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ADD. ALT. 1: TXY B, F WEST, AND RIAT RD.

55. AA1: SITE PLAN / CONTROL (1SP1)
56. AA1: TYPICAL SECTION TXY F WEST (1TY1)
57. AA1: TYPICAL SECTION TXY B (1TY2)
58. AA1: EXISTING CONDITIONS REMOVALS (1EX1)
59. AA1: RIAT ROAD SITE PLAN AND TYPICAL SECTION (1RR1)
60. AA1: JOINTING PLAN (1JT1)

ADD. ALT. 2: TXY L, F EAST AND RWY 1/19

61. AA2: SITE PLAN / CONTROL (2SP1)
62. AA2: TYPICAL SECTION RWY 1/19 (2TY1)
63. AA2: TYPICAL SECTION TXY F EAST (2TY2)
64. AA2: TYPICAL SECTION TXY L (2TY3)
65. AA2: EXISTING CONDITIONS REMOVALS (2EX1)
66. AA2: RWY 1/19 PLAN AND PROFILE (2PP1)
67. AA2: JOINTING PLAN (2JT1)

GREATER ROCKFORD AIRPORT AUTHORITY ROCKFORD, ILLINOIS

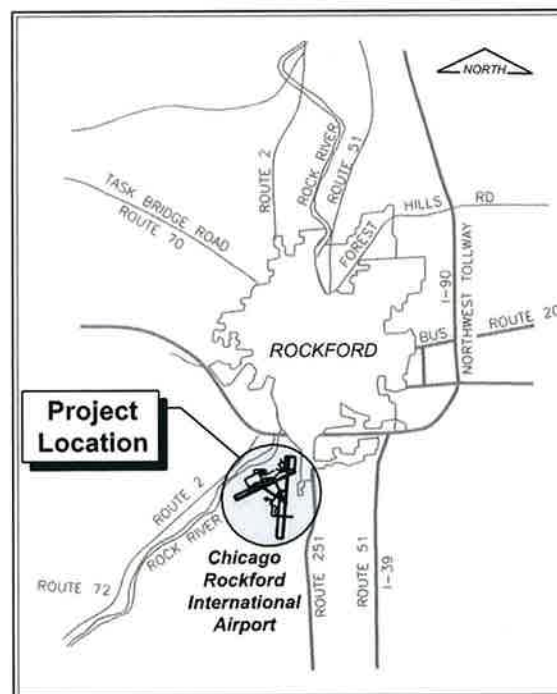
CONSTRUCTION PLANS FOR CHICAGO ROCKFORD INTERNATIONAL AIRPORT

REHABILITATE RUNWAY 1/19 PHASE 1

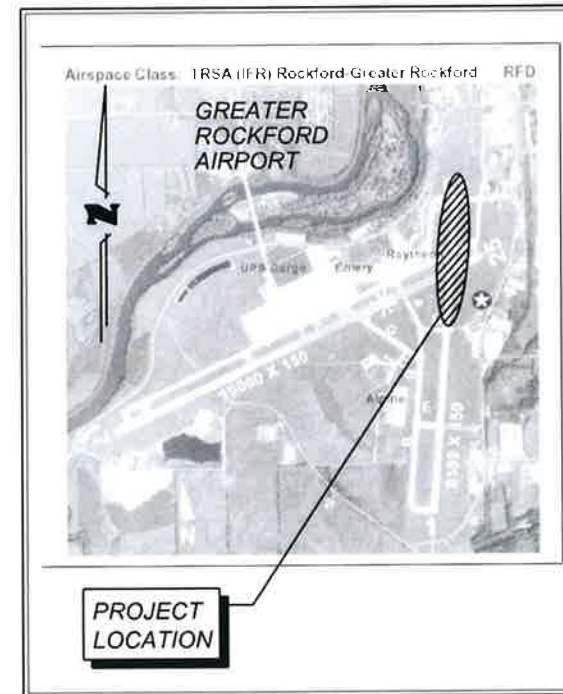
FINAL SUBMITTAL

**ILLINOIS PROJECT: RFD-4083
A.I.P. PROJECT: 3-17-0088-XX**

**APRIL 22, 2011
(JUNE 17, 2011 LETTING)**



LOCATION MAP



SITE PLAN



Greater Rockford Airport Authority

APPROVED BY 
FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR

DATE APRIL 22, 2011

DESIGN INFORMATION

TOWNSHIP: 43 NORTH COUNTY: WINNEBAGO
RANGE: 1 EAST ROCKFORD TOWNSHIP

ALP DATA: APPROACH CAT. / DESIGN GROUP: D5
DESIGN AIRCRAFT: 747-400

(MAXIMUM EQUIPMENT HEIGHT = 25')

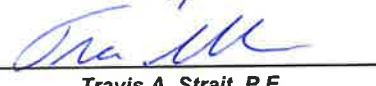


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SUBMITTED BY 
Travis A. Strait, P.E.

DATE 4/22/11
license expires 11-30-2011

RO019



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ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**SUMMARY
OF QUANTITIES
(QTY1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **2** REVISION

SHEET 2 OF 67 SHEETS

**SUMMARY OF QUANTITIES
(QTY1)**

SEQ NO	ITEM NO	DESCRIPTION	UNIT	BASE BID		ADD ALT 1		ADD ALT 2	
				QUANTITY (F/S/L)	RECORD QUANTITY	QUANTITY (L)	RECORD QUANTITY	QUANTITY (F/S/L)	RECORD QUANTITY
					(AR)				
							TXY B, TXY F WEST AND RIAT ROAD (AS)		TXY F EAST, TXY L AND 300' RWY 1/19 (AT)
1	AR108108	1/C #8 5 KV UG CABLE	LF	23,230.00					
2	AR108158	1/C #8 5 KV UG CABLE IN UD	LF	7,050.00					
3	AR109210	VAULT MODIFICATIONS	LS	1.00					
4	AR110501	1-WAY CONCRETE ENCASED DUCT	LF	6,670.00					
5	AR110502	2-WAY CONCRETE ENCASED DUCT	LF	115.00					
6	AR110554	EXTEND 4-WAY DUCT	LF	25.00					
7	AR110610	ELECTRICAL HANDHOLE	EACH	12.00					
8	AR125415	MITL-BASE MOUNTED	EACH	26.00					
9	AR125420	TAXIWAY LIGHT INPAVEMENT	EACH	9.00					
10	AR125444	TAXIWAY GUIDANCE SIGN, 4 CHARACTER	EACH	1.00					
11	AR125515	HIRL, BASE MOUNTED	EACH	16.00					
12	AR125525	HIRL, INPAVEMENT	EACH	9.00					
13	AR125530	RUNWAY CENTERLINE LIGHT	EACH	15.00					
14	AR125555	THRESHOLD LIGHTS, INPAVEMENT	EACH	2.00					
15	AR125615	PAPI (L-880 SYSTEM)	EACH	2.00					
16	AR125902	REMOVE BASE MOUNTED LIGHT	EACH	42.00					
17	AR125903	REMOVE INPAVEMENT LIGHT	EACH	26.00					
18	AR125909	REMOVE VASI	EACH	2.00					
19	AR125942	ADJUST BASE MOUNTED LIGHT	EACH	33.00					2.00
20	AR125943	ADJUST INPAVEMENT LIGHT	EACH	67.00					4.00
21	AR125947	ADJUST REILS	PAIR	1.00					
22	AR125964	RELOCATE TAXI GUIDANCE SIGN	EACH	9.00					
23	AR150515	FIELD LABORATORY	LS	1.00					
24	AR150520	MOBILIZATION	LS	1.00					
25	AR150540	HAUL ROUTE	LS	1.00					
26	AR152410	UNCLASSIFIED EXCAVATION	CY	175.00			2,400.00		
27	AR152540	SOIL STABILIZATION FABRIC	SY	500.00					
28	AR156510	SILT FENCE	LF	1,000.00			2,200.00		
29	AR156512	BALES	EACH	72.00					
30	AR208515	POROUS GRANULAR EMBANKMENT	CY	100.00			100.00		
31	AR209608	CRUSHED AGG. BASE COURSE - 8"	SY	500.00					
32	AR401610	BITUMINOUS SURFACE COURSE	TON	11,535.00					402.00
33	AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1.00					
34	AR401640	BITUMINOUS PAVEMENT GROOVING	SY	8,335.00					
35	AR401650	BITUMINOUS PAVEMENT MILLING	SY	76,670.00			7,460.00		10,880.00
36	AR401910	REMOVE AND REPLACE BIT. PAVEMENT	SY	645.00		50.00			
37	AR501520	20" PCC PAVEMENT	SY	46,500.00			7,250.00		10,600.00
38	AR501530	PCC TEST BATCH	EACH	1.00					
39	AR501540	PCC PAVEMENT GROOVING	SY	42,400.00					4,350.00
40	AR602510	BITUMINOUS PRIME COAT	GAL	3,717.00					
41	AR603510	BITUMINOUS TACK COAT	GAL	9,637.00					
42	AR620520	PAVEMENT MARKING - WATERBORNE	SF	112,414.20					2,595.60
43	AR620525	PAVEMENT MARKING - BLACK BORDER	SF	29,293.00					1,182.00
44	AR760506	6" DUCTILE IRON WATER MAIN	LF			5.00			
45	AR760508	8" DUCTILE IRON WATER MAIN	LF			430.00			
46	AR760800	FIRE HYDRANT	EACH			1.00			
47	AR770945	ADJUST SANITARY MANHOLE	EACH	1.00					
48	AR800812	ABANDON WATER VALVE	EACH			3.00			
49	AR800817	ABANDON WATER MAIN	EACH			4.00			
50	AR800830	CONNECT TO EXISTING WATER MAIN	EACH			2.00			
51	AR800868	SOIL GUARD	SY	12,200.00		1,785.00			1,000.00
52	AR800860	ADJUST PAVEMENT SENSOR	EACH	2.00					
53	AR800864	2-#4 XLP-USE, 1-#10 GND IN 1-1/4" UD	LF	3,065.00					
54	AR800879	TEMPORARY THRESHOLD - 19	LS	1.00					
55	AR800880	TEMPORARY THRESHOLD - 25	LS	1.00					
56	AR800881	RUNWAY 7 PAPI PCU RELOCATION	EACH	1.00					
57	AR800882	ABANDON SANITARY SEWER	LF	5,780.00					
58	AR800883	ABANDON SANITARY MANHOLE	EACH	29.00					
59	AR901510	SEEDING	ACRE	2.52		0.36			0.21
60	AR905530	TOPSOILING	SY	12,200.00					670.00



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**SITE PLAN
AND CONTROL (SPL1)**

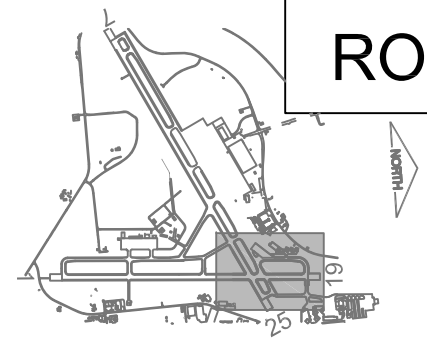
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PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **3** REVISION

SHEET 3 OF 67 SHEETS

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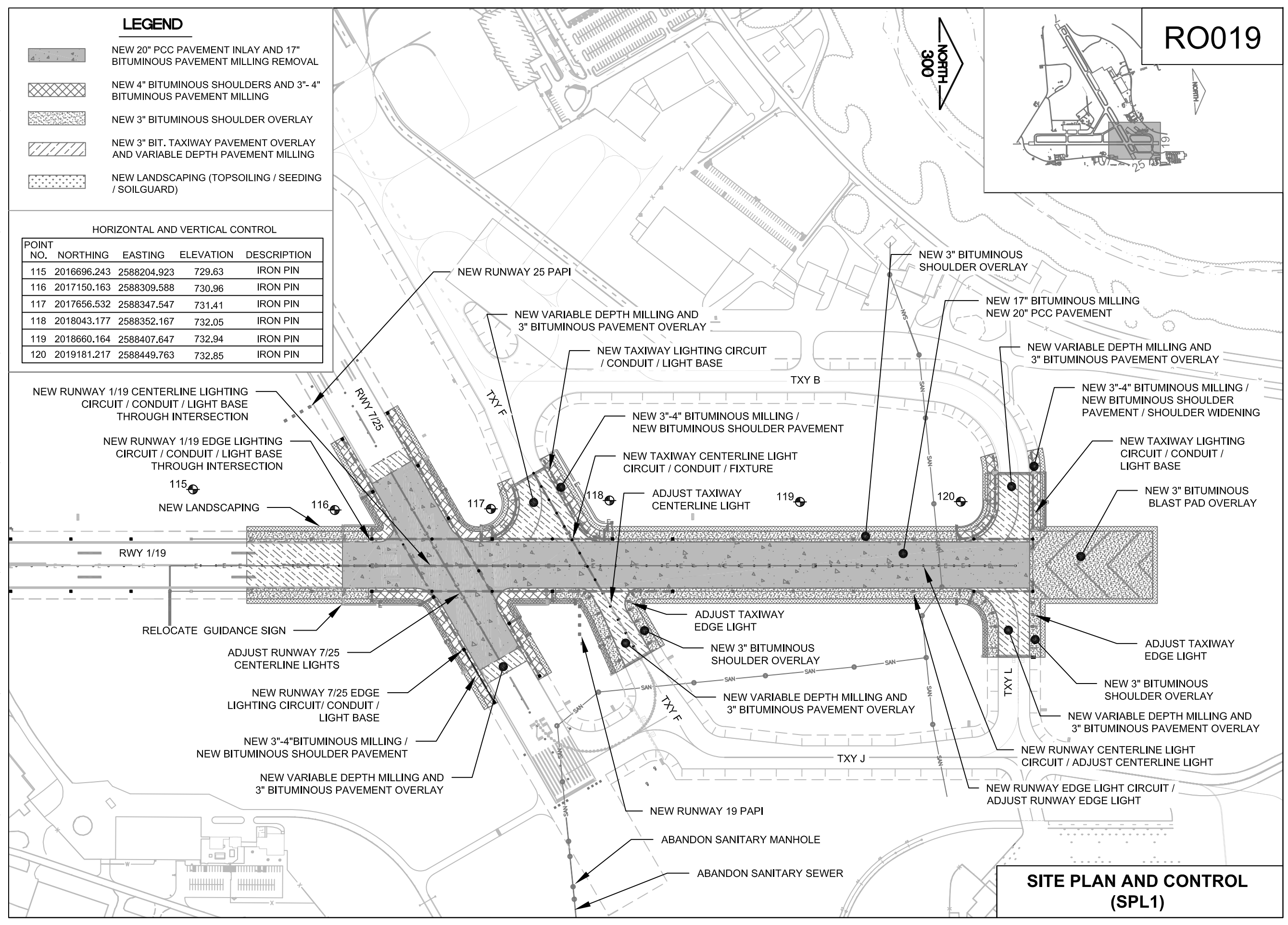


LEGEND

- NEW 20" PCC PAVEMENT INLAY AND 17" BITUMINOUS PAVEMENT MILLING REMOVAL
- NEW 4" BITUMINOUS SHOULDERS AND 3"-4" BITUMINOUS PAVEMENT MILLING
- NEW 3" BITUMINOUS SHOULDER OVERLAY
- NEW 3" BIT. TAXIWAY PAVEMENT OVERLAY AND VARIABLE DEPTH PAVEMENT MILLING
- NEW LANDSCAPING (TOPSOILING / SEEDING / SOILGUARD)

HORIZONTAL AND VERTICAL CONTROL

POINT NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
115	2016696.243	2588204.923	729.63	IRON PIN
116	2017150.163	2588309.588	730.96	IRON PIN
117	2017656.532	2588347.547	731.41	IRON PIN
118	2018043.177	2588352.167	732.05	IRON PIN
119	2018660.164	2588407.647	732.94	IRON PIN
120	2019181.217	2588449.763	732.85	IRON PIN



SITE PLAN AND CONTROL (SPL1)



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REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
TYPICAL SECTION
RWY 1/19 (TYP1)

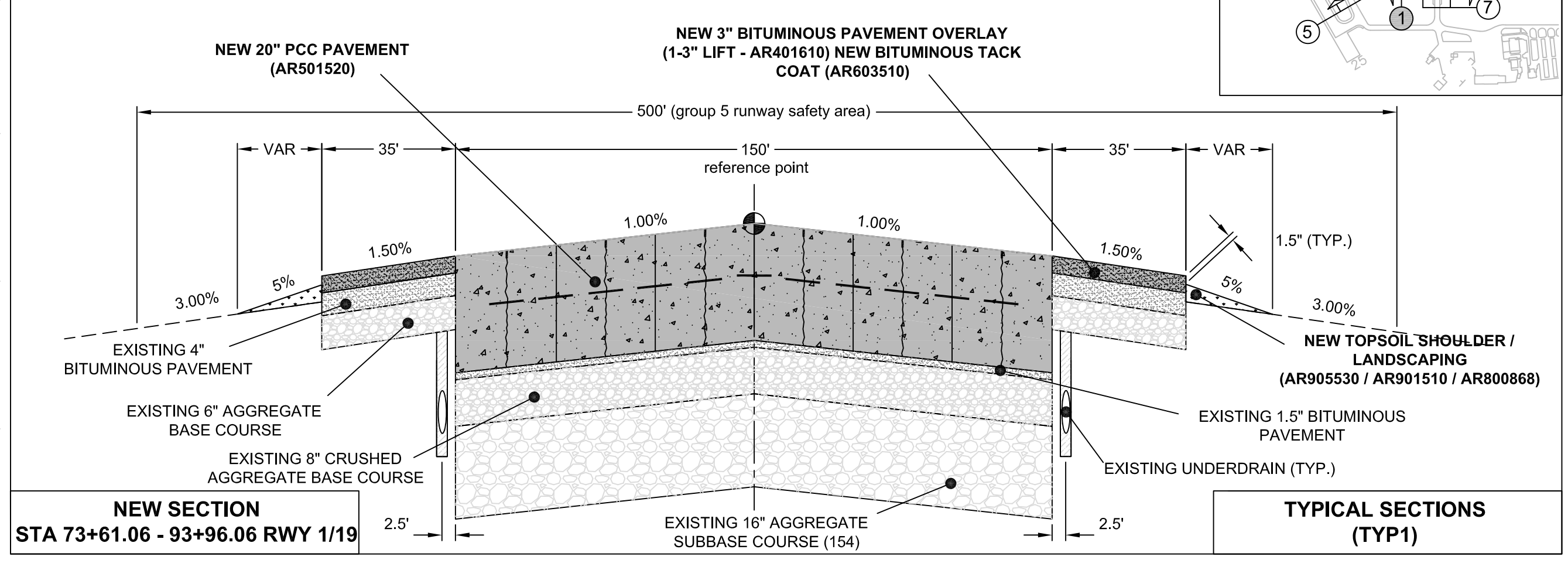
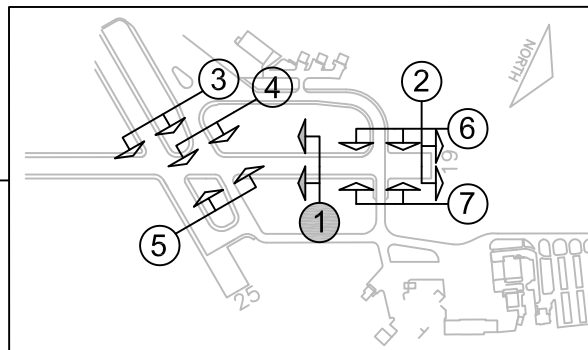
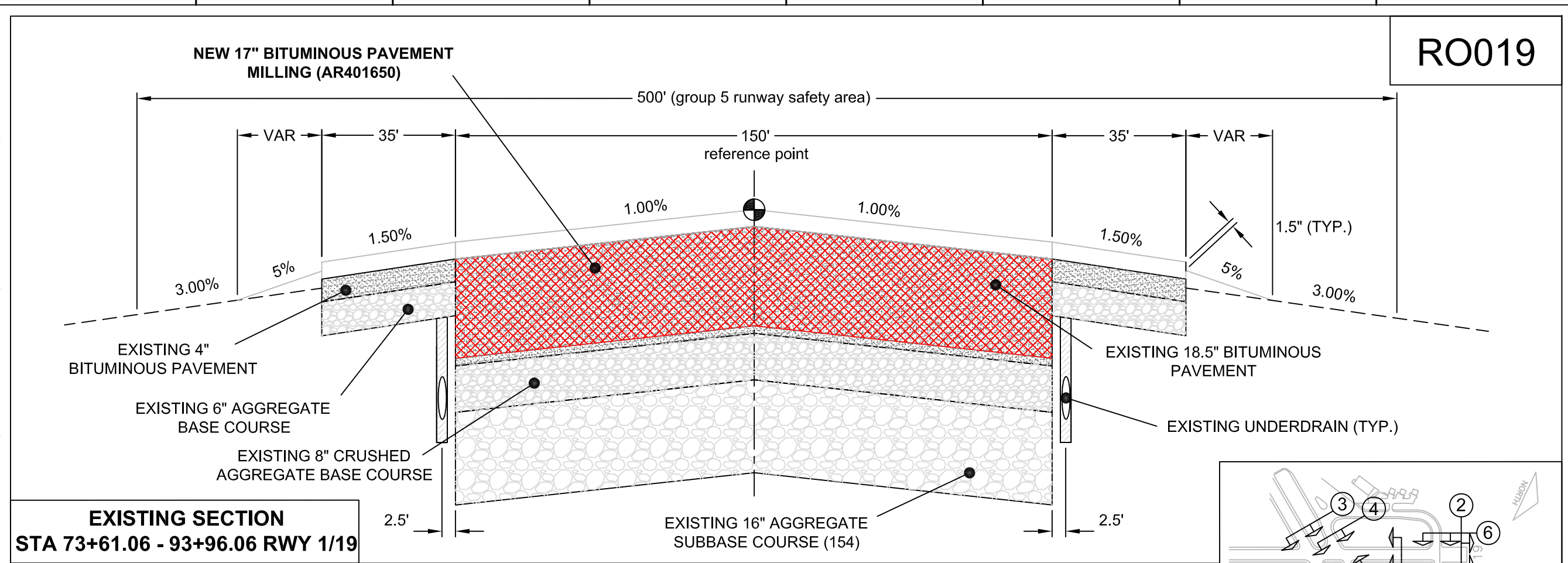
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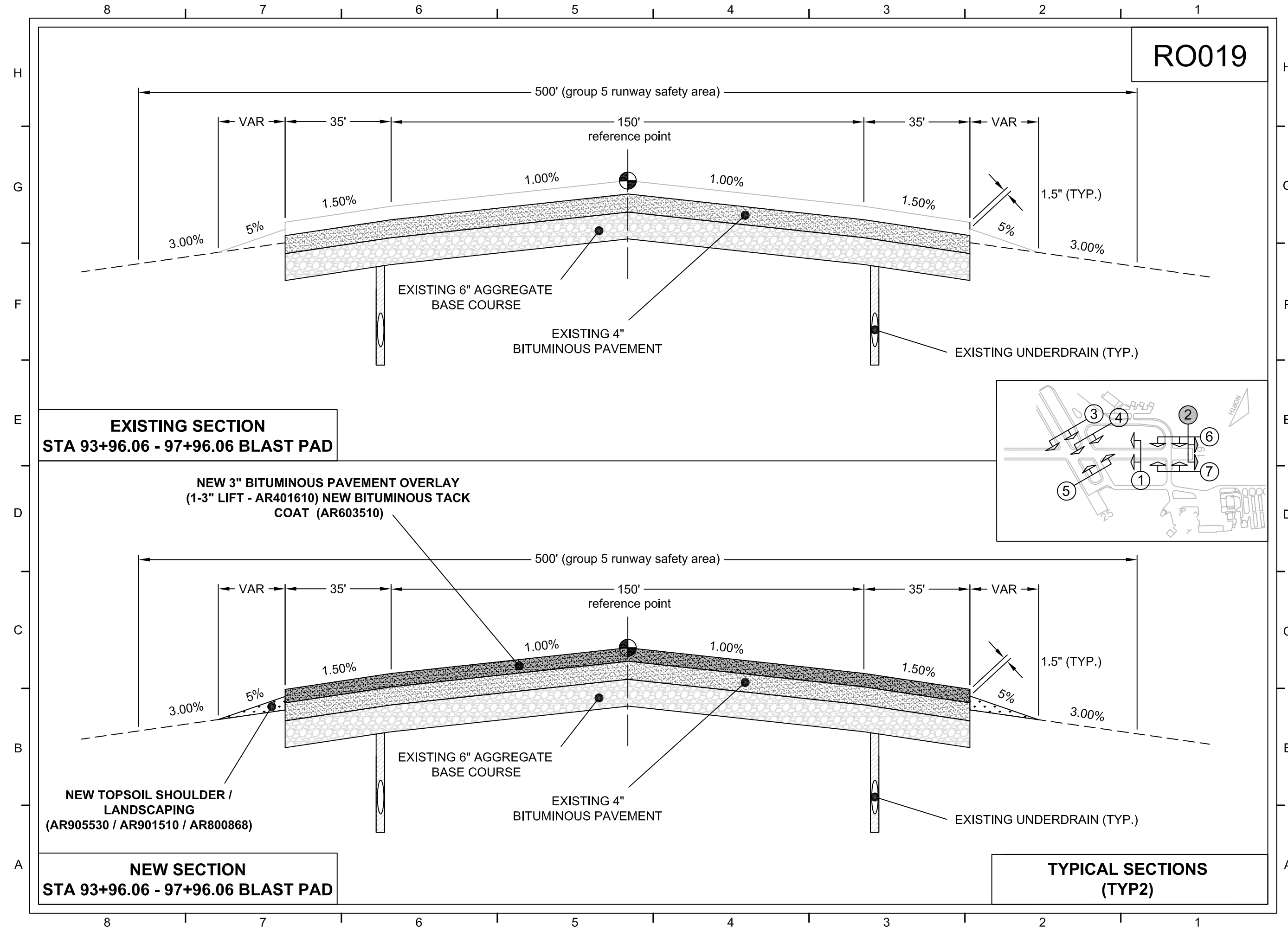
SHEET 4 OF 67 SHEETS

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TYPICAL SECTIONS
(TYP1)

8 7 6 5 4 3 2 1



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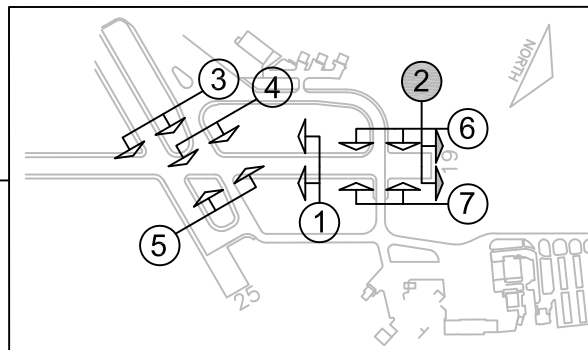
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PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
TYPICAL SECTION
BLAST PAD (TYP2)

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
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PROJECT NO.: 10258-03-00/06
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SHEET NO. 5	REVISION
SHEET 5 OF 67 SHEETS	

TYPICAL SECTIONS
(TYP2)



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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

TYPICAL SECTION
RWY 7/25 (TYP3)

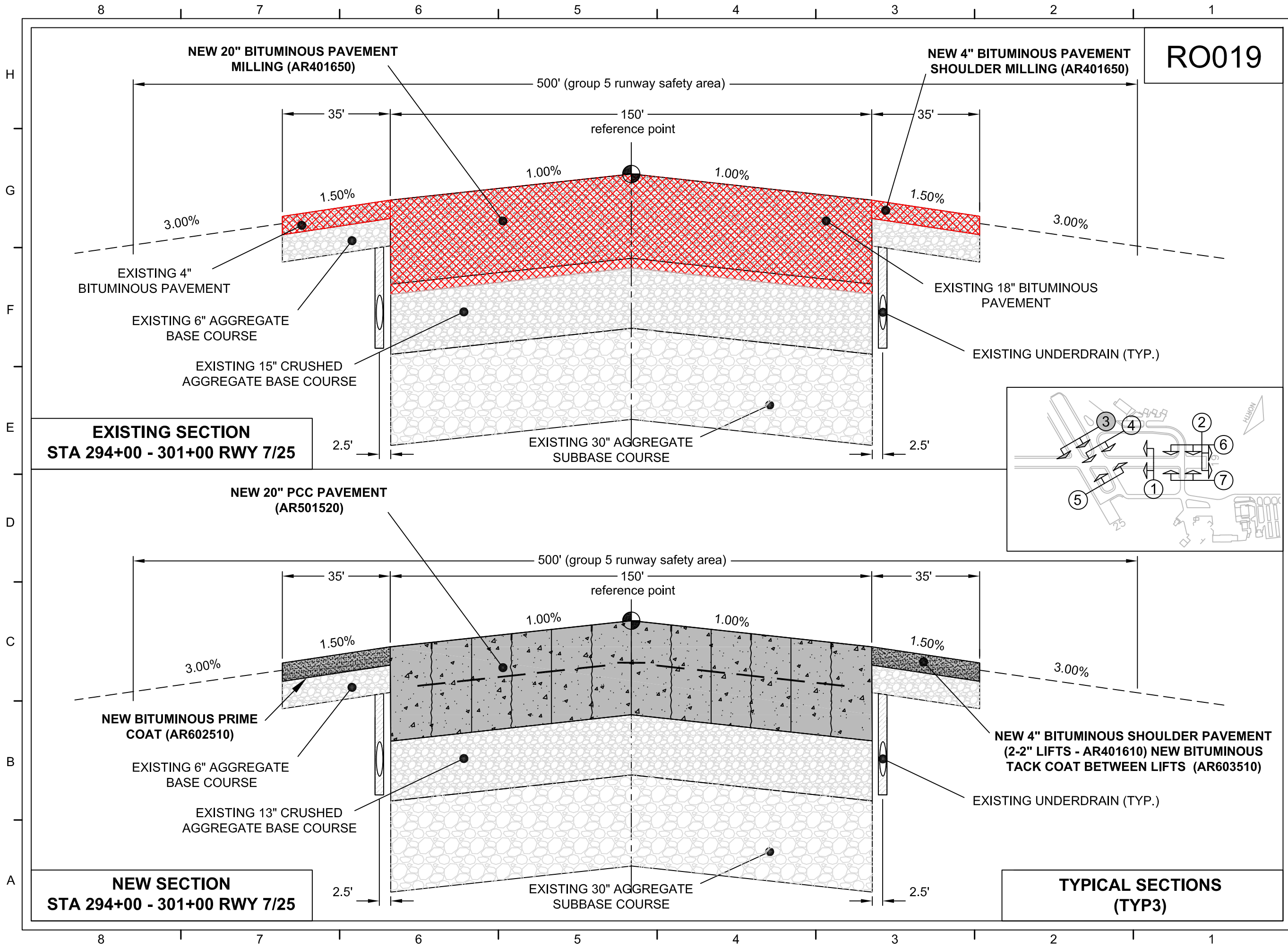
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SHEET NO. **6** REVISION

SHEET 6 OF 67 SHEETS

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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

TYPICAL SECTION
TXY F WEST (TYP4)

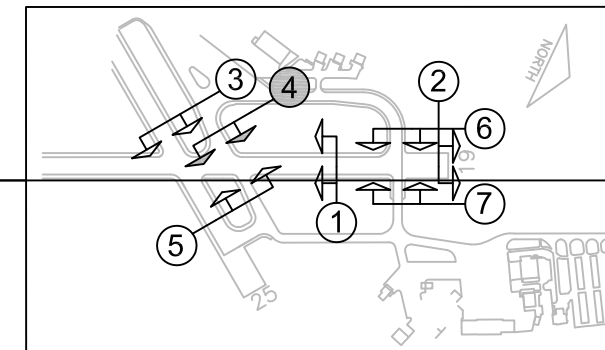
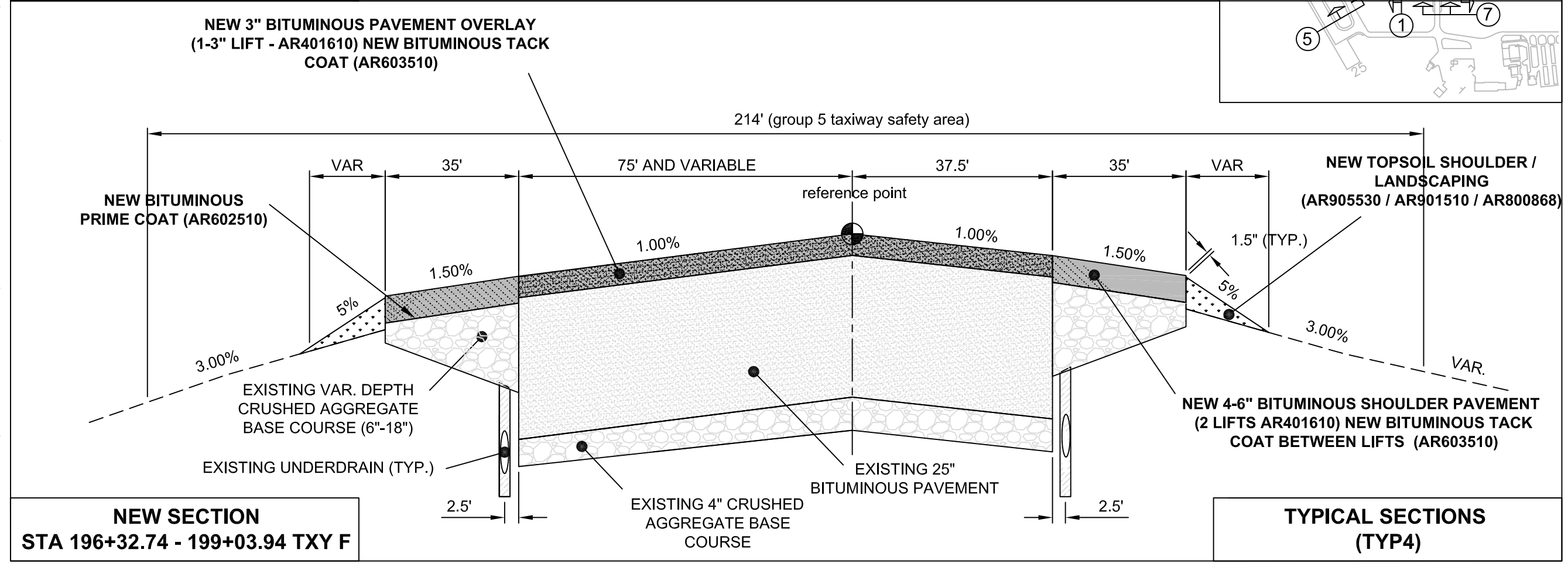
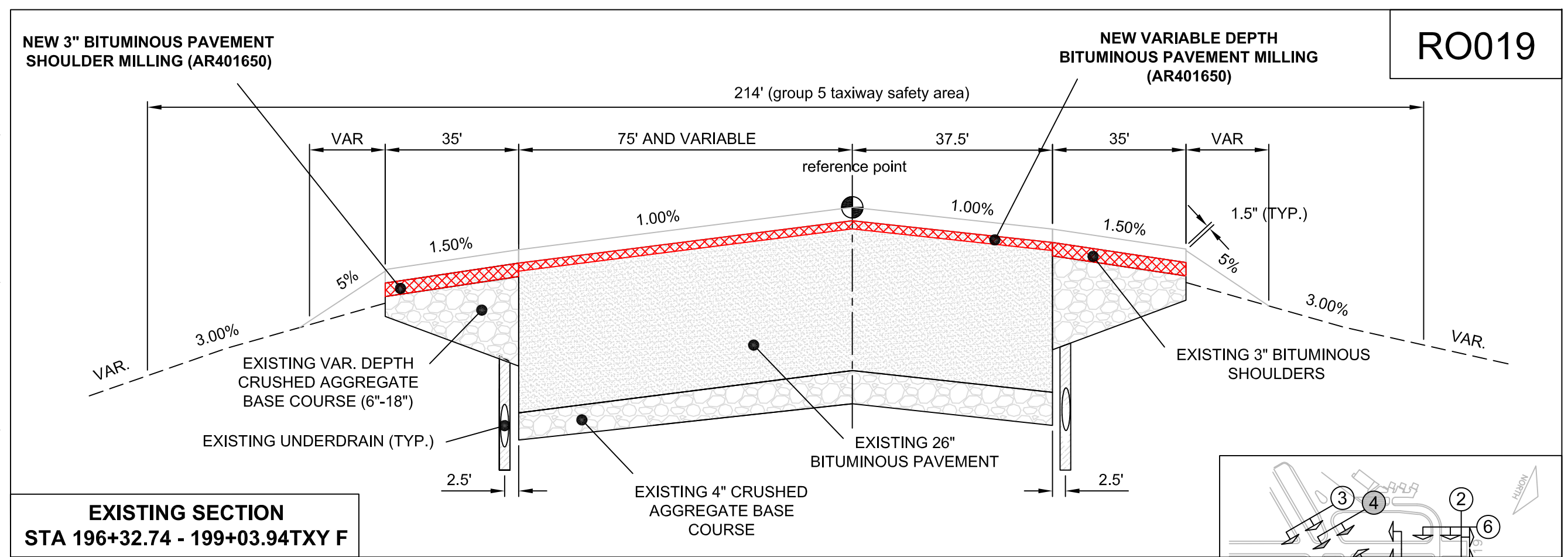
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DATE: APRIL 22, 2011

SHEET NO. **7** REVISION

SHEET 7 OF 67 SHEETS

RO019



TYPICAL SECTIONS (TYP4)



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IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

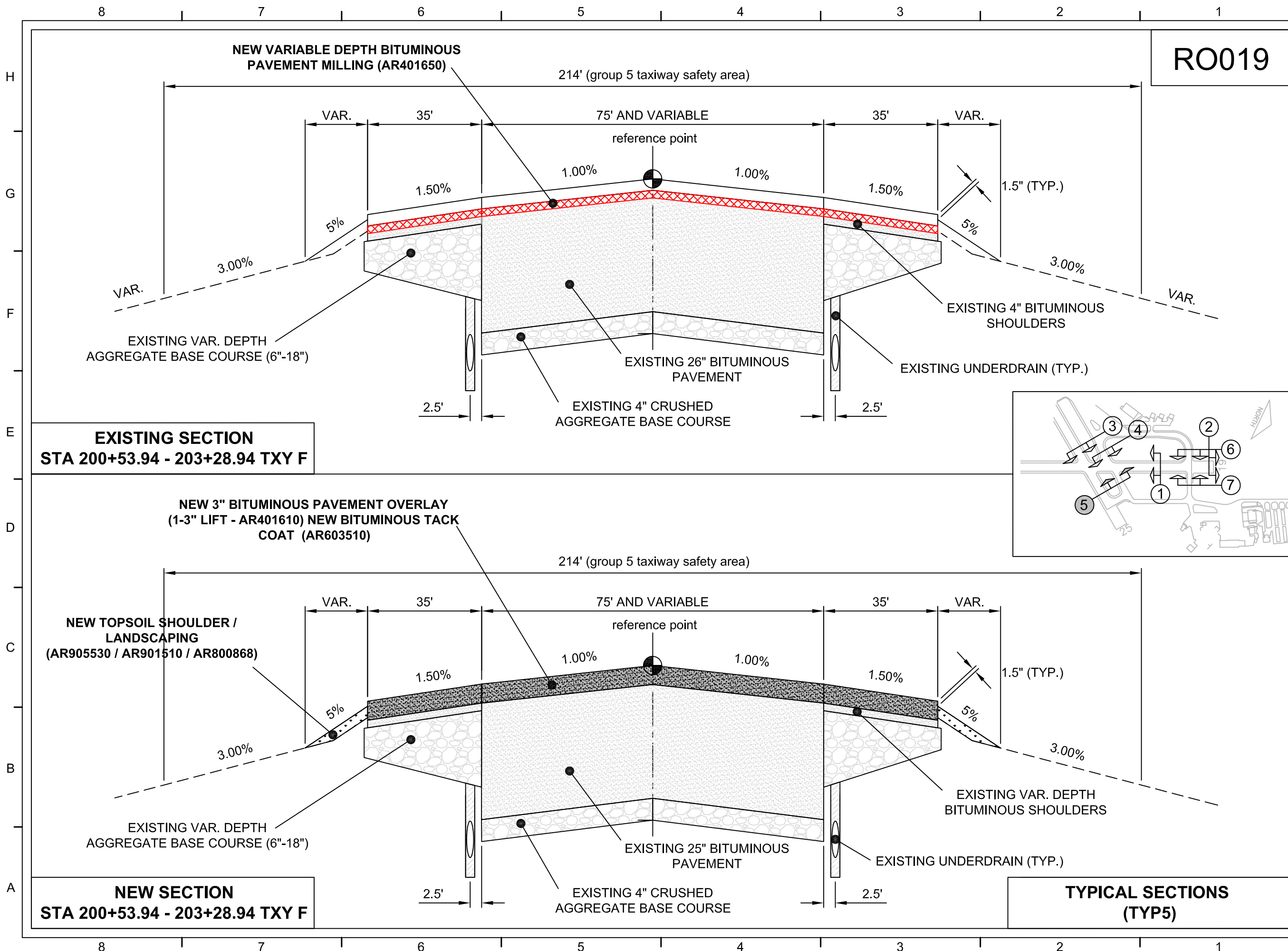
TYPICAL SECTION
TXY F EAST (TYP5)

DESIGNED:	DRAWN:	CHECKED:
CMT-RFD	CMT-RFD	CMT-ARR

PROJECT NO.:	10258-03-00/06
DATE:	APRIL 22, 2011

SHEET NO.	8	REVISION
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SHEET 8 OF 67 SHEETS



RO019

EXISTING SECTION
STA 200+53.94 - 203+28.94 TXY F

NEW SECTION
STA 200+53.94 - 203+28.94 TXY F

TYPICAL SECTIONS
(TYP5)



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Rockford, Illinois

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AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**TYPICAL SECTION
TXY B (TYP6)**

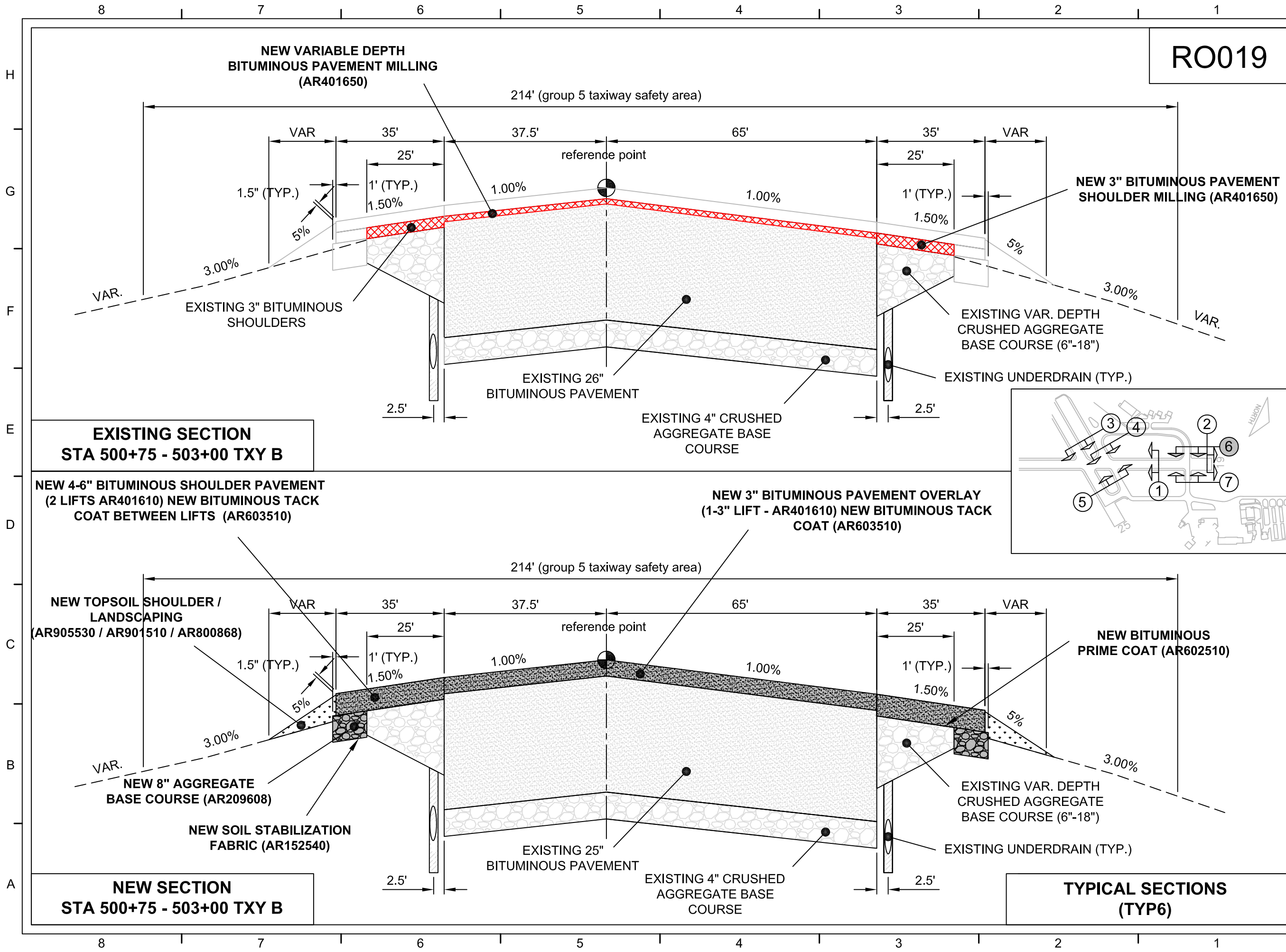
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PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **9** REVISION

SHEET 9 OF 67 SHEETS

RO019



**EXISTING SECTION
STA 500+75 - 503+00 TXY B**

**NEW 4-6" BITUMINOUS SHOULDER PAVEMENT
(2 LIFTS AR401610) NEW BITUMINOUS TACK
COAT BETWEEN LIFTS (AR603510)**

**NEW 3" BITUMINOUS PAVEMENT OVERLAY
(1-3" LIFT - AR401610) NEW BITUMINOUS TACK
COAT (AR603510)**

**NEW TOPSOIL SHOULDER /
LANDSCAPING
(AR905530 / AR901510 / AR800868)**

**NEW 8" AGGREGATE
BASE COURSE (AR209608)**

**NEW SOIL STABILIZATION
FABRIC (AR152540)**

**NEW SECTION
STA 500+75 - 503+00 TXY B**

**TYPICAL SECTIONS
(TYP6)**



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AIP Proj: 3-17-0088-XX
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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

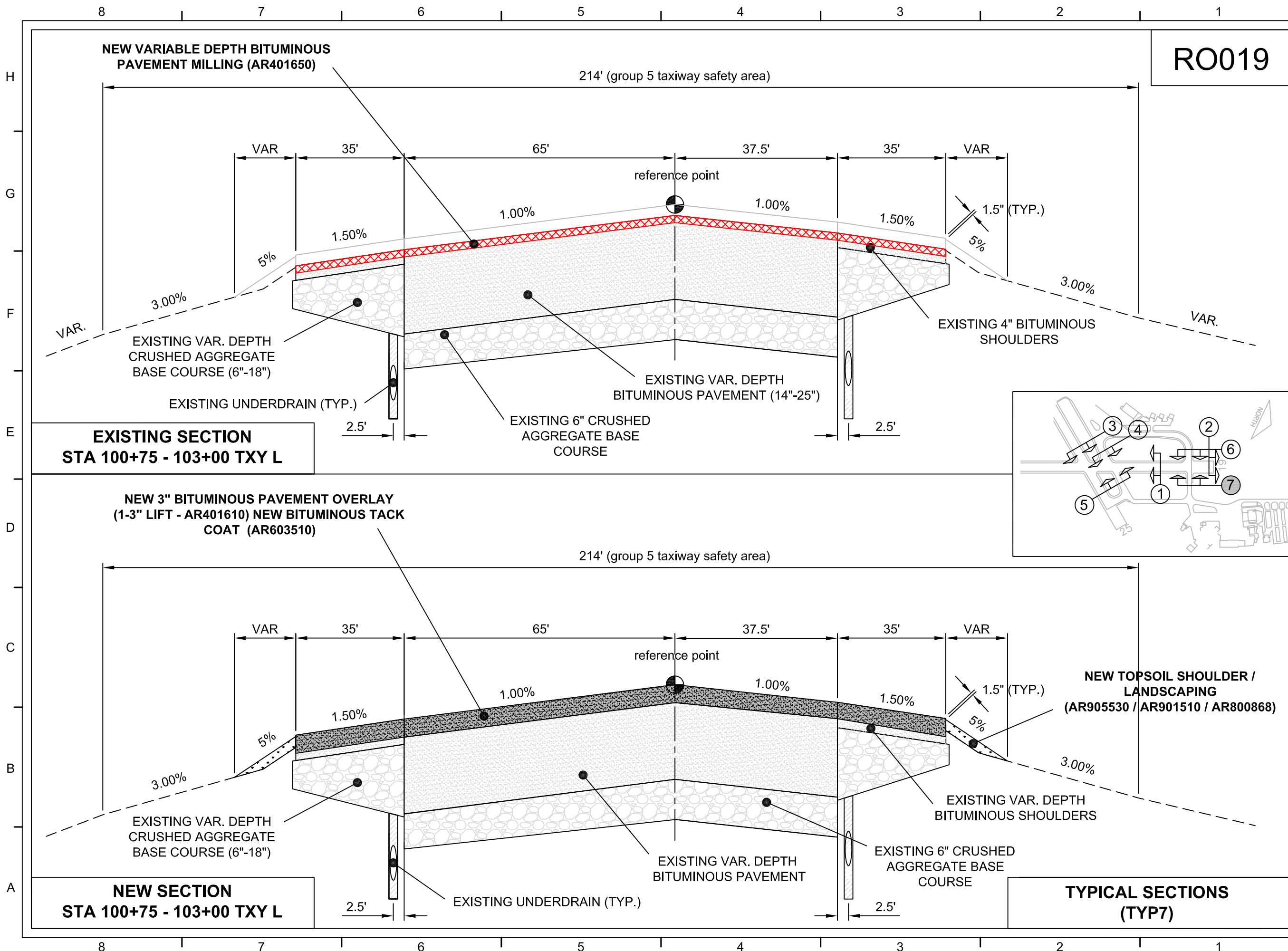
TYPICAL SECTION
TXY L (TYP7)

DESIGNED:	DRAWN:	CHECKED:
CMT-RFD	CMT-RFD	CMT-ARR

PROJECT NO.:	10258-03-00/06
DATE:	APRIL 22, 2011

SHEET NO.	10	REVISION
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SHEET 10 OF 67 SHEETS





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Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**PHASING PLAN
SUMMARY (PPM1)**

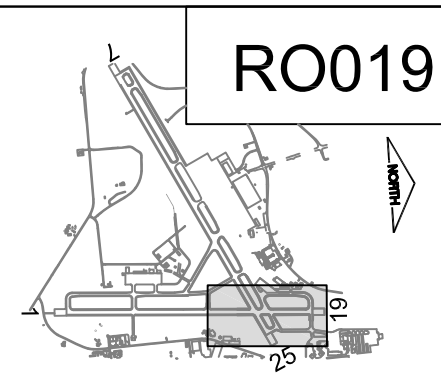
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PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **11** REVISION

SHEET 11 OF 67 SHEETS

RO019

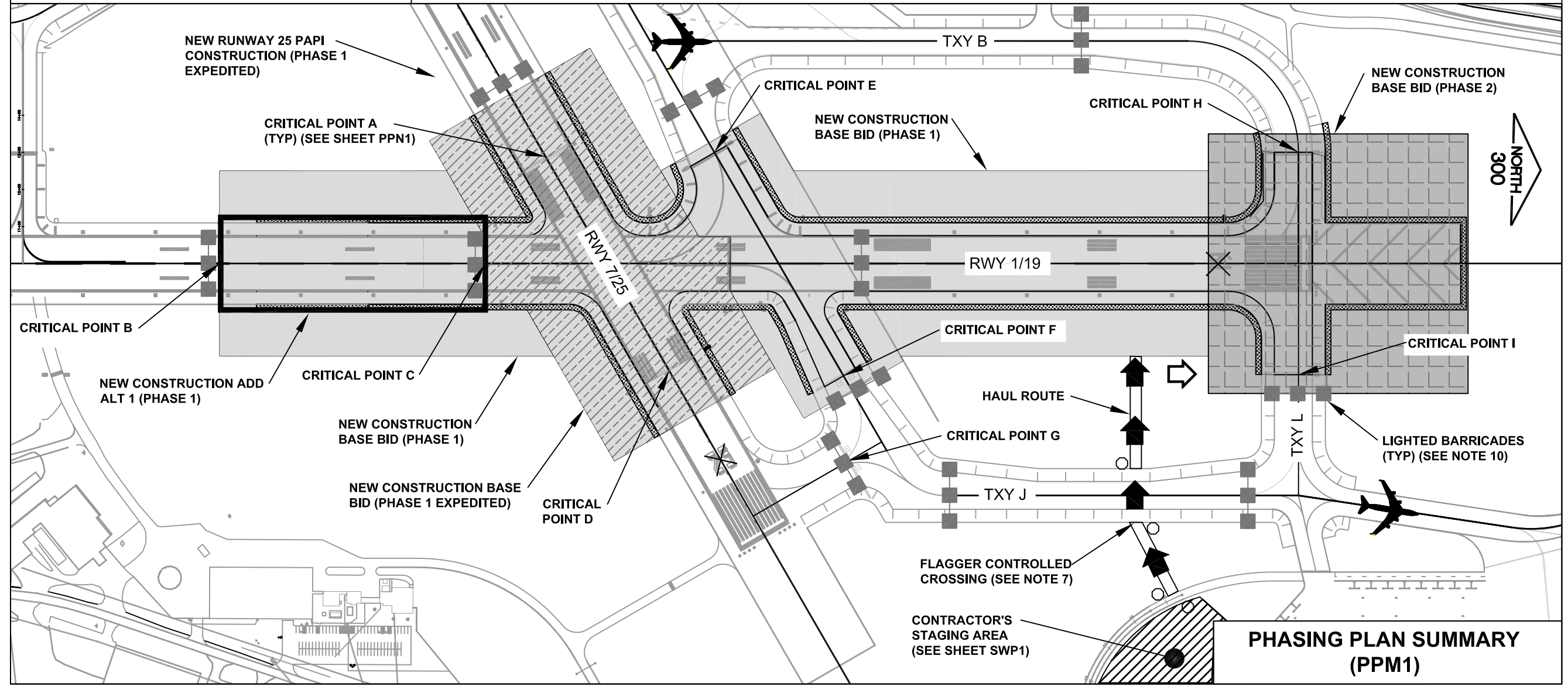


LEGEND

- PHASE 1 CONSTRUCTION
- PHASE 1 CONSTRUCTION EXPEDITED
- PHASE 2 CONSTRUCTION
- LIGHTED BARRICADES
- CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD (PHASE 1)
- CONTRACTOR'S CONSTRUCTION ACCESS/HAUL ROAD (PHASE 2)
- AOA
- AIRCRAFT MOVEMENT AREAS
- TEMPORARY STOP SIGNS
- ADD. ALT. 1

NOTES

1. SIMULTANEOUS RUNWAY CLOSURES WILL NOT BE PERMITTED.
2. SHOULD ONE OR BOTH OF THE ADDITIVE ALTERNATES BE AWARDED CONSTRUCTION ACTIVITIES SHALL TAKE PLACE CONCURRENTLY WITHIN THE PHASES SHOWN.
3. ALL PROPOSED HOMERUN CABLING IS NOT SHOWN FOR CLARITY. THIS WORK SHALL BE INCLUDED IN THE BASE BID PHASE 1.
4. ANY TIME THE HAUL ROUTE CROSSES EXISTING AIRPORT PAVEMENT, THE PAVEMENT SHALL BE SWEEPED AND CLEANED AS REQUIRED TO THE SATISFACTION OF THE AIRPORT AND/OR RESIDENT ENGINEER.
5. DURING ALL PHASES OF CONSTRUCTION, AIRFIELD ACCESS FOR T-HANGAR TENANTS SHALL BE MAINTAINED.
6. ALL WORK DURING PHASE 1, PHASE 1 EXPEDITED AND PHASE 2 WILL REQUIRE EITHER TEMPORARY CLOSURE OR RELOCATION OF THE RUNWAY THRESHOLD, EITHER RUNWAY 7/25, RUNWAY 1/19 OR BOTH. ONLY DAILY CLOSURES OF RUNWAY WILL BE PERMITTED, SEE NOTE 1. NIGHTTIME CLOSURES WILL NOT BE ALLOWED. ALL WORK SHALL BE EXPEDITED TO MINIMIZE IMPACT ON AIRPORT OPERATIONS.
7. THE CONTRACTOR SHALL EMPLOY A FULL-TIME FLAGGER AT THE LOCATION SHOWN IN THE PLANS. THE FLAGGER SHALL BE IN RADIO CONTACT WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AT ALL TIMES AND SHALL DIRECT CONSTRUCTION TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. THE CONTRACTOR SHALL PROVIDE A RADIO FOR THE FLAGGER. BEFORE CONSTRUCTION BEGINS, AIRPORT AND ATCT PERSONNEL SHALL APPROVE THE CONTRACTOR'S PROPOSED FLAGGER. THE COST OF THE FLAGGER SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL SANITARY SEWER WORK SHALL BE COMPLETED DURING PHASE 1 AND PHASE 1 EXPEDITED. ALL WORK WITHIN THE RUNWAY 25 SAFETY AREA AND TAXIWAY F SAFETY AREA SHALL BE COMPLETED DURING PHASE 1 EXPEDITED TO MINIMIZE THE IMPACT ON AIRPORT OPERATIONS.
9. IF ADD. ALT. 1 IS AWARDED, CONSTRUCTION OF THE RIAT ROAD (SEE SECTION AA1 FOR LOCATION) WILL BE CONSTRUCTED CONCURRENTLY WITH PHASE 1 AND PHASE 1 EXPEDITED.
10. THE LOCATION OF THE BARRICADES SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER PER THE INDIVIDUAL PHASES. CONSTRUCTION PHASING SHALL REQUIRE THE BARRICADES TO BE REPOSITIONED. ANY REPOSITIONING OF THE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
11. ITEM AR800881 RUNWAY 7 PAPI PCU RELOCATION IS NOT SHOWN FOR CLARITY. THE WORK INCLUDED IN THIS ITEM SHALL BE PERFORMED AND COMPLETED DURING PHASE 1.



**PHASING PLAN SUMMARY
(PPM1)**

PHASING PLAN PROJECT NOTES:

1. APPROVED PROGRESS SCHEDULE:

PRIOR TO THE START OF CONSTRUCTION, AN APPROVED PROGRESS SCHEDULE SHALL BE EXECUTED BY THE RESIDENT ENGINEER AND THE CONTRACTOR. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES, INCLUDING ALL AOA WORK REQUIRING DAYTIME ONLY CLOSURES OF TAXIWAYS AND RUNWAYS. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES 3 WORKING DAYS PRIOR TO START OF CONSTRUCTION.

2. NOTAM (NOTICE TO AIRMEN) COORDINATION:

THE RESIDENT ENGINEER SHALL COORDINATE NOTAM AND FAA FACILITY COORDINATION WITH AIRPORT / FAA PERSONNEL.

3. CONSTRUCTION SITE ACCESS AND STAGING AREA:

THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED. THIS WORK AREA SHALL BE RESTRICTED FROM ACTIVE AOA AREAS WITH THE BARRICADE/FENCING PERIMETERS SHOWN.

THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO THE CITY OF ROCKFORD AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.

4. CONSTRUCTION EQUIPMENT FLAGGING / BEACON REQUIREMENTS:

THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.

5. GATE SECURITY:

THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE DEPUTY DIRECTOR OF OPERATIONS. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

6. CONSTRUCTION OUTSIDE OF BARRICADED AREAS REQUIRING TAXIWAY / RUNWAY CLOSURES:

WORK OUTSIDE THE BARRICADED LINES WITHIN THE AOA AREAS SHOWN SHALL REQUIRE TEMPORARY DAYTIME ONLY CLOSURES OF THE AFFECTED TAXIWAYS OR RUNWAYS. THIS WORK SHALL BE EXPEDITED AND PRIORITIZED TO MINIMIZE CLOSURE TIME OF THE ACTIVE PAVEMENTS. CONSTRUCTION ACTIVITIES WILL NOT BE PERMITTED WITHIN 250' OF CENTERLINE ON ALL ACTIVE RUNWAYS AND WITHIN 160' OF CENTERLINE ON ALL ACTIVE TAXIWAYS. IN ADDITION, THIS WORK WILL REQUIRE ALL CREWS TO SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO TO COMMUNICATE DIRECTLY WITH THE ATCT (AIR TRAFFIC CONTROL TOWER). THE OPERATOR OF THE AIRPORT RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND BE TUNED INTO THE GROUND CONTROL FREQUENCY AT ALL TIMES.

7. UNAUTHORIZED ACCESS TO AIRFIELD:

THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

CRITICAL POINTS

POINT NO.	LATITUDE	LONGITUDE	ELEV.*	POINT NO.	LATITUDE	LONGITUDE	ELEV.*
A	42° 12' 01.06"	89° 05' 25.21"	758.50	F	42° 12' 08.41"	89° 05' 16.38"	758.50
B	42° 11' 52.15"	89° 05' 22.49"	758.00	G	42° 12' 08.34"	89° 05' 13.33"	759.00
C	42° 11' 59.20"	89° 05' 21.60"	759.00	H	42° 12' 21.03"	89° 05' 22.80"	759.00
D	42° 12' 03.80"	89° 05' 17.16"	760.50	I	42° 12' 20.47"	89° 05' 14.88"	760.00
E	42° 12' 05.55"	89° 05' 24.78"	756.50				

* ELEVATION INCLUDES 25' ANTICIPATED EQUIPMENT HEIGHT

ANTICIPATED PROJECT START DATE:

APPROXIMATE SCHEDULE SHOWN ASSUMING THE AVAILABILITY OF PROJECT FUNDING. THERE IS NO GUARANTEE THAT THIS PROJECT WILL BE AWARDED TO ACCOMMODATE A 2011 SCHEDULE. IF THE FUNDING IS DELAYED, IT IS POSSIBLE THAT THE PROJECT WILL NOT BEGIN UNTIL THE 2012 CONSTRUCTION SEASON. PRIOR TO THE START OF CONSTRUCTION, THE SCHEDULE WILL BE REVISED ACCORDINGLY.

PHASE	WORK AREA	ALLOWABLE WORK PERIODS	AIRPORT OPERATIONAL RESTRICTIONS	ANTICIPATED SCHEDULE
1	RUNWAY 1/19 STA. 71+60 TO 91+15, TAXIWAY F EAST AND WEST CONNECTOR, RUNWAY 7/25 (PHASE 1 EXPEDITED) IF AWARDED ADDITIVE ALTERNATE 1 AND 2 APPLICABLE AREAS	7:00 A.M. - 7:00 P.M.	MAINTAIN AIRFIELD ACCESS TO T-HANGAR TENANTS DURING CONSTRUCTION VIA TAXIWAY B, TAXIWAY L AND TAXIWAY J NORTH OF TAXIWAY L TAXIWAY CLOSURES INCLUDE TAXIWAY F EAST OF TAXIWAY B TO CONNECTOR F. TEMPORARY THRESHOLD RUNWAY 19	JULY 29 - OCTOBER 14, 2011 (78 CALENDAR DAYS)
1 (EXPEDITED)	AREA WITHIN RUNWAY 7/25 RSA	7:00 A.M. - 7:00 P.M.	MAINTAIN AIRFIELD ACCESS TO T-HANGAR TENANTS DURING CONSTRUCTION VIA TAXIWAY B, TAXIWAY L AND TAXIWAY J NORTH OF TAXIWAY L. TAXIWAY CLOSURES INCLUDE TAXIWAY F EAST OF TAXIWAY B, TAXIWAY J SOUTH OF TAXIWAY L. TEMPORARY THRESHOLD RUNWAY 25	AUGUST 8 - SEPTEMBER 4, 2011 (28 CALENDAR DAYS)
2	RUNWAY 1/19 STA. 91+15 TO 98+15, TAXIWAY B WEST CONNECTOR TAXIWAY L EAST CONNECTOR. IF AWARDED ADDITIVE ALTERNATE 1 AND 2 APPLICABLE AREAS	7:00 A.M. - 7:00 P.M.	MAINTAIN AIRFIELD ACCESS TO T-HANGAR TENANTS DURING CONSTRUCTION VIA TAXIWAY F AND TAXIWAY J. TAXIWAY CLOSURES INCLUDE TAXIWAY B NORTH OF TAXIWAY F AND TAXIWAY L. TEMPORARY THRESHOLD RUNWAY 19	OCTOBER 15 - NOVEMBER 11, 2011 (28 CALENDAR DAYS)
				106 CALENDAR DAYS

IF ADD ALT #1 IS AWARDED, 14 ADDITIONAL CALENDAR WILL BE AWARDED (TOTAL PROJECT CALENDAR DAYS = 120 DAYS).
IF ADD ALT #1 AND #2 ARE AWARDED, 31 ADDITIONAL CALENDAR DAYS WILL BE AWARDED (TOTAL PROJECT CALENDAR DAYS = 137 DAYS)

PHASING PLAN GENERAL NOTES:

1. SUGGESTED SEQUENCE OF CONSTRUCTION:

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS.

2. STAGING AREA RESTORATION:

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

3. HAUL ROUTE / HAUL ROUTE RESTORATION:

THE CONTRACTOR SHALL CONSTRUCT THE HAUL ROUTE AS SHOWN IN THESE PLANS AND SHALL BE PAID FOR ONCE AS AR150540 - HAUL ROUTE. ANY OTHER HAUL ROUTE(S) SHALL BE INCIDENTAL TO AR150540. THE COST OF MAINTAINING THE HAUL ROUTE(S) SHALL BE INCIDENTAL TO AR150540. ALL HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO AR150540 - HAUL ROUTE.

4. AIRPORT APPROVAL OF PHASING:

THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

5. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

(NOTES CONTINUED ON PHASING PLAN DETAILS - PPD1)

RO019



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

PHASING PLAN
NOTES (PPN1)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **12** REVISION

SHEET 12 OF 67 SHEETS

**PHASING PLAN NOTES
(PPN1)**



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**PHASING PLAN
DETAILS (PPD1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **13** REVISION

SHEET 13 OF 67 SHEETS

RO019

PHASING PLAN GENERAL NOTES (CONTD):

(NOTES CONTINUED ON PHASING PLAN NOTES - PPN1)

6. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:
WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

7. EXISTING UTILITY COORDINATION:
COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.

8. TRAFFIC CONTROL PAYMENT:
PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE PAID FOR AS AR800825 - TRAFFIC CONTROL AND PROTECTION. TYPE 2 BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE PLACED ON 15' CENTERS AND HAVE ORANGE CONSTRUCTION FENCING BETWEEN EACH SET OF BARRICADES. TYPE 2 BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE ENGINEER FOR WORK ADJACENT TO THE EXPEDITED WORK AREA. LOW PROFILE BARRICADES SHALL BE USED ON ALL PAVEMENT AREAS OR AS DIRECTED BY THE ENGINEER. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL TYPE II AND TYPE III BARRICADES SHALL CONFORM TO IDOT STANDARD DETAIL 702001. ALL PAVEMENT DROP-OFFS GREATER THAN 24" REQUIRE TYPE II BARRICADES WITH EXTENDED LEGS. FOR AIRSIDE BARRICADE PLACEMENT, SEE SEQUENCE OF CONSTRUCTION SHEETS. FOR ROADWAY PROTECTION, SEE TEMPORARY TRAFFIC CONTROL PLAN AND GENERAL NOTES AND DETAILS SHEETS.

9. DRIVERS TRAINING AND BADGING:
DRIVER'S TRAINING AND BADGING SHALL BE REQUIRED FOR THE CONTRACTOR'S SUPERVISORY PERSONNEL, OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. THE DRIVER'S TRAINING AND BADGING OF THE INITIAL SUPERVISORY PERSONNEL MUST BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.

10. DUST CONTROL REQUIREMENTS:
THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

11. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2E):
ALL WORK SHALL CONFORM TO AC 150/5370-2E OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION. THIS AC IS AVAILABLE AT www.faa.gov/arp/pdf/5370-2e.pdf.

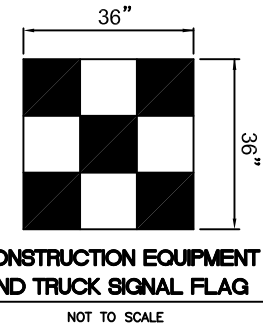
12. STAGING AREA:
THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

13. AIRFIELD LIGHTING COORDINATION:
THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.

14. WEEKLY COORDINATION MEETINGS:
WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. AS A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

15. TEMPORARY CABLES:
ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT: • REHABILITATE TAXIWAY J AND L

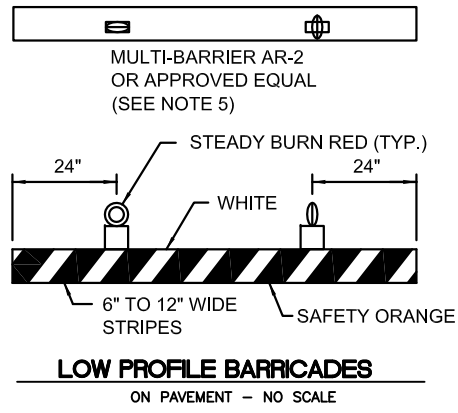


1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE A MATERIAL APPROVED BY THE ENGINEER AND THE AIRPORT.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
4. MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET OR AS DIRECTED BY THE ENGINEER AND AIRPORT. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



CLOSED RUNWAY MARKER DETAIL
ON PAVEMENT - NO SCALE



LOW PROFILE BARRICADES
ON PAVEMENT - NO SCALE

1. LOW PROFILE BARRICADES SHALL BE PLACED 15' ON CENTER AT THE EDGE OF ALL ACTIVE TAXIWAY PAVEMENTS WHERE THERE IS AN EDGE DROP-OFF GREATER THAN 3" AT THE EDGE OF THE TAXIWAY. BARRICADES SHALL BE WEIGHTED WITH A MINIMUM OF 6 SAND BAGS TO PREVENT THEM FROM BEING BLOWN OVER.
2. LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
3. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
4. BARRICADES TO BE PLACED WITH A MAXIMUM OF 15' SPACING BETWEEN ENDS OF BARRICADES ALONG OPERATIONAL TAXIWAY PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER.
5. ALTERNATE LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
6. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
7. A MINIMUM OF 60 LOW PROFILE BARRICADES SHALL BE ON SITE AT ALL TIMES. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

**PHASING PLAN DETAILS
(PPD1)**



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

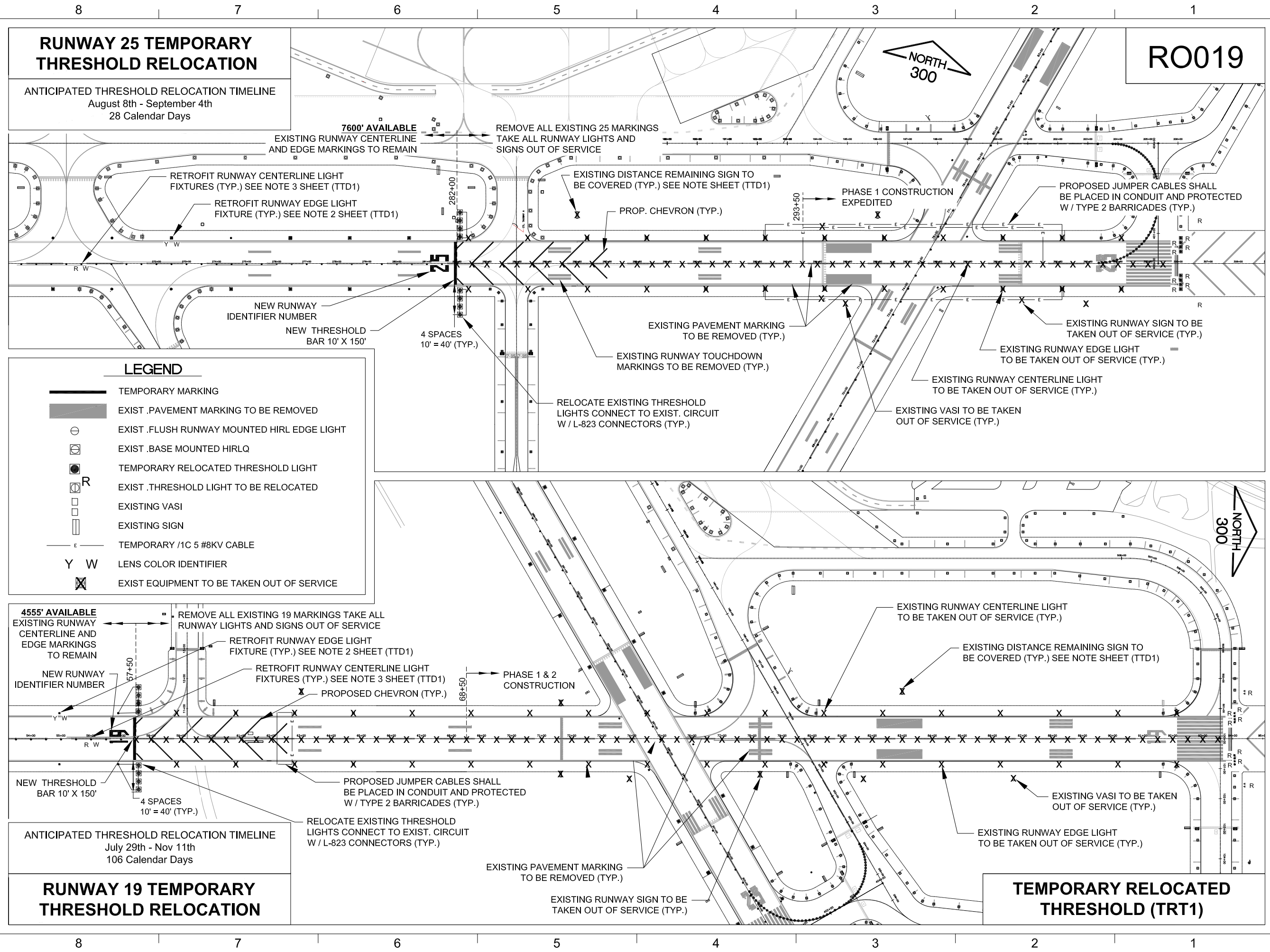
SHEET TITLE:
**TEMPORARY RELOCATED
THRESHOLD (TRT1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **14** REVISION

SHEET 14 OF 67 SHEETS



**RUNWAY 25 TEMPORARY
THRESHOLD RELOCATION**

ANTICIPATED THRESHOLD RELOCATION TIMELINE
August 8th - September 4th
28 Calendar Days

RO019

LEGEND

- TEMPORARY MARKING
- █ EXIST .PAVEMENT MARKING TO BE REMOVED
- ⊙ EXIST .FLUSH RUNWAY MOUNTED HIRLQ
- ⊙ EXIST .BASE MOUNTED HIRLQ
- ⊙ TEMPORARY RELOCATED THRESHOLD LIGHT
- ⊙ R EXIST .THRESHOLD LIGHT TO BE RELOCATED
- EXISTING VASI
- EXISTING SIGN
- e — TEMPORARY /1C 5 #8KV CABLE
- Y W LENS COLOR IDENTIFIER
- ⊗ EXIST EQUIPMENT TO BE TAKEN OUT OF SERVICE

4555' AVAILABLE
EXISTING RUNWAY
CENTERLINE AND
EDGE MARKINGS
TO REMAIN

REMOVE ALL EXISTING 19 MARKINGS TAKE ALL
RUNWAY LIGHTS AND SIGNS OUT OF SERVICE

ANTICIPATED THRESHOLD RELOCATION TIMELINE
July 29th - Nov 11th
106 Calendar Days

**RUNWAY 19 TEMPORARY
THRESHOLD RELOCATION**

**TEMPORARY RELOCATED
THRESHOLD (TRT1)**



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**TEMPORARY RELOCATED
THRESHOLD DETAILS
(TTD1)**

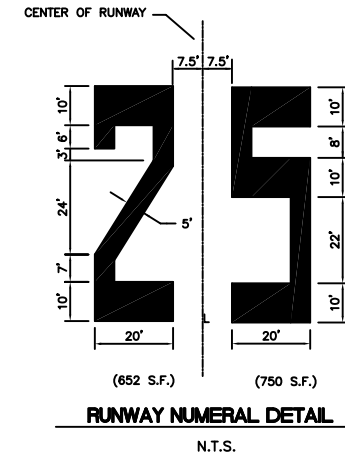
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **15** REVISION

RO019

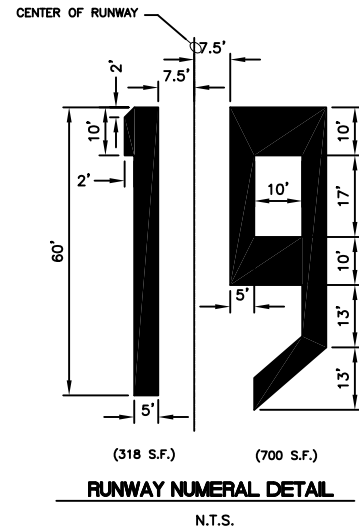
RUNWAY 7/25 TEMPORARY THRESHOLD RELOCATION



TEMPORARY PAVEMENT MARKING (PAINT)	COLOR	TOTAL AREA
1 - THRESHOLD BAR (10'X150')	WHITE	1,500 S.F.
2 - NUMERALS (7,25)	WHITE	1,402 S.F.
5 - CHEVRONS (3' WIDE)	YELLOW	1,061 S.F.
6" BLACK BORDER (ALL MARKINGS)	BLACK	2,494 S.F.
TOTAL AREA =		6,457 S.F.
PAVEMENT MARKING REMOVALS	EXIST. DIMENSION	TOTAL AREA
12 - THRESHOLD MARKERS	5.75' X 150'	10,350 S.F.
18 - TOUCHDOWN ZONE MARKERS	6' X 75'	8,100 S.F.
2 - AIMING POINT MARKERS	30' X 150'	9,000 S.F.
17 - CENTERLINE STRIPES	3' X 120'	6,120 S.F.
2 - RUNWAY EDGE STRIPES	3' X 3,650'	21,900 S.F.
2 - NUMERALS (1,9)	(SEE DETAIL)	1,061 S.F.
6" BLACK BORDER (ALL MARKINGS)	6" BORDER	11,425 S.F.
TOTAL AREA =		67,956 S.F.

(INCIDENTAL TO TEMPORARY RELOCATED THRESHOLD)

RUNWAY 1/19 TEMPORARY THRESHOLD RELOCATION

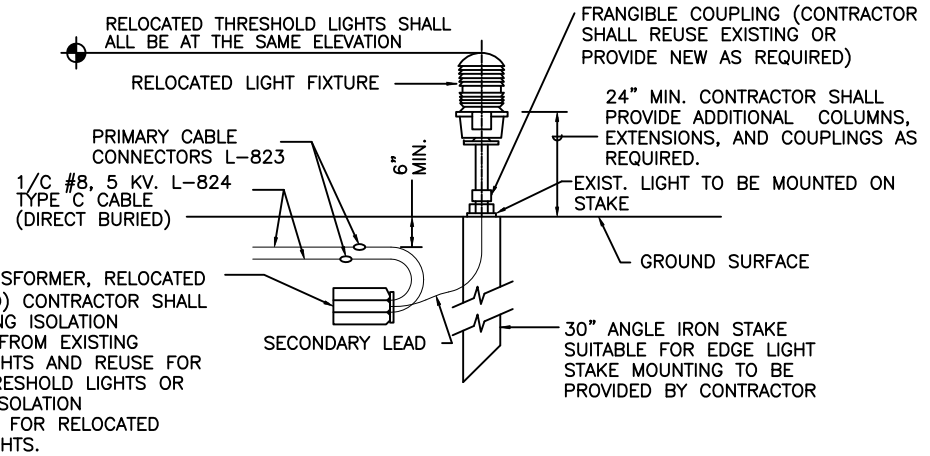


TEMPORARY PAVEMENT MARKING (PAINT)	COLOR	TOTAL AREA
1 - THRESHOLD BAR (10'X150')	WHITE	1,500 S.F.
2 - NUMERALS (1,9)	WHITE	1,018 S.F.
5 - CHEVRONS (3' WIDE)	YELLOW	1,061 S.F.
6" BLACK BORDER (ALL MARKINGS)	BLACK	2,379 S.F.
TOTAL AREA =		5,958 S.F.
PAVEMENT MARKING REMOVALS	EXIST. DIMENSION	TOTAL AREA
12 - THRESHOLD MARKERS	5.75' X 150'	10,350 S.F.
18 - TOUCHDOWN ZONE MARKERS	6' X 75'	8,100 S.F.
2 - AIMING POINT MARKERS	30' X 150'	9,000 S.F.
17 - CENTERLINE STRIPES	3' X 120'	6,120 S.F.
2 - RUNWAY EDGE STRIPES	3' X 3,650'	21,900 S.F.
2 - NUMERALS (1,9)	(SEE DETAIL)	1,061 S.F.
6" BLACK BORDER (ALL MARKINGS)	6" BORDER	11,425 S.F.
TOTAL AREA =		67,956 S.F.

(INCIDENTAL TO TEMPORARY RELOCATED THRESHOLD)

GENERAL NOTES

1. THE TEMPORARY RELOCATED THRESHOLD IS TO BE CONSTRUCTED BY THE CONTRACTOR. THE CONTRACTOR SHALL TAKE CAUTION AS TO NOT DISTURB THIS SYSTEM OR ANY OTHER EXISTING UTILITIES. SHOULD ANY UTILITY BE DISTURBED, THE CONTRACTOR SHALL REPAIR/CORRECT THE DISRUPTION IMMEDIATELY. THE CONTRACTOR IS RESPONSIBLE TO LOCATE AND PROTECT ALL EXISTING UTILITIES.
2. REMOVE YELLOW FILTERS FROM THE RUNWAY EDGE LIGHTS WITHIN THE DISPLACED PORTIONS OF RUNWAY 1/19 AND RUNWAY 7/25. INSTALL YELLOW FILTER IN EDGE LIGHTS 2000 FEET FROM TEMPORARY THRESHOLD. COST INCIDENTAL TO TEMPORARY RELOCATED THRESHOLD.
3. REMOVE RED RUNWAY CENTERLINE LIGHT FIXTURES (1,000' AT 50' CENTERS AND 2,000' AT 100' CENTERS). INSTALL RED CENTERLINE LIGHT FIXTURE CONFIGURATION 3,000' FROM RELOCATED THRESHOLDS.
4. TEMPORARY PAVEMENT MARKING AND MARKING REMOVALS SHALL BE CONSIDERED INCIDENTAL TO TEMPORARY RELOCATED THRESHOLD.
5. ALL MARKINGS SHALL CONFORM TO AC150/5340-1 (LATEST EDITION) STANDARDS FOR AIRPORT MARKINGS.



MOUNTING DETAILS
TEMPORARY THRESHOLD LIGHT
NOT TO SCALE

TEMPORARY RELOCATED THRESHOLD ELECTRICAL WORK TO BE COMPLETED

- RUNWAY DISTANCE REMAINING SIGNS SHALL BE OBSCURED
- TAKE EXISTING RUNWAY EDGE LIGHTS OUT OF SERVICE WITHIN THE CLOSED PORTION OF THE RUNWAYS
- CHANGE FILTERS/FIXTURES IN EXISTING EDGE LIGHTS FOR 2000'
- CHANGE FIXTURES IN EXISTING CTL LIGHTS FOR 3,000'
- RELOCATE RED/GREEN SPLIT LENS THRESHOLD LIGHTS AND TRANSFORMERS AND PROVIDE STAKES .
- CONNECT TEMPORARY THRESHOLD LIGHTS TO EXISTING CIRCUIT
- RELOCATE REILS TO TEMPORARY THRESHOLD
- INSTALL TEMPORARY JUMPERS FOR RUNWAY AND TAXIWAY CIRCUITS AS APPROVED BY THE RESIDENT ENGINEER

**** COST OF ABOVE WORK AND REPLACEMENT TO ORIGINAL CONDITIONS SHALL BE CONSIDERED INCIDENTAL TO TEMPORARY RELOCATED THRESHOLD**

**TEMPORARY RELOCATED
THRESHOLD DETAILS (TTD1)**



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
**STORMWATER
POLLUTION PREVENTION
PLAN (SWP1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06

DATE: APRIL 22, 2011

SHEET NO. **16** REVISION

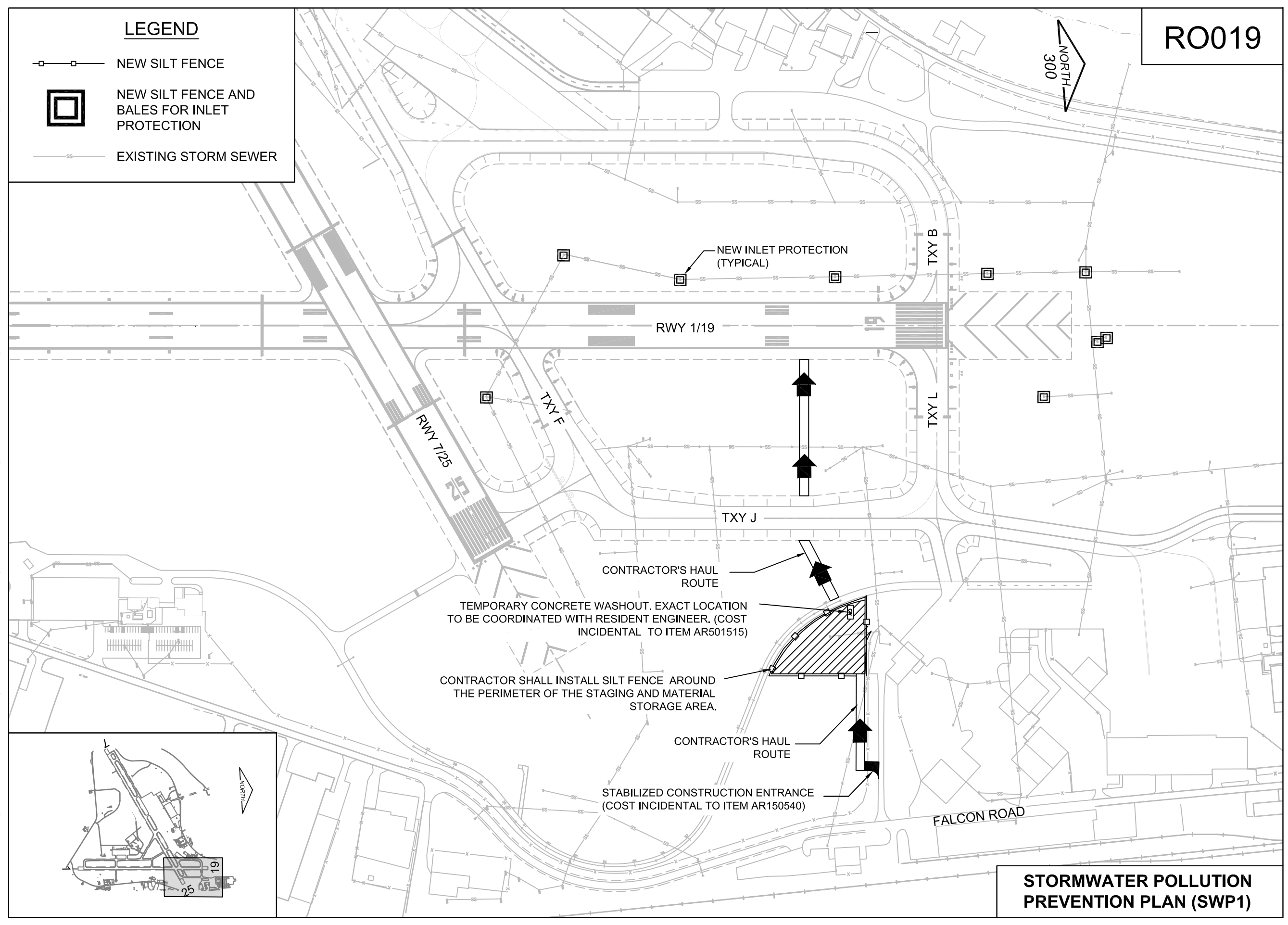
SHEET 16 OF 67 SHEETS

RO019

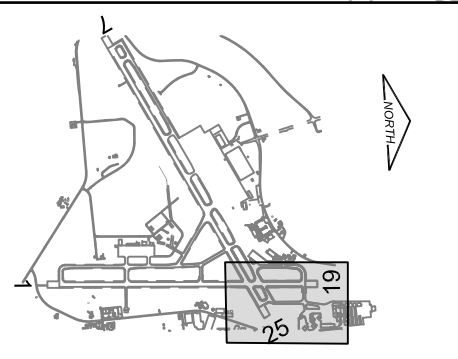


LEGEND

- NEW SILT FENCE
- NEW SILT FENCE AND BALES FOR INLET PROTECTION
- EXISTING STORM SEWER



STORMWATER POLLUTION PREVENTION PLAN (SWP1)



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IL Proj: RFD-4083

Table with 3 columns: REV, DATE, DESCRIPTION. Includes 'APPROVED AS WORKING PLAN BY:' section.

REHABILITATE
RUNWAY 1/19
PHASE 1

STORMWATER
POLLUTION PREVENTION
PLAN NOTES (SWN1)

DESIGNED: CMT-RFD
DRAWN: CMT-RFD
CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. 17
REVISION
SHEET 17 OF 67 SHEETS

RO019

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATING RUNWAYS, TAXIWAYS, AND CONSTRUCTING A RIAT ROAD AT THE CHICAGO ROCKFORD INTERNATIONAL AIRPORT. THE PROJECT INCLUDES GRADING, EXCAVATION, FILL, TOPSOIL PLACEMENT, PAVEMENT CONSTRUCTION, ELECTRICAL, LANDSCAPING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

- 1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE AND INLET PROTECTION.
2. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS.
3. PAVEMENT CONSTRUCTION.
4. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 40.0 ACRES OF WHICH 6.0 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

- 1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE KISHWAUKEE RIVER VIA OVERLAND FLOW AND THROUGH A STORM SEWER SYSTEM.

CONTROLS - EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

- 1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

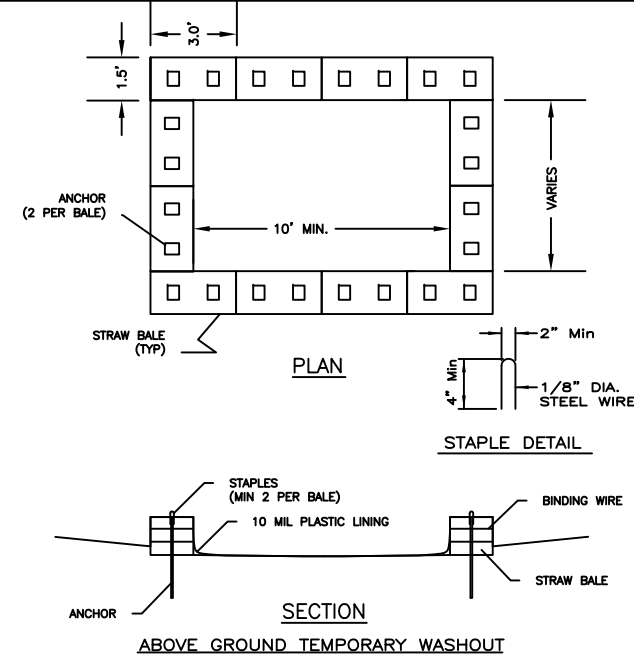
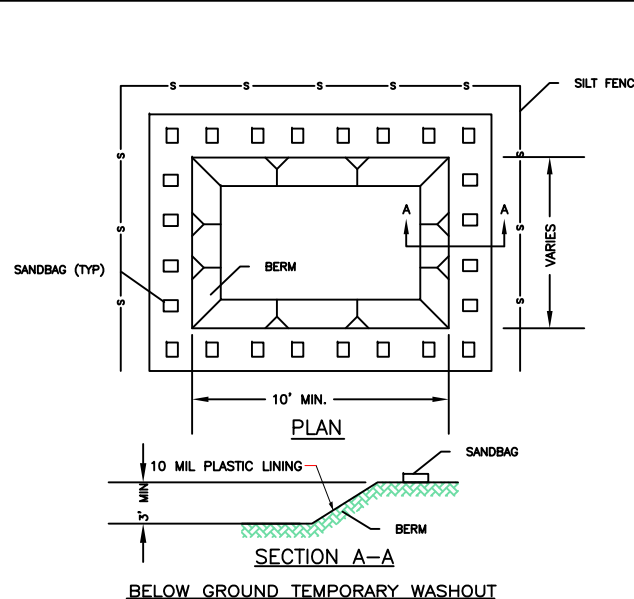
DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

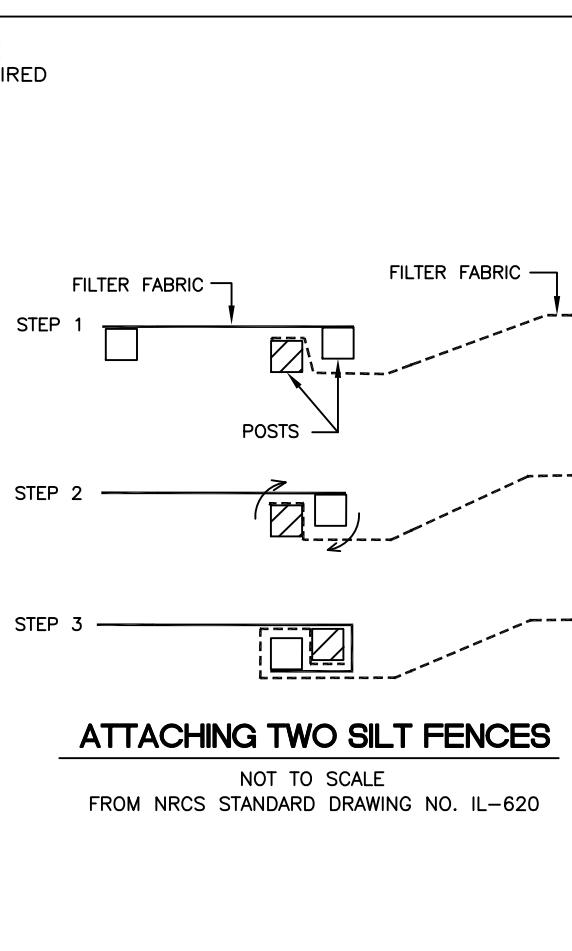
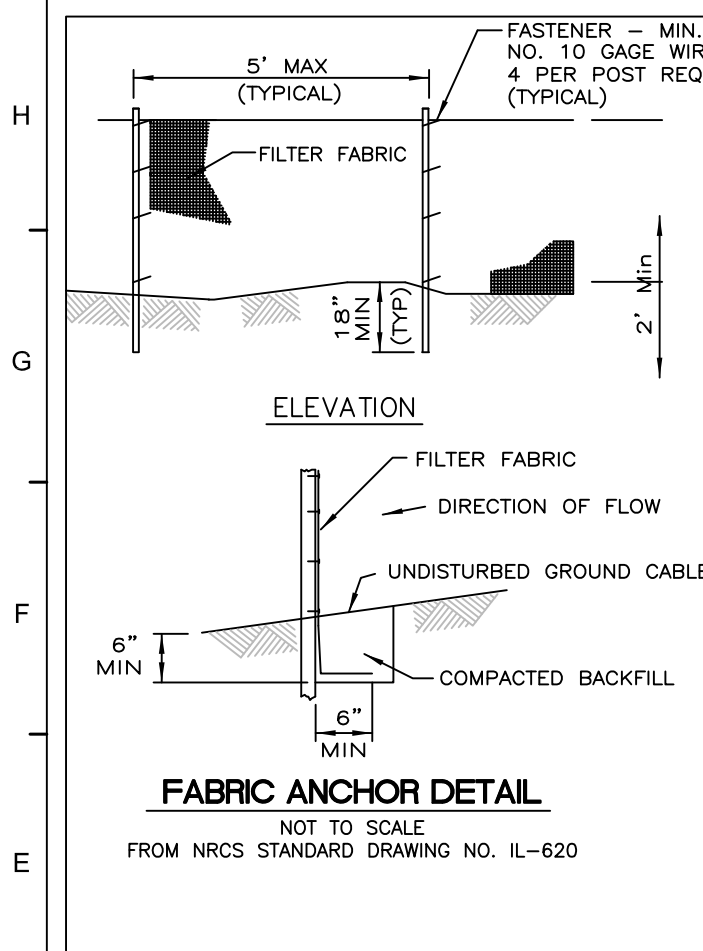


- NOTES:
1. CONTRACTOR SHALL DETERMINE LOCATION AND SIZE OF WASHOUT.
2. WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
3. SANDBAGS SHALL BE INSTALLED TO ANCHOR THE LINING. THE NUMBER OF SANDBAGS SHALL BE DETERMINED BY THE CONTRACTOR. THE CONTRACTOR SHALL ADD SANDBAGS SO AS TO MAINTAIN ANCHORING OF THE LINING.
4. A CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 20 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY. AT A MINIMUM, THE SIGN SHALL READ "CONCRETE WASHOUT" IN 6" TALL LETTERS.
5. THE TEMPORARY WASHOUT FACILITY SHALL BE SURROUNDED BY SILT FENCE ON ALL SIDES.
6. INSPECTION SHALL OCCUR ONCE PER WEEK AND DAILY DURING CONCRETE OPERATIONS. REPAIR/REPLACEMENT OF THE FACILITY SHALL BE MADE SUCH THAT CONCRETE WASTE IS CONTAINED.
7. MEDIA SHALL BE REMOVED AND DISPOSED OF AT A LEGAL OFF-SITE LOCATION WHEN THE FACILITY HAS REACHED 50% CAPACITY.
8. UPON COMPLETION OF CONCRETE OPERATIONS, THE CONCRETE WASHOUT AND ALL MATERIALS CONTAINED WITHIN SHALL BE DISPOSED OF AT A LEGAL OFF-SITE LOCATION.
9. COST INCIDENTAL TO ITEM AR501515.

- NOTES:
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2. WASHOUT SIZE AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
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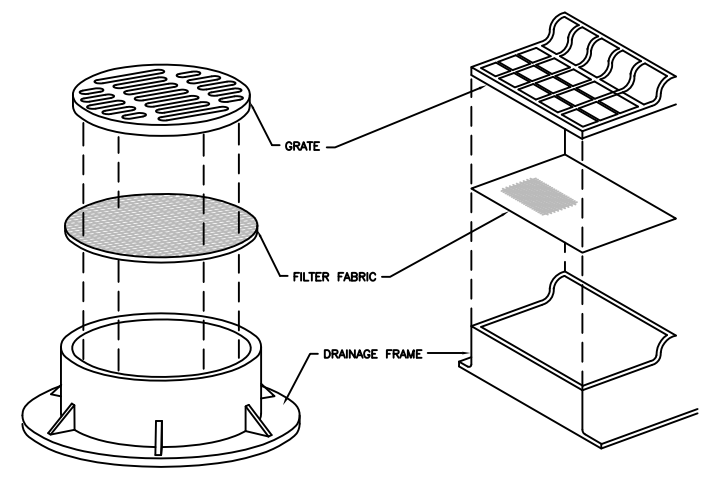
CONCRETE WASHOUT
NOT TO SCALE

STORMWATER POLLUTION PREVENTION PLAN NOTES (SWN1)



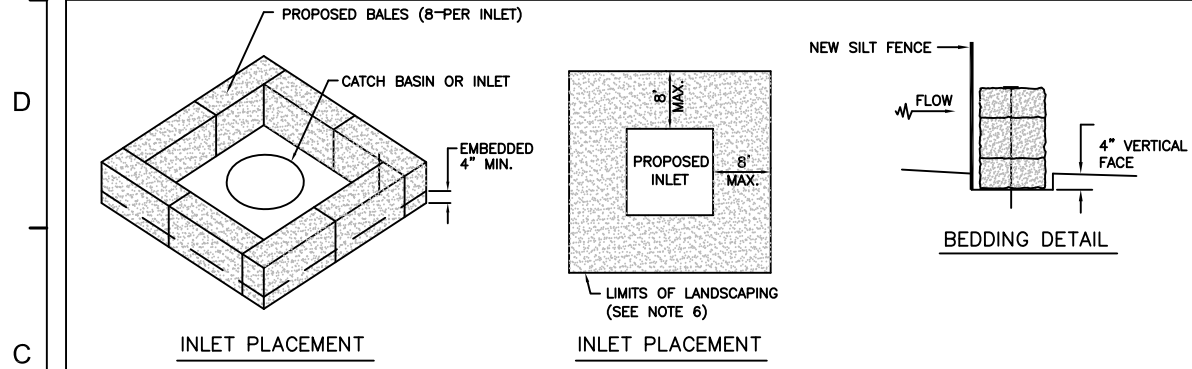
- NOTES:**
1. TEMPORARY SEDIMENT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
 2. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR ITEM AR156000 EROSION CONTROL IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.
 3. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.
 4. SILT FENCE SHALL BE INSTALLED PER STORM WATER POLLUTION PREVENTION PLAN OR AS DIRECTED BY THE ENGINEER.
 5. FENCE POSTS SHALL BE EITHER STANDARD STEEL POSTS OR WOOD POSTS WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN..
 6. PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE.
 7. ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
 8. DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.

RO019

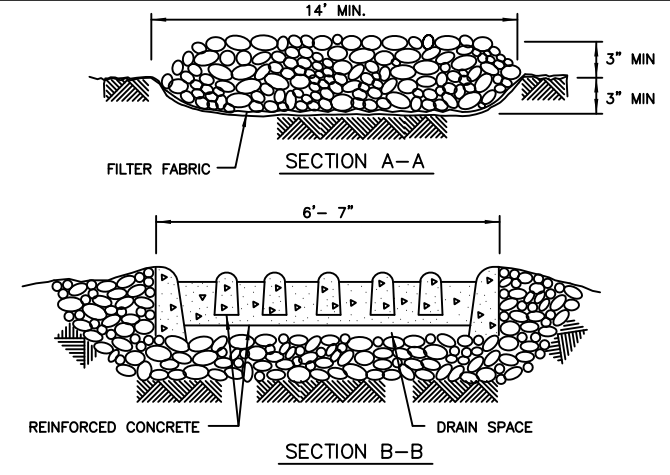


DRAINAGE STRUCTURE FILTER WRAP
N.T.S.

- NOTES**
1. FILTER WRAP TO BE PLACED IN ALL INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
 2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR SILT FENCE.
 3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
 4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
 5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MIN. OF 80% OF COVERAGE.
 6. COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO BALES.

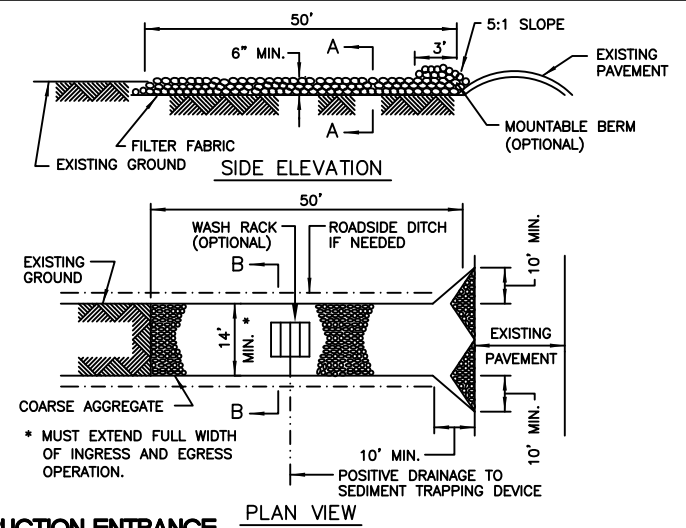


- INLET PROTECTION - TURF AREAS**
N.T.S.
- INLET PROTECTION NOTES:**
1. BALES SHALL BE PLACED AT THE TOE OF SLOPE OR ON A CONTOUR AND IN A ROW WITH ENDS TIGHTLY ABUTTING THE ADJACENT BALES.
 2. EACH BALE SHALL BE EMBEDDED IN THE SOIL A MINIMUM OF 4 INCHES, AND PLACED SO THE BINDINGS ARE HORIZONTAL.
 3. BALES SHALL BE SECURELY ANCHORED IN PLACE BY EITHER TWO STAKES OR REBARS DRIVEN THROUGH THE BALE. THE FIRST STAKE IN EACH BALE SHALL BE DRIVEN TOWARD THE PREVIOUSLY LAID BALE AT AN ANGLE TO FORCE THE BALES TOGETHER. STAKES SHALL BE DRIVEN FLUSH WITH THE BALE.
 4. INSPECTION SHALL BE FREQUENT AND REPAIR / REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
 5. BALES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. COST OF REMOVAL / REPLACEMENT TO BE INCLUDED IN UNIT PRICE FOR BALES.
 6. AFTER FINAL APPROVAL OF THE ENGINEER, STRAW BALES MAY BE REMOVED. CONTRACTOR SHALL PLACE SEED AND MULCH OVER THE DISTURBED AREAS, COST INCIDENTAL TO BALES.
 7. SILT FENCE SHALL BE MEASURED AND PAID FOR SEPARATELY.



STABILIZED CONSTRUCTION ENTRANCE
FROM NRCS STANDARD DRAWING NO. IL-630

1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED UNDER SECTION 1080.03, OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2007.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. COMPACTION SHALL BE TO THE SATISFACTION OF THE ENGINEER.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. IF WASH RACK ARE USED THEY SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.
7. THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE INCIDENTAL TO ITEM AR501515.



STABILIZED CONSTRUCTION ENTRANCE
PLAN VIEW

STORMWATER POLLUTION PREVENTION PLAN DETAILS (SWD1)



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS

Chicago Rockford International Airport
Rockford, Illinois

Greater Rockford Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION PROJECT NAME:

REHABILITATE RUNWAY 1/19 PHASE 1

SHEET TITLE:
STORMWATER POLLUTION PREVENTION PLAN DETAILS (SWD1)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06 DATE: APRIL 22, 2011

SHEET NO. **18** REVISION

SHEET 18 OF 67 SHEETS



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AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**EXISTING CONDITIONS
AND REMOVALS -1 (EXC1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **19** REVISION

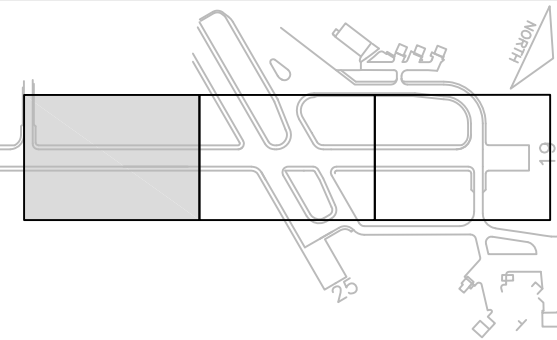
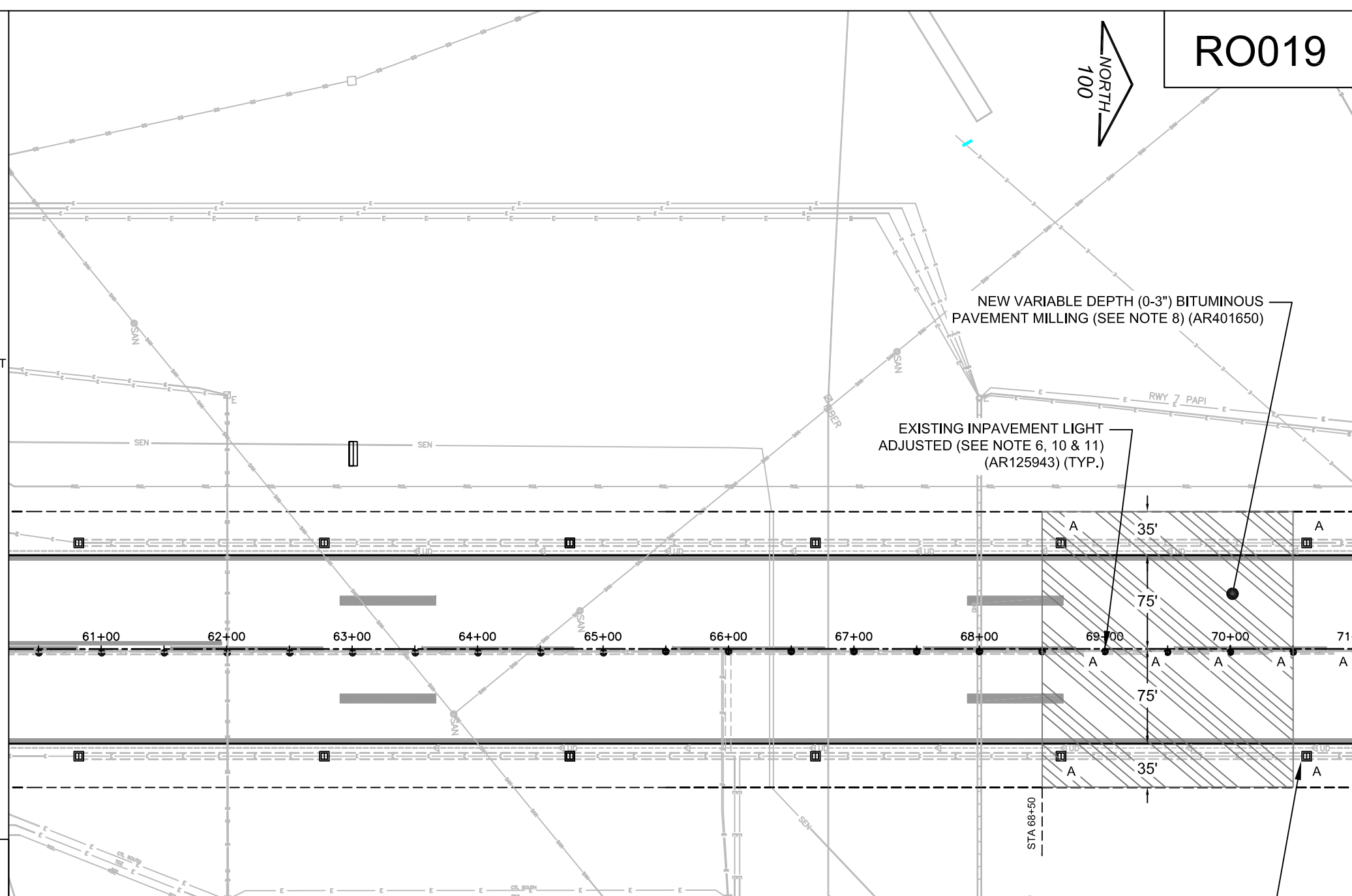
SHEET 19 OF 67 SHEETS

RO019



LEGEND

	NEW 17" BIT. PAVEMENT MILLING (AVE.)
	NEW 20" BIT. PAVEMENT MILLING (AVE.)
	NEW VARIABLE DEPTH (0-3") BIT. PAVEMENT MILLING (AVE.)
	NEW 3-4" BIT. PAVEMENT MILLING (AVE.)
	REMOVE AND REPLACE BIT. PAVEMENT FULL DEPTH
	EXISTING SANITARY LINE
	EXISTING STORM SEWER
	EXISTING UNDERDRAIN
	EXISTING WATERMAIN
	EXISTING DUCT
	EXISTING RWY GUIDANCE LIGHT CIRCUIT
	EXISTING AIRFIELD CIRCUIT
	EXISTING SURFACE SENSOR CIRCUIT
	EXISTING MITL, BASE MOUNTED
	EXISTING HIRL, BASE MOUNTED
	EXISTING ELECTRICAL HANDHOLE
	EXISTING RGL
	EXISTING TAXIWAY GUIDANCE SIGN
	EXISTING DRAINAGE INLET
	EXISTING MANHOLE
	EXISTING ITEM TO BE REMOVED
	EXISTING ITEM TO BE ADJUSTED
	EXISTING ITEM TO BE RELOCATED
	EXISTING PAVEMENT SENSOR
	EXISTING IN-PAVEMENT LIGHT
	EXISTING VASI
	EXISTING SPLICE CAN
	EXISTING REILS



- NOTES:**
- EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
 - TRANSFORMER TO BE REMOVED SHALL BE TURNED OVER TO THE AIRPORT. CONCRETE LIGHT BASES SHALL BE DISPOSED OF OFF SITE.
 - ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
 - ITEMS REMOVED DUE TO PROPOSED PAVEMENT EXCAVATION WILL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED INCIDENTAL TO UNCLASSIFIED EXCAVATION UNLESS OTHERWISE NOTED ON THE PLANS. ALL EXISTING PAVEMENT TO BE REMOVED SHALL BE MILLED AND USED AS FILL MATERIAL FOR THE NEW RIAT ROAD. ANY EXTRA PAVEMENT MILLINGS SHALL BE STOCKPILED ON AIRPORT PROPERTY AT LOCATION DESIGNATED BY THE RESIDENT ENGINEER.
 - ALL LIGHT BASES REMOVED SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 208 COST OF BACKFILL SHALL BE INCLUDED IN THE REMOVAL PAY ITEM.
 - ALL EXISTING LIGHT FIXTURES AND SIGNS WILL BE REUSED FOR THE FINAL ASSEMBLY OF THE ASSOCIATED PAY ITEM.
 - ANY PAVEMENT REMOVED BY THE CONTRACTOR BEYOND THE LIMITS SHOWN IN THE PLANS TO FACILITATE PCC PAVEMENT CONSTRUCTION AND SUBSEQUENT PAVEMENT REPLACEMENT SHALL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE PAVEMENT REMOVAL ITEM.
 - THE EXACT PAVEMENT MILLING LIMIT AND DEPTH REQUIRED SHALL BE DETERMINED FROM THE PLAN AND PROFILE AND THE INTERSECTION GRADE DETAIL. THE PAVEMENT SHALL BE MILLED IN SUCH A MANNER TO FACILITATE A 3" BITUMINOUS OVERLAY.
 - ALL REGRADING AND RESHAPING OF EXISTING AGGREGATE BASE COURSE SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAVING PAY ITEM.
 - IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN EXISTING INPAVEMENT BASE CAN AND CONDUIT. ANY BASE CAN OR CONDUIT DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
 - ALL WORK REQUIRED TO PREPARE EXISTING INPAVEMENT BASE CAN AND CONDUIT FOR PAVING OPERATIONS SHALL BE INCIDENTAL TO THE ASSOCIATED PAVING PAY ITEM.
 - IF THE HMA PAVEMENT SECTION IS LESS THAN THE 17" PAVEMENT MILLING SHOWN, THE REMOVAL OF THE EXISTING AGGREGATE SUBBASE TO THE CORRECT PROFILE AND DIMENSIONS SHALL BE REMOVED BY PAVEMENT MILLING. THIS COST SHALL BE INCIDENTAL TO PAY ITEM AR401650 "BITUMINOUS PAVEMENT MILLING".

EXISTING BASE MOUNTED LIGHT ADJUSTED (SEE NOTE 6) (AR125942) (TYP.)

EXISTING INPAVEMENT LIGHT ADJUSTED (SEE NOTE 6, 10 & 11) (AR125943) (TYP.)

NEW VARIABLE DEPTH (0-3") BITUMINOUS PAVEMENT MILLING (SEE NOTE 8) (AR401650)

EXISTING CONDITIONS AND REMOVALS - 1 (EXC1)

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REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

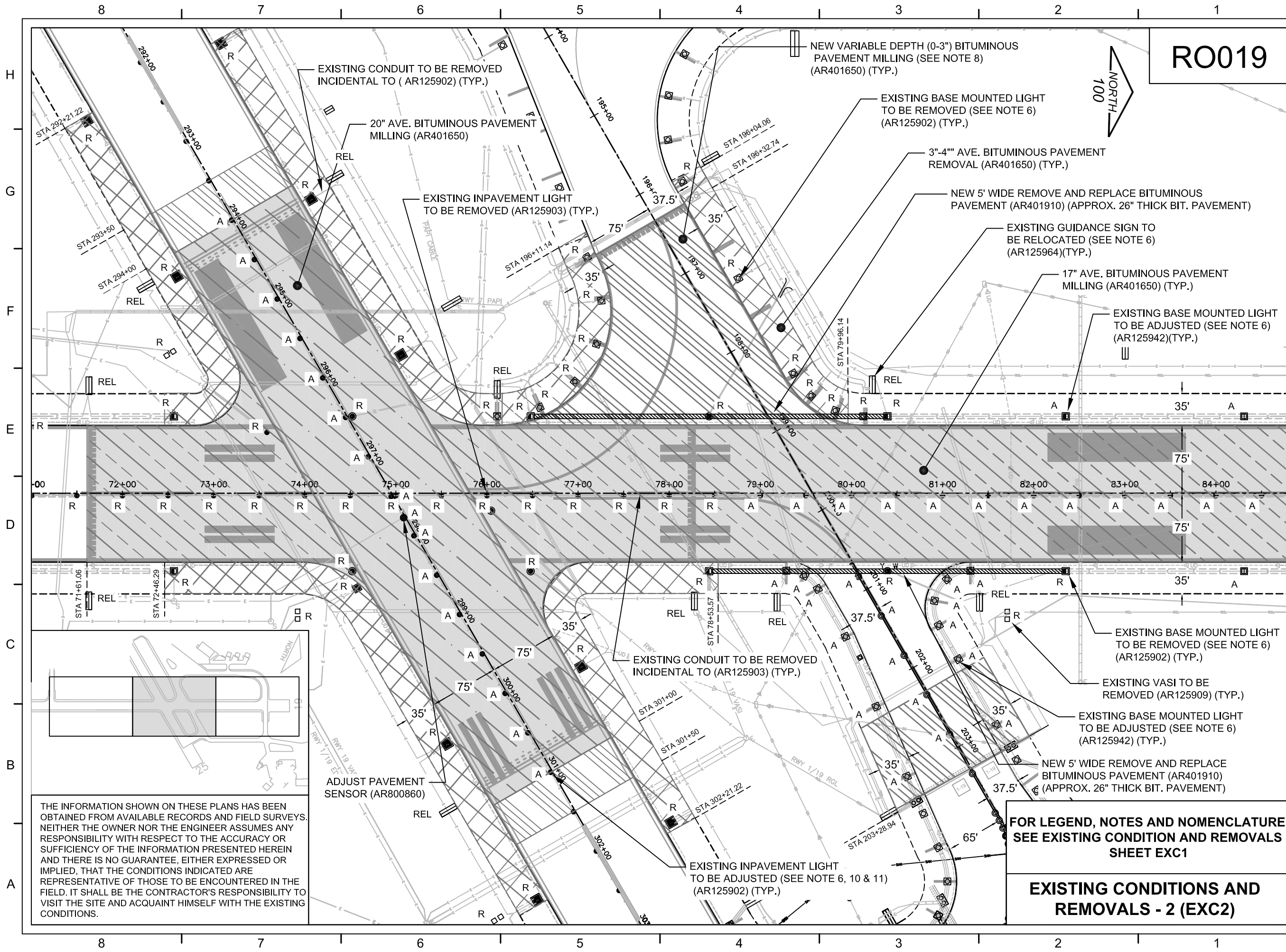
SHEET TITLE:
**EXISTING CONDITIONS
AND REMOVALS -2 (EXC2)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
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SHEET NO. **20** REVISION

SHEET 20 OF 67 SHEETS



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**FOR LEGEND, NOTES AND NOMENCLATURE
SEE EXISTING CONDITION AND REMOVALS
SHEET EXC1**

**EXISTING CONDITIONS AND
REMOVALS - 2 (EXC2)**



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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

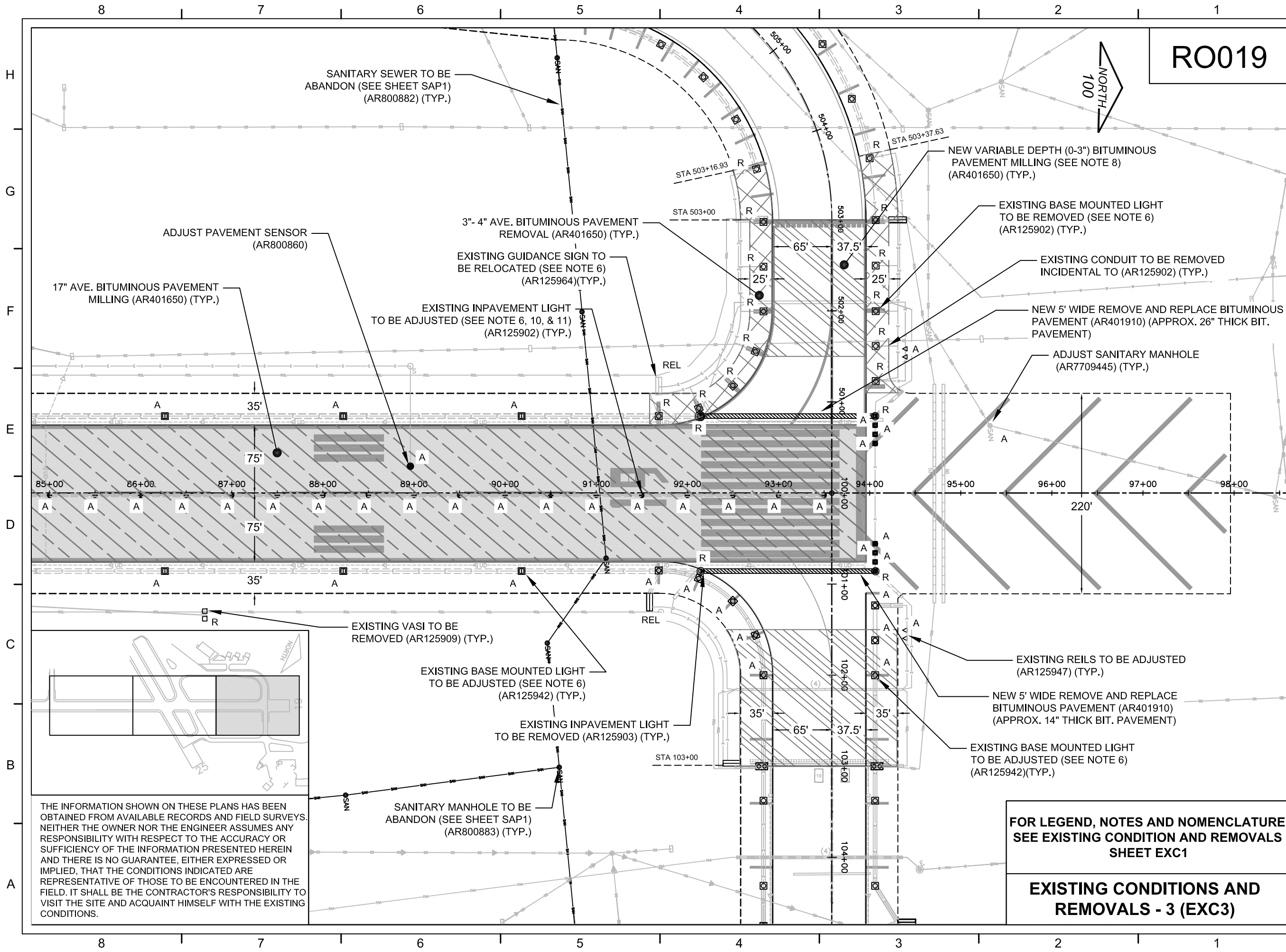
SHEET TITLE:
**EXISTING CONDITIONS
AND REMOVALS -3 (EXC3)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **21** REVISION

SHEET 21 OF 67 SHEETS



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**FOR LEGEND, NOTES AND NOMENCLATURE
SEE EXISTING CONDITION AND REMOVALS
SHEET EXC1**

**EXISTING CONDITIONS AND
REMOVALS - 3 (EXC3)**



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REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**SANITARY SEWER
ABANDONMENT PLAN
(SAP1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

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RO019



SANITARY SEWER TO BE
ABANDONED (SEE NOTE 2)
(AR800882) (TYP.)

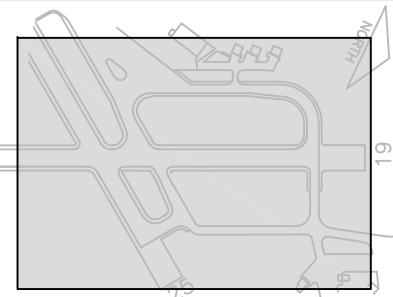
NEW 5' X 5' REMOVE AND REPLACE
BITUMINOUS PAVEMENT (AR401910)
(APPROX. 19" THICK BIT. PAVEMENT)

NEW 5' X 5' REMOVE AND REPLACE
BITUMINOUS PAVEMENT (AR401910)
(APPROX. 4" THICK BIT. PAVEMENT)

SANITARY MANHOLE TO BE
ABANDONED (SEE NOTE 1)
(AR800883) (TYP.)

LEGEND

- EXISTING PIPE TO BE ABANDONED
- EXISTING MANHOLE TO BE ABANDONED
- REMOVE AND REPLACE BIT. PAVEMENT



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- NOTES:**
- MANHOLE ABANDONMENT SHALL CONSIST OF REMOVING THE TOP 5' OF THE MANHOLE STRUCTURE. THE REMAINING PORTION OF THE MANHOLE SHALL BE COMPLETELY FILLED WITH CLSM MATERIAL.
 - SANITARY SEWER ABANDONMENT SHALL CONSIST OF COMPLETELY FILLING THE SANITARY SEWER PIPE WITH CLSM MATERIAL.
 - ALL SANITARY ABANDONMENT SHALL BE COMPLETED IN ACCORDANCE WITH ALL ROCK RIVER WATER RECLAMATION DISTRICT REQUIREMENTS.
 - EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
 - MANHOLES SHALL BE BACKFILLED IN ACCORDANCE WITH ITEM 208. COST OF BACKFILLING INCIDENTAL TO ABANDON MANHOLE PAY ITEM.
 - ALL CONSTRUCTION DEBRIS SHALL BE DISPOSED OF OFF SITE. COST INCIDENTAL TO ASSOCIATED PAY ITEM.

**SANITARY SEWER
ABANDONMENT PLAN (SAP1)**



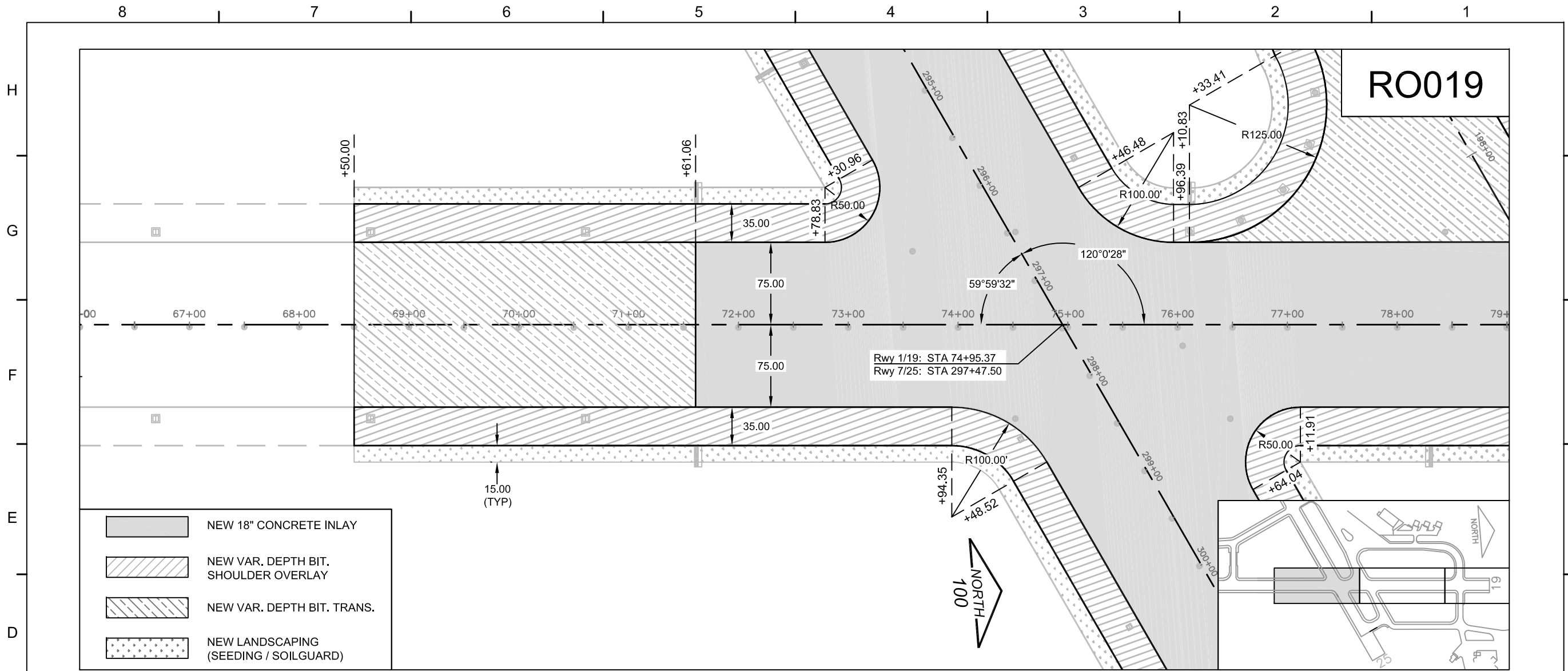
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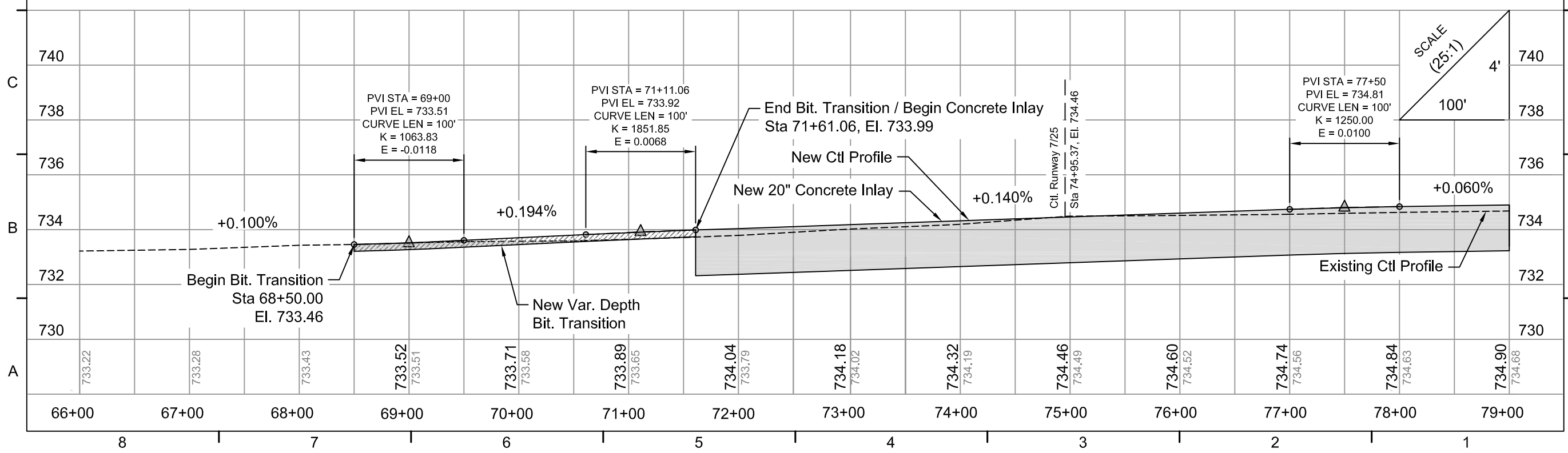
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- NEW 18" CONCRETE INLAY
- NEW VAR. DEPTH BIT. SHOULDER OVERLAY
- NEW VAR. DEPTH BIT. TRANS.
- NEW LANDSCAPING (SEEDING / SOILGUARD)



APPROVED AS WORKING PLAN BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

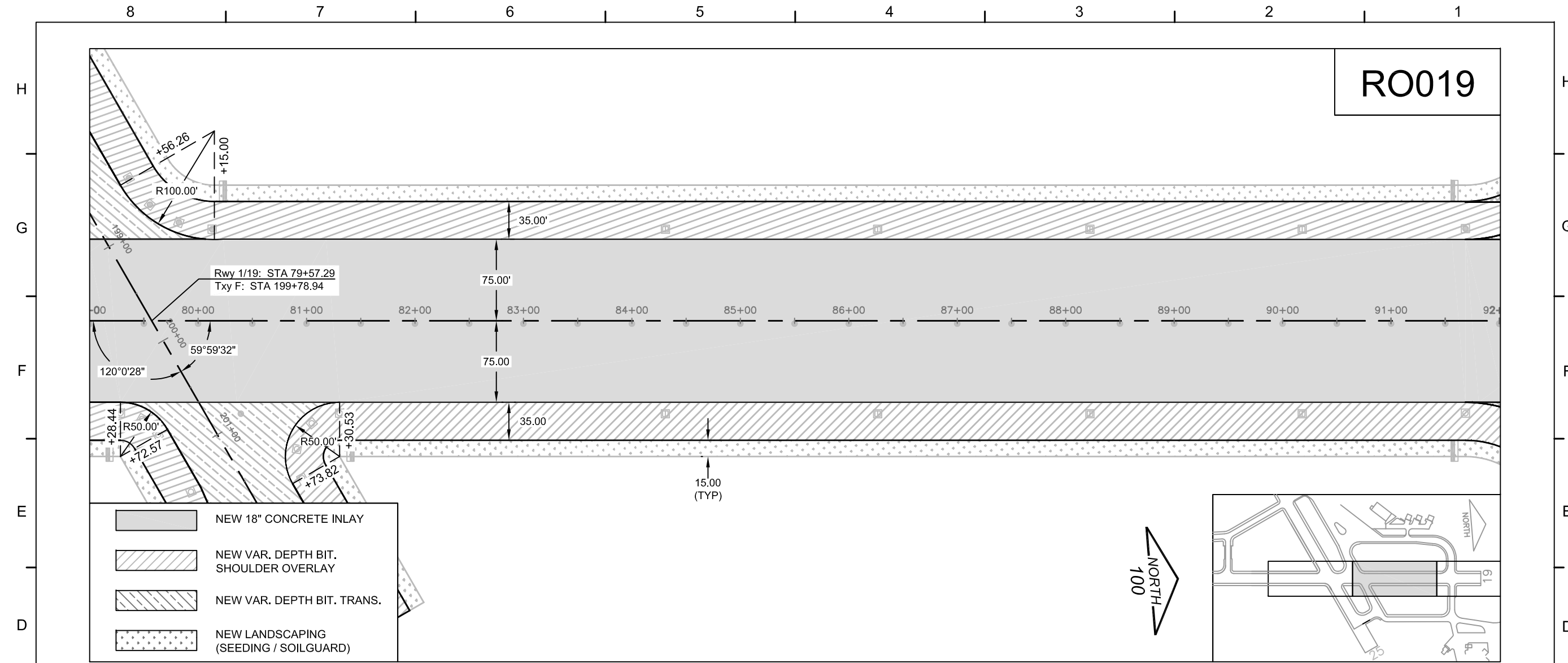
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**RUNWAY 1/19
PLAN/PROFILE -1 (RPP1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO.	REVISION
23	



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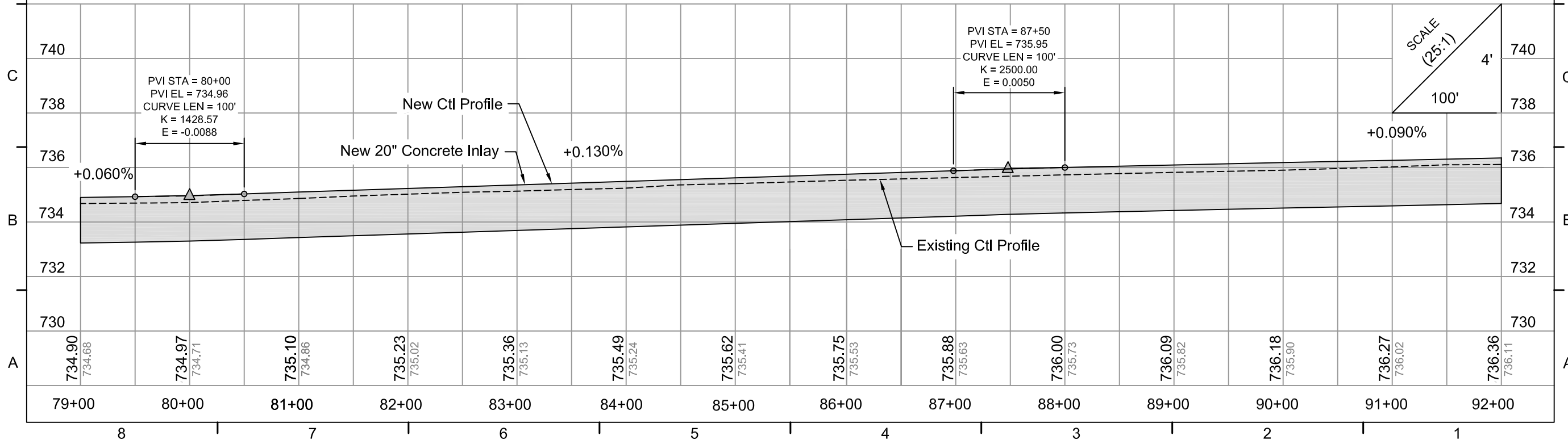
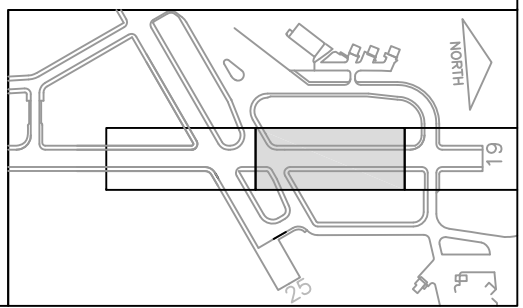
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REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
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SHEET TITLE:
RUNWAY 1/19
PLAN/PROFILE - 2 (RPP2)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

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DATE: APRIL 22, 2011

SHEET NO.	24	REVISION
SHEET 24 OF 67 SHEETS		



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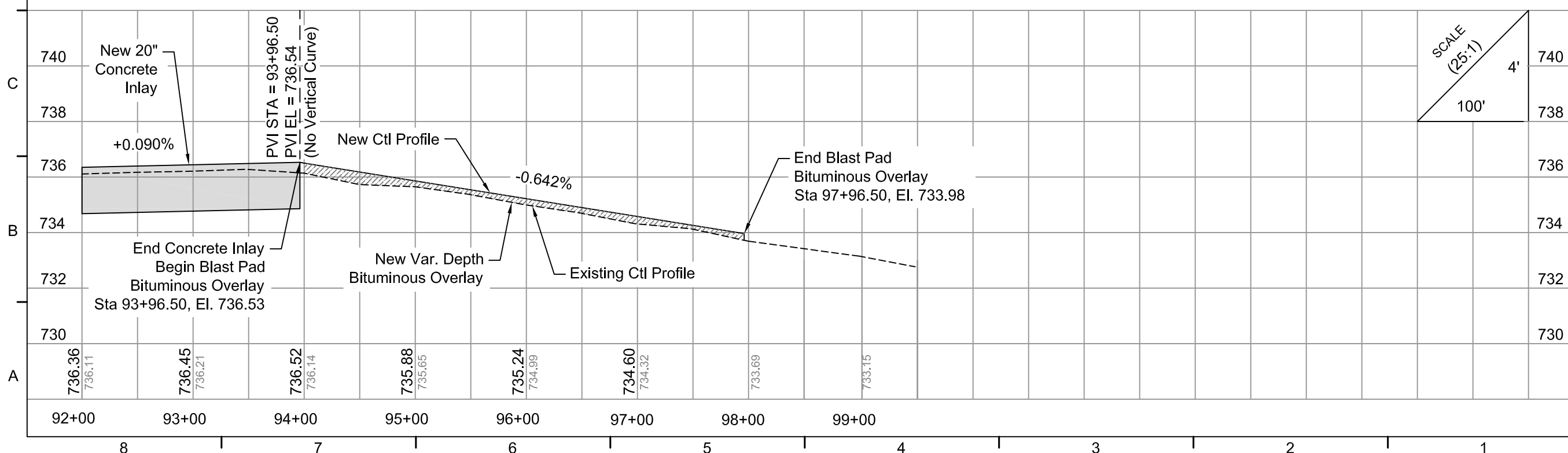
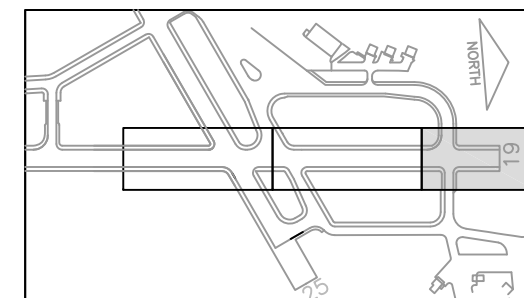
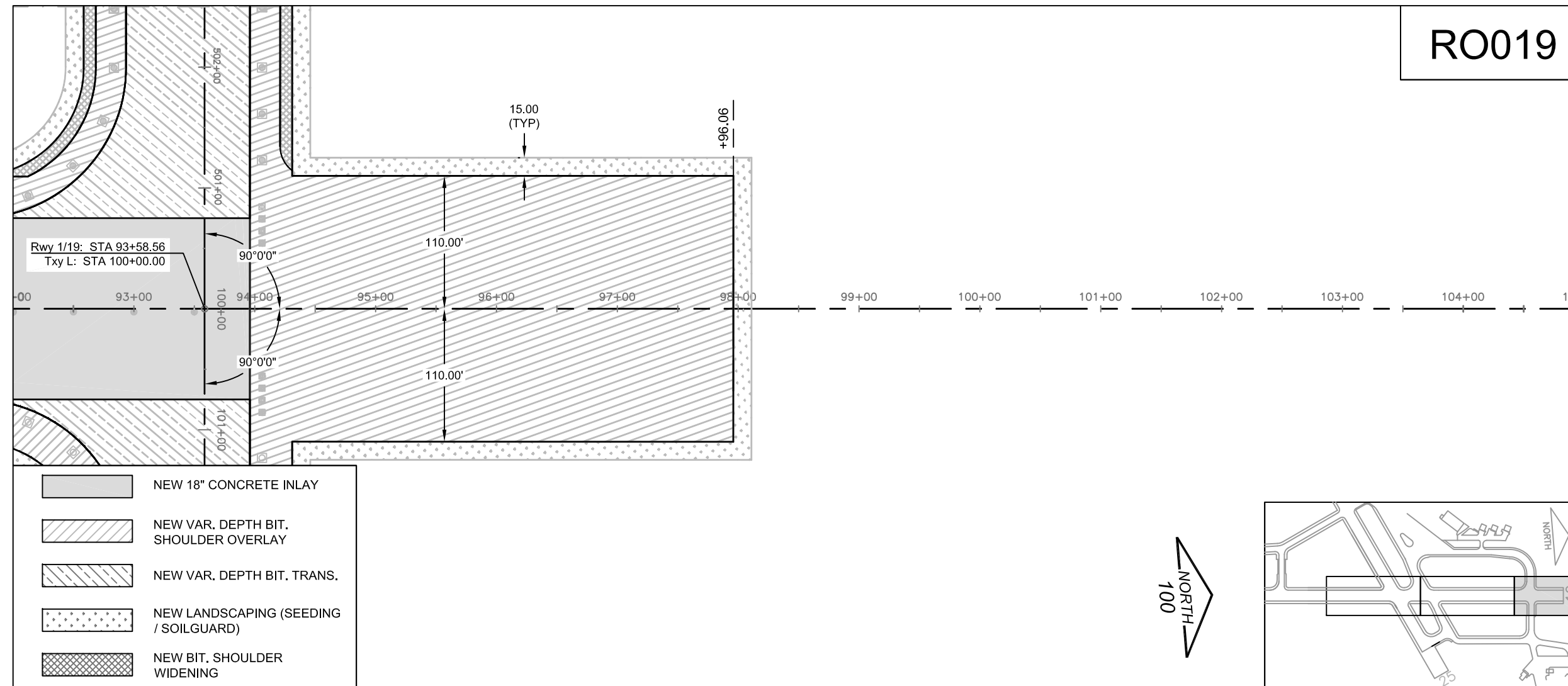


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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**RUNWAY 1/19
PLAN/PROFILE - 3 (RPP3)**

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
PROJECT NO.: 10258-03-00/06		
DATE: APRIL 22, 2011		
SHEET NO.	25	REVISION
SHEET 25 OF 67 SHEETS		



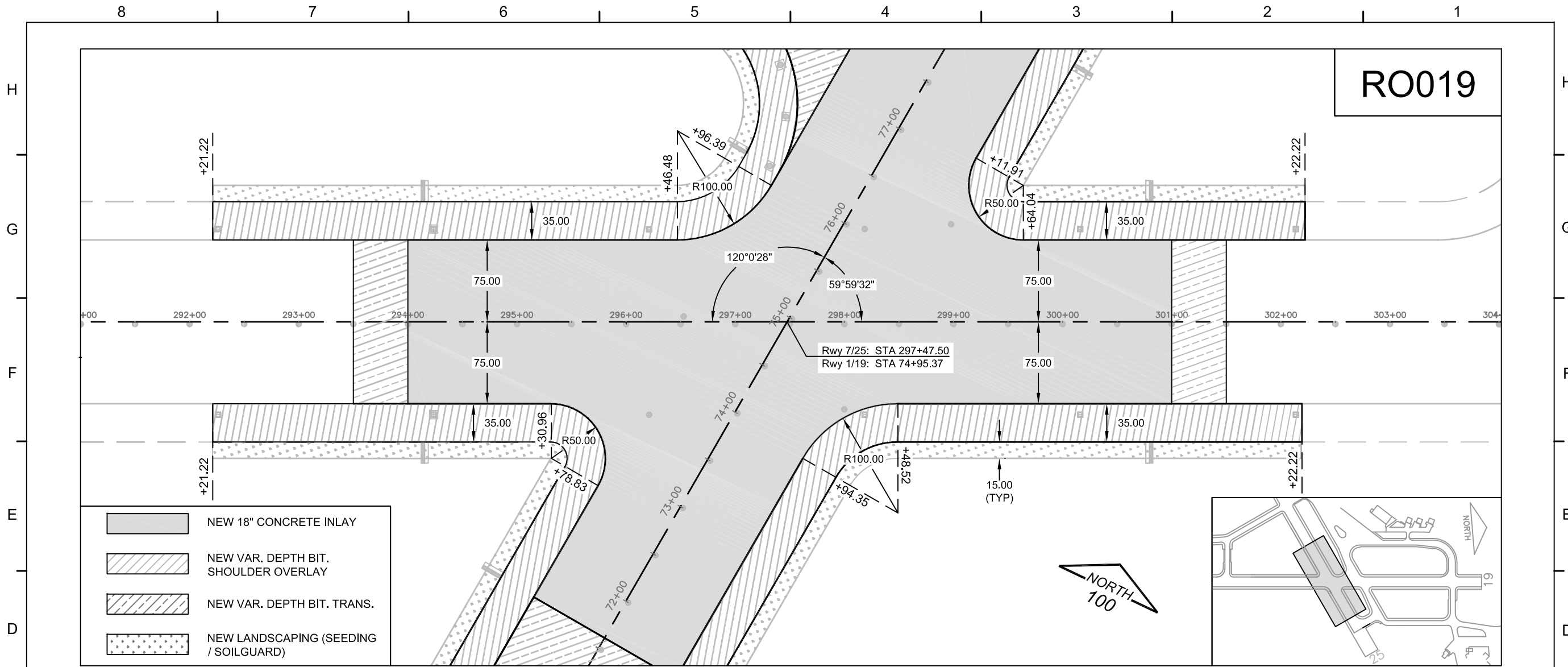
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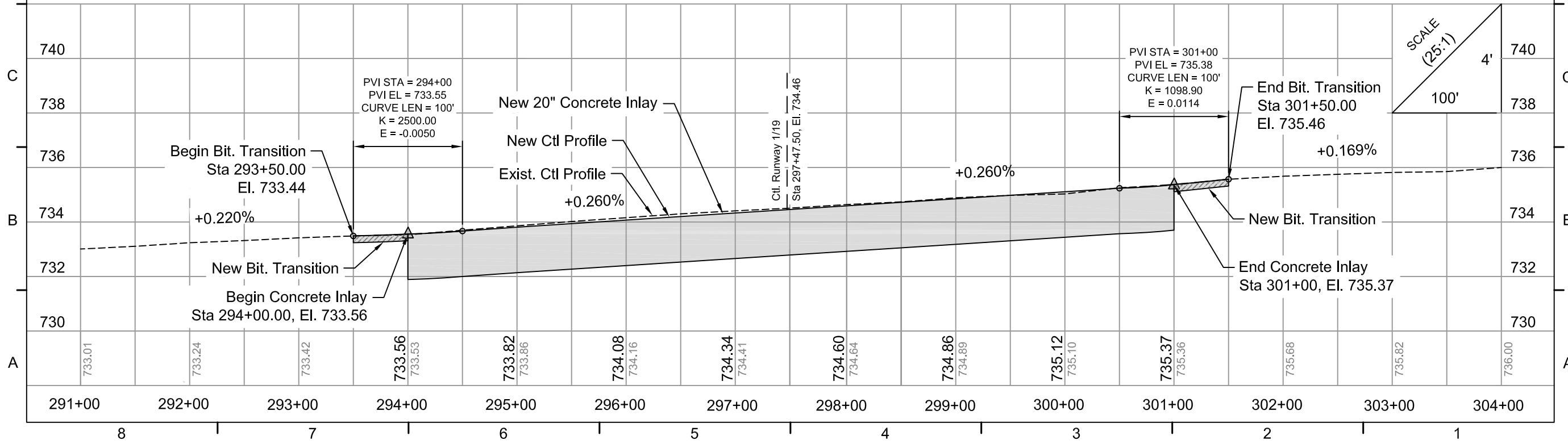
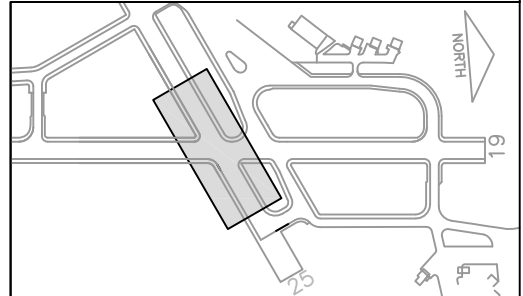
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	NEW 18" CONCRETE INLAY
	NEW VAR. DEPTH BIT. SHOULDER OVERLAY
	NEW VAR. DEPTH BIT. TRANS.
	NEW LANDSCAPING (SEEDING / SOILGUARD)



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REV	DATE	DESCRIPTION

PROJECT NAME: _____

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RUNWAY 1/19
PHASE 1

SHEET TITLE:
RUNWAY 7/25
PLAN/PROFILE (RPP4)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **26** REVISION

SHEET 26 OF 70 SHEETS



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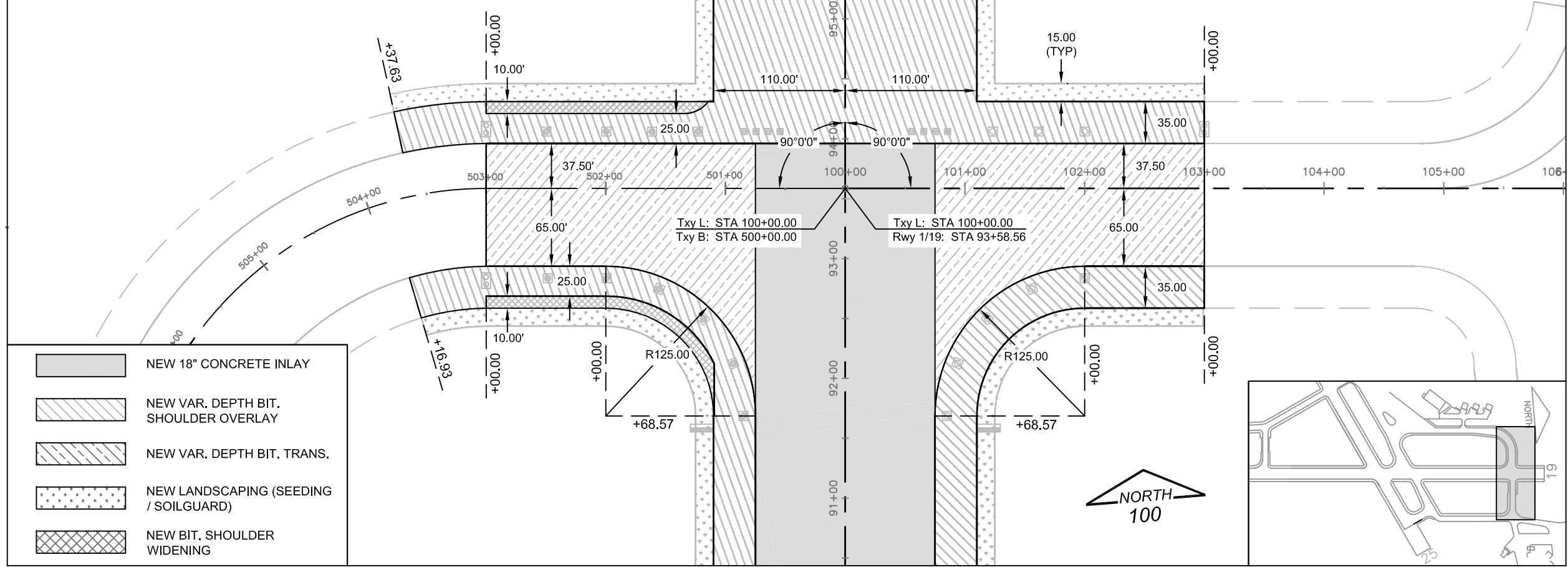


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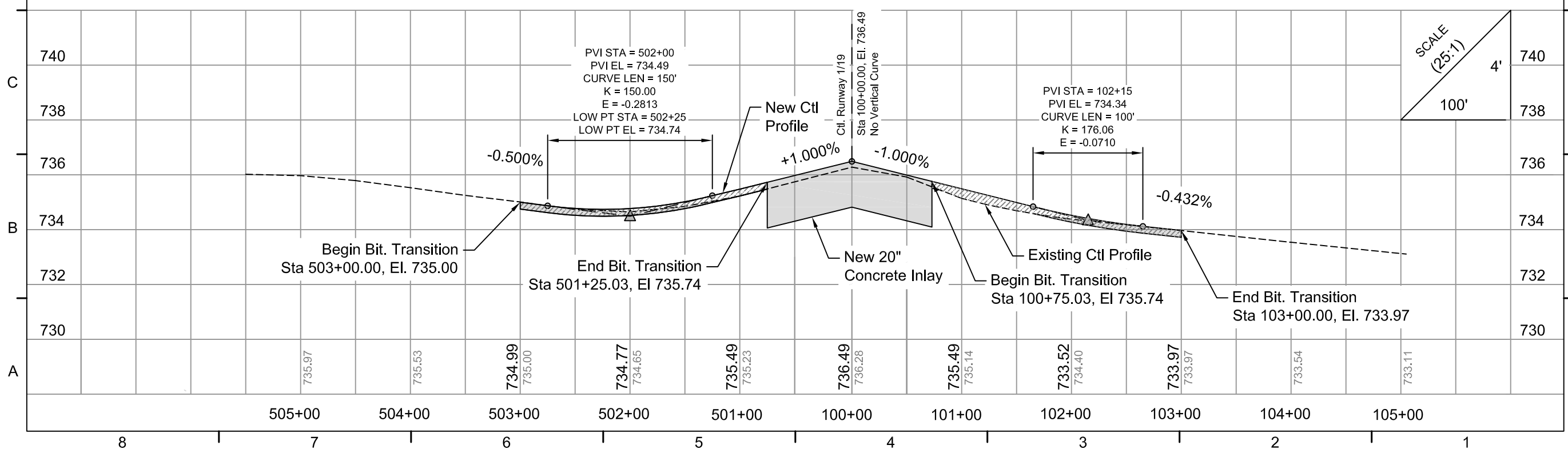
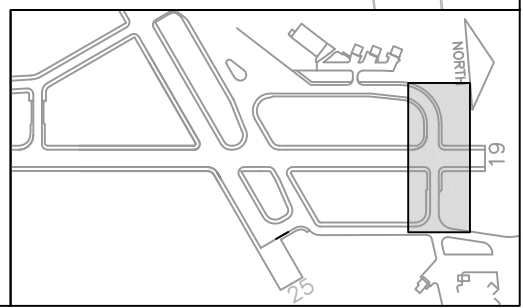
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IL Proj: RFD-4083

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- NEW 18" CONCRETE INLAY
- NEW VAR. DEPTH BIT. SHOULDER OVERLAY
- NEW VAR. DEPTH BIT. TRANS.
- NEW LANDSCAPING (SEEDING / SOILGUARD)
- NEW BIT. SHOULDER WIDENING



SCALE
(25:1)
4'
100'

APPROVED AS WORKING PLAN		
BY:		
REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**TAXIWAY L & B
PLAN/PROFILE (LPP1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **27** REVISION

SHEET 27 OF 67 SHEETS



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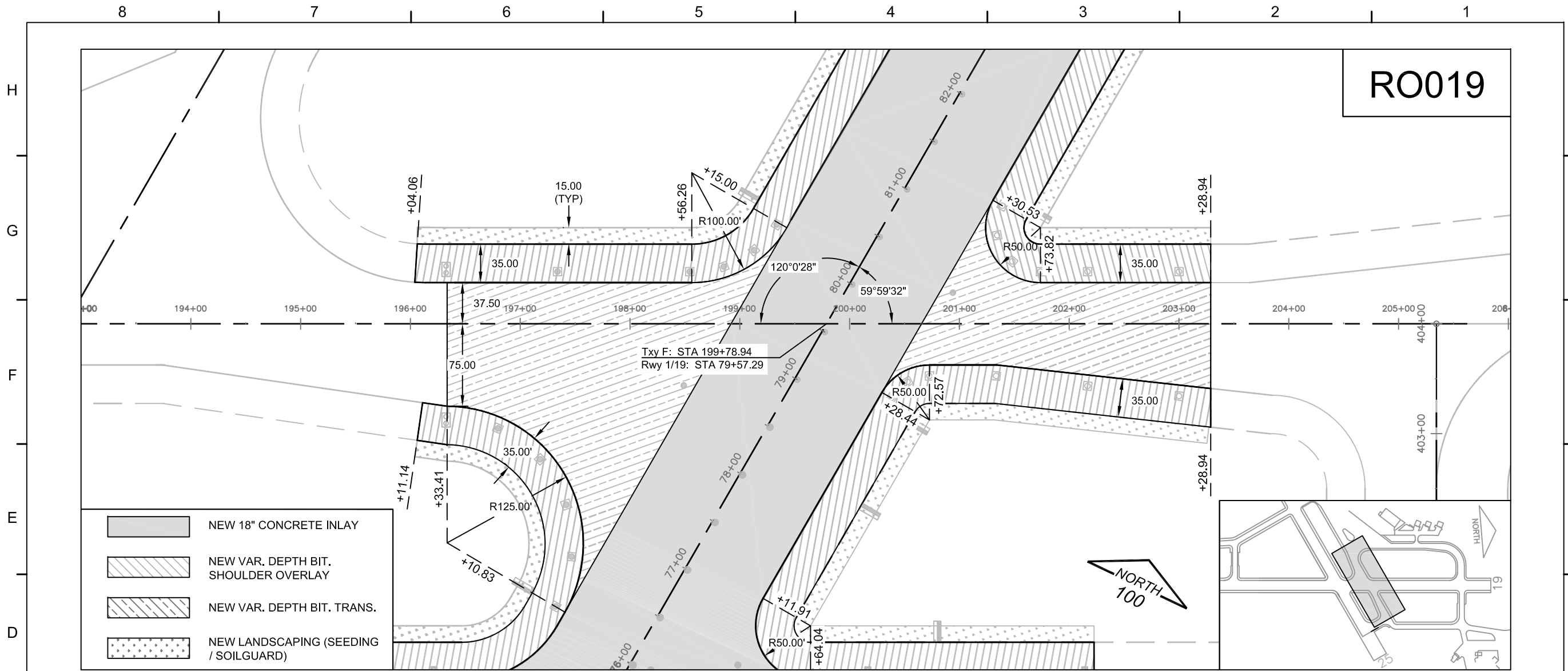



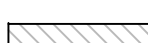
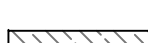
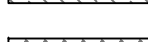
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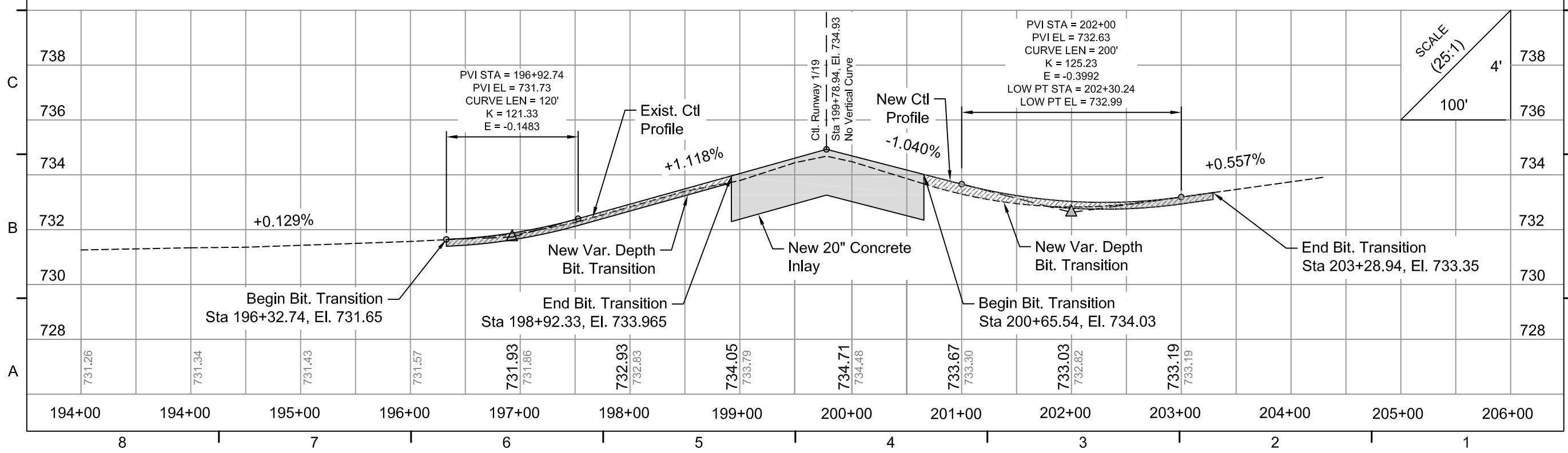
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-  NEW 18" CONCRETE INLAY
-  NEW VAR. DEPTH BIT. SHOULDER OVERLAY
-  NEW VAR. DEPTH BIT. TRANS.
-  NEW LANDSCAPING (SEEDING / SOILGUARD)



SCALE
(25:1)
4'
100'

APPROVED AS WORKING PLAN	
BY:	
REV	DATE DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**TAXIWAY F
PLAN/PROFILE (FPP1)**

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
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PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO.	28	REVISION
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SHEET 28 OF 67 SHEETS



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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
**TAXIWAY L AND B
INTERSECTION GRADING
(LIG1)**

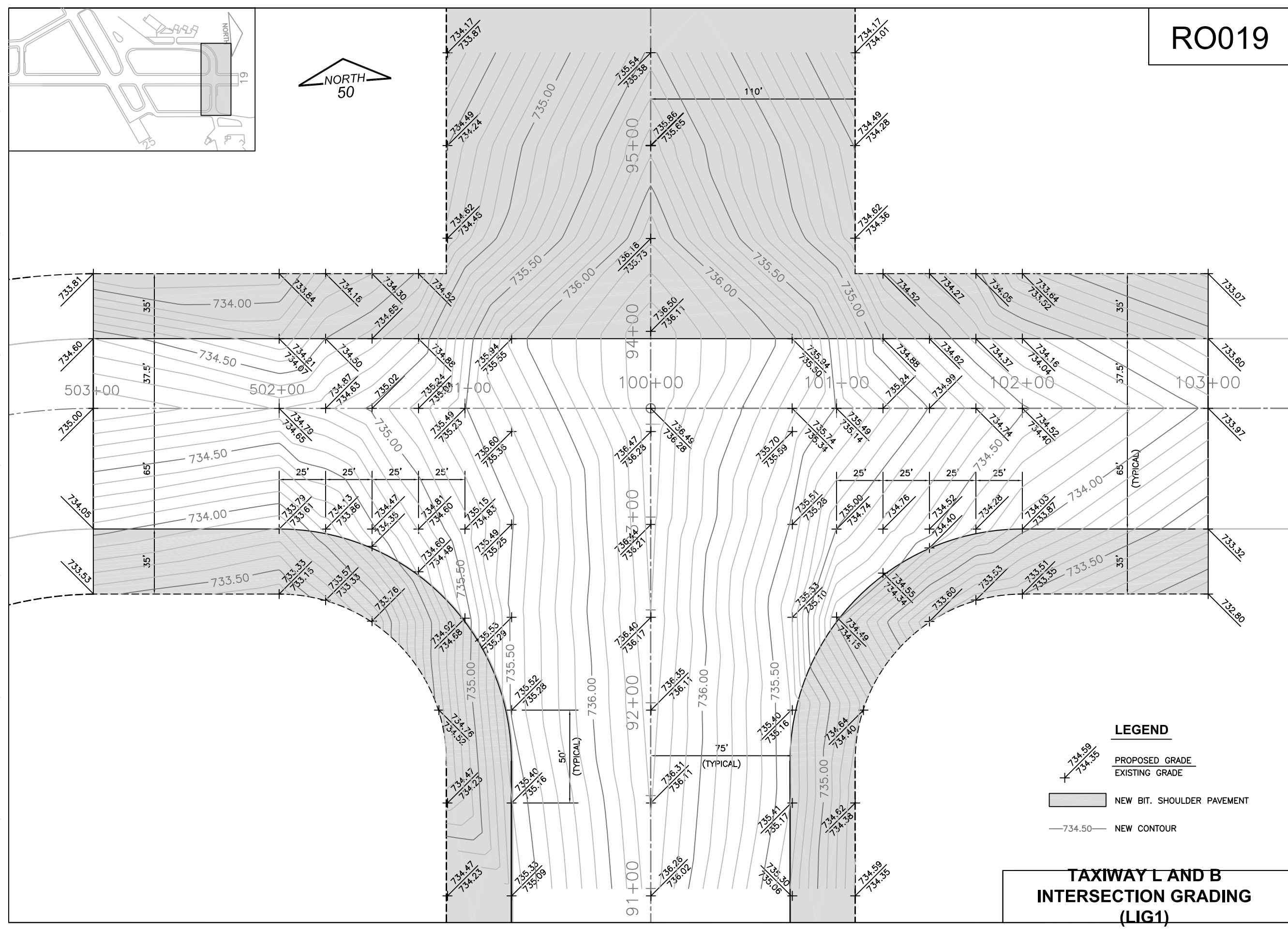
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PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **29** REVISION

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**TAXIWAY L AND B
INTERSECTION GRADING
(LIG1)**



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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**TAXIWAY F INTERSECTION
GRADING (FIG1)**

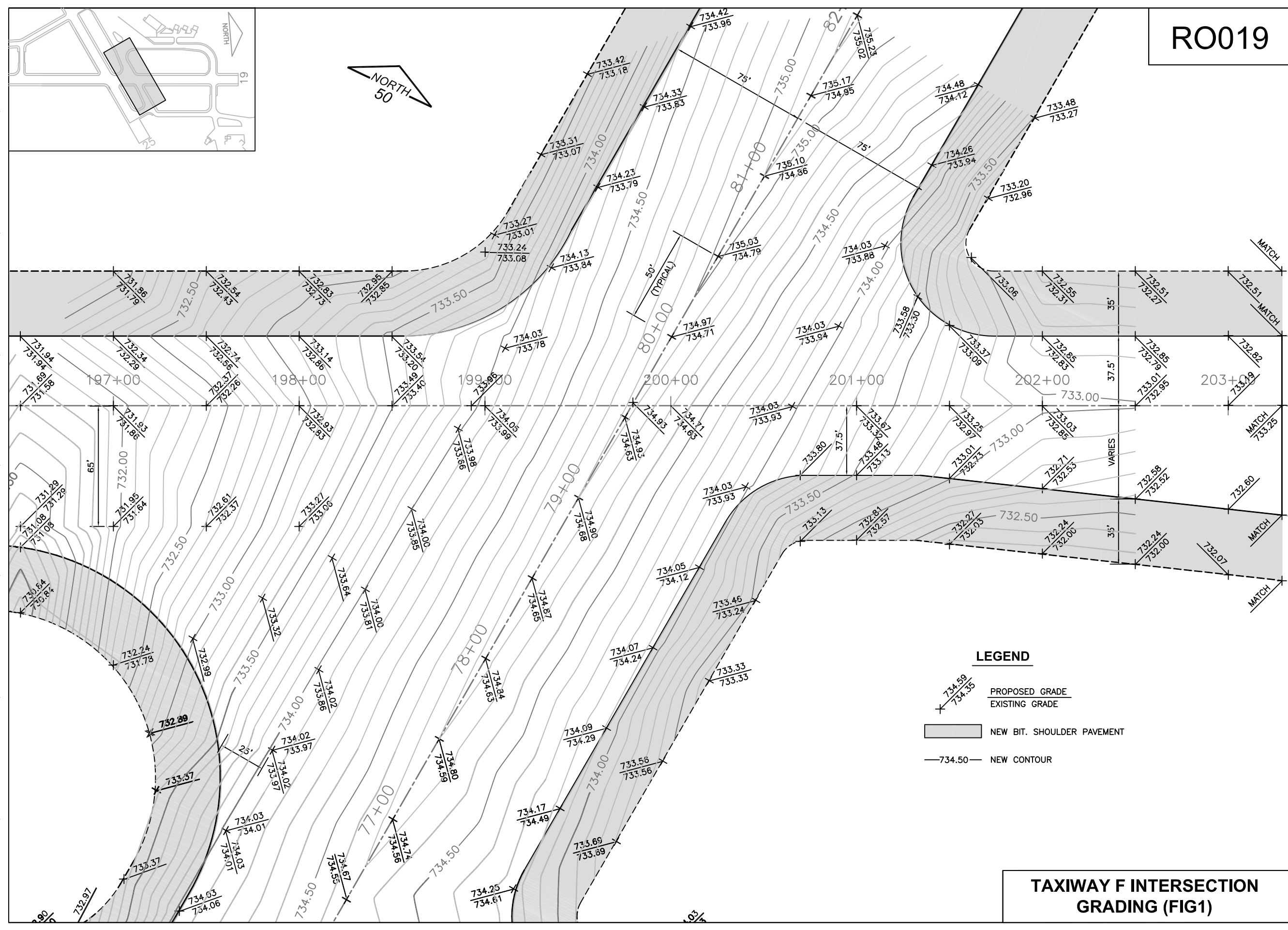
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PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **30** REVISION

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RO019



LEGEND

- PROPOSED GRADE
- EXISTING GRADE
- NEW BIT. SHOULDER PAVEMENT
- NEW CONTOUR

**TAXIWAY F INTERSECTION
GRADING (FIG1)**



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REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

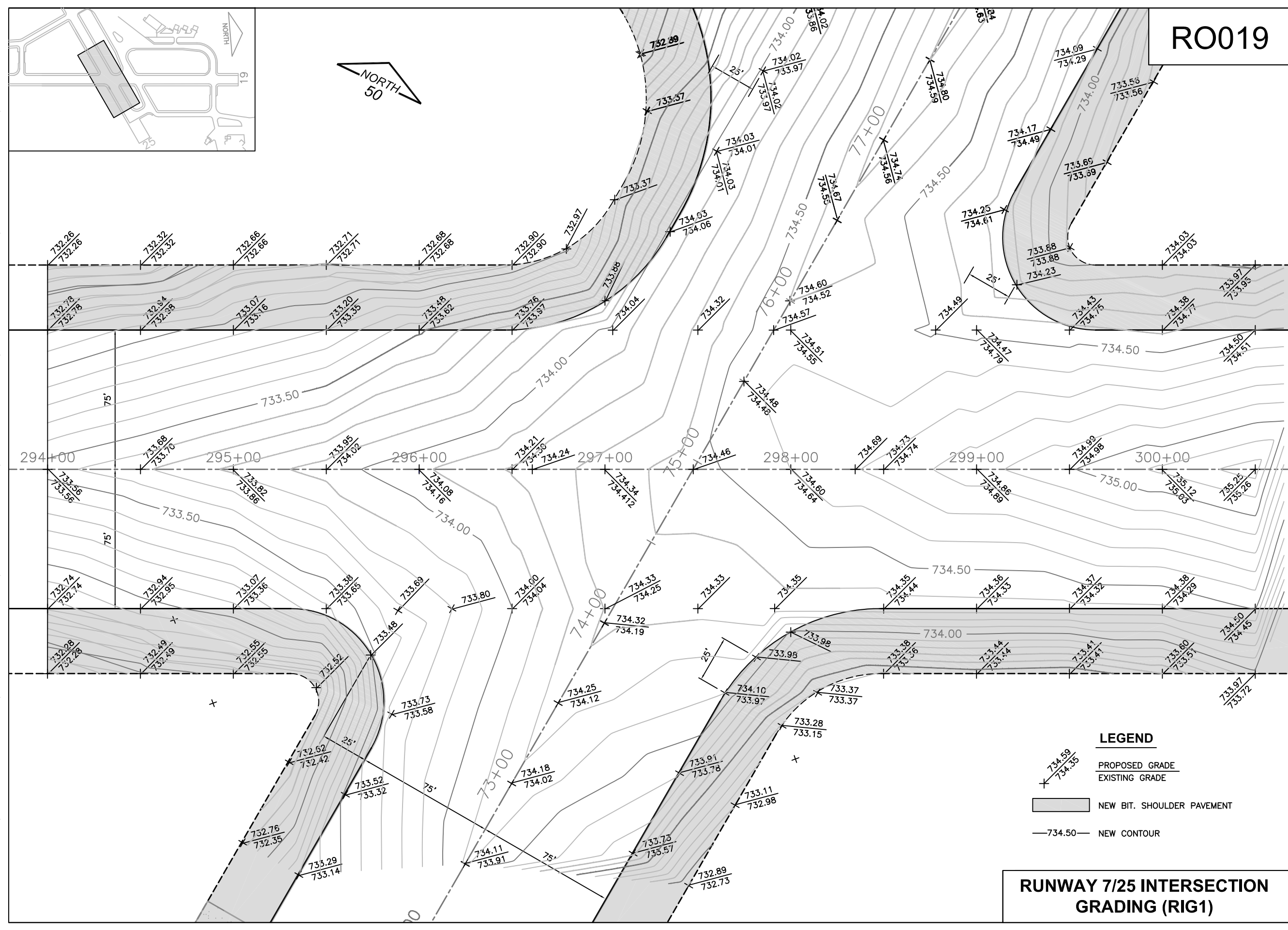
SHEET TITLE:
**RUNWAY 7/25 INTERSECTION
GRADING (RIG1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **31** REVISION
SHEET 31 OF 67 SHEETS

RO019



**RUNWAY 7/25 INTERSECTION
GRADING (RIG1)**



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REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

JOINTING PLAN 1
(JTP1)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **32** REVISION

SHEET 32 OF 67 SHEETS

RO019



LEGEND

- NEW TYPE D DOWELED CONSTRUCTION JOINT
- NEW TYPE F DOWELED CONTRACTION JOINT
- NEW THICKENED EDGE
- EXPANSION JOINT

SEE REINFORCING DETAIL SHEET JTP2 (TYP.)

SEE NOTE 2

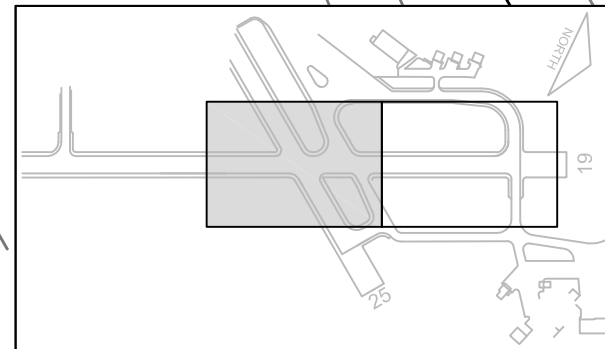
SEE SKEWED JOINT DETAIL SHEET JTP2 (TYP.)

SEE FILLET DETAIL SHEET JTP2 (TYP.)

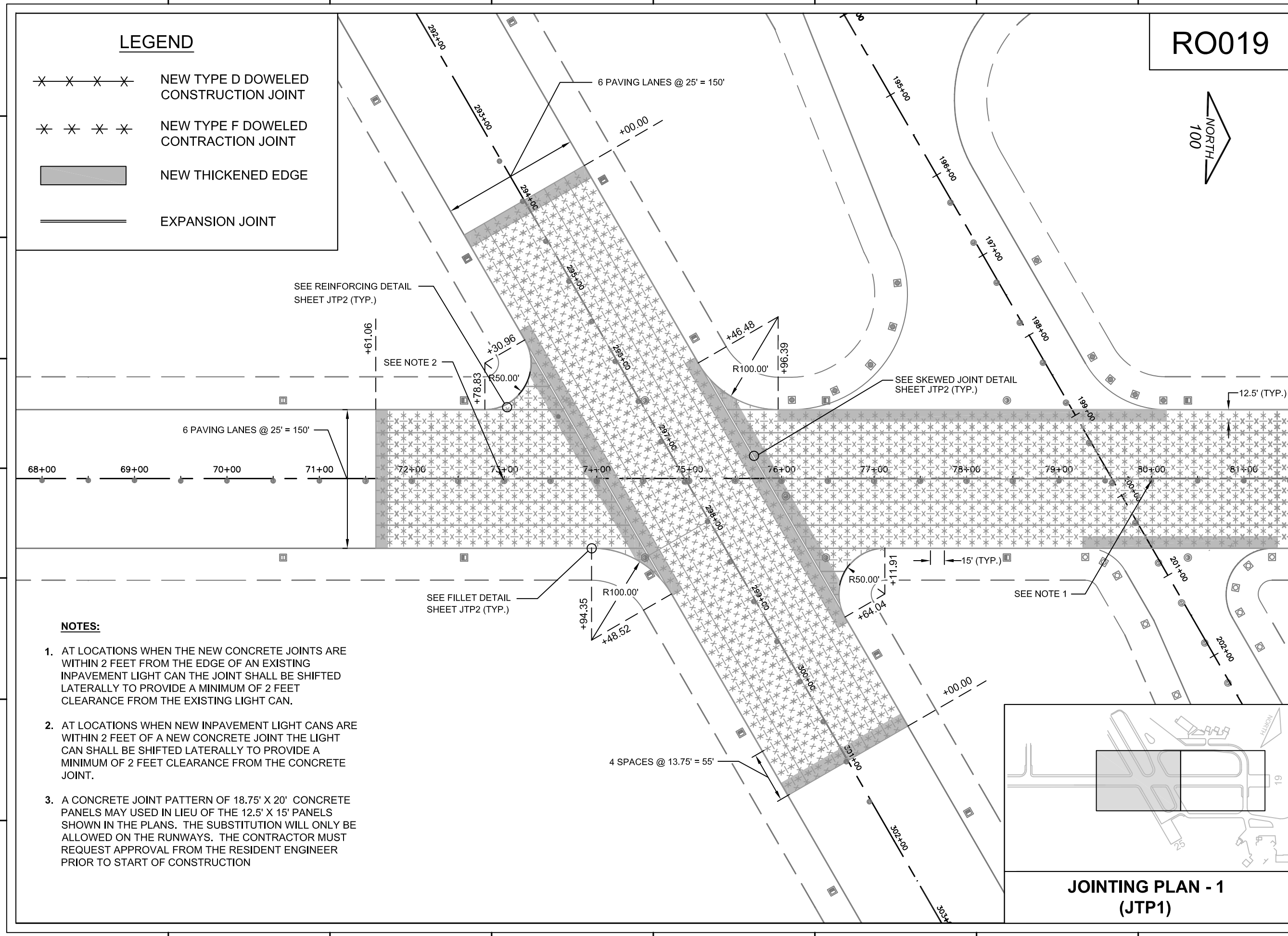
SEE NOTE 1

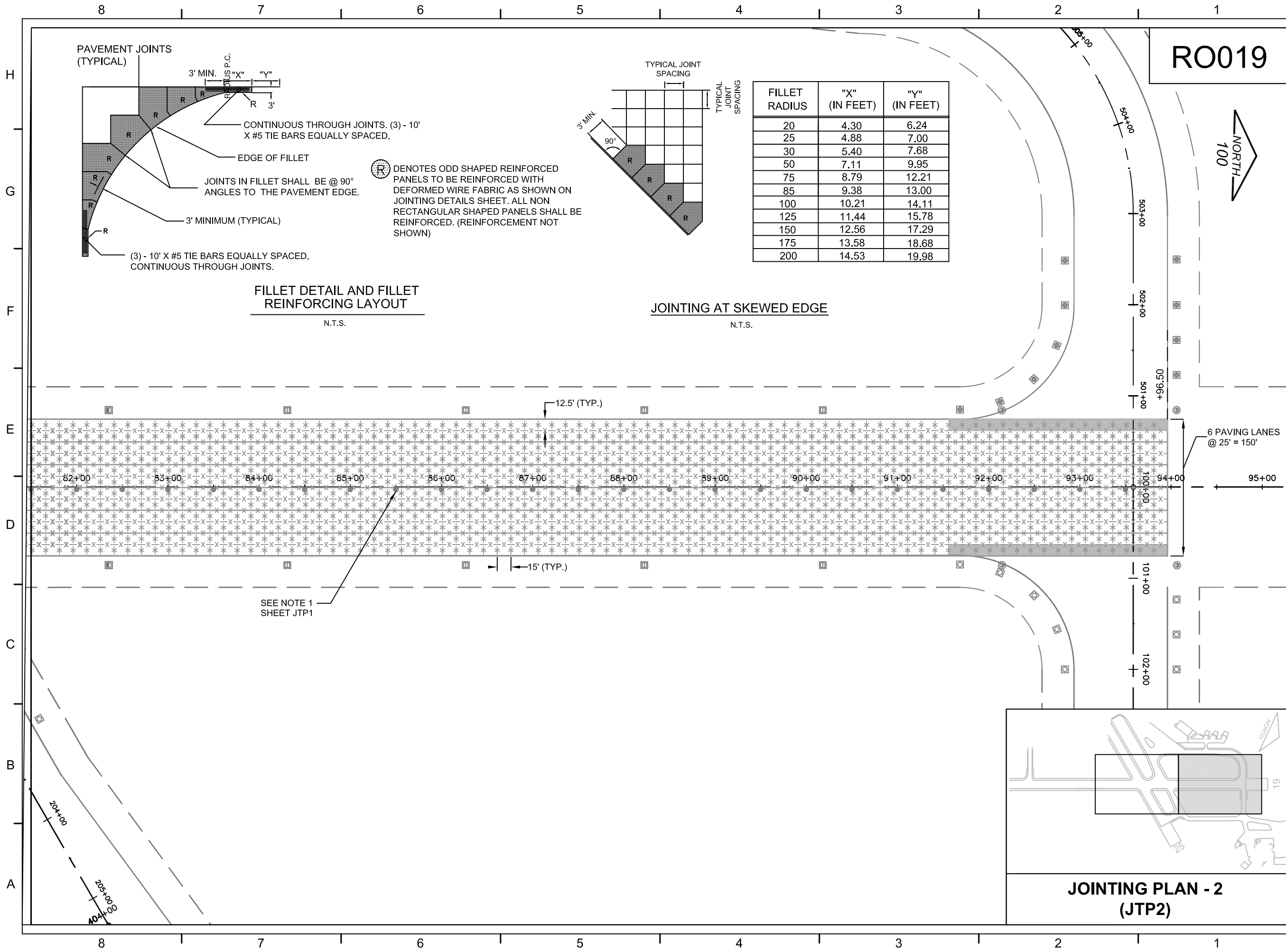
NOTES:

1. AT LOCATIONS WHEN THE NEW CONCRETE JOINTS ARE WITHIN 2 FEET FROM THE EDGE OF AN EXISTING INPAVEMENT LIGHT CAN THE JOINT SHALL BE SHIFTED LATERALLY TO PROVIDE A MINIMUM OF 2 FEET CLEARANCE FROM THE EXISTING LIGHT CAN.
2. AT LOCATIONS WHEN NEW INPAVEMENT LIGHT CANS ARE WITHIN 2 FEET OF A NEW CONCRETE JOINT THE LIGHT CAN SHALL BE SHIFTED LATERALLY TO PROVIDE A MINIMUM OF 2 FEET CLEARANCE FROM THE CONCRETE JOINT.
3. A CONCRETE JOINT PATTERN OF 18.75' X 20' CONCRETE PANELS MAY USED IN LIEU OF THE 12.5' X 15' PANELS SHOWN IN THE PLANS. THE SUBSTITUTION WILL ONLY BE ALLOWED ON THE RUNWAYS. THE CONTRACTOR MUST REQUEST APPROVAL FROM THE RESIDENT ENGINEER PRIOR TO START OF CONSTRUCTION



**JOINTING PLAN - 1
(JTP1)**





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PROJECT NAME:
REHABILITATE RUNWAY 1/19 PHASE 1

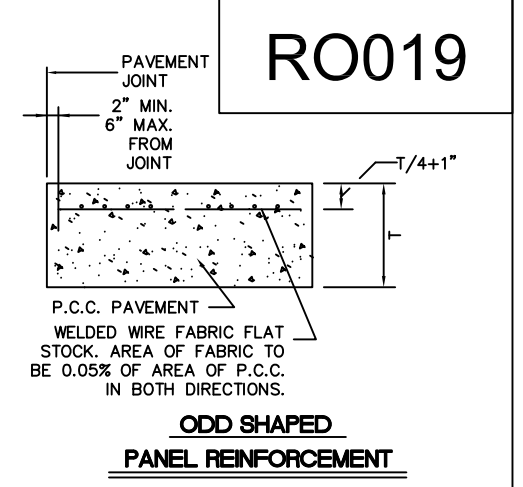
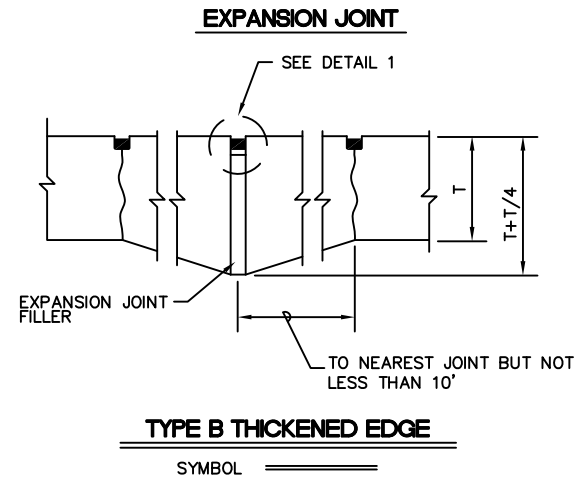
SHEET TITLE:
JOINTING PLAN - 2 (JTP2)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06 DATE: APRIL 22, 2011

TABLE 1	
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"
19	6.33"
20	6.67"

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"
19	1 - 1/2"	20"	18"	#5	30"	30"
20	1 - 1/2"	20"	18"	#5	30"	30"



RO019



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JOINTING NOTES

1. ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
2. THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO INSTALLATION. ALTERNATE METHODS OF PLACEMENT OF DOWEL BARS MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. TRANSVERSE DOWEL BAR IMPLANTING WILL NOT BE ALLOWED.
4. ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
5. THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
6. COST OF ALL JOINT SAWING, CLEANING AND SEALING SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
7. SHOULD THE POURING OPERATION REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
8. EPOXY-COATED DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE RESIDENT ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
9. CONCRETE / BITUMINOUS INTERFACE SHALL BE SEALED PER TYPE E HINGED JOINT DETAIL ABOVE.
10. ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
11. TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
12. CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS AT NO ADDITIONAL COST.
13. JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.

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PROJECT NAME:		

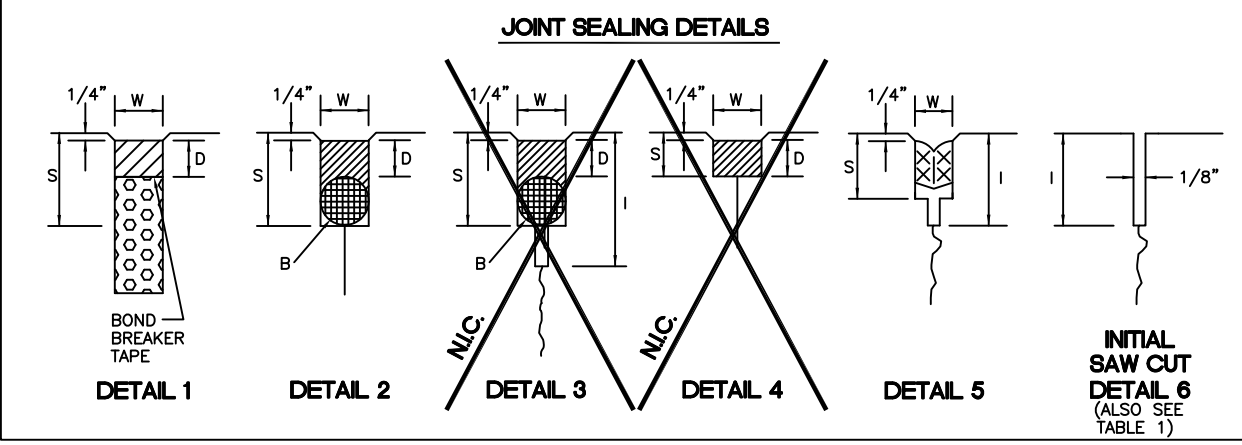
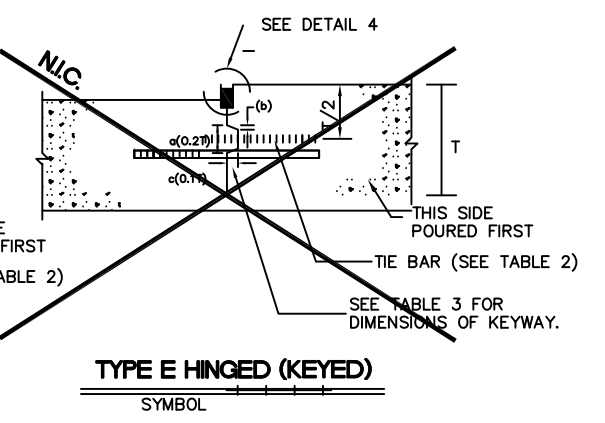
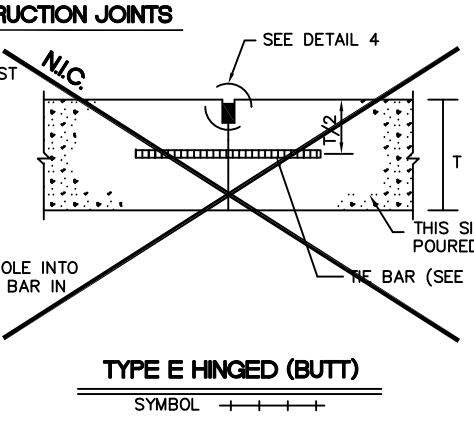
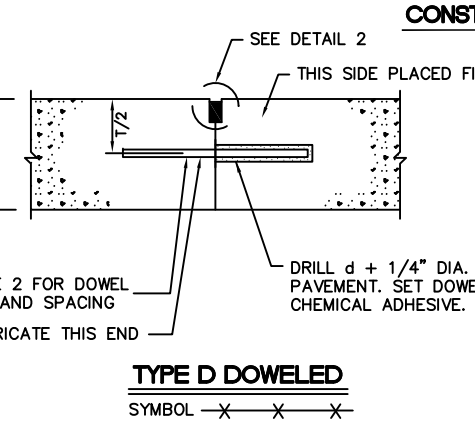
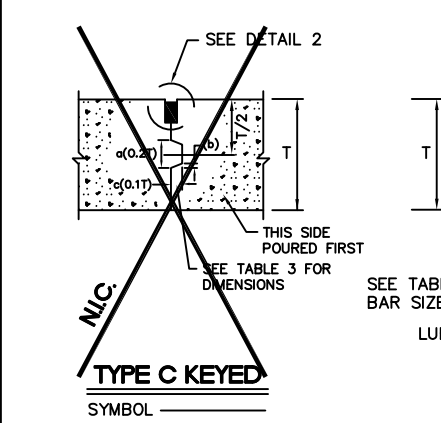
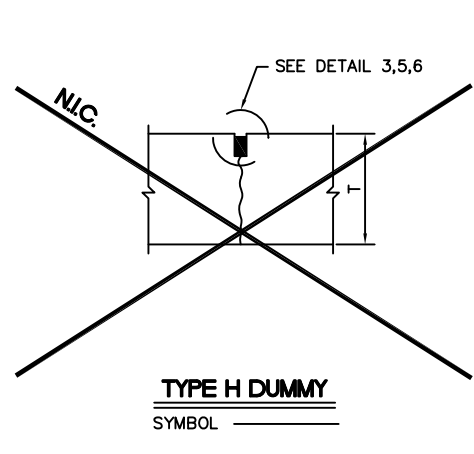
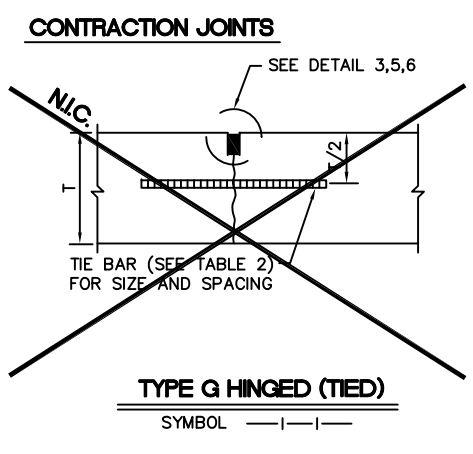
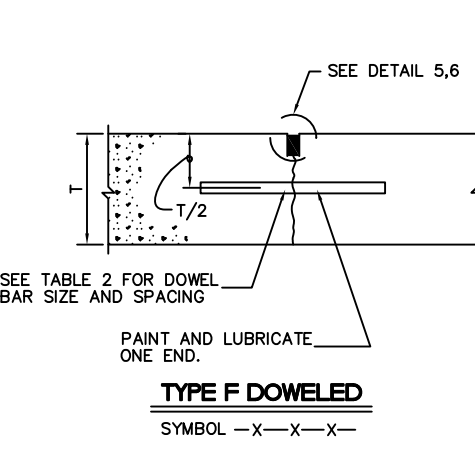
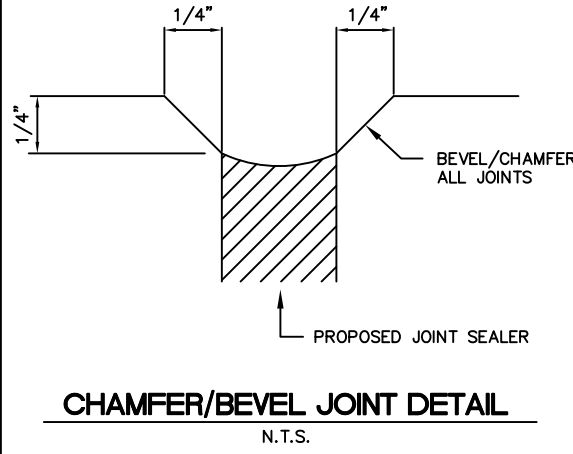
REHABILITATE RUNWAY 1/19
PHASE 1

SHEET TITLE:
JOINTING DETAILS - 1 (JDT1)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

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NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS.

	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT POUR	HOT POUR	HOT POUR	HOT/COLD POUR	SILICONE PREFORMED
W=WIDTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	1/2	1/2	3/8 (COM-PRESSED)
D=DEPTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	1/2	1/2	N/A
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	3/4	1-1/2

JOINTING DETAILS - 1 (JDT1)



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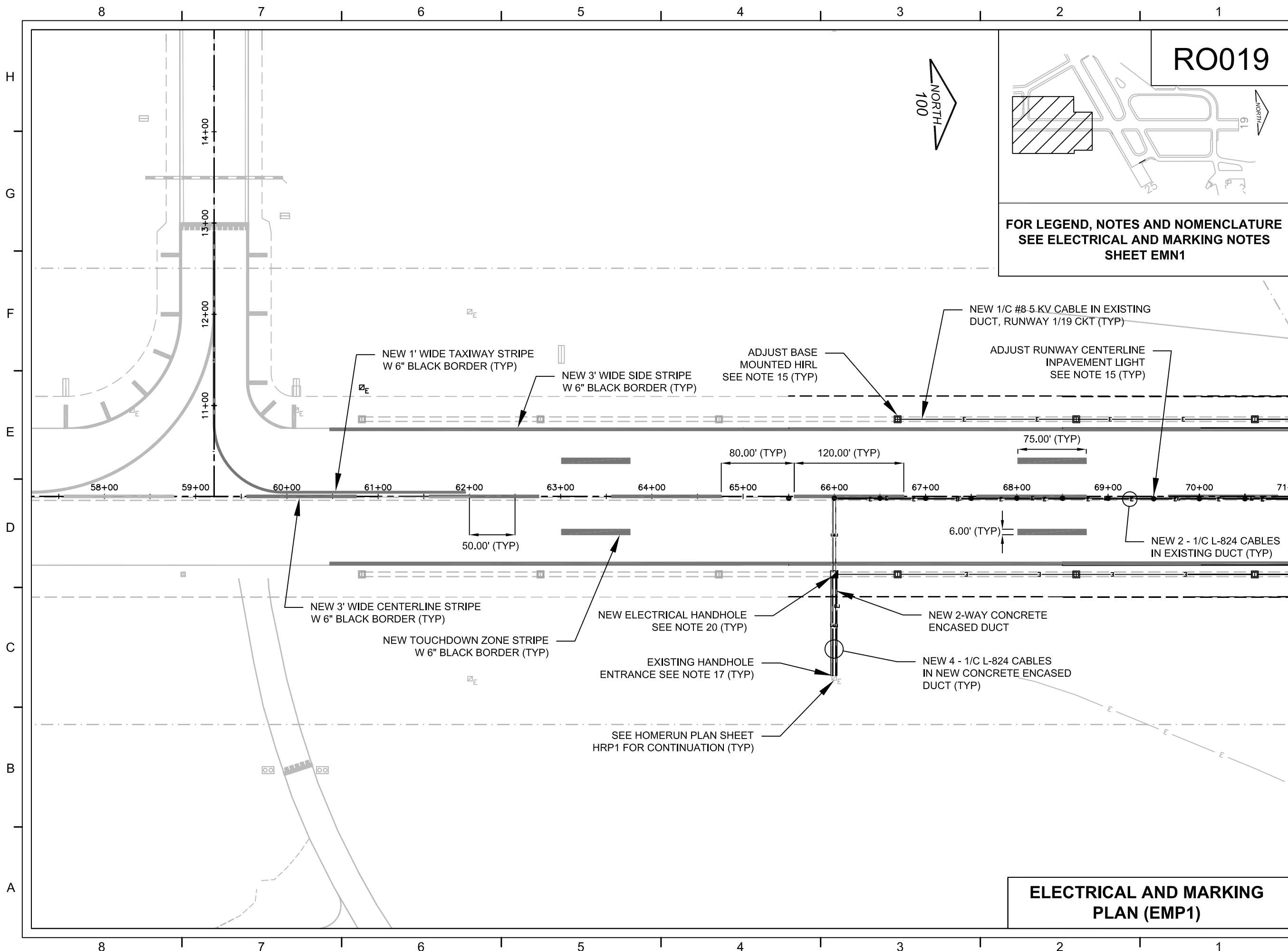
REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
**ELECTRICAL AND
MARKING PLAN (EMP1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

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REV DATE DESCRIPTION

PROJECT NAME:

**REHABILITATE
RUNWAY 1/19
PHASE 1**

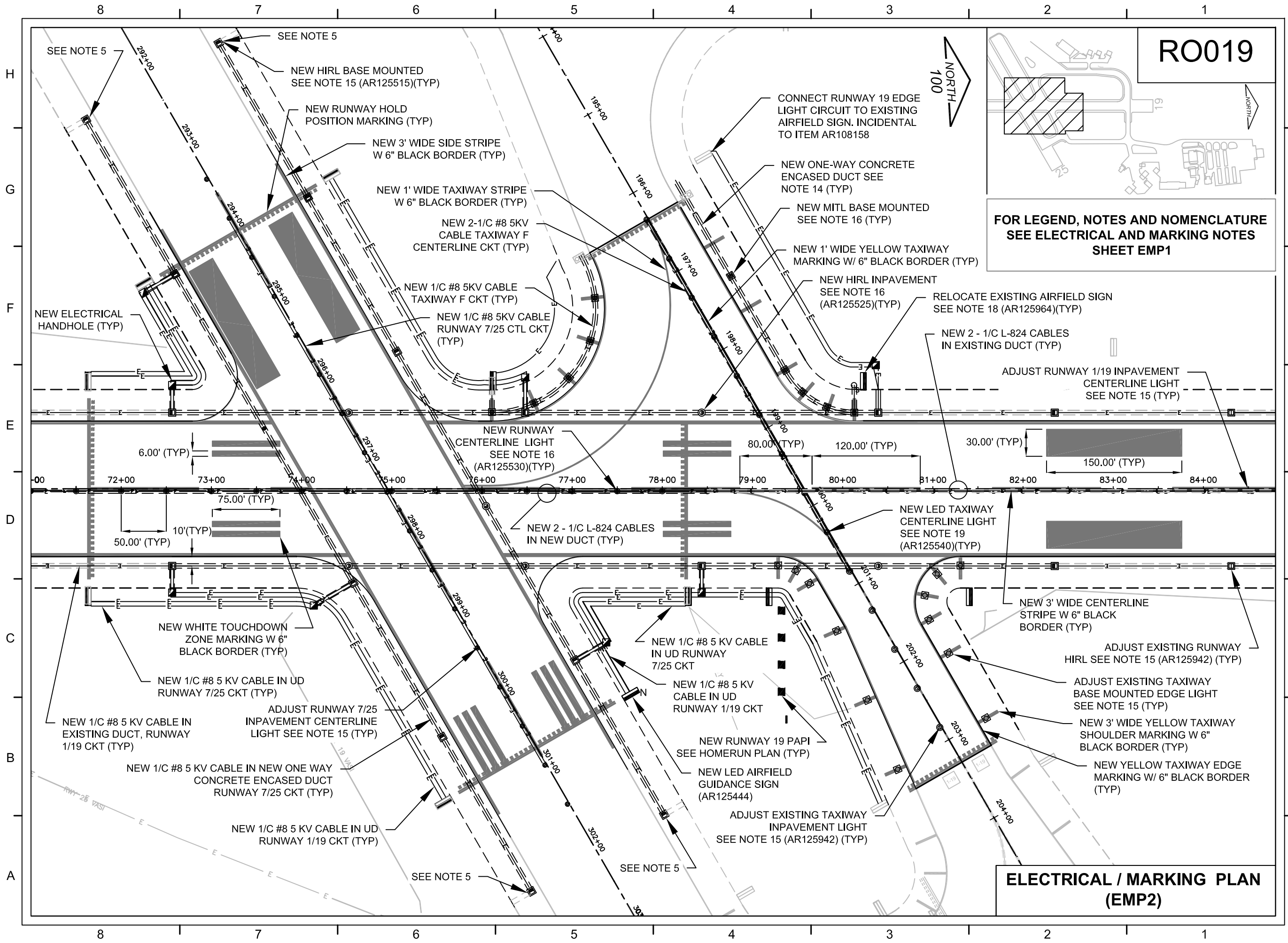
SHEET TITLE:
**ELECTRICAL / MARKING
PLAN (EMP2)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
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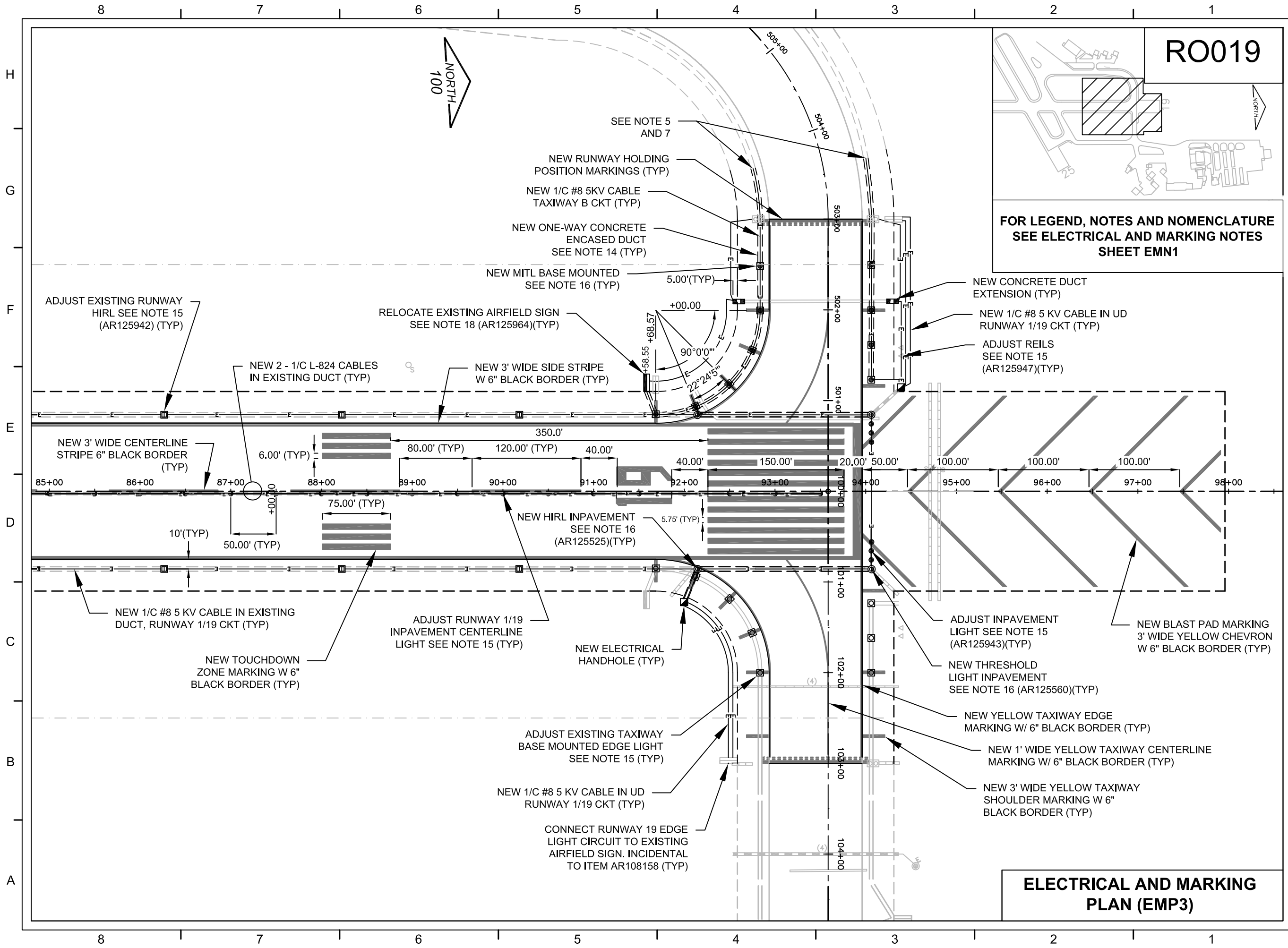
SHEET 36 OF 67 SHEETS



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**FOR LEGEND, NOTES AND NOMENCLATURE
SEE ELECTRICAL AND MARKING NOTES
SHEET EMP1**

**ELECTRICAL / MARKING PLAN
(EMP2)**



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REV	DESCRIPTION

PROJECT NAME:
REHABILITATE RUNWAY 1/19 PHASE 1

SHEET TITLE:
ELECTRICAL AND MARKING PLAN (EMP3)

DESIGNED:	DRAWN:	CHECKED:
CMT-RFD	CMT-RFD	CMT-ARR

PROJECT NO.: 10258-03-00/06
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SHEET NO.	37	REVISION
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FOR LEGEND, NOTES AND NOMENCLATURE SEE ELECTRICAL AND MARKING NOTES SHEET EMN1

ELECTRICAL AND MARKING PLAN (EMP3)

LEGEND

- NEW LIGHTING CIRCUIT - 1/C#8, 5KV L-824
TYPE C IN CONCRETE ENCASED 2" PVC DUCT
- NEW LIGHTING CIRCUIT - 1/C#8, 5KV L-824
TYPE C IN EXISTING DUCT
- NEW TAXIWAY CIRCUIT - 1/C#8, 5KV L-824
TYPE C IN 3/4" UNIT DUCT
- EXISTING CIRCUITS
- NEW PAVEMENT MARKING
- EXISTING PAVEMENT MARKING
- NEW BASE MOUNTED MEDIUM INTENSITY
TAXIWAY LIGHT
- ADJUST EXISTING BASE MOUNTED MEDIUM
INTENSITY TAXIWAY LIGHT
- EXISTING ELEVATED RUNWAY GUARD LIGHT
- NEW ELECTRICAL HANDHOLE
- EXISTING ELECTRICAL HANDHOLE
- NEW CONDUIT/DUCT EXTENSION
- EXISTING CONDUIT/DUCT
- NEW AIRFIELD GUIDANCE LED SIGN
- RELOCATE EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING AIRFIELD GUIDANCE SIGN
- NEW BASE MOUNTED RUNWAY EDGE LIGHT
- ADJUST BASE MOUNTED RUNWAY EDGE LIGHT
- NEW INPAVEMENT LIGHT
- ADJUST INPAVEMENT LIGHT

ELECTRICAL NOTES:

1. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
2. AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
3. THE LOCATION OF EXISTING UTILITIES ARE APPROXIMATE. THE CONTRACTOR SHALL OPEN THE ENTIRE TRENCH BETWEEN MANHOLES BEFORE ANY CONDUIT IS LAID TO ASCERTAIN THE EXISTENCE AND POSITION OF ANY OBSTRUCTIONS.
4. CONTRACTOR SHALL COORDINATE THE LOCATION OF THE EXISTING AND PROPOSED UTILITIES PRIOR TO INSTALLATION OF THE PROPOSED UNIT DUCTS, CONDUITS AND DUCT BANKS. ANY DAMAGES TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
5. CONTRACTOR SHALL CONNECT EXISTING CABLE TO PROPOSED CABLE IN SIGN/LIGHT/MANHOLE/HANDHOLE. COST OF CONNECTION INCIDENTAL TO CABLE.
6. ALL NEW CABLE UNDER EXISTING PAVEMENT TO REMAIN SHALL BE INSTALLED WITHIN EXISTING CONDUITS. EXISTING CABLES SHALL BE REMOVED. COST OF REMOVAL SHALL BE INCIDENTAL TO NEW CABLE.
7. CONTRACTOR SHALL CORE HOLE AND INSTALL BUSHING IN EXISTING LIGHT CAN AND CONNECT NEW 2" PVC DUCT (INCIDENTAL TO ONE WAY CONCRETE ENCASED DUCT).
8. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
9. THE ROUTING OF THE PROPOSED DUCTS AND CONDUITS ARE SHOWN FOR INFORMATION ONLY. THE EXACT ROUTING SHALL BE COORDINATED WITH THE RESIDENT ENGINEER.
10. SEE DETAILS ON SHEETS ELD1-3.
11. ALL EXISTING UNUSED AIRFIELD LIGHTING CABLE SHALL BE REMOVED FROM THE UNIT DUCT. UNIT DUCT SHALL BE ABANDONED IN PLACE. COST SHALL BE INCIDENTAL TO INSTALLATION OF NEW CABLE.
12. INSTALLATION AND SPLICING OF FAA CONTROL CABLE SHALL BE COORDINATED WITH FAA. INSTALLATION AND SPLICING OF FAA CABLE SHALL BE IN COMPLIANCE WITH FAA STANDARD SPECIFICATIONS.
13. CONNECT IN-PAVEMENT 1/C#6 INSULATED GROUNDING CONDUCTOR TO GROUND ROD AT EDGE LIGHT BASE. TAXIWAY CENTERLINE LIGHTS SHALL BE INSTALLED SO THAT THE NEAREST EDGE IS MINIMUM OF 2 FEET FROM ANY CONCRETE PAVEMENT JOINT.
14. EXISTING PAVEMENT SHALL BE SAWED AND REMOVED IN ACCORDANCE WITH THE ELECTRICAL DETAILS AND NEW PVC CONDUIT INSTALLED. THE TRENCH SHALL BE BACKFILLED WITH 610 CONCRETE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND IN ACCORDANCE WITH THE ELECTRICAL DETAILS. PAVEMENT REMOVAL AND CONCRETE BACKFILL SHALL BE INCIDENTAL TO THE CONCRETE ENCASED DUCT PAY ITEM.
15. ADJUST BASE-MOUNTED AND IN-PAVEMENT LIGHT SHALL INCLUDE INSTALLATION OF REQUIRED GASKETS, EXTENSIONS, SPACER RINGS, Y-FLANGES AND MUD PLATES AS NEEDED, SEALANT, NEW L-830 ISOLATION TRANSFORMERS, L-823 CONNECTORS AND GROUNDING. EXISTING LIGHT FIXTURES AND CANS SHALL BE RE-USED.
16. NEW BASE MOUNTED AND IN-PAVEMENT LIGHT SHALL INCLUDE INSTALLATION OF NEW BASE CANS, REQUIRED GASKETS, EXTENSIONS, SPACER RINGS, Y-FLANGES AND MUD PLATES AS NEEDED, SEALANT, NEW L-830 ISOLATION TRANSFORMERS, L-823 CONNECTORS AND GROUNDING. EXISTING LIGHT FIXTURES WILL BE RE-USED.
17. CONNECT NEW CONCRETE ENCASED DUCT TO EXISTING HANDHOLE. COST SHALL BE INCIDENTAL TO NEW CONCRETE ENCASED DUCT.
18. RELOCATE EXISTING SIGN SHALL INCLUDE NEW CONCRETE FOUNDATION, NEW L-867 CAN, NEW L-830 TRANSFORMER AND GROUNDING. EXISTING SIGN SHALL BE REMOVED AND RE-INSTALLED. EXISTING FOUNDATION SHALL BE COMPLETELY REMOVED.
19. NEW TAXIWAY CENTERLINE LIGHT SHALL INCLUDE NEW LIGHT FIXTURE, NEW L-868 CAN, REQUIRED GASKETS, EXTENSIONS, SPACER RINGS, Y-FLANGES AND MUD PLATES AS NEEDED, GROUNDING AND NEW L-830 ISOLATION TRANSFORMERS.
20. THE CONTRACTOR SHALL INTERCEPT THE RUNWAY EDGE LIGHT CIRCUIT CONDUIT AND RUNWAY CENTERLINE CIRCUIT CONDUIT AND CONNECT TO THE NEW ELECTRICAL HANDHOLE. COST SHALL BE INCIDENTAL TO THE ELECTRICAL HANDHOLE.

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PAVEMENT MARKING NOTES

1. SEE DETAILS ON SHEETS MRD 1-2.
2. ALL RUNWAY MARKINGS ARE WHITE WITH A 6" BLACK BORDER.
3. ALL TAXIWAY MARKINGS ARE YELLOW WITH A 6" BLACK BORDER.
4. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
5. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE. EXCEPT RUNWAY/RUNWAY HOLDING.
6. CURING COMPOUND ON CONCRETE PAVEMENTS SHALL BE REMOVED BY WATER BLASTING OR OTHER METHODS APPROVED BY THE ENGINEER PRIOR TO MARKING.
7. ALL PROPOSED MARKING MAY NOT BE CALLED OUT FOR CLARITY. ALL MARKING WITHIN THE PROPOSED IMPROVEMENT LIMITS TO INCLUDE THE ADDITIVE ALTERNATES, IF AWARDED, WILL BE RE-MARKED. THE RESIDENT ENGINEER WILL PROVIDE MARKING DIMENSIONS AS NEEDED.

**ELECTRICAL AND MARKING NOTES
(EMN1)**



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PROJECT NAME:

**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:

**ELECTRICAL AND
MARKING NOTES (EMN1)**

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
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PROJECT NO.: 10258-03-00/06
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NOTES

1. CONTRACTOR SHALL CORE HOLES INTO EXISTING MANHOLE, INSTALL NEW DUCT BANK, CABLE AND SEAL ALL PENETRATIONS INTO MANHOLE. COST SHALL BE INCIDENTAL TO PAY ITEM AR110502.
2. CONTRACTOR SHALL INSTALL L-823 CONNECTORS FOR RUNWAY 19 CENTERLINE AND EDGE LIGHT CIRCUIT HOMERUNS TO VAULT. COST SHALL BE INCIDENTAL TO PAY ITEM AR108158.
3. UPGRADE EXISTING REGULATOR AND ALCMS FOR RUNWAY 19 CENTERLINE NORTH CIRCUIT AND RUNWAY 19 EDGE LIGHT CIRCUIT. MEGGAR CIRCUIT BEFORE AND AFTER COMPLETION OF PROJECT. UPGRADING OF REGULATOR AND MEGGARING SHALL BE PAID UNDER LUMP SUM PAY ITEM AR109210 "VAULT MODIFICATIONS".
4. EACH CABLE SHALL HAVE A MINIMUM OF 10' SLACK IN EACH ELECTRICAL MANHOLE AND SHALL BE TAGGED AND SHALL BE PAID PER LINEAR FOOT. ALL OTHER CABLE SLACK IN LIGHTS AND SPLICE CANS WILL BE INCIDENTAL TO THE LIGHT OR SPLICE CAN.
5. CONTRACTOR SHALL REMOVE EXISTING VASI TRANSFORMERS AND CIRCUITS INSIDE VAULT. INSTALL NEW CIRCUITS TO EXISTING 480V DISTRIBUTION PANEL, AND INSTALL (2) 20A, 2 POLE CIRCUIT BREAKERS. COST SHALL BE INCIDENTAL TO VAULT MODIFICATIONS.
6. CONTRACTOR SHALL REMOVE EXISTING RUNWAY 1/19 HIRL AND CENTERLINE CIRCUITS. COST SHALL BE INCIDENTAL TO NEW CIRCUITS.



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REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
HOMERUN PLAN
(HRP1)

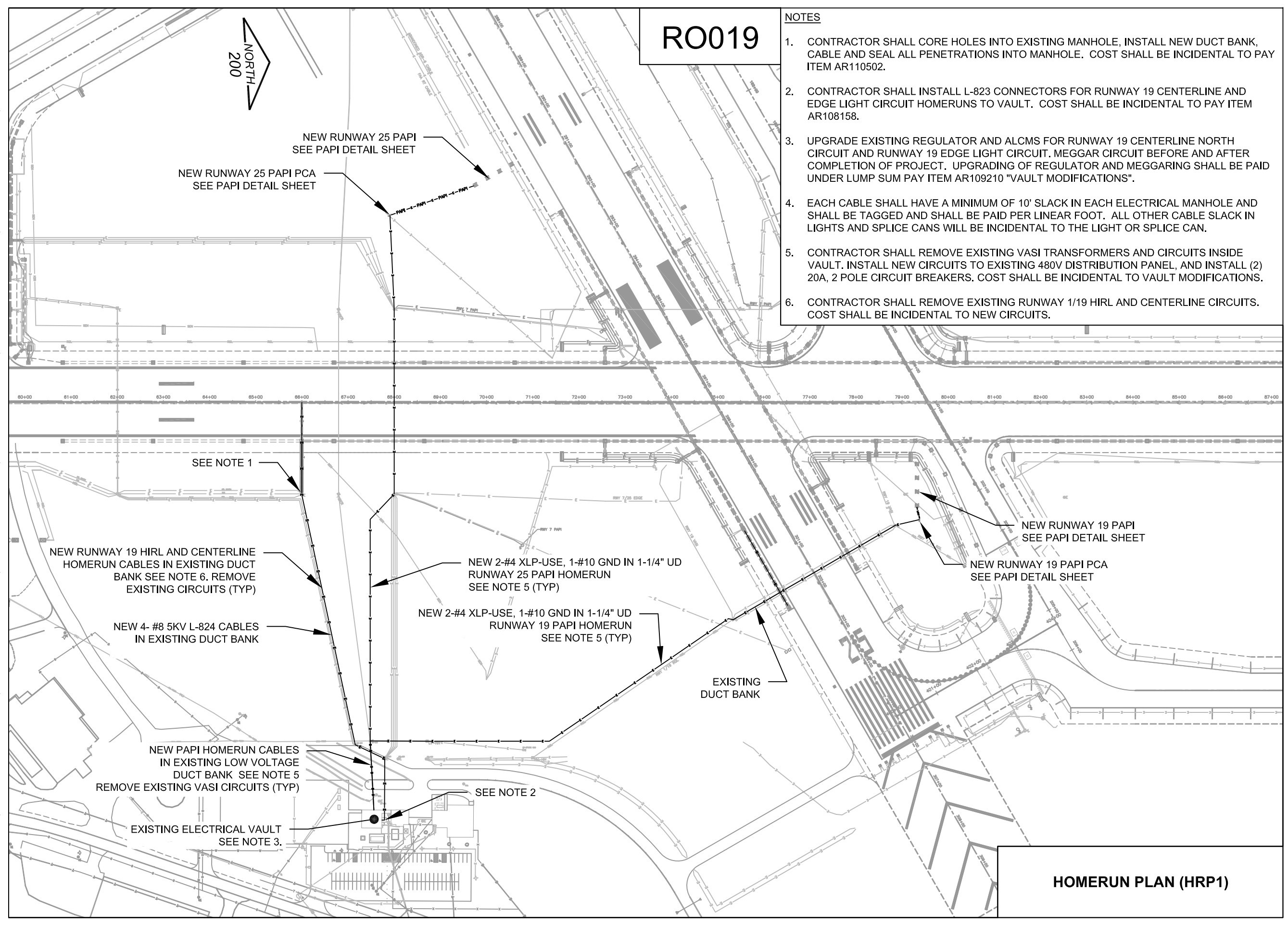
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HOMERUN PLAN (HRP1)



NEW RUNWAY 25 PAPI
SEE PAPI DETAIL SHEET

NEW RUNWAY 25 PAPI PCA
SEE PAPI DETAIL SHEET

SEE NOTE 1

NEW RUNWAY 19 HIRL AND CENTERLINE
HOMERUN CABLES IN EXISTING DUCT
BANK SEE NOTE 6. REMOVE
EXISTING CIRCUITS (TYP)

NEW 4- #8 5KV L-824 CABLES
IN EXISTING DUCT BANK

NEW PAPI HOMERUN CABLES
IN EXISTING LOW VOLTAGE
DUCT BANK SEE NOTE 5
REMOVE EXISTING VASI CIRCUITS (TYP)

EXISTING ELECTRICAL VAULT
SEE NOTE 3.

NEW 2-#4 XLP-USE, 1-#10 GND IN 1-1/4" UD
RUNWAY 25 PAPI HOMERUN
SEE NOTE 5 (TYP)

NEW 2-#4 XLP-USE, 1-#10 GND IN 1-1/4" UD
RUNWAY 19 PAPI HOMERUN
SEE NOTE 5 (TYP)

SEE NOTE 2

EXISTING
DUCT BANK

NEW RUNWAY 19 PAPI
SEE PAPI DETAIL SHEET

NEW RUNWAY 19 PAPI PCA
SEE PAPI DETAIL SHEET

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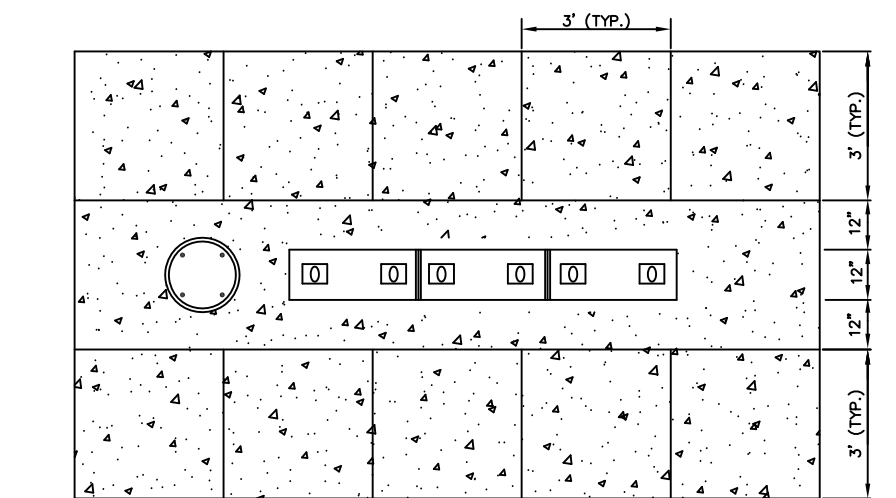
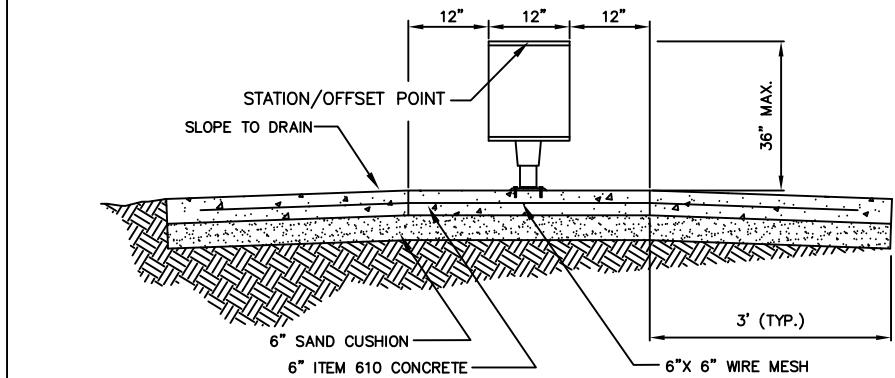
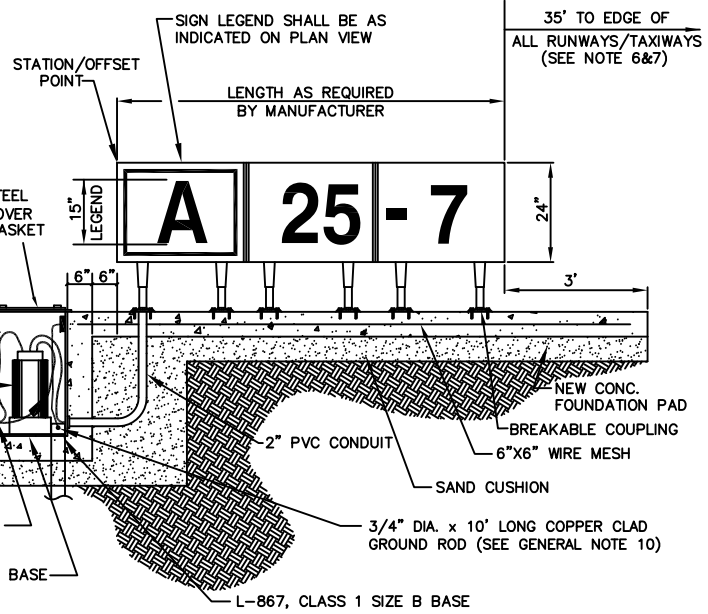
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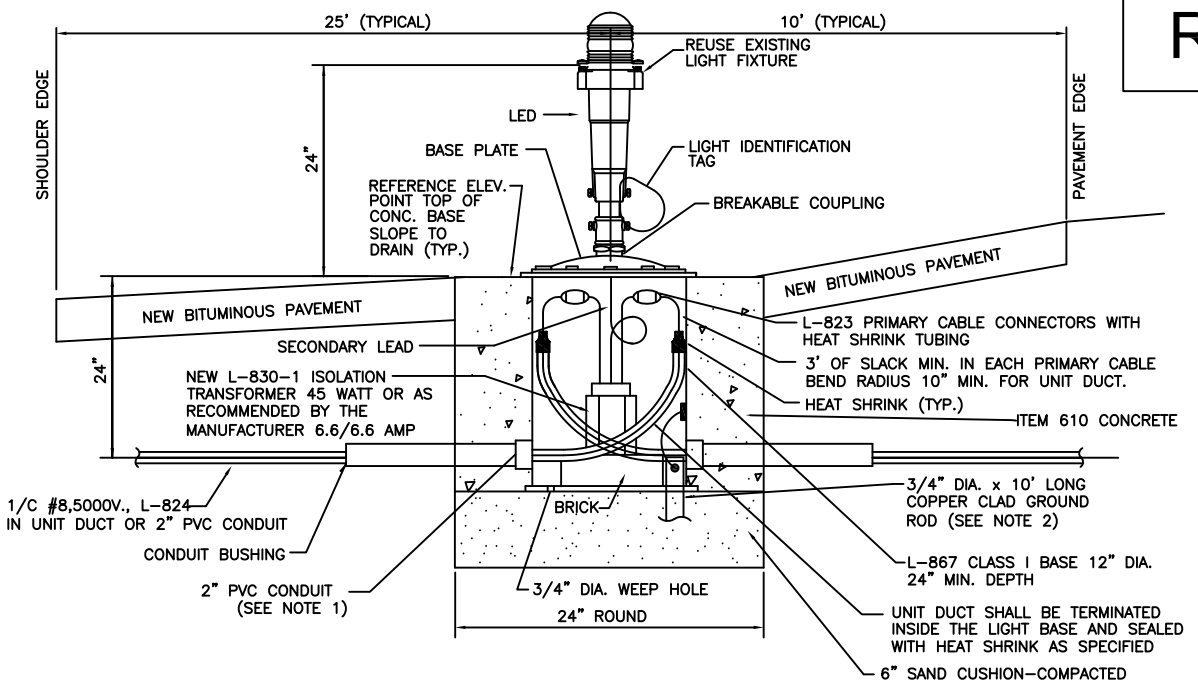
- ALL SIGNS ARE 2 - SIDED SIGNS.
- TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER
- LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
- UNIT DUCT SHALL BE TERMINATED IN THE CAN AND SEALED TO THE CABLE WITH HEAT SHRINK AS SPECIFIED.
- ALL NEW SIGNS SHALL BE LED TYPE, L-858 APPROVED BY FAA.
- TYPICAL PERPENDICULAR DISTANCES FOR TAXIWAY INTERSECTION SIGNS FROM CENTERLINE OF CROSSING TAXIWAY IS 160' UNLESS OTHERWISE NOTED.
- TYPICAL DISTANCES FOR RUNWAY HOLD SIGNS FROM RUNWAY CENTERLINE ARE AS FOLLOWS:
1/19 = 300'
7/25 = 300'
- ALL SIGNS CONNECTED TO HIGH INTENSITY RUNWAY CIRCUIT (5-STEP) SHALL BE STYLE 3 (5-STEP).
- FOR ITEM AR125964 EXISTING SIGN IS TO BE RE-USED.



**NEW / RELOCATED TAXI GUIDANCE SIGN, X CHARACTER
HOLD LINE/TAXIWAY GUIDANCE SIGN LED L-858, SIZE 2, STYLE 2/3, CLASS 2**

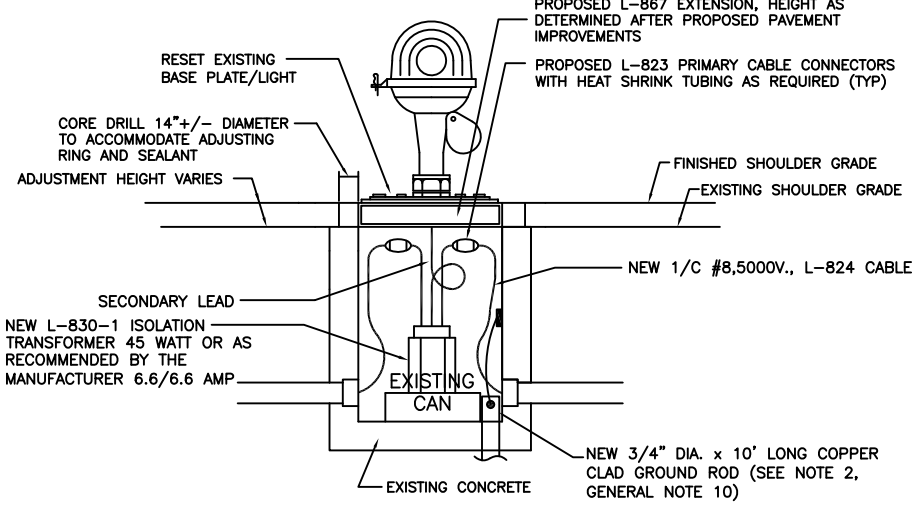
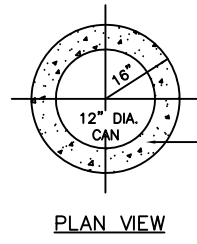
CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWING INCLUDING SIGN, COLOR, SIZE, PROPOSED LEGEND, IN ENOUGH DETAIL AND DETERMINE NEW SPACING AND OTHER INFORMATION REQUIRED BY SPECIAL PROVISIONS. CONTRACTOR TO VERIFY NEW SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION. SIGN SHALL BE FAA APPROVED LED TYPE SIGN.

RO019



NEW HIRL/MIL BASE MOUNTED LIGHT

- NOT TO SCALE
- INSTALL 2" PVC TO GRS ADAPTER WHERE PVC CONCRETE ENCASED DUCT WILL BE INSTALLED UNDER PAVED SHOULDER.
 - INSTALL 1/8" #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELD TO GROUND ROD.
 - PROVIDE MINIMUM OF ONE LOOP OF CABLE SLACK FOR EACH CABLE INSIDE LIGHT BASE/HANDHOLE.



ADJUST BASE MOUNTED LIGHT

- NOT TO SCALE
- INSTALL 2" PVC TO GRS ADAPTER WHERE PVC CONCRETE ENCASED DUCT WILL BE INSTALLED UNDER PAVED SHOULDER.
 - INSTALL 1/8" #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELD TO GROUND ROD.
 - PROVIDE MINIMUM OF ONE LOOP OF CABLE SLACK FOR EACH CABLE INSIDE LIGHT BASE/HANDHOLE.

GENERAL NOTES:

- THE CONCRETE BASE FOR BASE MTD. LIGHTS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE. SLOPE TO DRAIN (610).
- TRANSFORMER HOLDER SHALL BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS COME WITH A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/8" No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- ALL SIGNS, LIGHTS, CABLES AND TRANSFORMERS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE AIRPORT. AT THE DISCRETION OF THE AIRPORT DIRECTOR, THE CONTRACTOR MAY BE REQUIRED TO DISPOSE OF THESE MATERIALS OFFSITE AT NO ADDITIONAL COST.
- TAXIWAY LIGHTS SHALL HAVE A BLUE LENS, RUNWAY LIGHTS SHALL HAVE A CLEAR OR 180° AMBER/CLEAR LENS AS DESIGNATED ON PLANS.
- DUCT MARKERS SHALL BE INSTALLED AT EVERY NEW DUCT AND AT EVERY EXISTING DUCT USED FOR THIS PROJECT.
- CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.
- INSTALL 1/8" #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD. INSTALL GROUND LUG FOR EXISTING CANS, IF REQUIRED.
- PROVIDE MINIMUM OF ONE LOOP OF CABLE SLACK FOR EACH CABLE INSIDE LIGHT BASE/HANDHOLE.
- EXISTING FIXTURES SHALL BE USED FOR ALL GUIDANCE SIGNS, TAXIWAY AND RUNWAY LIGHTS EXCEPT WHERE NOTED.

**ELECTRICAL DETAILS - 1
(ELD1)**



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**ELECTRICAL DETAILS - 1
(ELD1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO.	40	REVISION
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ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

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IL Proj: RFD-4083

APPROVED AS WORKING PLAN BY:

REV DATE DESCRIPTION
PROJECT NAME:

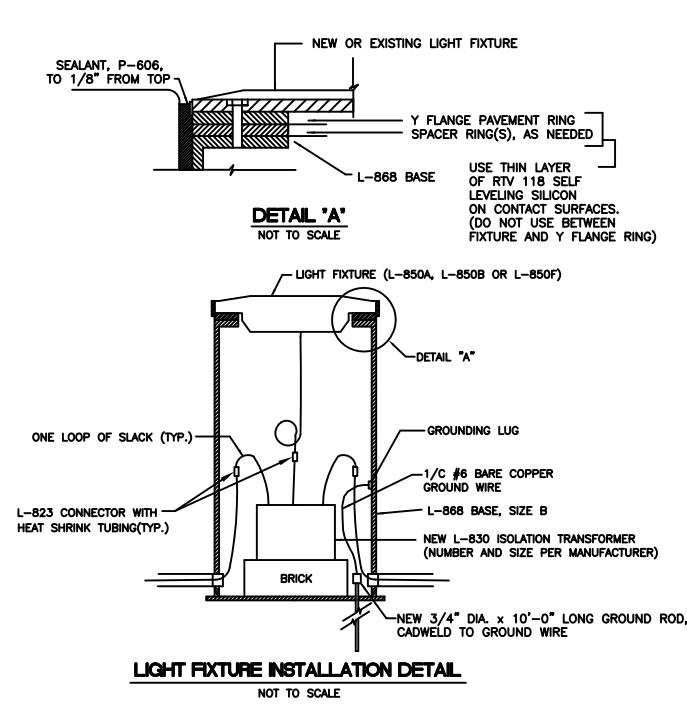
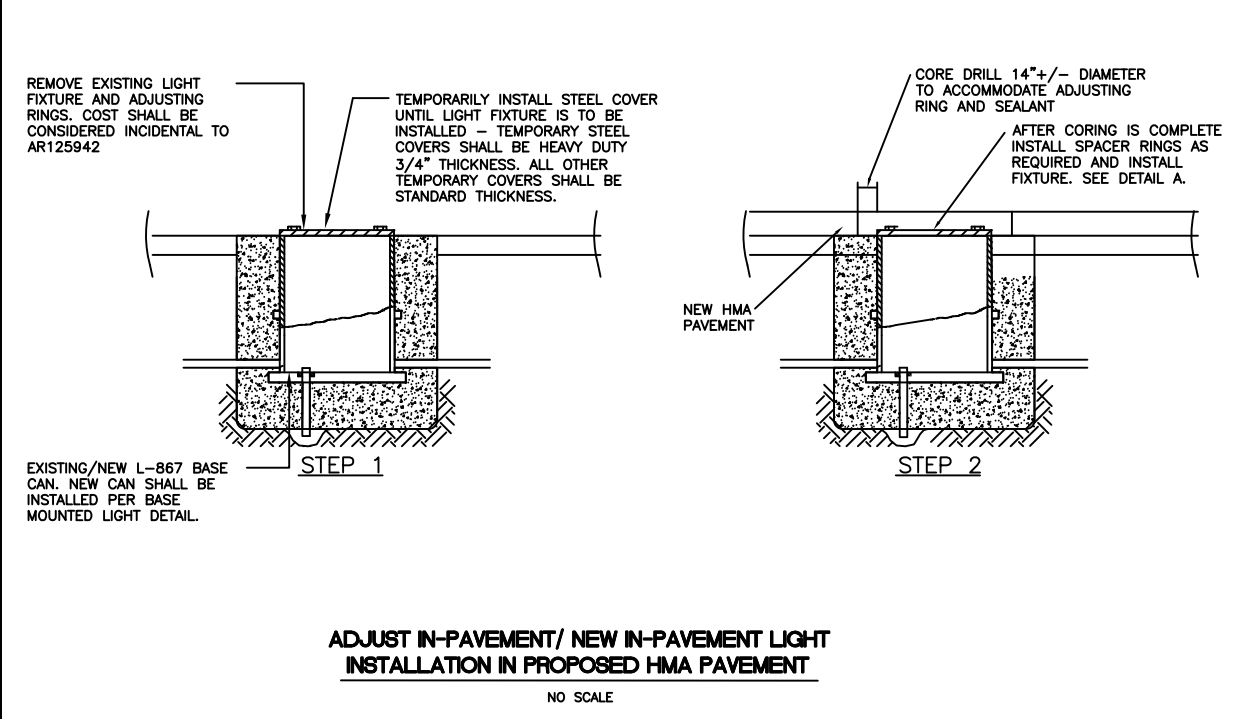
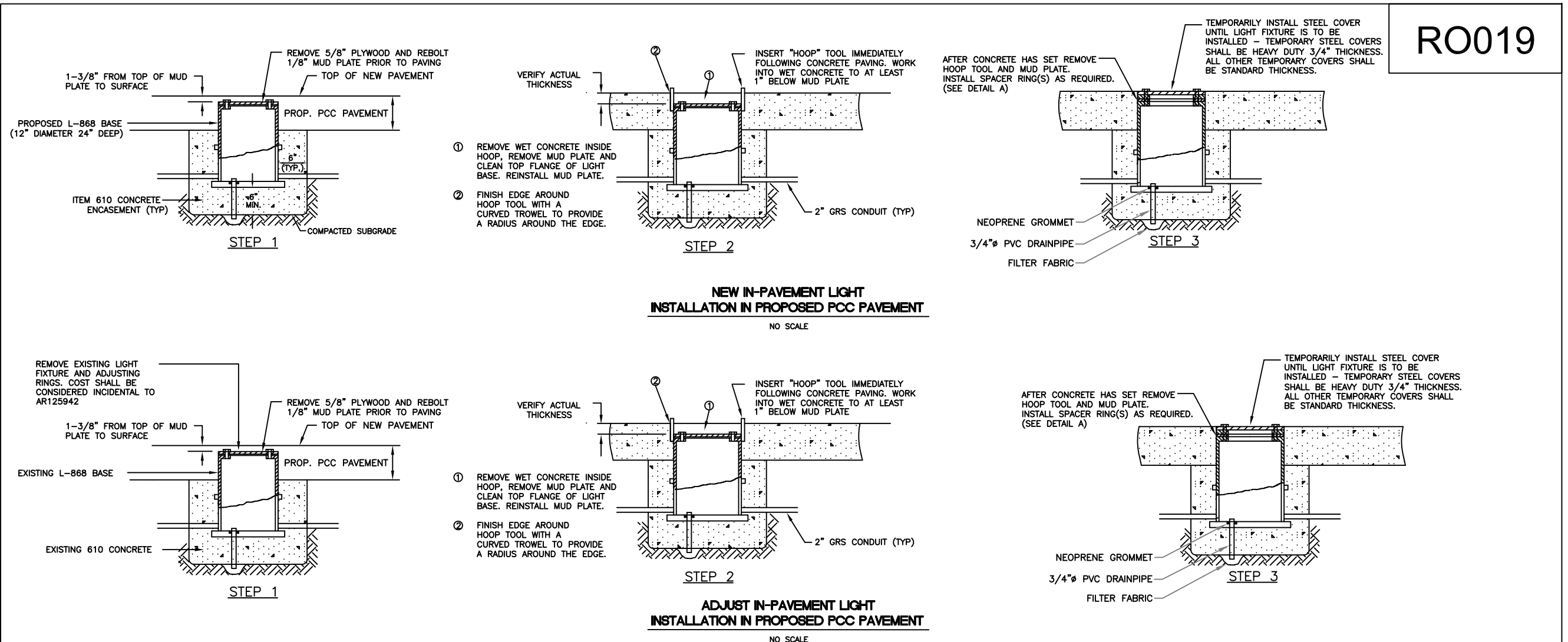
REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
ELECTRICAL DETAILS - 2
(ELD2)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. 41 REVISION



- PCC PAVEMENT NOTES**
- EXCAVATE TO PROPER DEPTH TO ALLOW 6" CONCRETE ENCASUREMENT UNDER NEW BASES AND 8" UNDER NEW CONDUIT. CLEAN CUT EDGES AND COMPACT BOTTOM OF EXCAVATION.
 - USE MANUFACTURER'S SETTING JIG (OR OTHER DEVICE APPROVED BY THE ENGINEER FOR PROPERLY ALIGNING NEW L-868 BASES. SECURE SETTING JIG TO PREVENT MOVEMENT DURING CONCRETE ENCASUREMENT. ALL CONDUITS TO BE SUPPORTED DURING CONSTRUCTION AS REQUIRED.
 - ALL LIGHT BASES SHALL BE PROPERLY POSITIONED AND ALIGNED AND CONDUIT CONNECTING THE BASES PROPERLY SECURED IN PLACE BEFORE POURING CONCRETE. ENSURE PROPER ALIGNMENT AFTER CONCRETE ENCASUREMENT OF NEW BASE BEFORE CONCRETE SETS UP. TIGHT CONNECTIONS MUST BE ASSURED TO PREVENT CONCRETE FROM ENTERING BASE OR CONDUIT.
 - AFTER INSTALLATION OF THE L-868 BASE AND WHILE PCC PAVING AT THE FIXTURE LOCATION IS WET, INSERT "HOOP" TOOL TO AT LEAST 1" BELOW MUD PLATE. REMOVE WET CONCRETE FROM INSIDE HOOP AND CLEAN TOP FLANGE SURFACE. FINISH THE CONCRETE AROUND THE HOOP TOOL WITH A CURVED RADIUS TROWEL. USE CAUTION TO AVOID AGGREGATE SEGREGATION DURING THIS PROCEDURE.
 - AFTER CONCRETE HAS SET, REMOVE HOOP TOOL AND MUD PLATE. INSTALL FLANGE AND SPACER RINGS AS REQUIRED AND LIGHT ASSEMBLY. A TEMPORARY STEEL COVER MAY BE INSTALLED IF LIGHT FIXTURE IS TO BE INSTALLED LATER.
 - AFTER FIXTURE INSTALLATION, FILL THE ANNULAR SPACE BETWEEN THE FIXTURE BASE AND SURROUNDING PAVEMENT WITH P-606 SEALANT. ANNULAR SPACE SHALL BE NO MORE THAN 3/4" WIDE.
 - INSTALL BASE AND RINGS SO THAT OUTER EDGE OF LIGHT FIXTURE WILL BE AT SAME ELEVATION AS THE FINISHED PAVEMENT SURFACE TO (+) 0" (-) 1/16" TOLERANCE. THE TOTAL THICKNESS OF THE SPACER/FLANGE RINGS SHALL BE NO LARGER THAN 3/4" MAX. NO MORE THAN 3 RINGS SHALL BE USED.

ELECTRICAL DETAILS - 2
(ELD2)

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REV	DATE	DESCRIPTION
PROJECT NAME:		

**REHABILITATE
RUNWAY 1/19
PHASE 1**

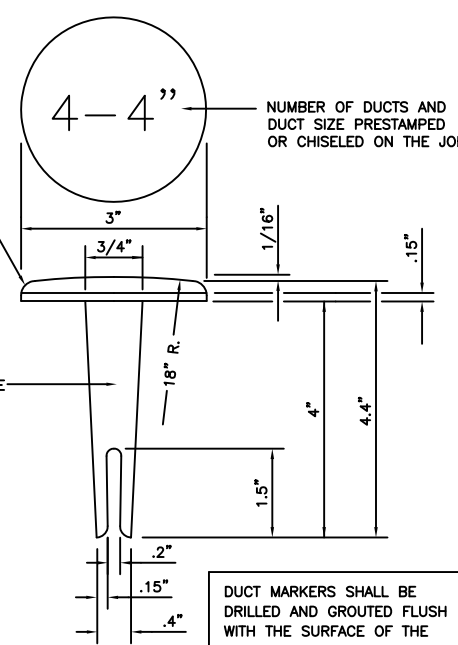
SHEET TITLE:
**ELECTRICAL DETAILS - 3
(ELD3)**

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
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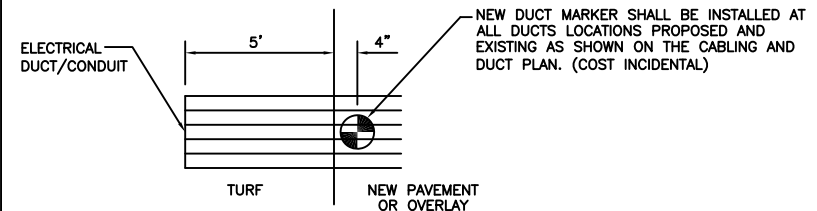
PROJECT NO.: 10258-03-00/06	DATE: APRIL 22, 2011
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SHEET NO. 42	REVISION
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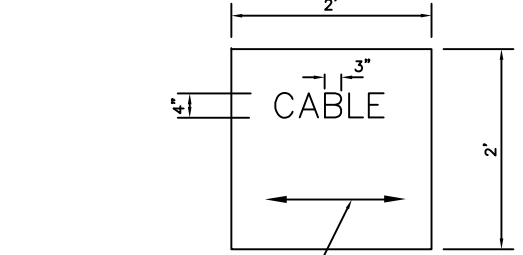
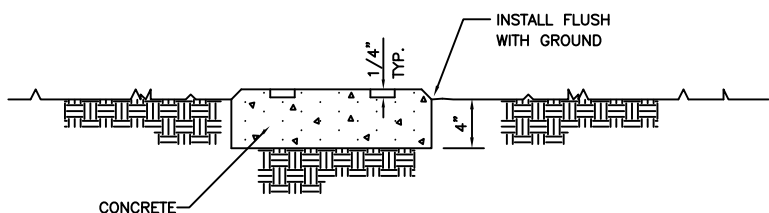
SHEET 42 OF 67 SHEETS



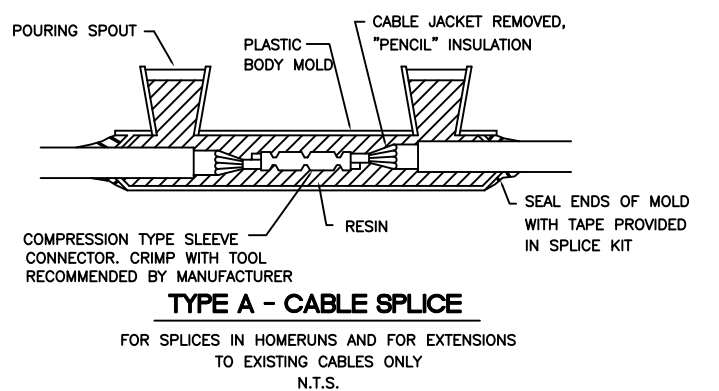
DUCT/CONDUIT MARKER DETAIL
NOT TO SCALE



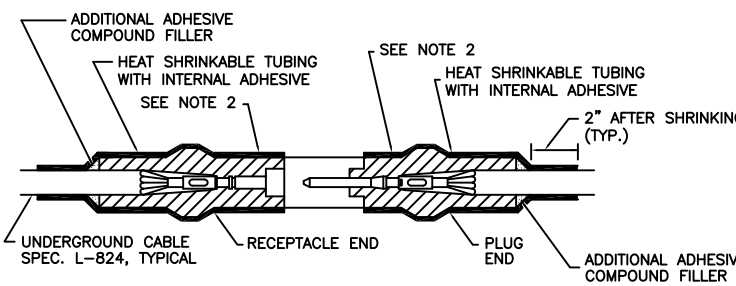
TURF CABLE MARKER DETAIL
NOT TO SCALE



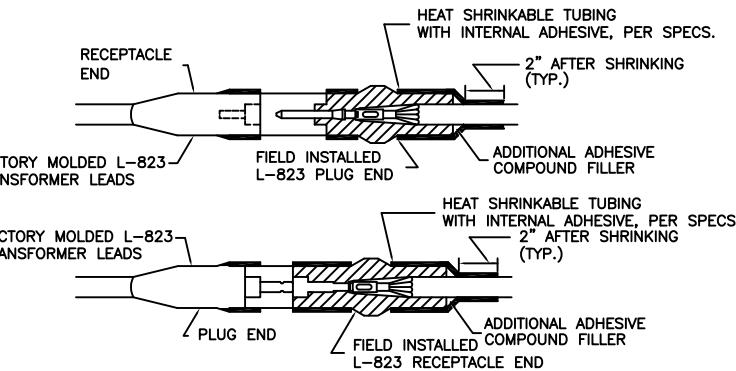
- NOTES:**
- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
 - ITEM 610 CONCRETE SHALL BE USED.
 - ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
 - THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
 - 0.049 CU. YD. CONCRETE PER MARKER.



TYPE A - CABLE SPLICE
FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
N.T.S.

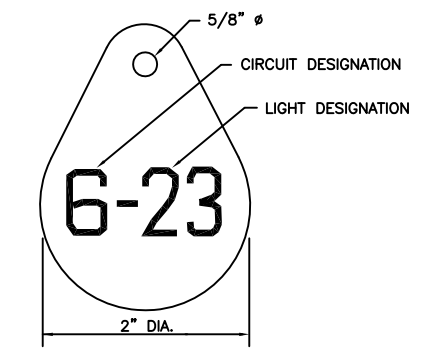


TYPE B - CABLE SPLICE
FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT
N.T.S.



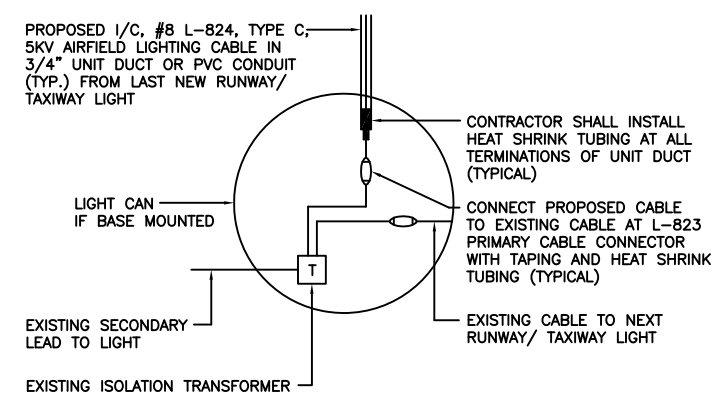
TYPE C AND D - CABLE SPLICE
FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
N.T.S.

- NOTES**
- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
 - WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
 - THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
 - THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.



LIGHT IDENTIFICATION DETAIL
NOT TO SCALE

- NOTES:**
- INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
 - NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (NEW OR RELOCATED LIGHTS) SHALL BE RETAGGED.
 - COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



RUNWAY/TAXIWAY LIGHTING CIRCUIT CONNECTION DETAIL
NOT TO SCALE

**ELECTRICAL DETAILS - 3
(ELD3)**

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BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
ELECTRICAL DETAILS - 4
(ELD4)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

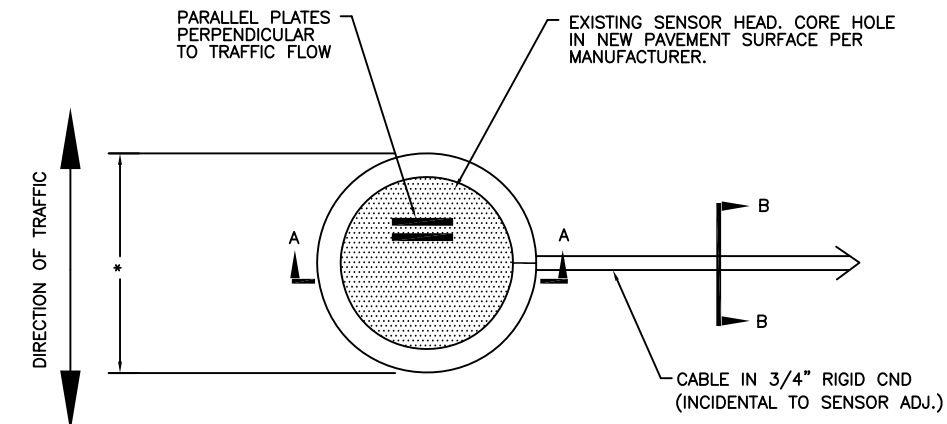
PROJECT NO.: 10258-03-00/06
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SHEET NO. 43 REVISION

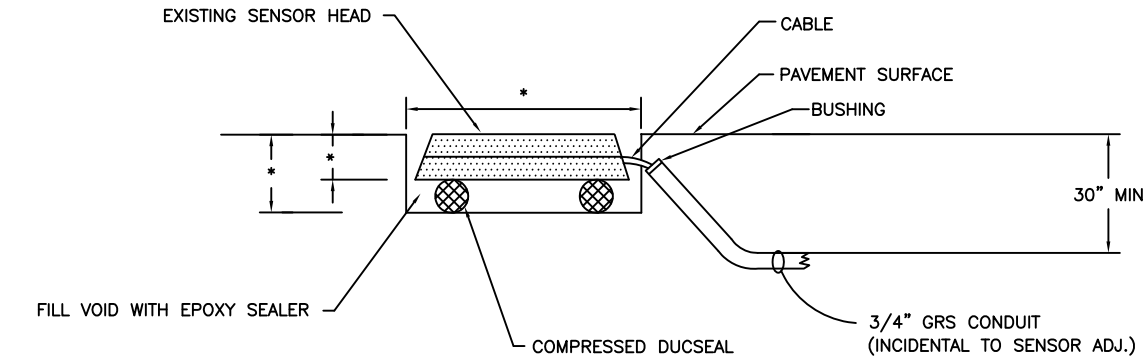
SHEET 43 OF 67 SHEETS

NOTES FOR UPGRADING SURFACE SENSOR SYSTEM

1. EXISTING SURFACE SENSOR SYSTEM IS SCAN, MANUFACTURED BY SURFACE SYSTEMS, INC. (QUIXOTE).
2. REMOVE AND RE-INSTALL EXISTING (FP2000 TYPE) SURFACE SENSOR IN PAVEMENT AT PROXIMITY OF EXISTING SENSOR LOCATION AS SHOWN IN THE PLANS. INSTALL PROPOSED SPLICE CAN AT EDGE OF THE PAVEMENT TO INTERCEPT EXISTING SENSOR CABLE. INSTALL EXISTING SENSOR CABLE IN PROPOSED CONDUIT TO SPLICE CAN. SPLICE EXISTING SENSOR CABLE IN SPLICE CAN.
3. SPLICE CAN PAID FOR SEPARATELY.



PLAN

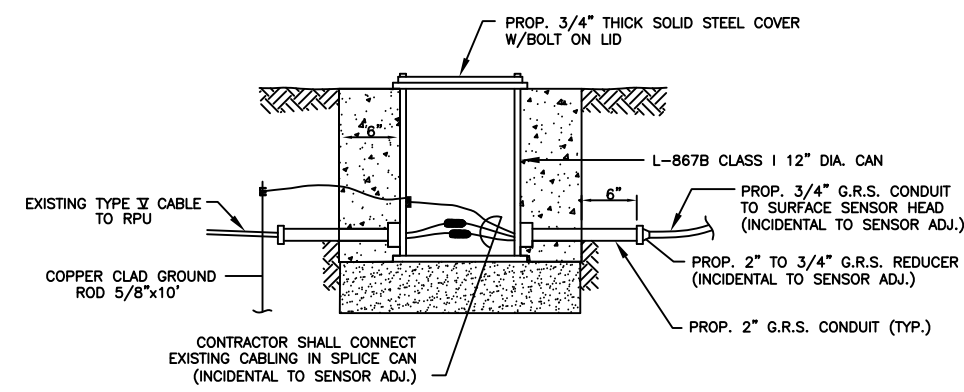


SECTION A-A

SURFACE SENSOR HEAD DETAIL

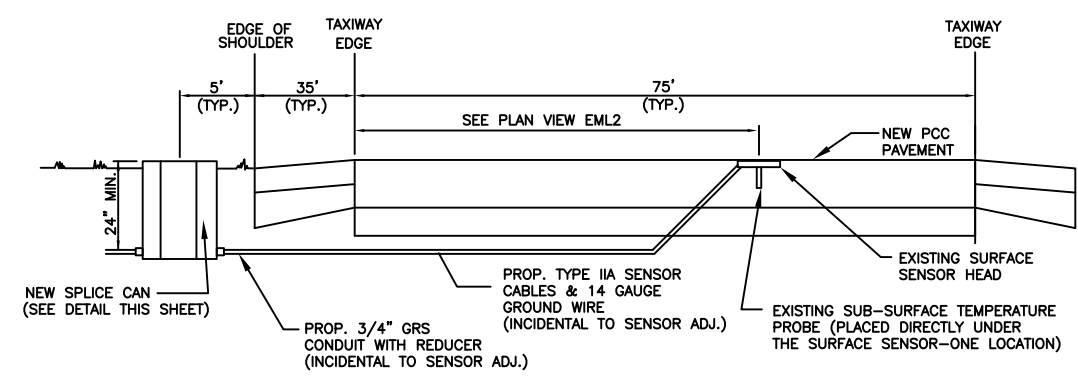
N.T.S.

* DIMENSIONS AS REQUIRED BY SENSOR MANUFACTURER.



PROPOSED PAVEMENT SENSOR SPLICE CAN DETAIL

N.T.S.



SURFACE SENSOR INSTALLATION OVERVIEW

N.T.S.

**ELECTRICAL DETAILS - 4
(ELD4)**

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Chicago Rockford
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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**ELECTRICAL DETAILS - 5
(ELD5)**

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
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PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

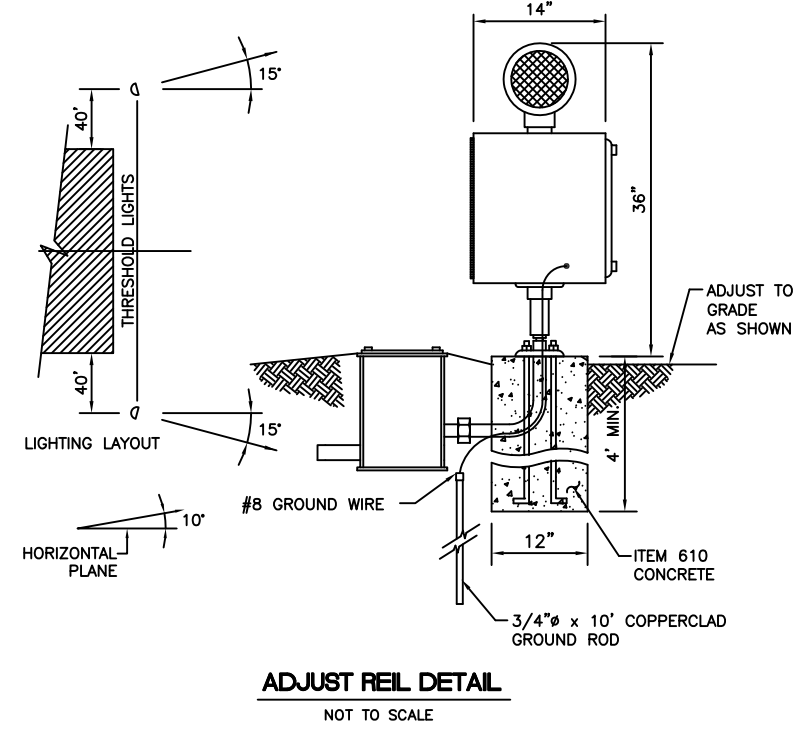
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SHEET 44 OF 67 SHEETS

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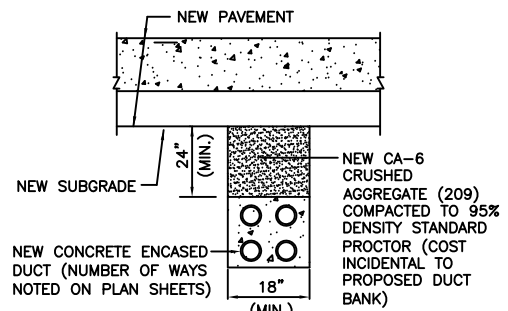
NOTES

1. CONTRACTOR TO DISCONNECT EXISTING POWER AND CONTROL WIRES, ADJUST REIL VERTICALLY AND RE-CONNECT POWER AND CONTROL WIRES.



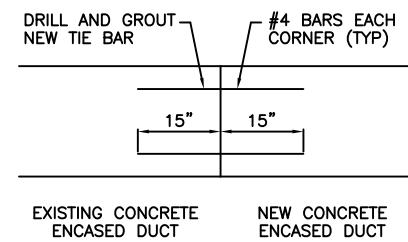
ADJUST REIL DETAIL

NOT TO SCALE



CONCRETE ENCASED DUCT BACKFILL

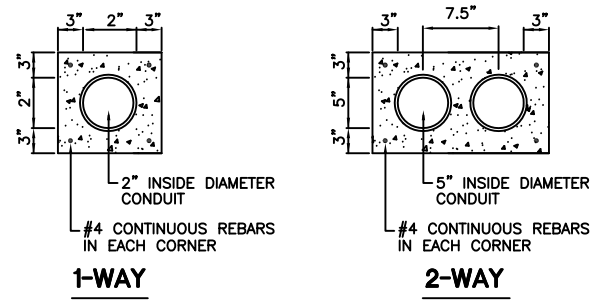
NOT TO SCALE



EXTENSION OF EXISTING DUCT

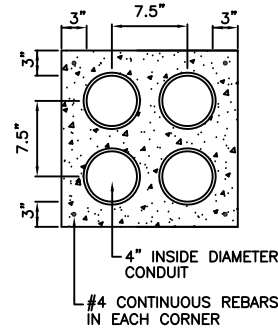
NO SCALE

NOTE: COST OF CONNECTION SHALL BE CONSIDERED INCIDENTAL TO NEW DUCT.



1-WAY

2-WAY



4-WAY

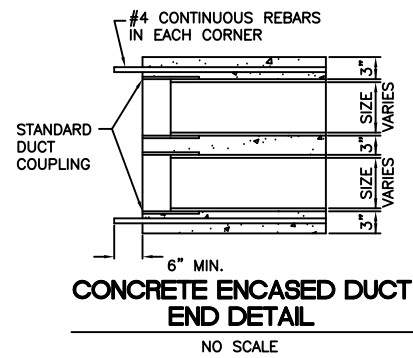
CONCRETE ENCASED DUCT BANKS

NOT TO SCALE

CONCRETE ENCASED

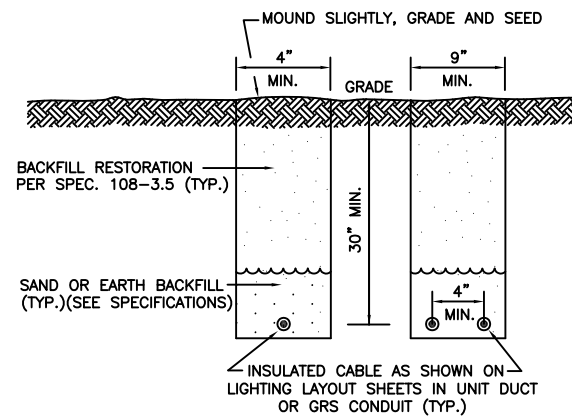
DUCT NOTES:

1. DIMENSIONS ARE MINIMUM.
2. CONCRETE SHALL CONFORM TO ITEM 610.
3. ALL CONDUIT SHALL BE SCHEDULE 40 PVC.
4. TOP OF CONCRETE ENCASEMENT IN TURF AREAS SHALL NOT BE LESS THAN 24" BELOW FINISHED GRADE.



CONCRETE ENCASED DUCT END DETAIL

NO SCALE



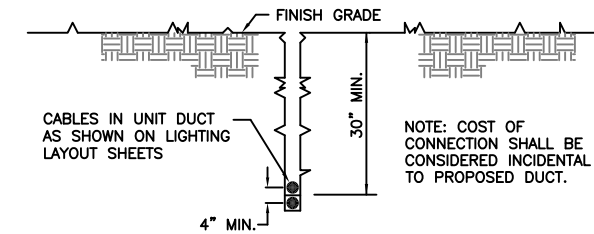
TRENCH DETAIL

NOT TO SCALE

NOTES

1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.

NOTE: AT CONTRACTOR'S OPTION, CABLE PLOWING MAY BE USED IN LIEU OF TRENCHING.



CABLE IN UNIT DUCT - PLOWED

NOT TO SCALE

CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.

PROPOSED SIGN TYPE LEGEND

- 0 — BLANK PANEL — BLACK
- 1 — RUNWAY/TAXIWAY HOLDLINE — WHITE LEGEND ON RED BACKGROUND
- 2 — LOCATION SIGN — YELLOW LEGEND ON BLACK BACKGROUND
- 3 — DIRECTION SIGN — BLACK LEGEND ON YELLOW BACKGROUND
- 4 — RUNWAY APPROACH AREA BOUNDARY SIGN — BLACK LEGEND ON YELLOW BACKGROUND

NOTE:

PROPOSED GUIDANCE SIGNS PANEL SIZE WILL BE BASED ON THE MANUFACTURERS RECOMMENDATION.

AIRFIELD SIGNAGE SCHEDULE				
PROPOSED SIGN	PROPOSED SIGN FACE	PROPOSED SIGN LEGEND	PROPOSED SIGN TYPE	PROPOSED SIGN LOCATION
N	W	BLANK 1-19	0	STA. 300+79.50, 110.0' LT. CENTERLINE RUNWAY 7/25

**ELECTRICAL DETAILS - 5
(ELD5)**



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

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IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**MARKING DETAILS - 1
(MRD1)**

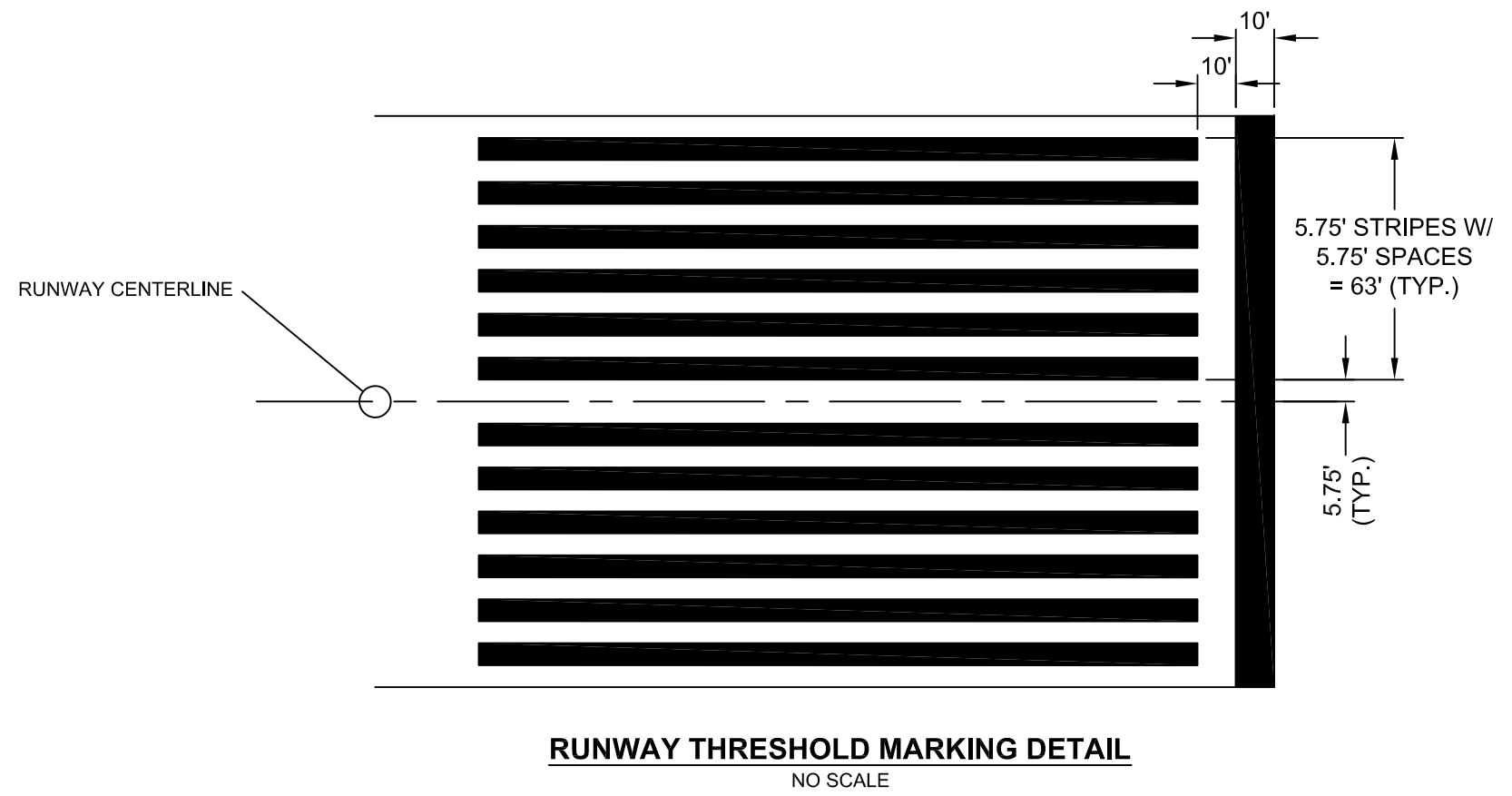
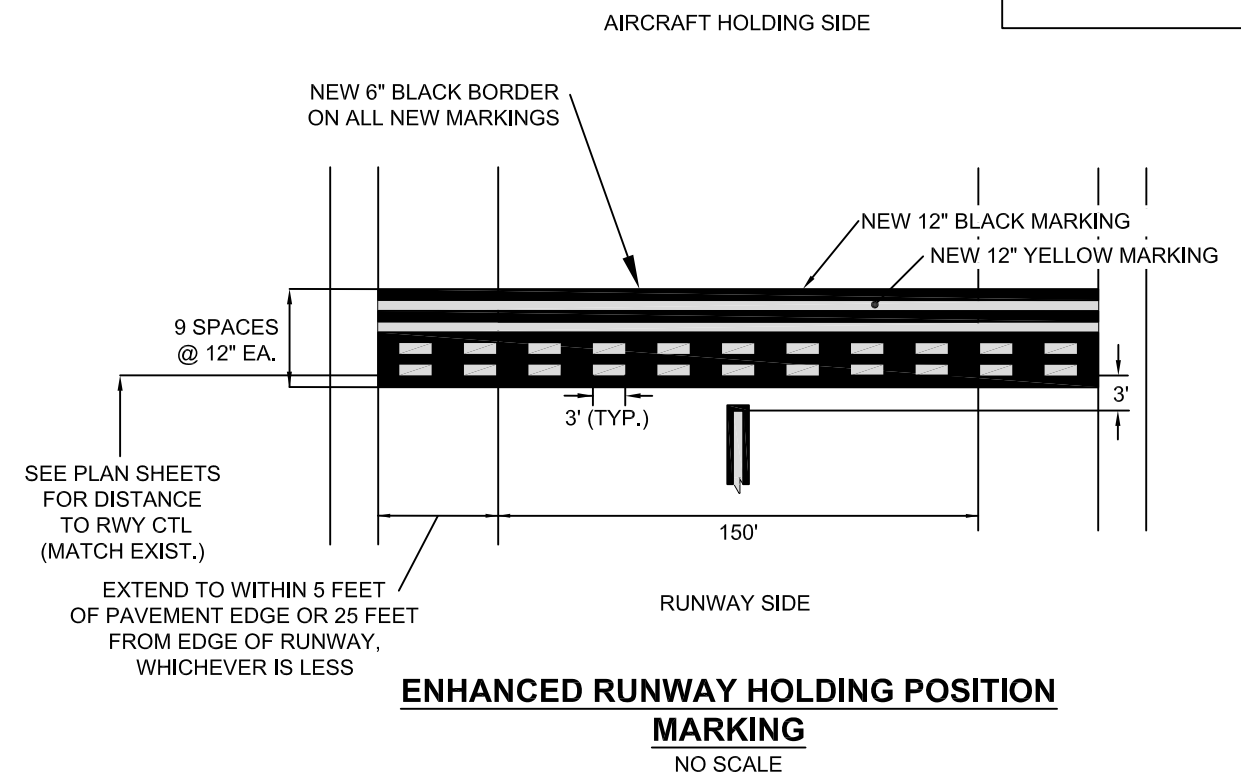
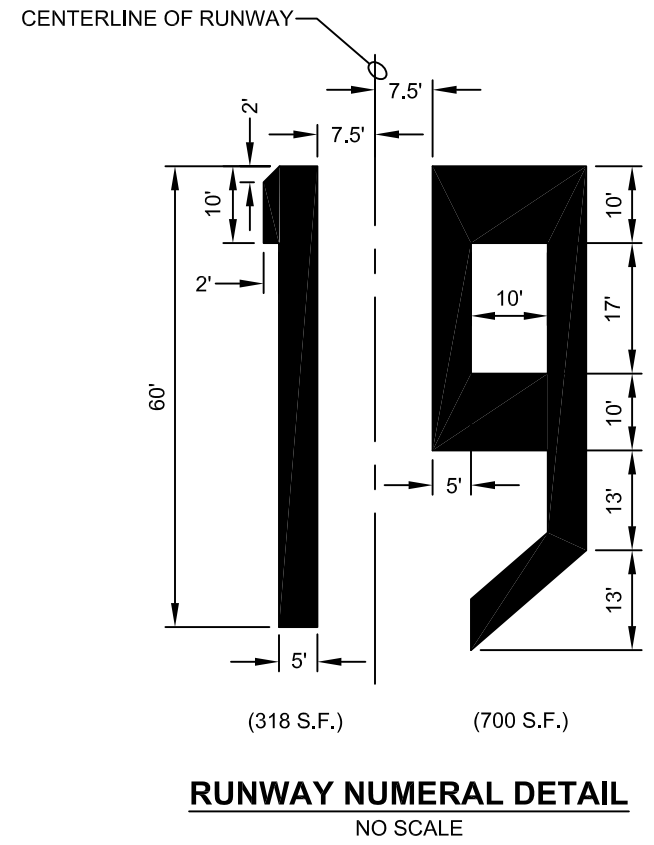
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **45** REVISION

SHEET 45 OF 67 SHEETS

RO019





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Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
**MARKING DETAILS - 2
(MRD2)**

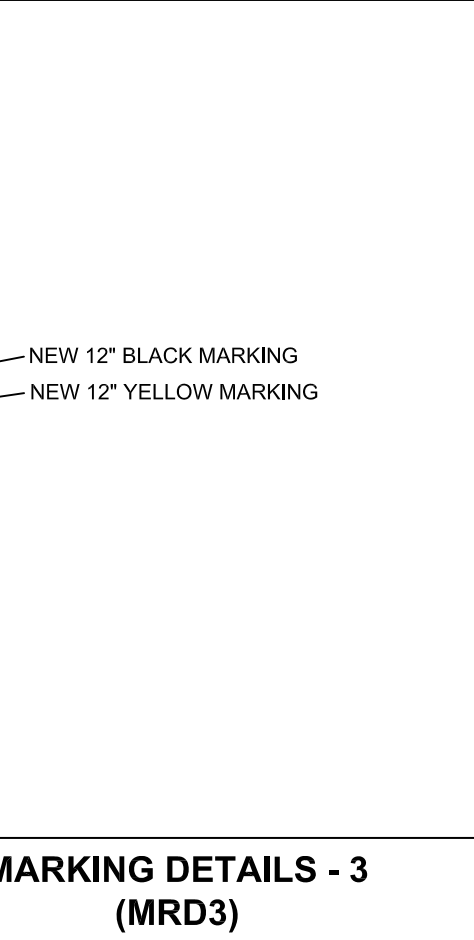
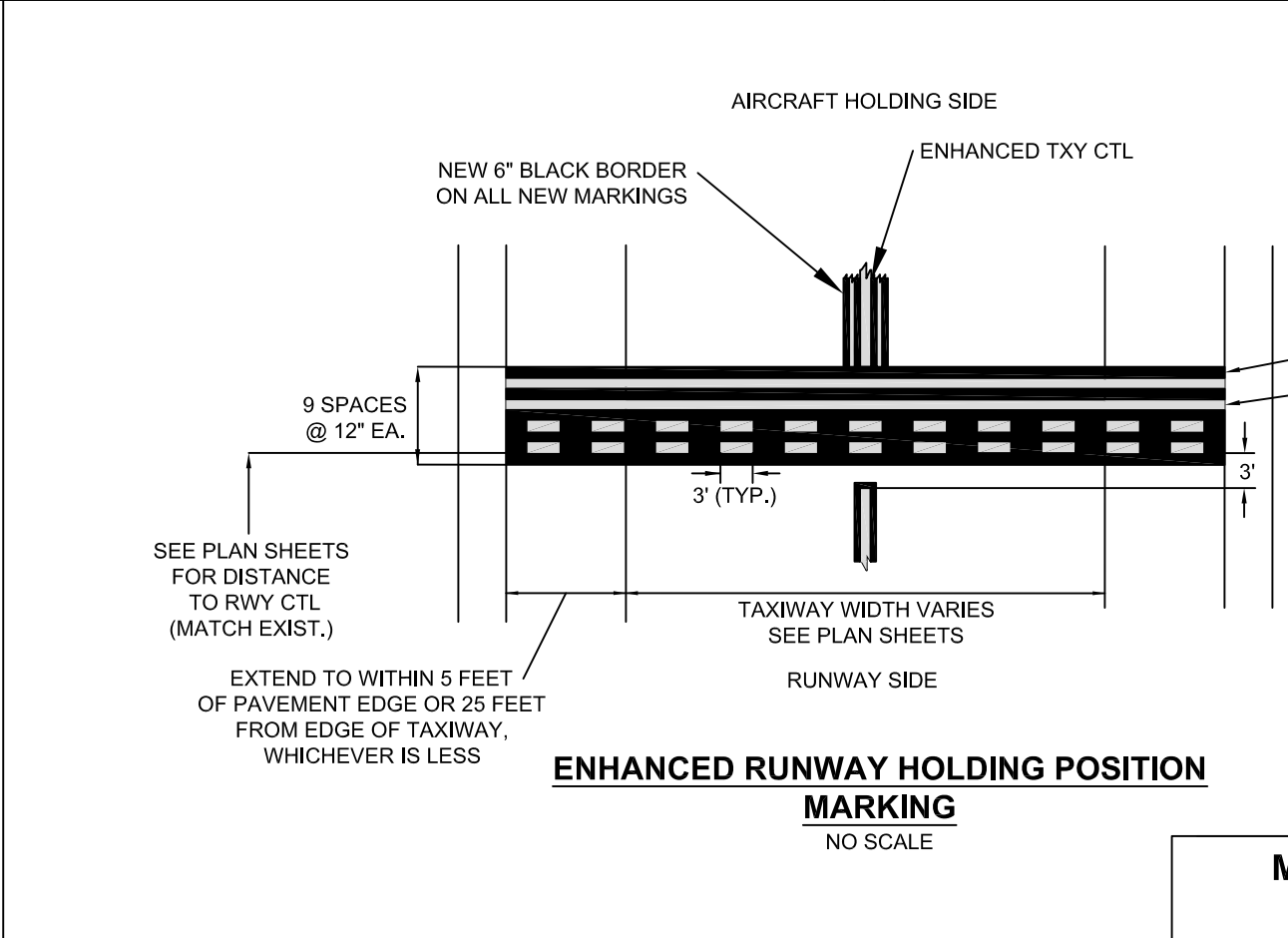
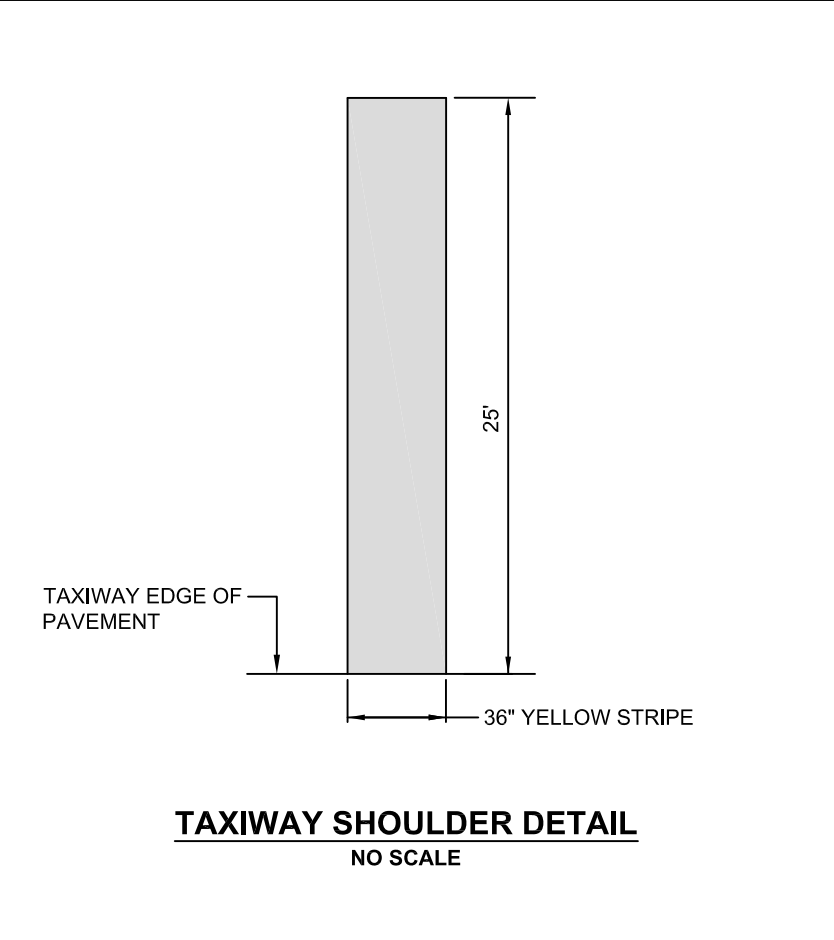
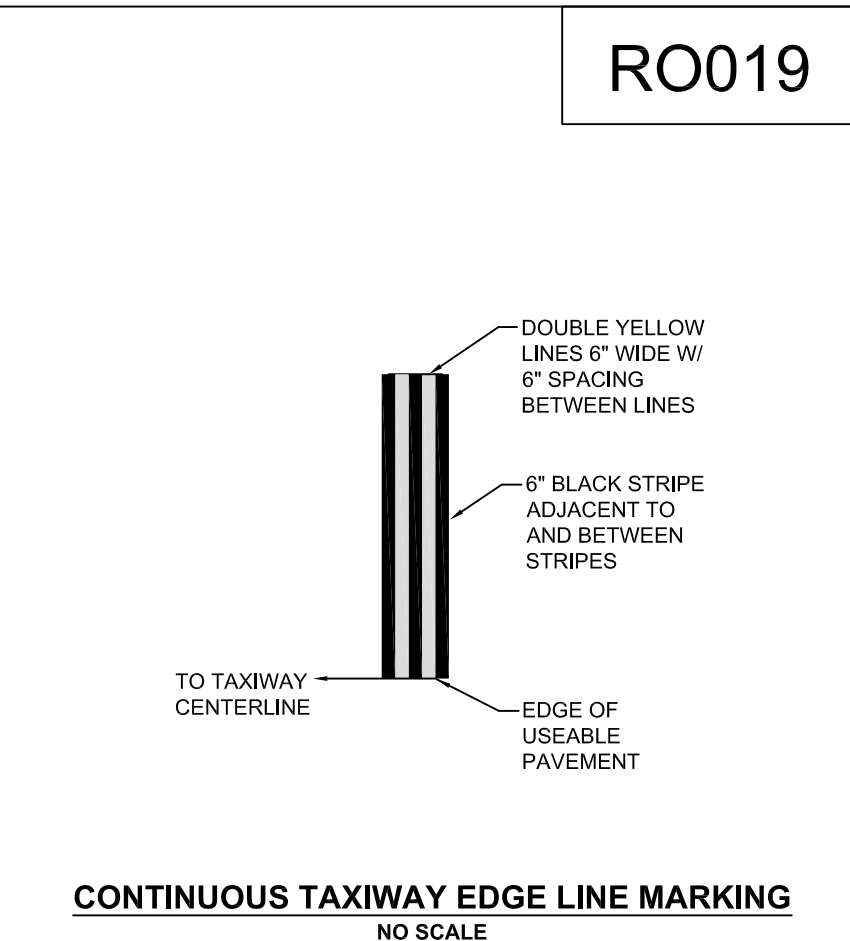
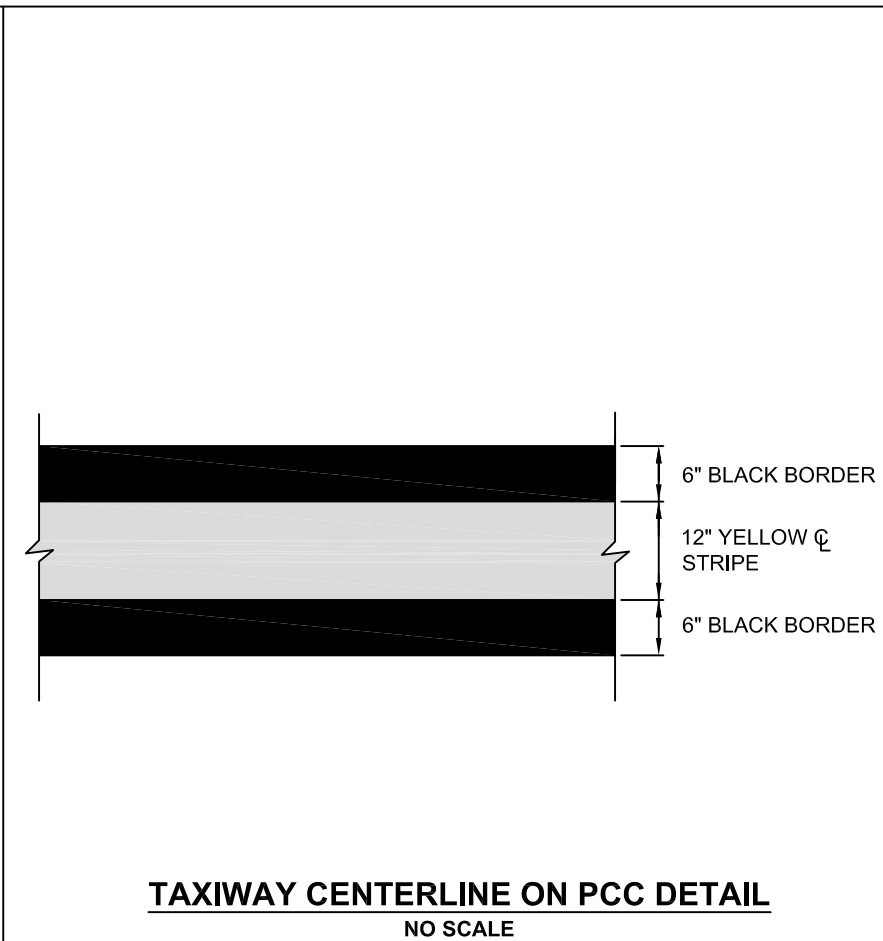
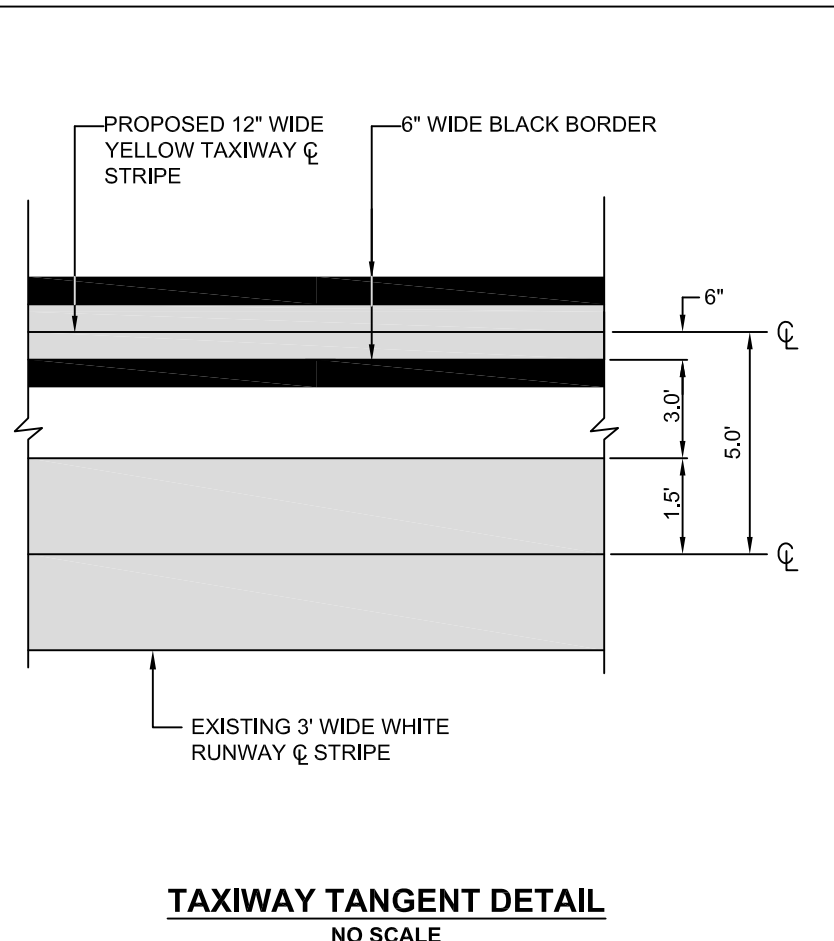
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **46** REVISION

SHEET 46 OF 67 SHEETS

RO019



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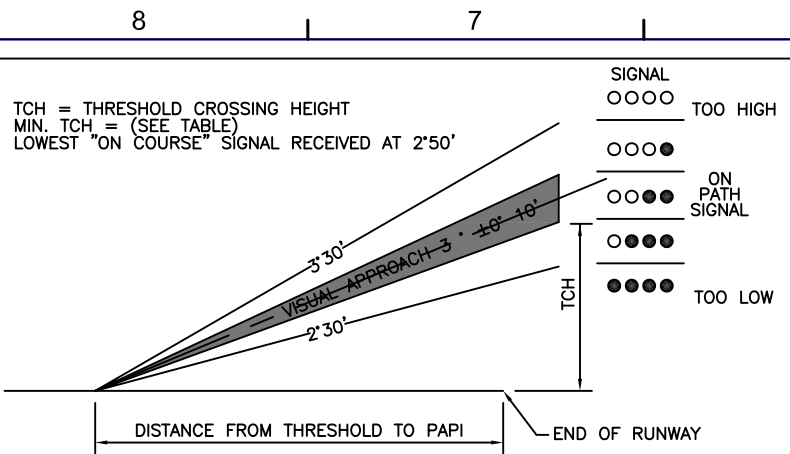
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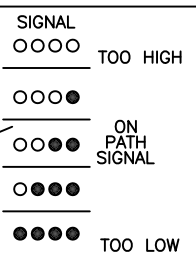
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RUNWAY TYPICAL PROFILE
N.T.S.

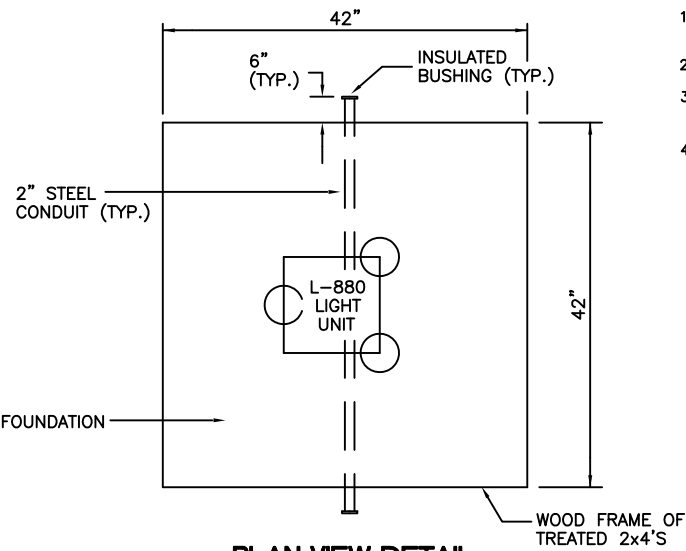


SIGHTING TABLE

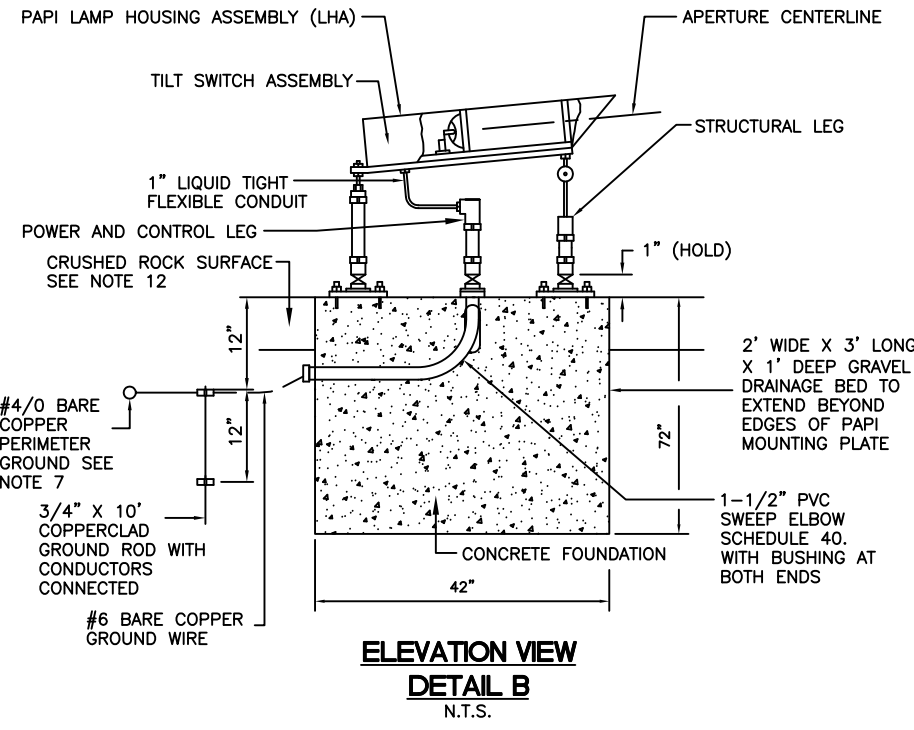
DESCRIPTION	RUNWAY END 19	RUNWAY END 25
DIMENSION A	1,462.08	1,495.40
DIMENSION B	60'	50'
DIMENSION C	30'	30'
DIMENSION D	(SEE NOTE 4) 27'	
PCA LOCATION = STA. / OFFSET		290+45.70, 405.0' RT
HEIGHT GROUP USED FOR SITING	4	4
THRESHOLD STATION	93+96.06	303+99.00
THRESHOLD ELEVATION	736.53	736.37
THRESHOLD CROSSING HEIGHT	75	75
STATION FOR MIDPOINT OF PROJECTORS	79+33.98	289+03.60
GLIDE PATH ANGLE	3'	3'
ELEVATION C OF APERTURE	734.92	732.99
ELEVATION C OF APERTURE	734.92	732.99
ELEVATION C OF APERTURE	734.92	732.99
ELEVATION C OF APERTURE	734.92	732.99
AIMING ANGLE UNIT 1	3' 30"	3' 30"
AIMING ANGLE UNIT 2	3' 10"	3' 10"
AIMING ANGLE UNIT 3	2' 50"	2' 50"
AIMING ANGLE UNIT 4	2' 30"	2' 30"

SIGHTING NOTES:

1. BASE DESIGN AND LAYOUT ON ADVISORY CIRCULAR 150/5345-28F. CONTRACTOR SHALL MEET THE REQUIREMENTS IN ADVISORY CIRCULAR 150/5345-28F.
2. SEE PAPI LIGHT UNIT LOCATION FOR UNIT LOCATIONS AND DIMENSIONS
3. THE VISUAL GLIDE PATH ANGLE IS THE CENTER OF THE ON COURSE ZONE AND IS MEASURED FROM THE HORIZONTAL.
4. THE RUNWAY 19 PAPI SHALL BE AS LISTED ON THE FAA NAVAIDS "FIXED BY FUNCTION" APPROVED LIST.



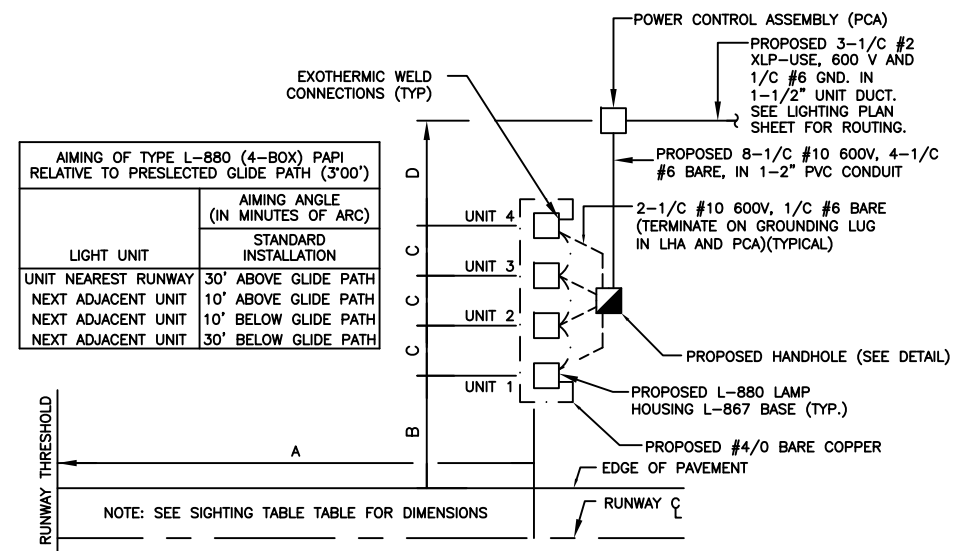
PLAN VIEW DETAIL
N.T.S.



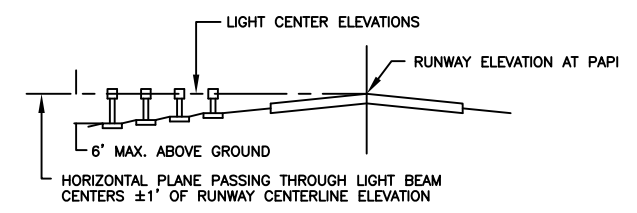
ELEVATION VIEW DETAIL B
N.T.S.

GENERAL NOTES

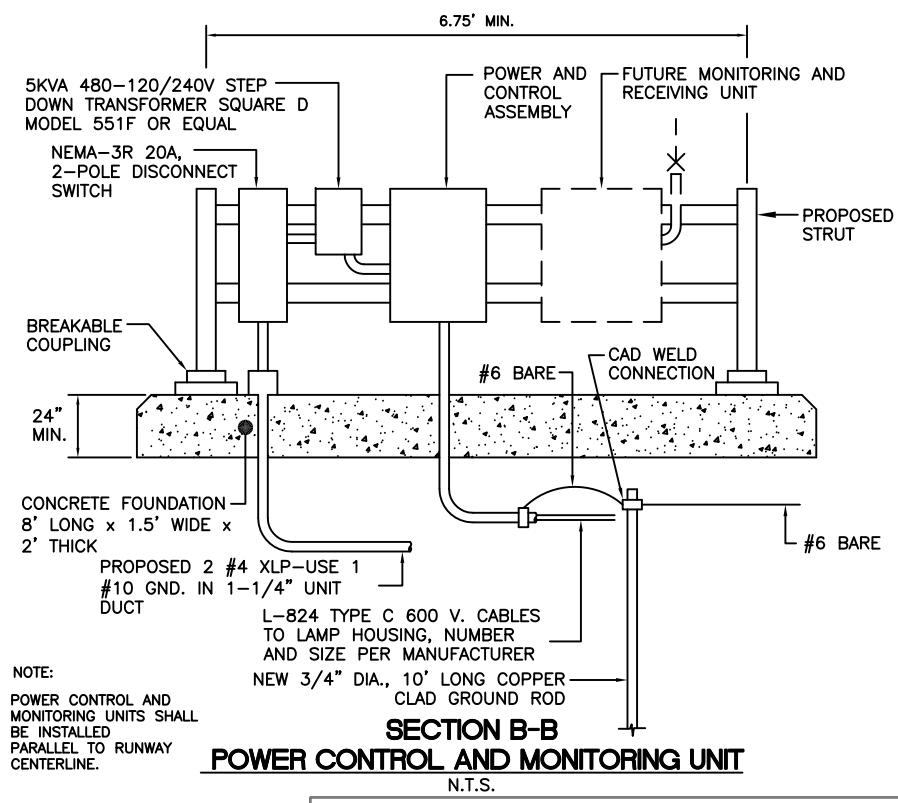
- FOUNDATIONS:**
1. FOUNDATIONS FOR MOUNTING LIGHT BOXES SHALL BE MADE OF ITEM 610 CONCRETE. ALL LIGHT BOXES SHALL BE FRANGIBLY MOUNTED TO THE FOUNDATION.
- AZIMUTHAL AIMING:**
2. EACH LIGHT UNIT SHALL BE AIMED OUTWARD INTO THE APPROACH ZONE ON A LINE PARALLEL TO THE RUNWAY CENTERLINE WITHIN A TOLERANCE OF $\pm 1/2$ DEGREE.
- MOUNTING HEIGHT TOLERANCES:**
3. THE BEAM CENTERS OF ALL LIGHT UNITS SHALL BE WITHIN ± 1 INCH OF A HORIZONTAL PLANE AT THE ELEVATION GIVEN IN THE TABLE.
- TOLERANCE ALONG LINE PERPENDICULAR TO RUNWAY:**
4. THE FRONT FACE OF EACH LIGHT UNIT IN A BAR SHALL BE LOCATED ON A LINE PERPENDICULAR TO THE RUNWAY CENTERLINE WITHIN ± 6 INCHES.
5. PROVIDE FRANGIBLE MOUNTS FOR ALL LEGS OF LIGHT UNITS AND POWER ADAPTERS.
 6. NUMBER AND CONFIGURATION OF LEGS PER MANUFACTURER. (3 LEGS MINIMUM)
 7. GROUND EACH LAMP HOUSING AND POWER ADAPTER PER MANUFACTURER
 8. THE POWER AND CONTROL UNIT SHALL BE STYLE A, CLASS II.
 9. THE DIFFERENCE IN LATERAL SPACING BETWEEN THE LIGHT UNITS SHALL NOT EXCEED ONE FOOT.
 10. EACH LIGHT UNIT SHALL HAVE TWO LAMPS AND SHALL PROVIDE A BEAM OF LIGHT SPLIT HORIZONTALLY TO PRODUCE WHITE LIGHT IN THE TOP SECTOR AND RED LIGHT IN THE BOTTOM SECTOR. THEY SHALL HAVE A MINIMUM OF THREE MOUNTING LEGS WHICH SHALL BE ADJUSTABLE TO PERMIT LEVELING WHERE ONE SIDE OF THE UNIT IS INSTALLED UP TO 1 INCH LOWER THAN THE OPPOSITE SIDE.
 11. APPLY "NEVER SEEZ" OR APPROVED EQUAL TO ALL THREADED BOLTS AND CONNECTIONS.
 12. THE COST OF THE PAPI SHALL INCLUDE ANY SITE WORK AROUND THE PAPI, INCLUDING BUT NOT LIMITED TO THE CRUSHED STONE, EARTHWORK AND LANDSCAPING.
 13. PAPI SYSTEM PAY ITEM SHALL INCLUDE BUT NOT LIMITED TO PAPI LIGHT UNITS AND POWER CONTROL. ALL CABLES BETWEEN PCA AND LIGHT UNITS, GROUNDING SYSTEM, CONCRETE FOUNDATIONS AND ANY NECESSARY EQUIPMENT FOR A COMPLETE AND OPERATIONAL SYSTEM.
 14. THE RUNWAY 19 PAPI SHALL BE AS LISTED ON THE FAA NAVAIDS "FIXED BY FUNCTION" APPROVED LIST
 15. THE RUNWAY 7 PAPI PCU RELOCATION SHALL INCLUDE REMOVAL OF EXISTING PCA, DISCONNECT AND TRANSFORMER, AND INSTALLATION ON NEW CONCRETE FOUNDATION AS SHOWN ON THIS SHEET. INSTALL NEW GROUNDING SYSTEM AND NEW POWER AND CONTROL CABLES. INSTALLATION OF NEW POWER CABLES FROM EXISTING LOCATION, REMOVAL OF PCU FOUNDATION AND SITE RESTORATION SHALL BE INCIDENTAL TO PAY ITEM AR800881.



PAPI LIGHT UNIT LOCATION
N.T.S.



ELEVATION - TYPICAL
N.T.S.



SECTION B-B POWER CONTROL AND MONITORING UNIT
N.T.S.

PAPI DETAILS (PPD1)



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford International Airport
Rockford, Illinois

Greater Rockford Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE RUNWAY 1/19
PHASE 1

SHEET TITLE:
PAPI DETAILS (PPD1)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **47** REVISION

SHEET 47 OF 67 SHEETS



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Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

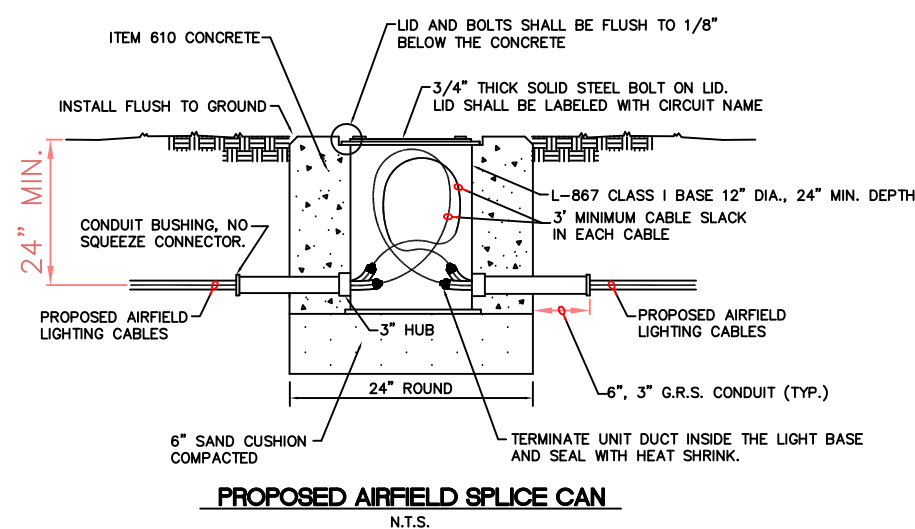
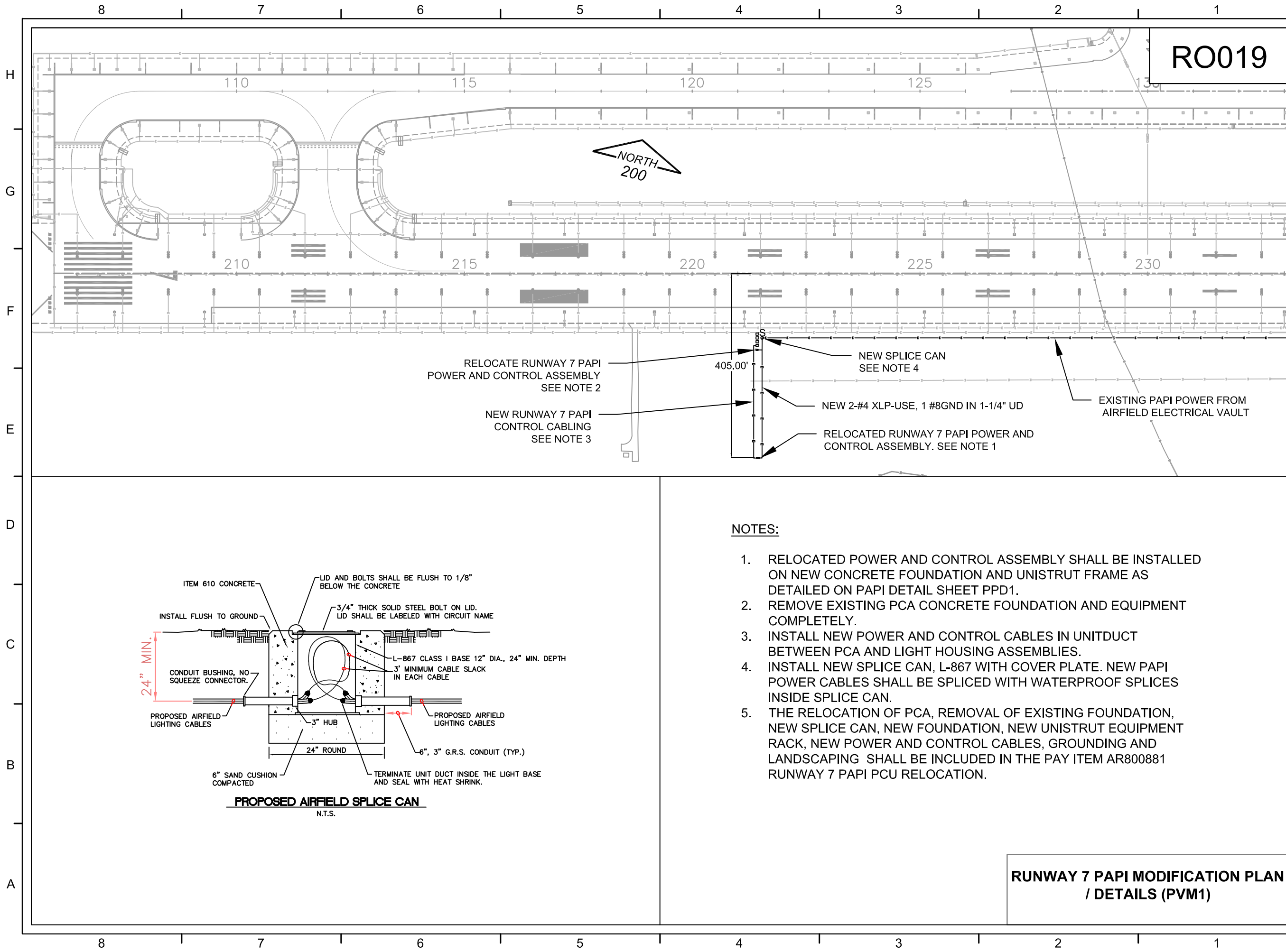
SHEET TITLE:
RUNWAY 7 PAPI
MODIFICATION PLAN /
DETAILS (PVM1)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **48** REVISION

SHEET 48 OF 67 SHEETS



NOTES:

1. RELOCATED POWER AND CONTROL ASSEMBLY SHALL BE INSTALLED ON NEW CONCRETE FOUNDATION AND UNISTRUT FRAME AS DETAILED ON PAPI DETAIL SHEET PPD1.
2. REMOVE EXISTING PCA CONCRETE FOUNDATION AND EQUIPMENT COMPLETELY.
3. INSTALL NEW POWER AND CONTROL CABLES IN UNIDUCT BETWEEN PCA AND LIGHT HOUSING ASSEMBLIES.
4. INSTALL NEW SPLICE CAN, L-867 WITH COVER PLATE. NEW PAPI POWER CABLES SHALL BE SPICED WITH WATERPROOF SPICES INSIDE SPLICE CAN.
5. THE RELOCATION OF PCA, REMOVAL OF EXISTING FOUNDATION, NEW SPLICE CAN, NEW FOUNDATION, NEW UNISTRUT EQUIPMENT RACK, NEW POWER AND CONTROL CABLES, GROUNDING AND LANDSCAPING SHALL BE INCLUDED IN THE PAY ITEM AR800881 RUNWAY 7 PAPI PCU RELOCATION.

RUNWAY 7 PAPI MODIFICATION PLAN / DETAILS (PVM1)



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

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APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

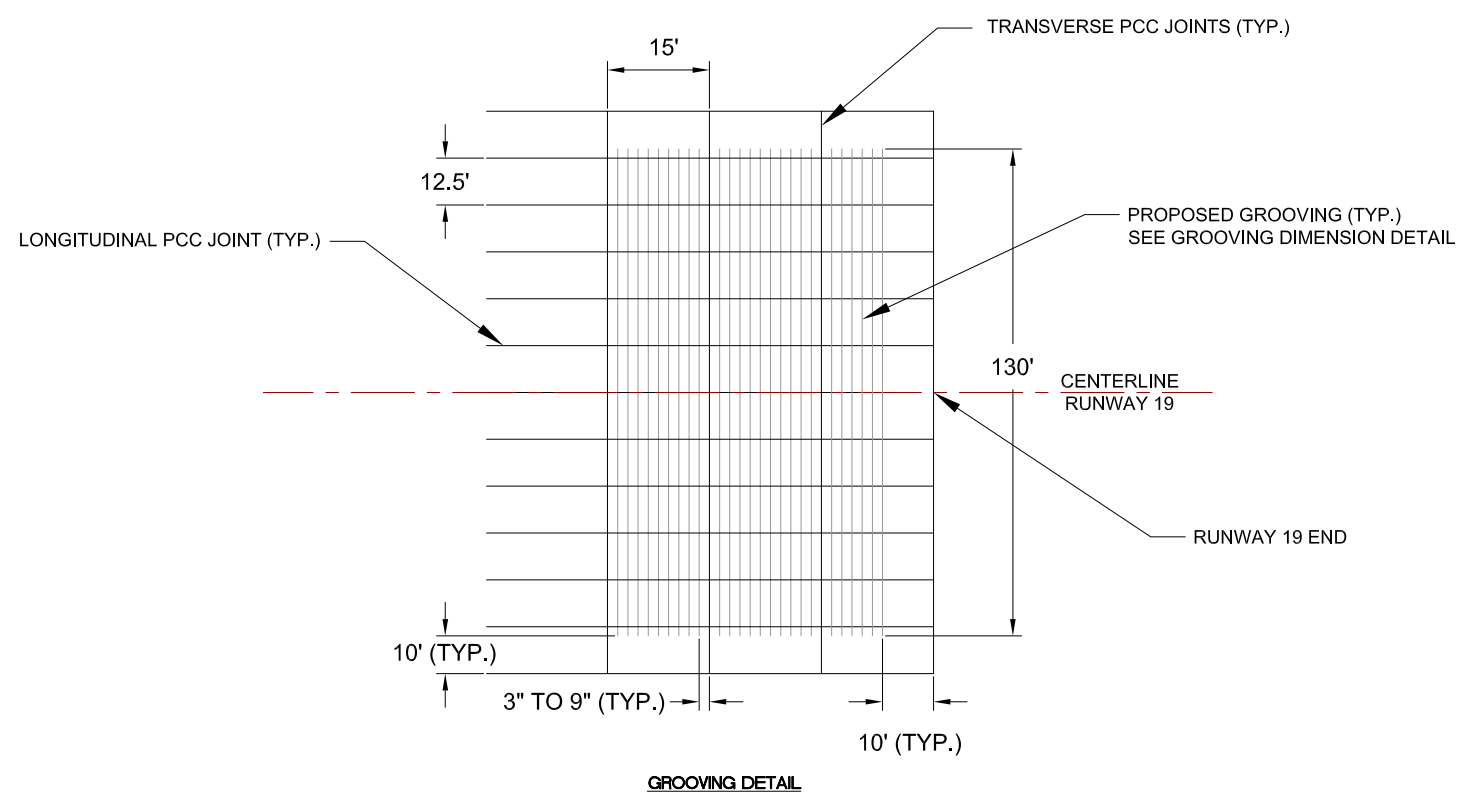
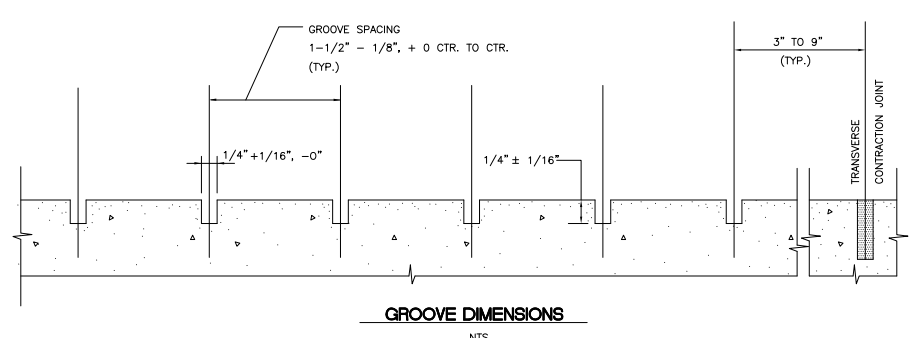
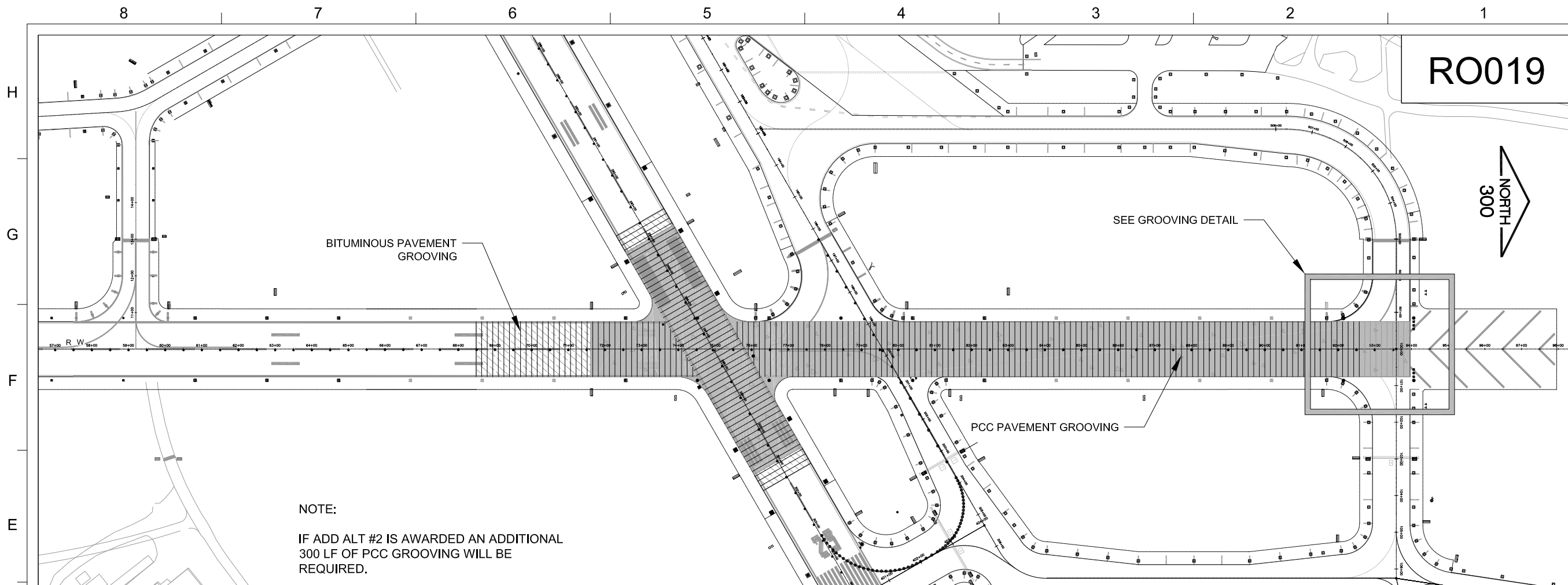
SHEET TITLE:

**GROOVING PLAN
AND DETAILS (GPD1)**

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
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PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO.	49	REVISION
SHEET 49 OF 67 SHEETS		



**GROOVING PLAN AND
DETAILS (GPD1)**

- GENERAL NOTES
1. TRANSVERSE CONTRACTION JOINT SPACING IS AS SHOWN ON THE PLANS.
 2. GROOVING SHALL BE CONTINUOUS ACROSS LONGITUDINAL JOINTS
 3. SUCCESSIVE PASSES OF GROOVING MACHINE SHALL NOT OVERLAP.
 4. GROOVES SHALL NOT CONTINUE THROUGH LONGITUDINAL OR DIAGONAL SAW KERFS (IF PRESENT) WHERE LIGHTING CABLES ARE INSTALLED.
 5. EXTREME CARE SHOULD BE TAKEN WHEN GROOVING NEAR IN-PAVEMENT LIGHT FIXTURES. GROOVES SHALL BE SAWED NO CLOSER THAN 6" AND NO MORE THAN 18" FROM SUCH FACILITIES.
 6. CLEANUP OF WASTE MATERIAL SHALL BE CONTINUOUS AND TIMELY DURING THE GROOVING OPERATION. WASTE MATERIAL SHALL BE DISPOSED OF BY THE USE OF A VACUUM PICK UP SWEEPER AND SHALL BE DISPOSED OF OFF OF AIRPORT PROPERTY.
 7. EXTREME CARE SHOULD BE TAKEN WHEN GROOVING NEAR PAVEMENT SENSOR FIXTURES. GROOVES SHALL BE SAWED NO CLOSER THAN 6" AND NO MORE THAN 18" FROM SUCH FACILITIES.

BITUMINOUS PAVEMENT CORES

BORING / CORE NUMBER	HMA THICKNESS SURFACE & BINDER (INCHES)	AGG. BASE THICKNESS (INCHES)
B-8	15	11
B-9	15	13
B-10	23.1	9
B-11	20.2	9
B-12	18.5	8

NOTES:
ADDITIONAL SOIL BORING INFORMATION IS AVAILABLE UPON REQUEST.

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ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**ENGINEERING
INFORMATION
(ENG1)**

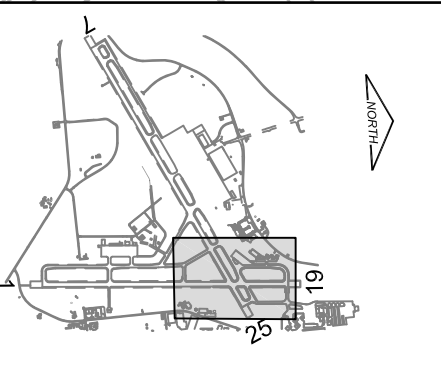
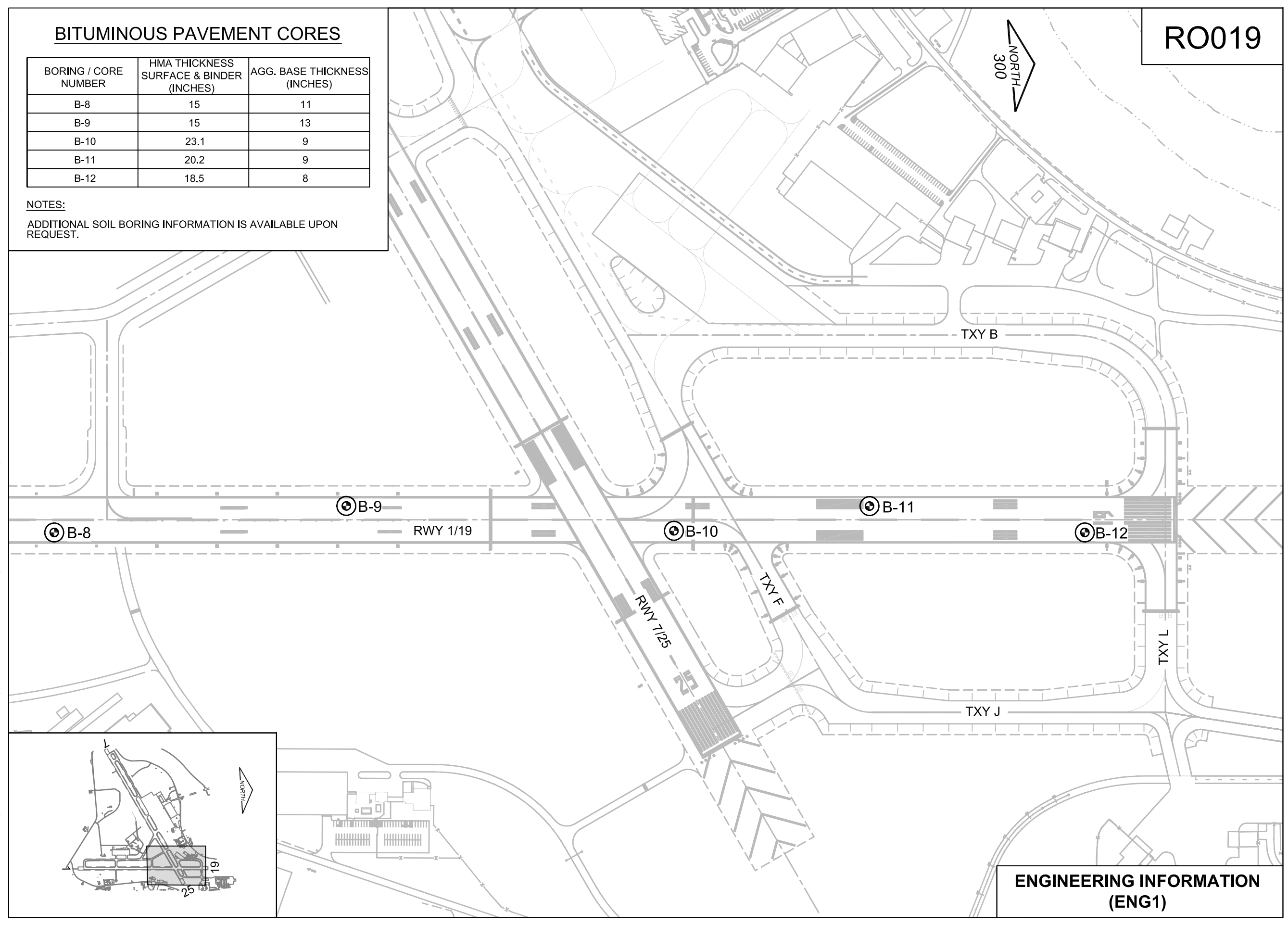
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

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**ENGINEERING INFORMATION
(ENG1)**





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Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN	
BY:	
REV	DESCRIPTION

**REHABILITATE
RUNWAY 1/19
PHASE 1**

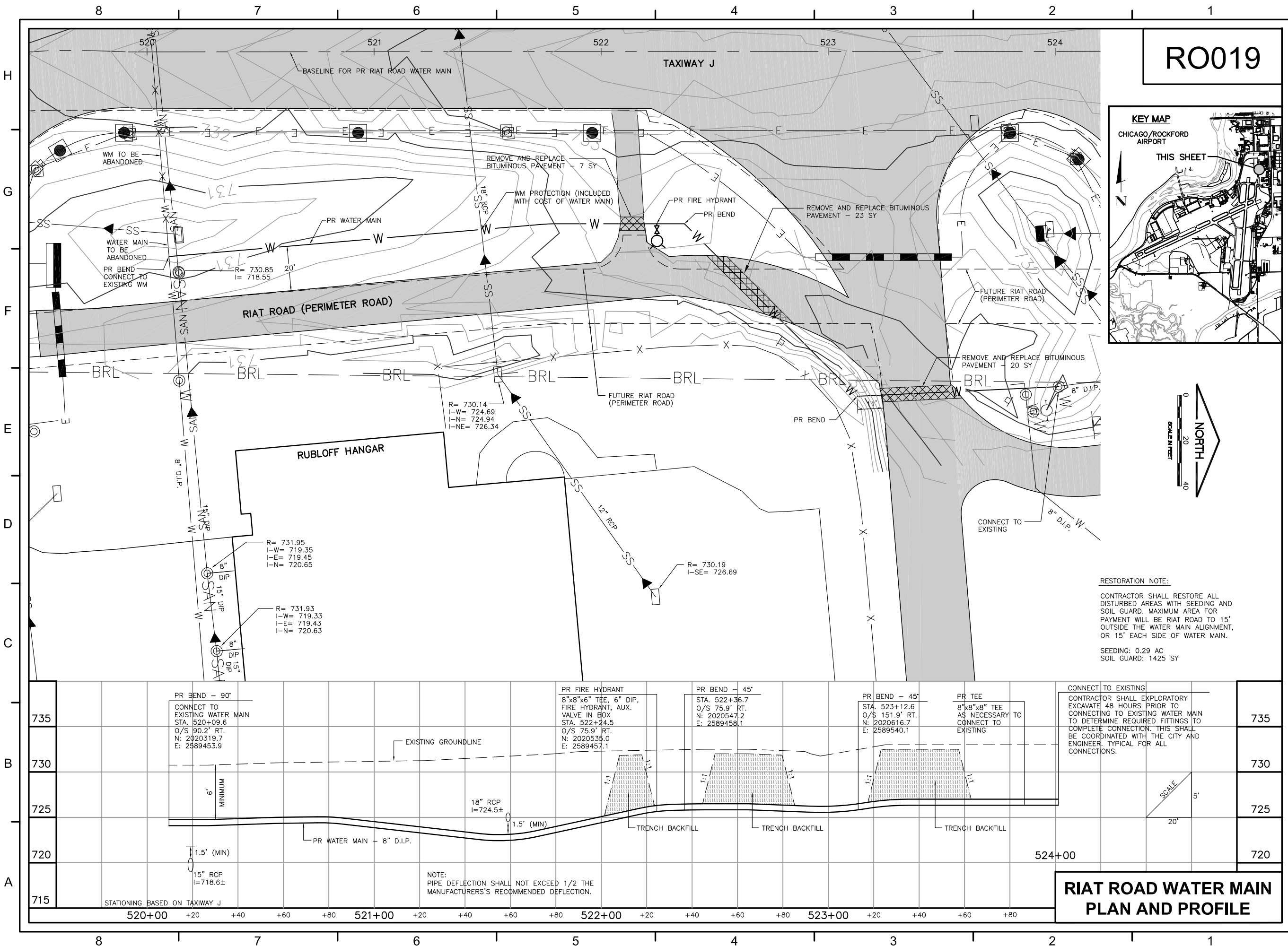
SHEET TITLE:
**RIAT ROAD
WATER MAIN
PLAN AND PROFILE**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

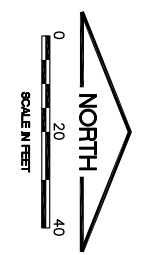
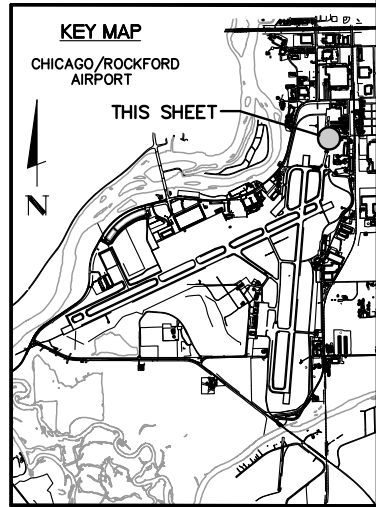
PROJECT NO.: 10258-03-00/06
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SHEET NO. **51** REVISION

SHEET 51 OF 67 SHEETS



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**RIAT ROAD WATER MAIN
PLAN AND PROFILE**

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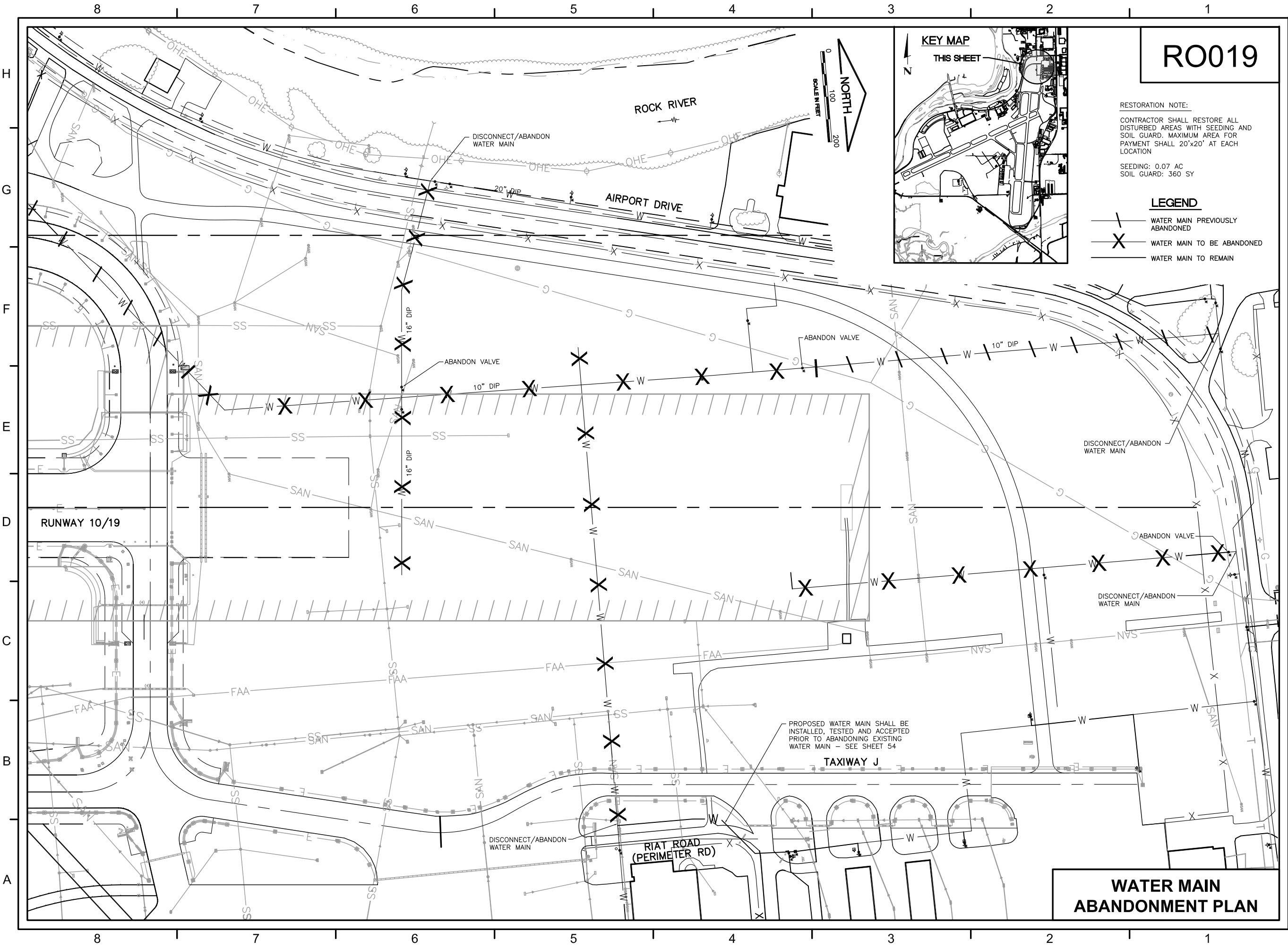
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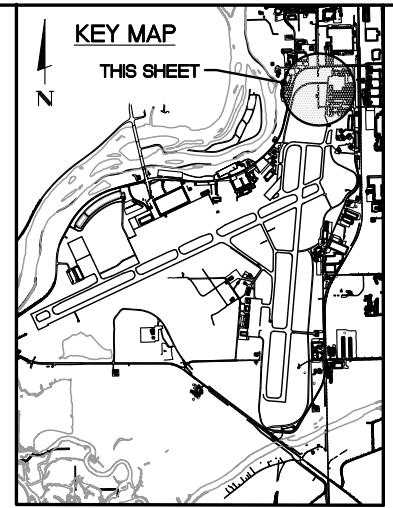
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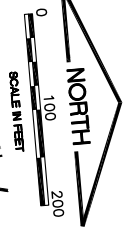


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RESTORATION NOTE:
 CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS WITH SEEDING AND SOIL GUARD. MAXIMUM AREA FOR PAYMENT SHALL 20'x20' AT EACH LOCATION
 SEEDING: 0.07 AC
 SOIL GUARD: 360 SY

LEGEND
 --- WATER MAIN PREVIOUSLY ABANDONED
 X WATER MAIN TO BE ABANDONED
 --- WATER MAIN TO REMAIN



PROPOSED WATER MAIN SHALL BE INSTALLED, TESTED AND ACCEPTED PRIOR TO ABANDONING EXISTING WATER MAIN - SEE SHEET 54

WATER MAIN ABANDONMENT PLAN



FRANZ L. OLSON
 ACTING EXECUTIVE DIRECTOR



Chicago Rockford International Airport
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Greater Rockford Airport Authority

AIP Proj: 3-17-0088-XX
 IL Proj: RFD-4083

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REV DATE DESCRIPTION
 PROJECT NAME:

REHABILITATE RUNWAY 1/19
 PHASE 1

SHEET TITLE:
WATER MAIN ABANDONMENT PLAN

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
 DATE: APRIL 22, 2011

SHEET NO. **52** REVISION

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AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**RIAT ROAD
WATER MAIN
DETAILS - 1**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

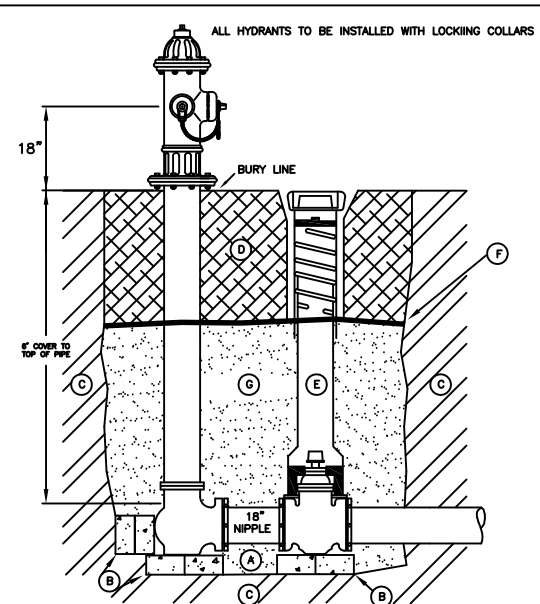
PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **53** REVISION

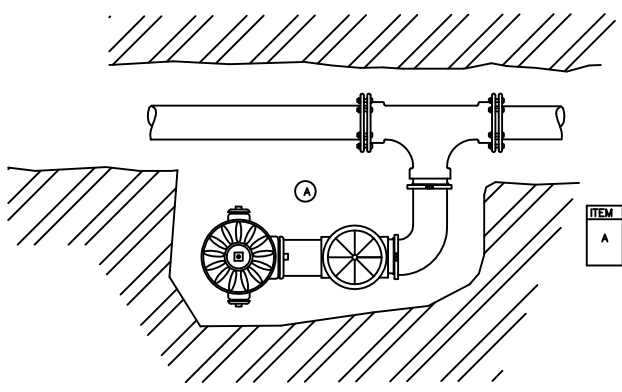
SHEET 53 OF 67 SHEETS

**RIAT ROAD WATER MAIN
DETAILS - 1**

ITEM	DESCRIPTION
A	APPROVED BEDDING MATERIAL
B	16"x 8"x 4" CONCRETE BLOCKING
C	UNDISTURBED SOIL
D	APPROVED GRANULAR, TRENCH BACKFILL MATERIAL
E	VALVE BOXES WILL BE THE TYLER / UNION CAST IRON 6850 SERIES- WITH DEBRIS CAP & ADAPTER II INSTALLED
F	A WATER PERMEABLE GEOTEXTILE IS TO BE PLACED BETWEEN 1" WASHED STONE AND BACKFILL
G	VALVE AND HYDRANT TO BE BACK FILLED WITH- 1" WASHED STONE MINIMUM OF 24 INCHES IN DEPTH

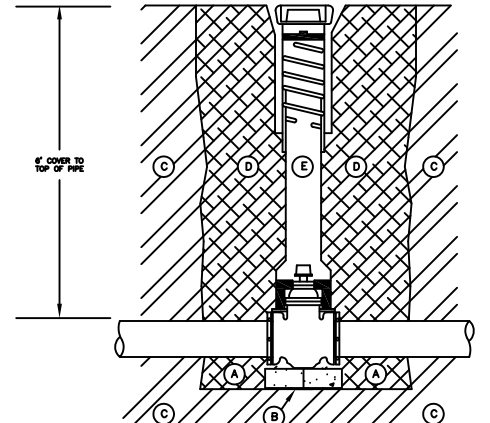


WDTL-FH FIRE HYDRANT COMPLETE NOT TO SCALE



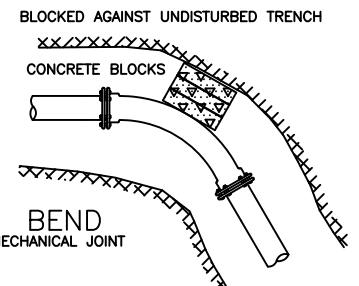
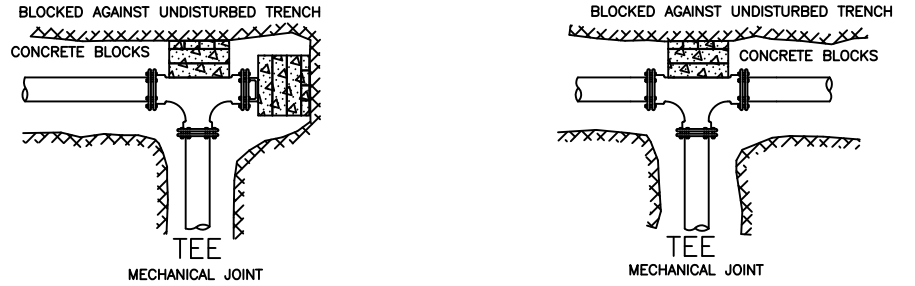
ITEM	DESCRIPTION
A	SEE FIRE HYDRANT DETAIL SHEET FOR ALL SPECIFICATIONS

WDTL-FHO OFFSET FIRE HYDRANT COMPLETE WITH ELL NOT TO SCALE

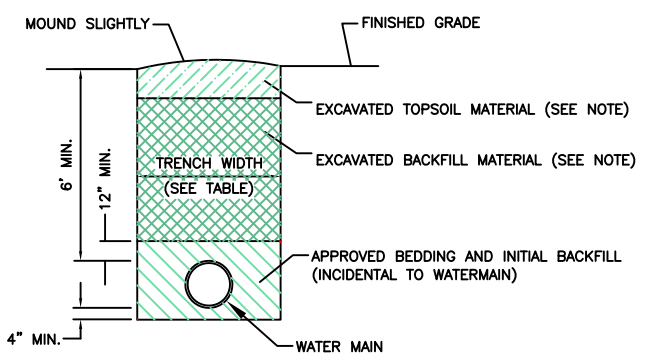


ITEM	DESCRIPTION
A	APPROVED BEDDING MATERIAL
B	16"x 8"x 4" CONCRETE BLOCKING
C	UNDISTURBED SOIL
D	APPROVED GRANULAR, TRENCH BACKFILL MATERIAL
E	VALVE BOXES WILL BE THE TYLER / UNION CAST IRON 6850 SERIES- WITH DEBRIS CAP & ADAPTER II INSTALLED

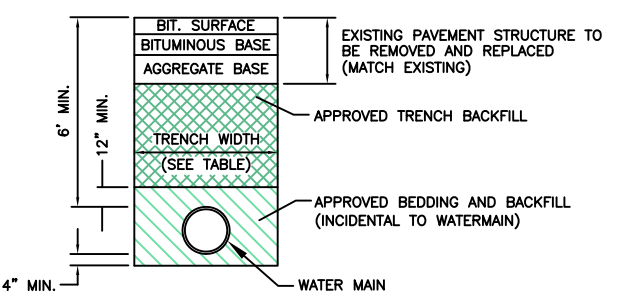
WDTL-VB VALVE & VALVE BOX DETAIL NOT TO SCALE



WDTL-THBK THRUST BLOCK INSTALLATIONS NOT TO SCALE



TYPICAL FARM/TURF TRENCH DETAIL
(NTS)
NOTE: CONTRACTOR SHALL SEPARATE TOPSOIL AND CLAY/SAND EXCAVATED MATERIAL FROM TRENCH INTO TWO SEPARATE STOCKPILES AND PLACE TOPSOIL ON TOP 12" OF TRENCH PER DETAIL. INCIDENTAL TO CONTRACT.



DRIVEWAY AND ACCESS ROAD TRENCH DETAIL
(NTS)

FROM STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS 5TH EDITION

INSIDE DIAMETER OF CONDUIT IN INCHES "D"	MAXIMUM TRENCH WIDTH IN FEET FOR PAYMENT
6	3.58
8	3.78
10	3.97
12	4.17
14	4.36
15	4.46
16	4.56
18	4.75
20	4.94
21	5.04
24	5.33

RO019



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ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
RIAT ROAD
WATER MAIN
DETAILS - 2

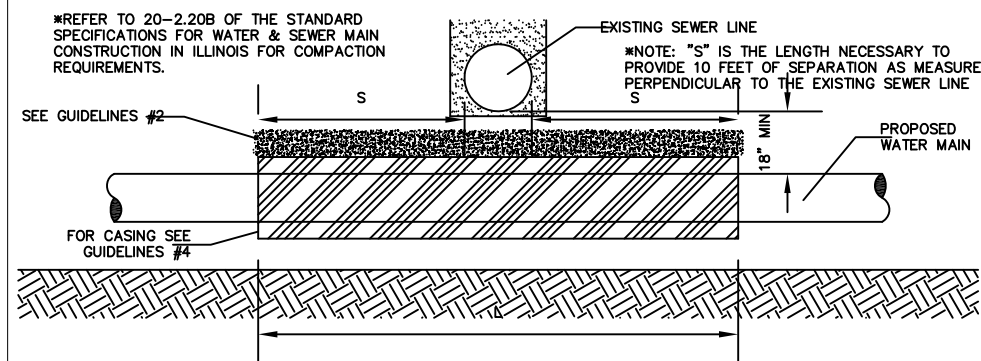
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

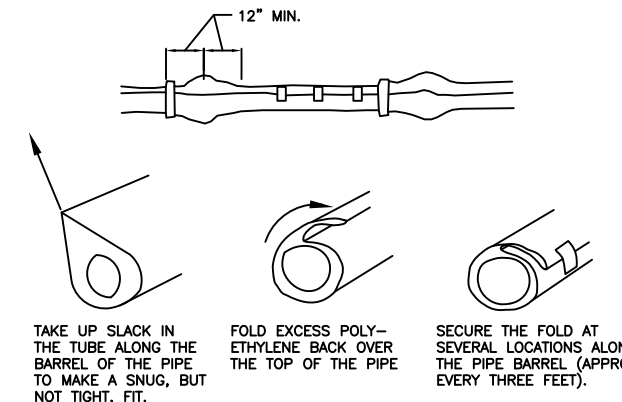
SHEET NO. 54 REVISION

SHEET 54 OF 67 SHEETS

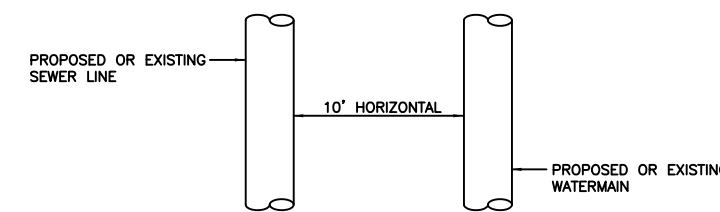
****THIS DETAIL APPLIES ONLY FOR PROPOSED WATER MAIN INSTALLED BELOW AN EXISTING SEWER LINE WITH AN 18" MINIMUM VERTICAL SEPARATION.**



- GUIDELINES**
- 1.) OMIT SELECT GRANULAR EMBEDMENT AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF WATER MAIN AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT THE LENGTH OF "L".
 - 2.) IF SELECT GRANULAR BACKFILL EXISTS, REMOVE WITHIN WIDTH OF EXISTING SEWER LINE TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.
 - 3.) PROVIDE ADEQUATE SUPPORT FOR EXISTING SEWER LINE TO PREVENT DAMAGE DUE TO SETTLEMENT.
 - 4.) USE "L" FEET OF WATER MAIN MATERIAL FOR CASING OF PROPOSED WATER MAIN AND SEAL ENDS OF CASING
- PROPOSED WATER MAIN BELOW EXISTING SEWER LINE W/ 18" MIN SEPARATION**
10/15/08 KRL N.T.S.

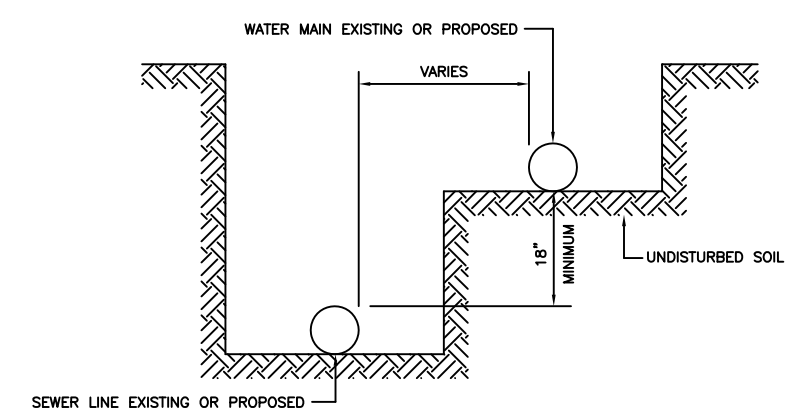


NOTE: REPAIR ALL SMALL RIPS, TEARS OR OTHER TUBE DAMAGE WITH ADHESIVE TAPE.
POLYETHYLENE ENCASEMENT
N.T.S.
NOTE:
ONE LENGTH OF POLYETHYLENE TUBE FOR EACH LENGTH OF PIPE, OVERLAPPED AT JOINT.



WHEN PROPOSED SEWER (OR WATER) IS LOCATED 10 FEET OR MORE FROM EXISTING WATER (OR SEWER), NO SPECIAL CONSTRUCTION REQUIRED. SEE SECTION 41-2.01B (1).

PLAN VIEW



WHEN PROPOSED SEWER (OR WATER) IS LOCATED LESS THAN 10 FEET FROM EXISTING WATER (OR SEWER), DETAILS BELOW SHALL APPLY. SEE SECTION 41-2.01B (2) OF STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS - 5TH EDITION.

WATER AND SEWER SEPARATION REQUIREMENTS

HORIZONTAL SEPARATION
N.T.S.
DIV. V/STANDARD DRAWING NO. 18

**RIAT ROAD WATER MAIN
DETAILS - 2**



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APPROVED AS WORKING PLAN
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REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
SITE PLAN
AND CONTROL (1SP1)

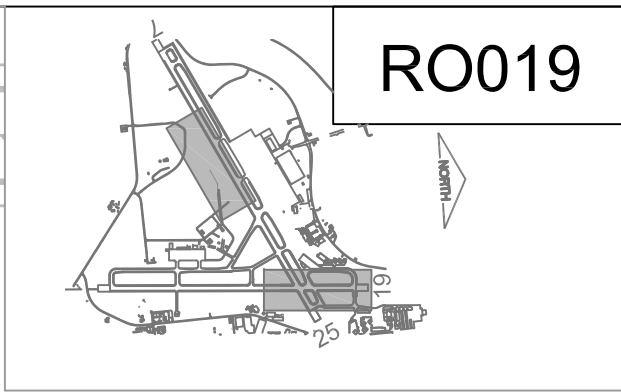
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

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DATE: APRIL 22, 2011

SHEET NO. 55 REVISION

SHEET 55 OF 67 SHEETS

RO019

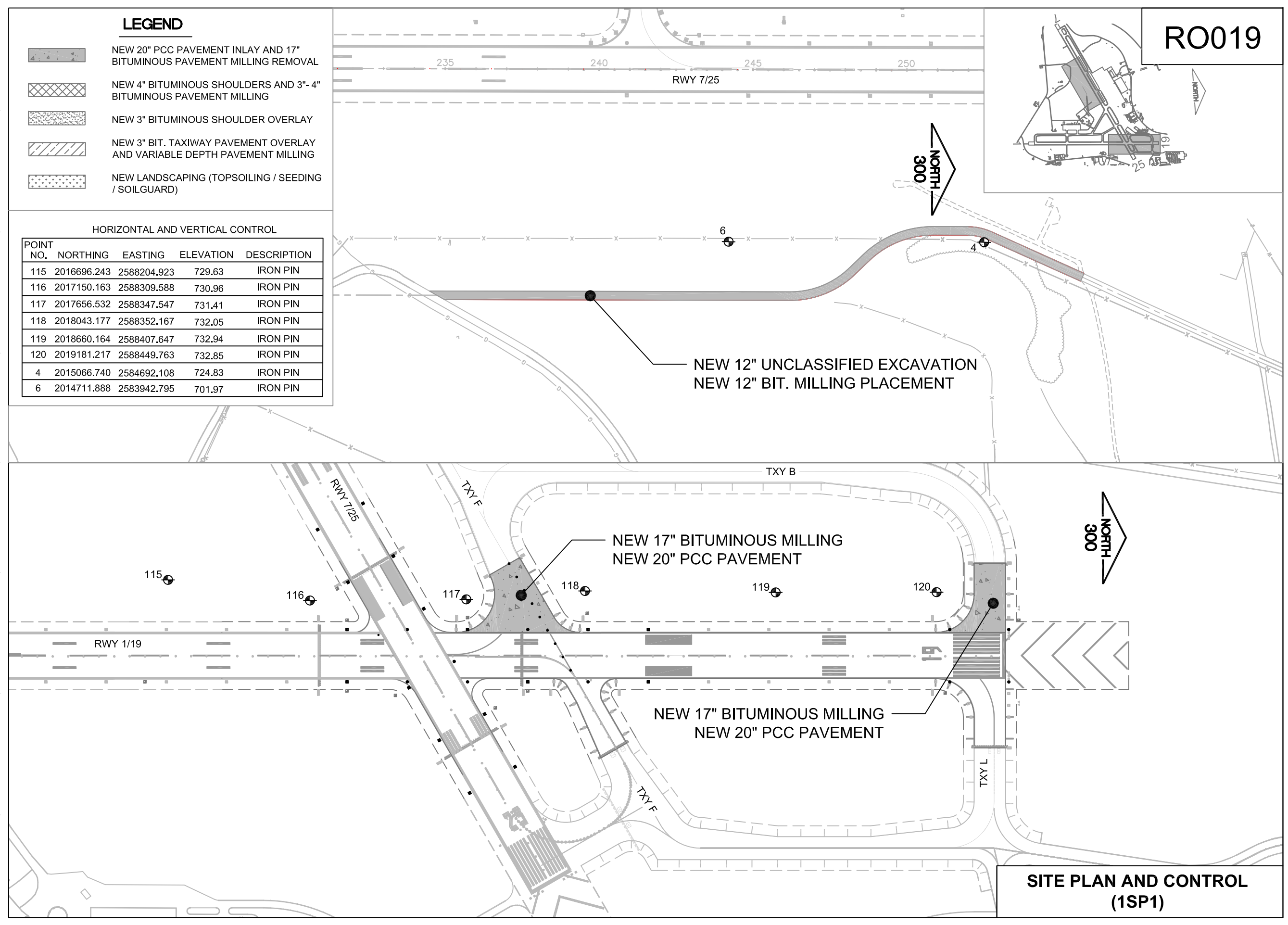


LEGEND

- NEW 20" PCC PAVEMENT INLAY AND 17" BITUMINOUS PAVEMENT MILLING REMOVAL
- NEW 4" BITUMINOUS SHOULDERS AND 3"- 4" BITUMINOUS PAVEMENT MILLING
- NEW 3" BITUMINOUS SHOULDER OVERLAY
- NEW 3" BIT. TAXIWAY PAVEMENT OVERLAY AND VARIABLE DEPTH PAVEMENT MILLING
- NEW LANDSCAPING (TOPSOILING / SEEDING / SOILGUARD)

HORIZONTAL AND VERTICAL CONTROL

POINT NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
115	2016696.243	2588204.923	729.63	IRON PIN
116	2017150.163	2588309.588	730.96	IRON PIN
117	2017656.532	2588347.547	731.41	IRON PIN
118	2018043.177	2588352.167	732.05	IRON PIN
119	2018660.164	2588407.647	732.94	IRON PIN
120	2019181.217	2588449.763	732.85	IRON PIN
4	2015066.740	2584692.108	724.83	IRON PIN
6	2014711.888	2583942.795	701.97	IRON PIN



**SITE PLAN AND CONTROL
(1SP1)**



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APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

TYPICAL SECTION
TXY F WEST (1TY1)

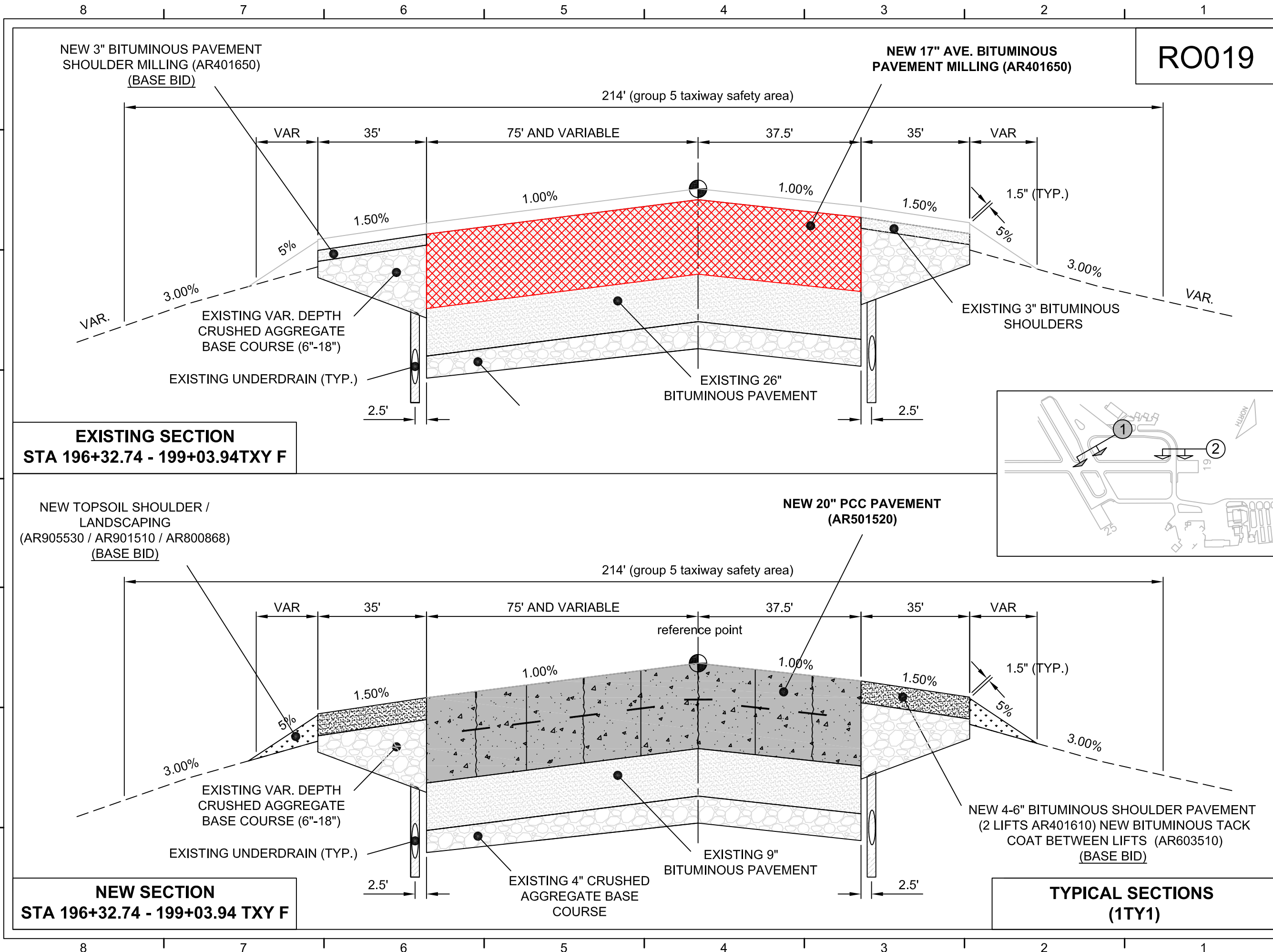
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **56** REVISION

SHEET 56 OF 67 SHEETS

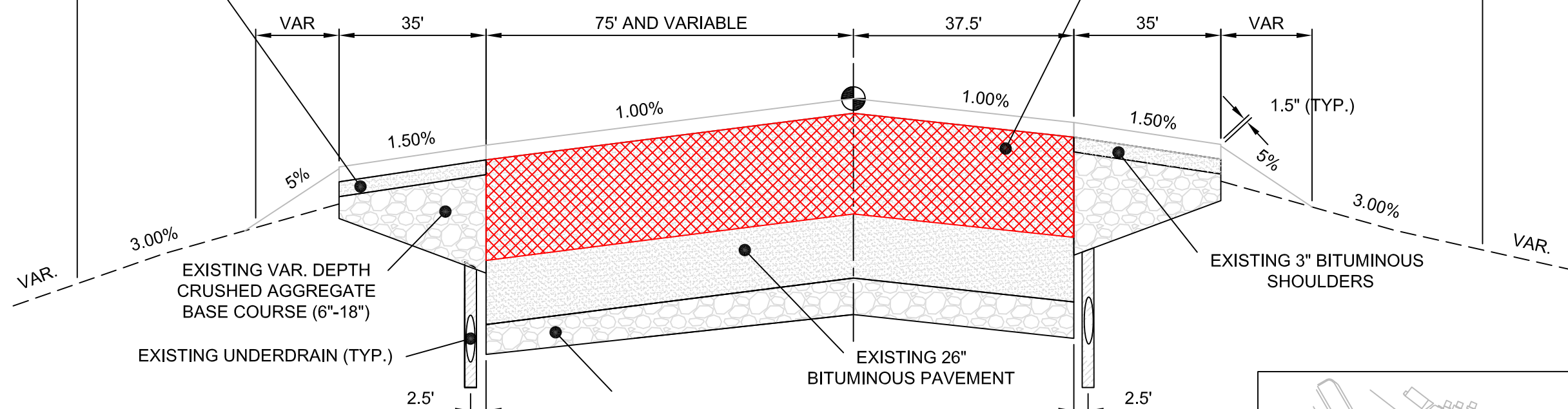
RO019



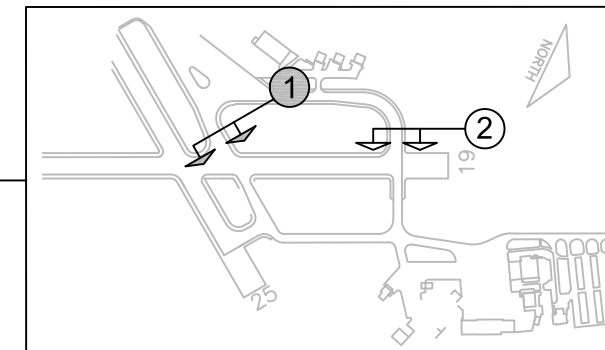
NEW 3" BITUMINOUS PAVEMENT
SHOULDER MILLING (AR401650)
(BASE BID)

NEW 17" AVE. BITUMINOUS
PAVEMENT MILLING (AR401650)

214' (group 5 taxiway safety area)



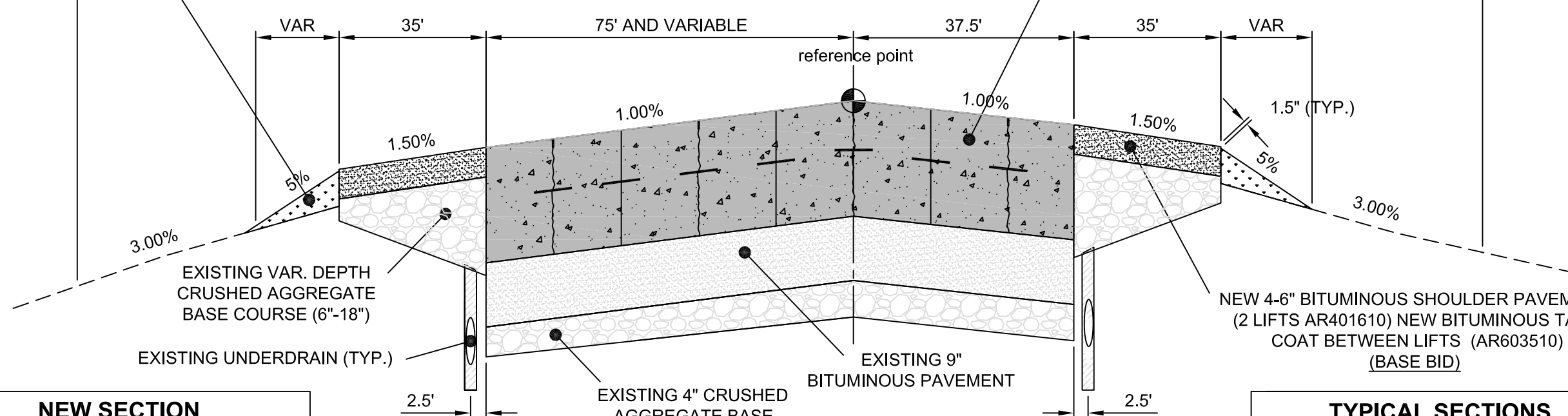
EXISTING SECTION
STA 196+32.74 - 199+03.94 TXY F



NEW TOPSOIL SHOULDER /
LANDSCAPING
(AR905530 / AR901510 / AR800868)
(BASE BID)

NEW 20" PCC PAVEMENT
(AR501520)

214' (group 5 taxiway safety area)



NEW SECTION
STA 196+32.74 - 199+03.94 TXY F

TYPICAL SECTIONS
(1TY1)



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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

TYPICAL SECTION
TTY B (1TY2)

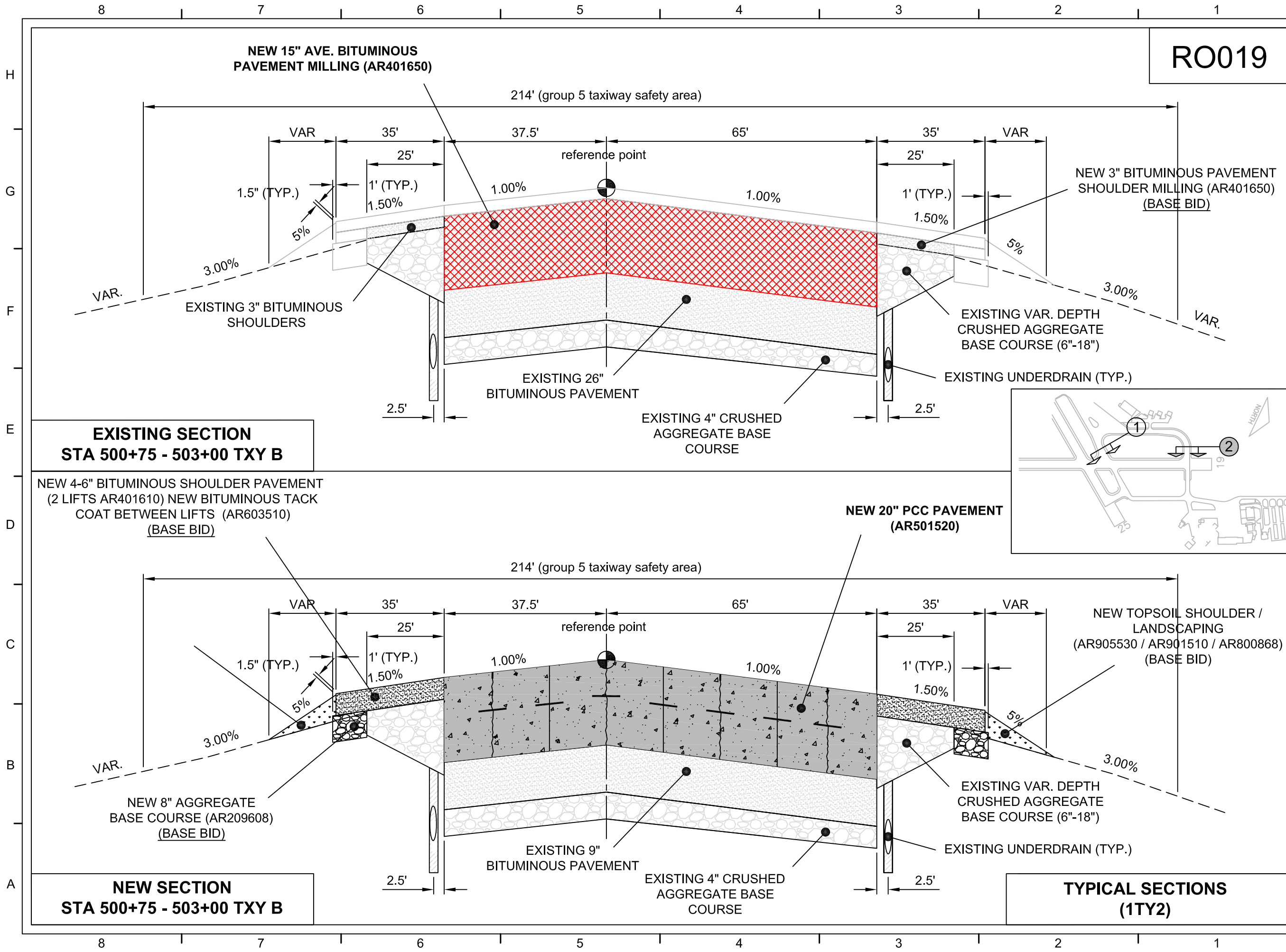
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

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DATE: APRIL 22, 2011

SHEET NO. **57** REVISION

SHEET 57 OF 67 SHEETS

RO019



EXISTING SECTION
STA 500+75 - 503+00 TXY B

NEW 4-6" BITUMINOUS SHOULDER PAVEMENT
(2 LIFTS AR401610) NEW BITUMINOUS TACK
COAT BETWEEN LIFTS (AR603510)
(BASE BID)

NEW SECTION
STA 500+75 - 503+00 TXY B

TYPICAL SECTIONS
(1TY2)



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APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
EXISTING CONDITIONS
AND REMOVALS (1EX1)

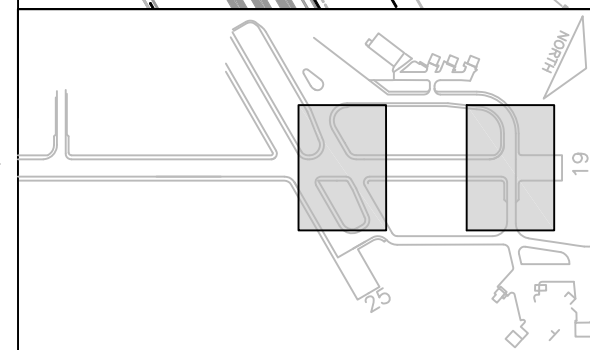
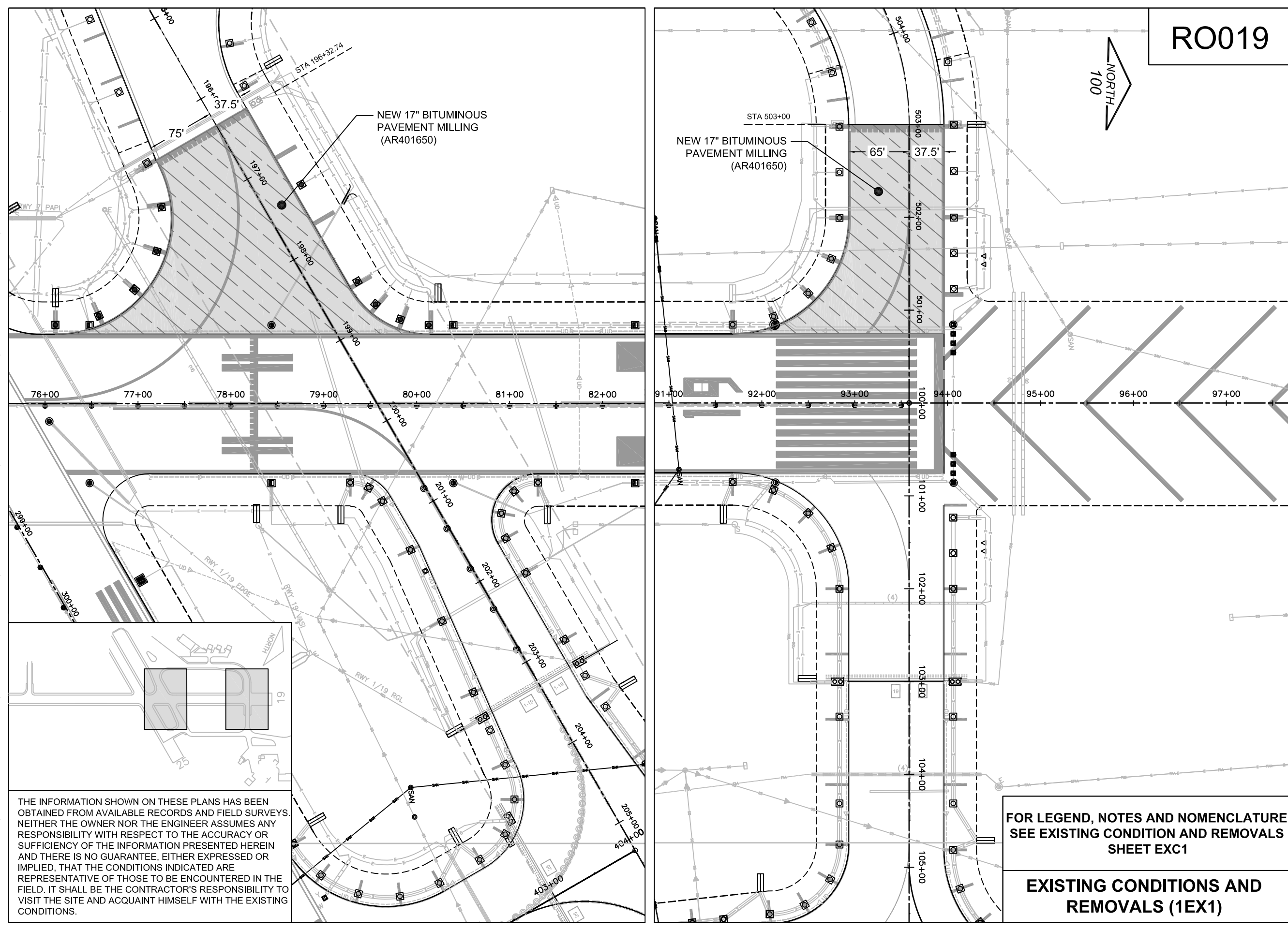
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

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DATE: APRIL 22, 2011

SHEET NO. **58** REVISION

SHEET 58 OF 67 SHEETS

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FOR LEGEND, NOTES AND NOMENCLATURE
SEE EXISTING CONDITION AND REMOVALS
SHEET EXC1

**EXISTING CONDITIONS AND
REMOVALS (1EX1)**



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BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
RIAT ROAD SITE PLAN
AND TYPICAL SECTION
(1RR1)

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DATE: APRIL 22, 2011

SHEET NO. 59 REVISION

SHEET 59 OF 67 SHEETS

RO019



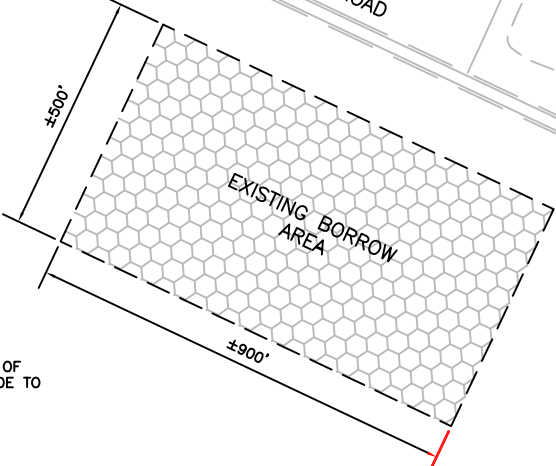
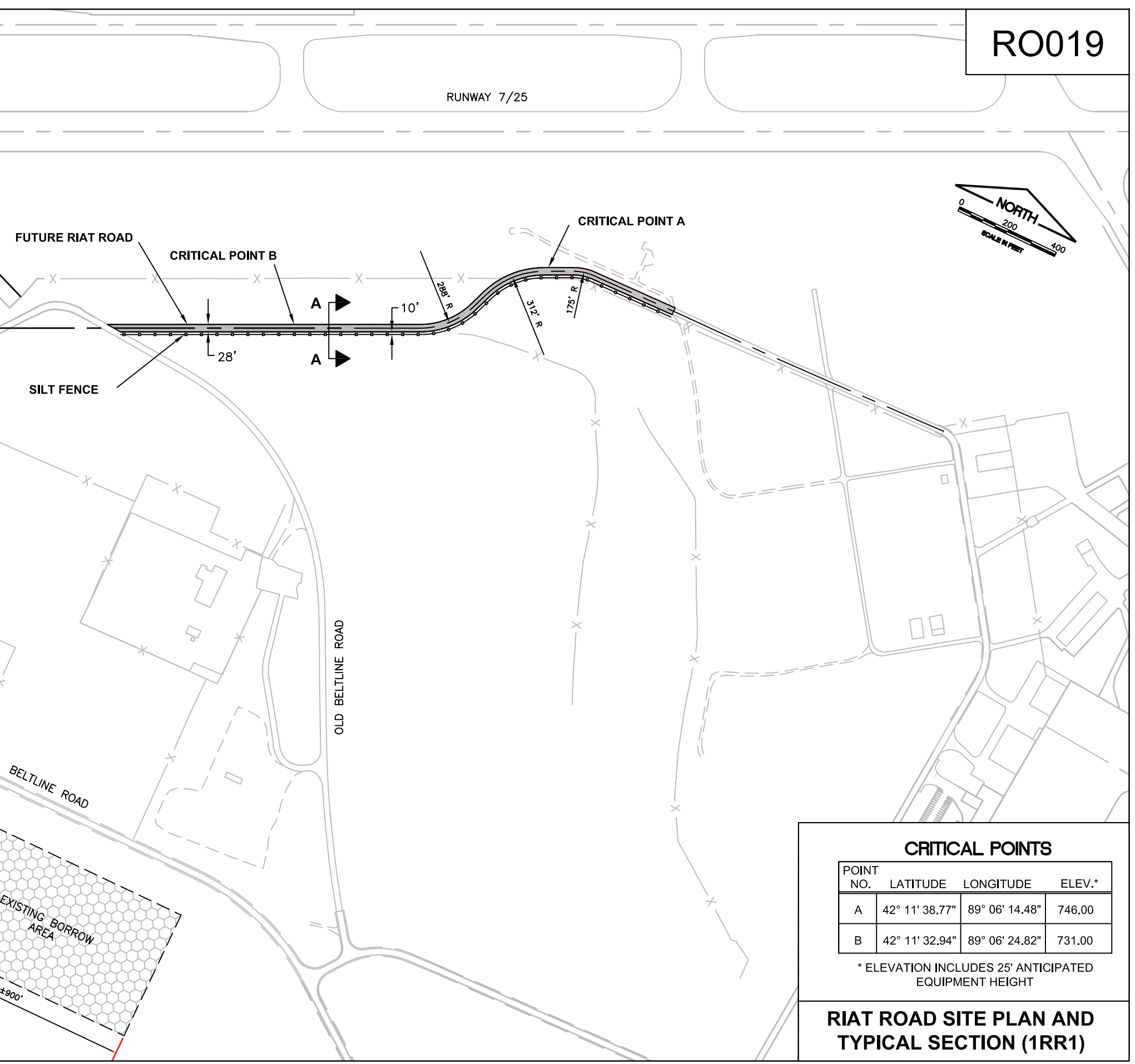
RUNWAY 7/25

LEGEND

- NEW 12" UNCLASSIFIED EXCAVATION AND 12" PLACEMENT OF BITUMINOUS MILLINGS
- PROPOSED PLACEMENT OF EXCESS EXCAVATION FROM RIAT ROAD
- NEW SILT FENCE
- EXISTING AIRFIELD FENCE

TYPICAL SECTION A-A
NOT TO SCALE

EXISTING GROUNDLINE
12" PLACEMENT OF BITUMINOUS MILLING
12" UNCLASSIFIED EXCAVATION



NOTES

- CONTRACTOR SHALL PLACE A MINIMUM OF 4" OF TOPSOIL IN EXISTING BORROW AREA AND GRADE TO DRAIN.

CRITICAL POINTS

POINT NO.	LATITUDE	LONGITUDE	ELEV.*
A	42° 11' 38.77"	89° 06' 14.48"	746.00
B	42° 11' 32.94"	89° 06' 24.82"	731.00

* ELEVATION INCLUDES 25' ANTICIPATED EQUIPMENT HEIGHT

RIAT ROAD SITE PLAN AND TYPICAL SECTION (1RR1)



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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:

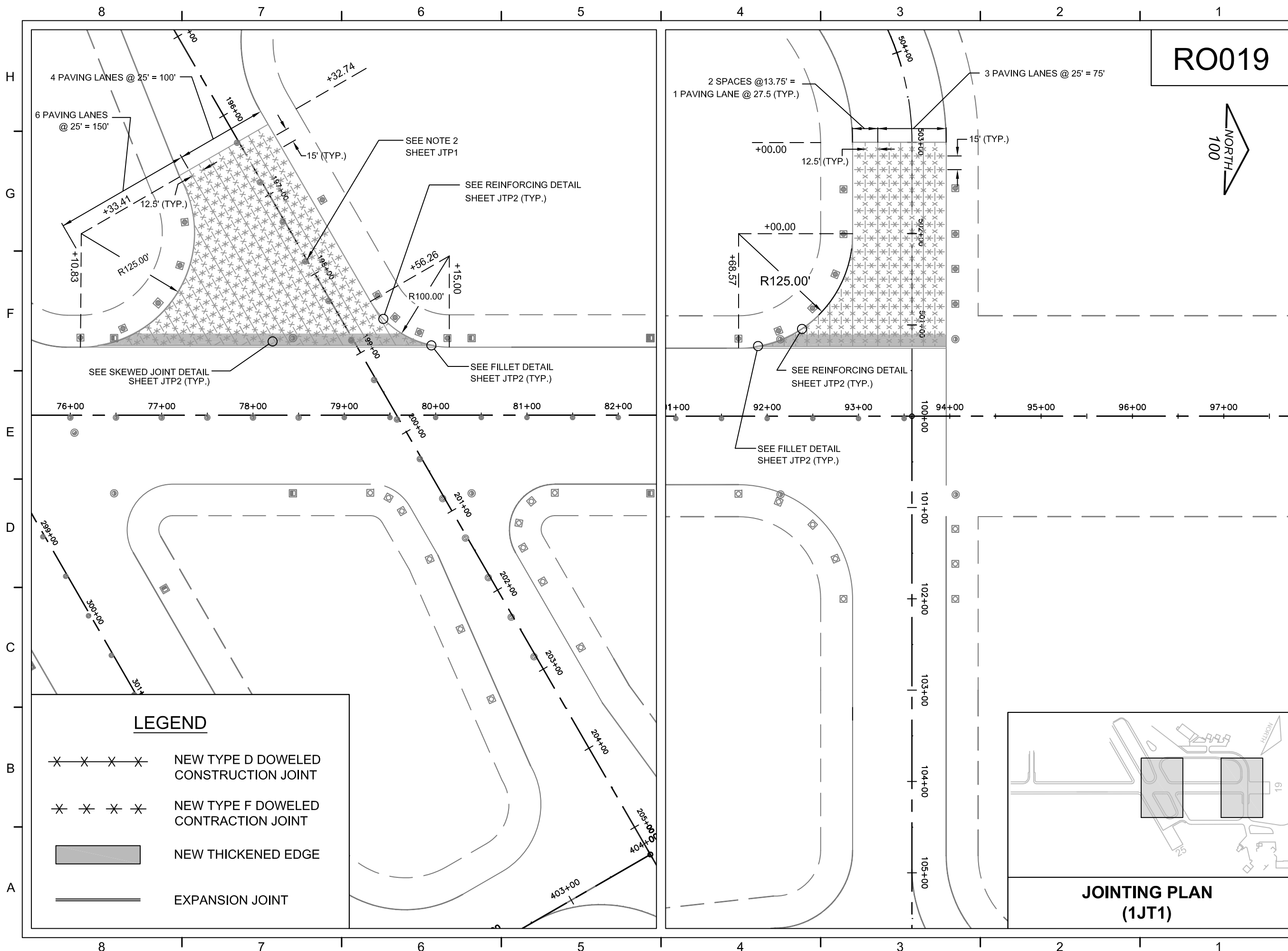
**JOINTING PLAN
(1JT1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

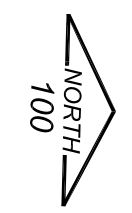
PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **60** REVISION

SHEET 60 OF 67 SHEETS

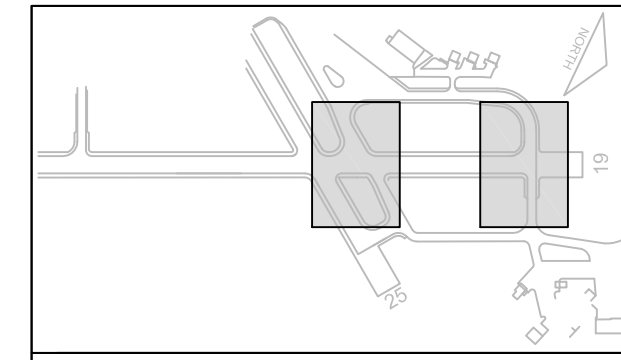


RO019



LEGEND

- NEW TYPE D DOWELED CONSTRUCTION JOINT
- NEW TYPE F DOWELED CONTRACTION JOINT
- NEW THICKENED EDGE
- EXPANSION JOINT



**JOINTING PLAN
(1JT1)**



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BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:
SITE PLAN
AND CONTROL (2PL1)

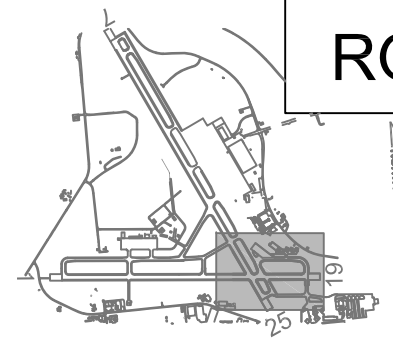
DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **61** REVISION

SHEET 61 OF 67 SHEETS

RO019



LEGEND

- NEW 20" PCC PAVEMENT INLAY AND 17" BITUMINOUS PAVEMENT MILLING REMOVAL
- NEW 4" BITUMINOUS SHOULDERS AND 3" - 4" BITUMINOUS PAVEMENT MILLING
- NEW 3" BITUMINOUS SHOULDER OVERLAY
- NEW 3" BIT. TAXIWAY PAVEMENT OVERLAY AND VARIABLE DEPTH PAVEMENT MILLING
- NEW LANDSCAPING (TOPSOILING / SEEDING / SOILGUARD)

HORIZONTAL AND VERTICAL CONTROL

POINT NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
115	2016696.243	2588204.923	729.63	IRON PIN
116	2017150.163	2588309.588	730.96	IRON PIN
117	2017656.532	2588347.547	731.41	IRON PIN
118	2018043.177	2588352.167	732.05	IRON PIN
119	2018660.164	2588407.647	732.94	IRON PIN
120	2019181.217	2588449.763	732.85	IRON PIN

NEW RUNWAY CENTERLINE LIGHT CIRCUIT / ADJUST CENTERLINE LIGHT

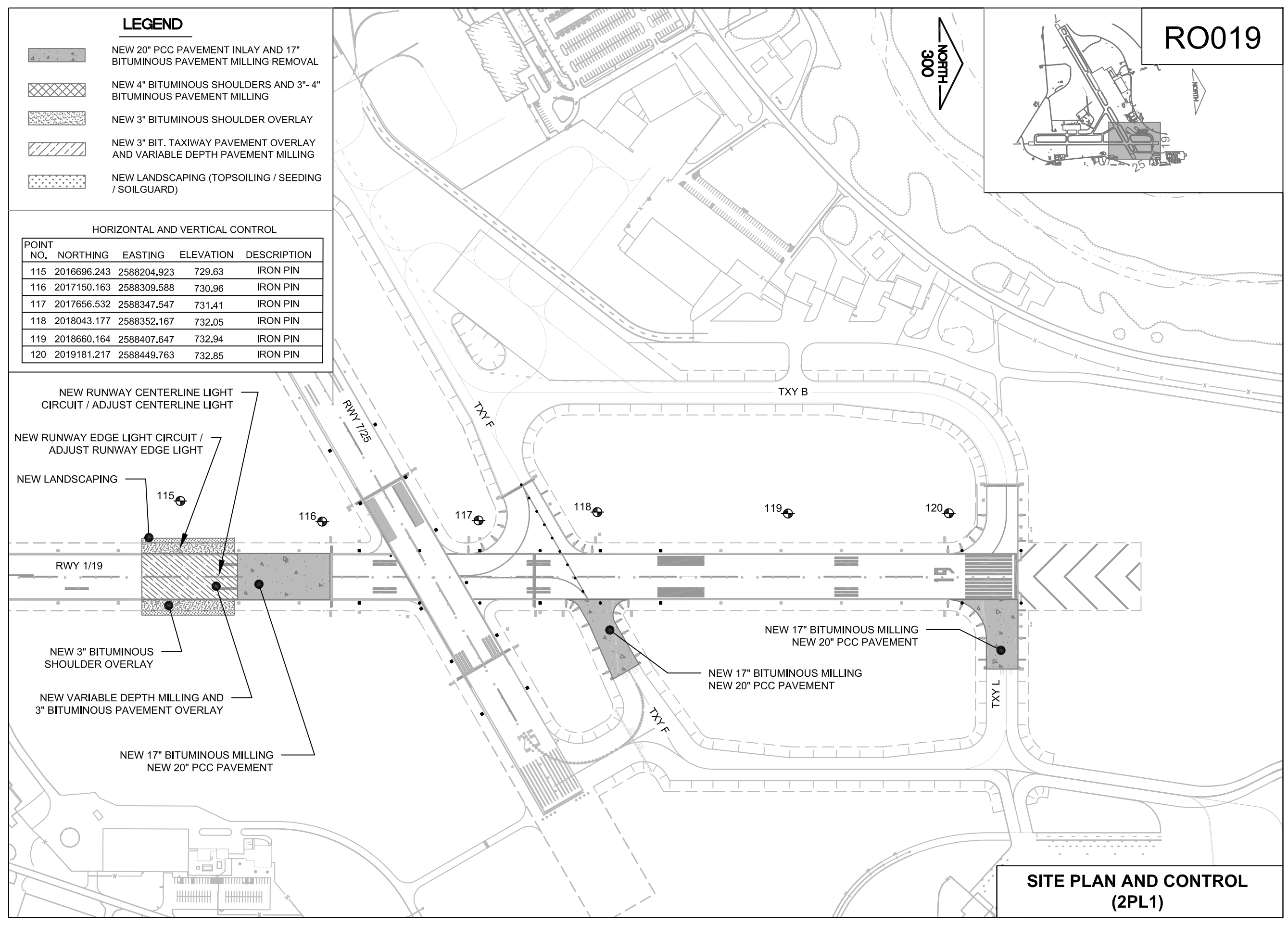
NEW RUNWAY EDGE LIGHT CIRCUIT / ADJUST RUNWAY EDGE LIGHT

NEW LANDSCAPING

NEW 3" BITUMINOUS SHOULDER OVERLAY

NEW VARIABLE DEPTH MILLING AND 3" BITUMINOUS PAVEMENT OVERLAY

NEW 17" BITUMINOUS MILLING
NEW 20" PCC PAVEMENT



**SITE PLAN AND CONTROL
(2PL1)**



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IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

TYPICAL SECTION
RWY 1/19 (2TY1)

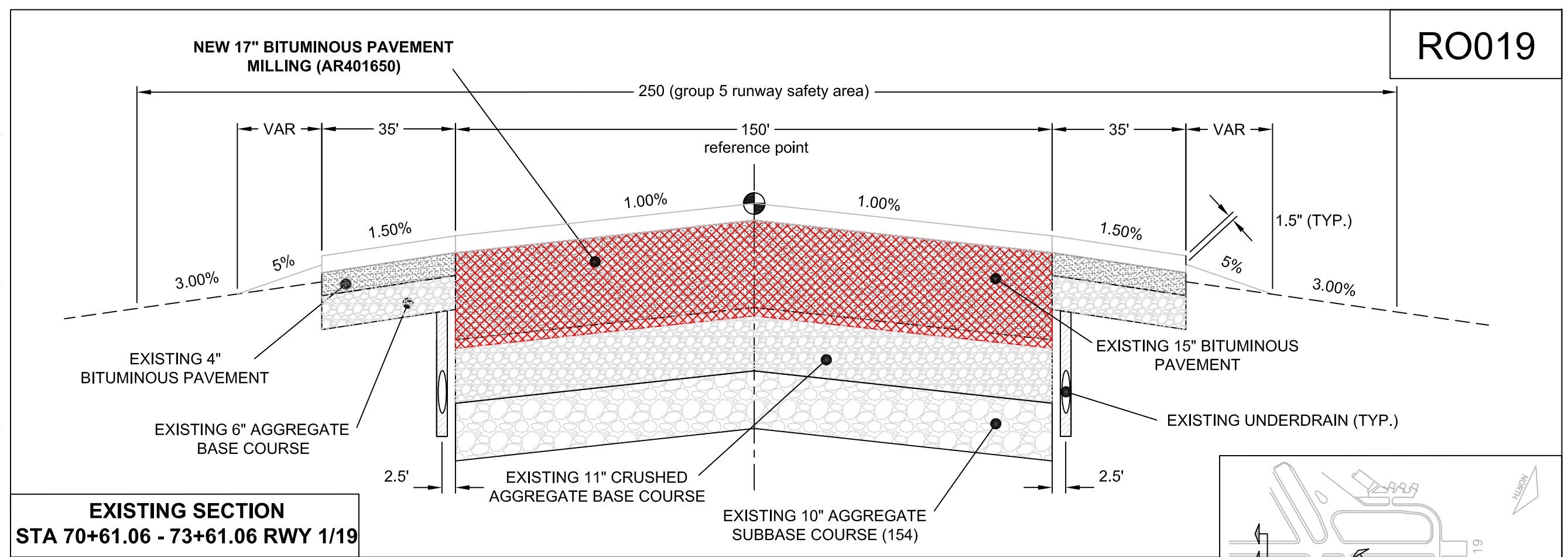
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CMT-RFD	CMT-RFD	CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

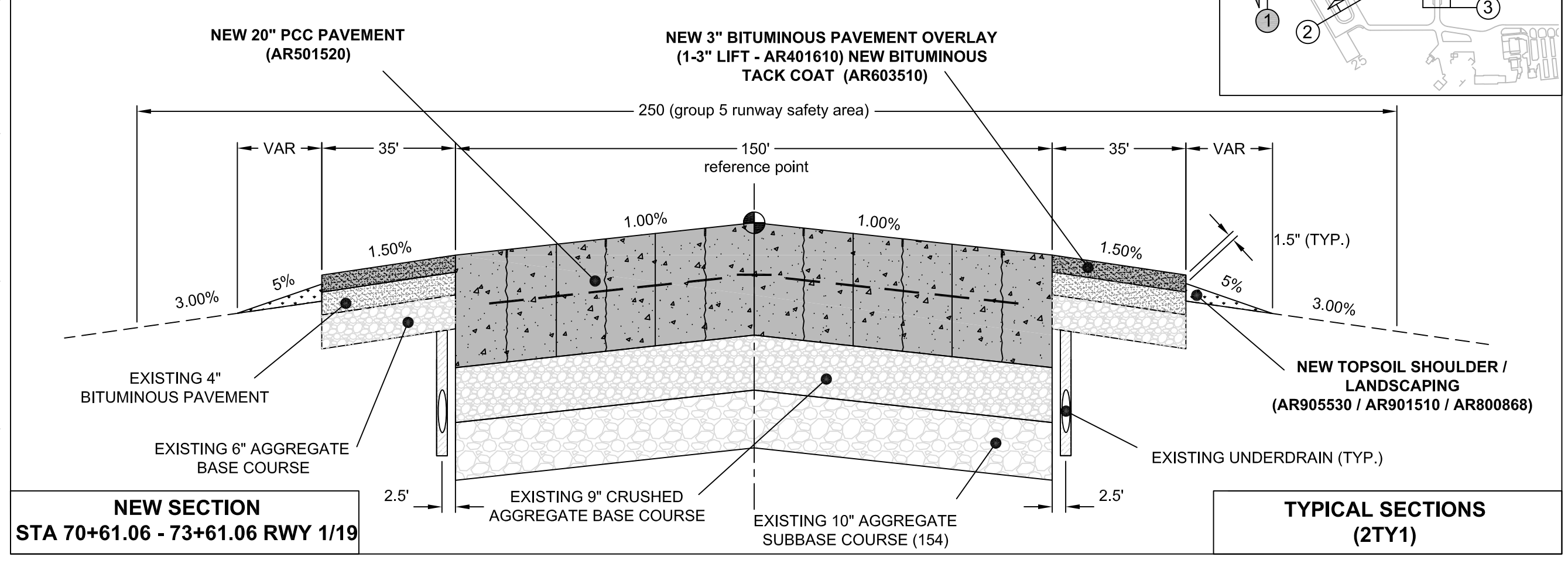
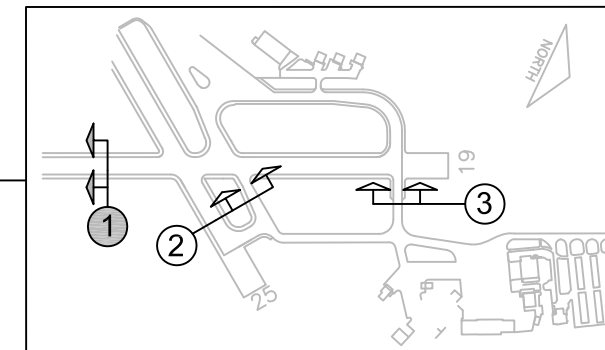
SHEET NO.	62	REVISION
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SHEET 62 OF 67 SHEETS

RO019



EXISTING SECTION
STA 70+61.06 - 73+61.06 RWY 1/19



NEW SECTION
STA 70+61.06 - 73+61.06 RWY 1/19

TYPICAL SECTIONS
(2TY1)



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APPROVED AS WORKING PLAN
BY:

REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

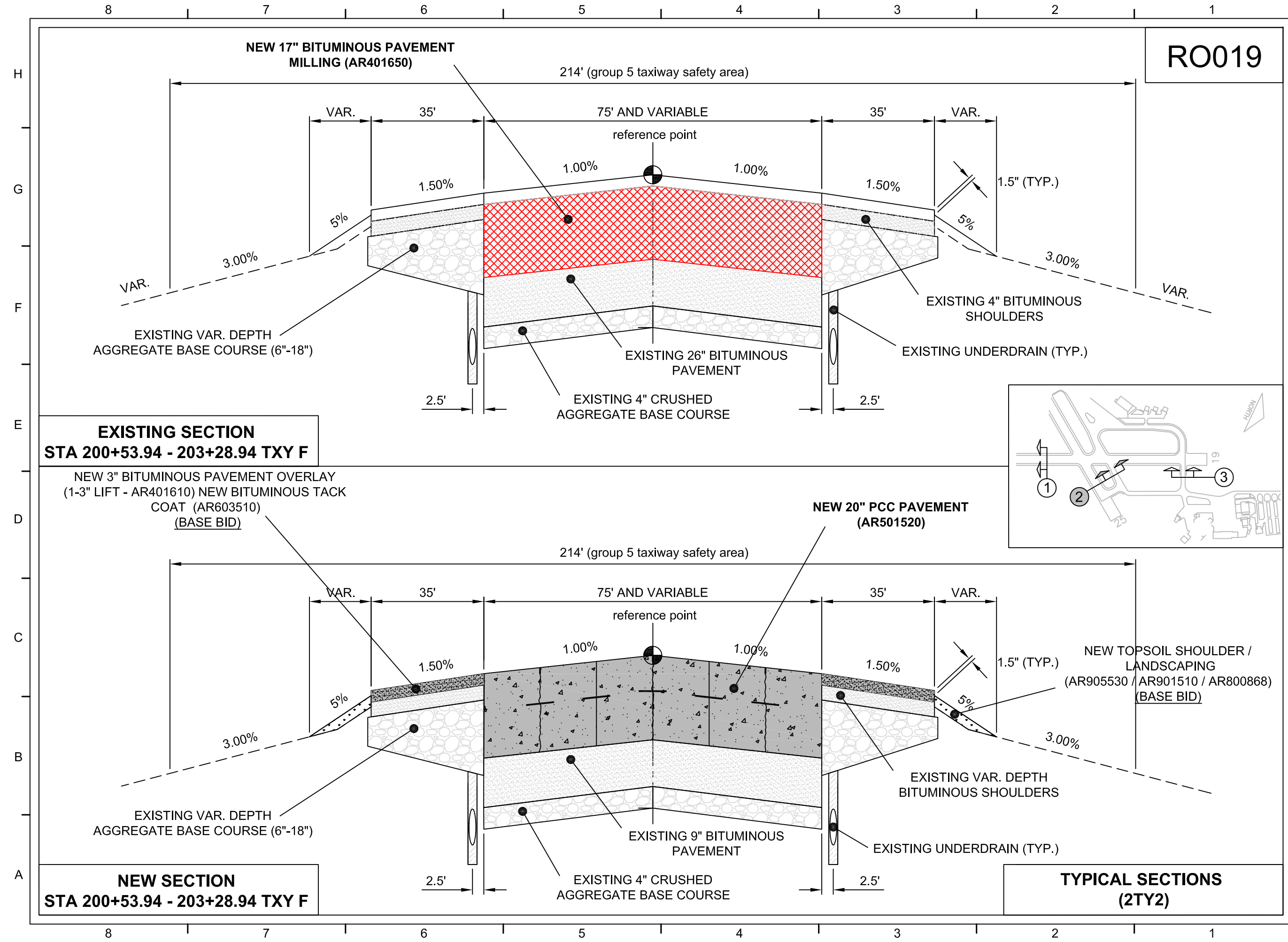
SHEET TITLE:
TYPICAL SECTION
TXY F EAST (2TY2)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **63** REVISION

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EXISTING SECTION
STA 200+53.94 - 203+28.94 TXY F

NEW SECTION
STA 200+53.94 - 203+28.94 TXY F

TYPICAL SECTIONS
(2TY2)



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APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

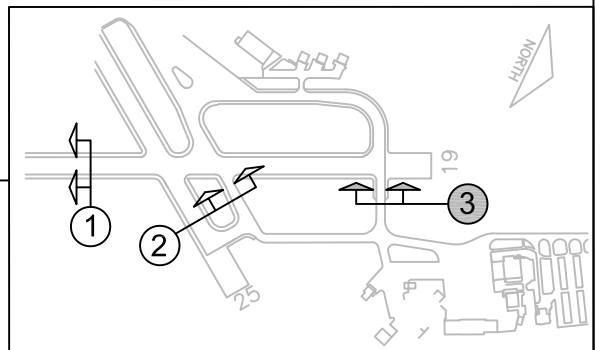
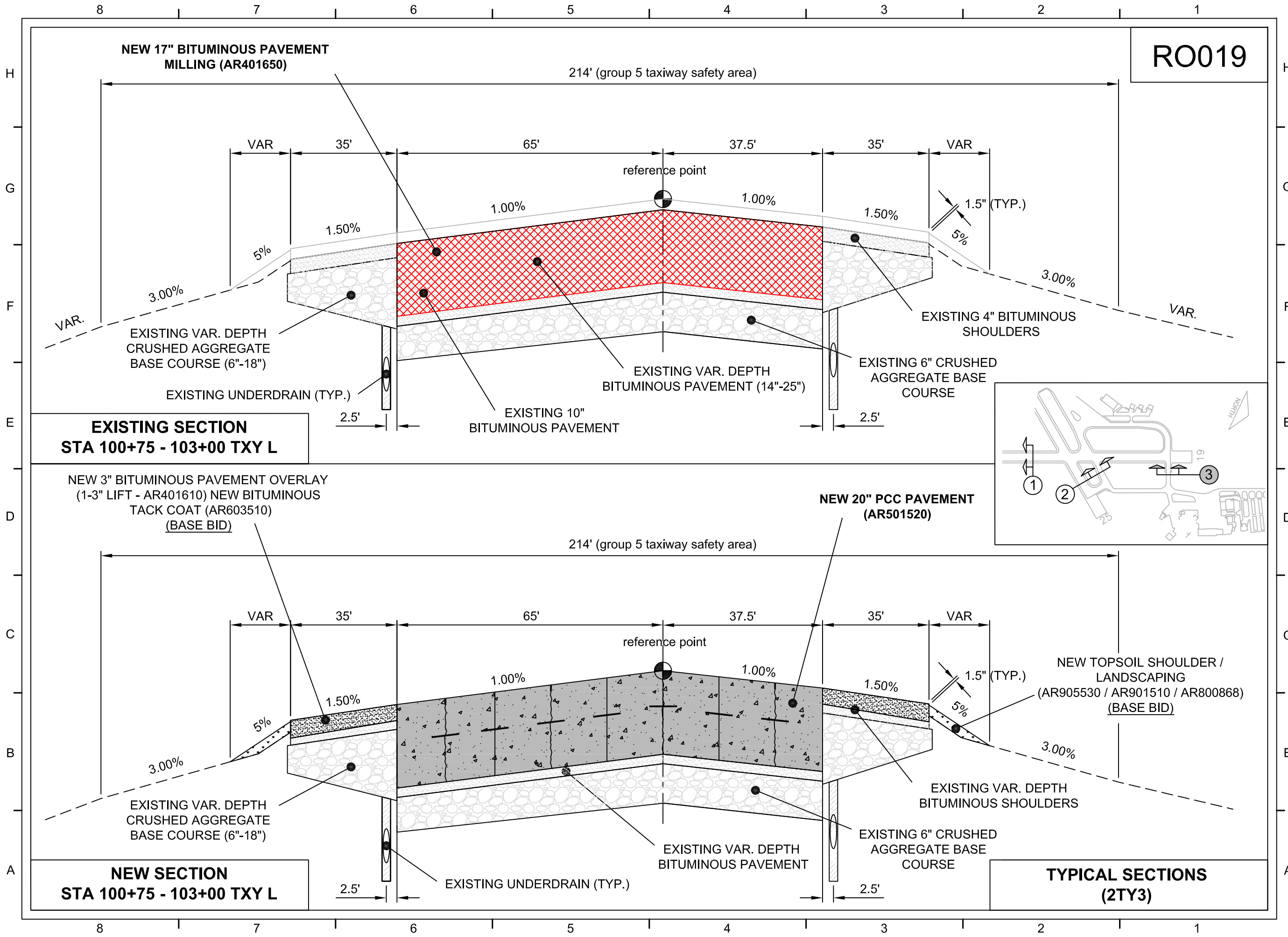
TYPICAL SECTION
TTY L (2TY3)

DESIGNED: CMT-RFD	DRAWN: CMT-RFD	CHECKED: CMT-ARR
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PROJECT NO.: 10258-03-00/06
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SHEET NO. **64** REVISION

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**TYPICAL SECTIONS
(2TY3)**



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REV DATE DESCRIPTION
PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

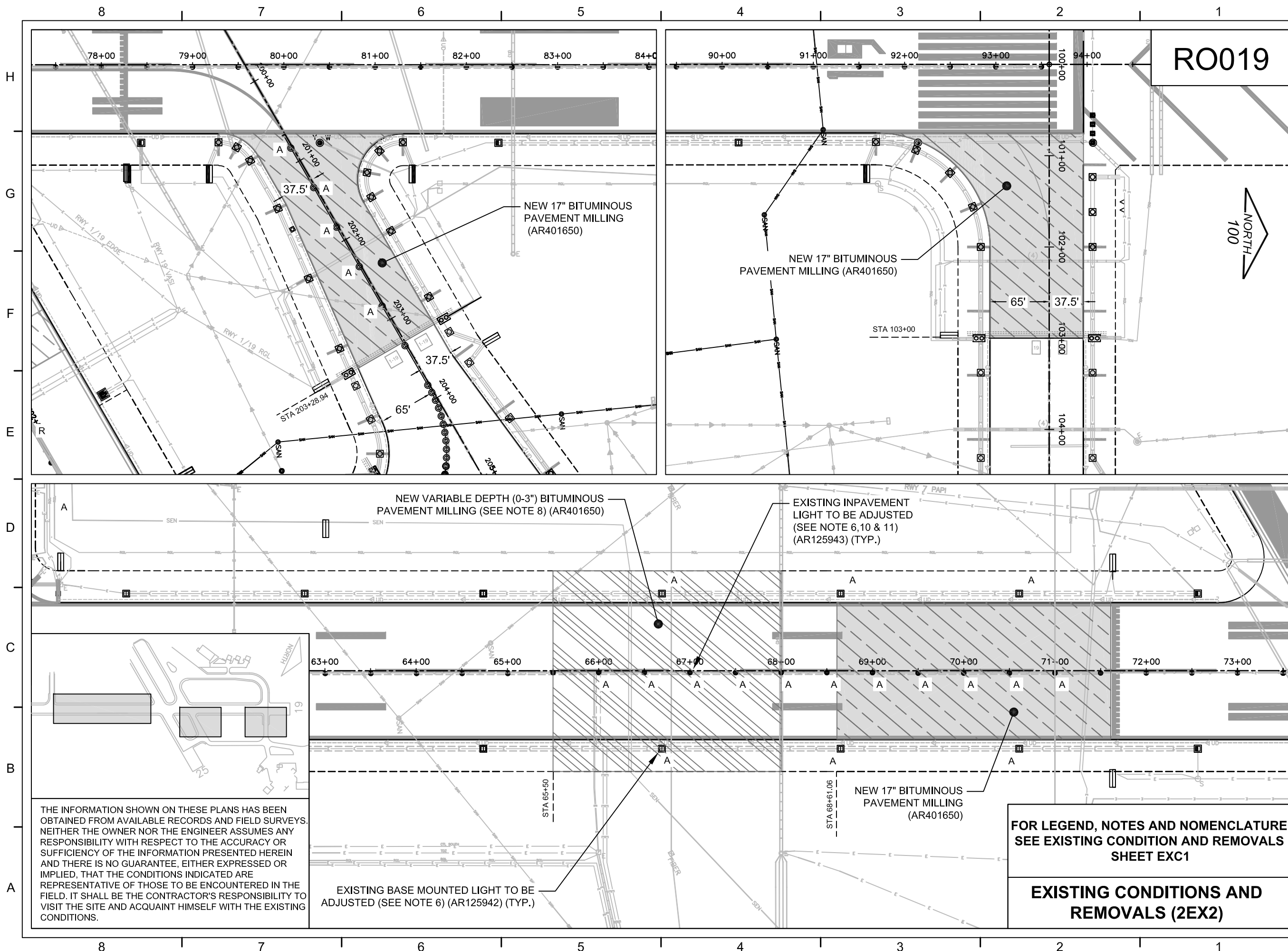
SHEET TITLE:
EXISTING CONDITIONS
AND REMOVALS (2EX1)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

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SHEET NO. **65** REVISION

SHEET 65 OF 67 SHEETS



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**FOR LEGEND, NOTES AND NOMENCLATURE
SEE EXISTING CONDITION AND REMOVALS
SHEET EXC1**

**EXISTING CONDITIONS AND
REMOVALS (2EX2)**



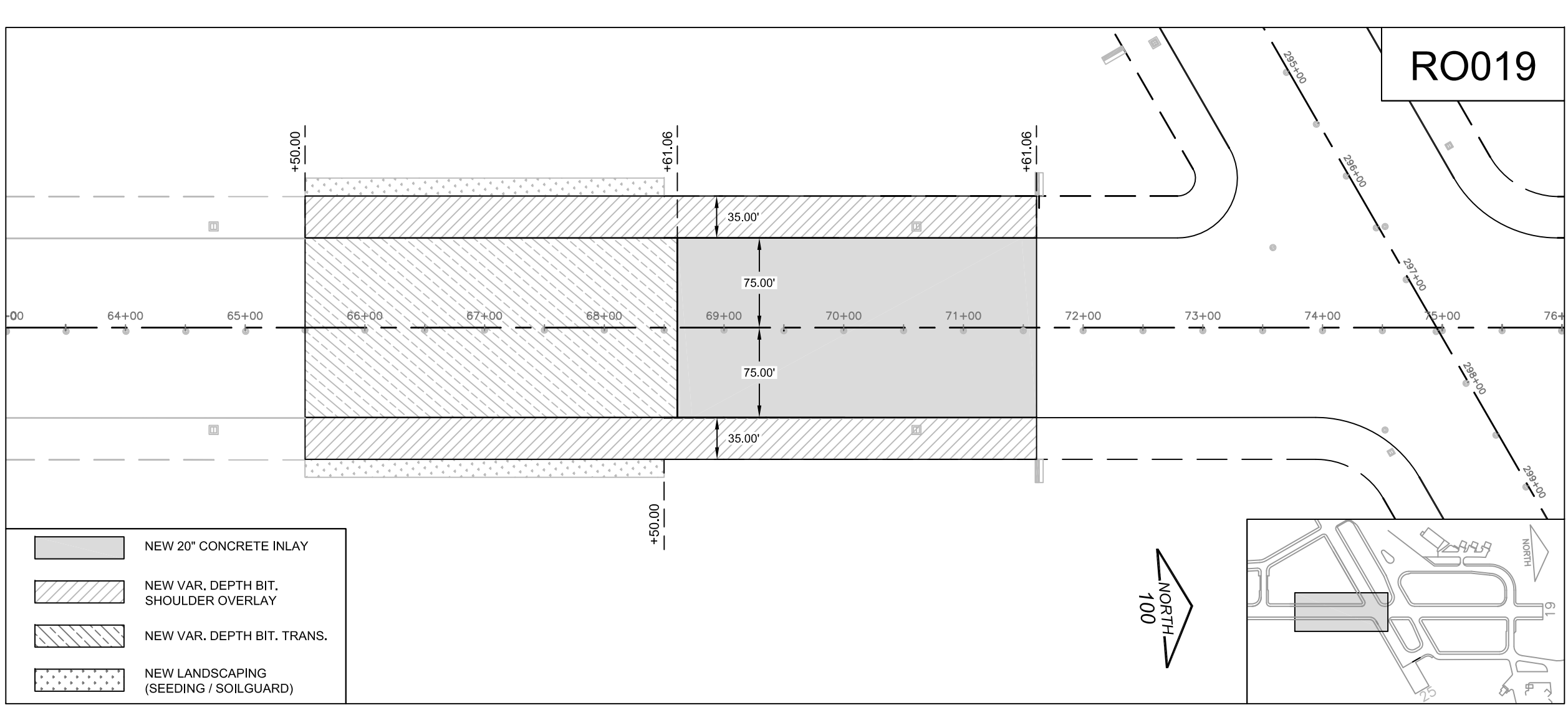
FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



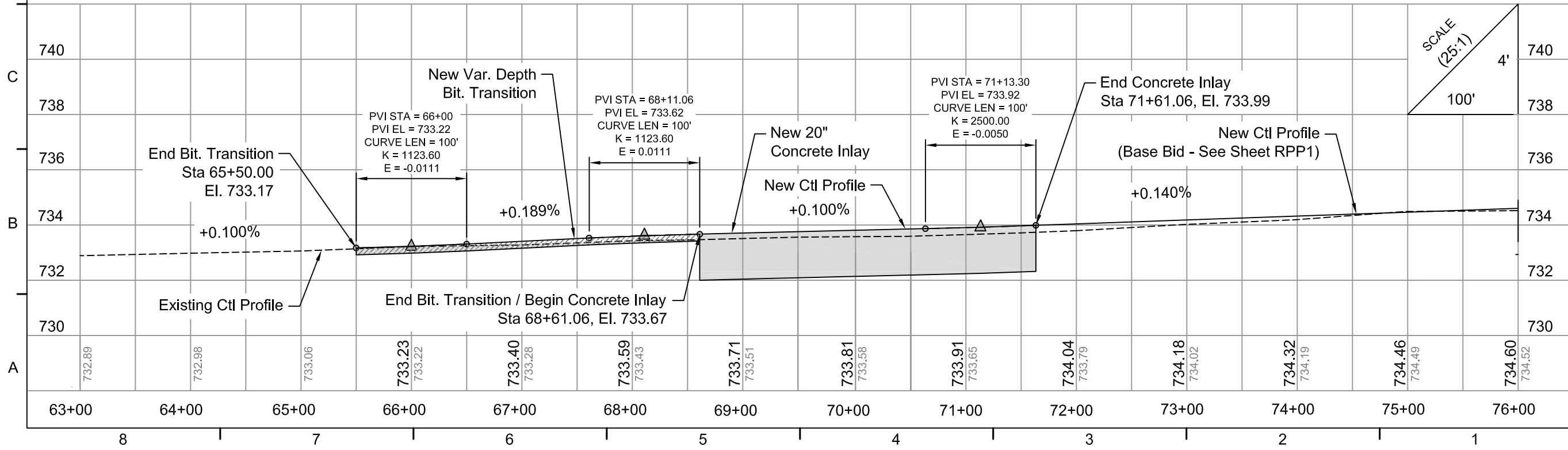
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IL Proj: RFD-4083



	NEW 20" CONCRETE INLAY
	NEW VAR. DEPTH BIT. SHOULDER OVERLAY
	NEW VAR. DEPTH BIT. TRANS.
	NEW LANDSCAPING (SEEDING / SOILGUARD)



APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:
**REHABILITATE
RUNWAY 1/19
PHASE 1**

SHEET TITLE:
**RUNWAY 1/19
PLAN/PROFILE (2PP1)**

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
DATE: APRIL 22, 2011

SHEET NO. **66** REVISION

SHEET 66 OF 67 SHEETS



FRANZ L. OLSON
ACTING EXECUTIVE DIRECTOR



Chicago Rockford
International Airport
Rockford, Illinois

Greater Rockford
Airport Authority

AIP Proj: 3-17-0088-XX
IL Proj: RFD-4083

APPROVED AS WORKING PLAN
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE
RUNWAY 1/19
PHASE 1

SHEET TITLE:

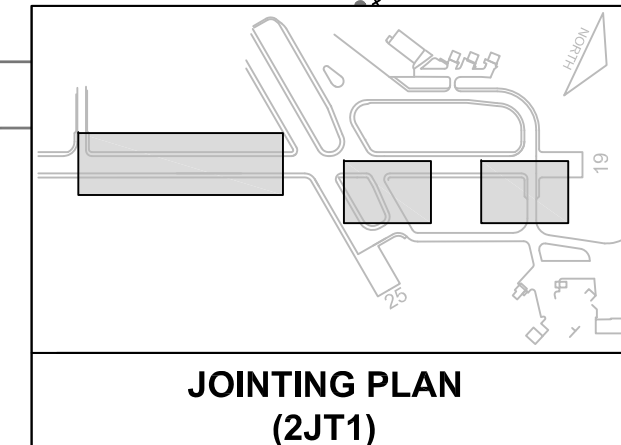
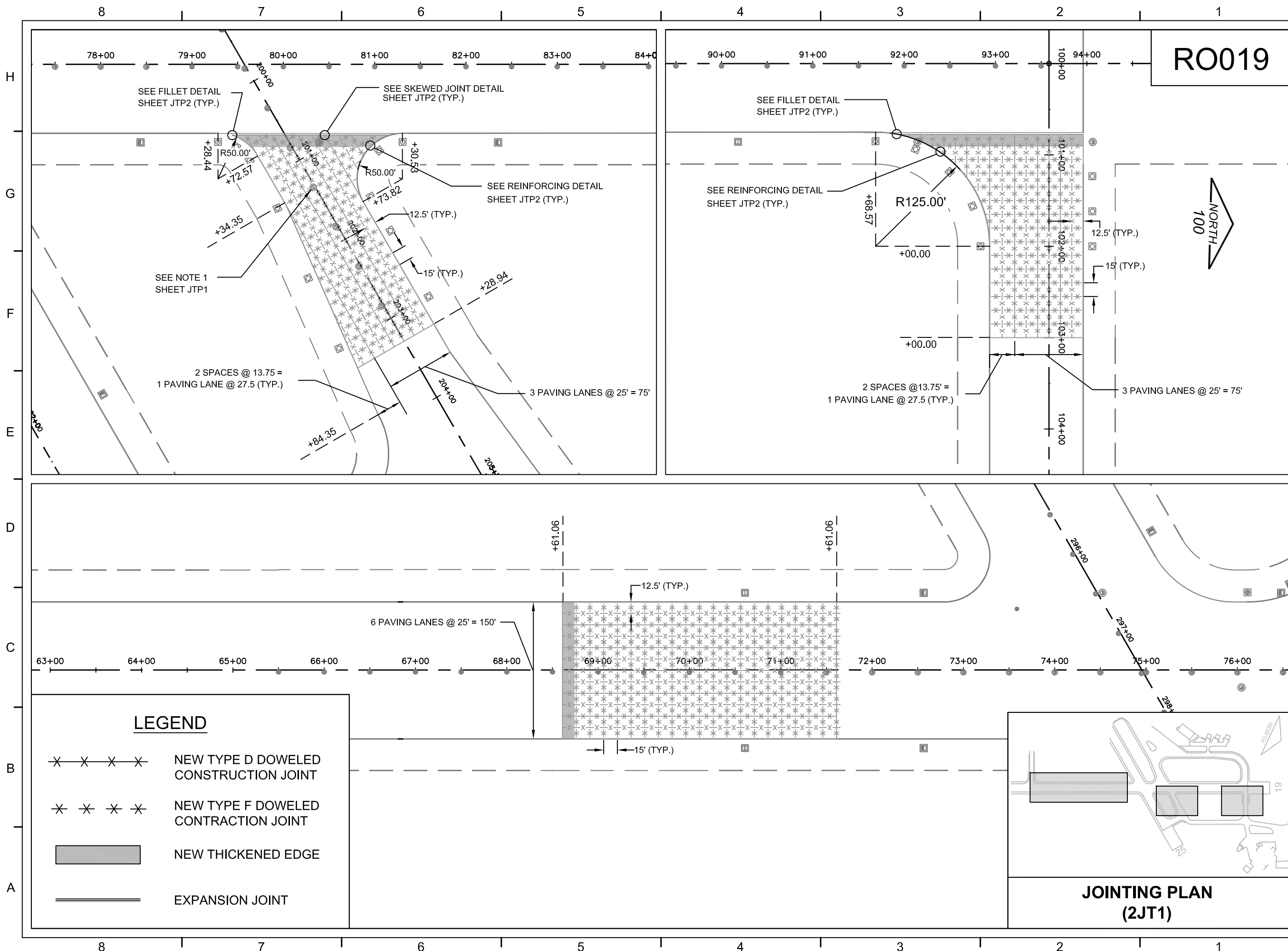
JOINTING PLAN
(2JT1)

DESIGNED: CMT-RFD DRAWN: CMT-RFD CHECKED: CMT-ARR

PROJECT NO.: 10258-03-00/06
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LEGEND

- NEW TYPE D DOWELED CONSTRUCTION JOINT
- NEW TYPE F DOWELED CONSTRUCTION JOINT
- NEW THICKENED EDGE
- EXPANSION JOINT