

# GREATER ROCKFORD AIRPORT AUTHORITY ROCKFORD, ILLINOIS

## CONSTRUCTION PLANS FOR CHICAGO ROCKFORD INTERNATIONAL AIRPORT



Greater Rockford Airport Authority

APPROVED BY *Jeffrey Polsean*  
JEFFREY POLSEAN  
ECONOMIC DEVELOPMENT MANAGER

DATE                      JUNE 3, 2016

### INDEX TO SHEETS

1. COVER SHEET
2. SUMMARY OF QUANTITIES
3. SITE PLAN AND PROJECT CONTROL
4. SEQUENCE OF CONSTRUCTION PHASES 1 AND 2
5. PHASING PLAN - NOTES AND DETAILS - 1
6. PHASING PLAN - NOTES AND DETAILS - 2
7. TYPICAL SECTION
8. EXISTING CONDITIONS / PROPOSED REMOVALS
9. EXISTING CONDITIONS / PROPOSED REMOVALS - ADDITIVE ALTERNATE
10. PROPOSED PAVEMENT / JOINTING PLAN
11. PROPOSED PAVEMENT / JOINTING PLAN - ADDITIVE ALTERNATE
12. JOINTING DETAILS
13. GRADING PLAN
14. DRAINAGE PLAN
15. DRAINAGE SCHEDULES AND DETAILS
16. TRENCH DRAIN DETAILS
17. PAVEMENT MARKING PLAN
18. PAVEMENT MARKING PLAN - ADDITIVE ALTERNATE
19. PAVEMENT MARKING DETAILS
20. ENGINEERING INFORMATION

**REHABILITATE THE AIR CARGO APRON**  
  
**ILLINOIS PROJECT: RFD-4549**  
**A.I.P.PROJECT: 3-17-0088-XX**

JUNE 03, 2016

### DESIGN INFORMATION

TOWNSHIP: 43 NORTH                      COUNTY: WINNEBAGO  
RANGE: 1 EAST                              ROCKFORD TOWNSHIP

**MAXIMUM EQUIPMENT HEIGHT = 25'**



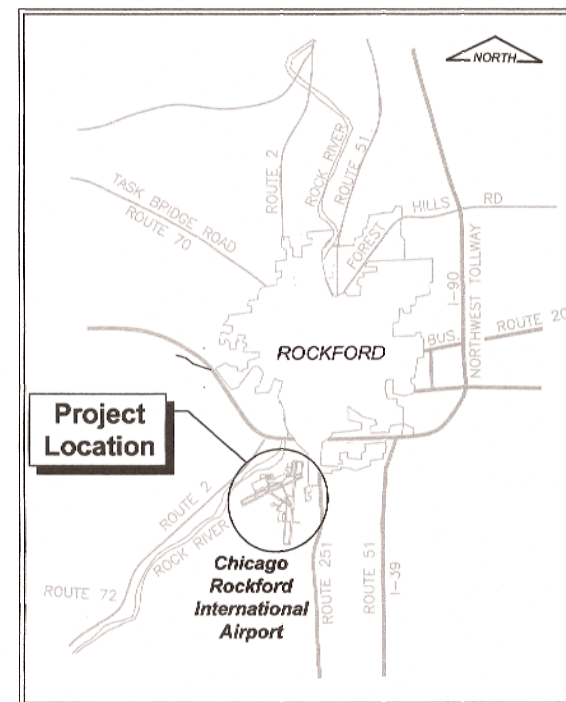
Illinois Professional Engineering Practice Act of 1989

**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

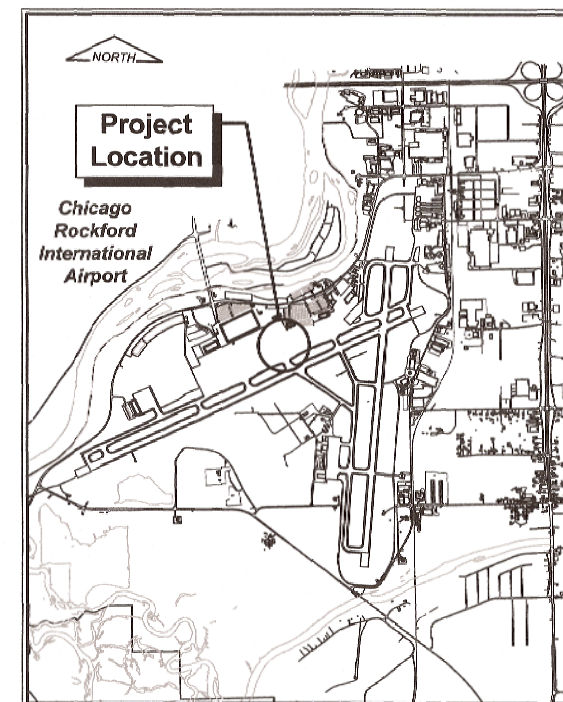


SUBMITTED BY *Daniel L. Pape*  
**Daniel L. Pape, P.E.**

DATE 6/3/16  
license expires 11-30-2017



LOCATION MAP



SITE PLAN



MICHAEL P. DUNN  
DIRECTOR

© Copyright CMT, Inc.



CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: R0026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN

BY:


REV | DATE | DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

SHEET TITLE:

**SUMMARY OF QUANTITIES**

DESIGNED: DRAWN: CHECKED:  
**ABM JRO DLP**

PROJECT NO.: 15258-04  
DATE: 06/03/2016

SHEET NO. 2 OF 20 REVISION

**SUMMARY OF QUANTITIES**

**BASE BID - REHABILITATE THE AIR CARGO APRON**

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150520	MOBILIZATION	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	120	
AR208515	POROUS GRANULAR EMBANKMENT	CY	120	
AR209650	AGGREGATE BASE PREPARATION	SY	2,350	
AR501518	18" PCC PAVEMENT	SY	2,350	
AR501900	REMOVE PCC PAVEMENT	SY	2,280	
AR605510	JOINT SEALING FILLER	LF	4,875	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	285	
AR620525	PAVEMENT MARKING - BLACK BORDER	SF	160	
AR701512	12" RCP, CLASS IV	LF	320	
AR705920	REPLACE UNDERDRAIN	LF	800	
AR751001	TRENCH DRAIN	LF	420	
AR751417	TYPE 2 INLET	EACH	2	
AR751900	REMOVE INLET	EACH	2	
AR800845	REMOVE TRENCH DRAIN	LF	780	

**ADDITIVE ALTERNATE NO. 1 - PAVEMENT REPAIRS**

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AS209650	AGGREGATE BASE PREPARATION	SY	205	
AS401610	BITUMINOUS SURFACE COURSE	TON	20	
AS403610	BITUMINOUS BASE COURSE	TON	80	
AS501518	18" PCC PAVEMENT	SY	115	
AS501604	4" PCC SIDEWALK	SF	34	
AS501690	PCC SIDEWALK REMOVAL	SF	34	
AS501900	REMOVE PCC PAVEMENT	SY	205	
AS602510	BITUMINOUS PRIME COAT	GAL	50	
AS603510	BITUMINOUS TACK COAT	GAL	50	
AS605510	JOINT SEALING FILLER	LF	270	
AS620520	PAVEMENT MARKING - WATERBORNE	SF	115	
AS620525	PAVEMENT MARKING - BLACK BORDER	SF	134	



MICHAEL P. DUNN  
DIRECTOR

© Copyright CMT, Inc.



CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

SHEET TITLE:  
**SITE PLAN  
AND PROJECT CONTROL**

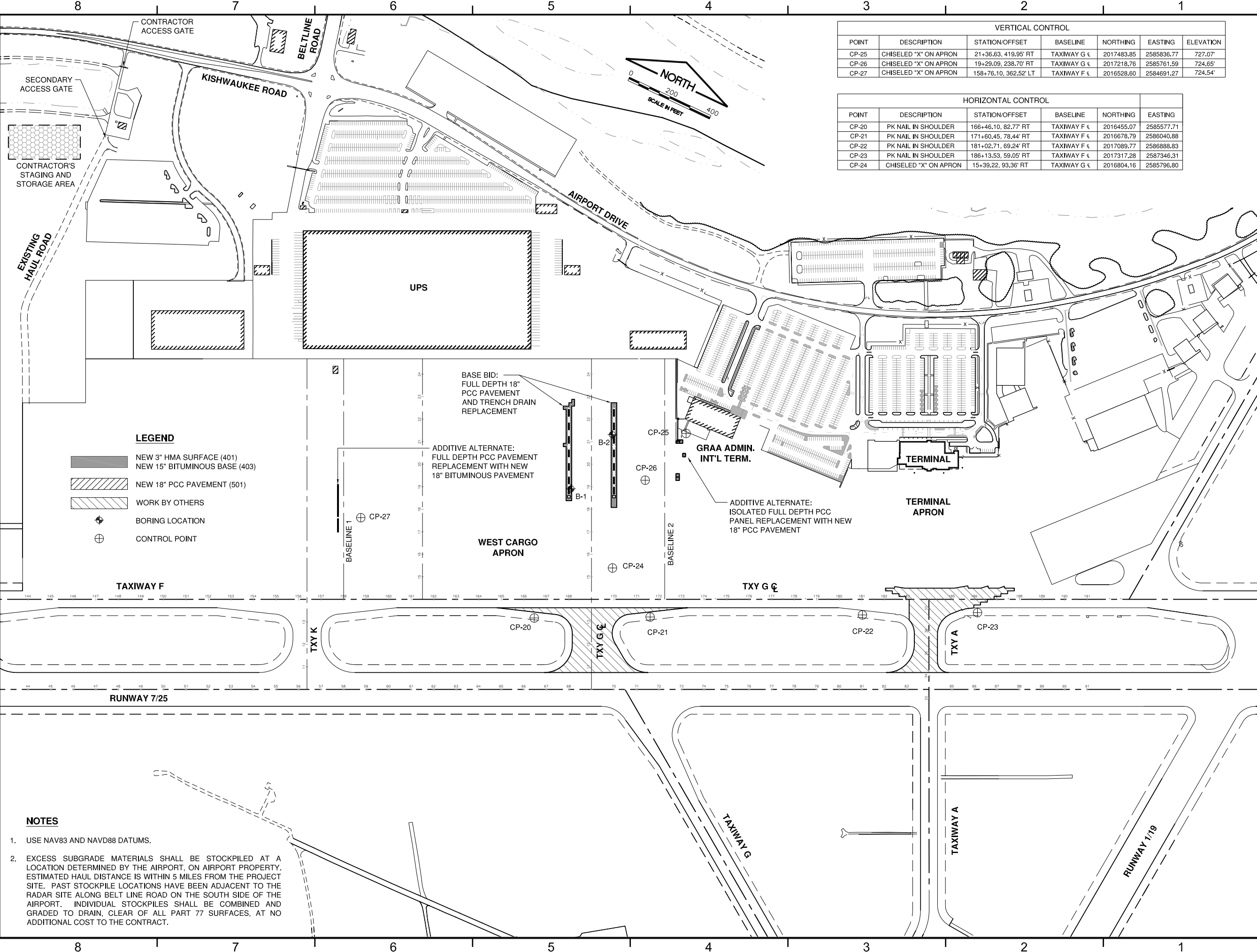
DESIGNED: **ABM**      DRAWN: **JRO**      CHECKED: **DLP**

PROJECT NO.: 15258-04  
DATE: 06/03/2016

SHEET NO. **3 OF 20**      REVISION

VERTICAL CONTROL						
POINT	DESCRIPTION	STATION/OFFSET	BASELINE	NORTHING	EASTING	ELEVATION
CP-25	CHISELED "X" ON APRON	21+36.63, 419.95' RT	TAXIWAY G €	2017483.85	2585836.77	727.07'
CP-26	CHISELED "X" ON APRON	19+29.09, 238.70' RT	TAXIWAY G €	2017218.76	2585761.59	724.65'
CP-27	CHISELED "X" ON APRON	158+76.10, 362.52' LT	TAXIWAY F €	2016528.60	2584691.27	724.54'

HORIZONTAL CONTROL					
POINT	DESCRIPTION	STATION/OFFSET	BASELINE	NORTHING	EASTING
CP-20	PK NAIL IN SHOULDER	166+46.10, 82.77' RT	TAXIWAY F €	2016455.07	2585577.71
CP-21	PK NAIL IN SHOULDER	171+60.45, 78.44' RT	TAXIWAY F €	2016678.79	2586040.88
CP-22	PK NAIL IN SHOULDER	181+02.71, 69.24' RT	TAXIWAY F €	2017089.77	2586888.83
CP-23	PK NAIL IN SHOULDER	186+13.53, 59.05' RT	TAXIWAY F €	2017317.28	2587346.31
CP-24	CHISELED "X" ON APRON	15+39.22, 93.36' RT	TAXIWAY G €	2016804.16	2585796.80



**LEGEND**

- NEW 3" HMA SURFACE (401)
- NEW 15" BITUMINOUS BASE (403)
- NEW 18" PCC PAVEMENT (501)
- WORK BY OTHERS
- BORING LOCATION
- CONTROL POINT

BASE BID:  
FULL DEPTH 18"  
PCC PAVEMENT  
AND TRENCH DRAIN  
REPLACEMENT

ADDITIVE ALTERNATE:  
FULL DEPTH PCC PAVEMENT  
REPLACEMENT WITH NEW  
18" BITUMINOUS PAVEMENT

ADDITIVE ALTERNATE:  
ISOLATED FULL DEPTH PCC  
PANEL REPLACEMENT WITH NEW  
18" PCC PAVEMENT

**NOTES**

1. USE NAV83 AND NAVD88 DATUMS.
2. EXCESS SUBGRADE MATERIALS SHALL BE STOCKPILED AT A LOCATION DETERMINED BY THE AIRPORT, ON AIRPORT PROPERTY. ESTIMATED HAUL DISTANCE IS WITHIN 5 MILES FROM THE PROJECT SITE. PAST STOCKPILE LOCATIONS HAVE BEEN ADJACENT TO THE RADAR SITE ALONG BELT LINE ROAD ON THE SOUTH SIDE OF THE AIRPORT. INDIVIDUAL STOCKPILES SHALL BE COMBINED AND GRADED TO DRAIN, CLEAR OF ALL PART 77 SURFACES, AT NO ADDITIONAL COST TO THE CONTRACT.





MICHAEL P. DUNN  
DIRECTOR



CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

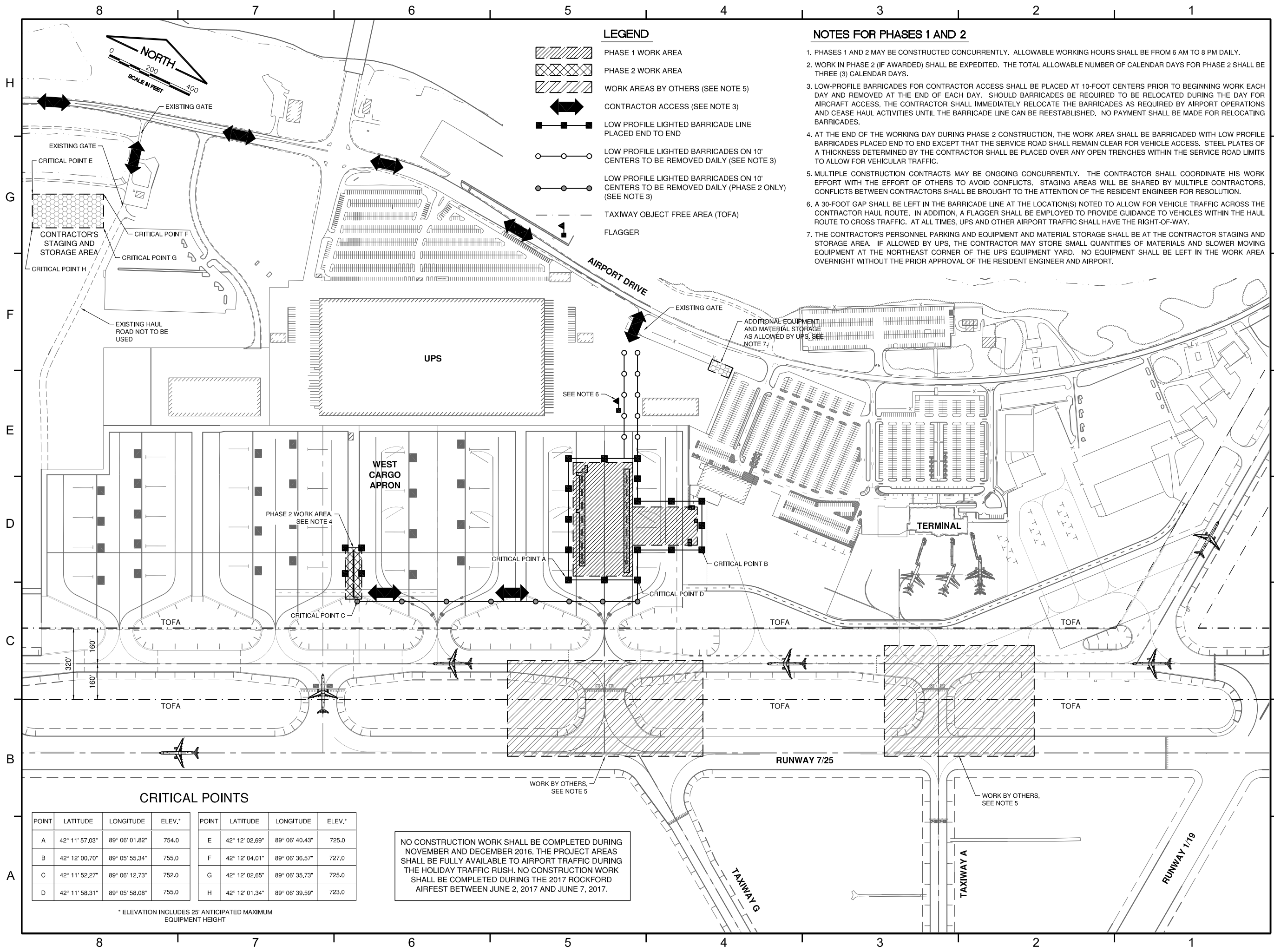
PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

SHEET TITLE:

**SEQUENCE OF CONSTRUCTION  
PHASES 1 AND 2**

DESIGNED:	DRAWN:	CHECKED:
SMS	JRO	DLP
PROJECT NO.:	15258-04	
DATE:	06/03/2016	
SHEET NO.	4 OF 20	REVISION



**LEGEND**

- PHASE 1 WORK AREA
- PHASE 2 WORK AREA
- WORK AREAS BY OTHERS (SEE NOTE 5)
- CONTRACTOR ACCESS (SEE NOTE 3)
- LOW PROFILE LIGHTED BARRICADE LINE PLACED END TO END
- LOW PROFILE LIGHTED BARRICADES ON 10' CENTERS TO BE REMOVED DAILY (SEE NOTE 3)
- LOW PROFILE LIGHTED BARRICADES ON 10' CENTERS TO BE REMOVED DAILY (PHASE 2 ONLY) (SEE NOTE 3)
- TAXIWAY OBJECT FREE AREA (TOFA)
- FLAGGER

**NOTES FOR PHASES 1 AND 2**

1. PHASES 1 AND 2 MAY BE CONSTRUCTED CONCURRENTLY. ALLOWABLE WORKING HOURS SHALL BE FROM 6 AM TO 8 PM DAILY.
2. WORK IN PHASE 2 (IF AWARDED) SHALL BE EXPEDITED. THE TOTAL ALLOWABLE NUMBER OF CALENDAR DAYS FOR PHASE 2 SHALL BE THREE (3) CALENDAR DAYS.
3. LOW-PROFILE BARRICADES FOR CONTRACTOR ACCESS SHALL BE PLACED AT 10-FOOT CENTERS PRIOR TO BEGINNING WORK EACH DAY AND REMOVED AT THE END OF EACH DAY. SHOULD BARRICADES BE REQUIRED TO BE RELOCATED DURING THE DAY FOR AIRCRAFT ACCESS, THE CONTRACTOR SHALL IMMEDIATELY RELOCATE THE BARRICADES AS REQUIRED BY AIRPORT OPERATIONS AND CEASE HAUL ACTIVITIES UNTIL THE BARRICADE LINE CAN BE REESTABLISHED. NO PAYMENT SHALL BE MADE FOR RELOCATING BARRICADES.
4. AT THE END OF THE WORKING DAY DURING PHASE 2 CONSTRUCTION, THE WORK AREA SHALL BE BARRICADED WITH LOW PROFILE BARRICADES PLACED END TO END EXCEPT THAT THE SERVICE ROAD SHALL REMAIN CLEAR FOR VEHICLE ACCESS. STEEL PLATES OF A THICKNESS DETERMINED BY THE CONTRACTOR SHALL BE PLACED OVER ANY OPEN TRENCHES WITHIN THE SERVICE ROAD LIMITS TO ALLOW FOR VEHICULAR TRAFFIC.
5. MULTIPLE CONSTRUCTION CONTRACTS MAY BE ONGOING CONCURRENTLY. THE CONTRACTOR SHALL COORDINATE HIS WORK EFFORT WITH THE EFFORT OF OTHERS TO AVOID CONFLICTS. STAGING AREAS WILL BE SHARED BY MULTIPLE CONTRACTORS. CONFLICTS BETWEEN CONTRACTORS SHALL BE BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER FOR RESOLUTION.
6. A 30-FOOT GAP SHALL BE LEFT IN THE BARRICADE LINE AT THE LOCATION(S) NOTED TO ALLOW FOR VEHICLE TRAFFIC ACROSS THE CONTRACTOR HAUL ROUTE. IN ADDITION, A FLAGGER SHALL BE EMPLOYED TO PROVIDE GUIDANCE TO VEHICLES WITHIN THE HAUL ROUTE TO CROSS TRAFFIC. AT ALL TIMES, UPS AND OTHER AIRPORT TRAFFIC SHALL HAVE THE RIGHT-OF-WAY.
7. THE CONTRACTOR'S PERSONNEL PARKING AND EQUIPMENT AND MATERIAL STORAGE SHALL BE AT THE CONTRACTOR STAGING AND STORAGE AREA. IF ALLOWED BY UPS, THE CONTRACTOR MAY STORE SMALL QUANTITIES OF MATERIALS AND SLOWER MOVING EQUIPMENT AT THE NORTHEAST CORNER OF THE UPS EQUIPMENT YARD. NO EQUIPMENT SHALL BE LEFT IN THE WORK AREA OVERNIGHT WITHOUT THE PRIOR APPROVAL OF THE RESIDENT ENGINEER AND AIRPORT.

**CRITICAL POINTS**

POINT	LATITUDE	LONGITUDE	ELEV.*	POINT	LATITUDE	LONGITUDE	ELEV.*
A	42° 11' 57.03"	89° 06' 01.82"	754.0	E	42° 12' 02.69"	89° 06' 40.43"	725.0
B	42° 12' 00.70"	89° 05' 55.34"	755.0	F	42° 12' 04.01"	89° 06' 36.57"	727.0
C	42° 11' 52.27"	89° 06' 12.73"	752.0	G	42° 12' 02.65"	89° 06' 35.73"	725.0
D	42° 11' 58.31"	89° 05' 58.08"	755.0	H	42° 12' 01.34"	89° 06' 39.59"	723.0

\* ELEVATION INCLUDES 25' ANTICIPATED MAXIMUM EQUIPMENT HEIGHT

NO CONSTRUCTION WORK SHALL BE COMPLETED DURING NOVEMBER AND DECEMBER 2016. THE PROJECT AREAS SHALL BE FULLY AVAILABLE TO AIRPORT TRAFFIC DURING THE HOLIDAY TRAFFIC RUSH. NO CONSTRUCTION WORK SHALL BE COMPLETED DURING THE 2017 ROCKFORD AIRFEST BETWEEN JUNE 2, 2017 AND JUNE 7, 2017.



**PHASING PLAN PROJECT NOTES:**

**PHASING PLAN GENERAL NOTES:**

**1. APPROVED PROGRESS SCHEDULE:**

PRIOR TO THE START OF CONSTRUCTION, AN APPROVED PROGRESS SCHEDULE SHALL BE EXECUTED BY THE CONTRACTOR AND MONITORED BY THE RESIDENT ENGINEER. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES, INCLUDING ALL AOA WORK REQUIRING DAYTIME ONLY CLOSURES OF TAXIWAYS AND RUNWAYS. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES AT LEAST 3 WORKING DAYS PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMMODATE AIRPORT EVENTS (I.E. AIRSHOW). SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.

**2. NOTAM (NOTICE TO AIRMEN) COORDINATION:**

THE RESIDENT ENGINEER SHALL COORDINATE NOTAM AND FAA FACILITY COORDINATION WITH AIRPORT / FAA PERSONNEL.

**3. CONSTRUCTION SITE ACCESS AND STAGING AREA:**

THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED. THIS WORK AREA SHALL BE RESTRICTED FROM ACTIVE AOA AREAS WITH THE BARRICADE/FENCING PERIMETERS SHOWN.

THE ENTRANCES SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO THE CITY OF ROCKFORD AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.

**4. CONSTRUCTION EQUIPMENT FLAGGING / BEACON REQUIREMENTS:**

THE CONTRACTOR WILL BE REQUIRED TO MOUNT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION, EXCEPT HAUL VEHICLES.

**5. GATE SECURITY:**

THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE DEPUTY DIRECTOR OF OPERATIONS. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD EMPLOYED BY THE CONTRACTOR SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY. AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

**6. UNAUTHORIZED ACCESS TO AIRFIELD:**

THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AN AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS, MAY BE FINED \$10,000.00 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND DEDUCTED FROM ANY MONIES DUE OR THAT MAY BECOME DUE TO THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

**7. VEHICLE SIGNAGE**

THE CONTRACTOR'S VEHICLES SHALL HAVE MAGNETIC SIGNS INDICATING THE COMPANY NAME AND VEHICLE NUMBER FOR LONG DISTANCE IDENTIFICATION BY ATCT PERSONNEL. SIGNAGE AND NUMBERING SHALL BE COORDINATED PRIOR TO CONSTRUCTION WITH AIRPORT PERSONNEL. ALL COSTS ASSOCIATED WITH SIGNING OF VEHICLES SHALL BE INCIDENTAL TO THE CONTRACT.

**1. SUGGESTED SEQUENCE OF CONSTRUCTION:**

THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS.

**2. STAGING AREA RESTORATION:**

ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

**3. HAUL ROUTE / HAUL ROUTE RESTORATION:**

THE CONTRACTOR SHALL USE THE HAUL ROUTE(S) AS SHOWN IN THESE PLANS. THE COST OF MAINTAINING THE HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO CONTRACT.

**4. AIRPORT APPROVAL OF PHASING:**

THE RESIDENT ENGINEER AND AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT OR BARRICADES AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AND AT NO EXTRA COST TO THE OWNER.

**5. AIRFIELD PAVEMENT / SITE DEBRIS REMOVAL:**

THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

**6. PROJECT LIGHTING OUTSIDE OF DAYLIGHT HOURS:**

WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.

**7. EXISTING UTILITY COORDINATION:**

COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.

**8. TRAFFIC CONTROL PAYMENT:**

PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES USED ON THE AIRFIELD MUST BE INTERLOCKING LOW PROFILE BARRICADES. INTERLOCKING LOW PROFILE BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE RESIDENT ENGINEER. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL NON-AIRFIELD LOCATIONS REQUIRING BARRICADES SHALL BE TYPE II BARRICADES AND SHALL CONFORM TO IDOT STANDARD 702001. FOR AIRSIDE BARRICADE PLACEMENT, SEE PHASING PLAN SUMMARY SHEETS.

(NOTES CONTINUED ON PHASING PLAN - NOTES AND DETAILS - 2)



MICHAEL P. DUNN  
DIRECTOR



CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

SHEET TITLE:

**PHASING PLAN -  
NOTES AND DETAILS - 1**

DESIGNED:	DRAWN:	CHECKED:
SMS	JRO	DLP

PROJECT NO.:	15258-04
DATE:	06/03/2016

SHEET NO.	5 OF 20	REVISION
-----------	---------	----------

**PHASING PLAN GENERAL NOTES (CONT'D):**

(NOTES CONTINUED FROM PHASING PLAN - NOTES AND DETAILS - 1)

**9. DRIVERS TRAINING AND BADGING:**

DRIVER'S TRAINING AND BADGING SHALL BE REQUIRED FOR THE CONTRACTOR'S SUPERVISORY PERSONNEL, OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. THE DRIVER'S TRAINING AND BADGING OF THE INITIAL SUPERVISORY PERSONNEL MUST BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.

**10. DUST CONTROL REQUIREMENTS:**

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

**11. OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION (AC 150/5370-2F):**

ALL WORK SHALL CONFORM TO AC 150/5370-2F "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION". THIS AC IS AVAILABLE AT [www.faa.gov/arp/pdf/5370-2f.pdf](http://www.faa.gov/arp/pdf/5370-2f.pdf).

**12. STAGING AREA:**

THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

**13. AIRFIELD LIGHTING COORDINATION:**

THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DEPUTY DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.

**14. WEEKLY COORDINATION MEETINGS:**

WEEKLY COORDINATION MEETINGS SHALL BE REQUIRED TO DISCUSS PROJECT PROGRESS. AS A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY.

**15. TEMPORARY CABLES:**

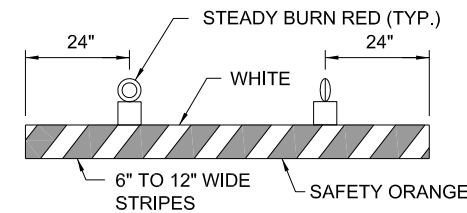
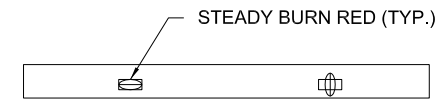
ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

**16. SAFETY PLAN COMPLIANCE DOCUMENT:**

THE CONTRACTOR SHALL PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD). THE SPCD SHALL DETAIL HOW THE CONTRACTOR WILL COMPLY WITH THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). DURING THE DEVELOPMENT OF THE CSPP, IT IS NOT POSSIBLE TO DETERMINE ALL SAFETY PLAN DETAILS, SUCH AS SPECIFIC EQUIPMENT HAZARDS AND LIGHTING, CONTRACTOR'S POINTS OF CONTACT, CONSTRUCTION EQUIPMENT HEIGHT, ETC. THE SUCCESSFUL CONTRACTOR MUST DEFINE SUCH DETAILS BY PREPARING AN SPCD THAT IS REVIEWED BY THE AIRPORT OPERATOR FOR APPROVAL PRIOR TO ISSUANCE OF THE NOTICE TO PROCEED. THE SPCD IS A SUBSET OF THE CSPP AND WILL BE ADDRESSED IN THE SAME FASHION AS A SHOP DRAWING REVIEW FOR TECHNICAL COMPLIANCE WITH THE CSPP.

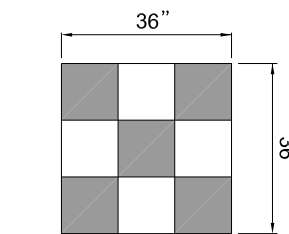
CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE SPECIAL PROVISIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT:

- REHABILITATE TAXIWAY F INTERSECTIONS WITH TAXIWAYS A AND G
- MIDFIELD AUTO PARKING LOT AND UTILITY IMPROVEMENTS
- TERMINAL EXPANSION



**INTERLOCKING LOW PROFILE BARRICADES - ON PAVEMENT**  
NOT TO SCALE

1. LOW PROFILE BARRICADES SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE WEIGHTED WITH A MINIMUM OF 6 SAND BAGS TO PREVENT THEM FROM BEING BLOWN OVER OR DISPLACED UNLESS NOTED OTHERWISE.
2. THE BARRICADE LINE SHALL EXTEND ONE BARRICADE PAST THE EDGE OF PAVEMENT INTO THE TURF.
3. LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
4. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
5. ALTERNATE LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
6. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
7. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES, INCLUDING RELOCATING AS NEEDED FOR THE OCCASIONAL PASSAGE OF AIRCRAFT, SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**  
NOT TO SCALE



MICHAEL P. DUNN  
DIRECTOR



CHICAGO ROCKFORD INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE THE AIR CARGO APRON

SHEET TITLE:

**PHASING PLAN - NOTES AND DETAILS - 2**

DESIGNED: **SMS** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-04  
DATE: 06/03/2016

SHEET NO. **6 OF 20** REVISION





MICHAEL P. DUNN  
DIRECTOR



CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:


REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

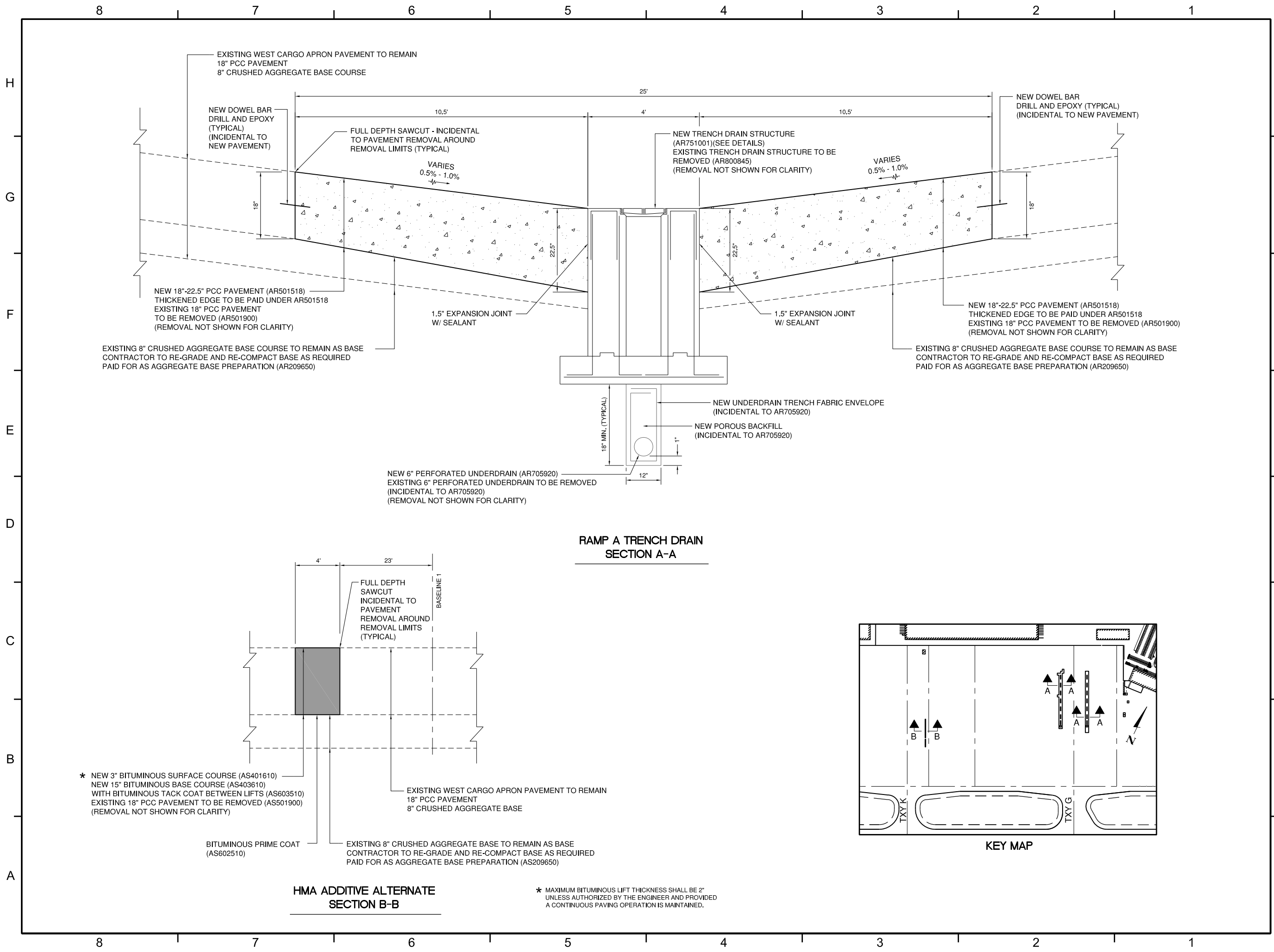
SHEET TITLE:

**TYPICAL SECTION**

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP

PROJECT NO.:	15258-04
DATE:	06/03/2016

SHEET NO.	7 OF 20	REVISION
-----------	---------	----------





MICHAEL P. DUNN  
DIRECTOR

**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

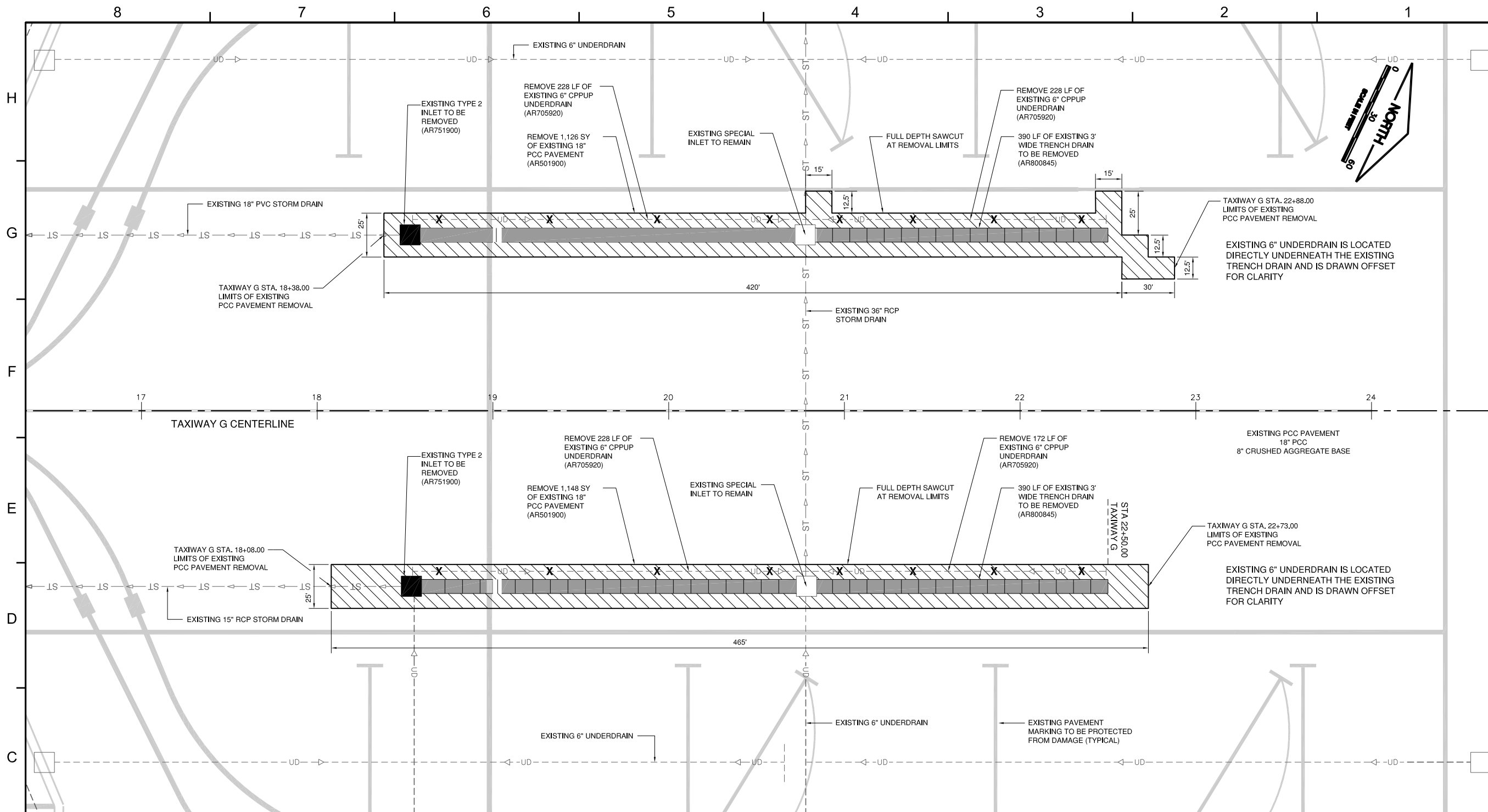
SHEET TITLE:

**EXISTING CONDITIONS/  
PROPOSED REMOVALS**

DESIGNED: **ABM** DRAWN: **JRO** CHECKED: **DLP**

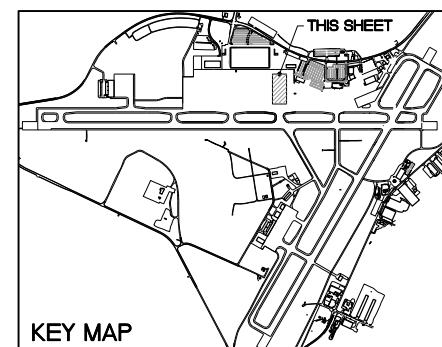
PROJECT NO.: 15258-04  
DATE: 06/03/2016

SHEET NO. **8 OF 20** REVISION



**NOTES**

- SEE PROPOSED PAVEMENT/JOINTING PLAN FOR GEOMETRIC LAYOUT POINTS.
- SEE DRAINAGE SCHEDULES FOR STRUCTURE STATIONS AND OFFSETS.
- CONTRACTOR TO SAWCUT FULL DEPTH AT THE REMOVAL LIMITS PRIOR TO BEGINNING REMOVAL OPERATIONS, COSTS INCLUDED IN THE REMOVAL ITEMS.
- EXISTING PAVEMENT, STRUCTURES OR STORM DRAIN TO REMAIN THAT ARE DAMAGED DURING REMOVAL OPERATIONS TO BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR TO LAYOUT LIMITS OF REMOVAL, TO BE APPROVED BY THE ENGINEER BEFORE BEGINNING REMOVAL OPERATIONS.



**LEGEND**

- EXISTING 18" PCC PAVEMENT TO BE REMOVED
- EXISTING TRENCH DRAIN TO BE REMOVED
- EXISTING DRAINAGE STRUCTURE TO BE REMOVED
- EXISTING DRAINAGE STRUCTURE
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING UNDERDRAIN TO BE REMOVED

A

A







MICHAEL P. DUNN  
DIRECTOR



CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

REV	DATE	DESCRIPTION

APPROVED AS WORKING PLAN  
BY: \_\_\_\_\_

PROJECT NAME: \_\_\_\_\_

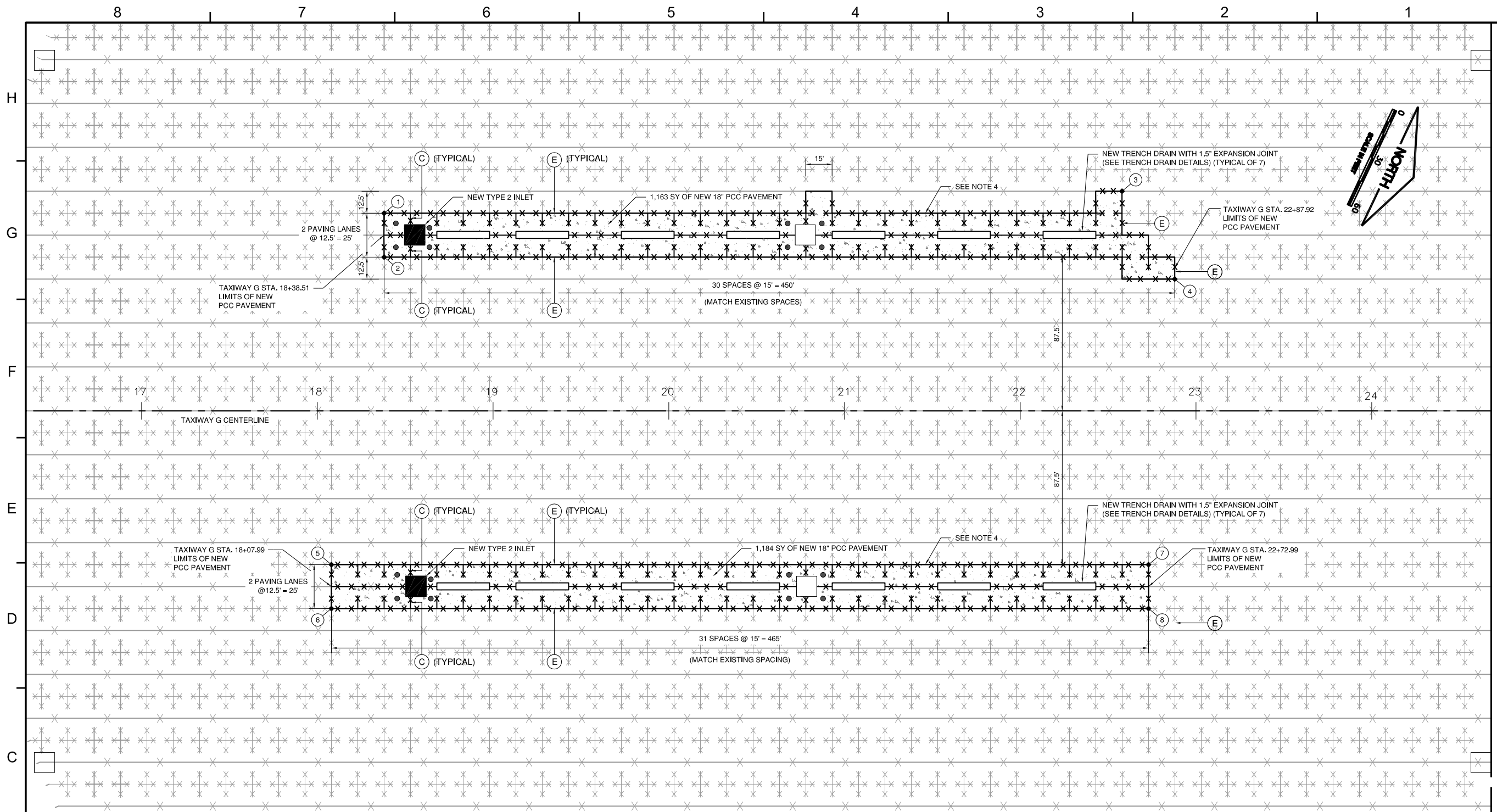
B REHABILITATE THE AIR CARGO APRON

SHEET TITLE:  
**PROPOSED PAVEMENT/  
JOINTING PLAN**

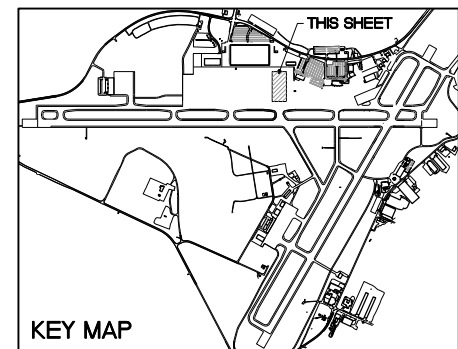
DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP

PROJECT NO.:	15258-04
DATE:	06/03/2016

SHEET NO.	10 OF 20	REVISION
-----------	----------	----------



GEOMETRIC POINTS					
POINT #	STATION/OFFSET	BASELINE	NORTHING	EASTING	DESCRIPTION
1	18+38.00, 112.50' LT	TAXIWAY G ¶	2016986.32	2585483.00	PCC PANEL CORNER
2	18+38.00, 87.50' LT	TAXIWAY G ¶	2016997.00	2585505.61	PCC PANEL CORNER
3	22+57.92, 125.00' LT	TAXIWAY G ¶	2017360.62	2585292.25	PCC PANEL CORNER
4	22+87.92, 75.00' LT	TAXIWAY G ¶	2017409.11	2585324.64	PCC PANEL CORNER
5	18+08.00, 87.50' RT	TAXIWAY G ¶	2017044.66	2585676.64	PCC PANEL CORNER
6	18+08.00, 112.50' RT	TAXIWAY G ¶	2017055.34	2585699.24	PCC PANEL CORNER
7	22+73.00, 87.50' RT	TAXIWAY G ¶	2017465.06	2585477.93	PCC PANEL CORNER
8	22+73.00, 112.50' RT	TAXIWAY G ¶	2017475.75	2585500.53	PCC PANEL CORNER



**NOTES**

- MATCH EXISTING JOINT SEALING.
- SEE DRAINAGE PLAN FOR NEW TYPE 2 INLET DETAILS.
- JOINT SEALING TO BE PAID UNDER AR605510.
- CONTRACTOR SHALL INSTALL DOWEL BARS INTO THE EXISTING CONCRETE PAVEMENT. COST INCLUDED IN NEW 18" PCC PAVEMENT (AR501518).

**LEGEND**

- NEW 18" PCC PAVEMENT
- NEW TRENCH DRAIN
- NEW TYPE 2 INLET
- EXISTING DRAINAGE STRUCTURE
- EXISTING TYPE E DOWELED CONSTRUCTION JOINT
- EXISTING TYPE C DOWELED CONSTRUCTION JOINT
- NEW TYPE E DOWELED CONSTRUCTION JOINT DRILL AND EPOXY AS NECESSARY (INCIDENTAL)
- NEW TYPE C DOWELED CONSTRUCTION JOINT
- ODD SHAPED PANEL REINFORCEMENT

B  
A

B  
A





MICHAEL P. DUNN  
DIRECTOR



CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

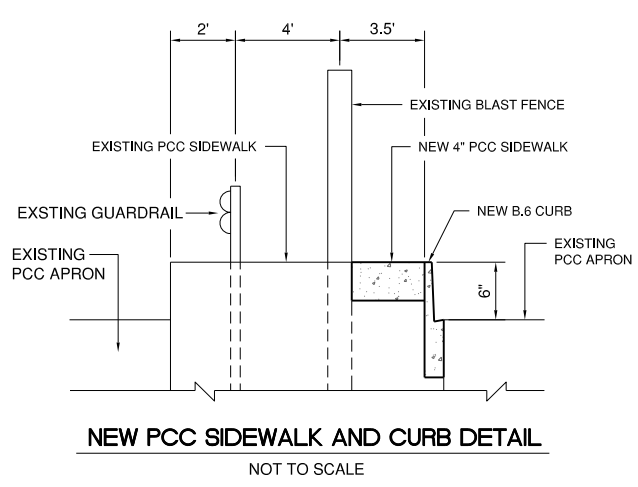
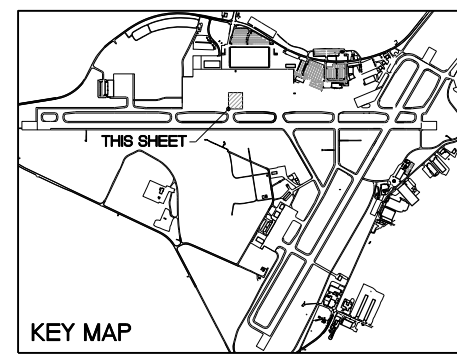
REHABILITATE THE  
AIR CARGO APRON

SHEET TITLE:  
**PROPOSED PAVEMENT/  
JOINTING PLAN -  
ADDITIVE ALTERNATE**

DESIGNED: **ABM** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-04  
DATE: 06/03/2016

SHEET NO. 11 OF 20 REVISION

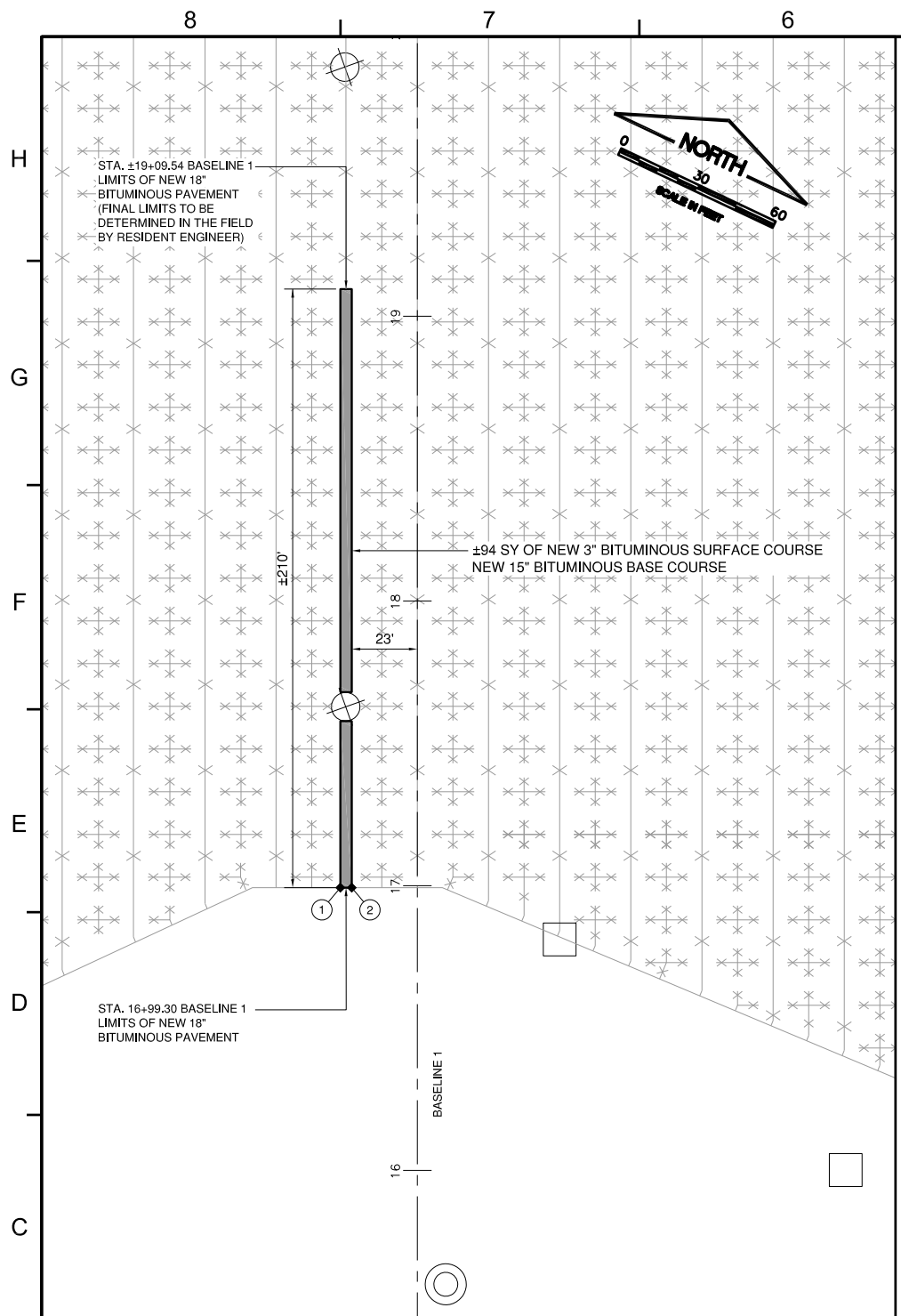


**NOTES**

- NEW B-6 CURB FOR THE BLAST FENCE AREA SHALL BE INCIDENTAL TO 4" PCC SIDEWALK (AS501604).
- JOINT SEALING TO BE PAID UNDER AR605510.
- CONTRACTOR SHALL INSTALL DOWEL BARS INTO THE EXISTING CONCRETE PAVEMENT. COST INCLUDED IN NEW 18" PCC PAVEMENT (AS501518)

**LEGEND**

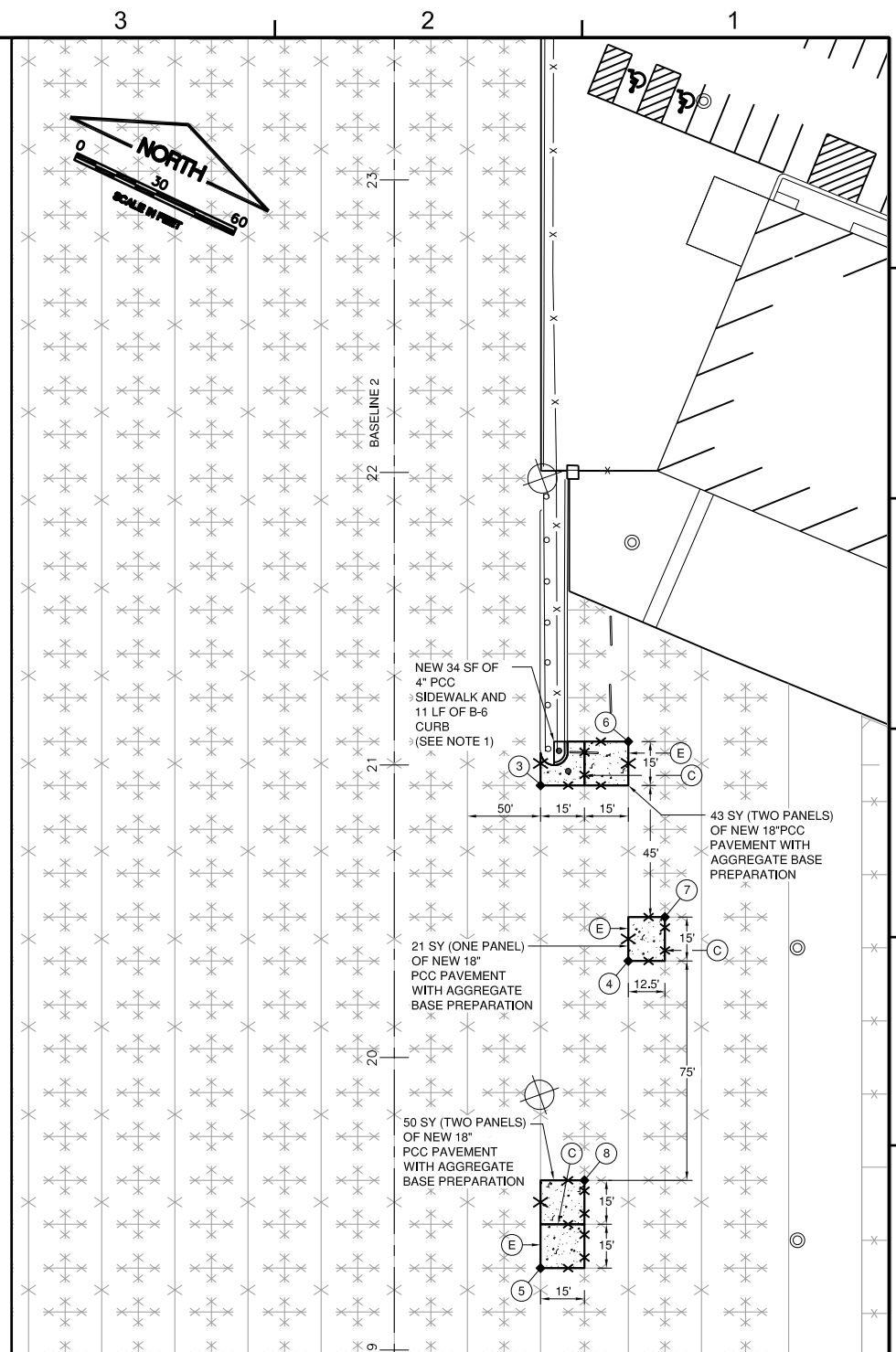
- NEW 18" PCC PAVEMENT
- NEW 3" BITUMINOUS SURFACE COURSE  
NEW 15" BITUMINOUS BASE COURSE
- EXISTING DRAINAGE STRUCTURE
- EXISTING TYPE E DOWELED CONSTRUCTION JOINT
- EXISTING TYPE C DOWELED CONTRACTION JOINT
- NEW TYPE E DOWELED CONSTRUCTION JOINT -  
DRILL AND EPOXY AS NECESSARY (INCIDENTAL)
- NEW TYPE C DOWELED CONTRACTION JOINT
- ODD SHAPED PANEL REINFORCEMENT



ADDITIVE ALTERNATE 1 - AREA 1

GEOMETRIC POINTS					
POINT #	STATION/OFFSET	BASILINE	NORTHING	EASTING	DESCRIPTION
1	16+99.30, 27.00' LT	BASILINE 1	2016427.38	2584625.06	-
2	16+99.30, 23.00' LT	BASILINE 1	2016429.09	2584628.68	-
3	20+93.00, 50.00' RT	BASILINE 2	2017425.19	2585814.78	PCC PANEL CORNER
4	20+33.00, 80.00' RT	BASILINE 2	2017383.76	2585867.54	PCC PANEL CORNER
5	19+28.00, 50.00' RT	BASILINE 2	2017276.01	2585885.29	PCC PANEL CORNER
6	21+08.00, 80.00' RT.	BASILINE 1	2017451.57	2585835.49	PCC PANEL CORNER
7	20+48.00, 92.50' RT.	BASILINE 1	2017402.67	2585872.43	PCC PANEL CORNER
8	19+58.00, 65.00' RT.	BASILINE 1	2017309.55	2585886.03	PCC PANEL CORNER

ADDITIVE ALTERNATE 1 - AREA 2



ADDITIVE ALTERNATE 1 - AREA 2

B  
A

B  
A



MICHAEL P. DUNN  
DIRECTOR



CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
I.A.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

SHEET TITLE:

**JOINTING DETAILS**

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP

PROJECT NO.: 15258-04  
DATE: 06/03/2016

SHEET NO. 12 OF 20 REVISION

**TABLE 1**

THICKNESS T - INCHES	INITIAL SAW CUT I, INCHES I=(T/3) ±1/4"
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"
19	6.33"
20	6.67"

**TABLE 2**

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"
19	1 - 1/2"	20"	18"	#5	30"	30"
20	1 - 1/2"	20"	18"	#5	30"	30"

**ODD SHAPED PANEL REINFORCEMENT**

**CONSTRUCTION JOINTS**

**CHAMFER/BEVEL JOINT DETAIL**

**CONTRACTION JOINTS**

**MISCELLANEOUS JOINT**

**JOINTING AT SKEWED EDGE**

**JOINT SEALING DETAILS**

**JOINTING NOTES**

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO INSTALLATION. ALTERNATE METHODS OF PLACEMENT OF DOWEL BARS MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. TRANSVERSE DOWEL BAR IMPLANTING WILL NOT BE ALLOWED.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING SHALL BE PAID UNDER AR605510.
- SHOULD THE POURING OPERATION REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- EPOXY-COATED DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE RESIDENT ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
- CONCRETE / BITUMINOUS INTERFACE SHALL BE SEALED PER JOINT DETAIL ABOVE.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS AT NO ADDITIONAL COST.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.

**FILLET DETAIL AND FILLET REINFORCING LAYOUT**

FILLET RADIUS	"X" (IN FEET)	"Y" (IN FEET)
20	4.30	6.24
25	4.88	7.00
30	5.40	7.68
50	7.11	9.95
75	8.79	12.21
85	9.38	13.00
100	10.21	14.11
125	11.44	15.78
150	12.56	17.29
175	13.58	18.68
200	14.53	19.98

	DETAIL 1 SILICONE	DETAIL 2 SILICONE	DETAIL 4 SILICONE
W=WIDTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	3/8
D=DEPTH OF SEALANT RESERVOIR (IN.)	1-1/2	1/2	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1/2

NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS. DIMENSIONS MAY VARY WITH MANUFACTURER RECOMMENDATIONS.







MICHAEL P. DUNN  
DIRECTOR

**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

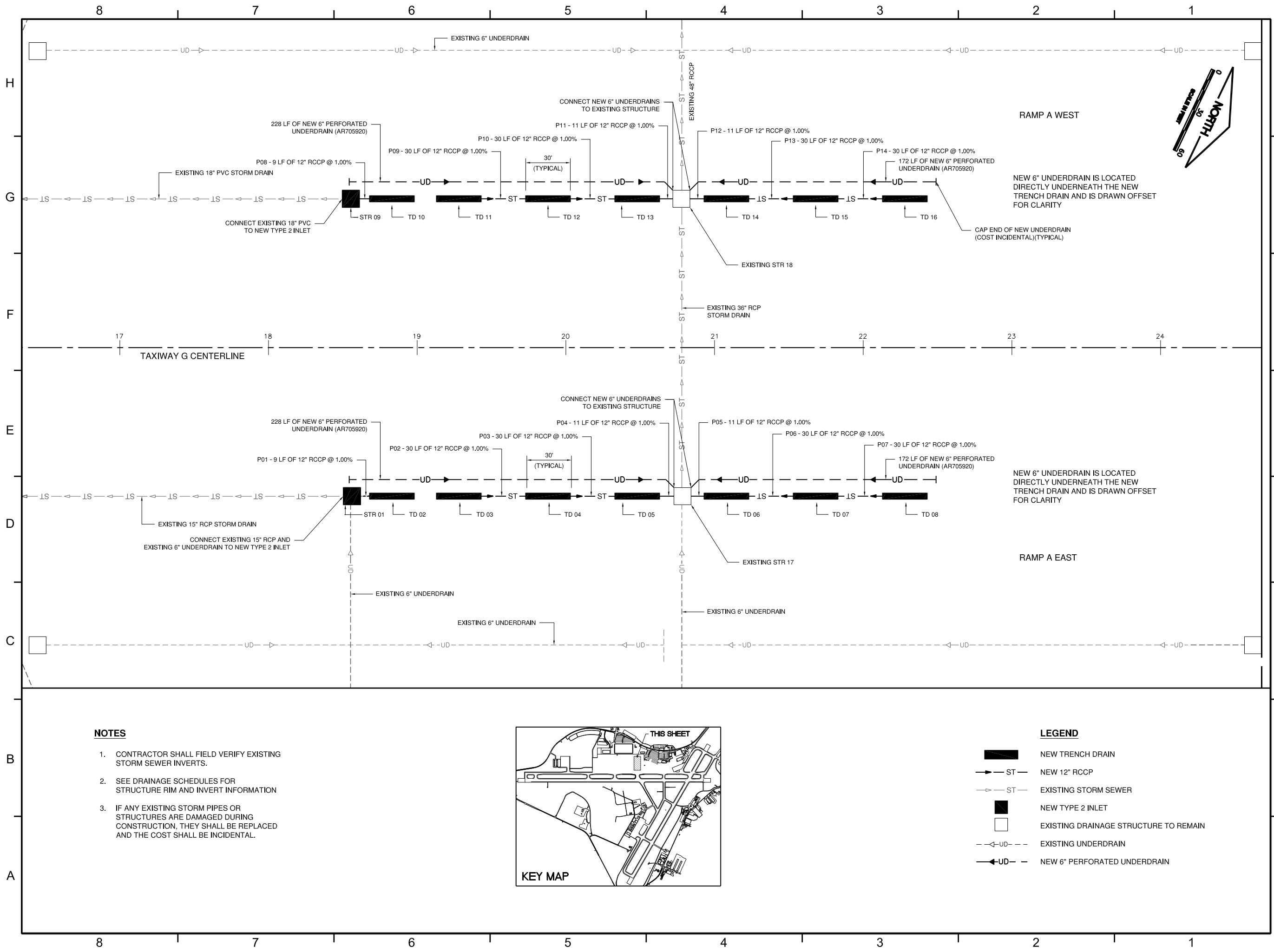
SHEET TITLE:

**DRAINAGE PLAN**

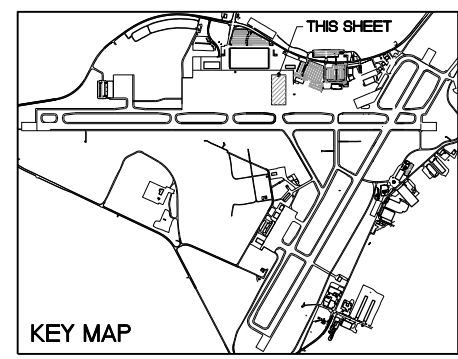
DESIGNED: **ABM**      DRAWN: **JRO**      CHECKED: **DLP**

PROJECT NO.: 15258-04  
DATE: 06/03/2016

SHEET NO. 14 OF 20      REVISION

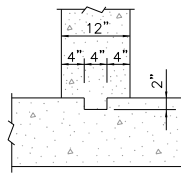


- NOTES**
1. CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER INVERTS.
  2. SEE DRAINAGE SCHEDULES FOR STRUCTURE RIM AND INVERT INFORMATION
  3. IF ANY EXISTING STORM PIPES OR STRUCTURES ARE DAMAGED DURING CONSTRUCTION, THEY SHALL BE REPLACED AND THE COST SHALL BE INCIDENTAL.

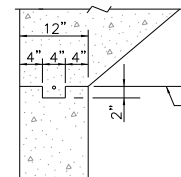


**LEGEND**

	NEW TRENCH DRAIN
	NEW 12" RCCP
	EXISTING STORM SEWER
	NEW TYPE 2 INLET
	EXISTING DRAINAGE STRUCTURE TO REMAIN
	EXISTING UNDERDRAIN
	NEW 6" PERFORATED UNDERDRAIN



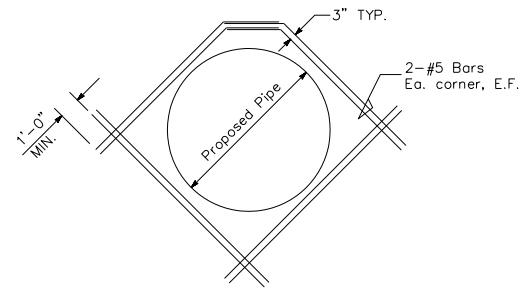
BASE SLAB



TOP SLAB

**SHEAR KEY DETAILS**

N.T.S.



**ADDITIONAL REINFORCEMENT AROUND PIPE OPENING**

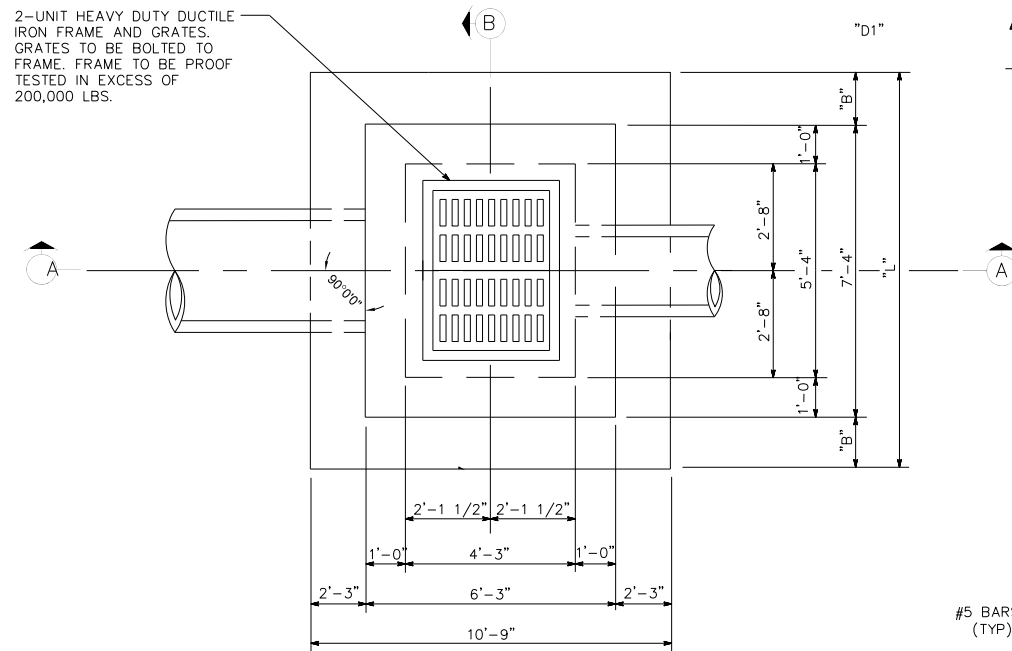
N.T.S.

TRENCH DRAIN SCHEDULE						
STRUCTURE	RIM	INVERT(N)	INVERT(S)	DIM. A	DIM. B	STATION/OFFSET
TD 02	723.49'	-	719.31'	4.00'	4.47'	18+98.00, 100' RT
TD 03	723.50'	719.50'	-	4.00'	4.47'	19+43.00, 100' RT
TD 04	723.50'	718.81'	719.20'	4.69'	5.16'	20+03.00, 100' RT
TD 05	723.48'	718.12'	718.51'	5.36'	5.83'	20+63.00, 100' RT
TD 06	723.48'	718.49'	718.10'	5.38'	5.85'	21+23.00, 100' RT
TD 07	723.48'	719.18'	718.79'	4.69'	5.16'	21+83.00, 100' RT
TD 08	723.48'	-	719.48'	4.00'	4.47'	22+43.00, 100' RT
TD 10	723.02'	-	719.02'	4.00'	4.47'	18+98.00, 100' LT
TD 11	722.99'	718.99'	-	4.00'	4.47'	19+43.00, 100' LT
TD 12	722.98'	718.30'	718.69'	4.68'	5.15'	20+03.00, 100' LT
TD 13	722.99'	717.61'	718.00'	5.38'	5.85'	20+63.00, 100' LT
TD 14	722.98'	718.00'	717.61'	5.37'	5.84'	21+23.00, 100' LT
TD 15	722.98'	718.69'	718.30'	4.68'	5.15'	21+83.00, 100' LT
TD 16	722.99'	-	718.94'	4.00'	4.47'	22+43.00, 100' LT

STATIONS AND OFFSETS IN THE TRENCH DRAIN SCHEDULE ARE MEASURED FROM THE NORTH EDGE OF THE TRENCH DRAIN ON THE TRENCH DRAIN CENTERLINE TO THE TAXIWAY G CENTERLINE.

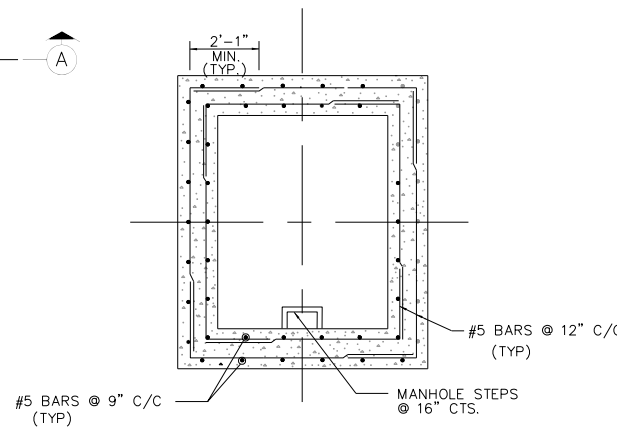
MANHOLE SCHEDULE				
STRUCTURE	TYPE	RIM	INVERTS	STATION/OFFSET
STR 01	TYPE 2 INLET	723.50'	IN 719.40' (N) EX. OUT 716.17' (S)	18+56.13, 100' RT
STR 09	TYPE 2 INLET	722.99'	IN 718.93' (N) EX. OUT 715.99' (S)	18+56.13, 100' LT
EXISTING STR 17	SPECIAL INLET	EX. 723.44'	IN 717.99' (N) IN 718.01' (S) EX. OUT 714.89' (W)	20+77.97, 100' RT
EXISTING STR 18	SPECIAL INLET	EX. 722.95'	IN 717.50' (N) EX. IN 714.60' (E) IN 717.50' (S) EX. OUT 714.06' (W)	20+77.97, 100' LT

STATIONS AND OFFSETS IN THE MANHOLE SCHEDULE ARE REFERENCED FROM TAXIWAY G CENTERLINE. CONTRACTOR TO VERIFY EXISTING PIPE INVERTS.



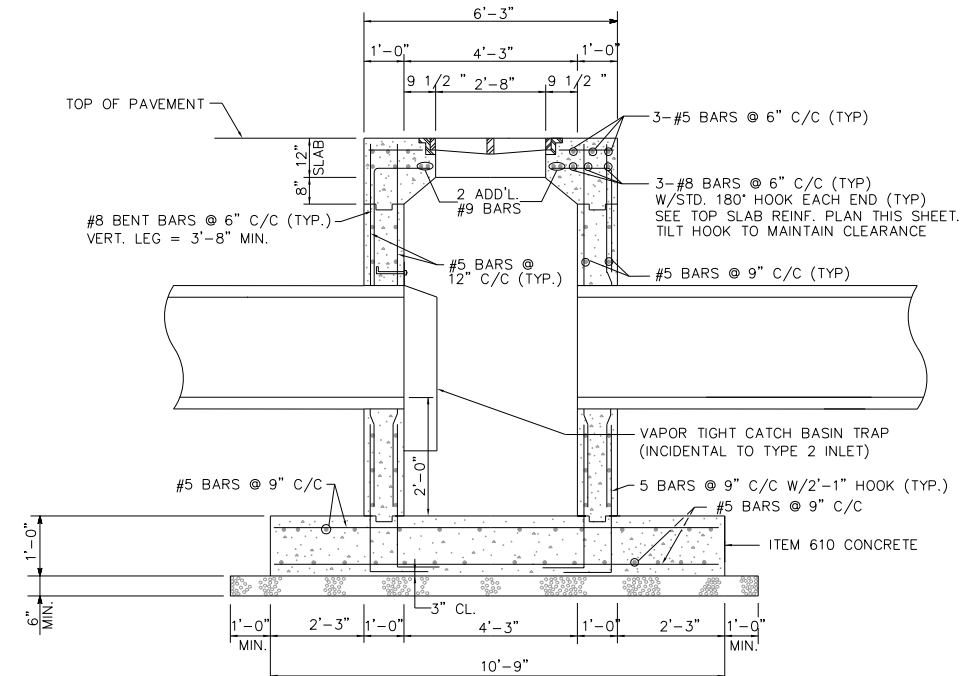
**NEW TYPE 2 INLET**

N.T.S.



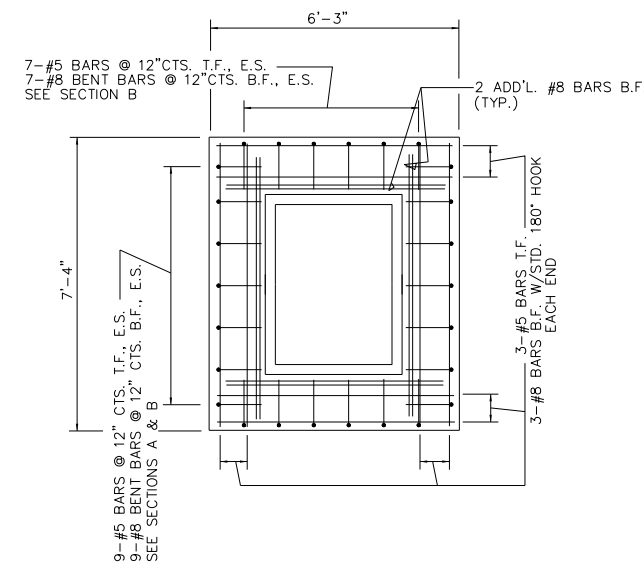
**WALL REINFORCEMENT PLAN**

N.T.S.



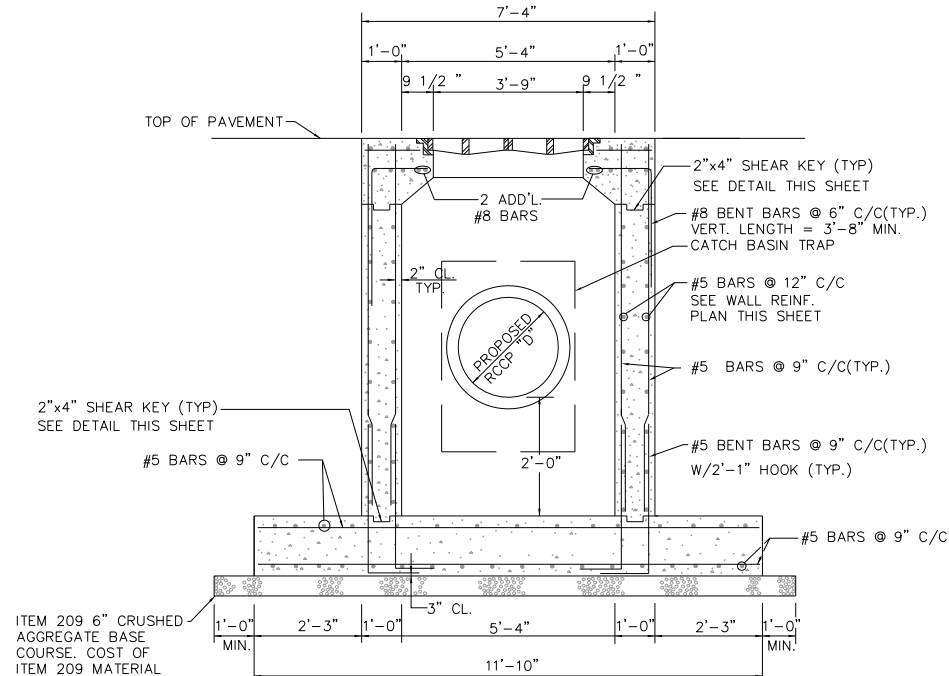
**SECTION A-A**

N.T.S.



**TOP SLAB REINFORCEMENT PLAN**

N.T.S.



**SECTION B-B**

N.T.S.



MICHAEL P. DUNN  
DIRECTOR



CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

REHABILITATE THE  
AIR CARGO APRON

SHEET TITLE:

**DRAINAGE SCHEDULES AND DETAILS**

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP
PROJECT NO.:	15258-04	
DATE:	06/03/2016	
SHEET NO.	15 OF 20	REVISION







MICHAEL P. DUNN  
DIRECTOR

**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:

REV	DATE	DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

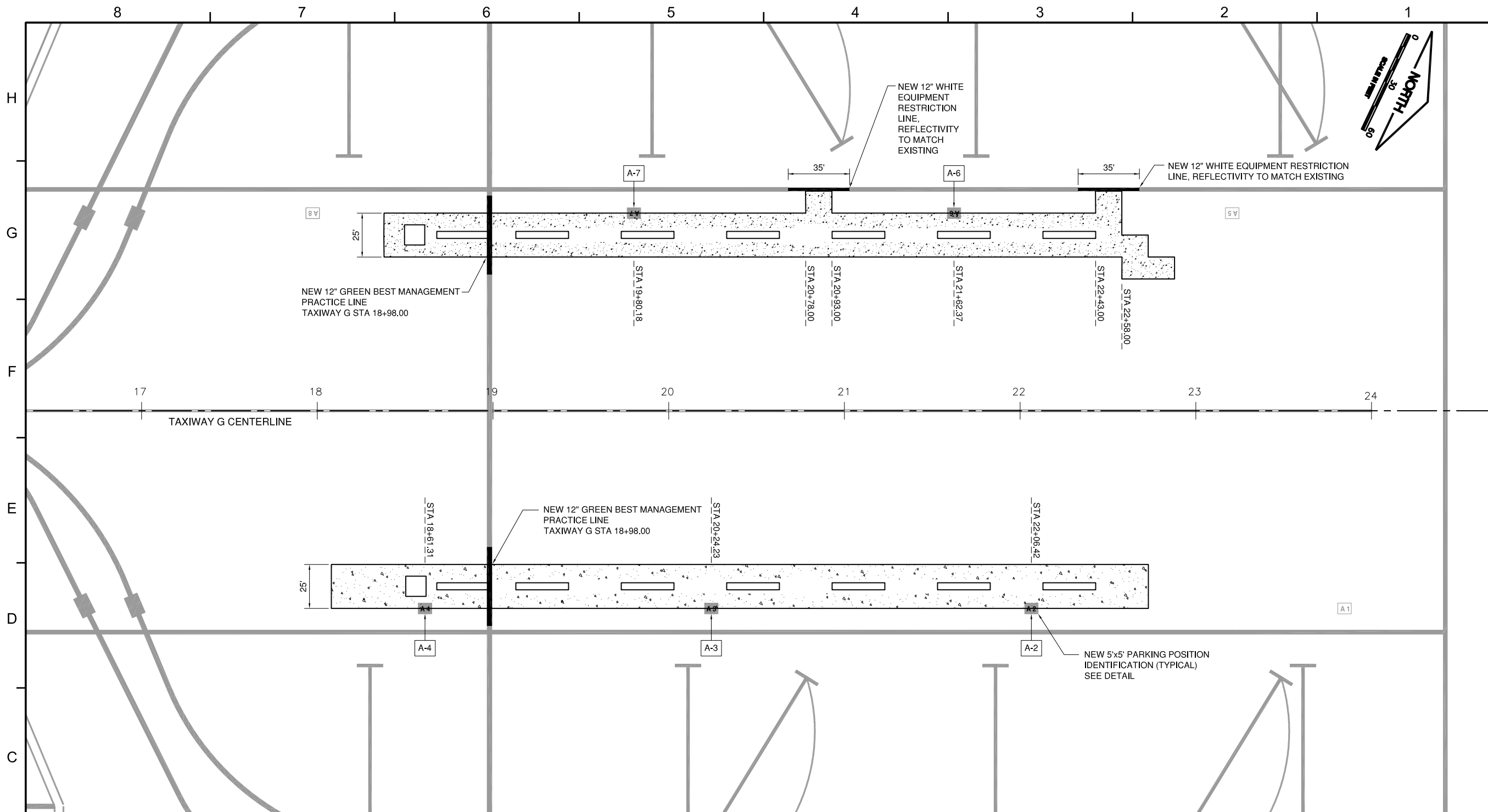
SHEET TITLE:

**PAVEMENT MARKING PLAN**

DESIGNED: **ABM** DRAWN: **JRO** CHECKED: **DLP**

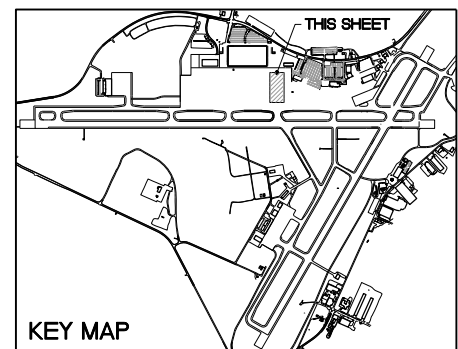
PROJECT NO.: 15258-04  
DATE: 06/03/2016

SHEET NO. 17 OF 20 REVISION



**NOTES**

1. ALL PAVEMENT MARKINGS WITHIN LIMITS OF PAVEMENT WORK SHALL BE REPLACED PLUS 10' OUTSIDE REMOVAL LIMITS.
2. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
3. ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1L (LATEST EDITION).
4. MARKING DAMAGED DURING CONSTRUCTION SHALL BE REPLACED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.
5. ALL NEW PAVEMENT MARKING SHALL BE OUTLINED BY A 6" BLACK BORDER.



**LEGEND**

- NEW PCC PAVEMENT
- NEW PAVEMENT MARKING
- EXISTING PAVEMENT MARKING



MICHAEL P. DUNN  
DIRECTOR

**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: R0026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

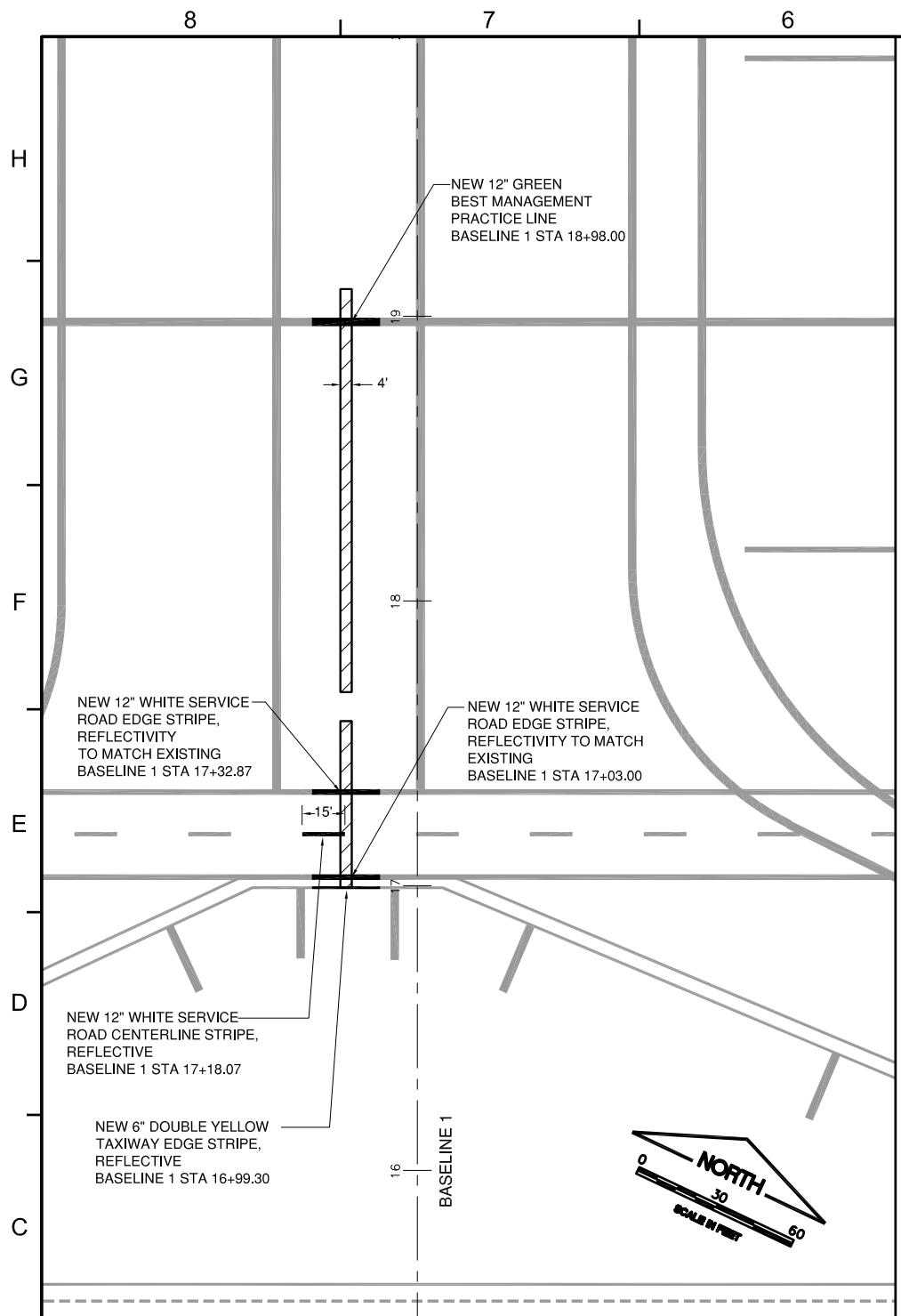
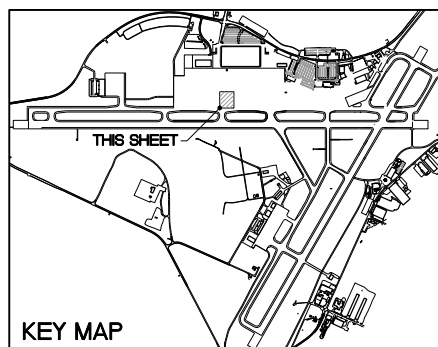
SHEET TITLE:

**PAVEMENT MARKING PLAN  
ADDITIVE ALTERNATE**

DESIGNED: **ABM** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-04  
DATE: 06/03/2016

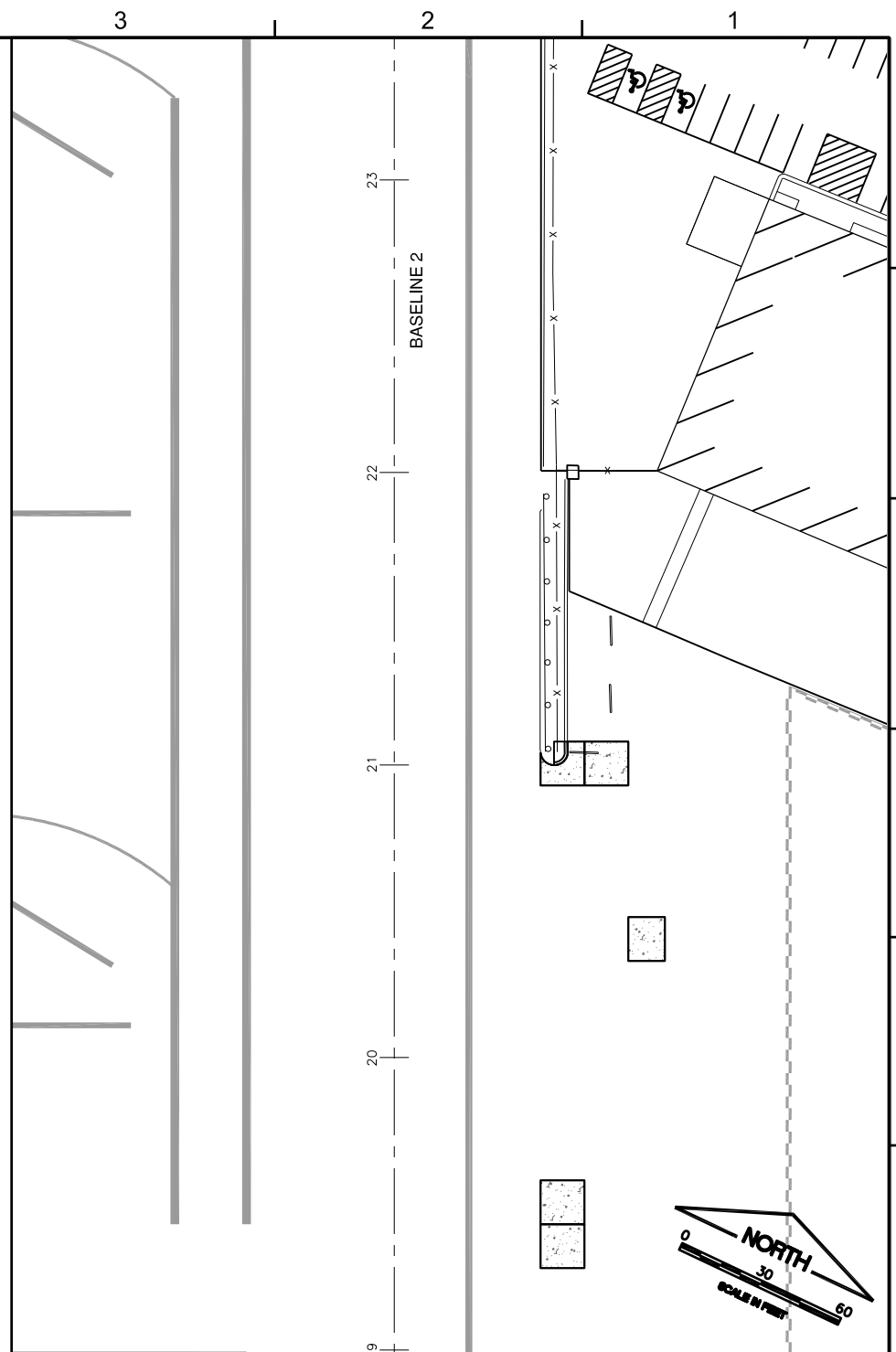
SHEET NO. 18 OF 20 REVISION



ADDITIVE ALTERNATE 1 - AREA 1

**LEGEND**

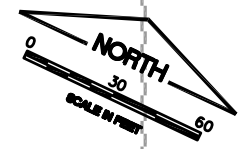
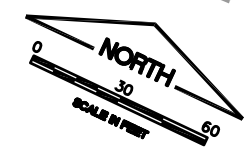
	NEW 4' WIDE BITUMINOUS PATCH ADDITIVE ALTERNATE
	NEW ISOLATED PCC PANELS ADDITIVE ALTERNATE
	NEW PAVEMENT MARKING
	EXISTING PAVEMENT MARKING



ADDITIVE ALTERNATE 1 - AREA 2

**NOTES**

1. ALL PAVEMENT MARKINGS WITHIN LIMITS OF PAVEMENT WORK SHALL BE REPLACED PLUS 10' OUTSIDE REMOVAL LIMITS.
2. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
3. ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1L (LATEST EDITION).
4. MARKING DAMAGED DURING CONSTRUCTION SHALL BE REPLACED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST.
5. ALL NEW PAVEMENT MARKINGS SHALL BE OUTLINED BY A 6" BLACK BORDER.





MICHAEL P. DUNN  
DIRECTOR

© Copyright CMT, Inc.

**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

CHICAGO ROCKFORD  
INTERNATIONAL AIRPORT  
ROCKFORD, ILLINOIS

GREATER ROCKFORD  
AIRPORT AUTHORITY

IL. CONTRACT: RO026  
IL. LETTING ITEM: 20A  
IL. PROJECT: RFD-4549  
A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN  
BY:


REV DATE DESCRIPTION

PROJECT NAME:

REHABILITATE THE  
AIR CARGO APRON

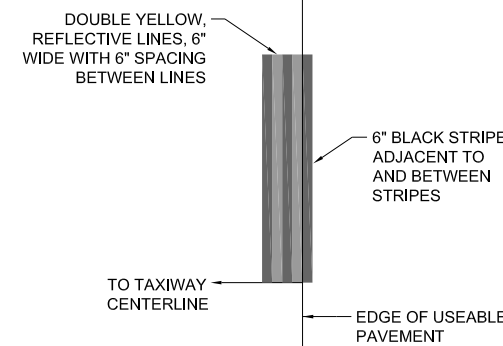
SHEET TITLE:

**PAVEMENT MARKING  
DETAILS**

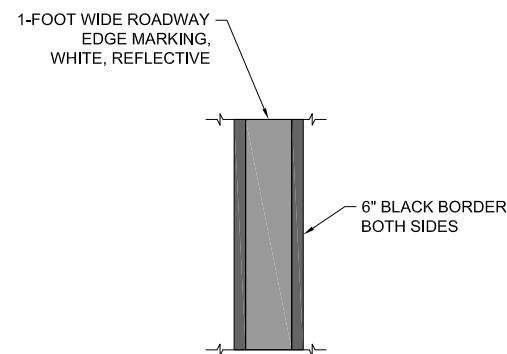
DESIGNED: **ABM** DRAWN: **JRO** CHECKED: **DLP**

PROJECT NO.: 15258-04  
DATE: 06/03/2016

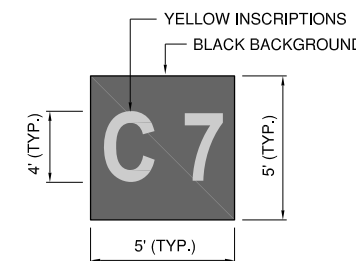
SHEET NO. 19 OF 20 REVISION



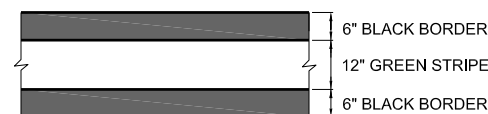
**CONTINUOUS TAXIWAY EDGE LINE MARKING**  
NOT TO SCALE



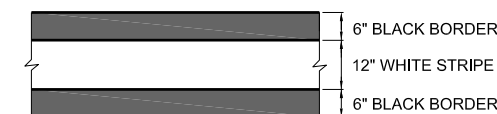
**ROADWAY EDGE MARKINGS, CONTINUOUS**  
NO SCALE



**AIRCRAFT POSITION MARKINGS**  
NOT TO SCALE



**"BEST MANAGEMENT PRACTICES" LINE DETAIL**  
NOT TO SCALE



**"EQUIPMENT RESTRICTION" LINE DETAIL**  
NOT TO SCALE



PROJECT **Rehabilitation of West Cargo Apron, Chicago-Rockford Intl. Airport, Rockford, IL**  
 CLIENT **Crawford, Murphy and Tilly, Inc., Rockford, Illinois**  
 CORE **B1** DATE STARTED **3-11-16** DATE COMPLETED **3-11-16** JOB **L-84,828**



**CORE DATA**

ELEVATION \_\_\_\_\_  
 LOCATION \_\_\_\_\_ GROUNDWATER Dry

SAMPLE DEPTH	WATER CONTENT (%)	P.P.* READING	DRY DENSITY (pcf)	DEPTH	ELEV.	SOIL DESCRIPTIONS
0						
1						18.3" P. C. Concrete (PCC Core recovered in 2 pieces)
1.5						End of Core at 1.5 feet. Note 1: Granular Base Course not sampled.
5						

CORE 84828.GPJ TSC\_ALL.GDT 3/11/16

INSPECTOR LT

SEE SITE PLAN FOR BORING LOCATIONS

PROJECT **Rehabilitation of West Cargo Apron, Chicago-Rockford Intl. Airport, Rockford, IL**  
 CLIENT **Crawford, Murphy and Tilly, Inc., Rockford, Illinois**  
 BORING **B2** DATE STARTED **3-11-16** DATE COMPLETED **3-11-16** JOB **L-84,828**



ELEVATIONS \_\_\_\_\_ WATER LEVEL OBSERVATIONS \_\_\_\_\_  
 GROUND SURFACE \_\_\_\_\_ WHILE DRILLING \_\_\_\_\_ Dry  
 END OF BORING \_\_\_\_\_ AT END OF BORING \_\_\_\_\_ Dry  
 24 HOURS \_\_\_\_\_

SAMPLE NO.	TYPE	N	WC	Qu	γ DRY	DEPTH	ELEV.	SOIL DESCRIPTIONS
						1.3		19.0" P. C. Concrete (PCC Core recovered in 2 pieces)
						2.3		9" Crushed Stone Base Course
1	SS	23	7.3			3.0		FILL - Dark brown to brown clayey SAND, trace gravel, moist (SC)
2	SS	31	8.9					
3	SS	34	3.3					Dense to firm light brown GRAVEL and SAND, trace silt and clay, moist to slightly moist (GP-GC/SP-SC)
4	SS	26	5.4					
								* Approximate unconfined compressive strength based on measurements with a calibrated pocket penetrometer.

TSC 84828.GPJ TSC\_ALL.GDT 4/22/16

DRILL RIG NO. 334

Division lines between deposits represent approximate boundaries between soil types; In-situ, the transition may be gradual.

End of Boring at 10.0'



MICHAEL P. DUNN  
 DIRECTOR  
**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

CHICAGO ROCKFORD  
 INTERNATIONAL AIRPORT  
 ROCKFORD, ILLINOIS  
 GREATER ROCKFORD  
 AIRPORT AUTHORITY

IL. CONTRACT: RO026  
 IL. LETTING ITEM: 20A  
 IL. PROJECT: RFD-4549  
 A.I.P. PROJECT: 3-17-0088-XX

APPROVED AS WORKING PLAN BY: \_\_\_\_\_

REV	DATE	DESCRIPTION

PROJECT NAME: \_\_\_\_\_

REHABILITATE THE AIR CARGO APRON

SHEET TITLE: \_\_\_\_\_

**ENGINEERING INFORMATION**

DESIGNED:	DRAWN:	CHECKED:
ABM	JRO	DLP
PROJECT NO.:	15258-04	
DATE:	06/03/2016	
SHEET NO.	20 OF 20	REVISION