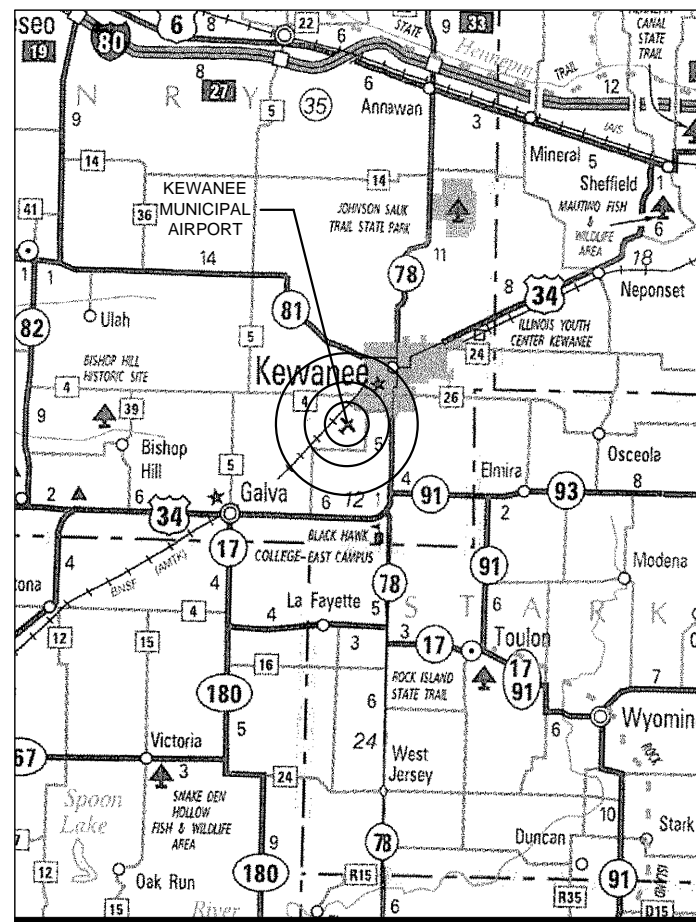


CONSTRUCTION PLANS

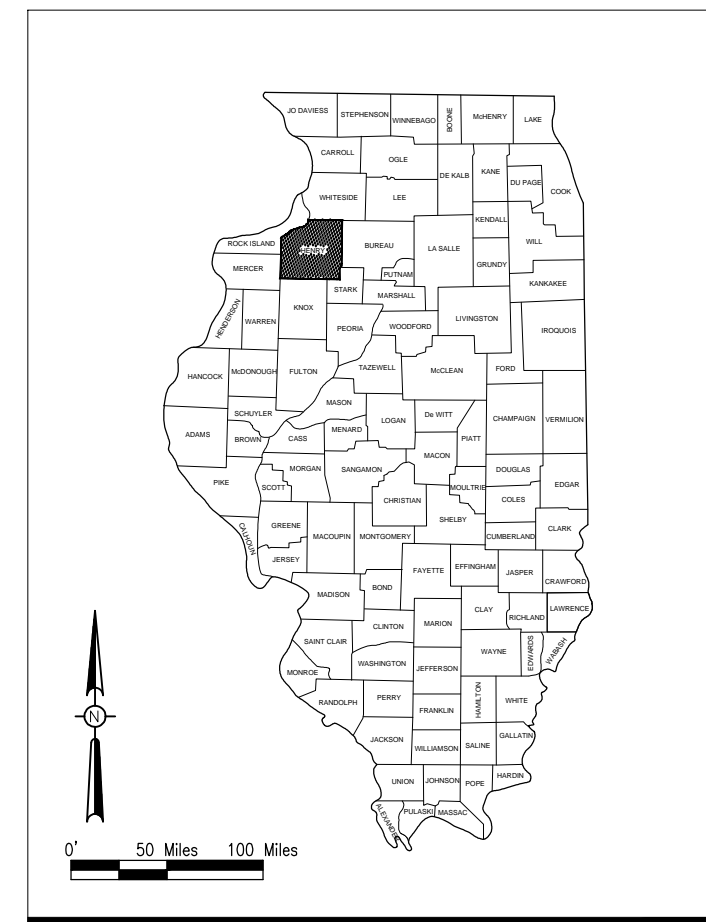
REHABILITATE RUNWAY 9-27 AND REMARK

KEWANEE AIRPORT AUTHORITY  
KEWANEE MUNICIPAL AIRPORT (EZI)  
KEWANEE, HENRY COUNTY, ILLINOIS

SBG PROJECT NO. 3-17-SBGP-TBD  
IDA PROJECT NO. EZI-4528



VICINITY MAP



LOCATION MAP

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	By

LIC. EXP. 11/30/17

**KYLE B. SCHWEIZER**  
062-064386

STATE OF ILLINOIS

DATE OF PLANS  
03 JUN 2016

KYLE B. SCHWEIZER, P.E.  
Project Engineer

**HANSON**

HANSON PROFESSIONAL SERVICES INC.  
1525 S. Sixth St.  
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Fax: 217.788.2503

**KEWANEE MUNICIPAL AIRPORT**

3761 Midland Road  
Kewanee, Illinois 61443  
Telephone: 309.853.8222

*Dele Lindstrom*  
Dele Lindstrom  
Airport Manager

03 JUN 2016



KEWANEE MUNICIPAL AIRPORT

3761 MIDLAND RD.  
KEWANEE IL, 61443

**SUMMARY OF QUANTITIES**

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150520	MOBILIZATION	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152480	SHOULDER ADJUSTMENT	S.Y.	800	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	L.F.	2,000	
AR401655	BUTT JOINT CONSTRUCTION	S.Y.	1,353	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	S.Y.	20	
AR402622	POROUS FRICTION COURSE, 0.10'	S.Y.	41,608	
AR603510	BITUMINOUS TACK COAT	GAL.	10,402	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	18,544	
AR620525	PAVEMENT MARKING-BLACK BORDER	S.F.	6,976	
AR800400	REFLECTIVE CRACK CONTROL TREATMENT	S.Y.	125	

**INDEX TO SHEETS**

SHEET NO.	SHEET TITLE
1	COVER
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	SAFETY PLAN
4	CONSTRUCTION PHASING PLAN - AREA 1
5	CONSTRUCTION PHASING PLAN - AREA 2
6	PAVING DETAILS AND NOTES
7	SITE PLAN - SHEET 1
8	SITE PLAN - SHEET 2
9	RUNWAY 9-27 & 1-19 INTERSECTION DETAIL
10	MARKING DETAILS AND NOTES

REHABILITATE  
RUNWAY 9-27  
AND REMARK

IDA No: EZI-4528  
SBG No: 3-17-SBGP-TBD

Contract No. KE014

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 3, 2016

PROJECT NO: 16A0020  
CAD FILE: G-002-FLP.DWG  
DESIGN BY: SJZ 04/11/2016  
DRAWN BY: RAD 04/20/2016  
REVIEWED BY: KBS 06/03/2016

SHEET TITLE

SUMMARY OF  
QUANTITIES AND  
INDEX TO SHEETS

**SCOPE OF WORK**

THIS PROJECT CONSISTS OF PAVEMENT IMPROVEMENTS TO RUNWAY 9-27. ASSOCIATED WORK ITEMS INCLUDE BITUMINOUS PAVEMENT REPAIRS, CRACK CLEANING AND SEALING, POROUS FRICTION COURSE OVERLAY AND PAVEMENT MARKING.

**HAUL ROUTE AND STAGING AREA**

THE CONTRACTOR WILL BE REQUIRED TO USE AN EXISTING GRAVEL LOT AS A STAGING AREA AND CONSTRUCT AN ALL-WEATHER HAUL ROUTE ALONG THE FIELD LINE EAST OF THE AIRPORT TO THE RUNWAY. SEPARATION FABRIC SHALL BE PLACED OVER THE EXISTING GROUND PRIOR TO ANY STABILIZATION MATERIAL IS PLACED. UPON COMPLETION OF THE PROJECT THE HAUL SHALL BE REMOVED AND RESTORED TO ITS ORIGINAL CONDITION. TILLED AREAS SHALL BE CHISEL PLOWED OR DISKED TO AN UNCOMPACTED STATE. TURF AREAS SHALL BE REWORKED AND RESEDED. ALL WORK AND MATERIALS ASSOCIATED WITH THE HAUL ROUTE AND STAGING AREA SHALL BE PAID FOR UNDER AR150540 "HAUL ROUTE" -- PER LUMP SUM.

**EROSION CONTROL**

THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND, THEREFORE NO N.P.D.E.S. PERMIT WILL BE REQUIRED.

**LIGHTING**

1. THERE ARE RUNWAY AND TAXIWAY LIGHTS ADJACENT TO THE WORK AREA. THE CONTRACTOR SHALL TAKE CARE TO AVOID DAMAGE TO THE LIGHTS. ANY DAMAGE TO THE LIGHTS WILL BE CONTRACTOR'S COST AND RESPONSIBILITY TO REPAIR.
2. TO PREVENT DAMAGE TO THE EXISTING THRESHOLD LIGHTS DURING PAVING OPERATIONS, THE CONTRACTOR MAY REMOVE THESE LIGHTS PROVIDED THEY ARE REINSTALLED TO THEIR ORIGINAL CONDITION AT CONTRACTOR'S COST.

**CERTIFIED PAYROLLS**

THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

**MATERIAL CERTIFICATION**

MATERIALS TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL. ALL MATERIALS TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. USE OF MATERIALS WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.

**UTILITY NOTE**

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

CRITICAL POINTS						
POINT #	DESCRIPTION	WORK AREA	LATITUDE	LONGITUDE	GRND. ELEV. (MSL)	HEIGHT (AGL)
1	CONST. EQUIP.	ALL	N041° 12' 02.836"	W089° 57' 31.283"	849.0	25
2	BARRICADE	1	N041° 12' 10.233"	W089° 57' 46.574"	854.0	N/A
3	CONST. EQUIP.	1	N041° 12' 14.178"	W089° 57' 45.524"	856.0	25
4	BARRICADE	1	N041° 12' 25.224"	W089° 57' 39.248"	853.0	N/A
5	BARRICADE	ALL	N041° 12' 10.949"	W089° 57' 37.402"	854.0	N/A
6	BARRICADE	ALL	N041° 12' 10.037"	W089° 57' 32.480"	853.0	N/A
7	BARRICADE	2	N041° 12' 10.262"	W089° 57' 38.339"	854.0	N/A
8	CONST. EQUIP.	1	N041° 12' 14.130"	W089° 57' 42.201"	858.0	25

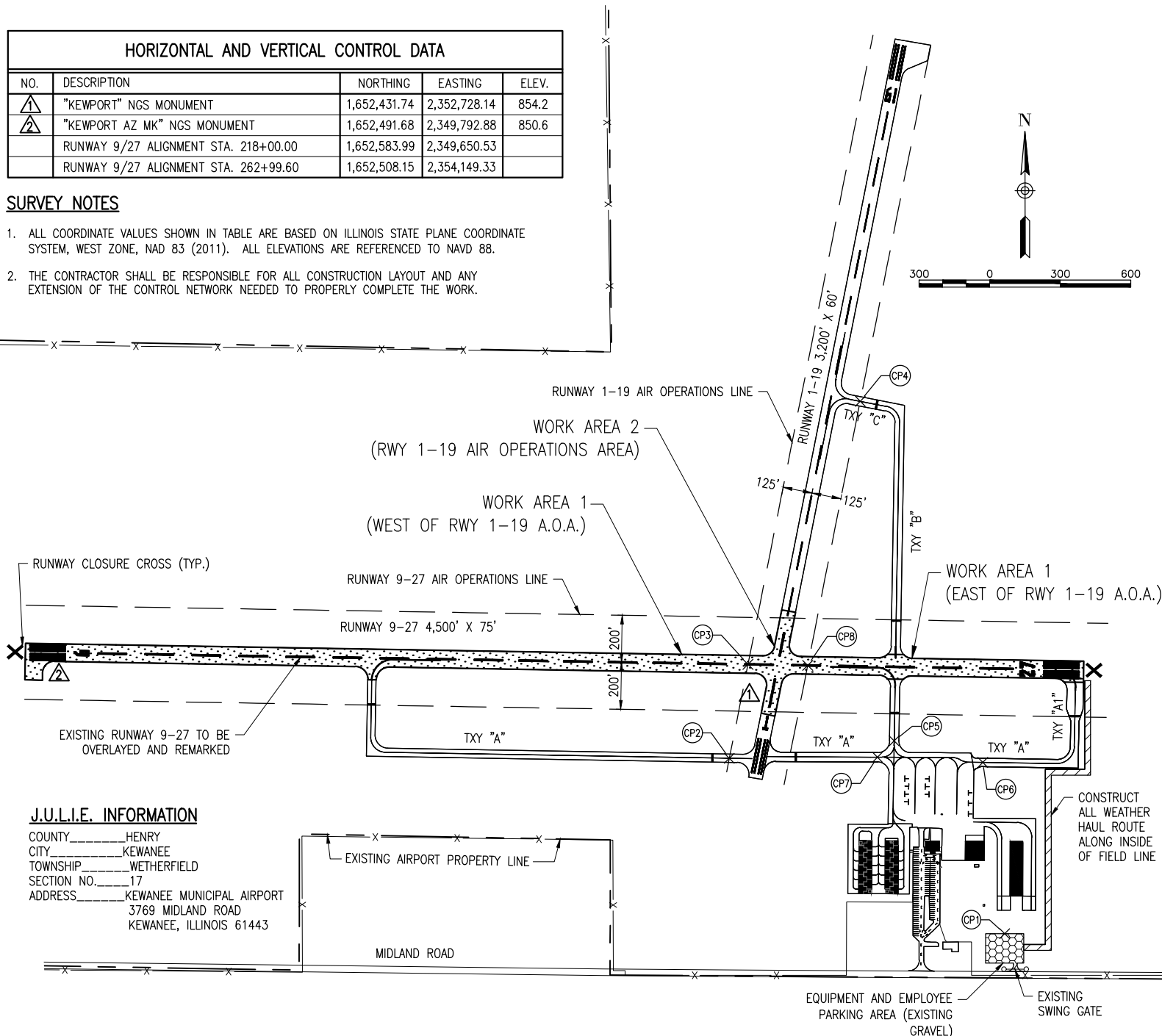
HORIZONTAL AND VERTICAL CONTROL DATA				
NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	"KEWPORT" NGS MONUMENT	1,652,431.74	2,352,728.14	854.2
2	"KEWPORT AZ MK" NGS MONUMENT	1,652,491.68	2,349,792.88	850.6
	RUNWAY 9/27 ALIGNMENT STA. 218+00.00	1,652,583.99	2,349,650.53	
	RUNWAY 9/27 ALIGNMENT STA. 262+99.60	1,652,508.15	2,354,149.33	

**SURVEY NOTES**

1. ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, WEST ZONE, NAD 83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.

**LEGEND**

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED EQUIPMENT AND EMPLOYEE PARKING AREA
- PROPOSED HAUL ROUTE



**J.U.L.I.E. INFORMATION**

COUNTY \_\_\_\_\_ HENRY  
 CITY \_\_\_\_\_ KEWANEE  
 TOWNSHIP \_\_\_\_\_ WETHERFIELD  
 SECTION NO. \_\_\_\_\_ 17  
 ADDRESS \_\_\_\_\_ KEWANEE MUNICIPAL AIRPORT  
 3769 MIDLAND ROAD  
 KEWANEE, ILLINOIS 61443

**PROPOSED SAFETY PLAN AND NOTES**

1. GENERAL - THE KEWANEE MUNICIPAL AIRPORT IS COMPRISED OF TWO RUNWAYS. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING BOTH RUNWAY 9-27 AND 1-19 AND PORTIONS OF TAXIWAY A, TAXIWAY B AND TAXIWAY C. RUNWAY 9-27, TAXIWAY B AND TAXIWAY C WILL BE CLOSED AT THE START OF THIS PROJECT AND WILL REMAIN CLOSED UNTIL THE COMPLETION OF THIS PROJECT. RUNWAY 1-19 WILL BE CLOSED WHEN CONSTRUCTION ACTIVITIES ARE WITHIN 125 FEET OF ITS CENTERLINE. TAXIWAY A WILL BE CLOSED WEST OF RUNWAY 1 END TO RUNWAY 9-27 AND EAST OF APRON TO THE RUNWAY 27 END. THE CLOSURE OF RUNWAY 1-19 WILL CLOSE THE AIRPORT; THEREFORE, THE CONTRACTOR WILL BE ALLOWED 7 CALENDAR DAYS TO COMPLETE THE REQUIRED WORK WITHIN AREA 2. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
2. ALL CONSTRUCTION/OPERATIONS ARE TO BE PERFORMED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2F "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND AC 150/5300-13A "AIRPORT DESIGN".
3. ALL CONSTRUCTION EQUIPMENT ON THE AIRPORT SHALL BE MARKED, LIGHTED AND/OR FLAGGED WITH A 3'x3' ORANGE AND WHITE CHECKERED FLAG IN ACCORDANCE WITH AC 150/5210-5D AND 70/7460-1.
4. RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT WITH THE AIRPORT UNICOM (122.80 MHz.) ANY TIME HE HAS EQUIPMENT OR PERSONNEL ON THE AIRFIELD.
5. THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THE STAGING AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.
6. EXCEPT FOR THE PERIODS OF TIME WHEN RUNWAY 1-19 IS ALLOWED TO BE CLOSED, THE CONTRACTOR SHALL MAINTAIN TAXIWAY ACCESS VIA TAXIWAY "A" BETWEEN RUNWAY 1-19 AND ALL HANGARS AND ADMINISTRATIVE AREAS.
7. AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE THE EXISTING GATE IN THE HAUL ROUTE WHENEVER HE IS NOT IN A CONTINUOUS HAULING OPERATION.
8. ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE RUNWAY CLOSURE.
9. THE CONTRACTOR SHALL PROVIDE A WRITTEN STATEMENT OF HIS ACCEPTANCE AND ASSURANCE THAT HE WILL COMPLY WITH THE PROPOSED CONSTRUCTION SAFETY AND PHASING PLAN PRIOR TO BE ISSUED THE NOTICE TO PROCEED.

**RUNWAY AND TAXIWAY CLOSURE PROCEDURES**

1. THE AIRPORT MANAGER OR REPRESENTATIVE WILL ISSUE ALL APPLICABLE NOTAM'S TO OPEN AND CLOSE THE RUNWAYS AND TAXIWAYS.
2. THE CONTRACTOR WILL PROVIDE NOTICE TO THE AIRPORT MANAGER AT LEAST 72 HOURS IN ADVANCE OF CLOSING ANY RUNWAY OR TAXIWAY.
3. THE CONTRACTOR WILL OBTAIN APPROVAL FROM THE AIRPORT MANAGER OR REPRESENTATIVE PRIOR TO ENTERING ONTO ANY ACTIVE RUNWAY OR TAXIWAY.
4. THE CONTRACTOR WILL CONTACT THE AIRPORT MANAGER OR REPRESENTATIVE FOR VERIFICATION OF RUNWAY OR TAXIWAY CLOSURE PRIOR TO ENTERING ONTO A RUNWAY OR TAXIWAY TO PLACE EQUIPMENT SUCH AS BARRICADES AND RUNWAY DESIGNATION CROSSES. THE AIRPORT MANAGER OR REPRESENTATIVE WILL DIRECT THE LOCATION OF SUCH EQUIPMENT.
5. THE CONTRACTOR WILL CONTACT THE AIRPORT MANAGER OR REPRESENTATIVE OF WORK COMPLETION ON THE RUNWAY OR TAXIWAY PRIOR TO REMOVING ANY EQUIPMENT SUCH AS BARRICADES AND RUNWAY DESIGNATION CROSSES.
6. THE AIRPORT MANAGER OR REPRESENTATIVE WILL INSPECT THE AREA PRIOR TO REMOVING EQUIPMENT AND CANCELLING NOTAM'S.

**AIRCRAFT OPERATION LINE**

THE AIRCRAFT OPERATION LINE WILL DESIGNATE HOW CLOSE TO A RUNWAY/TAXIWAY ANY CONTRACTOR PERSON/VEHICLE MAY GET WITHOUT CLOSING THE RUNWAY/TAXIWAY. AS THE CONTRACTOR STARTS WORKING IN EACH CONSTRUCTION AREA AS DESCRIBED ON THE FOLLOWING SHEETS, HE WILL PLACE BARRICADES ACROSS THE INACTIVE (CLOSED) PAVEMENT WHERE HIS PERSONNEL WILL BE CROSSING THE ACTIVE RUNWAY/TAXIWAY. THE BARRICADES WILL BE PLACED 10' APART FOR A DISTANCE OF 30' (ON TAXIWAYS) AND 75' (ON RUNWAYS). THE CONTRACTOR WILL MAINTAIN THE AIRCRAFT OPERATION LINES FOR THE DURATION OF THIS PROJECT.



Offices Nationwide  
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Hanson Professional Services Inc.  
1525 S. 6th Street  
Springfield, IL 62568  
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Illinois Licensed  
Professional Service Corporation  
#184-001084



KEWANEE MUNICIPAL AIRPORT

3761 MIDLAND RD.  
KEWANEE IL, 61443

**REHABILITATE RUNWAY 9-27 AND REMARK**

IDA No: EZI-4528  
SBG No: 3-17-SBGP-TBD

Contract No. KE014

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 3, 2016  
 PROJECT NO: 16A0020  
 CAD FILE: G-003-SFY.DWG  
 DESIGN BY: SJZ 04/12/2016  
 DRAWN BY: RAD/MAW 04/26/2016  
 REVIEWED BY: KBS 06/03/2016

SHEET TITLE

**SAFETY PLAN**

**PROPOSED SEQUENCE OF WORK**

**AREA 1**

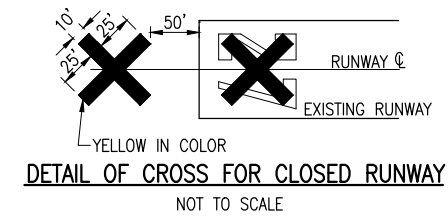
1. AREA 1 CONSTRUCTION SHALL CONSIST OF ALL WORK LOCATED ON RUNWAY 9-27 AND PORTIONS OF TAXIWAY CONNECTORS 125' FROM RUNWAY 1-19 CENTERLINE.
2. AT THE START OF CONSTRUCTION ACTIVITIES THE CONTRACTOR SHALL CLOSE RUNWAY 9-27, TAXIWAY "B" AND TAXIWAY "C" IN ACCORDANCE WITH THE PROCEDURE SET FORTH ON THE PROPOSED SAFETY PLAN. THE ASSOCIATED RUNWAY LIGHTING CIRCUIT AND NAVAIDS WILL BE TURNED OFF.
3. RUNWAY 9-27 AND TAXIWAYS "B" AND "C" WILL BE CLOSED FOR THE DURATION OF THIS PROJECT.
4. TAXIWAY "A" WILL BE CLOSED FROM RUNWAY 9-27 TO RUNWAY 1 END AND FROM THE APRON TO RUNWAY 27 END FOR THE DURATION FOR THIS PROJECT
5. ONLY AFTER THE CROSSES AND BARRICADES HAVE BEEN PLACED WILL THE CONTRACTOR BE ALLOWED TO MOVE ONTO RUNWAY 9-27 AND BEGIN WORKING.
6. ONCE ALL THE CRACK CLEANING AND SEALING, PAVEMENT REPAIRS AND OTHER PREPARATORY WORK WITHIN AREA 1 HAVE BEEN COMPLETED, THE CONTRACTOR WILL BE ALLOWED TO START WORK WITHIN AREA 2.
7. THE CONTRACTOR MAY FINISH SHOULDER ADJUSTMENT AND BALANCE OF MARKINGS ON RUNWAY 9-27 AND ADJACENT TAXIWAYS (AREA 1) AFTER THE COMPLETION OF THE CONSTRUCTION IN THE AREA 2.
8. WHEN WORK IS OCCURRING IN AREA 1 AND RUNWAY 1-19 IS OPEN, ALL CROSSING OF RUNWAY 1-19 SHALL OCCUR AT THE INTERSECTION WITH RUNWAY 9-27. THE CONTRACTOR SHALL SUPPLY GUARDS AT THE CROSSING TO ENSURE THERE ARE NO AIRCRAFT PRESENT BEFORE CONSTRUCTION TRAFFIC MAY CROSS RUNWAY 1-19. AIRCRAFT HAVE THE RIGHT OF WAY AT ALL TIMES.

**TAXIWAY LIGHTING CLOSURE NOTES**

1. WHEN AREA 1 CONSTRUCTION ACTIVITIES BEGIN, THE ENTIRE TAXIWAY B AND TAXIWAY C AND SECTIONS OF TAXIWAY A WILL BE CLOSED (BARRICADED) TO AIRCRAFT MOVEMENT. THOSE SECTIONS OF TAXIWAY A, TAXIWAY B AND TAXIWAY C THAT ARE CLOSED TO AIRCRAFT MOVEMENT WILL NEED TO HAVE THEIR LIGHTING CIRCUIT DEACTIVATED.
2. TAXIWAY A, B AND C ARE ON THE SAME LIGHTING CIRCUIT. THE CONTRACTOR WILL "BAG" THE EXISTING TAXIWAY LIGHTS THAT ARE LOCATED ON THE SECTION OF TAXIWAY A, TAXIWAY B AND TAXIWAY C THAT ARE BEING CLOSED. THE "BAG" WILL NOT ALLOW ANY LIGHT TO EMIT FROM THE TAXIWAY LIGHT. THERE ARE OVER 80 TAXIWAY LIGHTS THAT WILL REQUIRE "BAGGING". ANY TAXI GUIDANCE SIGNS LOCATED WITHIN THE CLOSED AREA WILL ALSO BE "BAGGED".
3. THE "BAGGING" OF TAXIWAY LIGHTS AND SIGNS WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

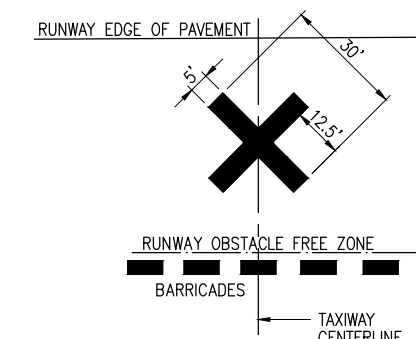
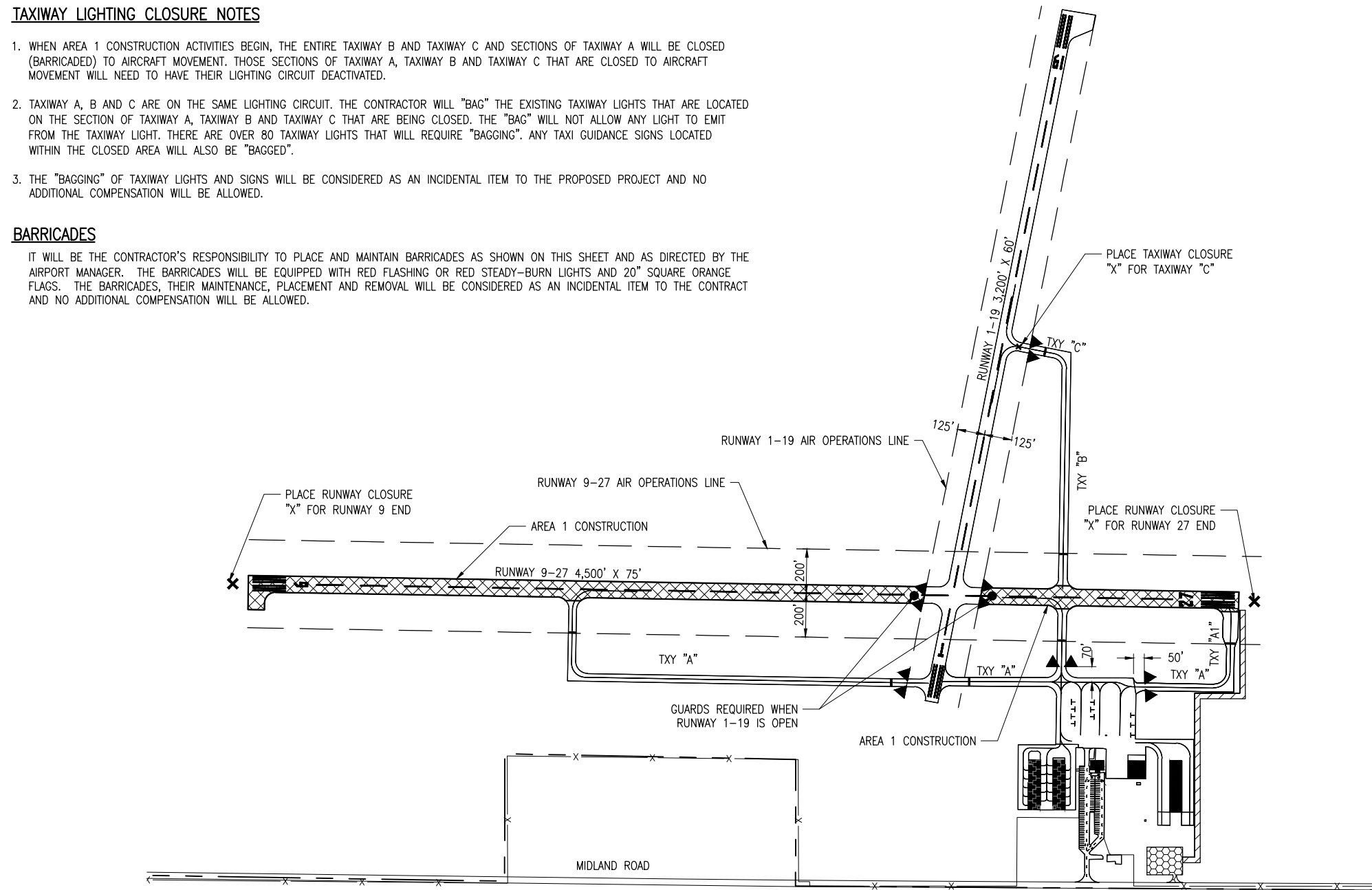
**BARRICADES**

IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AS SHOWN ON THIS SHEET AND AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



**NOTE:**

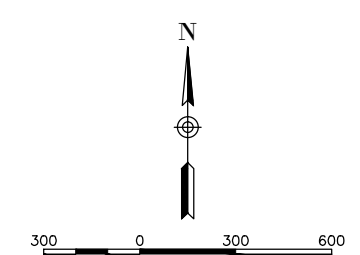
COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED AS SHOWN IN THE ABOVE DETAIL AND SECURED IN A MANNER APPROVED BY THE AIRPORT MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING, PLACING, AND REMOVING THE CROSSES AS AN INCIDENTAL COST TO THE PROJECT.



1. TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE "AVIATION YELLOW"
2. TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE CONSTRUCTED USING TEMPORARY PAINT SUCH AS SEYMOUR TEMPORARY MARKER, PART NO. 20-636, OR AN EQUIVALENT PAINT EASILY REMOVED WITH WATER WITHOUT DEFACING THE PAVEMENT.
3. TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE PLACED WHEN THE RUNWAY IS OPEN AND THE TAXIWAY IS CLOSED MORE THAN 72 HOURS. THE "CLOSED TAXIWAY" MARKINGS SHALL BE PLACED AS SHOWN ON THIS SHEET. IT IS ANTICIPATED THAT ONLY TAXIWAY "C" WILL REQUIRE A TEMPORARY "CLOSED TAXIWAY" MARKING.
4. THE PROPOSED CROSS WILL BE REMAIN IN PLACE EACH DAY THE TAXIWAY IS CLOSED AND REMOVED WHEN THE TAXIWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. "CLOSED TAXIWAY" MARKINGS SHALL NOT BE A PAY ITEM AND SHALL BE INCIDENTAL TO OTHER CONTRACT BID ITEMS.

**TEMPORARY TAXIWAY CLOSURE CROSS DETAIL**  
NOT TO SCALE

- LEGEND**
- EXISTING IMPROVEMENTS
  - EXISTING BUILDINGS
  - WORK AREA LIMITS
  - HAUL ROUTE
  - BARRICADES
  - GUARD/FLAGGER
  - EQUIPMENT AND EMPLOYEE PARKING AREA



**REHABILITATE RUNWAY 9-27 AND REMARK**

IDA No: EZI-4528  
SBG No: 3-17-SBGP-TBD

Contract No. KE014

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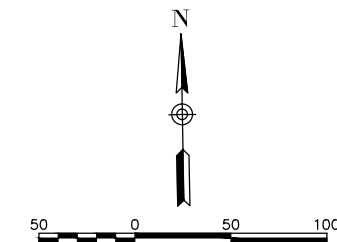
ISSUE: JUNE 3, 2016  
PROJECT NO: 16A0020  
CAD FILE: G-004-PHS.DWG  
DESIGN BY: SJZ 04/18/2016  
DRAWN BY: RAD/MAW 05/02/2016  
REVIEWED BY: KBS 06/03/2016

SHEET TITLE

**CONSTRUCTION PHASING PLAN - AREA 1**

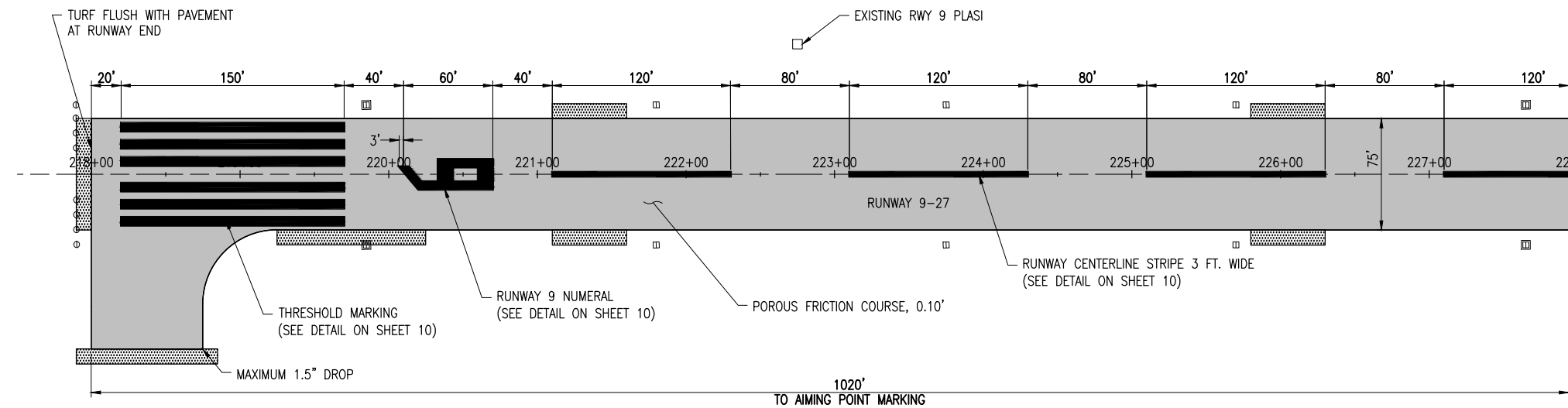




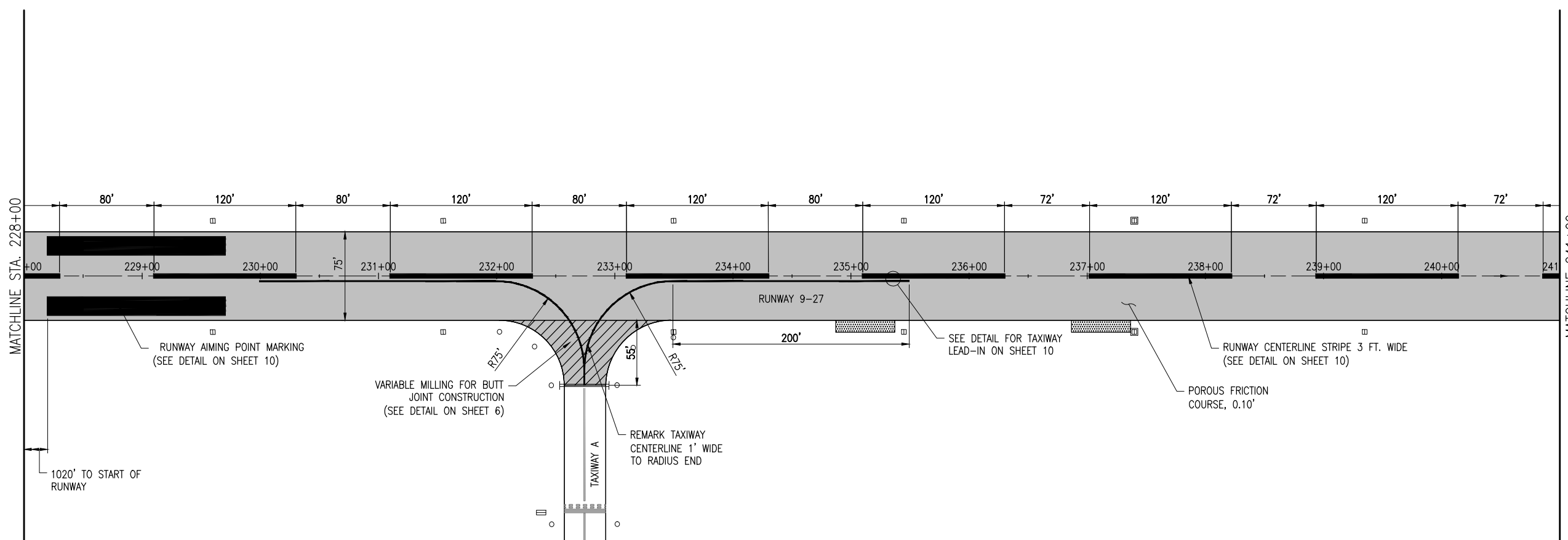


**LEGEND**

- AR402622 - POROUS FRICTION COURSE, 0.10'
- AR152480 - SHOULDER ADJUSTMENT
- AR401655 - BUTT JOINT CONSTRUCTION
- AR401910 - REMOVE AND REPLACE BIT. PVMT.  
AR800400 - REFL. CRACK CONTROL TREATMENT
- AIRFIELD LIGHTING/SIGNAGE
- ELECTRICAL DUCTS/SERVICE POINTS



MATCHLINE STA. 228+00



MATCHLINE STA. 228+00

MATCHLINE STA. 241+00

**REHABILITATE  
RUNWAY 9-27  
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IDA No: EZI-4528  
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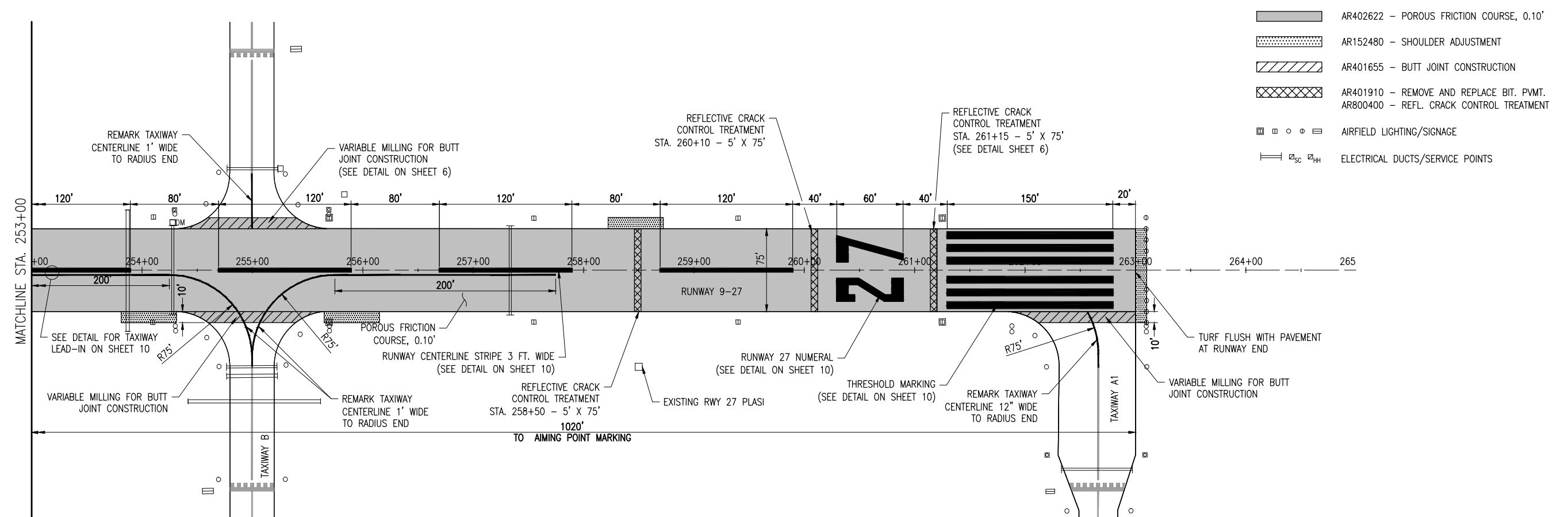
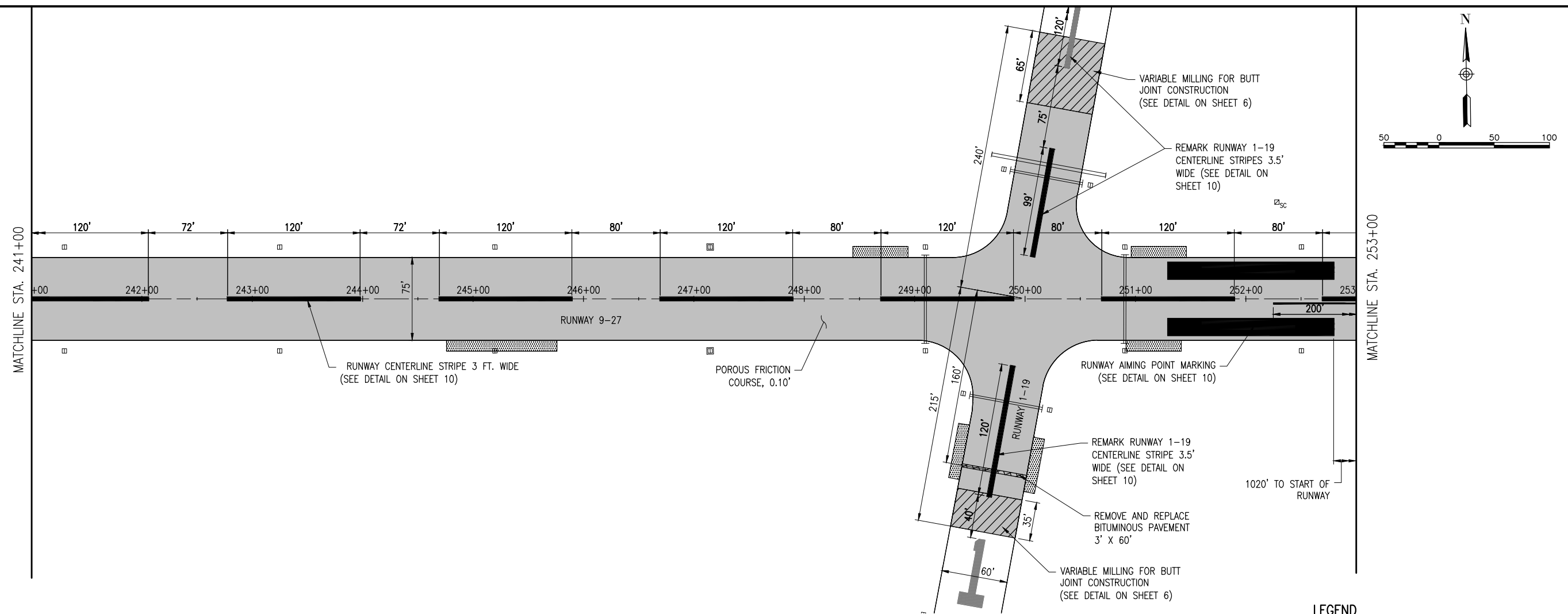
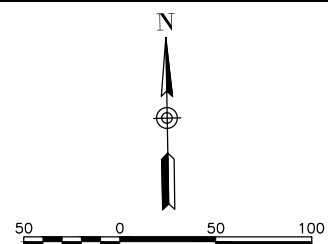
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CAD FILE: C-121-CON.DWG

DESIGN BY: SJZ 04/12/2016  
DRAWN BY: RAD/MAW 04/19/2016

REVIEWED BY: KBS 06/03/2016

SHEET TITLE

SITE PLAN - SHEET 1



**LEGEND**

- AR402622 - POROUS FRICTION COURSE, 0.10'
- AR152480 - SHOULDER ADJUSTMENT
- AR401655 - BUTT JOINT CONSTRUCTION
- AR401910 - REMOVE AND REPLACE BIT. PVMT. AR800400 - REFL. CRACK CONTROL TREATMENT
- AIRFIELD LIGHTING/SIGNAGE
- ELECTRICAL DUCTS/SERVICE POINTS

**REHABILITATE RUNWAY 9-27 AND REMARK**

IDA No: EZI-4528  
SBG No: 3-17-SBGP-TBD

Contract No. KE014

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

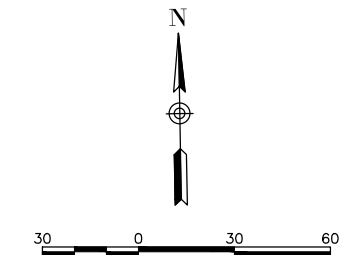
ISSUE: JUNE 3, 2016  
PROJECT NO: 16A0020  
CAD FILE: C-121-CON.DWG  
DESIGN BY: SJH 04/12/2016  
DRAWN BY: RAD/MAW 04/19/2016  
REVIEWED BY: KBS 06/03/2016

SHEET TITLE

SITE PLAN - SHEET 2

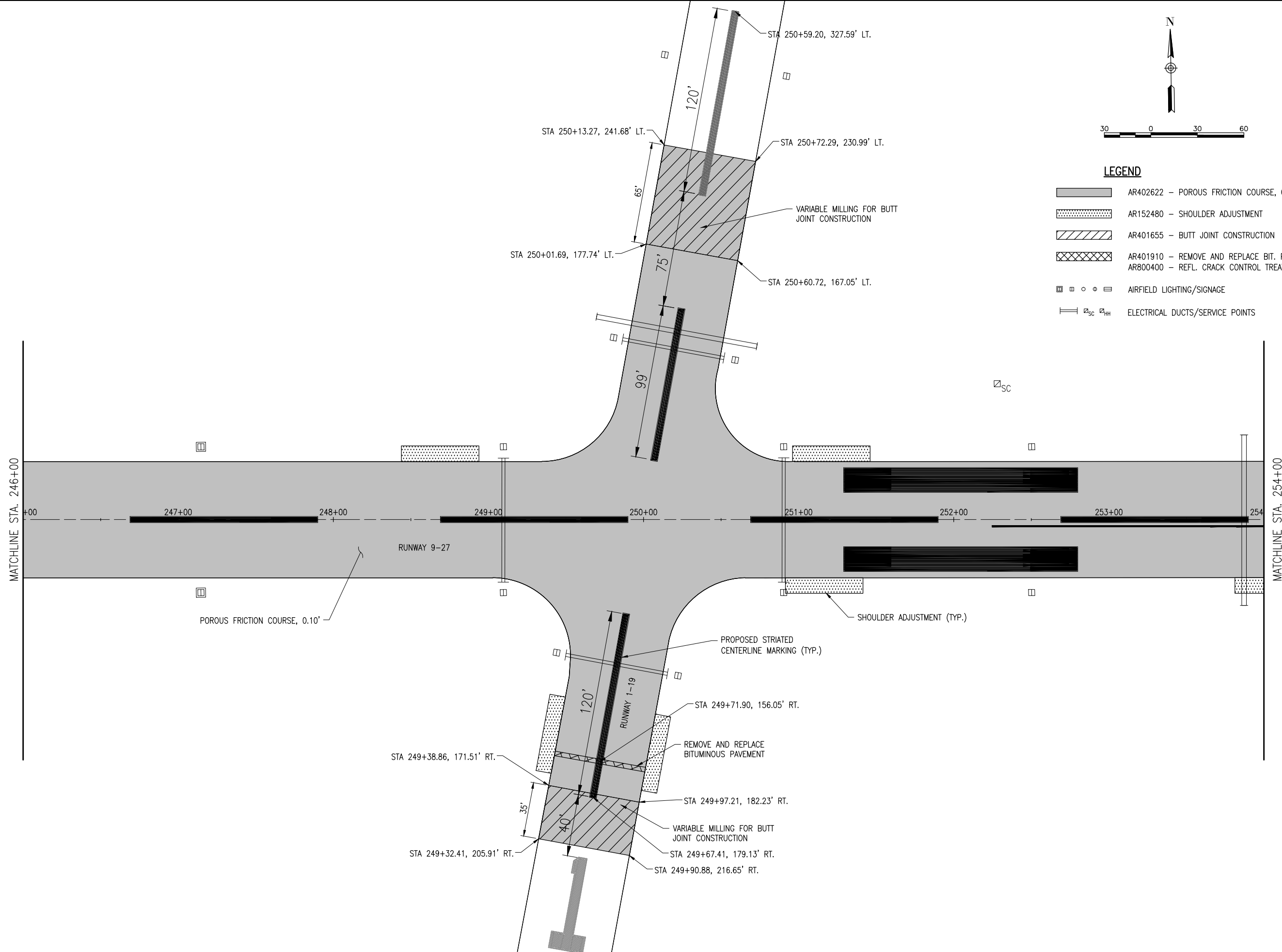
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**LEGEND**

- AR402622 - POROUS FRICTION COURSE, 0.10'
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- AIRFIELD LIGHTING/SIGNAGE
- ELECTRICAL DUCTS/SERVICE POINTS



**REHABILITATE  
RUNWAY 9-27  
AND REMARK**

IDA No: EZI-4528  
SBG No: 3-17-SBGP-TBD

Contract No. KE014


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JUNE 3, 2016  
PROJECT NO: 16A0020  
CAD FILE: C-122-INT.DWG  
DESIGN BY: SJZ 04/12/2016  
DRAWN BY: RAD/MAW 04/19/2016  
REVIEWED BY: KBS 06/03/2016

SHEET TITLE

**RUNWAY 9-27 & 1-19  
INTERSECTION  
DETAIL**

