

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY  
FAU ROUTE 2729 (MONTROSE AVENUE)  
CANFIELD AVENUE TO SAYRE AVENUE  
RESURFACING  
SECTION NO. 10-00060-00-RS  
PROJECT NO. TCSP-M-9003(748)  
VILLAGE OF NORRIDGE  
COOK COUNTY  
JOB NO. C-91-215-11

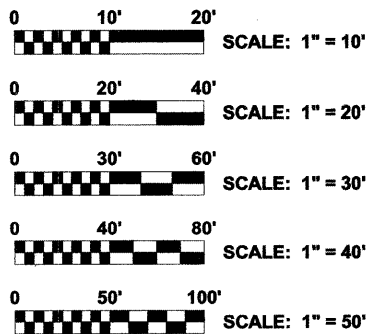
TRAFFIC DATA  
ADT (2010) = 20,300  
POSTED SPEED LIMIT = 35 MPH  
DESIGN SPEED LIMIT = 40 MPH

DESIGN DESIGNATION  
COLLECTOR

PROJECT LOCATED IN THE  
VILLAGE OF NORRIDGE

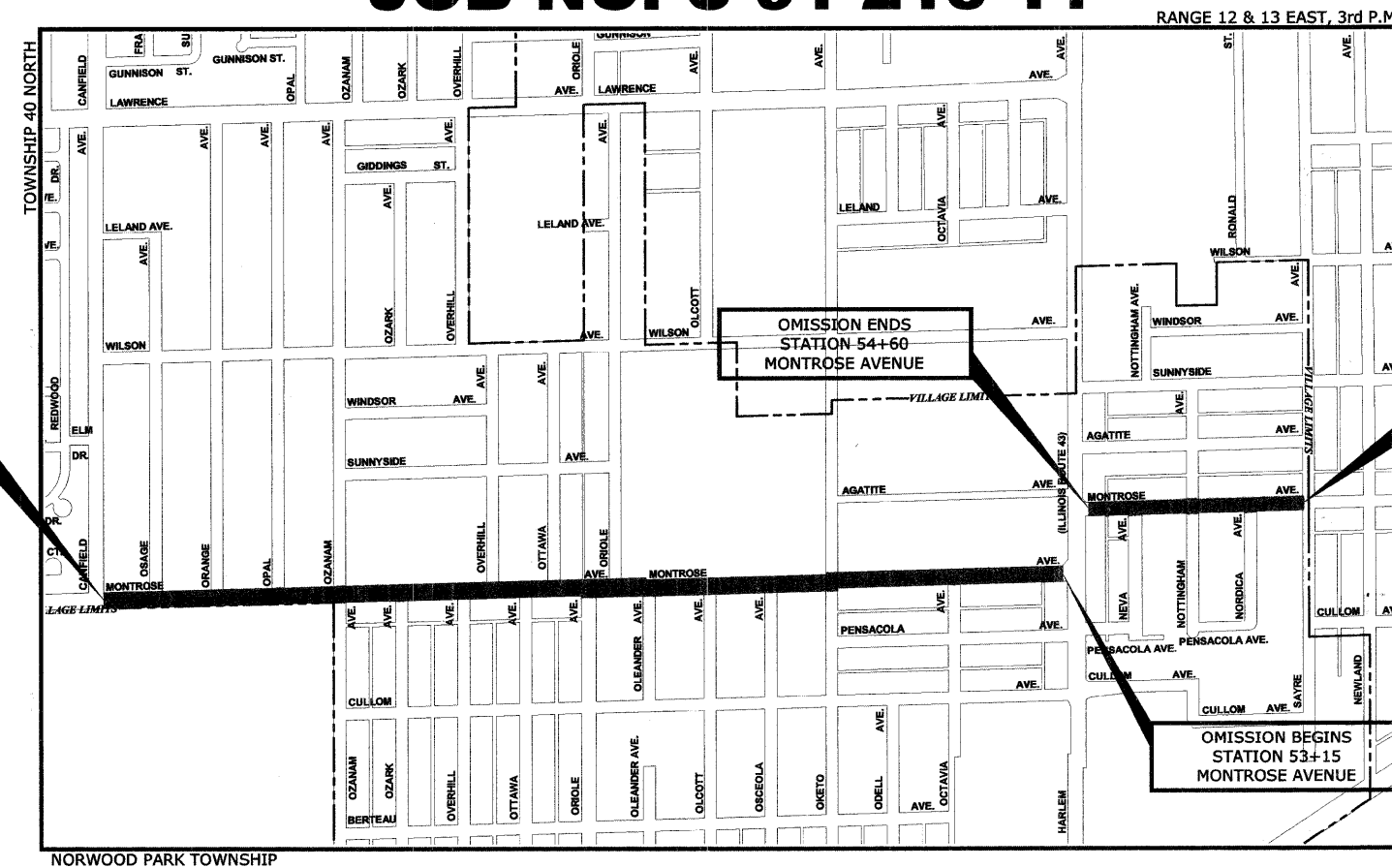


Call  
Before  
You Dig



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CONTRACT NO. 63582



NORWOOD PARK TOWNSHIP

■ - AREA OF IMPROVEMENT  
NOT TO SCALE  
GROSS LENGTH OF IMPROVEMENT = 6,545 FT (1.24 MI.)  
NET LENGTH OF IMPROVEMENT = 6,400 FT (1.21 MI.)

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAU 2729	10-00060-00-RS	COOK	17	1
ILLINOIS PROJECT TCSP-M-9003(748)				
CONTRACT NO. 63582				



LOCATION OF SECTION  
INDICATED THUS: ■

STATE OF ILLINOIS ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	<i>Mark A. Riddle</i> 20 11 VILLAGE OF NORRIDGE, PRESIDENT
PASSED	<i>April 11</i> 20 11 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASED FOR BID BASED ON LIMITED REVIEW	<i>April 11</i> 20 11 DEPUTY DIRECTOR OF HIGHWAYS, 92 REGION 1 ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS



DATE SIGNED: 03-29-11  
LICENSE EXPIRES: 11/30/11

EDWIN HANCOCK ENGINEERING COMPANY 9933  
ROOSEVELT ROAD PHONE : (708) 865-0300  
WESTCHESTER, ILLINOIS 60154

Drawing file: W:\Projects\61210060 - Montrose Ave LAPP (Harlem to Sayre)\Index-MontroseAve.dwg Mar 30, 2011 - 9:32am













## INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET, LOCATION MAP
2	INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS, LEGEND OF SYMBOLS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	EXISTING AND PROPOSED TYPICAL CROSS SECTIONS
5-9	MONTROSE AVENUE PAVING PLAN
10	DETECTOR LOOP DETAILS
11	DETAILS
12	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD 08)
13	PAVEMENT PATCHING FOR HMA SURFACE PAVEMENT (BD 22)
14	BUTT JOINT AND HMA TAPER DETAILS (BD 32)
15	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC 10)
16	TYPICAL APPLICATIONS RAISED REFLECTIVE MARKERS (SNOW-PLOW RESISTANT) (TC 11)
17	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC 13)

## I.D.O.T. STANDARD DRAWINGS

STANDARD NO.	TITLE OR DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C&D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
701501-06	URBAN LANE CLOSURE, 2-LANE, 2-WAY, UNDIVIDED
701606-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-07	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

## LEGEND OF SYMBOLS

EXISTING	PROPOSED	DESCRIPTION
		MANHOLE
		INLET
		CATCH BASIN
		FIRE HYDRANT
		WATER MAIN VALVE VAULT
		POWER POLE
		STREET LIGHT
		RIM ELEVATION INVERT ELEVATION
	<b>A</b>	STRUCTURE TO BE ADJUSTED
	<b>A*</b>	STRUCTURE TO BE ADJUSTED (SPECIAL)
	<b>1C</b>	TYPE 1 FRAME AND CLOSED LID
	<b>1P</b>	TYPE 1 FRAME AND OPEN LID
	<b>RC</b>	STRUCTURE TO BE RECONSTRUCTED

### STANDARDS

REFERENCES TO STANDARDS IN THE PLANS AND SPECIAL PROVISIONS SHALL BE THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION AS SHOWN ON THE INDEX OF SHEETS. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2007, THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED JANUARY 1, 2011, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JULY 2009 SIXTH EDITION, THE DETAILS IN THE PLANS, AND THE SPECIAL PROVISIONS.

### UNDERGROUND UTILITIES

BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED).

THE LOCATIONS OF THE UNDERGROUND UTILITIES, IF SHOWN ON THE PLANS, HAVE BEEN OBTAINED BY FIELD SURVEYS AND SEARCHES OF AVAILABLE RECORDS. IT IS BELIEVED THAT DATA IS ESSENTIALLY CORRECT, BUT THE VILLAGE OF NORRIDGE, THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND/OR OTHER OFFICES AND AGENCIES ASSOCIATED WITH THE DEVELOPMENT OF THESE PLANS DO NOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. THE CONTRACTOR WILL BE REQUIRED TO VERIFY THE LOCATION OF EACH FACILITY WITH THE UTILITY COMPANY, AND SHALL TAKE DUE CARE IN ALL PHASES OF THE CONSTRUCTION TO PROTECT SUCH FACILITIES THAT MAY BE AFFECTED BY THE WORK. ALL DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE IN ACCORDANCE WITH ARTICLES 107.20 AND 107.31.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY OWNERS AND THE ENGINEER.

### NOTIFICATION OF RESIDENTS

THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING WRITTEN NOTICE TO ALL RESIDENCES AND/OR PLACES OF BUSINESS AT LEAST ONE (1) WORKING DAY PRIOR TO PERFORMING ALL CONSTRUCTION ACTIVITY THAT WILL ELIMINATE ACCESS TO THEIR PROPERTY. THE WRITTEN NOTICE SHALL BE APPROVED BY THE ENGINEER AND A COPY PROVIDED TO THE VILLAGE PRIOR TO THE BEGINNING OF CONSTRUCTION.

### MAINTENANCE OF TRAFFIC

MONTROSE AVENUE WILL REMAIN OPEN TO THRU TRAFFIC DURING ALL OPERATIONS. FLAGGERS SHALL BE USED FOR THE INSTALLATION OF THE POLYMERIZED LEVELING BINDER, THE HOT-MIX ASPHALT SURFACE AND THERMOPLASTIC STRIPING.

### FIELD OFFICE

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR VILLAGE PROPERTY ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.

### BARRICADES

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED, ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.

### OPEN EXCAVATIONS

THE CONTRACTOR SHALL NOT BE ALLOWED TO LEAVE EXCAVATION NECESSARY FOR PAVEMENT PATCHES OR STRUCTURE ADJUSTMENTS OPEN OVERNIGHT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETELY BACKFILLING OR INSTALLING A PLATE OVER ALL EXCAVATIONS AT THE END OF EACH DAY IN ACCORDANCE WITH ARTICLE 107.09.

### CONCRETE BREAKERS

WHEN REMOVING PAVEMENT AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS SUCH AS DROP HAMMERS, WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES, WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.

### SAW CUTTING

THE CONTRACTOR SHALL SAW CUT ASPHALT PAVEMENT AS INDICATED ON THE PLANS TO SEPARATE THE EXISTING PAVEMENT TO BE REMOVED BY APPROVED MEANS OR AN APPROVED CONCRETE SAW TO A DEPTH AS DIRECTED BY THE ENGINEER. SUITABLE GUIDELINES OR DEVICES SHALL BE USED TO ASSURE CUTTING A NEAT, STRAIGHT LINE AS SHOWN ON THE PLANS. CARE SHALL BE TAKEN BY THE CONTRACTOR NOT TO DAMAGE THE REMAINING PAVEMENT DIRECTLY ADJACENT TO THE PAVEMENT TO BE REMOVED. DAMAGE TO THE EXISTING PAVEMENT RESULTING FROM PAVEMENT REMOVAL OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTTING DESCRIBED ABOVE SHALL BE INCLUDED IN THE COST OF ITEM BEING REMOVED.

### CURB AND GUTTER TRANSITIONS

TRANSITIONS FROM THE PROPOSED CURB AND GUTTERS TO THE DEPRESSED CURB AND GUTTERS AND PROPOSED CURB AND GUTTERS TO EXISTING CURB AND GUTTERS SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.

THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE OF THE RESPECTIVE COMBINATION CONCRETE CURB AND GUTTER ITEMS.

### PAVEMENT PATCHING

LOCATIONS OF PATCHES WILL BE DETERMINED IN FIELD BY ENGINEER AFTER MILLING OPERATIONS.

### CURING AND PROTECTION

AFTER THE CONCRETE HAS BEEN FINISHED AND THE WATER SHEEN HAS DISAPPEARED FROM THE SURFACE OF THE CONCRETE, THE SURFACE SHALL BE SEALED WITH MEMBRANE CURING COMPOUND OF A TYPE APPROVED BY THE ENGINEER. THE SEAL SHALL BE MAINTAINED FOR THE SPECIFIED CURING PERIOD. THE EDGES OF THE CONCRETE SHALL ALSO BE SEALED IMMEDIATELY AFTER THE FORMS ARE REMOVED. IN ADDITION, ALL CONCRETE PLACED DURING PERIODS OF COLD WEATHER SHALL BE PROTECTED IN ACCORDANCE WITH ARTICLE 720.13 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

THE WORK SHALL BE UNDER THE CHARGE AND CARE OF THE CONTRACTOR UNTIL FINAL ACCEPTANCE BY THE DEPARTMENT. THE CONTRACTOR SHALL ASSUME ALL RESPONSIBILITY FOR INJURY OR DAMAGE TO THE WORK FROM ANY CAUSE WHATSOEVER AND HE SHALL REBUILD, REPAIR OR RESTORE THE DAMAGED WORK AT HIS OWN EXPENSE.

THIS ITEM WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS CONCRETE ITEMS IN THE CONTRACT.

## GENERAL NOTES

### MAINTENANCE OF EXISTING DRAINAGE STRUCTURES

WHEN LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS AND DRAINAGE STRUCTURE OBSTRUCTING THE NATURAL FLOW OF WATER, THE LOOSE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS, ALL DRAINAGE FACILITIES SHALL BE CLEAN AND FREE OF ALL OBSTRUCTIONS CAUSED BY CONSTRUCTION OPERATIONS.

### MAINTENANCE OF SEWER FLOWS

THE CONTRACTOR SHALL CONDUCT HIS/HER OPERATIONS TO MAINTAIN FLOW AT ALL TIMES THROUGH EXISTING STORM AND SANITARY SEWER SYSTEMS. HE/SHE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER COLLECTED IN A SAFE MANNER WITHOUT DAMAGE OF ANY KIND TO ADJACENT PROPERTIES. THE ENDS OF EXISTING DRAINAGE LINES THAT ARE NOT TO BE INCORPORATED INTO THE PROJECT ARE TO BE SEALED AS SPECIFIED IN THE SPECIAL PROVISIONS. EXISTING STRUCTURES ARE TO BE INSPECTED BEFORE CONSTRUCTION STARTS - ACCUMULATION OF MATERIAL IN THE STRUCTURE DUE TO CONSTRUCTION OPERATIONS SHALL BE REMOVED BY THE CONTRACTOR AT HIS/HER EXPENSE.

### FRAMES AND LIDS

THE LOCATION AND TYPE OF FRAMES AND LIDS REQUIRED FOR ALL MANHOLES AND VALVE VAULTS LISTED IN THE SUMMARY OF QUANTITIES MAY BE FOUND ON THE PLANS. WHERE LIDS ARE CALLED FOR ON THE PLANS, THEY SHALL BE IN ACCORDANCE WITH ARTICLE 604.04 OF THE STANDARD SPECIFICATIONS AND THE TERM LID IS USED IN LIEU OF GRATE. ALL LIDS ON SANITARY MANHOLES, COMBINED SEWER MANHOLES, AND VALVE VAULTS SHALL BE OF THE SELF SEALING TYPE.

THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES, AND VALVE VAULTS THAT ARE TO BE ABANDONED ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF NORRIDGE. THE CONTRACTOR IS TO DELIVER FRAMES AND LIDS TO THE VILLAGE OF NORRIDGE PUBLIC WORKS YARD LOCATED AT 8415 W FOSTER AVENUE. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF EACH BID ITEM.

### FRAMES AND LIDS, TYPE 1

ALL COMBINED SEWER CLOSED LIDS SHALL BE SELF-SEALING WITH RECESSED PICK HOLES. ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR CONSTRUCTION, ADJUSTMENT, OR RECONSTRUCTION OF ANY MANHOLE OR VALVE VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS:

STRUCTURE TYPE	NEENAH FOUNDRY* FRAME AND LID	SELF SEALING	WORD CAST INTO LID
SANITARY MANHOLE	R-1713-B	YES	SANITARY
COMBINATION MANHOLE	R-1713-B	YES	SANITARY
STORM MANHOLE	R-1713-B	NO	STORM
VALVE VAULT	R-1713-B	YES	WATER
CATCH BASIN	R-2504-D	NO	-----
INLET	R-2504-D	NO	-----

\* NOTE: OR ENGINEER APPROVED EQUAL.

### PRECAST CONCRETE SECTIONS FOR STRUCTURES

ALL NEW STRUCTURES AND STRUCTURES TO BE RECONSTRUCTED SHALL BE BUILT WITH PRECAST REINFORCED CONCRETE SECTIONS FABRICATED IN ACCORDANCE WITH A.S.T.M. DESIGNATION C-478.

### CONNECTING EXISTING PIPES TO STRUCTURES

ANY PIPE, UP TO FOUR FEET (4') IN LENGTH PER EACH PIPE AND THE NECESSARY TRENCH BACKFILL, USED TO CONNECT EXISTING PIPES TO THE STRUCTURE TO BE RECONSTRUCTED OR TO THE NEW STRUCTURE SHALL BE INCLUDED IN THE COST OF THE STRUCTURE TO BE RECONSTRUCTED OR THE NEW STRUCTURE.

### CONNECTION OF DISSIMILAR PIPE MATERIALS

NON-SHEAR CONNECTORS SHALL BE USED WHERE NEW SEWER PIPES ARE TO BE CONNECTED TO EXISTING SEWER PIPES. THE NON-SHEAR CONNECTORS SHALL CONSIST OF A SYNTHETIC RUBBER SLEEVE, WHICH WILL RESIST DETERIORATION CAUSED BY WASTES FOUND IN SEWERS AND TWO STAINLESS STEEL STRAPS. THE CONNECTORS SHALL BE "MISSION" COUPLINGS, "FERNCO" COUPLINGS, OR ENGINEER APPROVED EQUAL. ALL LABOR AND MATERIAL REQUIRED TO CONNECT NEW SEWER PIPE TO EXISTING SEWER PIPE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

### FORMS FOR CONCRETE SIDEWALKS, DRIVEWAYS AND GUTTER FLAGS

THIS WORK SHALL BE IN ACCORDANCE WITH ARTICLE 424.05, 423.05, AND 1103.05 EXCEPT THAT A 2 x 6 BOARD WILL BE USED AS A FORM FOR ALL SIDEWALKS TO BE INSTALLED FIVE INCHES (5") IN THICKNESS. A 2 x 8 BOARD WILL BE USED AS A FORM FOR ALL DRIVEWAYS TO BE INSTALLED SEVEN INCHES (7") IN THICKNESS. A 2 x 10 BOARD WILL BE USED AS A FORM FOR ALL DRIVEWAYS TO BE INSTALLED EIGHT INCHES (8") IN THICKNESS. A 2 x 12 BOARD WILL BE USED AS A FORM FOR ALL GUTTER FLAGS TO BE INSTALLED TEN INCHES (10") IN THICKNESS. ALL FORMS MUST BE OF A MINIMUM HEIGHT OF THE PROPOSED THICKNESS OF THE RESPECTIVE CONCRETE ITEMS TO BE INSTALLED.

### AGGREGATE BEDDING FOR CONCRETE WORK

NEW SIDEWALK AND DRIVEWAY PAVEMENT SHALL BE PLACED ON A MINIMUM OF TWO INCHES (2") OF COMPACTED CA-6 STONE BEDDING. 8" DRIVEWAY PAVEMENT SHALL BE PLACED ON A MINIMUM OF FOUR INCHES (4") OF COMPACTED CA-6 STONE BEDDING. ADDITIONAL AGGREGATE REQUIRED TO ADJUST THE EXISTING ELEVATION OF THE SUBGRADE TO THE PROPOSED ELEVATION WILL BE INCLUDED AS PART OF THIS WORK.

THIS ITEM WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE FOR THE RESPECTIVE CONCRETE ITEMS IN THE CONTRACT.

### BUTT JOINTS

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER" DETAIL SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

### PROTECTIVE COAT

ALL NEW CONCRETE SURFACES SHALL BE PROTECTIVELY COATED WITH A BOILED LINSEED OIL MIXTURE REGARDLESS OF THE TIME OF YEAR THE CONCRETE WORK IS CONSTRUCTED. THE PREPARATION AND APPLICATION OF THIS MIXTURE SHALL BE IN ACCORDANCE WITH ARTICLE 426.18 OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".



◆ Civil Engineers  
◆ Municipal Consultants  
◆ Established 1911

9533 Riverdale Road  
Westchester, Illinois 60154-2780  
Phone: 708/965-0390  
Fax: 708/965-1212

DESIGNED	—	—	REVISED	—
DRAWN	—	—	REVISED	—
CHECKED	—	—	REVISED	—
DATE	—	03/25/11	REVISED	—

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, I.D.O.T. STANDARD DRAWINGS,  
LEGEND OF SYMBOLS, AND GENERAL NOTES

SCALE: NONE

SHEET NO. 1 OF 1 SHEETS

STA.

TO STA.

F.A.U.  
RTE.

2729

SECTION

10-00060-00-RS

COUNTY

COOK

TOTAL  
SHEETS

17

SHEET  
NO.

2

CONTRACT NO. 63582

FED. ROAD DIST. NO. 1 ILLINOIS

FED. AID PROJECT —

Drawing file: W:\Projects\61210080 - Montrose Ave LAPP (Horlem to Sore)\Quantities-MontroseAve.dwg Apr 05, 2011 - 9:12am

SUMMARY OF QUANTITIES				
CODE	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE RESURFACING 005 80% FEDERAL 20% STATE
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	500	500
25200100	SODDING	SQ YD	500	500
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	6900	6900
40600300	AGGREGATE (PRIME COAT)	TON	140	140
40600895	CONSTRUCTING TEST STRIP	EACH	2	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	360	360
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4000	4000
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	15	15
42101300	PROTECTIVE COAT	SQ YD	1125	1125
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	250	250
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	40	40
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3100	3100
42400800	DETECTABLE WARNINGS	SQ FT	560	560
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	34500	34500
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	360	360
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1900	1900
44000600	SIDEWALK REMOVAL	SQ FT	3700	3700
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	30	30
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	60	60
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	100	100
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	150	150

SUMMARY OF QUANTITIES				
CODE	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE RESURFACING 005 80% FEDERAL 20% STATE
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	15	15
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	55	55
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	25	25
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	40	40
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	825	825
60604400	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18	FOOT	1075	1075
67100100	MOBILIZATION	L SUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	8000	8000
★ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	475	475
★ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	21500	21500
★ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1800	1800
★ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	750	750
★ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	400	400
★ 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	160	160
★ 78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	100	100
★ 88600600	DETECTOR LOOP REPLACEMENT	FOOT	700	700
X4060826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2000	2000
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	78	78

★ DENOTES SPECIALTY ITEM



◆ Civil Engineers  
◆ Municipal Consultants  
◆ Established 1911

9533 Roseville Road  
Waukegan, Illinois 60154-2780  
Phone: 708/865-4380  
Fax: 708/865-1212

DESIGNED	—	—
DRAWN	—	—
CHECKED	—	—
DATE	03/25/11	—

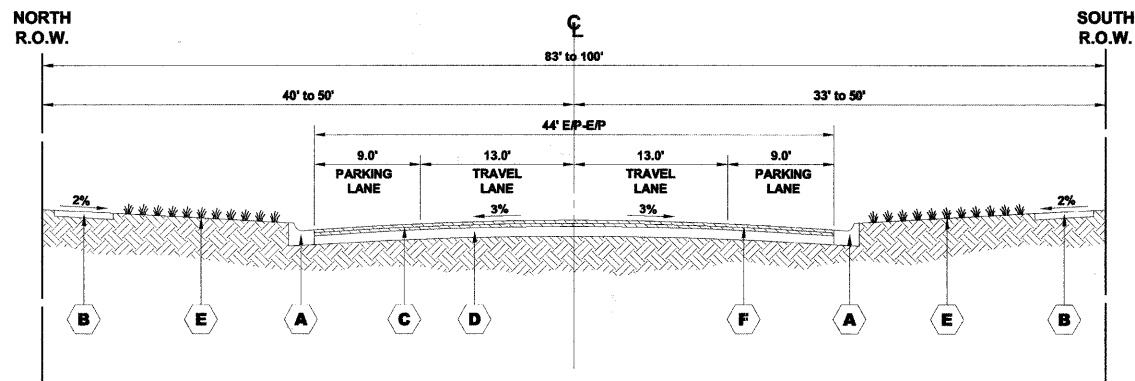
REVISED	—
REVISED	—
REVISED	—
REVISED	—

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

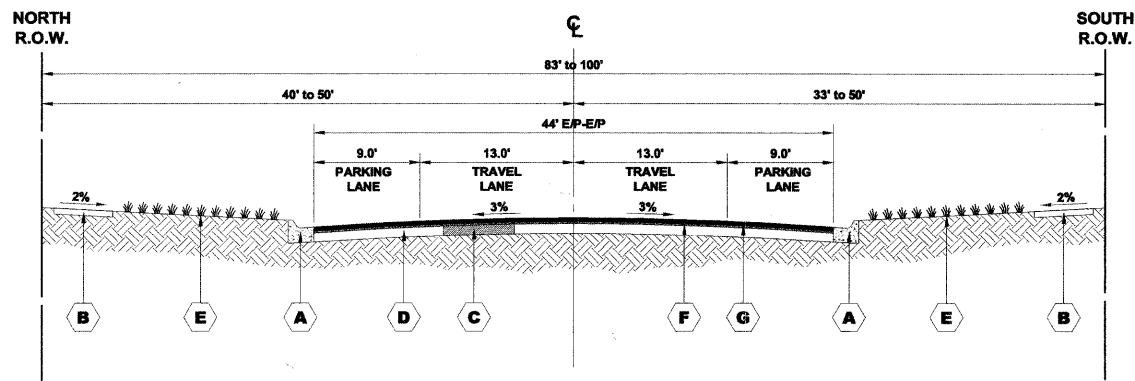
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	3
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 63582	
		FED. AID PROJECT	—	



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.18	D	EXISTING HOT-MIX ASPHALT BASE COURSE (APPROX. 10")
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	E	EXISTING LANDSCAPED PARKWAY
C	EXISTING HOT-MIX ASPHALT SURFACE COURSES, 2"	F	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"

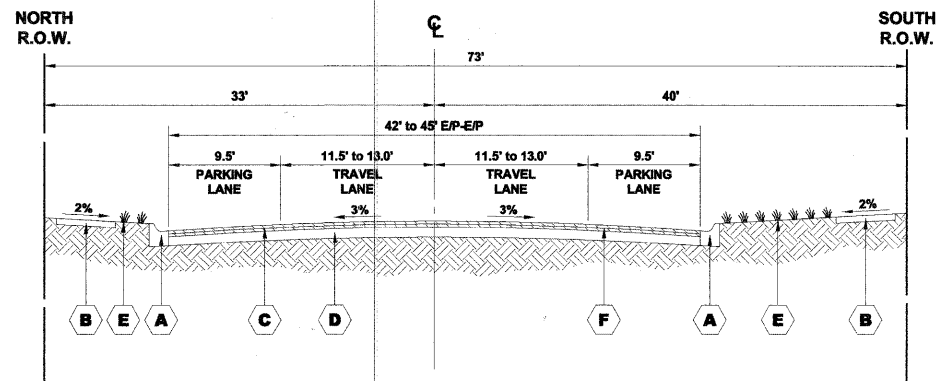
**EXISTING TYPICAL SECTION**  
(CANFIELD ROAD TO HARLEM AVENUE)  
STA. 1+00 TO STA. 53+15



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	E	EXISTING LANDSCAPED PARKWAY
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	F	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
C	PROPOSED CLASS D PATCH	G	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
D	EXISTING HOT-MIX ASPHALT BASE COURSE		

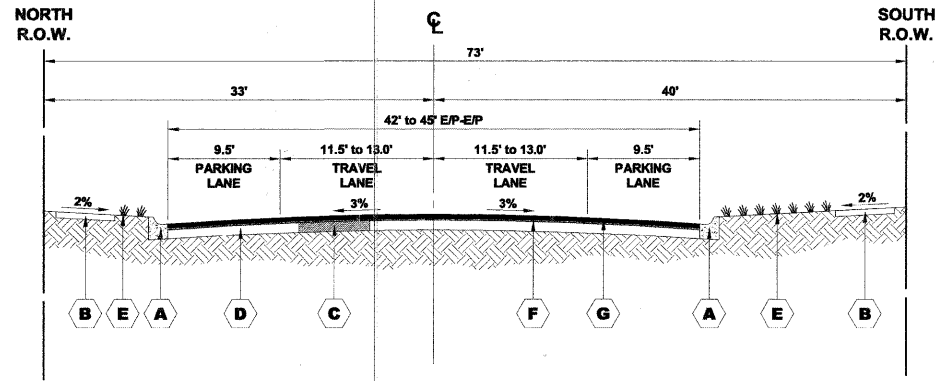
**PROPOSED TYPICAL SECTION**  
(CANFIELD ROAD TO HARLEM AVENUE)  
STA. 1+00 TO STA. 53+15

HOT-MIX ASPHALT (HMA) MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
<b>RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL - 9.5mm), 2"	4% @70 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL - 4.75, N50, 1"	4% @ 50 GYR.
<b>INCIDENTAL HMA SURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL - 9.5mm)	4% @50 GYR.
<b>PATCHING</b>	
CLASS D PATCHES (HOT-MIX ASPHALT BINDER COURSE, IL - 19.0), 8" (IN 3 LIFTS)	4% @ 70 GYR.



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12	D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE (APPROX. 8")
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	E	EXISTING LANDSCAPED PARKWAY
C	EXISTING HOT-MIX ASPHALT SURFACE COURSES, 2"	F	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"

**EXISTING TYPICAL SECTION**  
(HARLEM AVENUE TO SAYRE AVENUE)  
STA. 54+60 TO STA. 66+45



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
A	PROPOSED INTERMITTENT COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	E	EXISTING LANDSCAPED PARKWAY
B	EXISTING PORTLAND CEMENT CONCRETE SIDEWALK	F	PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
C	PROPOSED CLASS D PATCH	G	PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
D	EXISTING PORTLAND CEMENT CONCRETE BASE COURSE		

**PROPOSED TYPICAL SECTION**  
(HARLEM AVENUE TO SAYRE AVENUE)  
STA. 54+60 TO STA. 66+45

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

THE CONTRACTOR SHALL MILL BEFORE PATCHING.



◆ Civil Engineers  
◆ Municipal Consultants  
◆ Established 1911

9933 Roosevelt Road  
Westchester, Illinois 60154-2780  
Phone: 708/965-0390  
Fax: 708/965-1212

DESIGNED	---	REVISED	---
DRAWN	---	REVISED	---
CHECKED	---	REVISED	---
DATE	03/25/11	REVISED	---

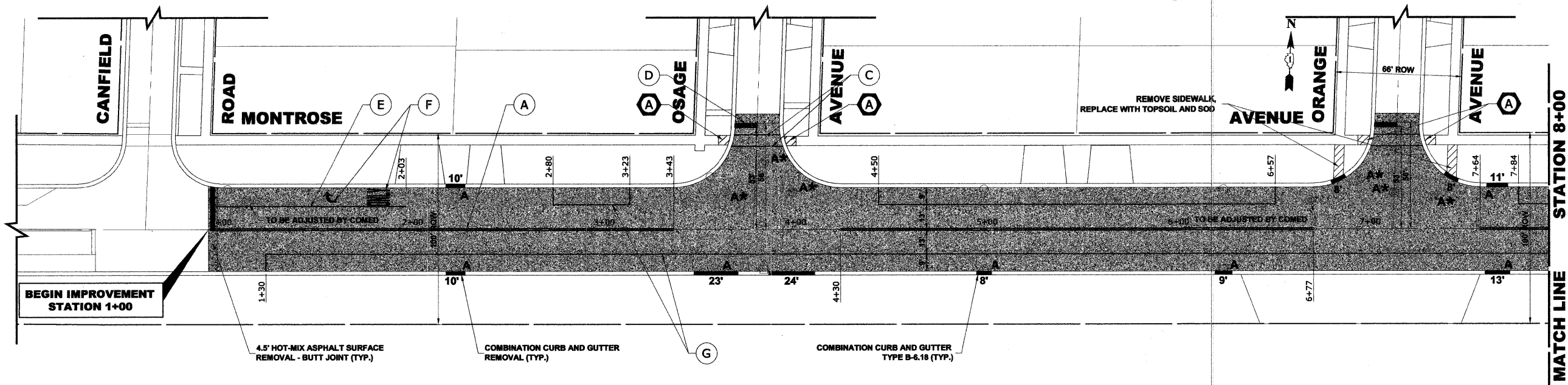
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED  
TYPICAL CROSS SECTIONS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

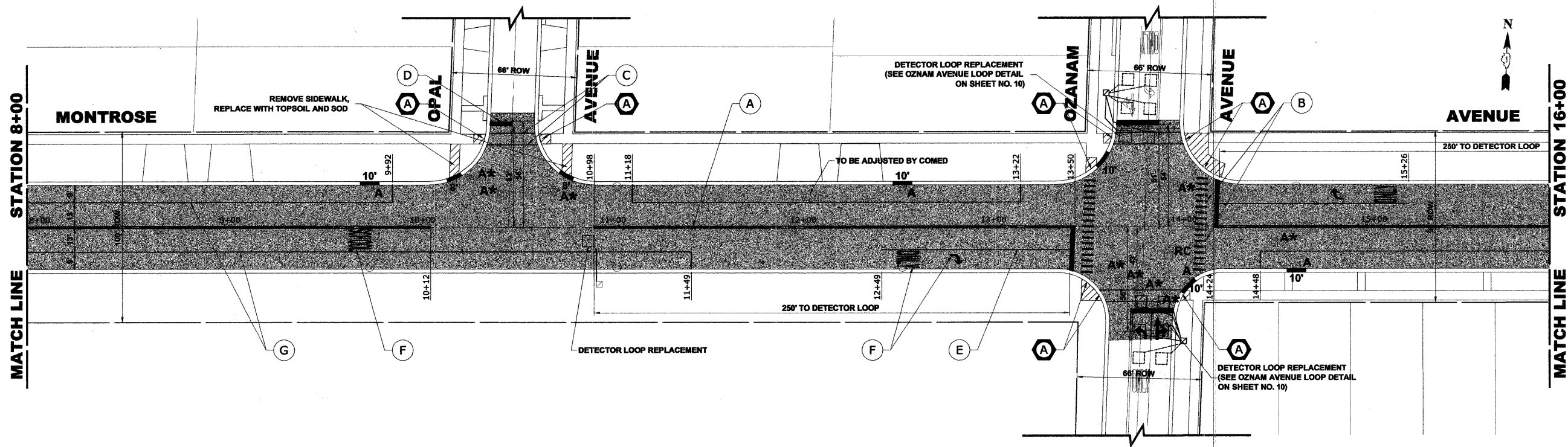
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	4
		CONTRACT NO. 63582		
		FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT		



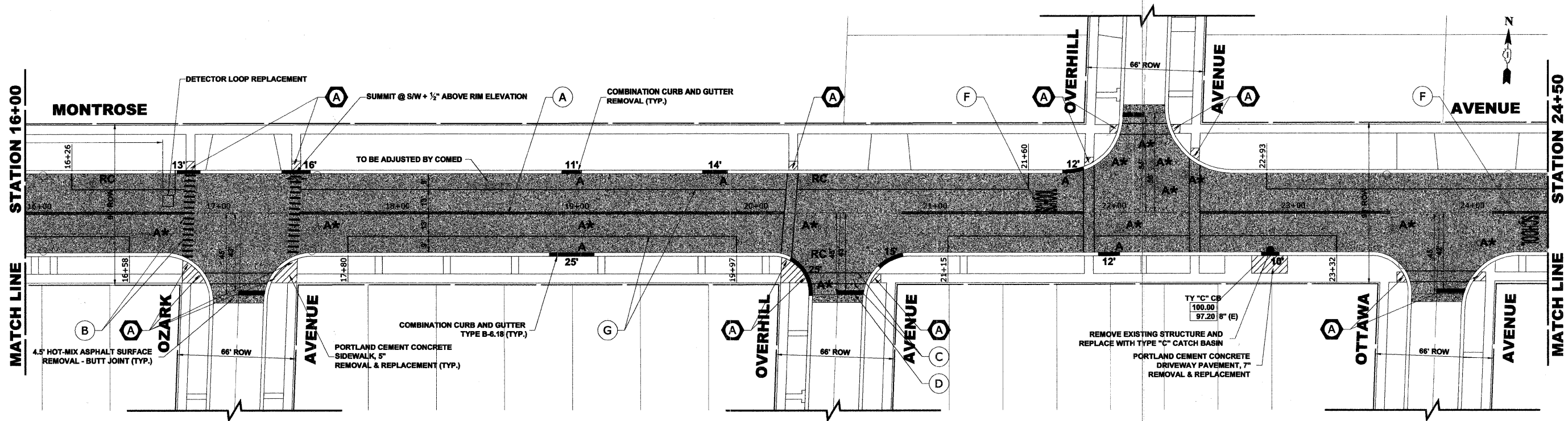


**NOTE:**  
 \* DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)  
 (A) DETECTABLE WARNING

PAVEMENT MARKINGS LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 4", DOUBLE @ 11" C-C, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE, 12", CROSS WALK @ 36" C-C, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", TURN LANE, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PARKING LINE, WHITE	(G)



Drawing file: W:\Projects\61210060 - Montrose Ave LAPP (Harcort to Scyre)\MontroseAveLAPP.dwg Mar 30, 2011 - 9:33am

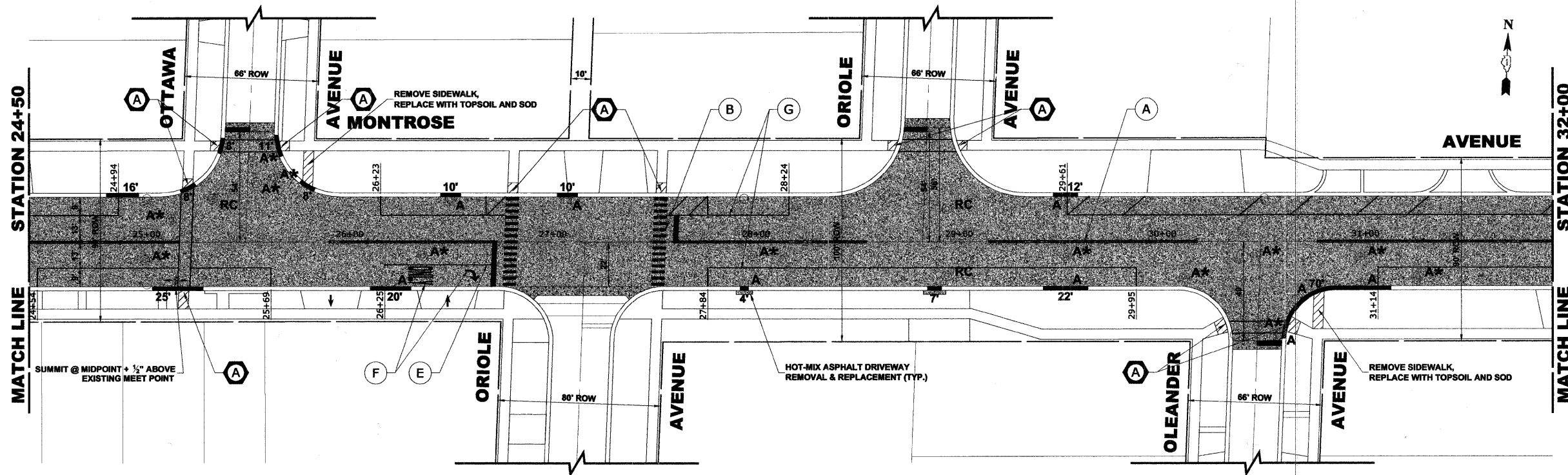


# **NOTE:**

\* DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

**(A)** DETECTABLE WARNING

PAVEMENT MARKINGS LEGEND	
ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 4", DOUBLE @ 11" C-C, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE, 12", CROSS WALK @ 36" C-C, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", TURN LANE, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PARKING LINE, WHITE	(G)



♦ Civil Engineers  
 ♦ Municipal Consultants  
 ♦ Established 1911

9933 Koenigs Road  
 Wheaton, Illinois 60154-2780  
 Phone: 708/945-4390  
 Fax: 708/945-1212

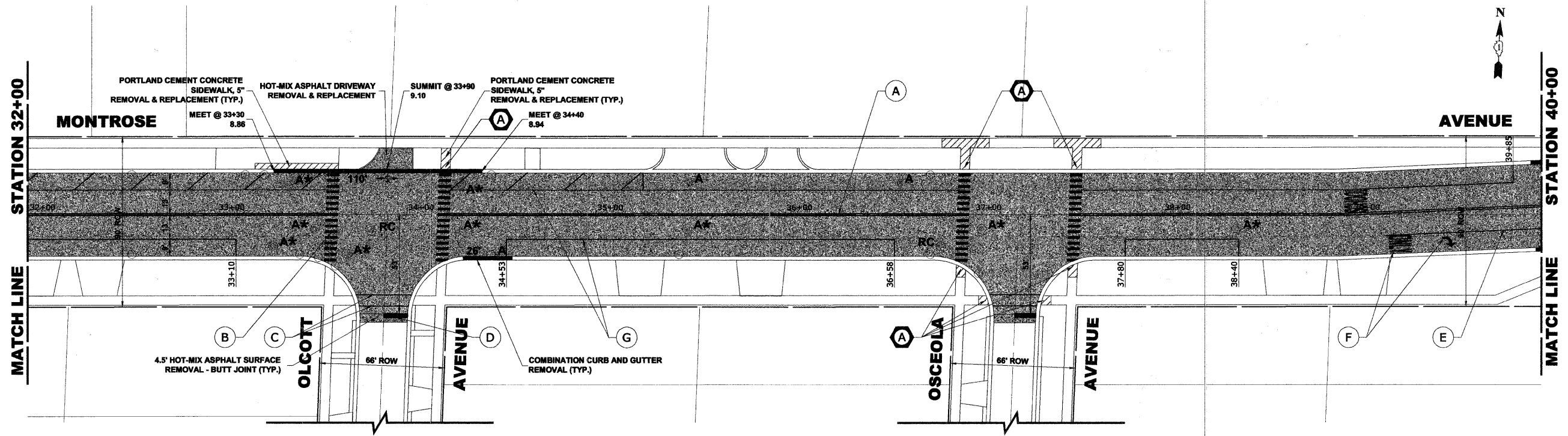
DESIGNED	---	REVISED	---
DRAWN	---	REVISED	---
CHECKED	---	REVISED	---
DATE	03/25/11	REVISED	---

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

## **MONTROSE AVENUE PAVING PLAN**

SCALE: 1" = 30'    SHEET NO. 2 OF 5 SHEETS    STA. 16+00    TO STA. 32+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	6
CONTRACT NO. 63582			FED. AID PROJECT	



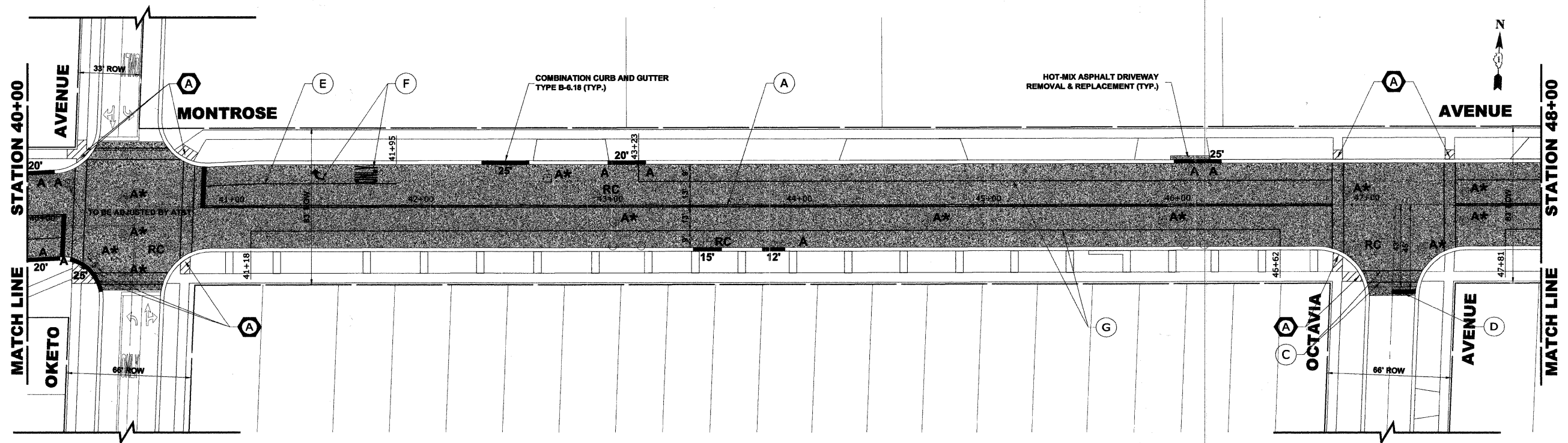
### PAVEMENT MARKINGS LEGEND

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 4", DOUBLE @ 11" C-C, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE, 12", CROSS WALK @ 36" C-C, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", TURN LANE, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PARKING LINE, WHITE	(G)

### NOTE:

\* DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

(A) DETECTABLE WARNING



◆ Civil Engineers  
◆ Municipal Consultants  
◆ Established 1911

9933 Rosemont Road  
Westchester, Illinois 60154-3780  
Phone: 708/665-4300  
Fax: 708/665-1212

DESIGNED	---	REVISED	---
DRAWN	---	REVISED	---
CHECKED	---	REVISED	---
DATE	03/25/11	REVISED	---

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

### MONTROSE AVENUE PAVING PLAN

SCALE: 1" = 30'

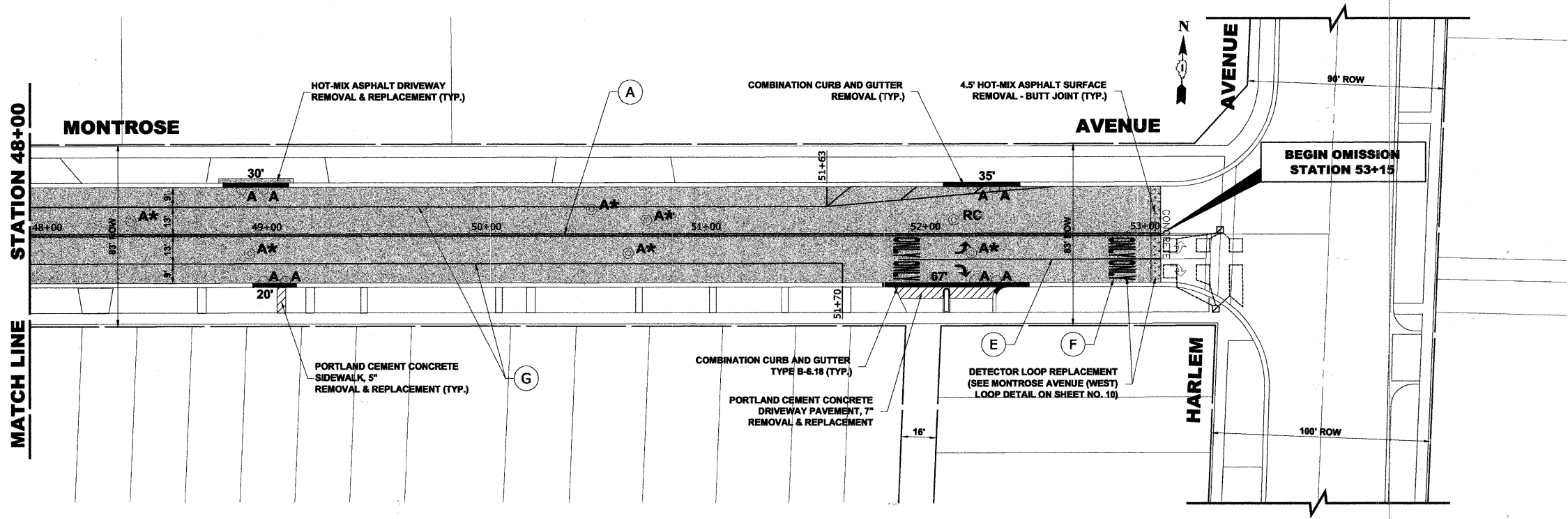
SHEET NO. 3 OF 5 SHEETS

STA. 32+00

TO STA. 48+00

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	7
FED. ROAD DIST. NO. 1   ILLINOIS				CONTRACT NO. 63582
FED. AID PROJECT				---





**NOTE:**

\* DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

**(A)** DETECTABLE WARNING

**PAVEMENT MARKINGS LEGEND**

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 4", DOUBLE @ 11" C-C, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE, 12", CROSS WALK @ 36" C-C, WHITE	(B)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", TURN LANE, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PARKING LINE, WHITE	(G)

DESIGNED	---	REVISED	---
DRAWN	---	REVISED	---
CHECKED	---	REVISED	---
DATE	03/25/11	REVISED	---

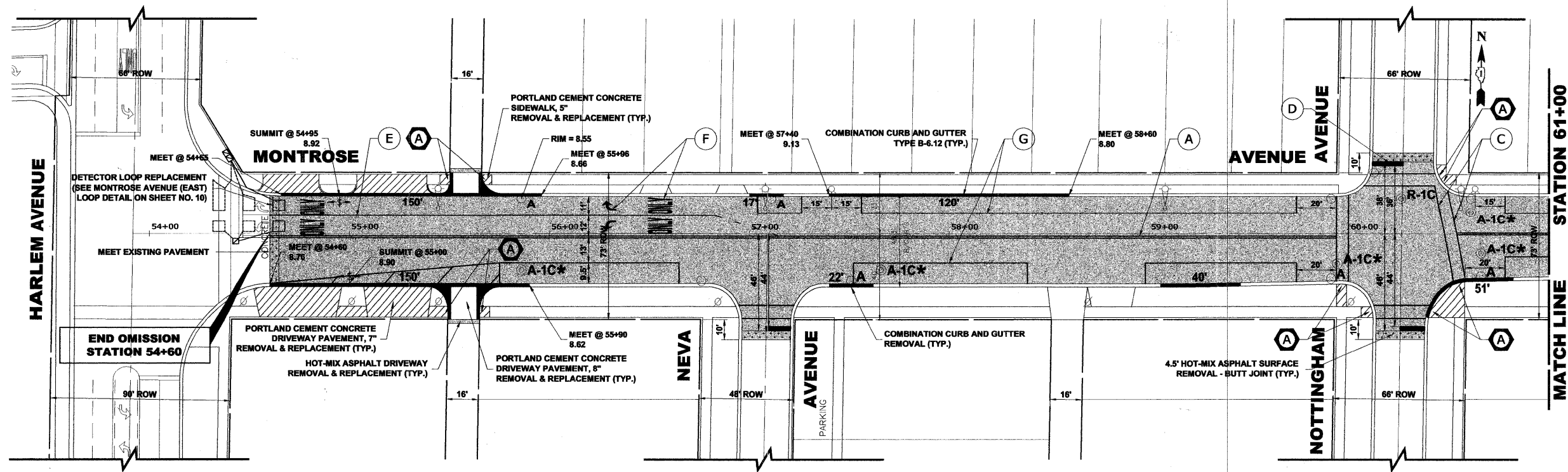
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**MONTROSE AVENUE PAVING PLAN**

SCALE: 1" = 30'    SHEET NO. 4 OF 5 SHEETS    STA. 48+00    TO STA. 53+15

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	8
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO.	63582
			FED. AID PROJECT	---





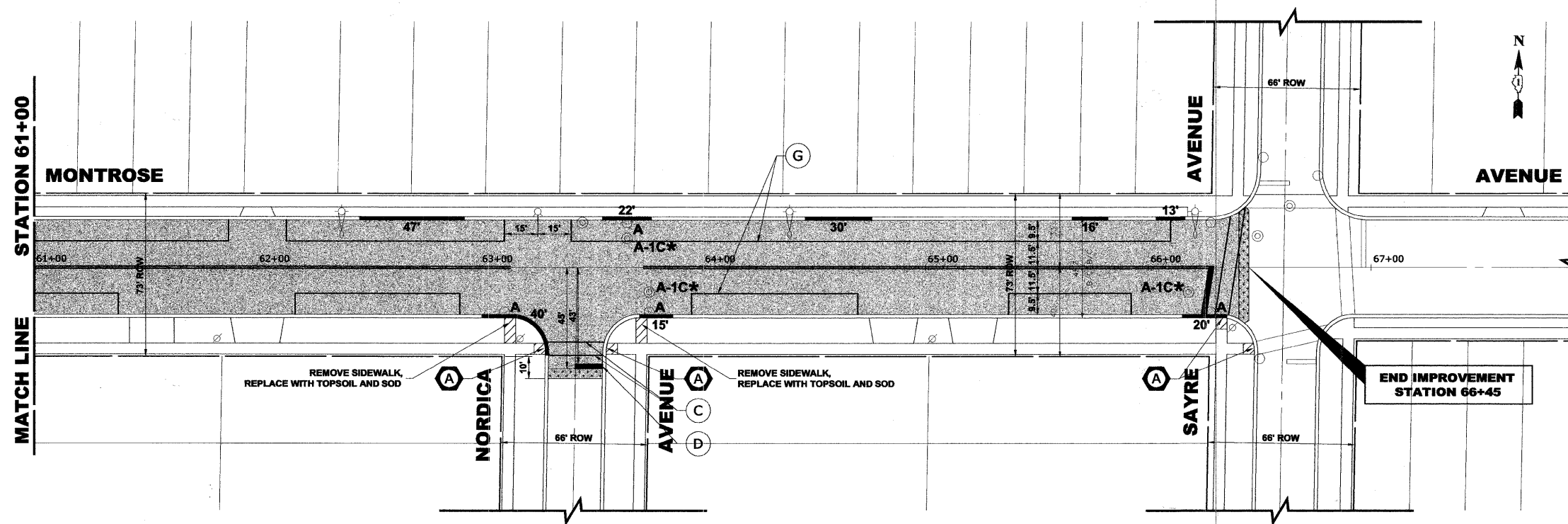
**NOTE:**

\* DENOTES FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

**(A)** DETECTABLE WARNING

**PAVEMENT MARKINGS LEGEND**

ITEM DESCRIPTION	SYMBOL
THERMOPLASTIC PAVEMENT MARKING LINE, 4", DOUBLE @ 11" C-C, YELLOW	(A)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", CROSS WALK, WHITE	(C)
THERMOPLASTIC PAVEMENT MARKING LINE, 24", STOP BAR, WHITE	(D)
THERMOPLASTIC PAVEMENT MARKING LINE, 6", TURN LANE, WHITE	(E)
THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, WHITE	(F)
THERMOPLASTIC PAVEMENT MARKING LINE, 4", PARKING LINE, WHITE	(G)



Drawing file: W:\Projects\61210060 - Montrose Ave LAPP (Harlem to Sayre)\MontroseAveLAPP.dwg Apr 05, 2011 - 9:07am



◆ Civil Engineers  
◆ Municipal Consultants  
◆ Established 1911

9933 Roseville Road  
Westchester, Illinois 60154-2788  
Phone: 708/965-4380  
Fax: 708/965-1212

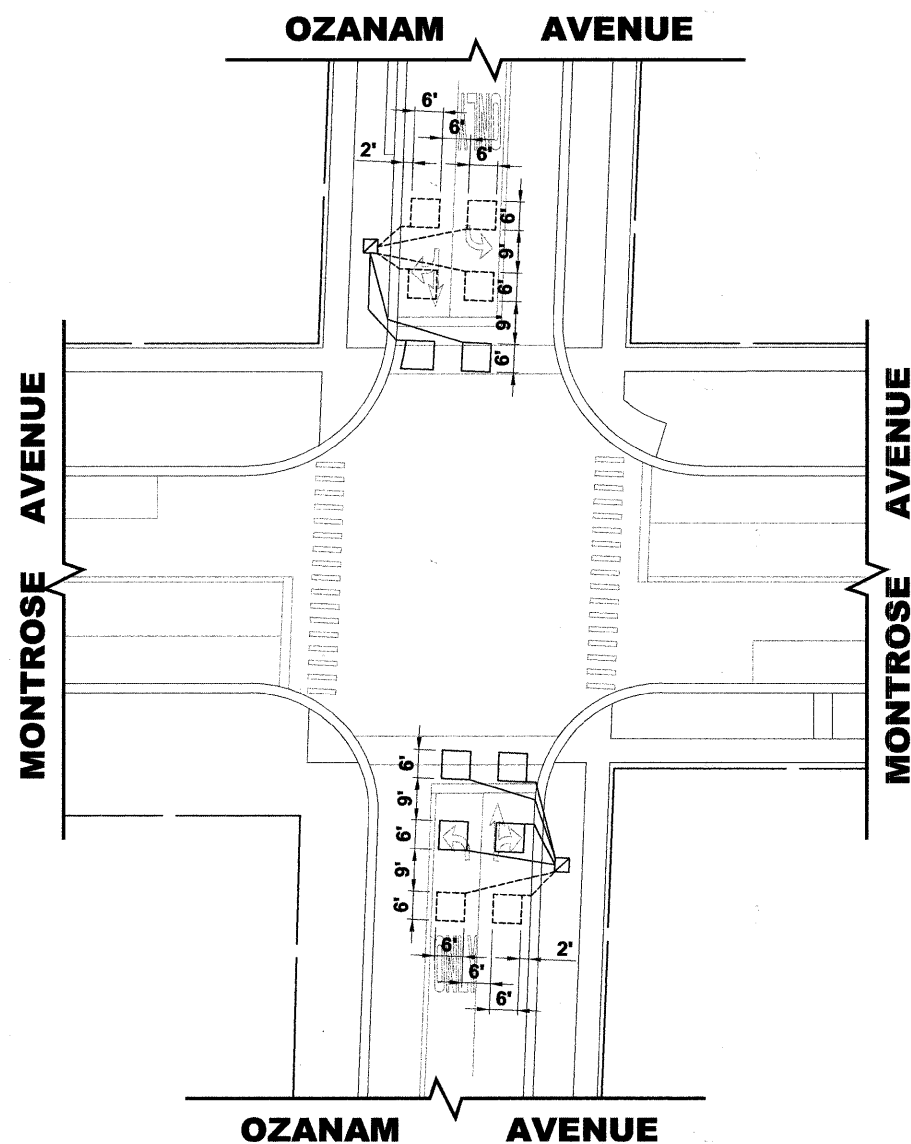
DESIGNED -	---	REVISED -	---
DRAWN -	---	REVISED -	---
CHECKED -	---	REVISED -	---
DATE -	03/25/11	REVISED -	---

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

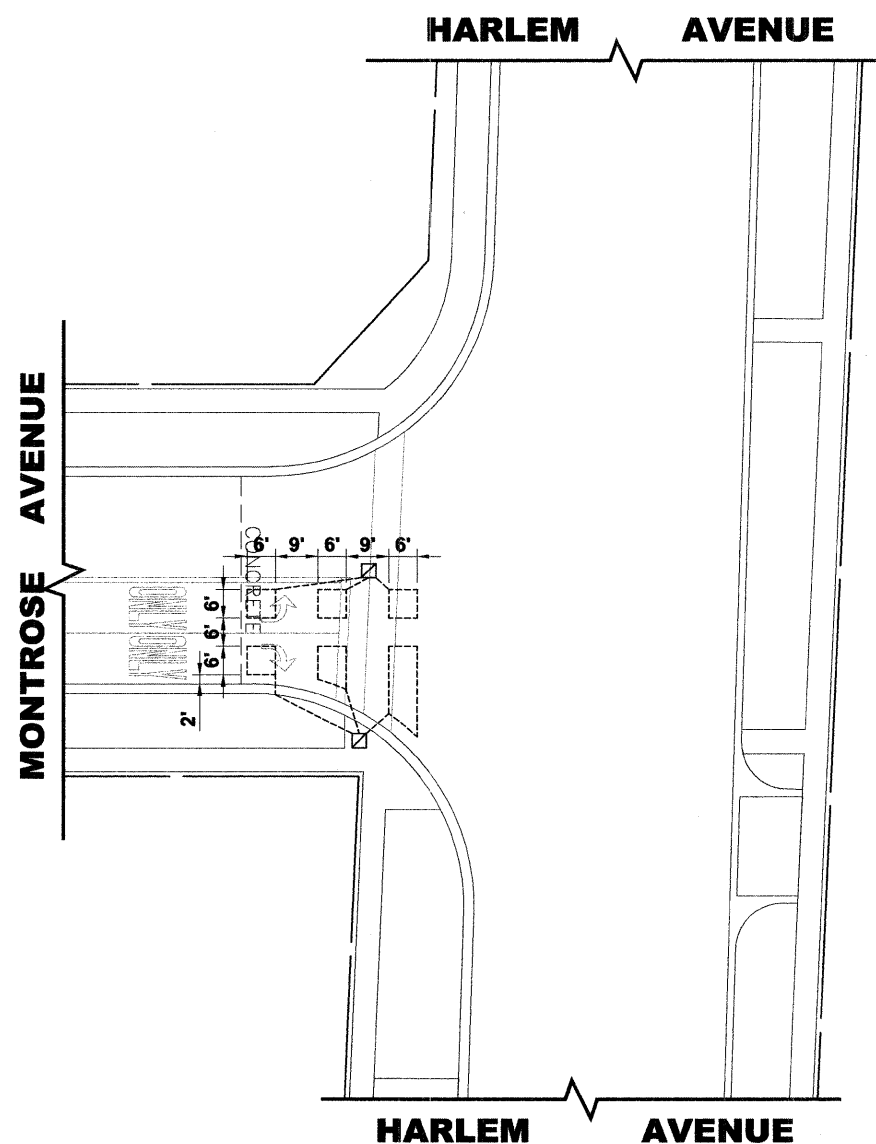
**MONTROSE AVENUE PAVING PLAN**

SCALE: 1" = 30' SHEET NO. 5 OF 5 SHEETS STA. 54+00 TO STA. 66+45

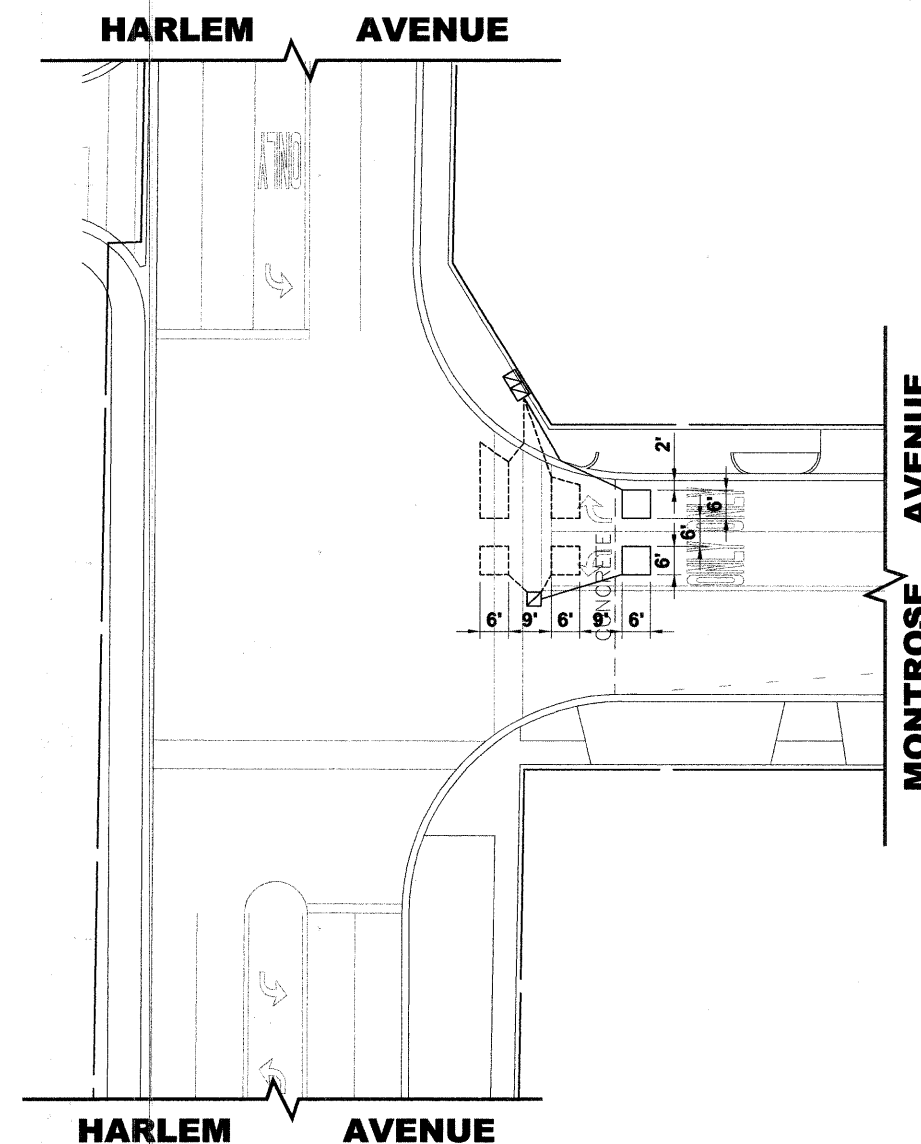
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	9
CONTRACT NO. 63582				FED. AID PROJECT
FED. ROAD DIST. NO. 1 ILLINOIS				



**OZANAM AVENUE LOOP DETAIL**



**MONTROSE AVENUE (WEST) LOOP DETAIL**



**MONTROSE AVENUE (EAST) LOOP DETAIL**

Drawing file: W:\Projects\61210060 - Montrose Ave LAPP (Harlem to Ogden)\MontroseAveLAPP.dwg Mar 30, 2011 - 9:33am



◆ Civil Engineers  
◆ Municipal Consultants  
◆ Established 1911

9933 Roosevelt Road  
Westchester, Illinois 60154-2780  
Phone: 708/865-4200  
Fax: 708/865-1212

DESIGNED	---	REVISED	---
DRAWN	---	REVISED	---
CHECKED	---	REVISED	---
DATE	03/25/11	REVISED	---

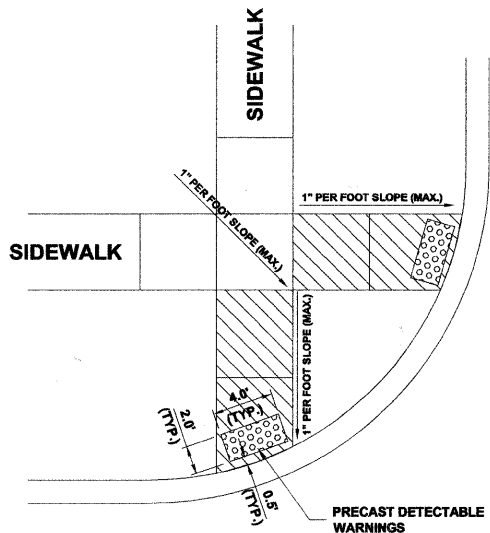
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP DETAILS**

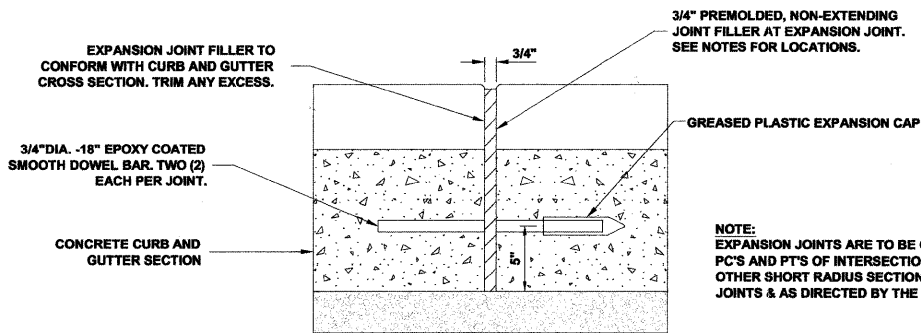
SCALE: 1" = 20' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 2729	SECTION 10-00060-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 10
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63582	
FED. AID PROJECT				

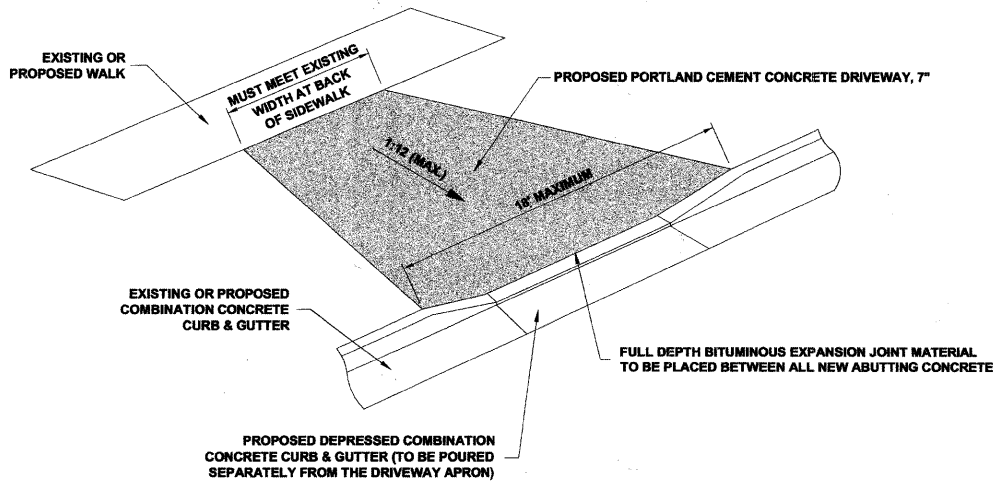
Drawing file: W:\Projects\51210060 - Montrose Ave LAPP (Hiram to Sore)\Detail-MontroseAve.dwg Mar 30, 2011 9:32am



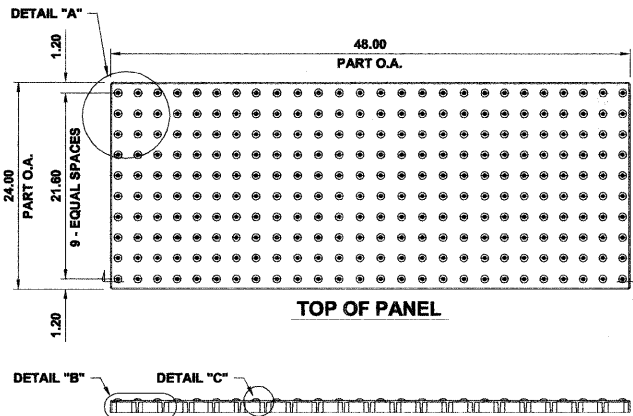
A.D.A. RAMP



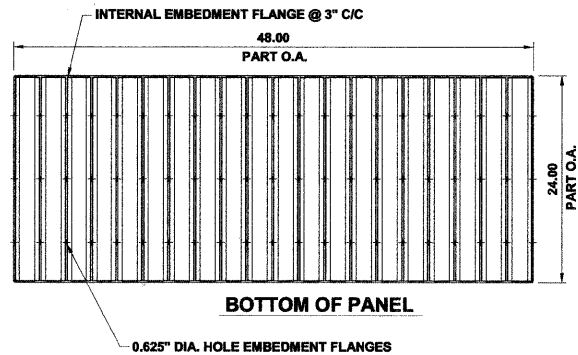
TYPICAL CURB AND GUTTER EXPANSION JOINT



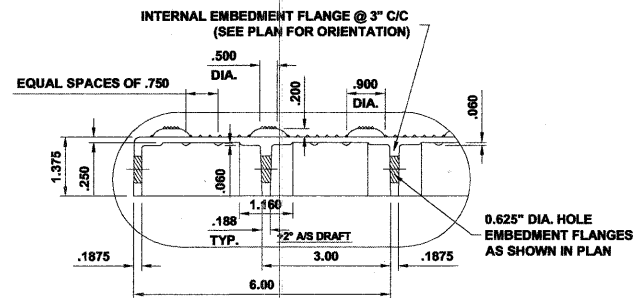
DRIVEWAY



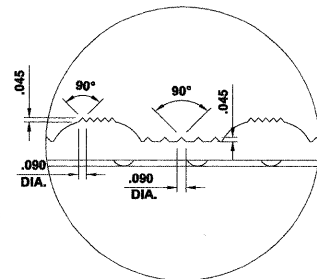
DETAIL "A"



DETAIL "C"

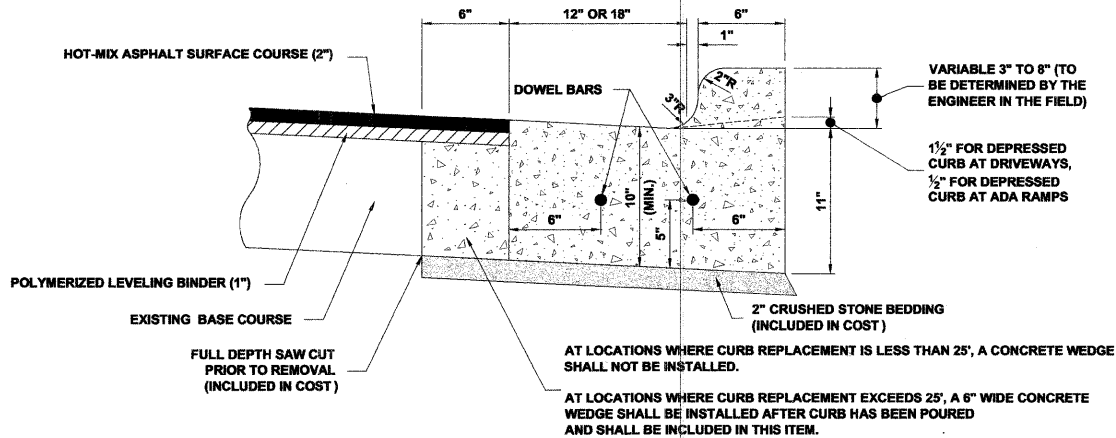


DETAIL "B"



NOTE: COMPOSITE PAVER TILE USED SHALL BE AS MANUFACTURED BY ADA SOLUTIONS, INC., OR AS APPROVED BY ENGINEER.

CAST-IN-PLACE COMPOSITE PAVER TILE FOR DETECTABLE WARNING



NOTES:

1. PROVIDE 2 - 3/4" DIAMETER, 18" LONG EPOXY COATED SMOOTH BARS WITH PLASTIC EXPANSION CAPS AT EACH EXPANSION JOINT.
2. CONTRACTION JOINT - 2" DEEP CONTRACTION JOINTS SHALL BE SAWS AT EQUAL SPACES (NOT EXCEEDING 15 FEET) BETWEEN NORMAL EXPANSION JOINTS, IN THE UPPER 1/2 OF CURB & GUTTERS WITHIN 24 HOURS OF PLACEMENT.
3. A LEAN CONCRETE WEDGE IS TO BE POURED IN THE SPACE BETWEEN THE NEW CURB & THE EXISTING PAVEMENT AFTER THE CURB HAS REACHED ITS INITIAL SET.

COMBINATION CONCRETE CURB & GUTTER



Civil Engineers  
Municipal Consultants  
Established 1911

9333 Riverdale Road  
Winchester, Illinois 60154-2780  
Phone: 708/965-4380  
Fax: 708/965-1312

DESIGNED	---	REVISED	---
DRAWN	---	REVISED	---
CHECKED	---	REVISED	---
DATE	03/25/11	REVISED	---

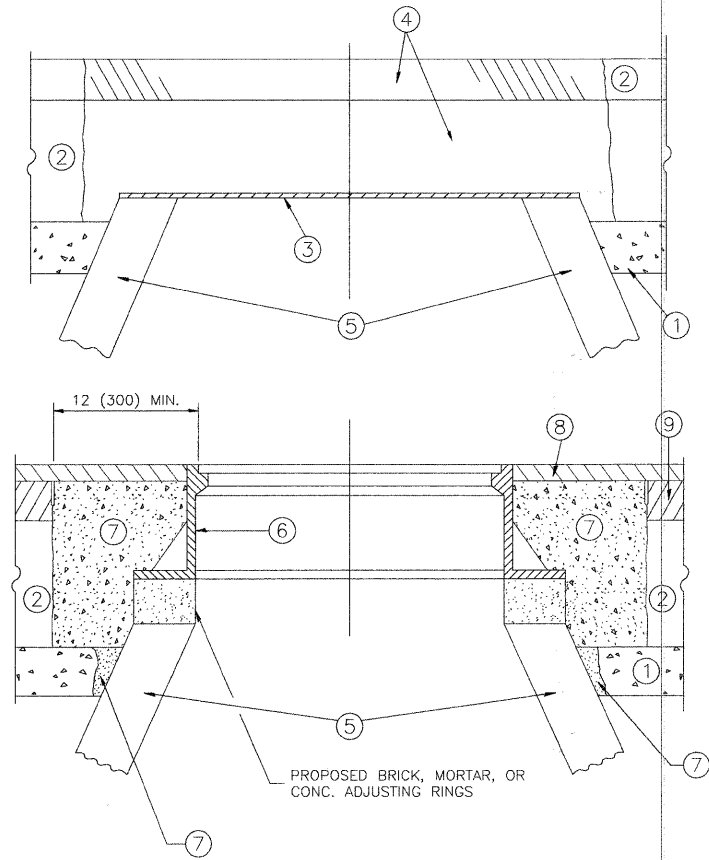
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	11
FED. ROAD DIST. NO. 1   ILLINOIS		CONTRACT NO. 63582 FED. AID PROJECT		

Drawing file: W:\Projects\61210060 - Montrose Ave LAPP (Harlem to Sayre)\Workings-MontroseAve.dwg      Mar 24, 2011 - 1:55pm



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- |                                              |                               |
|----------------------------------------------|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-1* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME =	DESIGNED - R. SHAH	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - R. WIEDEMAN 05-14-04
PLOT SCALE =	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE =	DATE - 10-25-94	REVISED - R. BORO 03-09-11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING

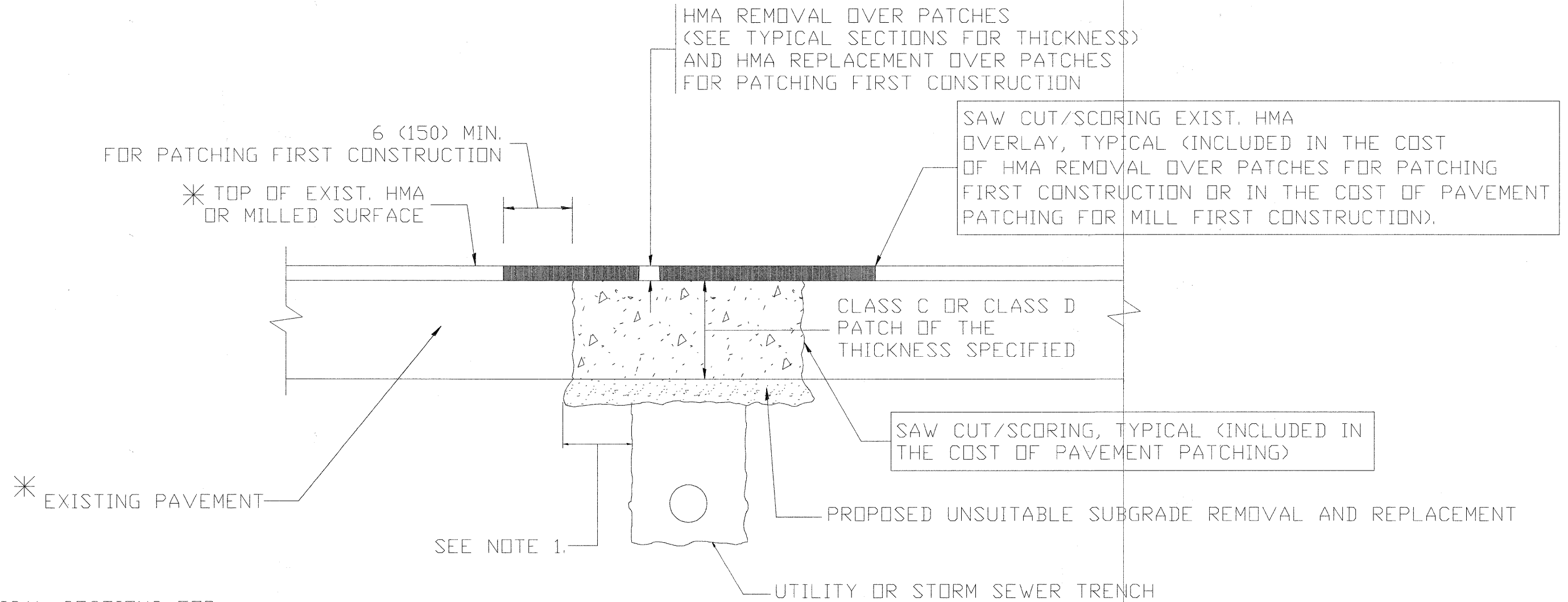
SCALE: NONE      SHEET NO. 1 OF 1 SHEETS      STA.      TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	12
BD600-03 (BD-8)		CONTRACT NO. 63582		
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT —		

E.H.E. PROJECT NO. 612-10-06001



Drawing file: W:\Projects\612\0060 - Montrose Ave LAPP (Harden to Spire)\Workings-MontroseAve.dwg  
Mar 24, 2011 - 1:55pm



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

#### NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

#### SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

#### SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME =	DESIGNED - R.SHAH	REVISED - A. ABBAS 04-27-98
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE =	CHECKED -	REVISED - R. BORO 09-04-07
PLOT DATE =	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

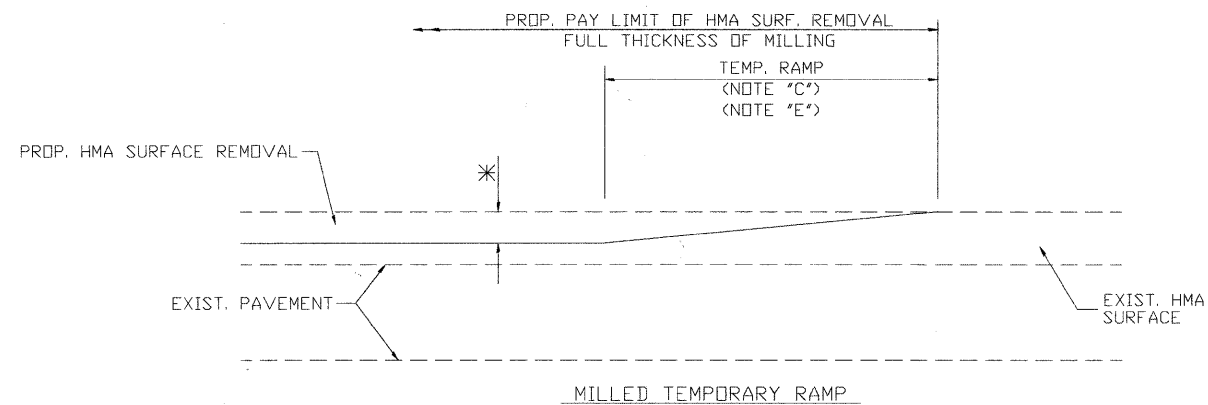
PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	13
BD400-04 (BD-22)		CONTRACT NO. 63582		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT —				

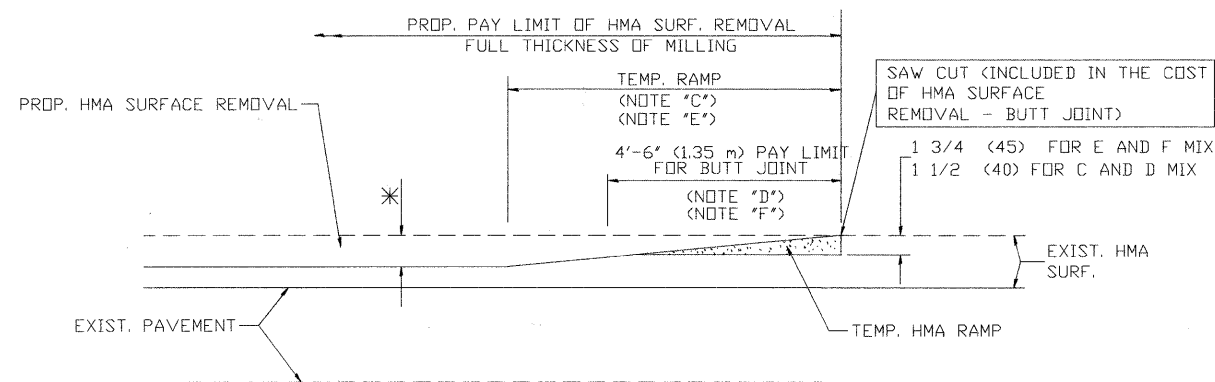
E.H.E. PROJECT NO. 612-10-06001

Drawing file: W:\Projects\61210060 - Montrose Ave LAPP (Harlem to Spire)\Workings-MontroseAve.dwg Mar 30, 2011 - 10:04am



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

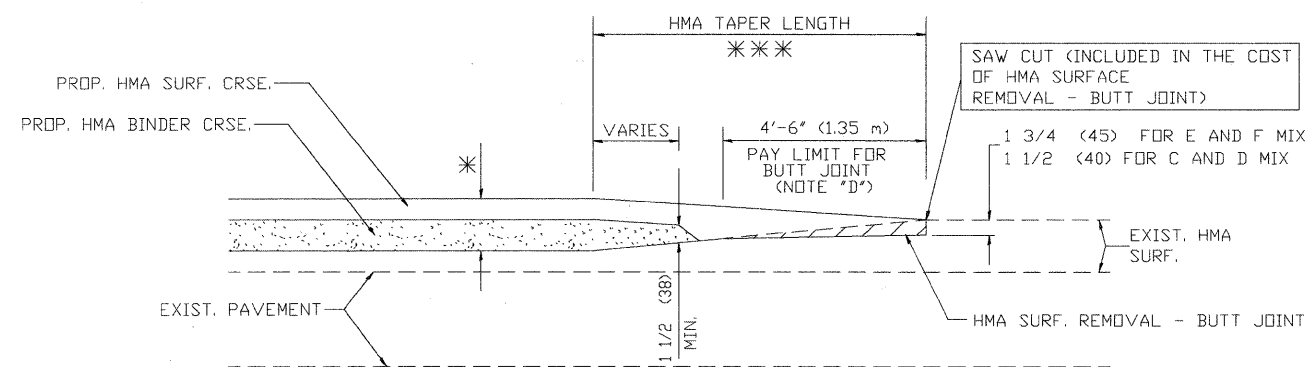
OPTION 1



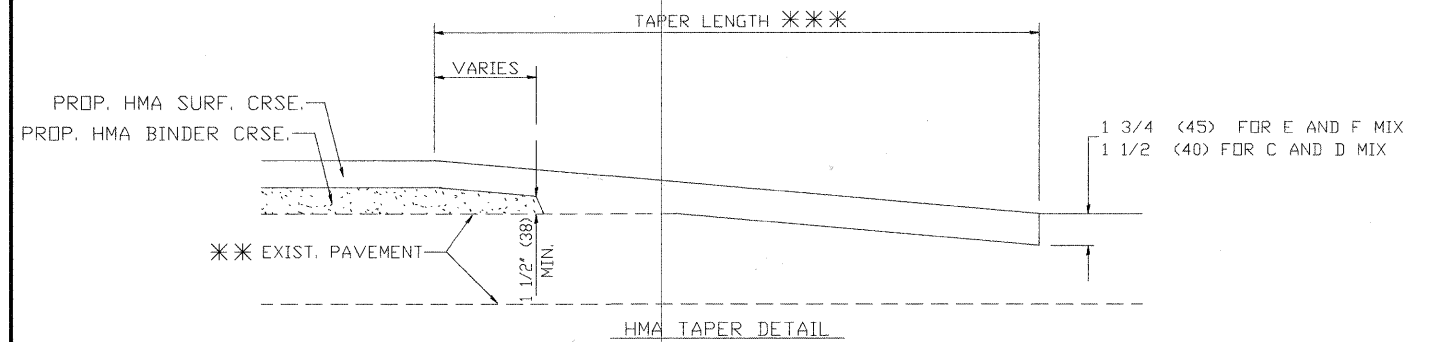
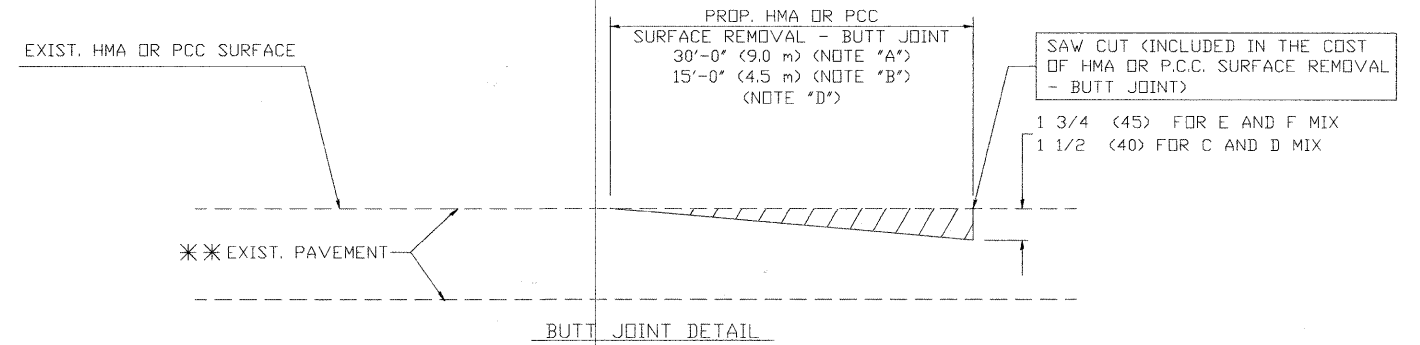
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.  
B: MINOR SIDE ROADS.  
C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.  
D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.  
E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT  
G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE 'A')  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE 'B')

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME =	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
DRAWN -	REVISOR - A. ABBAS 03-21-97	
PLOT SCALE =	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE =	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

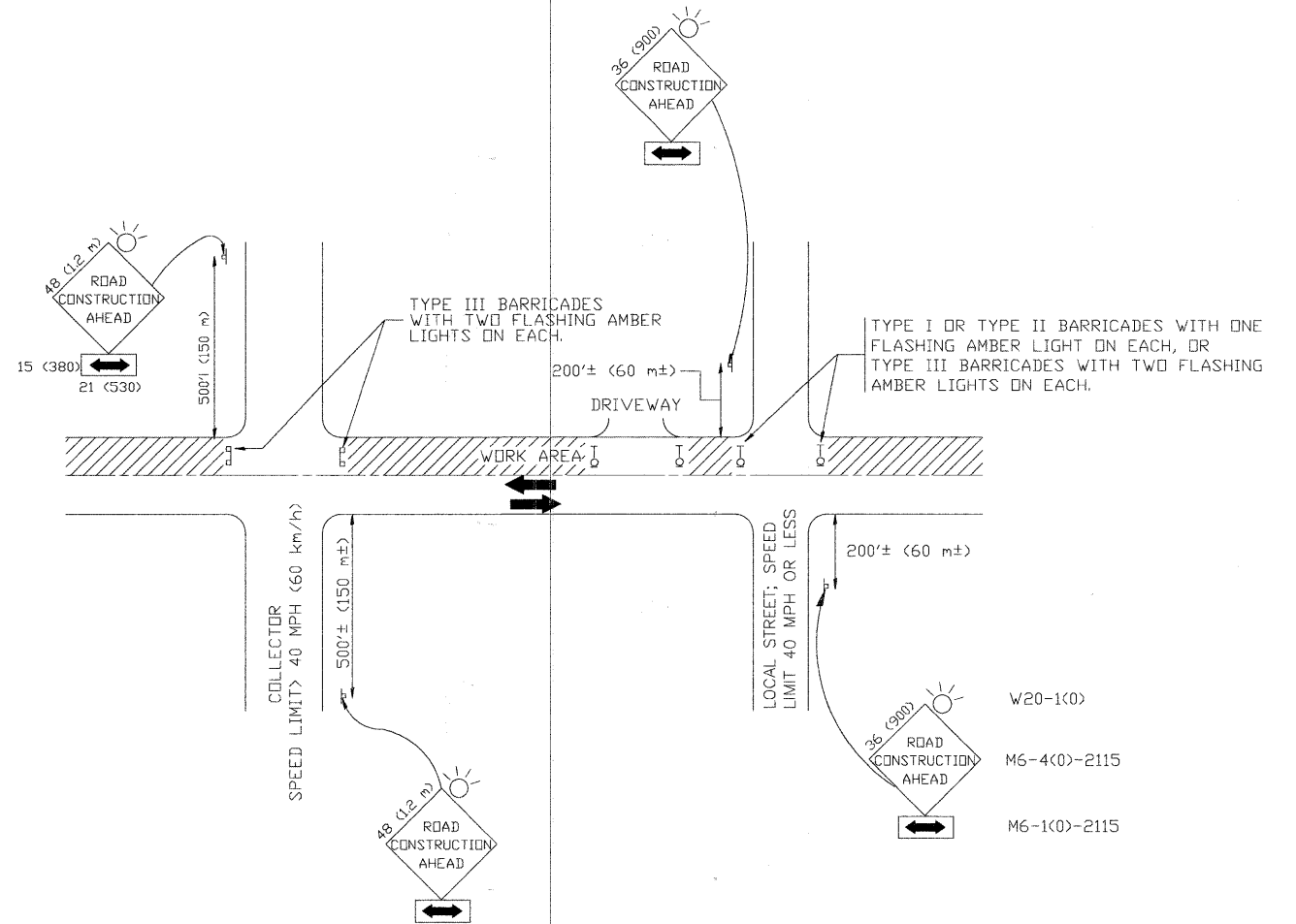
BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	14
BD400-05 (BD-32)		CONTRACT NO. 63582		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT —				

E.H.E. PROJECT NO. 612-10-06001

Drawing file: W:\Projects\61210060 - Montrose Ave LAPP (Hickory to Sayre)\Workings-Montroseave.dwg Mar 24, 2011 - 1:55pm



## TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

### NOTES:

#### A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

- a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

- a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (12 m x 12 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

#### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

USER NAME =	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
	DRAWN -	REVISED - A. HOUSEH 03-06-96
PLOT SCALE =	CHECKED -	REVISED - A. HOUSEH 10-15-96
PLOT DATE =	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

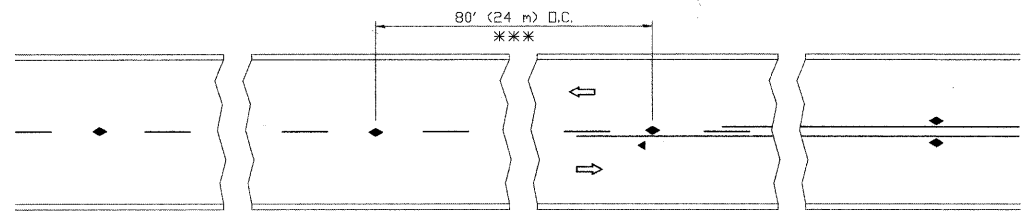
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

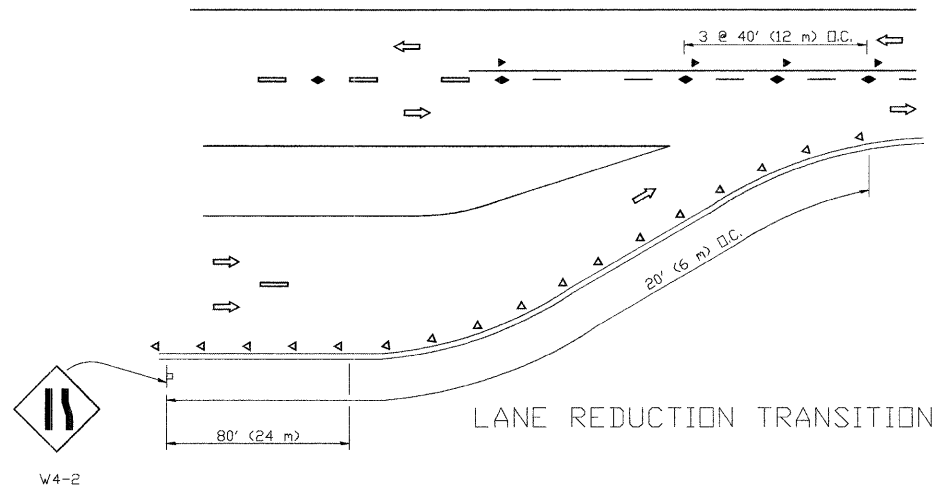
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	15
TC-10		CONTRACT NO. 63582		
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT ---		

E.H.E. PROJECT NO. 612-10-06001

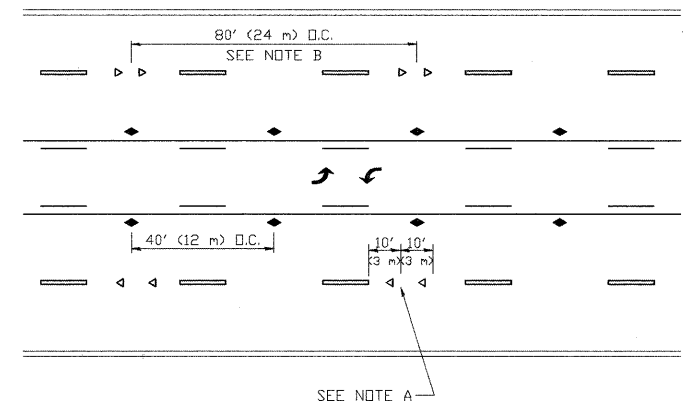


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

## TWO-LANE/TWO-WAY

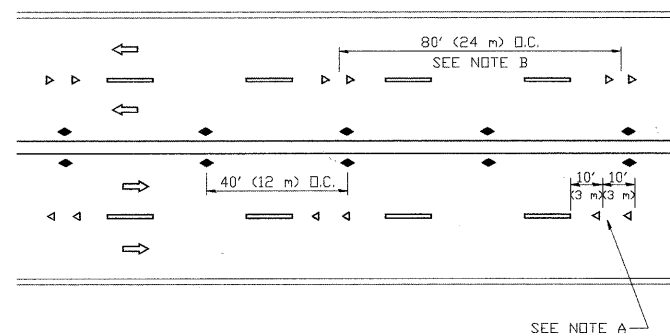


## LANE REDUCTION TRANSITION



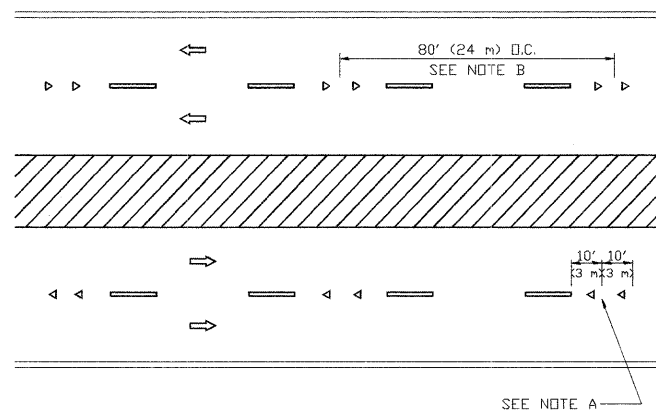
SEE NOTE A

## TWO-WAY LEFT TURN



SEE NOTE A

## MULTI-LANE/UNDIVIDED



SEE NOTE A

## GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

## LANE MARKER NOTES

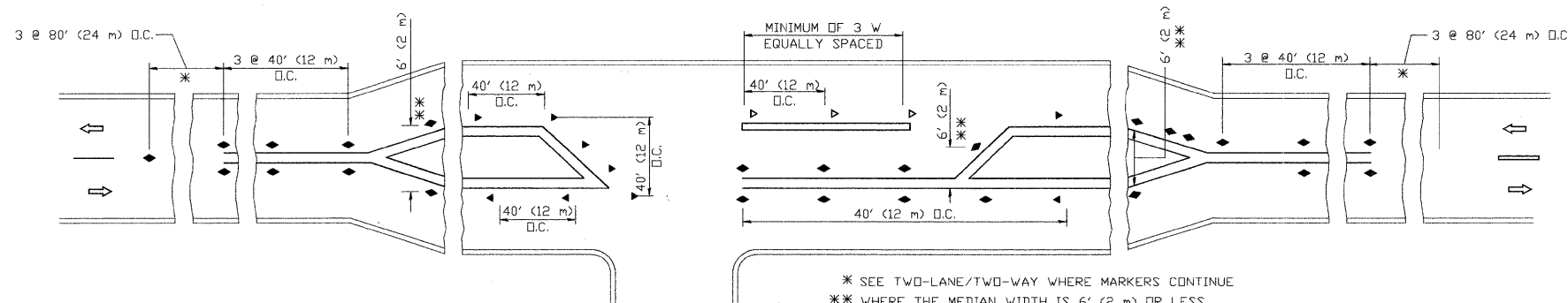
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

## SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ▲ ONE-WAY AMBER MARKER
- ▲ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

## DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
\*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS  
USE TWO-WAY MARKERS.

## LEFT TURN

All dimensions are in millimeters (inches)  
unless otherwise shown.

USER NAME =	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
PLOT SCALE =	DRAWN -	REVISED - T. RAMMACHER 03-12-99
PLOT DATE =	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

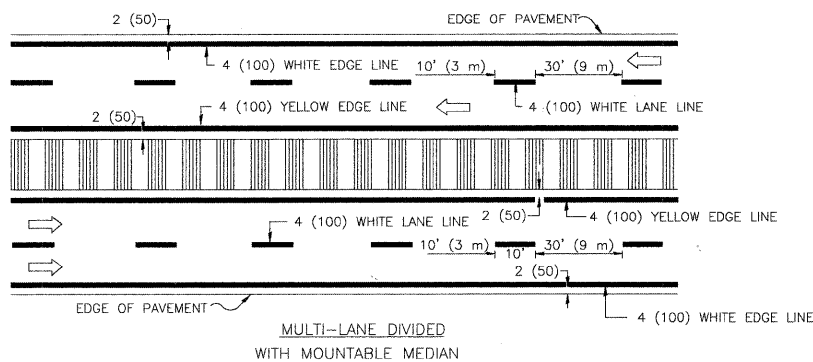
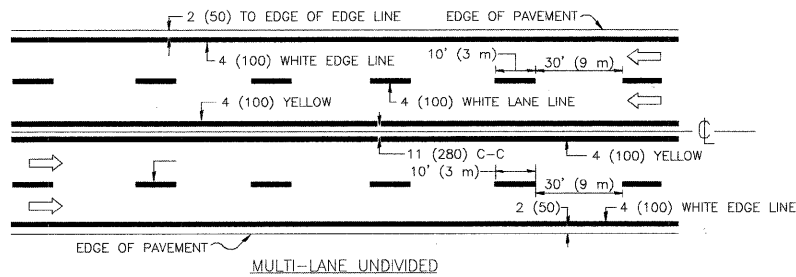
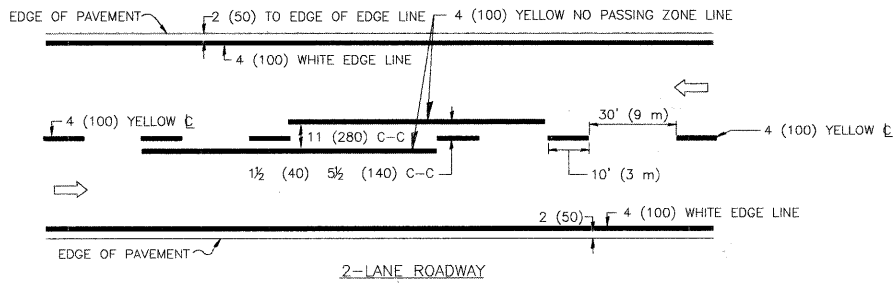
TYPICAL APPLICATIONS  
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	16
TC-11		CONTRACT NO. 63582		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT —				

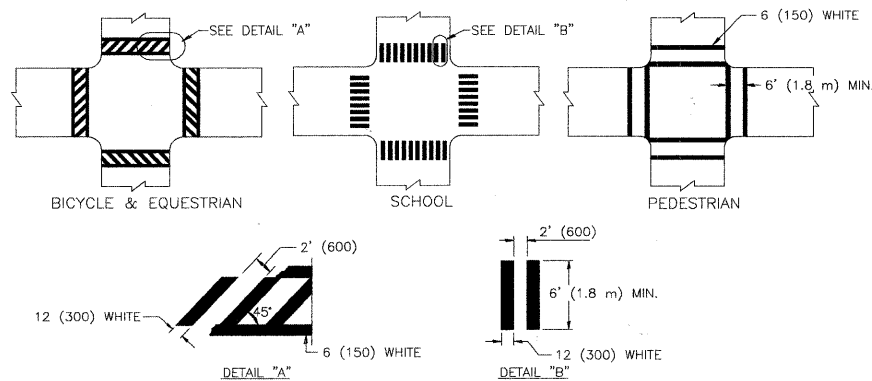
E.H.E. PROJECT NO. 612-10-06001



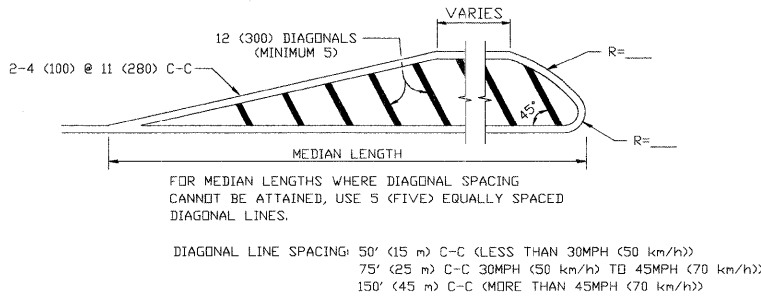
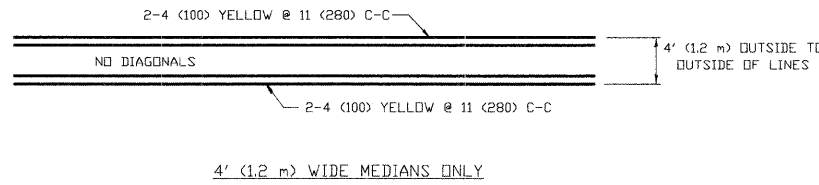


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

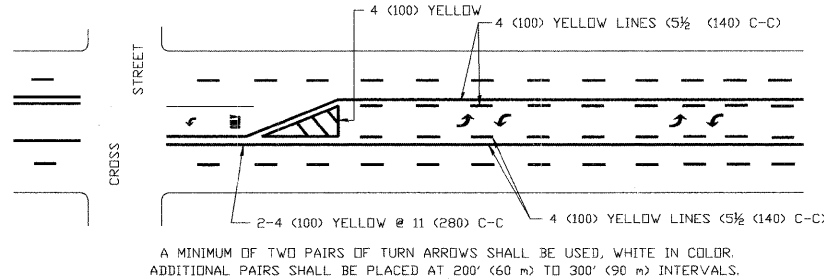
TYPICAL LANE AND EDGE LINE MARKING



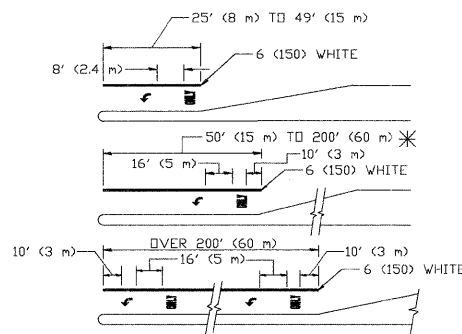
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



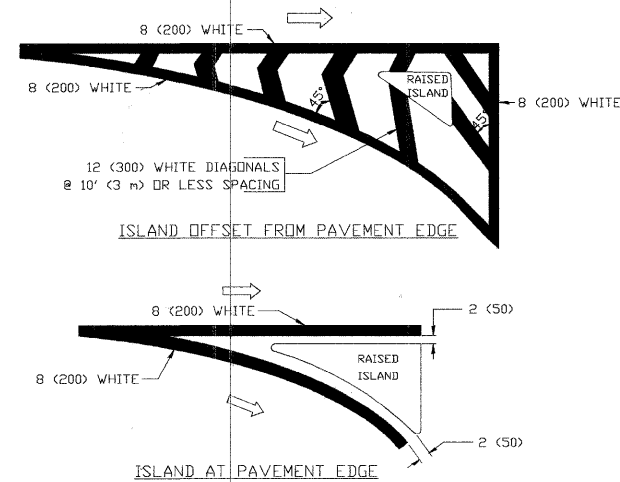
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16' (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = drivakogn	DESIGNED = EVERS	REVISED = T.RAMMACHER 10-27-94
PLOT SCALE = 50,000' / IN.	DRAWN =	REVISED = C. JUCIUS 09-09-09
PLOT DATE = 9/9/2009	CHECKED =	REVISED =
	DATE = 03-19-90	REVISED =

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2729	10-00060-00-RS	COOK	17	17
TC-13		CONTRACT NO. 63582		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				