

7A
JULY 29, 2016 LETTING

QU022
TOTAL SHEETS 33

CONSTRUCTION PLANS FOR
QUAD CITY INTERNATIONAL AIRPORT
METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND COUNTY
MOLINE, ILLINOIS

ILLINOIS PROJECT MLI-4531
A.I.P. PROJECT 3-17-0068-XX

CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1

JUNE 3, 2016



Exp: 11/30/17

811 Know what's below. Call before you dig. COMMON GROUND ALLIANCE
www.call811.com or Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

DESIGN INFORMATION

GEOMETRIC CRITERIA

LOW SPEED URBAN STREET

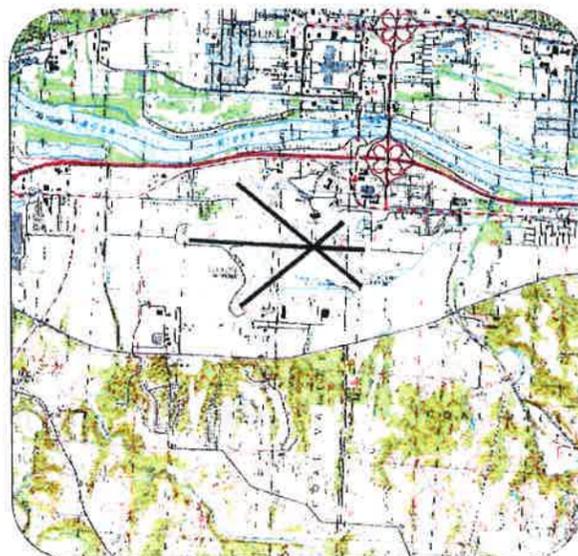
20 MPH DESIGN SPEED

PAVEMENT DESIGN CRITERIA

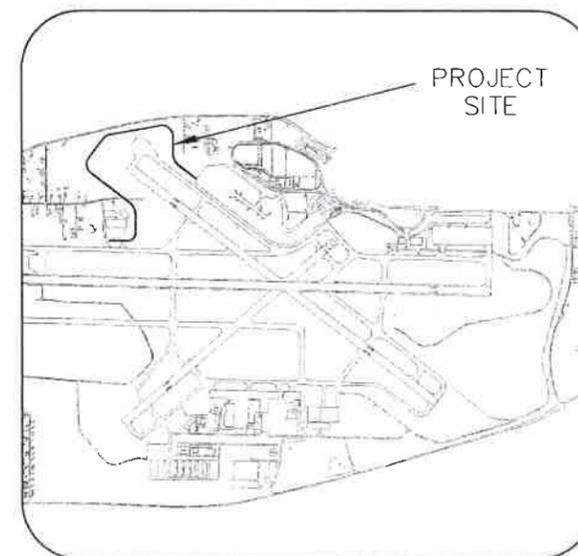
AVERAGE DAILY TRAFFIC (ADT) < 400

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 17N
RANGE: 1W
SECTION: 20 & 21
COUNTY: ROCK ISLAND
CIVIL TOWNSHIP: UNINCORPORATED
ROCK ISLAND COUNTY



LOCATION MAP



SITE PLAN

MAXIMUM EQUIPMENT HEIGHT - 25'
UNICOM FREQUENCY - 122.95

METROPOLITAN AIRPORT
AUTHORITY OF ROCK ISLAND COUNTY
MOLINE, ILLINOIS

APPROVED *Bruce Carter*
DATE *6/7/16*

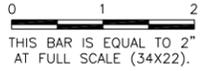
CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS

SUBMITTED BY *[Signature]*
DATE *6/17/16*

CMT JOB NUMBER 15014-06

QU022

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

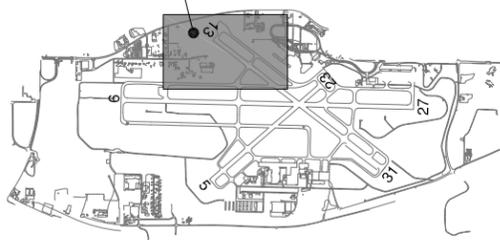
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 INDEX TO SHEETS AND
 SUMMARY OF QUANTITIES**

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR110550	SPLIT DUCT	LF	550.00	
AR150520	MOBILIZATION	LS	1.00	
AR152410	UNCLASSIFIED EXCAVATION	CY	16,000.00	
AR152531	EXPLORATION TRENCH	LF	80.00	
AR152540	SOIL STABILIZATION FABRIC	SY	15,850.00	
AR156510	SILT FENCE	LF	4,750.00	
AR156511	DITCH CHECK	EACH	20.00	
AR156520	INLET PROTECTION	EACH	18.00	
AR208515	POROUS GRANULAR EMBANKMENT	CY	1,610.00	
AR209608	CRUSHED AGG. BASE COURSE - 8"	SY	15,850.00	
AR401610	BITUMINOUS SURFACE COURSE	TON	1,280.00	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	1,050.00	
AR403610	BITUMINOUS BASE COURSE	TON	2,140.00	
AR602510	BITUMINOUS PRIME COAT	GAL	2,850.00	
AR603510	BITUMINOUS TACK COAT	GAL	1,500.00	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	7,500.00	
AR701512	12" RCP, CLASS IV	LF	155.00	
AR701524	24" RCP, CLASS IV	LF	190.00	
AR752412	PRECAST REINFORCED CONC. FES 12"	EACH	4.00	
AR752424	PRECAST REINFORCED CONC. FES 24"	EACH	4.00	
AR800890	38"X24" ELLIPTICAL RCP, CL. III	LF	81.00	
AR800891	PREC. REINF. CONC. EL. FES 38"X24"	EACH	2.00	
AR901510	SEEDING	ACRE	9.00	
AR908510	MULCHING	ACRE	7.30	
AR908520	EXCELSIOR BLANKET	SY	8,300.00	
AR910200	ROADWAY SIGN	EACH	8.00	

INDEX TO SHEETS	
SHEET NUMBER	SHEET TITLE
01	COVER
02	INDEX TO SHEETS AND SUMMARY OF QUANTITIES
03	SITE PLAN AND CONTROL
04	SEQUENCE OF CONSTRUCTION PLAN 1
05	SEQUENCE OF CONSTRUCTION PLAN 2
06	SEQUENCE OF CONSTRUCTION GENERAL NOTES
07	SEQUENCE OF CONSTRUCTION DETAILS
08	STORMWATER POLLUTION PREVENTION PLAN / MARKING / SIGNAGE / LANDSCAPING
09	STORMWATER POLLUTION PREVENTION PLAN NOTES
10	STORMWATER POLLUTION PREVENTION PLAN DETAILS
11	TYPICAL SECTIONS
12	EXISTING CONDITIONS AND REMOVALS 1
13	EXISTING CONDITIONS AND REMOVALS 2
14	PLAN AND PROFILE 1
15	PLAN AND PROFILE 2
16	PLAN AND PROFILE 3
17	PLAN AND PROFILE 4
18	SIGNAGE AND MARKING DETAILS
19	INDEX TO CROSS SECTIONS
20	CROSS SECTIONS 1
21	CROSS SECTIONS 2
22	CROSS SECTIONS 3
23	CROSS SECTIONS 4
24	CROSS SECTIONS 5
25	CROSS SECTIONS 6
26	CROSS SECTIONS 7
27	CROSS SECTIONS 8
28	CROSS SECTIONS 9
29	CROSS SECTIONS 10
30	CROSS SECTIONS 11
31	CROSS SECTIONS 12
32	ENGINEERING INFORMATION 1
33	ENGINEERING INFORMATION 2

DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO	3-17-0068-XX
SHEET	02 OF 33 SHEETS

PROJECT LOCATION

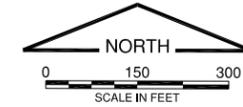


NOTES

1. THE CONTRACTOR SHALL VERIFY THE HORIZONTAL AND VERTICAL CONTROL POINTS PRIOR TO BEGINNING CONSTRUCTION.
2. DISCREPANCIES IN THE CONTROL POINTS SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER AS SOON AS THEY ARE DISCOVERED. THE CONTRACTOR SHALL NOT PROCEED WITH ANY WORK THAT COULD BE IMPACTED BY CONTROL POINT DISCREPANCIES UNTIL RESOLUTION IS COMPLETE.

HORIZONTAL AND VERTICAL CONTROL

POINT	STATION/OFFSET	NORTHING	EASTING	ELEVATION
A - IRON PIN	103+44.41 / 38.20 LT	1742765.13	2200940.77	587.30
B - IRON PIN	111+20.23 / 192.02 RT	1743512.17	2201199.58	587.66
C - IRON PIN	133+80.71 / 68.30 RT	1744726.98	2200958.14	577.71
D - IRON PIN	153+88.38 / 297.09 LT	1744129.01	2202440.87	590.46



LEGEND

- CONTROL POINT - IRON PIN
- NEW BITUMINOUS PAVEMENT
- NEW LANDSCAPING / SEEDING / MULCH / EROSION CONTROL BLANKET

FILE: 03 SITE PLAN.dwg
 UPDATE BY: Travis Strait
 PLOT DATE: 6/9/2016 5:39 PM

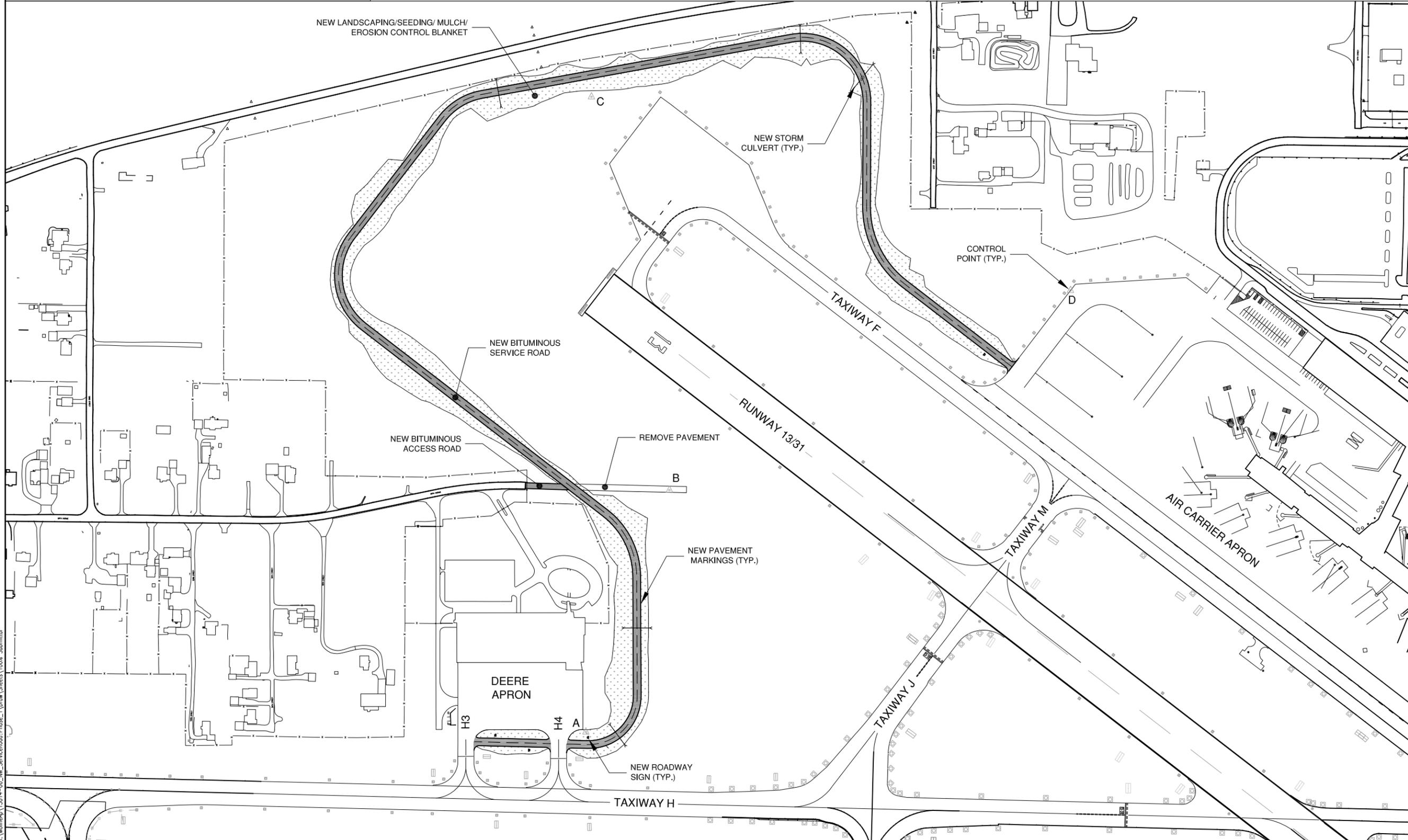
quad-plan
 Existing Topo
 Proposed Geometry
 NW Service Road Base

QUO22

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 SITE PLAN AND CONTROL



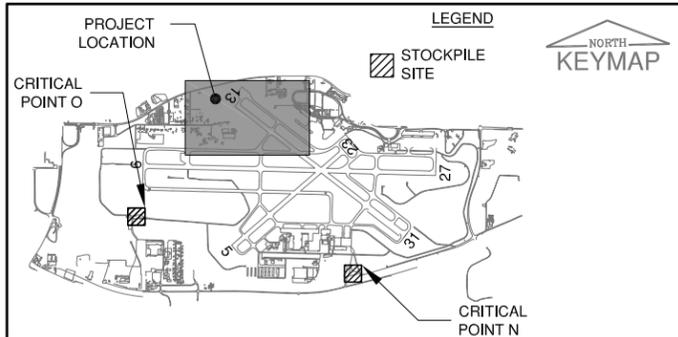
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 DRAWN BY: CMT - MLI
 CHECKED BY: CMT - MLI
 APPROVED BY: CMT - MLI
 DATE: JUNE 3, 2016
 JOB No: 15014-06

IL PROJ. NO. MLI - 4531
 AIP PROJ. NO. 3-17-0068-XX

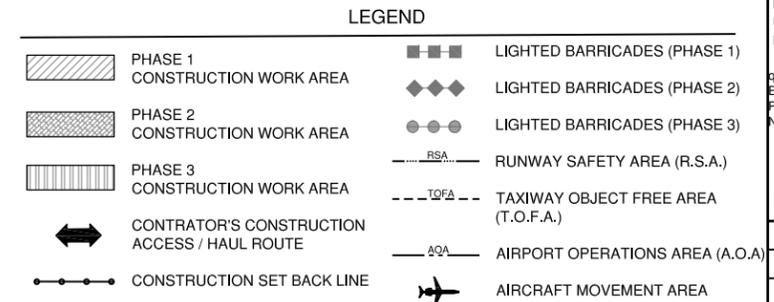
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CRITICAL POINTS			
POINT	LATTITUDE	LONGITUDE	ELEVATION*
A	41° 27' 04.02"	90° 31' 01.24"	611.92
B	41° 27' 03.90"	90° 30' 54.05"	610.36
C	41° 27' 05.60"	90° 30' 53.01"	610.22
D	41° 27' 09.24"	90° 30' 53.05"	611.61
E	41° 27' 11.99"	90° 30' 51.99"	612.00
F	41° 27' 17.40"	90° 31' 04.56"	602.38
G	41° 27' 19.78"	90° 31' 04.26"	597.17
H	41° 27' 21.73"	90° 31' 02.28"	595.30

CRITICAL POINTS			
POINT	LATTITUDE	LONGITUDE	ELEVATION*
I	41° 27' 23.31"	90° 30' 59.64"	590.34
J	41° 27' 24.98"	90° 30' 47.65"	593.61
K	41° 27' 23.74"	90° 30' 45.66"	592.63
L	41° 27' 18.88"	90° 30' 45.12"	606.76
M	41° 27' 14.82"	90° 30' 38.05"	611.00
N	41° 26' 21.47"	90° 30' 00.61"	613.00
O	41° 26' 21.42"	90° 30' 38.46"	604.00

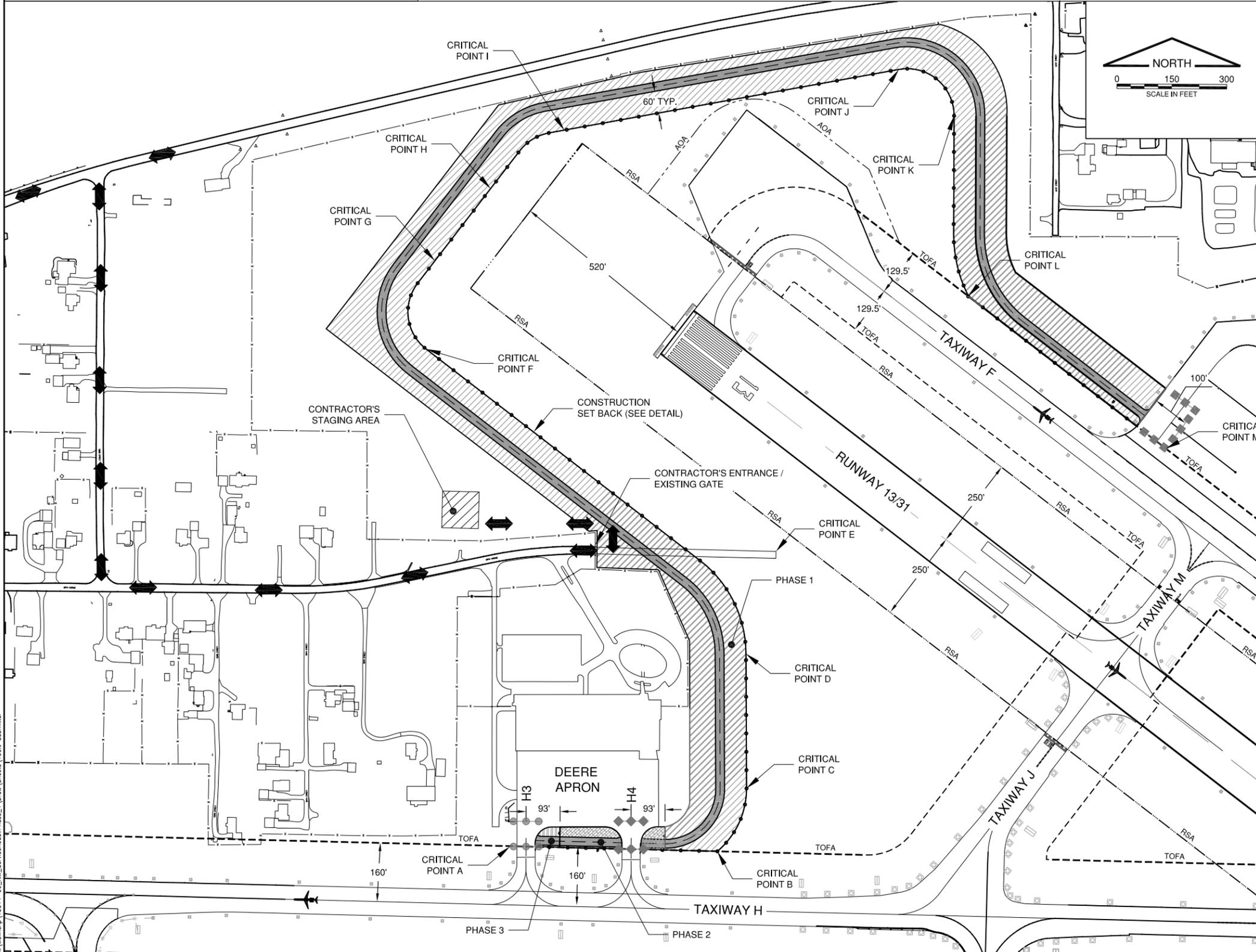
* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT



GENERAL NOTES

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE TAXIWAYS AND RUNWAYS.
- CONSTRUCTION ACTIVITY (PHASE 2 AND 3) WITHIN THE TOFA OF TAXIWAY H3 AND TAXIWAY H4 WILL REQUIRE CLOSURE OF THE CORRESPONDING TAXIWAY. CONTRACTOR SHALL MAINTAIN ACCESS TO THE DEERE RAMP AT ALL TIMES DURING CONSTRUCTION ACTIVITIES. SIMULTANEOUS CLOSURE OF TAXIWAY H3 AND H4 WILL NOT BE PERMITTED. WORK WITHIN THE TOFA SHALL BE COORDINATED TO MINIMIZE IMPACT TO THE AIRPORT.
- GRADING AND LANDSCAPING WORK (PHASE 1A, 2A AND 3A) WITHIN THE TOFA OF TAXIWAY F AND TAXIWAY H WILL REQUIRE CLOSURE OF THE CORRESPONDING TAXIWAY. SIMULTANEOUS CLOSURE OF TAXIWAY H AND F WILL NOT BE PERMITTED. ONLY DAILY CLOSURE OF TAXIWAY F AND H WILL BE ALLOWED. WORK WITHIN THE TOFA SHALL BE COORDINATED TO MINIMIZE IMPACT TO THE AIRPORT.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE CONTRACTOR'S STAGING AREA. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- CONSTRUCTION TRAFFIC SHALL STOP AND YIELD TO AIRCRAFT TRAFFIC WHEN ENTERING AIRCRAFT OPERATIONAL AREA.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND TO ORIGINAL CONDITION BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH ARE OR WILL BE OPENED TO AIR TRAFFIC.
- CONSTRUCTION ACTIVITIES WILL REQUIRE THE CLOSURE OF VARIOUS AIRFIELD PAVEMENTS. ALL PAVEMENT CLOSURES SHALL BE COORDINATED WITH THE AIRPORT AND/OR RESIDENT ENGINEER A MINIMUM OF 72 HOURS PRIOR TO CLOSURE.
- SEE CONSTRUCTION ACTIVITY PLAN NOTES AND DETAILS SHEET FOR NOTES REGARDING THE CONTRACTOR'S ACCESS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- THE LOCATION OF THE BARRICADES SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. SHOULD THE PHASING REQUIRE THE BARRICADES TO BE REPOSITIONED THE WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

GROUND CONTROL FREQUENCY - 121.90
 AIR CONTROL FREQUENCY - 119.40
 MAX. HEIGHT OF EQUIPMENT - 25'

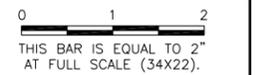


FILE: 04_SEQUENCE 1.dwg
 UPDATE BY: Travis Strait
 PLOT DATE: 6/10/2016 12:01 AM

quad-plan
 Existing Topo
 Proposed Geometry
 NW Service Road Base

QUO22

REVISIONS		
NUMBER	BY	DATE

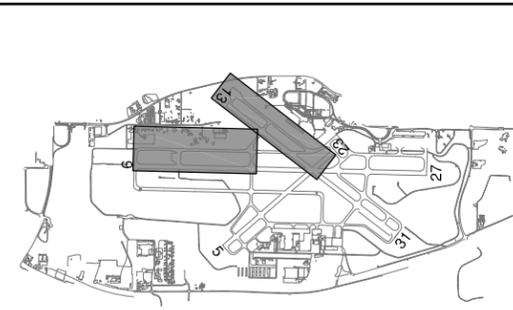


**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 SEQUENCE OF CONSTRUCTION PLAN 1**



DESIGN BY:	CMT - MLI
DRAWN BY:	CMT - MLI
CHECKED BY:	CMT - MLI
APPROVED BY:	CMT - MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO. MLI - 4531	
AIP PROJ. NO. 3-17-0068-XX	
SHEET 04 OF 33 SHEETS	

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CRITICAL POINTS			
POINT	LATTITUDE	LONGITUDE	ELEVATION'
P	41° 27' 18.64"	90° 30' 45.37"	610.01
Q	41° 27' 15.22"	90° 30' 39.38"	613.90
R	41° 27' 03.71"	90° 31' 00.57"	612.02
S	41° 27' 03.61"	90° 30' 54.05"	609.78

* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT

LEGEND	
	PHASE 1A CONSTRUCTION WORK AREA
	PHASE 2A CONSTRUCTION WORK AREA
	PHASE 3A CONSTRUCTION WORK AREA
	PHASE 1A AND PHASE 2A CLOSED PAVEMENTS
	PHASE 3A CLOSED PAVEMENTS
	CONSTRUCTION SET BACK LINE
	LIGHTED BARRICADES (PHASE 1A)
	LIGHTED BARRICADES (PHASE 2A)
	LIGHTED BARRICADES (PHASE 3A)
	RUNWAY SAFETY AREA (R.S.A.)
	TAXIWAY OBJECT FREE AREA (T.O.F.A.)
	AIRCRAFT MOVEMENT AREA

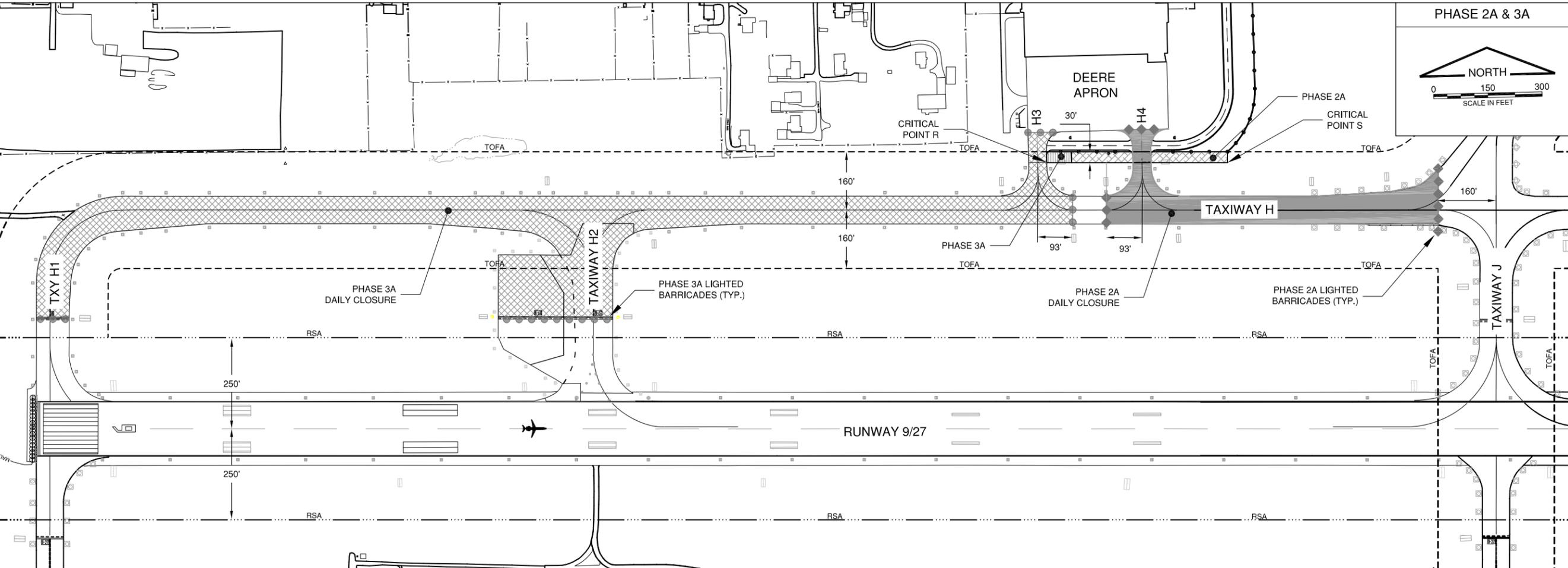
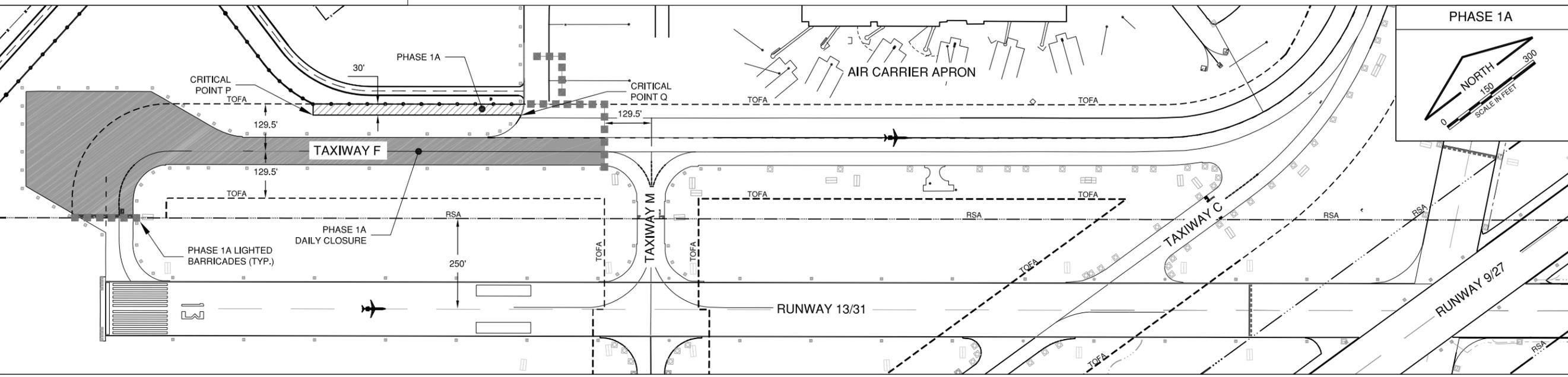
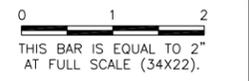
SEE SEQUENCE OF CONSTRUCTION PLAN 1 FOR GENERAL NOTES

FILE: 05 SEQUENCE 2.dwg
 UPDATE BY: Travis Strait
 PLOT DATE: 6/9/2016 6:34 PM

quad-plan
 Existing Topo
 Proposed Geometry
 NW Service Road Base

QUO22

REVISIONS		
NUMBER	BY	DATE



METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 SEQUENCE OF CONSTRUCTION PLAN 2



DESIGN BY:	CMT - MLI
DRAWN BY:	CMT - MLI
CHECKED BY:	CMT - MLI
APPROVED BY:	CMT - MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO. MLI - 4531	
AIP PROJ. NO. 3-17-0068-XX	
SHEET 05 OF 33 SHEETS	

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GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL.
- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ONSITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT. PAYMENT FOR DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DIRECTOR OF AVIATION OR HIS/HER DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD CIRCUITS IMPACTED BY CONSTRUCTION ACTIVITY.
- CONTRACTOR SHALL PLAN HIS/HER WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORT AND SPECIAL PROVISIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:

- CONSTRUCT TAXIWAY K CONNECTOR (K12)
- REHABILITATE TAXIWAY H2
- RECONSTRUCT TAXIWAY G

2. PHASING

- TOTAL CONTRACT TIME SHALL BE 68 CALENDAR DAYS.
- PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
- SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

- THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

- CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- THE CONTRACTOR IS TO ACCESS THE SITE USING THE EXISTING GATE SHOWN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLES ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO CITY OF MOLINE AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.
- CERTAIN CONTRACTOR EMPLOYEES SHALL OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
- ALL CONTRACTOR EMPLOYEES WHO ARE DESIGNATED AS DRIVERS FOR THE CONTRACTOR WITHIN THE AIRFIELD OPERATION AREA (AOA) SHALL ALSO ATTEND AND PASS THE AIRPORT DRIVERS TRAINING PROGRAM. PERMITTED TO OPERATE VEHICLES OR EQUIPMENT ON THE AIRPORT WITHOUT AN ESCORT. ALL COST ASSOCIATED WITH THE DRIVERS TRAINING PROGRAM SHALL BE BORNE BY THE CONTRACTOR.
- DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
- THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN AND CONSTRUCTION PHASING PLAN.
- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
- WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
- ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS, STORAGE AREAS AND/OR STAGING AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY THE AIRCRAFT RESCUE AND FIRE FIGHTING (ARFF) FACILITY IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
- THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
- CONTACTS FOR THIS PROJECT ARE AS LISTED BELOW.

AIRPORT OPERATOR
 BRUCE CARTER - DIRECTOR OF AVIATION (309) 757-1732
 KEN CARLEY - ASSISTANT DIRECTOR OF AVIATION (309) 757-1754

ENGINEER
 CMT - RESIDENT ENGINEER (309) 278-6214

10. INSPECTION REQUIREMENTS

- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F MAY BE USED TO AID IN THE INSPECTIONS.
- THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. SEE SECTION 70-17 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
- BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.
- SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.
- THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT DIRECTOR OF AVIATION. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

12. PENALTIES (CONT.)

- THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DIRECTOR OF AVIATION, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- RUNWAY OR TAXIWAY CLOSURES ARE AS DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2F.

15. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F AND 150/5210-5D AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND CONSTRUCTION SETBACK LINES ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, REPOSITION, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
- ACCESS TO ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON TYPE II BARRICADES (2 EACH, RIGHT AND LEFT). IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE".

16. PROTECTION

- CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.

17. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMMODATE AIRPORT EVENTS (I.E. AIRSHOW). SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS.
- THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
- BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

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 UPDATE BY: Trovis Strait
 PLOT DATE: 6/9/2016 5:59 PM

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REVISIONS		
NUMBER	BY	DATE

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 AT FULL SCALE (34X22).

METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS
 CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 SEQUENCE OF CONSTRUCTION GENERAL
 NOTES



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DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO.	3-17-0068-XX
SHEET	06 OF 33 SHEETS

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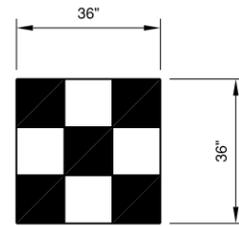
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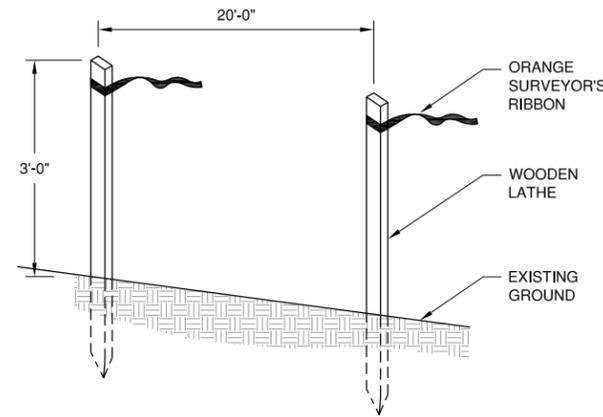
**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**
CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
SEQUENCE OF CONSTRUCTION DETAILS



DESIGN BY:	CMT-MLI
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CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
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SHEET 07	OF 33 SHEETS



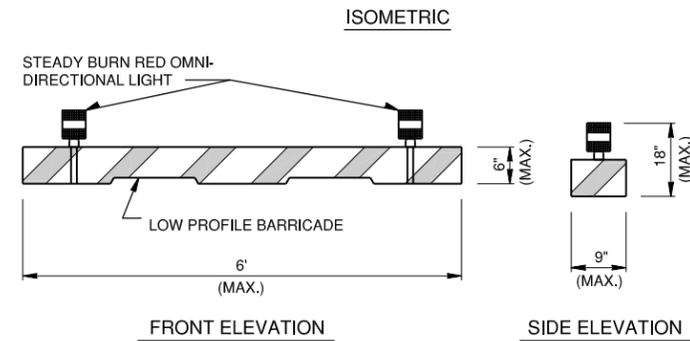
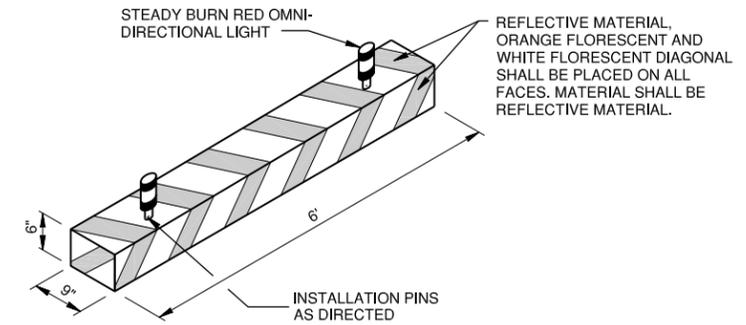
**CONSTRUCTION EQUIPMENT
 AND TRUCK SIGNAL FLAG**



CONSTRUCTION SETBACK LINE DETAIL
 NOT TO SCALE

CONSTRUCTION SETBACK NOTES

1. CONTRACTOR SHALL MARK THE CONSTRUCTION SETBACK LINE AS SHOWN ON THE SEQUENCE OF CONSTRUCTION PLAN PER THE CONSTRUCTION SETBACK DETAIL.
2. ALL COST ASSOCIATED WITH THE CONSTRUCTION SETBACK LINE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

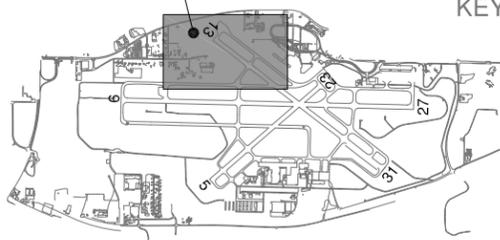


INTERLOCKING LOW PROFILE BARRICADE DETAILS
 NOT TO SCALE

INTERLOCKING LOW PROFILE BARRICADE NOTES

1. LOW PROFILE BARRICADES SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THE BARRICADES SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES.
2. BARRICADES SHALL BE WEIGHTED TO WITHSTAND DISPLACEMENT BY JET OR PROP BLAST.
2. THE BARRICADE LINE SHALL EXTEND ONE BARRICADE PAST THE EDGE OF PAVEMENT INTO THE TURF.
4. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT'S COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
6. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

PROJECT LOCATION



NOTES

- ADDITIONAL EROSION CONTROL MEASURES REQUIRED AT THE CONTRACTOR STAGING AND STORAGE AREA SHALL BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR SHALL ENSURE THAT THE EXISTING DRAINAGE SWALE IS PROTECTED FROM SILTATION AND CONSTRUCTION TRAFFIC AT ALL TIMES, COSTS INCLUDED IN THE CONTRACT.
- FLARED END SECTIONS AND HEADWALLS REQUIRING PROTECTION SHALL HAVE A MINIMUM OF FOUR STRAW BALES PLACED IN ACCORDANCE WITH THE DETAIL.
- SEE CROSS SECTIONS FOR TURF GRADING LIMITS.
- A 4-INCH THICK LAYER OF TOPSOIL SHALL BE SPREAD OVER THE DISTURBANCE LIMITS TO PROMOTE THE ESTABLISHMENT OF TURF GRASSES. THE CONTRACTOR SHALL SEED THE DISTURBED LIMITS WITHIN FOURTEEN (14) DAYS OF FINISH GRADING.
- EXCESS TOPSOIL SHALL BE SPREAD AT A 4-INCH THICK LAYER ON SITE AT LOCATIONS AS DIRECTED BY THE RESIDENT ENGINEERING. IF ADDITIONAL TOPSOIL REMAINS, CONTRACTOR SHALL STOCKPILE ON AIRPORT PROPERTY AT LOCATION DESIGNATED BY THE RESIDENT ENGINEER.
- SILT FENCE SHALL BE INSTALLED AT THE TOP OF SLOPE OF EXISTING DRAINAGE SWALE.
- DITCH CHECKS SHALL BE INSTALLED ON DITCHES WITH SLOPES OF 3% OR GREATER.
- EROSION CONTROL BLANKET SHALL BE INSTALLED CENTERED ON ALL DITCH FLOWLINES AT A WIDTH OF 10'.
- SEE SIGNAGE SCHEDULE ON SHEET 18 FOR ADDITIONAL INFORMATION.

LEGEND

- NEW BITUMINOUS PAVEMENT
- NEW SEEDING AND MULCHING
- NEW SEEDING AND EROSION CONTROL BLANKET
- NEW GRADING AND LANDSCAPING LIMITS
- NEW NLET PROTECTION
- NEW SILT FENCE
- NEW DITCH CHECK
- NEW PAVEMENT MARKING
- NEW ROADWAY SIGN
- SIGN LOCATION IDENTIFIER (SEE NOTE 10)
- EXISTING MANOLE / INLET / CATCH BASIN
- EXISTING FLARED END SECTION / HEADWALL
- EXISTING STORM SEWER

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 UPDATE BY: Travis Strait
 PLOT DATE: 6/14/2016 3:54 PM

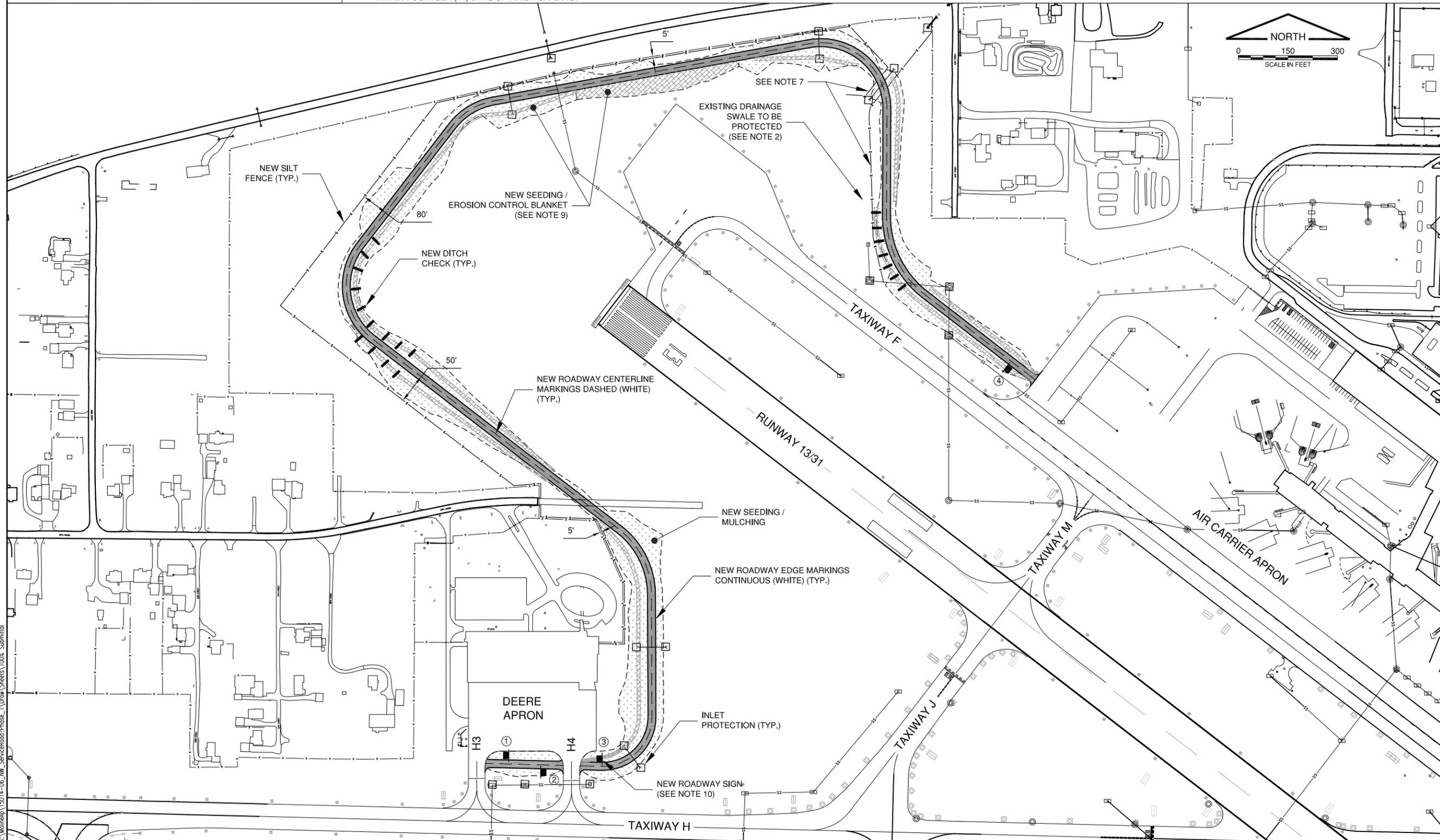
Service Road Base
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 Proposed Geometry
 NW Service Road Base

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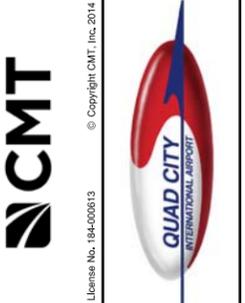
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METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1

STORMWATER POLLUTION PREVENTION PLAN /
 MARKING / SIGNAGE / LANDSCAPING



DESIGN BY:	CMT - MLI
DRAWN BY:	CMT - MLI
CHECKED BY:	CMT - MLI
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DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO	MLI - 4531
AIP PROJ. NO.	3-17-0068-XX
SHEET	08 OF 33 SHEETS

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STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTION OF THE NORTHWEST SERVICE ROAD AT THE QUAD CITY INTERNATIONAL AIRPORT. THE PROJECT INCLUDES GRADING, EXCAVATION, FILL, TOPSOIL PLACEMENT, PAVEMENT CONSTRUCTION, LANDSCAPING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS PERIMETER SILT FENCE AND INLET PROTECTION.
2. EXCAVATION AND EMBANKMENT WILL BE COMPLETED WITHIN THE PROJECT LIMITS.
3. PAVEMENT CONSTRUCTION.
4. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCHING.
6. REMOVAL OF TEMPORARY EROSION CONTROL / PROTECTION FACILITIES.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 10 ACRES OF WHICH 10 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE ROCK RIVER VIA OVERLAND FLOW AND THROUGH A STORM SEWER SYSTEM.

CONTROLS - EROSION CONTROLS AND SEDIMENT CONTROL

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS POSSIBLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
2. AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTION AND PERIMETER SILT FENCE SHALL BE INSTALLED AS CALLED OUT IN THE PLANS OR AS DIRECTED BY THE ENGINEER.
3. THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
- B. CONSTRUCT DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
- C. BUILD NECESSARY EMBANKMENT AT CULVERT/STORM SEWER LOCATIONS AND THEN EXCAVATE AND PLACE PIPE.
- D. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

5. THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2 INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD.

6. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

7. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ALLOWABLE NON-STORM WATER DISCHARGES:

1. DISCHARGES FROM FIRE-FIGHTING ACTIVITIES;
2. FIRE HYDRANT FLUSHINGS;
3. WATER USED TO CONTROL DUST;
4. POTABLE WATER INCLUDING UNCONTAMINATED WATER LINE FLUSHINGS;
5. UNCONTAMINATED GROUND WATER OR SPRING WATER;
6. UNCONTAMINATED EXCAVATION DEWATERING;
7. LANDSCAPE IRRIGATION;

RUNOFF COEFFICIENT AFTER CONSTRUCTION = 0.90

MAINTENANCE:

WHEN REQUESTED BY THE CONTRACTOR, THE RESIDENT ENGINEER WILL PROVIDE GENERAL MAINTENANCE GUIDES TO THE CONTRACTOR FOR THE PRACTICES ASSOCIATED WITH THIS PROJECT. THE FOLLOWING ADDITIONAL PROCEDURES WILL BE USED TO MAINTAIN, IN GOOD AND EFFECTIVE OPERATING CONDITIONS, THE VEGETATION, EROSION AND SEDIMENT CONTROL MEASURES AND OTHER PROTECTIVE MEASURES IDENTIFIED IN THIS PLAN. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ATTAIN MAINTENANCE GUIDELINES FOR ANY MANUFACTURED BMPS WHICH ARE TO BE INSTALLED AND MAINTAINED PER MANUFACTURE'S SPECIFICATIONS.

1. SEEDING - WHERE TEMPORARY CESSATION OF THE EARTH DISTURBING ACTIVITIES OCCURS, STABILIZATION OF DISTURBED AREAS MUST BE INITIATED WITHIN 1 WORKING DAY IF WORK WILL NOT RESUME FOR A PERIOD EXCEEDING 14 DAYS.
2. PERIMETER EROSION BARRIER - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE FENCING IS IN JEOPARDY AND ANY FENCE KNOCKED DOWN WILL BE REPAIRED IMMEDIATELY.
3. DITCH CHECK - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE DITCH CHECK IS IN JEOPARDY. ANY DITCH CHECKS WHICH FAILS WILL BE REPAIRED OR REPLACED IMMEDIATELY.
4. INLET PROTECTION/FILTERS - SEDIMENT WILL BE REMOVED IF THE INTEGRITY OF THE INLET PROTECTION IS IN JEOPARDY. ANY INLET PROTECTION FILTERS WHICH FAILS WILL BE REPLACED IMMEDIATELY.

INSPECTIONS:

QUALIFIED PERSONNEL SHALL INSPECT DISTURBED AREAS OF THE CONSTRUCTION SITE WHICH HAVE NOT YET BEEN FINALLY STABILIZED, STRUCTURAL CONTROL MEASURES, AND LOCATIONS WHERE VEHICLES AND EQUIPMENT ENTER AND EXIT THE SITE USING IDOT STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (BC 2259). SUCH INSPECTIONS SHALL BE CONDUCTED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN TWENTY-FOUR (24) HOURS OF THE END OF A STORM OR BY THE END OF THE FOLLOWING BUSINESS OR WORK DAY THAT IS 0.5 INCH OR GREATER OR EQUIVALENT SNOWFALL.

INSPECTIONS MAY BE REDUCED TO ONCE PER MONTH WHEN CONSTRUCTION ACTIVITIES HAVE CEASED DUE TO FROZEN CONDITIONS. WEEKLY INSPECTIONS WILL RECOMMENCE WHEN CONSTRUCTION ACTIVITIES ARE CONDUCTED, OR IF THERE IS 0.5" OR GREATER RAIN EVENT, OR A DISCHARGE DUE TO SNOWMELT OCCURS. IF ANY VIOLATION OF THE PROVISIONS OF THIS PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION WORK COVERED BY THIS PLAN, THE RESIDENT ENGINEER SHALL NOTIFY THE APPROPRIATE IEPA FIELD OPERATIONS SECTION OFFICE BY EMAIL AT: EPA.SWNONCOMP@ILLINOIS.GOV, TELEPHONE OR FAX WITHIN TWENTY-FOUR (24) HOURS OF THE INCIDENT. THE RESIDENT ENGINEER SHALL THEN COMPLETE AND SUBMIT AN "INCIDENCE OF NON-COMPLIANCE" (ION) REPORT FOR THE IDENTIFIED VIOLATION WITHIN FIVE (5) DAYS OF THE INCIDENT. THE RESIDENT ENGINEER SHALL USE FORMS PROVIDED BY IEPA AND SHALL INCLUDE SPECIFIC INFORMATION ON THE CAUSE OF NONCOMPLIANCE, ACTIONS WHICH WERE TAKEN TO PREVENT ANY FURTHER CAUSES OF NONCOMPLIANCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NON-COMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G OF THE PERMIT ILR10. THE INCIDENCE OF NON-COMPLIANCE SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL
ATTN: COMPLIANCE ASSURANCE SECTION
1021 NORTH GRAND EAST
POST OFFICE BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276
ADDITIONAL INSPECTIONS REQUIRED:

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

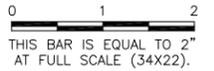
MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

QUO22

REVISIONS

NUMBER	BY	DATE



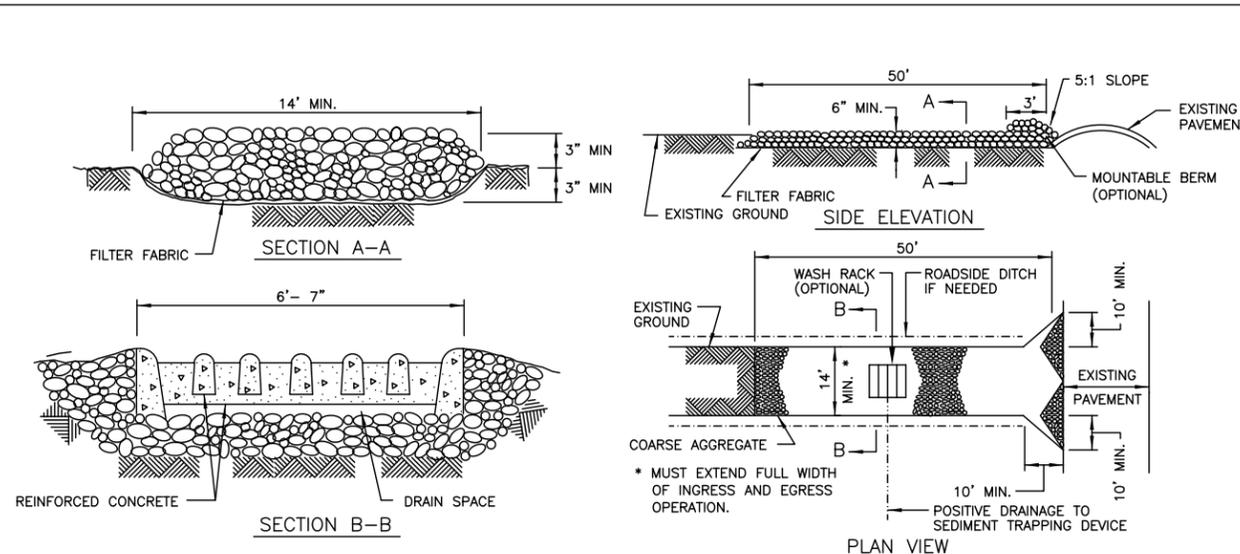
**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1

**STORMWATER POLLUTION PREVENTION PLAN
NOTES**

STABILIZED CONSTRUCTION ENTRANCE NOTES

1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED UNDER SECTION 1080.03, OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED APRIL 1, 2016.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. COMPACTION SHALL BE TO THE SATISFACTION OF THE ENGINEER.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. IF WASH RACK ARE USED THEY SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.
7. THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE INCIDENTAL TO THE CONTRACT.



STABILIZED CONSTRUCTION ENTRANCE
FROM NRCS STANDARD DRAWING NO. IL-630



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DRAWN BY:	CMT - MLI
CHECKED BY:	CMT - MLI
APPROVED BY:	CMT - MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL. PROJ. NO MLI - 4531 AIP PROJ. NO. 3-17-00XX-XX	
SHEET	09 OF 33 SHEETS

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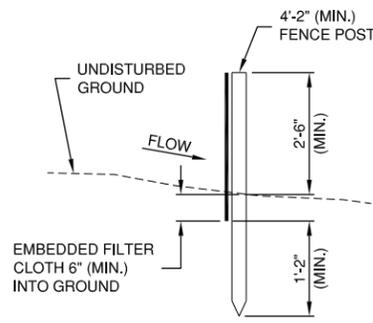
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 STORMWATER POLLUTION PREVENT PLAN
 DETAILS**

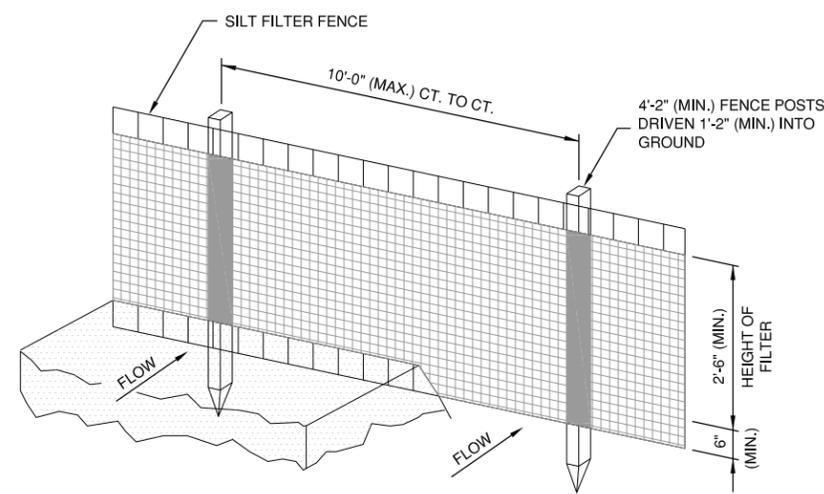


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 AIP PROJ. NO. 3-17-0068-XX



SECTION

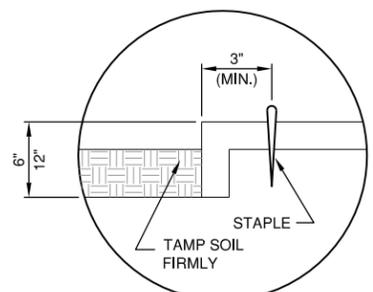


PERSPECTIVE VIEW

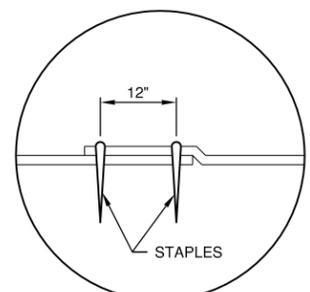
EROSION CONTROL FABRIC FENCE DETAILS
 N.T.S.

EROSION CONTROL FABRIC FENCE NOTES

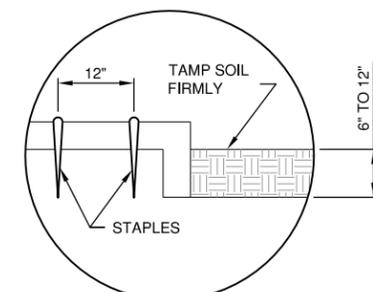
1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 2'-0" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY 6" MINIMUM AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE EROSION CONTROL FENCE.



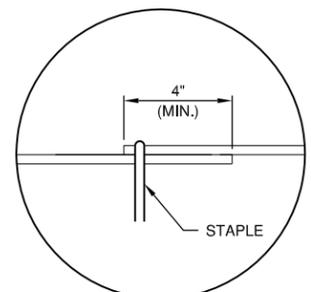
DETAIL 1 - TERMINAL FOLD



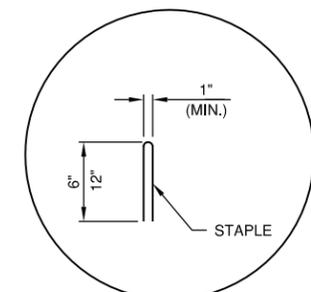
DETAIL 2 - JUNCTION SLOT



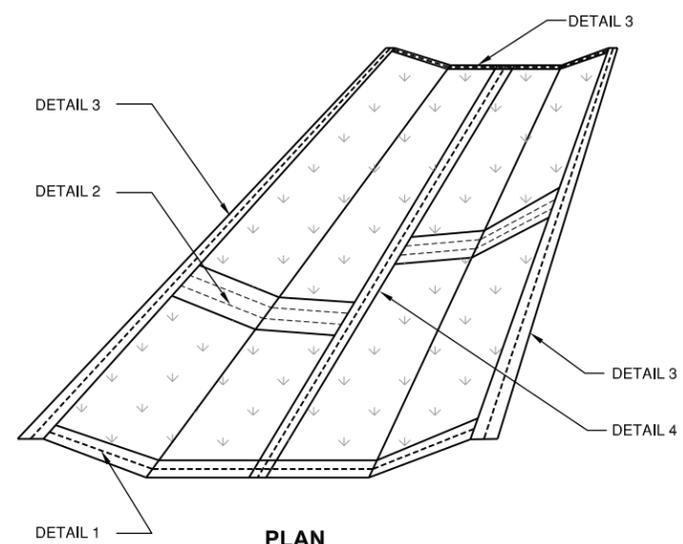
DETAIL 3 - ANCHOR SLOT



DETAIL 4 - LAP JOINT



DETAIL 5 - STAPLE DETAIL

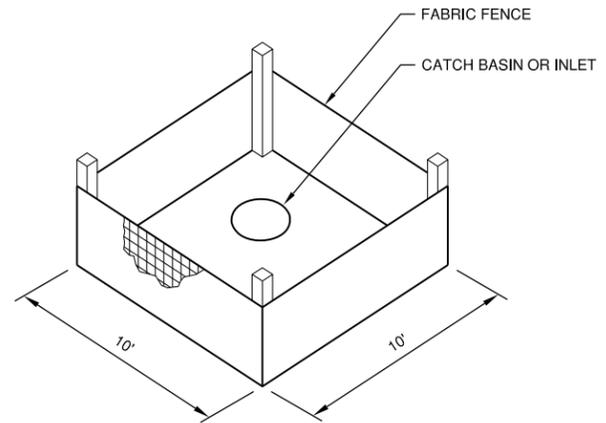


PLAN

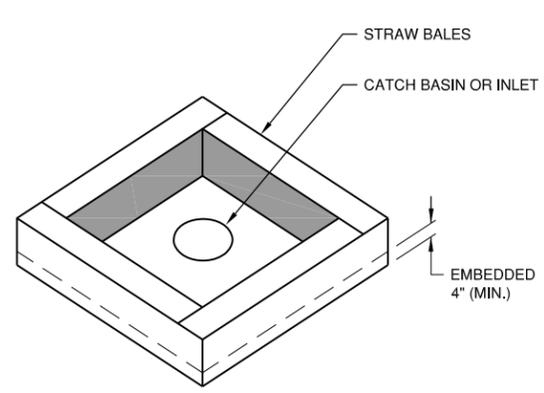
EXCELSIOR BLANKET DETAILS
 N.T.S.

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINALS ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



INLET PROTECTION WITH FABRIC
 N.T.S.

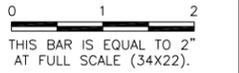


INLET PROTECTION WITH STRAW BALES
 N.T.S.

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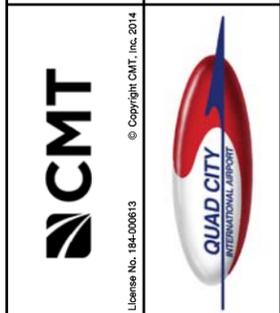
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NUMBER	BY	DATE



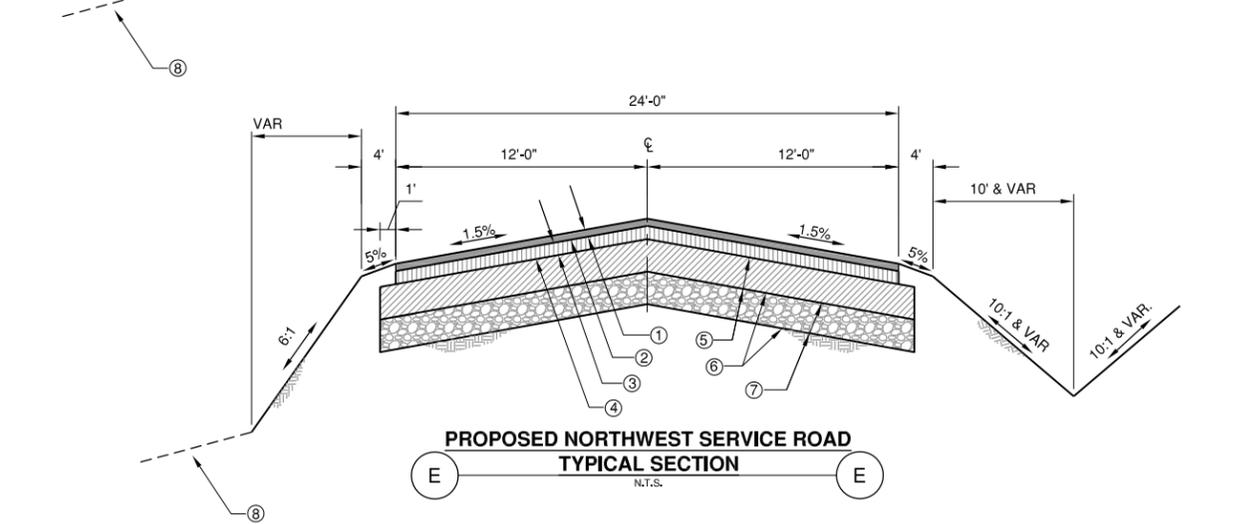
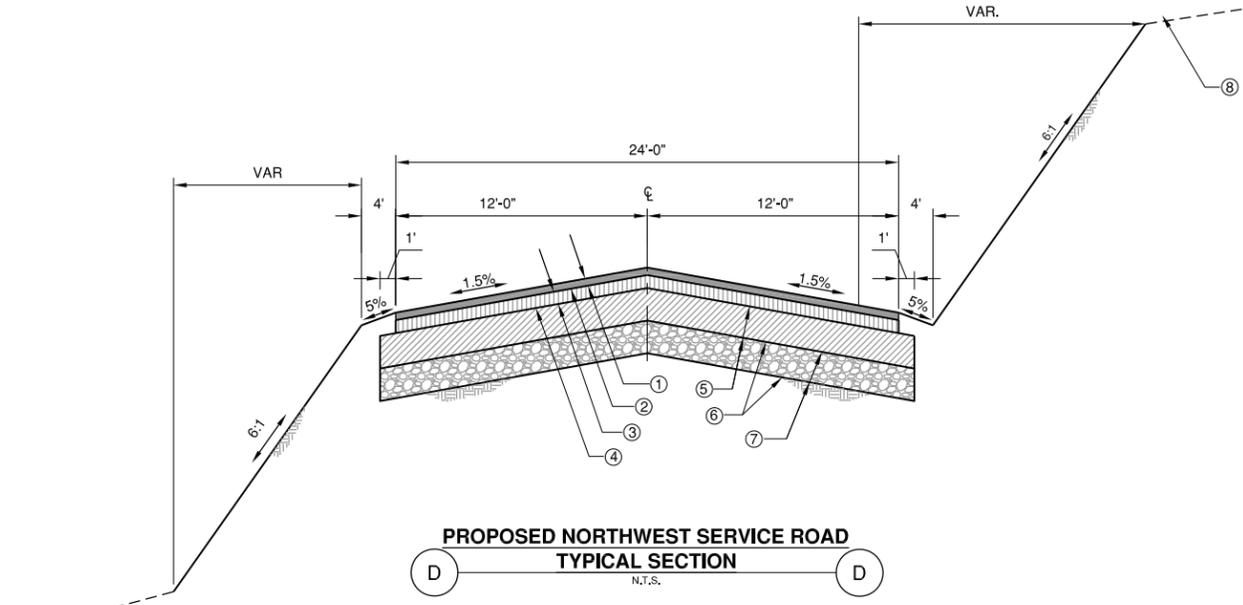
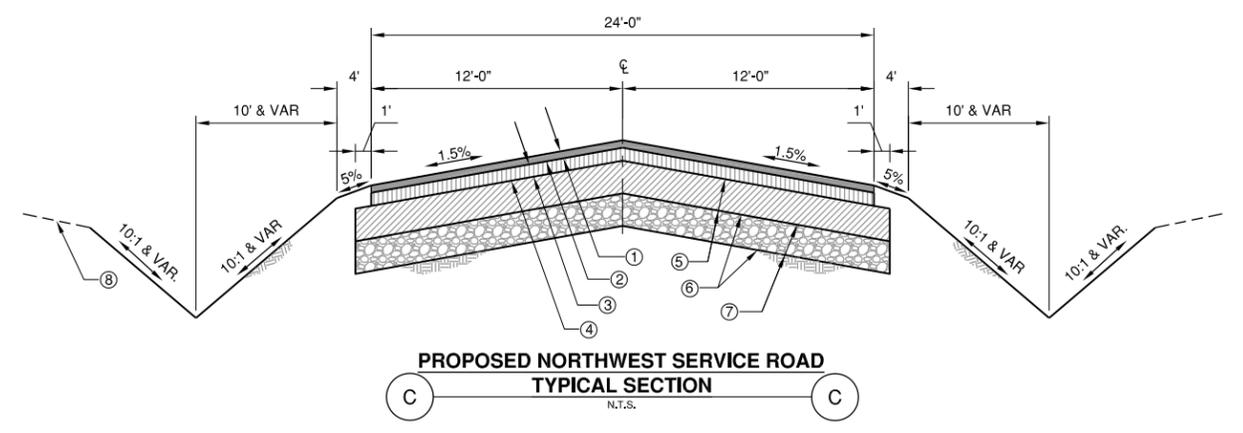
**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1

TYPICAL SECTIONS

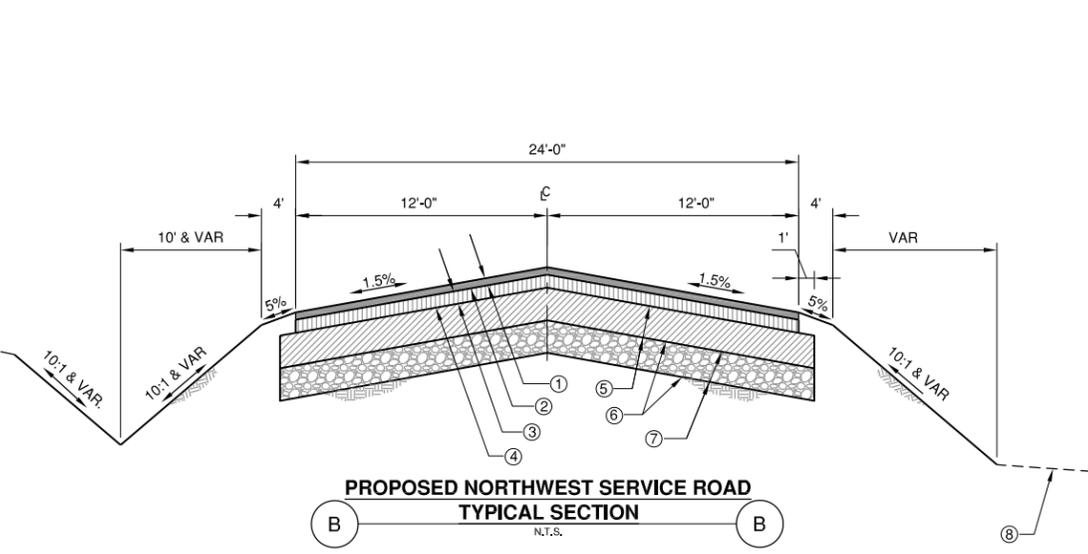
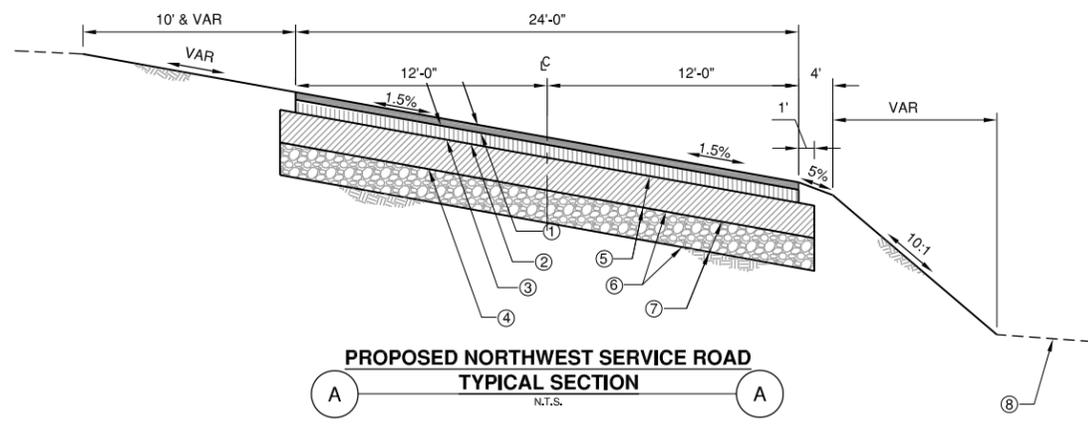


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APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO.	3-17-0068-XX
SHEET	11 OF 33 SHEETS

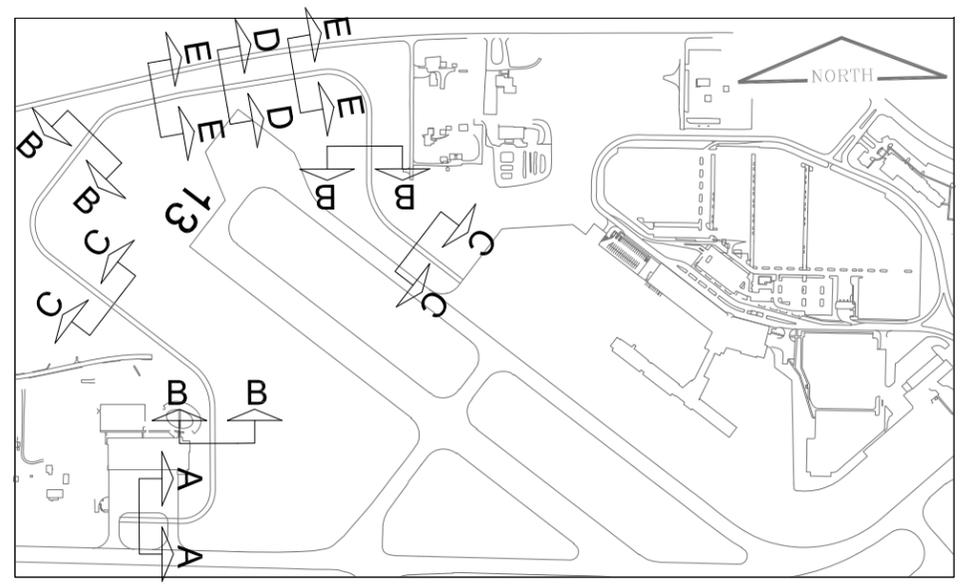


LEGEND

- | | |
|---|---|
| 1. 1.5" BITUMINOUS SURFACE COURSE (401) | 5. 8" CRUSHED AGGREGATE BASE COURSE (209) |
| 2. BITUMINOUS TACK COAT (603) | 6. SOIL STABILIZATION FABRIC (152) |
| 3. 2.5" BITUMINOUS BASE COURSE (403) | 7. 12" POROUS GRANULAR EMBANKMENT (208)
(AT LOCATION SHOWN ON PLANS / AS REQUIRED) |
| 4. BITUMINOUS PRIME COAT (602) | 8. EXISTING GROUND |



KEYMAP

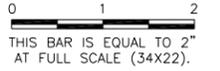


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quad-plan
 Existing Topo
 Proposed Geometry
 NW Service Road Base

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NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 EXISTING CONDITIONS AND REMOVALS 1**



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SHEET	12 OF 33 SHEETS

REMOVALS LEGEND

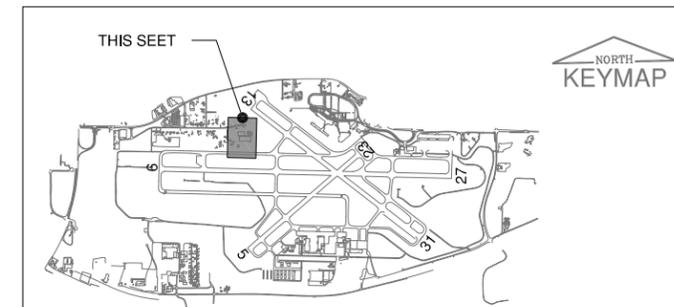
- REMOVE BITUMINOUS PAVEMENT (AR401900) (SEE NOTES 1 AND 2)
- POROUS GRANULAR EMBANKMENT (UNDERCUT) (AR152410 AND AR208515) (SEE NOTE 6)
- SPLIT DUCT
- P** ITEM TO BE PROTECTED

EXISTING FEATURES LEGEND

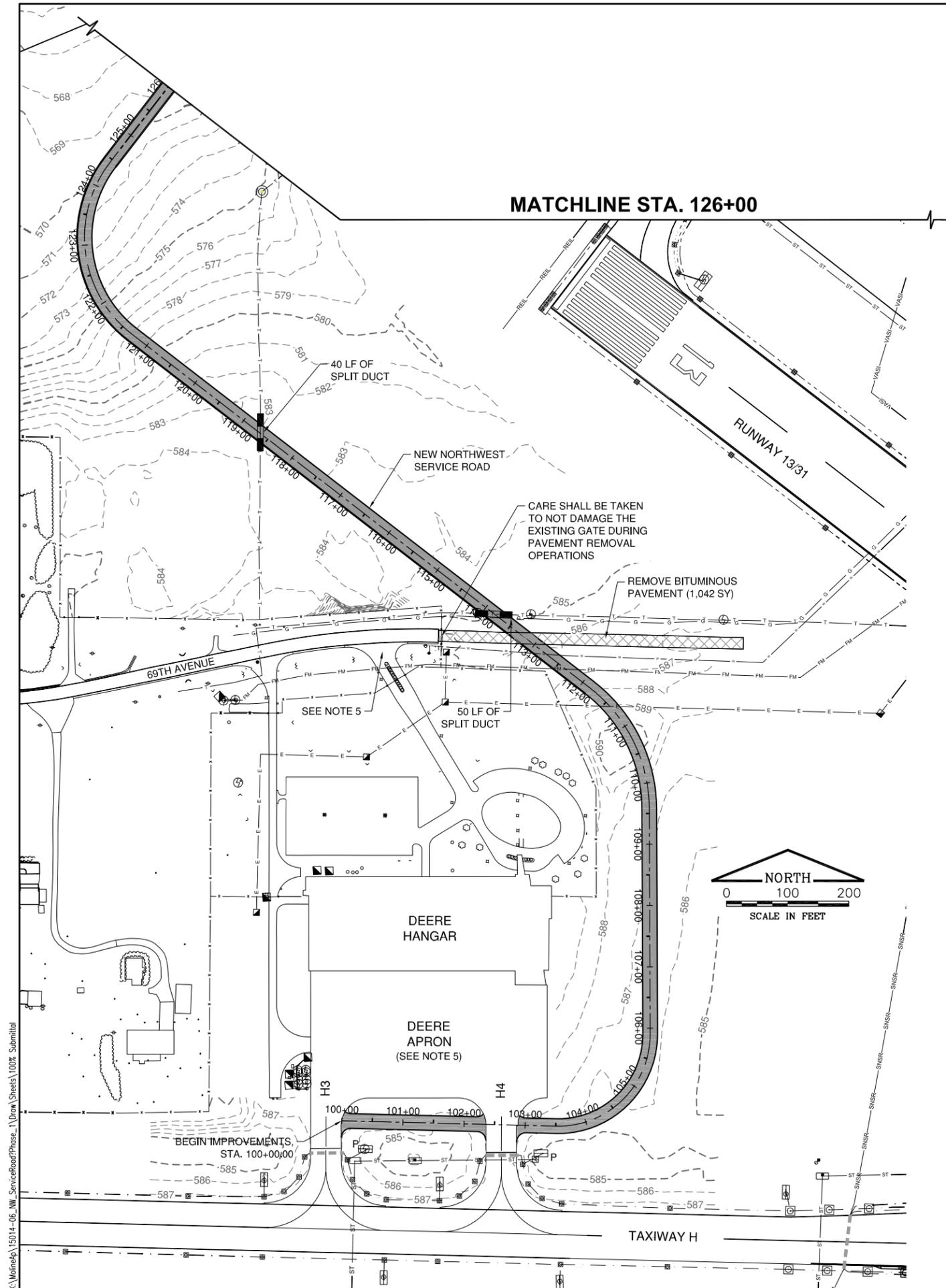
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED RUNWAY LIGHT
- EXISTING IN-PAVEMENT RUNWAY LIGHT
- EXISTING WINDCONE
- EXISTING TAXI GUIDANCE SIGN
- EXISTING SPLICE CAN / MARKER
- EXISTING ELECTRICAL HANDHOLE
- EXISTING RUNWAY GUARD LIGHT
- EXISTING UNDERDRAIN COLLECTION STRUCTURE
- EXISTING STORM DRAIN/INLET/MANHOLE
- EXISTING STORM SEWER
- EXISTING ELECTRICAL DUCT
- EXISTING RUNWAY 13/31 CIRCUIT
- EXISTING TAXIWAY F CIRCUIT
- EXISTING RGL CIRCUIT
- EXISTING VASI CIRCUIT
- EXISTING REIL CIRCUIT
- EXISTING PAVEMENT SENSOR CIRCUIT
- EXISTING ELECTRICAL CIRCUIT
- EXISTING TELEPHONE LINE
- EXISTING MASTERMAIN
- EXISTING SANITARY FORCEMAIN
- EXISTING FENCE
- EXISTING GROUND CONTOUR

NOTES

- ALL BITUMINOUS PAVEMENT REMOVAL TO BE PAID UNDER AR401900. REMOVE BITUMINOUS PAVEMENT. ALL REMOVED BITUMINOUS PAVEMENT TO BE DISPOSED OF OFF AIRPORT PROPERTY AT NO ADDITIONAL COST TO THE CONTRACT. PAVEMENT IS ESTIMATED TO BE 8" BITUMINOUS PAVEMENT ON 8" CRUSHED AGGREGATE BASE COURSE. CONTRACTOR TO SPREAD TOPSOIL WITHIN REMOVAL LIMITS, SEED AND MULCH.
- A FULL DEPTH SAWCUT SHALL BE REQUIRED AT THE LIMITS OF PAVEMENT REMOVAL. ANY PAVEMENT TO REMAIN THAT IS DAMAGED BY THE CONTRACTOR'S REMOVAL OPERATIONS SHALL BE REPAIRED BY HIM TO THE SATISFACTION OF THE ENGINEER.
- EXISTING EDGE LIGHTS AND TAXI GUIDANCE SIGNS TO BE PROTECTED FROM DAMAGE. CONTRACTOR SHALL REMOVE FIXTURES AND PLACE A COVER OVER THE REMAINING BASE CAN TO PREVENT CONSTRUCTION DEBRIS FROM ENTERING THE CAN. FIXTURES SHALL BE REINSTALLED AT THE COMPLETION OF WORK, COSTS INCLUDED IN THE CONTRACT.
- EXISTING UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY BY HIM AT NO ADDITIONAL COST TO THE CONTRACT. THE CONTRACTOR SHALL HAVE ALL EXISTING UTILITIES MARKED PRIOR TO THE BEGINNING OF WORK. THE MARKS SHALL THEN BE SURVEYED SO THAT THEY MAY BE RESTORED AT ANY TIME WITHOUT DELAY.
- CONTRACTOR SHALL NOT HAVE ACCESS TO DEERE PROPERTY AT ANY TIME WITHOUT THE EXPRESS CONSENT OF AN AUTHORIZED DEERE REPRESENTATIVE.
- SOILS REPORT INDICATES POOR SOIL CONDITIONS AT THE LOCATION SHOWN ON THE PLANS. SUBGRADE MATERIAL SHALL BE EXCAVATED TO 1' BELOW SUBBASE (AR152410). 1' OF POROUS GRANULAR EMBANKMENT (AR208515) SHALL THEN BE PLACED TO THE BOTTOM OF THE SUBBASE. ADDITIONAL UNDERCUT AT VARIOUS LOCATIONS ON THE ROADWAY MAY BE REQUIRED AND SHALL BE COMPLETED AS DIRECTED BY THE RESIDENT ENGINEER.



THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION, AND THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



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quad-plan
 Existing Topo
 Proposed Geometry
 NW Service Road Base

QUO22

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

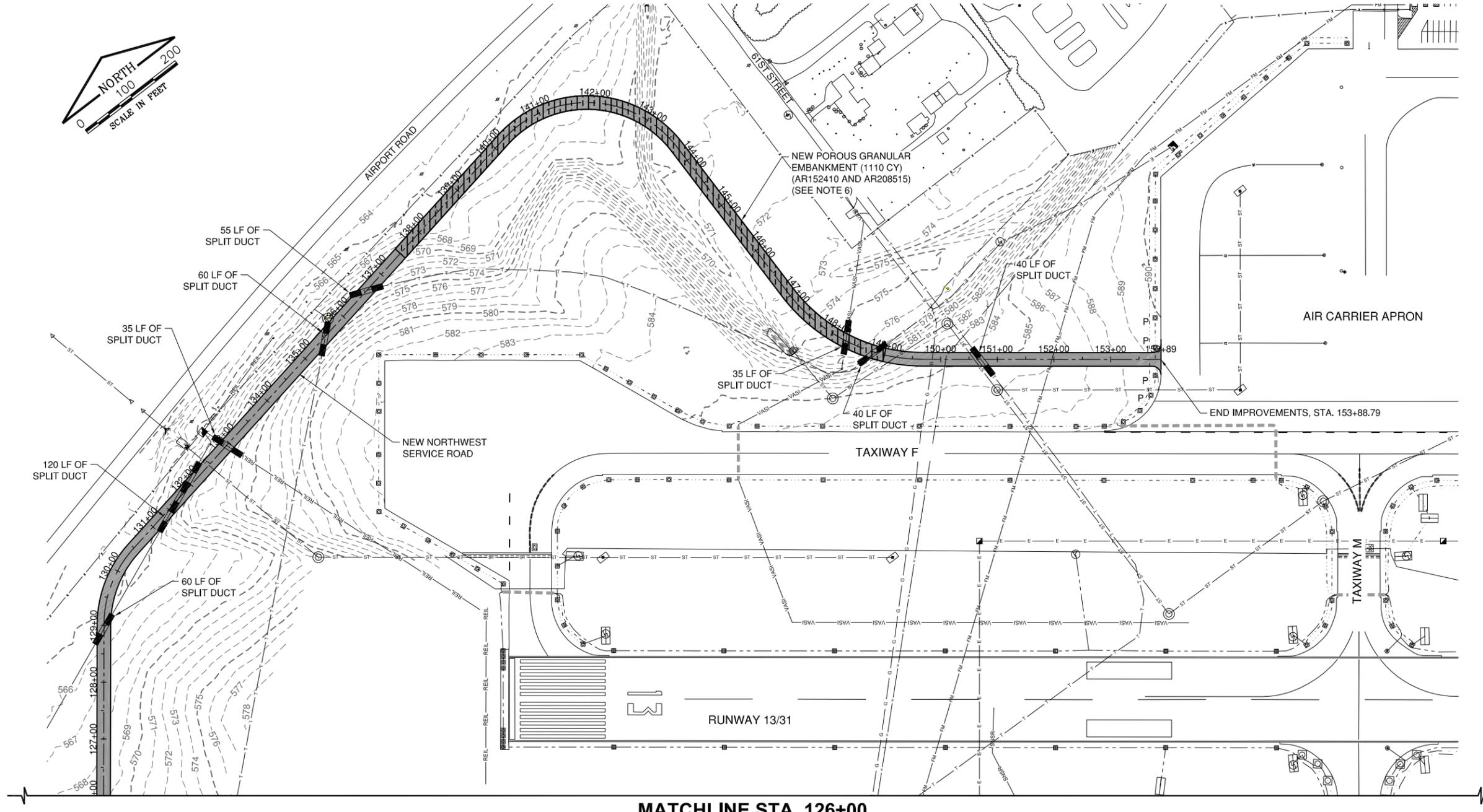
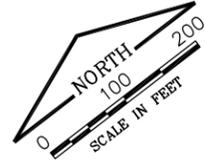
**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 EXISTING CONDITIONS AND REMOVALS 2**



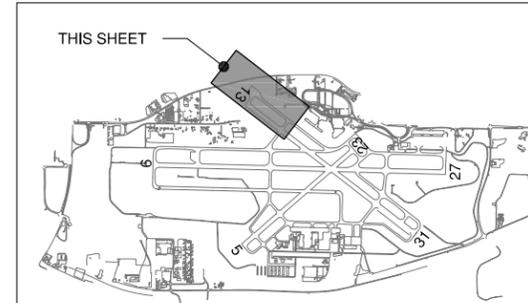
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APPROVED BY:	CMT - MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06

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MATCHLINE STA. 126+00

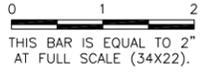
SEE EXISTING CONDITIONS
 AND REMOVALS SHEET 1
 FOR LEGEND AND NOTES



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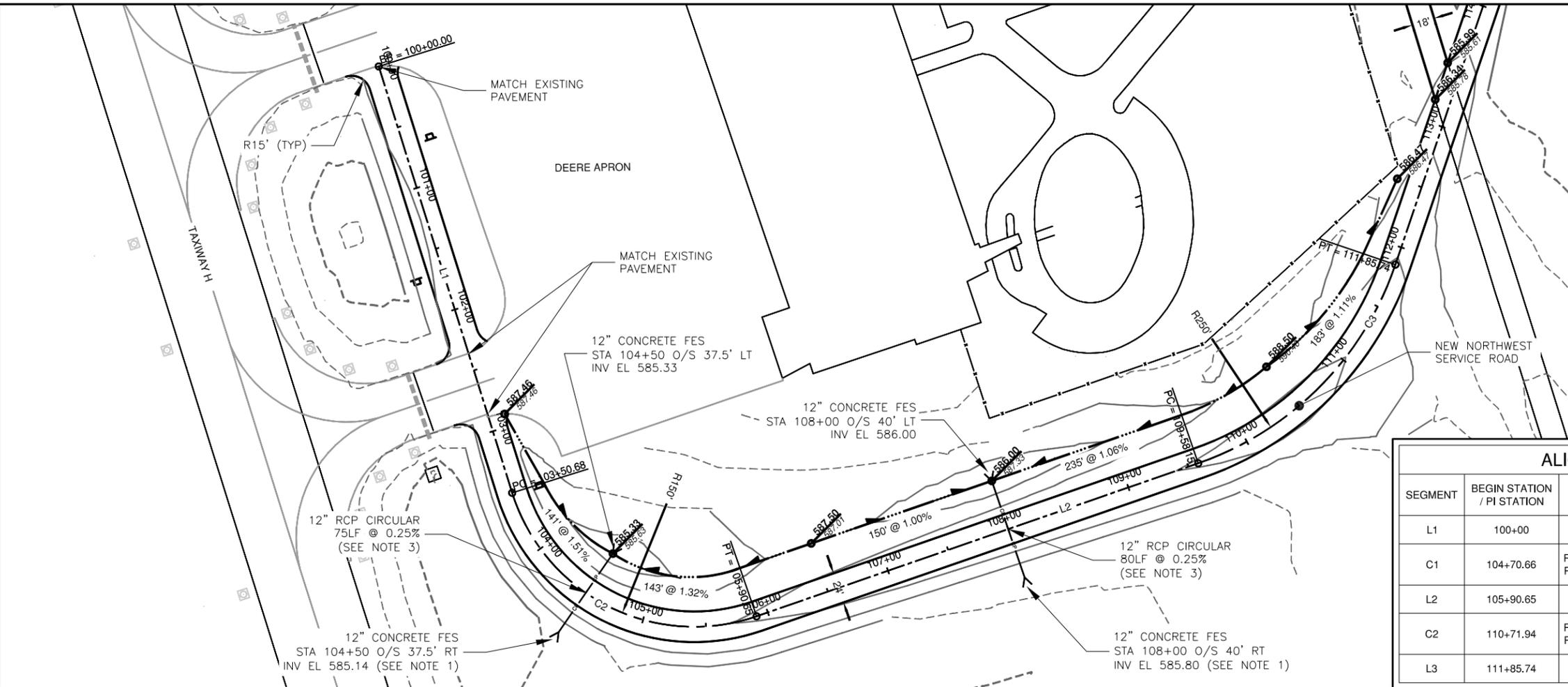


LEGEND

- NEW CONTOURS
- - - EXISTING CONTOURS
- - - DITCH FLOWLINE
- NEW RCP CULVERT

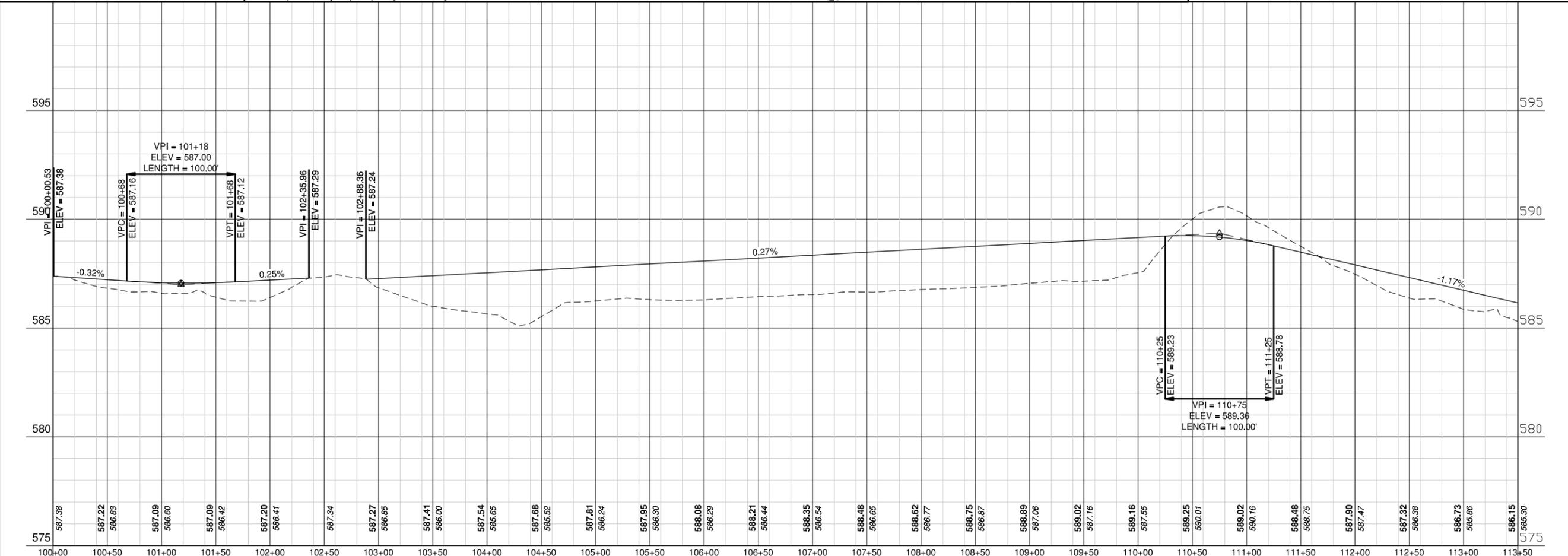
NEW GRADE
 — EXISTING GRADE

- NOTES**
1. THE CONTRACTOR SHALL GRADE AREA AROUND OUTFLOW INVERT TO DRAIN.
 2. SEE CROSS SECTIONS FOR DITCH FLOWLINE OFFSET.
 3. CONCRETE PIPE LENGTHS SHOWN INCLUDED FLARED END SECTION.



ALIGNMENT DATA

SEGMENT	BEGIN STATION / PI STATION	NORTHING / EASTING	END STATION / CURVE RADIUS	NORTHING / EASTING
L1	100+00	N:1742735.8389 E:2200595.5094	103+50.68	N:1742726.7873 E:2200946.0754
C1	104+70.66	PI N:1742722.9337 PI E:2201100.4423	R150	RADIUS POINT N:1742876.7406 E:2200949.8189
L2	105+90.65	N:1742877.3474 E:2201099.8177	109+58.15	N:1743244.8435 E:2201098.3310
C2	110+71.94	PI N:1743367.2106 PI E:2201097.8360	R250	RADIUS POINT N:1743243.8328 E:2200848.3330
L3	111+85.74	N:1743441.8852 E:2201000.8952	121+29.5	N:1744018.0911 E:2200252.8789



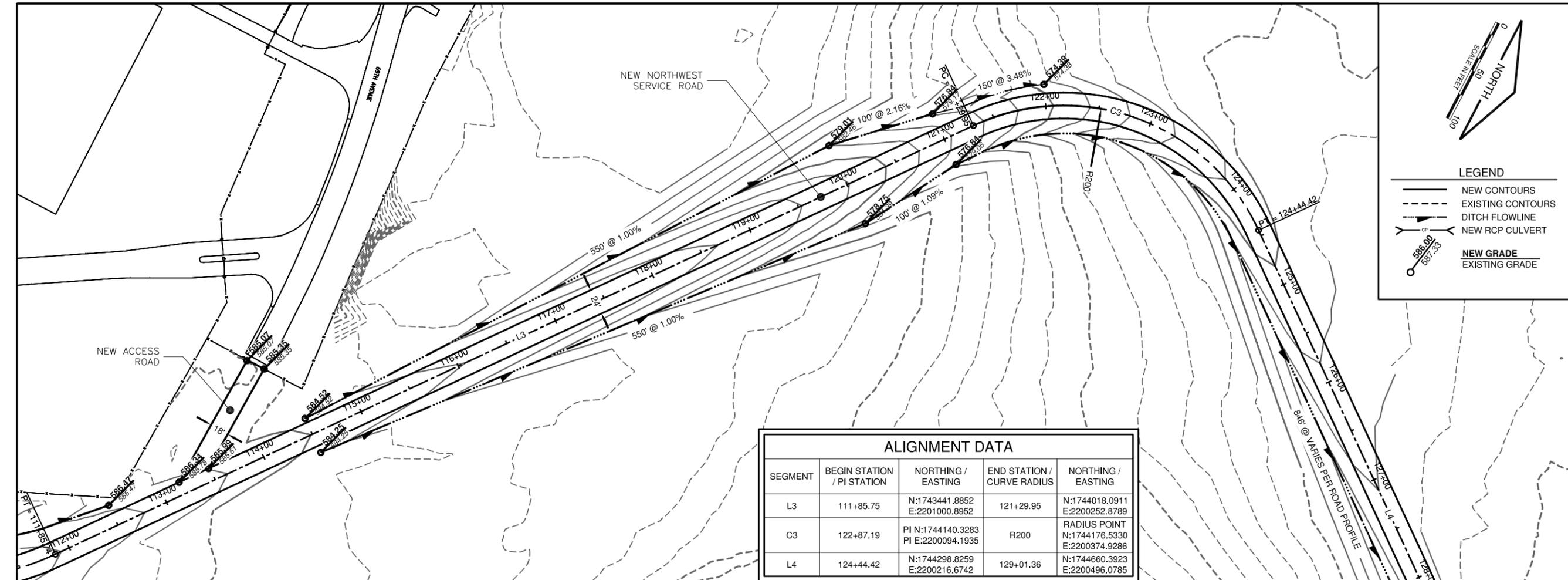
**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 PLAN AND PROFILE 1**



DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO.	3-17-0068-XX

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FILE: 14-17 PLAN AND PROFILE.dwg
 UPDATE BY: Travis Strait
 PLOT DATE: 6/13/2016 12:12 PM

Design
 Existing Topo
 NW Service Road Base
 Proposed Geometry

QUO22

REVISIONS		
NUMBER	BY	DATE

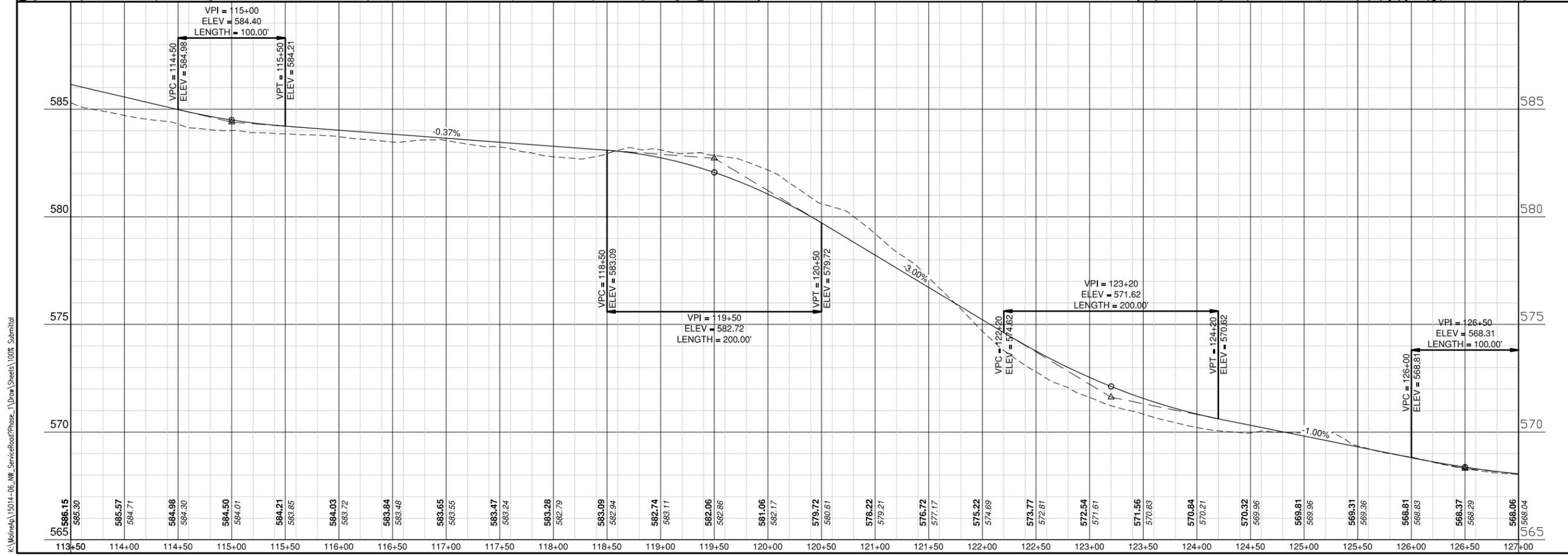
LEGEND

- NEW CONTOURS
- EXISTING CONTOURS
- DITCH FLOWLINE
- NEW RCP CULVERT

NEW GRADE
EXISTING GRADE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).

ALIGNMENT DATA				
SEGMENT	BEGIN STATION / PI STATION	NORTHING / EASTING	END STATION / CURVE RADIUS	NORTHING / EASTING
L3	111+85.75	N:1743441.8852 E:2201000.8952	121+29.95	N:1744018.0911 E:2200252.8789
C3	122+87.19	PI N:1744140.3283 PI E:2200094.1935	R200	RADIUS POINT N:1744176.5330 E:2200374.9286
L4	124+44.42	N:1744298.8259 E:2200216.6742	129+01.36	N:1744660.3923 E:2200496.0785



**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 PLAN AND PROFILE 2**

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**QUAD CITY
 INTERNATIONAL AIRPORT**

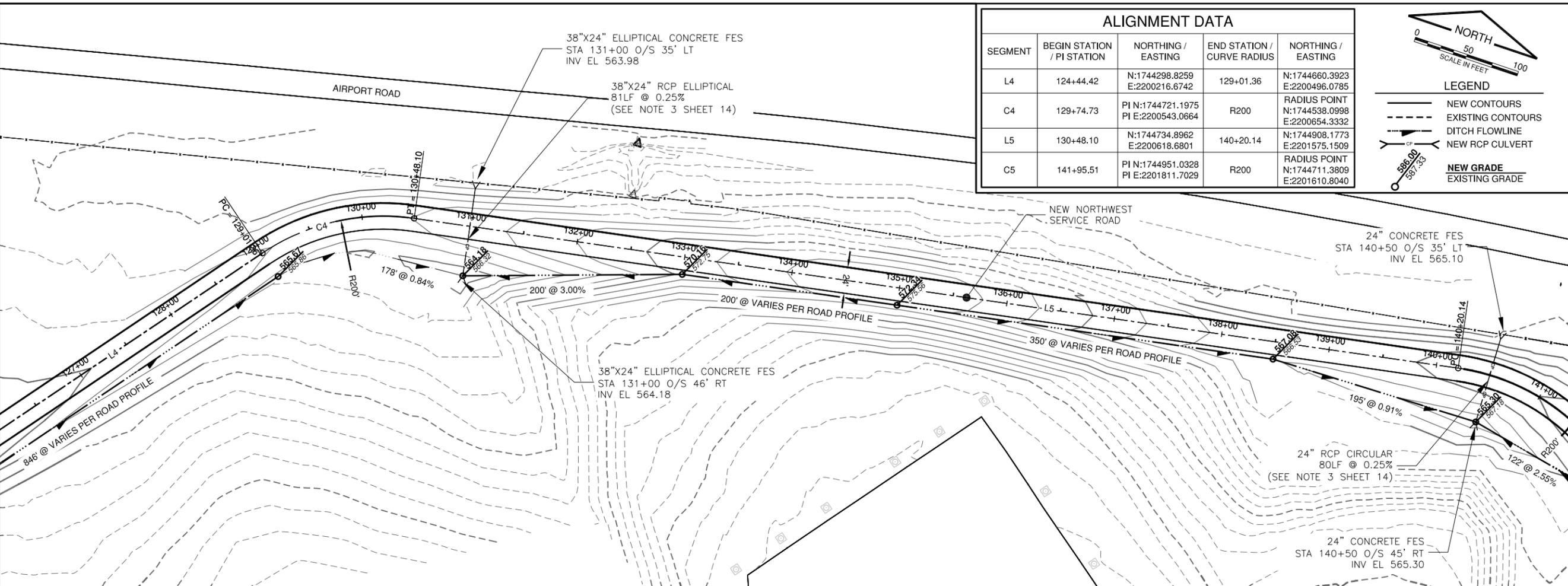
DESIGN BY: CMT-MLI
 DRAWN BY: CMT-MLI
 CHECKED BY: CMT-MLI
 APPROVED BY: CMT-MLI
 DATE: JUNE 3, 2016
 JOB No: 15014-06

IL PROJ. NO. MLI-4531
 AIP PROJ. NO. 3-17-0068-XX

SHEET 15 OF 33 SHEETS

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K:\Moline\15014-06_NW_ServiceRoad\Phase_1\Draw\Sheets\100%_Submittal



ALIGNMENT DATA				
SEGMENT	BEGIN STATION / PI STATION	NORTHING / EASTING	END STATION / CURVE RADIUS	NORTHING / EASTING
L4	124+44.42	N:1744298.8259 E:2200216.6742	129+01.36	N:1744660.3923 E:2200496.0785
C4	129+74.73	PI N:1744721.1975 PI E:2200543.0664	R200	RADIUS POINT N:1744538.0998 E:2200654.3332
L5	130+48.10	N:1744734.8962 E:2200618.6801	140+20.14	N:1744908.1773 E:2201575.1509
C5	141+95.51	PI N:1744951.0328 PI E:2201811.7029	R200	RADIUS POINT N:1744711.3809 E:2201610.8040

NORTH
0 50 100
SCALE IN FEET

LEGEND

- NEW CONTOURS
- EXISTING CONTOURS
- DITCH FLOWLINE
- NEW RCP CULVERT
- NEW GRADE
- EXISTING GRADE

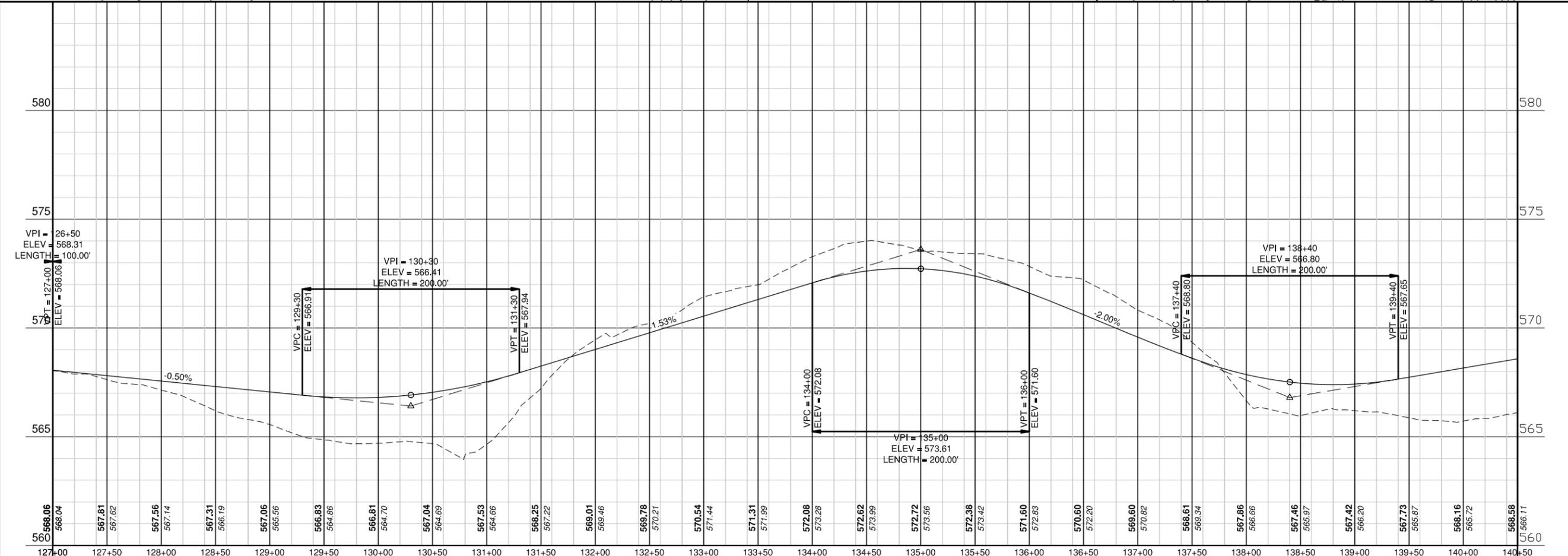
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UPDATE BY: Travis Strait
PLOT DATE: 6/14/2016 3:51 PM

Design
Existing Topo
NW Service Road Base
Proposed Geometry

QUO22

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
PLAN AND PROFILE 3**

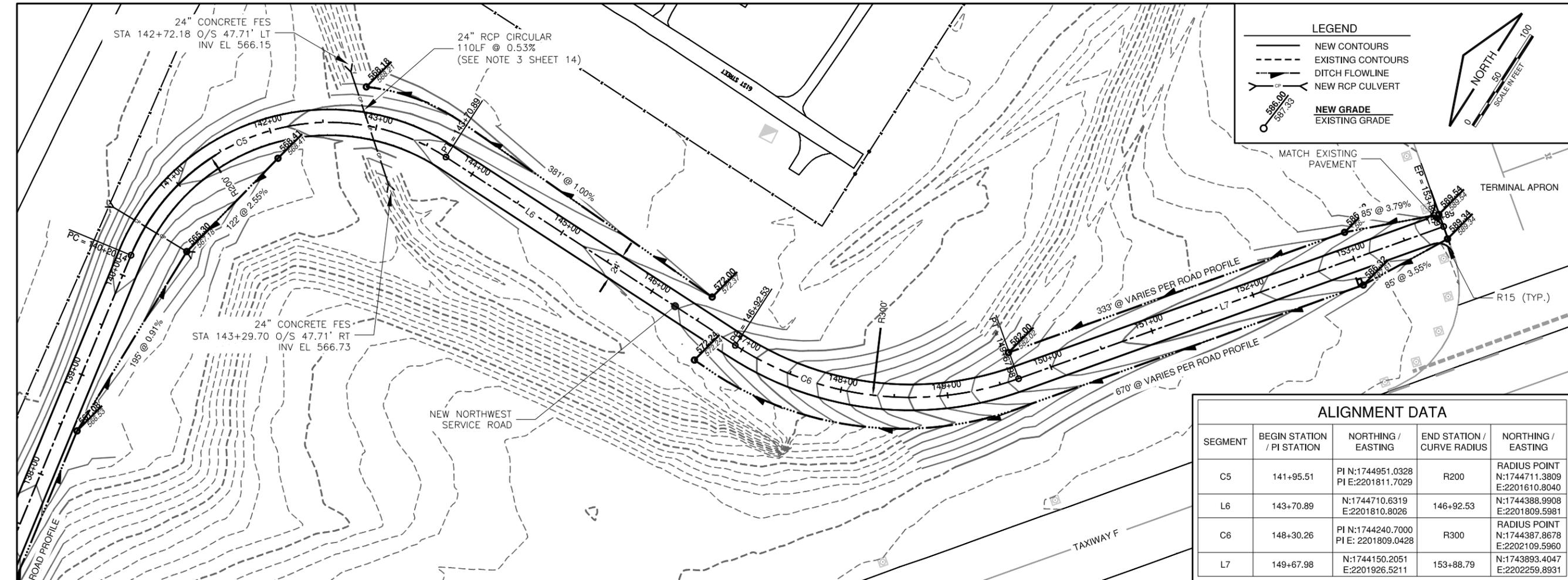
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QUAD CITY INTERNATIONAL AIRPORT

DESIGN BY: CMT-MLI
DRAWN BY: CMT-MLI
CHECKED BY: CMT-MLI
APPROVED BY: CMT-MLI
DATE: JUNE 3, 2016
JOB No: 15014-06

IL PROJ. NO. MLI-4531
AIP PROJ. NO. 3-17-0068-XX

SHEET 16 OF 33 SHEETS



FILE: 14-17 PLAN AND PROFILE.dwg
 UPDATE BY: Travis Strait
 PLOT DATE: 6/13/2016 12:14 PM

Design
 Existing Topo
 NW Service Road Base
 Proposed Geometry

QUO22

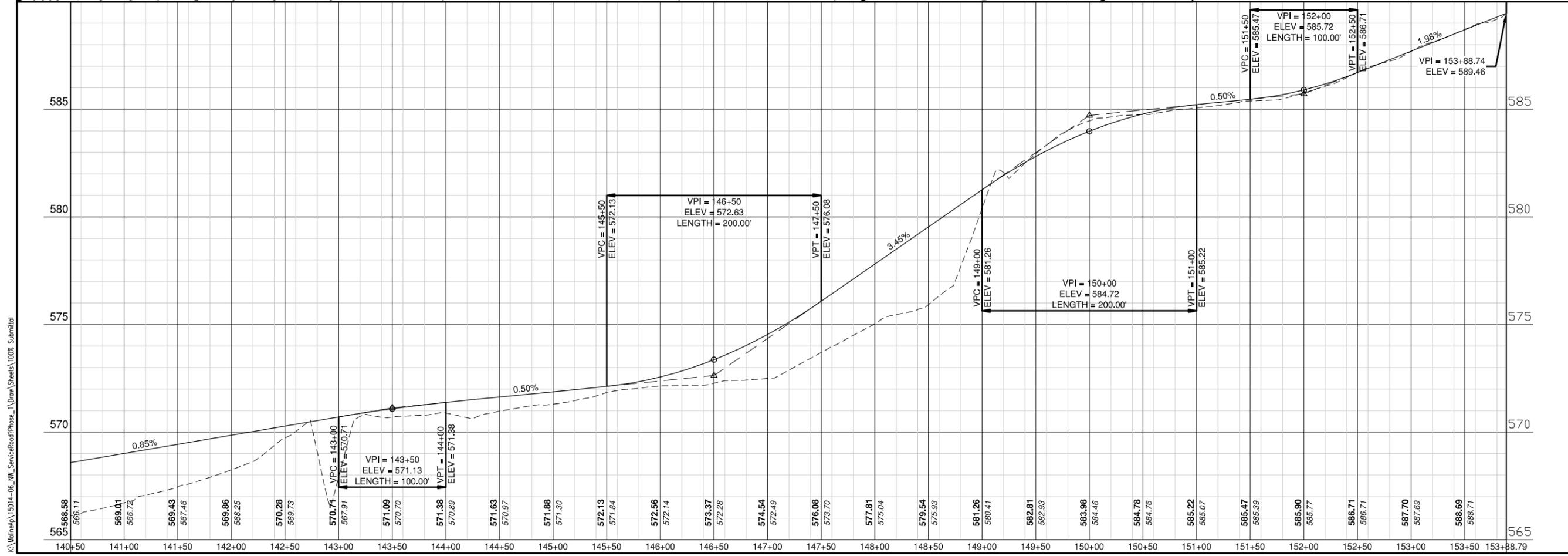
REVISIONS

NUMBER	BY	DATE

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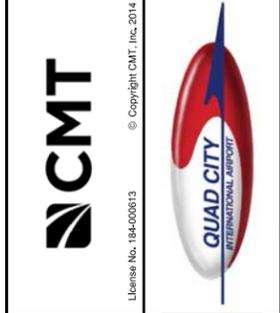
ALIGNMENT DATA

SEGMENT	BEGIN STATION / PI STATION	NORTHING / EASTING	END STATION / CURVE RADIUS	NORTHING / EASTING
C5	141+95.51	PI N:1744951.0328 PI E:2201811.7029	R200	RADIUS POINT N:1744711.3809 E:2201610.8040
L6	143+70.89	N:1744710.6319 E:2201810.8026	146+92.53	N:1744388.9908 E:2201809.5981
C6	148+30.26	PI N:1744240.7000 PI E: 2201809.0428	R300	RADIUS POINT N:1744387.8678 E:2202109.5960
L7	149+67.98	N:1744150.2051 E:2201926.5211	153+88.79	N:1743893.4047 E:2202259.8931



**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 PLAN AND PROFILE 4**

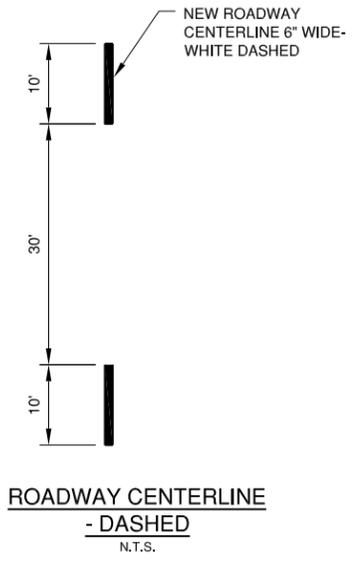


DESIGN BY: CMT-MLI
 DRAWN BY: CMT-MLI
 CHECKED BY: CMT-MLI
 APPROVED BY: CMT-MLI
 DATE: JUNE 3, 2016
 JOB No: 15014-06

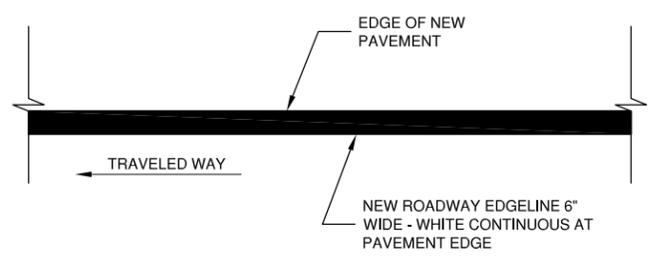
IL PROJ. NO. MLI-4531
 AIP PROJ. NO. 3-17-0068-XX

SHEET 17 OF 33 SHEETS

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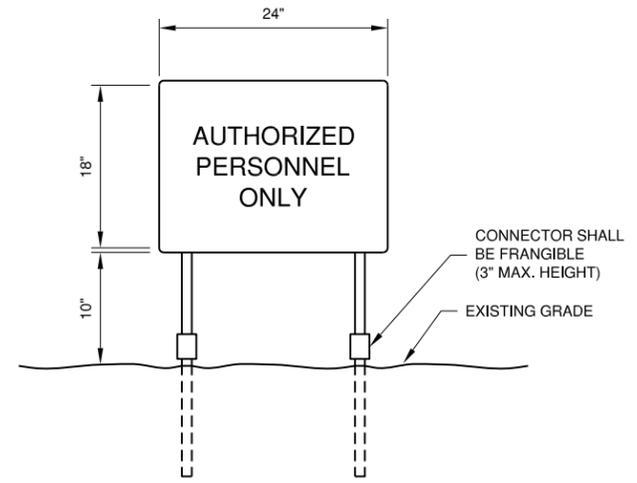
**ROADWAY CENTERLINE
 - DASHED**
 N.T.S.



ROADWAY EDGELINES DETAIL
 N.T.S.

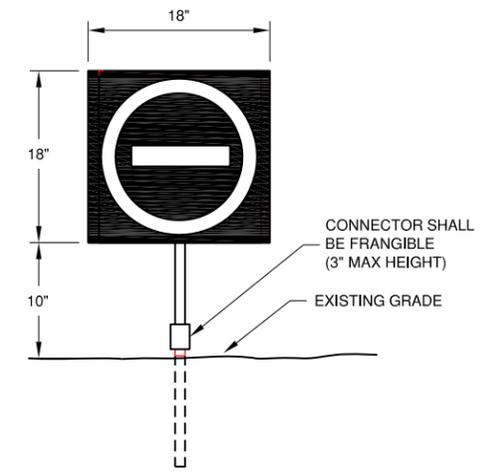
PAVEMENT MARKING NOTE:
 1. NO BLACK BORDER AND NO BLACK PAINT WILL BE USED FOR ANY OF THE SURFACE PAINTED PAVEMENT MARKINGS.

SIGNAGE SCHEDULE				
LOCATION	STATION	OFFSET	VIEWING DIRECTION	SIGN DETAIL
1	100+68	22' LT	EAST	3
2	101+68	17' RT	WEST	3
3	103+54	22' LT	EAST	3
3	103+54	22' LT	WEST	1
3	103+54	32' LT	WEST	2
4	152+88	22' RT	NORTH	3
4	152+88	22' RT	SOUTH	1
4	152+88	32' RT	SOUTH	2



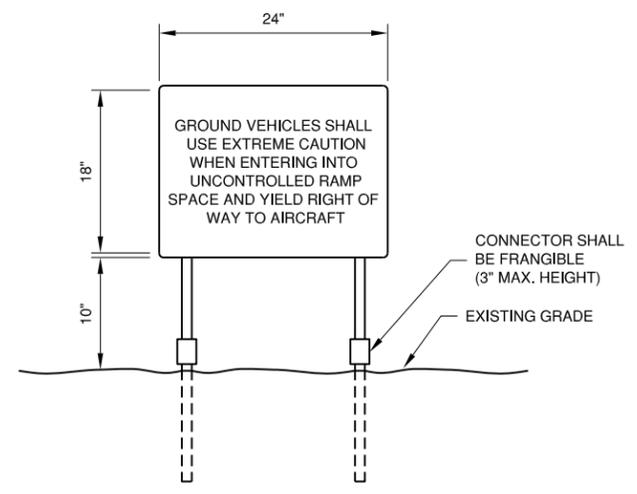
2 INFORMATION SIGN LEGEND 2
 N.T.S.

INFORMATION SIGN NOTES
 1. FINAL SIGN FACE LEGENDS SHALL BE DETERMINED BY THE AIRPORT.
 2. SIGN LEGENDS SHALL BE BLACK LETTERING ON WHITE BACKGROUND.



1 NO ENTRY SIGN DETAIL 1
 N.T.S.

NOTES:
 SIGN LEGEND SHALL BE WHITE LETTERING ON RED BACKGROUND

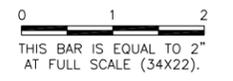


3 INFORMATION SIGN LEGEND 3
 N.T.S.

INFORMATION SIGN NOTES
 1. FINAL SIGN FACE LEGENDS SHALL BE DETERMINED BY THE AIRPORT.
 2. SIGN LEGENDS SHALL BE BLACK LETTERING ON WHITE BACKGROUND.

QUO22

REVISIONS		
NUMBER	BY	DATE

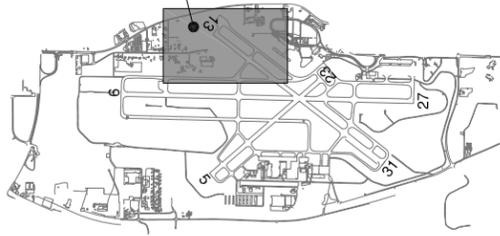


**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 SIGNAGE AND MARKING DETAILS**



DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO.	3-17-0068-XX
SHEET	18 OF 33 SHEETS

PROJECT LOCATION



EARTHWORK SUMMARY TABLE

DESCRIPTION	CUT		FILL		
	TOPSOIL STRIPPING (INITIAL POSITION)	UNCLASSIFIED EXCAVATION (INITIAL POSITION)	EMBANKMENT FILL (FINAL POSITION)	SHOULDER FILL (FINAL POSITION)	TOPSOIL PLACEMENT (FINAL POSITION)
FILL ADJUSTED FOR SHRINKAGE (10%)	10,340 CY	5,150 CY	3,418 CY	1,988 CY	2,180 CY
UNSUITABLE MATERIAL		500 CY			
TOTAL UNCLASSIFIED EXCAVATION (AR152410)		15,990 CY	3,760 CY	2,187 CY	2,398 CY
TOTAL FILL				8,345 CY	
EXCESS MATERIAL			7,645 CY		

NOTES

- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
- PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AND IS TO BE PAID FOR AS PAY ITEM AR152410 IN ITS INITIAL POSITION.
- PLACEMENT OF TOPSOIL, SHOULDER FILL AND EMBANKMENT FILL GENERATED FROM UNCLASSIFIED EXCAVATION SHALL BE INCIDENTAL TO AR152410 UNCLASSIFIED EXCAVATION.
- TOPSOIL SHALL BE USED FOR SHOULDER FILL.
- AT THE END OF THE PROJECT, ALL SURPLUS TOPSOIL AND SUBGRADE MATERIALS SHALL BE STOCKPILED ON THE AIRPORT PROPERTY AT THE LOCATIONS NOTED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT. THE STOCKPILES SHALL HAVE A MINIMUM 4" THICK LAYER OF TOPSOIL PLACED OVER ITS ENTIRETY AT NO ADDITIONAL COST TO THE CONTRACT.
- THE CONTRACTOR SHALL ENSURE THAT A 4" THICK LAYER OF TOPSOIL CAN BE PLACED OVER THE ENTIRETY OF THE DISTURBANCE LIMITS. IT MAY BE NECESSARY TO OVER-EXCAVATE IN AREAS IN ORDER TO PROVIDE THE REQUIRED THICKNESS OF TOPSOILING. OVER-EXCAVATION TO PROVIDE THE MINIMUM TOPSOIL REQUIREMENTS HAS BEEN INCLUDED IN THE EARTHWORK QUANTITIES SHOWN IN THE EARTHWORK SUMMARY TABLE, AND SHALL BE PAID UNDER ITEM AR152410, UNCLASSIFIED EXCAVATION.R
- A SHRINKAGE FACTOR OF 10% HAS BEEN ASSUMED FOR THIS PROJECT. NO ADDITIONAL COMPENSATION WILL BE MADE FOR THE ACTUAL SHRINKAGE FACTOR OF THE MATERIALS ENCOUNTERED.
- EXCAVATION ALONG THE EDGES OF THE NEW PAVEMENT TO FACILITATE SUBBASE CONSTRUCTION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE NEW PAVEMENT CONSTRUCTION.
- ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- ANY CONTRACTOR HAUL ROADS TO THE SITE SHALL BE RESTORED WITH 4" MINIMUM OF TOPSOIL PLACED. ALL HAUL ROAD RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- ANY TEMPORARY STOCKPILING AND RE-HANDLING OF EARTH MATERIALS SHALL BE INCIDENTAL TO THE CONTRACT.
- EXCESS TOPSOIL SHALL BE SPREAD AT A 4-INCH THICK LAYER ONSITE AT THE LOCATIONS AS DIRECTED BY THE RESIDENT ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT.

FILE: Design_recover002.dwg
 UPDATE BY: Travis Strait
 PLOT DATE: 6/13/2016 12:04 PM

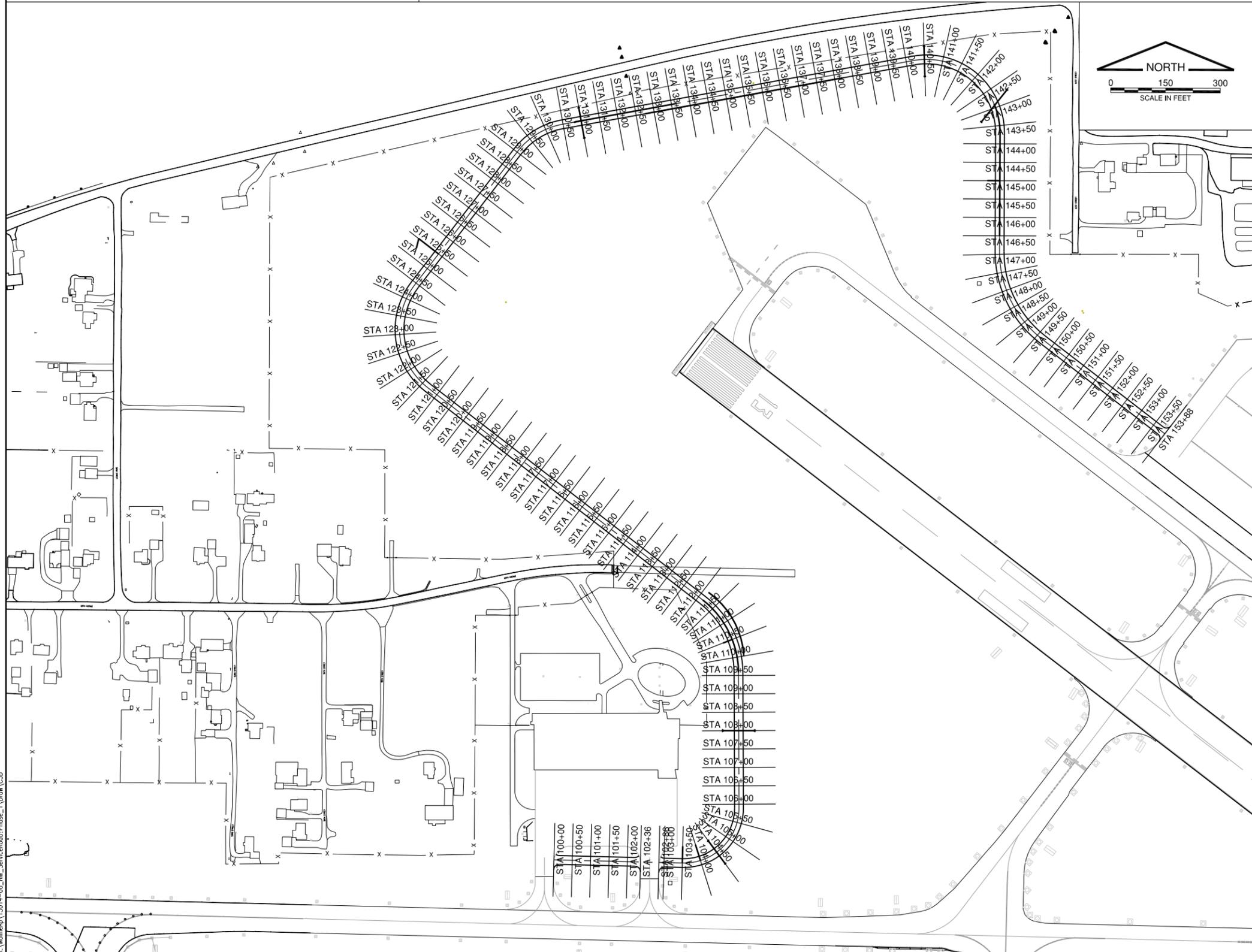
Existing Topo
 NW Service Road Base

QU022

REVISIONS

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 INDEX TO CROSS SECTIONS**



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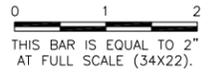


DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO	3-17-0068-XX
SHEET	19 OF 33 SHEETS

Existing Topo
 NW Service Road Base

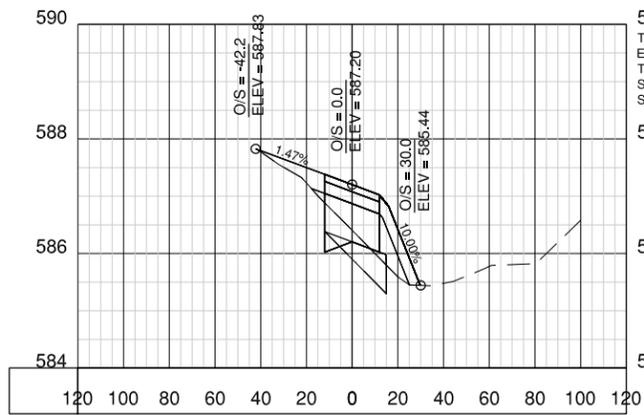
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REVISIONS		
NUMBER	BY	DATE



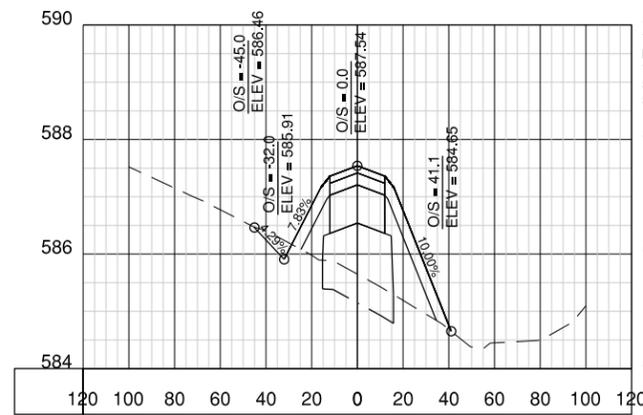
**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 CROSS SECTIONS 1**



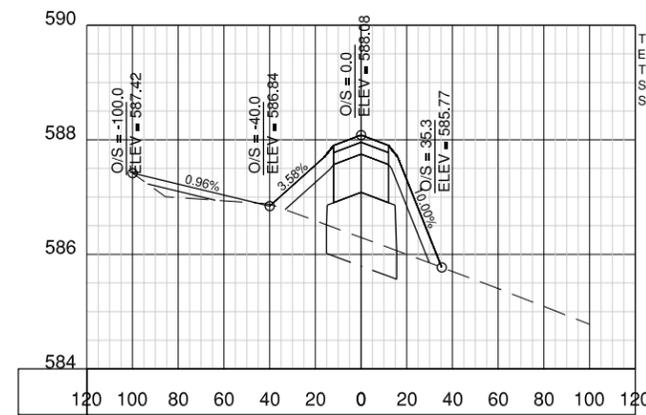
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 EMBANKMENT CUT = 0 SF
 TOPSOIL PLACEMENT = 13.5 SF
 STRUCTURAL FILL = 10 SF
 SHOULDER FILL = 6 SF

102
+
00



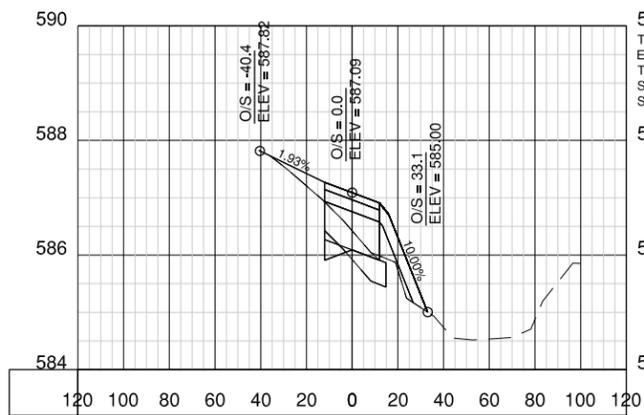
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 TOPSOIL PLACEMENT = 17 SF
 STRUCTURAL FILL = 29 SF
 SHOULDER FILL = 22 SF

104
+
00



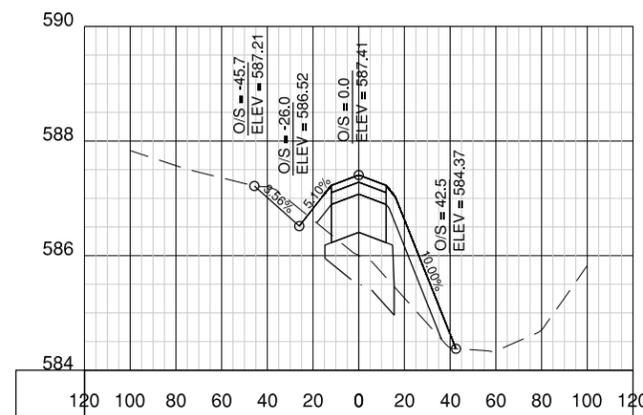
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 EMBANKMENT CUT = 0 SF
 TOPSOIL PLACEMENT = 22 SF
 STRUCTURAL FILL = 36 SF
 SHOULDER FILL = 24 SF

106
+
00



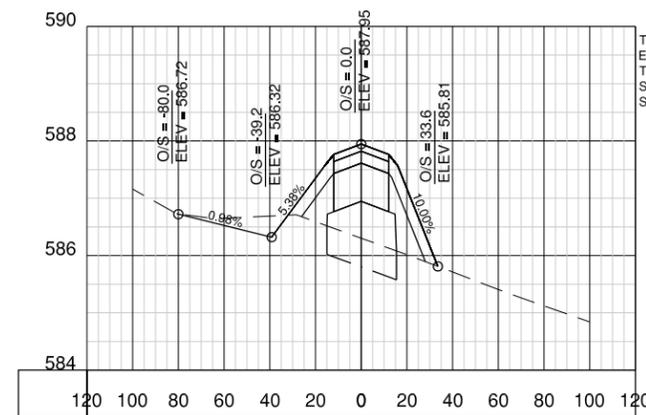
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 EMBANKMENT CUT = 1 SF
 TOPSOIL PLACEMENT = 12 SF
 STRUCTURAL FILL = 6 SF
 SHOULDER FILL = 4 SF

101
+
50



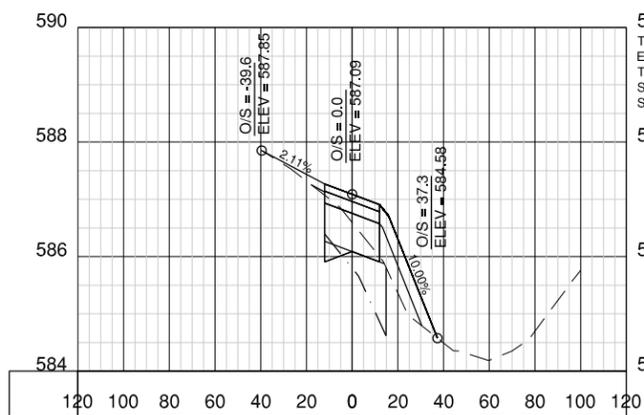
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103
+
50



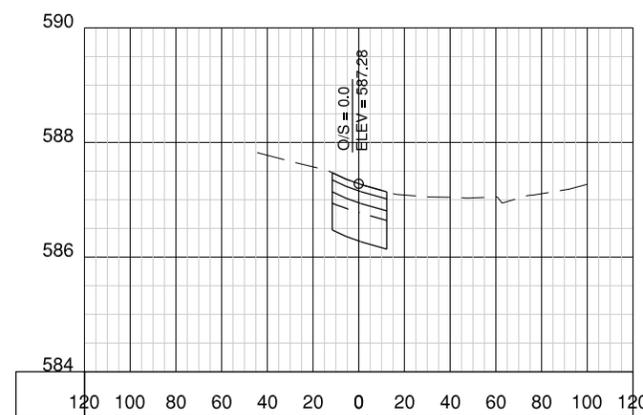
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 TOPSOIL PLACEMENT = 14 SF
 STRUCTURAL FILL = 31 SF
 SHOULDER FILL = 15 SF

105
+
50



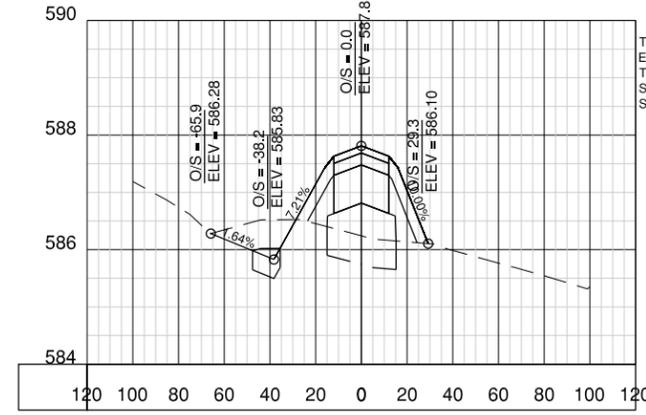
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 EMBANKMENT CUT = 1 SF
 TOPSOIL PLACEMENT = 12 SF
 STRUCTURAL FILL = 12 SF
 SHOULDER FILL = 8 SF

101
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00



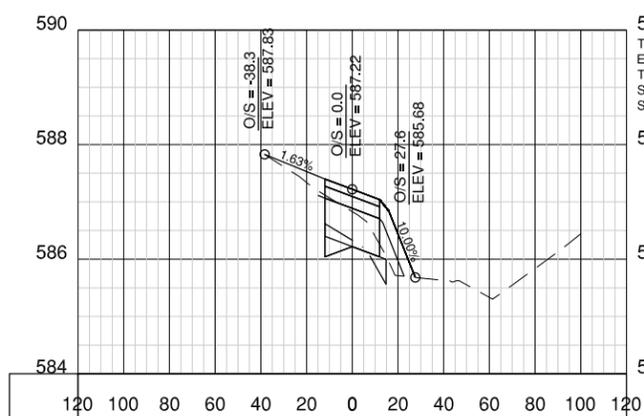
TOPSOIL STRIPPING = 12 SF
 EMBANKMENT CUT = 12 SF
 TOPSOIL PLACEMENT = 0 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 0 SF

102
+
86



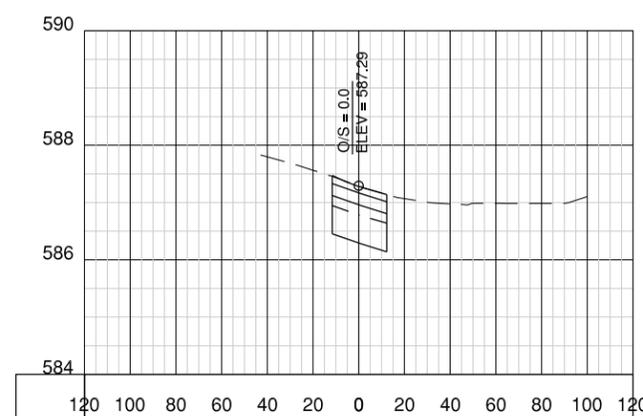
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 TOPSOIL PLACEMENT = 12 SF
 STRUCTURAL FILL = 29 SF
 SHOULDER FILL = 15 SF

105
+
00



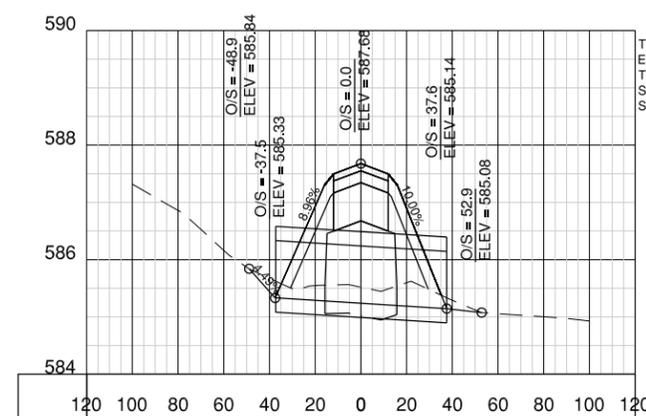
TOPSOIL STRIPPING = 13 SF
 EMBANKMENT CUT = 4 SF
 TOPSOIL PLACEMENT = 9 SF
 STRUCTURAL FILL = 2 SF
 SHOULDER FILL = 4 SF

100
+
50



TOPSOIL STRIPPING = 12 SF
 EMBANKMENT CUT = 12 SF
 TOPSOIL PLACEMENT = 0 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 0 SF

102
+
35



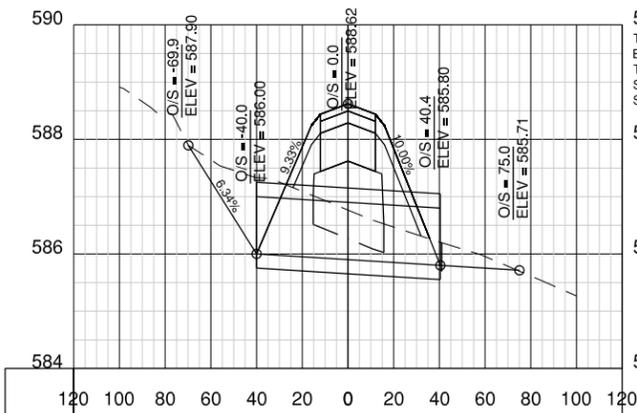
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 TOPSOIL PLACEMENT = 18 SF
 STRUCTURAL FILL = 49 SF
 SHOULDER FILL = 24 SF

104
+
50

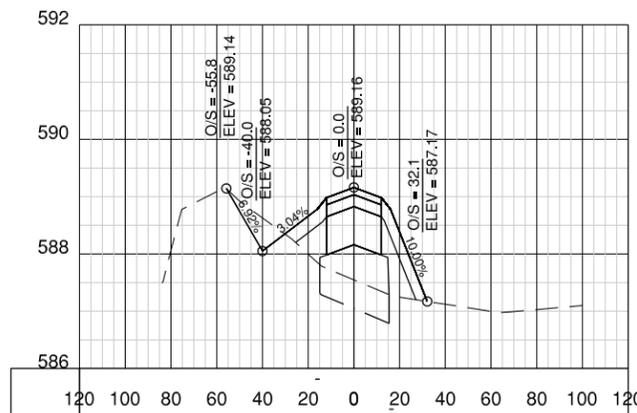
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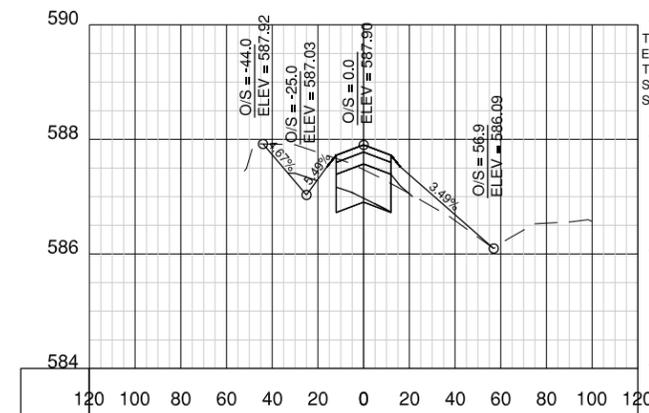
DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL. PROJ. NO. MLI-4531 AIP PROJ. NO 3-17-0068-XX	
SHEET	20 OF 33 SHEETS



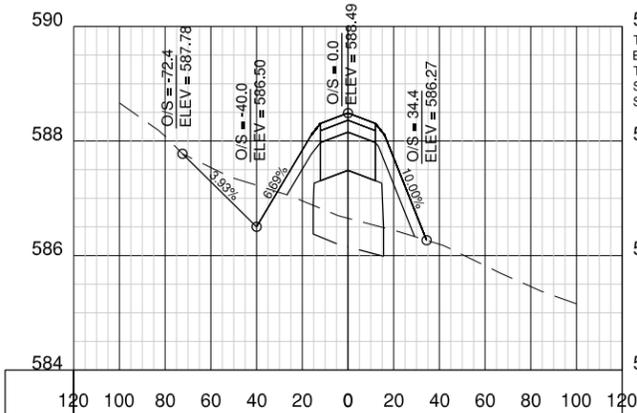
590
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EMBANKMENT CUT = 19 SF
TOPSOIL PLACEMENT = 20 SF
STRUCTURAL FILL = 38 SF
SHOULDER FILL = 20 SF



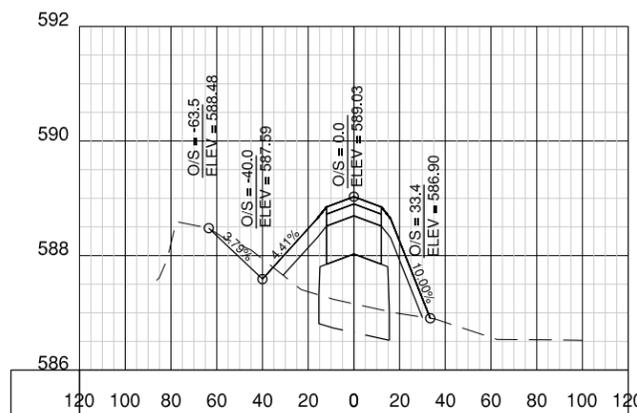
592
TOPSOIL STRIPPING = 15 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 13 SF
STRUCTURAL FILL = 30 SF
SHOULDER FILL = 14 SF



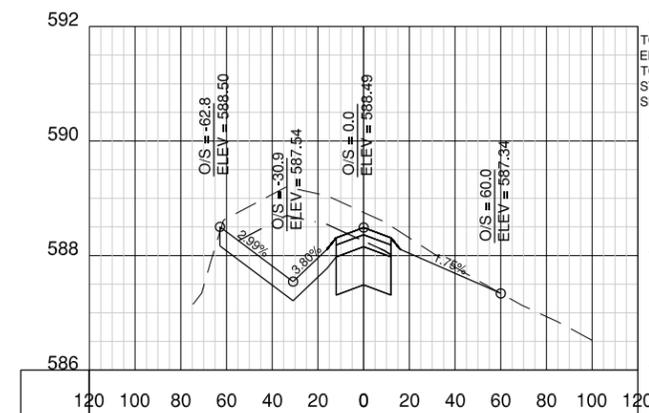
590
TOPSOIL STRIPPING = 24 SF
EMBANKMENT CUT = 10 SF
TOPSOIL PLACEMENT = 13 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 1 SF



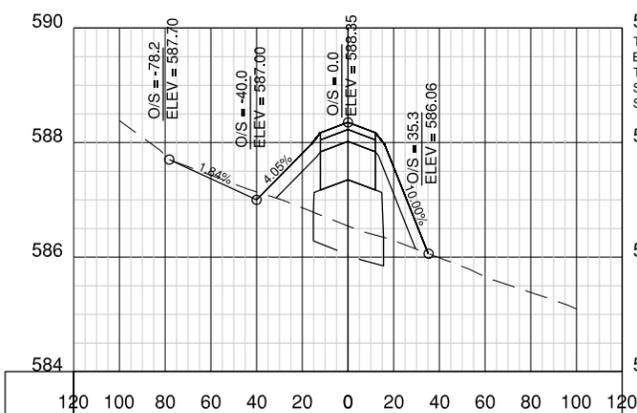
590
TOPSOIL STRIPPING = 28 SF
EMBANKMENT CUT = 5 SF
TOPSOIL PLACEMENT = 18 SF
STRUCTURAL FILL = 37 SF
SHOULDER FILL = 18 SF



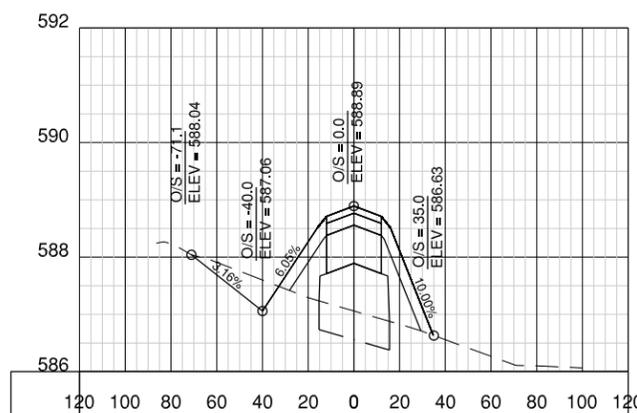
592
TOPSOIL STRIPPING = 15 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 14 SF
STRUCTURAL FILL = 38 SF
SHOULDER FILL = 23 SF



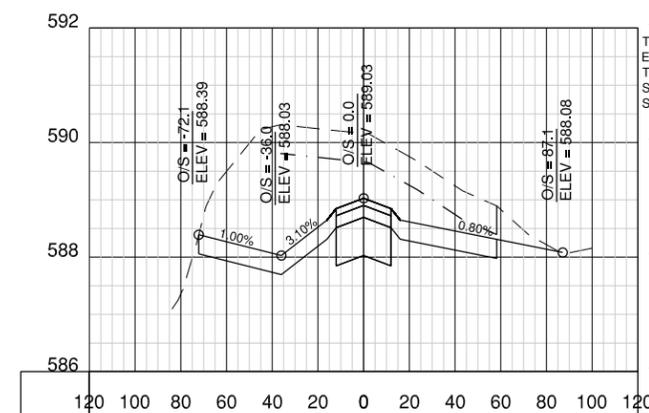
592
TOPSOIL STRIPPING = 47 SF
EMBANKMENT CUT = 42 SF
TOPSOIL PLACEMENT = 14 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



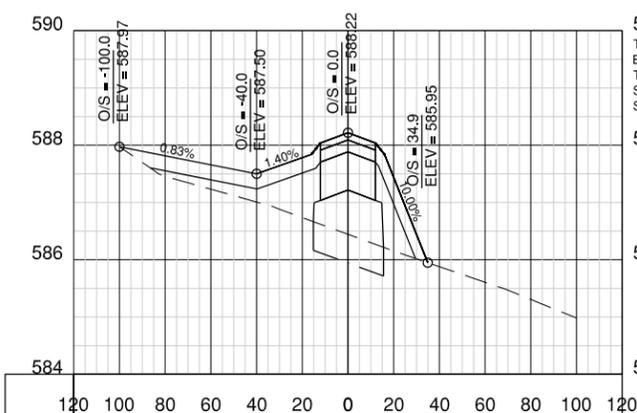
590
TOPSOIL STRIPPING = 18 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 16 SF
STRUCTURAL FILL = 40 SF
SHOULDER FILL = 20 SF



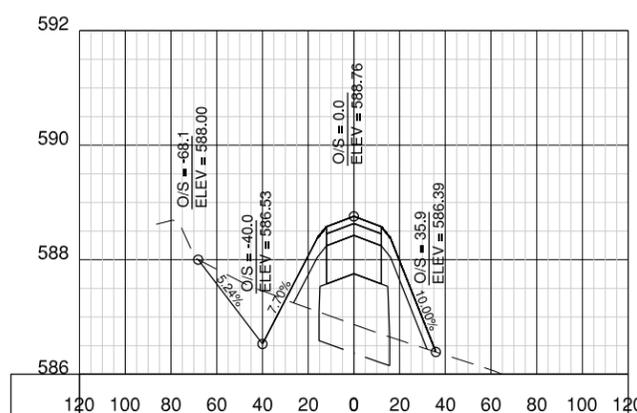
592
TOPSOIL STRIPPING = 26 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 16 SF
STRUCTURAL FILL = 37 SF
SHOULDER FILL = 19 SF



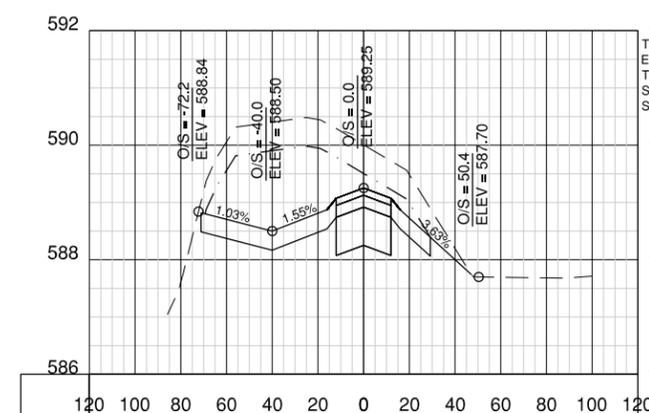
592
TOPSOIL STRIPPING = 70 SF
EMBANKMENT CUT = 149 SF
TOPSOIL PLACEMENT = 44 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



590
TOPSOIL STRIPPING = 15 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 37 SF
STRUCTURAL FILL = 35 SF
SHOULDER FILL = 34 SF



592
TOPSOIL STRIPPING = 29 SF
EMBANKMENT CUT = 9 SF
TOPSOIL PLACEMENT = 18 SF
STRUCTURAL FILL = 39 SF
SHOULDER FILL = 22 SF



592
TOPSOIL STRIPPING = 54 SF
EMBANKMENT CUT = 98 SF
TOPSOIL PLACEMENT = 25 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF

FILE: Design_recover002.dwg
UPDATE BY: Travis Strait
PLOT DATE: 6/13/2016 12:02 PM

Existing Topo
NW Service Road Base

QU022

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

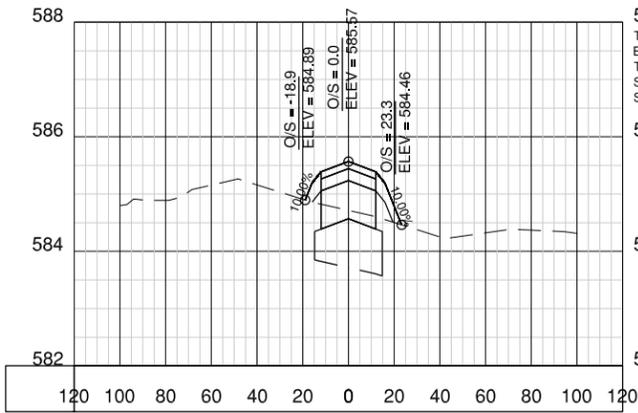
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
CROSS SECTIONS 2**

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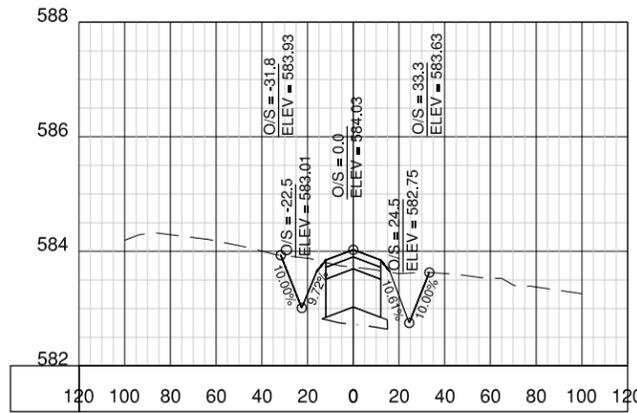
DESIGN BY: CMT-MLI
DRAWN BY: CMT-MLI
CHECKED BY: CMT-MLI
APPROVED BY: CMT-MLI
DATE: JUNE 3, 2016
JOB No: 15014-06

IL PROJ. NO. MLI-4531
AIP PROJ. NO 3-17-0068-XX

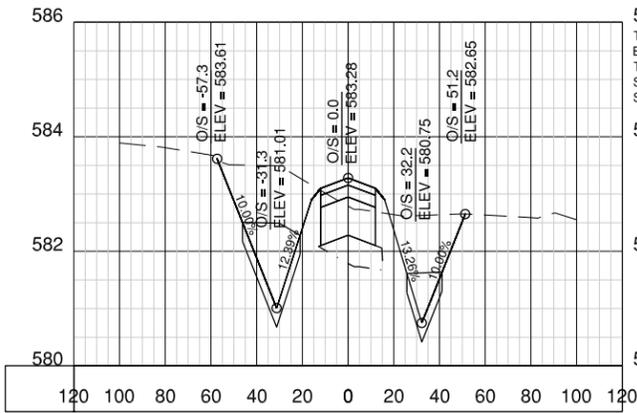
SHEET 21 OF 33 SHEETS



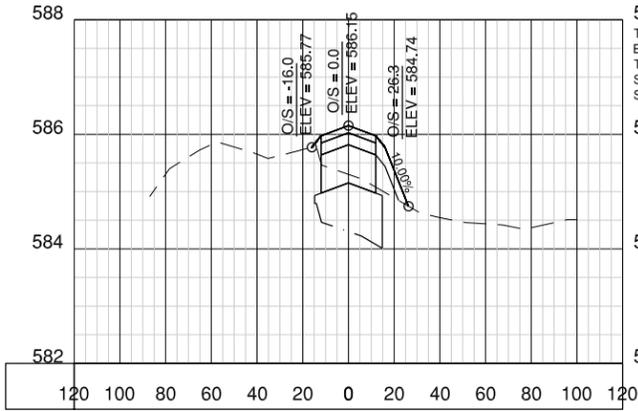
588
TOPSOIL STRIPPING = 26 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 5 SF
STRUCTURAL FILL = 20 SF
SHOULDER FILL = 3 SF



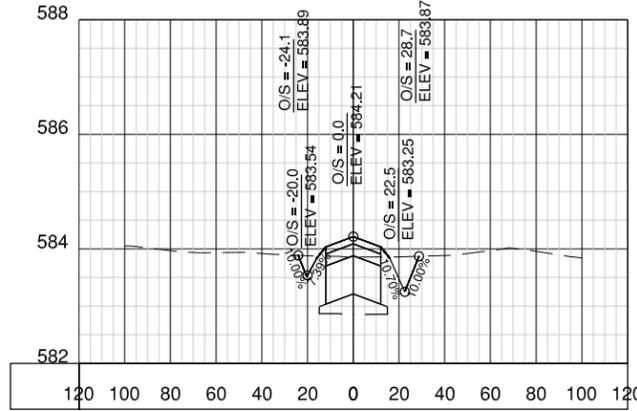
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TOPSOIL STRIPPING = 40 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 1 SF
STRUCTURAL FILL = 5 SF
SHOULDER FILL = 0 SF



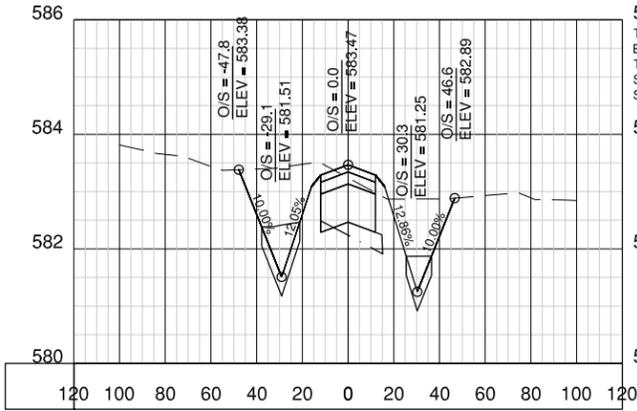
586
TOPSOIL STRIPPING = 83 SF
EMBANKMENT CUT = 26 SF
TOPSOIL PLACEMENT = 2 SF
STRUCTURAL FILL = 9 SF
SHOULDER FILL = 1 SF



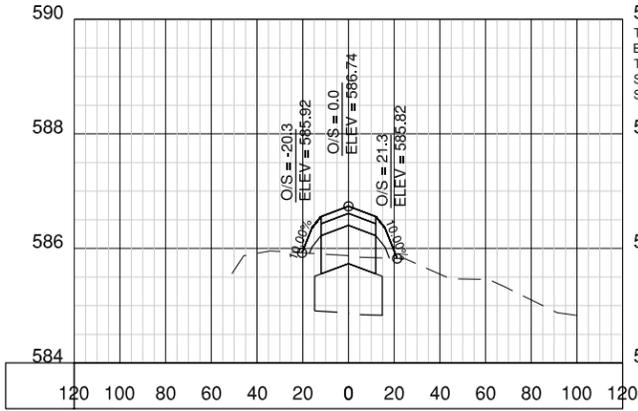
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TOPSOIL STRIPPING = 26 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 5 SF
STRUCTURAL FILL = 20 SF
SHOULDER FILL = 4 SF



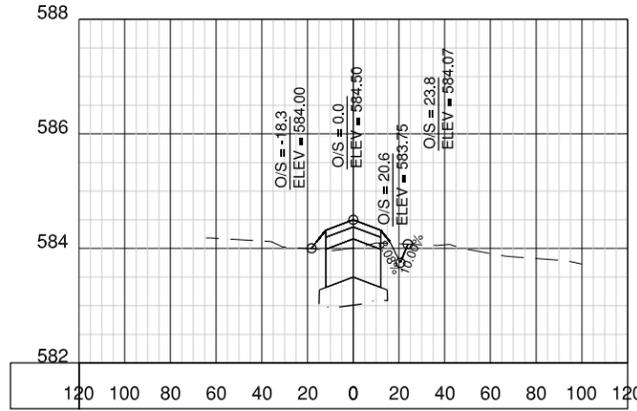
588
TOPSOIL STRIPPING = 30 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 1 SF
STRUCTURAL FILL = 7 SF
SHOULDER FILL = 0 SF



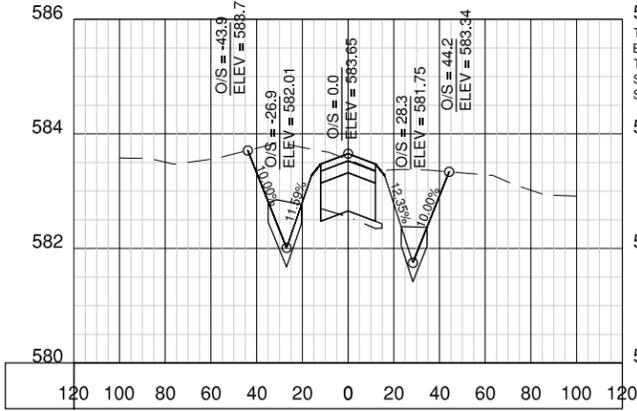
586
TOPSOIL STRIPPING = 71 SF
EMBANKMENT CUT = 12 SF
TOPSOIL PLACEMENT = 1 SF
STRUCTURAL FILL = 4 SF
SHOULDER FILL = 0 SF



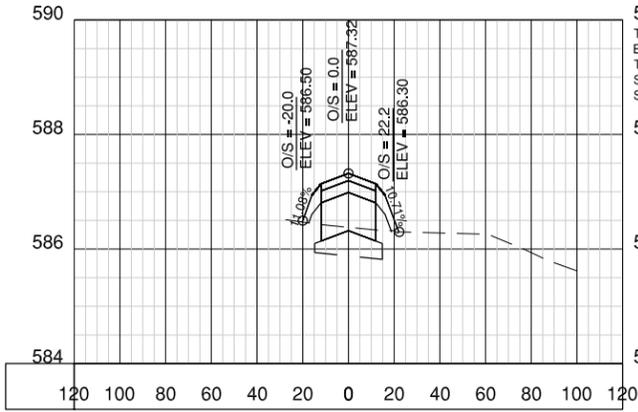
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TOPSOIL STRIPPING = 26 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 5 SF
STRUCTURAL FILL = 21 SF
SHOULDER FILL = 2 SF



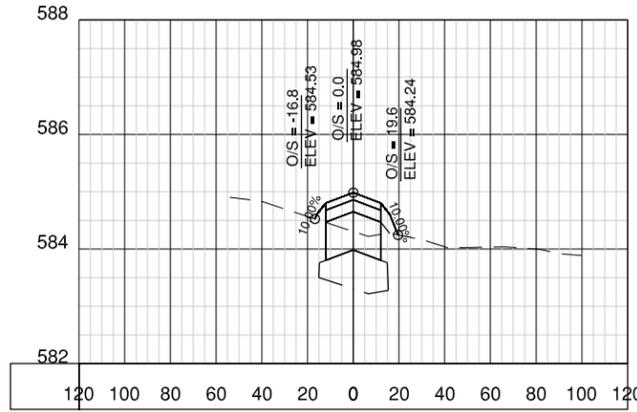
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EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 2 SF
STRUCTURAL FILL = 10 SF
SHOULDER FILL = 1 SF



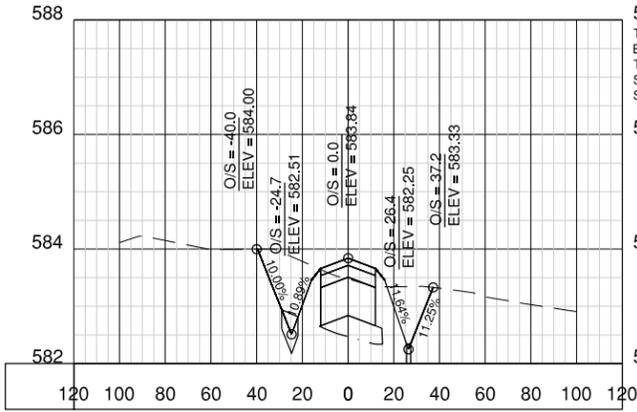
586
TOPSOIL STRIPPING = 68 SF
EMBANKMENT CUT = 2 SF
TOPSOIL PLACEMENT = 1 SF
STRUCTURAL FILL = 2 SF
SHOULDER FILL = 0 SF



590
TOPSOIL STRIPPING = 26 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 5 SF
STRUCTURAL FILL = 23 SF
SHOULDER FILL = 3 SF



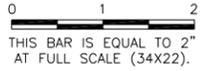
588
TOPSOIL STRIPPING = 25 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 3 SF
STRUCTURAL FILL = 15 SF
SHOULDER FILL = 1 SF



588
TOPSOIL STRIPPING = 52 SF
EMBANKMENT CUT = 2 SF
TOPSOIL PLACEMENT = 2 SF
STRUCTURAL FILL = 7 SF
SHOULDER FILL = 0 SF

QU022

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

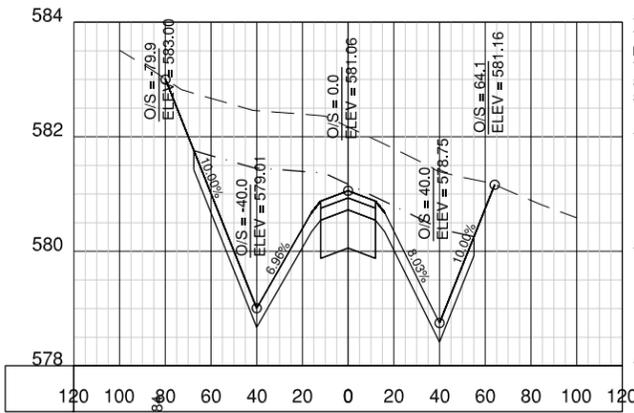
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
CROSS SECTIONS 3**



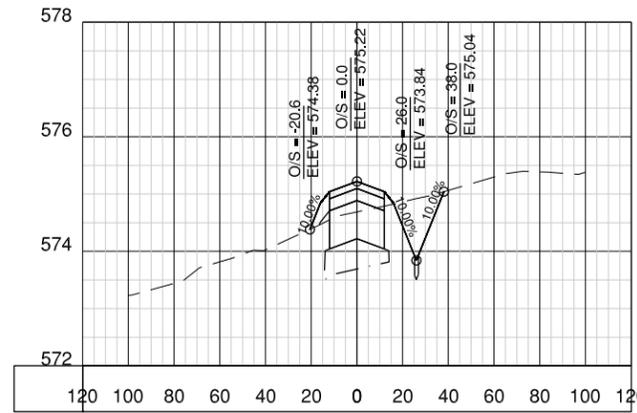
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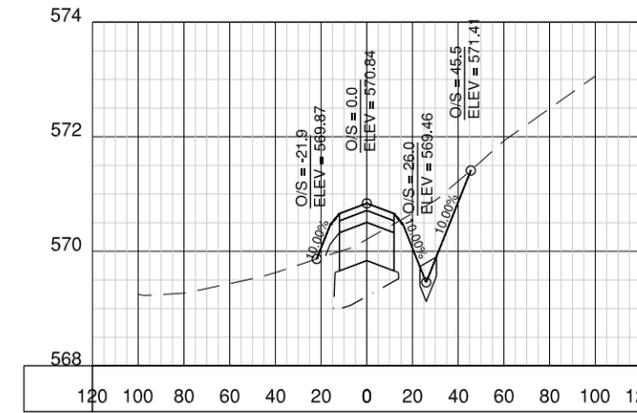
DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO. MLI-4531 AIP PROJ. NO 3-17-0068-XX	
SHEET 22 OF 33 SHEETS	



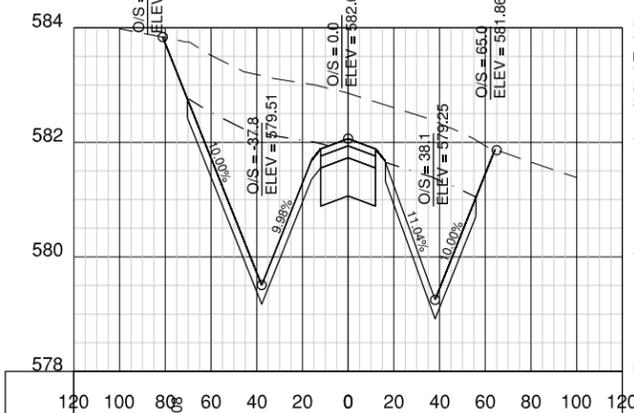
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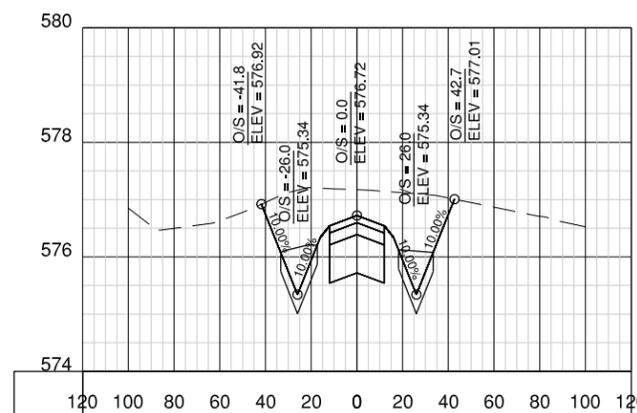
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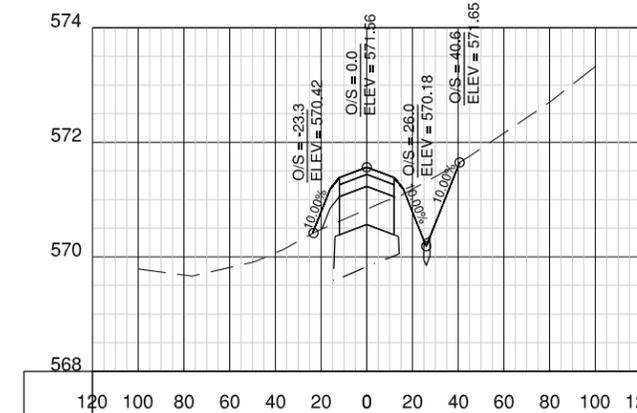
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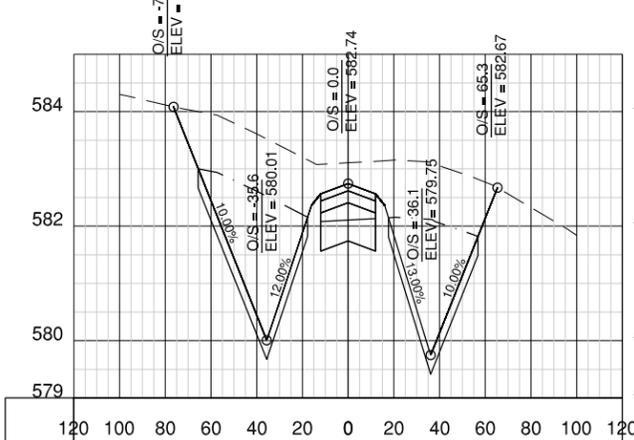
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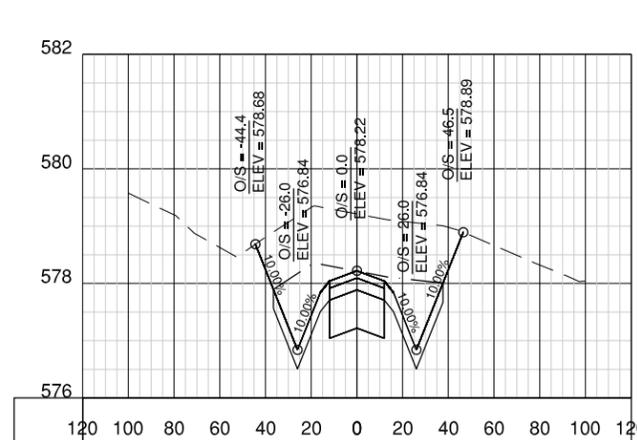
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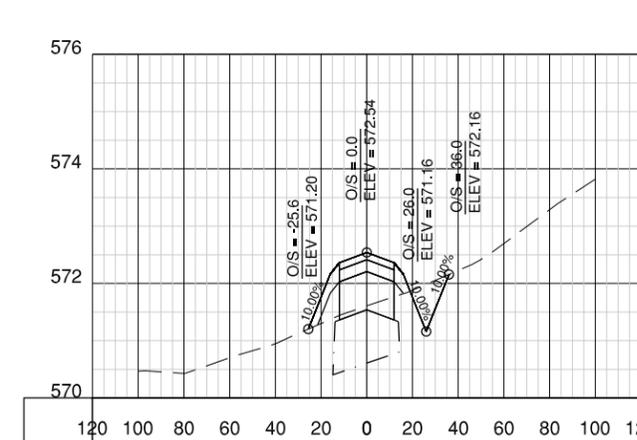
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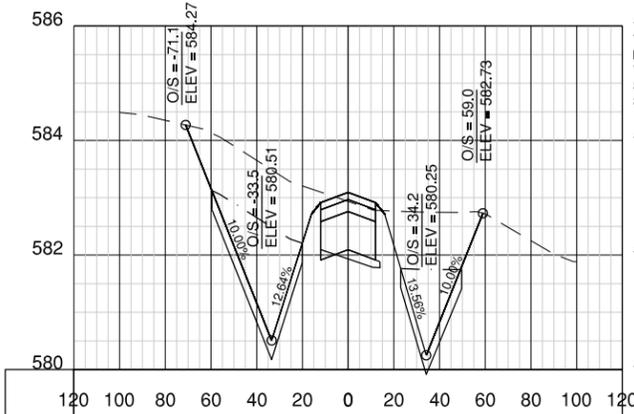
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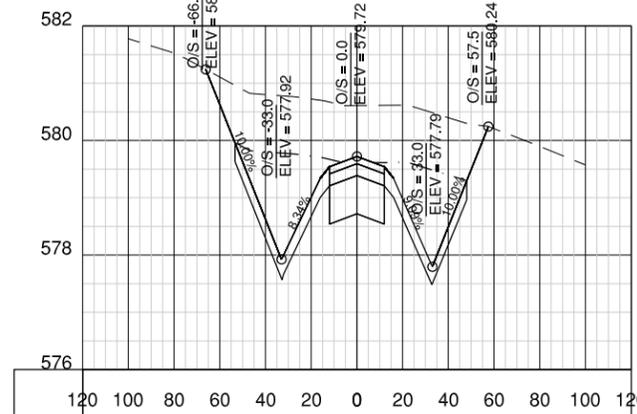
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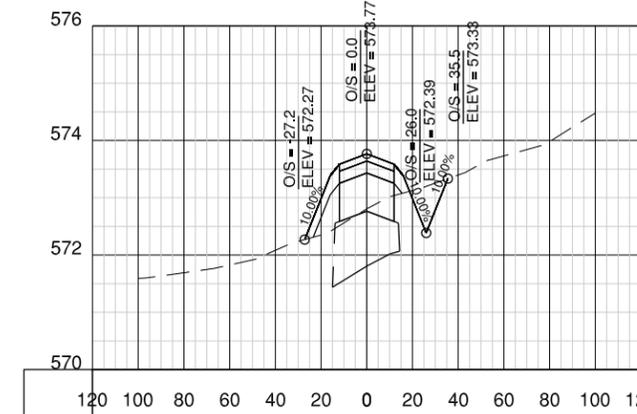
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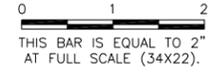


576
574
572
570

Existing Topo
NW Service Road Base

QU022

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NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

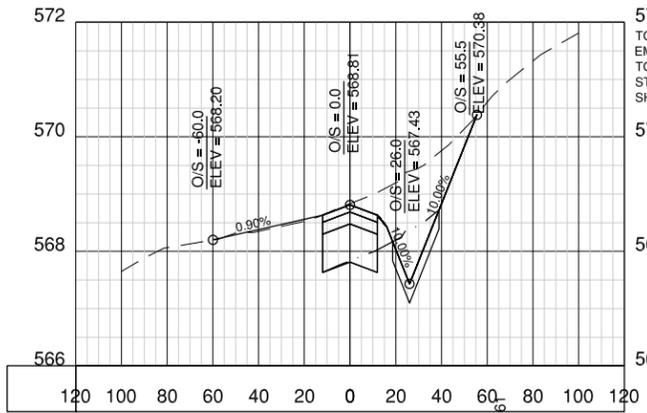
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
CROSS SECTIONS 4**



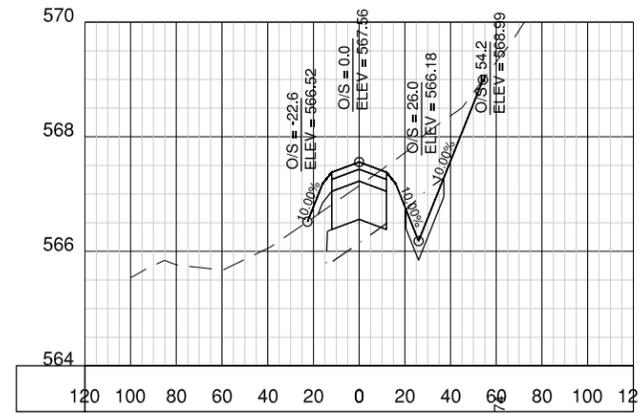
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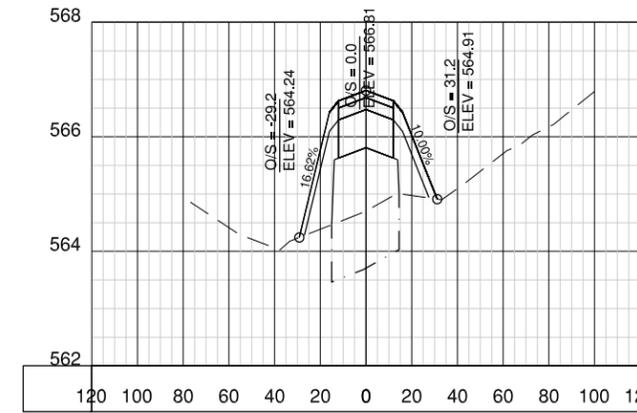
DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL. PROJ. NO. MLI-4531 AIP PROJ. NO 3-17-0068-XX	
SHEET 23 OF 33 SHEETS	



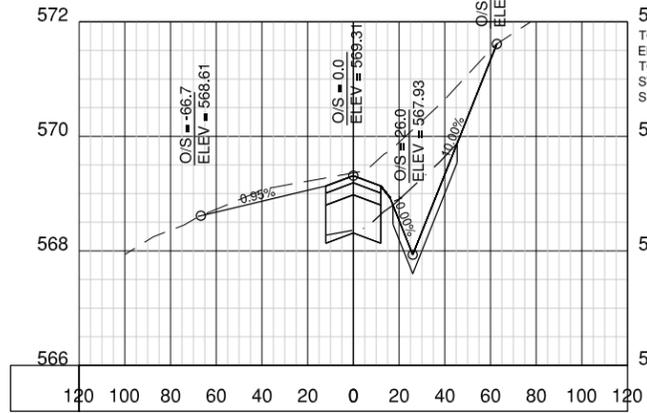
TOPSOIL STRIPPING = 57 SF
EMBANKMENT CUT = 19 SF
TOPSOIL PLACEMENT = 8 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



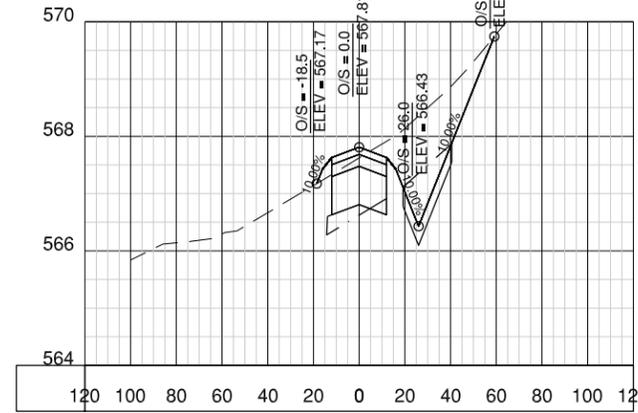
TOPSOIL STRIPPING = 45 SF
EMBANKMENT CUT = 12 SF
TOPSOIL PLACEMENT = 8 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



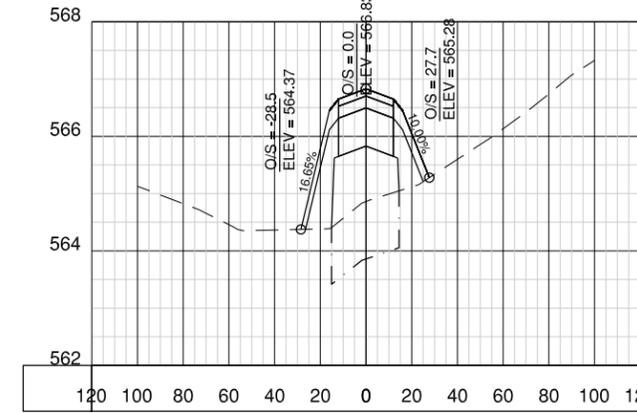
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EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 11 SF
STRUCTURAL FILL = 59 SF
SHOULDER FILL = 25 SF



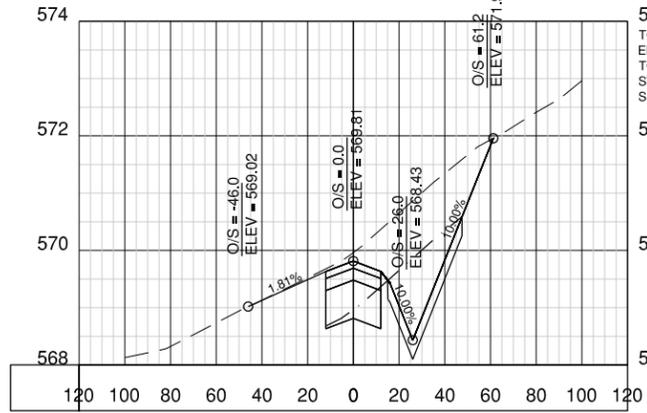
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EMBANKMENT CUT = 30 SF
TOPSOIL PLACEMENT = 9 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



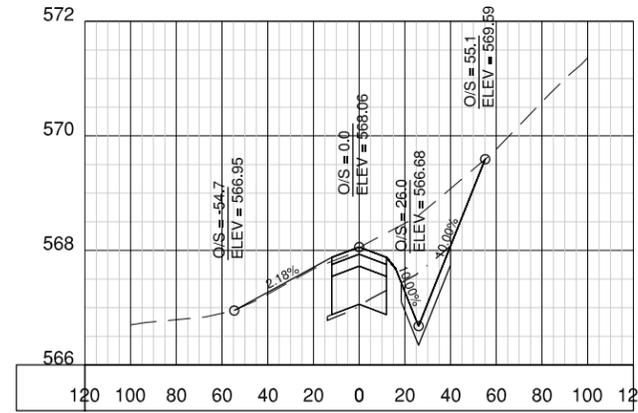
TOPSOIL STRIPPING = 55 SF
EMBANKMENT CUT = 18 SF
TOPSOIL PLACEMENT = 8 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



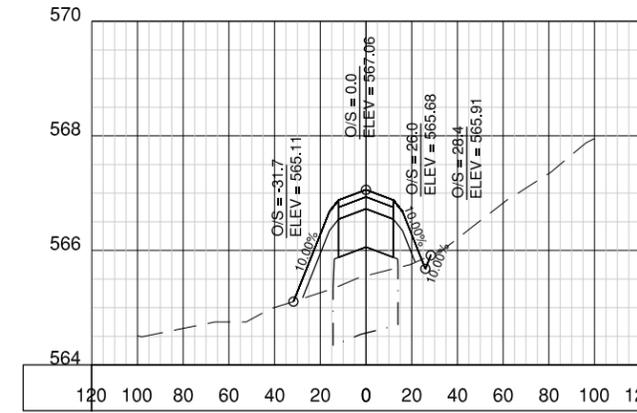
TOPSOIL STRIPPING = 14 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 10 SF
STRUCTURAL FILL = 42 SF
SHOULDER FILL = 23 SF



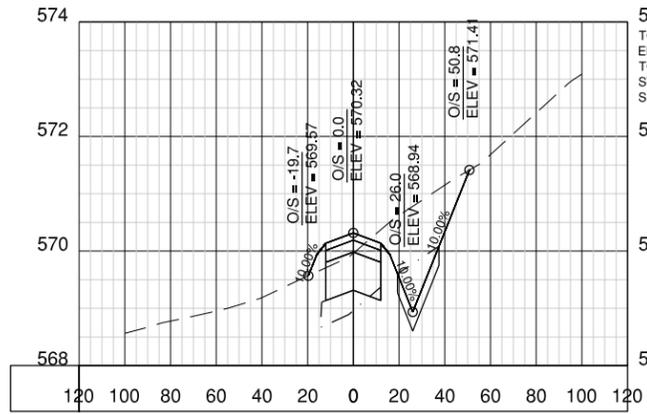
TOPSOIL STRIPPING = 67 SF
EMBANKMENT CUT = 40 SF
TOPSOIL PLACEMENT = 71 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



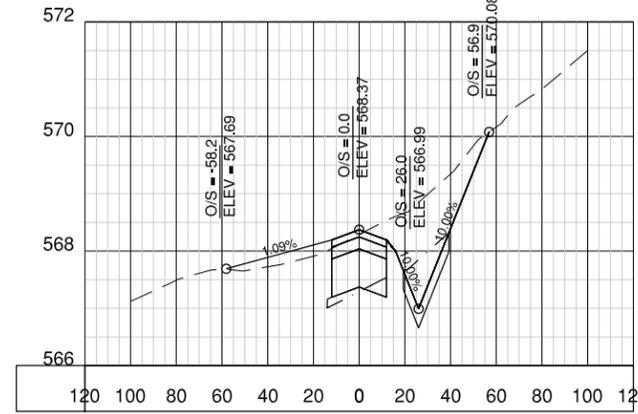
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EMBANKMENT CUT = 20 SF
TOPSOIL PLACEMENT = 9 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



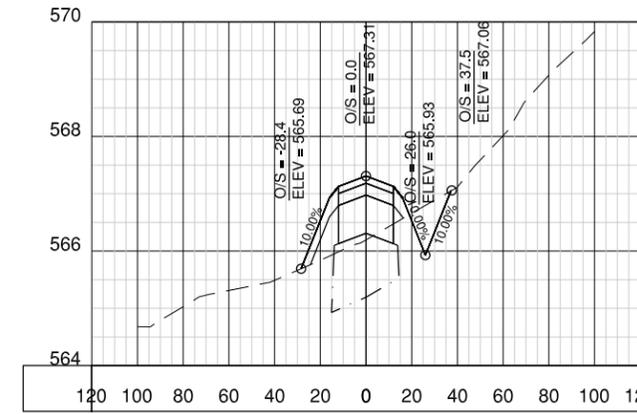
TOPSOIL STRIPPING = 28 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 10 SF
STRUCTURAL FILL = 41 SF
SHOULDER FILL = 15 SF



TOPSOIL STRIPPING = 47 SF
EMBANKMENT CUT = 14 SF
TOPSOIL PLACEMENT = 8 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



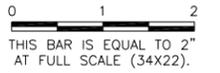
TOPSOIL STRIPPING = 56 SF
EMBANKMENT CUT = 17 SF
TOPSOIL PLACEMENT = 8 SF
STRUCTURAL FILL = 0 SF
SHOULDER FILL = 0 SF



TOPSOIL STRIPPING = 34 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 7 SF
STRUCTURAL FILL = 29 SF
SHOULDER FILL = 1 SF

QU022

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
CROSS SECTIONS 5**



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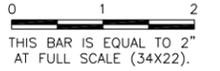


DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO. MLI-4531 AIP PROJ. NO 3-17-0068-XX	
SHEET 24 OF 33 SHEETS	

Existing Topo
 NW Service Road Base

QU022

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

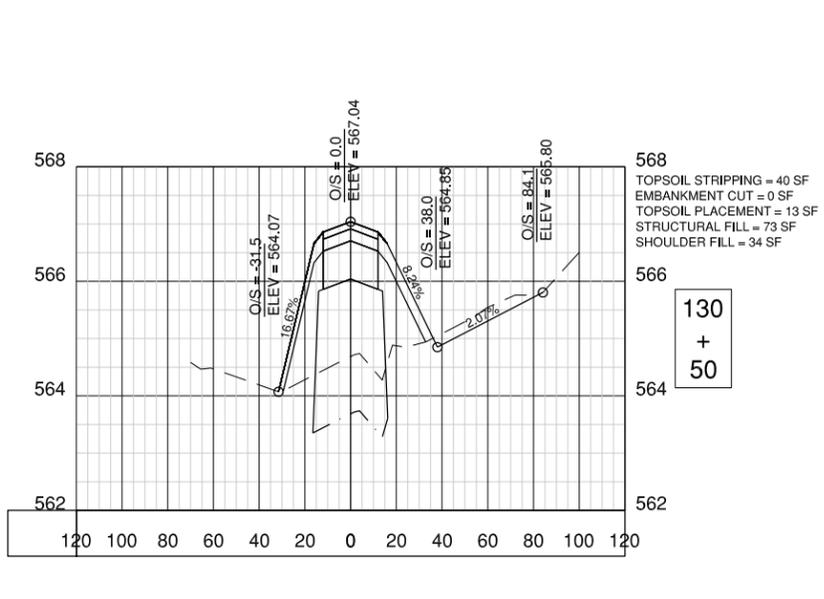
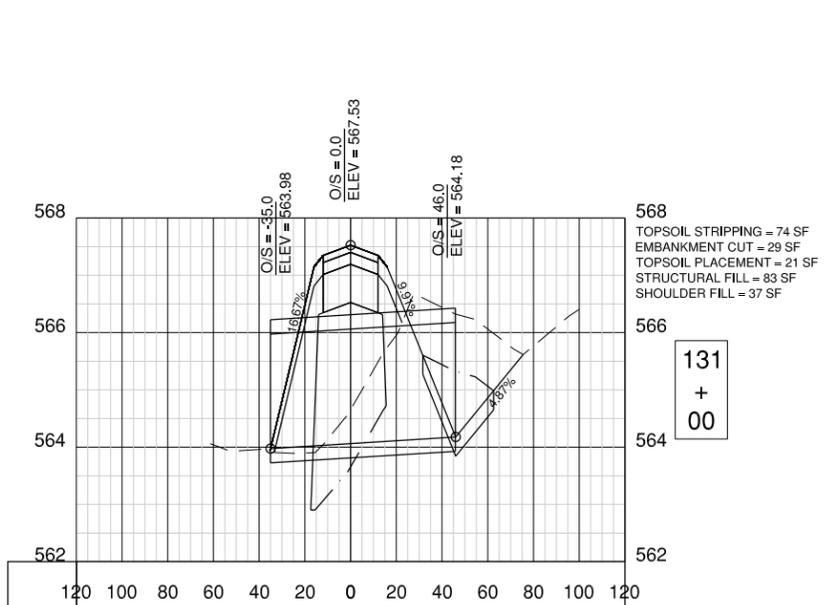
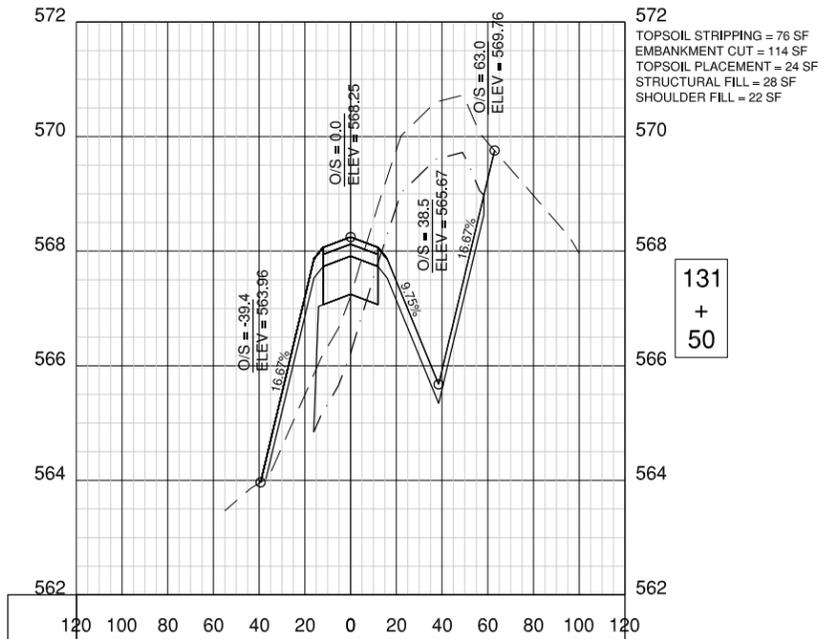
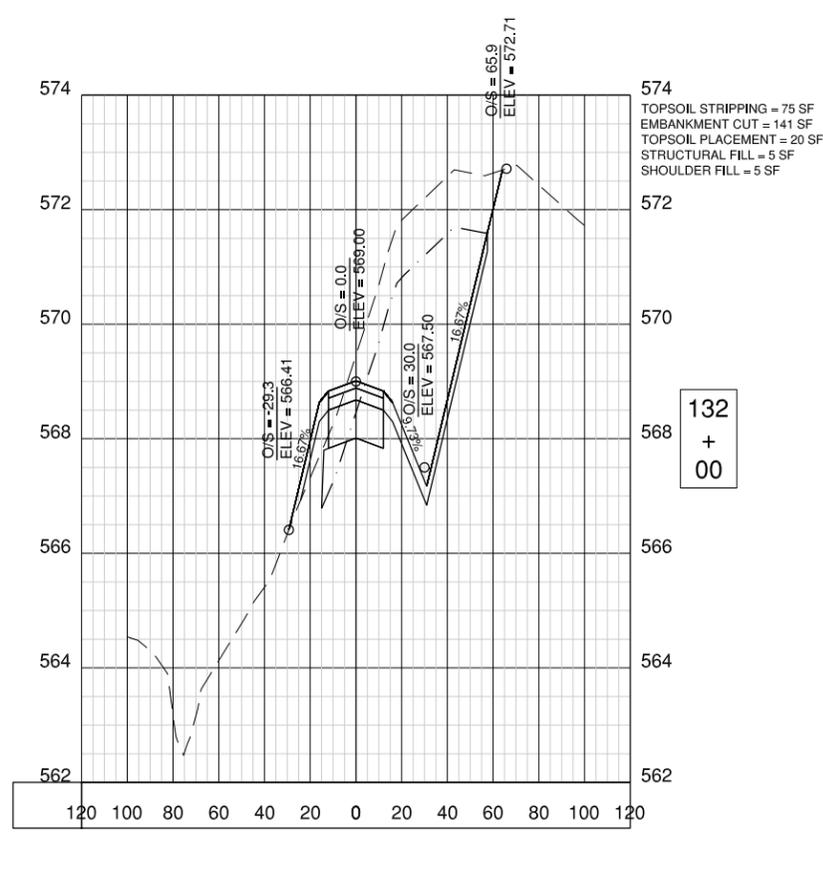
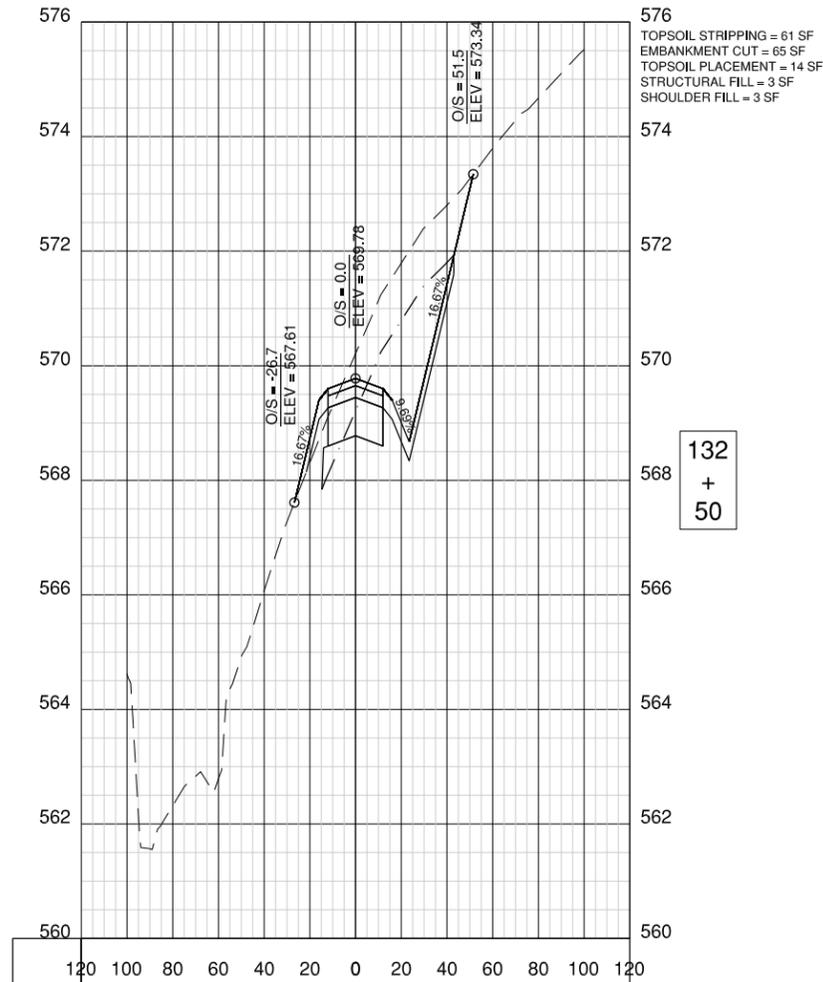
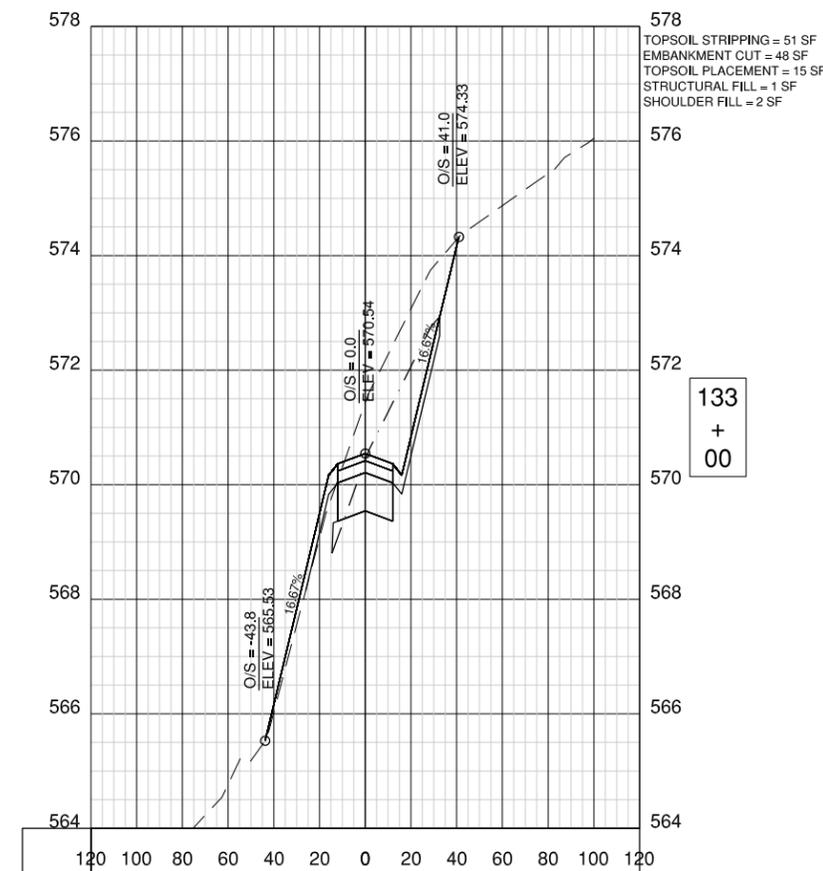
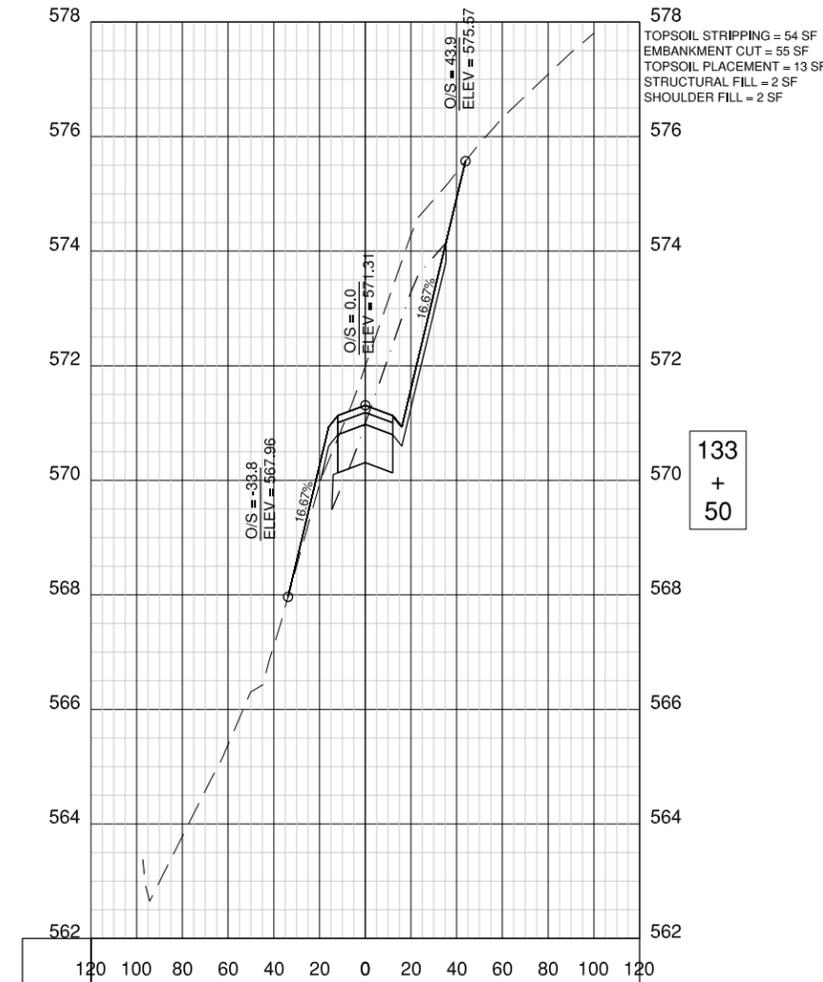
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 CROSS SECTIONS 6**



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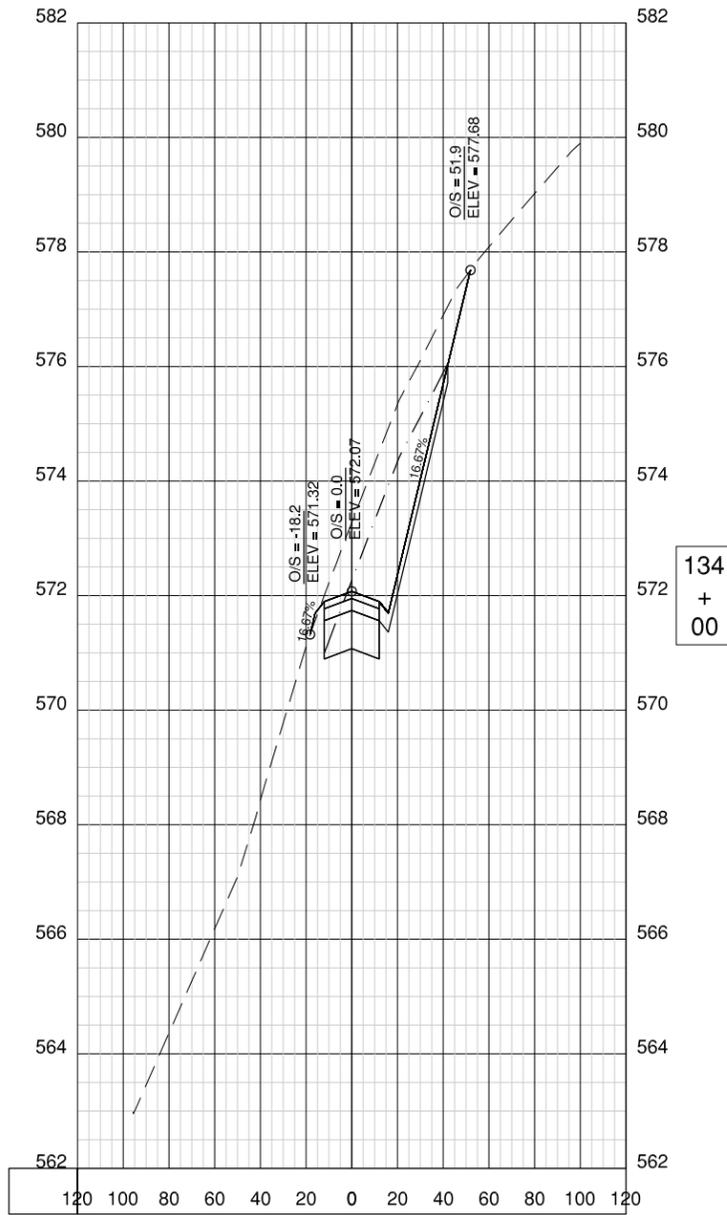
DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL. PROJ. NO. MLI-4531 AIP PROJ. NO 3-17-0068-XX	
SHEET 25	OF 33 SHEETS



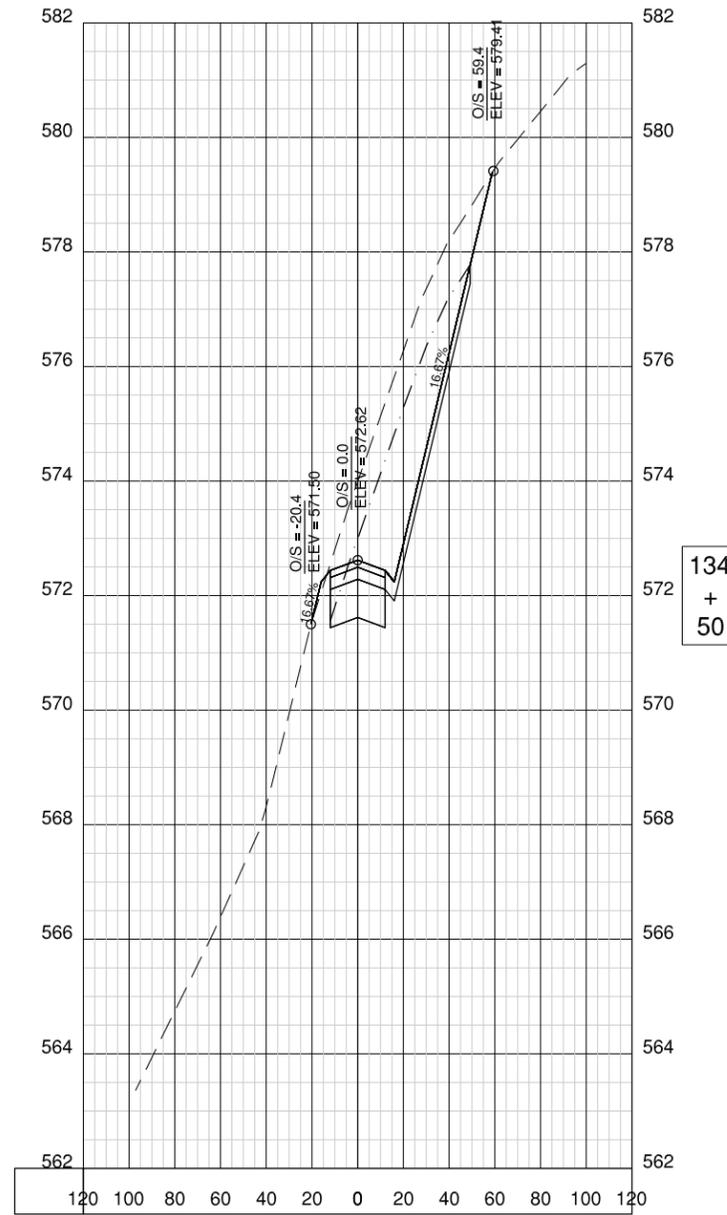
QU022

REVISIONS		
NUMBER	BY	DATE

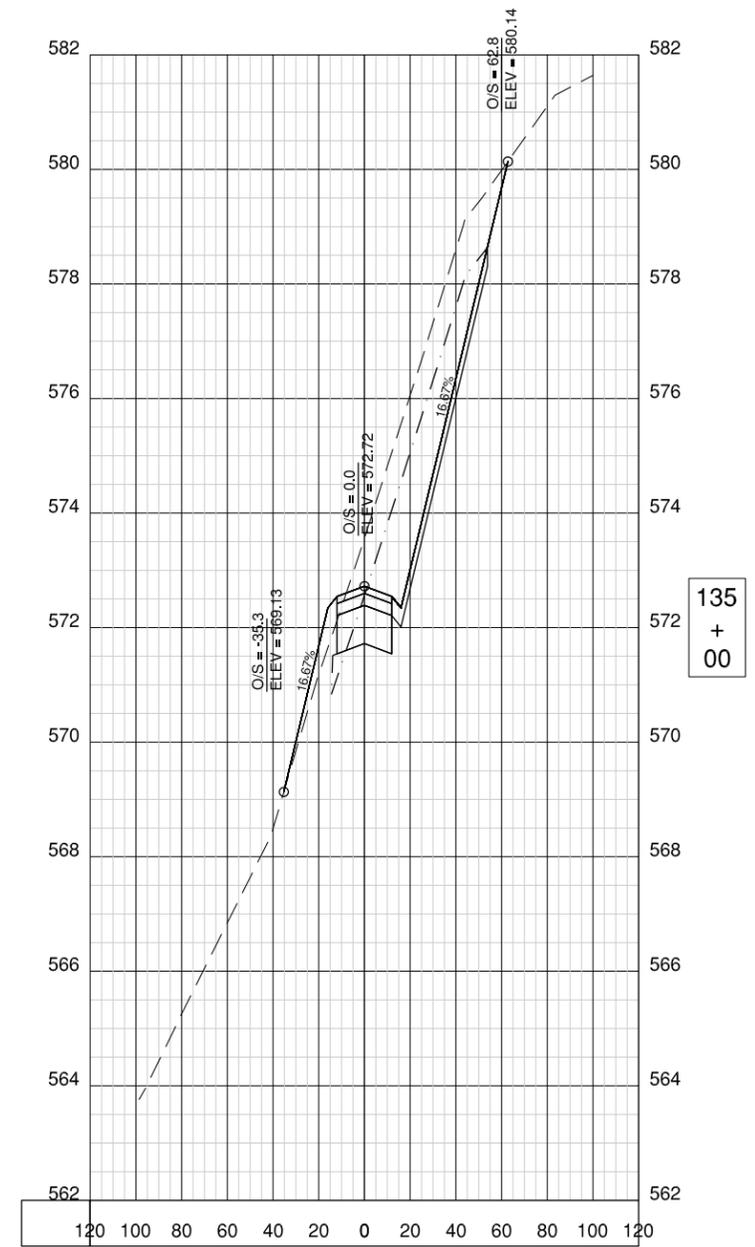
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



TOPSOIL STRIPPING = 60 SF
 EMBANKMENT CUT = 79 SF
 TOPSOIL PLACEMENT = 10 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 0 SF



TOPSOIL STRIPPING = 66 SF
 EMBANKMENT CUT = 105 SF
 TOPSOIL PLACEMENT = 13 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 0 SF



TOPSOIL STRIPPING = 69 SF
 EMBANKMENT CUT = 96 SF
 TOPSOIL PLACEMENT = 20 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 2 SF

**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 CROSS SECTIONS 7**

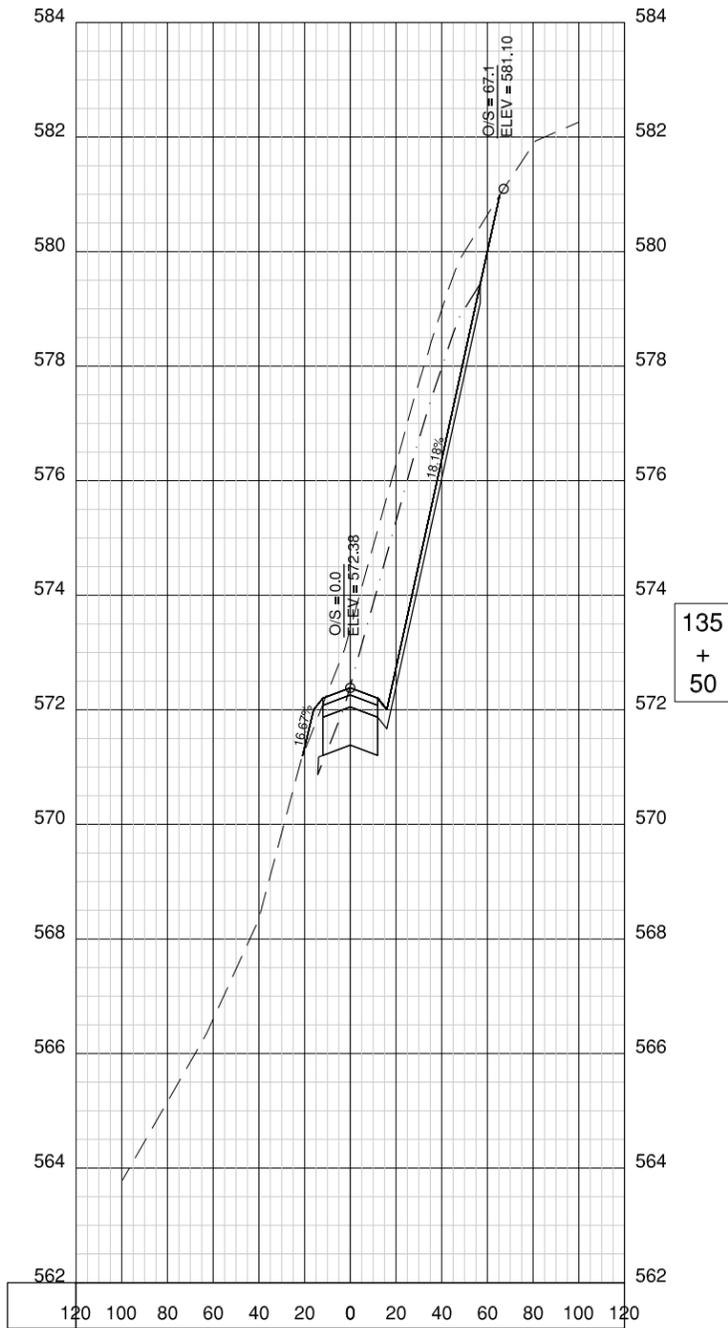


DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO	3-17-0068-XX
SHEET	26 OF 33 SHEETS

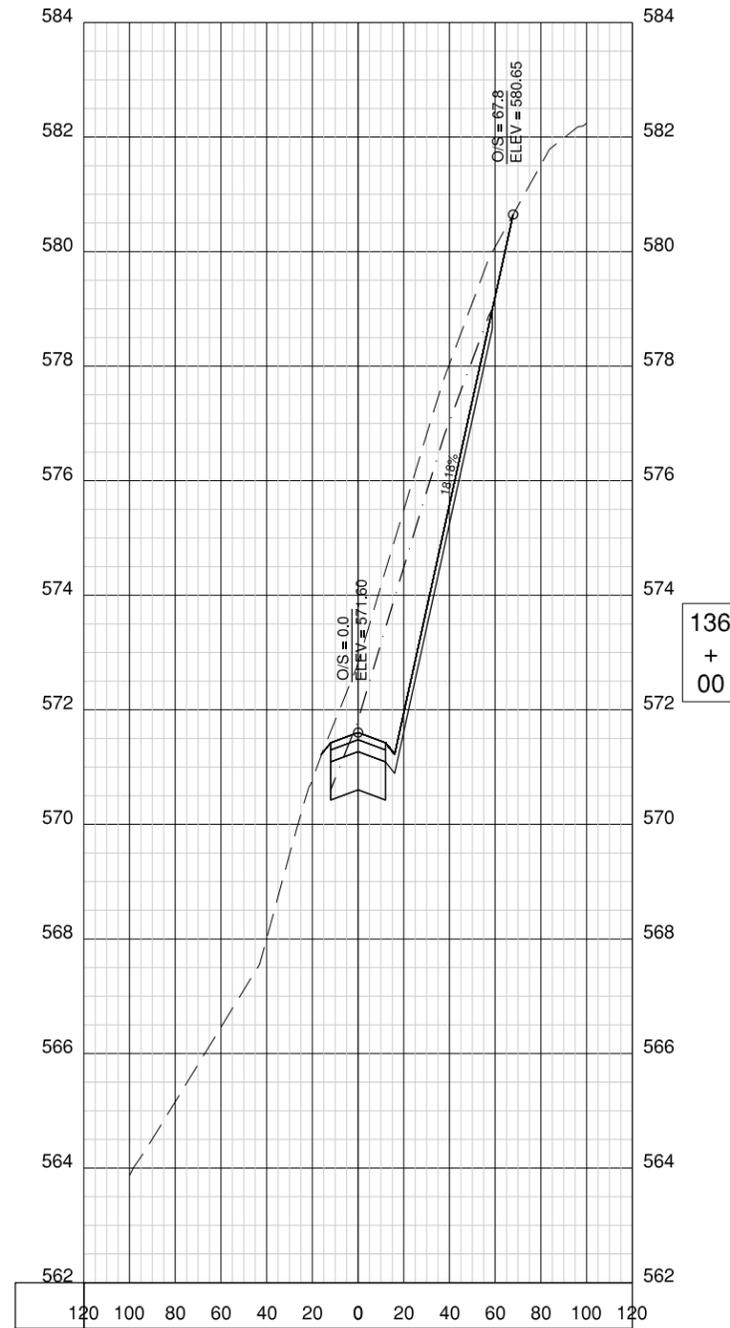
QU022

REVISIONS		
NUMBER	BY	DATE

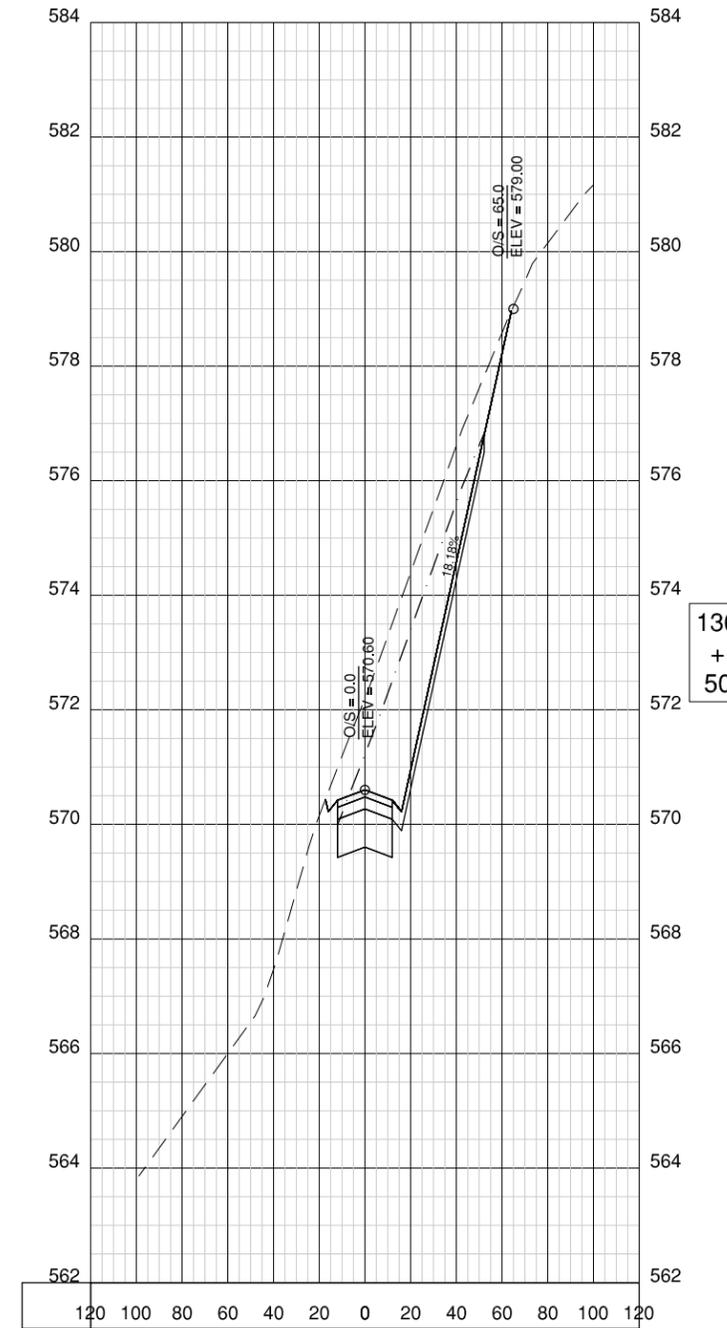
0 1 2
 THIS BAR IS EQUAL TO 2"
 AT FULL SCALE (34X22).



TOPSOIL STRIPPING = 73 SF
 EMBANKMENT CUT = 122 SF
 TOPSOIL PLACEMENT = 17 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 0 SF



TOPSOIL STRIPPING = 76 SF
 EMBANKMENT CUT = 124 SF
 TOPSOIL PLACEMENT = 16 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 0 SF



TOPSOIL STRIPPING = 72 SF
 EMBANKMENT CUT = 116 SF
 TOPSOIL PLACEMENT = 14 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 0 SF

METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS

CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
CROSS SECTIONS 8



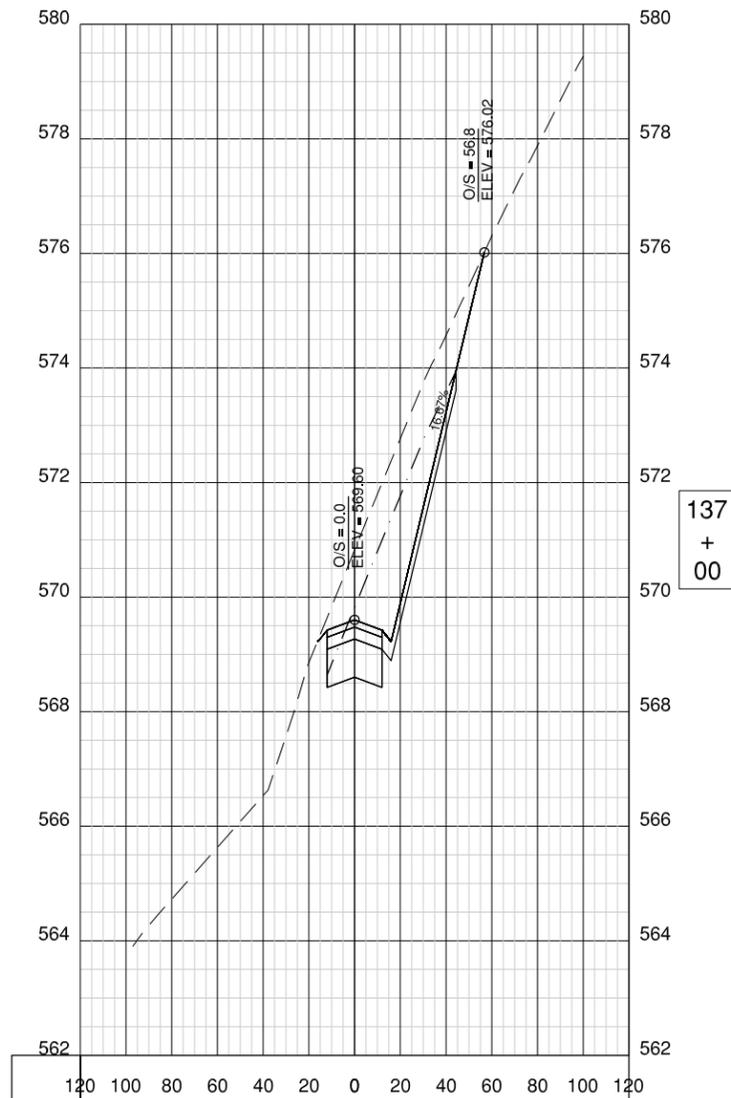
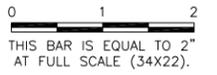
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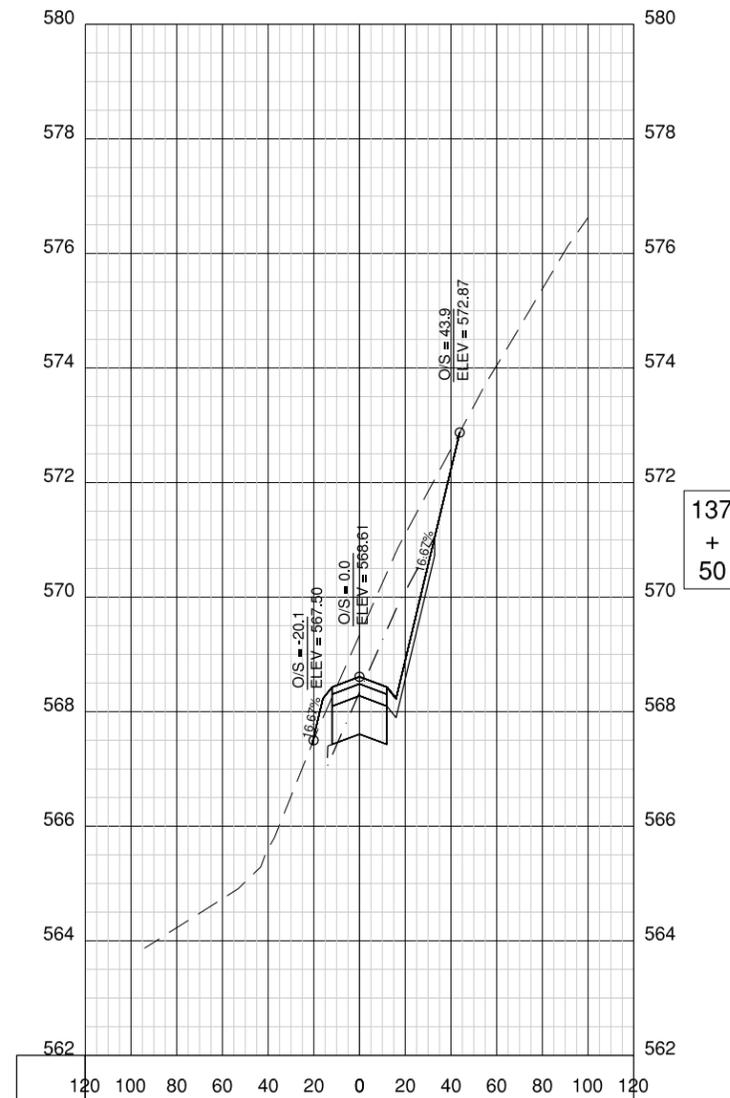
DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO	3-17-0068-XX
SHEET	27 OF 33 SHEETS

QU022

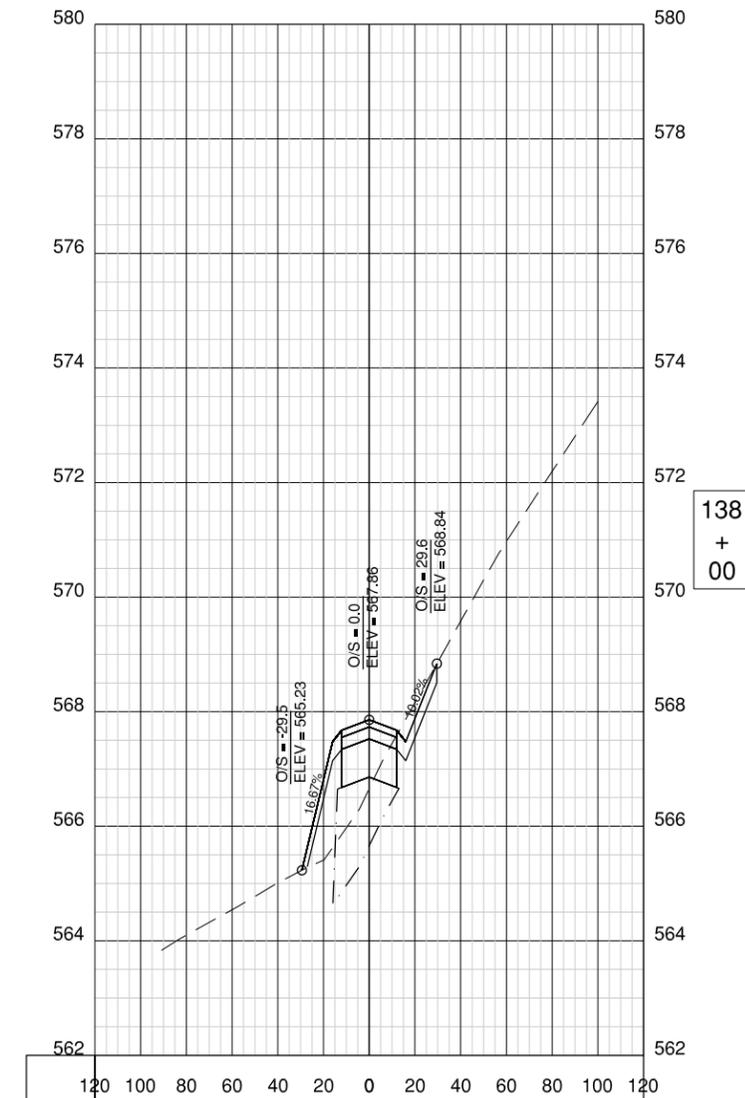
REVISIONS		
NUMBER	BY	DATE



TOPSOIL STRIPPING = 63 SF
 EMBANKMENT CUT = 82 SF
 TOPSOIL PLACEMENT = 11 SF
 STRUCTURAL FILL = 0 SF
 SHOULDER FILL = 0 SF



TOPSOIL STRIPPING = 53 SF
 EMBANKMENT CUT = 45 SF
 TOPSOIL PLACEMENT = 9 SF
 STRUCTURAL FILL = 1 SF
 SHOULDER FILL = 1 SF



TOPSOIL STRIPPING = 44 SF
 EMBANKMENT CUT = 1 SF
 TOPSOIL PLACEMENT = 6 SF
 STRUCTURAL FILL = 19 SF
 SHOULDER FILL = 14 SF

K:\Molinda\15014-06_NW_ServiceRoad\Phase_1\Draw\CSD

**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

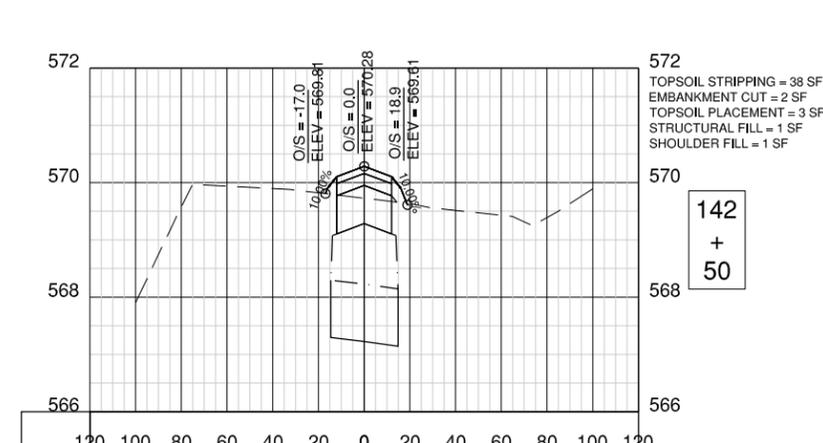
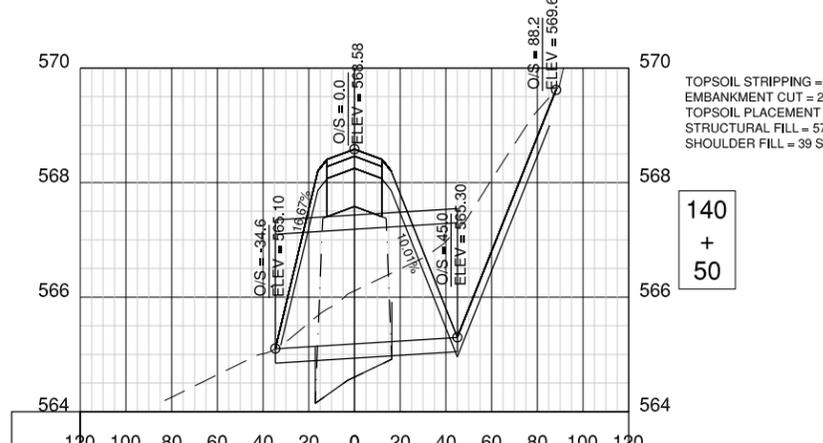
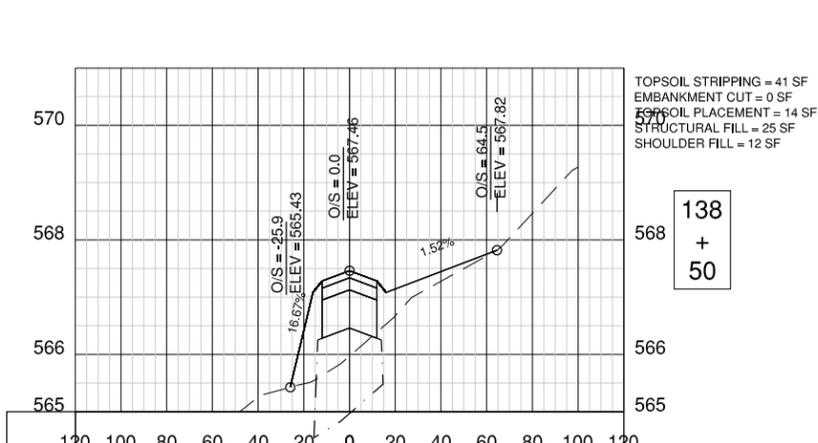
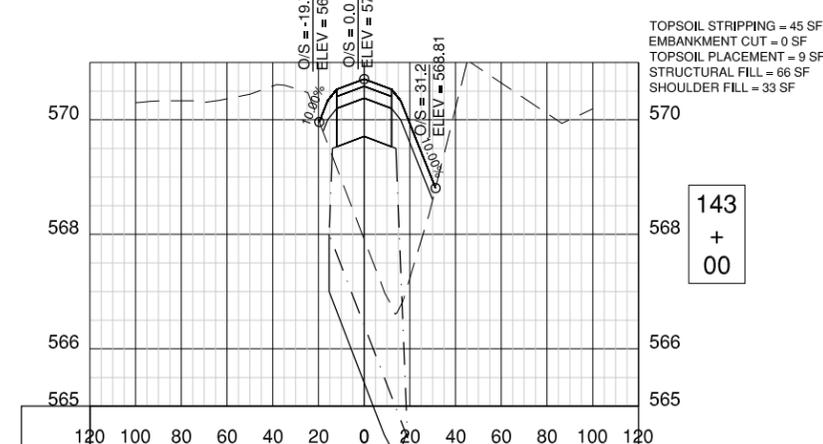
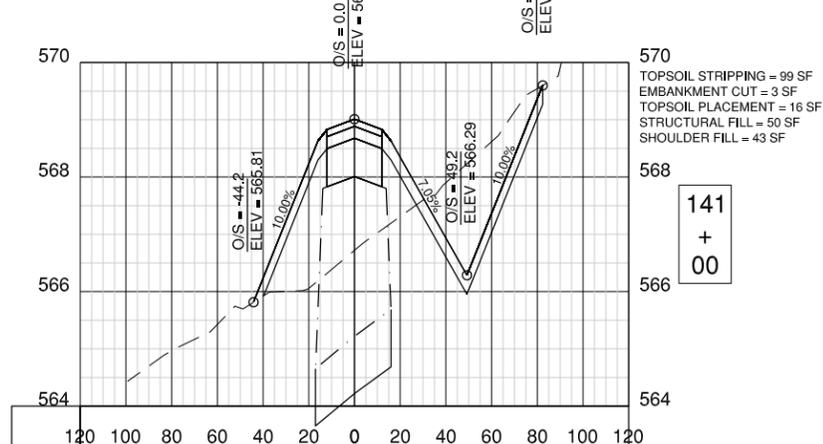
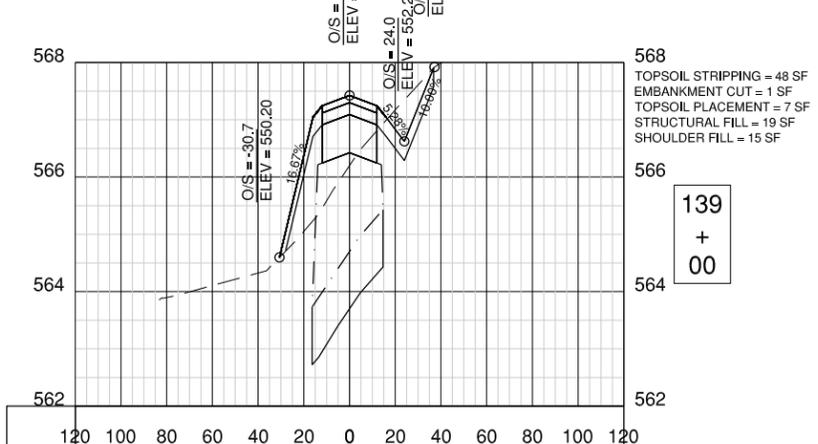
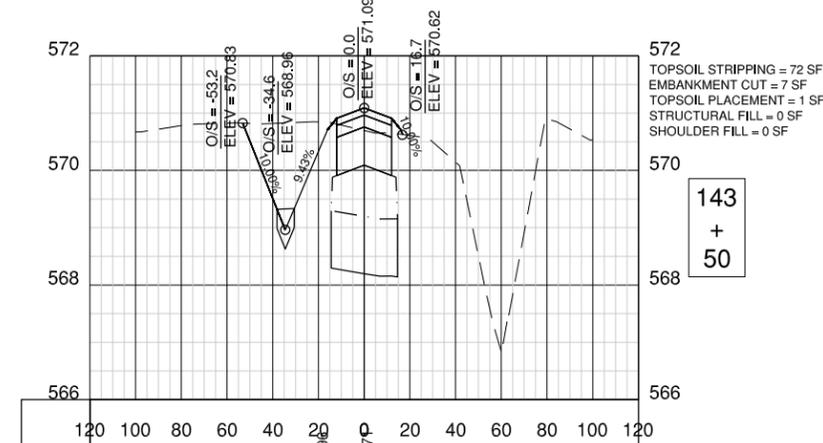
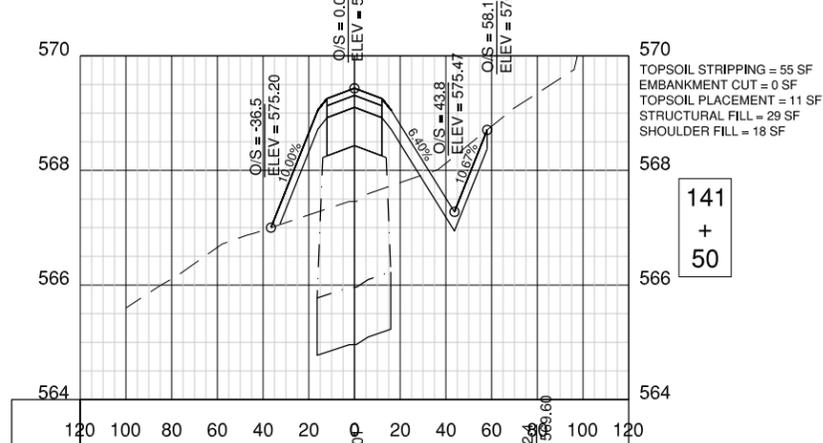
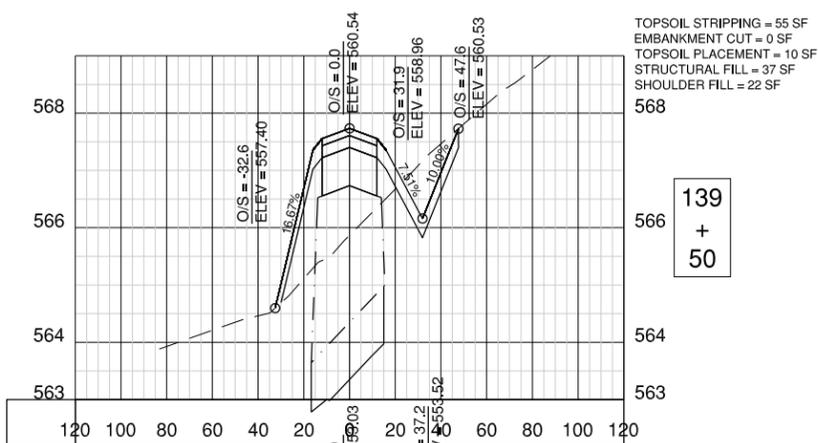
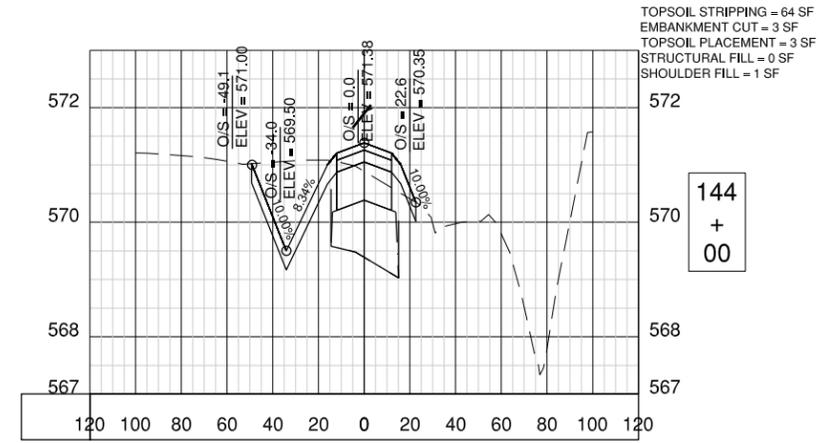
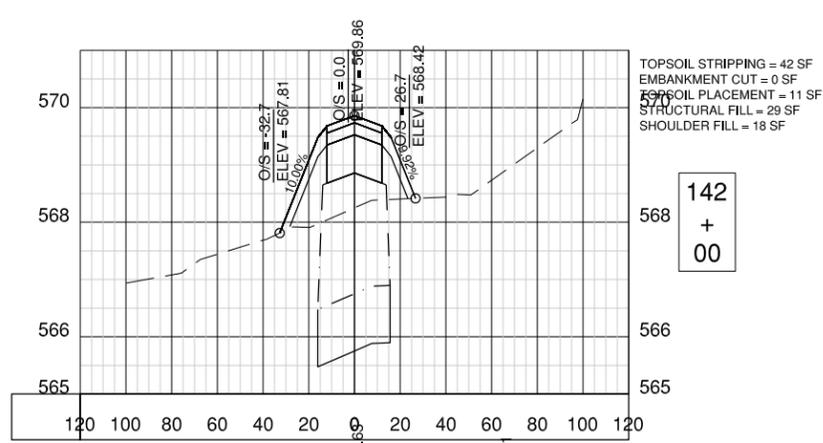
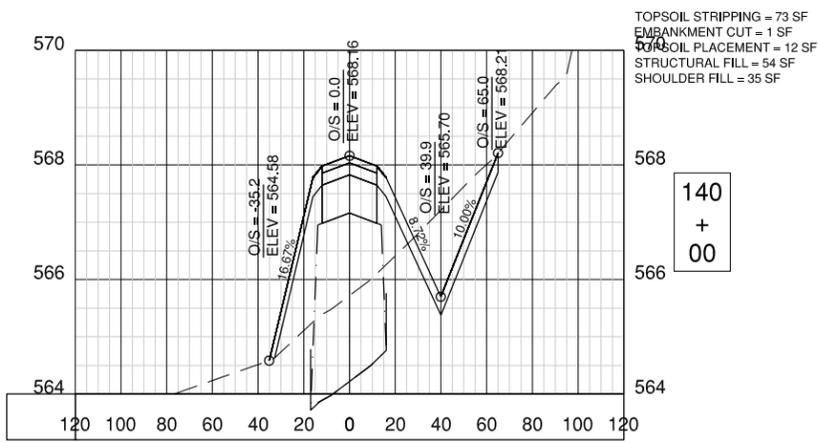
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 CROSS SECTIONS 9**



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DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO.	MLI-4531
AIP PROJ. NO	3-17-0068-XX
SHEET	28 OF 33 SHEETS



FILE: Design_recover002.dwg
UPDATE BY: Travis Strait
PLOT DATE: 6/13/2016 11:53 AM

Existing Topo
NW Service Road Base

QU022

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

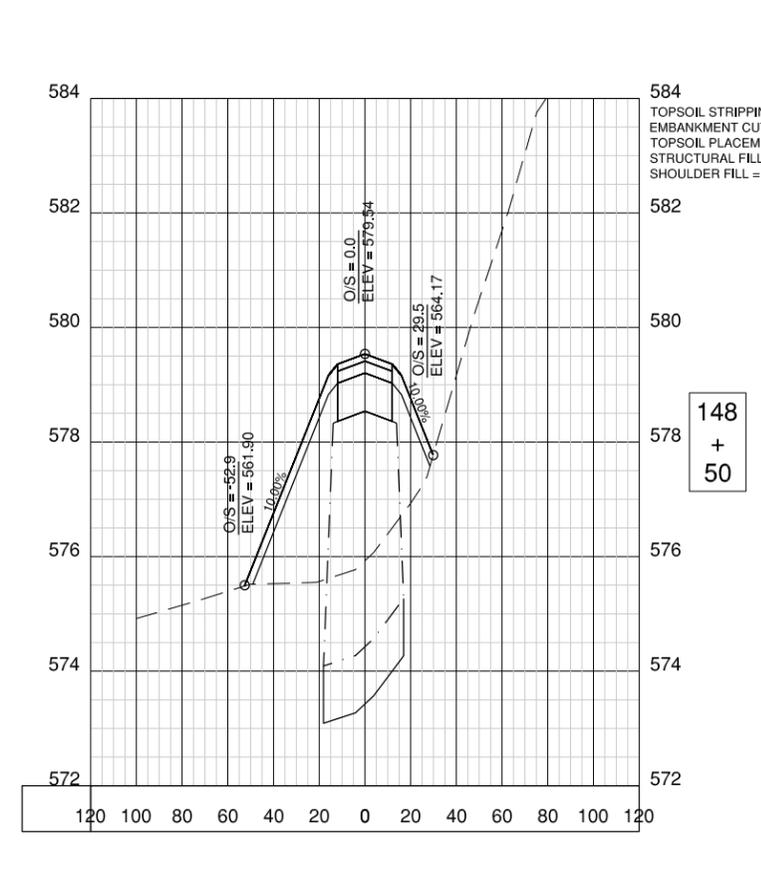
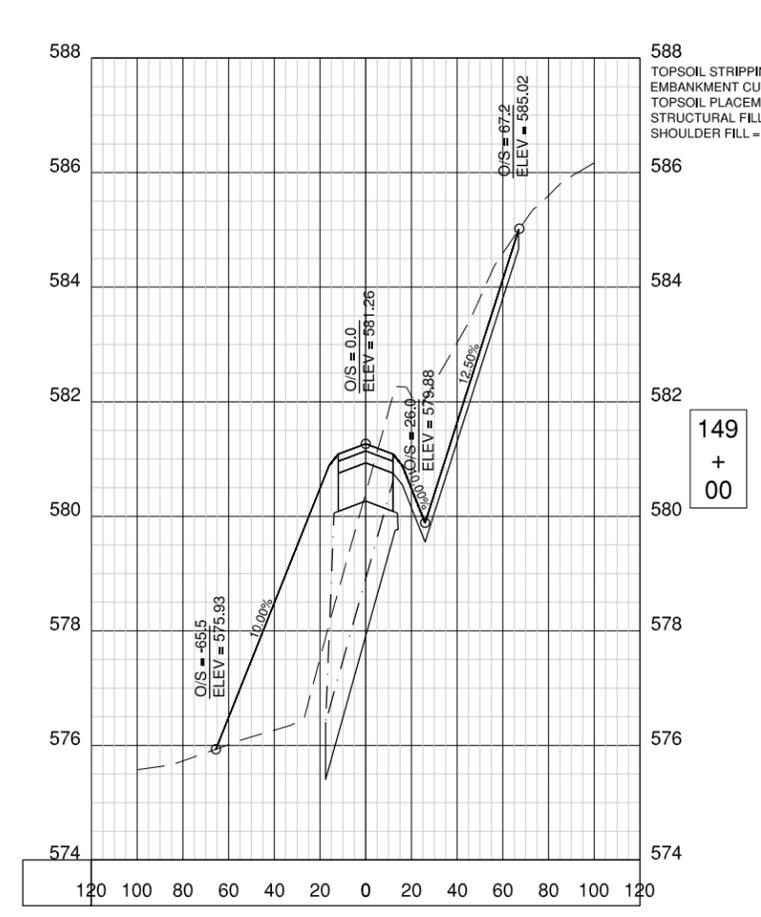
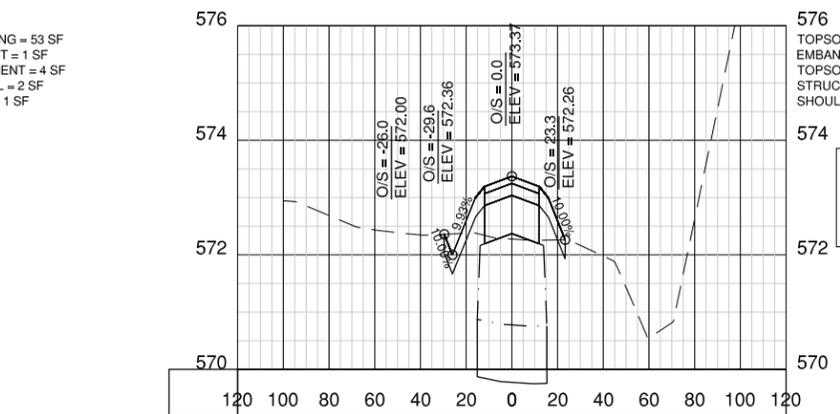
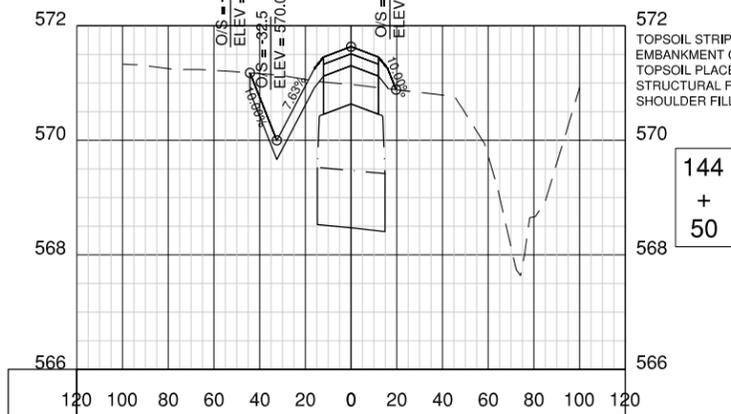
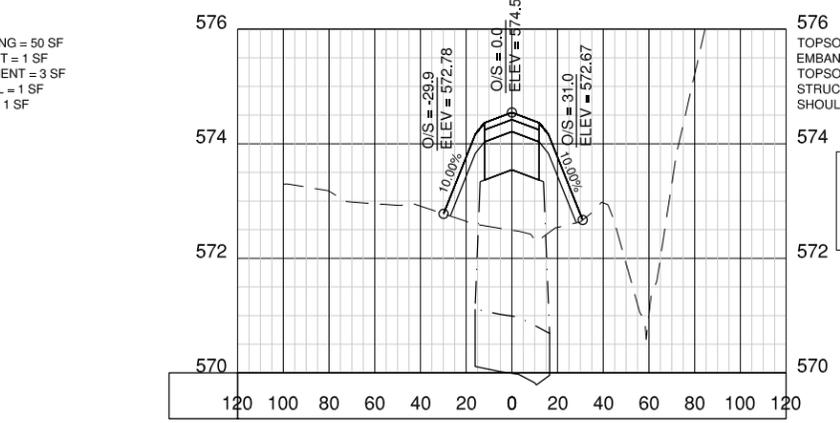
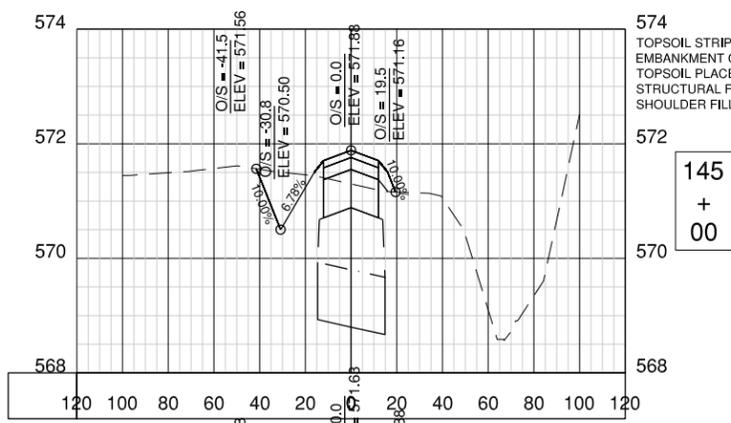
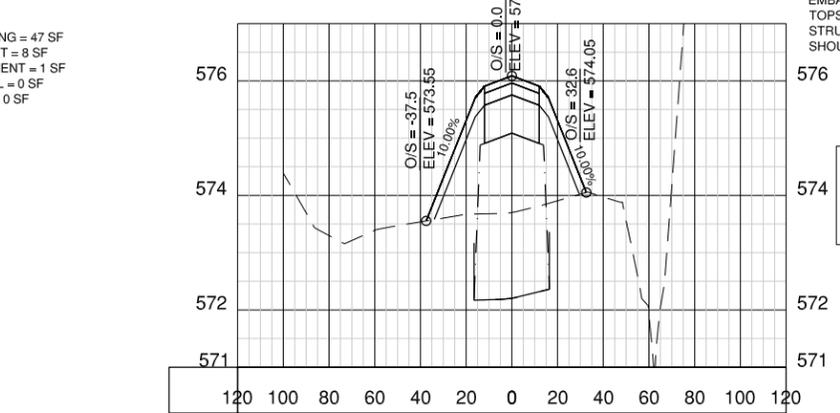
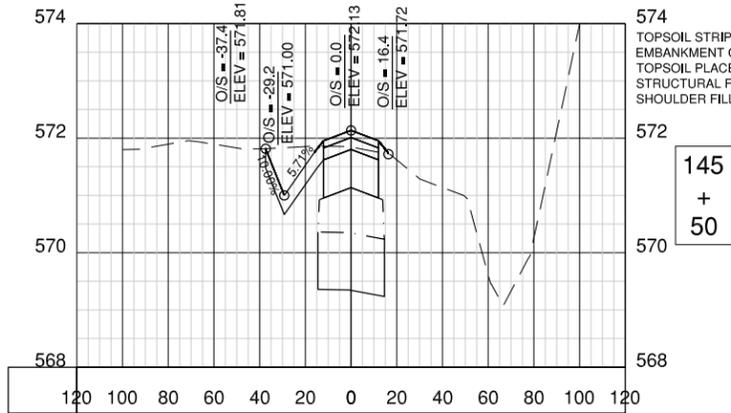
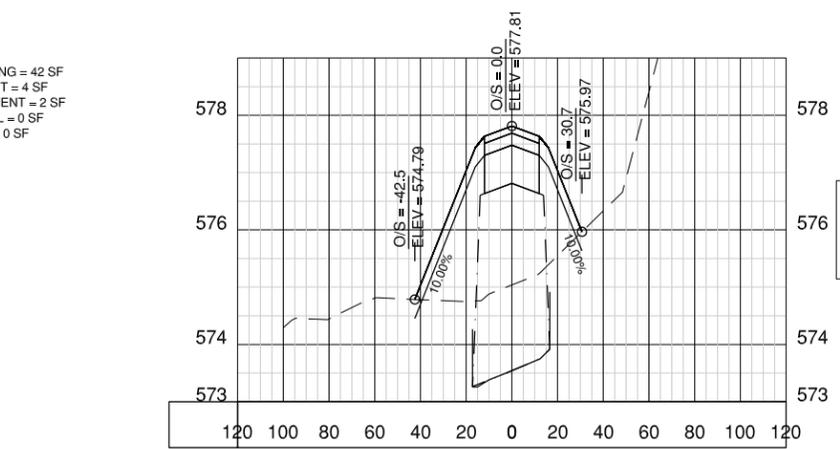
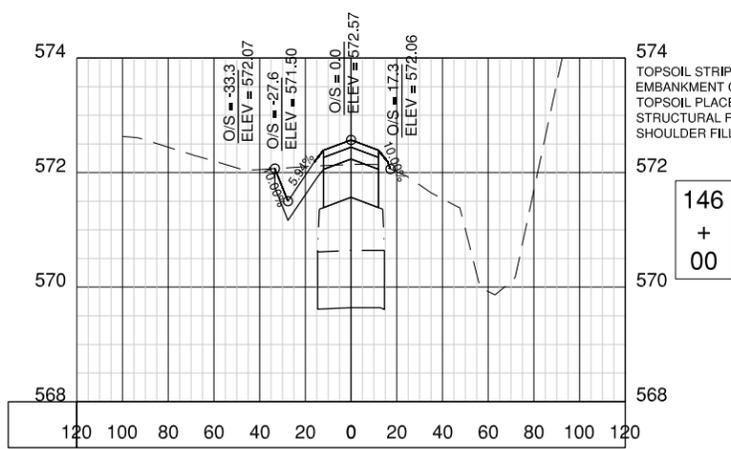
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
CROSS SECTIONS 10**

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DRAWN BY: CMT-MLI
CHECKED BY: CMT-MLI
APPROVED BY: CMT-MLI
DATE: JUNE 3, 2016
JOB No: 15014-06

IL PROJ. NO. MLI-4531
AIP PROJ. NO 3-17-0068-XX

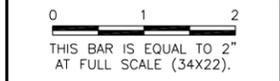
SHEET 29 OF 33 SHEETS



Existing Topo
NW Service Road Base

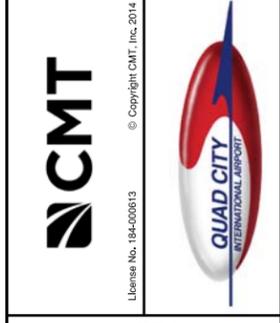
QU022

REVISIONS		
NUMBER	BY	DATE



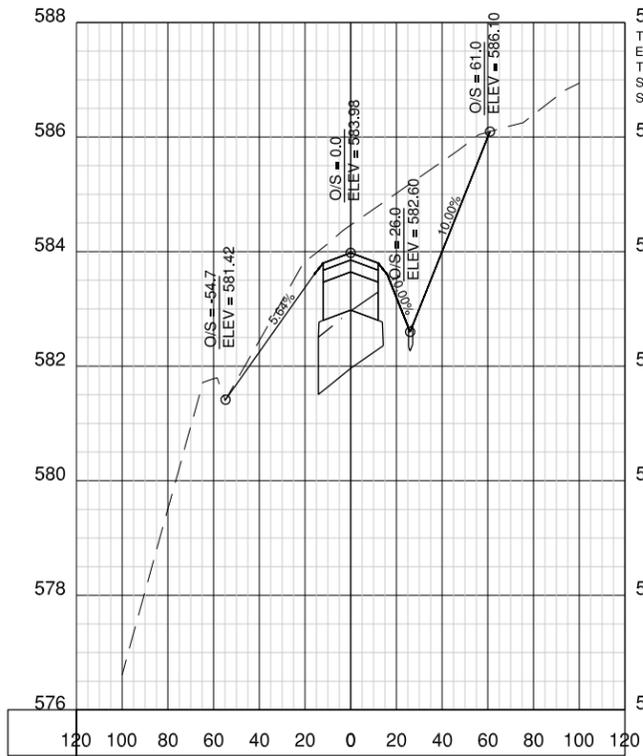
**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
CROSS SECTIONS 11**



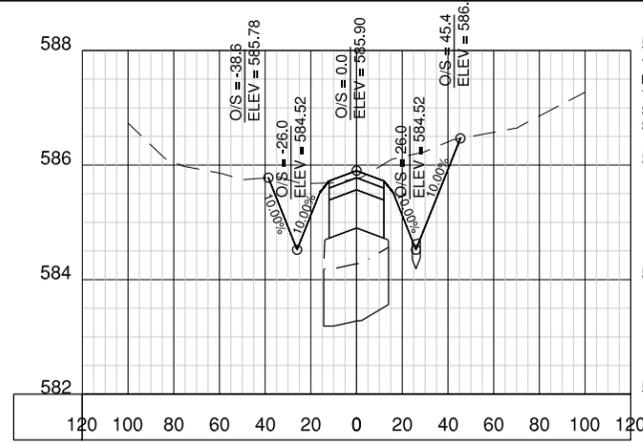
DESIGN BY:	CMT-MLI
DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06

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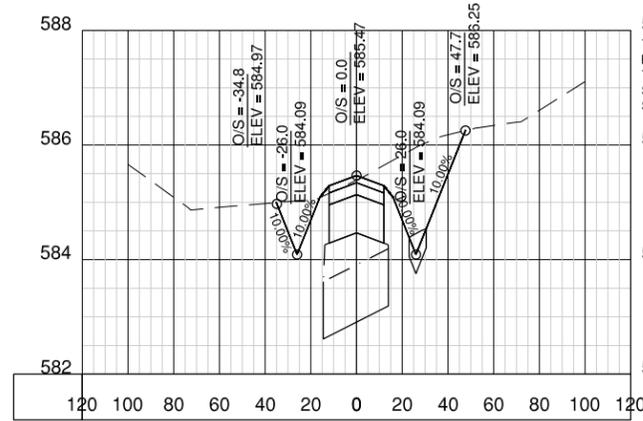
588
TOPSOIL STRIPPING = 105 SF
EMBANKMENT CUT = 16 SF
TOPSOIL PLACEMENT = 0 SF
STRUCTURAL FILL = 2 SF
SHOULDER FILL = 0 SF

150
+
00



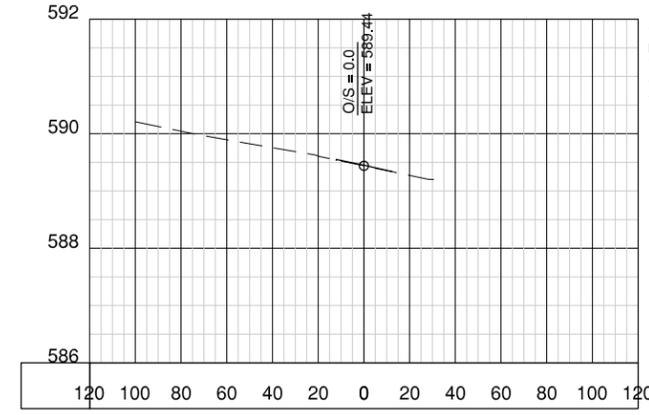
588
TOPSOIL STRIPPING = 81 SF
EMBANKMENT CUT = 1 SF
TOPSOIL PLACEMENT = 1 SF
STRUCTURAL FILL = 14 SF
SHOULDER FILL = 0 SF

152
+
00



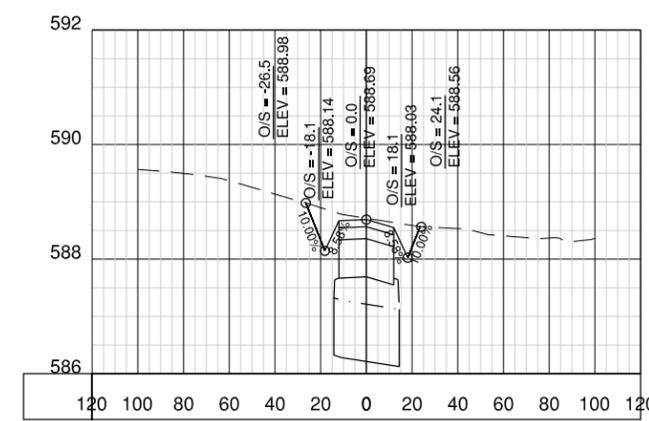
588
TOPSOIL STRIPPING = 80 SF
EMBANKMENT CUT = 1 SF
TOPSOIL PLACEMENT = 1 SF
STRUCTURAL FILL = 13 SF
SHOULDER FILL = 0 SF

151
+
50



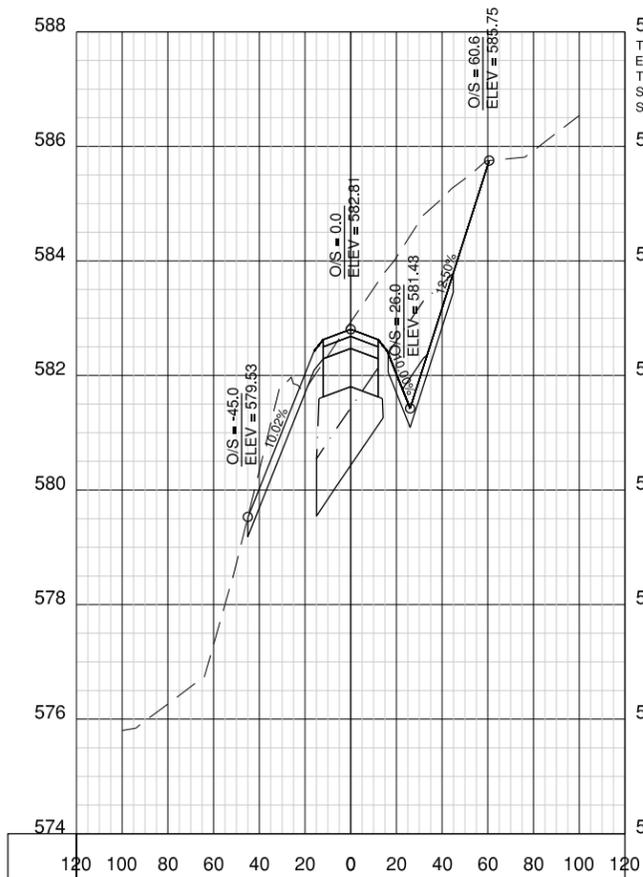
592
TOPSOIL STRIPPING = 0
EMBANKMENT CUT = 0
TOPSOIL PLACEMENT = 0
STRUCTURAL FILL = 0
SHOULDER FILL = 0

153
+
88



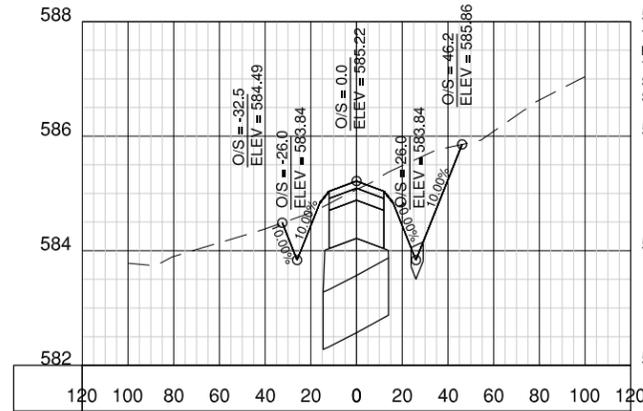
592
TOPSOIL STRIPPING = 46 SF
EMBANKMENT CUT = 0 SF
TOPSOIL PLACEMENT = 0 SF
STRUCTURAL FILL = 11 SF
SHOULDER FILL = 0 SF

153
+
50



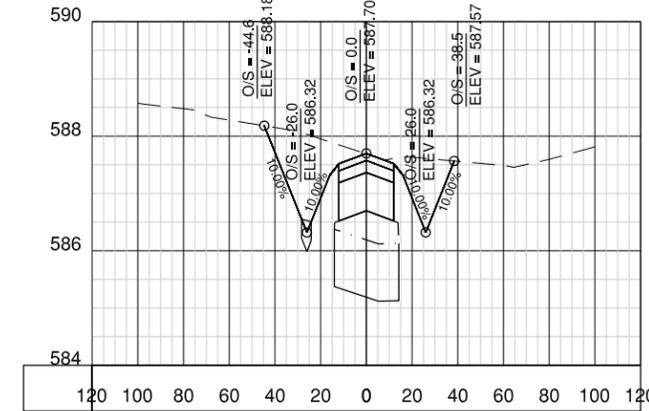
588
TOPSOIL STRIPPING = 108 SF
EMBANKMENT CUT = 24 SF
TOPSOIL PLACEMENT = 3 SF
STRUCTURAL FILL = 10 SF
SHOULDER FILL = 1 SF

149
+
50



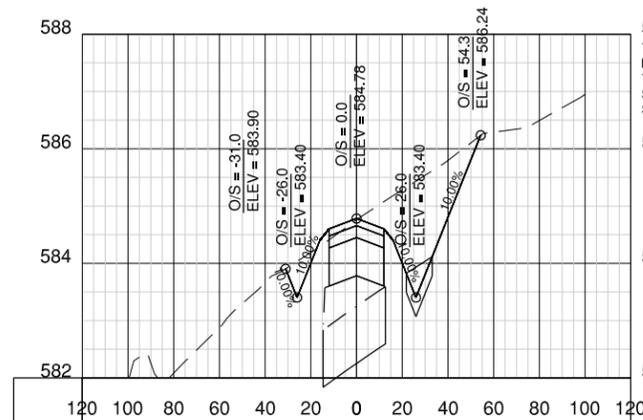
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TOPSOIL STRIPPING = 74 SF
EMBANKMENT CUT = 1 SF
TOPSOIL PLACEMENT = 1 SF
STRUCTURAL FILL = 15 SF
SHOULDER FILL = 0 SF

151
+
00



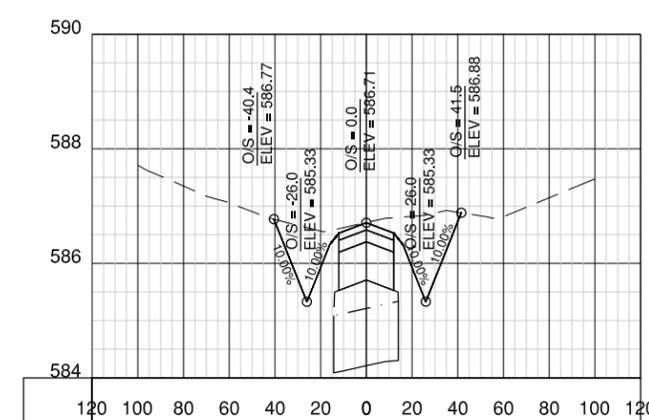
590
TOPSOIL STRIPPING = 83 SF
EMBANKMENT CUT = 1 SF
TOPSOIL PLACEMENT = 0 SF
STRUCTURAL FILL = 11 SF
SHOULDER FILL = 0 SF

153
+
00



588
TOPSOIL STRIPPING = 83 SF
EMBANKMENT CUT = 3 SF
TOPSOIL PLACEMENT = 1 SF
STRUCTURAL FILL = 12 SF
SHOULDER FILL = 0 SF

150
+
50



590
TOPSOIL STRIPPING = 78 SF
EMBANKMENT CUT = 1 SF
TOPSOIL PLACEMENT = 0 SF
STRUCTURAL FILL = 11 SF
SHOULDER FILL = 0 SF

152
+
50

FILE: Design_recover002.dwg
UPDATE BY: Travis Strait
PLOT DATE: 6/13/2016 11:49 AM

Existing Topo
NW Service Road Base

QU022

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

**METROPOLITAN AIRPORT AUTHORITY
OF ROCK ISLAND COUNTY
QUAD CITY INTERNATIONAL AIRPORT
MOLINE, ILLINOIS**

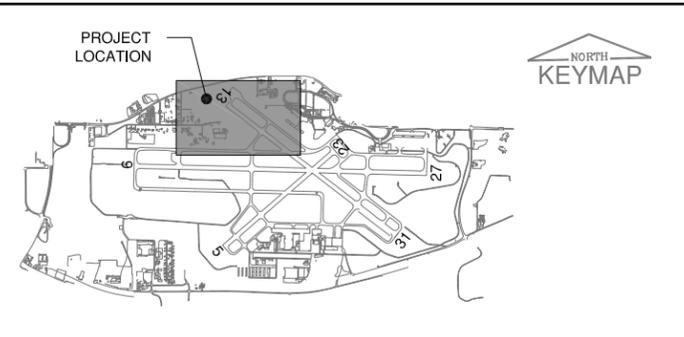
**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
CROSS SECTIONS 12**



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DRAWN BY:	CMT-MLI
CHECKED BY:	CMT-MLI
APPROVED BY:	CMT-MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO. MLI-4531 AIP PROJ. NO 3-17-0068-XX	
SHEET 31 OF 33 SHEETS	



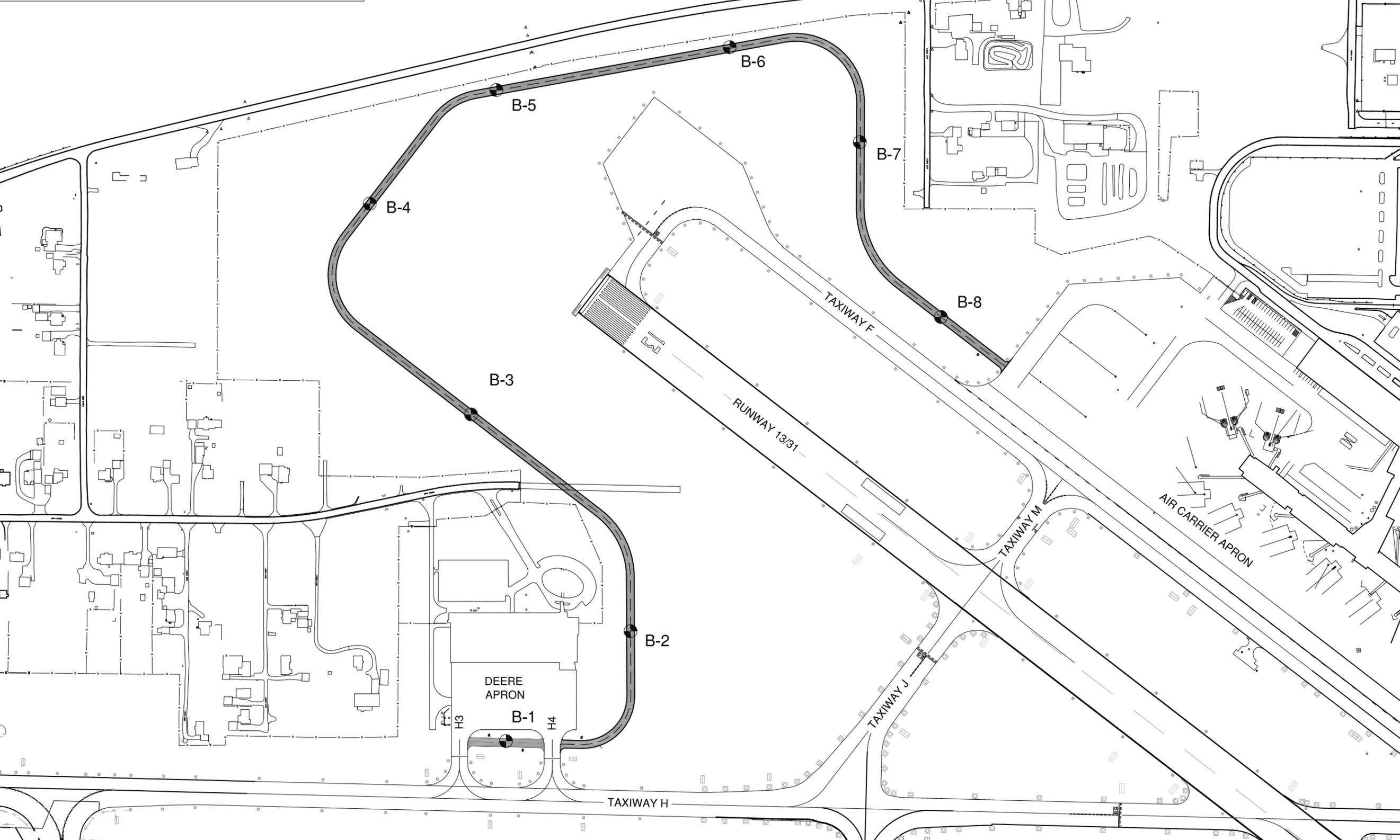
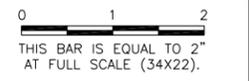
LEGEND
 BORING



FILE: 32 ENG INFO - 1.dwg
 UPDATE BY: Travis Strait
 PLOT DATE: 6/9/2016 4:36 PM
 Service Road Base
 Existing Topo
 Proposed Geometry
 NW Service Road Base

QUO22

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 ENGINEERING INFORMATION 1**

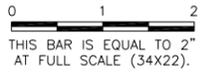


DESIGN BY:	CMT - MLI
DRAWN BY:	CMT - MLI
CHECKED BY:	CMT - MLI
APPROVED BY:	CMT - MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO MLI - 4531 AIP PROJ. NO. 3-17-0068-XX	
SHEET 32 OF 33 SHEETS	

K:\Moline\15014-06_NW_ServiceRoad\Phase_1\Drawn\Sheets\100%_Submittal

QUO22

REVISIONS		
NUMBER	BY	DATE



**METROPOLITAN AIRPORT AUTHORITY
 OF ROCK ISLAND COUNTY
 QUAD CITY INTERNATIONAL AIRPORT
 MOLINE, ILLINOIS**

**CONSTRUCT NORTHWEST SERVICE ROAD - PHASE 1
 ENGINEERING INFORMATION 2**



License No. 184-000613

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CHECKED BY:	CMT - MLI
APPROVED BY:	CMT - MLI
DATE:	JUNE 3, 2016
JOB No:	15014-06
IL PROJ. NO MLI - 4531	
AIP PROJ. NO. 3-17-0068-XX	
SHEET 33 OF 33 SHEETS	

LOG OF BORING NO. 4									
OWNER		ARCHITECT/ENGINEER							
SITE		PROJECT							
Moline, Illinois		NW Service Road							
GRAPHIC LOG	DEPTH (ft.)	SAMPLES				TESTS			
		USCS SYMBOL NUMBER	RECOVERY	SPT-N BLOWS / FT.	MOISTURE, %	DRY DENSITY PCF	UNCONSOLIDATED STRENGTH PSF		
DESCRIPTION		Approx. Surface Elev.: 567.7 ft.							
TOPSOIL - Sandy lean CLAY, trace organic matter, very dark brown, stiff		CL 1	ST	15"	25.6	89	3500*		
2.5 GLACIAL TILL - Sandy lean CLAY, trace gravel, light olive brown and gray, stiff		CL 2	SS	16"	2	26.6			
6.0 PALEOSOL - Lean to fat CLAY, trace sand and sand seams, light olive brown and gray, very stiff		CL 3	SS	17"	5	22.5	6000*		
8.5 GLACIAL TILL - Sandy lean CLAY, trace gravel, dark gray, hard		CL 4	SS	16"	36	12.6			
Bottom of Boring		576.7		10					

LOG OF BORING NO. 3									
OWNER		ARCHITECT/ENGINEER							
SITE		PROJECT							
Moline, Illinois		NW Service Road							
GRAPHIC LOG	DEPTH (ft.)	SAMPLES				TESTS			
		USCS SYMBOL NUMBER	RECOVERY	SPT-N BLOWS / FT.	MOISTURE, %	DRY DENSITY PCF	UNCONSOLIDATED STRENGTH PSF		
DESCRIPTION		Approx. Surface Elev.: 583.6 ft.							
TOPSOIL - Clayey SAND, trace organic matter, very dark brown, hard		SC 1	ST	13"	13.3	115	9000*		
2.0 EOLIAN SAND - Silty fine SAND, yellowish brown								581.6	
4.0 GLACIAL TILL - Sandy lean CLAY, light olive brown and gray, stiff		CL 2	SS	16"	7	19.7		579.6	
6.0 GLACIAL OUTWASH - Fine SAND, yellowish brown, loose		SP 3	SS	16"	9			577.6	
8.0 GLACIAL OUTWASH - Fine to coarse SAND, trace gravel, yellowish brown, loose		SP 4	SS	17"	9			575.6	
Bottom of Boring		573.6		10					

LOG OF BORING NO. 2									
OWNER		ARCHITECT/ENGINEER							
SITE		PROJECT							
Moline, Illinois		NW Service Road							
GRAPHIC LOG	DEPTH (ft.)	SAMPLES				TESTS			
		USCS SYMBOL NUMBER	RECOVERY	SPT-N BLOWS / FT.	MOISTURE, %	DRY DENSITY PCF	UNCONSOLIDATED STRENGTH PSF		
DESCRIPTION		Approx. Surface Elev.: 587.5 ft.							
EOLIAN SAND - Fine to coarse SAND, yellowish brown		SP 1	ST	20"					
- becomes loose after 3'									
- fine to medium gradation after 6'									
- fine to coarse gradation after 8'									
Bottom of Boring		577.5		10					

LOG OF BORING NO. 1									
OWNER		ARCHITECT/ENGINEER							
SITE		PROJECT							
Moline, Illinois		NW Service Road							
GRAPHIC LOG	DEPTH (ft.)	SAMPLES				TESTS			
		USCS SYMBOL NUMBER	RECOVERY	SPT-N BLOWS / FT.	MOISTURE, %	DRY DENSITY PCF	UNCONSOLIDATED STRENGTH PSF		
DESCRIPTION		Approx. Surface Elev.: 586.7 ft.							
EOLIAN SAND - Fine to coarse SAND, with silt, yellowish brown		SP 1	ST	21"	5.0				
- fine to medium gradation, becomes loose after 3'									
Bottom of Boring		576.7		10					

LOG OF BORING NO. 8									
OWNER		ARCHITECT/ENGINEER							
SITE		PROJECT							
Moline, Illinois		NW Service Road							
GRAPHIC LOG	DEPTH (ft.)	SAMPLES				TESTS			
		USCS SYMBOL NUMBER	RECOVERY	SPT-N BLOWS / FT.	MOISTURE, %	DRY DENSITY PCF	UNCONSOLIDATED STRENGTH PSF		
DESCRIPTION		Approx. Surface Elev.: 585.0 ft.							
FILL - Sandy lean CLAY, trace gravel, very dark brown and light olive brown		CL 1	ST	18"	13.2	125		563.7	
1.0 GLACIAL TILL - Sandy lean CLAY, trace gravel, light olive brown and gray, hard								584.0	
8.0 GLACIAL TILL - Sandy lean CLAY, trace gravel, light olive brown and gray, hard		CL 2	SS	18"	29	7.2	9000*	561.2	
6.0 GLACIAL TILL - Sandy lean CLAY, trace gravel, dark gray and olive brown, hard		CL 3	SS	18"	36	10.0		558.2	
8.0 GLACIAL OUTWASH - Fine SAND, with silt, dark gray, medium dense		SP 4	SS	18"	24	14.2		577.0	
Bottom of Boring		575.0		10					

LOG OF BORING NO. 7									
OWNER		ARCHITECT/ENGINEER							
SITE		PROJECT							
Moline, Illinois		NW Service Road							
GRAPHIC LOG	DEPTH (ft.)	SAMPLES				TESTS			
		USCS SYMBOL NUMBER	RECOVERY	SPT-N BLOWS / FT.	MOISTURE, %	DRY DENSITY PCF	UNCONSOLIDATED STRENGTH PSF		
DESCRIPTION		Approx. Surface Elev.: 571.2 ft.							
TOPSOIL - Sandy lean CLAY, trace organic matter, very dark brown, stiff		CL 1	ST	15"	35.3	78	2500*		
1.5 RESIDUAL LIMESTONE - Sandy lean CLAY, trace gravel, light olive brown								569.7	
4.0 Auger Refusal		CL 2	AS	6"				567.2	

LOG OF BORING NO. 6									
OWNER		ARCHITECT/ENGINEER							
SITE		PROJECT							
Moline, Illinois		NW Service Road							
GRAPHIC LOG	DEPTH (ft.)	SAMPLES				TESTS			
		USCS SYMBOL NUMBER	RECOVERY	SPT-N BLOWS / FT.	MOISTURE, %	DRY DENSITY PCF	UNCONSOLIDATED STRENGTH PSF		
DESCRIPTION		Approx. Surface Elev.: 566.0 ft.							
TOPSOIL - Sandy lean CLAY, trace organic matter, very dark brown, stiff		CL 1	ST	22"	26.8	90	2500*		
3.0 RESIDUAL SANDSTONE - Fine SAND, light olive brown and gray, loose								563.0	
- medium dense after 6'									
7.5 BEDROCK - SHALE, gray, soft								558.5	
Bottom of Boring		556.0		10					

LOG OF BORING NO. 5									
OWNER		ARCHITECT/ENGINEER							
SITE		PROJECT							
Moline, Illinois		NW Service Road							
GRAPHIC LOG	DEPTH (ft.)	SAMPLES				TESTS			
		USCS SYMBOL NUMBER	RECOVERY	SPT-N BLOWS / FT.	MOISTURE, %	DRY DENSITY PCF	UNCONSOLIDATED STRENGTH PSF		
DESCRIPTION		Approx. Surface Elev.: 564.2 ft.							
FILL - Sandy lean CLAY, trace gravel and organic matter, light olive brown, gray, and very dark brown		CL 1	ST	13"	26.4	95		563.7	
TOPSOIL - Sandy lean CLAY, trace organic matter, very dark brown, very stiff								29.7 88 5000*	
3.0 PALEOSOL - Lean to fat CLAY, trace sand, light olive brown and gray, stiff		CL 2	SS	16"	10	24.8	3500*	561.2	
6.0 GLACIAL TILL - Sandy lean CLAY, trace gravel, dark gray and olive brown, hard		CL 3	SS	18"	36	16.4		558.2	
Bottom of Boring		554.2		10					

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