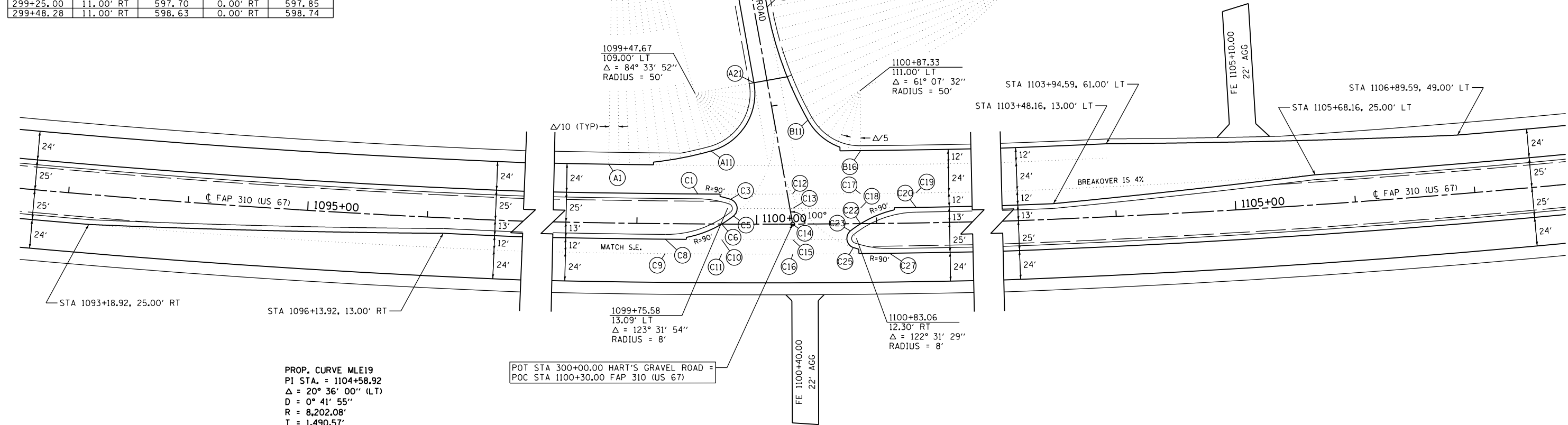


8:46:54 AM

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	FAP 310 (US 67)		HART'S GRAVEL ROAD		ELEVATION
A1	1098+75.66	49.00' LT	299+23.68	142.32' RT	596.14
A2	1098+84.36	49.12' LT	299+25.22	133.81' RT	596.31
A3	1098+93.05	49.48' LT	299+26.52	125.26' RT	596.47
A4	1099+01.72	50.08' LT	299+27.56	116.68' RT	596.63
A5	1099+10.38	50.92' LT	299+28.36	108.07' RT	596.78
A6	1099+19.01	52.00' LT	299+28.91	99.45' RT	596.92
A7	1099+27.61	53.31' LT	299+29.21	90.81' RT	597.05
A8	1099+36.18	54.86' LT	299+29.26	82.16' RT	597.18
A9	1099+44.69	56.65' LT	299+29.07	73.52' RT	597.30
A10	1099+53.16	58.67' LT	299+28.62	64.88' RT	597.38
A11	1099+61.56	60.93' LT	299+27.93	56.27' RT	597.41
A12	1099+68.54	63.46' LT	299+26.69	49.00' RT	597.40
A13	1099+75.07	66.98' LT	299+24.40	41.99' RT	597.34
A14	1099+81.01	71.41' LT	299+21.10	35.40' RT	597.26
A15	1099+86.24	76.66' LT	299+16.87	29.36' RT	597.13
A16	1099+90.64	82.61' LT	299+11.79	24.01' RT	596.97
A17	1099+94.12	89.13' LT	299+05.99	19.46' RT	596.77
A18	1099+96.59	96.09' LT	298+99.57	15.82' RT	596.53
A19	1099+98.00	103.33' LT	298+92.70	13.16' RT	596.26
A20	1099+98.32	110.69' LT	298+85.50	11.54' RT	595.94
A21	1099+97.54	118.02' LT	298+78.15	11.00' RT	595.60

HART'S GRAVEL ROAD C & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
298+78.15	11.00' RT	595.60	0.00' RT	595.82
299+00.00	11.00' RT	596.64	0.00' RT	596.83
299+25.00	11.00' RT	597.70	0.00' RT	597.85
299+48.28	11.00' RT	598.63	0.00' RT	598.74



PROP. CURVE C25  
 PI STA. = 296+27.63  
 $\Delta = 19^\circ 20' 54''$  (RT)  
 $D = 10^\circ 08' 27''$   
 $R = 565.00'$   
 $T = 96.32'$   
 $L = 190.80'$   
 $E = 8.15'$   
 $e = 0.040$   
 P.C. STA. = 295+31.32  
 P.T. STA. = 297+22.12  
 S.E. TRANSITION  
 STA 294+43.00 TO STA 295+57.00  
 STA 296+97.00 TO STA 298+11.00

	FAP 310 (US 67)		HART'S GRAVEL ROAD		ELEVATION
B1	1100+08.73	181.29' LT	298+17.83	11.00' LT	592.29
B2	1100+10.71	171.39' LT	298+27.92	11.17' LT	592.89
B3	1100+13.03	161.56' LT	298+37.99	11.68' LT	593.46
B4	1100+15.68	151.81' LT	298+48.04	12.53' LT	594.01
B5	1100+18.66	142.16' LT	298+58.06	13.71' LT	594.54
B6	1100+21.95	132.61' LT	298+68.03	15.23' LT	595.05
B7	1100+25.56	123.17' LT	298+77.94	17.09' LT	595.55
B8	1100+29.49	113.85' LT	298+87.79	19.27' LT	596.05
B9	1100+33.72	104.67' LT	298+97.56	21.79' LT	596.55
B10	1100+38.25	95.63' LT	299+07.23	24.63' LT	597.05
B11	1100+43.07	86.75' LT	299+16.81	27.80' LT	597.55
B12	1100+49.28	78.05' LT	299+26.43	32.36' LT	598.08
B13	1100+57.19	70.85' LT	299+34.87	38.86' LT	598.58
B14	1100+66.46	65.46' LT	299+41.73	47.00' LT	599.01
B15	1100+76.65	62.13' LT	299+46.72	56.40' LT	599.35
B16	1100+87.32	61.00' LT	299+49.61	66.65' LT	599.61

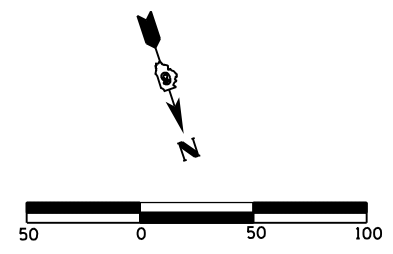
HART'S GRAVEL ROAD C & EOP ELEVATIONS				
STATION	OFFSET	ELEVATION	OFFSET	ELEVATION
298+78.15	11.00' LT	595.60	0.00' LT	595.82
299+00.00	11.00' LT	596.73	0.00' LT	596.83
299+25.00	11.00' LT	597.85	0.00' LT	597.85
299+52.18	11.00' LT	599.01	0.00' LT	N/A

PROP. CURVE MLE19  
 PI STA. = 1104+58.92  
 $\Delta = 20^\circ 36' 00''$  (LT)  
 $D = 0^\circ 41' 55''$   
 $R = 8,202.08'$   
 $T = 1,490.57'$   
 $L = 2,948.96'$   
 $E = 134.34'$   
 $e = 0.025$   
 P.C. STA. = 1089+68.35  
 P.T. STA. = 1119+17.31  
 S.E. TRANSITION  
 STA 1088+44.00 TO STA 1090+06.00  
 STA 1118+80.00 TO STA 1120+42.00

	FAP 310 (US 67)		ELEVATION
C1	1099+50.63	25.00' LT	598.17
C2	1099+64.50	23.92' LT	598.40
C3	1099+78.03	20.70' LT	598.61
C4	1099+83.59	13.00' LT	598.63
C5	1099+80.57	6.84' LT	598.50
C6	1099+70.73	0.00' RT	598.18
C7	1099+53.91	7.90' RT	597.92
C8	1099+23.92	13.00' RT	597.37
C9	1099+23.92	25.00' RT	597.67
C10	1099+70.73	13.00' RT	598.36
C11	1099+70.73	25.00' RT	598.53
C12	1100+30.00	25.00' LT	599.57
C13	1100+30.00	13.00' LT	599.57
C14	1100+30.00	0.00' RT	599.57
C15	1100+30.00	13.00' RT	599.57
C16	1100+30.00	25.00' RT	599.57
C17	1100+86.79	25.00' LT	600.50
C18	1100+86.79	13.00' LT	600.40
C19	1101+33.16	25.00' LT	601.23
C20	1101+33.16	13.00' LT	601.05
C21	1101+04.03	8.11' LT	600.59
C22	1100+86.79	0.00' RT	600.30
C23	1100+78.12	6.01' RT	600.23
C24	1100+75.10	13.00' RT	600.24
C25	1100+80.41	19.85' RT	600.36
C26	1100+95.24	23.70' RT	600.63
C27	1101+10.50	25.00' RT	600.88

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
310	69-3(3HB)	MORGAN	793	225

STA. TO STA.  
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GEOMETRIC DETAILS  
 INTERSECTION OF FAP 310  
 AND HART'S GRAVEL ROAD  
 FAP 310 (US 67/IL 104)  
 SHEET 20 OF 23  
 DRAWN BY EBB  
 CHECKED BY  
 DATE 6/06

\$FILE\$