ITEM NO. 10A

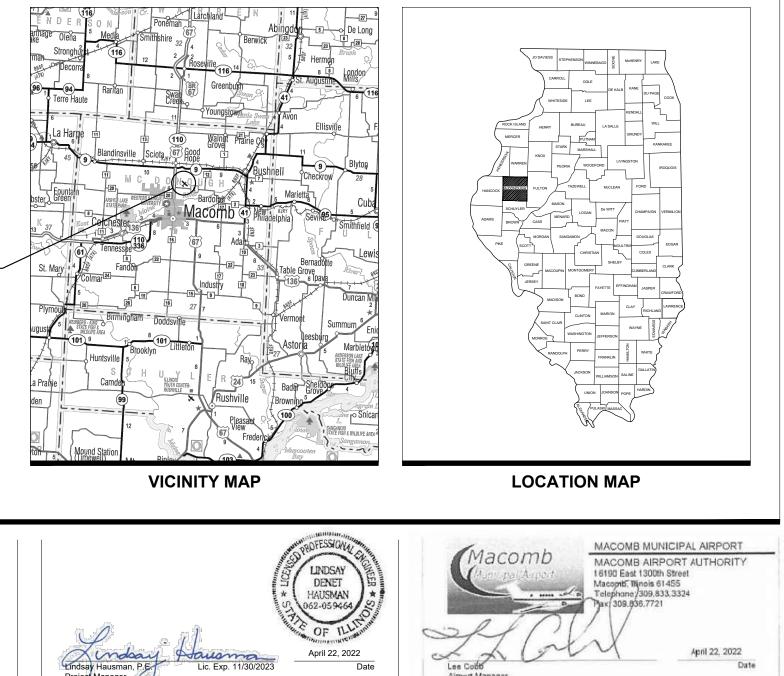
CONSTRUCTION PLANS

REHABILITATE RUNWAY 9-27

MACOMB AIRPORT AUTHORITY MACOMB MUNICIPAL AIRPORT (MQB) MACOMB, MCDONOUGH COUNTY, ILLINOIS

ILLINOIS PROJECT NO. MQB-4945 SBG PROJECT NO. 3-17-SBGP-171/TBD

APRIL 22, 2022 (100% SUBMITTAL)



NOTICE TO CONTRACTORS AND BIDDERS

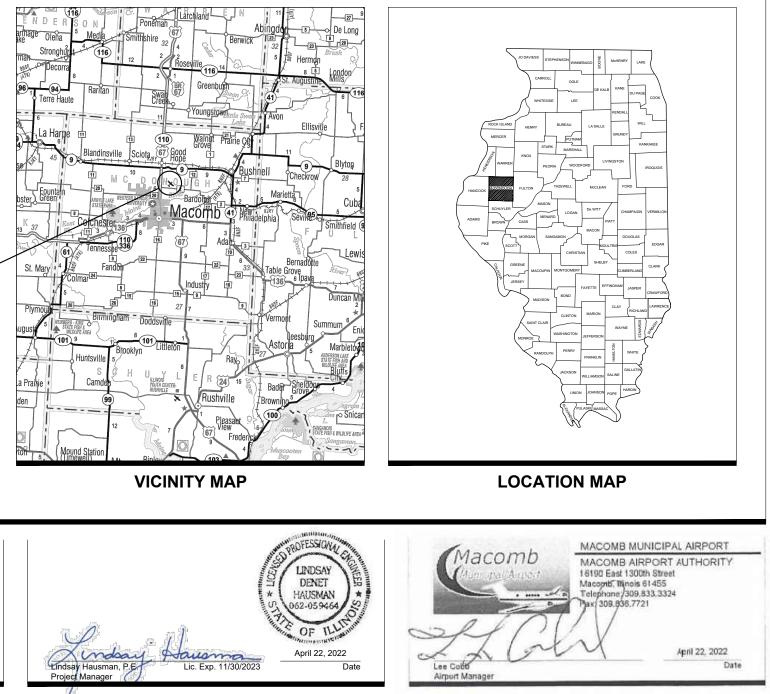
THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	By

PREPARED BY:

HANSON HANSON PROFESSIONAL SERVICES INC. 1525 S. Sixth St. Springfield, Illinois 62703 Telephone: 217.788.2450 Fax: 217.788.2503

MACOMB MUNICIPAL AIRPORT



MB034 TOTAL SHEETS = 28

SHEET LIST			
SHEET NO.	SHEET TITLE		
01	COVER SHEET		
02	SHEET INDEX AND SUMMARY OF QUANTITIES		
03	SCOPE OF WORK		
04	SAFETY PLAN - PHASE 1 AND 2		
05	SAFETY PLAN NOTES & DETAILS		
06	STORMWATER POLLUTION PREVENTION PLAN (SWPPP)		
07	SWPPP NOTES & DETAILS		
08	TYPICAL SECTIONS		
09	PAVEMENT PATCHING DETAILS		
10	PAVEMENT PREPARATION PLAN - 1		
11	PAVEMENT PREPARATION PLAN - 2		
12	PAVEMENT PREPARATION PLAN - 3		
13	MILLING GRADE PLAN 1		
14	MILLING GRADE PLAN 2		
15	PLAN & PROFILE 1 - RUNWAY 9-27		
16	PLAN & PROFILE 2 - RUNWAY 9-27		
17	PLAN & PROFILE 3 - RUNWAY 9-27		
18	PLAN & PROFILE 4 - RUNWAY 9-27		
19	CROSS SECTIONS 1 - RUNWAY 9-27		
20	CROSS SECTIONS 2 - RUNWAY 9-27		
21	CROSS SECTIONS 3 - RUNWAY 9-27		
22	CROSS SECTIONS 4 - RUNWAY 9-27		
23	CROSS SECTIONS 5 - RUNWAY 9-27		
24	CROSS SECTIONS 6 - RUNWAY 9-27		
25	CROSS SECTIONS 7 - RUNWAY 9-27		
26	MARKING PLAN 1		
27	MARKING PLAN - 2		
28	MARKING DETAILS		

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1.0	QUANTIT
AR150520	MOBILIZATION	L SUM	1.0	
AR150530	TRAFFIC MAINTENANCE	L SUM	1.0	
AR152480	SHOULDER ADJUSTMENT	SQ YD	11,329.0	
AR156510	SILT FENCE	FOOT	516.0	
AR156511	DITCH CHECK	EACH	3.0	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	FOOT	10,244.0	
AR201670	CRACK CONTROL FABRIC	SQ YD	2,193.0	
AR401614	BIT. SURF. CSEMETHOD II, SUPERPAVE	TON	7,773.0	
AR401630	BITUMINOUS SURFACE TEST SECTION	EACH	1.0	
AR401640	BITUMINOUS PAVEMENT GROOVING	SQ YD	45,149.0	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	57,330.0	
AR401660	SAW & SEAL BIT. JOINTS	FOOT	471.0	
AR401663	LONGITUDINAL JOINT SEALANT	FOOT	26,070.0	
AR401915	REM & REP BIT PAVEMENT-TYPE A	SQ YD	667.0	
AR401916	REM & REP BIT PAVEMENT-TYPE B	SQ YD	796.0	
AR603510	BITUMINOUS TACK COAT	GALLON	6,880.0	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	32,693.0	
AR620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	8,114.0	
AR901510	SEEDING	ACRE	2.4	
AR908510	MULCHING	ACRE	2.4	

GENERAL NOTES

- 1 AND SPECIFICATIONS.
- 2. ENGINEER
- 3. THE GENERAL PROJECT AREA.
- ADDITIONAL COST TO THE CONTRACT
- 5. FACILITIES
- 6. THE PRE-CONSTRUCTION MEETING.
- PERSONNEL
- DRAWINGS HE WILL PREPARE
- EXISTING UNDERGROUND UTILITIES.
- 11
- THAT PERIOD HAVE BEEN RECEIVED.

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTACT THE FAA COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS

THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE

ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE

4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES ANY DAMAGE TO EXISTING ELECTRICAL DRAINAGE AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO

CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM

THE LOCATION OF THE ENGINEER'S FIELD OFFICE WILL BE DETERMINED AT

THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.

8 THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR

9. THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD

10. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING

NPDES PERMIT - THIS PROJECT WILL DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS REQUIRED.

12. MATERIAL CERTIFICATIONS - MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.

13. CERTIFIED PAYROLLS - THE RESIDENT ENGINEER CANNOT FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR

J.U.L.I.E. INFORMATION

COUNTY	MCDONOUGH
CITY	MACOMB
TOWNSHIP	MACOMB T6N, R2W
SECTION NO.	7,8
ADDRESS	MACOMB MUNICIPAL AIRPORT
	16190 EAST 1300TH ST
	MACOMB. ILLINOIS 61455



Offices Nationwide www.hanson-inc.com

Hanson Professional Services Inc. 1525 S. 6th Street Springfield, IL 62568 phone: 217-788-2450 fax: 217-788-2503

Illinois Licensed Professional Service Corporation #184-001084



MACOMB MUNICIPAL AIRPORT 16190 East 1300th Street Macomb, Illinois 61455 Telephone: 309.833.3324 Fax: 309.836.7721

REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

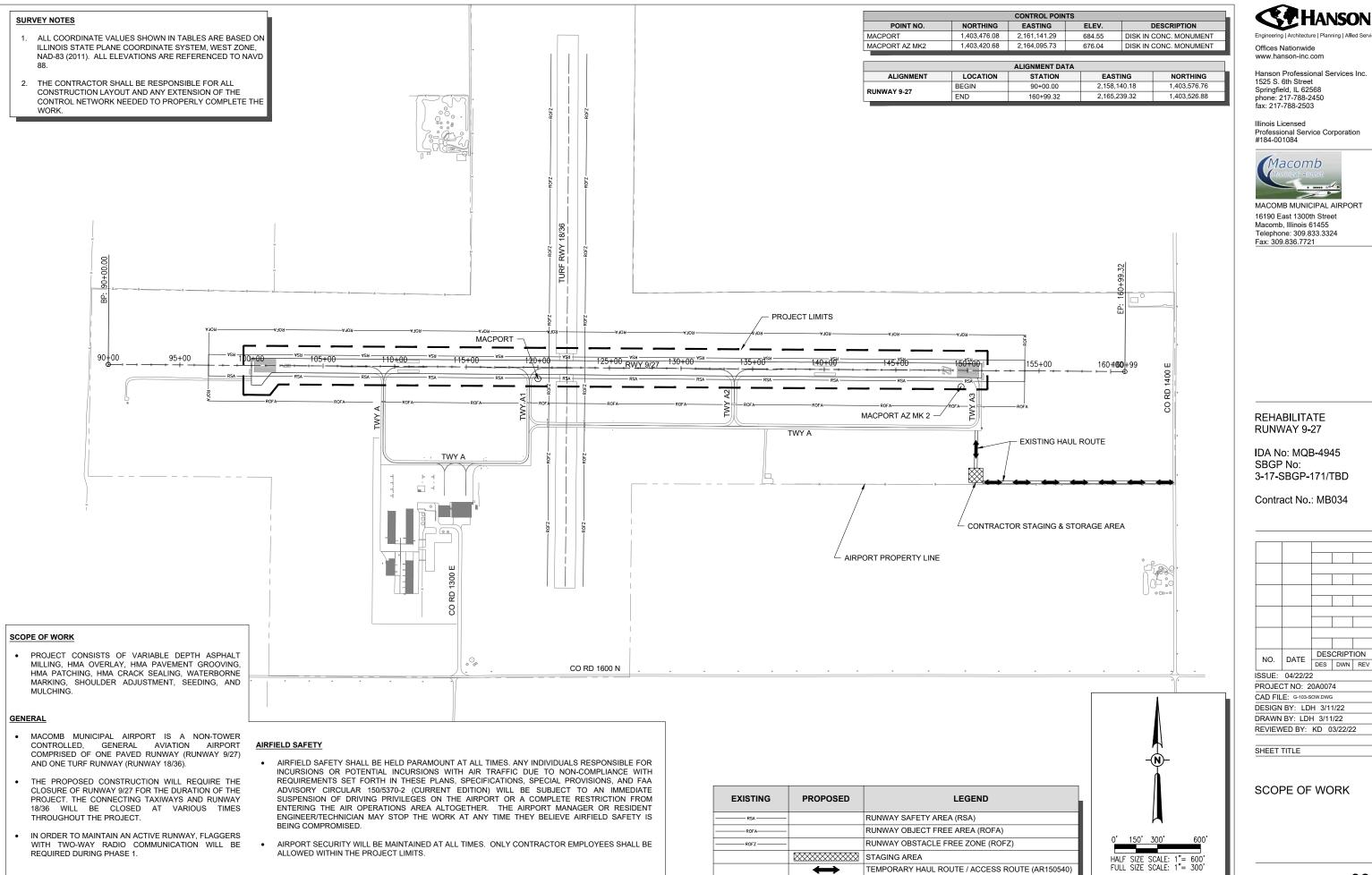
Contract No.: MB034

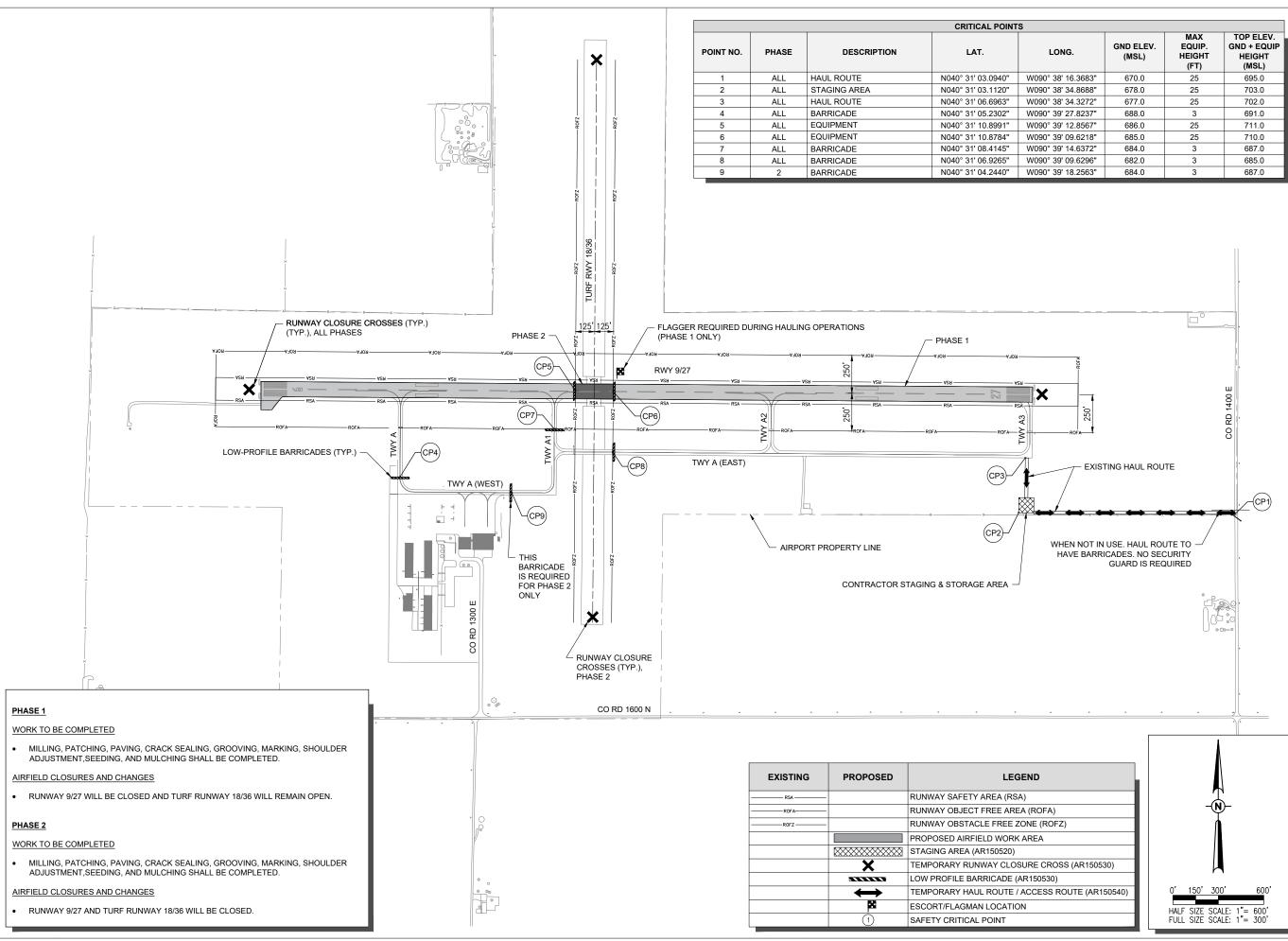
NO.	DATE	DES	CRIPT	ION		
NO.	DAIL	DES	DWN	REV		
ISSUE:	04/22/22	2				
PROJECT NO: 20A0074						
CAD FILE: G-002-SOQ.DWG						
DESIGN	DESIGN BY: LDH 3/24/22					
DRAWN BY: I DH 3/24/22						

DRAWN BY: LDH 3/24/22 REVIEWED BY: KD 03/22/22

SHEET TITLE

SHEET INDEX AND SUMMARY OF QUANTITIES





ING.	GND ELEV. (MSL)	MAX EQUIP. HEIGHT (FT)	TOP ELEV. GND + EQUIP HEIGHT (MSL)
8' 16.3683"	670.0	25	695.0
8' 34.8688"	678.0	25	703.0
8' 34.3272"	677.0	25	702.0
9' 27.8237"	688.0	3	691.0
9' 12.8567"	686.0	25	711.0
9' 09.6218"	685.0	25	710.0
9' 14.6372"	684.0	3	687.0
9' 09.6296"	682.0	3	685.0
9' 18.2563"	684.0	3	687.0



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Contract No.: MB034

NO.	DATE	DESCRIPTION					
110.	DAIL	DES	DWN	REV			
ISSUE:	04/22/22	2					
PROJEC	CT NO: 2	0A007	4				
CAD FIL	E: G-104-C	SPP.DW	G				
DESIGN	BY: LD	H 3/1	1/22				
DRAWN	DRAWN BY: LDH 3/11/22						
REVIEW	/ED BY:	KD 0	3/22/2	2			

SHEET TITLE

SAFETY PLAN -PHASE 1 AND 2

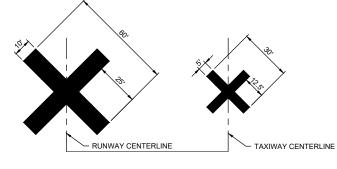
SAFETY NOTES

- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR 2. PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND 3. OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT
- PRIOR TO ACCESSING THE AIRFIELD, ANY DESIGNATED CONTRACTOR OR SUBCONTRACTOR EMPLOYEES WHO WILL BE OPERATING OR ESCORTING A VEHICLE ON AN ACTIVE AREA OF THE AIRFIELD MUST BE FAMILIAR WITH THE "FAA GUIDE TO GROUND VEHICLE OPERATIONS", AND KEEP A HARD COPY IN THE VEHICLE FOR REFERENCE. THE GUIDE CAN BE FOUND AT: https://www.faa.gov/airports/runway_safety/media/Ground_Vehicle_Guide_Proof_Final.pdf
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE (OPEN) AIRFIELD PAVEMENT 5. AREA WITHOUT AN APPROPRIATE ESCORT. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF RWY 9/27 AND/OR 125' OF RWY 18/36 (DISTANCES MEASURED FROM ACTIVE CENTERLINES) UNLESS CLOSED OR OTHERWISE NOTED. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL ALSO NOT BE PERMITTED WITHIN WITHIN $\underline{65.5}$ OF ANY ACTIVE AIRPORT TAXIWAY CENTERLINE OR APRON UNLESS OTHERWISE NOTED
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRE-CONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 65.5' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION", LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- 10. OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- 11. NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND ADDITIONAL AIRSPACE APPROVAL BY THE FAA. AIRSPACE APPROVALS REQUIRE CONSIDERABLE LEAD TIME AND SHOULD BE REQUESTED WELL IN ADVANCE.
- 12. NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT
- 13. SOIL DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEPT, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- 14. CONTRACTOR SHALL TAKE MEASURES TO AVOID TRACKING BITUMINOUS TACK COAT ASSOCIATED WITH PAVING PROJECTS ONTO ADJACENT PAVEMENT AREAS, ESPECIALLY GROOVED RUNWAY PAVEMENTS, UNLESS SUFFICIENT PROTECTION HAS BEEN APPLIED, HEAVY TRACKING OR DAMAGE TO ADJACENT PAVEMENTS AND GROOVED SURFACES MAY BE CAUSE FOR STOPPING THE WORK UNTIL ACCEPTABLE PROTECTION OR CHANGE IN WORK METHODS HAS BEEN PROVIDED
- 15. EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION, A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE
- 16. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- 17. CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN
- 18. CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED

- 19. THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE
- 20. CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST
- AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND 21. CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
 - 22. CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE
 - 23. THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
 - 24. THE CONTRACTOR WILL BE RESPONSIBLE FOR IMPLEMENTING MEASURES TO CONTROL OR AVOID CREATING ATTRACTANTS TO WILDLIFE. MEASURES MAY INCLUDE CONTINUOUSLY REMOVING ANY WASTE OR LOOSE MATERIALS, PLACEMENT OF MATERIALS IN APPROPRIATE STORAGE CONTAINERS, PROPERLY MAINTAINING FENCES AND GATES TO PREVENT ACCESS, AND PREVENTING PONDING OF WATER THROUGHOUT THE SITE.
 - 25. UNLESS SPECIFIED OTHERWISE, COST FOR SAFETY, STAGING, AND TRAFFIC MAINTENANCE ITEMS IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.
 - 26. THE CONTRACTOR SHALL HAVE THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS DETAILED IN THE SPECIAL PROVISIONS, SUBMITTED AND APPROVED PRIOR TO BEING ISSUED "NOTICE TO
 - 27. ALL RUNWAY/TAXIWAY CLOSURES SHALL BE COORDINATED WITH AIRPORT MANAGEMENT A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME TO ALLOW FOR THE PROPER COORDINATION. AIRPORT MANAGEMENT HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED
 - 28. RUNWAY/TAXIWAY CLOSURE PROCEDURES:
 - CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE A MINIMUM OF 7 DAYS BEFORE THE DESIRED CLOSING TIME.
 - ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
 - PLACEMENT OF CROSSES AND BARRICADES.
 - ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA
 - RUNWAY/TAXIWAY RE-OPENING PROCEDURES:
 - ENSURE ALL PERSONNEL, EQUIPMENT AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS ARFA
 - INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES
 - CONTACT AIRPORT MANAGEMENT OR REPRESENTATIVE FOR FINAL INSPECTION OF THE AREA.
 - REMOVE BARRICADES AND CROSSES.

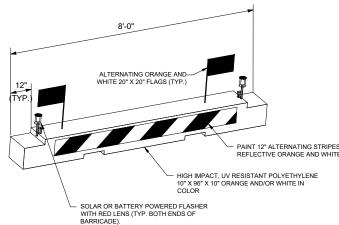
BARRICADE NOTES

• ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.



TEMPORARY CLOSURE CROSS DETAIL

- 1 SHALL BE "AVIATION YELLOW"
- 3
- OTHERWISE NOTED



LOW PROFILE AIRCRAFT BARRICADE DETAIL

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN
- 2. BARRICADES SHALL BE INTERLOCKED END TO END OVER THE LENGTH OF THE PAVEMENT WHERE PROTECTING OPEN RUNWAYS, AND SPACED END TO END A MAXIMUM OF 4 FEET IN OTHER ALL OTHER AREAS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY 3. MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- 4. THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION
- 5. BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT UNLESS OTHERWISE NOTED

NOT TO SCALE

TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS

TEMPORARY "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.

TEMPORARY "CLOSED RUNWAY" MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN

7. COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING "CLOSED RUNWAY" AND "CLOSED TAXIWAY" MARKINGS SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT, UNLESS

HIGH IMPACT, UV RESISTANT POLYETHYLENE 10" X 96" X 10" ORANGE AND/OR WHITE IN



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REHABILITATE RUNWAY 9-27

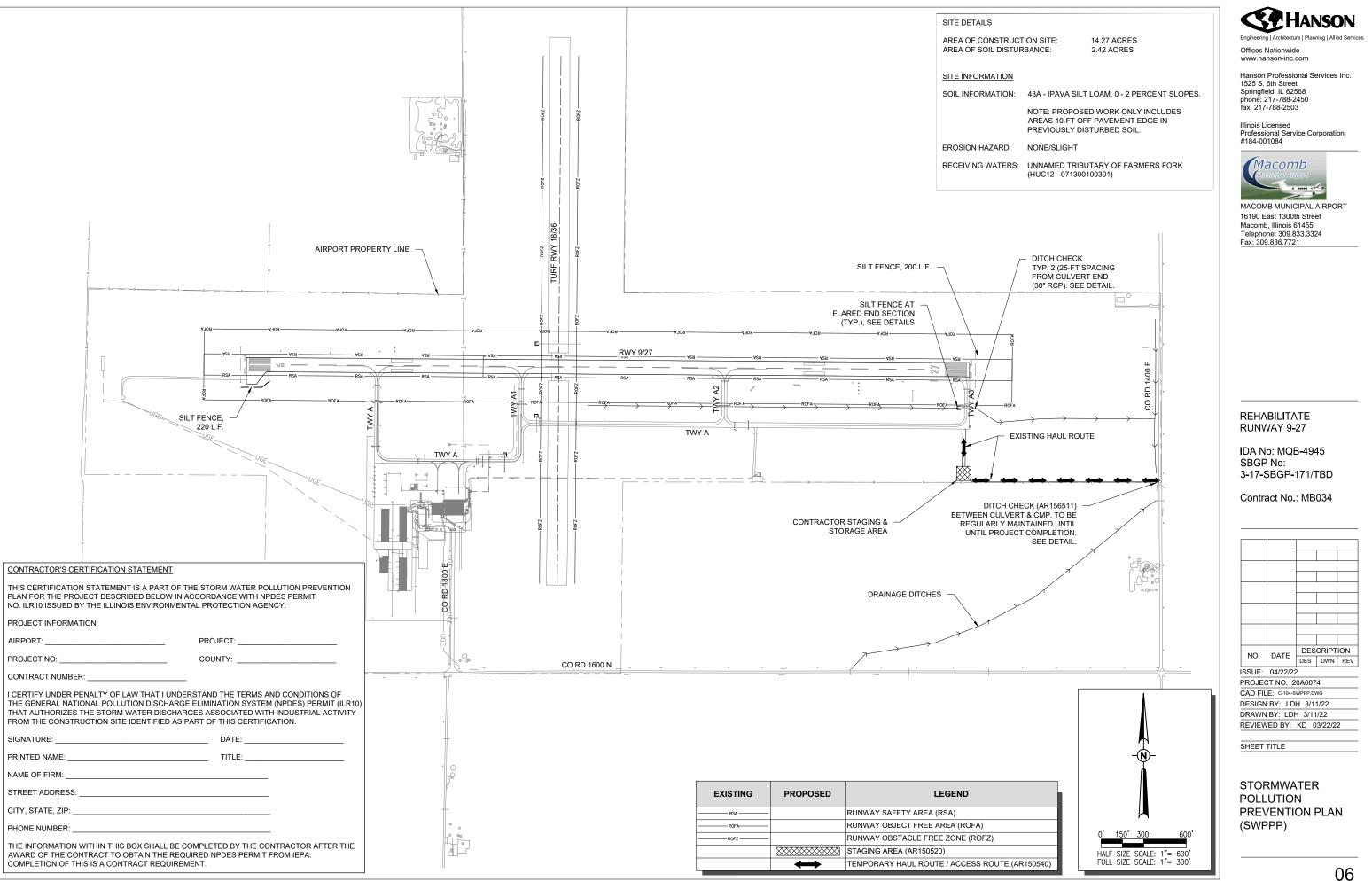
IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

NO.	DATE		CRIPT				
		DES	DWN	REV			
ISSUE:	04/22/2	2					
PROJE	CT NO: 2	20A007	'4				
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SHEET TITLE

SAFETY PLAN NOTES & DETAILS



SEDIMENTATION AND EROSION CONTROL NOTES:

- A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM: UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND
- DIVERSIONS) PRIOR TO PROCEEDING WITH ANY OTHER FARTH DISTURBANCE OR GRADING AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN. Ε.
- F. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
- G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER
- APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- K. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE L. ENFORCEMENT OFFICER
- M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- N. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- 0. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.

STORM WATER POLLUTION PREVENTION NOTES

GENERAL

THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

POLLUTION PREVENTION MEASURES

THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH <u>STANDARDS AND</u> SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

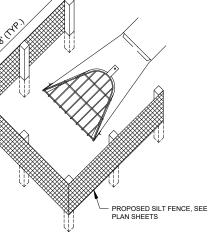
OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

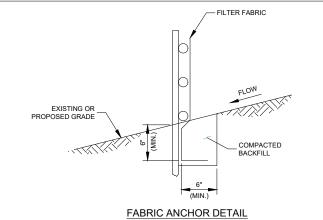
ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.

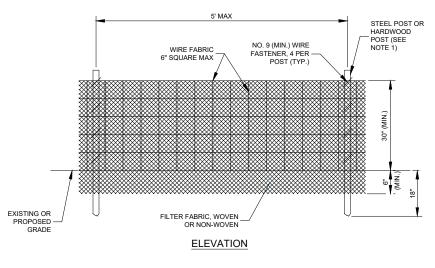
PLAN SHEETS

SILT FENCE PLACEMENT AT

FLARED END SECTIONS (FES)







NOTES:

- MEET SPECIFICATIONS

- FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
- MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1.j.1.f.i, AS AMENDED), OR EQUIVALENT.
- 7. A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
- AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- SHALL BE PROVIDED AFTER EACH RAIN EVENT.
- 11. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
- 12. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED BASIN, OR OTHER APPROPRIATE MEASURE).
- 13. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
- 14. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.



FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL

2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.

3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.

4. FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.

5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND

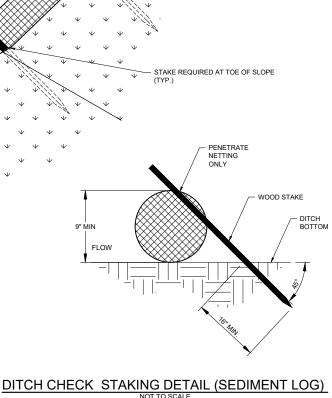
6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST

8. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS

9. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL

10. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE

THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT



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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

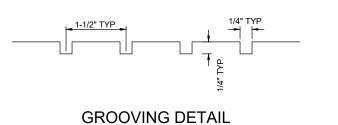
Contract No.: MB034

NO.	DATE	DESCRIPTION		ION		
NO.	DATE	DES	DWN	REV		
ISSUE:	ISSUE: 04/22/22					
PROJECT NO: 20A0074						
CAD FILE: C-104-SWPPP.DWG						
DESIGN BY: LDH 3/12/22						

DRAWN BY: LDH 3/12/22 REVIEWED BY: KD 03/22/22

SHEET TITLE

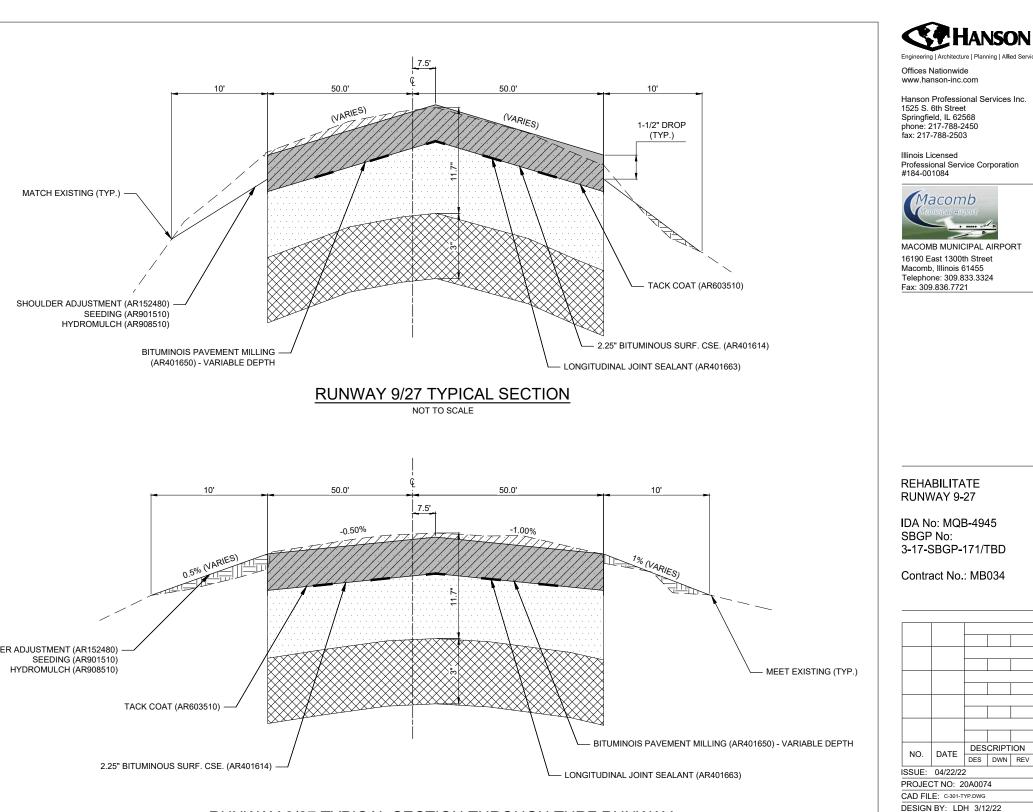
SWPPP NOTES & DETAILS



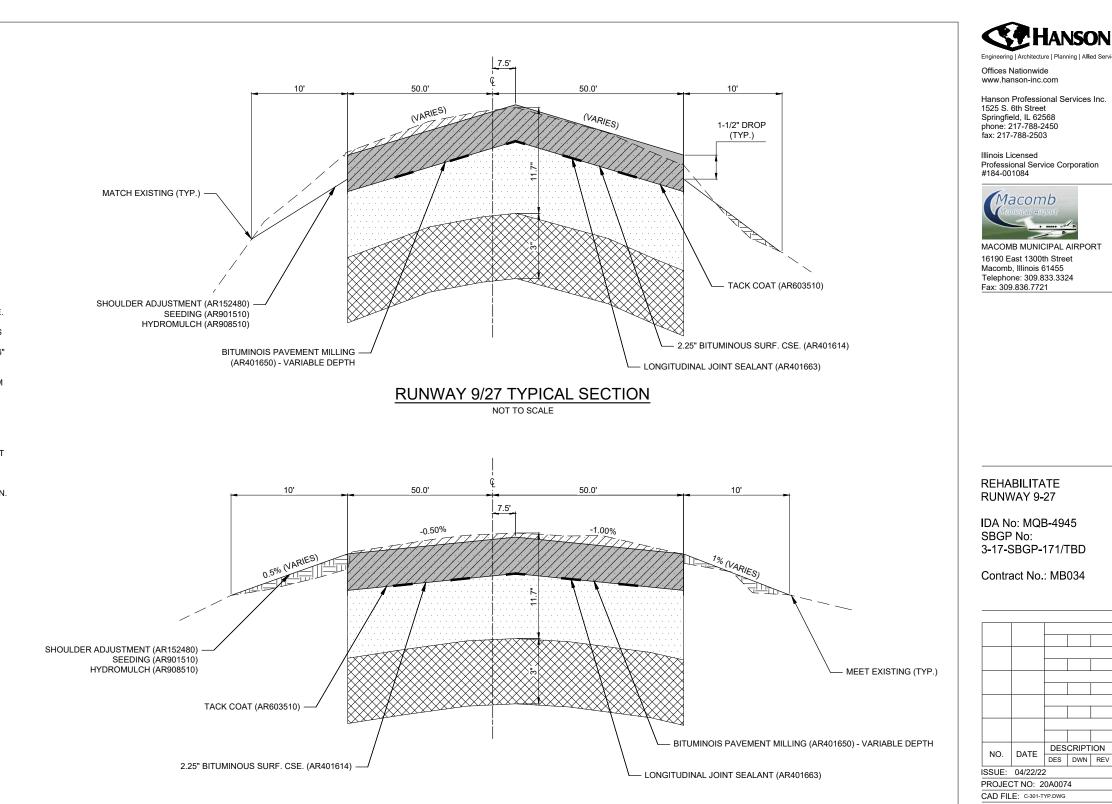
NOT TO SCALE

AR401640 - BITUMINOUS PAVEMENT GROOVING

- 1. THE PROPOSED GROOVES WILL BE CUT PERPENDICULAR TO THE RUNWAY CENTERLINE AND WILL TERMINATE 10' FROM THE RUNWAY PAVEMENT EDGE.
- 2. THE GROOVES WILL BE 1/4" WIDE, 1/4" DEEP AND PLACED ON 1 1/2" CENTERS
- 3. TOLERANCES ON THE GROOVING WILL BE 1 1/2" IN ALIGNMENT FOR 75', ±1/16" IN DEPTH, +1/16" IN WIDTH, -1/8" IN GROOVE SPACING.
- 4. GROOVES SHALL BE SAWED NO LESS THAN 6" AND NO MORE THAN 18" FROM AN IN-PAVEMENT LIGHT.
- 5. GROOVES SHALL NOT BE CLOSER THAN 3" OR MORE THAN 9" FROM TRANSVERSE JOINTS IN CONCRETE PAVEMENTS.
- 6. ALL SOLID MATERIAL GENERATED FROM GROOVING OPERATIONS WILL BE PICKED UP AND DISPOSED OF AT AN APPROVED LAND FILL OFF THE AIRPORT SITE, AT THE CONTRACTOR'S OWN EXPENSE.
- 7. AT THE END OF EACH WORK DAY THE RUNWAY WILL BE FLUSHED, CLEANED AND CLEARED OF ALL MATERIAL GENERATED BY THE GROOVING OPERATION.



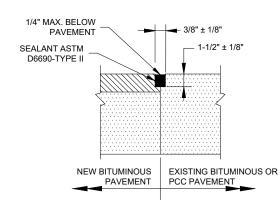




RUNWAY 9/27 TYPICAL SECTION THROUGH TURF RUNWAY

NOT TO SCALE

EXISTING PROPOSED		LEGEND
		BIT. SURF. CSEMETHOD I, SUPERPAVE
		BIT. PAVEMENT MILLING (VARIABLE)
		SHOULDER ADJUSTMENT (FILL)
		BITUMINOUS PAVEMENT
		CRUSHED AGGREGATE



NOTE: ALL BITUMINOUS/BITUMINOUS JOINT SEALING TO BE PAID UNDER SAW AND SEAL BITUMINOUS JOINTS, ITEM AR401660.

BITUMINOUS/BITUMINOUS SEAL



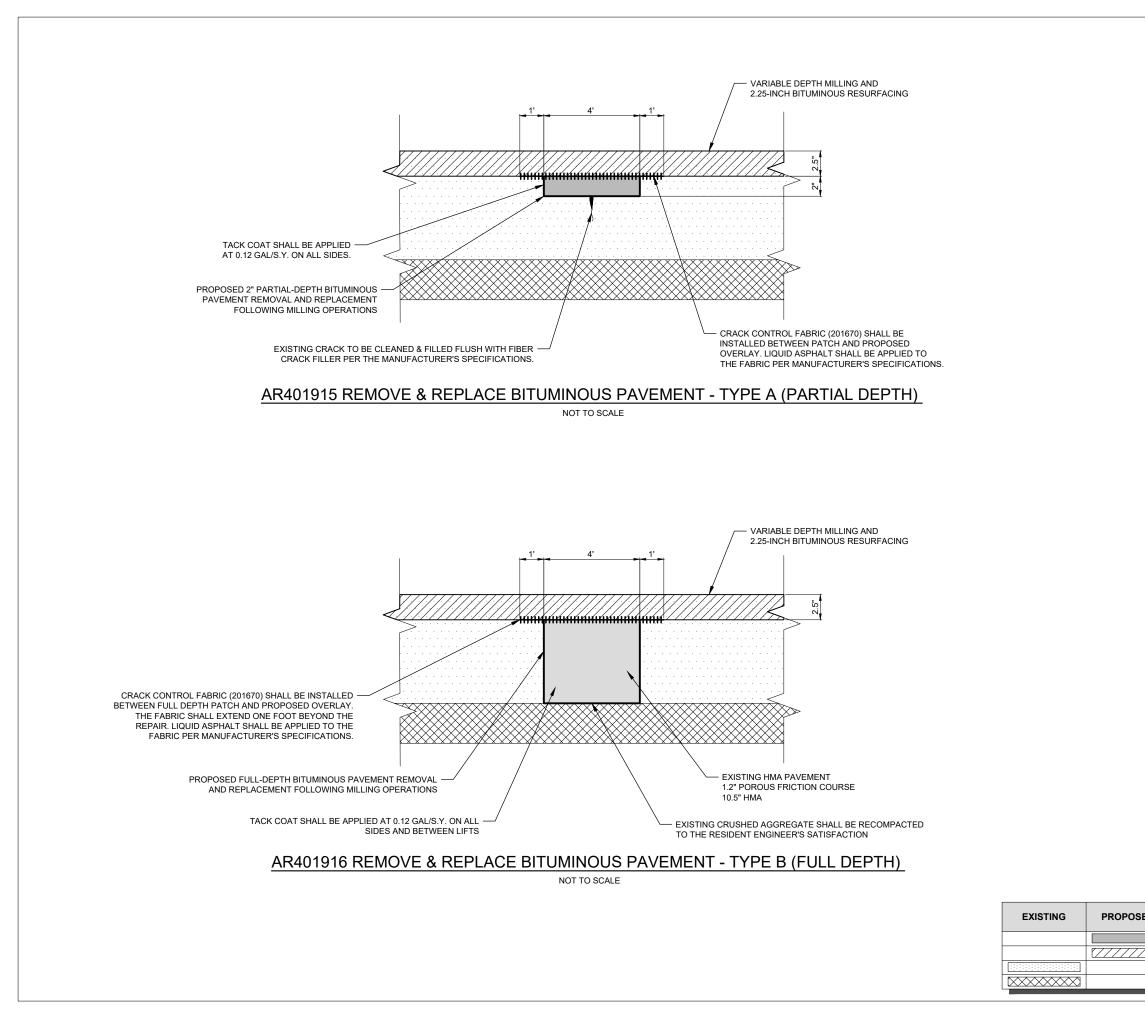
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DRAWN BY: LDH 3/12/22

SHEET TITLE

REVIEWED BY: KD 03/22/22

TYPICAL SECTIONS



ED	LEGEND
	BIT. PAVEMENT
\mathbb{Z}	BIT. PAVEMENT MILLING (VARIABLE)
	BITUMINOUS PAVEMENT
	CRUSHED AGGREGATE

PAVEMENT PATCHING DETAILS

NO.	DATE	DES	CRIPT	ION		
NO.	NO. DATE		DWN	REV		
ISSUE:	ISSUE: 04/22/22					
PROJECT NO: 20A0074						

CAD FILE: C-301-TYP.DWG

SHEET TITLE

DESIGN BY: LDH 3/12/22 DRAWN BY: LDH 3/12/22

REVIEWED BY: KD 03/22/22

3-17-SBGP-171/TBD Contract No.: MB034

REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No:

Illinois Licensed Professional Service Corporation #184-001084

MACOMB MUNICIPAL AIRPORT



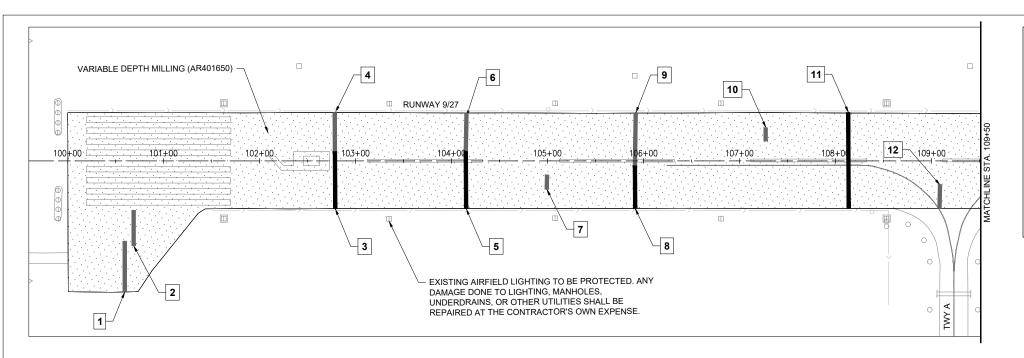
16190 East 1300th Street

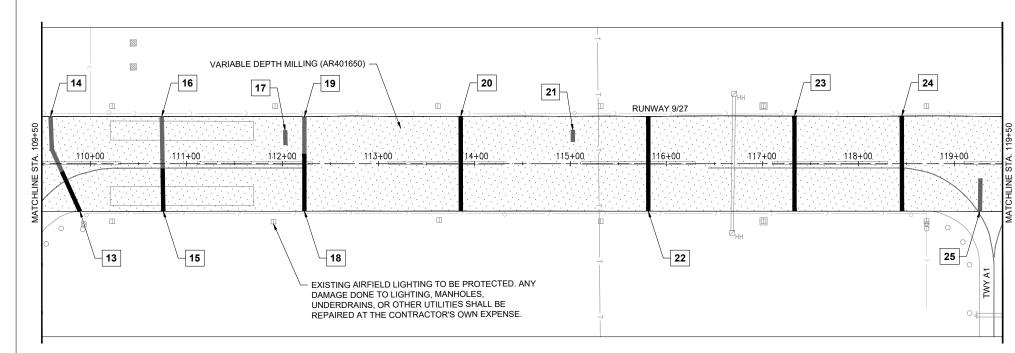
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EXISTING	PROPOSED	LEGEND
		AR401650 - BITUMINOUS PAVEMENT MILLING (VARIABLE) -
		REMOVE AND REPLACE BIT. PAVEMENT - FULL DEPTH
		REMOVE AND REPLACE BIT. PAVEMENT - PARTIAL DEPTH
		AIRPORT PROPERTY LINE
X		AIRPORT FENCE (AIRFIELD)
——————————————————————————————————————		UNDERGROUND ELECTRIC LINE
T		TELEPHONE/COMMUNICATION LINE
		UNDERDRAIN
0		MANHOLE
		AIRFIELD LIGHTING

ELECTRICAL NOTES

- LIGHTING SYSTEM SHALL BE SHUT OFF.
- 3. RESIDENT ENGINEER/TECHNICIAN.

NOTES

BITUMINOUS PAVEMENT MILLING

2

- 2.

1. THE CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS. THE CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, WORKING ON, RELOCATING, RECONNECTING, AND/OR INSTALLING THE RESPECTIVE AIRFIELD LIGHTING, TAXI SIGN, NAVAID, OR OTHER DEVICES. CONTRACTOR SHALL REPORT ANY VARIATIONS, DEFICIENCIES, AND/OR APPARENT SAFETY CONCERNS TO THE RESIDENT ENGINEER/TECHNICIAN.

2. WHEN A RUNWAY IS CLOSED THE RESPECTIVE LIGHTING AND NAVAIDS SHALL BE SHUT OFF. WHEN A TAXIWAY IS CLOSED THE RESPECTIVE

ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND

CLEAN & SEAL BITUMINOUS CRACKS

A NOMINAL QUANTITY OF AR201661 "CLEAN AND SEAL BITUMINOUS CRACKS" HAS BEEN INCLUDED AND WILL BE LAID OUT BY THE RESIDENT ENGINEER FOLLOWING PAVEMENT MILLING. THE NOMINAL QUANTITY WAS DETERMINED BY A FIELD SURVEY (CONDUCTED ON 02/22/22) AND ASSUMES THAT 50% OF SURVEYED CRACKS WILL BE ELIMINATED AFTER THE COMPLETION OF MILLING.

CONTRACTOR SHALL PROTECT ALL ADJACENT PAVEMENT EDGES DURING MILLING AND PAVING OPERATIONS. ANY DAMAGE DONE TO EXISTING PAVEMENT OR MARKINGS SHALL BE REPAIRED OR REMARKED AT THE CONTRACTOR'S EXPENSE

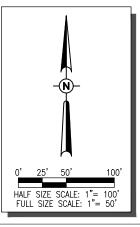
ALL MILLED MATERIAL SHALL BE HAULED OFFSITE UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN. MILLED MATERIAL MAY BE USED TO MAKE REPAIRS TO THE HAUL ROUTE AS NEEDED AND APPROVED BY THE RESIDENT ENGINEER.

REMOVE & REPLACE BITUMINOUS PAVEMENT

SEE DETAILS SHEET FOR PATCHING. PATCHING LOCATIONS ARE APPROXIMATE AND WILL BE CONFIRMED AND LAID OUT BY THE RESIDENT ENGINEER/TECHNICIAN FOLLOWING PAVEMENT MILLING.

ALL PATCHES INDICATED AS PARTIAL (TYPE A) OR FULL DEPTH (TYPE B) WERE DETERMINED DURING A FIELD SURVEY AND PRIOR TO MILLING. FOLLOWING MILLING, AND UPON INSPECTION BY RESIDENT ENGINEER/TECHNICIAN, PARTIAL PATCHES MAY BE CONVERTED TO TYPE B (FULL-DEPTH) PATCH TO BE MEASURED AND PAID FOR UNDER PAY ITEM AR401916 OR REMOVED.







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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

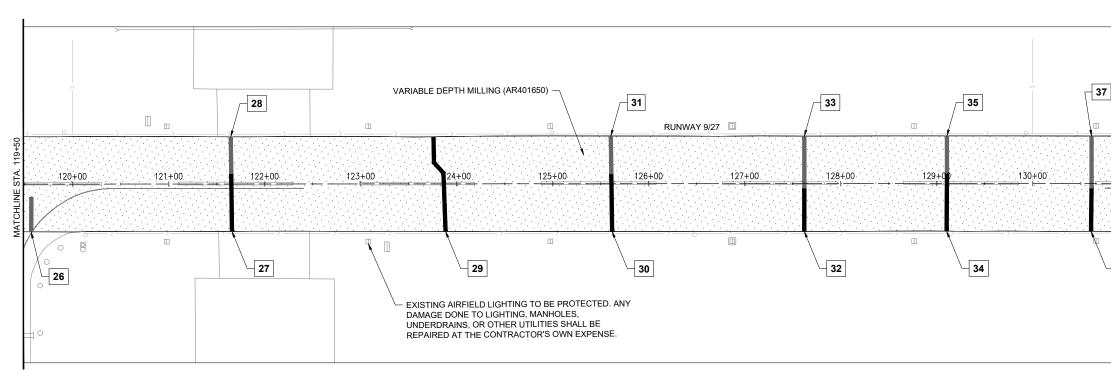
Contract No.: MB034

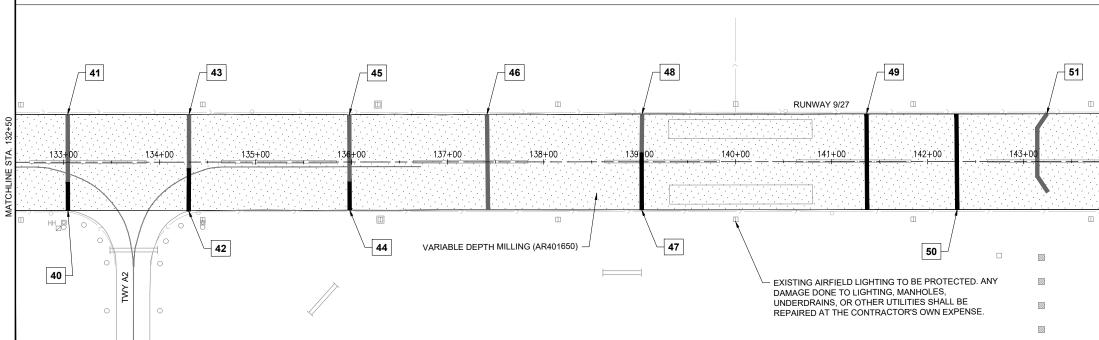
NO.	DATE	DES	CRIPT	ION
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ISSUE:	04/22/22	2		
PROJEC	CT NO: 2	0A007	4	

CAD FILE: C-101-PREP.DWG DESIGN BY: 1 DH 3/12/22 DRAWN BY: LDH 3/12/22 REVIEWED BY: KD 03/22/22

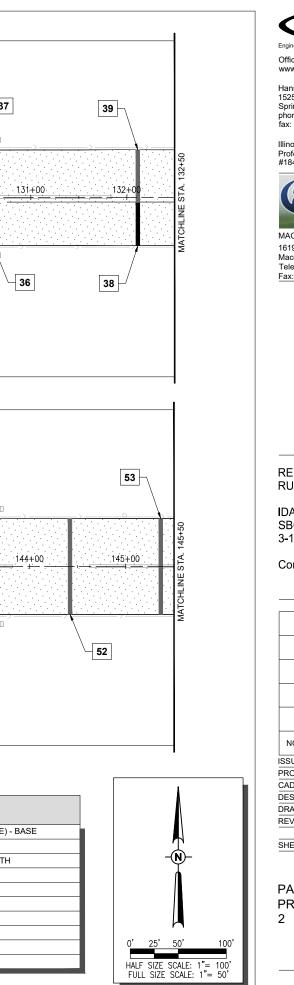
SHEET TITLE

PAVEMENT **PREPARATION PLAN -**





EXISTING	PROPOSED	LEGEND
		AR401650 - BITUMINOUS PAVEMENT MILLING (VARIABLE)
		REMOVE AND REPLACE BIT. PAVEMENT - FULL DEPTH
		REMOVE AND REPLACE BIT. PAVEMENT - PARTIAL DEPTH
		AIRPORT PROPERTY LINE
X		AIRPORT FENCE (AIRFIELD)
——————————————————————————————————————		UNDERGROUND ELECTRIC LINE
T		TELEPHONE/COMMUNICATION LINE
UD->		UNDERDRAIN
0		MANHOLE
		AIRFIELD LIGHTING





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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

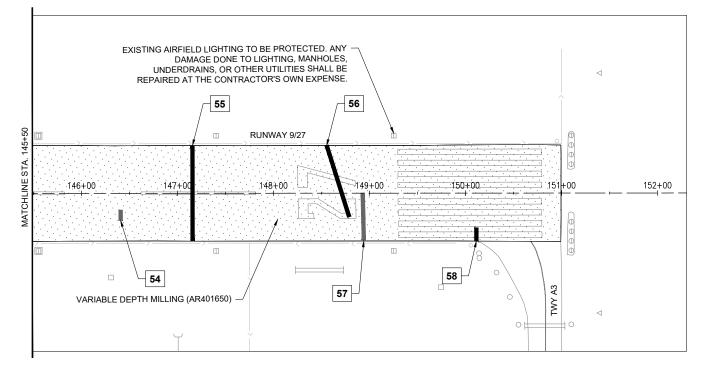
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NO.		DES	CRIPT	ION
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ISSUE:	04/22/22	2		
PROJEC	CT NO: 2	0A007	4	
CAD FIL	E: C-101-P	REP.DW0	G	
DESIGN	BY: LD	H 3/12	2/22	
DRAWN	BY: LDI	H 3/12	2/22	
REVIEW	ED BY:	KD 0	3/22/2	2

SHEET TITLE

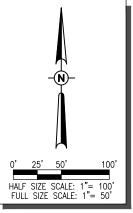
PAVEMENT PREPARATION PLAN -2

			PATCHING	SCHEDULE		1				
CRACK NO.	STATION (APPROXIMATE)	РАТСН ТҮРЕ	PATCHING LENGTH (FT)	PATCHING WIDTH (FT)	TYPE A TOTAL (AR401915) (SY)	TYPE B TOTAL (AR401916) (SY)	CRACK CONTROL FABRIC TOTA (AR201670) (SY)			
1	100+60	A	53	4	23.6		35.3			
2	100+69	A	37	4	16.4					
3	102+78	В	60	4		26.7				
4	102+78	A	40	4	17.8					
5	104+16	B	60	4		26.7				
6	104+16	A	40	4	17.8	20.1				
7	105+00	A	15	4	6.7					
8	105+92	В	45	4	0.1	20.0				
9	105+92	A	55	4	24.4	20.0				
10	107+27	A	14	4	6.2					
10		B	14	4	0.2	44.4				
	108+13				44.4	44.4				
12	109+09	<u>A</u>	25	4	11.1	00.0				
13	109+71	В	45	4		20.0				
14	109+59	A	60	4	26.7					
15	110+74	В	45	4		20.0				
16	110+74	A	55	4	24.4					
17	112+03	Α	15	4	6.7		10.0			
18	112+23	В	60	4		26.7	40.0			
19	112+23	Α	40	4	17.8		26.7			
20	113+86	В	100	4		44.4	66.7			
21	115+03	А	12	40 4 100 4			8.0			
22	115+81	В	100	4		44.4	66.7			
23	117+34	В	100	4		44.4	66.7			
24	118+45	В	100	4		44.4	66.7			
25	119+27	А	34	4	15.1		22.7			
26	119+57	А	36	4	16.0		24.0			
27	121+65	В	60	4		26.7	40.0			
28	121+65	А	40	4	17.8		26.7			
29	123+81	В	102	4		45.3	68.0			
30	125+60	В	60	4		26.7	40.0			
31	125+60	A	40	4	17.8		26.7			
32	127+62	В	45	4		20.0				
33	127+62	A	55	4	24.4					
34	129+11	В	60	4		26.7				
35	129+11	A	40	4	17.8					
36	130+61	В	45	4		20.0				
37	130+61	A	55	4	24.4					
38	132+11	В	45	4		20.0				
39	132+11	A	55	4	24.4	10.0	FABRIC TOT (AR201670 (SY) 35.3 24.7 40.0 26.7 40.0 26.7 10.0 30.0 36.7 9.3 66.7 16.7 30.0 40.0 36.7 9.3 66.7 16.7 30.0 40.0 30.0 40.0 26.7 66.7 16.7 30.0 36.7 9.3 36.7 22.7 24.0 40.0 26.7 66.7 22.7 24.0 40.0 26.7 60.7 22.7 24.0 40.0 26.7 30.0 36.7 20.0 46.7 30.0			
40	133+05	В	30	4	04.4	13.3				
41	133+05	A B	70	4	31.1	20.0				
42	134+31		45	4	04.4	20.0				
43	134+31	A B	55 30	4 4	24.4	12.2				
44 45	135+98 135+98	A	70	4	31.1	20.0 30.0 36.7 13.3 20.0				
45	137+41	A	100	4	44.4					
40	139+03	В	60	4		26.7				
48	139+03	A	40	4	17.8	20.7				
49	141+36	В	100	4	11.0	44.4				
50	142+30	В	100	4		44.4				
51	143+14	A	88	4	39.1					
52	144+42	A	100	4	44.4					
53	145+37	A	100	4	44.4					
54	146+41	A	11	4	4.9					
55	147+15	В	100	4		44.4				
56	148+67	B	78	4		34.7				
57	148+94	A	50	4	22.2		33.3			
58	150+11	В	14	4	1	6.2	9.3			

EXISTING	PROPOSED	LEGEND
		AR401650 - BITUMINOUS PAVEMENT MILLING (VARIABLE)
		REMOVE AND REPLACE BIT. PAVEMENT - FULL DEPTH
		REMOVE AND REPLACE BIT. PAVEMENT - PARTIAL DEPTH
		AIRPORT PROPERTY LINE
X		AIRPORT FENCE (AIRFIELD)
——— E ———		UNDERGROUND ELECTRIC LINE
T		TELEPHONE/COMMUNICATION LINE
UD->		UNDERDRAIN
0		MANHOLE
		AIRFIELD LIGHTING









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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

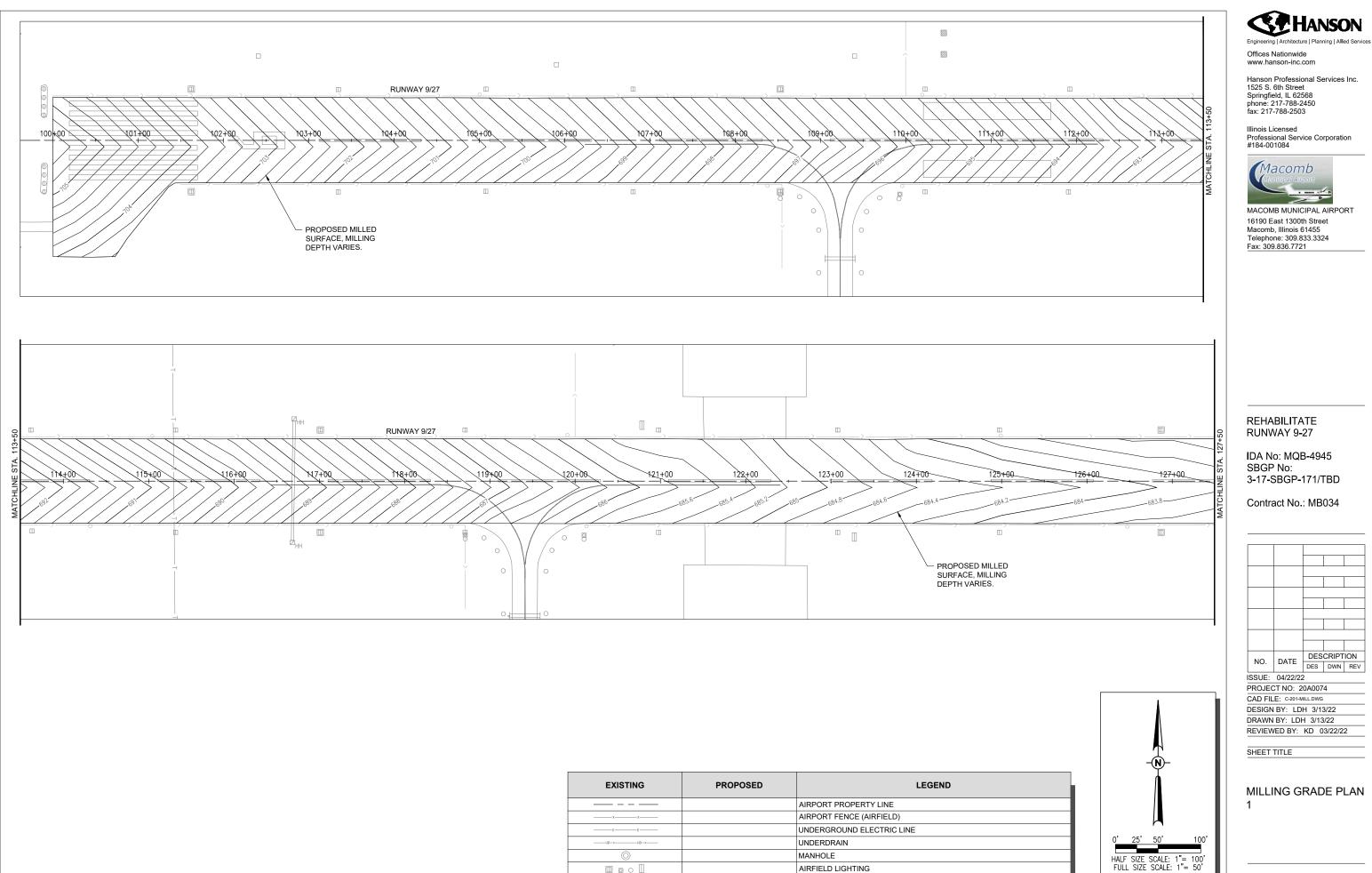
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PROJEC	CT NO: 2	0A007	4	
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 LDH
 3/12/22

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 KD
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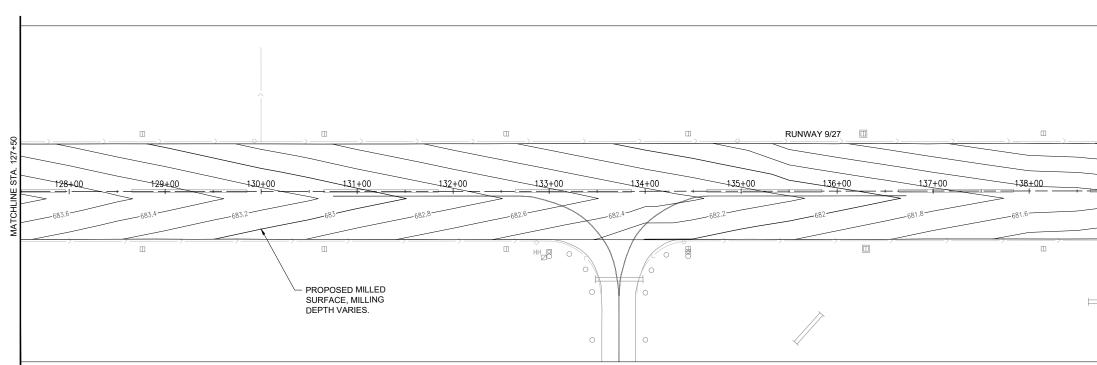
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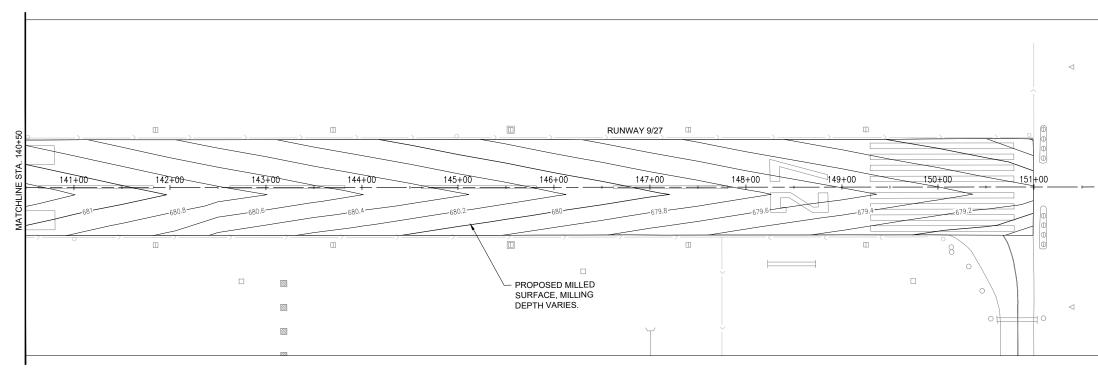
PAVEMENT PREPARATION PLAN -3

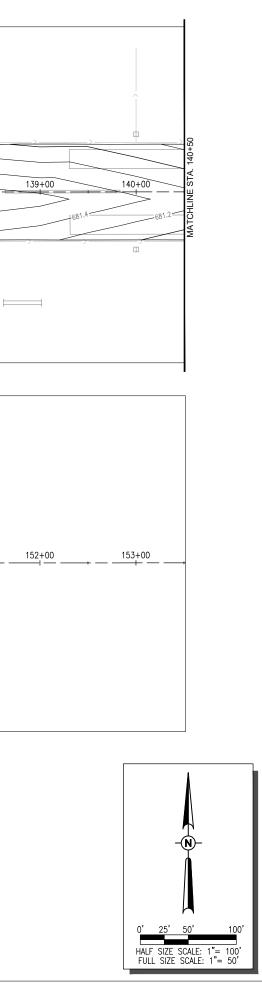


EXISTING	PROPOSED	LEGEND
		AIRPORT PROPERTY LINE
xx		AIRPORT FENCE (AIRFIELD)
εε		UNDERGROUND ELECTRIC LINE
		UNDERDRAIN
\bigcirc		MANHOLE
		AIRFIELD LIGHTING

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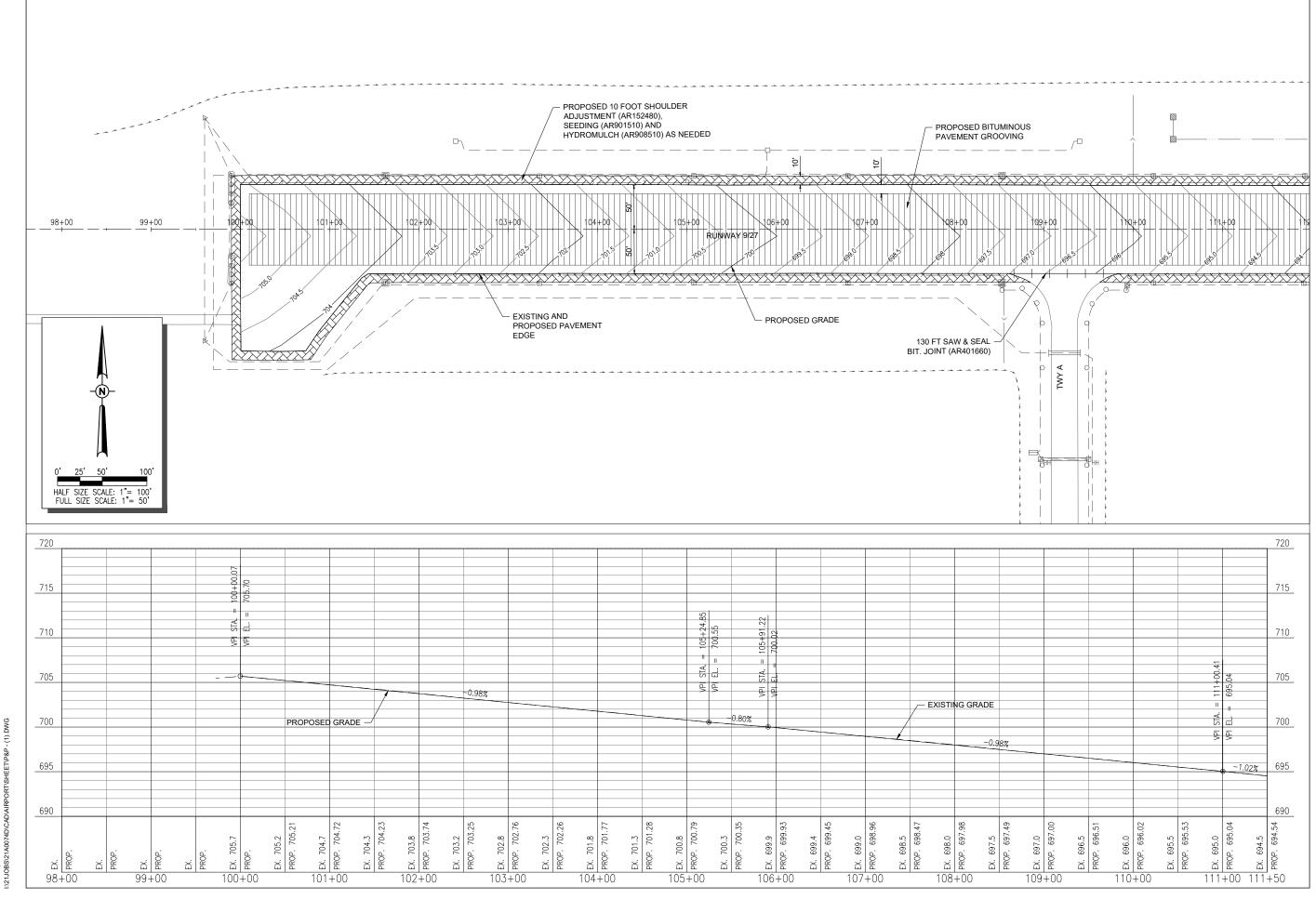
REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034



MILLING GRADE PLAN 2



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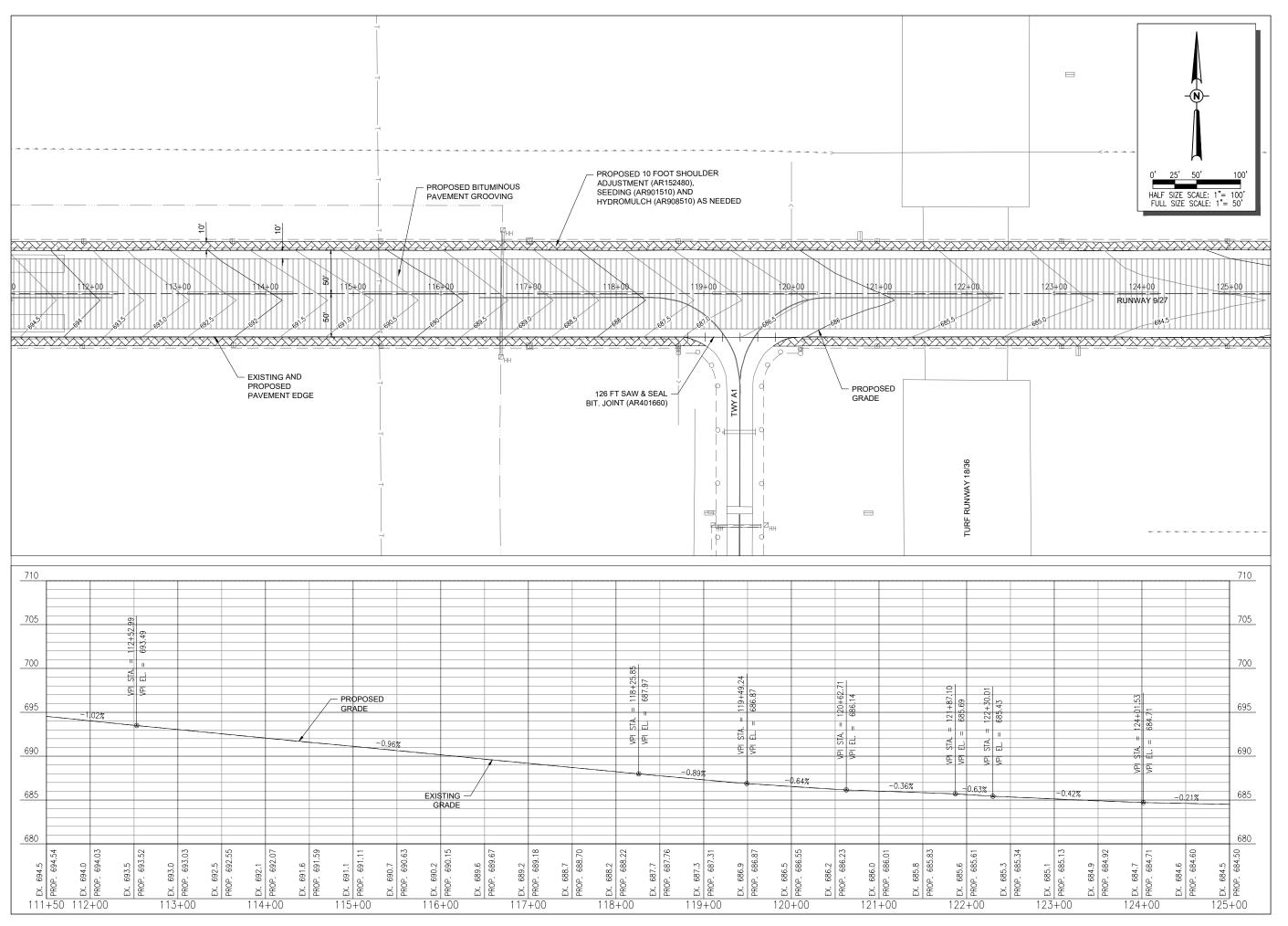
IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

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DESIGN	BY: LD	H 3/1:	3/22	
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PLAN & PROFILE 1 -RUNWAY 9-27



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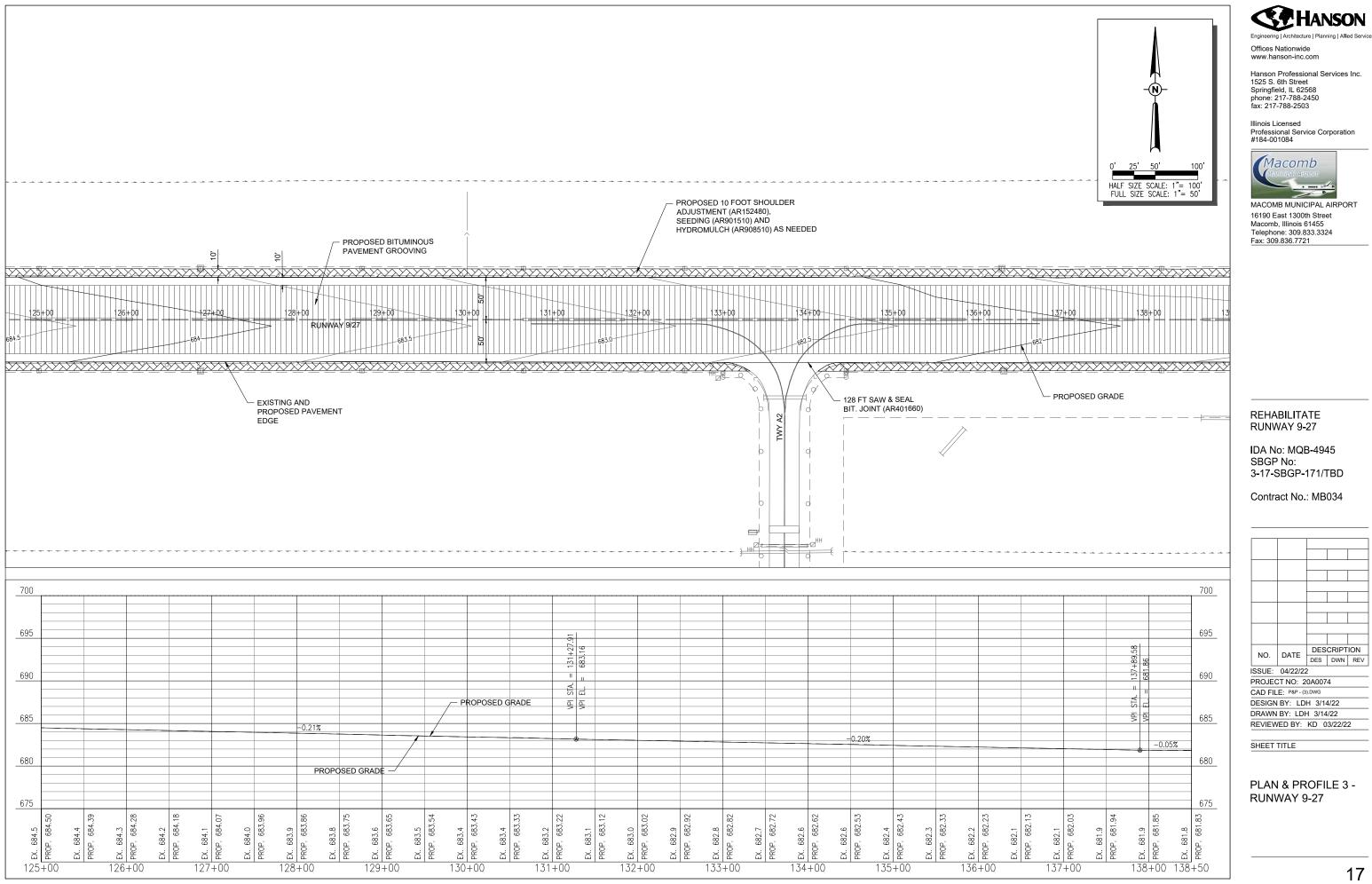
REHABILITATE RUNWAY 9-27

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Contract No.: MB034

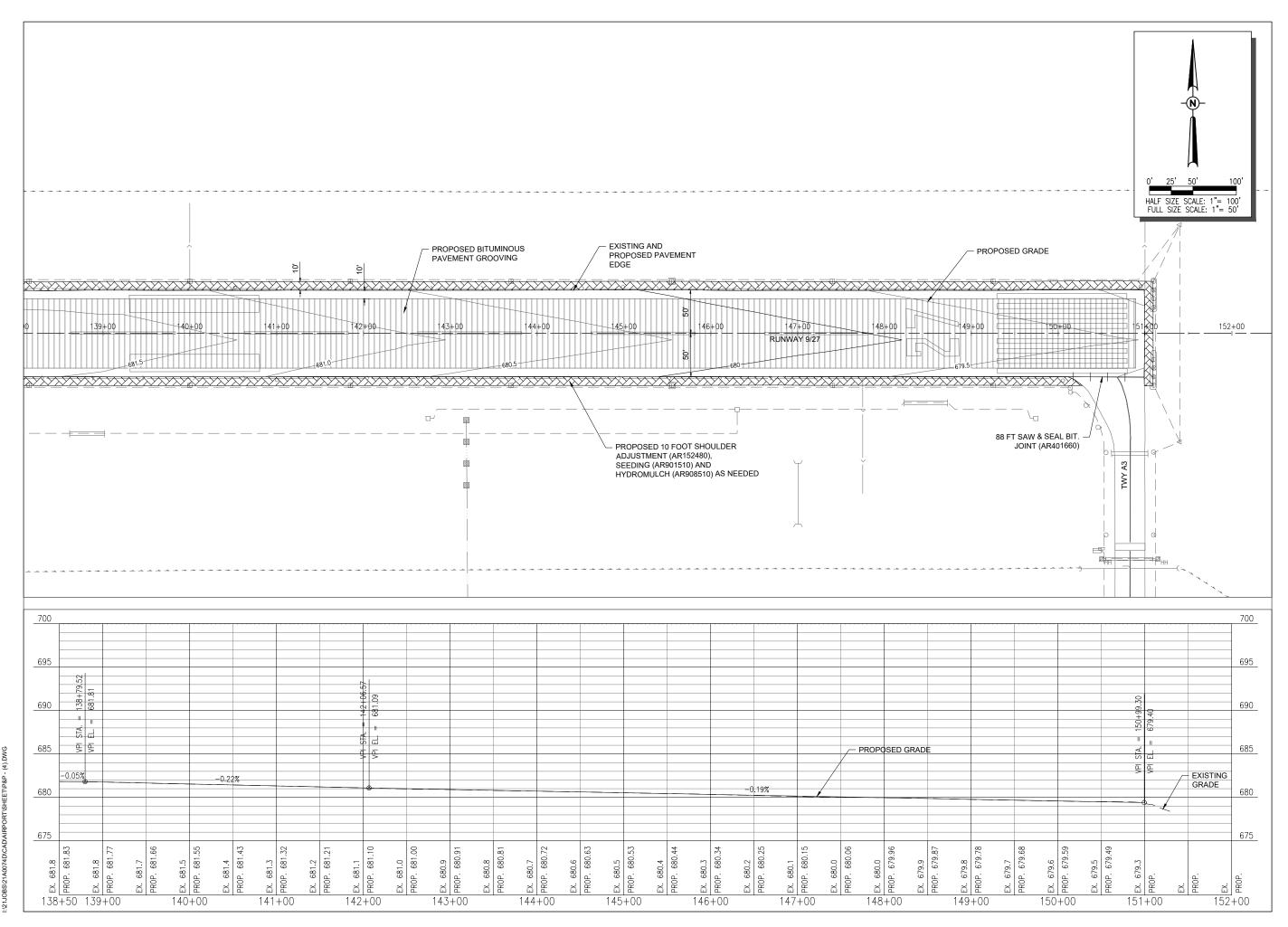
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PLAN & PROFILE 2 -RUNWAY 9-27



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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

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PLAN & PROFILE 4 -RUNWAY 9-27

STA. 102+00	706	<u>703:</u> 50.1	4	703 0.00			703. 7.50 -1.00%	R	703.39 49.64	70 9 R 70	6			ST/ 104-	4. ⊦50	704 702 700	<u>700</u> 50.).78 11 L	0.0			701	R	700.83 49.57 (704 702 702 700	ST 107	A. +00	702 700 698	<u>698.4</u> 50.28	6 L	<u>698.9</u> 0.00			699.04 7.50 R	8 6	698.62 49.39 R	702 700 698
	702					0	20	40 6		70 70 0 100	0					698 696		-80 -	60 -4	40 -20	0	20	40 6	50 80	698 696 100			696 694		80 -60	0 -40	-20	0 2	20 40	60		696 694 100
STA. 101+50	704	703.2			1.00%			R	-703.88 49.82	R 70 70 70	6 4 2 0			ST/ 104-	A. ⊢OO	700		.27 99 L 				701. 7.50 -1.25% 20	R	701.32 49.45 f	^२ 702 700 698	ST 106-	A. +50	698 696 694	<u>698.9</u> 49.93			.00%		699.52 7.50 R 1.00%		599.11 19.29 R	702 700 698 696 696 694 100
STA. 101+00	704 702 700	, <u>704.2</u> 50.24			1.00%	0) R -1.009		0 100	703.8 99.12		708 706 704 702 700 698 80	ST/ 103-	A. - 50	702 700 698	701	24 L	0.0			702. 7.50 - 1.25% 20	R	701.81 49.47 F	702 700 698	ST106-	A. +00	700 698 696	<u>699.4</u> 49.84	L		.00%		700,01 7.50 R -1.25% 20 40		699.48 19.39 R	704 702 700 698 696 100
STA. 100+50	704	704.7			1.00%			R	-1.09%	0 100	120	03.88 36.63 F	704 702 700	ST, 103-	A. + O O	702 700 698	702.	2 L \		1.00%		702. 7.50 	R	702.41 49.62	702 702 700 698	ST 105		700 698 696	<u>699.8</u> 49.60	5) L		.00%			69 49	9.50 R	704 702 700 698 696 100
STA. 100+00	704	705.0			1.25%			R		R 1.34%	120	7 <u>04.22</u> 36.17	R 704 702 700	ST/ 102-		702 700 698	<u>702</u> 50.:			1.00%		703. 7.50 1.002 20	R	702.90 49.81	704 702 700 698	ST 105	A. +00	700 698 696	<u>700.2</u> 49.90			.00%		700.86 7.50 R -1.25% 20 40	- 7(700.34 19:50 R 80	704 702 700 698 696 100

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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

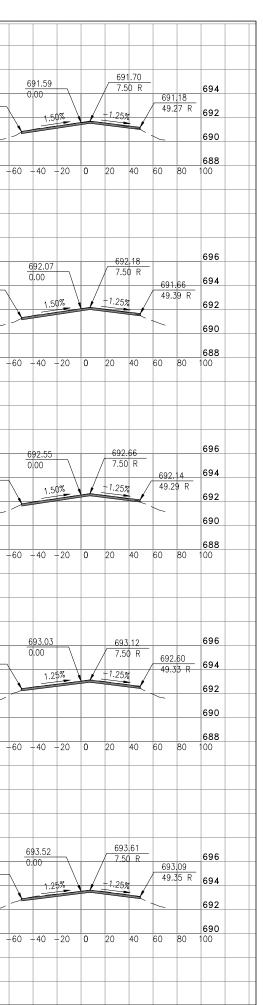
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DRAWN	BY: LDI	H 3/14	/22	
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SHEET TITLE

CROSS SECTIONS 1 -RUNWAY 9-27

STA. 109+50	694 - 692 -	51 696.59 700 7.50 7.50 696.06 49.47 696 1.00% 1.26% 696 696 696 694 692 692 0 -20 0 20 40 60 80 100	STA. 698 112+00 696 693.54 694 694.03 0,00 694 49.17 L 1.0 692 690	7.50 R 693.69 696 49.43 R 694 692 690	STA. 694 114+50 694 690.84 690 690 690 690 690 690 688 100 -80
Image: State of the second	696 - 694 - 692 -	.00 697.08 700 0 7.50 R 696.52 1.00% -1.34% 696 1.00% -1.34% 696 696 694 692 692 0 -20 0 20 40 60 80 100	STA. 698 111+50 696 694 694 694 1.0 692 690 100 -80	7.50 R 694.20 696 49.45 R 694 694 692 690	STA, 696 114+00 694 692 692 690 - 690 - 690 - 690 - 690 - 690 - 690 - 690 - 690 - 690 - 690 - 690 -
STA: 108+50	696 -	.49 .697.56 700 0 7.50 R 697.15 1.00% 49.42 R 698 1.00% 696 696 694 692 692 0 -20 0 20 40 60 80 100	698 698 698 694 694 694 694 694 694 694 694	7.50 R 694.70 49.45 R 696 694 694 692 690	696 STA, 13+50 694 691.81 49.40 L 692 690 690 688 -100 -80
STA. 108+00	696 -		694 692 690	695.61 698 7.50 695.19 49.47 696 1.00% 694 694 692 690 690 -20 0 20 40	STA. 696 113+00 694 692 692 690 690 688 -100 - 80
STA. STA. 107+50	696	.47 698.54 702 .47 698.54 700 1.00% -1.00% 49.42 R 698 698 698 696 694 60 0 -20 0 20 40 60 80 100	694	696.09 700 7.50 R 695.68 698 00% -1.00% 49.47 R 696 694 694 692 -20 0 20 40 60 80 100	STA. 696 112+50 696 692 692 692 690 690 -100

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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

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117+0	00	690 <u>688</u> 49.7	. <u>56</u> '4 L		5%	-1.25%	49	.39 R		119+	50 6	88 <u>686.3</u> 49.35		0.00 1.0	00%	-1.16		686.46 49.67 R 688	122	+00	00	35.36	<u>685.61</u> 0.00		685.65 7.50 R	685	23
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												90		687.31 0.00		687.	R	690									
STA. 116+5	50	692		689.67 0.00		689.76 7.50 R	68	0.24	692	STA 119+	00 6	88 <u>686.6</u> 49.73	9		5%	1.65		686.71 49.49 R 688	ST 121	A. +50	688 <u>68</u>	5.58	<u>685.83</u>		685.87 7.50 R	685	45
		690 <u>689</u> . 49.8	9 L	1.25	7%	-1.25%	49	.38 R	690		6	86						- 686			686 49	.22 L	0.5	50%	-1.00%	49.4	5 R
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		694							694												690						
STA. 116+0	50	692 689	.53	690.15 0.00		690.24 7.50 R			692	STA 118+	50 6	90 687.1		687.76 0.00		687	1 R	690	ST 121	A. +00	688 6	85.64	686.01 0.00	$ \downarrow $	686.07 7.50 R		.64
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REHABILITATE RUNWAY 9-27

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Contract No.: MB034

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NO.	DATE	DES	DWN	REV
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DRAWN	BY: LDH	H 3/14	/22	
REVIEW	/ED BY:	KD 0	3/22/2	2

SHEET TITLE

CROSS SECTIONS 3 -RUNWAY 9-27

STA. 124+50	684 682 680	0.00	684.68 688 7.50 684.25 684.25 686 .002 684 682 682 680 60 0 40 60 80 100 100	STA. 127+00	688 686 <u>683,57</u> 684 682 680 - 100 - 80 - 6	<u>684.07</u> 0.00 <u>1.00%</u> 0 -40 -20	684.14 7.50 R -1.002 0 20 40	683.72 49.60 R 684 682 680	STA. 129+50	686 683.54 684 683.04 684 49.62 L 1.00% 682 680 -100 - 80 - 60 - 40 - 20		R 684 682
STA. 124+00	49.40 L 684 682 680	0.00	684.79 688 7.50 684.37 686 49.36 684 682 680 680 600	STA. 126+50	688 686 <u>683.56</u> 684 682 680 - 100 - 80 - 6	<u>684.18</u> 0.00 1.25% 0 -40 -20	684.27 7.50 R -1.25% 0 20 40	683.75 686 49.65 R 684 682 682 680	STA. 129+00	683.15	683.72 7.50 R 683.31 49.51 0 20 40 60 80	R 684 682 680 680 680 680 680 680 680 680 680 680
STA. 123+50	684 682 680	0.00	384.98 688 7.50 R 684.56 686 .002 49.36 R 684 684 682 682 680 0040 60 80	STA. 126+00	688 686 <u>683 66</u> 684 682 680 -100 -80 -6	<u>684.28</u> 0.00 <u>1.25%</u> 0 -40 -20		683.85 49.74 R 684 682 680	STA. 128+50		683.82 7.50 R 1.002 49.54 0 20 40 60 80	R 684 682 680 680 680 680 680 680 680 680 680 680
STA. 123+00	686 <u>684.88</u> <u>49.37 L</u> 684		684	STA. 125+50	688 686 <u>683.77</u> 49.78 L 684 682 680 - 100 - 80 - 6	<u>684.39</u> 000 <u>1.25%</u> 0 -40 -20		683 96 49.79 R 684 682 680	STA. 128+00	686 683.86 684 0.00 684 49.54 1.00% 682 - - 680 - - -	683,94 7.50 R -1.002 49.75 0 20 40 60 80	R 684 682
STA. 122+50	685.09 686 49.45 L 684 682		684	STA. 125+00	688 686 <u>683.88</u> 684 682	<u>684.50</u> 0.00 <u>1.25%</u> 0 -40 -20		684.07 49.69 R 684 682 680	STA. 127+50	688 686 686 683.46 684 49.58 1.00% 682 680 -100 -80 -100	684.04 7.50 R 683.6 49.69 0 20 40 60 80	R 684 682 680 680 680 680 680 680 680 680 680 680

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NO.	DATE	DES	CRIPT	ION
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ISSUE:	04/22/22	2		
PROJEC	CT NO: 2	0A007	4	
CAD FIL	E: C-401-X	SECT.DV	/G	
DESIGN	BY: LD	H 3/14	4/22	
DRAWN	BY: LDH	H 3/14	/22	
REVIEW	/ED BY:	KD 0	3/22/2	2

SHEET TITLE

CROSS SECTIONS 4 -RUNWAY 9-27

			683.09									
STA. 132+00	686 684 <u>682.52</u> 49.58 L	<u>683.02</u> 0.00 1.00%	7.50 R 68	686 32.67 0.36 R 684	STA. 134+50	686 684 <u>682.03</u> 49.85 L	<u>682.53</u> 0.00	682.61 7.50 R 682 682 682 682 682 682 682 682 682 682	686 .18 684 2 R	STA. 137+00	686 684 <u>681.42</u> 49.15 L	
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STA.	686	<u>683.12</u>	683.19 7.50 R	686	STA.	686	682.62	682.69	686	STA.	686	
131+50	684 <u>682.63</u> 49.41 L	1.00%	1 50	32.77 0.09 R 684	134+00	684 <u>682.12</u> 49.58 L	0.00	7.50 R 682.	6 R	136+50	684 <u>681.51</u> 49.74 L	\
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STA.	686	683.22 0.00	683,29 7.50 R 68	686 82.87	STA.	686	<u>682.72</u> 0.00	682.79	686	STA.	686	
131+00	684 49.40 L	1.00%	-1.00%	9.92 R 684	133+50	684 <u>682.22</u> 49.54 L	1.00%	002	47 R 684	136+00	684 681.61 49.30 L	\
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130+50	686 684 <u>682.84</u> 49.40 L	0.00	49	686 82.98 9.86 R 684	133+00	684 <u>682.32</u> 49.64 L 682	1.00%	-1.00% 50.5	684 682	135+50	684 <u>681.71</u> 49.51 L 682	
	682 49.40 L	1.00%	1.00%	682		680			680		680	>
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130+00	686 682.93 684 49.55 L	0.00		686 83.08 9.73 R 684	132+50	684 <u>49.54</u> L 682	1.00%	1.00%		135+00	684 <u>681.93</u> 49.58 L	$\overline{\backslash}$
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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

NO.	DATE	DES	CRIPT	ION
NO.	DATE	DES	DWN	REV
ISSUE:	04/22/22	2		
PROJEC	CT NO: 2	0A007	4	
CAD FIL	E: C-401-X	SECT.DV	/G	
DESIGN	BY: LD	H 3/14	4/22	
DRAWN	BY: LDI	H 3/14	/22	
REVIEW	/ED BY:	KD 0	3/22/2	2

SHEET TITLE

CROSS SECTIONS 5 -RUNWAY 9-27

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REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

NO.	DATE	DESCRIPTION					
NU.	DATE	DES	DWN	REV			
ISSUE: 04/22/22							
PROJECT NO: 20A0074							
CAD FILE: C-401-XSECT.DWG							
DESIGN BY: LDH 3/14/22							
DRAWN	BY: LDH	H 3/14	/22				
REVIEW	/ED BY:	KD 0	3/22/2	2			

SHEET TITLE

CROSS SECTIONS 6 -RUNWAY 9-27

STA. 147+00	684 680.15 680.22 682 679.65 7.50 R 680 1.00% 1.25% 678 676 676 -100 -80 -60 40 -20 0 20 40	<u>679,70</u> 682 145 49.77 R 680 678 676	TA. 682 679.68 0.00 680 679.18 680 678 678 678 676 -100 -80 -60 -40 -20	679.76 7.50 R 682 1.257 49.81 R 680 678 678 678 678 678 678 678 678	
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REHABILITATE RUNWAY 9-27

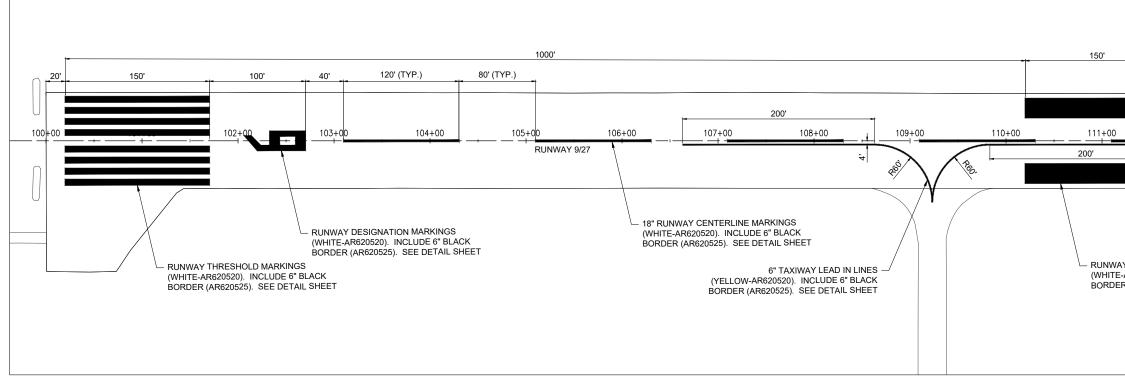
IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034

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NO.	DATE	DESCRIPTION					
NO.	DATE	DES	DWN	REV			
ISSUE:	ISSUE: 04/22/22						
PROJECT NO: 20A0074							
CAD FILE: C-401-XSECT.DWG							
DESIGN BY: LDH 3/14/22							
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SHEET TITLE

CROSS SECTIONS 7 -RUNWAY 9-27

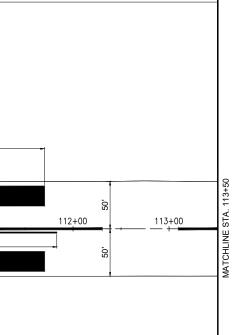


WATERBORNE PAVEMENT MARKING NOTES

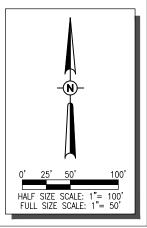
- A 30-DAY WAITING PERIOD IS REQUIRED BETWEEN THE PLACEMENT OF BITUMINOUS SURFACE COURSE AND THE FINAL APPLICATION OF WATERBORNE PAINT. AFTER THE PLACEMENT OF SURFACE COURSE AND PRIOR TO THE 30-DAY CURE PERIOD AN INITIAL COAT OF MARKINGS SHALL BE PLACED AT 50% OF THE NORMAL APPLICATION RATE. BLACK MARKINGS SHALL NOT BE PLACED DURING THE INITIAL COAT. THE REMAINDER OF THE MARKINGS, INCLUDING THE BLACK BORDERS, SHALL BE PLACED AFTER THE 30-DAY CURE PERIOD HAS ELAPSED AND GROOVING HAS BEEN COMPLETED. (IF ONLY GROOVING AND MARKING REMAIN, THE CALENDAR DAYS MAY BE SUSPENDED DURING THE 30 DAY CURE PERIOD.)
- 2. WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS AND WILL BE APPLIED IN <u>ONE APPLICATION</u>.
- 3. GLASS BEADS SHALL BE APPLIED ON THE SECOND COAT OF MARKING. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
- 4. CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR RE-MARKING ANY MARKINGS DAMAGED BY CONSTRUCTION ACTIVITIES THAT ARE OUTSIDE OF THE PROPOSED WORK LIMITS.

MADIZIN							
MARKING SCHEDULE AR620520 - PAVEMENT MARKING - WATERBORNE (WHITE)							
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)				
THRESHOLD MARKING	862.5	16	13,800.0				
DESIGNATION MARKING							
9	718.8	1	718.8				
2	651.5	1	651.5				
7	482.0	1	482.0				
AIMING POINT MARKING	3,000.0	4	12,000.0				
CENTERLINE MARKING	180.0	23	4,140.0				
AR620520 - PAVEMENT MARKING - WATERBORNE (YELLOW)							
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)				
6" TAXIWAY LEAD IN LINES	900.0	1	900.0				
AR620525 PAVEMENT N	ARKING - BLACK B	ORDER					
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)				
THRESHOLD MARKING	156.8	16	2,508.0				
DESIGNATION MARKING							
9	113.2	1	113.2				
2	106.3	1	106.3				
7	84.7	1	84.7				
AIMING POINT MARKING	171.0	4	684.0				
CENTERLINE MARKING	122.5	23	2,817.5				
6" TAXIWAY LEAD IN LINES	1,800.0	1	1,800.0				





- RUNWAY AIMING POINT MARKINGS (WHITE-AR620520). INCLUDE 6" BLACK BORDER (AR620525). SEE DETAIL SHEET





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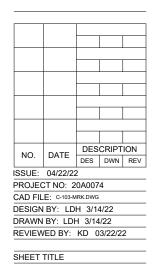


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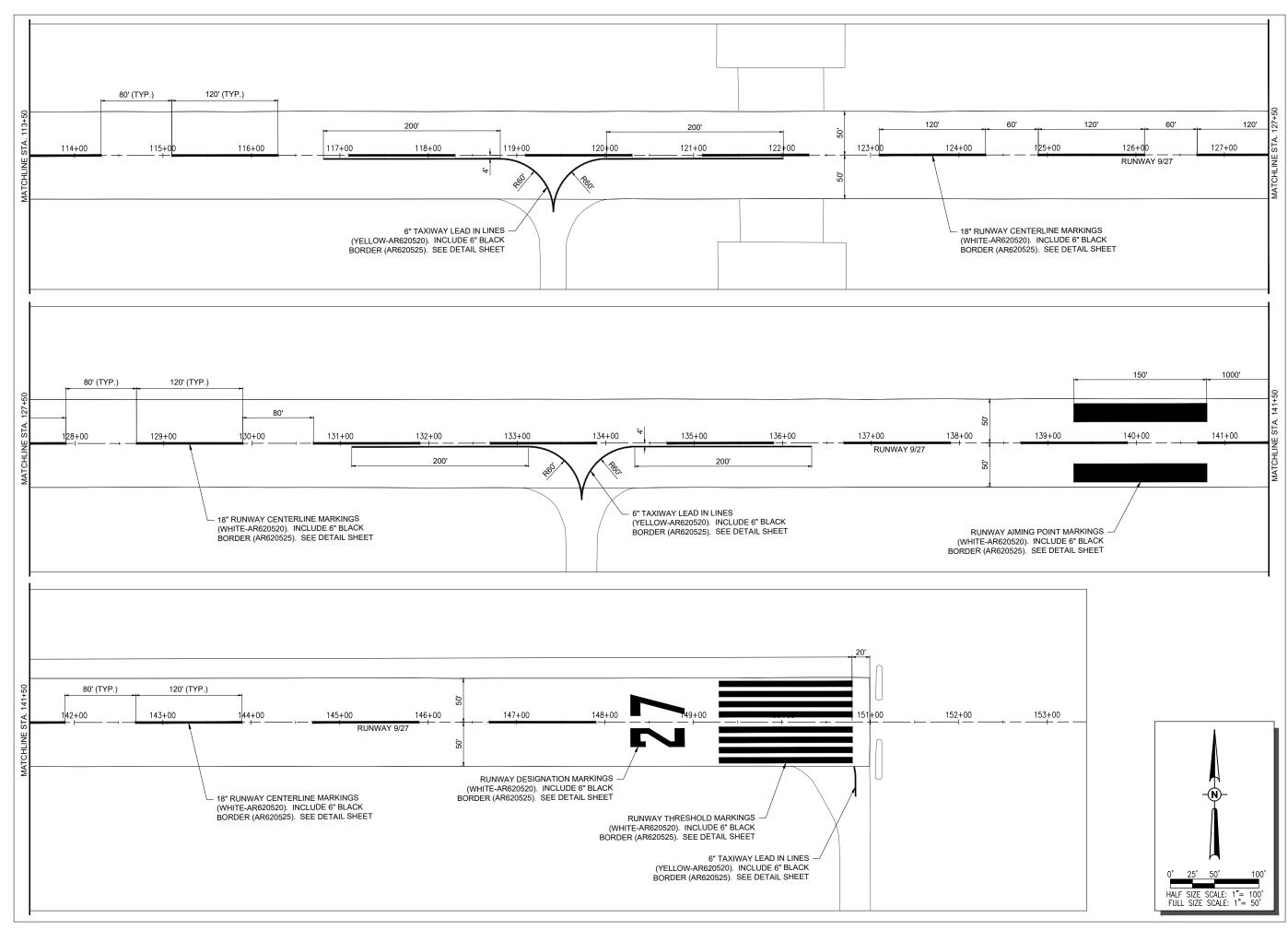
REHABILITATE RUNWAY 9-27

IDA No: MQB-4945 SBGP No: 3-17-SBGP-171/TBD

Contract No.: MB034



MARKING PLAN - 1



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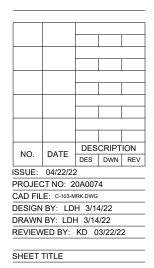


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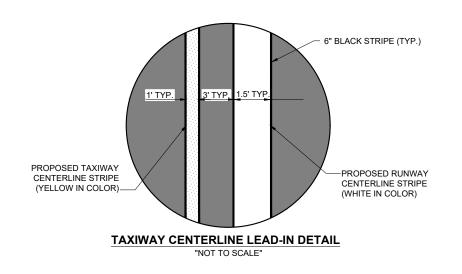
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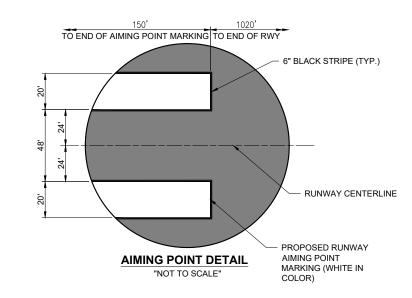
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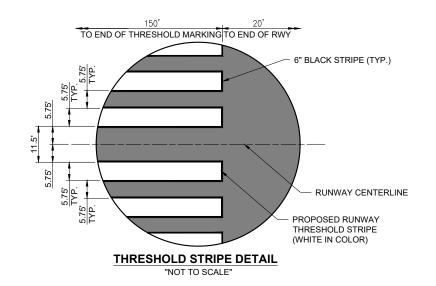
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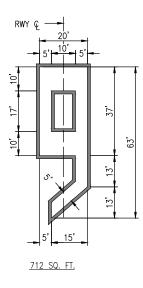


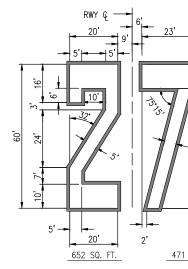
MARKING PLAN - 2











RUNWAY NUMERAL DIMENSIONING NOTES:

- 1. NUMERALS EXCEPT FOR THE NUMERAL ELEVEN (AS SHOWN) ARE HORIZONTALLY SPACED 15 FEET APART.
- 2. ALL DIMENSIONS ARE EXPRESSED IN INCREMENTS OF FEET.
- DOUBLE DIGIT NUMERAL DESIGNATIONS ARE CENTERED ON THE RUNWAY PAVEMENT CENTERLINE BASED ON THE CENTER OF THE OUTER EDGES OF THE TWO NUMERALS.
- 4. CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.

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50' 10'

471 SQ. FT.

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MARKING DETAILS