

FOR INDEX OF SHEETS,  
SEE SHEET NO. 2

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PLANS FOR PROPOSED**  
**FEDERAL AID HIGHWAY**  
FAU 2321 (DIETRICH ROAD)  
FROM FAP 525 (US ROUTE 20)  
TO 736' EAST OF FAP 525 (US ROUTE 20)  
LAPP RESURFACING  
SECTION NO: 10-00018-00-RS  
PROJECT NO: ARA-9003 (616)  
VILLAGE OF HAMPSHIRE  
KANE COUNTY  
C-91-481-10

F.A.U. B.Y.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	1

CONTRACT NO. 63460

PROJECT IS LOCATED IN  
THE VILLAGE OF HAMPSHIRE

**DESIGN DESIGNATION**

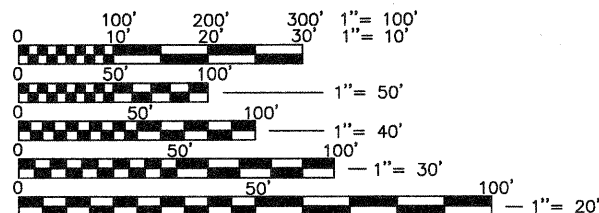
DIETRICH ROAD: MINOR ARTERIAL URBAN  
DESIGN SPEED = 40 M.P.H.  
POSTED SPEED = 40 M.P.H.  
ADT (2007) = 750 VPD



LOCATION OF SECTION INDICATED THUS: [Symbol]

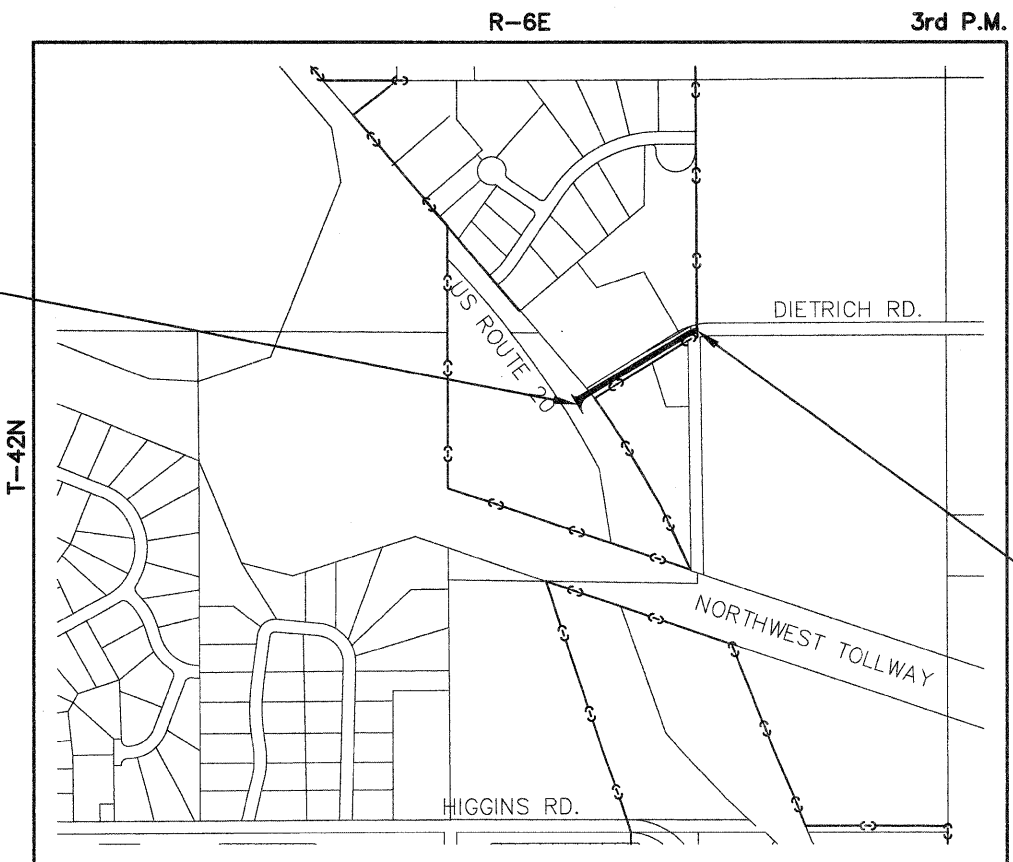
FIELD ENGINEER: MARLIN SOLOMON (847) 705-4407

CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC.  
CONTACT: JULIE MORRISON (630)-466-6700



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

BEGIN  
IMPROVEMENTS  
STA 10+00  
DIETRICH ROAD

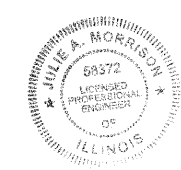


END  
IMPROVEMENTS  
STA 17+36  
DIETRICH ROAD

HAMPSHIRE TOWNSHIP W 1/2, SW 1/4, SECTION 2

LOCATION MAP  
N.T.S.

NET LENGTH OF PROJECT = 736 FEET (0.14 MILES)  
GROSS LENGTH OF PROJECT = 736 FEET (0.14 MILES)



**ENGINEER'S CERTIFICATION**  
I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED UNDER MY DIRECT SUPERVISION.  
DATED AT SUGAR GROVE, ILLINOIS,  
THIS 12<sup>th</sup> DAY OF May, 2010.  
*Julie A. Morrison*  
JULIE A. MORRISON, P.E.  
ILLINOIS LICENSED PROFESSIONAL ENGINEER NO. 062-058372  
EXPIRATION DATE: 11/30/11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED: MAY 13, 2010  
*Eric Johnson*  
HAMPSHIRE VILLAGE ADMINISTRATOR

PASSED: MAY 27, 2010  
*Chris Hebert*  
DISTRICT ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW: MAY 27, 2010  
*Dina M. O'Leary*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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**JULIE JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811**

Know what's below.  
Call before you dig.

CONTRACT NO. 63460

**Engineering Enterprises, Inc.**  
CONSULTING ENGINEERS  
52 Wheeler Road  
Sugar Grove, Illinois 60554  
Phone: (630) 466-6700

Plotted: May 11, 2010 @ 9:13 AM By: Larry Nolan - Tab: 01\_CVR\_22634

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	2
CONTRACT NO. 63460				

**GENERAL NOTES AND CONSTRUCTION SPECIFICATIONS**

- THE CONTRACTOR SHALL OBTAIN, ERECT, MAINTAIN AND REMOVE ALL SIGNS, BARRICADES, FLAGGERS AND OTHER TRAFFIC CONTROL DEVICES AS MAY BE NECESSARY FOR THE PURPOSE OF REGULATING, WARNING OR GUIDING TRAFFIC. PLACEMENT AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PARTS OF ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS AND THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS.
- LOCATION OF UTILITIES SHOWN ON PLANS ARE APPROXIMATE ONLY, AND ARE NOT NECESSARILY COMPLETE. CONTRACTOR SHALL MAKE HIS OWN INVESTIGATIONS AS TO LOCATION OF ALL EXISTING UNDERGROUND STRUCTURES, CABLES AND PIPE LINES.
- ALL ELEVATIONS ARE BASED ON U.S.G.S. DATUM.
- ALL ROAD SIGNS, STREET SIGNS AND TRAFFIC SIGNS THAT NEED TO BE RELOCATED OR MOVED DUE TO CONSTRUCTION SHALL BE TAKEN DOWN AND STORED BY THE CONTRACTOR EXCEPT THOSE THAT ARE NECESSARY FOR PROPER TRAFFIC CONTROL AND SHALL BE TEMPORARILY RESET UNTIL COMPLETION OF CONSTRUCTION OPERATIONS. AFTER COMPLETION OF THE WORK, THE CONTRACTOR SHALL RESET ALL SAID SIGNS. THE WORK SHALL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS, AND SHALL NOT BE PAID FOR SEPARATELY.
- ARTICLE 105.06 OF THE STANDARD SPECIFICATIONS REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT, SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVE CONTROL OF ALL WORK AS THE AGENT OF THE CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 108.08.
- ALL REFERENCES TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE DEPARTMENT AS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- THE UNIT PRICE FOR ALL REMOVAL PAY ITEMS SHALL INCLUDE ALL REQUIRED SAW CUTS.
- ALL WORK PERFORMED RELATIVE TO THIS IMPROVEMENT SHALL COMPLY WITH ALL APPLICABLE RULES AND REGULATIONS OF O.S.H.A. IN ACCORDANCE WITH ARTICLE 107.01.
- CONTRACTOR SHALL PROVIDE AND INSTALL TWO WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED. (ONE WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL) THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT THE COST SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR THE CONSTRUCTION ITEMS INVOLVED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS: AN ESTIMATED QUANTITY BASED ON FIELD OBSERVATION HAS BEEN INCLUDED IN THE CONTRACT. THE LOCATION AND LIMITS OF ALL JOINT OR CRACK FILLING SHALL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION. IF THE MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS IS NOT REQUIRED, THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
- BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- 10 FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY OWNERS AND THE VILLAGE OF HAMPSHIRE.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE THE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

- WHENEVER, DURING CONSTRUCTION OPERATIONS LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT PAY ITEMS.
- EXISTING PUBLIC AND PRIVATE UTILITIES ARE SHOWN ON THE PLANS ACCORDING TO INFORMATION OBTAINED FROM UTILITY COMPANIES, MUNICIPALITIES, AND SURVEYS. THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH THE LOCATION OF ALL UTILITIES AND STRUCTURES THAT MAY BE FOUND IN THE VICINITY OF THE CONSTRUCTION. HE WILL ALSO ASSUME RESPONSIBILITY FOR ALL UTILITIES WHETHER SHOWN OR NOT, IN ACCORDANCE WITH ARTICLE 107.31 AND MUST REALIZE THAT THE ACTUAL LOCATIONS AND/OR ELEVATIONS OF THE UTILITIES MAY BE DIFFERENT THAN INDICATED.  
  
SHOULD ANY DAMAGES OCCUR DUE TO THE CONTRACTOR'S NEGLIGENCE, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL MAKE REPAIRS IN A MANNER ACCEPTABLE TO THE ENGINEER. THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS OF HIS CONSTRUCTION SCHEDULE AND COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY OWNERS SO THAT RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER.
- DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWER, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE SHALL INFORM THE ENGINEER WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR TO REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF TO BE MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE IN ACCORDANCE WITH ARTICLES 107.20 & 105.07.
- DIETRICH ROAD SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. WHEN NECESSARY TO CLOSE ONE LANE OF DIETRICH ROAD DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE-WAY TRAFFIC DURING CONSTRUCTION HOURS WITH THE USE OF SIGNS AND FLAGMEN AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. TWO LANES OF TRAFFIC SHALL BE MAINTAINED AT ALL OTHER TIMES WHEN NON-CROSSING WORK OR NO CONSTRUCTION ACTIVITIES ARE BEING CARRIED ON. ACCESS TO ALL ADJOINING PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. ALL TEMPORARY ACCESS CLOSURES SHALL BE COORDINATED WITH THE AFFECTED PROPERTY OWNERS.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE ENGINEER AND ALL PROPERTY OWNERS PRIOR TO A TEMPORARY LOSS OF ACCESS TO THEIR HOUSEHOLD OR BUSINESS, IN WRITING, A MINIMUM OF 24 HOURS BUT NOT MORE THAN 48 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORNING OF THE WORK, THE CONTRACTOR WILL AGAIN BE REQUIRED TO NOTIFY EACH PROPERTY OWNER VERBALLY. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHOULD PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BE RESTORED. THE NOTICE SHALL BE PROVIDED TO THE ENGINEER FOR REVIEW PRIOR TO DISTRIBUTION. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW FOR WORK TO BE DONE IN A TIMELY MATTER TO PERMIT ACCESS TO THE ROADWAY.
- REPLACE NOTE 3 UNDERNEATH TABLE 1 OF ARTICLE 406.07(A) OF THE STANDARD SPECIFICATIONS WITH THE FOLLOWING. "THE CONTRACTOR HAS THE OPTION TO USE A VIBRATORY ROLLER (VD) IN LIEU OF A PNEUMATIC TIRE ROLLER."
- THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

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SHEET NO.	SHEET DESCRIPTION
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4.	TYPICAL EXISTING SECTIONS AND REMOVAL WORK
5.	TYPICAL PROPOSED SECTIONS
6.	SCHEDULE OF QUANTITIES
7.	PLAN, CONSTRUCTION NOTES AND DETAILS
<b>DISTRICT ONE DETAILS</b>	
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9.	(TC-13) DISTRICT ONE - TYPICAL PAVEMENT MARKINGS
10.	(TC-22) ARTERIAL ROAD INFORMATION SIGN
11.	(BD-8) FRAMES AND LIDS ADJUSTMENT WITH MILLING
12.	(BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
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14.	(BD-32) BUTT JOINT AND HMA TAPER DETAILS

**STATE STANDARDS**

STD. NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS

Printed: May 24, 2010 @ 10:50 AM By: Larry Nelson - Tab: 02 Notes 22x34  
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**VILLAGE OF HAMPSHIRE**  
 KANE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

**DIETRICH ROAD**  
**LAPP IMPROVEMENTS**

**GENERAL NOTES, STATE**  
**STANDARDS AND INDEX OF SHEETS**

DATE:	JANUARY 2010
PROJECT NO:	HA0957
FILE:	HA0957-CVR
SHEET	<b>2</b> OF <b>14</b>

**SUMMARY OF QUANTITIES**

CODE NO.	PAY ITEM	UNIT	CONST. CODE
			1000
			TOTAL QUANTITY
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	554
40600300	AGGREGATE (PRIME COAT)	TON	7
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	2
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	257
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	90
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	414
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	3,673
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	105
44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	6
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	48
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	16
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	246
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	3,673
Δ 48101900	AGGREGATE SHOULDERS (SPECIAL)	TON	35
Δ 60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	1
67100100	MOBILIZATION	L SUM	1
Δ 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
Δ 70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	2
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	250
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	2,943
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	80
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	72.8
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2,943
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	250
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	63
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	30
Δ X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	77.1
Δ XX006425	RESTORATION	SQ YD	23

Δ SEE SPECIAL PROVISIONS  
\* SPECIALITY ITEMS

Plotted: June 1, 2010 @ 3:35 PM By: Larry Nolan - Tab: 03 Quantities 22x34

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Sugar Grove, Illinois 60554  
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**VILLAGE OF HAMPSHIRE**  
KANE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

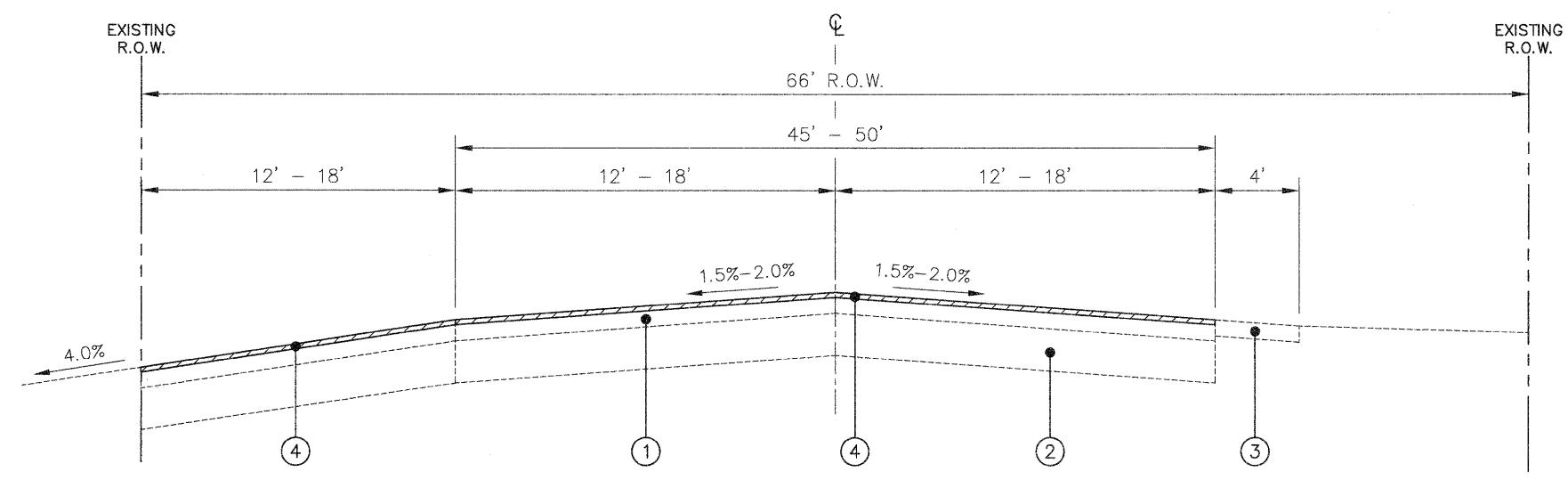
**DIETRICH ROAD**  
**LAPP IMPROVEMENTS**

**SUMMARY OF QUANTITIES**

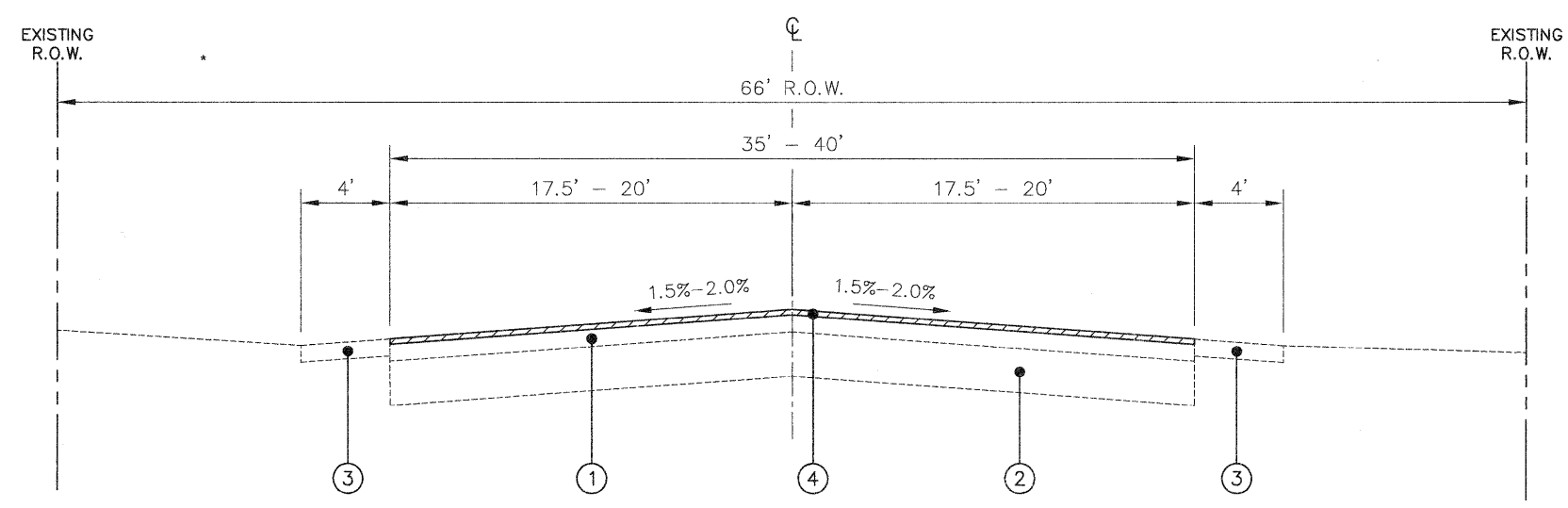
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PROJECT NO:	HA0957
FILE:	HA0957-CVR
SHEET	<b>3</b> OF <b>14</b>

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	4
CONTRACT NO. 63460				



**EXISTING TYPICAL SECTION  
STA 10+00 TO STA 15+43, DIETRICH ROAD**  
N.T.S.



**EXISTING TYPICAL SECTION  
STA 15+43 TO STA 17+36, DIETRICH ROAD**  
N.T.S.

LEGEND	
①	EXISTING ASPHALT PAVEMENT, 6"
②	EXISTING AGGREGATE BASE, 12"
③	EXISTING AGGREGATE SHOULDER
④	HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"
⑤	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2.0"
⑥	LEVELING BINDER (MACHINE METHOD), N50, 1.25"
⑦	AGGREGATE SHOULDERS (SPECIAL)
⑧	BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)

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 KANE COUNTY, ILLINOIS

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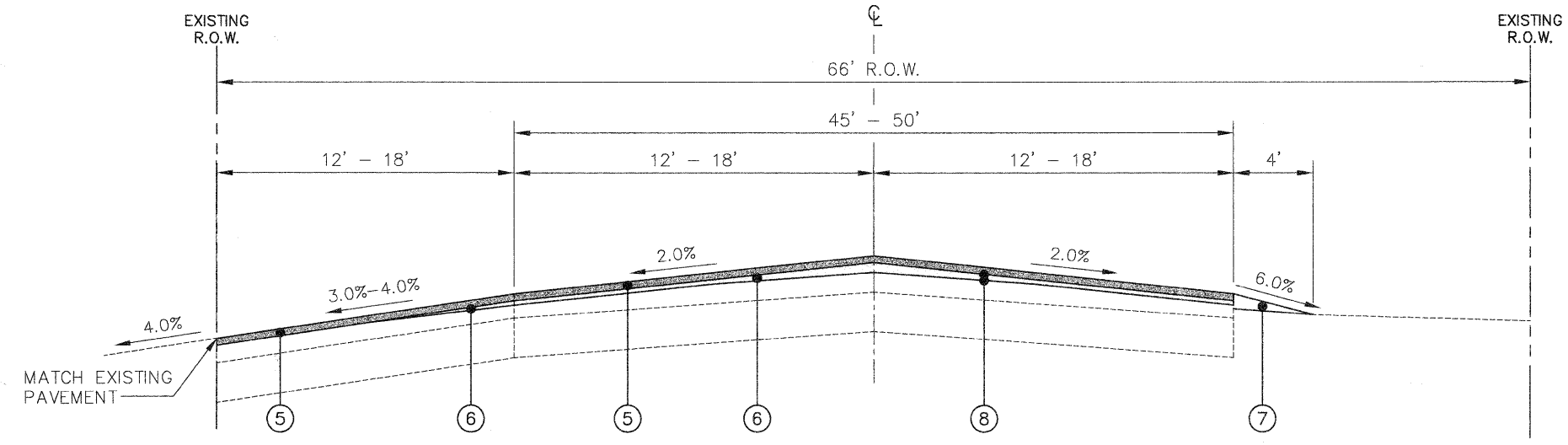
**DIETRICH ROAD**  
**LAPP IMPROVEMENTS**

**TYPICAL EXISTING SECTIONS**  
**AND REMOVAL WORK**

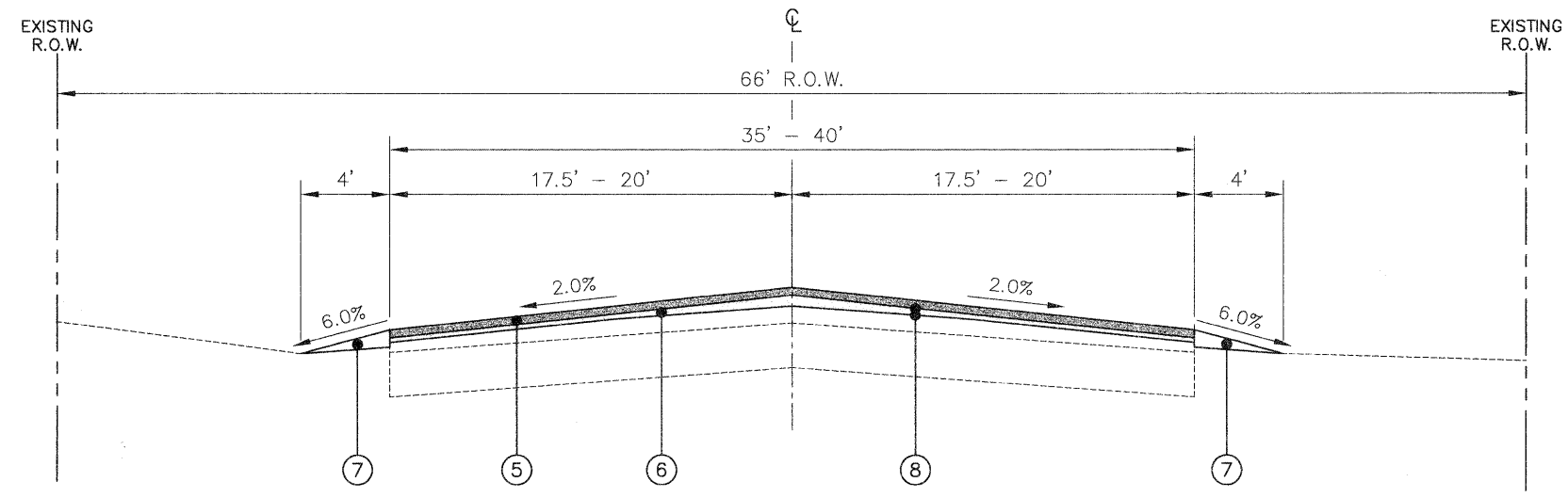
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PROJECT NO:	HA0957
FILE:	HA0957-CVR
SHEET	<b>4</b> OF <b>14</b>

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	5
CONTRACT NO. 63460				



**PROPOSED TYPICAL SECTION  
STA 10+00 TO STA 15+43, DIETRICH ROAD**  
N.T.S.



**PROPOSED TYPICAL SECTION  
STA 15+43 TO STA 17+36, DIETRICH ROAD**  
N.T.S.

LEGEND	
① EXISTING ASPHALT PAVEMENT, 6"	⑤ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2.0"
② EXISTING AGGREGATE BASE, 12"	⑥ LEVELING BINDER (MACHINE METHOD), N50, 1.25"
③ EXISTING AGGREGATE SHOULDER	⑦ AGGREGATE SHOULDERS (SPECIAL)
④ HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"	⑧ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)

CONTRACTOR SHALL MILL BEFORE PATCHING.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOID @ Ndes
<b>PAVEMENT RESURFACING</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL-9.5 MM), 2"	4% @ 50 GYR.
LEVELING BINDER (MACHINE METHOD), N50 (IL-9.5 MM), 1 1/4"	4% @ 50 GYR.
<b>PATCHING</b>	
CLASS D PATCHES (HMA BINDER IL-19.0 MM)	4% @ 70 GYR.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ON SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT QUANTITIES IS 112 LBS/SQ.YD./INCH.

Plotfile: May 24, 2010 @ 10:50 AM By: Larry Nelson - Tab: 05 Typ: Sections 22x34

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**VILLAGE OF HAMPSHIRE**  
KANE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

**DIETRICH ROAD  
LAPP IMPROVEMENTS**

**TYPICAL PROPOSED  
SECTIONS**

DATE:	JANUARY 2010
PROJECT NO:	HA0957
FILE:	HA0957-CVR
SHEET	<b>5</b> OF <b>14</b>

Path: \\SBSRPROG\100957\DWG\100957-CVR

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	6

CONTRACT NO. 63460

PAVEMENT QUANTITIES SCHEDULE

STA	LOCATION	PAVEMENT WIDTH (FT)	PAVEMENT LENGTH (FT)	PAVEMENT AREA (SQ FT)	PAVEMENT AREA (SQ YD)	40600625 LEVEL BIND (MM), N50 (TON)	40603310 HMA SURF. CSE, MIX "C", N50 (TON)	40600100 BIT MATL. PRIME COAT (GALLON)	40600300 AGGREGATE (PR. COAT) (TON)	44000155 HMA SURFACE REMOVAL, 1 1/2" (SQ YD)	48101900 SHOULDERS (SPECIAL) AGGREGATE (TON)
10+00 to 11+00	MAINLINE	50	100	5,000	556	39	62	83	1	556	4
11+00 to 12+00	MAINLINE	50	100	5,000	556	39	62	83	1	556	4
12+00 to 13+00	MAINLINE	50	100	5,000	556	39	62	83	1	556	4
13+00 to 14+00	MAINLINE	48	100	4,800	533	37	60	80	1	533	4
14+00 to 15+00	MAINLINE	45	100	4,500	500	35	56	75	1	500	4
15+00 to 16+00	MAINLINE	40	100	4,000	444	31	50	67	1	444	6
16+00 to 17+00	MAINLINE	35	100	3,500	389	27	44	58	1	389	8
17+00 to 17+36	MAINLINE	35	36	1,260	140	10	16	21	0	140	3
10+50 RT	FIELD ENTRANCE	12	21	252	28	0	3	3	0	0	0
<b>TOTALS</b>						<b>257</b>	<b>414</b>	<b>554</b>	<b>7</b>	<b>3,673</b>	<b>35</b>

CURB AND GUTTER REMOVAL AND REPLACEMENT SCHEDULE

STA	SIDE	44001700 CURB & GUTTER REM. & REPL. LENGTH (FT)
10+50	LT	105
<b>TOTALS</b>		
		<b>105</b>

RESTORATION SCHEDULE

STA/LOCATION	RESTORATION WIDTH (FT)	RESTORATION LENGTH (FT)	RESTORATION AREA (SQ FT)	RESTORATION AREA (SQ YD)
BEHIND CURB AND GUTTER REMOVAL	2	105	210	23
<b>TOTALS</b>				
				<b>23</b>

PAVEMENT MARKINGS SCHEDULE

TYPE	STA / DESCRIPTION	LOCATION	THERMOPLASTIC PAVEMENT MARKINGS					LETTERS & SYMBOLS ARROW & ONLY WHITE SQ FT	TEMPORARY PAVEMENT MARKING	SHORT TERM PAVEMENT MARKING	WORKZONE PAVEMENT MARKING REMOVAL
			4" YELLOW LF	4" WHITE LF	6" WHITE LF	12" WHITE LF	24" WHITE LF				
TEMP PVT MKING	10+00 TO 17+33	CONTINUOUS EXCEPT INTERSECTIONS									
SHORT TERM PVT MKING	10+00 TO 17+34	AS NEEDED							250		
PVT MKING REMOVAL	10+00 TO 17+35	AS NEEDED								80	
4" PVT MKING	10+00 TO 17+36	EDGE & CENTERLINE	1,438	1,505				2,943			
6" PVT MKING	10+18 TO 14+18	LEFT TURN LANE			250						
12" PVT MKING	14+00 TO 17+36	RIGHT SHOULDER				63					
24" PVT MKING	10+18	STOP BAR					30				
LETTERS & SYMBOLS	10+18 TO 12+00	LEFT TURN LANE					72.8				
<b>TOTALS</b>			<b>2,943</b>	<b>250</b>	<b>63</b>	<b>30</b>	<b>73</b>	<b>2,943</b>	<b>250</b>	<b>80</b>	

CLASS D PATCHES SCHEDULE

STA	OFFSET	WIDTH (FT)	LENGTH (FT)	PATCH AREA (SQ FT)	PATCH AREA (SQ YD)	44201713	44201717	44201721	44201723
						CLASS D PATCHES TYPE I, 6 INCH (SQ YD)	CLASS D PATCHES TYPE II, 6 INCH (SQ YD)	CLASS D PATCHES TYPE III, 6 INCH (SQ YD)	CLASS D PATCHES TYPE IV, 6 INCH (SQ YD)
10+40	10' RT	6	11	66	7				
10+70	0	34	35	1190	132				132
11+25	5' RT	13	40	520	58				58
11+80	12' RT	3	5	15	2	2			
11+83	12' RT	4	15	60	7		7		
12+15	9' RT	7	20	140	16			16	
12+16	8' LT	6	11	66	7		7		
12+35	12' LT	3	6	18	2	2			
12+90	6' RT	8	31	248	28				28
14+45	7' RT	7	36	252	28				28
14+60	15' LT	10	12	120	13			13	
14+80	0	5	24	120	13			13	
14+90	15' LT	5	5	25	3				
<b>TOTALS</b>						<b>6</b>	<b>48</b>	<b>16</b>	<b>246</b>

Plotted: June 1, 2010 @ 3:44 PM By: Larry Nolan - Tab: 05 Sched Quant 22x34

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**VILLAGE OF HAMPSHIRE**  
 KANE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

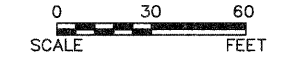
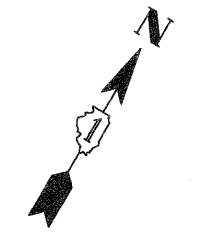
**DIETRICH ROAD**  
**LAPP IMPROVEMENTS**

**SCHEDULE OF QUANTITIES**

DATE:	JANUARY 2010
PROJECT NO:	HA0957
FILE:	HA0957-CVR
SHEET	<b>6</b> OF <b>14</b>

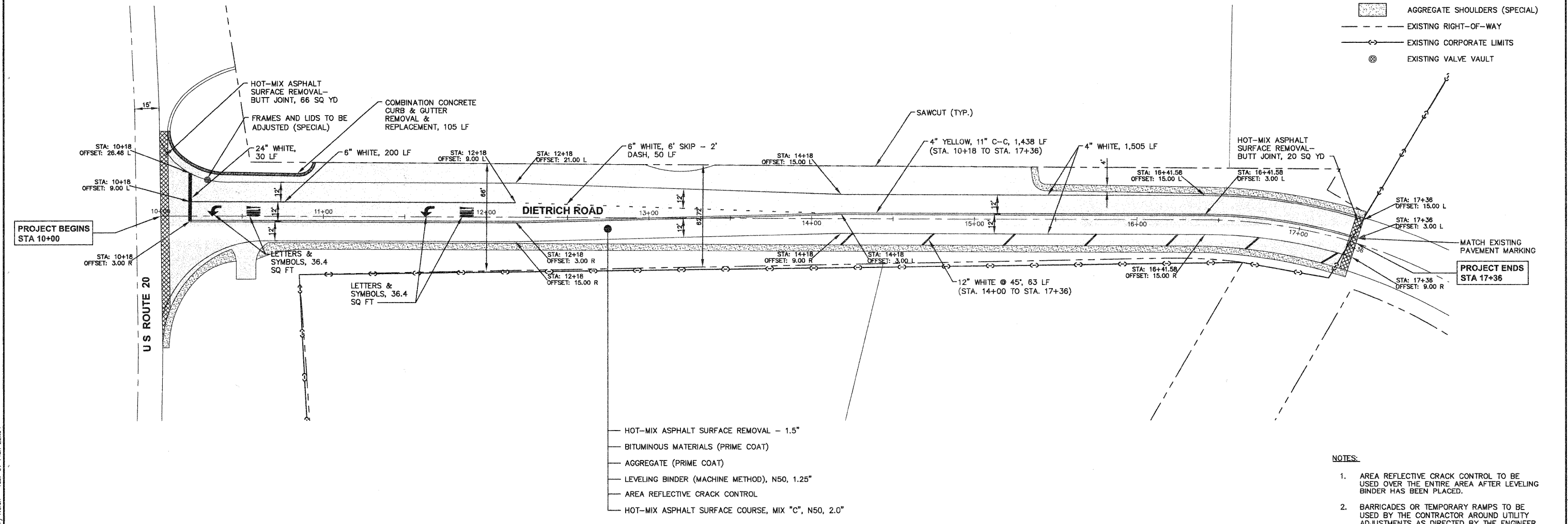
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	7
CONTRACT NO. 63460				



**LEGEND**

- RESURFACING
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- CURB & GUTTER REMOVAL AND REPLACEMENT
- AGGREGATE SHOULDERS (SPECIAL)
- EXISTING RIGHT-OF-WAY
- EXISTING CORPORATE LIMITS
- EXISTING VALVE VAULT



- NOTES:**
- AREA REFLECTIVE CRACK CONTROL TO BE USED OVER THE ENTIRE AREA AFTER LEVELING BINDER HAS BEEN PLACED.
  - BARRICADES OR TEMPORARY RAMPS TO BE USED BY THE CONTRACTOR AROUND UTILITY ADJUSTMENTS AS DIRECTED BY THE ENGINEER.
  - ALL PATCHING SHALL BE COMPLETED AFTER MILLING. FOR PATCHING QUANTITIES AND LOCATIONS SEE SHEET 6.

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VILLAGE OF HAMPSHIRE  
 KANE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

**DIETRICH ROAD  
 LAPP IMPROVEMENTS**

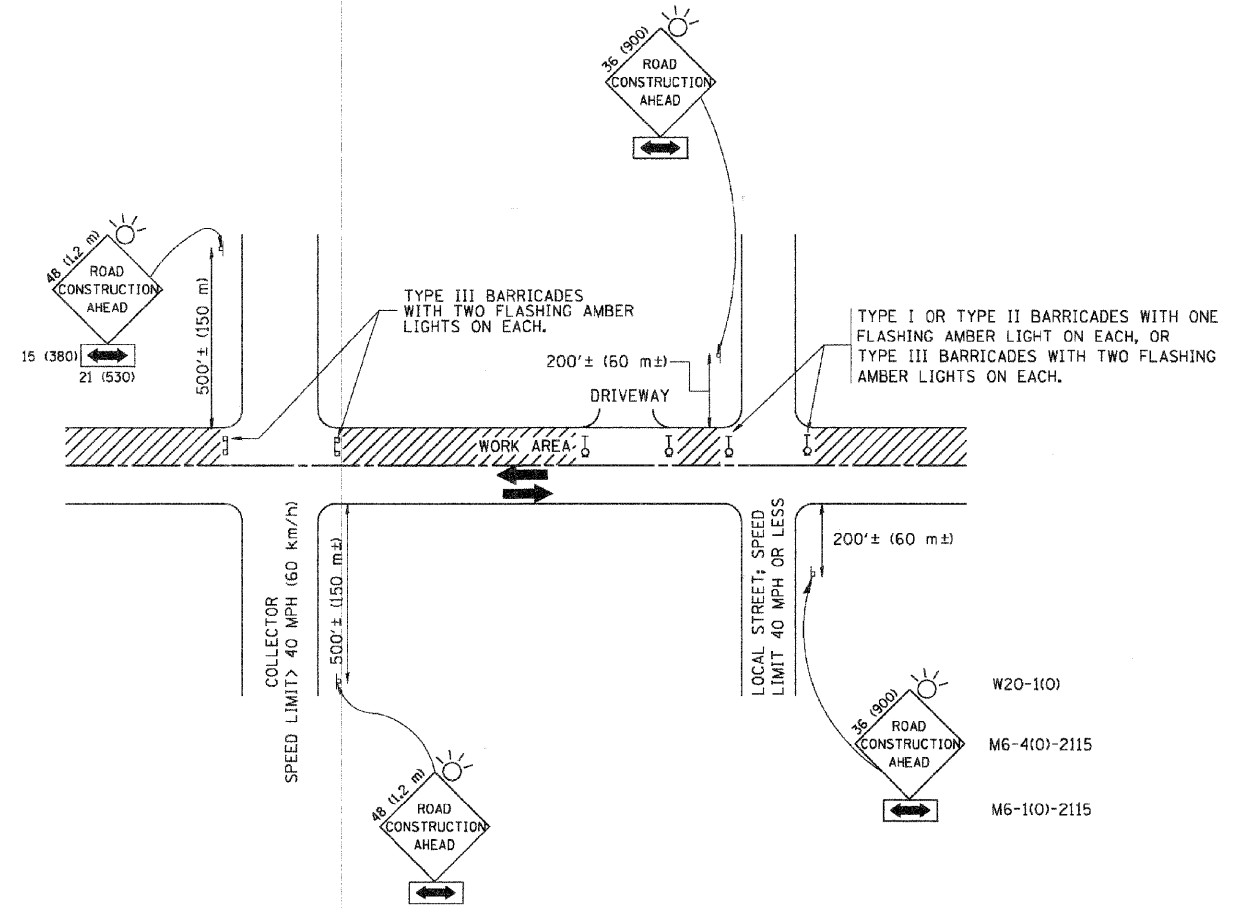
**PLAN, CONSTRUCTION NOTES  
 AND DETAILS**

DATE:	JANUARY 2010
PROJECT NO:	HA0957
FILE:	HA0957-PLAN
SHEET	<b>7</b> OF <b>14</b>

Plotted: June 1, 2010 @ 3:33 PM By: Larry Nolan - Tab: 07 Plan 22x34

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Plotted: May 24, 2010 @ 10:05 AM By: Larry Nelson - Tab: 08 Typ: Dets 22x34



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
  - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / 1" IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

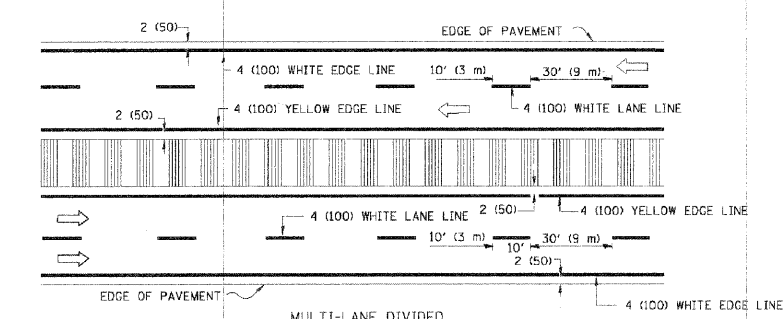
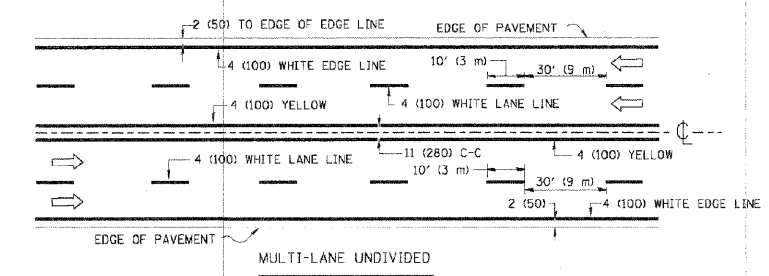
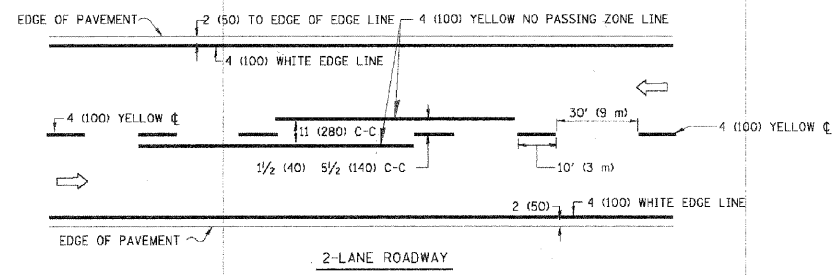
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2921	10-00018-00-RS	KANE	14	8
<b>TC-10</b>			CONTRACT NO. 63460	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

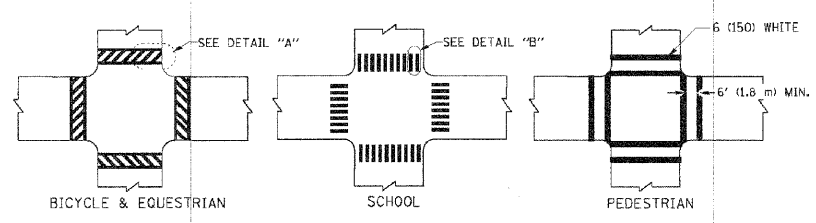
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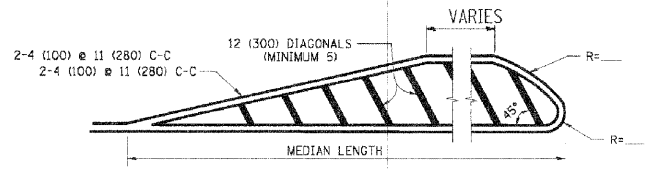
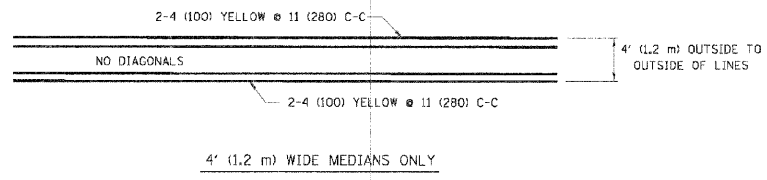


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**

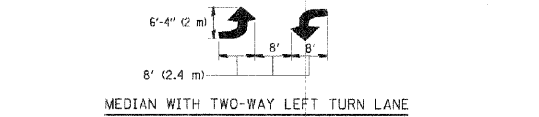
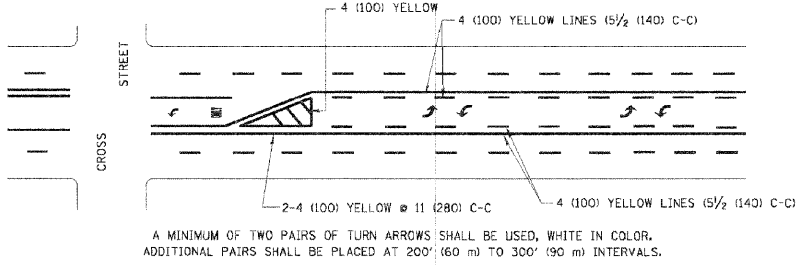


**TYPICAL CROSSWALK MARKING**

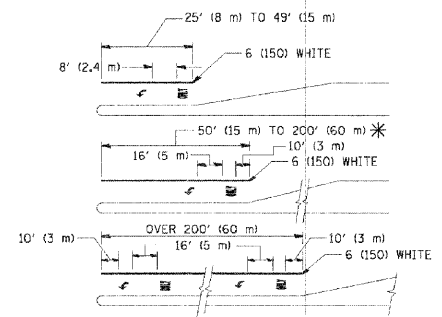


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**



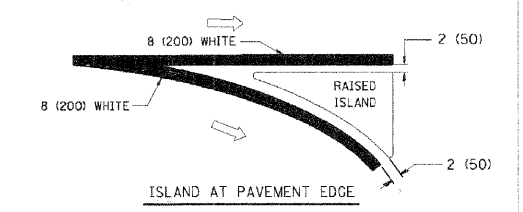
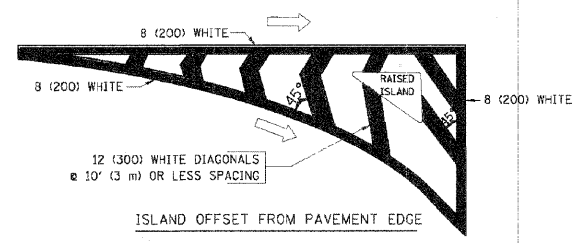
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
 \* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) (MIN) AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2' (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2' (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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FILE NAME =	USER NAME = drvekoagn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
PROJECT WORK\pwr\dot\drvekoagn\d0108315\103.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
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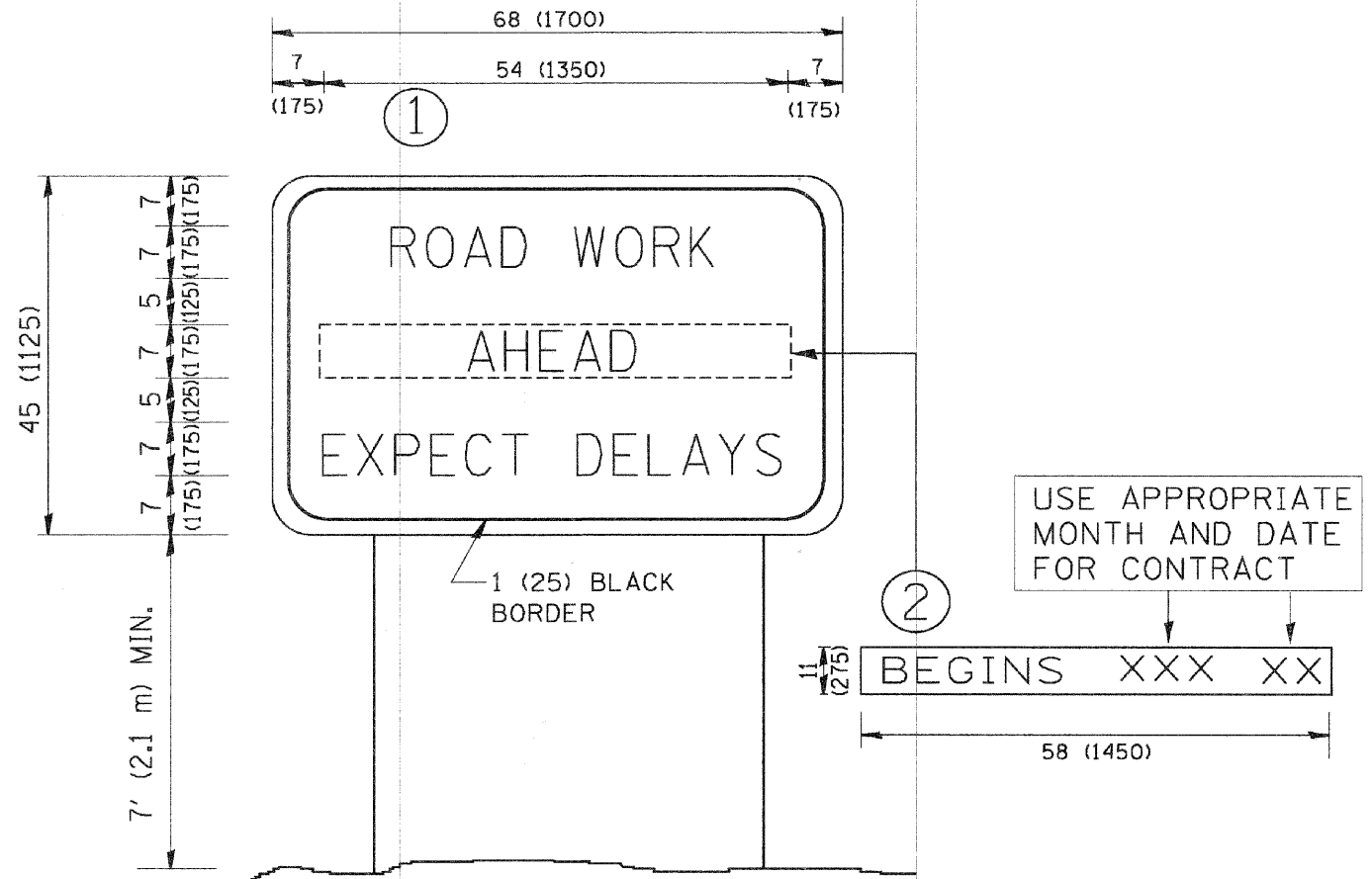
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE</b>		F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
<b>TYPICAL PAVEMENT MARKINGS</b>		2821	10-00018-00-RS	KANE	14	9
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	

<b>TC-13</b>		FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT
<b>CONTRACT NO. 63460</b>			

Printed: May 24, 2010 @ 10:03 AM By: Larry Nolan - Tab: 09 Typ: Dats 22x34

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**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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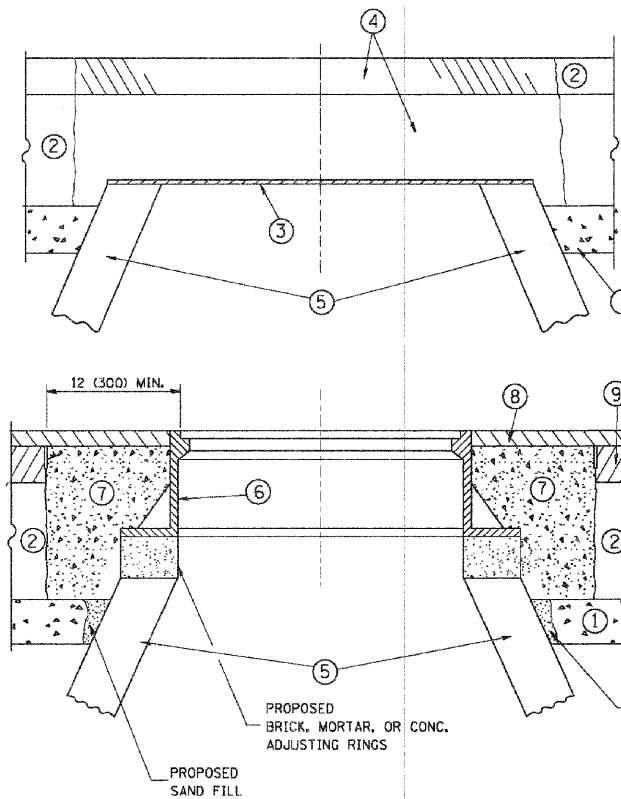
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	10
<b>TC-22</b>			<b>CONTRACT NO. 63460</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Plotted: May 24, 2010 @ 10:03 AM By: Larry Nelson - Tab: 11 Typ: Dets 22x34



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"  
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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FILE NAME =	USER NAME = gegl:anobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
W:\dist\td\22x34\brd88.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
		CHECKED -	REVISED - R. WIEDEMAN 05-14-04
		DATE - 10-25-94	REVISED - R. BORO 01-01-07

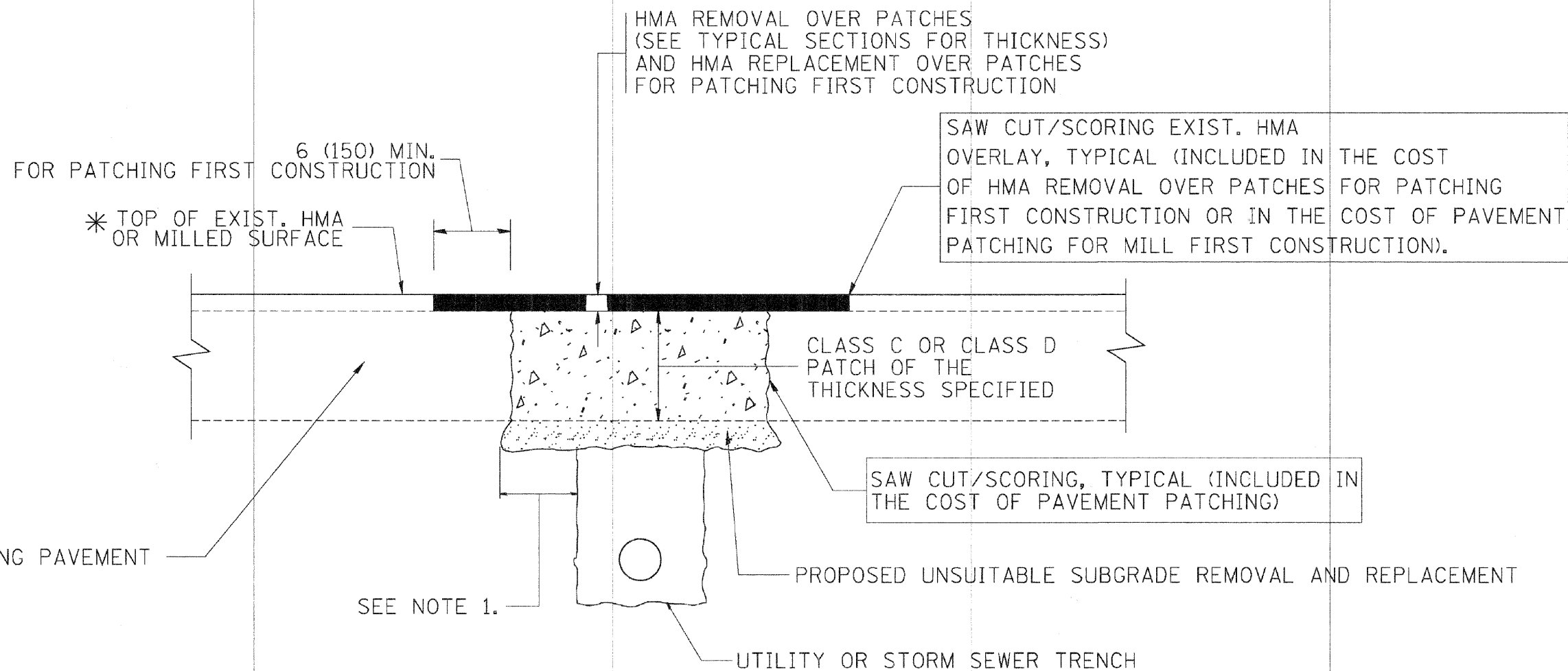
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	11
BD600-03 (BD-8)		CONTRACT NO. 63480		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Path: H:\SDS\PROJ\VA0657\DWG\DWG\_FINAL\_ENG\VA0657-CIV



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE NAME = c:\projects\distatd22v34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

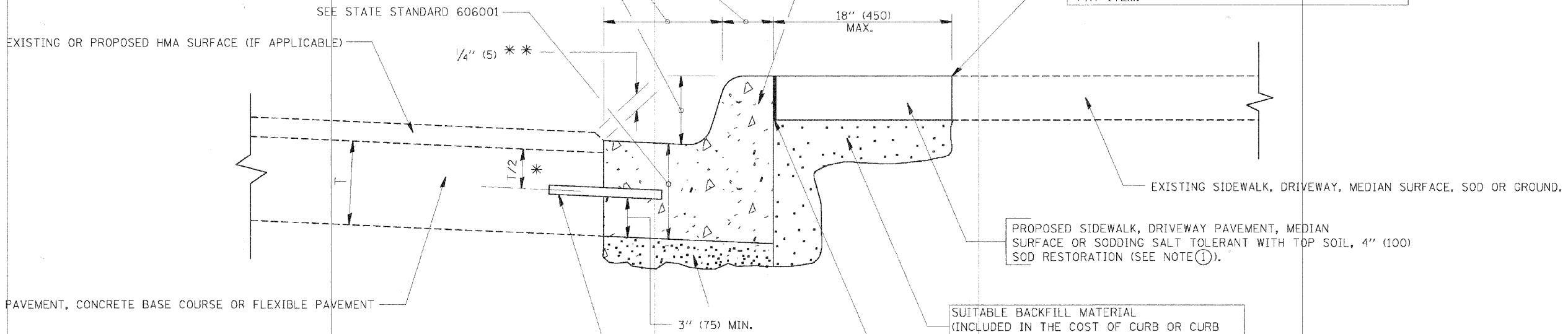
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	12
BD400-04 (BD-22)			CONTRACT NO. 63460	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96
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	PLOT SCALE = 50,000 / 1 IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09

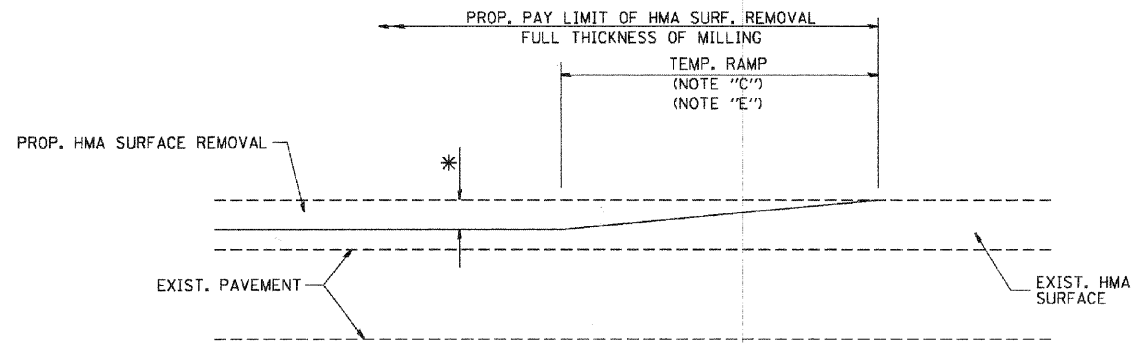
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-RS	KANE	14	13
<b>BD600-06 (BD-24)</b>		CONTRACT NO. 63480		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

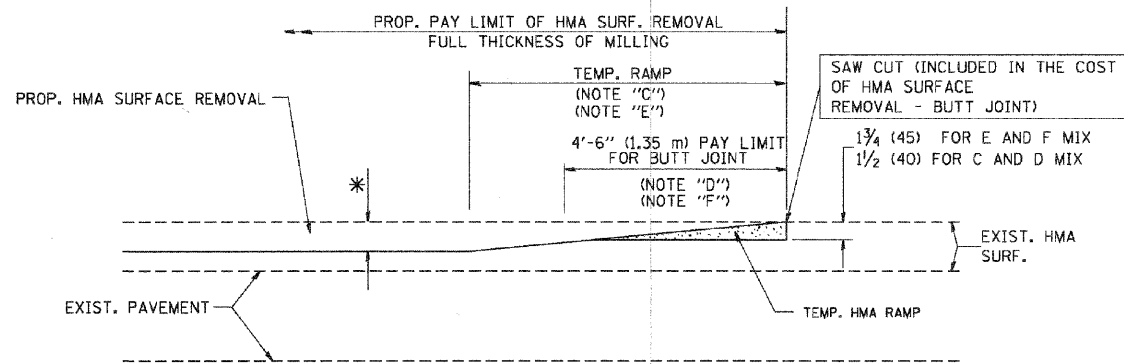
Plotted: May 24, 2010 @ 10:03 AM By: Larry Nelson - Tab: 13 Typ-Dets 22x34

Path: H:\SDB\PROJ\100857\DWG\FINAL ENO\100857-COR



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

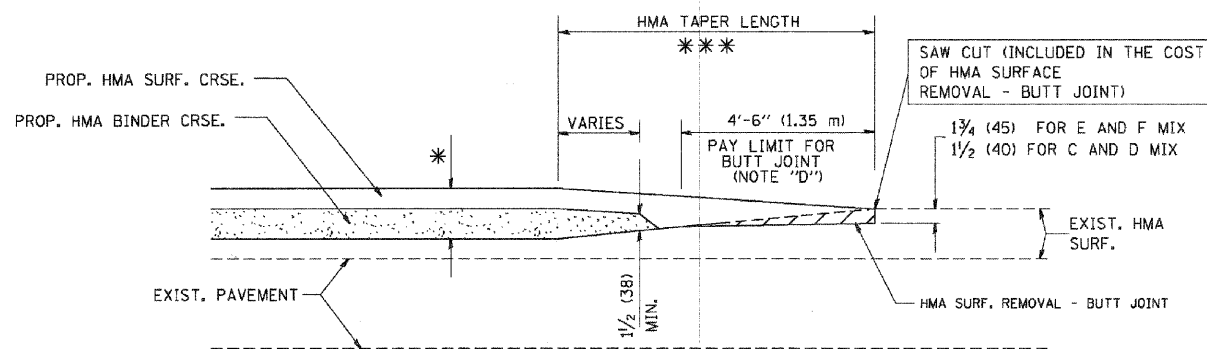
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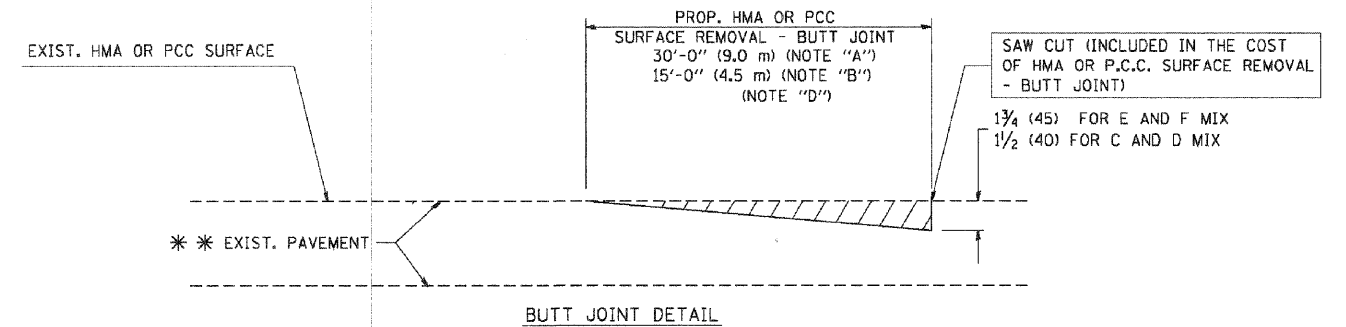
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

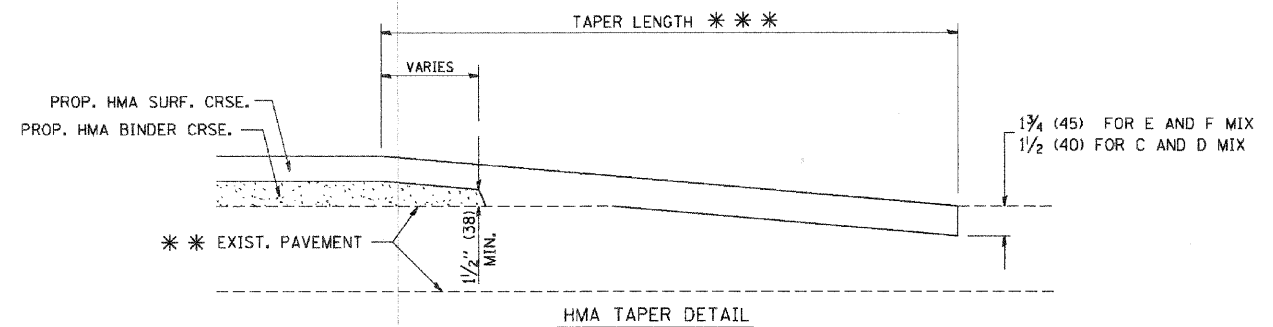
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE NAME =	USER NAME = gpg11enobts	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
W:\dst\td\22x34\bd32.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2009	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2321	10-00018-00-BS	KANE	14	14
<b>BD400-05 BD32</b>		CONTRACT NO. 63460		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				