

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF BUFFALO GROVE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
F.A.U. ROUTE 2666 MAIN STREET

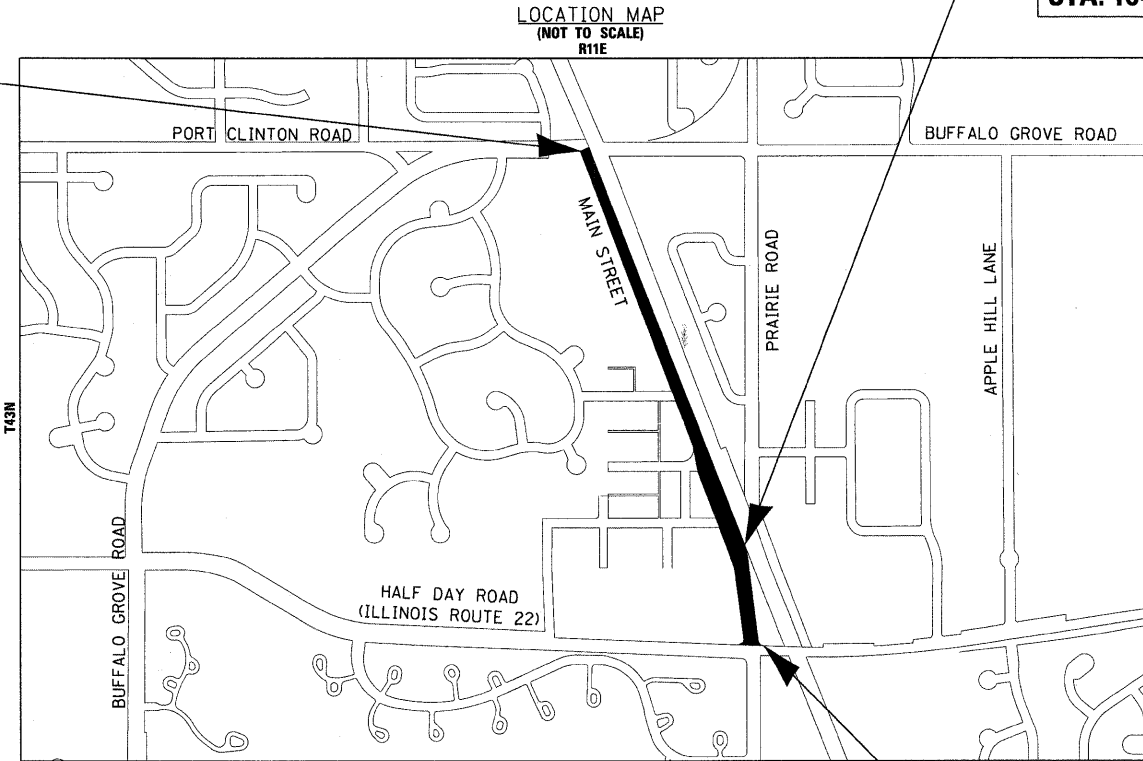
FROM ILLINOIS ROUTE 22 (F.A.P. 337) TO BUFFALO GROVE ROAD (F.A.U. 2657)
LAPP RESURFACING

PROJECT NO.: ARA-9003(507)
SECTION NO.: 09-00096-00-RS
JOB NO.: C-91-126-10
VILLAGE OF BUFFALO GROVE
LAKE COUNTY

THE REVIEW AND APPROVAL OF THESE FINAL ENGINEERING PLANS AND SPECIFICATIONS BY THE LAKE COUNTY DIVISION OF TRANSPORTATION DOES NOT CONSTITUTE A RELEASE FROM OR GRANT OF VARIATION FROM THE RULES, REGULATIONS, AND SPECIFICATIONS REQUIRED IN THE LAKE COUNTY HIGHWAY UTILITY AND FACILITY PLACEMENT ORDINANCE, LATEST EDITION, AS AMENDED, OR ANY SUCCESSOR DOCUMENT OR DOCUMENTS THERETO.

PROJECT NO. ARA-9003(507)
F.A.U. ROUTE 2666
MAIN STREET
END OMISSION/
BEGIN RESURFACING
STA. 104 + 57

PROJECT NO. ARA-9003(507)
F.A.U. ROUTE 2666
MAIN STREET
END RESURFACING
STA. 133 + 42



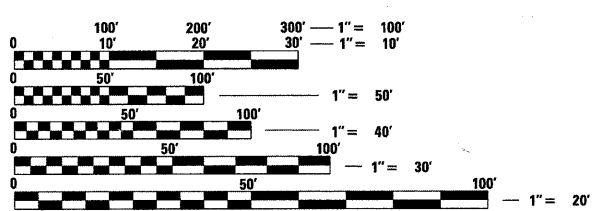
R11E
VERNON TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 3,312 LF OR 0.63 MILES
NET LENGTH OF IMPROVEMENT = 2,885 LF OR 0.55 MILES



LOCATION OF SECTION INDICATED THUS: [black rectangle symbol]

COPYRIGHT 2009, BY BAXTER & WOODMAN, INC.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. - 184-001121 - EXPIRES 4/30/2011

TRAFFIC DATA
MAIN STREET
POSTED & DESIGN SPEED LIMIT = 30 MPH
2009 ADT = 3,500
COLLECTOR

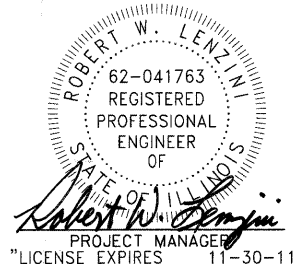


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. A2241909



CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:
COUNTY = LAKE
CITY-TWNSHP. = BUFFALO GROVE - VERNON
SEC. & 1/4 SEC. NO. = SW16, NW21
48 HOURS (2 working days) BEFORE YOU DIG



PROJECT NO. ARA-9003(507)
F.A.U. ROUTE 2666
MAIN STREET
BEGIN OMISSION
STA. 100 + 30

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS
APPROVED [Signature] OCTOBER 19, 2009 VILLAGE OF BUFFALO GROVE REPRESENTATIVE
PASSED MAY 27 2010 [Signature] DISTRICT 4 ENGINEER OF LOCAL ROADS AND STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW MAY 27, 2010 [Signature] DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 63367

Burlington, Wisconsin 262.763.7834 Chicago, Illinois 312.578.0050 Crystal Lake, Illinois 815.459.1260 DeKalb, Illinois 815.787.3111 Grayslake, Illinois 847.223.5088 Itasca, Illinois 630.773.1870 Madison, Wisconsin 608.347.1542 Mokena, Illinois 708.478.2090 Plainfield, Illinois 815.609.7425

B&W PROJECT NO.: 090803 DATE: 10-09-09

BUREAU OF LOCAL ROADS AND STREETS, ASSOCIATE FIELD ENGINEER KEVIN STALLWORTH, P.E., (847) 705-4169 SCHAUMBURG, IL

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
2. UTILITY LOCATIONS HAVE NOT BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY OR SPRINKLER SYSTEM THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER OR VILLAGE.
4. THE CONTRACTOR SHALL NOTIFY THE VILLAGE PUBLIC WORKS ADMINISTRATOR AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
5. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE VILLAGE, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
6. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
7. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.
8. ACCESS TO PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO.
9. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HMA OR CONCRETE.
10. ANY SIGNS OR MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH VILLAGE STANDARDS AND INCLUDED IN THE PAY ITEM FOR COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
11. EXISTING PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
12. IN AREAS WHERE THE EXISTING DRIVEWAY, SIDEWALK, OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEMS, HMA SURFACE REMOVAL - BUTT JOINT, SIDEWALK REMOVAL OR COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT.
13. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
14. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCH ABOVE THE GUTTER FLAG.
15. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET.
16. DETECTABLE WARNINGS SHALL CONSIST OF DYED CONCRETE STAMPED WITH APPROPRIATE STAMPING TOOLS. THE PIGMENT SHALL MEET THE REQUIREMENTS OF ASTM C 979, MATCH COLOR 30166 OF FEDERAL STANDARD 595. THE COST OF DYING CONCRETE IN ADDITION TO THE DETECTABLE WARNING AREA SHOWN ON THE DETAIL SHALL BE INCLUDED IN THE COST OF DETECTABLE WARNINGS BUT THE LIMITS OF THE DYED CONCRETE SHALL NOT EXTEND BEYOND THE BACK OF THE DETECTABLE WARNING AREA.

17. A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELOCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE PAY ITEM FOR MOBILIZATION.
18. ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS."
19. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY.
20. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.
21. THE DISTRICT 1 DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS S1 CONCRETE TO AN ELEVATION 2-1/2" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS POURED HIGHER THAN 2-1/2" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-1/2" DEPTH. THE AREA BEHIND THE PROPOSED CURB AND GUTTER SHALL BE RESTORED WITH NATIVE SOD IN ACCORDANCE WITH SECTION 1081.03 RATHER THAN SALT TOLERANT SOD.
22. ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).
23. THE DAYS PAVING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
24. THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTOR'S OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF CONSTRUCTION.
25. RESERVED
26. THE LOCATIONS OF CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
27. ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
28. THE QUANTITIES FOR PATCHING WILL NOT EXCEED THE AMOUNT LISTED IN THE SUMMARY OF QUANTITIES.

LAKE COUNTY GENERAL NOTES

(APPLIES TO WORK WITHIN LAKE COUNTY ROW)

1. THE CONSTRUCTION, INCLUDING MATERIALS USED, OF THIS UTILITY AND FACILITY SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF THE MOST RECENT EDITIONS OF THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS"; INCLUDING ALL AMENDMENTS AND SUCCESSOR DOCUMENTS TO THE AFOREMENTIONED DOCUMENTS AS PUBLISHED OR ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION UNLESS OTHERWISE STATED IN THESE ENGINEERING PLANS.
2. THE CONSTRUCTION OF THIS UTILITY OR FACILITY SHALL ALSO BE IN ACCORDANCE WITH THE "LAKE COUNTY HIGHWAY UTILITY AND FACILITY PLACEMENT ORDINANCE," LATEST EDITION, AS AMENDED, OR ANY SUCCESSOR DOCUMENT OR DOCUMENTS THERETO.
3. THE LAKE COUNTY DIVISION OF TRANSPORTATION SHALL NOT BE HELD LIABLE FOR ANY ERRORS OR OMISSIONS IN THESE ENGINEERING PLANS OR FOR ANY ADDITIONAL WORK, WHICH MAY BE NEEDED DUE TO ERRORS OR OMISSIONS IN THESE ENGINEERING PLANS.
4. THE PERMITTEE SHALL BE RESPONSIBLE FOR ANY ADDITIONAL WORK, AND ALL COSTS THEREOF, REQUIRED BECAUSE OF ERRORS OR OMISSIONS IN THESE ENGINEERING PLANS AND FOR THE CORRECTION OF ANY CONSTRUCTION, MAINTENANCE, OR SAFETY PROBLEMS, WHICH BECOME APPARENT DURING CONSTRUCTION OR BY INSPECTIONS MADE BY THE PERSON IN CHARGE OF THE COUNTY DIVISION OF TRANSPORTATION.
5. THE PERSON IN CHARGE SHALL NOTE ANY CHANGES FROM THESE ENGINEERING PLANS AND SHALL NOTIFY THE PERMIT DEPARTMENT OF THE LAKE COUNTY DIVISION OF TRANSPORTATION ABOUT ANY CHANGES THAT DEVIATE FROM THE INTENT OF THE ENGINEERING PLANS.



INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, DETAILS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS
5	SCHEDULE OF QUANTITIES
6 - 8	SCHEMATIC PLAN - MAIN STREET
9	DISTRICT 1 DETAIL - BD-08 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
10	DISTRICT 1 DETAIL - BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
11	DISTRICT 1 DETAIL - BD-24 CURB AND GUTTER REMOVAL AND REPLACEMENT
12	DISTRICT 1 DETAIL - BD-32 BUTT JOINTS AND HMA TAPER DETAILS
13	DISTRICT 1 DETAIL - TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
14	DISTRICT 1 DETAIL - TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
15	DISTRICT 1 DETAIL - TC-18 SIGNING FOR FLAGGING OPERATIONS AS WORK ZONE OPENINGS
16	DISTRICT 1 DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN
17 - 18	LAKE COUNTY TYPICAL PAVEMENT MARKING

HIGHWAY STANDARDS

00001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

LEGEND

	BUTT JOINT
	FRAME AND LIDS TO BE ADJUSTED (SPECIAL)

VILLAGE OF BUFFALO GROVE, ILLINOIS MAIN STREET ARRA (LAPP) RESURFACING

INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES

SCALE: _____ STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2666	09-00096-00-RS	LAKE	18	2
C-91-126-10			CONTRACT NO. 63367	
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT ARA-9003(507)	

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 522 PINE ST., SUITE 200, BLOOMINGDALE, IL 61710
 815-898-1200 FAX 815-898-1201
 4/13/2010 1:07:48 PM I:\CT\81581200\090903\ARRA\090903-GEN-NOTES.SHT



DESIGNED - DSH	REVISED - 10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090803-gen-notes.sht

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				ROADWAY 1000	NON-PARTICIPATING
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	10	10	-
25200100	SODDING	SQ YD	10	10	-
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	63	63	-
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1,046	1,024	22
40600300	AGGREGATE (PRIME COAT)	TON	21	20.5	0.5
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	5	5	-
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	444	434.5	9.5
40600895	CONSTRUCTING TEST STRIP	EACH	1	1	-
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	442	439	3
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	905	886	19
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	170	170	-
42400800	DETECTABLE WARNINGS	SQ FT	48	48	-
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	2,879	2,658	221
44000600	SIDEWALK REMOVAL	SQ FT	170	170	-
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	157	157	-
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	158	158	-
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	210	210	-
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	365	365	-
48101200	AGGREGATE SHOULDERS, TYPE B	TON	75	75	-
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	4	4	-
60260100	INLETS TO BE ADJUSTED	EACH	2	2	-
60255500	MANHOLES TO BE ADJUSTED	EACH	5	5	-
67100100	MOBILIZATION	LSUM	1	1	-
70102620	TRAFFIC CONTROL AND PROTECTION STANDARD 701501	LSUM	1	1	-
70102640	TRAFFIC CONTROL AND PROTECTION STANDARD 701801	LSUM	1	1	-
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1,401	1,401	-
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	467	467	-
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9,246	9,142	104
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	76	76	-
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	15	15	-
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	-
X4420132	CLASS D PATCHES (SPECIAL)	SQ YD	314	314	-

* INDICATES SPECIALTY ITEM

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM
 LICENSE NO. 061-077200-0001
 DATE 5/27/2009 9:55:55 AM
 FILE 090803-S00.sht



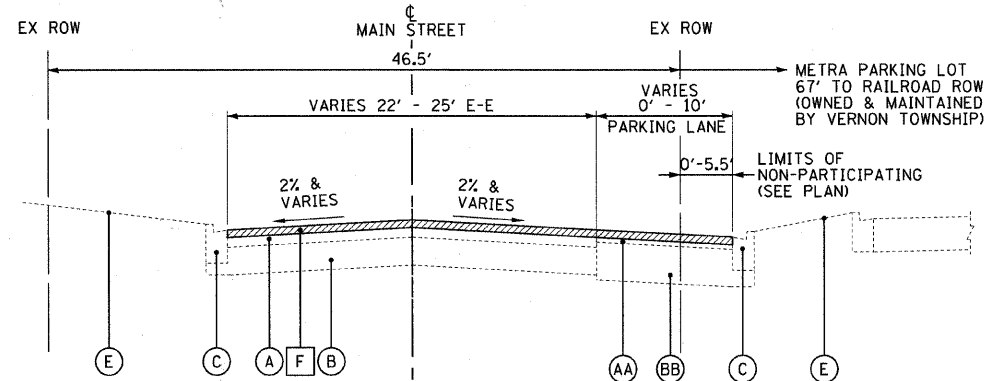
DESIGNED - DSH	REVISED 10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED 5-27-10 PER IDOT REVIEW
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090803-S00.sht

**VILLAGE OF BUFFALO GROVE, ILLINOIS
MAIN STREET ARRA (LAPP)
RESURFACING**

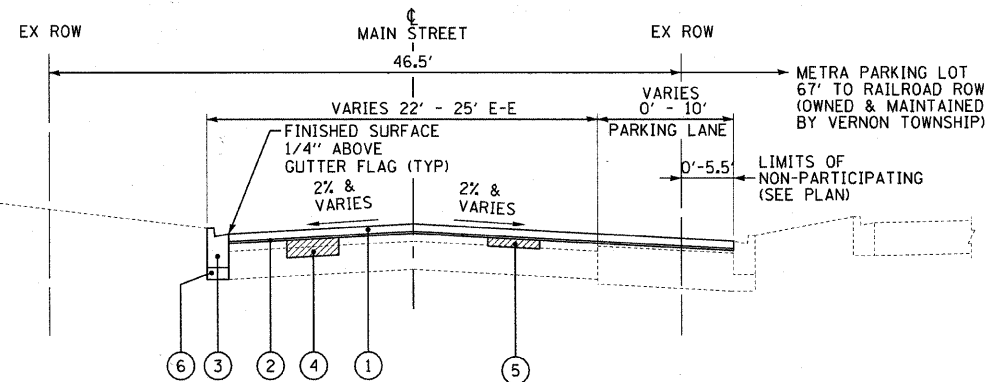
SUMMARY OF QUANTITIES

SCALE: STA. TO STA.

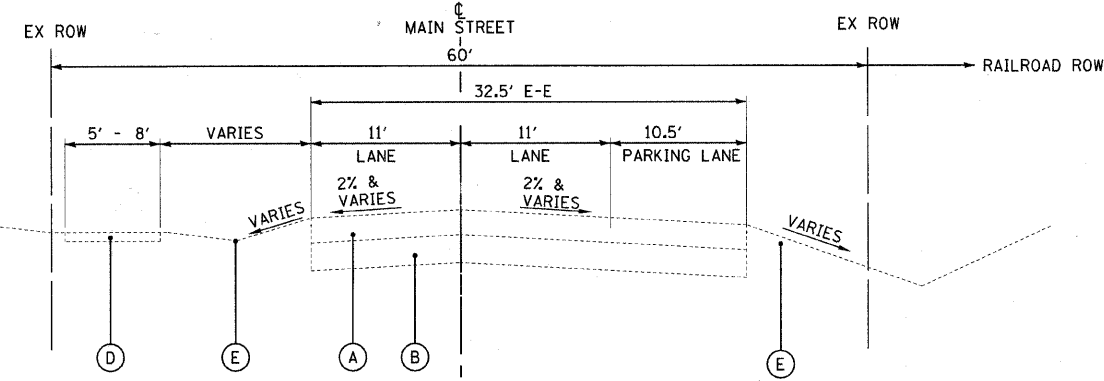
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2666	09-00096-00-RS	LAKE	18	3
C-91-126-10			CONTRACT NO. 63367	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-90036071				



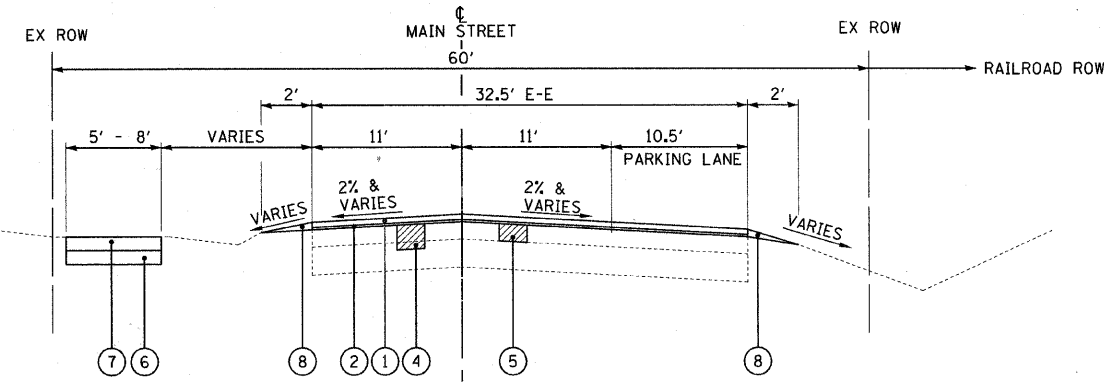
**EXISTING TYPICAL SECTION
MAIN STREET
STA. 104+57 TO STA. 112+81**



**PROPOSED TYPICAL SECTION
MAIN STREET
STA. 104+57 TO STA. 112+81**



**EXISTING TYPICAL SECTION
MAIN STREET
STA. 112+81 TO STA. 133+42**



**PROPOSED TYPICAL SECTION
MAIN STREET
STA. 112+81 TO STA. 133+42**

HOT-MIX ASPHALT MIXTURE REQUIREMENTS NOTES:

- CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm)	4% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75mm, N50	4% @ 50 GYR.
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm) - 6-INCH & 3-INCH (SPECIAL)	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ. YD./IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 6"
- (AA) HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 3" - 4" (IN PARKING LANE)
- (B) AGGREGATE BASE COURSE, 8"
- (BB) AGGREGATE BASE COURSE, 10" (IN PARKING LANE)
- (C) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (D) SIDEWALK (WHERE SHOWN ON PLAN)
- (E) GROUND SURFACE
- (F) HOT-MIX ASPHALT SURFACE REMOVAL, 2"

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 1/2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"
- (3) COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS SHOWN ON PLANS OR AS DETERMINED BY THE ENGINEER)
- (4) CLASS D PATCHES, TYPE II-IV, 6" (AS DETERMINED BY THE ENGINEER)
- (5) CLASS D PATCHES, 3" (SPECIAL) (AS DETERMINED BY THE ENGINEER)
- (6) AGGREGATE BASE COURSE, TYPE B - 4" (AS REQUIRED)
- (7) PORTLAND CEMENT CONCRETE SIDEWALK - 5" (SEE NOTE 1)
- (8) AGGREGATE SHOULDERS, TYPE B

ITEM TO BE REMOVED

LEGEND NOTES:

- LOCATIONS OF SIDEWALK REMOVAL AND PCC SIDEWALK - 5" TO BE AS SHOWN ON PLANS OR AS DETERMINED BY THE ENGINEER.

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC. ...
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 5/27/2009 9:23:30 AM
 FILE - 090803-typ sec.sht



DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED- 5-27-10 PER IDOT REVIEW
CHECKED - RWL	REVISED-
DATE - 10-09-09	FILE - 090803-typ sec.sht

**VILLAGE OF BUFFALO GROVE, ILLINOIS
MAIN STREET ARRA (LAPP)
RESURFACING**

**TYPICAL SECTIONS AND
HMA MIXTURE REQUIREMENTS**

SCALE: STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2666	09-00096-00-RS	LAKE	18	4
C-91-126-10			CONTRACT NO. 63367	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003607				

HOT-MIX ASPHALT SURFACE REMOVAL – BUTT JOINT

LOCATION	AREA (SQ YD)
MAIN STREET (STA 104+57)	16
EASTON AVENUE (STA 108+32 LT)	17
BROCKMAN AVENUE (STA 112+24 LT)	13
PARK AVENUE (STA 116+83 LT)	63
ORCHARD LANE (STA 120+28 LT)	65
VERNON TOWNSHIP ENTRANCE (STA 121+88 LT)	42
VERNON TOWNSHIP ENTRANCE (STA 122+37 LT)	58
VERNON TOWNSHIP ENTRANCE (STA 130+69 LT)	65
MAIN STREET (STA 133+42)	103
TOTAL	442

**SIDEWALK REMOVAL
PORTLAND CEMENT CONCRETE SIDEWALK – 5"**

LOCATION	AREA (SQ FT)
PARK AVENUE (STA 117+10 LT)	81
ORCHARD LANE (STA 120+12 LT)	56
ORCHARD LANE (STA 120+42 LT)	33
TOTAL	170

**COMBINATION CONCRETE CURB AND GUTTER
REMOVAL AND REPLACEMENT**

LOCATION	LENGTH (FT)
STA 106+48 TO STA 107+01 RT	53
EASTON AVE. (STA 108+17 TO STA 108+37 LT)	32
STA 108+29 TO STA 108+34 LT	5
STA 109+89 TO STA 108+99 LT	10
STA 112+12 TO STA 112+69 RT	57
TOTAL	157

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 014-000271 - EXPIRES 7/31/2011
 07/27/2009 08:55:40 PM
 ...\\pilot\090803-0001\PI...
 ...\\pilot\090803-0001\PI...
 ...\\pilot\090803-0001\PI...



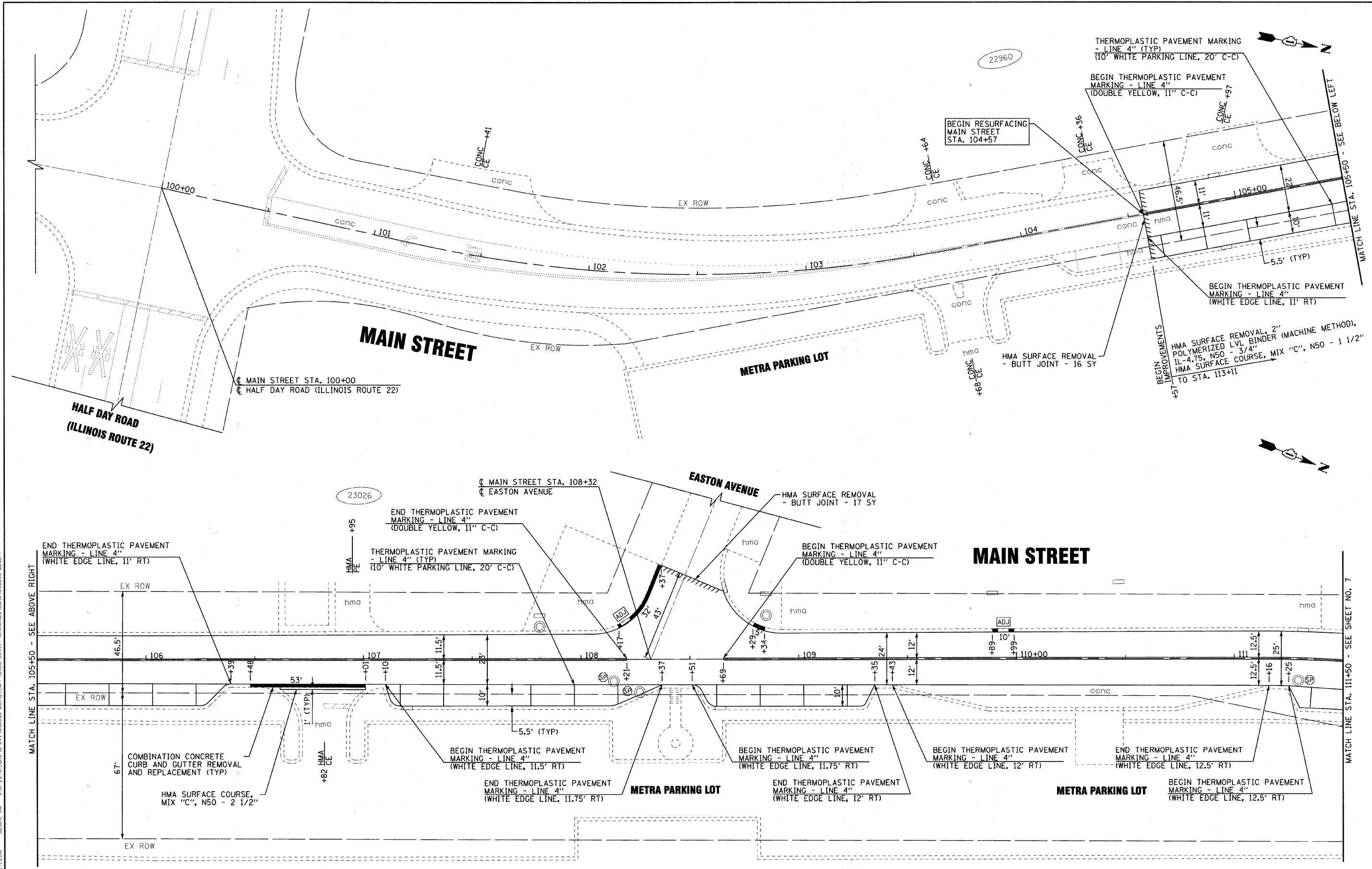
DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED-
CHECKED - RWL	REVISED-
DATE - 10-09-09	FILE - 090803-S00.shx

**VILLAGE OF BUFFALO GROVE, ILLINOIS
MAIN STREET ARRA (LAPP)
RESURFACING**

SCHEDULE OF QUANTITIES

SCALE: STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2666	09-00096-00-RS	LAKE	18	5
C-91-126-10			CONTRACT NO. 63367	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003507				



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM
 No. 09-0096-00-RS
 5/27/2010 9:29:42 AM
 PROJECT: VILLAGE OF BUFFALO GROVE, ILLINOIS
 MAIN STREET ARRA (LAPP) RESURFACING
 DRAWING NO. 09-0096-00-RS-06

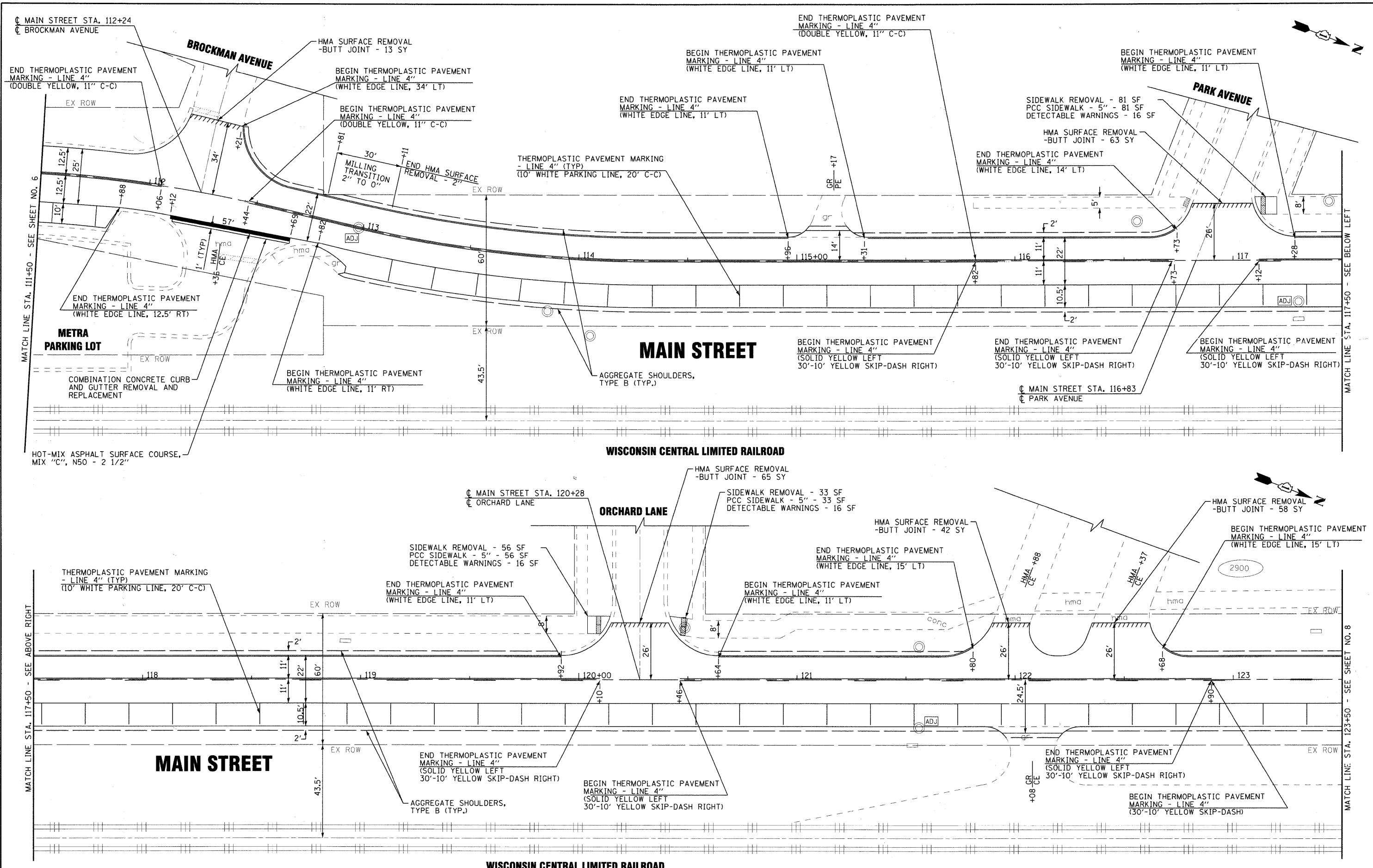


DESIGNED - DSH	REVISED - 10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED - 5-27-10 PER IDOT REVIEW
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090803-pp1.sht

VILLAGE OF BUFFALO GROVE, ILLINOIS
MAIN STREET ARRA (LAPP)
RESURFACING

SCHEMATIC PLAN - MAIN STREET

SCALE: 1" = 20'	STA. 100+00 TO STA. 111+50	F.A.J. RTE. 2666	SECTION 09-00096-00-RS	COUNTY LAKE	TOTAL SHEETS 18	SHEET NO. 6
		C-91-126-10		CONTRACT NO. 63367		
		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003607				



COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - BR-00022 - EXPIRES 4/20/2011
 1508 E. WYOMING ST., SUITE 100, BUFFALO GROVE, IL 60089
 TEL: 847.237.2005 FAX: 847.237.2009



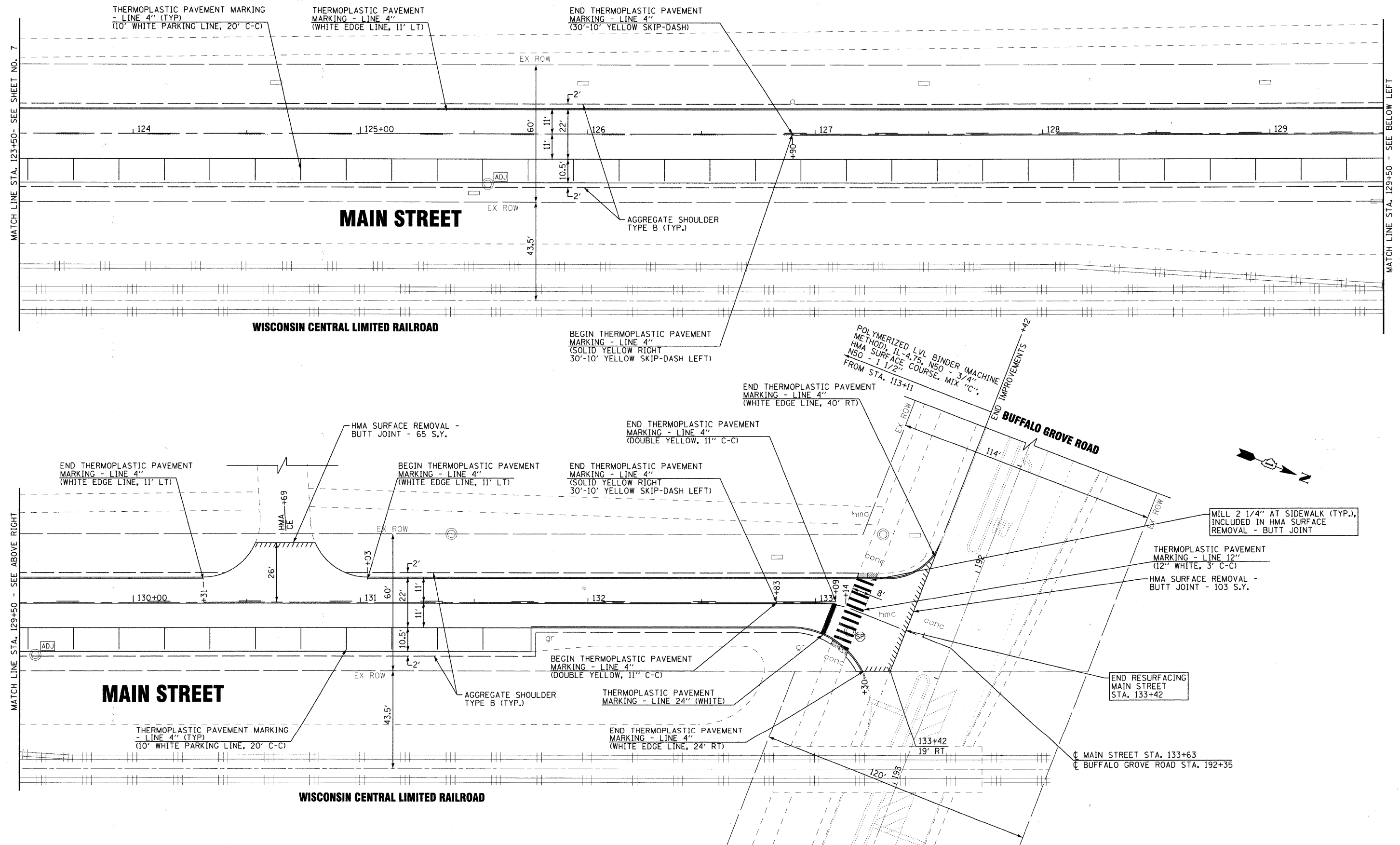
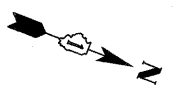
DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED-
CHECKED - RWL	REVISED-
DATE - 10-09-09	FILE - 090803-pp2.sht

VILLAGE OF BUFFALO GROVE, ILLINOIS
MAIN STREET ARRA (LAPP)
RESURFACING

SCHEMATIC PLAN - MAIN STREET

SCALE: 1" = 20' STA. 111+50 TO STA. 123+50

F.A.U. RTE. 2666	SECTION 09-00096-00-RS	COUNTY LAKE	TOTAL SHEETS 18	SHEET NO. 7
C-91-126-10		CONTRACT NO. 63367		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARRA-9003(507)				



COPYRIGHT © 2008 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00021 - EXPRESS #2307/2011
 530 PLYMOUTH SQUARE, SUITE 200, GAITHERSBURG, MD 20878
 301.251.2009 #655847 PM



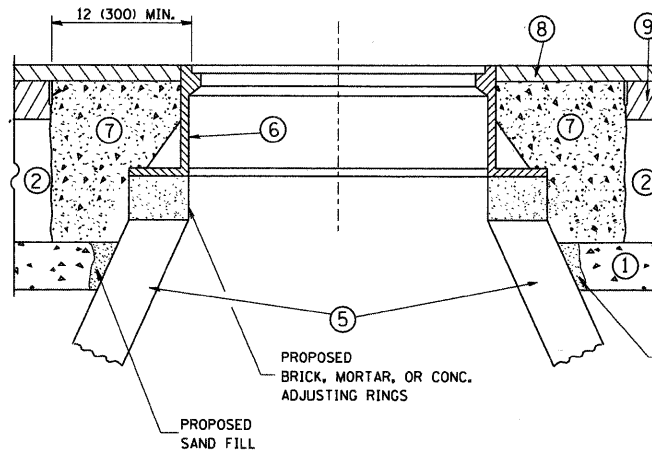
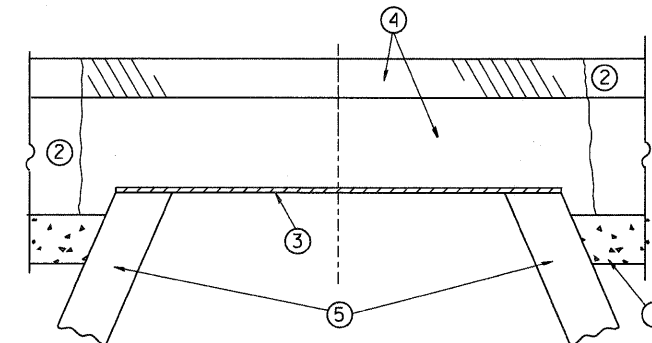
DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED-
CHECKED - RWL	REVISED-
DATE - 10-09-09	FILE - 090803-pp3.sht

VILLAGE OF BUFFALO GROVE, ILLINOIS
MAIN STREET ARRA (LAPP)
RESURFACING

SCHEMATIC PLAN - MAIN STREET

SCALE: 1" = 20' STA. 123+50 TO STA. 133+63

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2666	09-00096-00-RS	LAKE	18	8
C-91-126-10		CONTRACT NO. 63367		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003607				



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

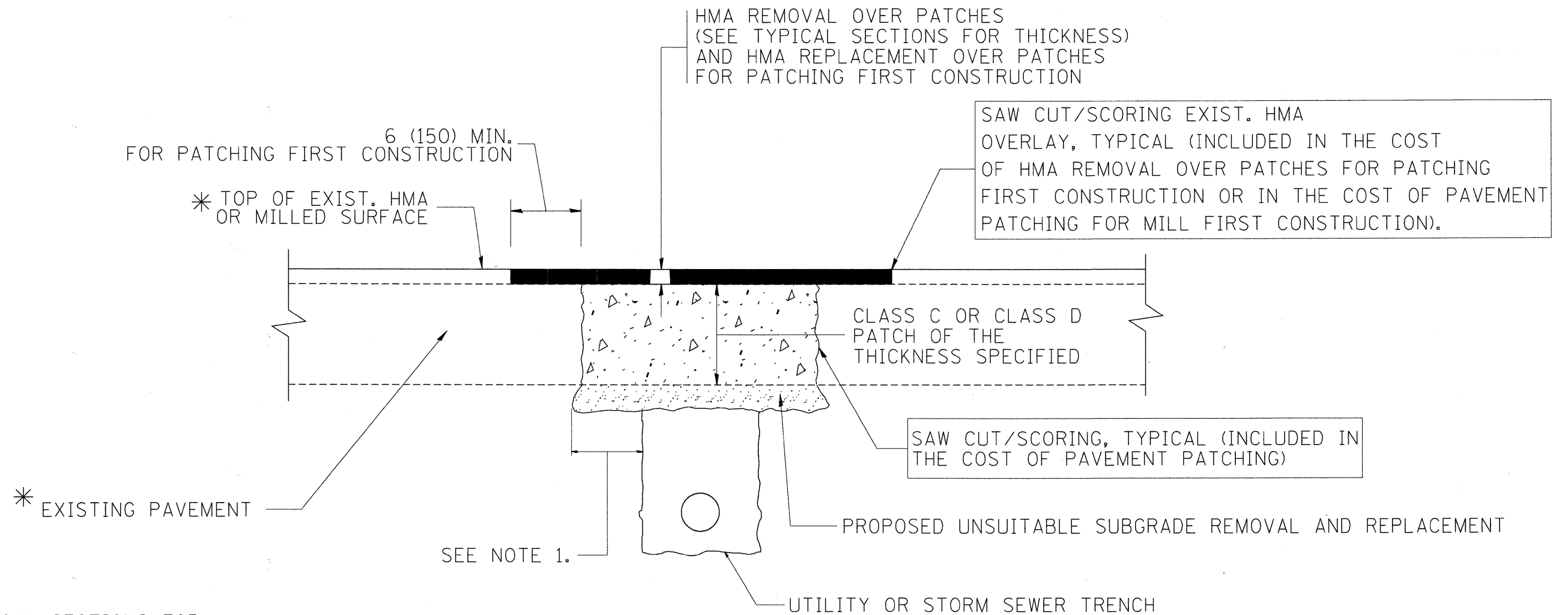
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME = W:\diststd\22x34\bd88.dgn	USER NAME = gaglienobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.I. RTE.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	2666	09-00096-00-RS	LAKE	18	9
		PLOT SCALE = 50.0000 ' / IN.	CHECKED -		REVISED - R. WIEDEMAN 05-14-04			BD600-03 (BD-8)			CONTRACT NO. 63367	
		PLOT DATE = 1/4/2008	DATE - 10-25-94		REVISED - R. BORO 01-01-07			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(507)			C-91-126-10	



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

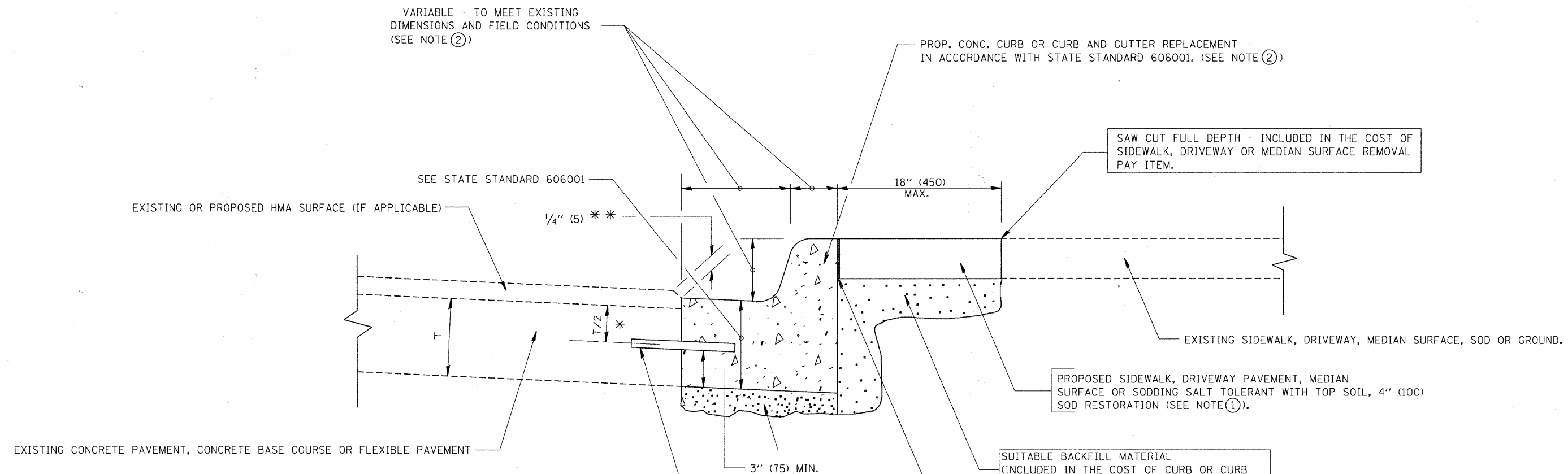
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT				F.A. RTE. 2666	SECTION 09-00096-00-RS	COUNTY LAKE	TOTAL SHEETS 18	SHEET NO. 10
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 63367			
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (507)				
		DATE = 10-25-94	REVISED - K. ENG 10-27-08						C-91-126-10				



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
 SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

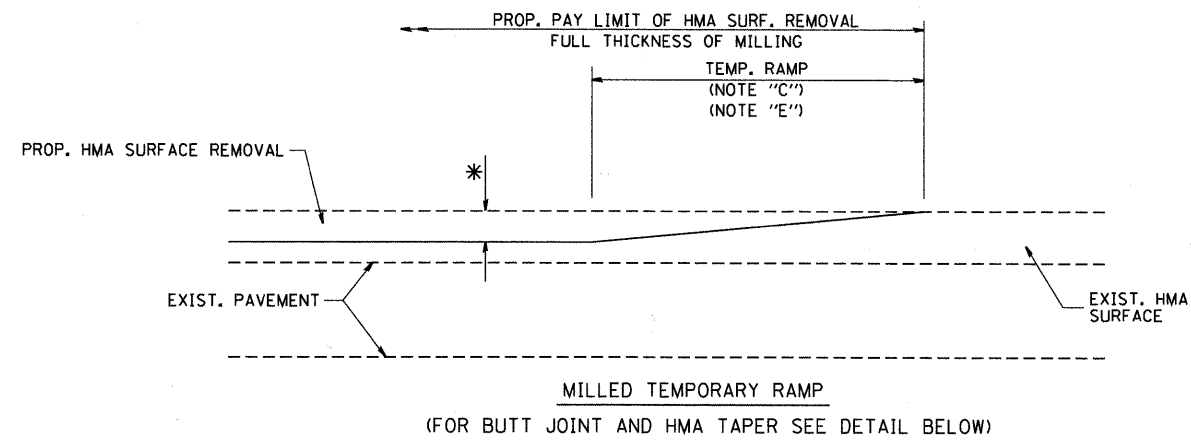
- SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

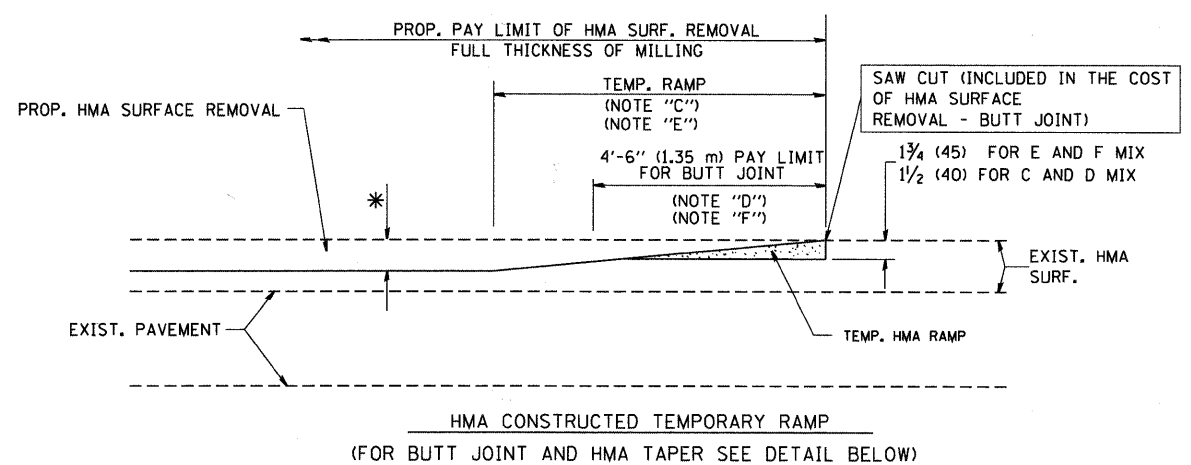
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

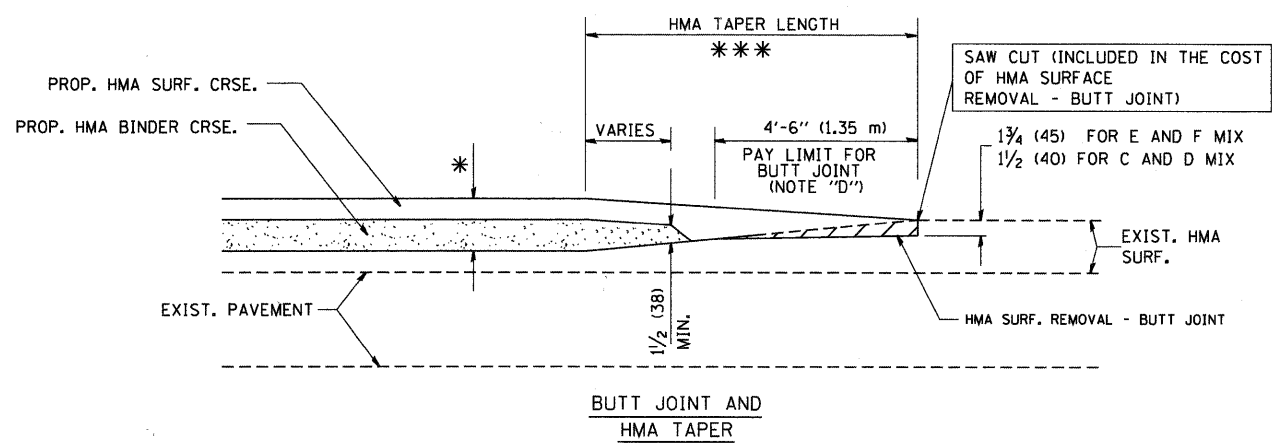
FILE NAME =	USER NAME = drivakasn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A. RTE. 2666	SECTION 09-00096-00-RS	COUNTY LAKE	TOTAL SHEETS 18	SHEET NO. 11
ct:\pwwork\pwwork\drivakasn\08108315\08108315.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-06 (BD-24)		CONTRACT NO. 63367		
	PLOT SCALE = 50.000 / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT AREA-9003(507)							
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09		C-91-126-10							



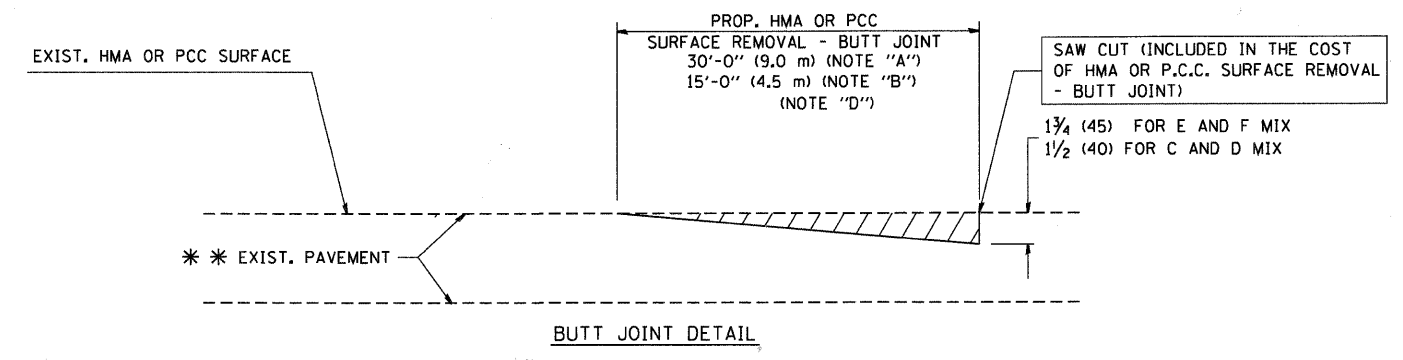
OPTION 1



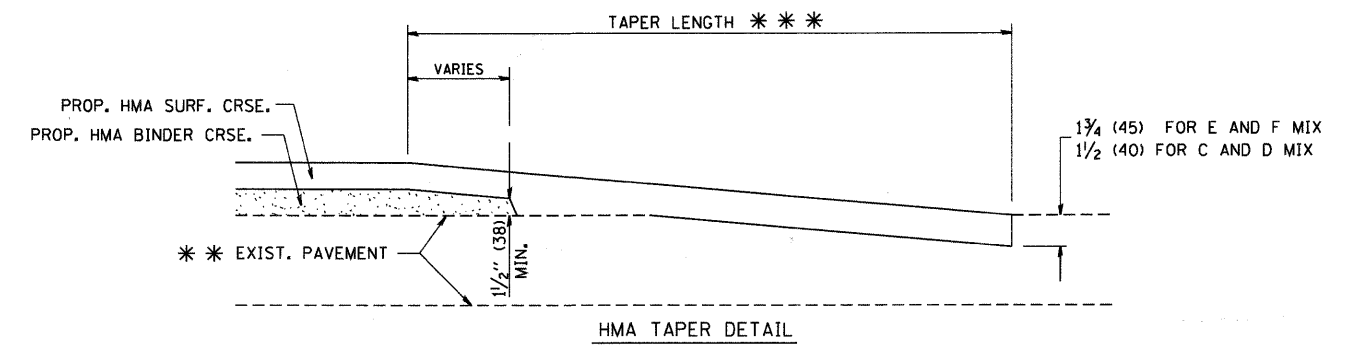
**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

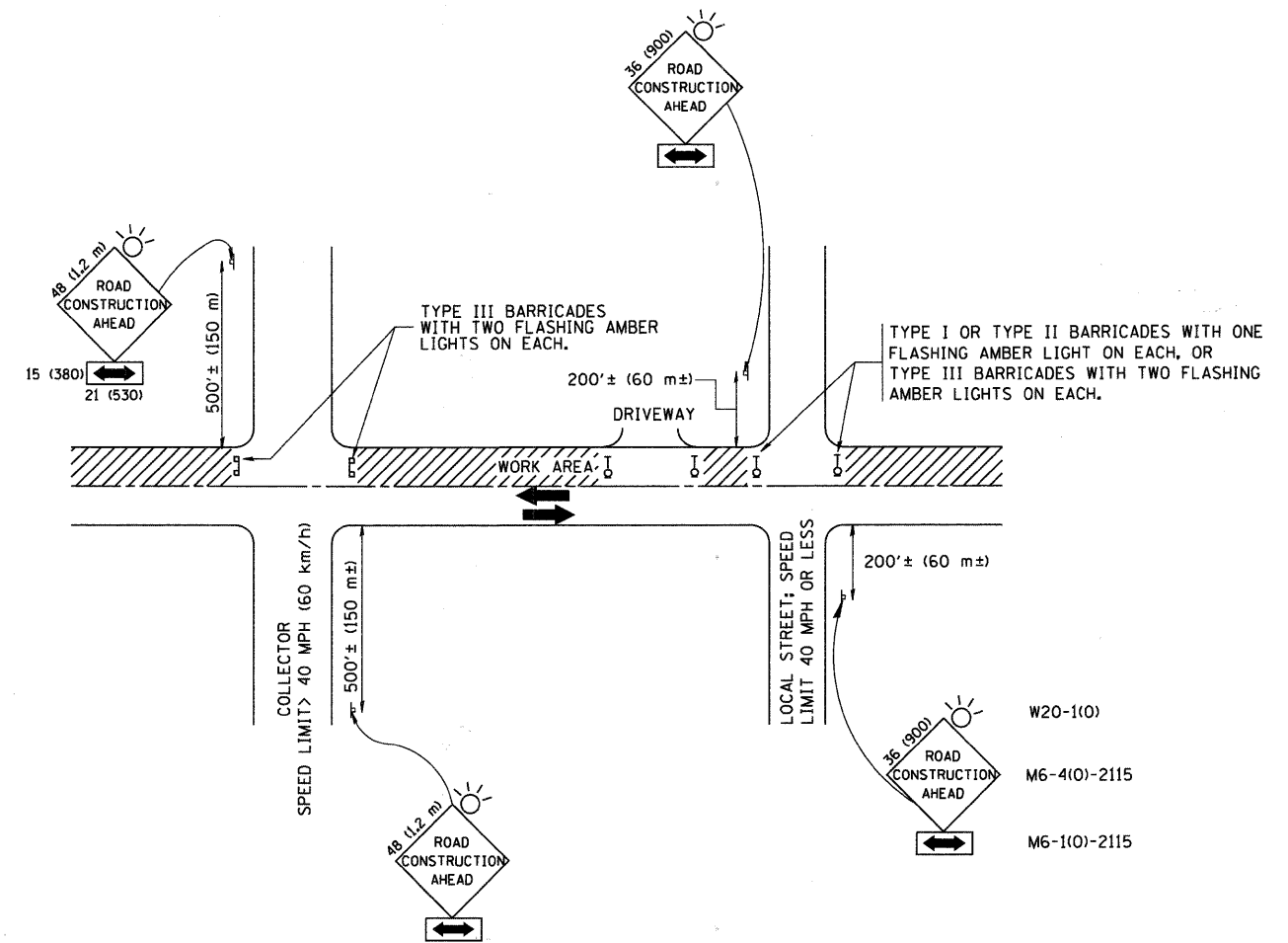
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distsd\22x34\bd32.dgn	USER NAME = geglionob	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 03-21-97		2666	09-00096-00-RS	LAKE	18	12			
		PLOT SCALE = 50.0000' / IN.	REVISOR - M. GOMEZ 04-06-01		BD400-05 BD32		CONTRACT NO. 63367					
		PLOT DATE = 1/4/2008	DATE - 06-13-90		REVISED - R. BORO 01-01-07	SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (507)		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

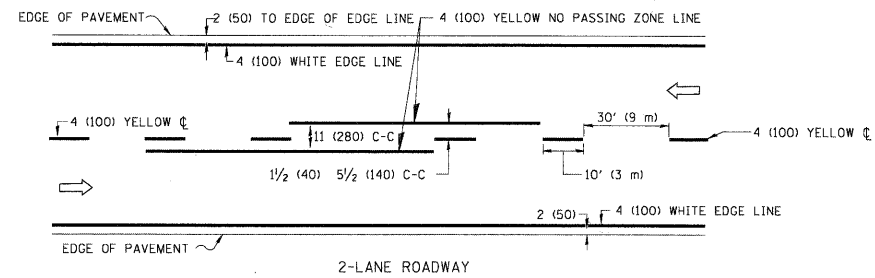
FILE NAME = W:\diststd\22x34\tbl8.dgn	USER NAME = gogionobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

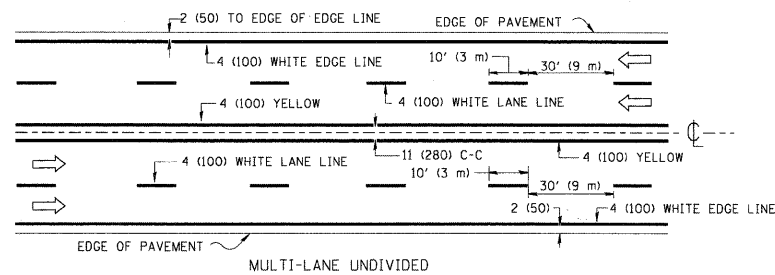
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2666	09-00096-00-RS	LAKE	18	13
TC-10			CONTRACT NO. 63367	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003 (507)				

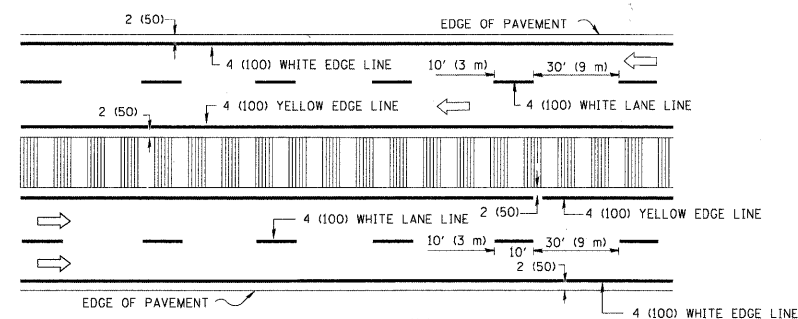
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.



2-LANE ROADWAY



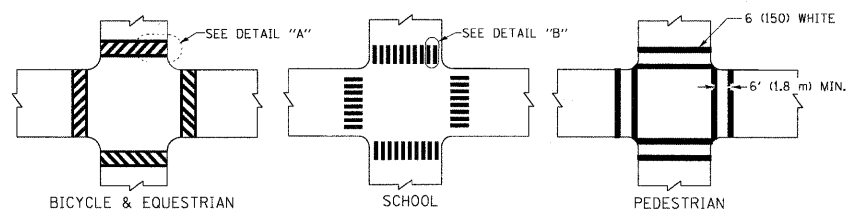
MULTI-LANE UNDIVIDED



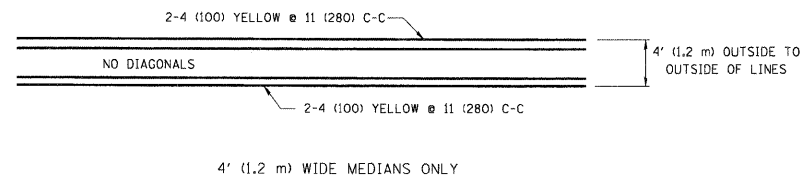
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

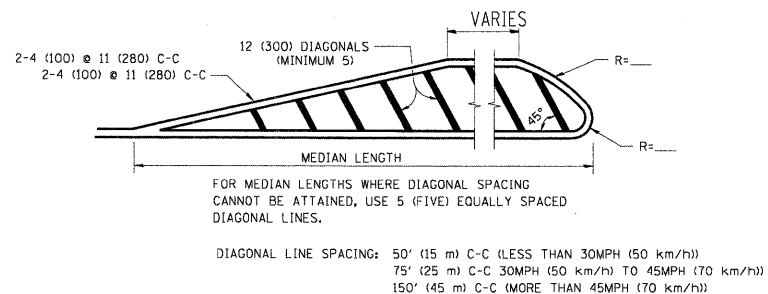
TYPICAL LANE AND EDGE LINE MARKING



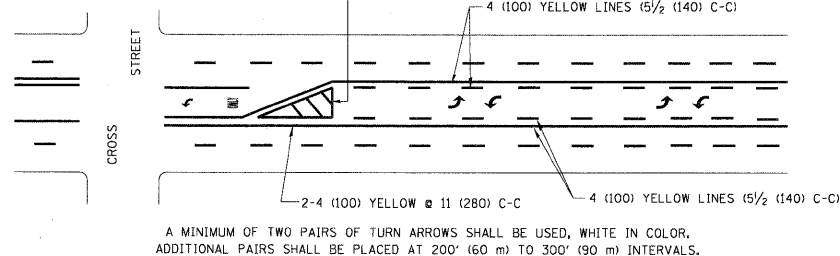
TYPICAL CROSSWALK MARKING



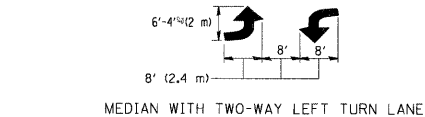
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

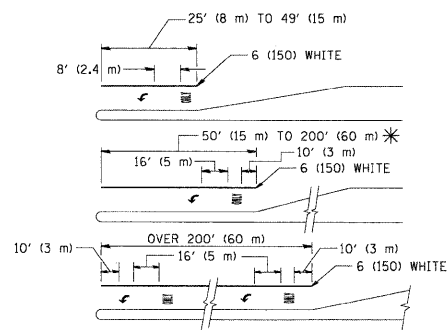


TYPICAL PAINTED MEDIAN MARKING



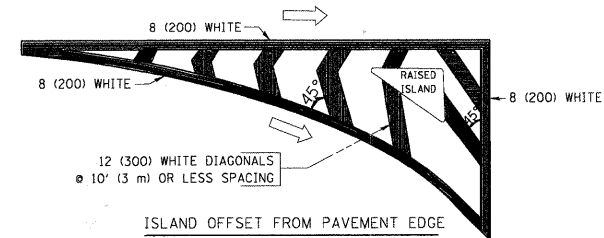
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

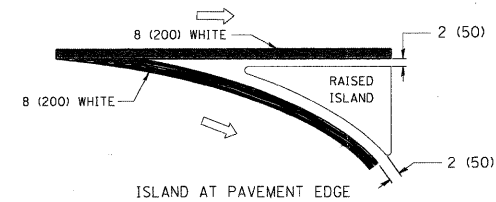


FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT; OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
as\pwork\pdat\drivakosgn\d108315\top3.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2666	09-00096-00-RS	LAKE	19	14

TC-13 CONTRACT NO.

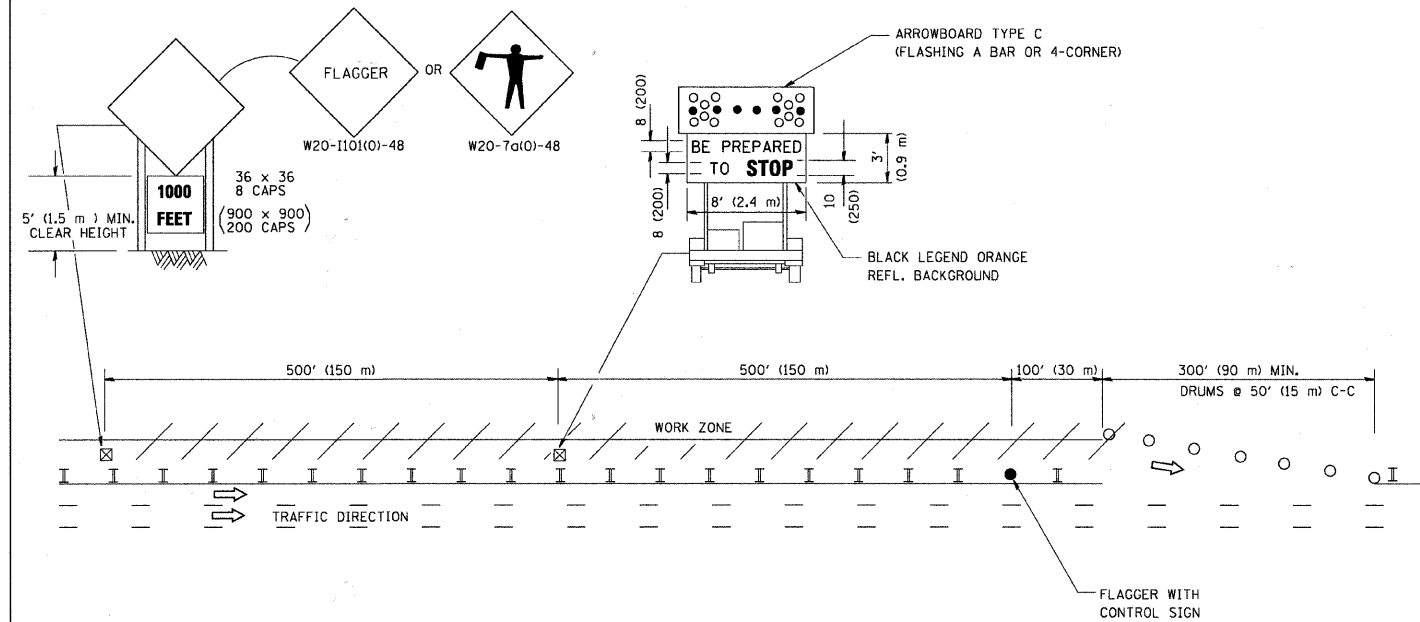
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(507)

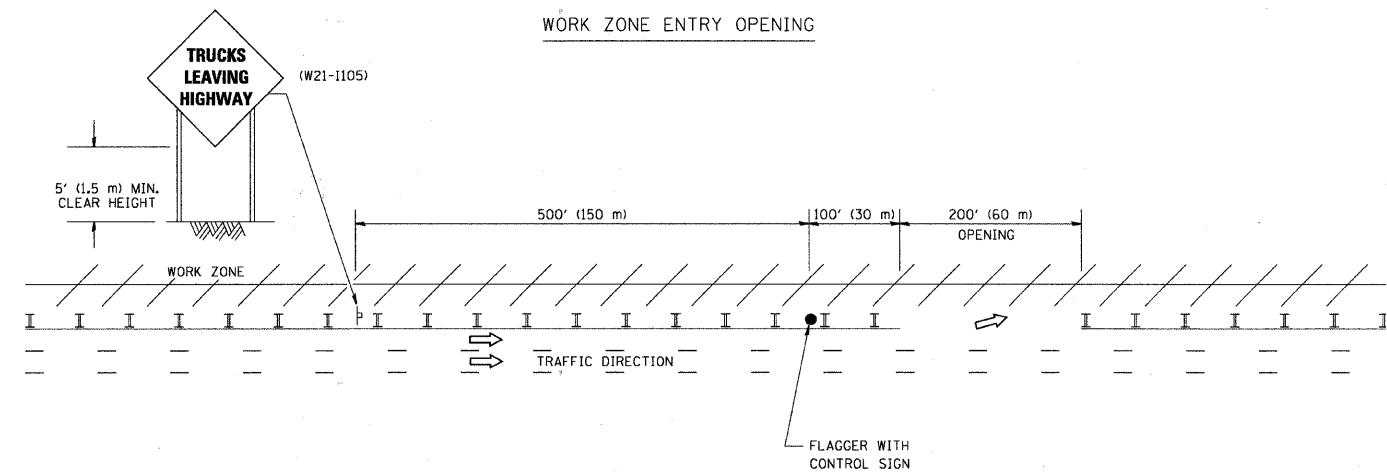
C-91-126-09

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

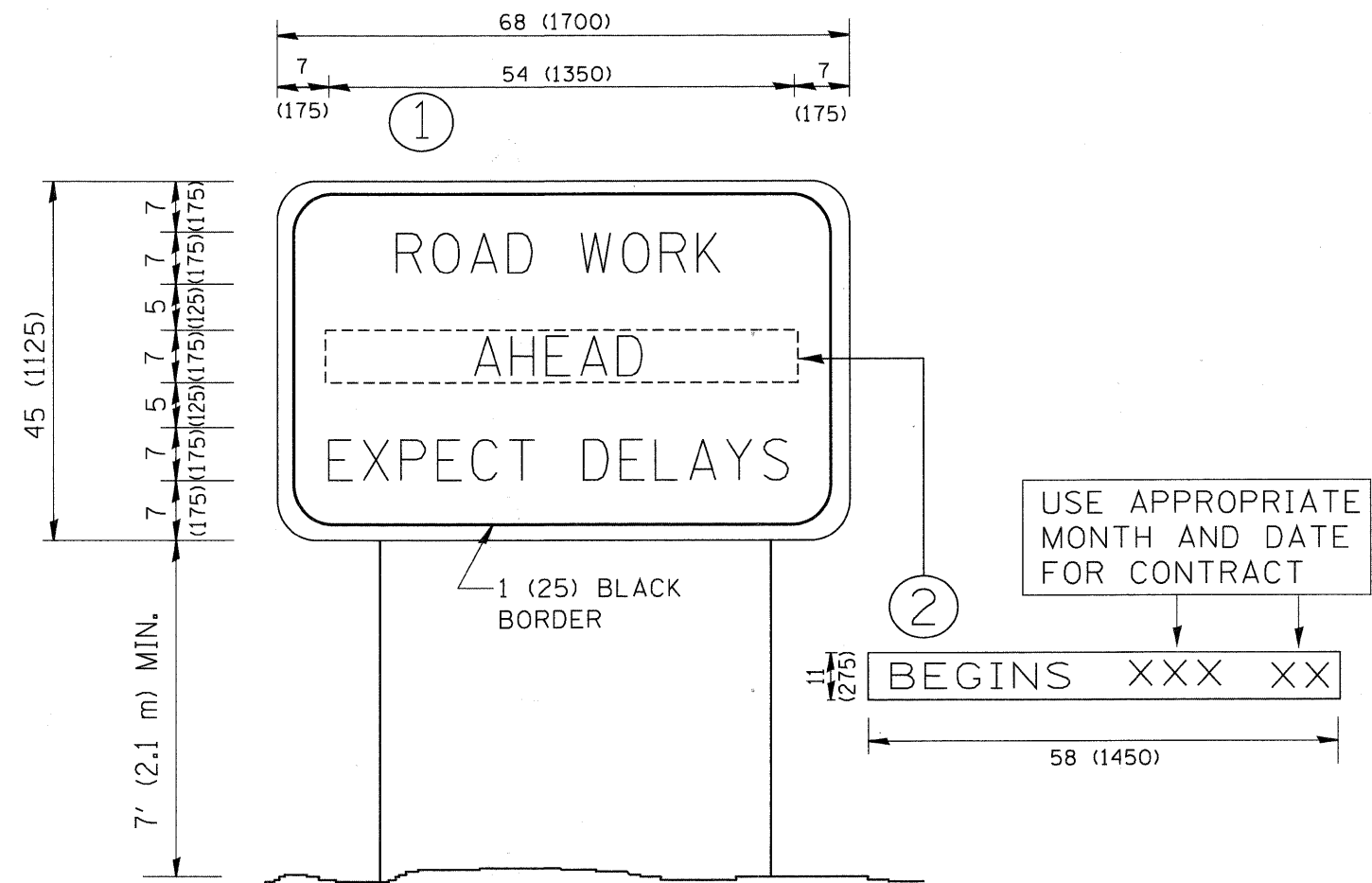
FILE NAME = W:\distatd\22x34\tcl8.dgn	USER NAME = leyo	DESIGNED -	REVISED - J.A.F. 04-03
		DRAWN -	REVISED - J.A.F. 02-06
		CHECKED -	REVISED - S.P.B. 01-07
		DATE -	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 2666	SECTION 09-00096-00-RS	COUNTY LAKE	TOTAL SHEETS 18	SHEET NO. 15
TC-18			CONTRACT NO. 63367	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT ARA-9003(507)				
C-91-126-10				



NOTES:

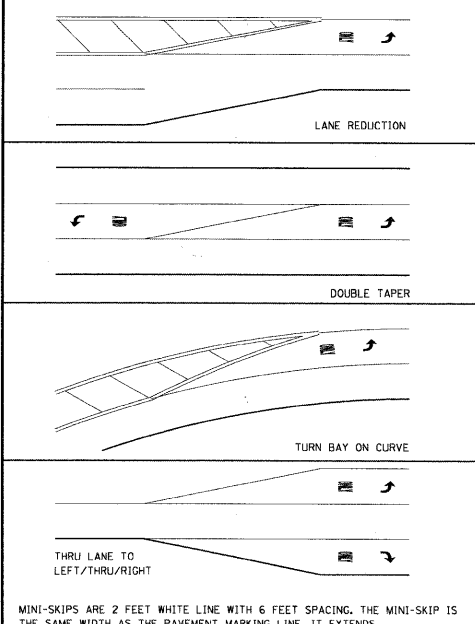
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\disto\22x34\to22.dgn	USER NAME = goglionob	DESIGNED - DRAWN -	REVISED - R. MIRS 09-15-97 REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	2666			09-00096-00-RS	LAKE	18	16	
PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUICIUS 01-31-07	TC-22			CONTRACT NO. 63367				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		

ROUTE	SECTION	COUNTY	SHEET	SHEETS
CH		LAKE		
TYPICAL PAVEMENT MARKINGS				
ILLINOIS				

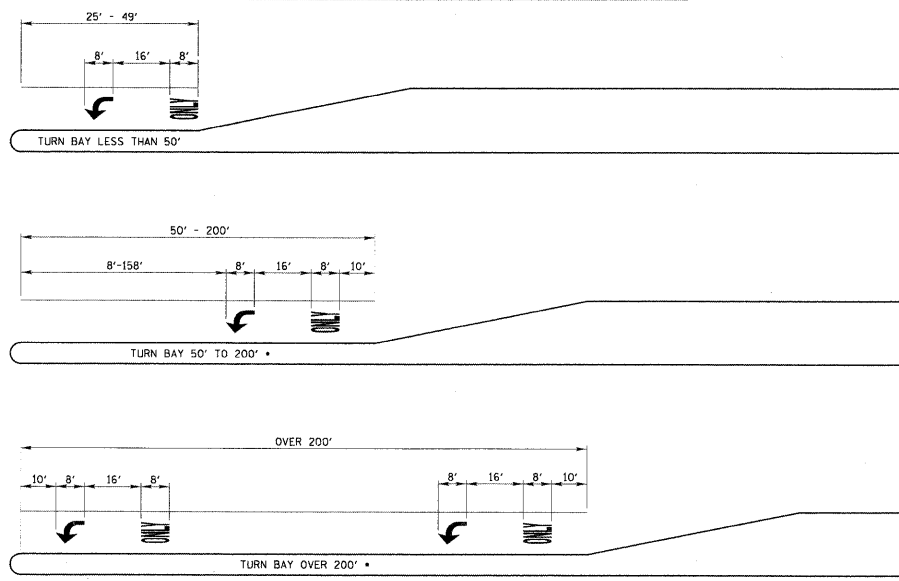
TYPICAL MINI-SKIP PAVEMENT MARKINGS



MINI-SKIPS ARE 2 FEET WHITE LINE WITH 6 FEET SPACING. THE MINI-SKIP IS THE SAME WIDTH AS THE PAVEMENT MARKING LINE, IT EXTENDS.

TYPICAL PAVEMENT MARKINGS

TYPICAL TURN BAY PAVEMENT MARKINGS



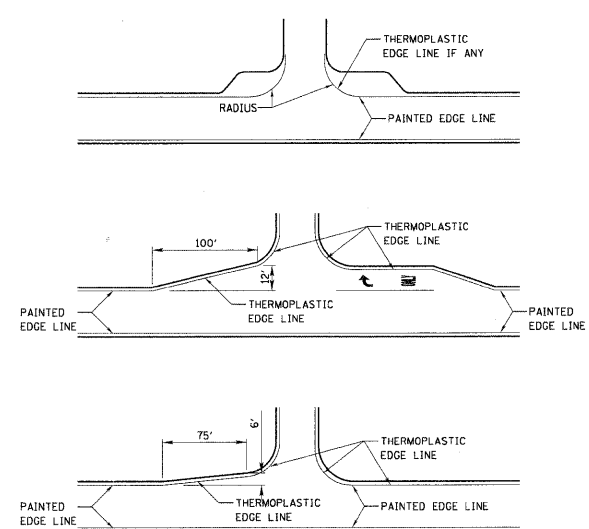
* AT INTERSECTIONS WITH VIDEO DETECTION, THE ARROW AND ONLY PAVEMENT MARKINGS SHALL BE A MINIMUM OF 30' BEHIND THE STOP BAR.

AREA = 15.6 SQ. FT.

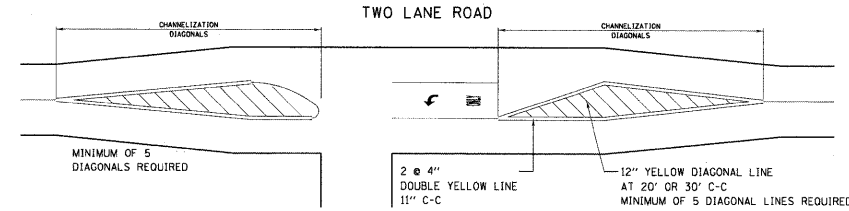
AREA = 20.8 SQ. FT.

FULL SIZE LETTERS (8") AND ARROWS SHALL BE USED. TURN LANES IN EXCESS OF 400' IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW W/ "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW W/ "ONLY".

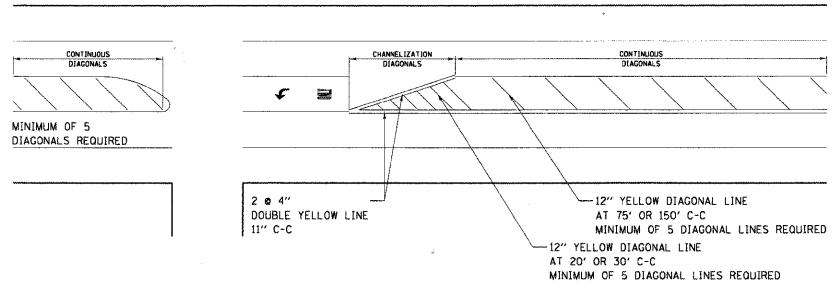
EDGE LINE RADII AT SIDE STREETS



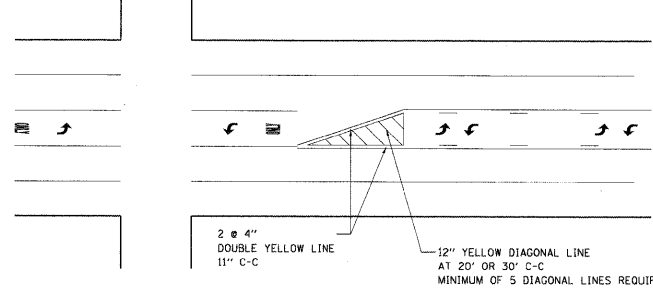
TYPICAL DIAGONAL SPACING



SPEED LIMIT RANGE	DIAGONAL SPACING	
	CONTINUOUS	INTERSECTION CHANNELIZATION
30-45 MPH	75 FT.	20 FT.
OVER 45 MPH	150 FT.	30 FT.



TWO-WAY LEFT TO LEFT TURN BAY



DUAL LEFT TURN ARROWS



A MINIMUM OF TWO PAIRS OF DUAL LEFT TURN ARROWS SHALL BE USED. THE DUAL LEFT TURN ARROWS SHALL BE WHITE IN COLOR. THE INTERVAL BETWEEN SETS OF DUAL LEFT TURN ARROWS SHOULD BE 200' AND 300'.

31.2 SQ. FT. MINIMUM OF 2 SETS REQUIRED

Lake County
Division of Transportation

TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS

REVISIONS	
NAME	DATE
JOHN SAUTER	7/7/98
JOHN SAUTER	11/01/00
JOHN SAUTER	5/4/05

SCALE: NONE SHEET 1 OF 3 DRAWN BY: JPS
DATE: JAN. 12, 1998 CHECKED BY: ANK

COPYRIGHT © 2005 BY BAXTER & WOODMAN, INC. ...
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00021 - EXPRESSES #252454
 07/27/2009 10:55:45 AM



DESIGNED - DSH	REVISED-10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED-
CHECKED - RWL	REVISED-
DATE - 10-09-09	FILE - 090803-PAVEMENT MARKINGS.SHT

**VILLAGE OF BUFFALO GROVE, ILLINOIS
MAIN STREET ARRA (LAPP)
RESURFACING**

LAKE COUNTY TYPICAL PAVEMENT MARKINGS

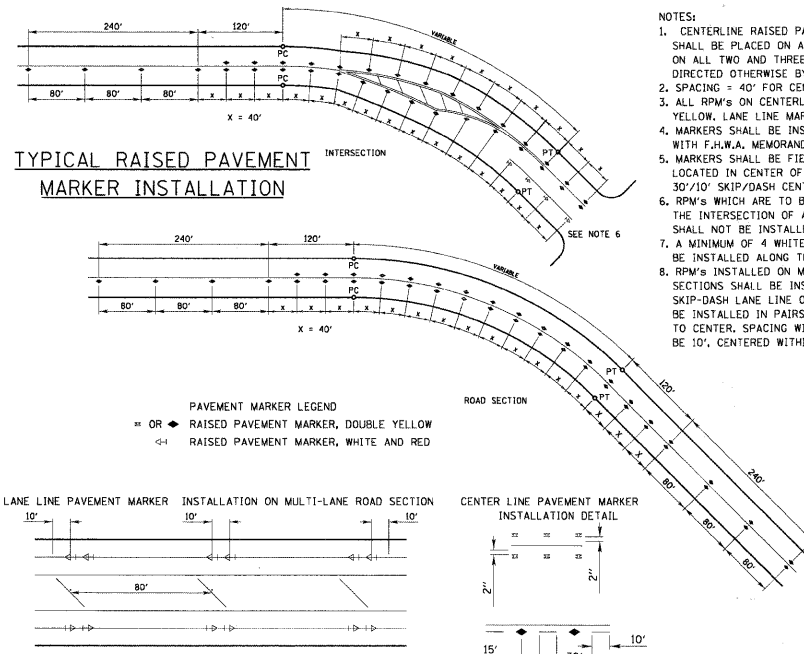
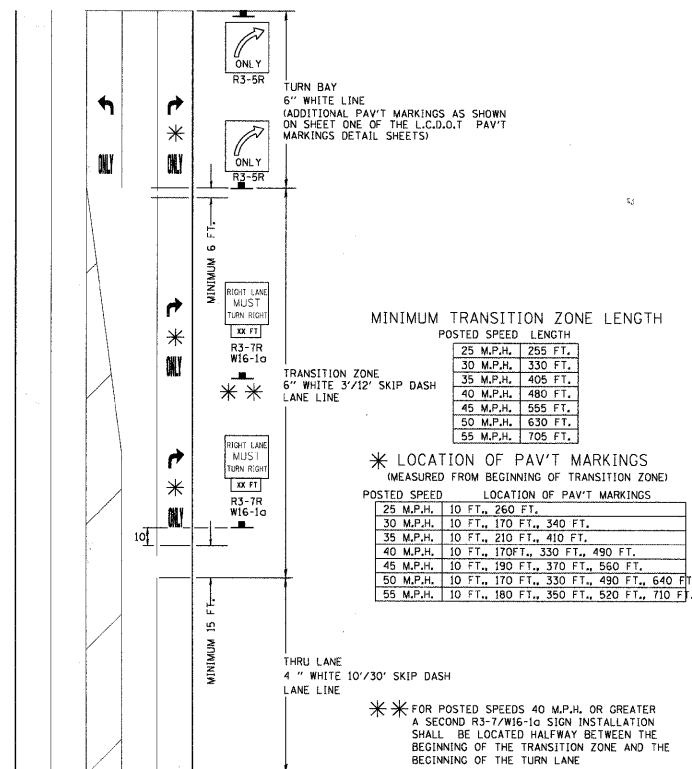
SCALE: STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2666	09-00096-00-RS	LAKE	18	17
C-91-126-10			CONTRACT NO. 63367	
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT ARA-90036507	

TYPICAL PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS

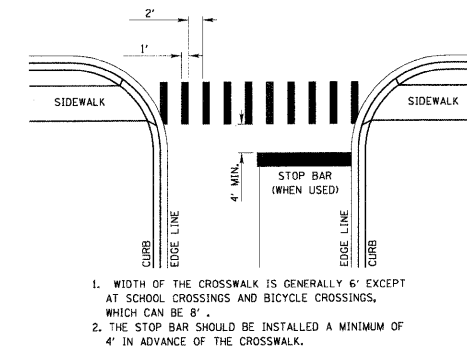
ROUTE	SECTION	COUNTY	SHEET	SHEETS
CH	LAKE			
TYPICAL PAVEMENT MARKINGS AND MARKERS				
ILLINOIS				

THRU LANE TO TURN LANE CONVERSION



- NOTES:
- CENTERLINE RAISED PAVEMENT MARKERS (RPM'S) SHALL BE PLACED ON ALL CURVES OVER 3 1/2 DEGREES ON ALL TWO AND THREE LANE HIGHWAYS, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - SPACING = 40' FOR CENTERLINE MARKERS.
 - ALL RPM'S ON CENTERLINE ARE 2-WAY YELLOW. LANE LINE MARKERS ARE WHITE/RED.
 - MARKERS SHALL BE INSTALLED IN ACCORDANCE WITH F.H.W.A. MEMORANDUM HD-21.
 - MARKERS SHALL BE FIELD ADJUSTED TO BE LOCATED IN CENTER OF THE 30' GAP OF A 30'/10' SKIP/DASH CENTERLINE.
 - RPM'S WHICH ARE TO BE LOCATED WITHIN THE INTERSECTION OF A CROSS STREET, SHALL NOT BE INSTALLED.
 - A MINIMUM OF 4 WHITE/RED MARKERS SHALL BE INSTALLED ALONG THE TURN LANE LINE.
 - RPM'S INSTALLED ON MULTI-LANE ROAD SECTIONS SHALL BE INSTALLED ON THE WHITE SKIP-DASH LANE LINE ONLY. THESE RPM'S SHALL BE INSTALLED IN PAIRS AND SHALL BE 80' CENTER TO CENTER. SPACING WITHIN EACH PAIR SHALL BE 10', CENTERED WITHIN THE 30' SKIP.

CROSSWALKS



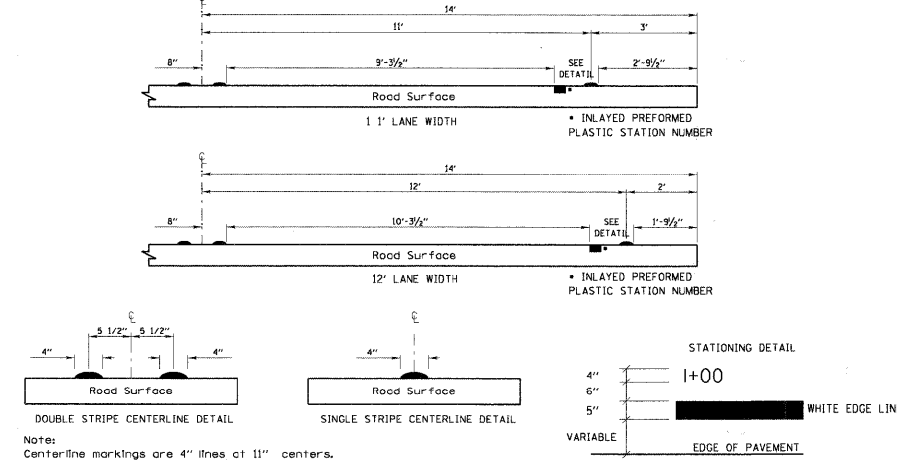
- WIDTH OF THE CROSSWALK IS GENERALLY 6' EXCEPT AT SCHOOL CROSSINGS AND BICYCLE CROSSINGS, WHICH CAN BE 8'.
- THE STOP BAR SHOULD BE INSTALLED A MINIMUM OF 4' IN ADVANCE OF THE CROSSWALK.

PAVEMENT MARKING GUIDELINES - ENGLISH MEASUREMENTS

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE OF 2 LANE PAVEMENT	4 IN.	SKIP-DASH	YELLOW	10 FT. LINE WITH 30 FT. SPACE
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 IN.	SOLID	YELLOW	5 1/2 IN. C.C. FROM SKIP-DASH CENTERLINE
FOR BOTH DIRECTIONS	2 @ 4 IN.	SOLID	YELLOW	11 IN. C.C. (OMIT SKIP-DASH CENTERLINE BETWEEN)
CENTERLINE ON MULTI-LANE UNDIVIDED	2 @ 4 IN.	SOLID	YELLOW	11 IN. C.C.
LANE LINES	4 IN.	SKIP-DASH	WHITE	10 FT. LINE WITH 30 FT. SPACE
DOTTED LINES (EXTENSIONS OF CENTERLINE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2 FT. LINE WITH 6 FT. SPACE
EDGE LINES	5 IN. WHITE / 4 IN. YELLOW	SOLID	WHITE - RIGHT / YELLOW - LEFT	OUTLINE RAISED MEDIANS IN YELLOW
TURN LANE MARKINGS	6 IN. LINE FULL SIZE LETTERS AND SYMBOLS 18 FT. J.	SOLID	WHITE	TURN ARROW 15.5 SO. FT. / STRAIGHT ARROW 11.5 SO. FT. / ONLY 20.0 SO. FT. / COMB. ARROW 26.0 SO. FT.
TWO WAY LEFT TURN MARKING	2 @ 4 IN. EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10 FT. LINE WITH 30 FT. SPACE FOR SKIP-DASH / 5 1/2 IN. C.C. BETWEEN SKIP-DASH LINE AND SOLID LINE
CROSSWALK	12 IN. @ 90°	SOLID	WHITE	12 IN. LONGITUDINAL BAR WITH 24 IN. SPACE / 5 FT. TO 12 FT. WIDE / SEE TYPICAL CROSSWALK MARKING DETAIL
STOP BARS	24 IN.	SOLID	WHITE	PLACE 4 FT. IN ADVANCE OF AND PARALLEL TO CROSSWALK IF PRESENT OTHERWISE PLACE AT DESIRED STOPPING POINT
PAINTED MEDIANS	2 @ 4 IN. WITH 11 IN. DIAGONALS @ 45° / NO DIAGONALS USED FOR 4 FT. WIDE MEDIAN	SOLID	YELLOW - 2WAY TRAFFIC / WHITE - 1WAY TRAFFIC	11 IN. C.C. FOR THE DOUBLE LINE / SEE TYPICAL PAINTED MEDIAN MARKING DETAIL / MINIMUM OF 5 DIAGONALS
CORNER MARKING AND CHANNELIZING LINES	8 IN. WITH 12 IN. DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS 15 FT. C.C. (LESS THAN 30 MPH) / 20 FT. C.C. (30 TO 45 MPH) / 30 FT. C.C. (OVER 45 MPH) / MINIMUM OF 5 DIAGONALS
P.R. CROSSING	24 IN. TRANSVERSE LINES / RR IS 6 FT. LETTER / 16 IN. LINE FOR "R"	SOLID	WHITE	SEE I.D.D.T. STD. 78000 / SOFT AREA OF 1' x 16 SO. FT. / "R" 5" x 540 SO. FT.
SHOULDER DIAGONALS	12 IN. @ 45°	SOLID	WHITE - RIGHT / YELLOW - LEFT	50 FT. C.C. (LESS THAN 30 MPH) / 75 FT. C.C. (30 TO 45 MPH) / 150 FT. C.C. (OVER 45 MPH) / MINIMUM OF 5 DIAGONALS

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO PART III MARKINGS IN THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" AND I.D.D.T. HIGHWAY STANDARD 78000 EFFECTIVE JAN. 9, 1998.

PAVEMENT CROSS SECTION SHOWING TYPICAL PAVEMENT MARKINGS (2-LANE ROADWAY)



- NOTES:
- PAINT CURB AND NOSE SOLID FOR 10' OR RADIUS OF NOSE, WHICHEVER IS GREATER.
 - PAINT MINIMUM OF 3 STRIPES IN DIRECTION OF TRAFFIC.
 - REDUCED SPACING USED TO OBTAIN 3 STRIPE MINIMUM.
 - STRIPING RECOMMENDED ONLY WHERE OPERATIONAL PROBLEMS DICTATE.
 - PAINT SOLID WHERE A MINIMUM OF 3 STRIPES CANNOT BE PLACED.

Revisions

NAME	DATE
JOHN SAUTER	7/7/99
JOHN SAUTER	1/10/03
JOHN SAUTER	1/31/04
JOHN SAUTER	5/4/05
JOHN SAUTER	5/24/05

TYPICAL PAVEMENT MARKINGS FOR COUNTY HIGHWAYS

SCALE: NONE SHEET 2 OF 3 DRAWN BY: JPS
DATE: JAN. 12, 1998 CHECKED BY: ANK

COPYRIGHT © 2009 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00121 - EXPIRES 4/30/2011
 1107/2209
 1107/2209



DESIGNED - DSH	REVISED - 10-09-09 PER IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-09-09	FILE - 090803-PAVEMENT MARKINGS.SHT

VILLAGE OF BUFFALO GROVE, ILLINOIS MAIN STREET ARRA (LAPP) RESURFACING

LAKE COUNTY TYPICAL PAVEMENT MARKINGS

SCALE:	STA.	TO STA.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			2666	09-00096-00-RS	LAKE	18	18
			C-91-126-10			CONTRACT NO. 63367	
			FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	ARA-9003507	