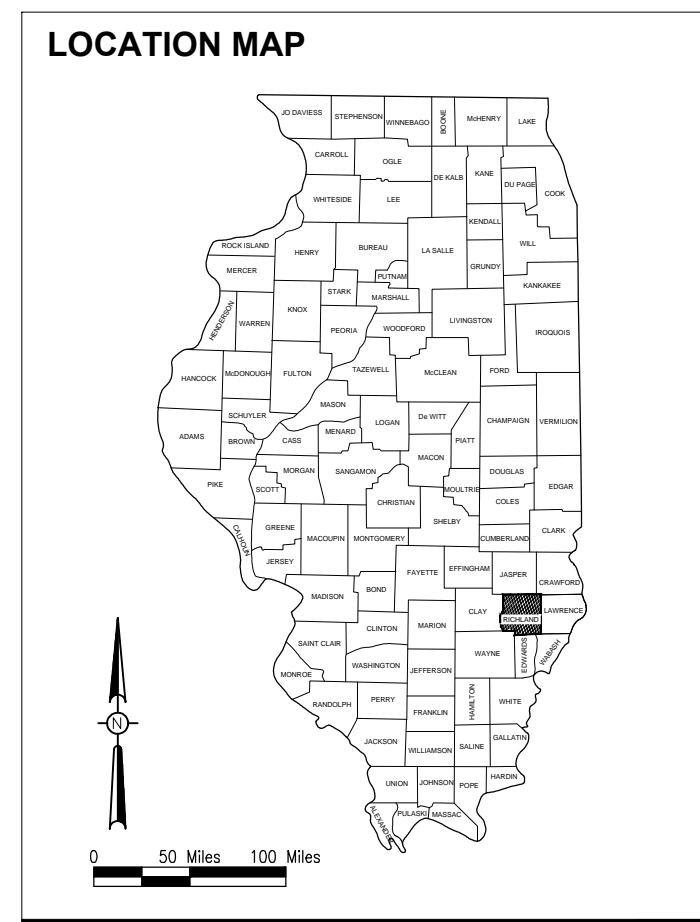
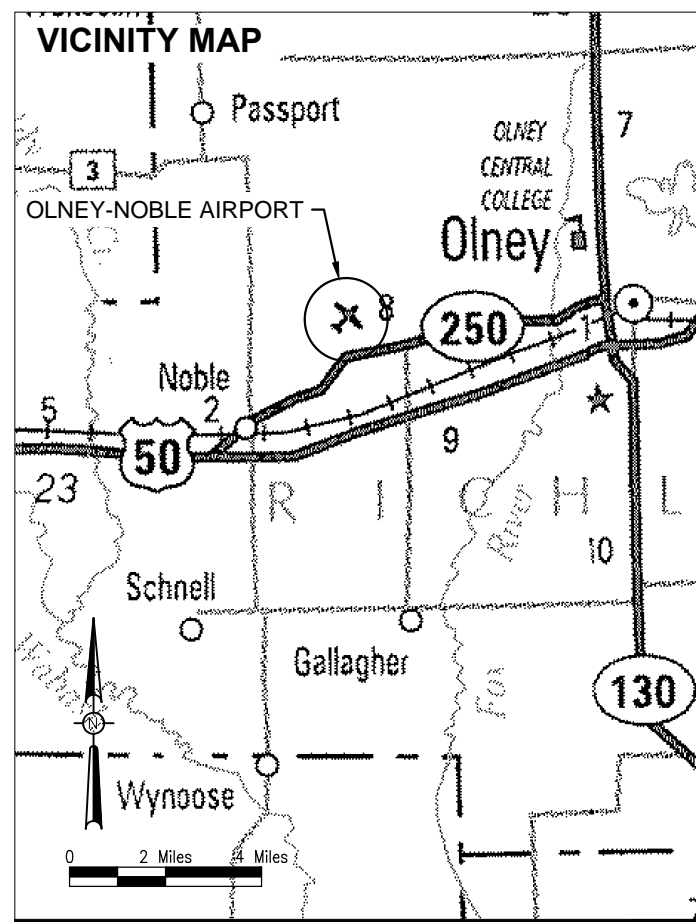


CONSTRUCTION PLANS

INSTALL CULVERT PIPE LINER IN 48" AND 54" CMP BENEATH RUNWAY 3-21

**OLNEY-NOBLE AIRPORT AUTHORITY
OLNEY-NOBLE AIRPORT (OLY)
NOBLE, RICHLAND COUNTY, ILLINOIS**

**IDA PROJECT NO. OLY-4535
SBG PROJECT NO. 3-17-SBGP-XX**



NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

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| No. | Issue/Description | Sheets Changed | Date | By |
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Date of Plans

KYLE B. SCHWEIZER
062-064386
STATE OF ILLINOIS

10 JUN 2016
Date

Kyle B. Schweizer
Kyle B. Schweizer, P.E.
Project Engineer
Lic. Exp. 11/30/2017

HANSON
HANSON PROFESSIONAL SERVICES INC.
1525 South Sixth Street
Springfield, Illinois 62703-2886
Telephone: 217-788-2450

OLNEY-NOBLE AIRPORT AUTHORITY

Approved: *[Signature]* CHAIRMAN
Date: 6/9/2016

Approved: *[Signature]* SECRETARY
Date: June 9, 2016

OLNEY-NOBLE AIRPORT

2632 E. IL 250
NOBLE, ILLINOIS 62868

INSTALL CULVERT PIPE LINER IN 48" AND 54" CMP BENEATH RUNWAY 3-21

SBG No: 3-17-SBGP-TBD
IDA No: OLY-4535

Contract No. OL016

| NO. | DATE | DESCRIPTION | | |
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ISSUE: JUNE 10, 2016

PROJECT NO: 15A0117

CAD FILE: G-002-FLP.DWG

LAYOUT BY: KBL 05/19/2016

DRAWN BY: CWS 05/19/2016

REVIEWED BY: RAW 05/24/2016

SHEET TITLE

SUMMARY OF QUANTITIES, INDEX TO SHEETS, AND GENERAL NOTES

| INDEX TO SHEETS | |
|-----------------|---|
| SHEET NO. | SHEET TITLE |
| 1 | COVER SHEET |
| 2 | SUMMARY OF QUANTITIES, INDEX TO SHEETS, AND GENERAL NOTES |
| 3 | SAFETY PLAN |
| 4 | SAFETY NOTES AND DETAILS |
| 5 | PROPOSED SITE PLAN |
| 6 | DETAILS |

| SUMMARY OF QUANTITIES | | | | |
|-----------------------|---------------------------|------|----------------|-------------------|
| ITEM NO. | DESCRIPTION | UNIT | TOTAL QUANTITY | AS-BUILT QUANTITY |
| AR150510 | ENGINEER'S FIELD OFFICE | L.S. | 1 | |
| AR150520 | MOBILIZATION | L.S. | 1 | |
| AR152411 | UNCLASSIFIED EXCAVATION | L.S. | 1 | |
| AR156530 | TEMPORARY SEEDING | ACRE | 0.5 | |
| AR156531 | EROSION CONTROL BLANKET | S.Y. | 2,700 | |
| AR156544 | RIPRAP-GRADATION NO. 4 | S.Y. | 100 | |
| AR156545 | RIPRAP-GRADATION NO. 5 | S.Y. | 64 | |
| AR752923 | REPLACE HEADWALL | EA. | 1 | |
| AR800490 | 48" PIPE LINER | L.F. | 333 | |
| AR800491 | 54" PIPE LINER | L.F. | 172 | |
| AR800492 | FLOWABLE FILL | C.Y. | 100 | |
| AR800501 | RESEAL CONCRETE STRUCTURE | L.S. | 1.00 | |
| AR901510 | SEEDING | ACRE | 0.5 | |

GENERAL NOTES

1. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
2. THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPERLY MONITORING ANY CONFINED SPACES AND PROVIDING ANY TRAINING TO HIS EMPLOYEES AS NECESSARY TO COMPLETE THE PROJECT.
4. ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
5. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
6. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
7. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
9. THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
10. CONTRACTOR SHALL NOTE THAT SOME AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING ANY WORK. ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITY WHICH HAVE BEEN PREVIOUSLY FARMED OR ARE DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION SHALL BE CHISEL PLOWED OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER) AT THE CONTRACTOR'S EXPENSE.
11. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
12. **DRAINAGE PLAN** – THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE AND EROSION CONTROL FOR ALL AREAS AFFECTED BY HIS WORK. A DRAINAGE PLAN DETAILING THE CONTRACTOR'S PLANNED SEQUENCING WITH REGARD TO DRAINAGE INSTALLATION, EROSION CONTROL, AND POTENTIAL DEWATERING SHALL BE INCLUDED AS PART OF THE PROPOSED SCHEDULE SUBMITTED AT THE PRE-CONSTRUCTION MEETING.
13. **NPDES PERMIT** – THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS NOT REQUIRED.
14. **MATERIAL CERTIFICATIONS** – MATERIALS **CANNOT** BE INSTALLED UNTIL **ALL** THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
15. **CERTIFIED PAYROLLS** – THE RESIDENT ENGINEER **CANNOT** FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL **ALL** CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY _____ RICHLAND
CITY _____ OLNEY
TOWNSHIP _____ NOBLE
SECTION NO. _____ 2
ADDRESS _____ OLNEY-NOBLE AIRPORT
2632 E. IL 250
NOBLE, ILLINOIS 62868

OLNEY-NOBLE AIRPORT

2632 E. IL 250
NOBLE, ILLINOIS 62868

INSTALL CULVERT PIPE LINER IN 48" AND 54" CMP BENEATH RUNWAY 3-21

SBG No: 3-17-SBGP-TBD
IDA No: OLY-4535

Contract No. OL016

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ISSUE: JUNE 10, 2016
PROJECT NO: 15A0117
CAD FILE: C-101-SFY.DWG
LAYOUT BY: KBS
DRAWN BY: KBS
REVIEWED BY: RAW 05/24/2016
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SHEET TITLE

SAFETY PLAN

CONSTRUCTION SAFETY PLAN

- GENERAL** - THE OLNEY-NOBLE AIRPORT IS COMPRISED OF TWO PAVED RUNWAYS. THE PROPOSED CONSTRUCTION WILL REQUIRE THE PERIODIC DAYTIME CLOSING OF THE SECONDARY RUNWAY 3/21. PRIMARY RUNWAY 11/29 AND ITS ASSOCIATED TAXIWAY WILL REMAIN OPEN THROUGHOUT THE PROJECT.
- PROJECT DESCRIPTION:** THIS PROJECT SHALL CONSIST OF SLIP-LINING EXISTING 48" AND 54" CORRUGATED METAL PIPES WITH HDPE PIPING. ASSOCIATED WORK INCLUDES GROUTING VOID BETWEEN THE PIPES, REPLACEMENT OF CONCRETE HEADWALL, INSTALLATION OF RIPRAP, GRADING, SEEDING AND EROSION CONTROL.
- AIR OPERATIONS AREA** - AT NO TIME MAY THE CONTRACTOR WORK INSIDE THE AIR OPERATIONS AREA (AOA) WHILE IT IS ACTIVE. ANY WORK DONE INSIDE THIS AREAS WILL REQUIRE TEMPORARY CLOSURE OF THE APPLICABLE RUNWAY OR TAXIWAY. THE AOA FOR RUNWAY 3-21 IS GOVERNED BY THE RUNWAY OBSTACLE FREE ZONE (OFZ) TO A WIDTH OF 125' FROM THE RUNWAY CENTERLINE. THE CONTRACTOR SHALL STAKE THESE LIMITS WITH FLAGGED LATHES IN THE PROJECT AREA PRIOR TO THE START OF WORK.
- THE CONTRACTOR SHALL CLOSE RUNWAY 3-21 ON A DAILY BASIS WHEN HE WILL HAVE MEN OR EQUIPMENT WORKING WITHIN THE AOA. NO OVERNIGHT CLOSURES WILL BE PERMITTED UNLESS AUTHORIZED BY THE AIRPORT MANAGER.
- ALL RUNWAY/TAXIWAY CLOSURES SHALL BE COORDINATED WITH THE AIRPORT MANAGER A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME. THE AIRPORT MANAGER HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.

RUNWAY/TAXIWAY CLOSURE PROCEDURES:

- CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME.
 - ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
 - PLACEMENT OF CROSSES AND BARRICADES.
- ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.

RUNWAY/TAXIWAY RE-OPENING PROCEDURES:

- ENSURE ALL PERSONNEL, EQUIPMENT AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
- INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES
- REMOVE BARRICADES AND CROSSES.
- NOTIFY THE AIRPORT MANAGEMENT OR REPRESENTATIVE TO CANCEL THE NOTAM.
- ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.

- THE CONTRACTOR SHALL PROVIDE A WRITTEN STATEMENT OF HIS ACCEPTANCE AND ASSURANCE THAT HE WILL COMPLY WITH THE PROPOSED CONSTRUCTION SAFETY AND PHASING PLAN PRIOR TO BEING ISSUED THE NOTICE TO PROCEED.
- REFER TO THE CONSTRUCTION SAFETY NOTES & DETAILS SHEET FOR ADDITIONAL SAFETY INFORMATION.

CRITICAL POINTS

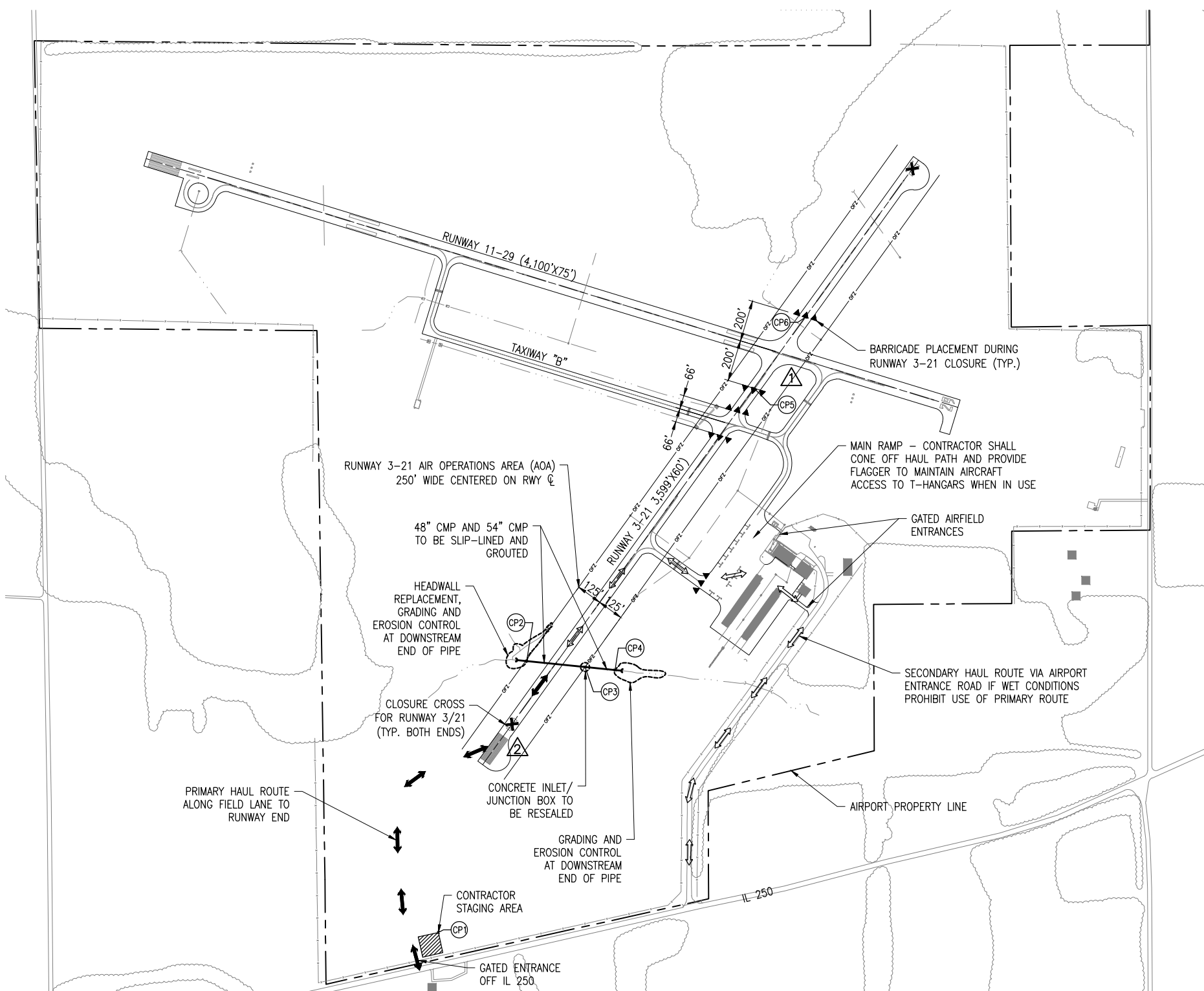
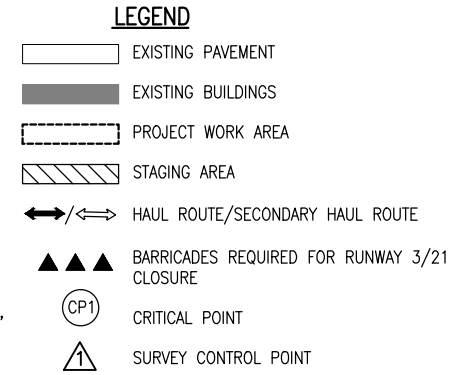
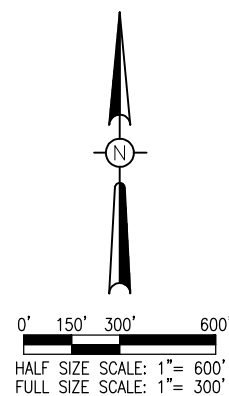
| POINT # | DESCRIPTION | LATITUDE | LONGITUDE | GRND. ELEV. (MSL) | HEIGHT (AGL) |
|---------|-------------|-------------------|-------------------|-------------------|--------------|
| 1 | EQUIPMENT | N038° 42' 51.571" | W088° 10' 46.386" | 475.0 | 10 |
| 2 | EQUIPMENT | N038° 43' 04.525" | W088° 10' 40.983" | 464.0 | 25 |
| 3 | EQUIPMENT | N038° 43' 04.238" | W088° 10' 37.421" | 464.0 | 15 |
| 4 | EQUIPMENT | N038° 43' 04.090" | W088° 10' 35.587" | 455.0 | 25 |
| 5 | BARRICADE | N038° 43' 17.533" | W088° 10' 27.094" | 470.0 | N/A |
| 6 | BARRICADE | N038° 43' 20.916" | W088° 10' 23.971" | 465.0 | N/A |

HORIZONTAL AND VERTICAL CONTROL DATA

| NO. | DESCRIPTION | NORTHING | EASTING | ELEV. |
|-----|-------------------------------|------------|--------------|--------|
| 1 | "OLNEPORT" NGS MONUMENT | 748,331.50 | 1,029,840.81 | 469.12 |
| 2 | "OLNEPORT AZ MK" NGS MONUMENT | 746,543.74 | 1,028,518.32 | 460.68 |

SURVEY NOTES

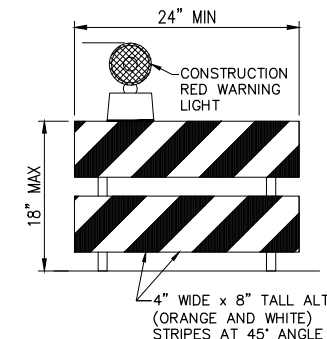
- ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD 83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.



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CONSTRUCTION SAFETY NOTES

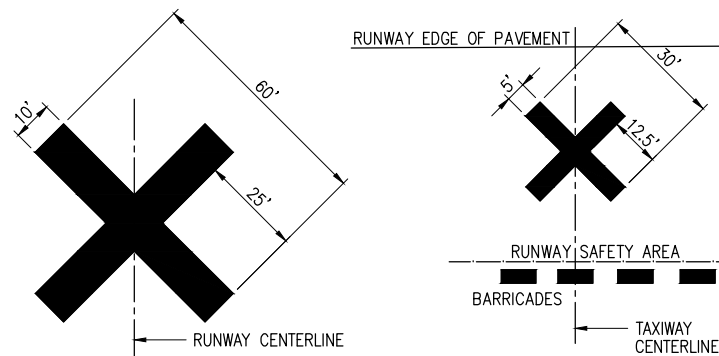
1. ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN.
2. ALL COSTS ASSOCIATED WITH THE WORK AND MATERIALS INVOLVING CONSTRUCTION SAFETY & PHASING AS SHOWN OR DESCRIBED IN THE CONTRACT DOCUMENTS, OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN OR AIRPORT MANAGER THROUGHOUT THE PROJECT, **SHALL BE INCIDENTAL TO THE CONTRACT.**
3. **AIRFIELD SAFETY ASSURANCE** – AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON-COMPLIANCE WITH REQUIREMENTS SET FOR IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR 150/5370-2F WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILEGES ON THE AIRPORT OR A COMPLETE RESTRICTION OF ACCESS TO THE AIRFIELD ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.
4. THE CONTRACTOR SHALL PROVIDE THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN WITH CONTACT INFORMATION FOR A PERSON ON CALL 24 HOURS A DAY FOR EMERGENCY MAINTENANCE OF BARRICADES AND HAZARD LIGHTING.
5. ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" (CURRENT EDITION).
6. CLOSED AIRFIELD WORK AREAS, OPEN TRENCHES, AND OTHER HAZARDOUS AREAS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
7. NO PAVEMENT EDGE DROP-OFFS GREATER THAN 3" ARE ALLOWED TO REMAIN ON ANY ACTIVE AIRFIELD PAVEMENT AREA. TEMPORARY EARTH SHOULDERS SHALL BE PLACED IF AN INCOMPLETE AREA IS TO BE REOPENED TO AIRCRAFT TRAFFIC.
8. **AIRPORT SECURITY** – AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. NO CONTRACTOR PERSONNEL OR EQUIPMENT WILL BE PERMITTED OUTSIDE OF THE CONSTRUCTION LIMITS DEPICTED ON THESE PLANS. PERSONAL VEHICLES OF CONTRACTOR EMPLOYEES SHALL BE PARKED IN THE DESIGNATED STAGING AREA AND ARE NOT ALLOWED ACCESS ON THE SITE. THE CONTRACTOR WILL CLOSE AND LOCK THE GATE AT THE DESIGNATED CONSTRUCTION ENTRANCE AT THE END OF EACH DAY. ALL CONTRACTOR EMPLOYEES SHALL WEAR A GARMENT IDENTIFYING THEM AS CONSTRUCTION PERSONNEL.
9. NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
10. SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
11. EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND ENGINEER. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
12. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
13. THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
14. **RADIO CONTROL** – THE CONTRACTOR WILL BE REQUIRED TO REMAIN CONSTANT TWO-WAY RADIO CONTACT WITH THE OLNEY-NOBLE AIRPORT UNICOM (122.8 MHz) AT ALL TIMES WHILE HE HAS PERSONNEL OR EQUIPMENT ON THE AIRFIELD.



LOW PROFILE BARRICADE EXAMPLE
NOT TO SCALE

BARRICADE NOTES

1. ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
2. LOW PROFILE BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT IN 10' INCREMENTS AS DIRECTED BY THE ENGINEER AND SET BACK FROM THE ACTIVE RUNWAY/TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.
3. CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
4. THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
5. BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
6. THE ONLY COLOR COMBINATION ON LOW PROFILE BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.



TEMPORARY CLOSURE CROSS DETAIL
NOT TO SCALE

TEMPORARY CLOSURE CROSS NOTES

1. TEMPORARY CLOSED RUNWAY AND CLOSED TAXIWAY MARKINGS SHALL BE AVIATION YELLOW.
2. TEMPORARY CLOSED RUNWAY MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
3. TEMPORARY CLOSED RUNWAY MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
4. TEMPORARY CLOSED TAXIWAY MARKINGS SHALL BE PLACED WHEN AN ADJACENT RUNWAY IS OPEN AND THE TAXIWAY IS CLOSED MORE THAN 72 HOURS. THE CLOSED TAXIWAY MARKINGS SHALL BE PLACED AS SHOWN ON THIS SHEET. MARKINGS SHALL BE CONSTRUCTED USING TEMPORARY PAINT SUCH AS SEYMOUR TEMPORARY MARKER, PART NO. 20-636, OR AN EQUIVALENT PAINT EASILY REMOVED WITH WATER WITHOUT DEFACING THE PAVEMENT.
5. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY OR TAXIWAY IS CLOSED AND REMOVED WHEN THE RUNWAY OR TAXIWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES.

OLNEY-NOBLE AIRPORT

2632 E. IL 250
NOBLE, ILLINOIS 62868

INSTALL CULVERT PIPE LINER IN 48" AND 54" CMP BENEATH RUNWAY 3-21

SBG No: 3-17-SBGp-TBD
IDA No: OLY-4535

Contract No. OL016

| NO. | DATE | DESCRIPTION | | |
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DRAWN BY: KBS
REVIEWED BY: RAW 05/24/2016
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SHEET TITLE

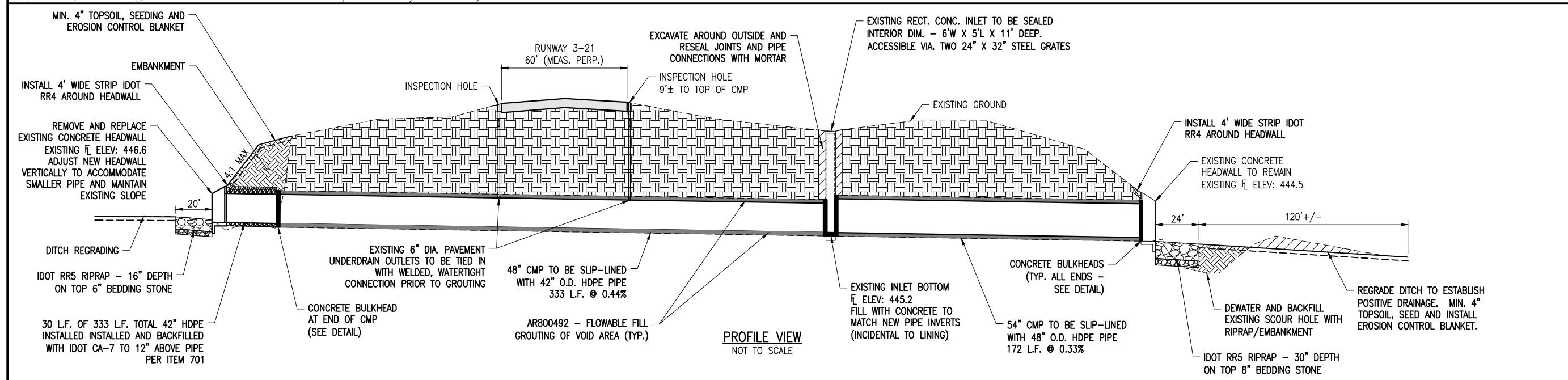
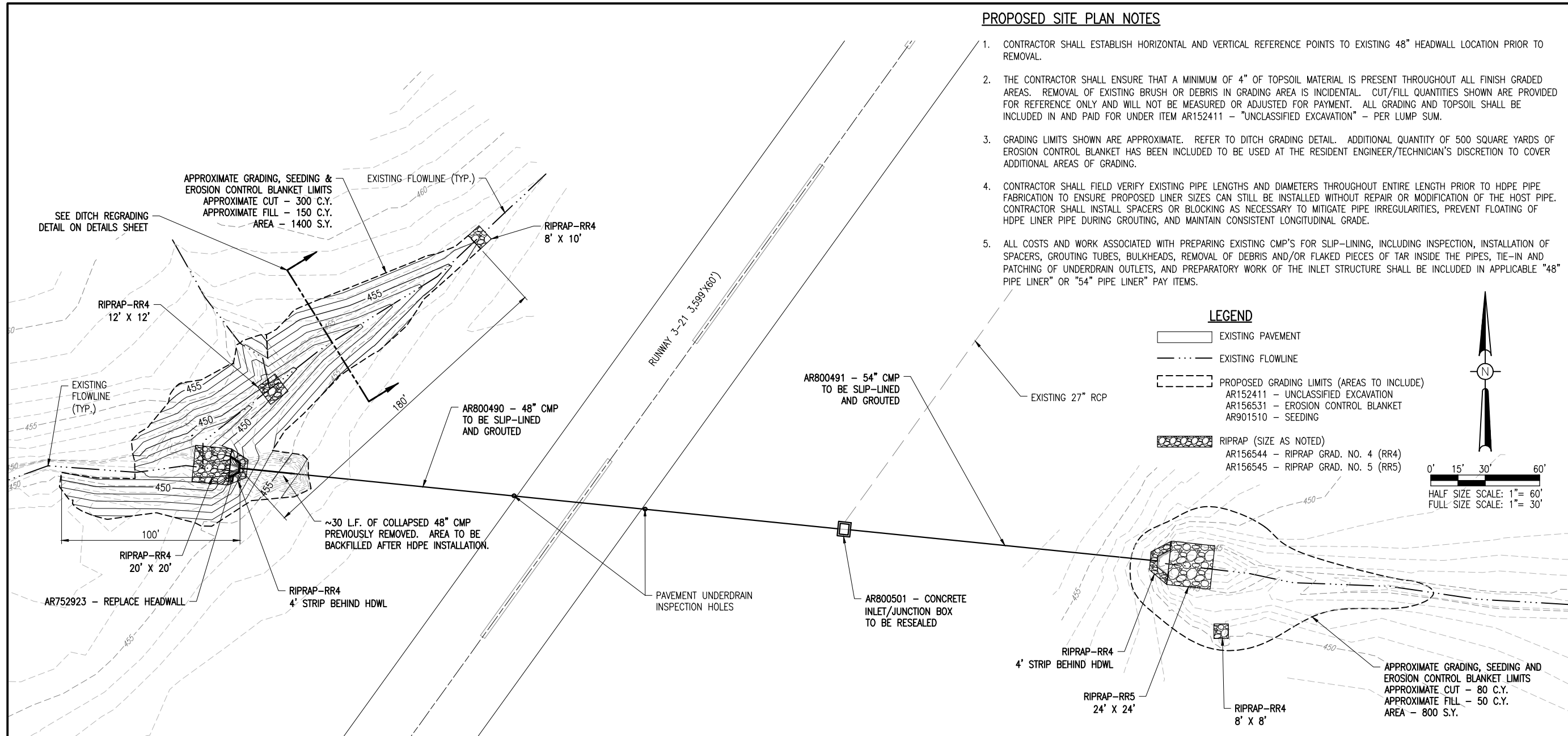
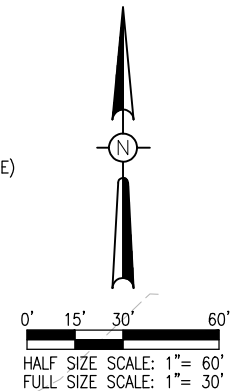
SAFETY NOTES AND DETAILS

PROPOSED SITE PLAN NOTES

1. CONTRACTOR SHALL ESTABLISH HORIZONTAL AND VERTICAL REFERENCE POINTS TO EXISTING 48" HEADWALL LOCATION PRIOR TO REMOVAL.
2. THE CONTRACTOR SHALL ENSURE THAT A MINIMUM OF 4" OF TOPSOIL MATERIAL IS PRESENT THROUGHOUT ALL FINISH GRADED AREAS. REMOVAL OF EXISTING BRUSH OR DEBRIS IN GRADING AREA IS INCIDENTAL. CUT/FILL QUANTITIES SHOWN ARE PROVIDED FOR REFERENCE ONLY AND WILL NOT BE MEASURED OR ADJUSTED FOR PAYMENT. ALL GRADING AND TOPSOIL SHALL BE INCLUDED IN AND PAID FOR UNDER ITEM AR152411 - "UNCLASSIFIED EXCAVATION" - PER LUMP SUM.
3. GRADING LIMITS SHOWN ARE APPROXIMATE. REFER TO DITCH GRADING DETAIL. ADDITIONAL QUANTITY OF 500 SQUARE YARDS OF EROSION CONTROL BLANKET HAS BEEN INCLUDED TO BE USED AT THE RESIDENT ENGINEER/TECHNICIAN'S DISCRETION TO COVER ADDITIONAL AREAS OF GRADING.
4. CONTRACTOR SHALL FIELD VERIFY EXISTING PIPE LENGTHS AND DIAMETERS THROUGHOUT ENTIRE LENGTH PRIOR TO HDPE PIPE FABRICATION TO ENSURE PROPOSED LINER SIZES CAN STILL BE INSTALLED WITHOUT REPAIR OR MODIFICATION OF THE HOST PIPE. CONTRACTOR SHALL INSTALL SPACERS OR BLOCKING AS NECESSARY TO MITIGATE PIPE IRREGULARITIES, PREVENT FLOATING OF HDPE LINER PIPE DURING GROUTING, AND MAINTAIN CONSISTENT LONGITUDINAL GRADE.
5. ALL COSTS AND WORK ASSOCIATED WITH PREPARING EXISTING CMP'S FOR SLIP-LINING, INCLUDING INSPECTION, INSTALLATION OF SPACERS, GROUTING TUBES, BULKHEADS, REMOVAL OF DEBRIS AND/OR FLAKED PIECES OF TAR INSIDE THE PIPES, TIE-IN AND PATCHING OF UNDERDRAIN OUTLETS, AND PREPARATORY WORK OF THE INLET STRUCTURE SHALL BE INCLUDED IN APPLICABLE "48" PIPE LINER" OR "54" PIPE LINER" PAY ITEMS.

LEGEND

- EXISTING PAVEMENT
- EXISTING FLOWLINE
- PROPOSED GRADING LIMITS (AREAS TO INCLUDE)
AR152411 - UNCLASSIFIED EXCAVATION
AR156531 - EROSION CONTROL BLANKET
AR901510 - SEEDING
- RIPRAP (SIZE AS NOTED)
AR156544 - RIPRAP GRAD. NO. 4 (RR4)
AR156545 - RIPRAP GRAD. NO. 5 (RR5)



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LINER IN 48" AND 54"
CMP BENEATH
RUNWAY 3-21**

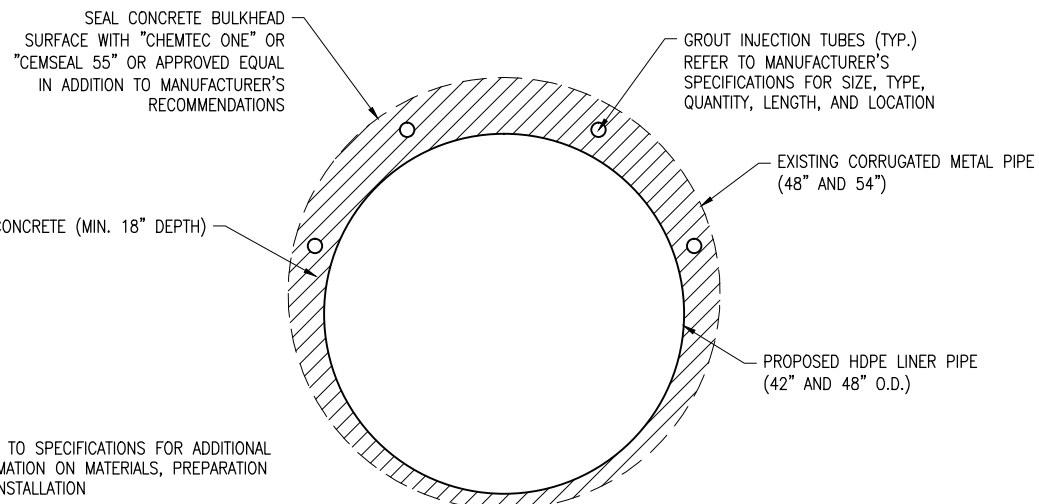
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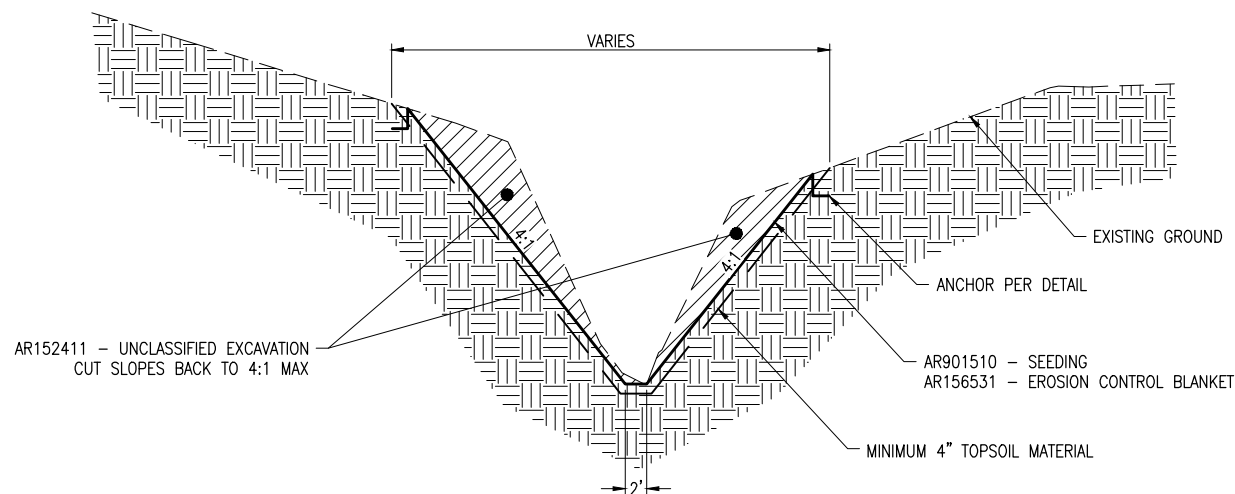
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**PROPOSED SITE
PLAN**

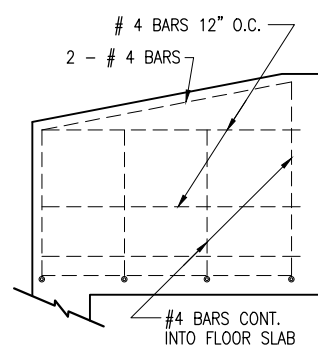


BULKHEAD DETAIL
NOT TO SCALE

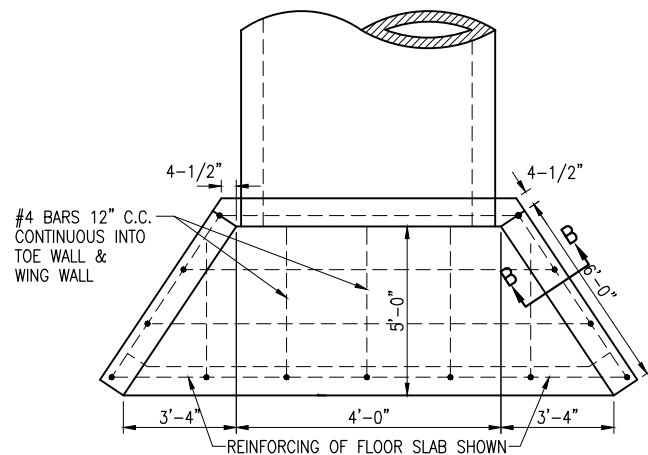
NOTE
REFER TO SPECIFICATIONS FOR ADDITIONAL
INFORMATION ON MATERIALS, PREPARATION
AND INSTALLATION



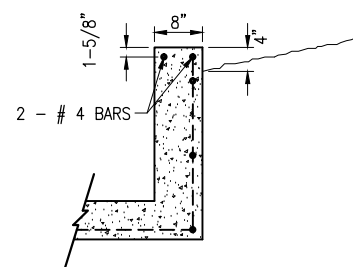
DITCH REGRADE DETAIL
NOT TO SCALE



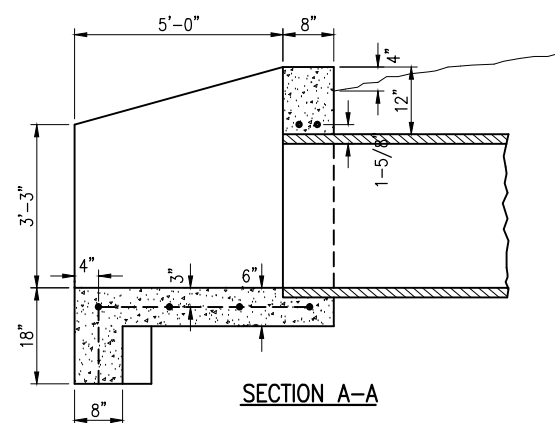
WING WALL REINFORCING



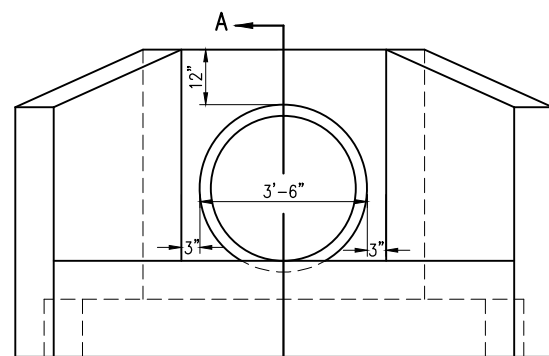
PLAN



SECTION B-B

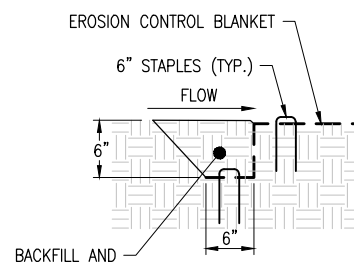


SECTION A-A

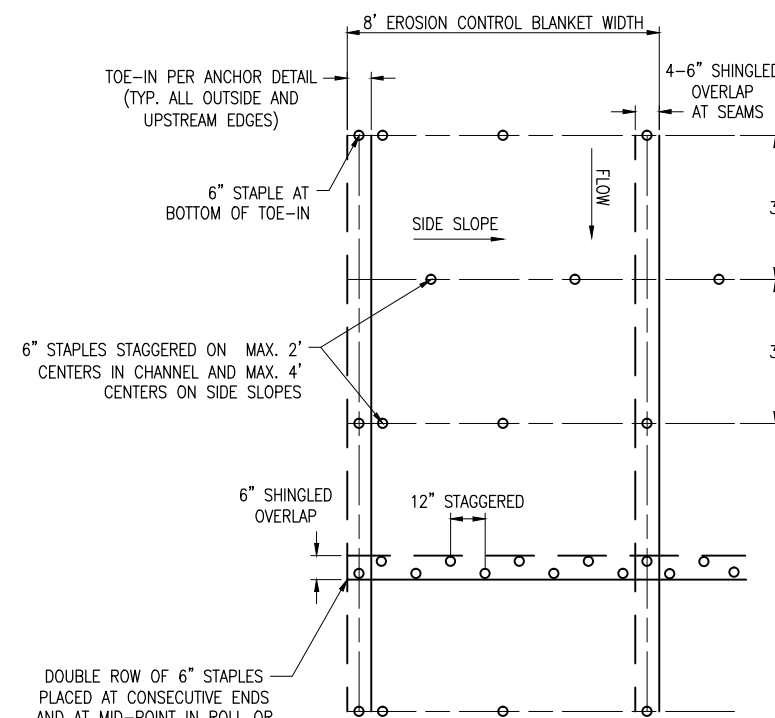


ELEVATION

HEADWALL DETAILS
NOT TO SCALE



ANCHOR DETAIL



EROSION CONTROL BLANKET DETAILS
NOT TO SCALE

NOTE
HEADWALL CONCRETE SHALL BE IDOT CLASS
S.I. AND CONFORM TO ITEM 610.

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CMP BENEATH
RUNWAY 3-21**

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DETAILS