

KANKAKEE VALLEY AIRPORT AUTHORITY KANKAKEE, ILLINOIS

CONSTRUCTION PLANS FOR GREATER KANKAKEE AIRPORT

REHABILITATE TAXIWAY G; REHABILITATE SOUTHEAST QUADRANT T-HANGAR PAVEMENT - PHASE 1

ILLINOIS PROJECT: IKK-4078
A.I.P. PROJECT: 3-17-0057-B19

MAY 06, 2011

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10075-04
DAVID K. PEABODY
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STATE OF ILLINOIS
5-6-11
EXP. 11-30-11

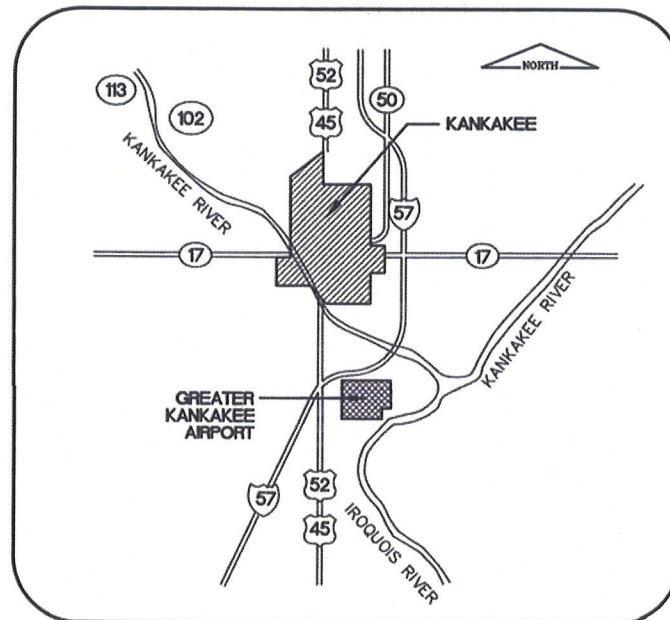
SUBMITTED BY D. Kyle Peabody, P.E.
D. KYLE PEABODY, P.E.

DATE 5-6 2011

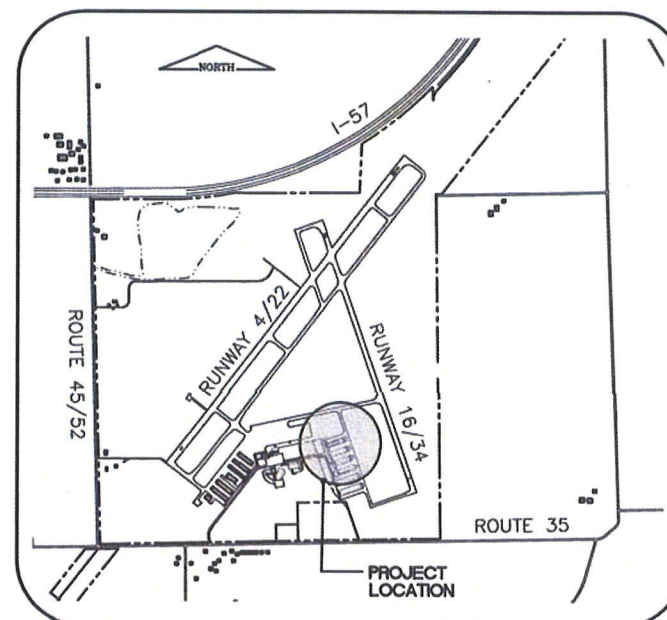
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KANKAKEE VALLEY AIRPORT AUTHORITY
GREATER KANKAKEE AIRPORT

SECTION: 21 COUNTY: KANKAKEE
RANGE: R 12 E TOWNSHIP: OTTO
TOWNSHIP: T 30 N



SITE PLAN



LOCATION MAP

DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY B
DESIGN AIRCRAFT GROUP II

KANKAKEE VALLEY
AIRPORT AUTHORITY

APPROVED [Signature]
AIRPORT MANAGER

DATE 5-6-11 2011

SUMMARY OF QUANTITIES

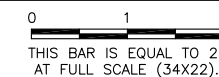
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
BASE BID (TXY G; T-HANGAR APRON H AND I; PART OF T-HANGAR APRON G AND H				
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	580	
AR152540	SOIL STABILIZATION FABRIC	SY	1,300	
AR156520	INLET PROTECTION	EACH	2	
AR208515	POROUS GRANULAR EMBANKMENT	CY	360	
AR209601	CR. AGG. BASE CSE.-VAR. DEPTH 1"-2"	SY	2,040	
AR209606	CRUSHED AGG. BASE COURSE - 6"	SY	1,300	
AR209800	AGG BASE REM.-VAR DEPTH 0"-2"	SY	5,140	
AR401610	BITUMINOUS SURFACE COURSE	TON	780	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	7,274	
AR403610	BITUMINOUS BASE COURSE	TON	1,215	
AR602510	BITUMINOUS PRIME COAT	GAL	2,176	
AR603510	BITUMINOUS TACK COAT	GAL	1,088	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	2,000	
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	LF	1,900	
AR800053	SOIL GUARD	SY	1,400	
AR901510	SEEDING	ACRE	0.30	
AR905530	TOPSOILING	SY	1,400	
ADDITIVE ALTERNATE NO. 1 (T-HANGAR APRON G-H)				
AS152410	UNCLASSIFIED EXCAVATION	CY	140	
AS152540	SOIL STABILIZATION FABRIC	SY	270	
AS208515	POROUS GRANULAR EMBANKMENT	CY	90	
AS209606	CRUSHED AGG. BASE COURSE - 6"	SY	270	
AS209800	AGG BASE REM.-VAR DEPTH 0"-2"	SY	1,840	
AS401610	BITUMINOUS SURFACE COURSE	TON	165	
AS401900	REMOVE BITUMINOUS PAVEMENT	SY	1,840	
AS403610	BITUMINOUS BASE COURSE	TON	275	
AS602510	BITUMINOUS PRIME COAT	GAL	550	
AS603510	BITUMINOUS TACK COAT	GAL	275	
AS620520	PAVEMENT MARKING - WATERBORNE	SF	500	

INDEX TO SHEETS

- COVER SHEET
- INDEX TO SHEETS / SUMMARY OF QUANTITIES
- SITE PLAN / PROJECT CONTROL PLAN
- SEQUENCE OF CONSTRUCTION PER AC 150/5370-2E (LATEST EDITION)
- SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS
- TYPICAL SECTIONS
- EXISTING CONDITIONS / PROPOSED REMOVALS
- LANDSCAPING & T-HANGAR PAVEMENT GRADING PLAN
- TAXIWAY G PLAN AND PROFILE / PAVEMENT MARKING PLAN
- MISCELLANEOUS DETAILS
- ENGINEERING INFORMATION

REVISIONS

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TAXIWAY G; REHABILITATE
 SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1**




**INDEX TO SHEETS /
 SUMMARY OF QUANTITIES**

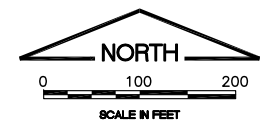
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JOB No:	10075-04
I.L. PROJECT: IKK-4078 A.I.P. PROJECT: 3-17-0057-B19 FINAL	
SHEET	2 OF 11 SHEETS

LEGEND

-  **BASE BID - TAXIWAY G PAVEMENT**
 NEW 2" BITUMINOUS SURFACE COURSE (401)
 NEW 3" BITUMINOUS BASE COURSE (403)
 EXISTING 3.5" (NOMINAL) HMA PAVEMENT TO BE REMOVED (401)
-  **BASE BID - SOUTHEAST QUADRANT T-HANGAR PAVEMENT**
 (PORTIONS OF HANGAR G-H PAVEMENT AND HANGAR H PAVEMENT)
 NEW 1.5" BITUMINOUS SURFACE COURSE (401)
 NEW 2.5" BITUMINOUS BASE COURSE (403)
 EXISTING 3.5" (NOMINAL) HMA PAVEMENT TO BE REMOVED (401)
-  **ADDITIVE ALTERNATE - SOUTHEAST QUADRANT T-HANGAR PAVEMENT**
 (PORTIONS OF HANGAR G-H PAVEMENT)
 NEW 1.5" BITUMINOUS SURFACE COURSE (401)
 NEW 2.5" BITUMINOUS BASE COURSE (403)
 EXISTING 3.5" (NOMINAL) HMA PAVEMENT TO BE REMOVED (401)



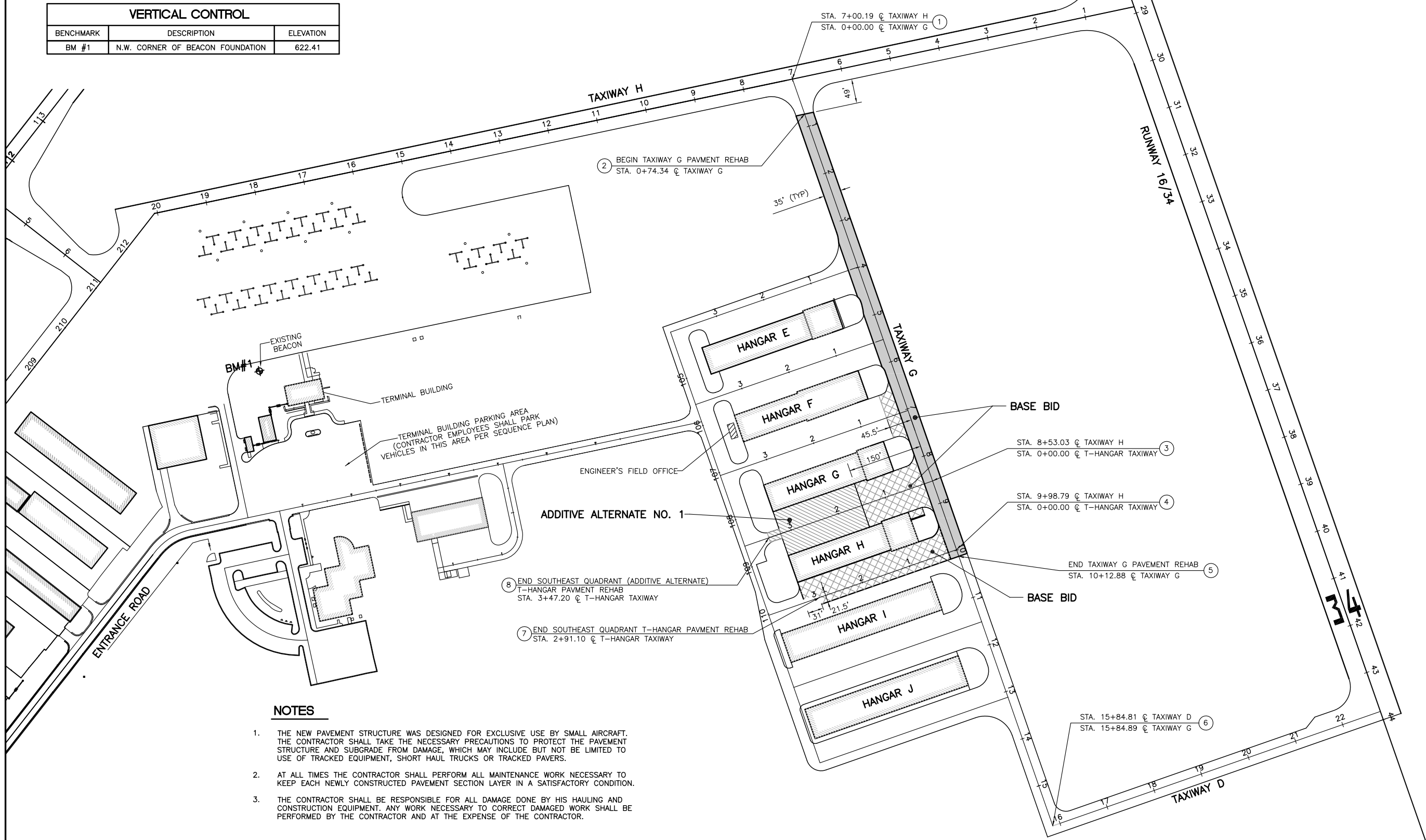
HORIZONTAL CONTROL			
POINT	DESCRIPTION	NORTHING	EASTING
1	TAXIWAY G - TAXIWAY H INTERSECTION	1603407.87	1118799.96
2	BEGIN PROPOSED IMPROVEMENTS - TAXIWAY G CENTERLINE STA 0+74.34	1603337.68	1118824.43
3	END PROPOSED IMPROVEMENTS - T-HANGAR PAVEMENT CENTERLINE STA 0+00.00	1602602.38	1119080.77
4	END PROPOSED IMPROVEMENTS - T-HANGAR PAVEMENT CENTERLINE STA 0+00.00	1602464.74	1119128.76
5	END PROPOSED IMPROVEMENTS - TAXIWAY G CENTERLINE STA 10+08.79	1602451.45	1119133.39
6	TAXIWAY G - TAXIWAY D INTERSECTION	1601911.32	1119321.69
7	END PROPOSED IMPROVEMENTS - T-HANGAR PAVEMENT CENTERLINE STA 3+22.38	1602368.72	1118853.96
8	END PROPOSED IMPROVEMENTS - T-HANGAR PAVEMENT CENTERLINE STA 3+22.41	1602488.56	1118752.76

ALL HORIZONTAL CONTROL POINTS SHALL BE VERIFIED BEFORE CONSTRUCTION BEGINS.

VERTICAL CONTROL		
BENCHMARK	DESCRIPTION	ELEVATION
BM #1	N.W. CORNER OF BEACON FOUNDATION	622.41

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



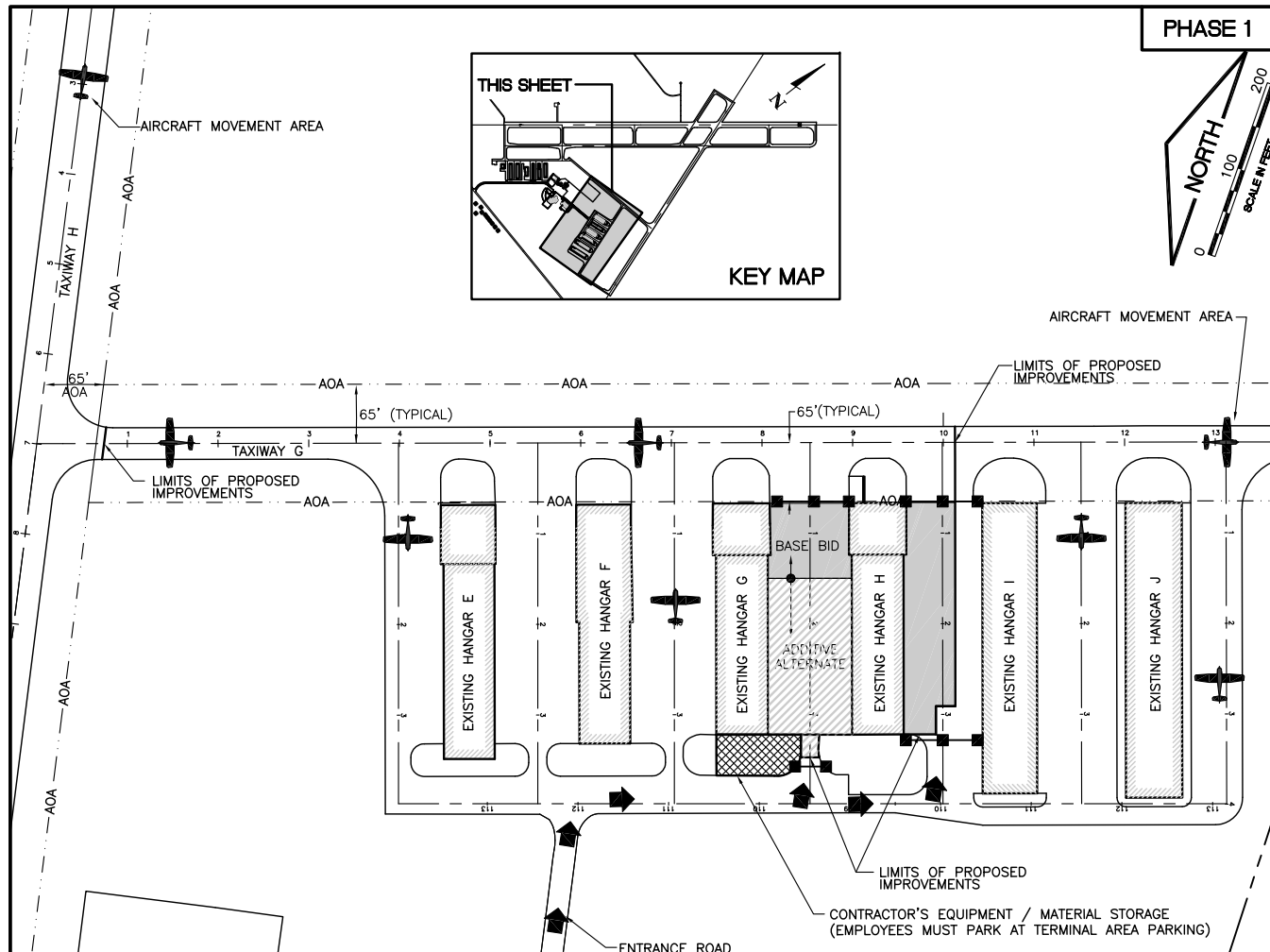
NOTES

- THE NEW PAVEMENT STRUCTURE WAS DESIGNED FOR EXCLUSIVE USE BY SMALL AIRCRAFT. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS.
- AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.

**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TAXIWAY G, REHABILITATE
 SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1
 SITE PLAN / PROJECT CONTROL PLAN**

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PHASE 1: SUGGESTED SEQUENCE OF CONSTRUCTION

- NOTIFY RESIDENT ENGINEER TWO (2) WEEKS PRIOR TO BEGINNING CONSTRUCTION SO THAT AIRCRAFT RELOCATION CAN BE COORDINATED WITH AIRPORT (BY OTHERS). CONTRACTOR SHALL SUBMIT CONSTRUCTION SCHEDULE DETAILING MAXIMUM AMOUNT OF TIME AIRCRAFT WILL BE DISPLACED FROM EXISTING HANGARS DURING ALL CONSTRUCTION PHASES FOR APPROVAL FROM THE AIRPORT.
- PLACE BARRICADES AS SHOWN.
- REMOVE EXISTING PAVEMENT PER PLAN.
- COMPLETE GRADING PER TYPICAL SECTION & PLAN AND PROFILE SHEET
- CONSTRUCT BITUMINOUS PAVEMENT.
- INSTALL PAVEMENT MARKINGS PER PLAN
- CLEAN PAVEMENTS AND REMOVE BARRICADES AND MISCELLANEOUS DEBRIS FROM CONSTRUCTION AREA.
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER SO THAT DISPLACED AIRCRAFT CAN BE RELOCATED BACK TO THEIR ORIGINAL STORAGE AREAS (BY OTHERS).

LEGEND

- WORK LIMITS
- EXISTING HANGAR/BUILDING
- EXISTING PAVEMENT
- EXISTING AIRPORT PROPERTY LINE
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS/HAUL ROUTE
- EXISTING AIR OPERATIONS AREA (AOA)
- BARRICADES WITH FLASHING RED LIGHTS AND SIGNS "DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA" (SEE GENERAL NOTE 11 ON NEXT SHEET)

POINT "A"
 NEAREST POINT ON CONSTRUCTION SITE TO ACTIVE RUNWAY 4/22
 • LATITUDE: 41° 04' 04.45" (NAD 83)
 • LONGITUDE: 87° 50' 42.95" (NAD 83)
 • EXISTING ELEVATION: 619.5'

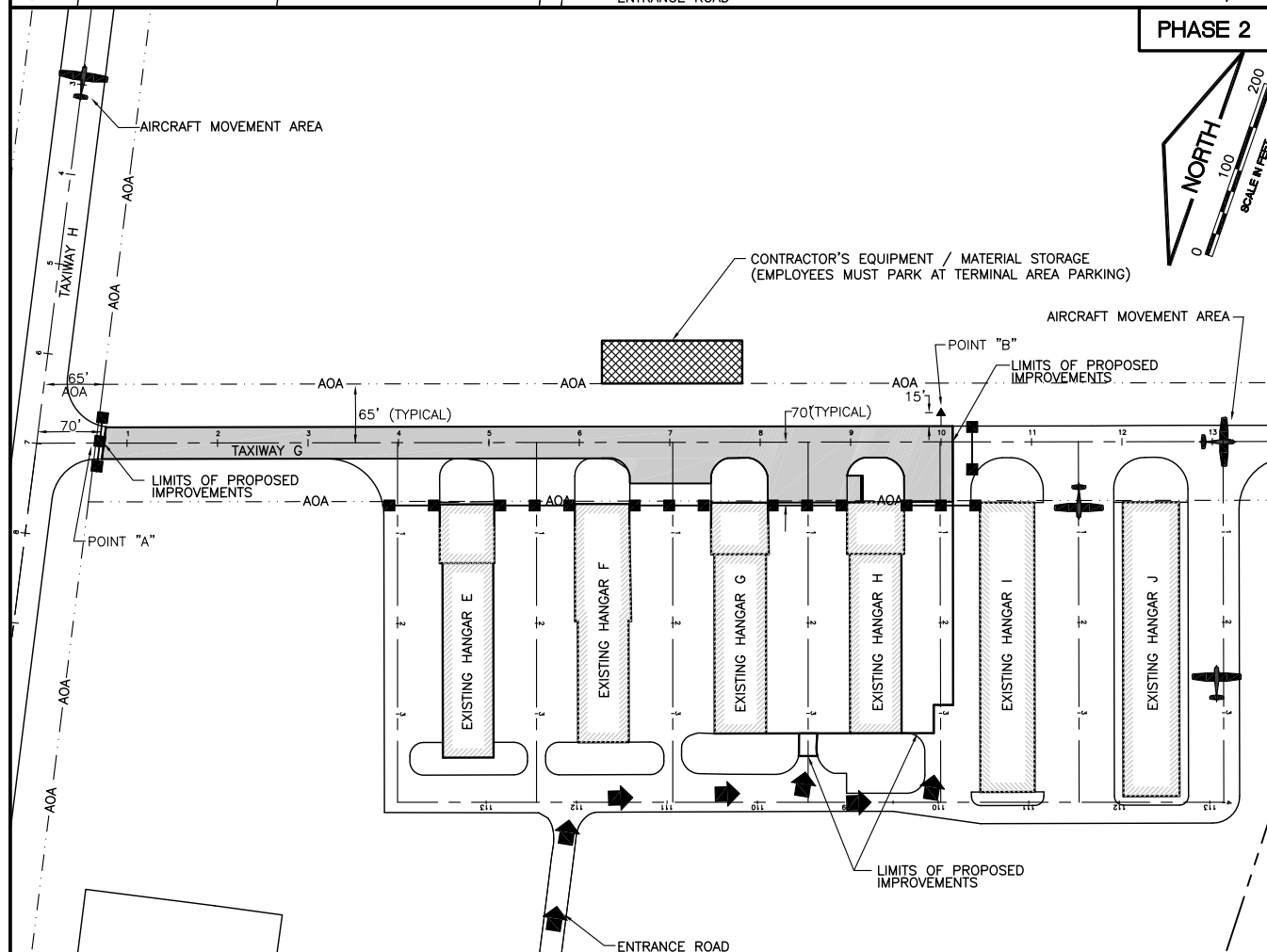
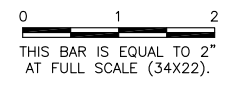
POINT "B"
 NEAREST POINT ON CONSTRUCTION SITE TO ACTIVE RUNWAY 16/34
 • LATITUDE: 41° 03' 55.82" (NAD 83)
 • LONGITUDE: 87° 50' 38.59" (NAD 83)
 • EXISTING ELEVATION: 616.2'

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 20 FEET

DESIGN AIRCRAFT APPROACH CATEGORY: B
 DESIGN AIRPORT GROUP: II

REVISIONS

NUMBER	BY	DATE



PHASE 2: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE RELOCATION OF ANY AIRCRAFT IN PHASE 2 WORK AREA (BY OTHERS) WITH RESIDENT ENGINEER AND AIRPORT MANAGER.
- PLACE BARRICADES AS SHOWN.
- REMOVE EXISTING PAVEMENT PER PLAN.
- COMPLETE GRADING PER TYPICAL SECTION & PLAN AND PROFILE SHEET
- CONSTRUCT BITUMINOUS PAVEMENT.
- INSTALL PAVEMENT MARKINGS PER PLAN
- CLEAN PAVEMENTS AND REMOVE BARRICADES AND MISCELLANEOUS DEBRIS FROM CONSTRUCTION AREA.
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER SO THAT DISPLACED AIRCRAFT CAN BE RELOCATED BACK TO THEIR ORIGINAL STORAGE AREAS (BY OTHERS).

NOTES (BOTH PHASES)

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON EACH PHASE OR AS DIRECTED BY THE RESIDENT ENGINEER.
2. MARK THE AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AS SHOWN BY THE CONSTRUCTION SETBACK LINE OR AS DIRECTED BY THE RESIDENT ENGINEER.
3. THE AIRPORT WILL REQUIRE TWO (2) WEEKS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
4. FOR EACH PHASE OF WORK, THE CONTRACTOR SHALL RETURN THE EQUIPMENT TO THE STAGING AND STORAGE AREA SHOWN.
5. CONTRACTOR SHALL HAVE A DEDICATED FLAGGER ONSITE WHEN WORKING ADJACENT TO ACTIVE TAXIWAYS AND YIELD TO ALL ONCOMING AIRCRAFT. COST SHALL BE INCIDENTAL TO THE CONTRACT.
6. IN THE EVENT ADDITIVE ALTERNATE IS AWARDED, TEMPORARY BARRICADE PLACEMENT WILL BE LOCATED AS SHOWN.

**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TAXIWAY G; REHABILITATE
 SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1**

**SEQUENCE OF CONSTRUCTION
 PER AC 150/5370-2E (LATEST EDITION)**

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SHEET	4 OF 11 SHEETS

GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2E (LATEST EDITION) SAFETY DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER, IN CONSULTATION WITH THE RESIDENT ENGINEER, SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES AT 10-FOOT CENTERS WITH ONE ORANGE FLAG (24" x 24") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER OR OWNER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A FLASHING RED LIGHT AND CONFORM TO IDOT STANDARD 702001, TYPE II. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER TWO (2) WEEKS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION (SEE FLAG DETAIL, THIS SHEET).
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE RESIDENT ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM POINT TO POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF AN FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS, IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

- COORDINATION MEETINGS – THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN THE AIR OPERATIONS AREA (A.O.A.) FOR ACTIVE TAXIWAYS/RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT MANAGER.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MAINTENANCE. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

CONTRACTOR CROSSING RUNWAY AND TAXIWAY AIR OPERATIONS AREA (A.O.A.)

- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

LIMITATIONS ON CONSTRUCTION WITHIN AIRPORT OPERATIONS AREA (A.O.A.)

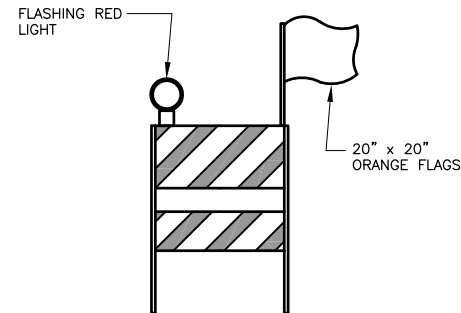
RUNWAYS: (NO WORK IS ANTICIPATED TO BE WITHIN RUNWAY A.O.A.)

ANY WORK WITHIN THE A.O.A. OF AN ACTIVE RUNWAY SHALL EITHER BE DONE ON WEEKENDS, OFF-PEAK DAYTIME OR NIGHTTIME HOURS, LOCAL TIME AS SHOWN ON THE SEQUENCE OF CONSTRUCTION PLAN SHEETS. ON ANY DAY WHEN CONSTRUCTION IS WITHIN THE A.O.A. OF THE RUNWAY, THE RUNWAY SHALL BE CLOSED. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. STEEL PLATES IF NECESSARY SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE A.O.A. IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT.

TAXIWAYS / TAXILANES / APRONS:

CONSTRUCTION WILL BE ALLOWED UP TO THE EDGE OF PAVEMENTS WITHOUT CLOSURE ON A LIMITED BASIS. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TWO (2) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

NOTE – ALL PHASES
ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

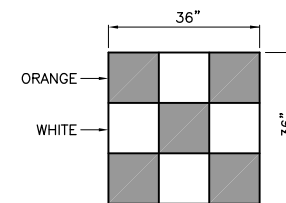


FLASHER BARRICADE DETAIL

N.T.S.

NOTES

- FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
- SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
- NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- PLACE AT TEN-FOOT (10') INTERVALS.
- ONE 20"x20" ORANGE FLAG TO BE INSTALLED ON EACH BARRICADE.

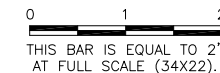


CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE

REVISIONS

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE TAXIWAY G; REHABILITATE
SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1**

**SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DETAILS**

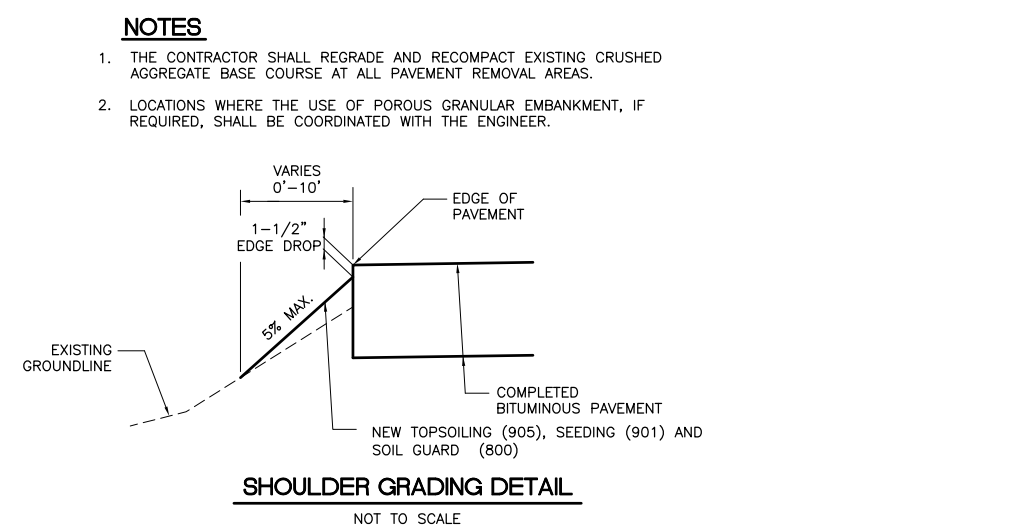
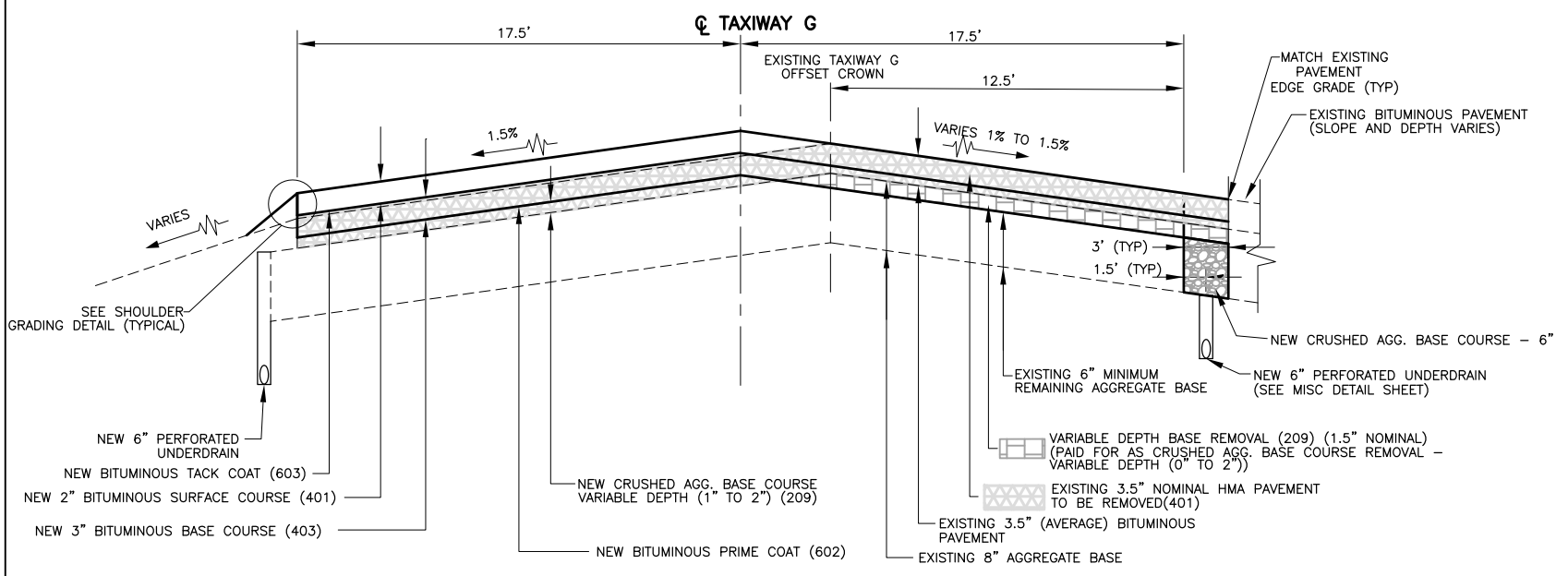
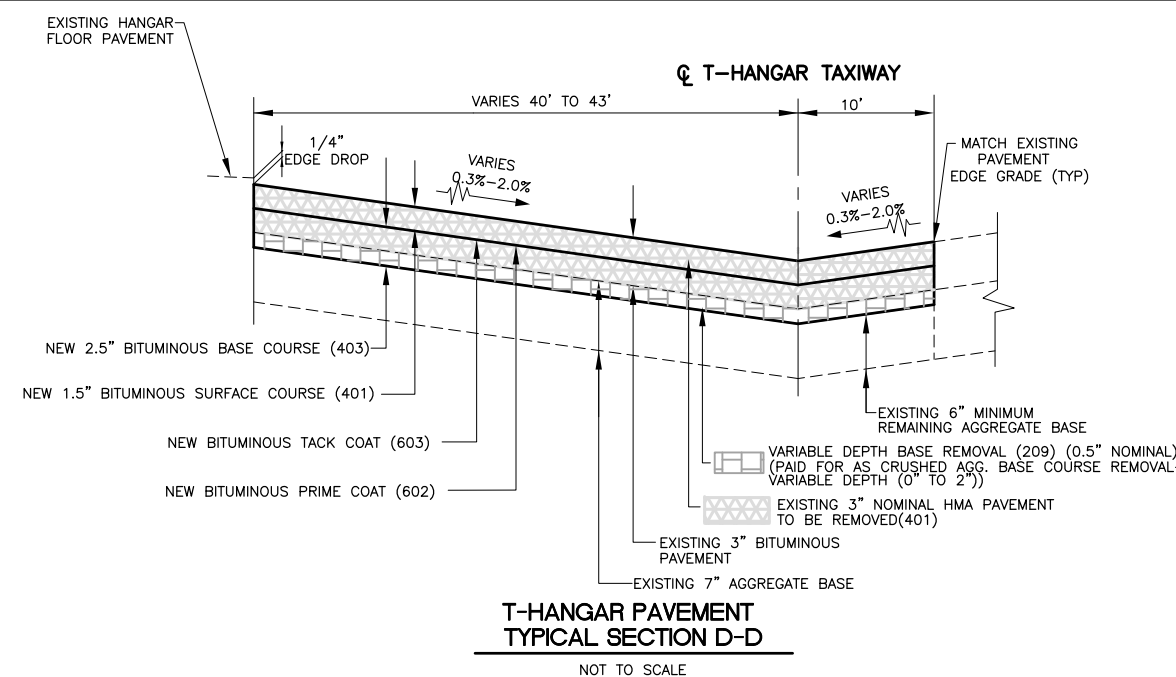
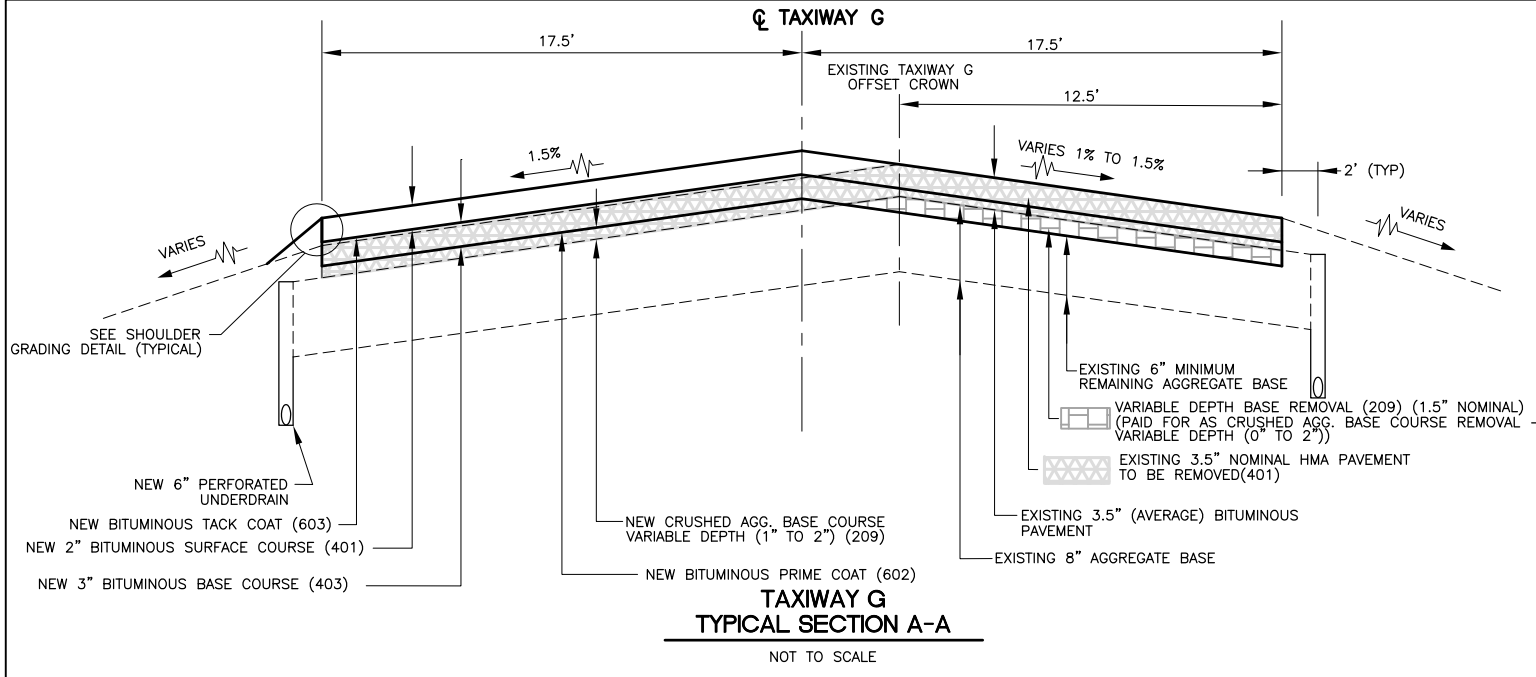
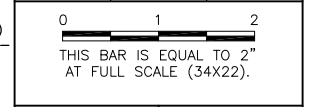
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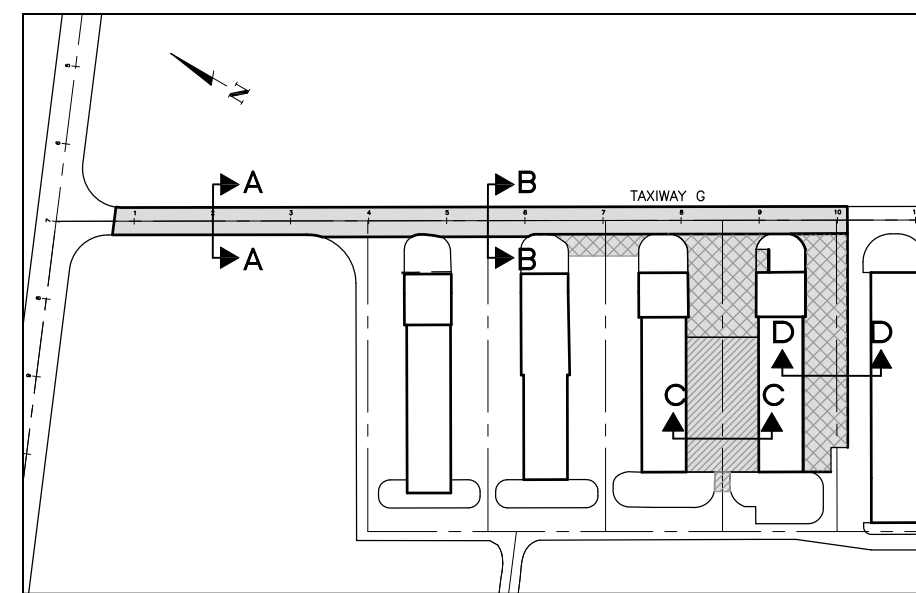
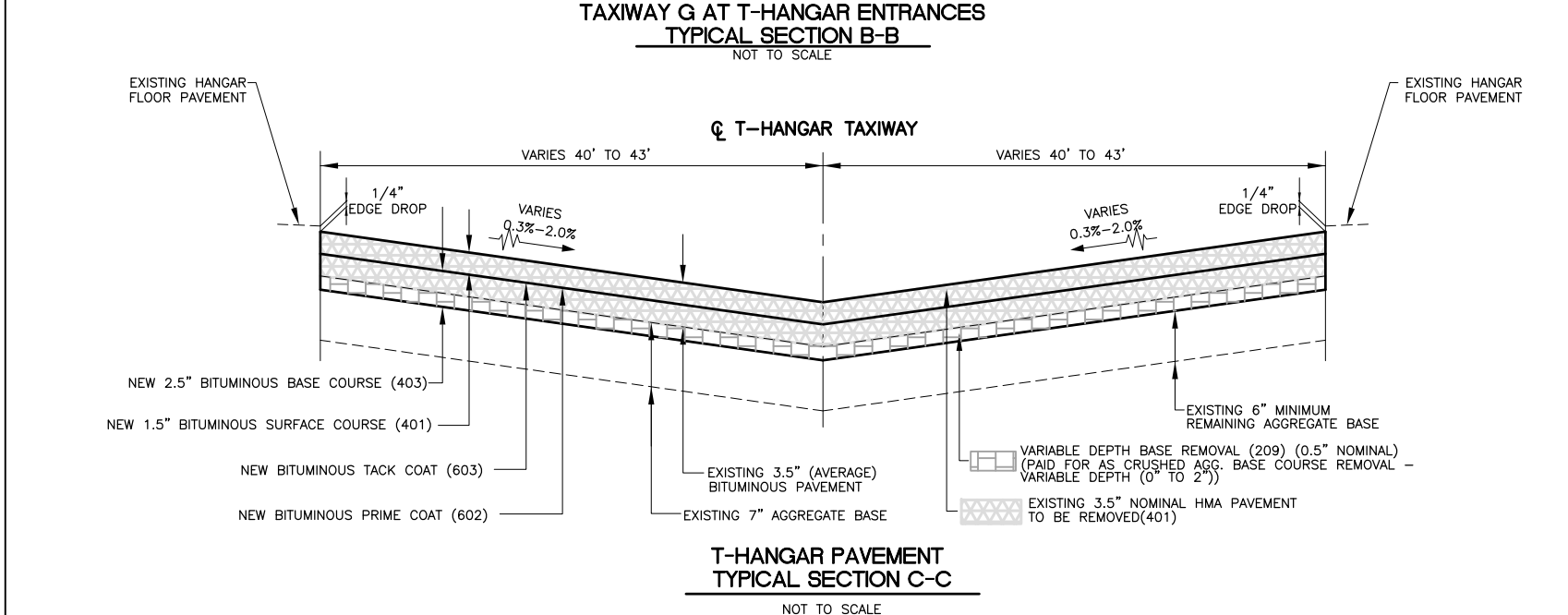
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IL PROJECT: IKK-4078 A.I.P. PROJECT: 3-17-0057-B19	FINAL
SHEET	5 OF 11 SHEETS

REVISIONS		
NUMBER	BY	DATE



NOTES

1. THE CONTRACTOR SHALL REGRADE AND RECOMPACT EXISTING CRUSHED AGGREGATE BASE COURSE AT ALL PAVEMENT REMOVAL AREAS.
2. LOCATIONS WHERE THE USE OF POROUS GRANULAR EMBANKMENT, IF REQUIRED, SHALL BE COORDINATED WITH THE ENGINEER.






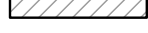

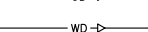

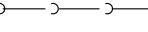
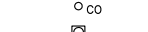
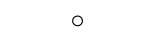





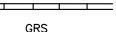
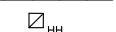
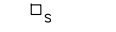

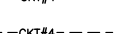
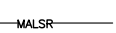
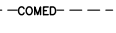
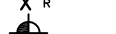
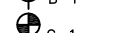




**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TAXIWAY G, REHABILITATE
 SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1**

TYPICAL SECTIONS

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A.I.P. PROJECT:	3-17-0057-B19
FINAL	
SHEET	6 OF 11 SHEETS

LEGEND

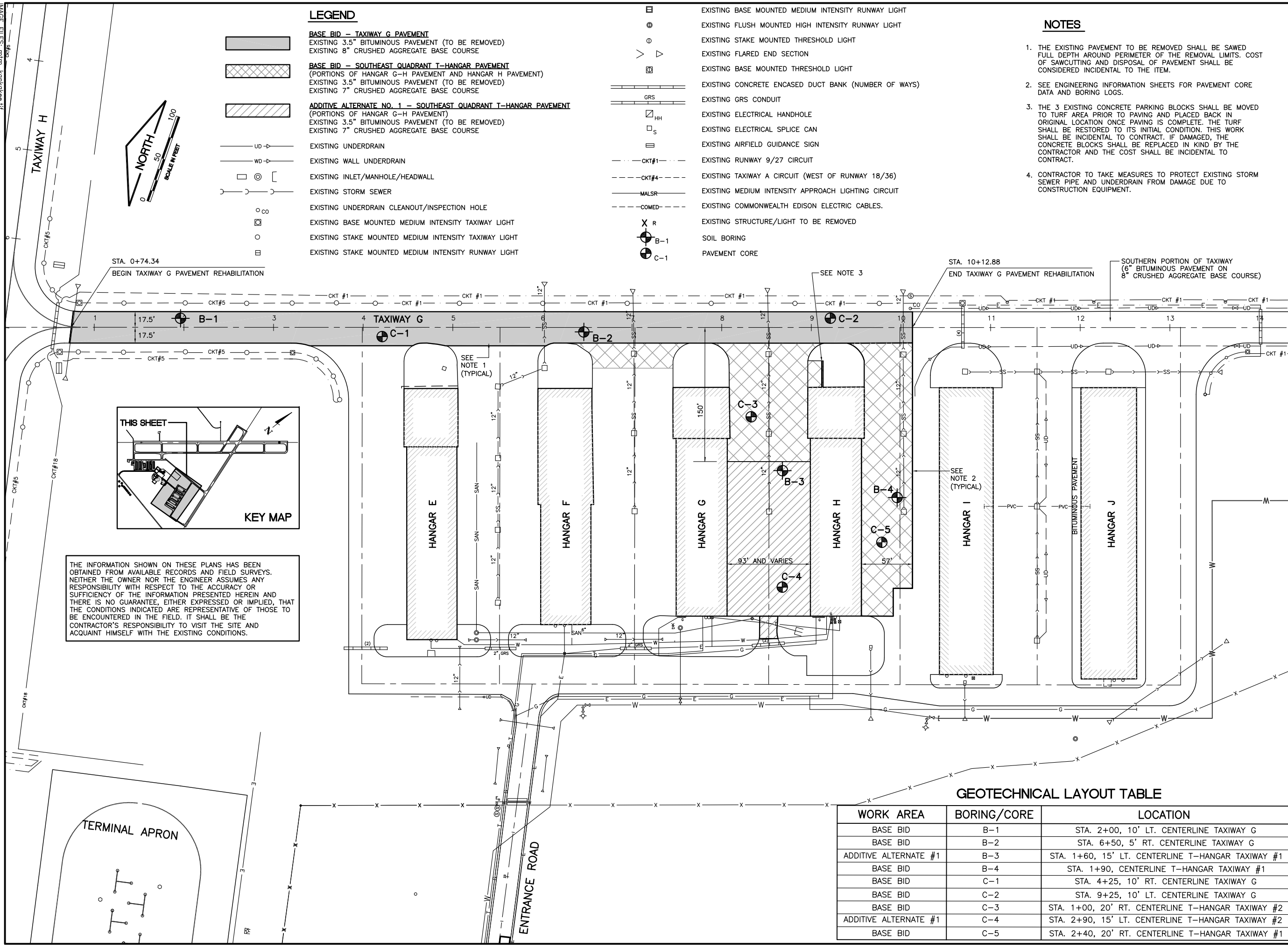
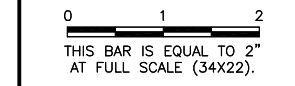
-  **BASE BID - TAXIWAY G PAVEMENT**
 EXISTING 3.5" BITUMINOUS PAVEMENT (TO BE REMOVED)
 EXISTING 8" CRUSHED AGGREGATE BASE COURSE
-  **BASE BID - SOUTHEAST QUADRANT T-HANGAR PAVEMENT**
 (PORTIONS OF HANGAR G-H PAVEMENT AND HANGAR H PAVEMENT)
 EXISTING 3.5" BITUMINOUS PAVEMENT (TO BE REMOVED)
 EXISTING 7" CRUSHED AGGREGATE BASE COURSE
-  **ADDITIVE ALTERNATE NO. 1 - SOUTHEAST QUADRANT T-HANGAR PAVEMENT**
 (PORTIONS OF HANGAR G-H PAVEMENT)
 EXISTING 3.5" BITUMINOUS PAVEMENT (TO BE REMOVED)
 EXISTING 7" CRUSHED AGGREGATE BASE COURSE
-  EXISTING UNDERDRAIN
-  EXISTING WALL UNDERDRAIN
-  EXISTING INLET/MANHOLE/HEADWALL
-  EXISTING STORM SEWER
-  EXISTING UNDERDRAIN CLEANOUT/INSPECTION HOLE
-  EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
-  EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
-  EXISTING STAKE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
-  EXISTING BASE MOUNTED MEDIUM INTENSITY RUNWAY LIGHT
-  EXISTING FLUSH MOUNTED HIGH INTENSITY RUNWAY LIGHT
-  EXISTING STAKE MOUNTED THRESHOLD LIGHT
-  EXISTING FLARED END SECTION
-  EXISTING BASE MOUNTED THRESHOLD LIGHT
-  EXISTING CONCRETE ENCASED DUCT BANK (NUMBER OF WAYS)
-  EXISTING GRS CONDUIT
-  EXISTING ELECTRICAL HANDHOLE
-  EXISTING ELECTRICAL SPLICE CAN
-  EXISTING AIRFIELD GUIDANCE SIGN
-  EXISTING RUNWAY 9/27 CIRCUIT
-  EXISTING TAXIWAY A CIRCUIT (WEST OF RUNWAY 18/36)
-  EXISTING MEDIUM INTENSITY APPROACH LIGHTING CIRCUIT
-  EXISTING COMMONWEALTH EDISON ELECTRIC CABLES.
-  EXISTING STRUCTURE/LIGHT TO BE REMOVED
-  SOIL BORING
-  PAVEMENT CORE

NOTES

1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. SEE ENGINEERING INFORMATION SHEETS FOR PAVEMENT CORE DATA AND BORING LOGS.
3. THE 3 EXISTING CONCRETE PARKING BLOCKS SHALL BE MOVED TO TURF AREA PRIOR TO PAVING AND PLACED BACK IN ORIGINAL LOCATION ONCE PAVING IS COMPLETE. THE TURF SHALL BE RESTORED TO ITS INITIAL CONDITION. THIS WORK SHALL BE INCIDENTAL TO CONTRACT. IF DAMAGED, THE CONCRETE BLOCKS SHALL BE REPLACED IN KIND BY THE CONTRACTOR AND THE COST SHALL BE INCIDENTAL TO CONTRACT.
4. CONTRACTOR TO TAKE MEASURES TO PROTECT EXISTING STORM SEWER PIPE AND UNDERDRAIN FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.

REVISIONS

NUMBER	BY	DATE



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS AND FIELD SURVEYS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION PRESENTED HEREIN AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TAXIWAY G, REHABILITATE
 SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1
 EXISTING CONDITIONS/PROPOSED
 REMOVALS**

GEOTECHNICAL LAYOUT TABLE

WORK AREA	BORING/CORE	LOCATION
BASE BID	B-1	STA. 2+00, 10' LT. CENTERLINE TAXIWAY G
BASE BID	B-2	STA. 6+50, 5' RT. CENTERLINE TAXIWAY G
ADDITIVE ALTERNATE #1	B-3	STA. 1+60, 15' LT. CENTERLINE T-HANGAR TAXIWAY #1
BASE BID	B-4	STA. 1+90, CENTERLINE T-HANGAR TAXIWAY #1
BASE BID	C-1	STA. 4+25, 10' RT. CENTERLINE TAXIWAY G
BASE BID	C-2	STA. 9+25, 10' LT. CENTERLINE TAXIWAY G
BASE BID	C-3	STA. 1+00, 20' RT. CENTERLINE T-HANGAR TAXIWAY #2
ADDITIVE ALTERNATE #1	C-4	STA. 2+90, 15' LT. CENTERLINE T-HANGAR TAXIWAY #2
BASE BID	C-5	STA. 2+40, 20' RT. CENTERLINE T-HANGAR TAXIWAY #1

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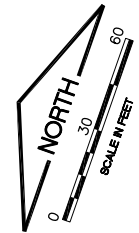
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LEGEND

- NEW ELEVATION (7.70)
- EXISTING ELEVATION (7.60) (SEE NOTE 5)
- NEW TOPSOIL, SEEDING, SOILGUARD
- NEW BITUMINOUS PAVEMENT
- NEW INLET PROTECTION

NOTE

1. ANY EXCAVATION OR GROUND DISTURBED OUTSIDE THE LANDSCAPING LIMITS SHALL BE REPAIRED BY THE CONTRACTOR. THE COST OF REPAIRING THE GROUNDLINE SHALL BE INCIDENTAL TO THE CONTRACT.
2. GRADING FOR TAXIWAY G SHALL BE PER TYPICAL SECTION AND PLAN AND PROFILE
3. TO MAINTAIN EXISTING DRAINAGE OFF TAXIWAY PAVEMENT, WHEN NECESSARY, THE CONTRACTOR SHALL UTILIZE THE MINIMUM TAXIWAY CROSS SLOPE OF 1% ON THE TAXIWAY ON THE WEST END TO MATCH EXISTING GROUND AT EDGE OF PAVEMENT / TIE IN. THE STATIONS WHICH 1% CROSS SLOPE SHALL BE UTILIZED INCLUDE 2+50, 3+00, 4+00, 7+50, 8+00, 9+50, 10+00
4. THE REQUIRED AMOUNTS OF TOPSOILING, SEEDING AND SOIL GUARD ON THE EAST END OF THE TAXIWAY VARIES AND SHALL DETERMINED IN FIELD.
5. FOR THE T-HANGAR PAVEMENTS, THE NEW PAVEMENT ELEVATION SHALL MATCH EXISTING PAVEMENT ELEVATION AND THEREFORE, FOR CLARITY, ONLY THE NEW ELEVATION IS SHOWN.
6. GRADES AT HANGAR FACE SHALL MAINTAIN A 1/4" DROP FROM HANGAR FINISH FLOOR AS NOTED ON TYPICAL SECTIONS

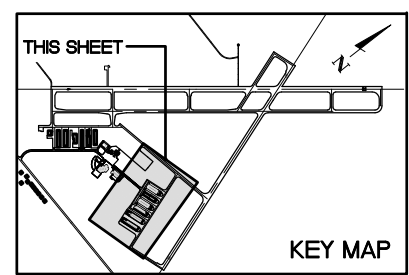
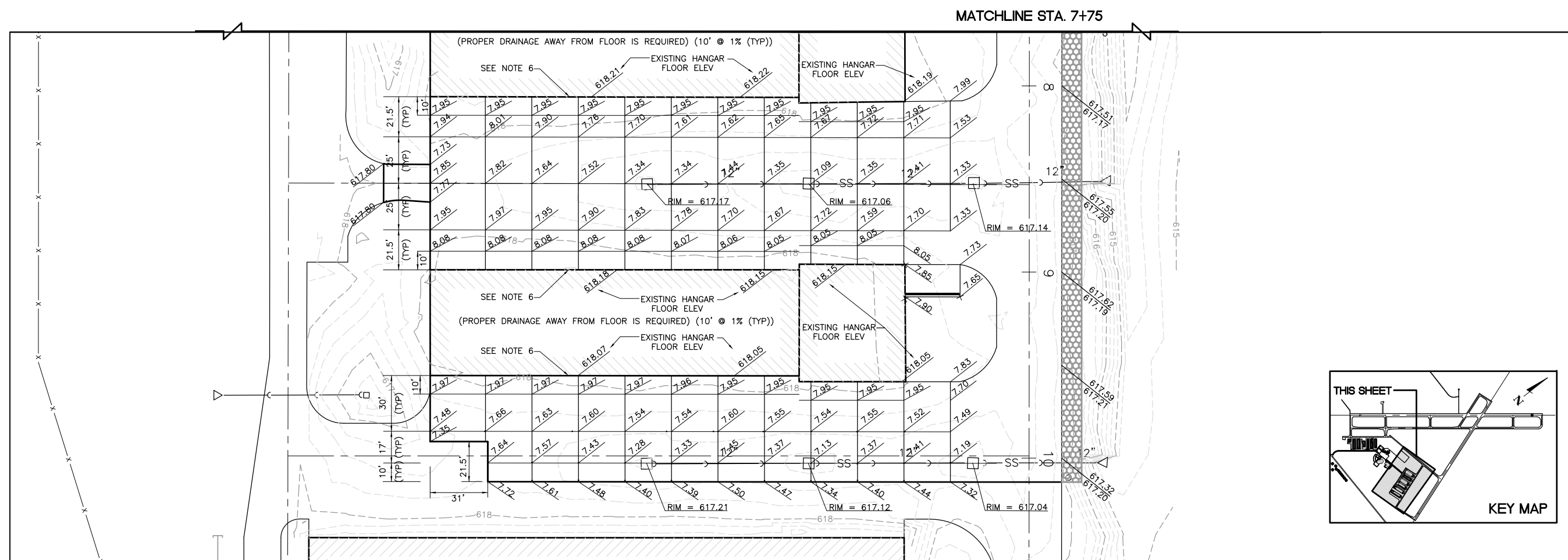
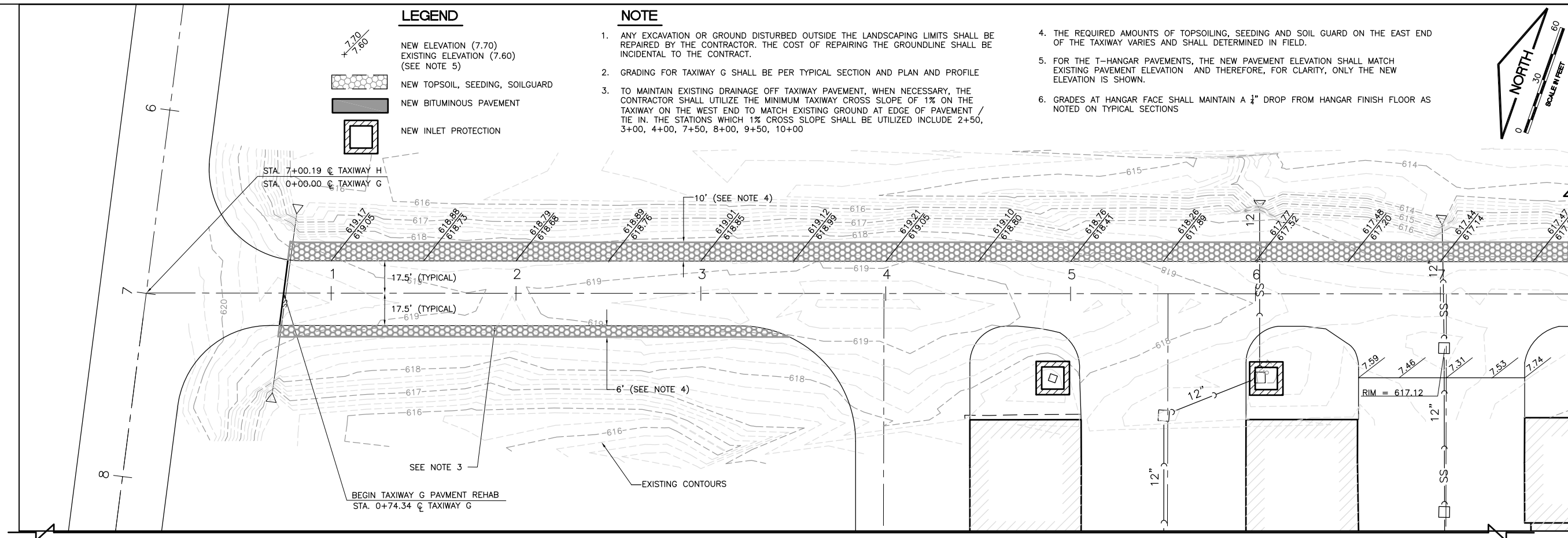


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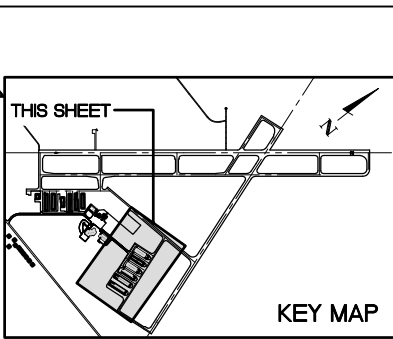
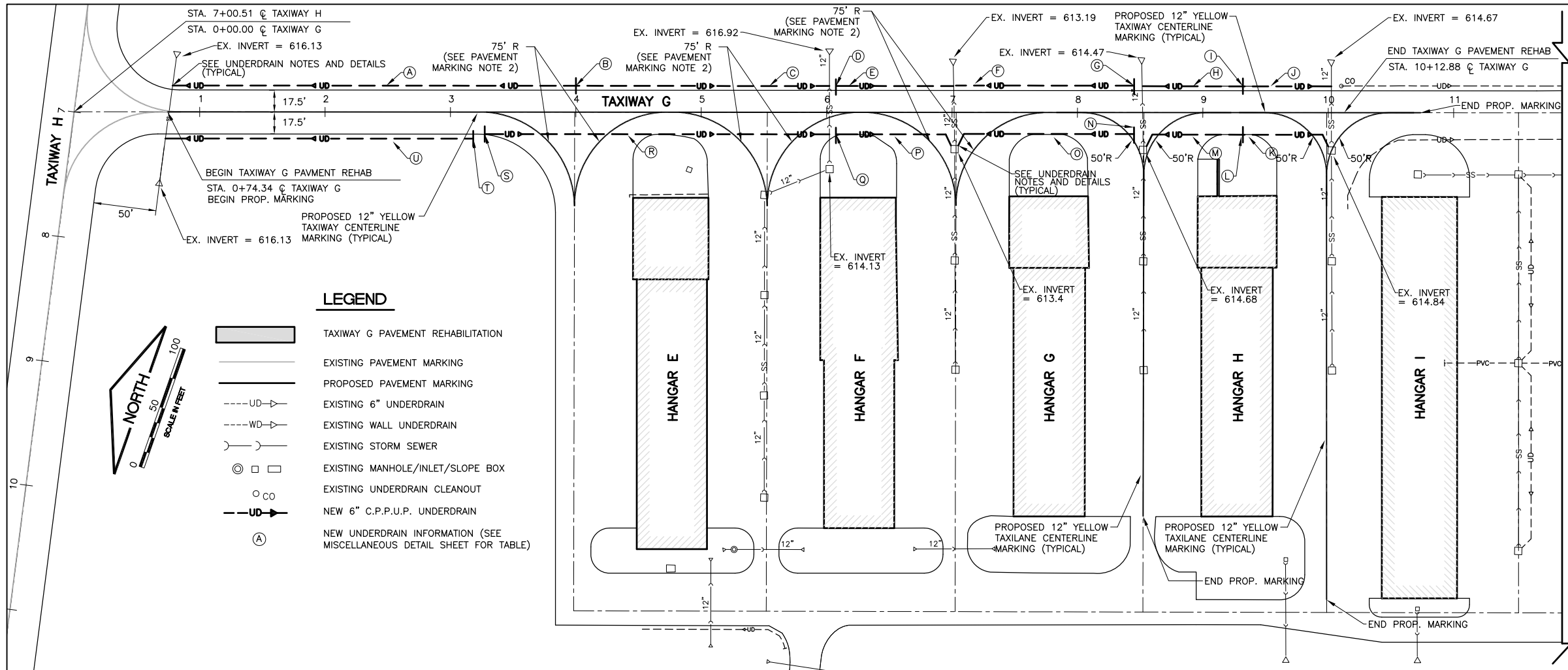


**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TAXIWAY G; REHABILITATE
 SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1**

**LANDSCAPING & T-HANGAR
 PAVEMENT GRADING PLAN**

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SHEET	8 OF 11 SHEETS

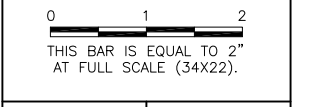


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NOTES

- EXISTING TAXIWAY G CENTERLINE SHOWN IS 5' RIGHT OF THE EXISTING TAXIWAY CROWN. SEE TYPICAL SECTION SHEET FOR LOCATION OF EXISTING TAXIWAY CROWN.

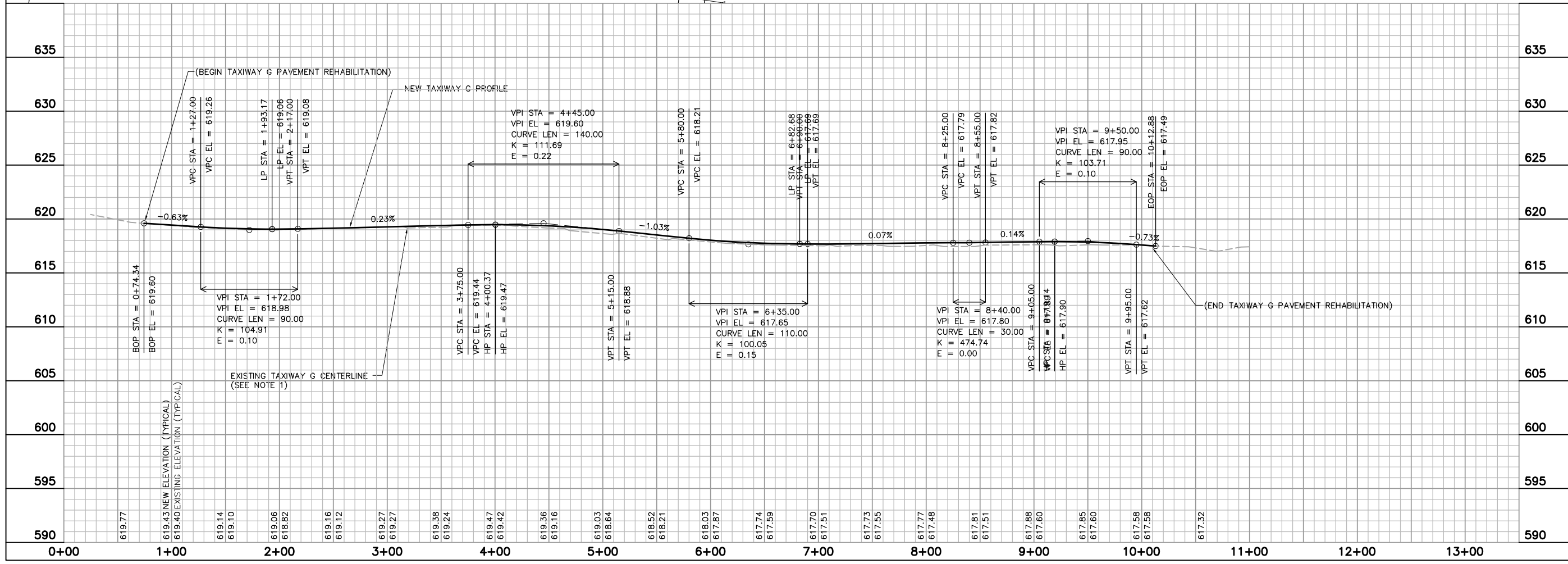


PAVEMENT MARKING NOTES

- ALL NEW AND EXISTING PAVEMENTS TO BE MARKED OR RE-MARKED SHALL BE CLEAN AND DRY PRIOR TO MARKING.
- CURVE RADIUS SET AT EXISTING PAVEMENT MARKING RADIUS.
- IF REQUIRED BY THE AIRPORT, THE CONTRACTOR SHALL INSTALL HANGAR LEAD IN LINES FOR HANGARS G & H (ONE SIDE FOR HANGAR G AND 2 SIDES FOR HANGAR H.) THE LEAD IN LINES ARE TYPICALLY THREE (3) 3" WIDE BY 30' AT THE CENTER OF THE HANGAR DOOR. THESE SHALL BE PAID FOR UNDER ITEM 620 PAVEMENT MARKING. IF NOT REQUIRED, THE QUANTITY SHALL BE REDUCED FROM THE CONTRACT AT NO COST TO THE CONTRACT.

LEGEND

	TAXIWAY G PAVEMENT REHABILITATION
	EXISTING PAVEMENT MARKING
	PROPOSED PAVEMENT MARKING
	EXISTING 6" UNDERDRAIN
	EXISTING WALL UNDERDRAIN
	EXISTING STORM SEWER
	EXISTING MANHOLE/INLET/SLOPE BOX
	EXISTING UNDERDRAIN CLEANOUT
	NEW 6" C.P.P.U.P. UNDERDRAIN
	NEW UNDERDRAIN INFORMATION (SEE MISCELLANEOUS DETAIL SHEET FOR TABLE)



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TAXIWAY G, REHABILITATE
 SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1**

PLAN AND PROFILE TAXIWAY G

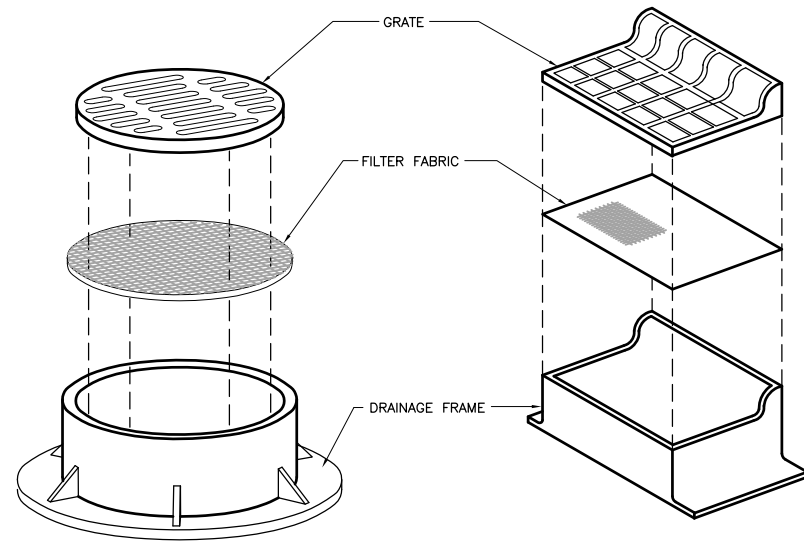
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DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	
DATE:	05/06/11
JOB No:	10075-04
IL PROJECT:	IKK-4078
A.I.P. PROJECT:	3-17-0057-B19

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SHEET 8 OF 11 SHEETS

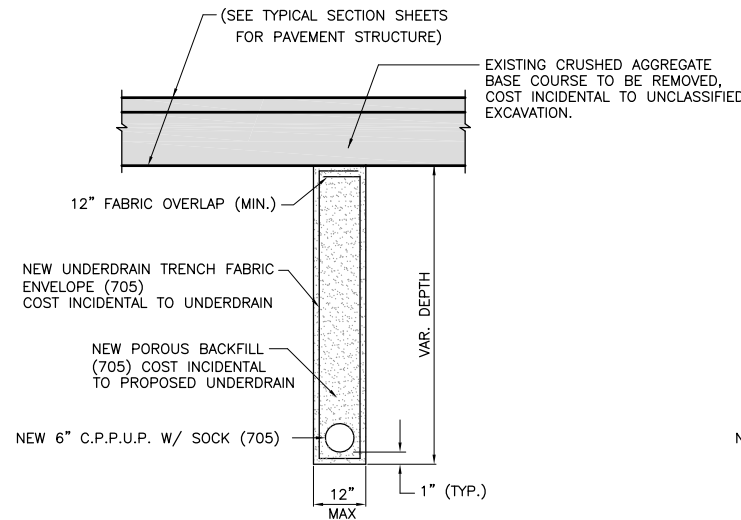


DRAINAGE STRUCTURE FILTER WRAP
N.T.S.

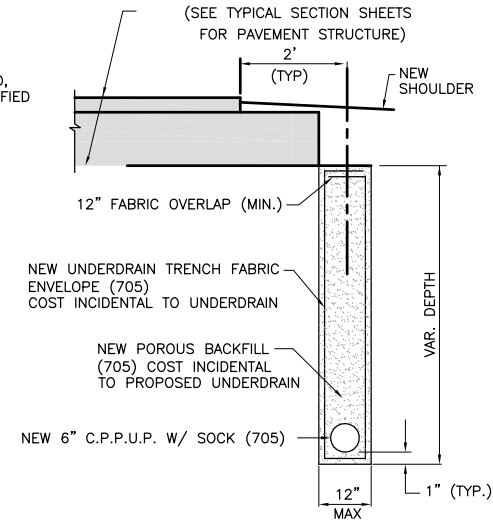
1. FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2007.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
6. COST OF FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

UNDERDRAIN INFORMATION TABLE

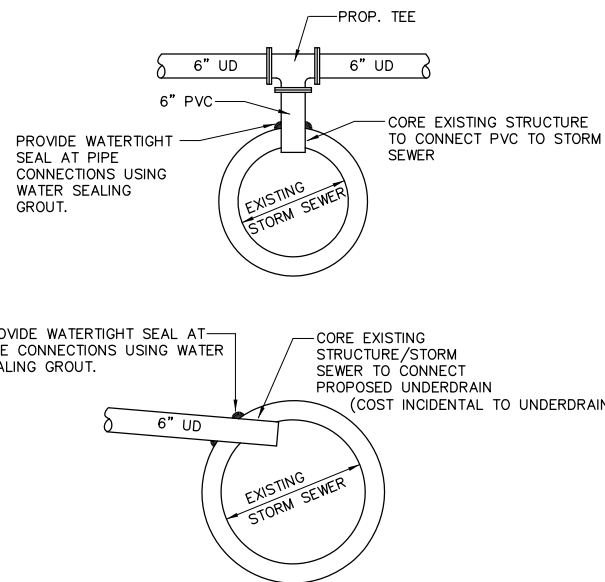
POINT	DESCRIPTION	LENGTH	SLOPE	STA AND INVERT(HIGH POINTS ONLY)
(A)	NEW 6" UNDERDRAIN W/ SOCK	321.6	0.16%	
(B)	HIGH POINT			STA. 4+00.00 INVERT = 617.47
(C)	NEW 6" UNDERDRAIN W/ SOCK	202.2	1.0%	
(D)	HIGH POINT			STA. 6+07.39 INVERT = 615.53
(E)	NEW 6" UNDERDRAIN W/ SOCK	93.5	1.0%	
(F)	NEW 6" UNDERDRAIN W/ SOCK	144.2	0.8%	
(G)	HIGH POINT			STA. 8+45.17 INVERT = 615.80
(H)	NEW 6" UNDERDRAIN W/ SOCK	79.8	0.4%	
(I)	HIGH POINT			STA. 9+31.96 INVERT = 615.35
(J)	NEW 6" UNDERDRAIN W/ SOCK	70.6	0.3%	
(K)	NEW 6" UNDERDRAIN W/ SOCK	73.8	0.3%	
(L)	HIGH POINT			STA. 9+31.96 INVERT = 615.85
(M)	NEW 6" UNDERDRAIN W/ SOCK	82.6	0.3%	
(N)	HIGH POINT			STA. 8+45.17 INVERT = 614.31
(O)	NEW 6" UNDERDRAIN W/ SOCK	145.9	0.21%	
(P)	NEW 6" UNDERDRAIN W/ SOCK	97.2	0.36%	
(Q)	HIGH POINT			STA. 6+07.39 INVERT = 614.35
(R)	NEW 6" UNDERDRAIN W/ SOCK	274.9	0.3%	
(S)	HIGH POINT			STA. 3+27.30 INVERT = 616.27
(T)	HIGH POINT			STA. 3+18.12 INVERT = 617.46
(U)	NEW 6" UNDERDRAIN W/ SOCK	245.45	0.1%	



UNDERDRAIN DETAIL - PAVED AREAS
NO SCALE



UNDERDRAIN DETAIL - EDGE OF PAVEMENT AREAS
NO SCALE



UNDERDRAIN CONNECTION DETAILS
NOT TO SCALE

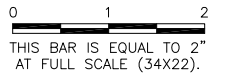
NOTE: ALL UNDERDRAIN CONNECTIONS, FITTINGS, TEES, AND ELBOWS USED FOR CONNECTIONS TO PROPOSED/EXISTING STRUCTURES AND STORM SEWERS, SHALL BE CONSIDERED INCIDENTAL TO THE PROPOSED UNDERDRAIN.

UNDER DRAIN NOTES:

1. CONTRACTOR SHALL FIELD VERIFY UNDERDRAIN INSPECTION HOLES INVERTS BEFORE INSTALLING UNDERDRAIN.
2. ALL UNDERDRAIN CONNECTIONS, TEES, BENDS, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
3. UNDERDRAIN CONFLICTS WITH THE EXISTING CONDITIONS SHALL BE RESOLVED AND SHALL BE INCIDENTAL TO THE COST OF THE UNDERDRAIN.
4. UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPES UNLESS NOTED OTHERWISE.
5. INSTALL PROPOSED ELECTRICAL DUCTS/CONDUITS TO BE CLEAR OF UNDERDRAIN, COST INCIDENTAL.

REVISIONS

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE TAXIWAY G; REHABILITATE
SOUTHEAST QUADRANT T-HANGAR PAVEMENTS - PHASE 1**

MISCELLANEOUS DETAILS

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DRAWN BY: AAO

CHECKED BY: DKP

APPROVED BY: DKP

DATE: 05/06/11

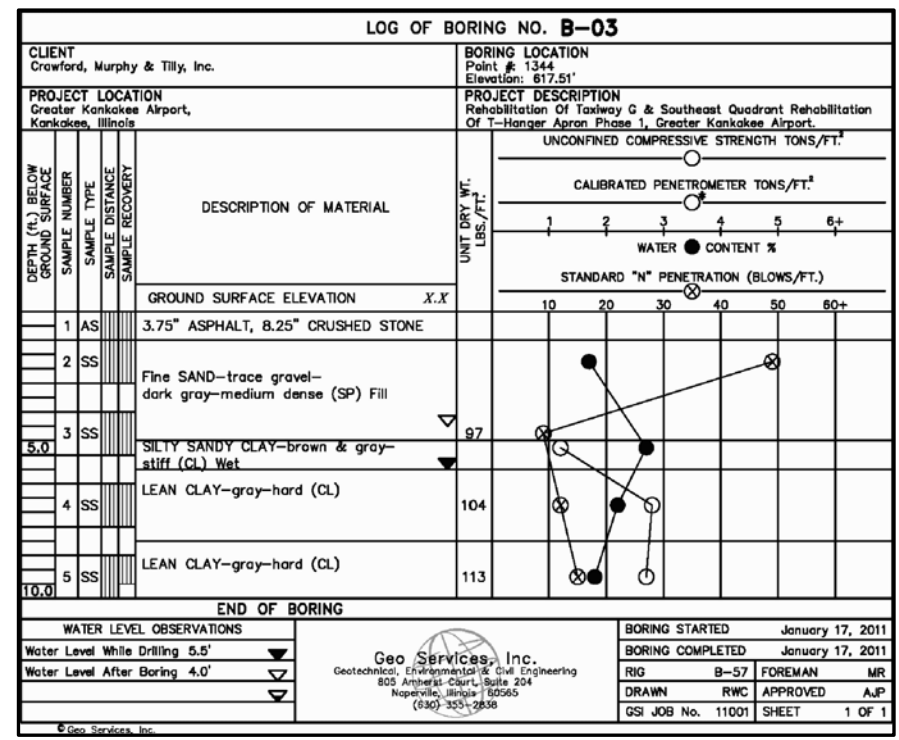
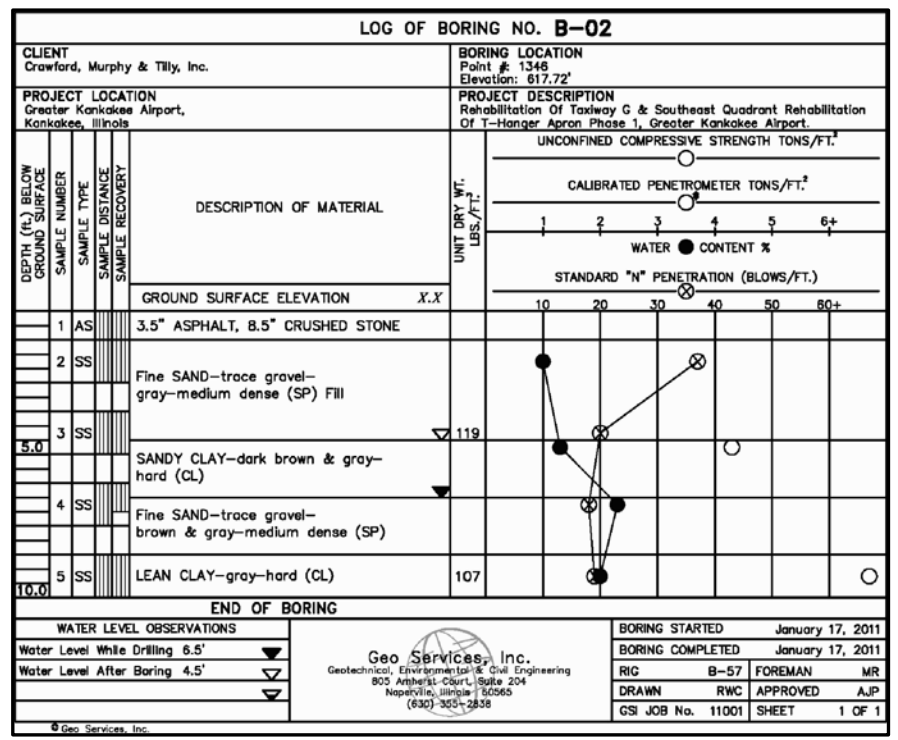
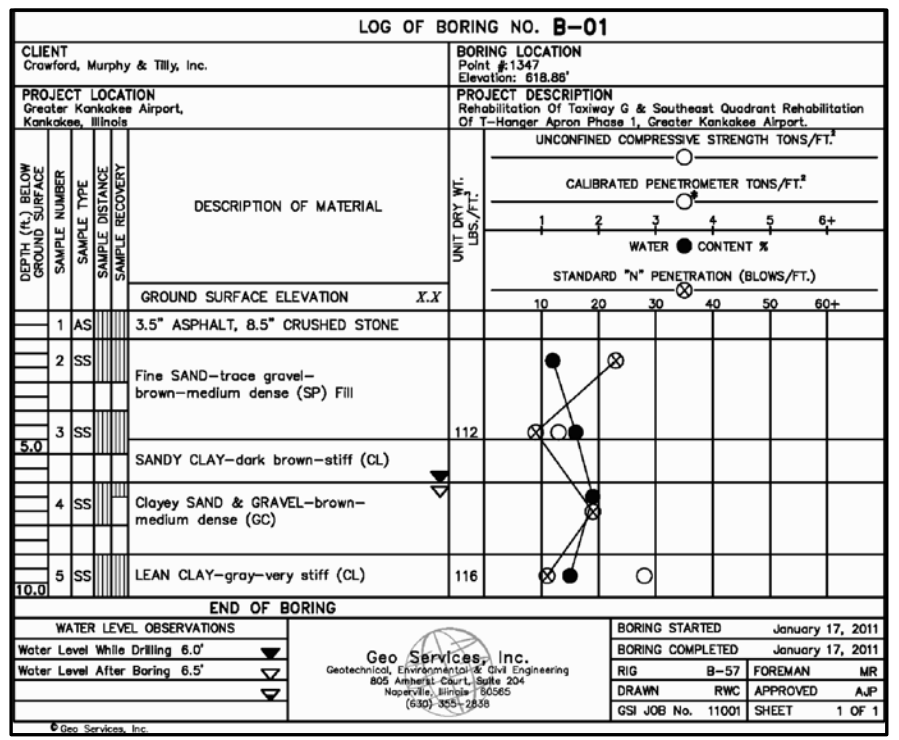
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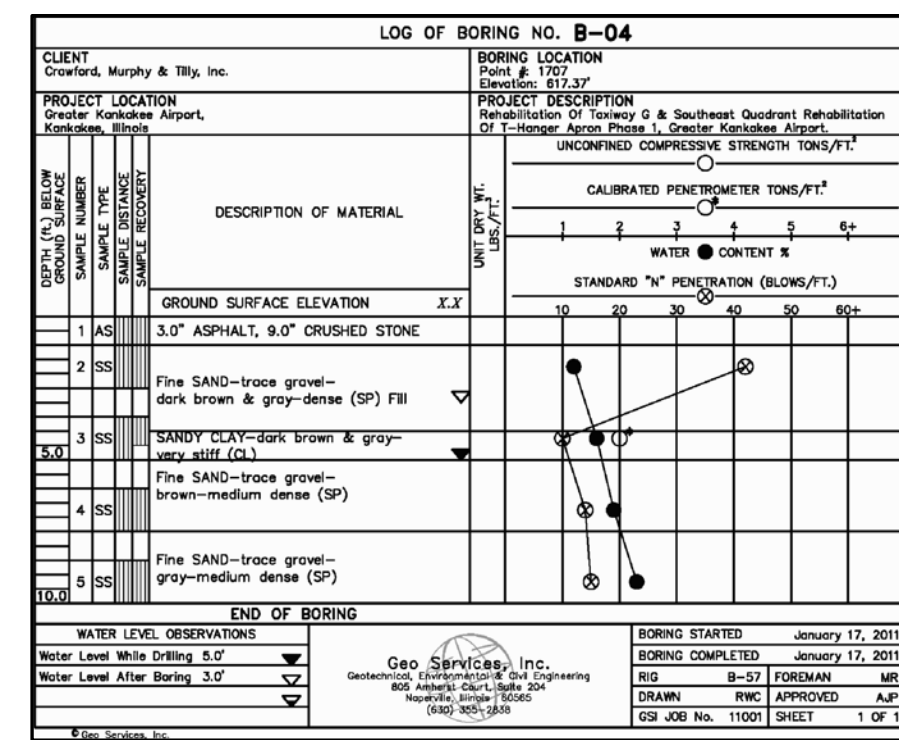
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 UPDATE BY: Adham Odeh
 SURVEY BOOK #
 DATE: Thursday, May 12, 2011 1:22:49 PM
 XREF DWG: tbcint.dwg
 tb.dwg



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0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE TAXIWAY G; REHABILITATE
 SOUTHEAST QUADRANT T-HANGER PAVEMENTS - PHASE 1
 ENGINEERING INFORMATION



CORE NO.		THICKNESS (in.)	MATERIAL DESCRIPTION
C-1		1.5 2.0 8.5 12.0+	Point # 1347 Elevation: 619.48' ASPHALT-well consolidated, fine to medium aggregate, surface mix. ASPHALT-well consolidated, fine to medium aggregate, surface mix. AGGREGATE-Crushed stone. SAND-Fine and medium dense.
C-2		1.25 2.0 8.75 12.0+	Point # 1342 Elevation: 617.33' ASPHALT-well consolidated, fine to medium aggregate, surface mix. ASPHALT-well consolidated, fine to medium aggregate, surface mix. AGGREGATE-Crushed stone. SAND-Fine and medium dense.
C-3		1.5 2.25 8.25 12.0+	Point # 1343 Elevation: 617.65' ASPHALT-porous, fine to medium aggregate, surface mix. ASPHALT-well consolidated, fine to medium coarse aggregate, binder mix. AGGREGATE-Crushed stone. SAND-Fine and medium dense.
C-4		1.75 1.75 11.0+	Point # 1345 Elevation: 617.81' ASPHALT-well consolidated, fine to medium aggregate, surface mix. ASPHALT-well consolidated, fine to medium coarse aggregate, binder mix. AGGREGATE-Crushed stone. SAND-Fine and medium dense.
C-5		1.25 1.75 8.0 11.0+	Point # 1708 Elevation: 617.72' ASPHALT-well consolidated, fine to medium aggregate, surface mix. ASPHALT-well consolidated, fine to medium coarse aggregate, binder mix. AGGREGATE-Crushed stone. SAND-Fine and dense.

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 SHEET 11 OF 11 SHEETS