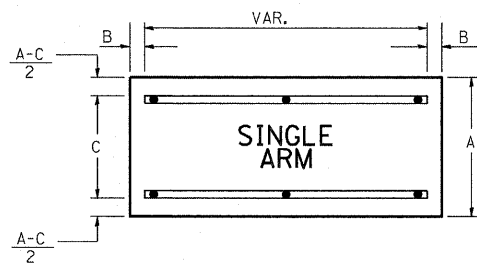
SUPPORTING CHANNELS



A	B	C
18"	2"	14"



SUPPORTING CHANNELS



A	B	C
18"	2"	12"
30"	2"	22"

DUAL ARM

SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM
Shall be used. See Note #5.

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS B34001, B34006 AND B34011, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 6'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
 - ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
 - THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 6'-0".
 - ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
 - SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:
 - * A.K.T. CORPORATION SCHAUMBURG, IL
 - * TUCKER COMPANY, INC. WAUWATOSA, WI
 - * AMERICAN FABRICATION CO. CHICAGO HEIGHTS, IL
 - * WESTERN TRAFFIC CONTROL INC. CICERO, IL
- PARTS LISTING:
SIGN CHANNEL PART *HPN053 (MED. CHANNEL)
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
BRACKETS PART *HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING
- OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

Upper Case To Lower Case
Spacing Chart 8-6 Inch Series "C & D"

EXAMPLE, 2 DENOTES 3/8"

SERIES	SECOND LETTER															
	acde		bhikl		f w		j		s t		v y		x		z	
	g	o	q	m	n	p	r	u								
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15
D O O R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21

Lower Case To Lower Case
Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER															
	acde		bhikl		f w		j		s t		v y		x		z	
	g	o	q	m	n	p	r	u								
ad h g i j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17
l m n q u																
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14

Number To Number
Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21
2 3 4	14	15	14	15	14	15	12	14	12	14	14	15	14	15	11	12	16	17	14	15
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15
6	16	17	14	15	14	15	12	15	12	14	14	15	14	15	11	12	14	15	14	15
7	12	14	12	14	14	15	12	15	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	15	12	14	14	15	16	17	12	14	16	17	14	15

UPPER AND LOWER CASE LETTER WIDTHS

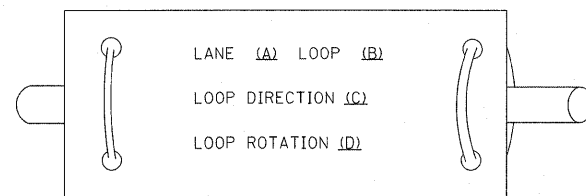
LETTERS	6 INCH UPPER CASE LETTERS				8 INCH UPPER CASE LETTERS				LETTERS	6 INCH LOWER CASE LETTERS			
	SERIES		SERIES		SERIES		SERIES			C	D	C	D
	C	D	C	D	C	D	C	D					
A	36	50	50	65	a	35	42						
B	32	40	43	53	b	35	42						
C	32	40	43	53	c	35	41						
D	32	40	43	53	d	35	42						
E	30	35	40	47	e	35	42						
F	30	35	40	47	f	23	26						
G	32	40	43	53	g	35	42						
H	32	40	43	53	h	35	42						
I	07	07	11	12	i	11	11						
J	30	36	40	50	j	20	22						
K	32	41	43	54	k	35	42						
L	30	35	40	47	l	11	11						
M	37	45	51	61	m	60	70						
N	32	40	43	53	n	35	42						
O	34	42	45	55	o	36	43						
P	32	40	43	53	p	35	42						
Q	34	42	45	55	q	35	42						
R	32	40	43	53	r	26	32						
S	32	40	43	53	s	36	42						
T	30	35	40	47	t	27	32						
U	32	40	43	53	u	35	42						
V	35	44	47	60	v	42	47						
W	44	52	60	70	w	55	64						
X	34	40	45	53	x	44	51						
Y	36	50	50	66	y	46	53						
Z	32	40	43	53	z	36	43						

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	12	14	15	20
2	32	40	43	53
3	32	40	43	53
4	35	43	47	57
5	32	40	43	53
6	32	40	43	53
7	32	40	43	53
8	32	40	43	53
9	32	40	43	53
0	34	42	45	55

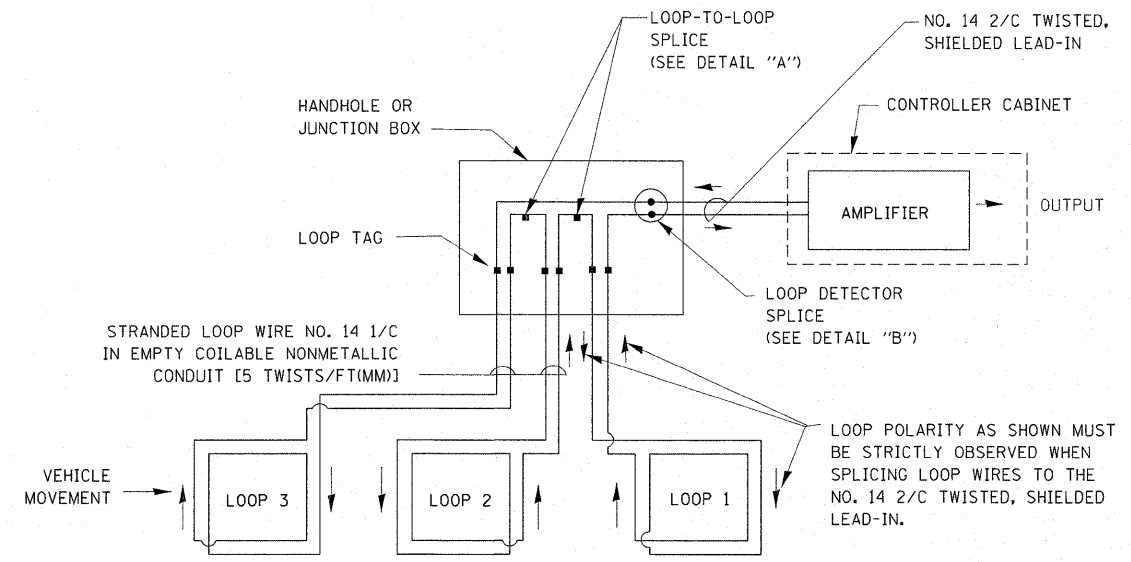
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

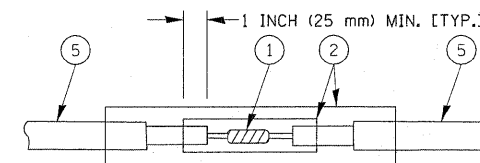


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

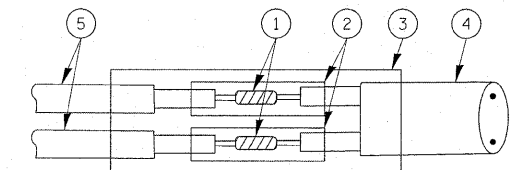


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

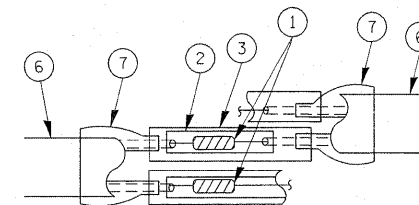


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

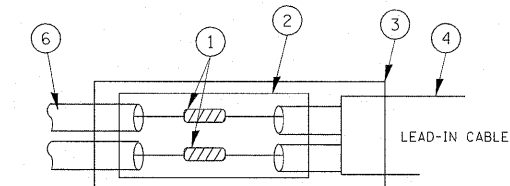


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

PREFORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = kanthaphixaybc	DESIGNED - DAD	REVISED -
c:\pwork\pwork\DOT\KANTHAPHIXAYBC\d0112614\trf\trf\legend.v7.dgn		DRAWN - BCK	REVISED -
PLOT SCALE = 20,0000' / IN.		CHECKED - DAD	REVISED -
PLOT DATE = 10/6/2009		DATE - 10/28/09	REVISED -

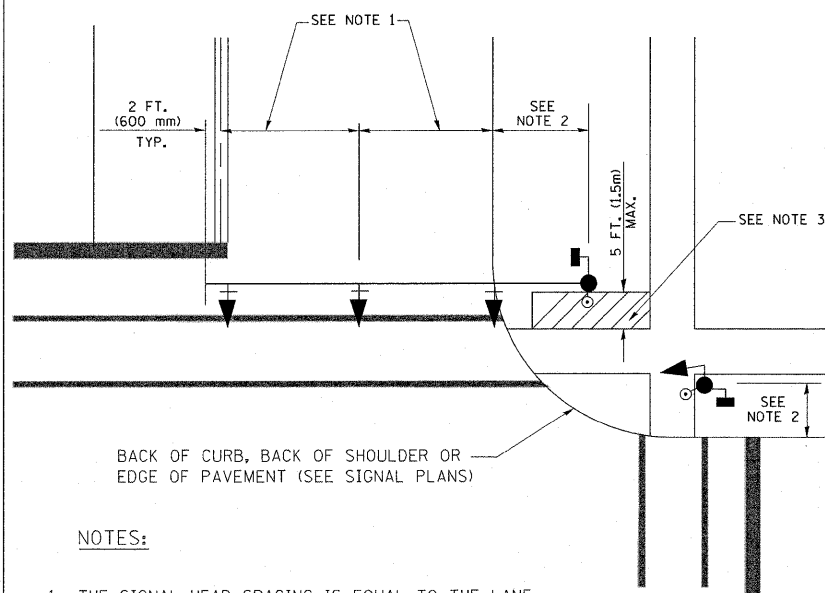
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

F.A.U. RTE. 2688	SECTION 44 R-11	COUNTY COCK/WILL	TOTAL SHEETS 68	SHEET NO. 39
SCALE:		SHEET NO. 1 OF 6 SHEETS		STA. TO STA.
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 60K65		

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.

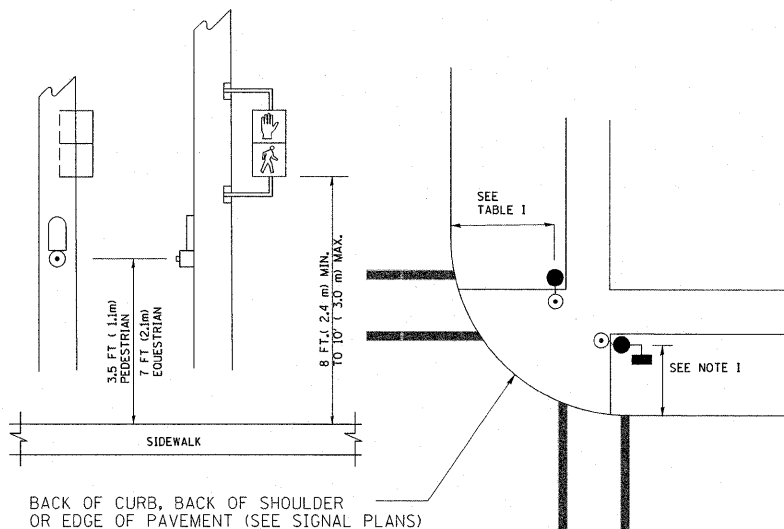


BACK OF CURB, BACK OF SHOULDER OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST

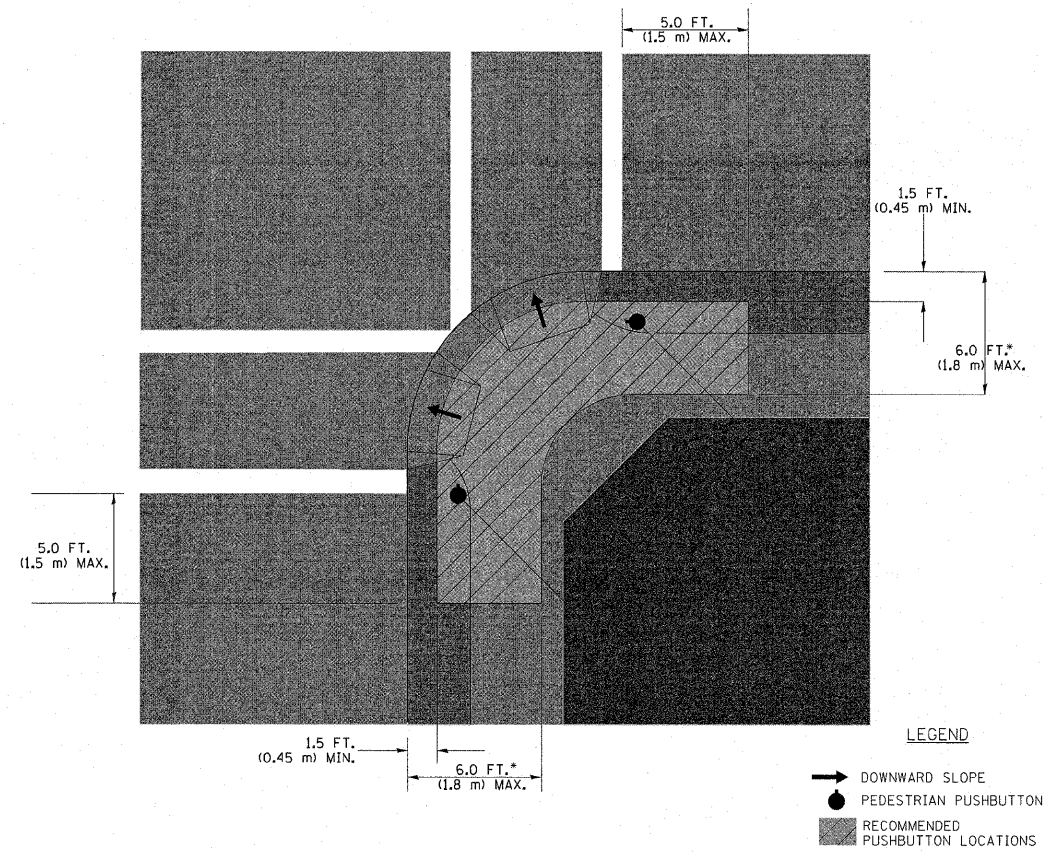


BACK OF CURB, BACK OF SHOULDER OR EDGE OF PAVEMENT (SEE SIGNAL PLANS)

NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

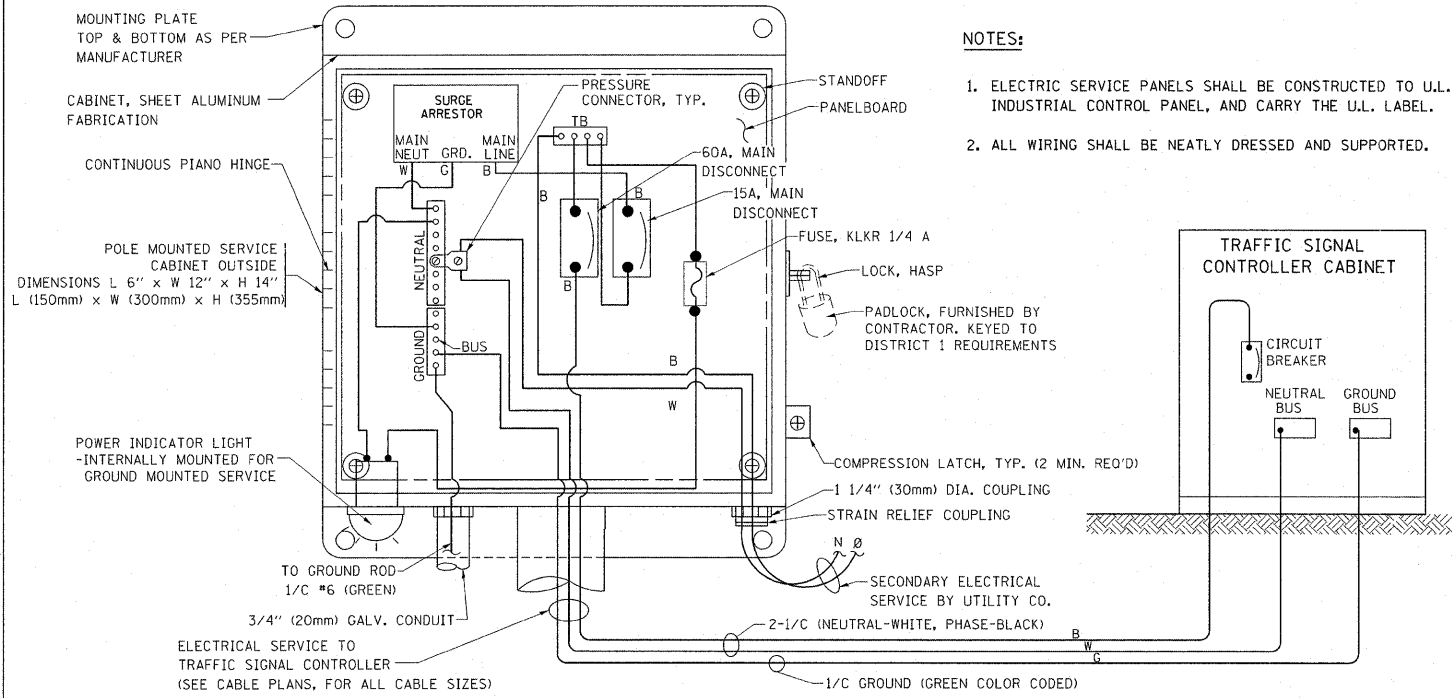
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

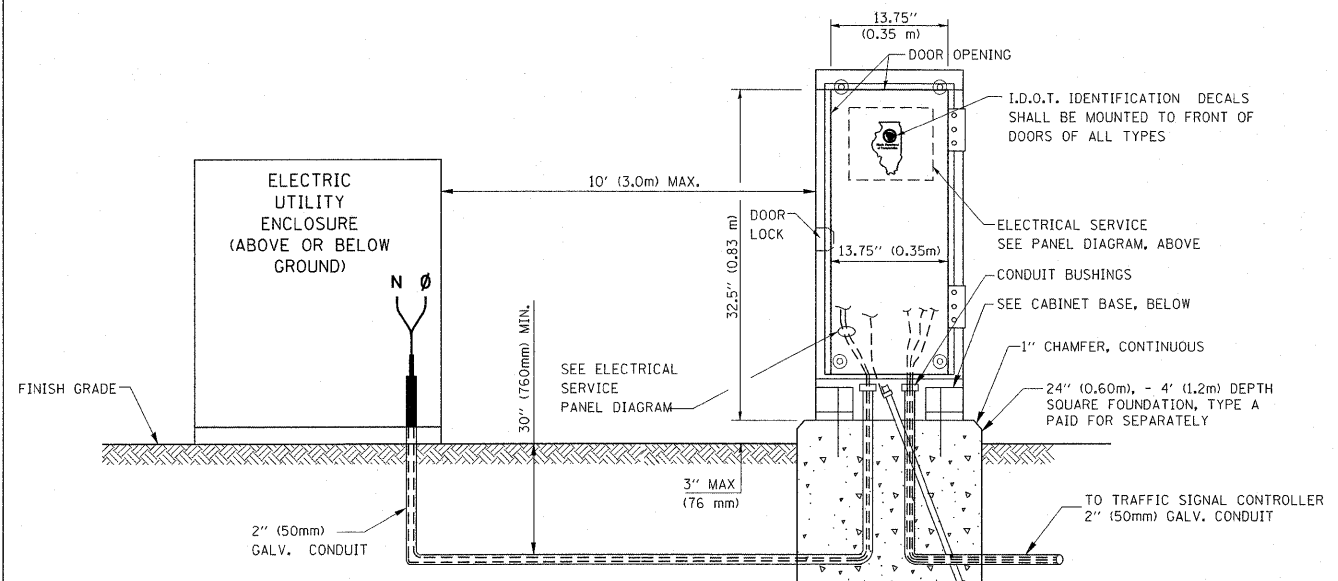
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

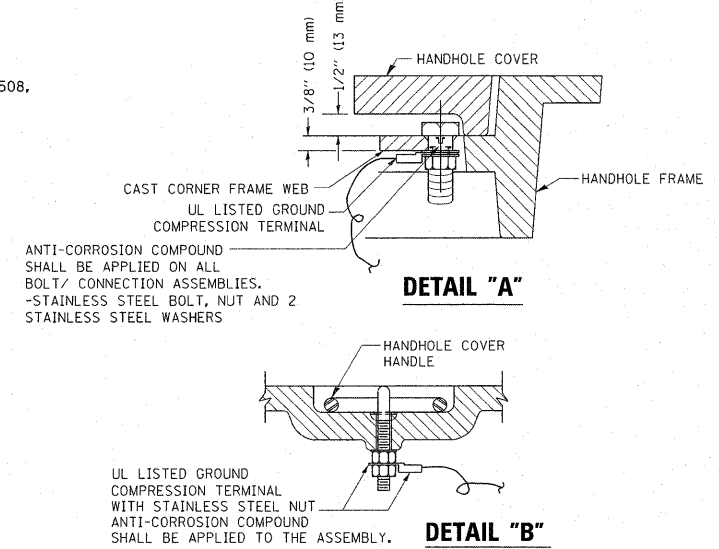
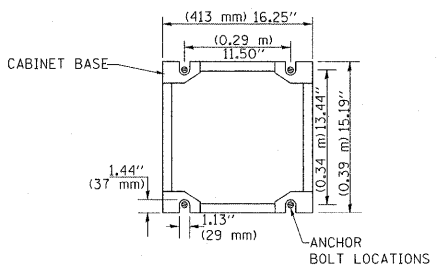


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)



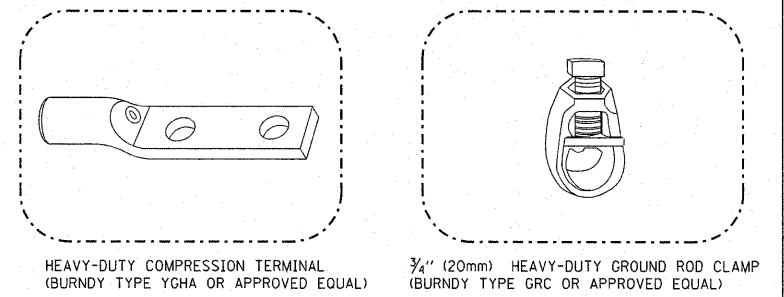
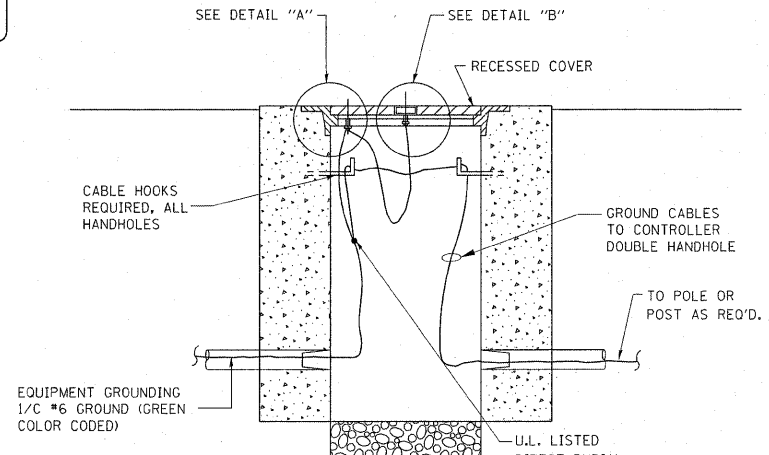
SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)

CABINET - BASE BOLT PATTERN
 (NOT TO SCALE)



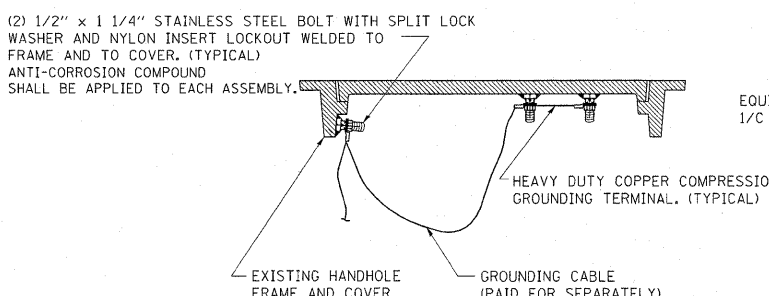
NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD, ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

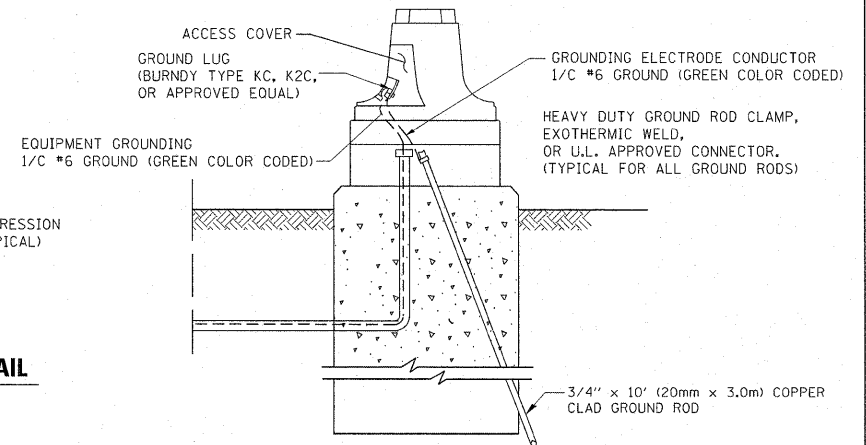


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)

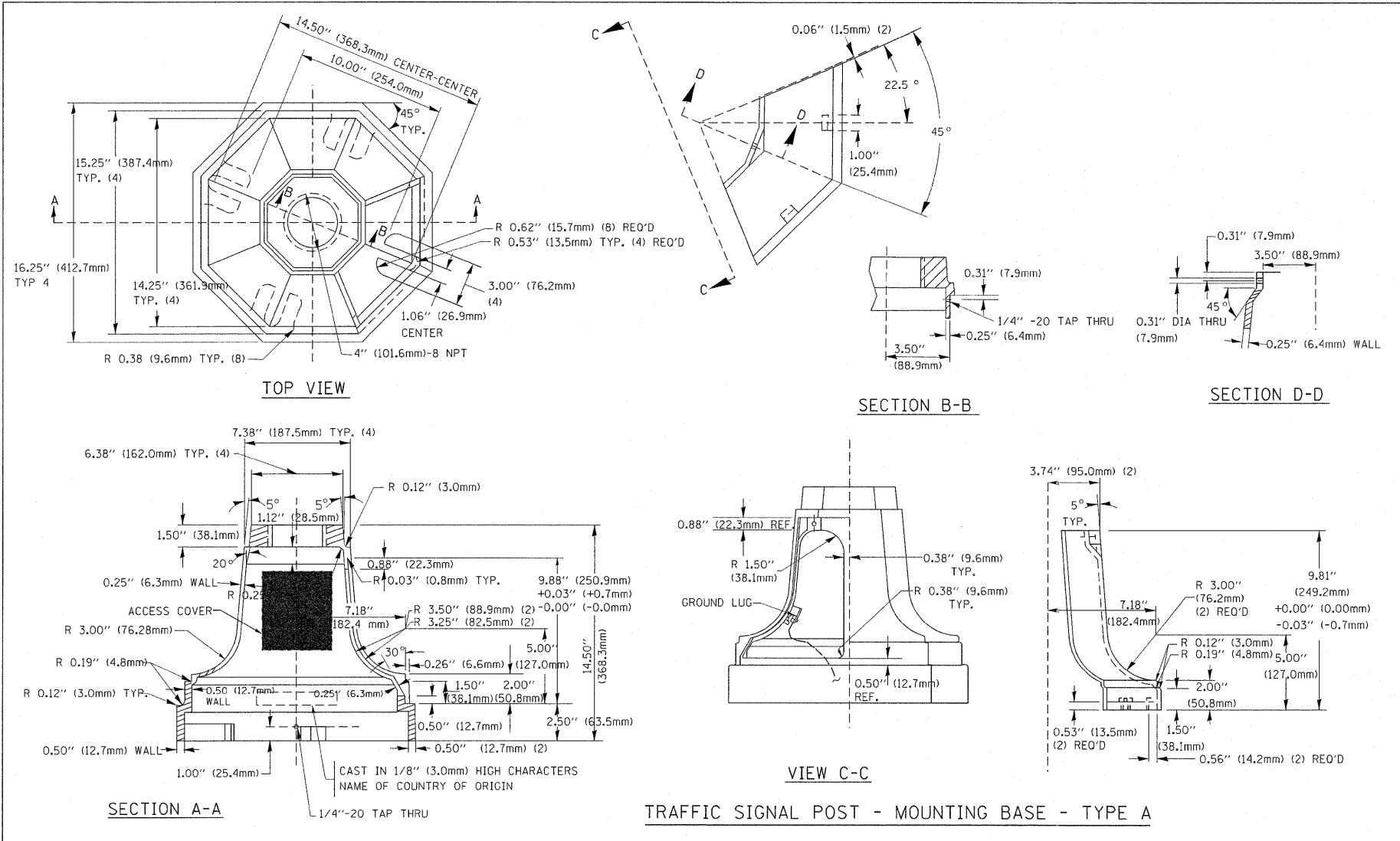


EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)

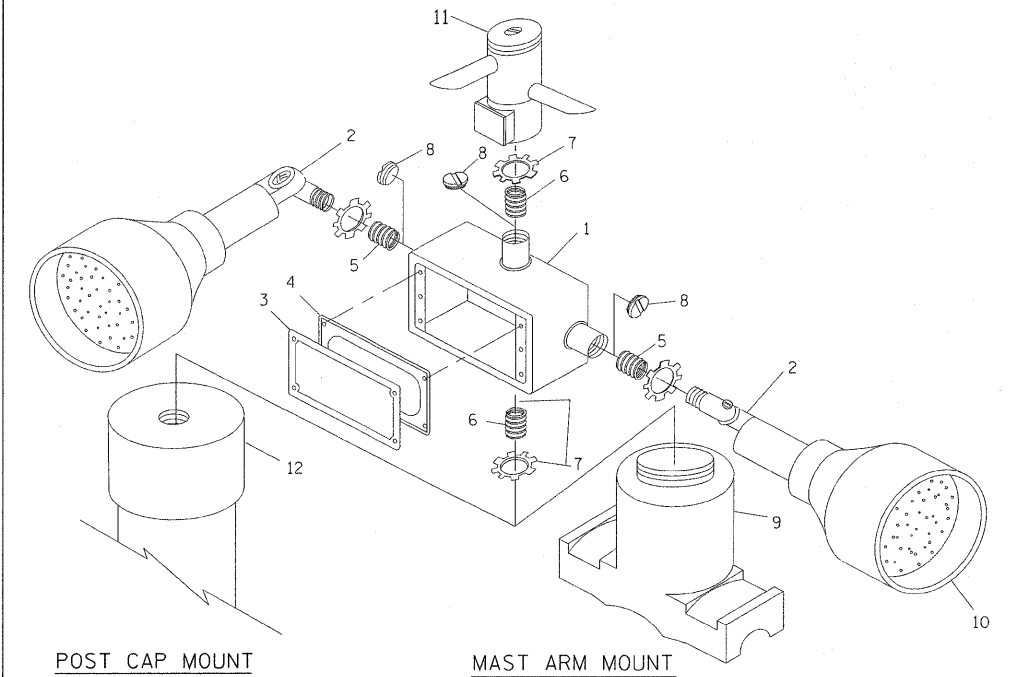


MAST ARM POLE / POST-GROUNDING DETAIL
 (NOT TO SCALE)

FILE NAME =	USER NAME = konthaphix@bc	DESIGNED - DAD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 41	
CONTRACT NO. 60K66	PLOT SCALE = 20,0000' / IN.	DRAWN - BCK	REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT				
	PLOT DATE = 10/6/2009	CHECKED - DAD	REVISED -			SCALE:	SHEET NO. 3 OF 6 SHEETS	STA.	TO STA.		
		DATE - 10/28/09	REVISED -								



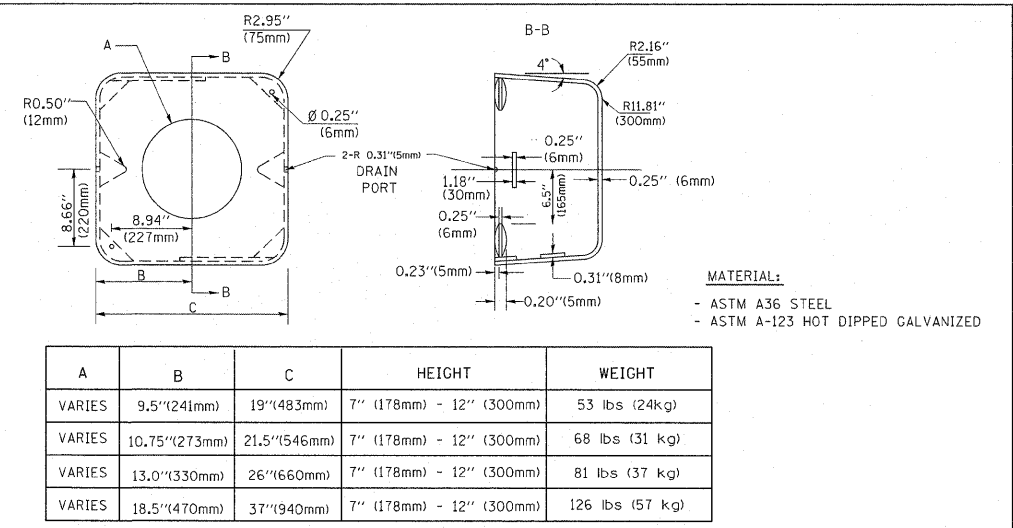
TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A



EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

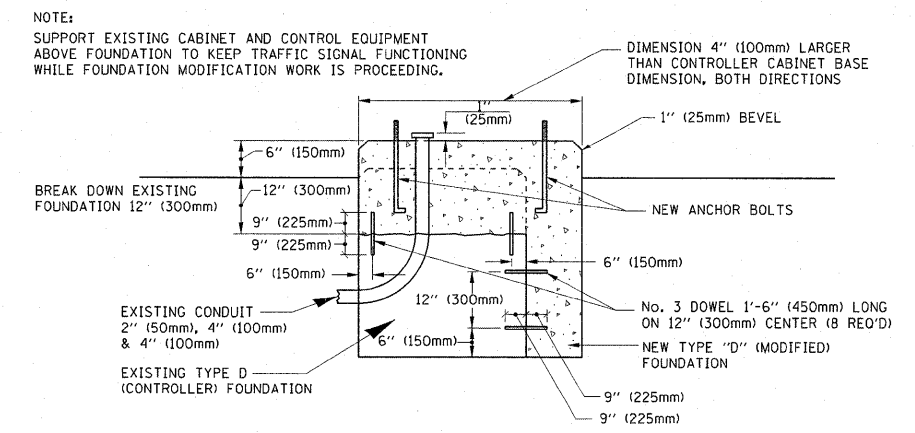
- NOTES:**
- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
 - ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
 - WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



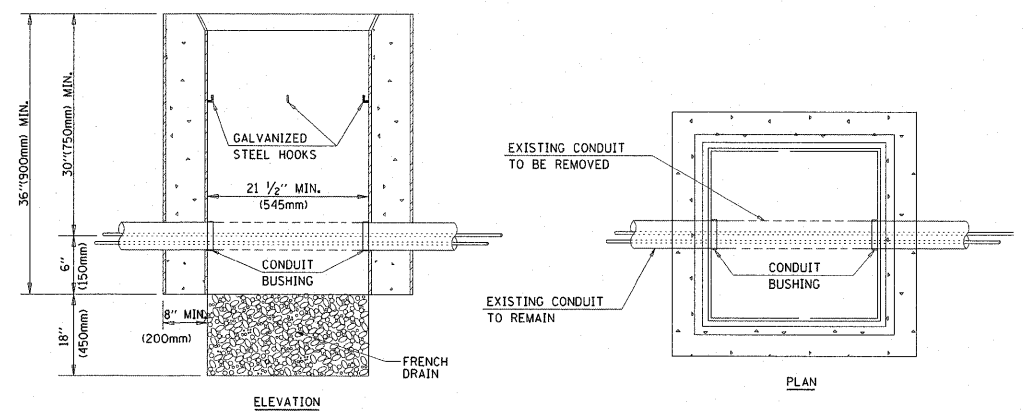
A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIABLES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIABLES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIABLES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

SHROUD

- NOTES:**
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
 - THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
 - THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

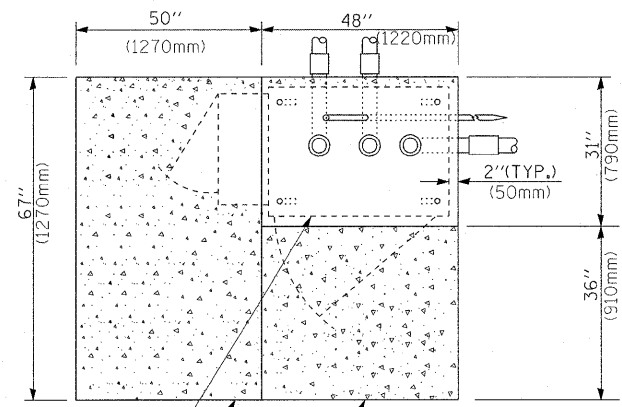


MODIFY EXISTING TYPE "D" FOUNDATION

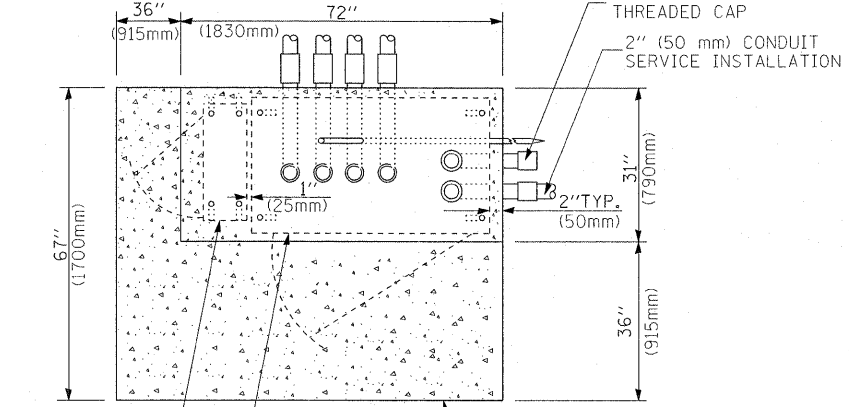


- NOTES:**
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
 - REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

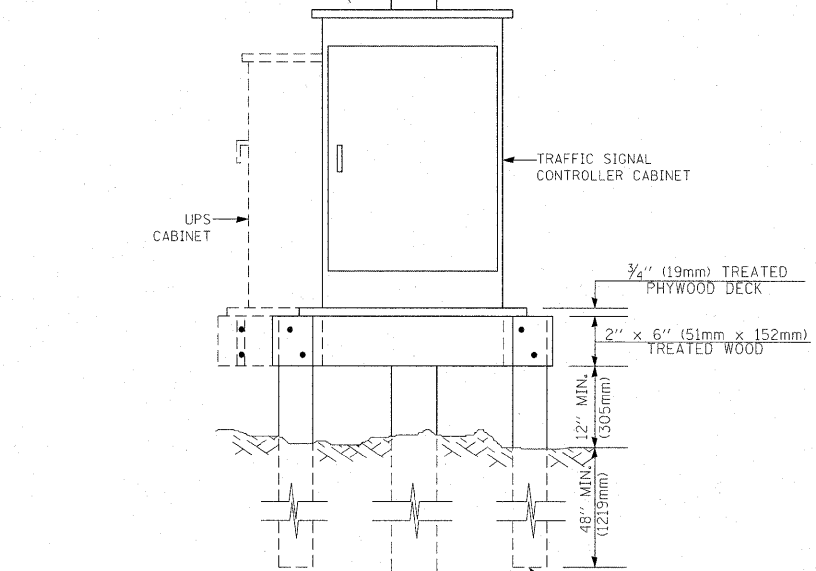
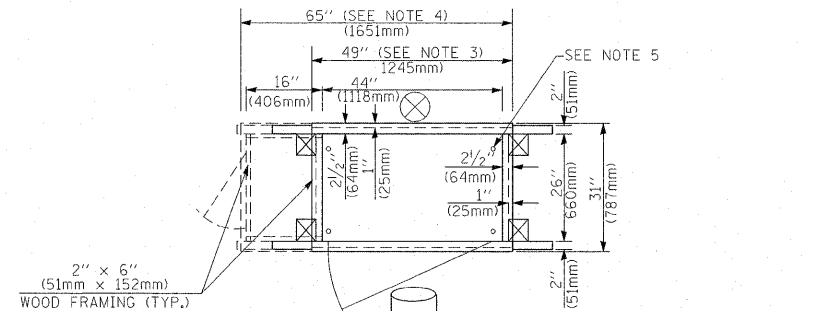
HANDHOLE TO INTERCEPT EXISTING CONDUIT



CONTROLLER CABINET BASE
PROPOSED APRON
EXISTING APRON
TOP VIEW

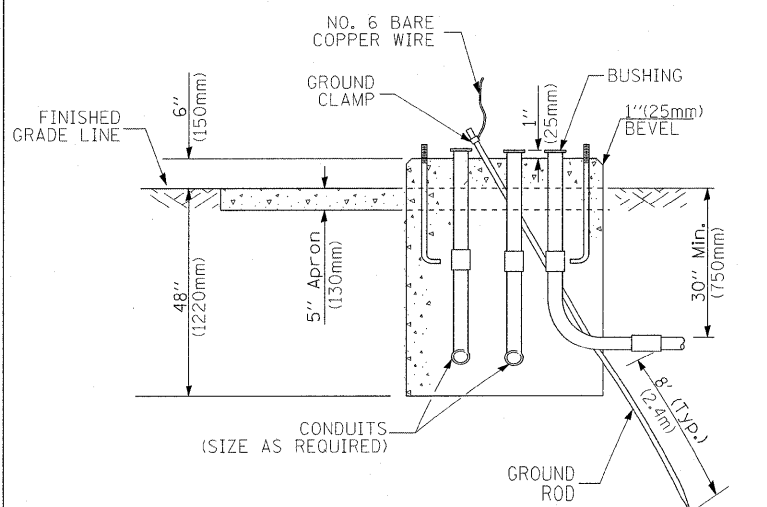


UPS CABINET BASE
CONTROLLER CABINET BASE
APRON
TOP VIEW

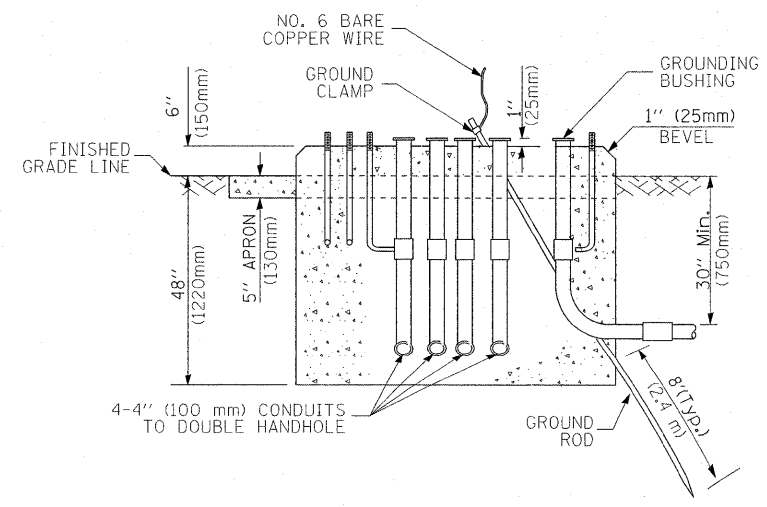


- NOTES:**
1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
 4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
 5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
 6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**



**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



**TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

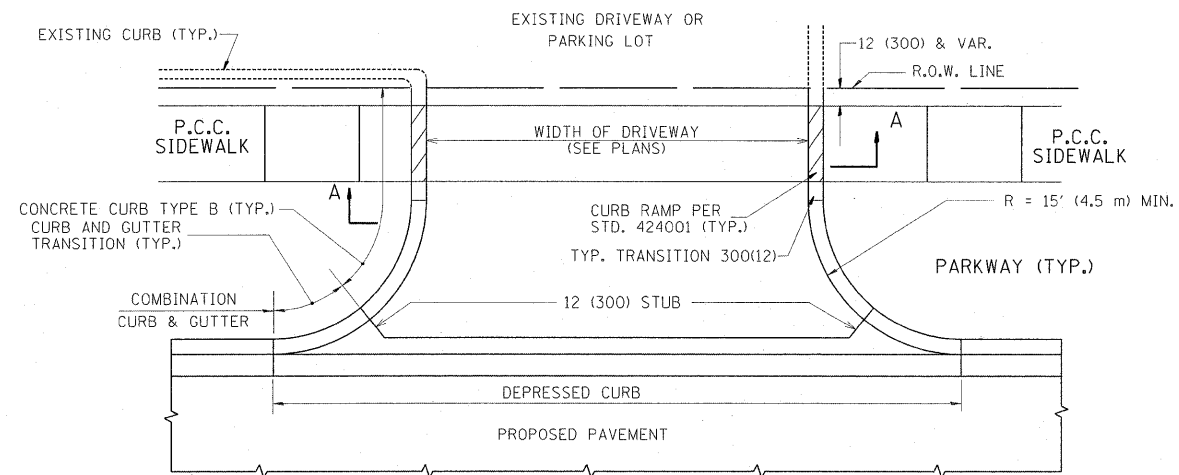
FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

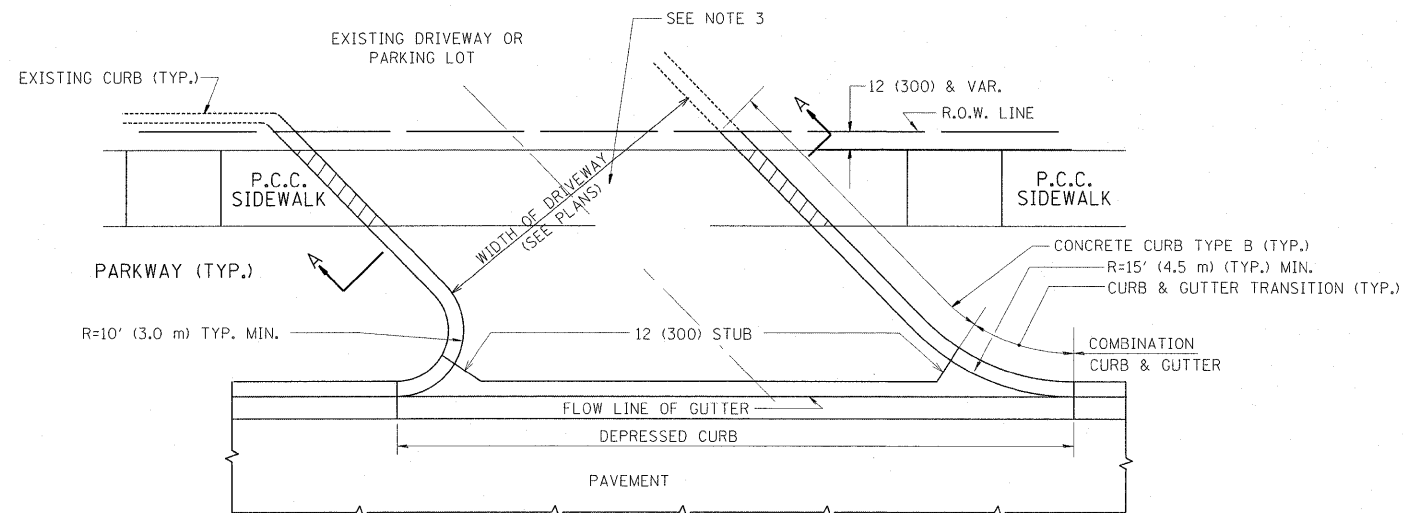
MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 56' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
 2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
 3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
 4. For mast arm assemblies with dual arms refer to state standard 878001.

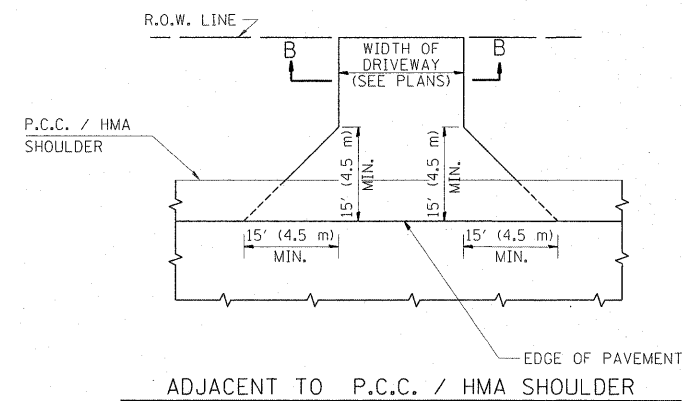
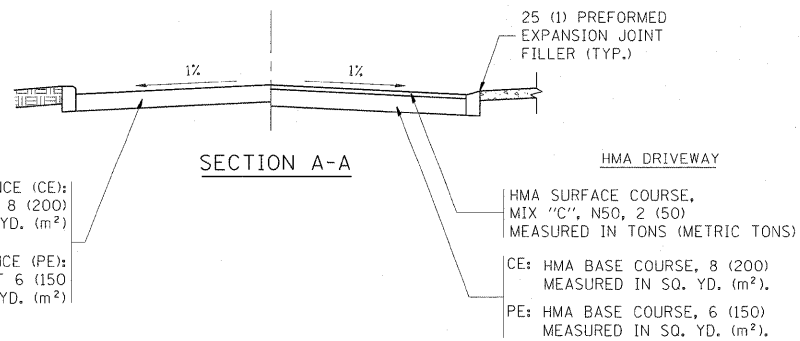
DEPTH OF MAST ARM FOUNDATIONS, TYPE E



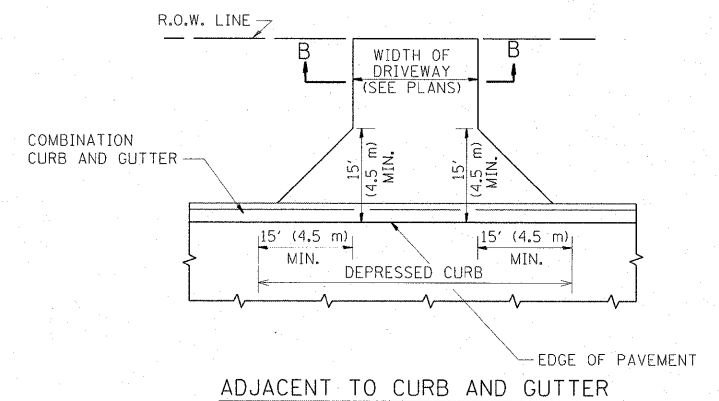
WITH CONCRETE CURB, TYPE B



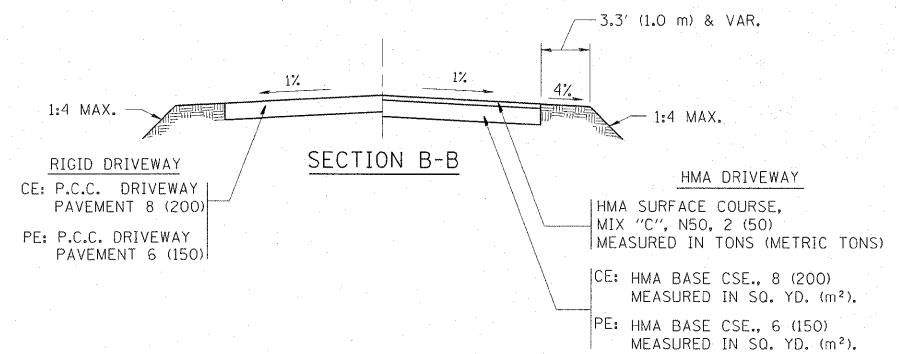
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



SECTION B-B

RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE CSE., 8 (200)
 MEASURED IN SQ. YD. (m²).
 PE: HMA BASE CSE., 6 (150)
 MEASURED IN SQ. YD. (m²).
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

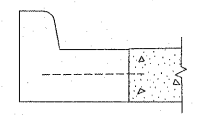
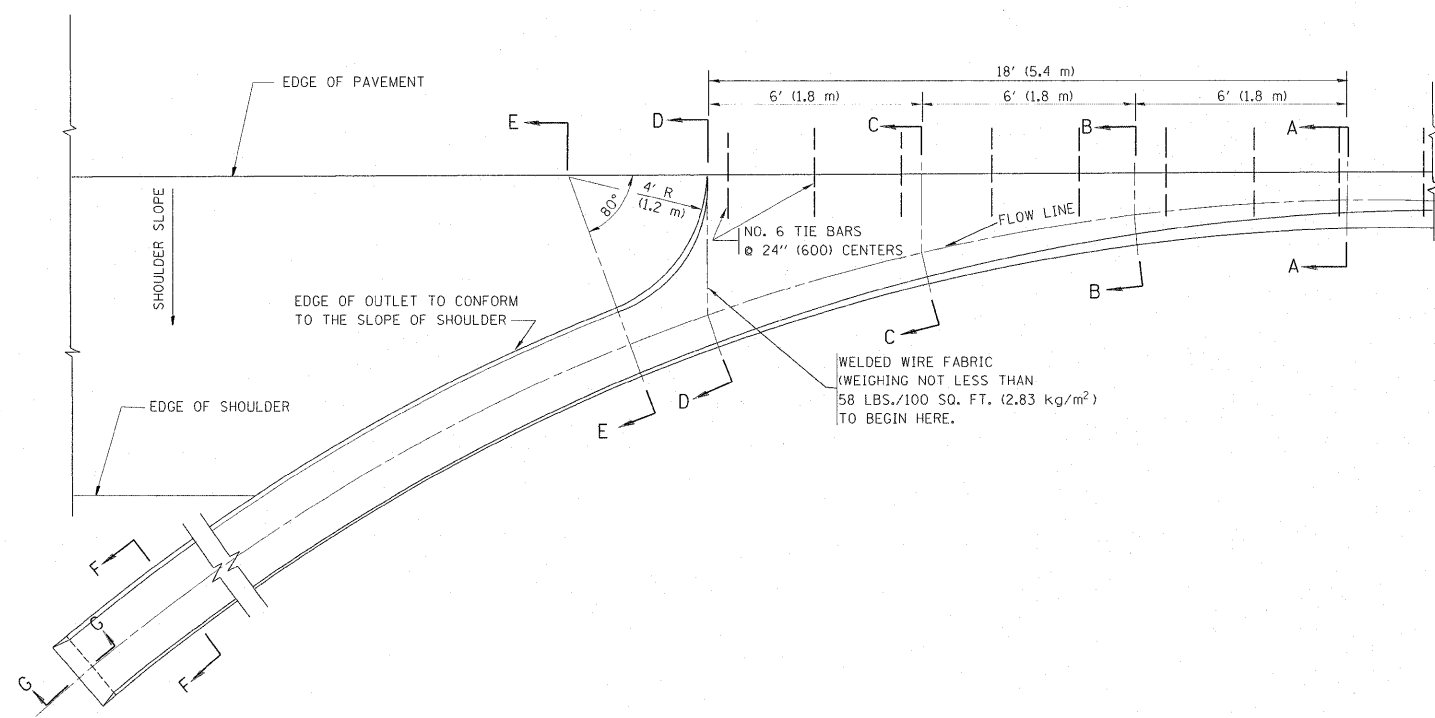
THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

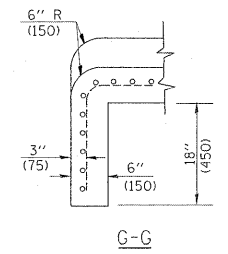
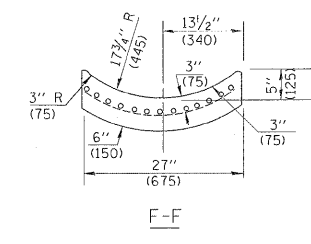
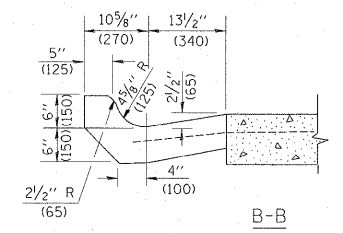
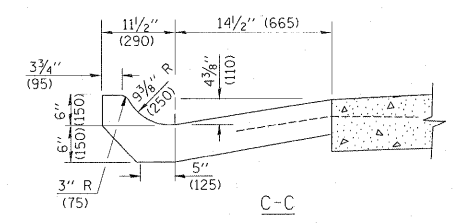
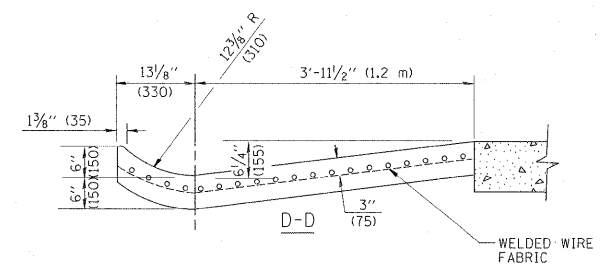
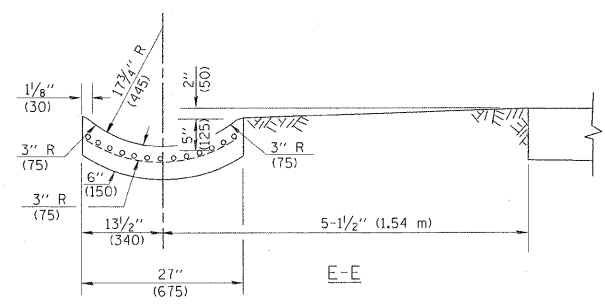
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = galbanjr	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
as:\pwork\pwork\galbanjr\0150417\01st\dtd.dgn		DRAWN -	REVISED - P. LOFLUER 04-15-03			2688	44 R-N	COOK/WILL	68	45
PLOT SCALE = 50.0000 / / IN.		CHECKED -	REVISED - R. BORO 01-01-07			BD0156-07 (BD-01)		CONTRACT NO. 60K66		
PLOT DATE = 3/26/2011		DATE - 11-04-95	REVISED - R. BORO 06-11-08			SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT		



A-A *

* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



GENERAL NOTES

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO.6) AT 24" (600) CENTERS UNLESS OTHERWISE SHOWN.

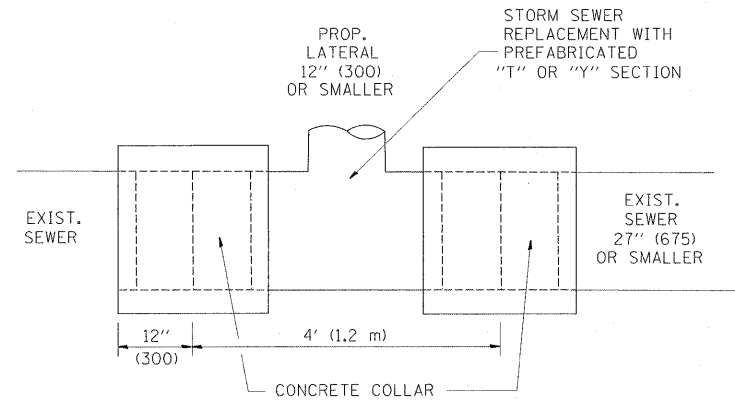
IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6' (1.8 m) FOR EACH 1% INCREASE IN GRADE.

QUANTITIES

FOR SECTION A-A TO E-E AND CURTAIN WALL =
 1.25 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 9" (225) PAV'T.
 1.27 CU. YDS. (0.96 m³) CLASS SI CONCRETE (OUTLET) FOR 10" (250) PAV'T.
 FOR SECTION F-F =
 0.045 CU. YDS. (0.03 m³) CLASS SI CONCRETE PER FT. (m).

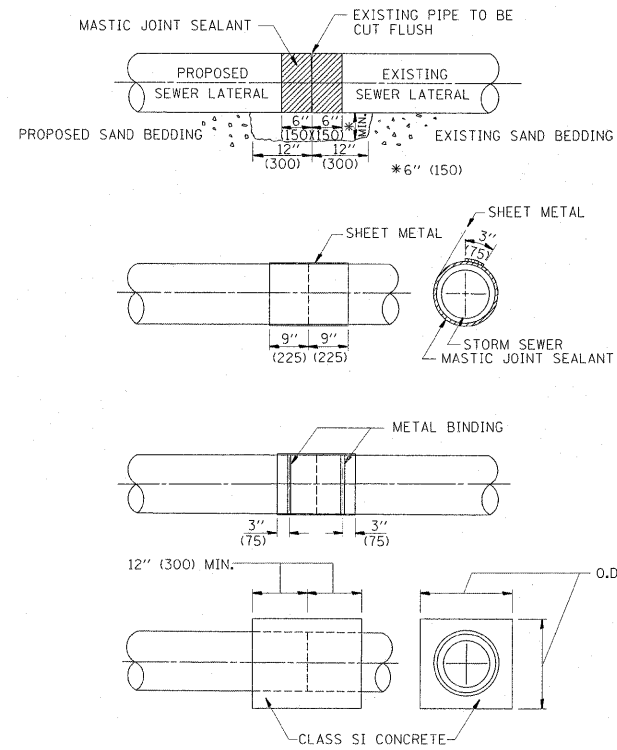
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbanjr	DESIGNED - M. DE YONG	REVISED - R. SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	OUTLET FOR CONCRETE CURB AND GUTTER			F.A.I. R.T.E. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw_work\pwidot\galbanjr\00150417\01st	td.dgn	DRAWN -	REVISED - R. SHAH 10-25-94		2688	44 R-N	COOK/WILL	68	46			
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - E. GOMEZ 12-21-00		BD600-01 (BD-03)			CONTRACT NO. 60K66				
	PLOT DATE = 3/26/2011	DATE - 08-04-86	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



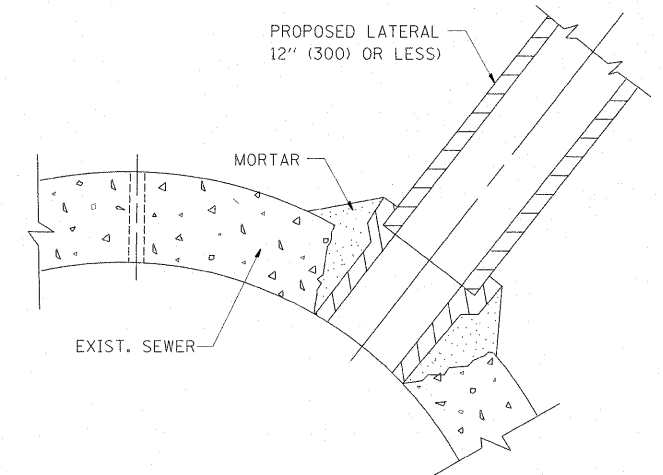
DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

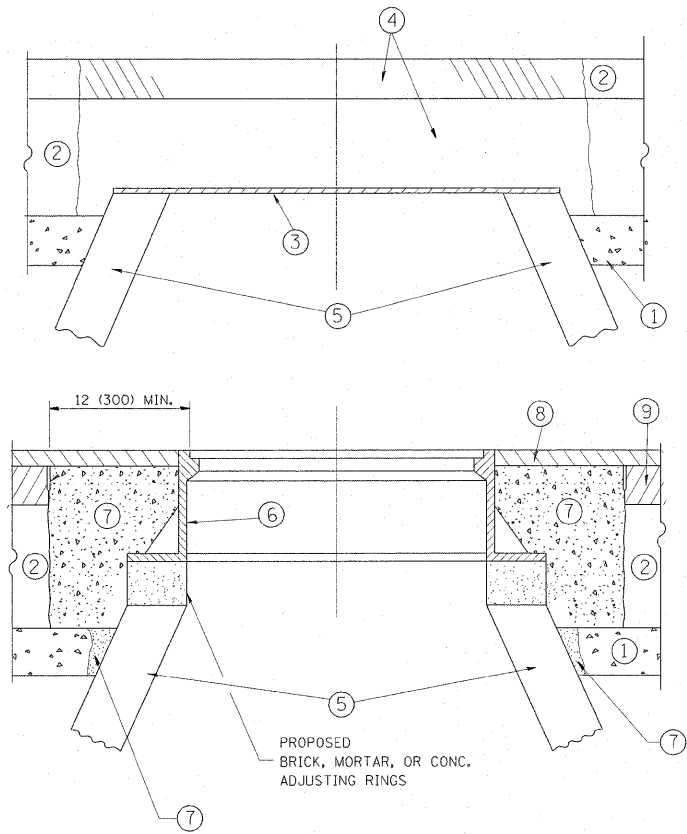
TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

FILE NAME =	USER NAME = galbanjr	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER			F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 47
ct:\pw_work\pwidot\galbanjr\00150417\01st	td.dgn	DRAWN -	REVISED - R. SHAH 09-09-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD500-01 (BD-7)		CONTRACT NO. 60K66		
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED - R. SHAH 10-25-94									
	PLOT DATE = 3/26/2011	DATE - 07-25-90	REVISED - R. SHAH 06-12-96									
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												



- CONSTRUCTION PROCEDURES**
- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

- LEGEND**
- ① SUB-BASE GRANULAR MATERIAL
 - ② EXISTING PAVEMENT
 - ③ 36 (900) DIAMETER METAL PLATE
 - ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
 - ⑤ EXISTING STRUCTURE
 - ⑥ FRAME AND LID (SEE NOTES)
 - ⑦ CLASS PP-1* CONCRETE
 - ⑧ PROPOSED HMA SURFACE COURSE
 - ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

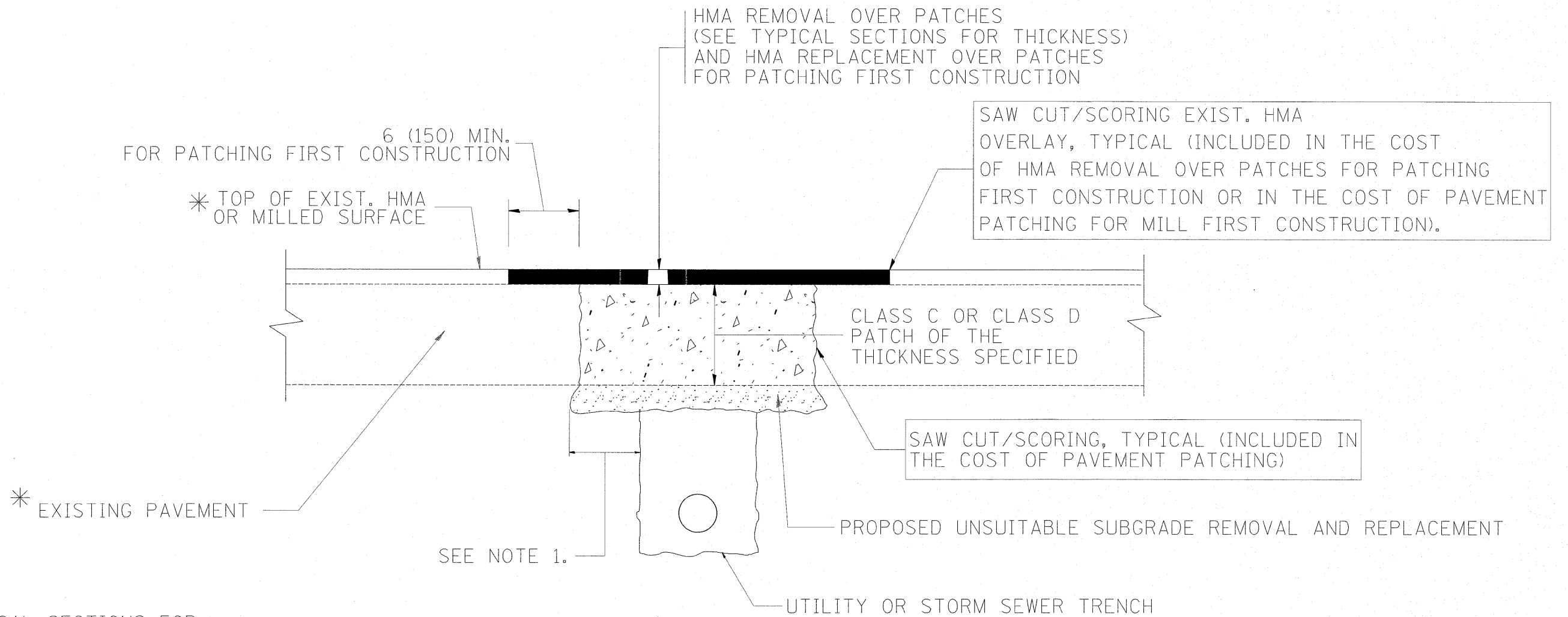
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = galbenjr	DESIGNED - R. SHAH	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.J. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 48
os:\pw_work\p\dot\galbenjr\d0150417\Dist	td.dgn	DRAWN -	REVISED - R. WIEDEMAN 05-14-04		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-03 (BD-8)		CONTRACT NO.	60K66	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
	PLOT DATE = 3/26/2011	DATE - 10-25-94	REVISED - R. BORO 03-09-11									



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbanjr	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\pwidot\galbanjr\d0150417\01st	tdudgn	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	2688	44 R-N	COOK/WILL	68	49
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 60K66				
	PLOT DATE = 3/26/2011	DATE - 10-25-94	REVISED - K. ENG 10-27-08		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

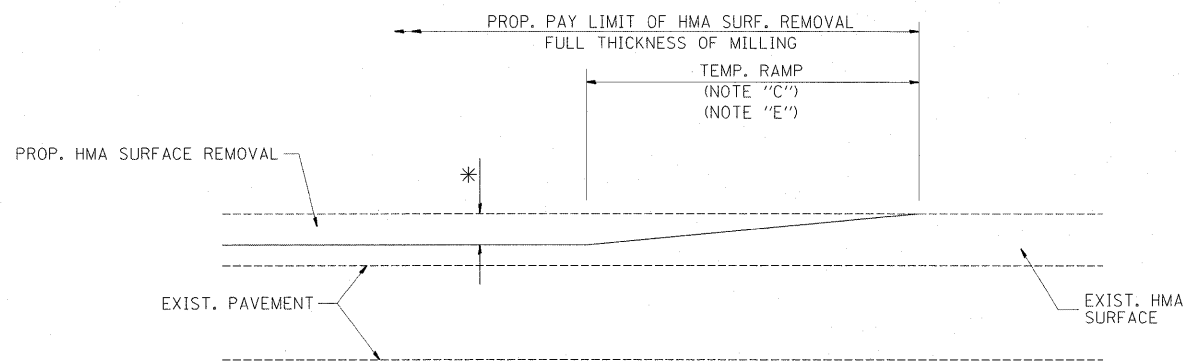
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

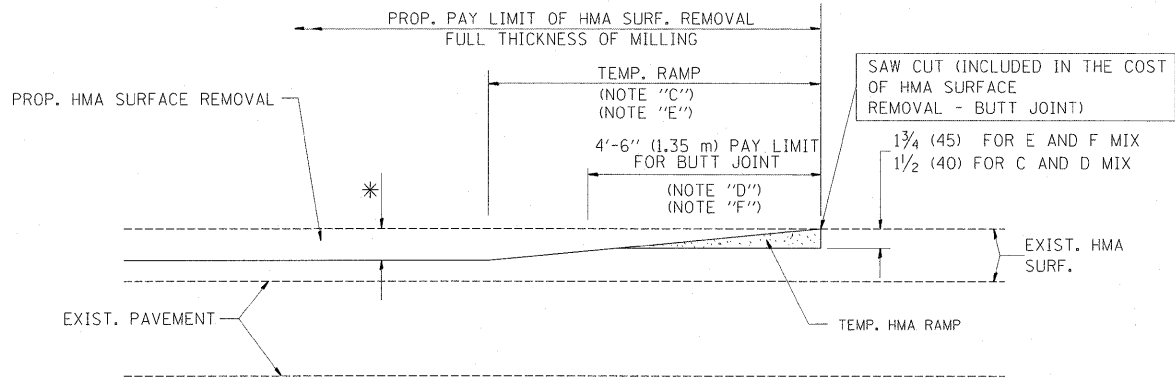
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbanjr	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
si:\pw_work\p\dot\galbanjr\d0150417\01st\d.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		2688	44 R-N	COOK/WILL	68	50			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)			CONTRACT NO. 60K66				
PLOT DATE = 3/26/2011		DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



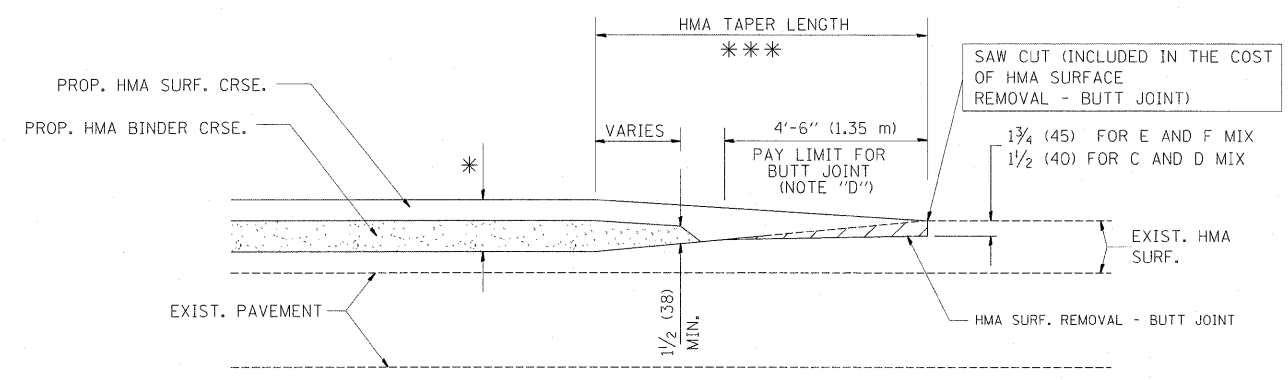
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

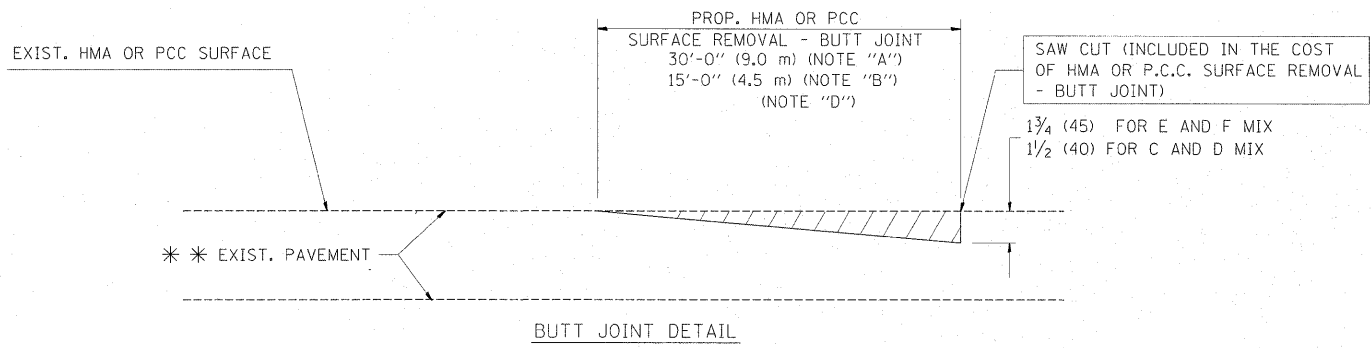


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

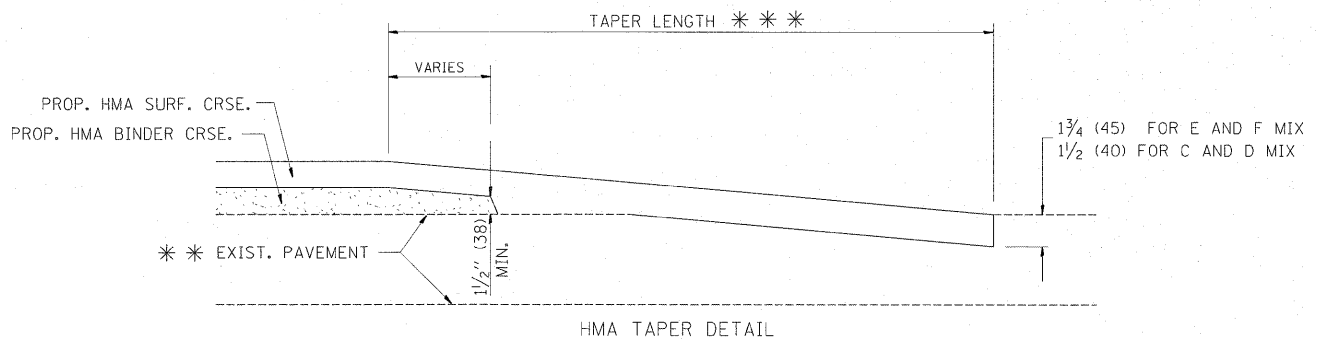
OPTION 2
TYPICAL TEMPORARY RAMP



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbanjr	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
ci:\pw_work\p\idot\galbanjr\d0150417\dst	td.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01	
PLOT DATE = 3/26/2011	DATE - 06-13-90	REVISED - R. BORO 01-01-07	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

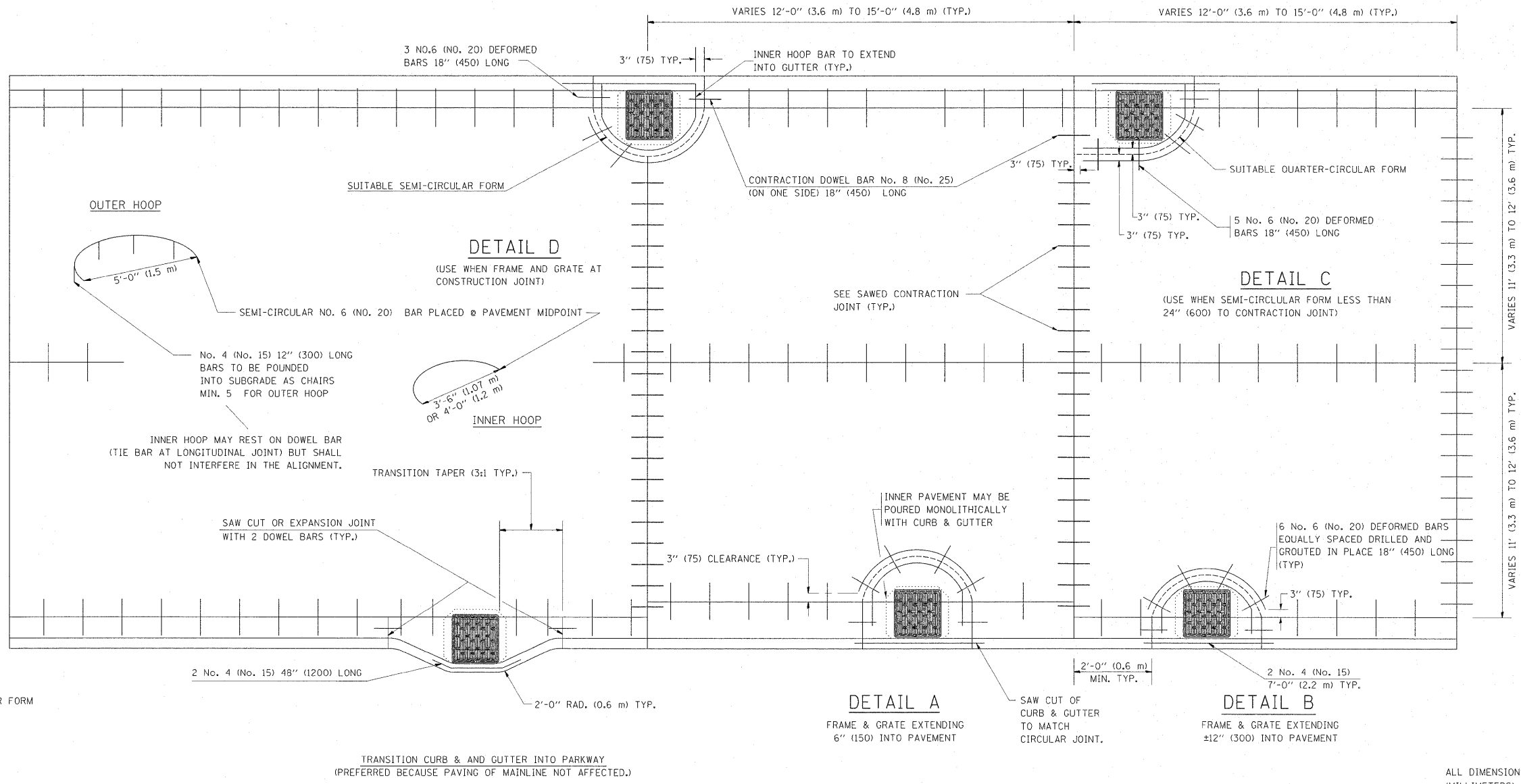
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2688	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BD400-05 BD32	COOK/WILL	68	51
CONTRACT NO.			60K66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FRAME EXTENSION INTO PAVEMENT	INNER HOOP REINFORCEMENT DIAMETER	SEMI CIRCULAR FORM DIAMETER	OUTER HOOP REINFORCEMENT DIAMETER
UP TO 8" (200)	3'-6" (1.1 m)	4'-0" (1.2 m)	5'-0" (1.5 m)
> 8" (200) TO 14" (360)	4'-0" (1.2 m)	4'-6" (1.4 m)	5'-0" (1.5 m)

DESIGNER NOTE:
THIS DETAIL IS TO BE USED WHEN THE GUTTER FLAG IS LESS THAN 24"

- NOTES :**
1. THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.
 2. TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT, EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 12" (300) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
 3. SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
 4. ALL REINFORCED BARS SHALL BE EPOXY COATED.
 5. DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 2" (50) CLEARANCE.
 6. WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
 7. HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
 8. CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
 9. CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.



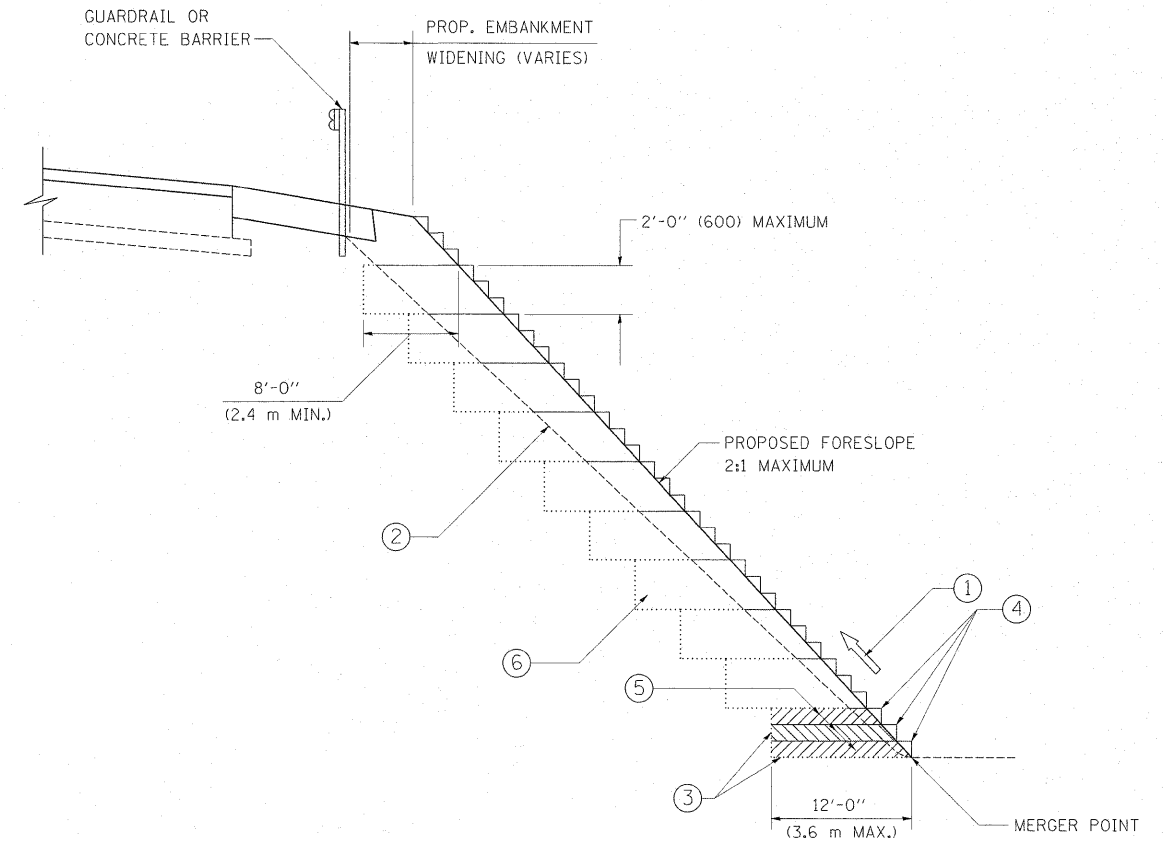
LEGEND:

..... CASTING

----- SUITABLE SEMI-CIRCULAR FORM

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

FILE NAME =	USER NAME = galberjr	DESIGNED - A. ABBAS	REVISED - T. MATOUSEK 08-28-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PCC PAVEMENT ROUNDOUTS AT CURB AND GUTTER			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwork\pvidot\galberjr\d0150417\01st	td.dgn	DRAWN - TOM MATOUSEK	REVISED - T. MATOUSEK 10-02-00					2688	44 R-N	COOK/WILL	68	52
PLOT SCALE = 50.0000' / IN.	CHECKED - A. ABBAS	REVISED - T. MATOUSEK 04-25-02	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD-48		CONTRACT NO. 60K66		
PLOT DATE = 3/26/2011	DATE - 01-04-99	REVISED - P. LAFLEUR 08-27-02	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT									



TYPICAL BENCHING DETAIL
FOR EMBANKMENT

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED -
ct:\pw_work\pwsdot\galbanjr\d0150417\Dist	td.dgn	DRAWN - CADD	REVISED -
PLOT SCALE = 50.0000' / IN.	CHECKED - S.E.B.	REVISED -	REVISED -
PLOT DATE = 3/26/2011	DATE - 06-16-04	REVISED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BENCHING DETAIL
FOR EMBANKMENT WIDENING

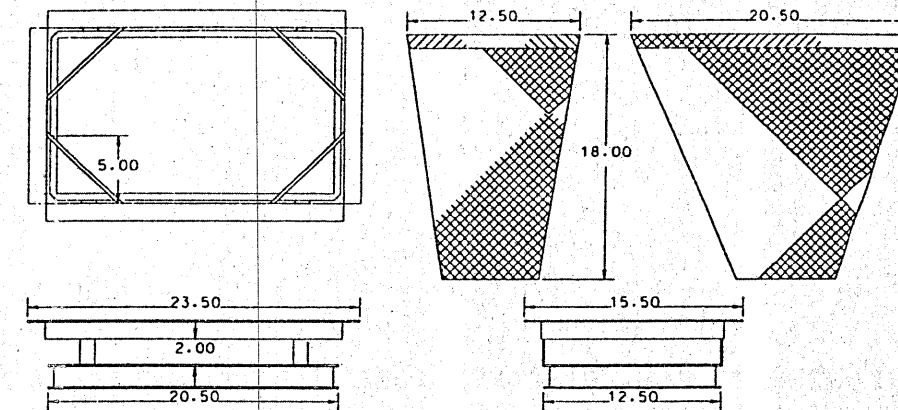
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44 R-N	COOK/WILL	68	53
BD-51			CONTRACT NO.	60K66
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44 R-N	COOK/WILL	68	54
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60K66		

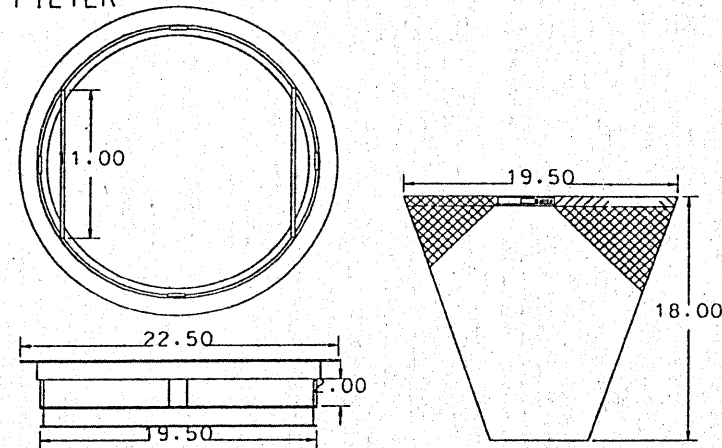
TYPE 23 INLET FILTER

NOTES:
 FRAME: Top piece shall be fabricated from 1 1/4" x 1 1/4" x 1/8" angle.
 Base piece shall be fabricated from 1 1/2" x 1/2" x 1/8" channel. Handles and suspension brackets shall be fabricated from 1 1/4" x 1/4" flat stock.



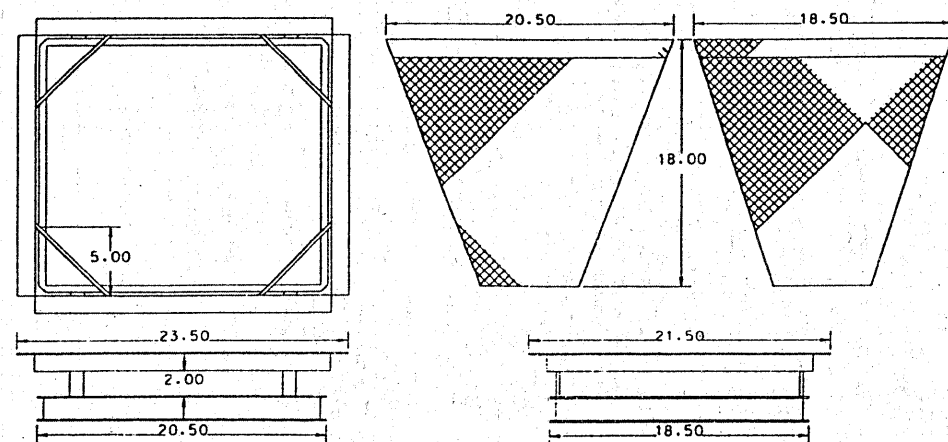
TYPE 1 INLET FILTER

NOTES:
 FRAME: Top ring shall be fabricated from 1 1/4" x 1 1/4" x 1/8" angle. Base ring shall be fabricated from 1 1/2" x 1/2" x 1/8" channel. Handles and suspension brackets shall be fabricated from 1 1/4" x 1/4" flat stock.



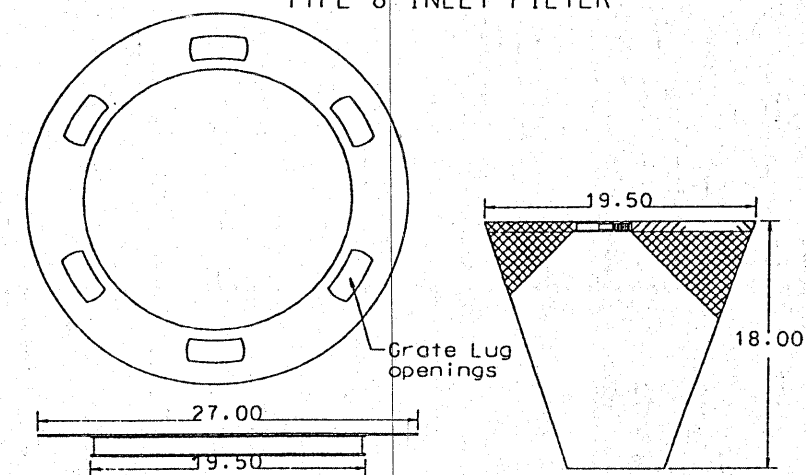
TYPE 24 INLET FILTER

NOTES:
 FRAME: Top piece shall be fabricated from 1 1/4" x 1 1/4" x 1/8" angle. Base piece shall be fabricated from 1 1/2" x 1/2" x 1/8" channel. Handles and suspension brackets shall be fabricated from 1 1/4" x 1/4" flat stock.



TYPE 8 INLET FILTER

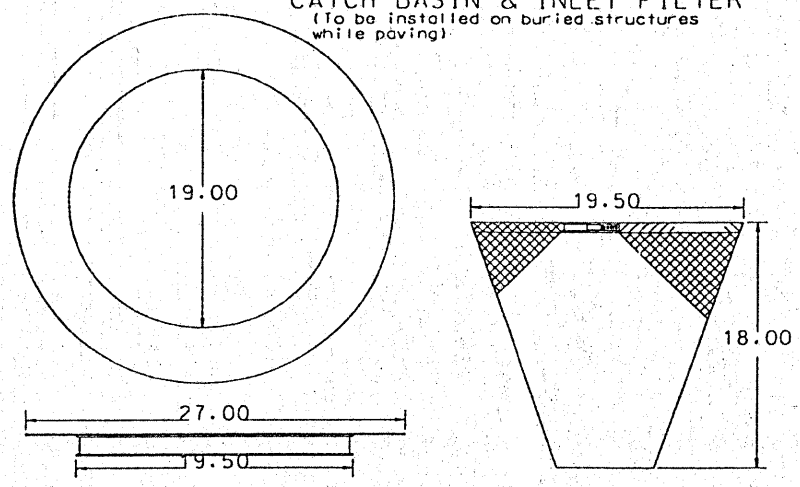
NOTES:
 FRAME: Flange shall be fabricated from 1/8" flat stock. Base ring shall be fabricated from 1 1/2" x 1/2" x 1/8" channel.



REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		INLET FILTER DETAILS
SCALE		DRAWN BY CHECKED BY
DATE		

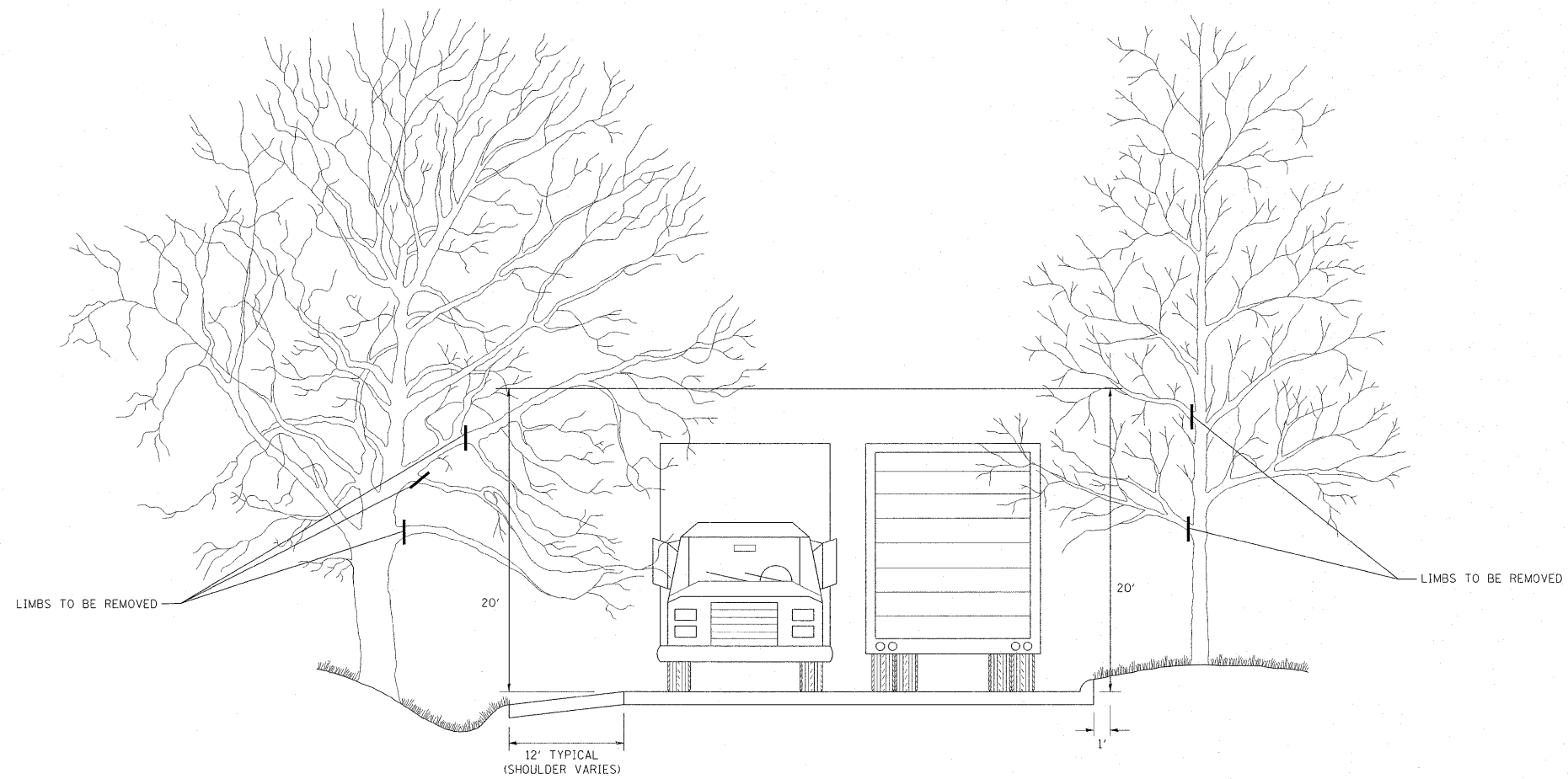
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44 R-N	COOK/WILL	68	55
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60K66		

CATCH BASIN & INLET FILTER
 (To be installed on buried structures while paving)



NOTES:
 FRAME: Flange shall be fabricated from 1/8" flat stock. Base ring shall be fabricated from 1 1/2" x 1/2" x 1/8" channel.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		INLET FILTER DETAILS
SCALE		DRAWN BY
DATE		



FILE NAME =
 c:\pw_work\pwidot\galbarjr\d0150417\01st

USER NAME = galbarjr
 td.dgn
 PLOT SCALE = 50.0000' / IN.
 PLOT DATE = 3/26/2011

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

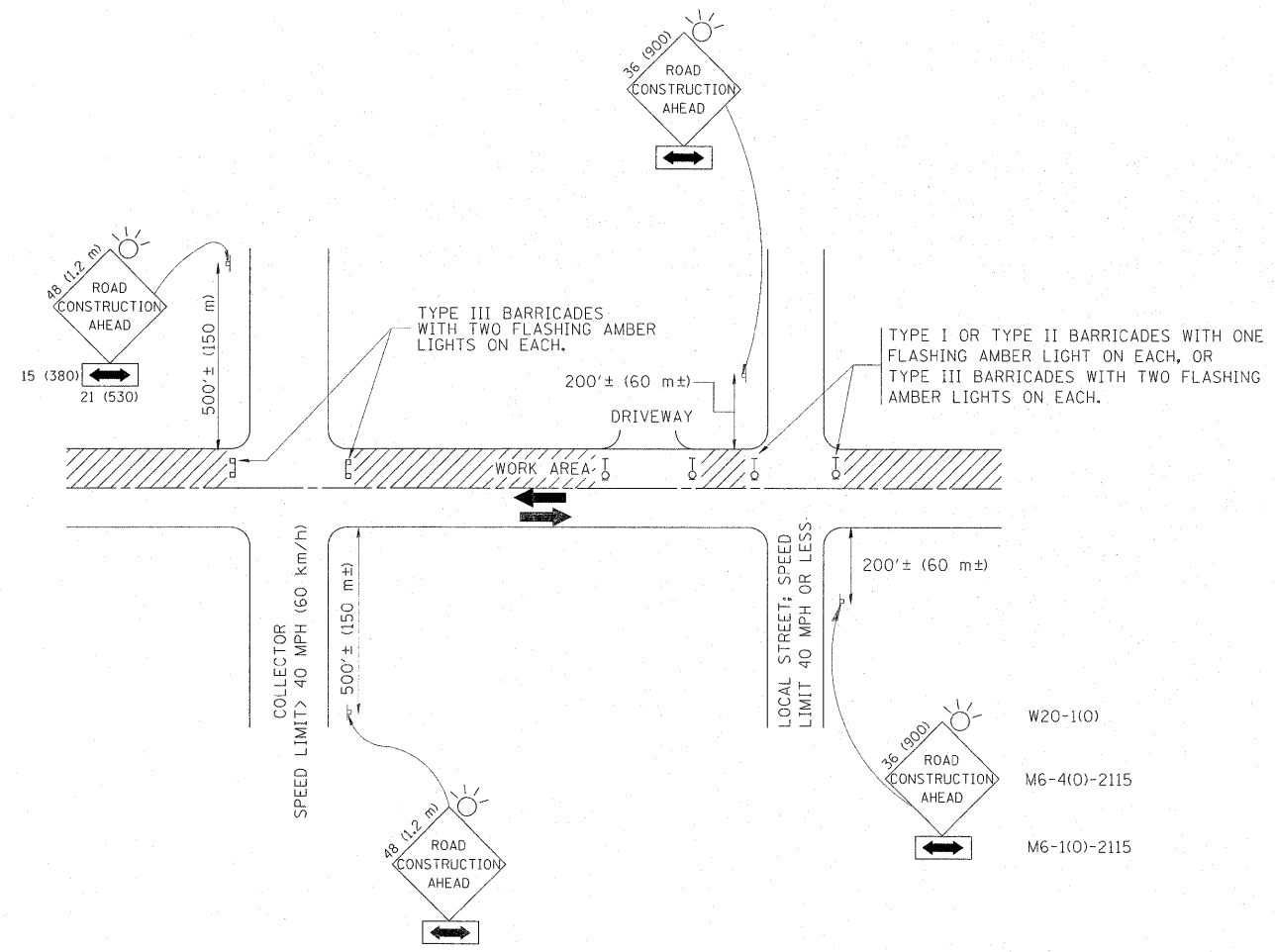
REVISED - R. BORO 10-31-06
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PRUNING FOR SAFETY AND
 EQUIPMENT CLEARANCE**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44 R-N	COOK/WILL	68	56
BM-20			CONTRACT NO. 60K66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

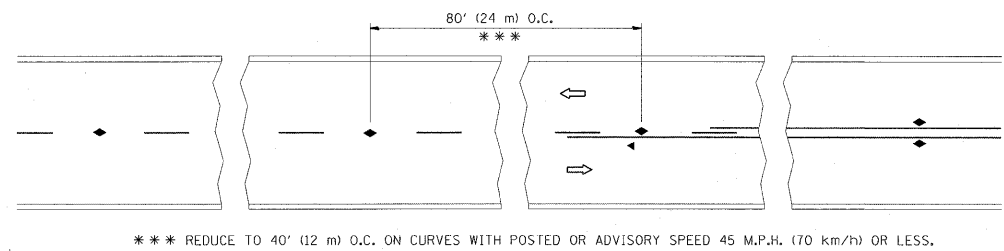
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = galbanjr	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
ct:\pw_work\p1dot\galbanjr\d0150417\01st	td.dgn	DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.0000" / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 3/26/2011	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

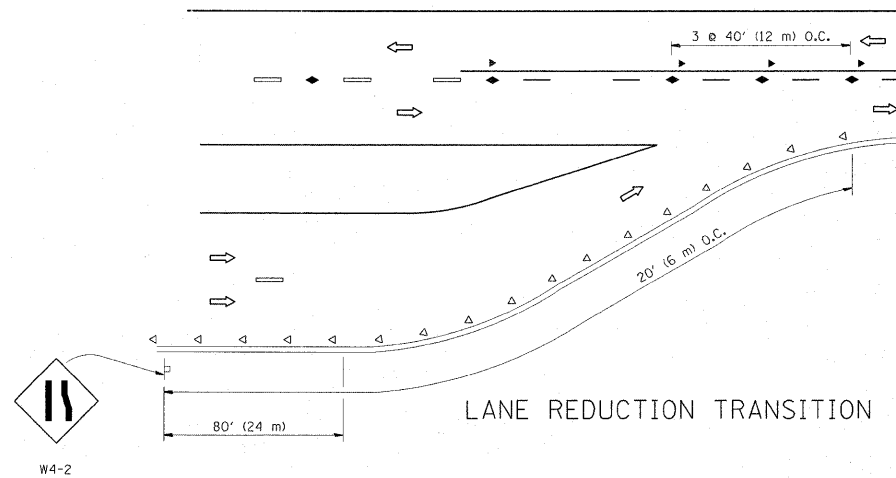
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44 R-N	COOK/WILL	68	57
TC-10			CONTRACT NO.	60K66
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

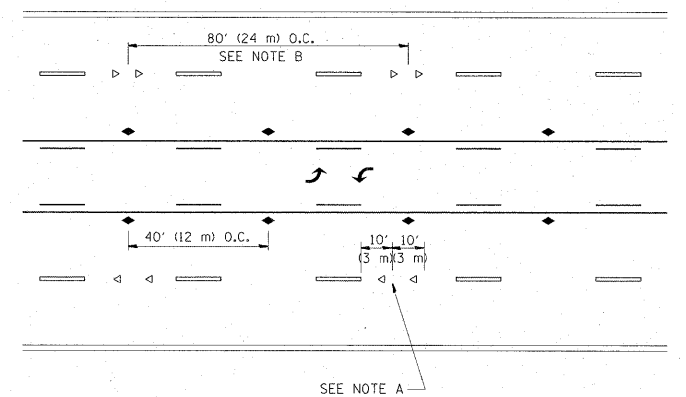


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

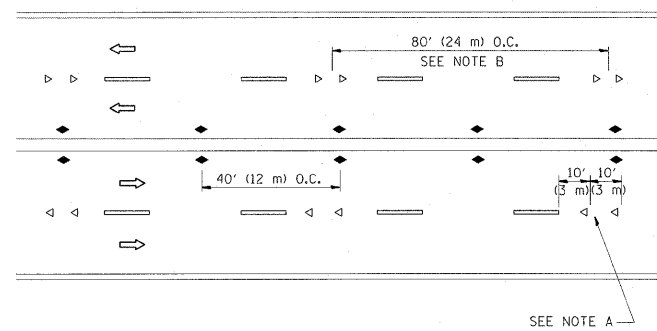
TWO-LANE/TWO-WAY



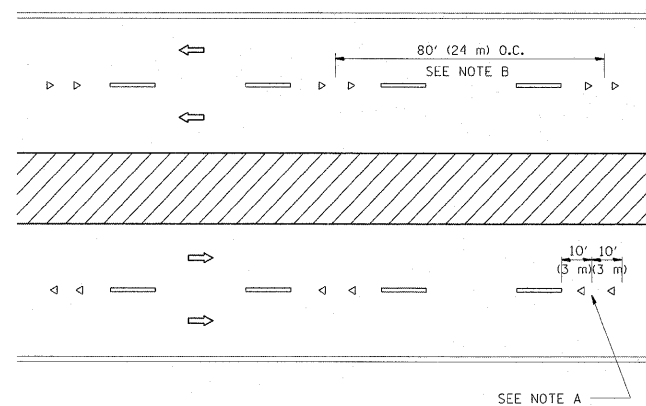
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

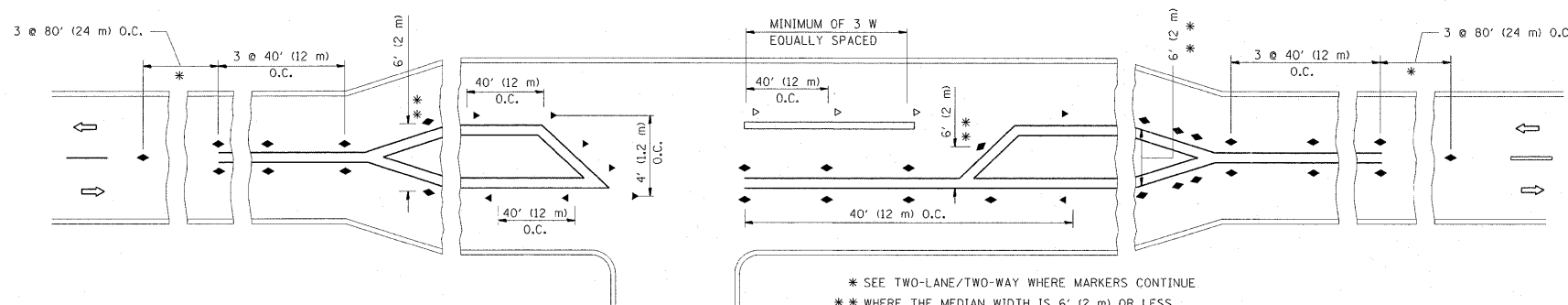
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

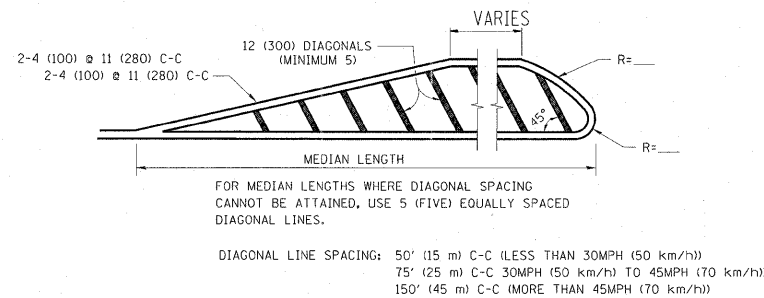
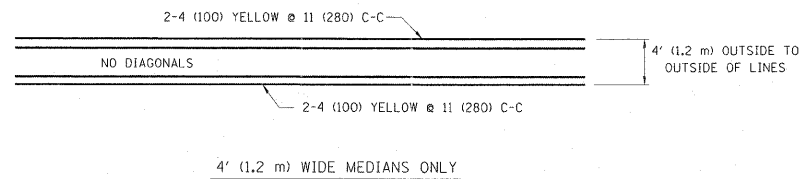
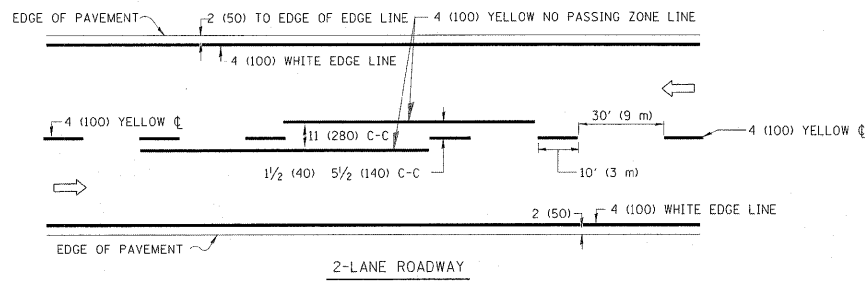


LEFT TURN

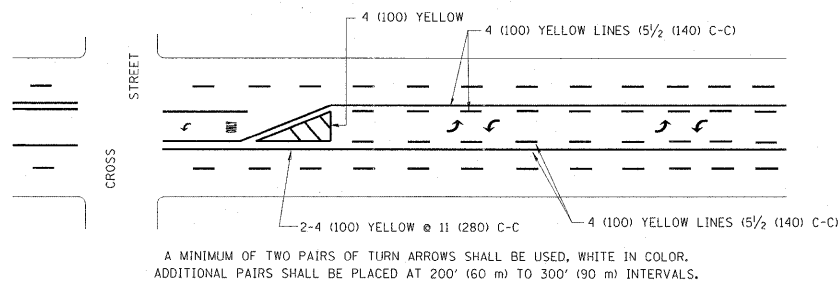
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS			F.A.J.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pw_work\pwwidot\galbanjr\d015041\11st\dtdgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			2688	44 R-N	COOK/WILL	68	58
		CHECKED -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-11				
		DATE -	REVISED - C. JUCIUS 09-09-09		CONTRACT NO. 60K66							
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

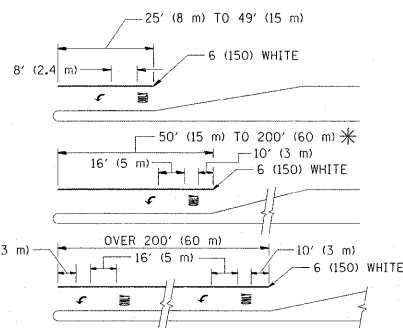


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

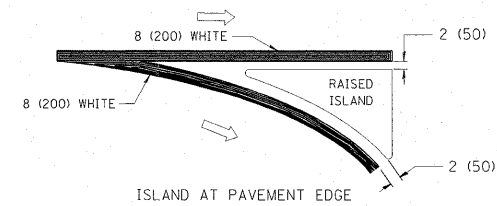
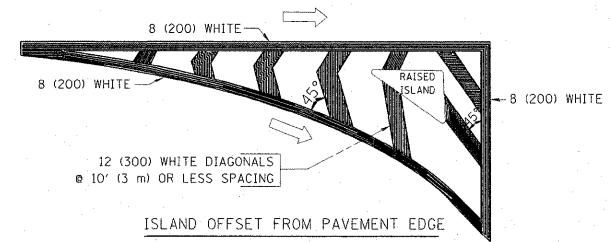


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

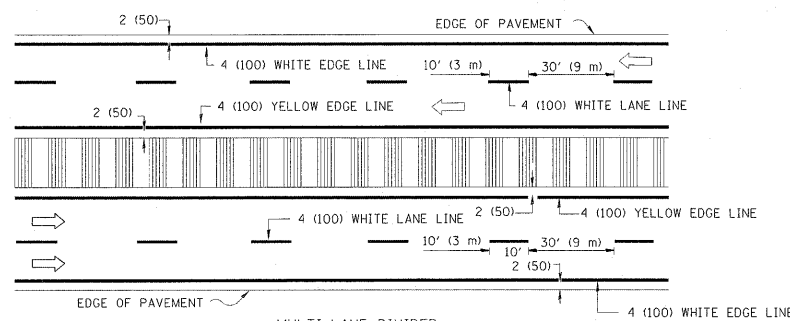
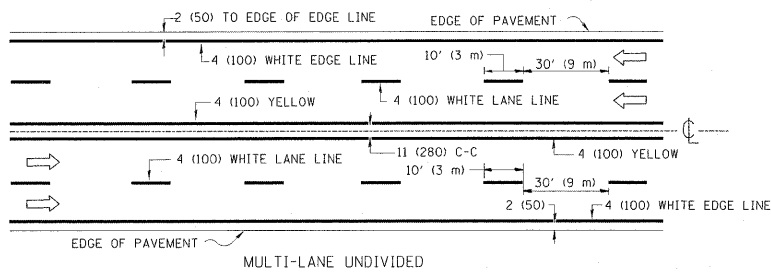
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

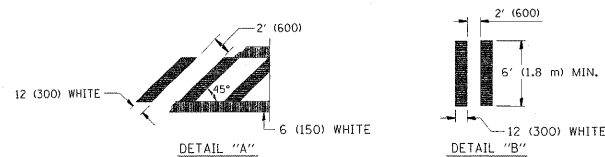
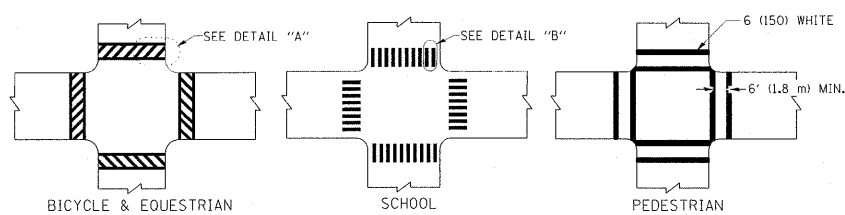


TYPICAL ISLAND MARKING



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

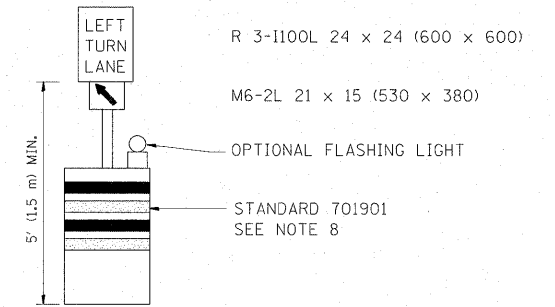
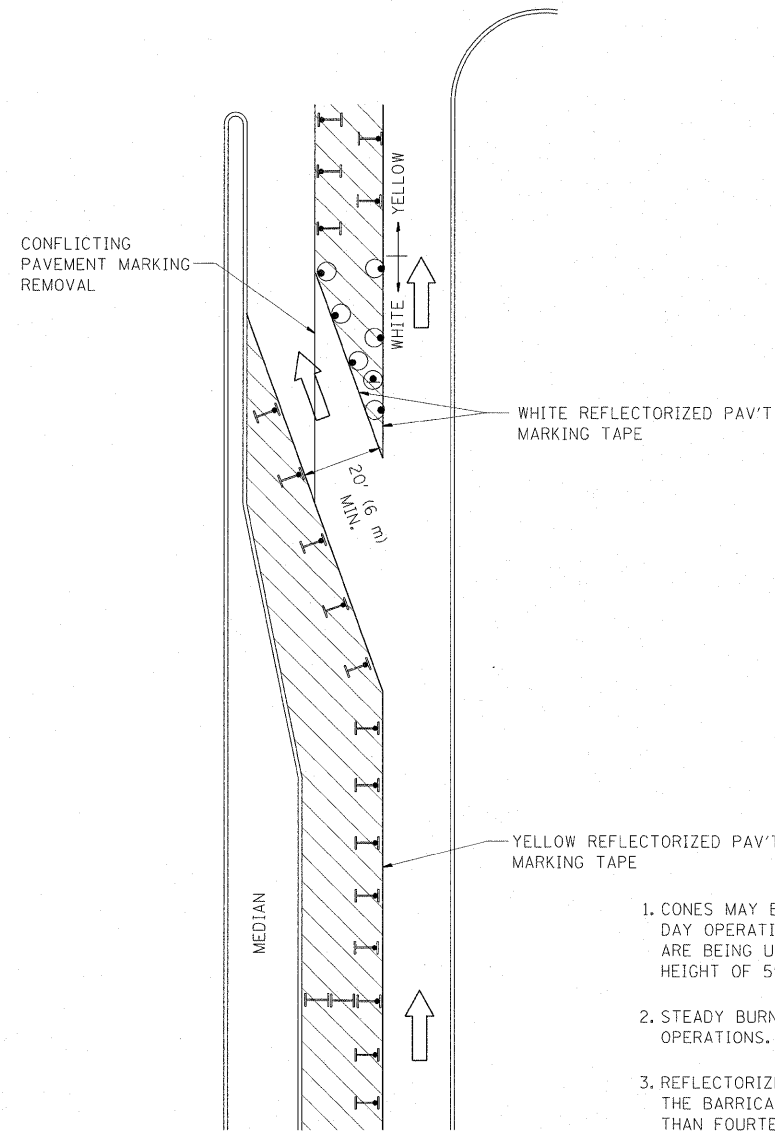
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PLOT SCALE = 50.0000" / IN.		CHECKED -	REVISED -
PLOT DATE = 3/26/2011		DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. R.T.E. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 59
TC-13		CONTRACT NO. 60K66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

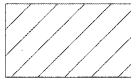







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHR 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

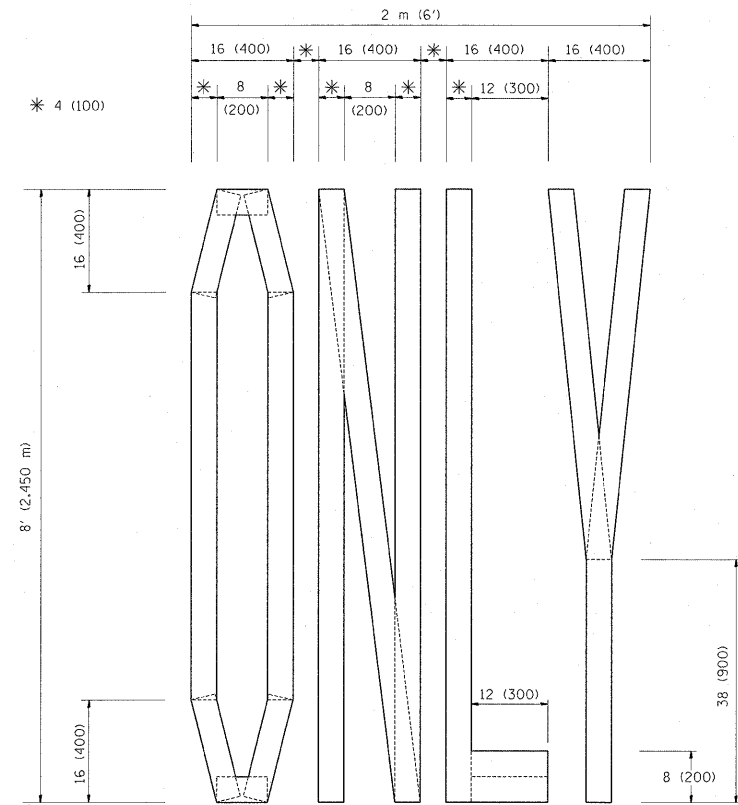
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

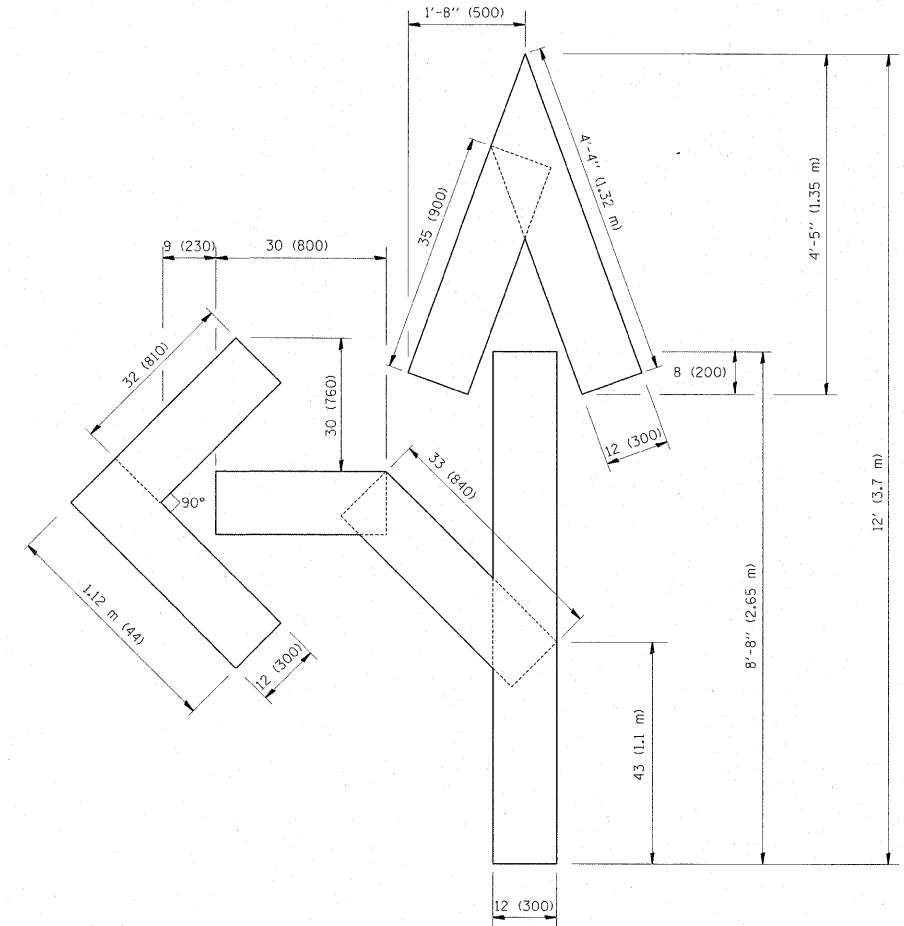
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

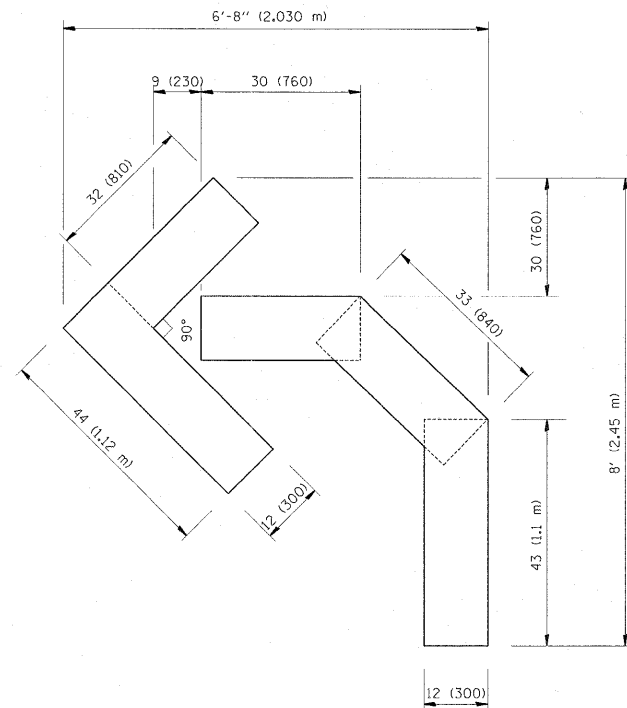
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44 R-N	COOK/WILL	68	60
TC-14			CONTRACT NO. 60K66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

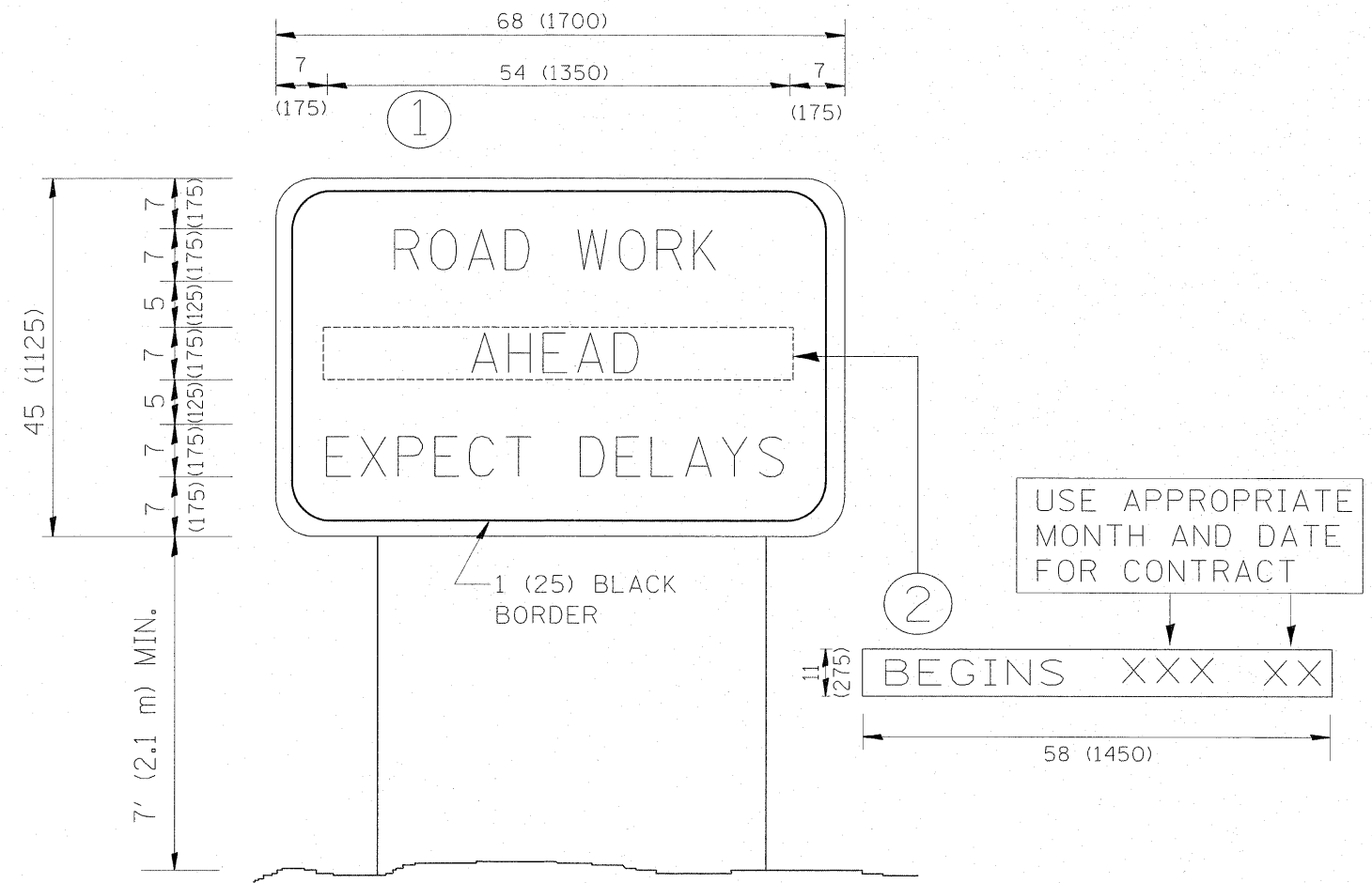
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PLOT SCALE = 50.0000 ' / IN.	CHECKED =	REVISED -T. RAMMACHER 03-02-98	
PLOT DATE = 3/26/2011	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2688	SECTION 44 R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 61
TC-16		CONTRACT NO. 60K66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

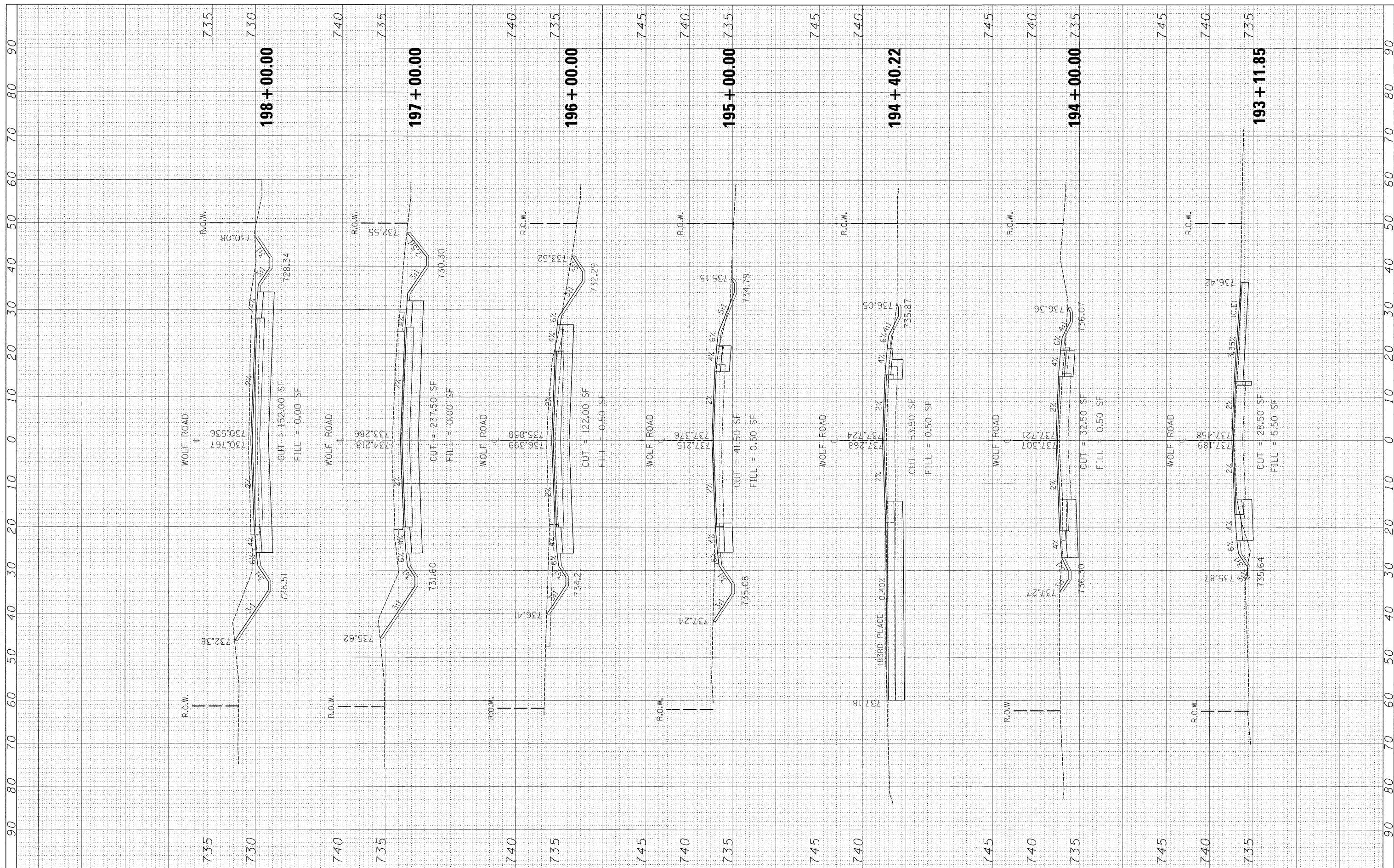
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FLO" DATE = 3/26/2011	DATE -	REVISED - C. JUCIUS 01-31-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
NO.	AREAS CHECKED		

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NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
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 PLOT DATE = 3/26/2011

DESIGNED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

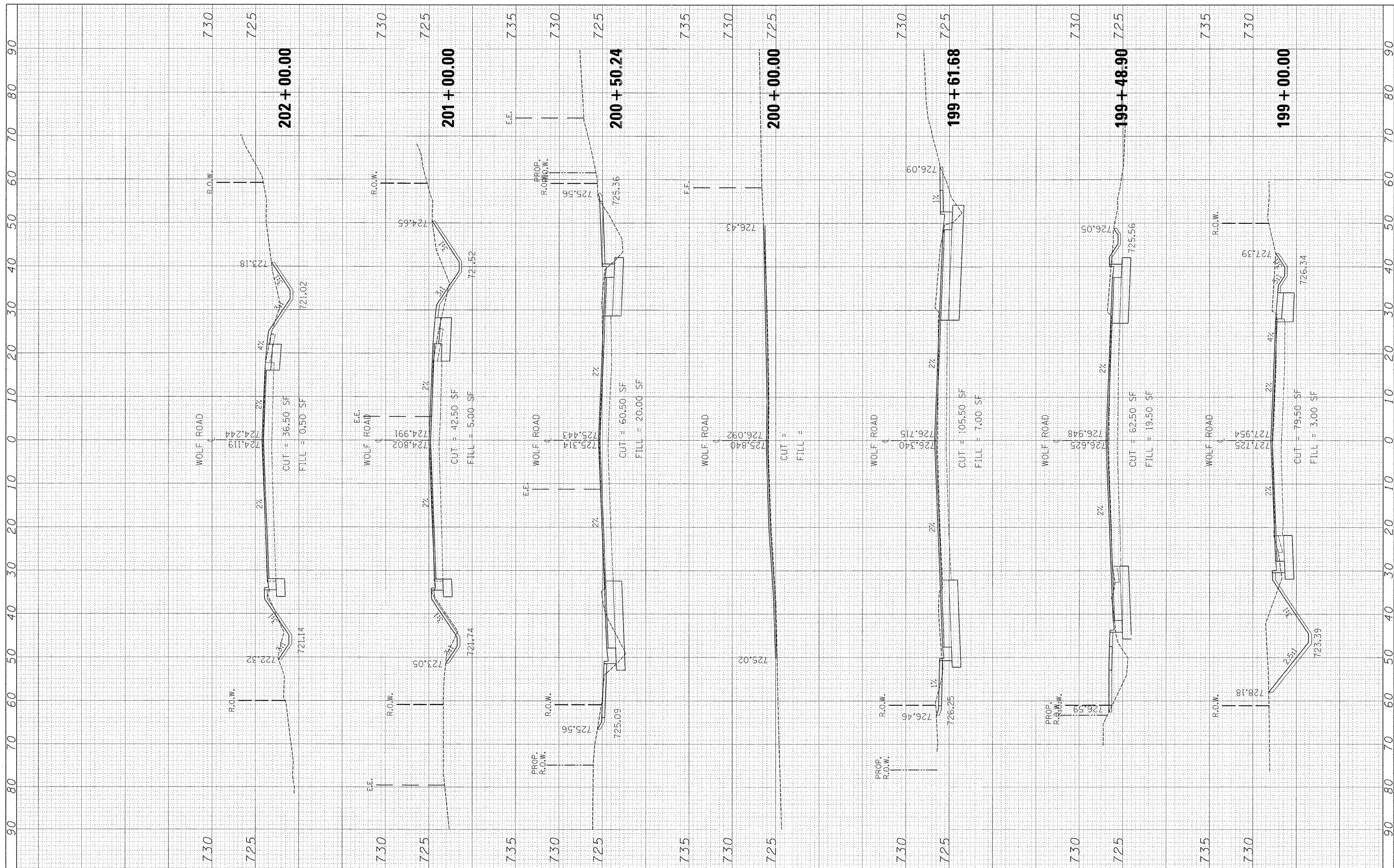
**WOLF ROAD
 CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 193+11.85 TO STA. 198+00.00

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CONTRACT NO. 60K66			ILLINOIS FED. AID PROJECT	

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 PLOT DATE = 3/26/2011

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**WOLF ROAD
 CROSS SECTIONS**

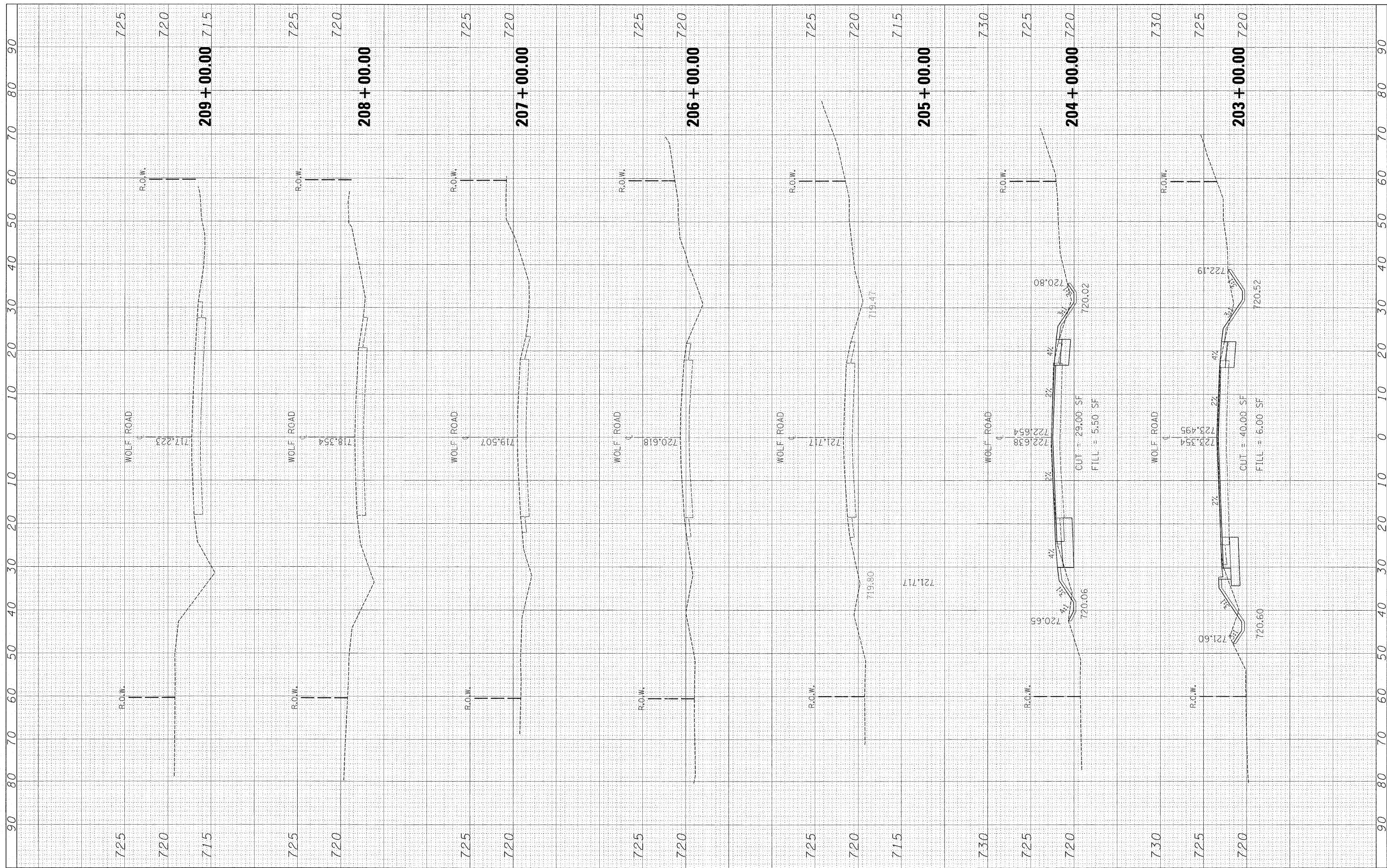
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CONTRACT NO. 60K66			ILLINOIS FED. AID PROJECT	

PROGRAM: 02/01/11 11:52 AM galbanjr

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ORIGINAL SURVEY	SURVEYED	BY	DATE
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

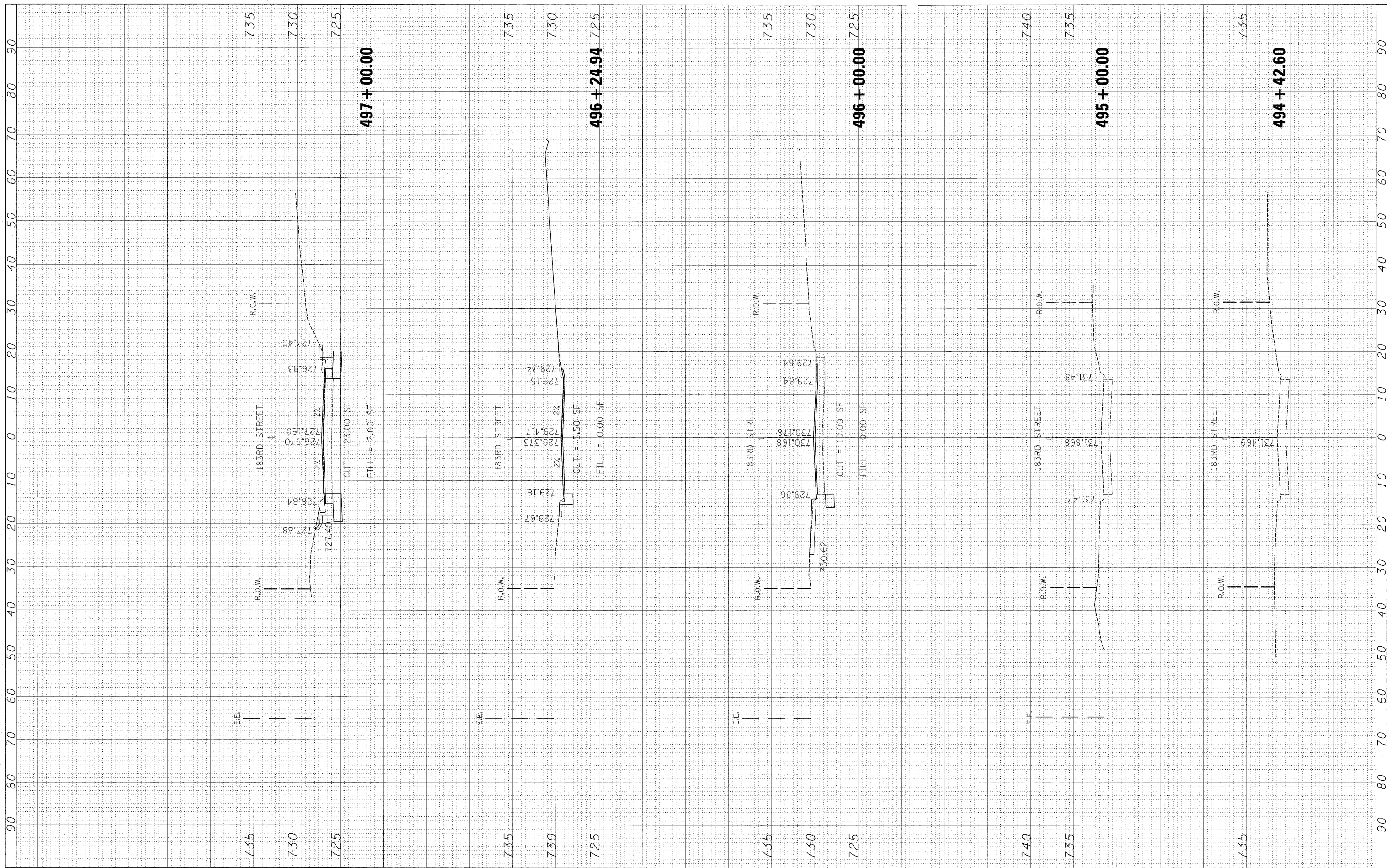
**WOLF ROAD
 CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 203+00.00 TO STA. 209+00.00

F.A.U. RTE 2688	SECTION 44-R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 66
CONTRACT NO. 60K66			ILLINOIS FED. AID PROJECT	

FINAL SURVEY	SURVEYED	BY	DATE
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ORIGINAL SURVEY	SURVEYED	BY	DATE
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DESIGNED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**183RD STREET (ORLAND PARKWAY)
 CROSS SECTIONS**

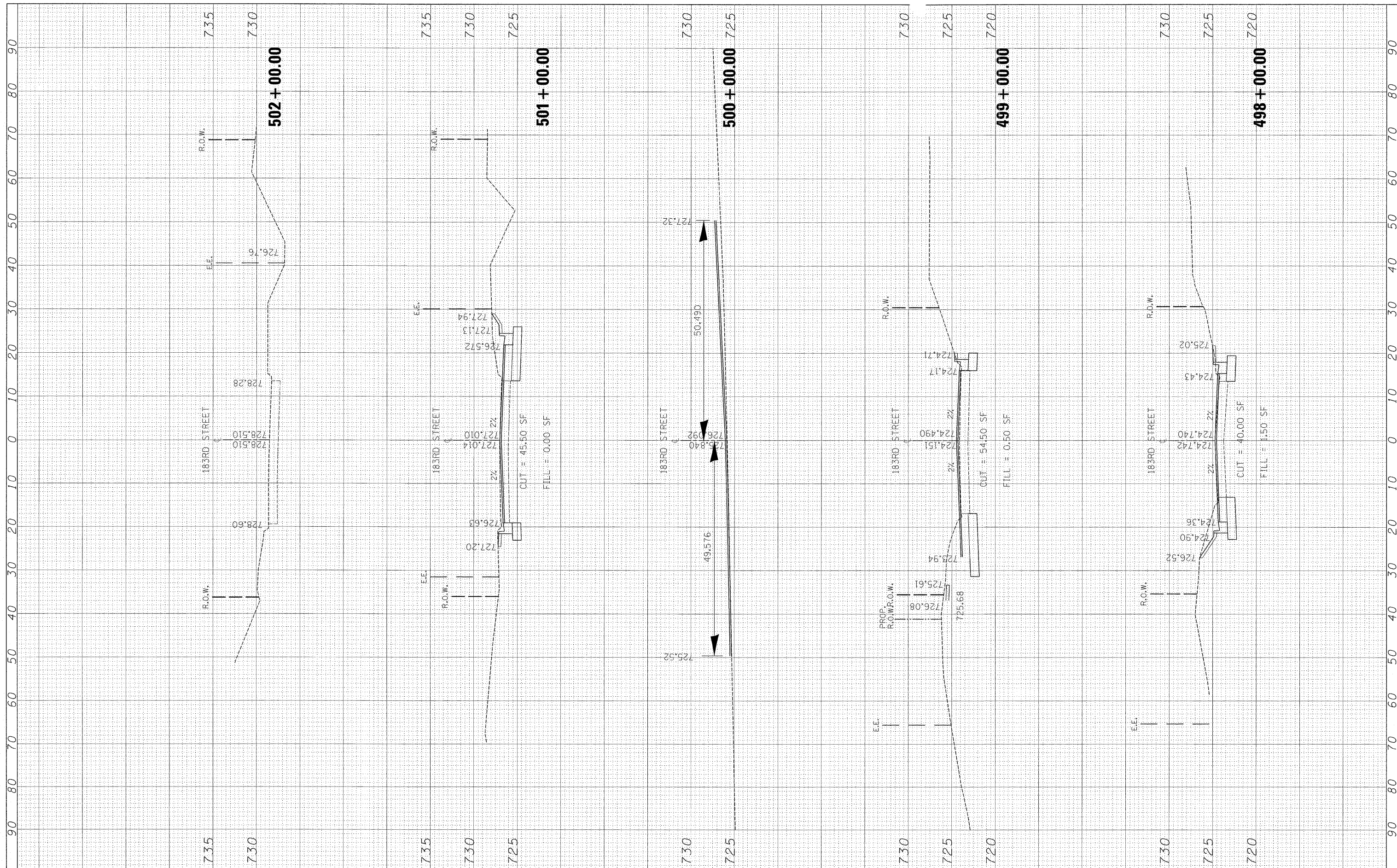
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F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2688	44-R-N	COOK/WILL	68	67
CONTRACT NO. 60K66			ILLINOIS FED. AID PROJECT	

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NO.	TEMPLATE		
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	AREAS CHECKED		



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DESIGNED -
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 DATE -

REVISED -
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 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**183RD STREET (ORLAND PARKWAY)
 CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 498+00.00 TO STA. 502+00.00

F.A.J. RTE. 2688	SECTION 44-R-N	COUNTY COOK/WILL	TOTAL SHEETS 68	SHEET NO. 68
CONTRACT NO. 60K66				
ILLINOIS FED. AID PROJECT				

11-600-000-0000-0000-0000-0000-0000-0000