

06-17-2022 LETTING ITEM 143

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**PROPOSED  
HIGHWAY PLANS**

FAS 933 (UNIONVILLE RD)  
SECTION 19-00100-00-BR  
PROJECT HQRA(195)  
BRIDGE REPLACEMENT  
MASSAC COUNTY

C-99-031-20

F.A.S. R.F.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
933	19-00100-00-BR	MASSAC	11	1
COUNTY UNIT ROAD DISTRICT		ILLINOIS	CONTRACT NO. 99685	

**INDEX OF SHEETS**

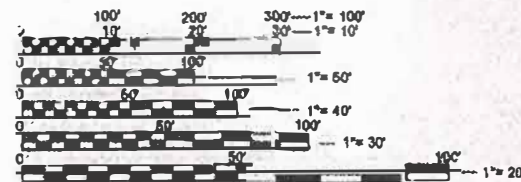
- 1 COVER SHEET
- 2 GENERAL NOTES, SUMMARY OF QUANTITIES
- 3 PLAN AND PROFILE
- 4-10 STRUCTURE PLANS
- 11 CROSS SECTIONS

**HIGHWAY STANDARDS**

- Q00001-08 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 515001-04 NAME PLATES FOR BRIDGES
- 630001-12 STEEL PLATE BEAM GUARDRAIL
- 701901-08 TRAFFIC CONTROL DEVICES
- 725001-01 OBJECT AND TERMINAL MARKERS
- 780001-05 TYPICAL PAVEMENT MARKINGS
- BLR 21-9 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL LOCAL HIGHWAYS
- BLR 22-7 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL LOCAL HIGHWAYS

**UTILITIES**

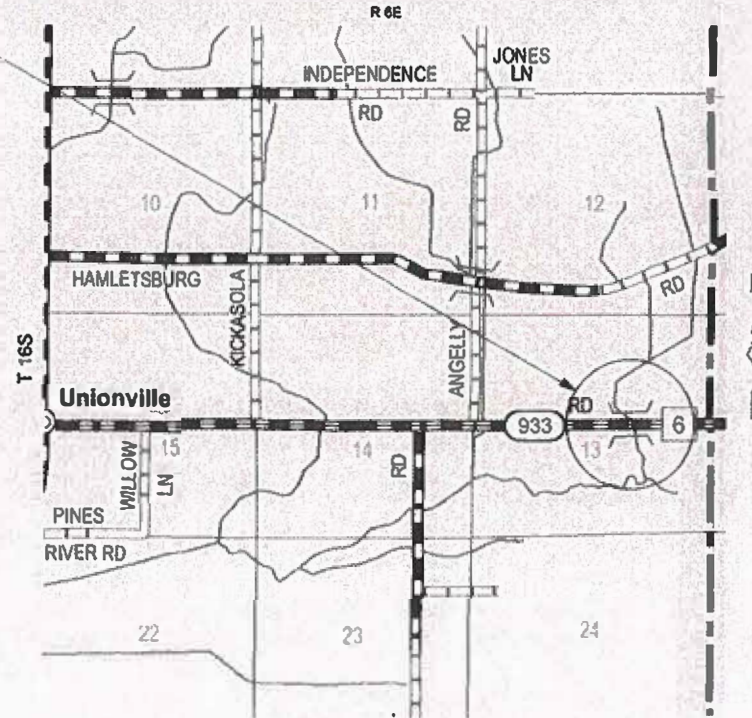
- ELECTRIC: SOUTHERN ILLINOIS ELECTRIC COOPERATIVE
- WATER: FORT MASSAC WATER DISTRICT
- TELEPHONE: FRONTIER



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROPOSED SECTION 19-00100-00-BR BEGINS STATION 48+25.00 ENDS STATION 51+74.00 PROPOSED SN 064-3153 EXISTING SN 064-3006



LOCATION MAP

PROJECT LENGTH= 349FT (0.07 MI)

THE PROPOSED IMPROVEMENT CONSISTS OF REMOVAL OF EXISTING BRIDGE DECK AND REPLACEMENT WITH A PPC DECK BEAM BRIDGE, 47'-7" CLEAR SPAN, 80° SKEW, AND OTHER MINOR COLLATERAL WORK.



FUNCTIONAL CLASSIFICATION - MAJOR COLLECTOR (RURAL)  
2018 ADT = 375  
DESIGN SPEED = 65 MPH

APPROVED *March 29* 20 22  
*Joseph G. Mattern*  
COU NTY ENGINEER

PASSED *3/29* 20 22  
*[Signature]*  
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

Releasing For Bid Based on Limited Review *3/29* 20 22  
*[Signature]*  
ACTING DEPUTY DIRECTOR OF HIGHWAYS  
REGION FIVE ENGINEER

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

HLC PROJECT NO. 7545  
HLC FIELD BOOK NO.  
CONTRACT NO. 99685

*[Signature]*  
BRIAN GILTNER, P.E., S.E. DATE  
LICENSE EXPIRES 11/30/2023

DESIGNED	BCG	REVISED	-
DRAWN	DMG	REVISED	-
CHECKED		REVISED	-
DATE	1-16-2022	REVISED	-



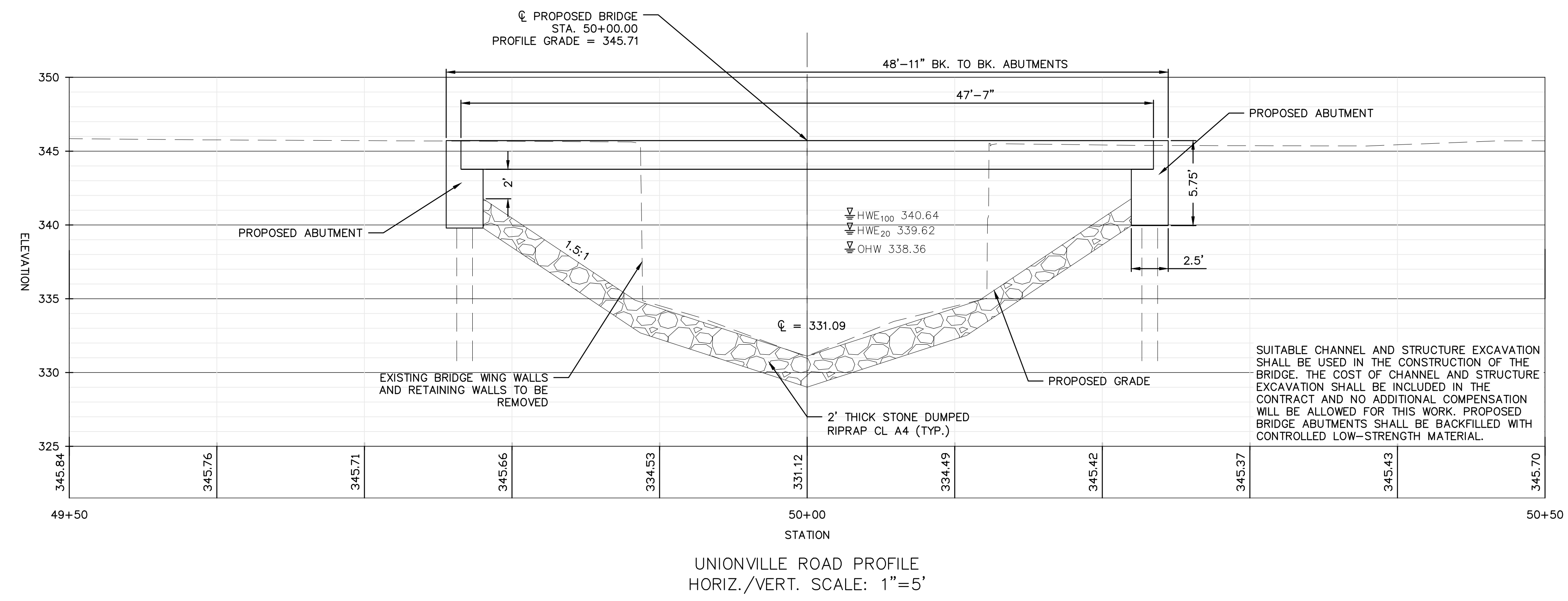
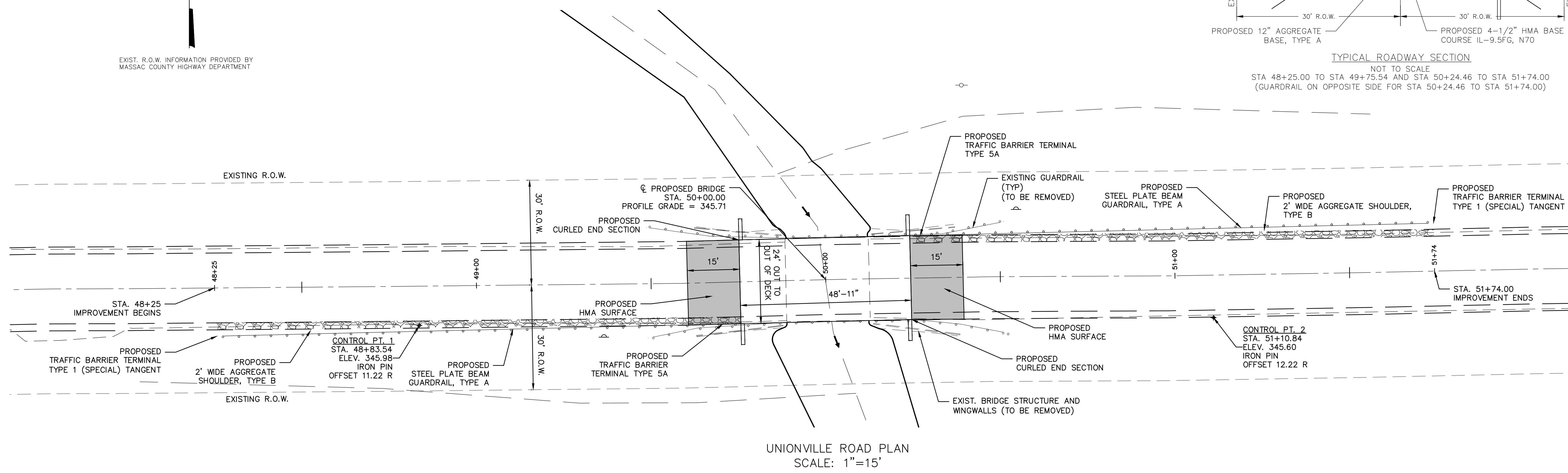
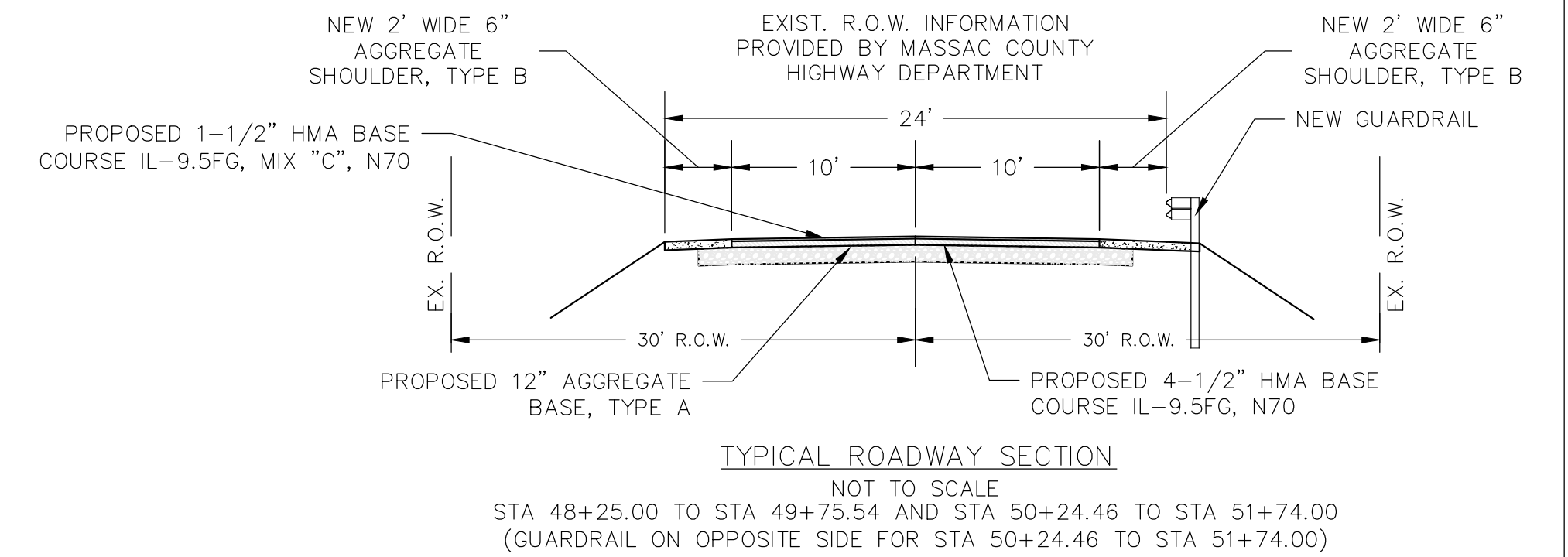
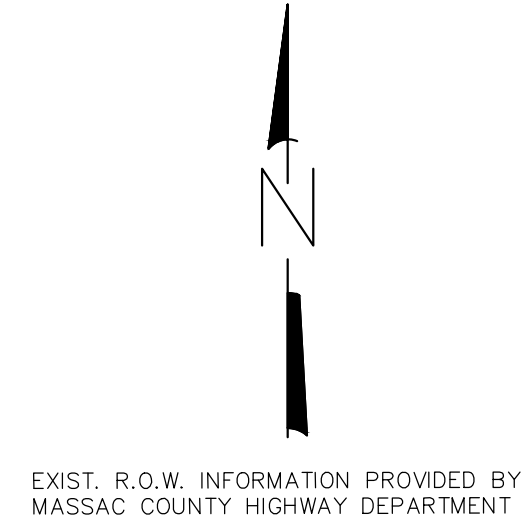
DECATUR (317) 424-8544  
SCHAUMBURG (773) 714-0050  
ROCKFORD (815) 489-0050  
BENTON (618) 435-4400  
PADUCAH (270) 558-5983  
184-001397

COVER SHEET  
UNIONVILLE ROAD OVER MUD CREEK  
SHEET NO. 1 OF 11 SHEETS STA 48+25 TO STA. 51+74

**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS  
DECATUR, IL BENTON, IL  
SCHAUMBURG, IL CHAMPAIGN, IL  
ROCKFORD, IL LARAVETTE, IN  
PADUCAH, KY  
(815) 424-2782  
184-001397

F.A.S. R.F.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
933	19-00100-00-BR	MASSAC	11	1
COUNTY UNIT ROAD DISTRICT		ILLINOIS	CONTRACT NO. 99685	





DESIGNED	BCG	REVISED	-
DRAWN	DMG	REVISED	-
CHECKED		REVISED	-
DATE	1-16-2022	REVISED	-



DECATUR (217) 422-8544  
 SCHAUMBURG (773) 714-0050  
 ROCKFORD (815) 489-0050  
 BENTON (618) 435-4400  
 PADUCAH (270) 558-5983  
 184-001397

PLAN & PROFILE  
 UNIONVILLE ROAD OVER MUD CREEK

SHEET NO. 3 OF 11 SHEETS STA. 48+25 TO STA. 51+74

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
933	19-00100-00-BR	MASSAC	11	3
ILLINOIS			FED. AID PROJECT HQRA(195)	
CONTRACT NO. 99685				

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS

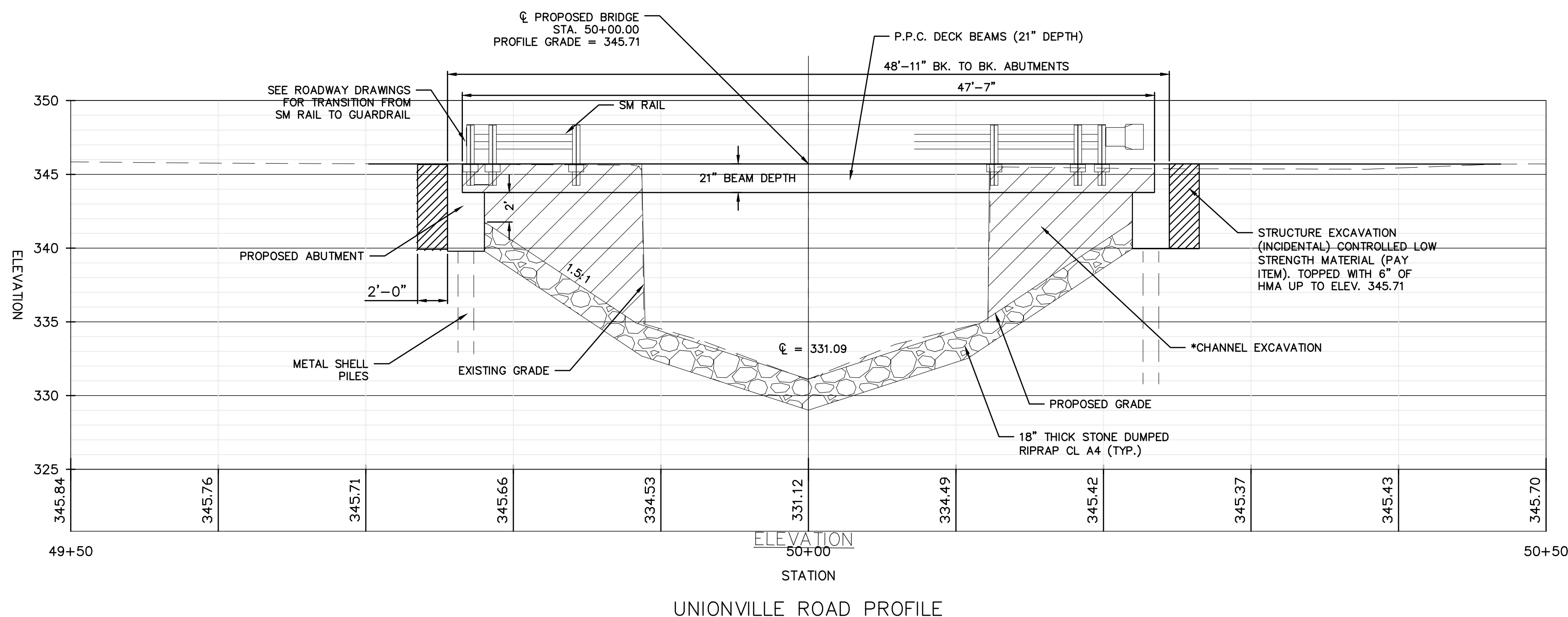
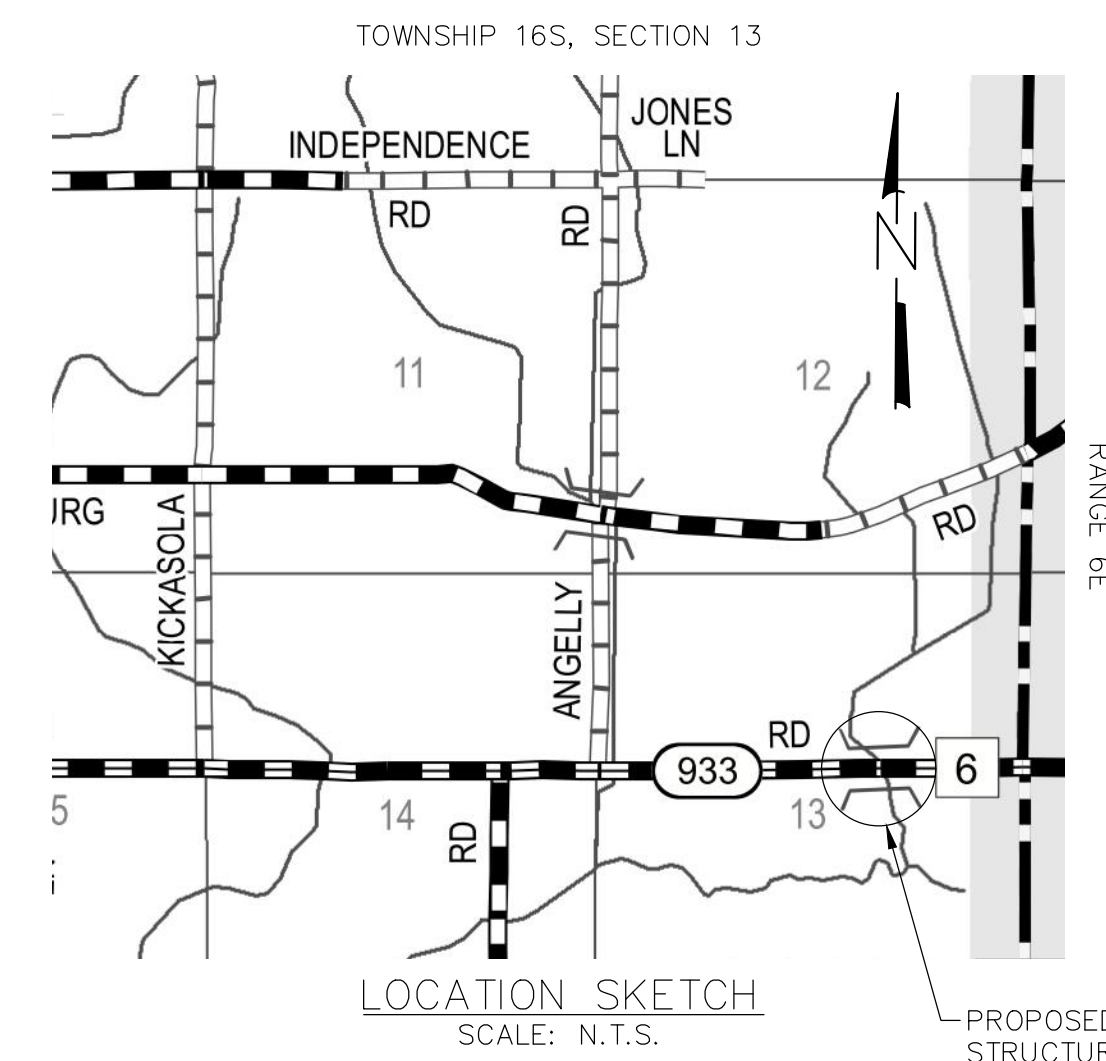
- 4 General Plan and Elevation
- 5 Steel Railing, Type SM
- 6 Superstructure Plan and Cross Section
- 7 21"x48" PPC Deck Beam
- 8 21"x48" PPC Deck Beam Details
- 9 Abutment Details
- 10 Metal Shell Pile Details

GENERAL NOTES:

1. Layout of slope protection system may be varied in the field to suit ground conditions as directed by the Engineer.
2. All Construction joints shall be bonded.
3. See Roadway Plans for channel excavation limits and quantities.
4. See Roadway Plans for additional riprap limits and quantities.
5. The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.
6. See special provisions for boring logs.
7. Layout of rip-rap may be varied in the field to suit ground conditions as directed by the County Engineer.

Existing Structure:  
The existing structure is a 25'-0" single span concrete channel beam bridge. Road to be closed during construction.  
No salvage value.

\*All excavation for the new structure, back to back of abutments as shown, ROW to ROW will not be paid for separately, and the cost of excavation, hauling excess material, and disposal of excess material, shall be included in the cost of Removal of Existing Structures. No additional compensation will be allowed.  
Estimated quantity = 254 Cu Yd



LOADING HL-93

Allow 50 lb/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS  
2018 AASHTO LRFD Bridge Design Specifications  
8th Edition with 2018 Interims

DESIGN STRESSES

FIELD UNITS

$f_c = 3,500$  psi  
 $f_y = 60,000$  psi (reinforcement)

PRECAST PRESTRESSED UNITS

$f_c = 6,000$  psi  
 $f_{ci} = 5,000$  psi  
 $f_s = 270,000$  psi ( $\frac{1}{2}\%$  low-relaxation strands)  
 $f_{si} = 202,000$  psi ( $\frac{1}{2}\%$  low-relaxation strands)

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1  
Design Spectral Acceleration at 1.0 sec ( $S_{D1}$ ) = 0.326g  
Design Spectral Acceleration at 0.2 sec ( $S_{D5}$ ) = 0.609g  
Soil Site Class = D

TOTAL BILL OF MATERIAL

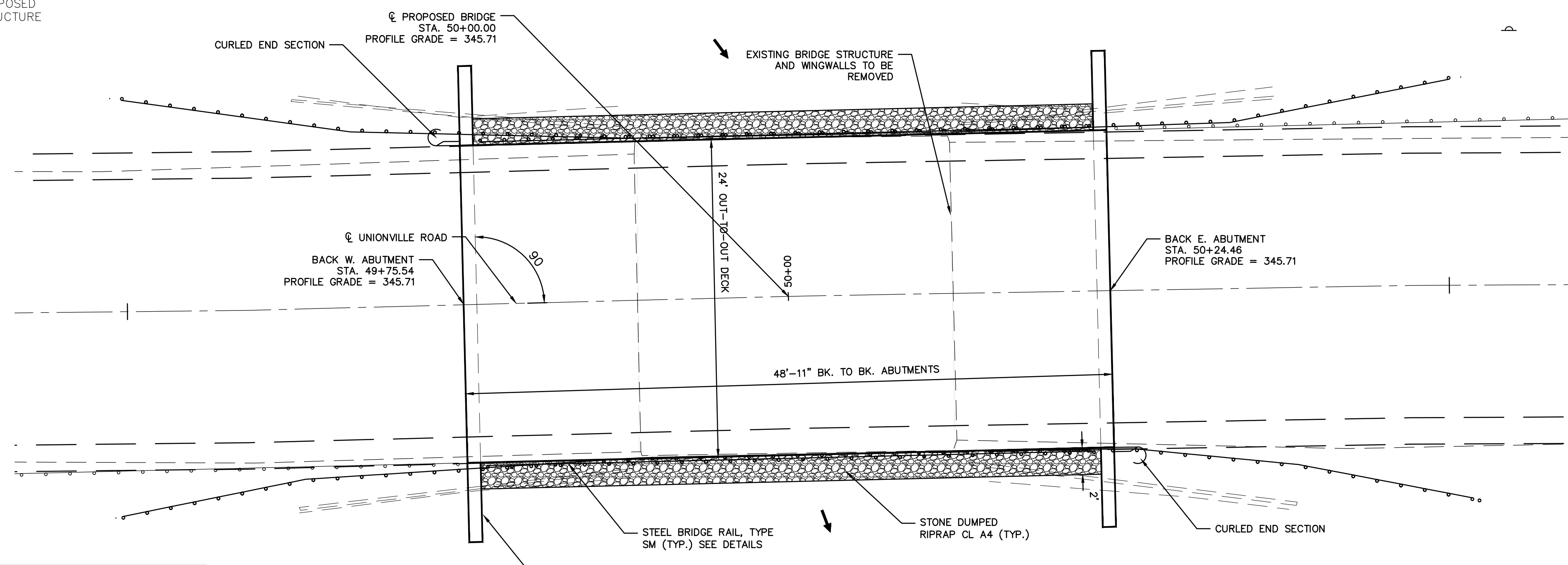
Item	Unit	Quantity
Removal of Existing Structures	Each	1
Concrete Structures	Cu Yd	25.2
Precast Prestressed Concrete Deck Beams (21" Depth)	Sq Ft	1142
Reinforcement Bars, Epoxy Coated	Pound	3330
Steel Railing, Type SM	Foot	98
Furnishing Metal Shell Piles, MS12x0.250	Foot	272
Driving Piles	Foot	272
Name Plates	Each	1
Membrane Waterproofing System	Sq Yd	142
Controlled Low-Strength Material	Cu Yd	20.0

BENCH MARK:  
CONTROL PT. 1 - IRON PIN 11.22  
RIGHT OF STATION 48+83.54  
ELEV. 345.98  
CONTROL PT. 2 - IRON PIN 12.22  
RIGHT OF STATION 51+10.84  
ELEV. 345.60

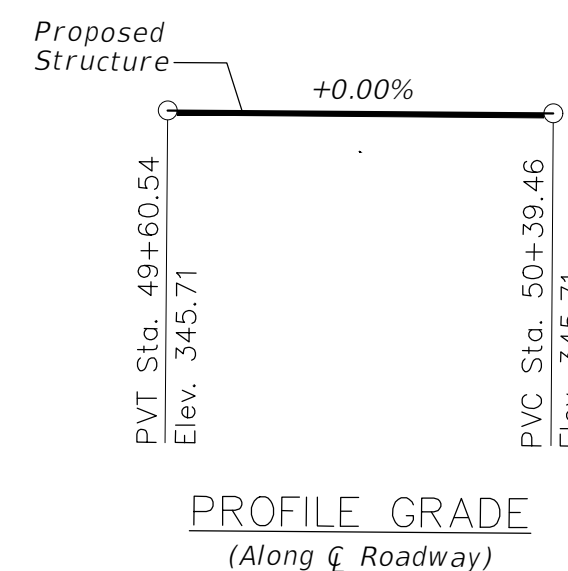
WATERWAY INFORMATION

Drainage Area = 1.48 sq.mi. Low Grade Elev. = 345.70 @ Sta. 50+00.00

FLOOD	FREQ. (YR.)	Q (C.F.S.)	OPENING (SQ.FT.)		NAT. H.W.E.	HEAD (FT.)		HEADWATER ELEV.	
			EXIST.	PROP.		EXIST.	PROP.	EXIST.	PROP.
Design	20	1075	242.00	368.00	340.00	0.00	-0.01	339.63	339.62
Base	100	1540	242.00	368.00	341.00	0.00	-0.18	340.82	340.64
Overtopping									
Max. Calc	500	2070	242.00	368.00	342.00	0.00	-0.54	342.18	341.64



PLAN



PROFILE GRADE  
(Along Roadway)

STRUCTURE NO. 064-3153  
STATION 50+00.00  
MUD CREEK  
SEC. 19-00100-00-BR - BUILT 20XX  
MASSAC COUNTY  
LOADING HL-93

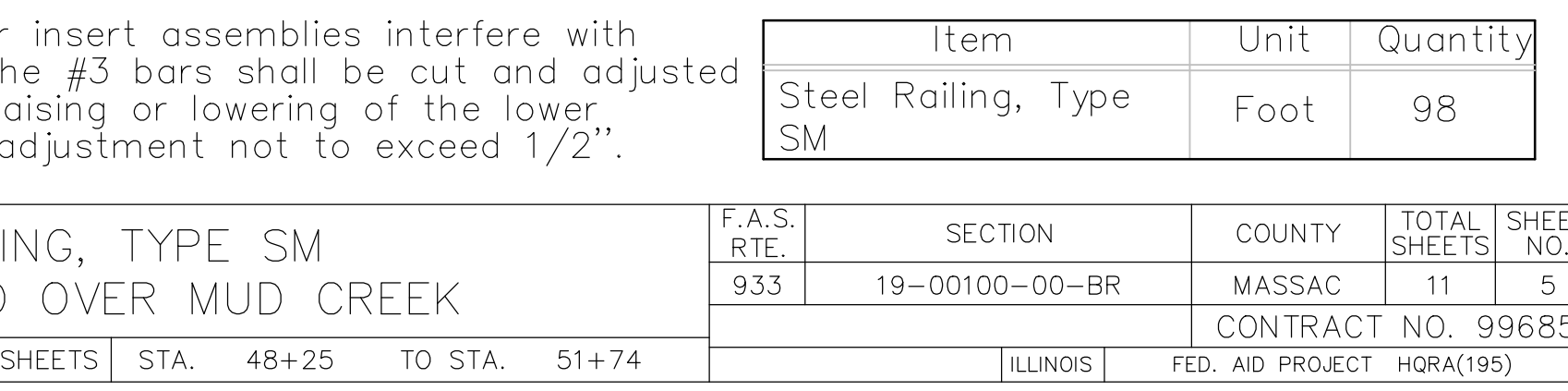
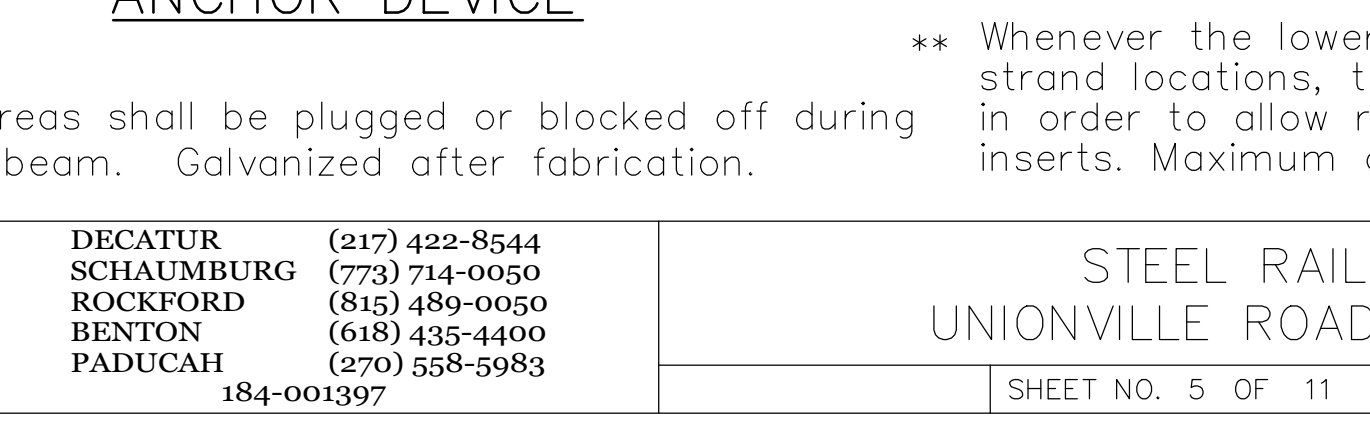
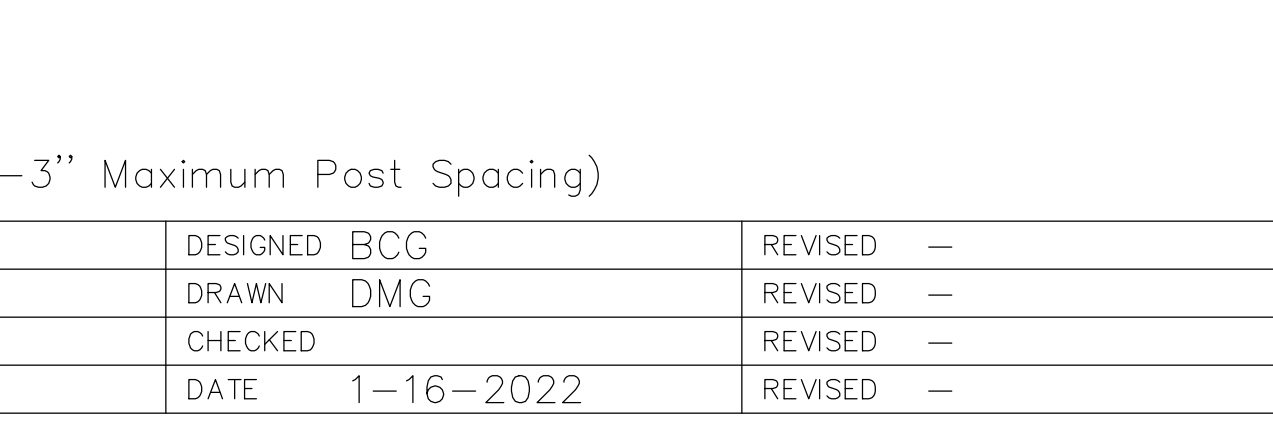
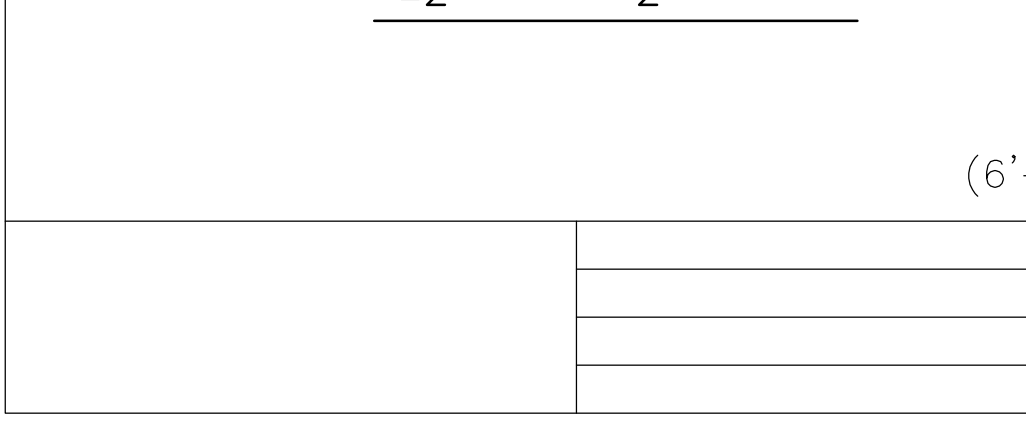
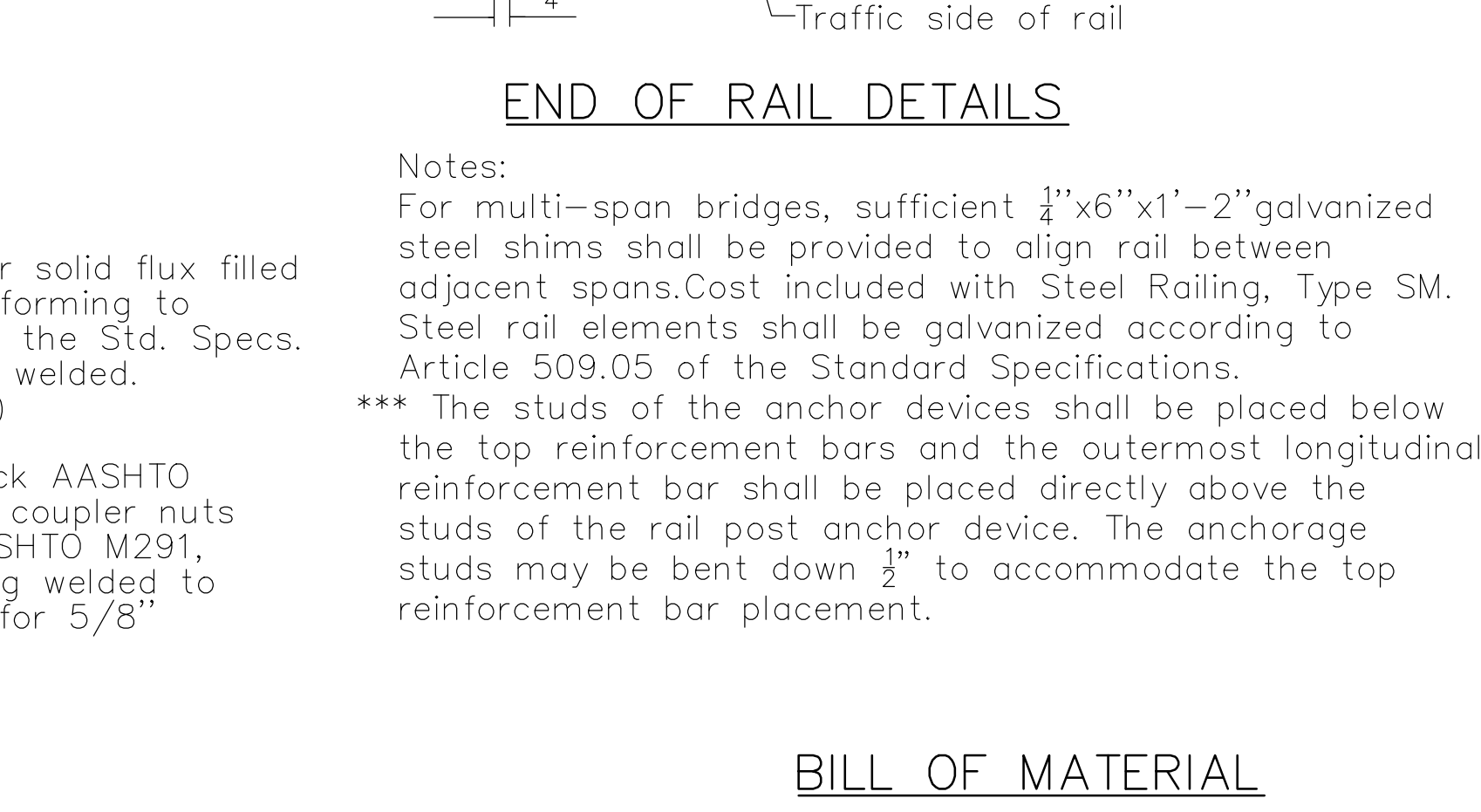
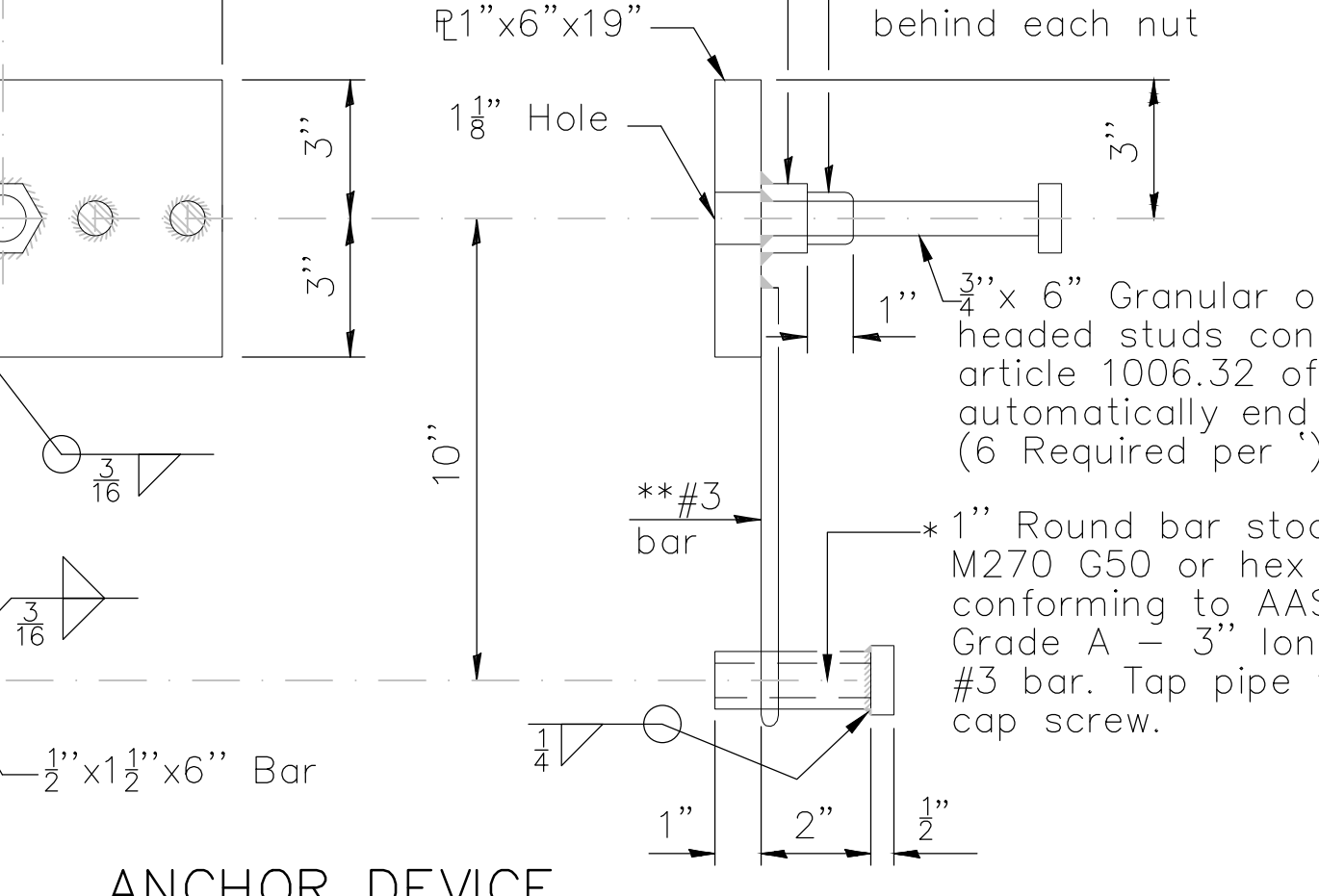
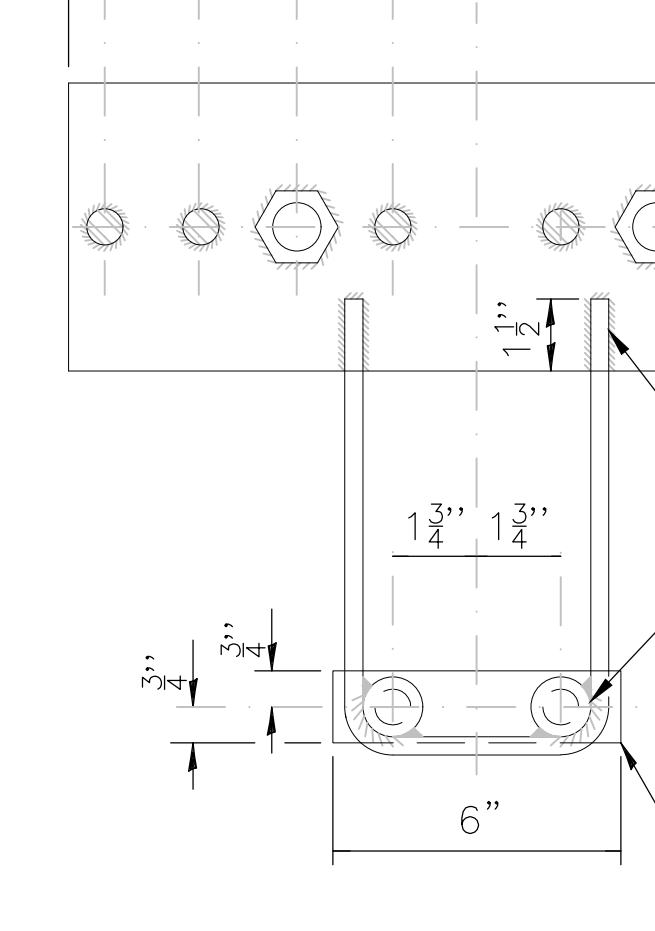
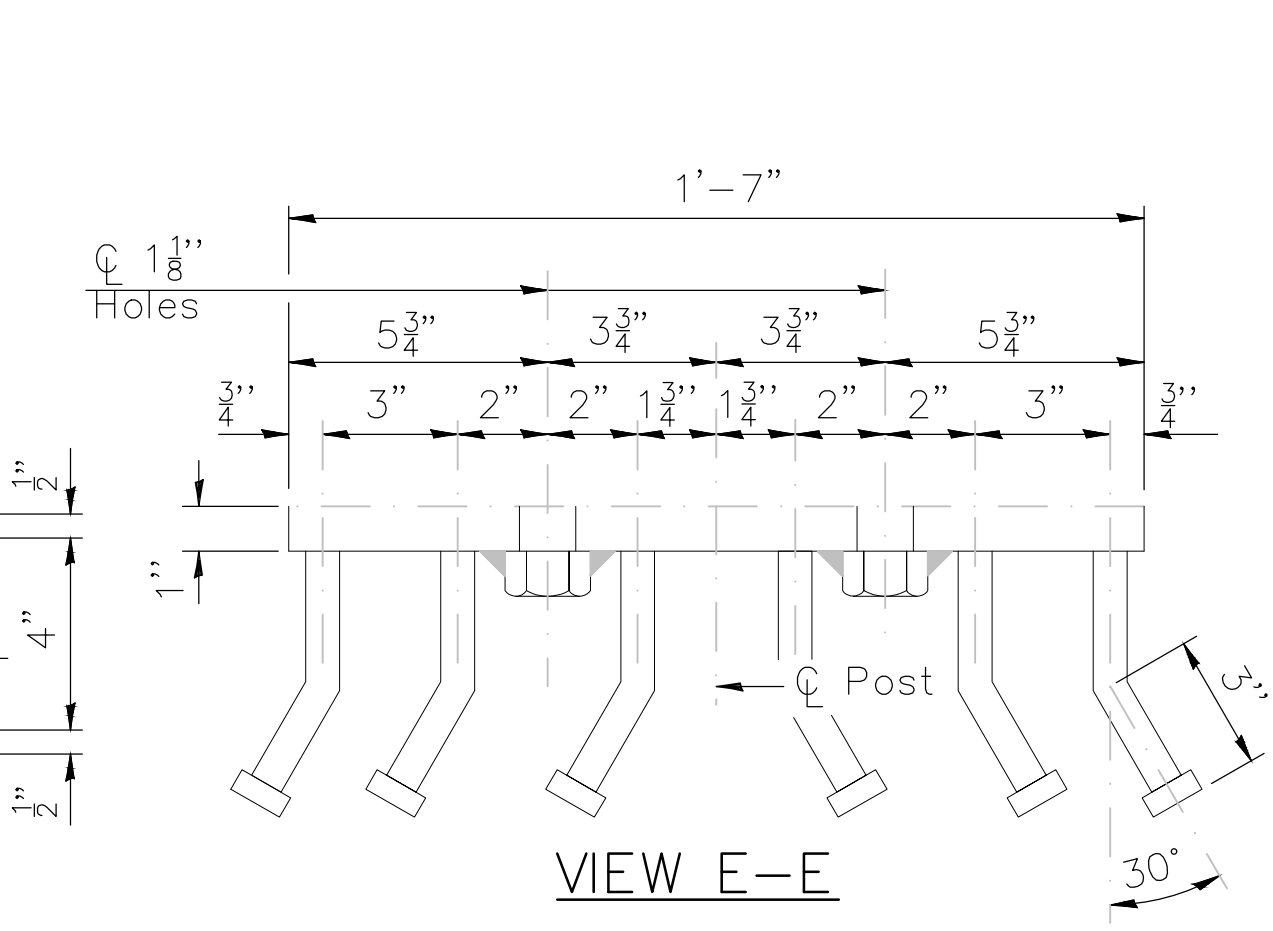
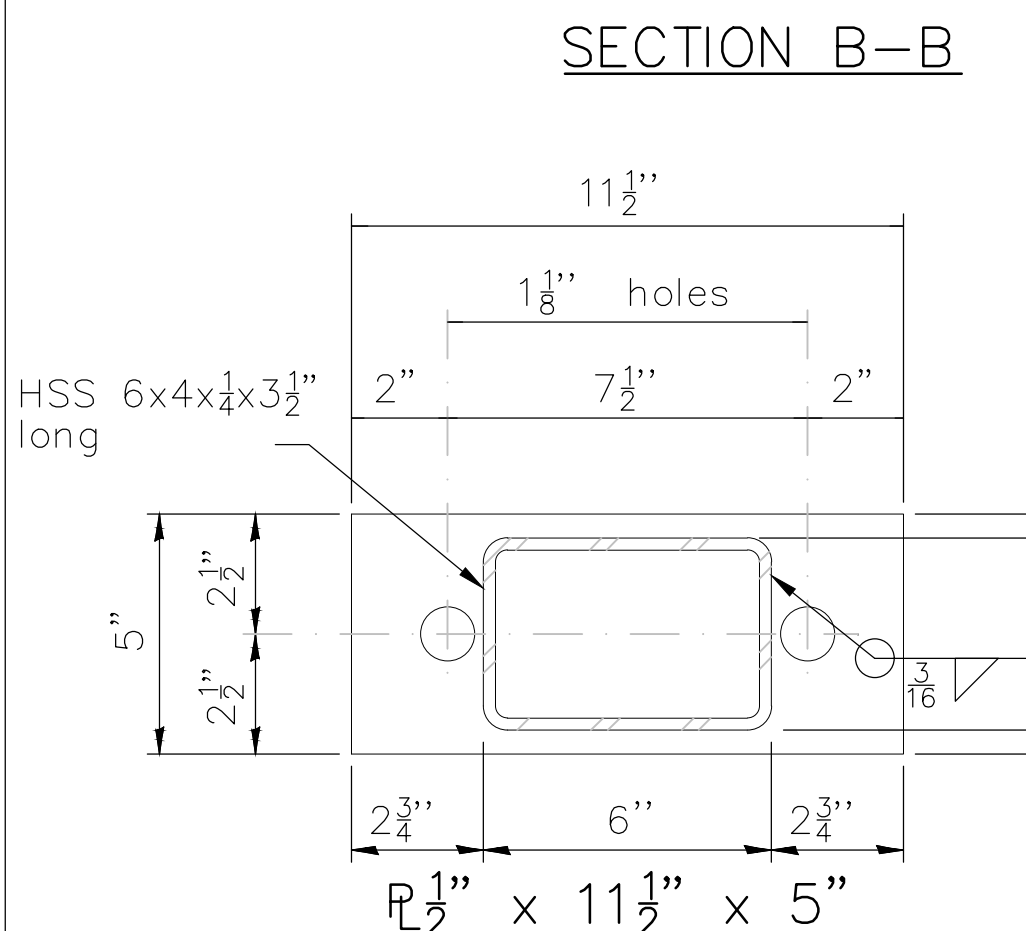
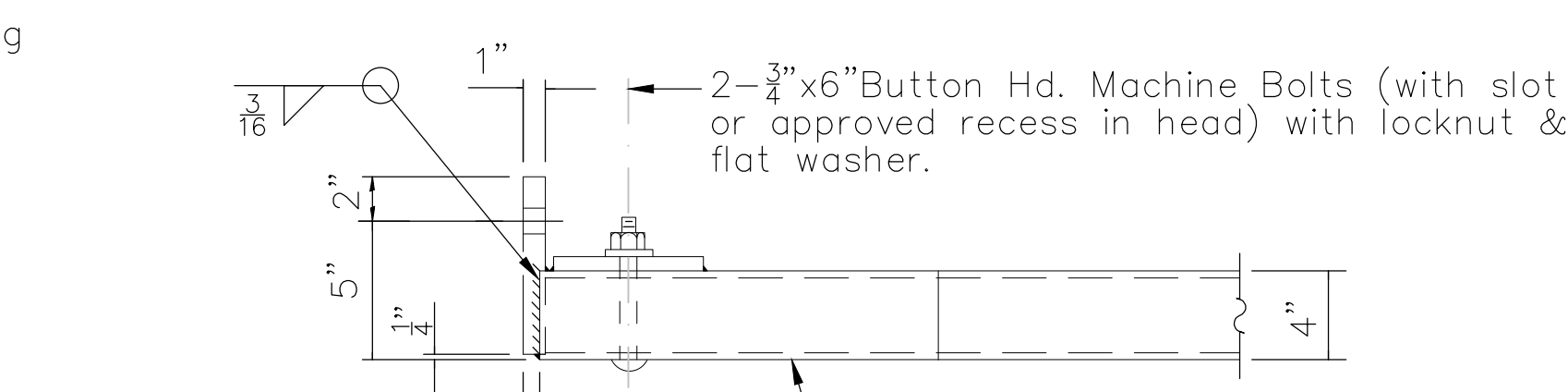
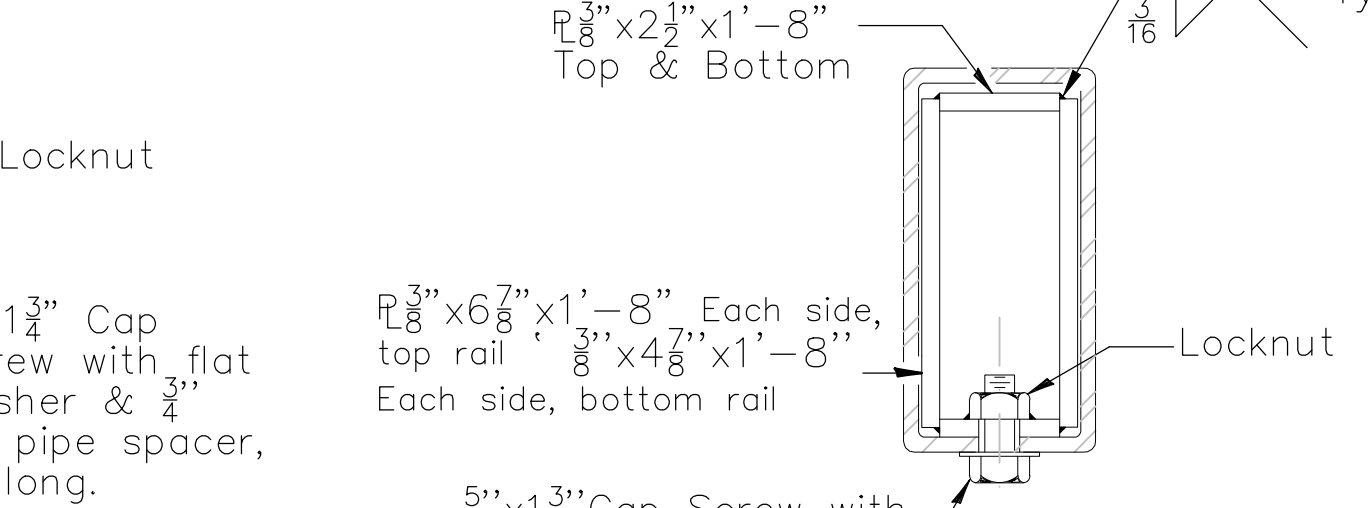
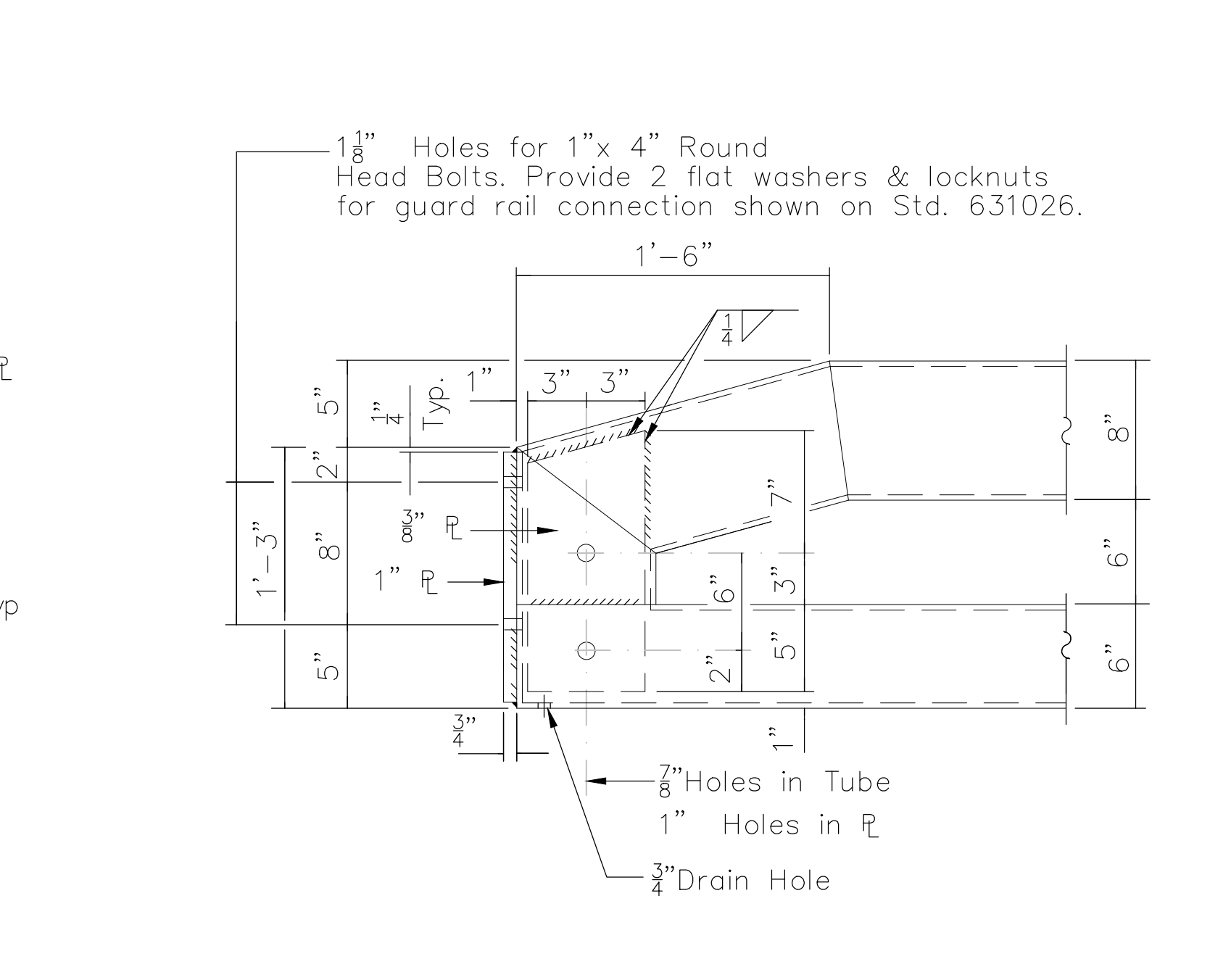
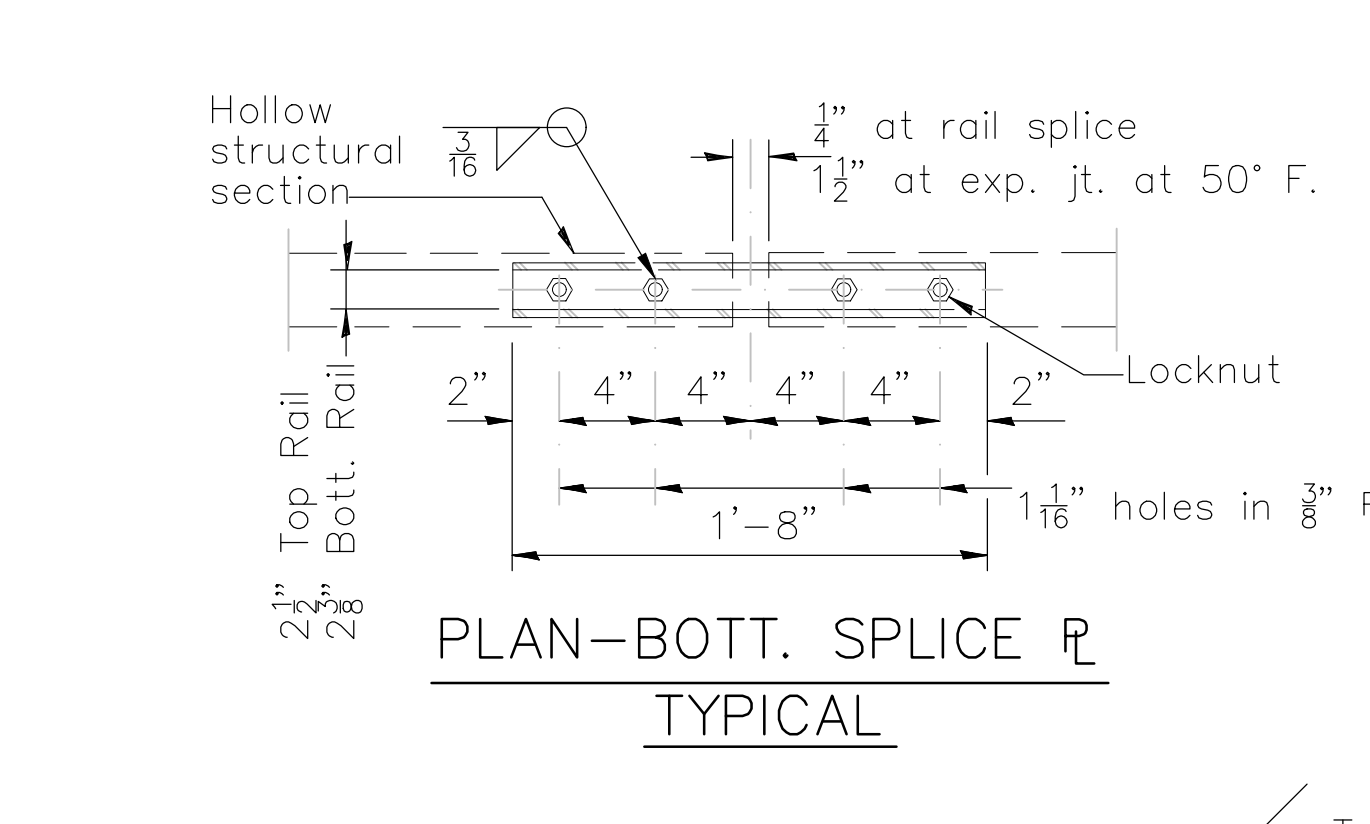
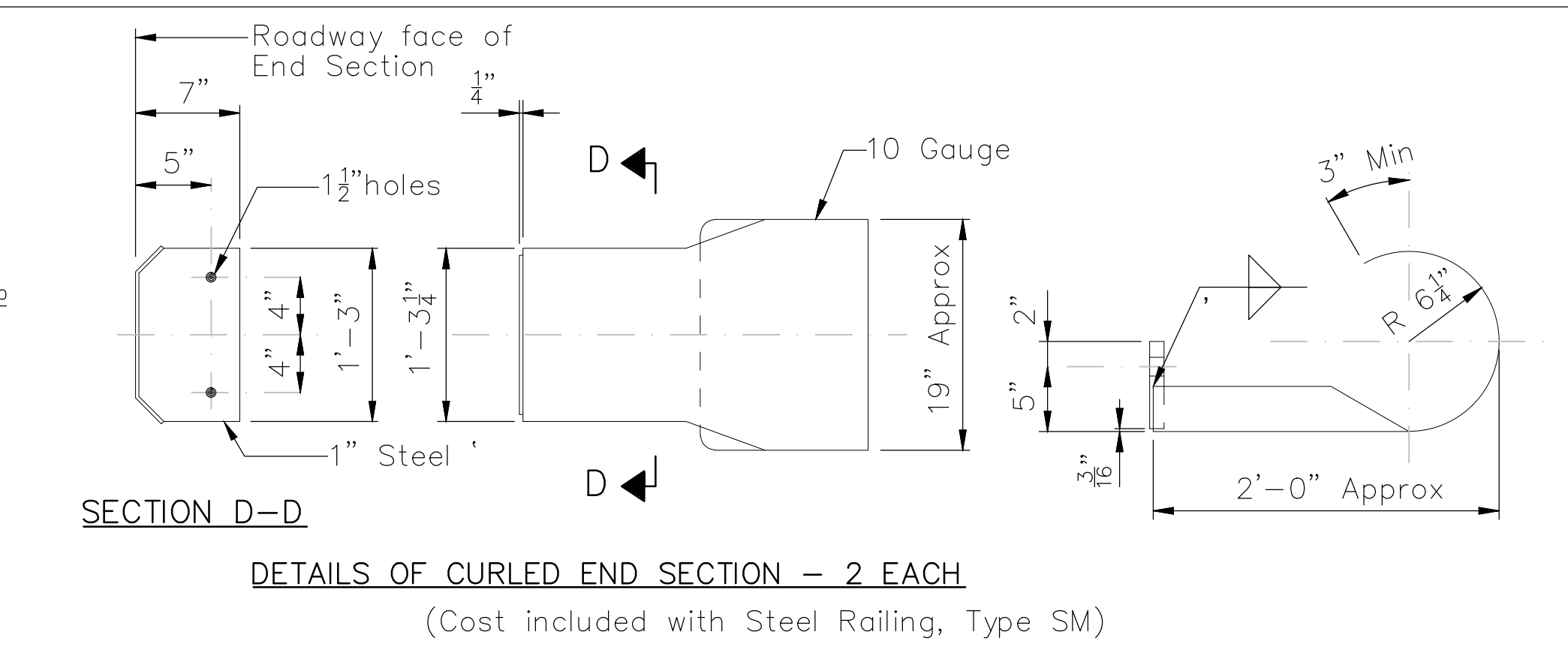
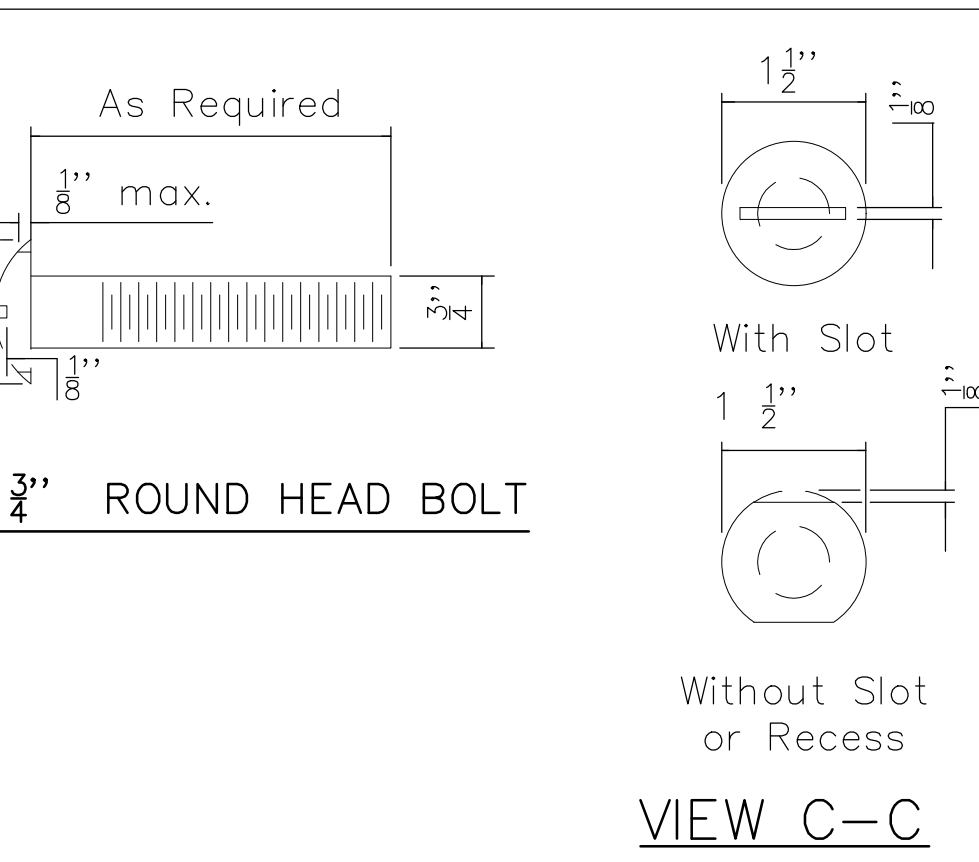
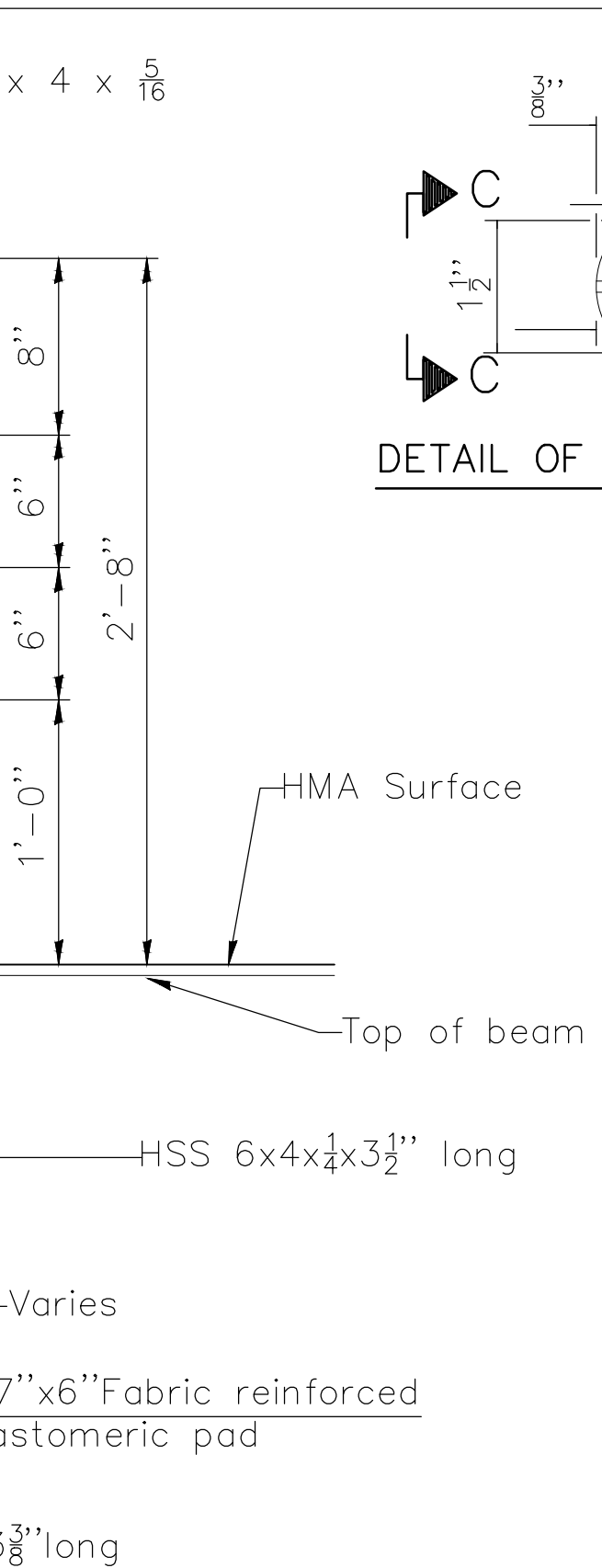
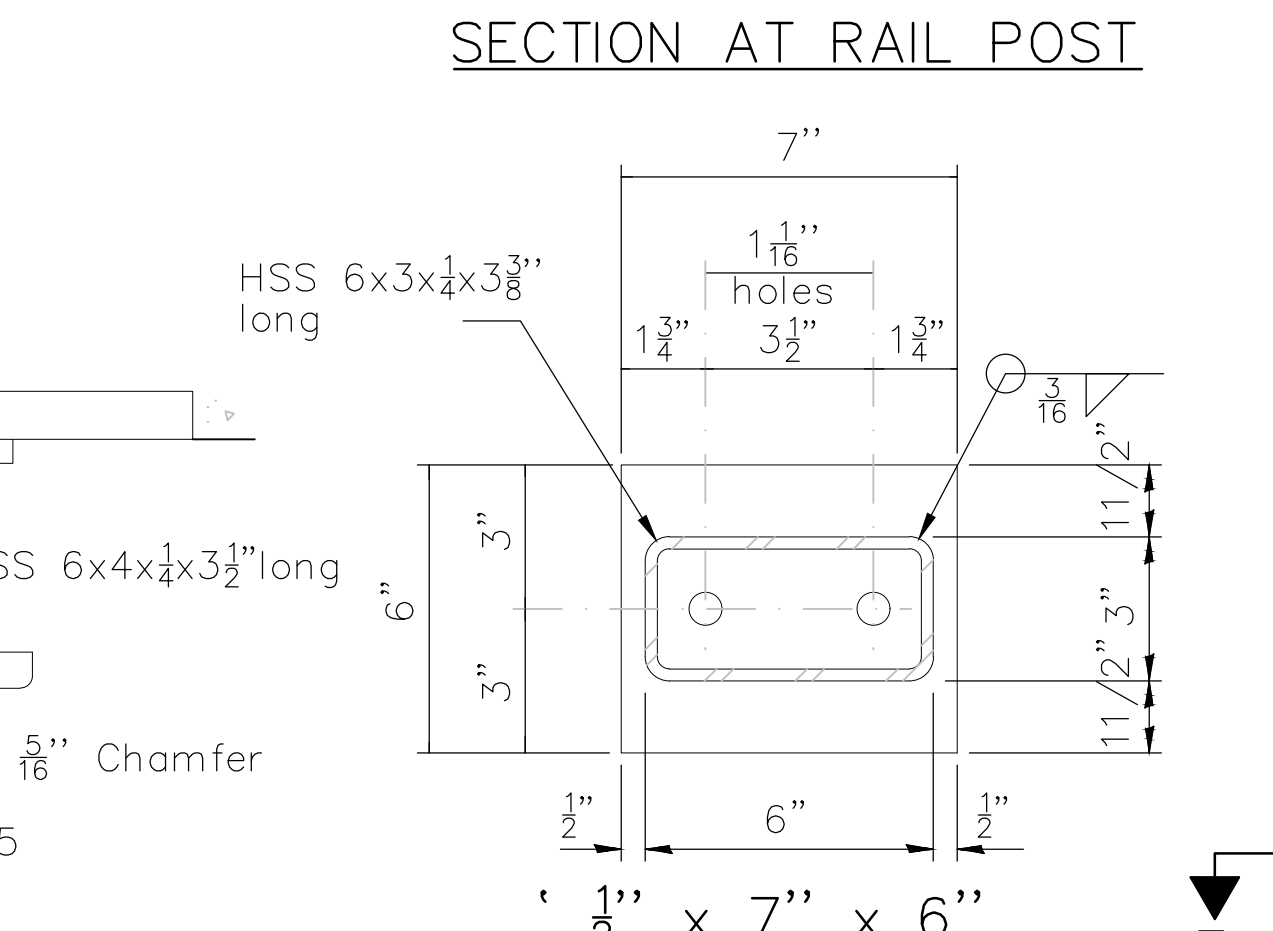
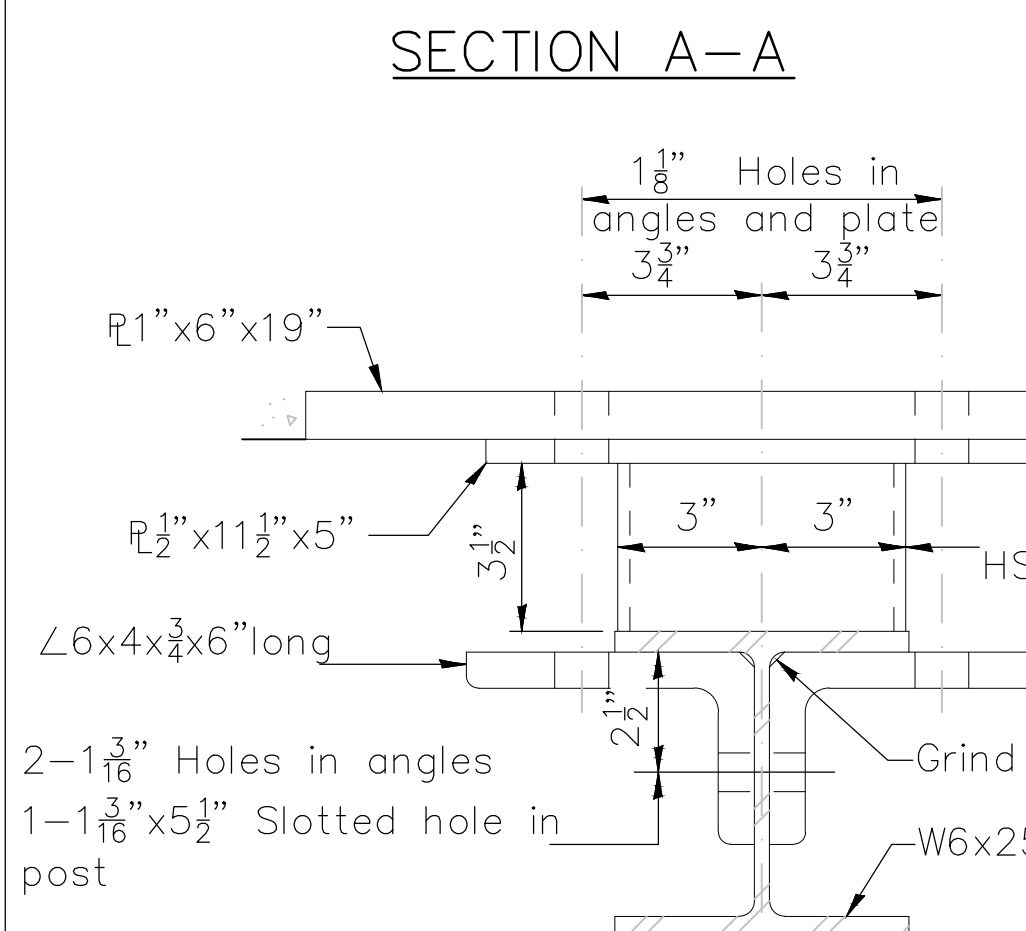
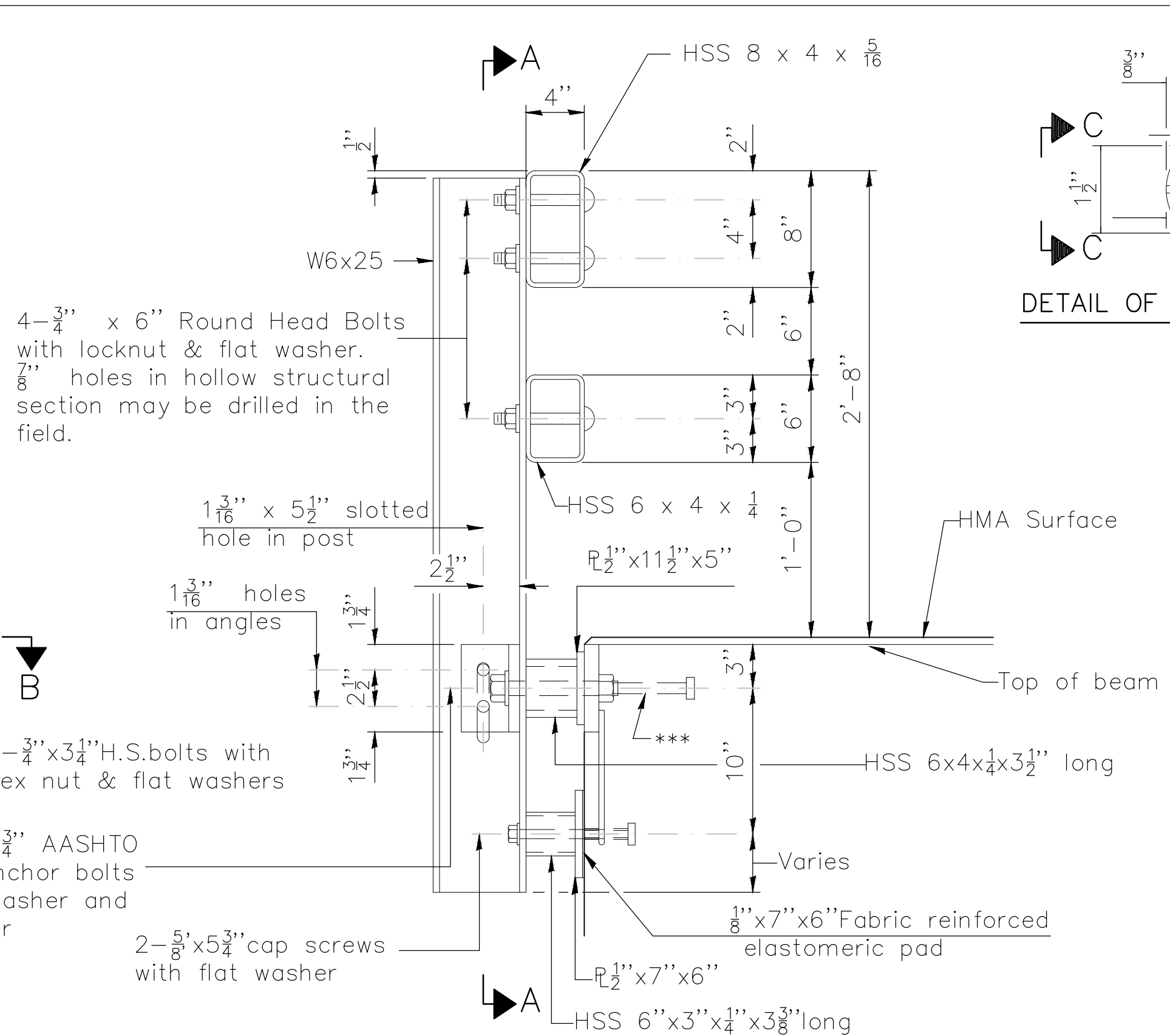
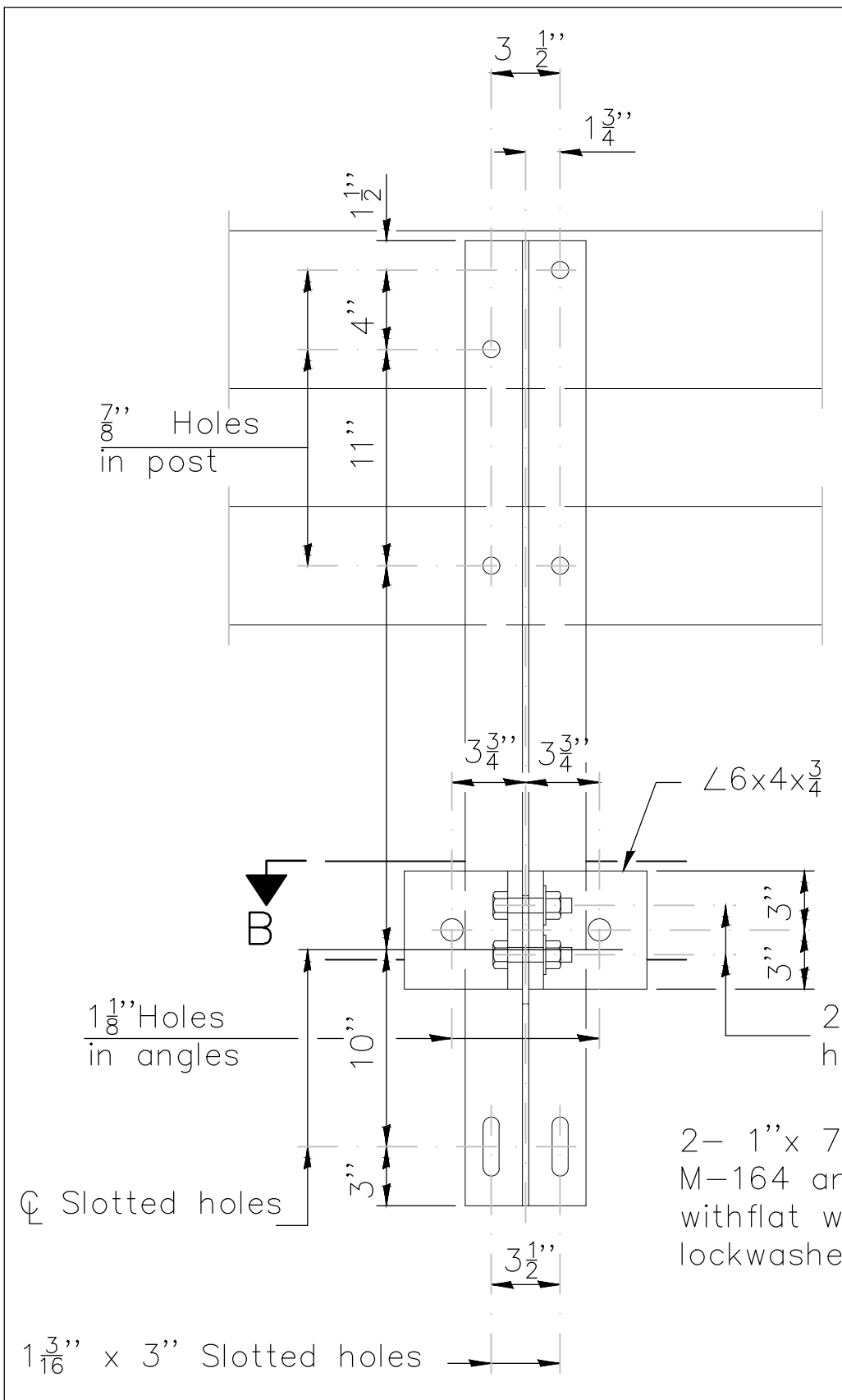
NAME PLATE  
SEE STD. 515001-03



I certify that to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with requirements of the current AASHTO Standard Specifications for Highway Bridges.

20-Year velocity through existing bridge = 8.18 ft/s  
20-Year velocity through proposed bridge = 6.25 ft/s

DESIGNED	BCG	REVISED	-		DECATUR	(217) 422-8544	PLAN & PROFILE UNIONVILLE ROAD OVER MUD CREEK SHEET NO. 4 OF 11 SHEETS STA. 48+25 TO STA. 51+74	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN	DMG	REVISED	-		ROCKFORD	(773) 714-0050		933	19-00100-00-BR	MASSAC	11	4
CHECKED		REVISED	-		BENTON	(815) 489-0050						
DATE	1-16-2022	REVISED	-		BENTON	(618) 435-4400						
					PADUCAH	(270) 558-5983						
						184-001397						



(6'-3" Maximum Post Spacing)

\*Threaded areas shall be plugged or blocked off during casting of beam. Galvanized after fabrication.

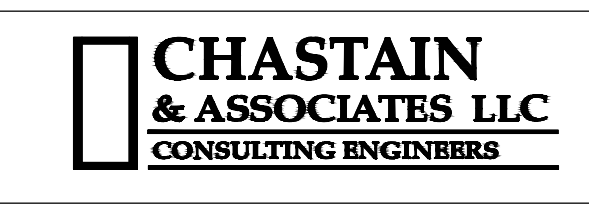
\*\* Whenever the lower insert assemblies interfere with strand locations, the #3 bars shall be cut and adjusted in order to allow raising or lowering of the lower inserts. Maximum adjustment not to exceed 1/2".

Notes:  
For multi-span bridges, sufficient 1/2"x6"x1'-2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Railing, Type SM. Steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.  
\*\*\* The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device. The anchorage studs may be bent down 1/2" to accommodate the top reinforcement bar placement.

BILL OF MATERIAL

Item	Unit	Quantity
Steel Railing, Type SM	Foot	98

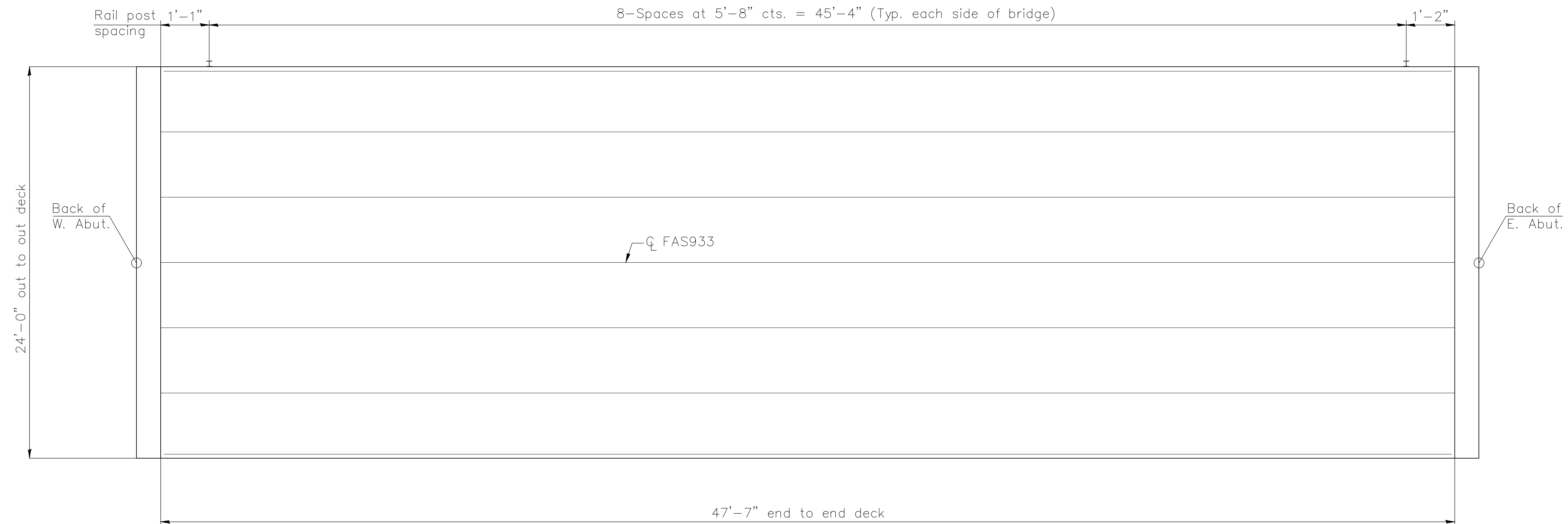
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DRAWN	DMG	REVISED	-
CHECKED		REVISED	-
DATE	1-16-2022	REVISED	-



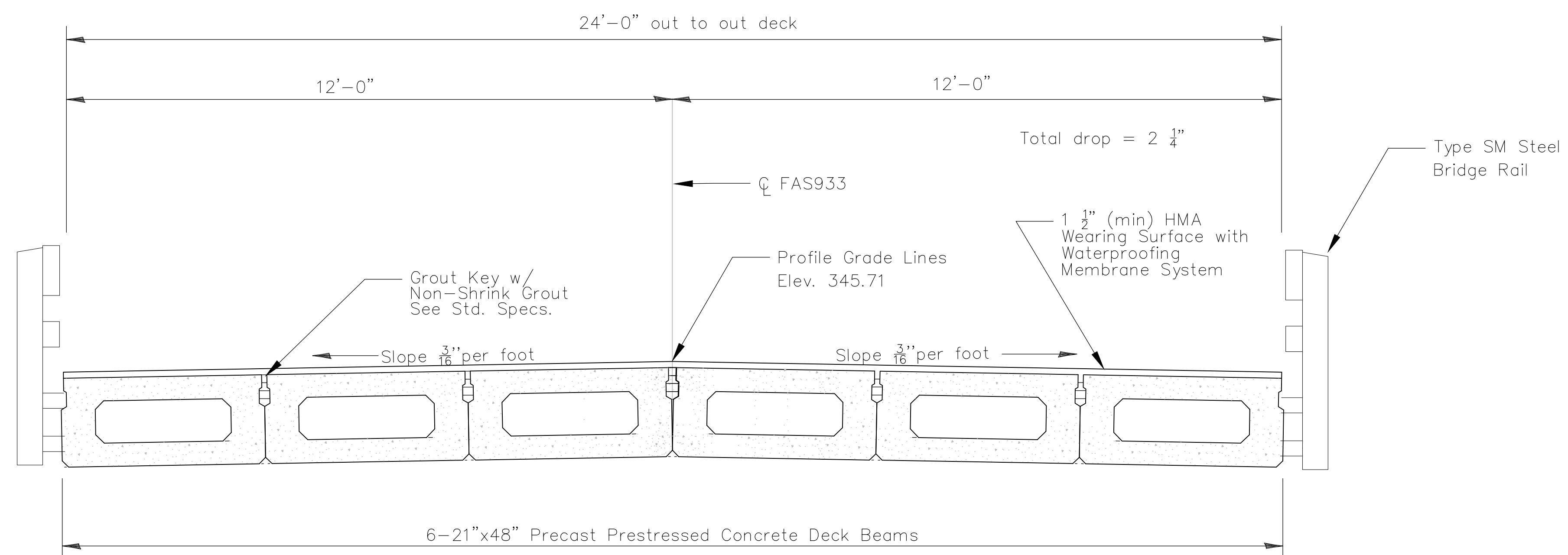
DECATUR (217) 422-8544  
SCHAUMBURG (773) 714-0050  
ROCKFORD (815) 489-0050  
BENTON (618) 435-4400  
PADUCAH (270) 558-5983  
184-001397

STEEL RAILING, TYPE SM  
UNIONVILLE ROAD OVER MUD CREEK  
SHEET NO. 5 OF 11 SHEETS STA. 48+25 TO STA. 51+74

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
933	19-00100-00-BR	MASSAC	11	5
CONTRACT NO. 99685			ILLINOIS FED. AID PROJECT HORA(195)	



PLAN



CROSS SECTION  
(Looking East)

Notes:  
See sheet 8 of 11 for Superstructure Details and Bill of Material.

DESIGNED	BCG	REVISED	—
DRAWN	DMG	REVISED	—
CHECKED		REVISED	—
DATE	1-16-2022	REVISED	—

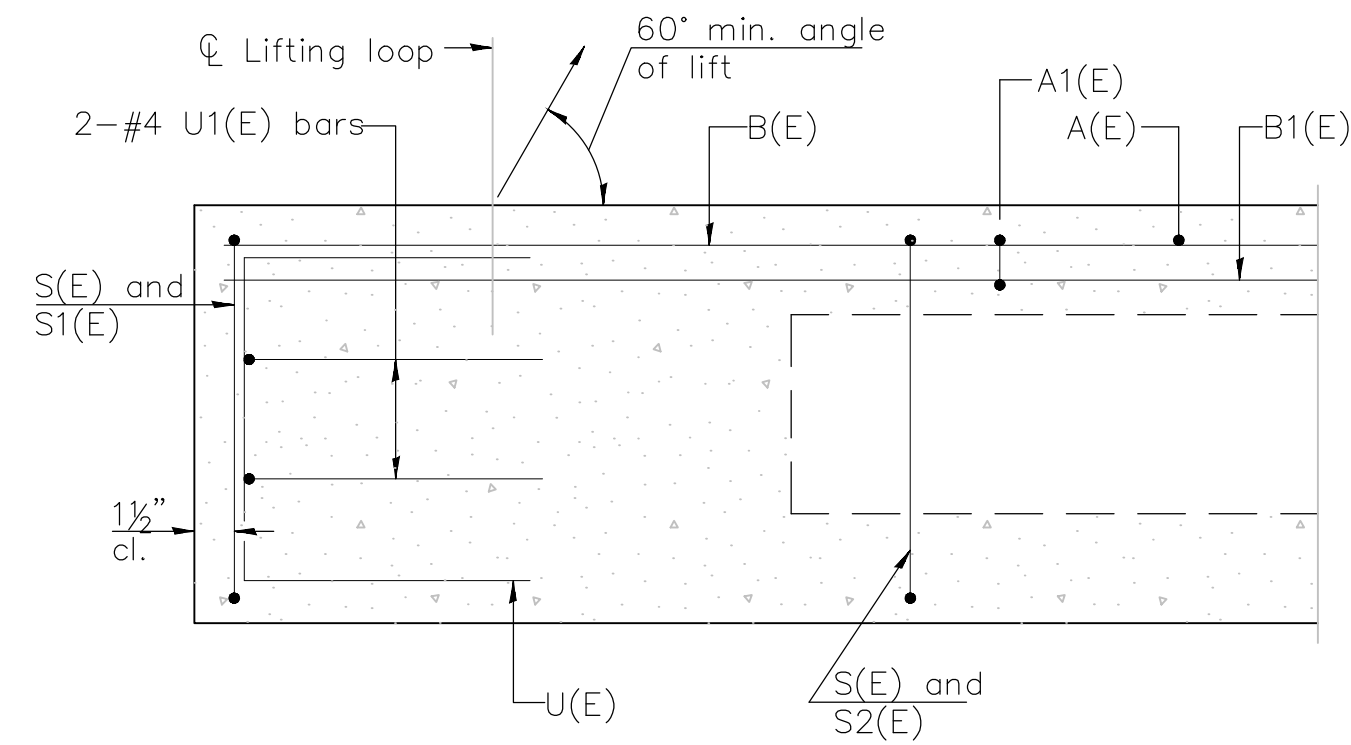
**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

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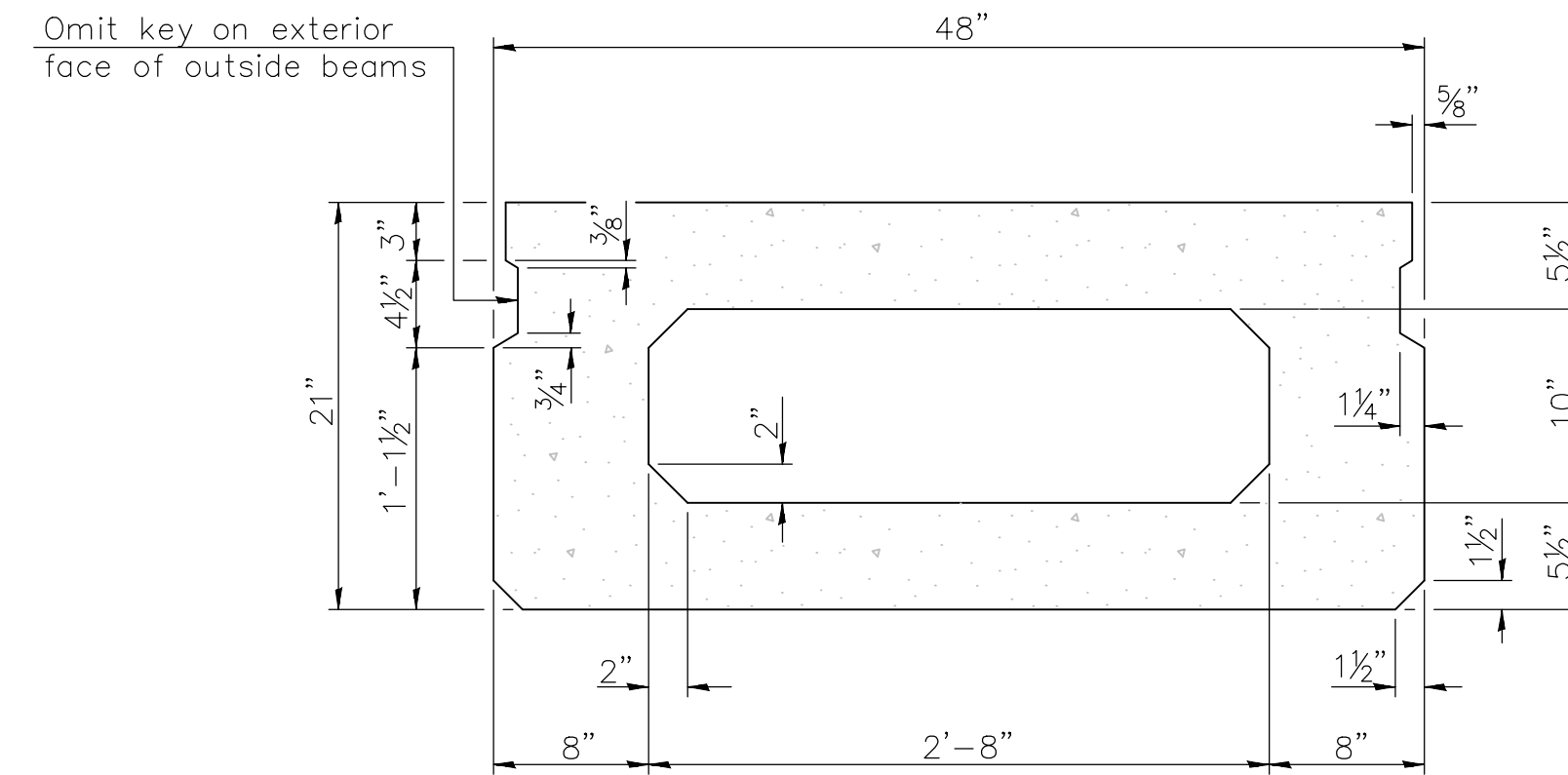
SUPERSTRUCTURE PLAN AND CROSS SECTION  
UNIONVILLE ROAD OVER MUD CREEK

SHEET NO. 6 OF 11 SHEETS STA. 48+25 TO STA. 51+74

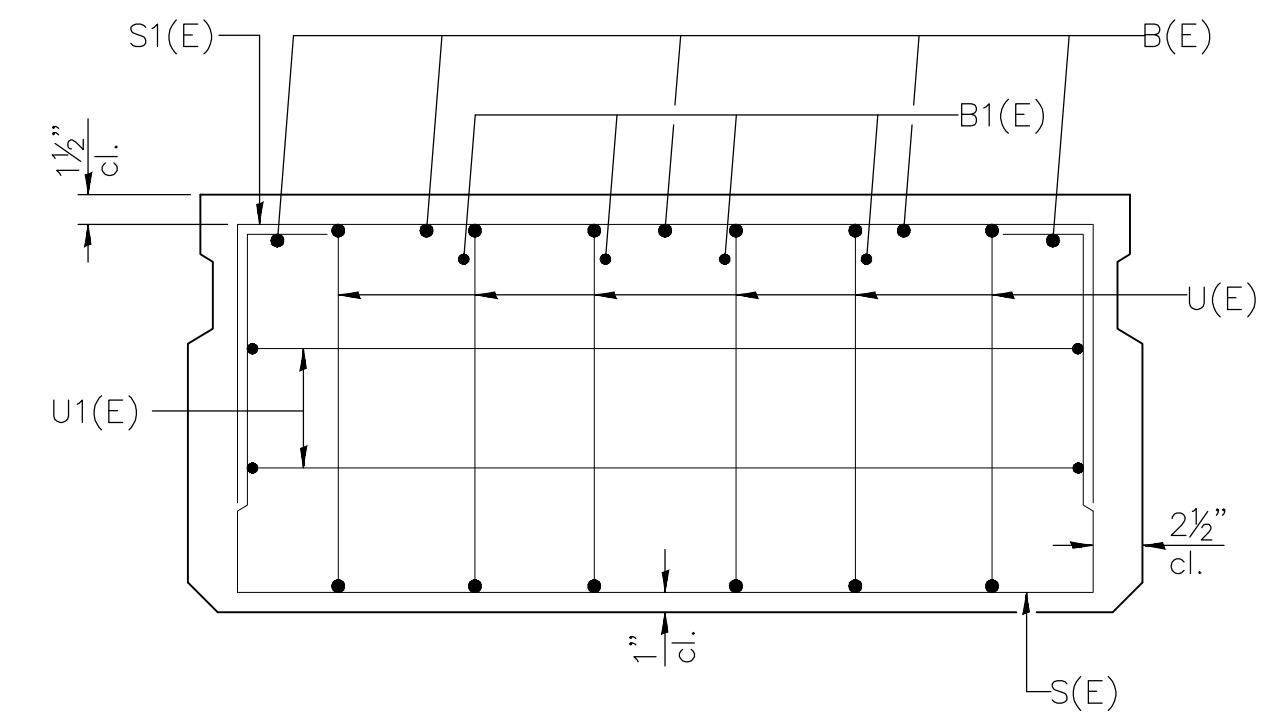
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CONTRACT NO. 99685				
ILLINOIS		FED. AID PROJECT HORA(195)		



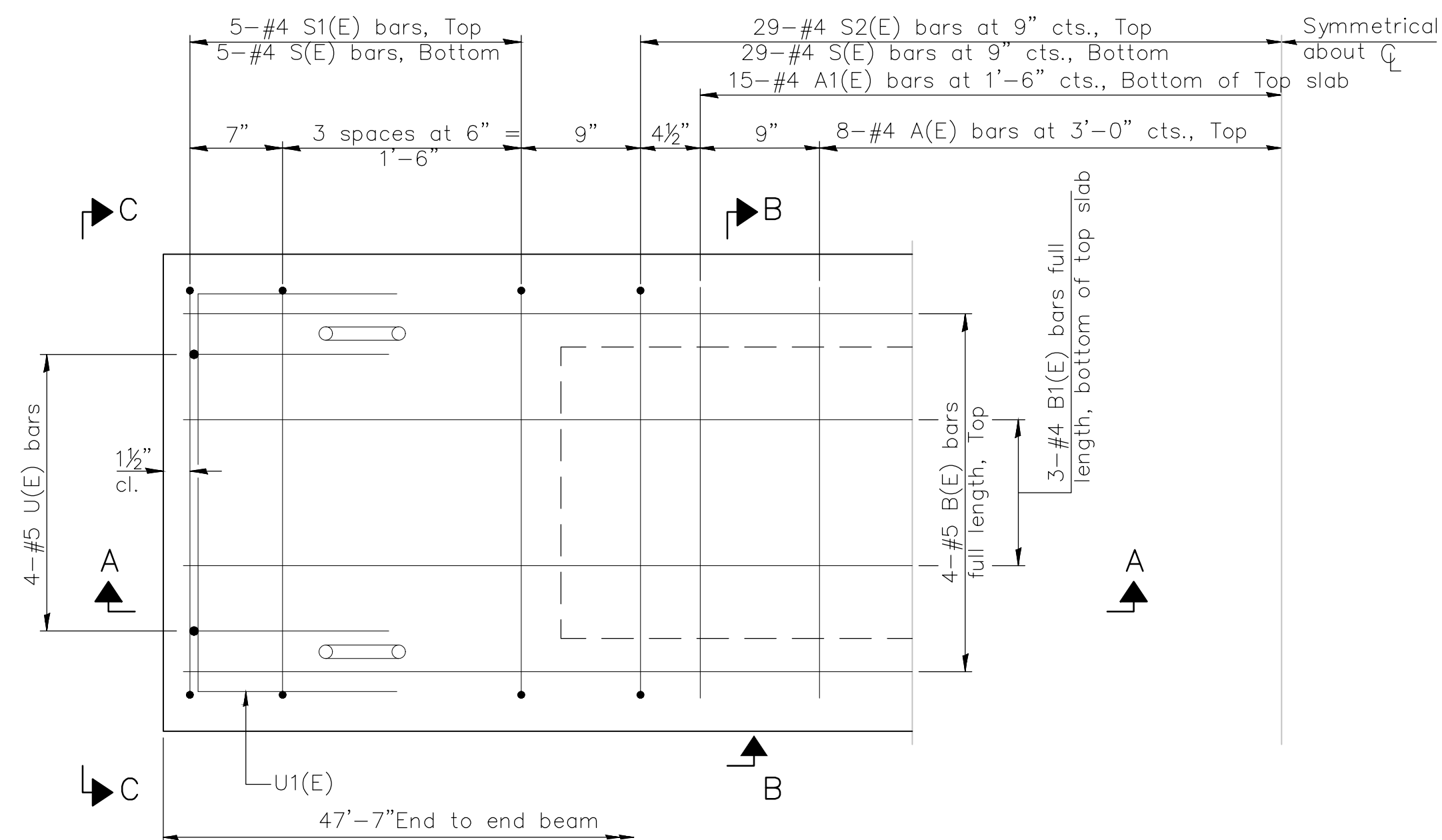
SECTION A-A



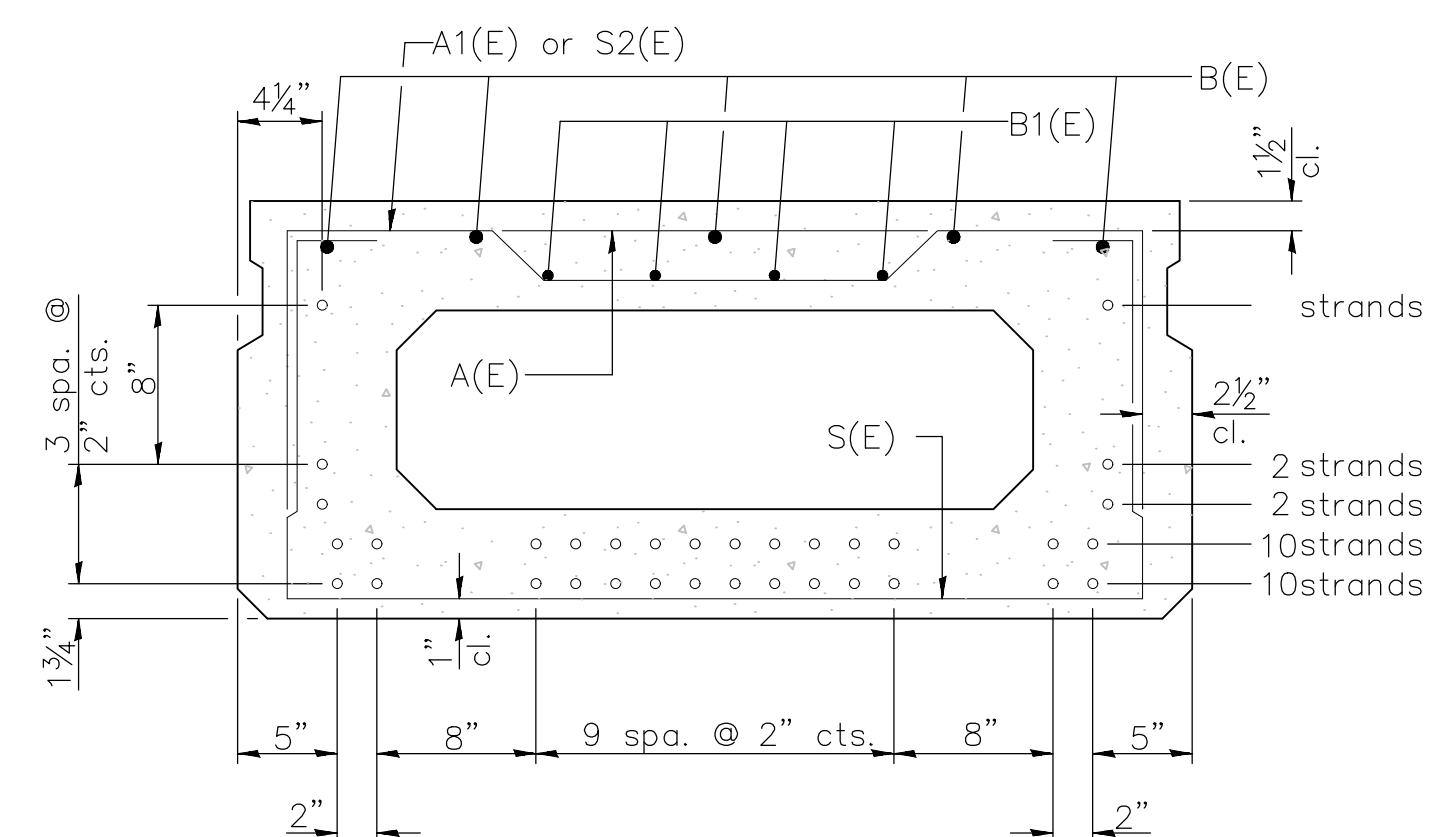
SECTION B-B  
(Showing dimensions)



VIEW C-C



PLAN VIEW



SECTION B-B

(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

**BAR LIST**  
**ONE BEAM ONLY**  
(For information only)

Bar	No.	Size	Length	Shape
A(E)	16	#4	2'-7"	—
A1(E)	30	#4	2'-10"	~
B(E)	4	#5	47'-4"	—
B1(E)	3	#4	47'-4"	—
S(E)	68	#4	6'-5"	□
S1(E)	10	#4	4'-11"	□
S2(E)	58	#4	5'-2"	□
U(E)	8	#5	4'-0"	□
U1(E)	4	#4	5'-0"	□

Note:  
See sheet 8 of 11 for additional details and Bill of Material.

Note: Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

**MINIMUM BAR LAP**

#4 bar = 1'-11"  
#5 bar = 2'-6"

DESIGNED	BCG	REVISED	—
DRAWN	DMG	REVISED	—
CHECKED		REVISED	—
DATE	1-16-2022	REVISED	—

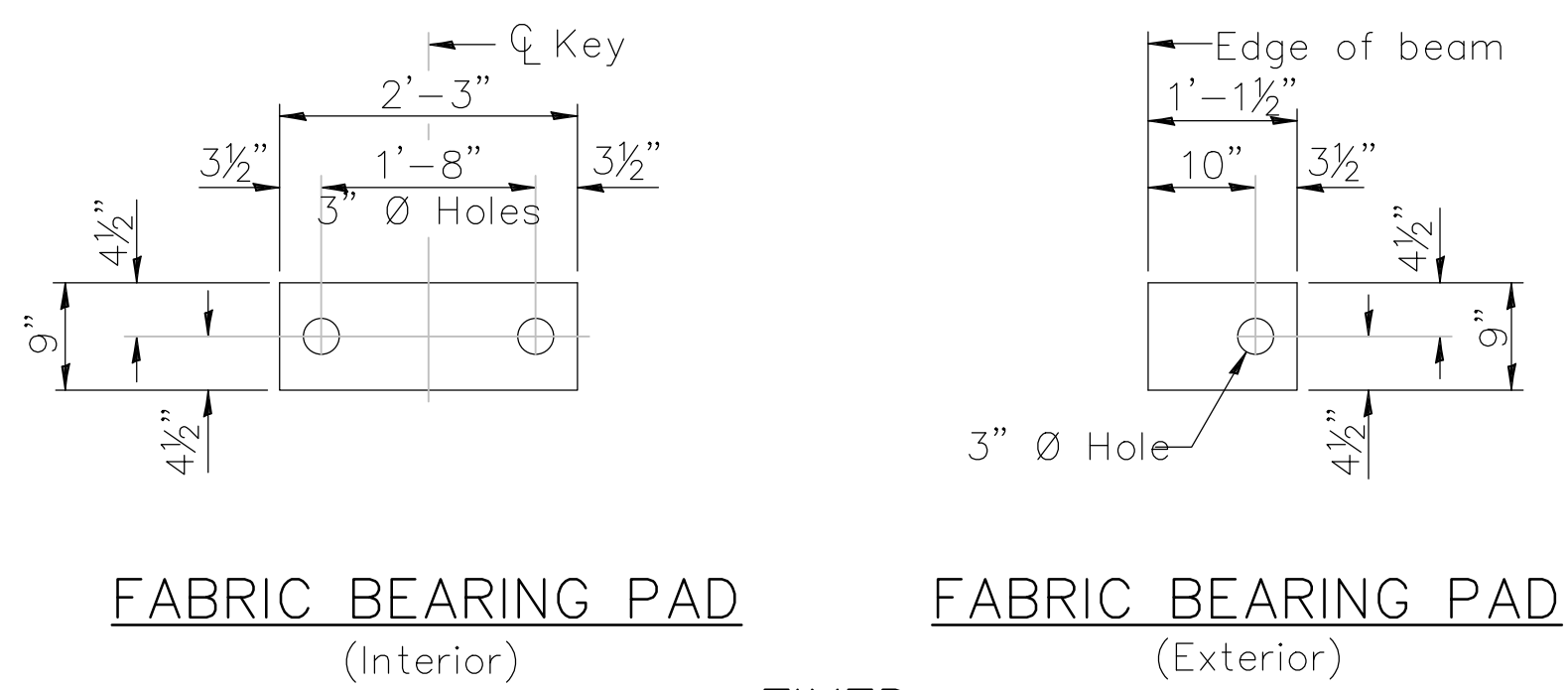
**CHASTAIN & ASSOCIATES LLC**  
CONSULTING ENGINEERS

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ROCKFORD (815) 489-0050  
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184-001397

21"x48" PPC DECK BEAM  
UNIONVILLE ROAD OVER MUD CREEK

SHEET NO. 7 OF 11 SHEETS STA. 48+25 TO STA. 51+74

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
933	19-00100-00-BR	MASSAC	11	7
CONTRACT NO. 99685			ILLINOIS FED. AID PROJECT HORA(195)	

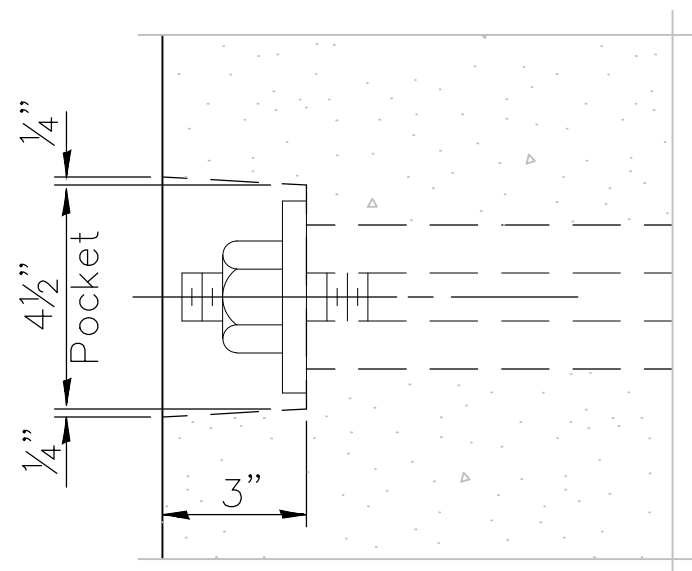


FABRIC BEARING PAD  
(Interior)

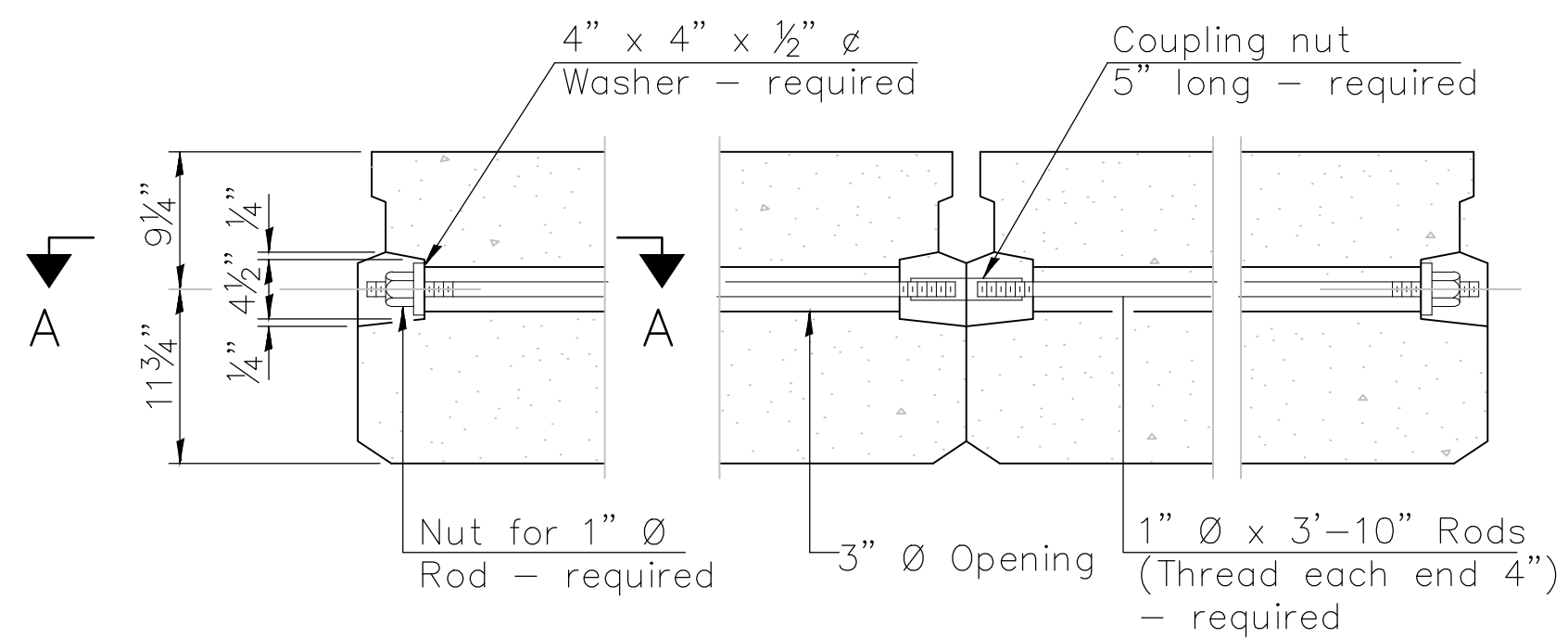
FABRIC BEARING PAD  
(Exterior)

FIXED

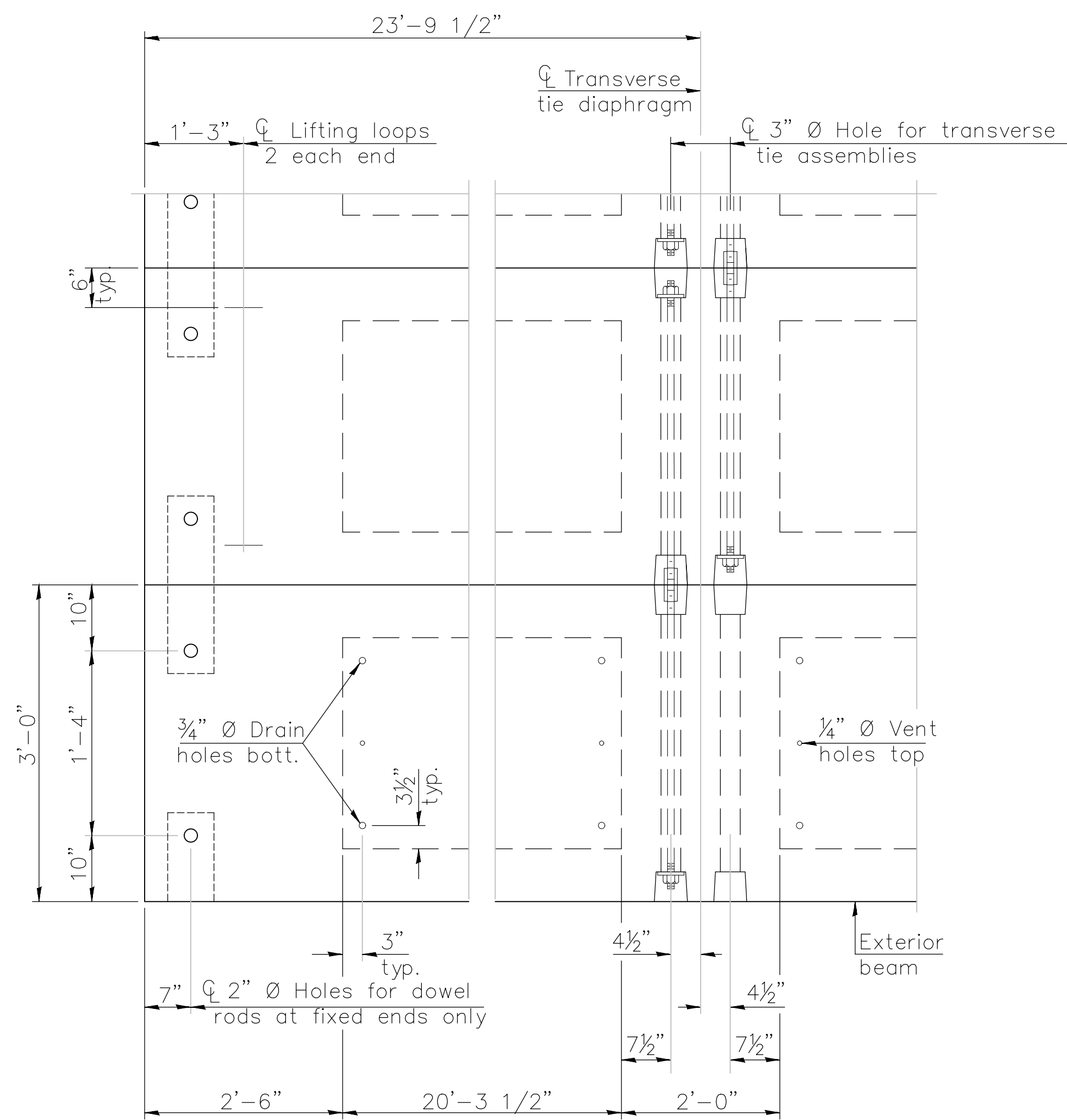
Notes:  
All bearing pads shall be 1" thick.  
Omit holes when using expansion bearings.  
Expansion bearing pad shall be bonded to the substructure.



SECTION A-A

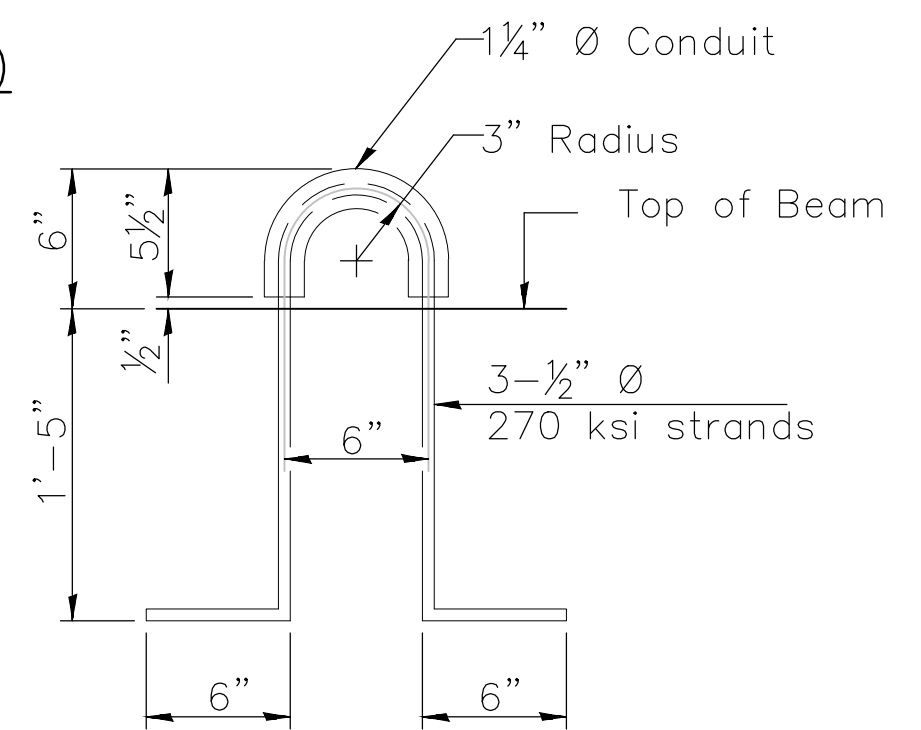
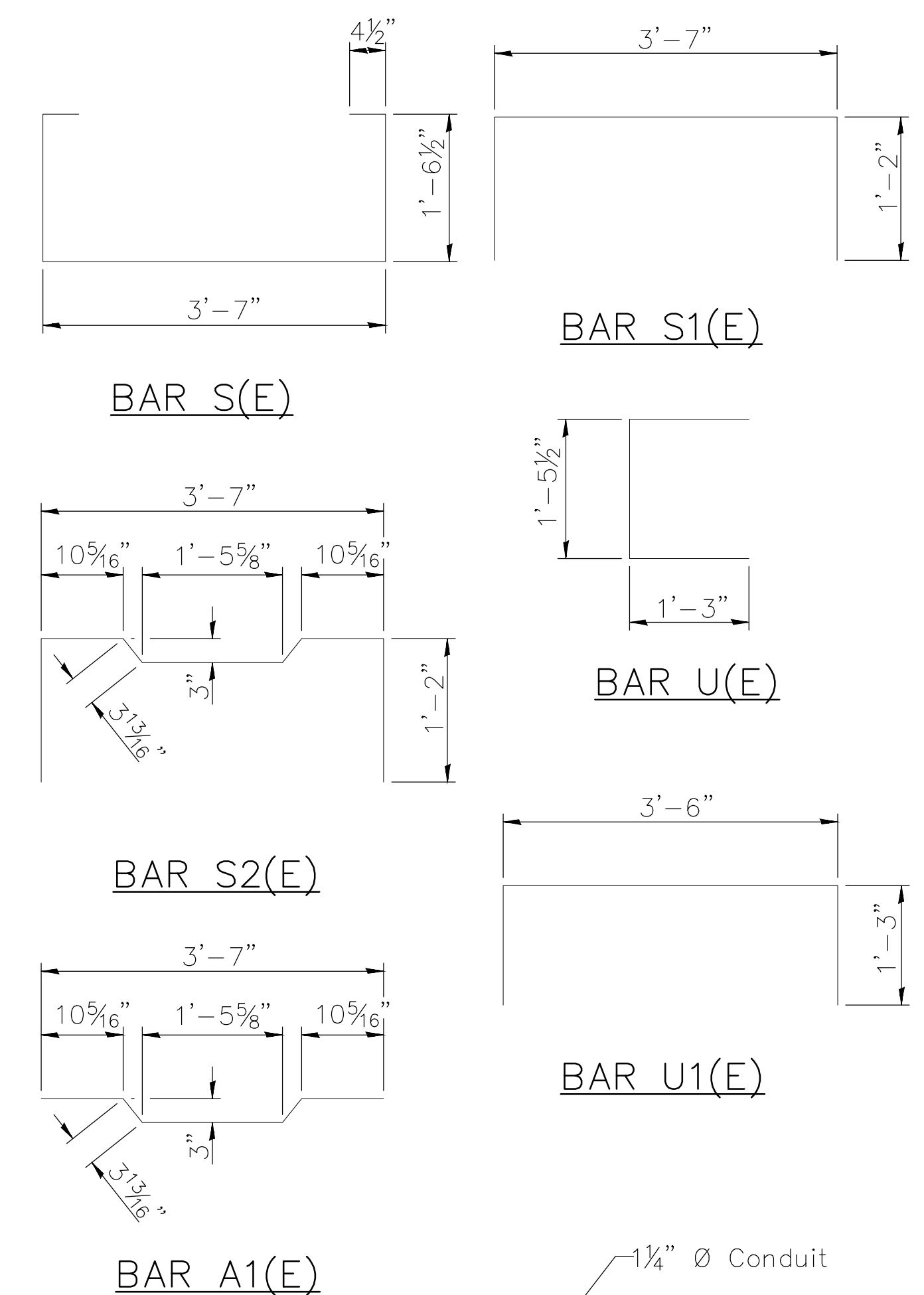


TYPICAL TRANSVERSE TIE ASSEMBLY



PLAN VIEW

Note: Connect beams in pairs with the transverse tie configuration shown.



LIFTING LOOP DETAIL

NOTES

Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in.  
The 1" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.  
Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.  
A minimum 2 1/2" diameter lifting pin shall be used to engage the lifting loops during handling.  
Corrosion Inhibitor, per Article 1020.05(b)(10) and 1021.07 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.  
Compressive strength of prestressed concrete, f'c, shall be 6000 psi.  
Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.

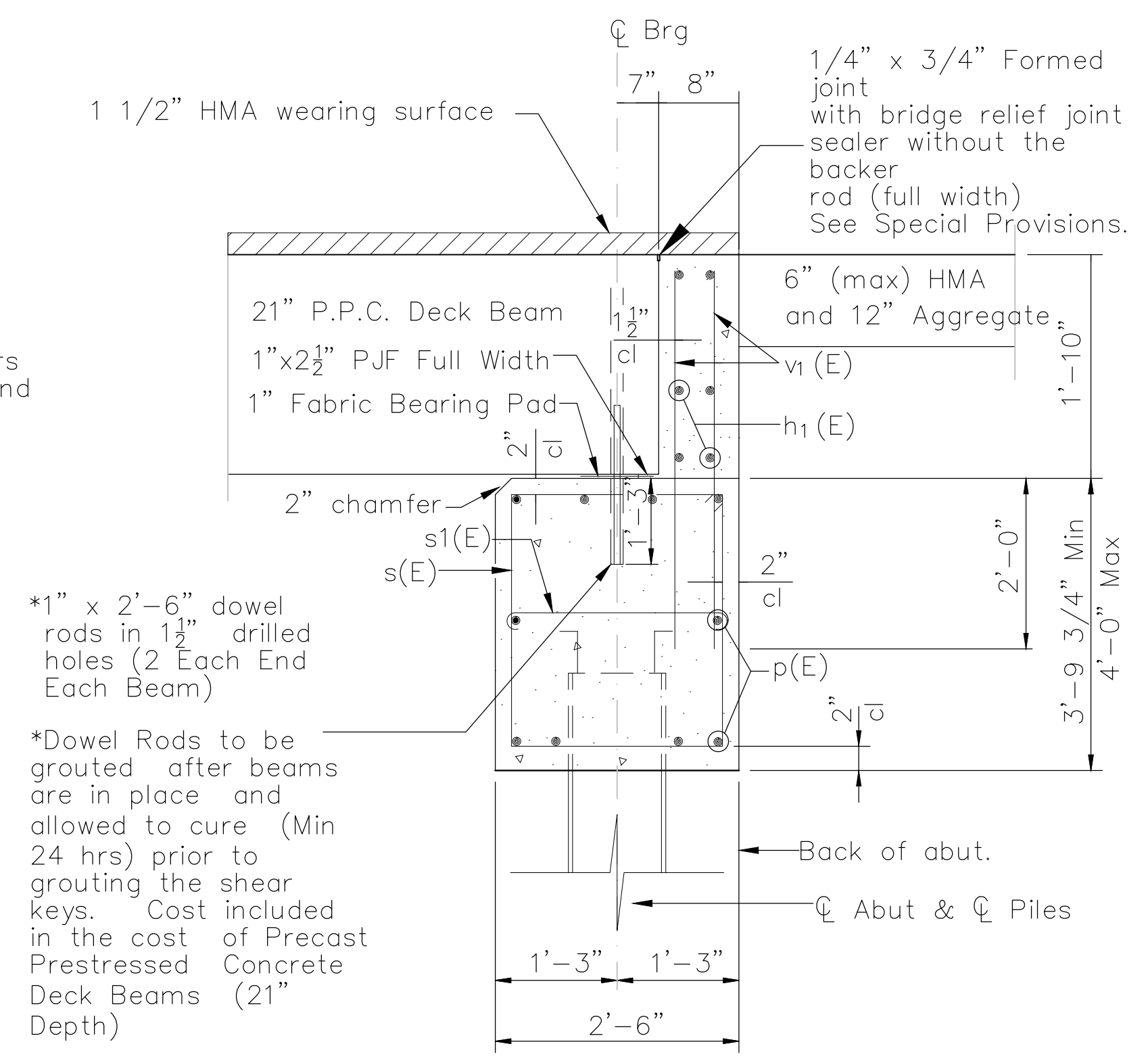
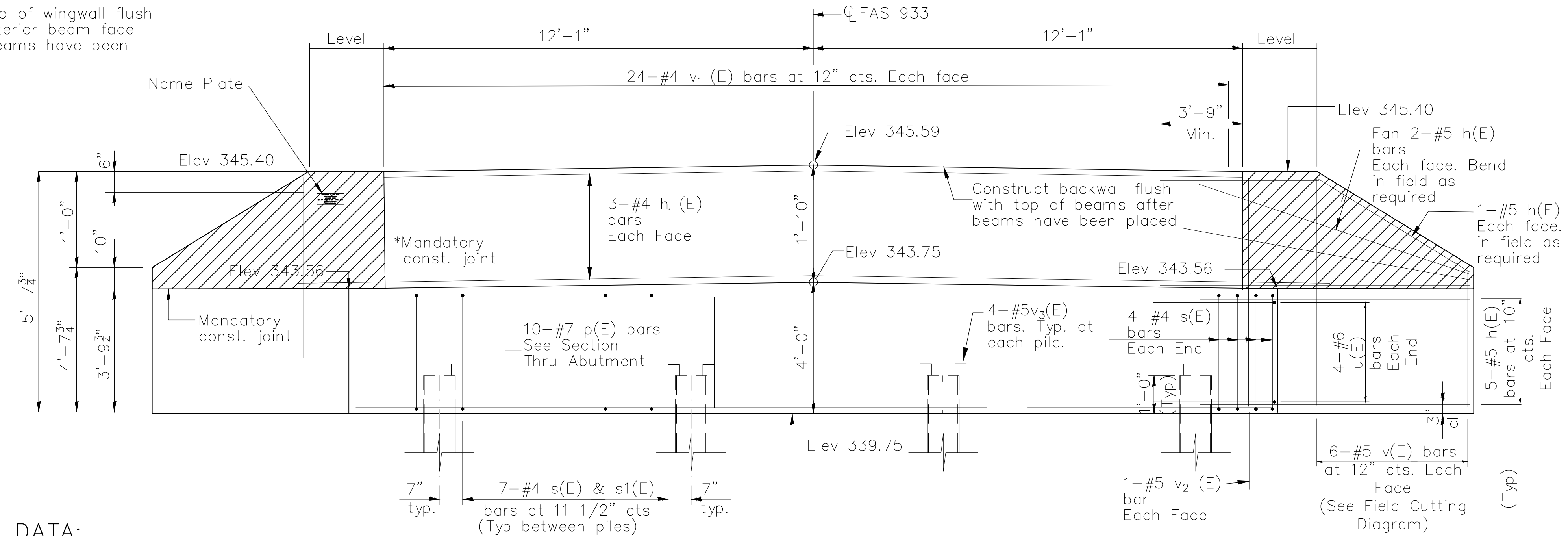
BILL OF MATERIAL

Precast Prestressed Conc. Deck Bms. (21" depth)	Sq. Ft.	1142
-------------------------------------------------	---------	------

DESIGNED	BCG	REVISED	-		DECATUR (217) 422-8544 SCHAUMBURG (773) 714-0050 ROCKFORD (815) 489-0050 BENTON (618) 435-4400 PADUCAH (270) 558-5983 184-001397	21"x48" PPC DECK BEAM DETAILS UNIONVILLE ROAD OVER MUD CREEK			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN	DMG	REVISED	-						933	19-00100-00-BR	MASSAC	11	8
CHECKED		REVISED	-						CONTRACT NO. 99685				
DATE	1-16-2022	REVISED	-						SHEET NO. 8 OF 11 SHEETS STA. 48+25 TO STA. 51+74 ILLINOIS FED. AID PROJECT HORA(195)				



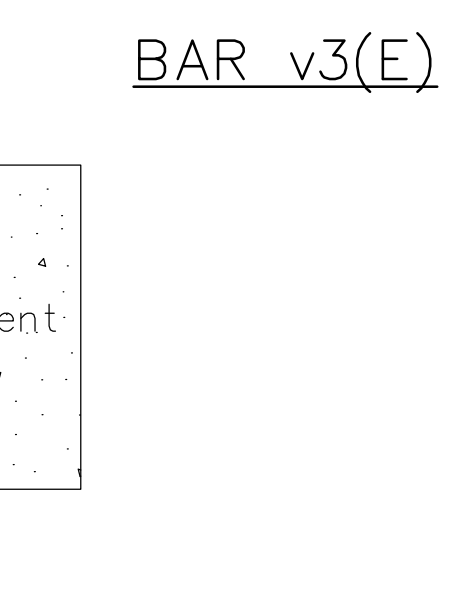
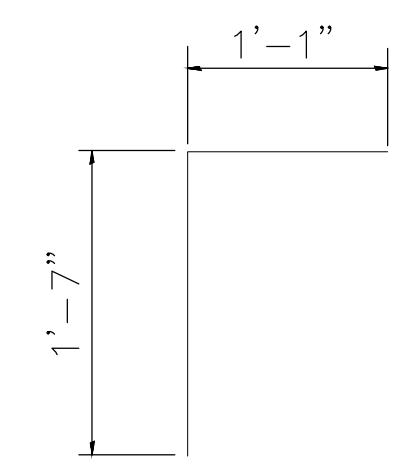
\*Cast top of wingwall flush with exterior beam face after beams have been erected.



\*1" x 2'-6" dowel rods in 1 1/2" drilled holes (2 Each End Each Beam)  
 \*Dowel Rods to be grouted after beams are in place and allowed to cure (Min 24 hrs) prior to grouting the shear keys. Cost included in the cost of Precast Prestressed Concrete Deck Beams (21" Depth)

Hatched portion of wingwalls to be poured with the backwall after the deck beams have been installed and grout has cured for a minimum of 24 hours.

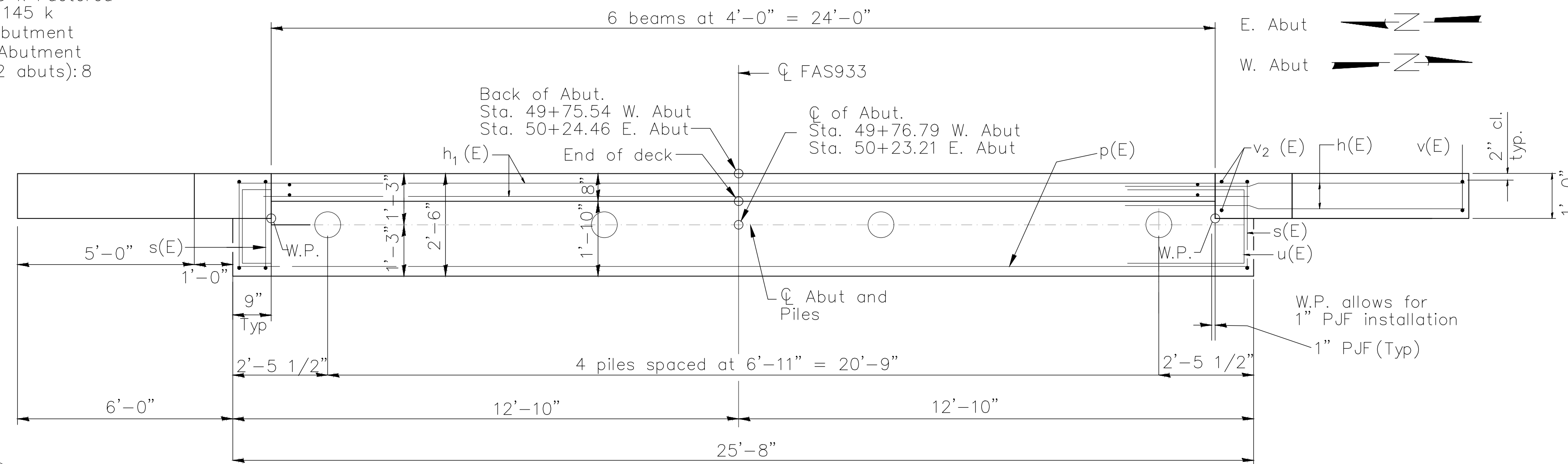
All exposed edges shall have standard 3/4" chamfers, except as noted.



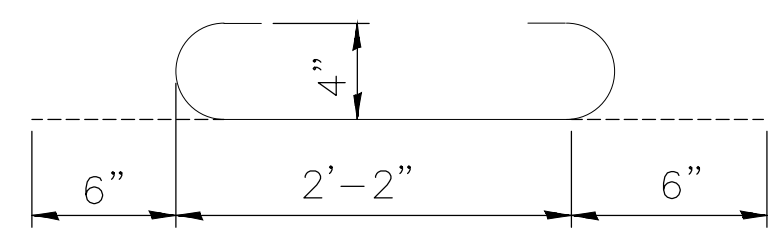
**PILE DATA:**

Type: MS 12x0.250" Nominal  
 Required Bearing: 353 k Factored  
 Resistance Available: 145 k  
 Est. Length: 31' East Abutment  
 37' West Abutment  
 No. Production Piles (2 abuts): 8

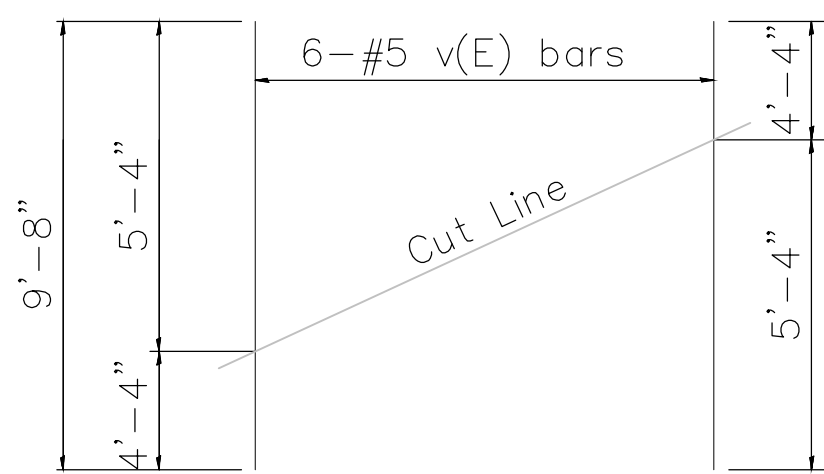
**ELEVATION**



**PLAN**

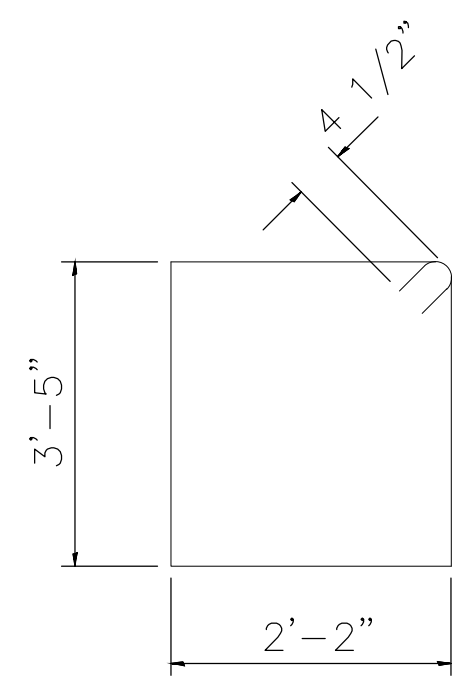


**BAR s1(E)**

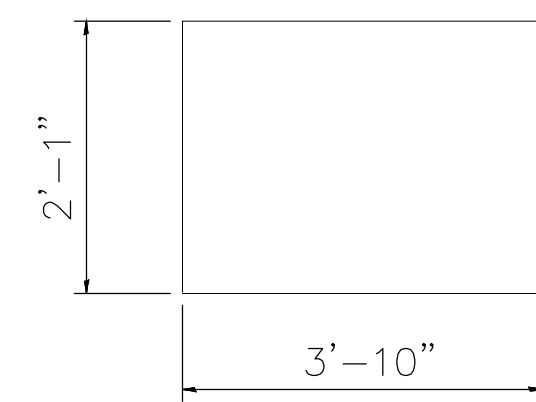


**FIELD CUTTING DIAGRAM**

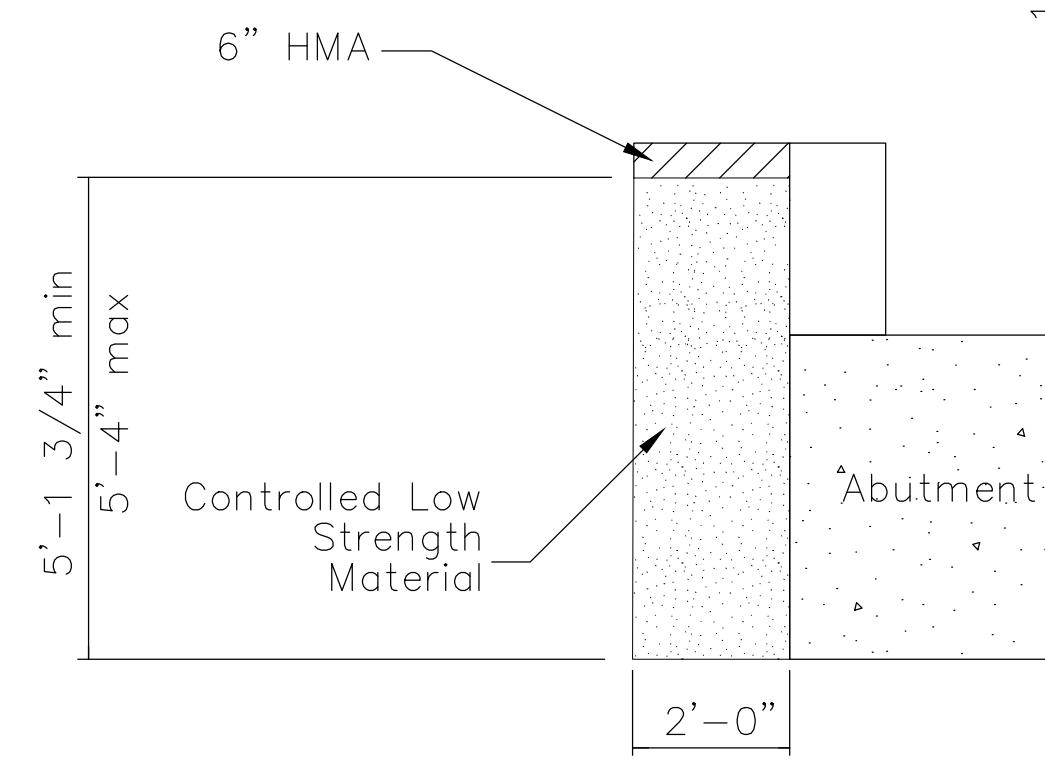
Order v(E) full length. Cut as shown and use remainder of bars in opposite face.



**BARS s(E)**



**BAR u(E)**



**CONTROLLED LOW STRENGTH MATERIAL LIMITS**

(only placed behind pile cap)

**SECTION THRU ABUTMENT**

(Dimensions at Rt L's)

**BILL OF MATERIAL (2 ABUTMENTS)**

Bar	No.	Size	Length	Shape
h(E)	64	#5	10'-1"	—
h1(E)	12	#4	23'-10"	—
p(E)	20	#7	25'-4"	—
s(E)	58	#4	11'-11"	□
s1(E)	58	#4	3'-2"	—
u(E)	16	#6	9'-9"	—
v(E)	24	#5	9'-8"	—
v1(E)	96	#4	3'-8"	—
v2(E)	8	#5	5'-3"	—
v3(E)	32	#5	2'-8"	—
Concrete Structures	Cu. Yd.		25.2	
Reinforcement Bars	Pound		3330	
Furnishing Metal Shell Pile MS 12x0.25	Foot		272	
Driving Piles	Foot		272	
Controlled Low Strength Material	Cu. Yd.		20.0	

**Notes:**

- For details of piles see sheet 10 of 11.
- Reinforcement bars designated (E) shall be epoxy coated.
- For concrete encasement detail, see sheet 10 of 11.
- Space reinforcement in cap to miss dowel rods.

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DATE	1-16-2022	REVISED	—

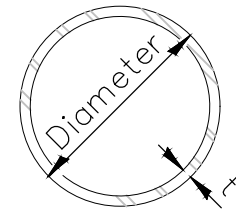
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ABUTMENT DETAILS  
 UNIONVILLE ROAD OVER MUD CREEK

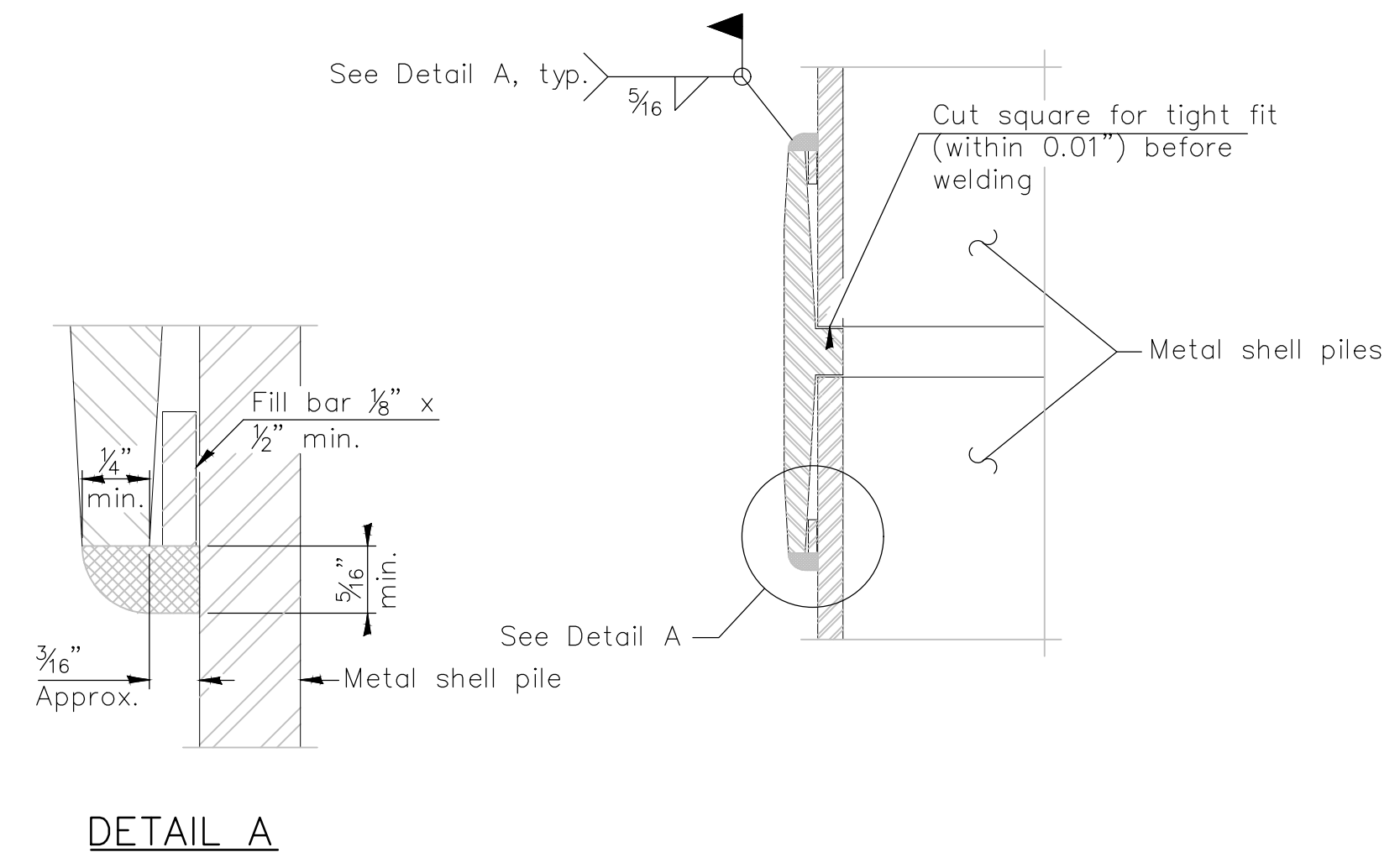
SHEET NO. 9 OF 11 SHEETS STA. 48+25 TO STA. 51+74

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
933	19-00100-00-BR	MASSAC	11	9
			CONTRACT NO. 99685	
			ILLINOIS FED. AID PROJECT HORA(195)	



**METAL SHELL PILE TABLE**

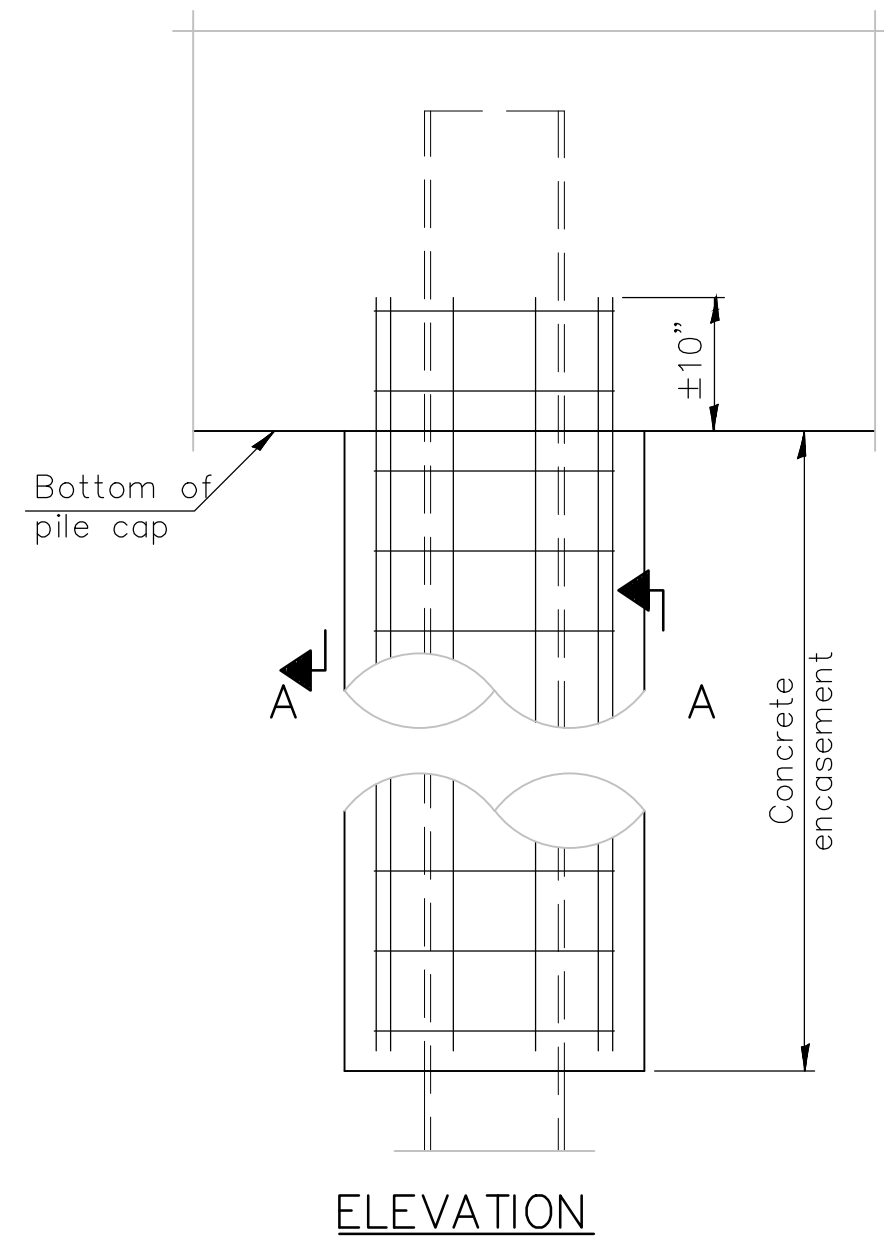
Designation and outside diameter	Wall thickness t	Weight per foot (Lbs./ft.)	Inside volume (yd. <sup>3</sup> /ft.)
PP12	0.250"	31.37	0.0267
PP14	0.250"	36.71	0.0368
PP14	0.312"	45.61	0.0361
PP16	0.312"	52.32	0.0478
PP16	0.375"	62.64	0.0470



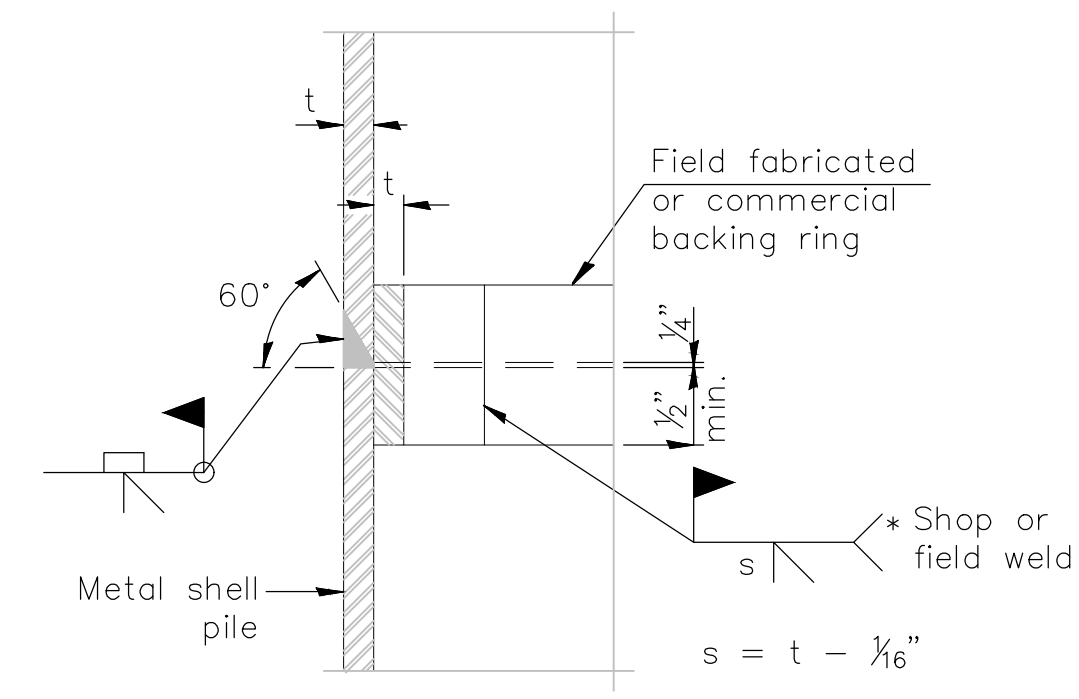
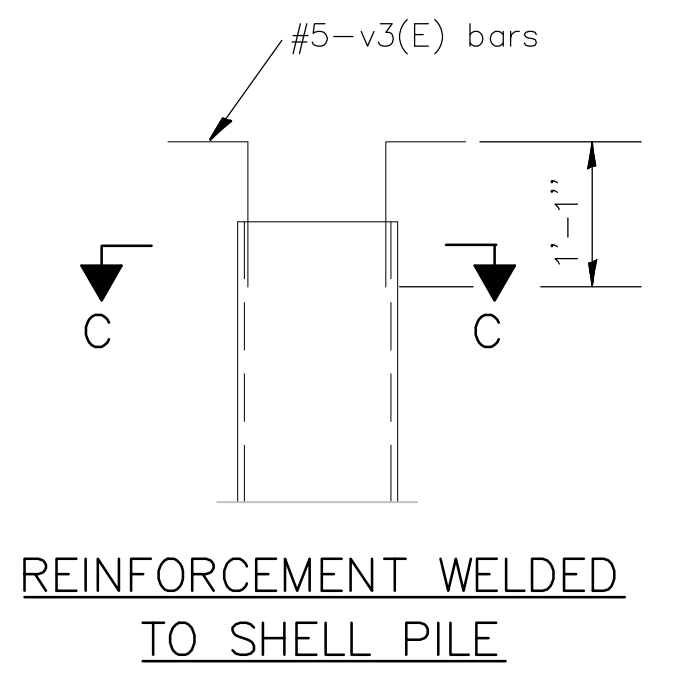
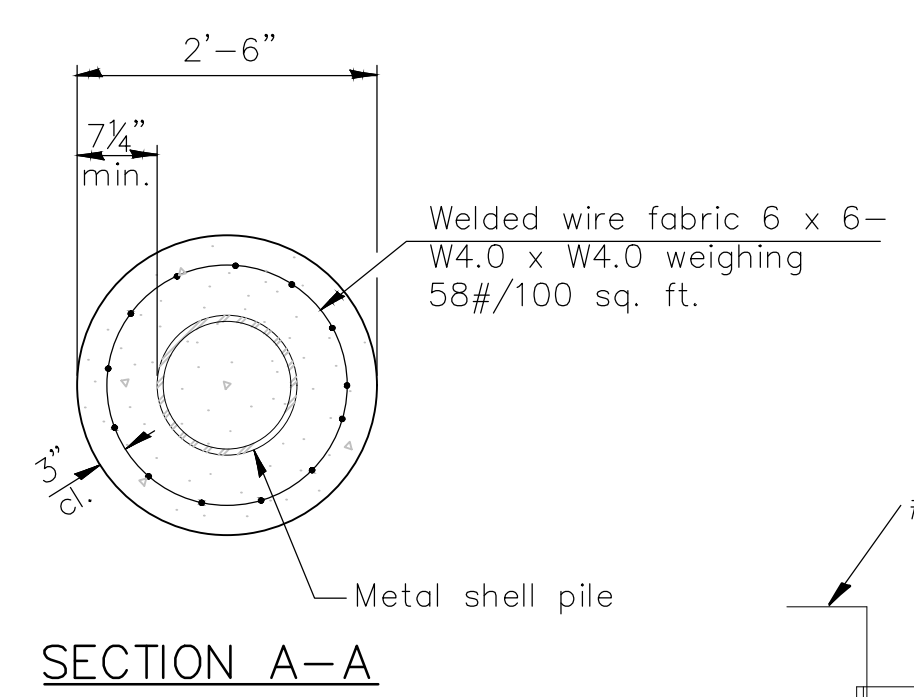
**DETAIL A**

**WELDED COMMERCIAL SPLICE**

Notes:  
 The  $\frac{1}{8}$ " x  $\frac{1}{2}$ " min. fill bar may be constructed of 2 bars with a  $\frac{1}{8}$ " max. gap between them.  
 Pile segments shall be driven to solid contact with splicer before welding.

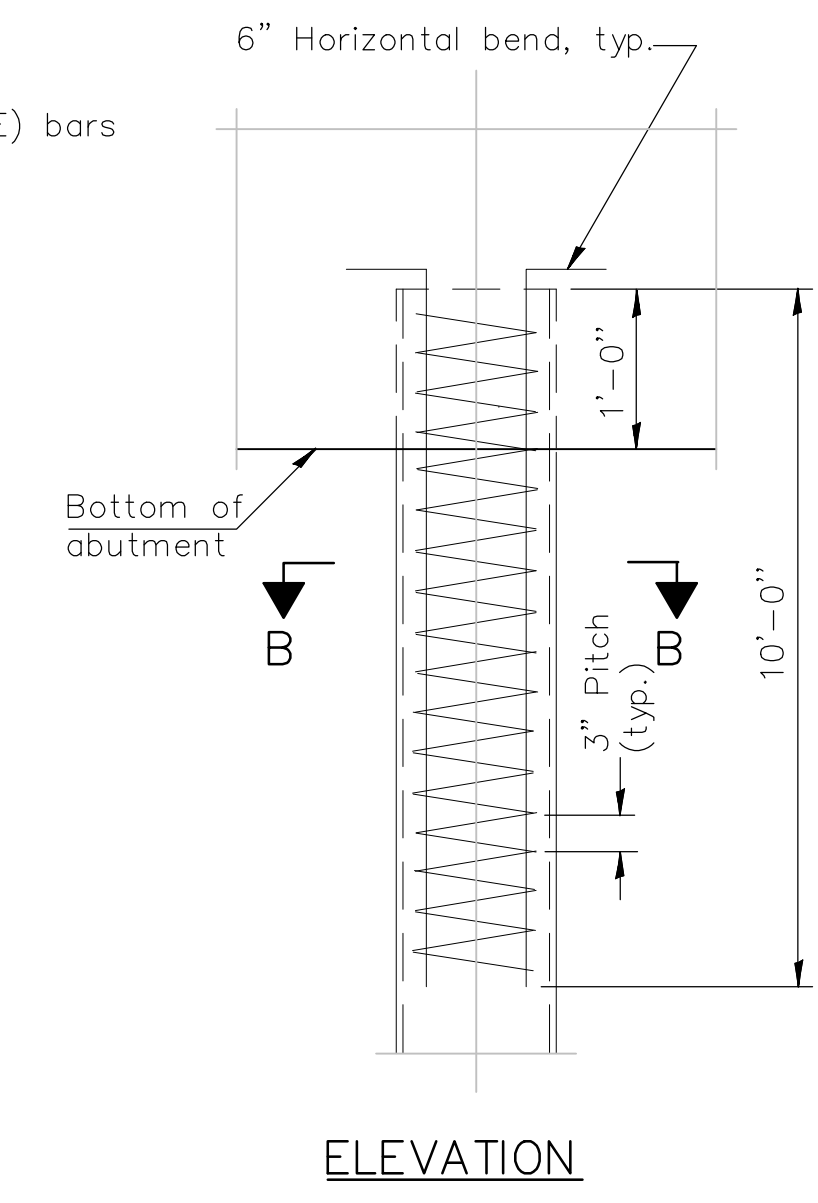
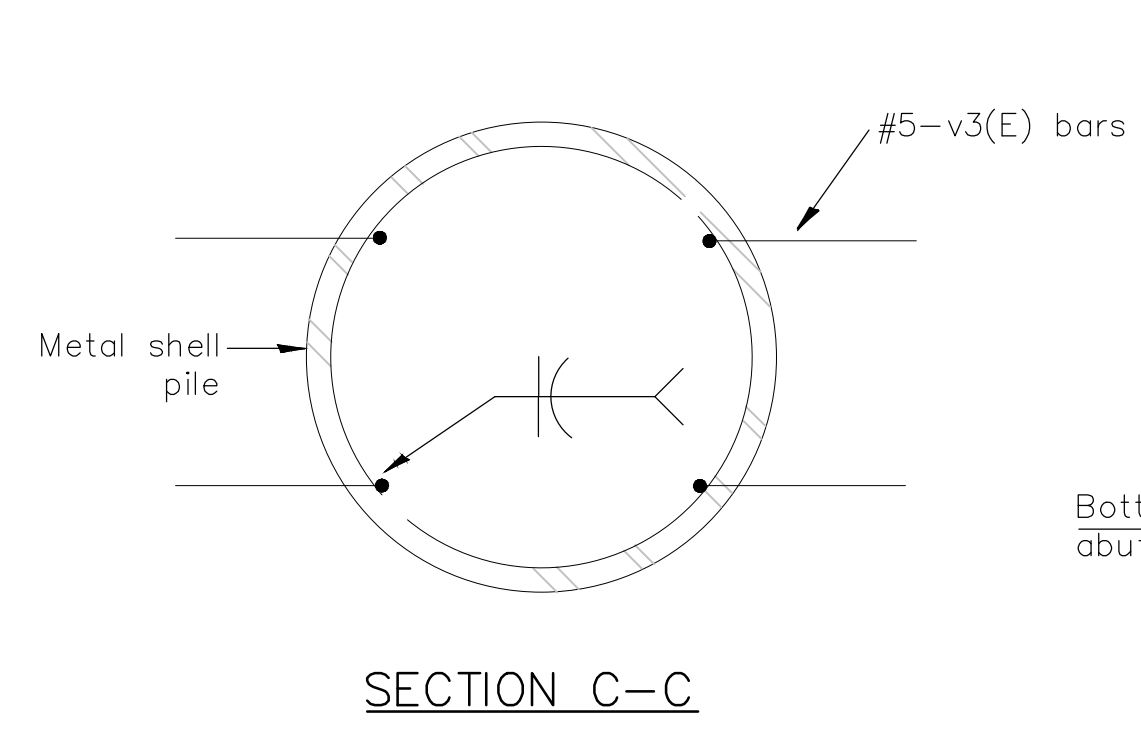


**INDIVIDUAL PILE CONCRETE ENCASEMENT AT PIERS**

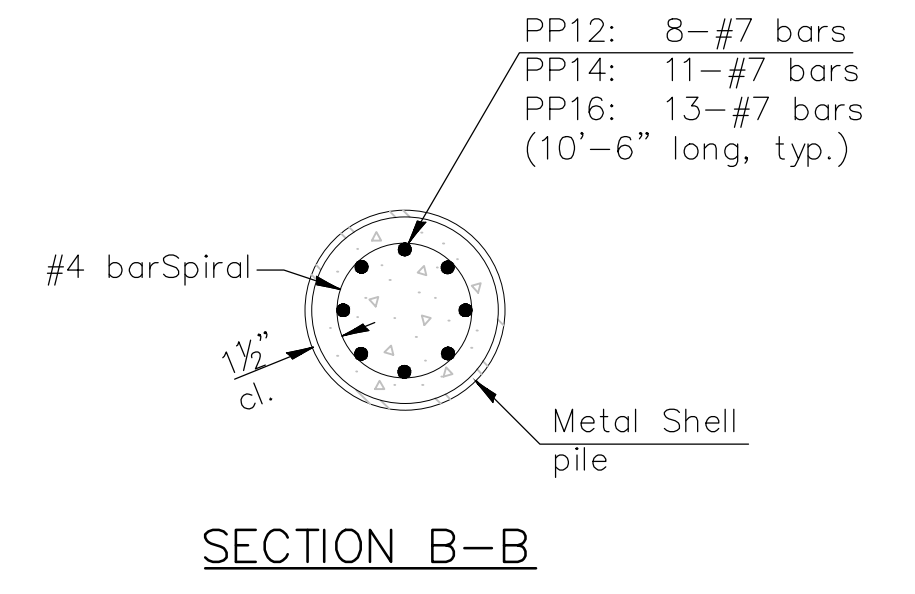


**COMPLETE PENETRATION WELD SPLICE**

\* Field fabricated backing ring may be made from pile shell by removing segment to allow reducing circumference and vertically rejoin with partial joint penetration weld.



**REINFORCEMENT AT ABUTMENTS**



Note:  
 The metal shell piles shall be according to Article 1006.05 of the Standard Specifications.  
 Metal shell piles shall be filled with concrete.  
 The cost of concrete to fill the metal shell and the steel reinforcement for the metal shell piles at the abutments, shall be included in the cost Furnishing Metal Shell Piles MS 12x0.25.

DESIGNED	BCG	REVISED	—
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CHECKED		REVISED	—
DATE	1-16-2022	REVISED	—

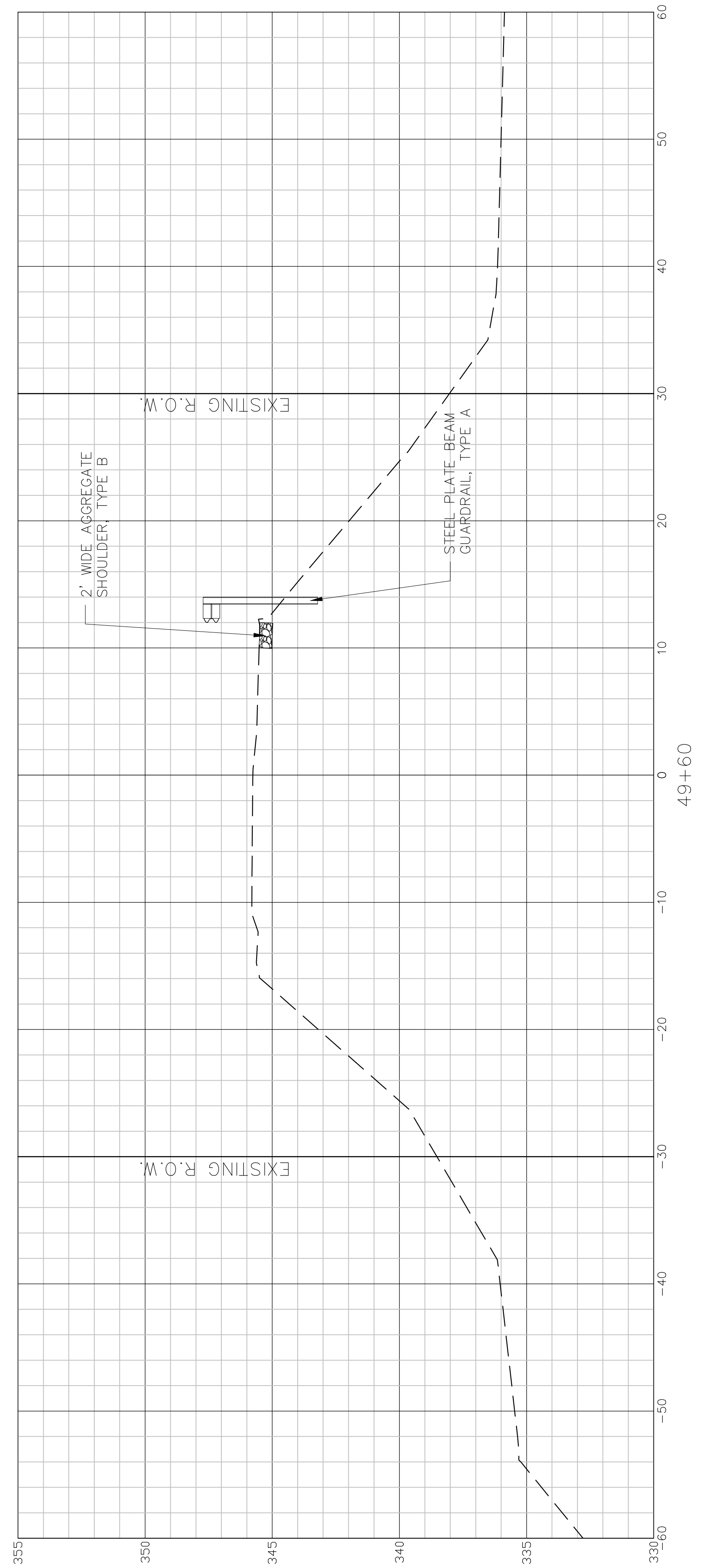
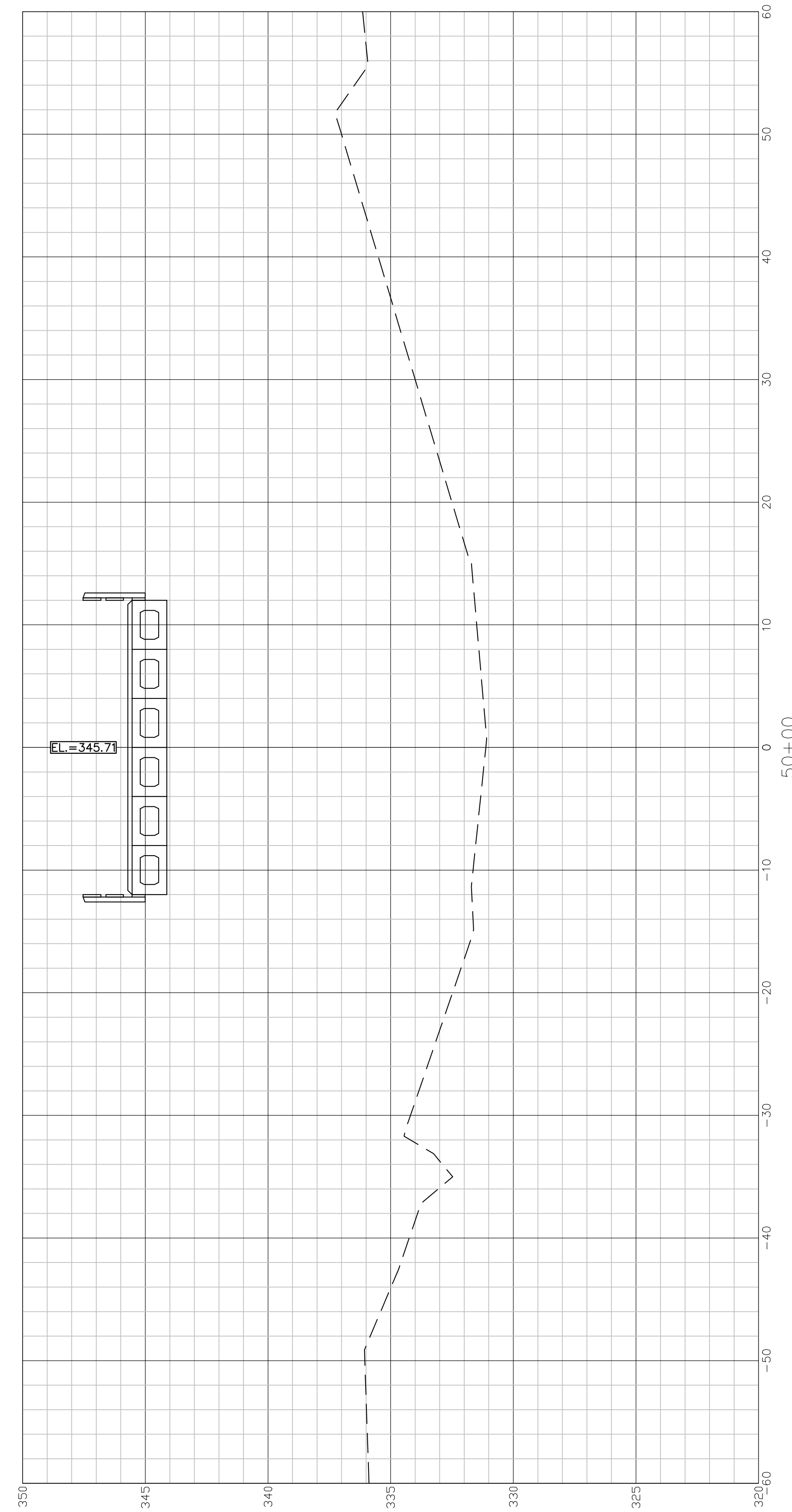
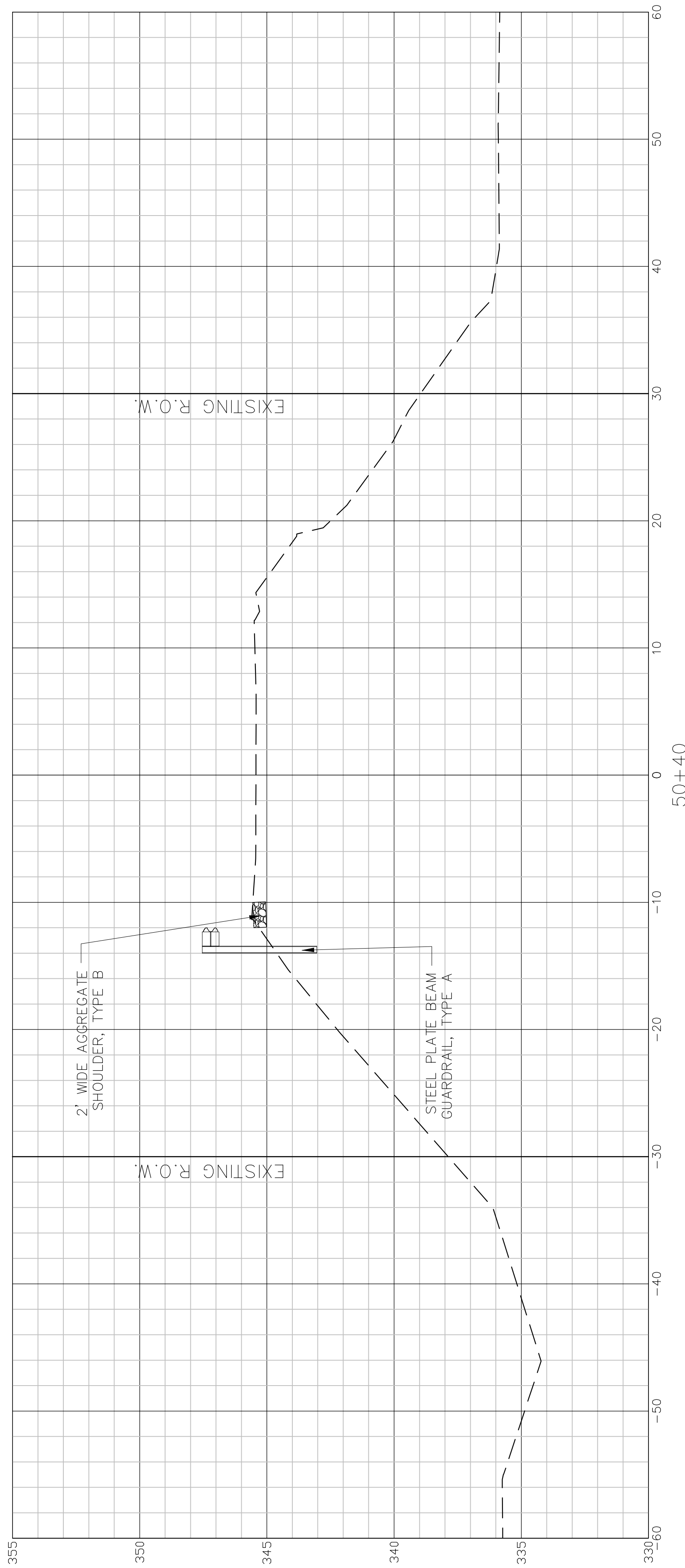


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**METAL SHELL PILE DETAILS  
 UNIONVILLE ROAD OVER MUD CREEK**

SHEET NO. 10 OF 11 SHEETS STA. 48+25 TO STA. 51+74

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
933	19-00100-00-BR	MASSAC	11	10
CONTRACT NO. 99685			ILLINOIS FED. AID PROJECT HORA(195)	



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CHECKED		REVISED	-
DATE	1-16-2022	REVISED	-

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PROPOSED ROADWAY CROSS SECTIONS  
 UNIONVILLE ROAD OVER MUD CREEK

SHEET NO. 11 OF 11 SHEETS STA. 48+25 TO STA. 51+74

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
933	19-00100-00-BR	MASSAC	11	11
ILLINOIS			FED. AID PROJECT HQRA(195)	