

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1503	10-00290-00-RS	KANE	14	01
03483		ILLINOIS		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

# PLANS FOR PROPOSED FEDERAL AID HIGHWAY

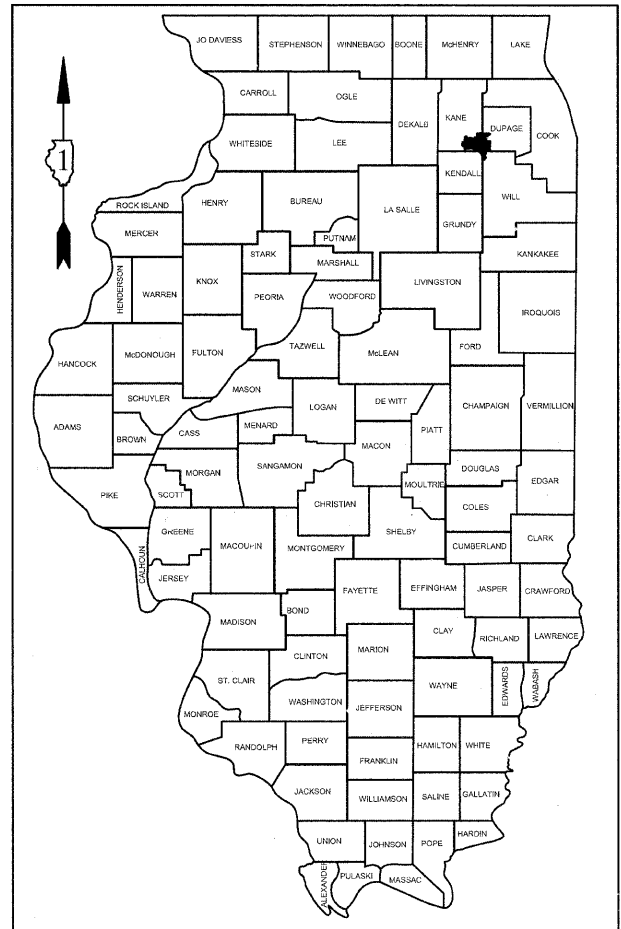
F.A.U. ROUTE 1503 (INDIAN TRAIL DRIVE)  
(RANDALL ROAD TO HIGHLAND AVENUE)  
RESURFACING  
SECTION NO.: 10-00290-00-RS  
PROJECT NO.: ARA-M-9003 (662)  
JOB NO.: C-91-648-10  
CITY OF AURORA  
KANE COUNTY

INDEX OF SHEETS

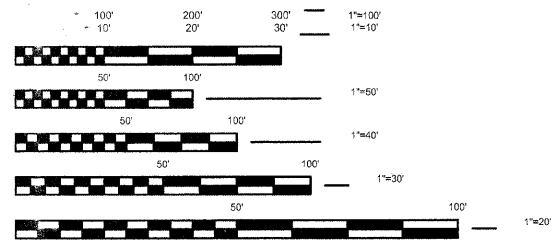
1	COVER SHEET
2	GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS
5-9	ROADWAY PLAN
10-14	CONSTRUCTION DETAILS

TRAFFIC DATA

INDIAN TRAIL (F.A.P. 1503)  
POSTED SPEED LIMIT = 35 MPH  
DESIGN SPEED LIMIT = 35 MPH  
2008 ADT = 21,000  
DESIGN DESIGNATION: **MINOR ARTERIAL**



LOCATION OF SECTION INDICATED THIS:



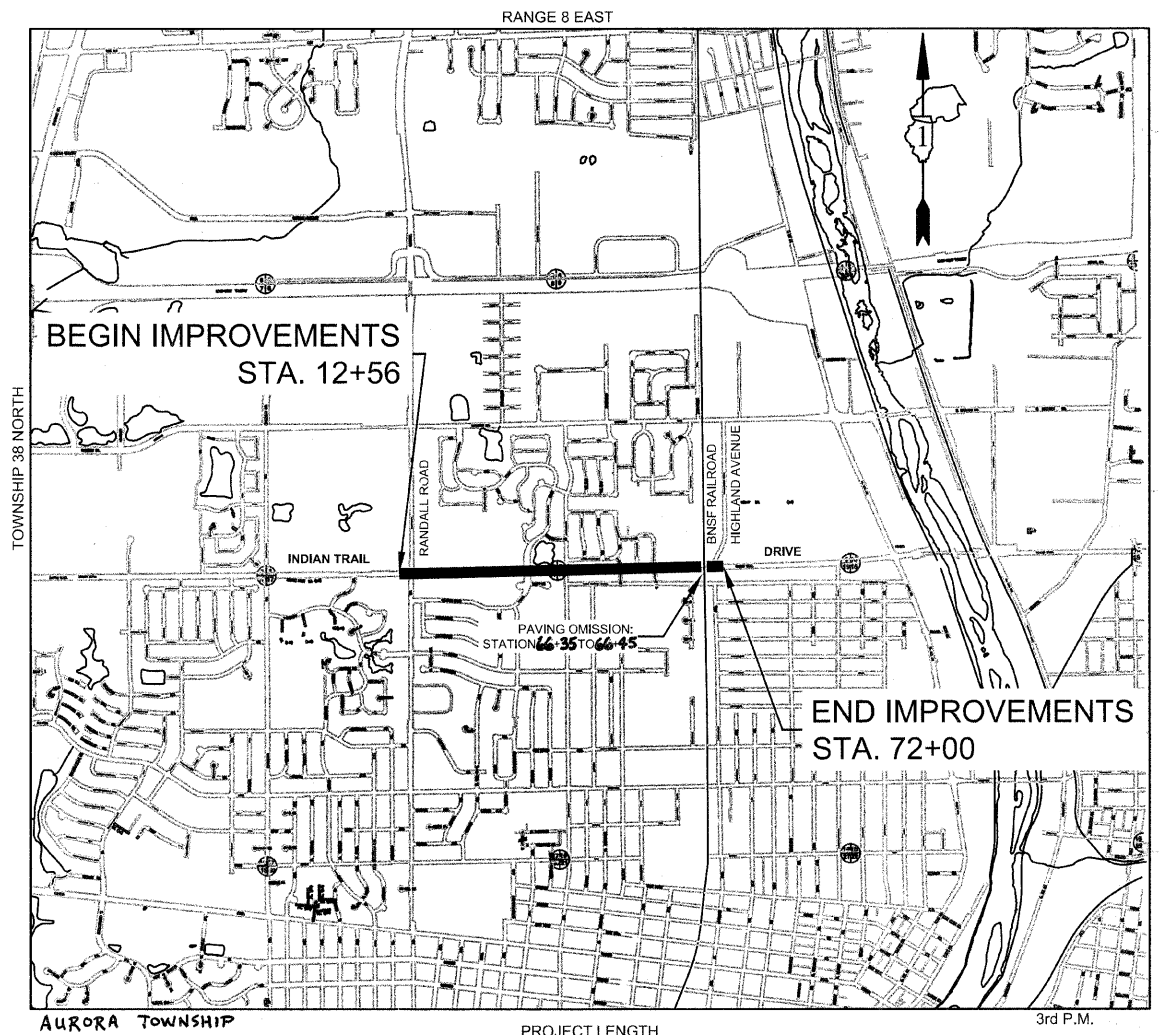
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

**JULIE**  
JOINT  
UTILITY  
LOCATION  
INFORMATION FOR  
EXCAVATION  
CALL 811

**CITY OF AURORA**  
ENGINEERING DIVISION  
44 E. DOWNER PLACE  
AURORA, IL 60507  
PHONE: (630) 844-3620 FAX: (630) 892-0322

PROJECT ENGINEER: CHRIS LIROT, P.E.  
PROJECT MANAGER: CHRIS LIROT, P.E.

CONTRACT NO. 63483



NET LENGTH OF IMPROVEMENT = 5,934 FT (1.12 MI)  
GROSS LENGTH OF PROJECT = 5,944 FT (1.13 MI)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED May 25, 2010  
*Chi E. Lirot*  
CITY OF AURORA, ROAD & BRIDGE COORDINATOR

PASSED May 27 2010  
*Chelt*  
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR  
BID BASED ON  
LIMITED REVIEW MAY 27 2010  
*Diane M. O'Keefe*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

*Chi E. Lirot* 5/25/10  
CHRISTOPHER E. LIROT, P.E.  
EXPIRES: 11-30-2011

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

FEDERAL AID DESIGN ENGINEER: MARILIN SOLOMON (847) 705-4407

**GENERAL NOTES**

1. ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007.
2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
4. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 1-800-892-0123 (48 HRS NOTICE IS REQUIRED)
5. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
6. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE ITEM REMOVED.
7. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
8. OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FORM THE ROADWAY CENTERLINE.
9. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
10. THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.10 GAL/SQ YD.
11. THE AGGREGATE PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 4 LBS/SQ YD.
12. QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL IDENTIFY FINAL PATCH LOCATIONS IN THE FIELD.

**STORM SEWERS, WATER MAINS, AND UTILITIES**

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
2. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN IN THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
4. ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
5. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
6. THE CONTRACTOR SHALL ENSURE THAT ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE TO THE CITY FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
7. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

**SIGNING AND STRIPING**

1. SEE IDOT STANDARD DETAIL 780001. DISTRICT ONE DETAIL AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
2. ALL EXISTING SIGNS LOCATED ON UTILITY/LIGHT POLES TO REMAIN THAT DO NOT CONFLICT WITH THE IMPROVEMENTS SHALL REMAIN IN PLACE UNLESS DIRECTED BY THE ENGINEER.
3. SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.
4. THE CONTRACTOR SHALL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
5. ALL SIGNS SHALL BE INSTALLED OR RELOCATED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. THIS WORK SHALL BE PAID FOR USING THE APPROPRIATE PAY ITEMS
6. ALL REMOVED SIGNS SHALL BE RETURNED TO THE STATE OR CITY AS APPLICABLE.

**TRAFFIC CONTROL**

1. SEE TRAFFIC CONTROL PLANS AND GENERAL NOTES CONCERNING TRAFFIC CONTROL AND PROTECTION.

**HIGHWAY STANDARDS**

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 424001-05 CURB RAMPS FOR SIDEWALKS
- 442201-03 CLASS C AND D PATCHES
- 606001-04 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701602-04 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
- 701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-04 LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
- 701901-01 TRAFFIC CONTROL DEVICES
- 780001-02 TYPICAL PAVEMENT MARKINGS
- 886001-01 DETECTOR LOOP INSTALLATIONS
- 886006-01 TYPICAL LAYOUTS FOR DETECTION LOOPS

**DISTRICT ONE DETAILS**

- TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDEROADS, INTERSECTIONS, AND DRIVEWAYS
- TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- BD-8 DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
- BD-32 BUTT JOINT AND HMA DETAILS



CITY OF AURORA  
ENGINEERING DIVISION  
44 EAST DOWNER PLACE

DESIGNED - CHRIS LIROT  
DRAWN - ALBERTO SANCHEZ  
CHECKED - CHRIS LIROT  
DATE - 10/1/09

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES AND HIGHWAY STANDARDS

SCALE: NONE

SHEET NO. 1 OF 1 SHEETS

STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1503	10-00290-00-RS	KANE	4	2
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	

CONTRACT NO. 63483

# SUMMARY OF QUANTITIES

CODE NUMBER	PAY ITEM DESCRIPTION	UNITS	CONSTRUCTION TYPE CODE 1000
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	5,944
40600300	AGGREGATE (PRIME COAT)	TON	10
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	1,693
40600895	CONSTRUCTING TEST STRIP	EACH	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	360
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	3,385
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY, 8"	SQ YD	100
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3,500
42400800	DETECTABLE WARNINGS	SQ FT	168
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	40,293
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	150
44000600	SIDEWALK REMOVAL	SQ FT	3,500
44001700	COMB CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,200
48101200	AGGREGATE SHOULDERS, TYPE B	TON	80
60252710	CATCH BASINS TO BE ADJUSTED WITH SPECIAL FRAME AND GRATE	EACH	10
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	22
60266600	VALVE BOXES TO BE ADJUSTED	EACH	5
67100100	MOBILIZATION	L SUM	1
70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1
70106810	CHANGEABLE MESSAGE SIGN, SPECIAL	CAL MO	2
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	6,500
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	464
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	28,776
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,850
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,300
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	900
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2,262
X4420132	CLASS D PATCHES (SPECIAL)	SQ YD	110
XX006425	RESTORATION	SQ YD	533
Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	50
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1

\* SPECIALTY ITEM



CITY OF AURORA  
ENGINEERING DIVISION  
44 EAST DOWNER PLACE

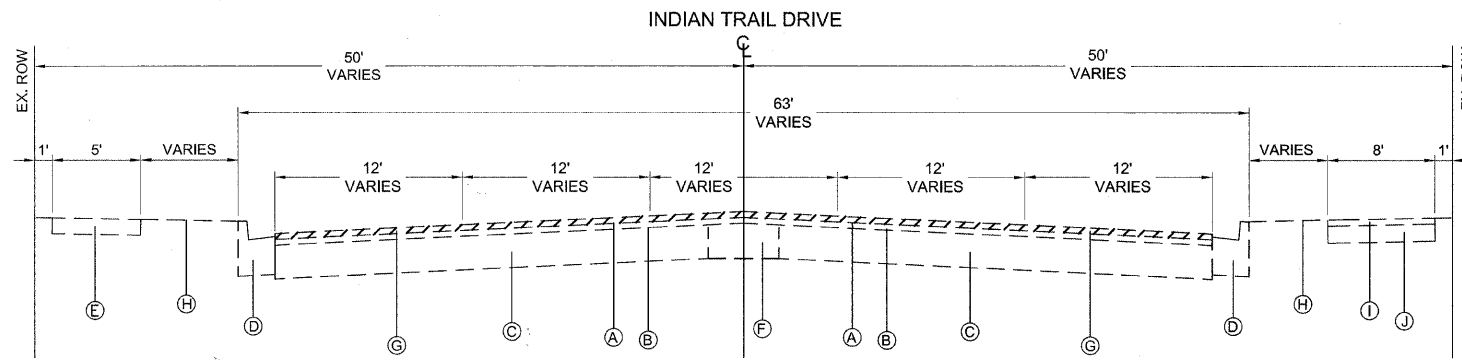
DESIGNED - CHRIS LIROT	REVISED - 4/15/10
DRAWN - ALBERTO SANCHEZ	REVISED -
CHECKED - CHRIS LIROT	REVISED -
DATE - 10/1/09	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

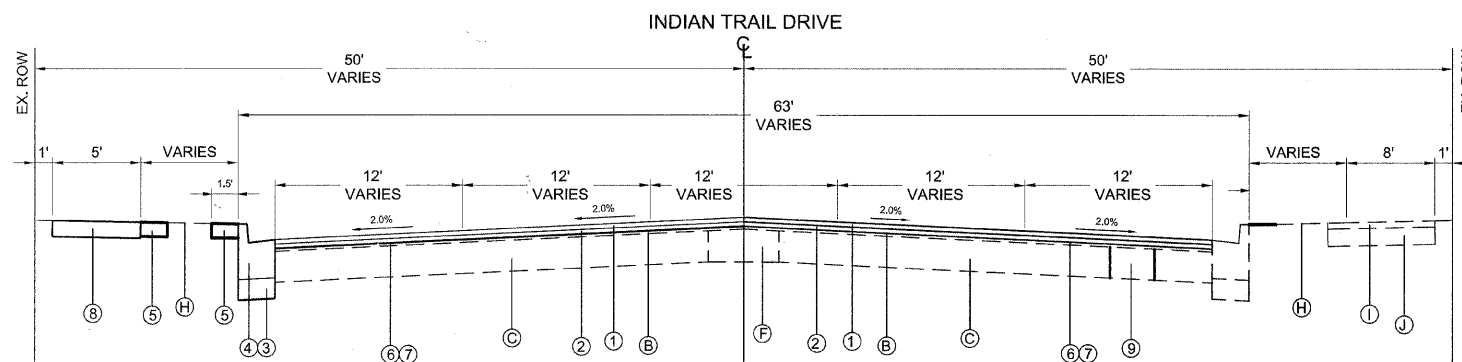
SCALE: NONE      SHEET NO. 1 OF 1 SHEETS      STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEET SHEETS	SHEET NO.
1503	10-00290-00-RS	KANE	14	3
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**EXISTING TYPICAL SECTION**

STA. 12+56 TO STA. 72+00,  
INDIAN TRAIL DRIVE



**PROPOSED TYPICAL SECTION**

STA. 12+56 TO STA. 72+00,  
INDIAN TRAIL DRIVE

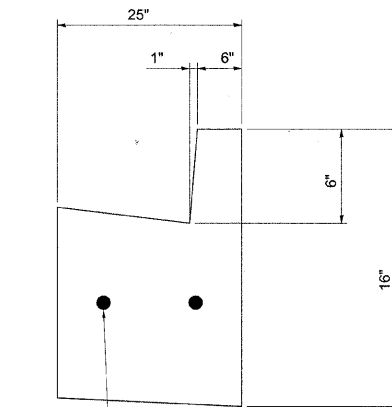
NOTE: OMIT SECTION  
AT RAILROAD  
CROSSING

**EXISTING LEGEND**

- (A) HOT-MIX ASPHALT SURFACE COURSE, 1½"
- (B) HOT-MIX ASPHALT BINDER COURSE, 4¾"
- (C) HOT-MIX ASPHALT BASE COURSE, 7"
- (D) COMBINATION CONCRETE CURB & GUTTER, TYPE B6.18
- (E) PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (F) PORTLAND CEMENT CONCRETE BASE COURSE
- (G) HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- (H) EXISTING GROUND
- (I) BITUMINOUS CONCRETE PATH, 2"
- (J) AGGREGATE BASE COURSE, TYPE B CA-6, 6"

**PROPOSED LEGEND**

- ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1½"
- ② LEVELING BINDER (MACHINE METHOD), N70, 3/4" (MIN.)
- ③ AGGREGATE BASE COURSE, TYPE B CA-7, 6" (INCLUDED IN THE COST OF CURB & GUTTER PAY ITEM)
- ④ COMB CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE RESIDENT ENGINEER)
- ⑤ PULVERIZED TOPSOIL 6", SEEDING, SALT TOLERANT, STRAW MULCH PAID AS RESTORATION
- ⑥ BITUMINOUS MATERIALS (PRIME COAT)
- ⑦ AGGREGATE (PRIME COAT)
- ⑧ SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (AS DIRECTED BY THE RESIDENT ENGINEER)
- ⑨ CLASS D PATCHES, SPECIAL, 12-INCH (AS DIRECTED BY THE RESIDENT ENGINEER)



**EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B6.18**

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	VOIDS
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm)	4% @ 70 GYR
LEVELING BINDER (MACHINE METHOD), N70 (IL 9.5mm)	4% @ 70 GYR
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19mm), 12-INCH	4% @ 70 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm)	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22 AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS



CITY OF AURORA  
ENGINEERING DIVISION  
44 EAST DOWNER PLACE

DESIGNED - CHRIS LIROT  
DRAWN - ALBERTO SANCHEZ  
CHECKED - CHRIS LIROT  
DATE - 10/1/09

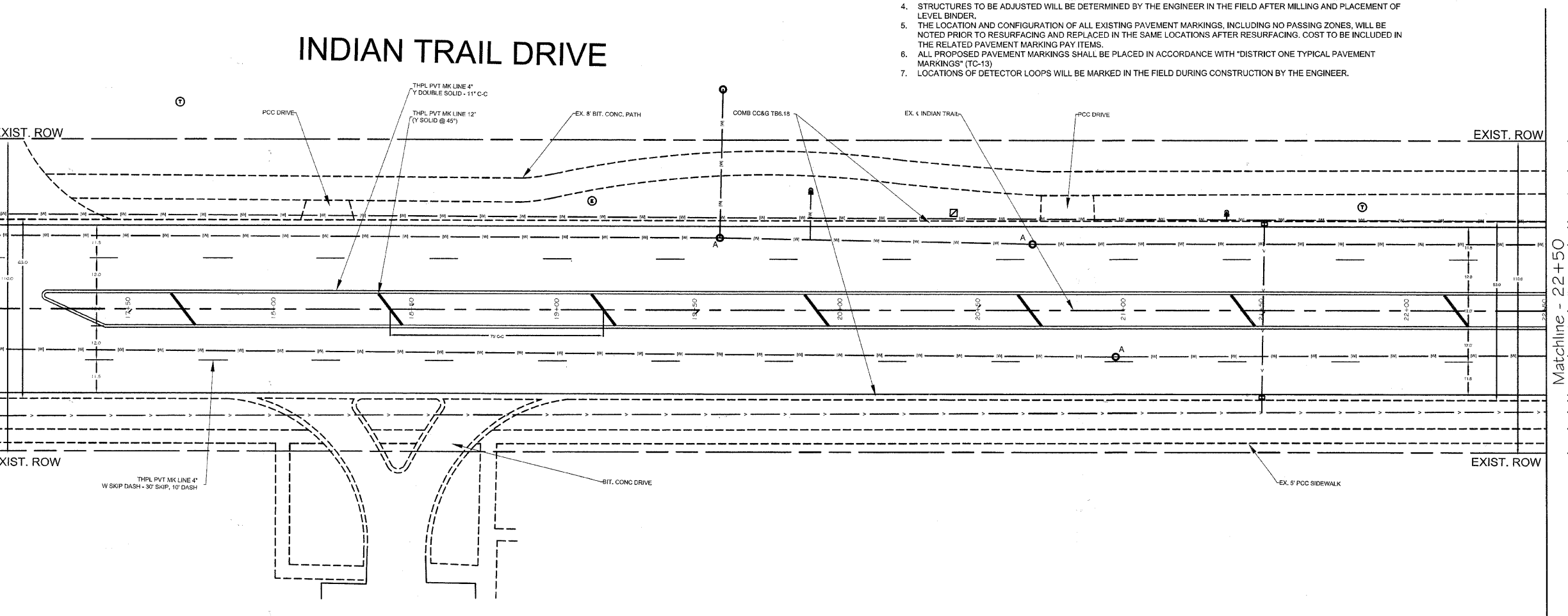
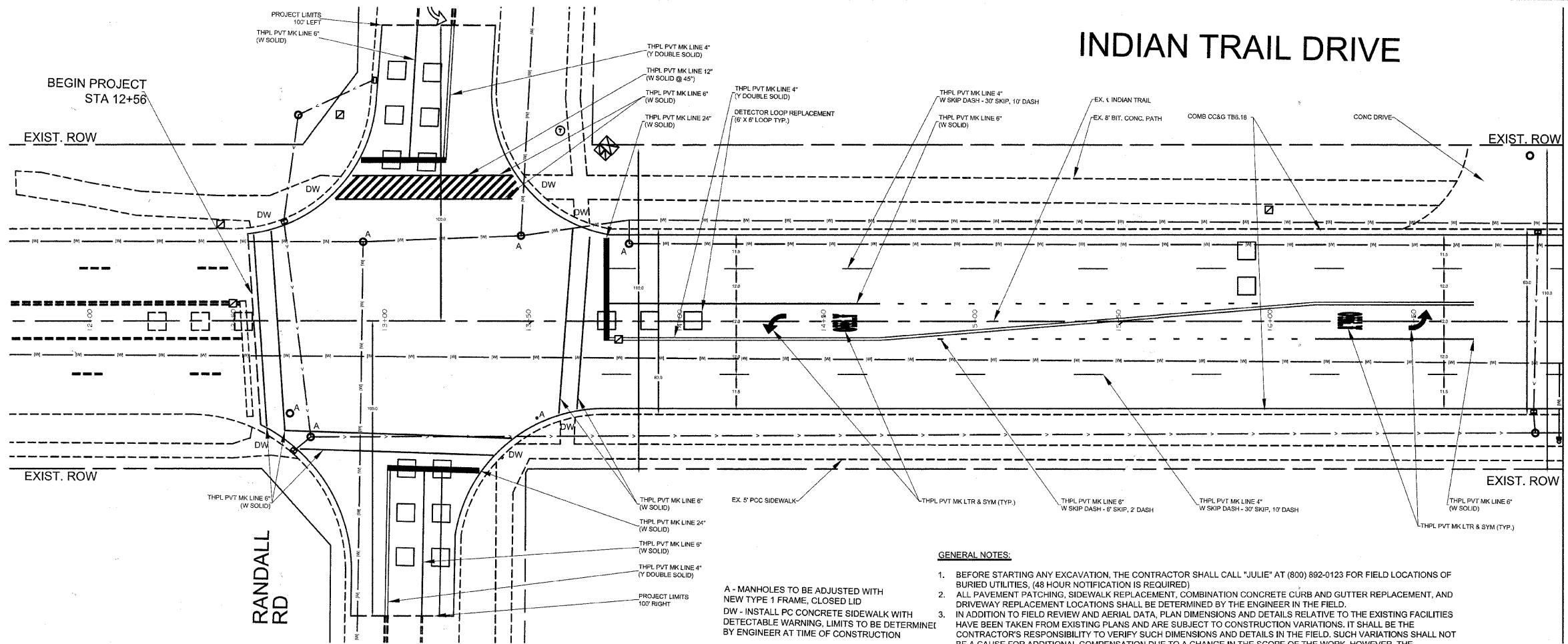
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REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A

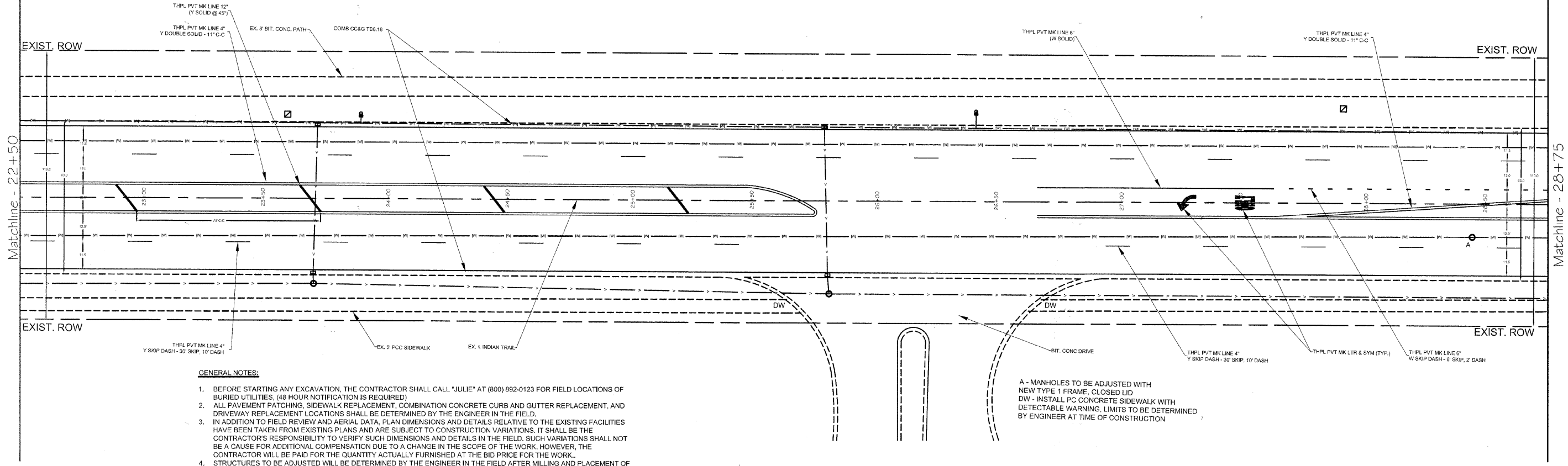
F.A.P. RTE. 1503	SECTION 10-00290-00-RS	COUNTY KANE	TOTAL SHEETS 14	SHEET NO. 4
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63483	



- GENERAL NOTES:**
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES, (48 HOUR NOTIFICATION IS REQUIRED)
  - ALL PAVEMENT PATCHING, SIDEWALK REPLACEMENT, COMBINATION CONCRETE CURB AND GUTTER REPLACEMENT, AND DRIVEWAY REPLACEMENT LOCATIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
  - IN ADDITION TO FIELD REVIEW AND AERIAL DATA, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE BID PRICE FOR THE WORK.
  - STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AFTER MILLING AND PLACEMENT OF LEVEL BINDER.
  - THE LOCATION AND CONFIGURATION OF ALL EXISTING PAVEMENT MARKINGS, INCLUDING NO PASSING ZONES, WILL BE NOTED PRIOR TO RESURFACING AND REPLACED IN THE SAME LOCATIONS AFTER RESURFACING. COST TO BE INCLUDED IN THE RELATED PAVEMENT MARKING PAY ITEMS.
  - ALL PROPOSED PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" (TC-13)
  - LOCATIONS OF DETECTOR LOOPS WILL BE MARKED IN THE FIELD DURING CONSTRUCTION BY THE ENGINEER.

A - MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID  
 DW - INSTALL PC CONCRETE SIDEWALK WITH DETECTABLE WARNING, LIMITS TO BE DETERMINED BY ENGINEER AT TIME OF CONSTRUCTION

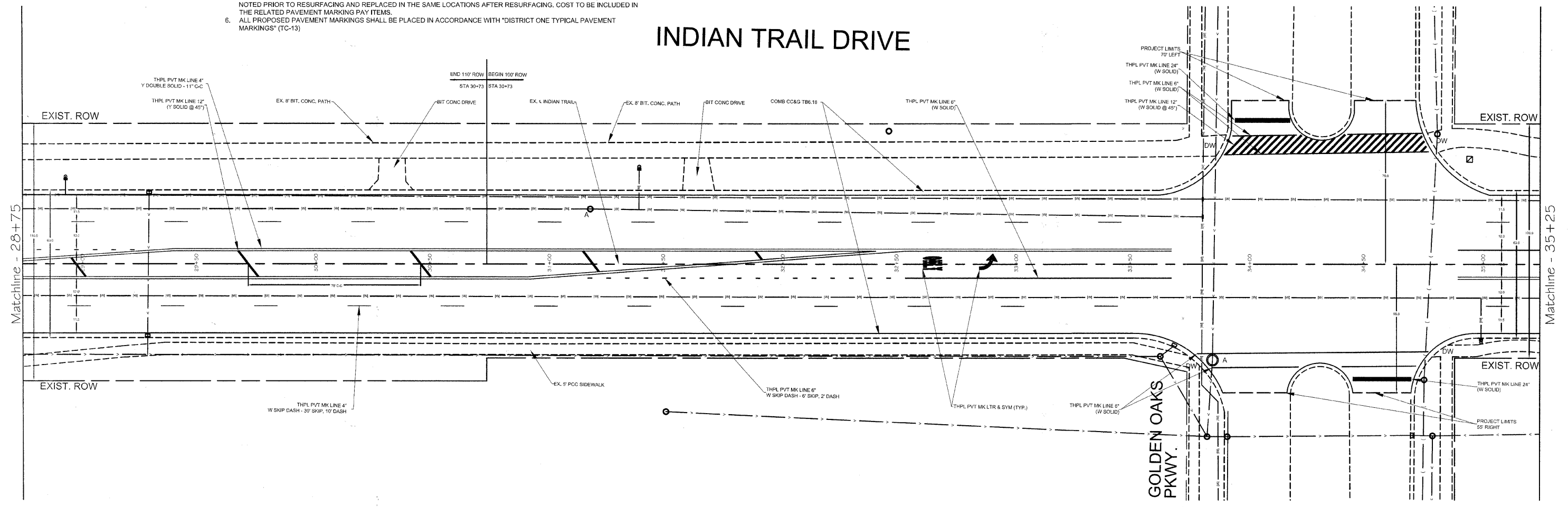
# INDIAN TRAIL DRIVE



**GENERAL NOTES:**

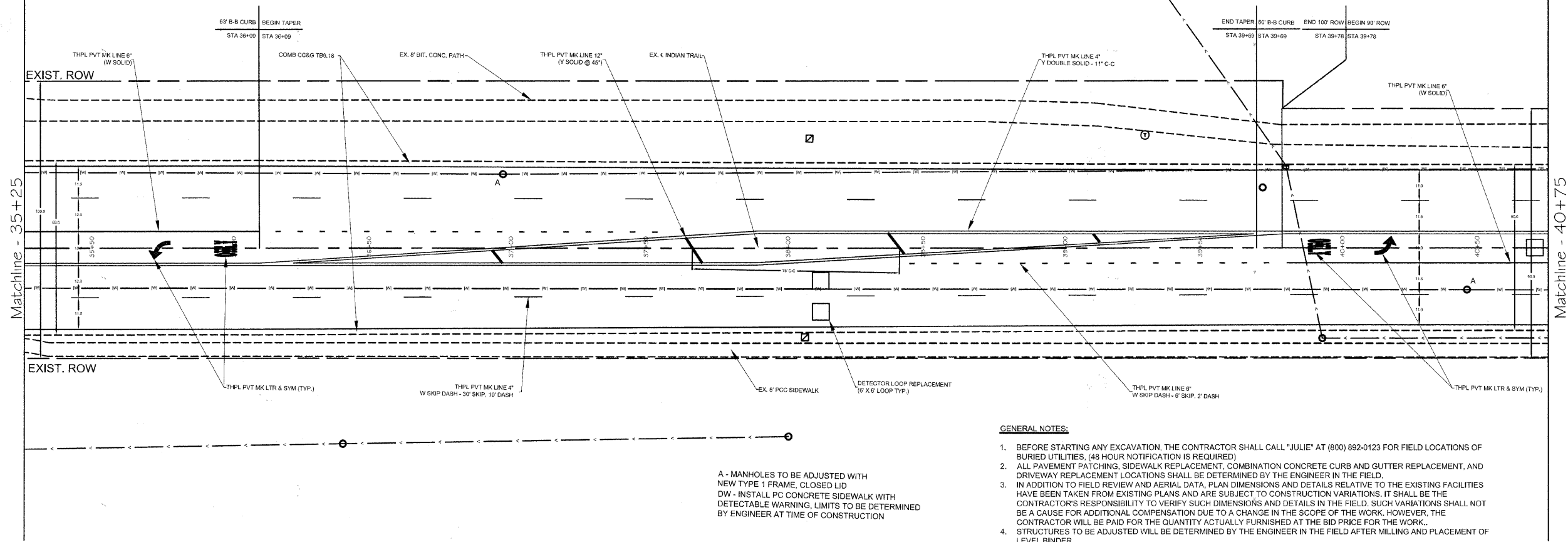
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# INDIAN TRAIL DRIVE





# INDIAN TRAIL DRIVE

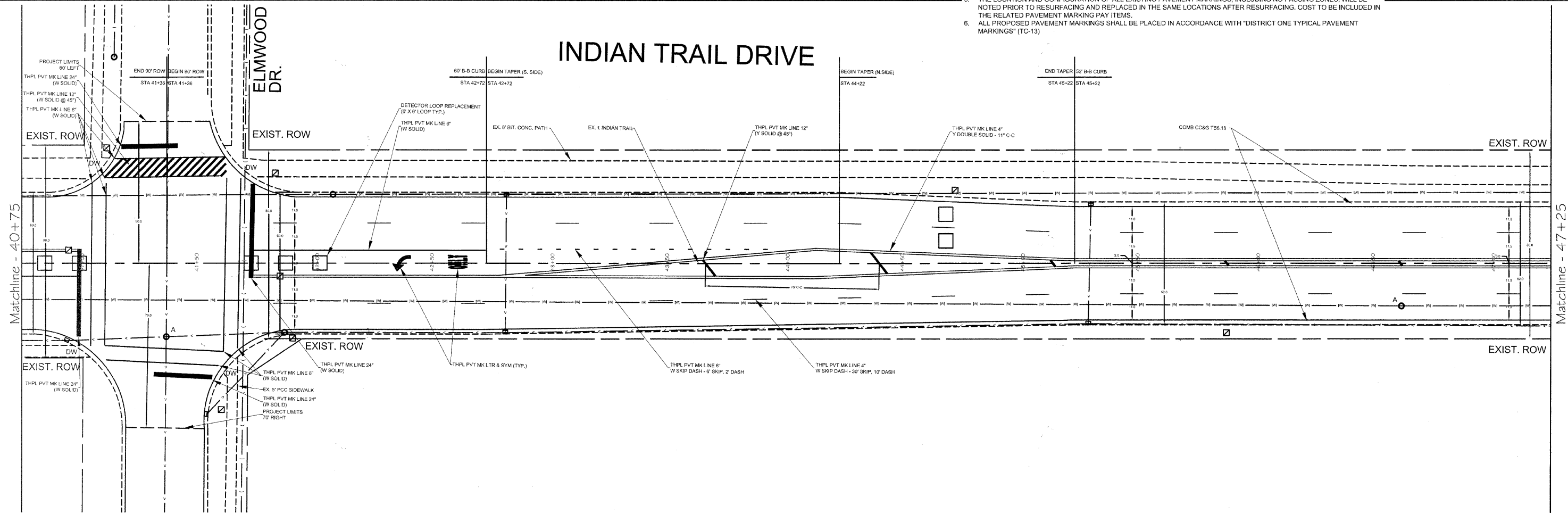


A - MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID  
 DW - INSTALL PC CONCRETE SIDEWALK WITH DETECTABLE WARNING, LIMITS TO BE DETERMINED BY ENGINEER AT TIME OF CONSTRUCTION

**GENERAL NOTES:**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- ALL PAVEMENT PATCHING, SIDEWALK REPLACEMENT, COMBINATION CONCRETE CURB AND GUTTER REPLACEMENT, AND DRIVEWAY REPLACEMENT LOCATIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
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# INDIAN TRAIL DRIVE



CITY OF AURORA  
 ENGINEERING DIVISION  
 44 EAST DOWNER PLACE

DESIGNED - CHRIS LIROT	REVISED -
DRAWN - ALBERTO SANCHEZ	REVISED -
CHECKED - CHRIS LIROT	REVISED -
DATE - 10/1/09	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN - INDIAN TRAIL

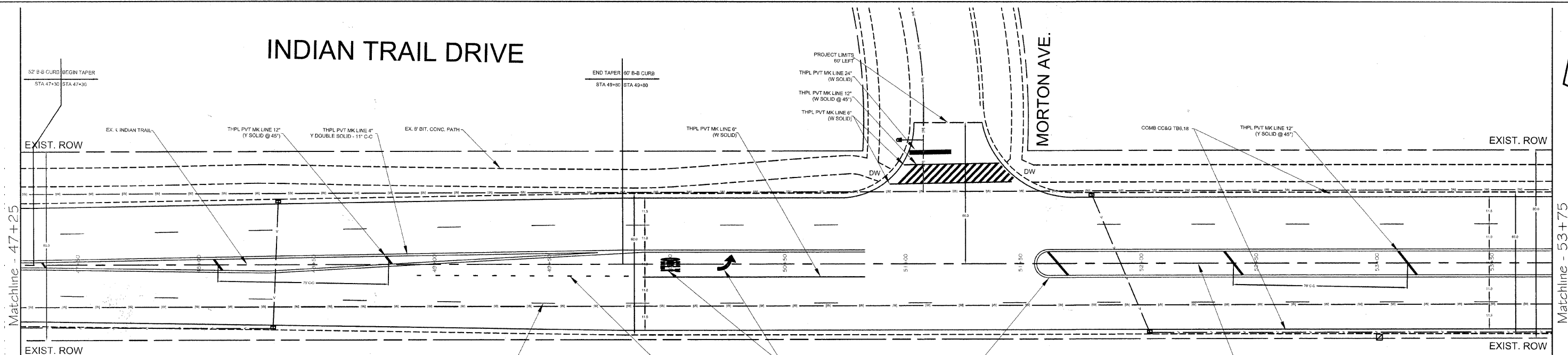
SCALE: 1" = 20'

SHEET NO. 3 OF 5 SHEETS

STA. 35+25 TO STA. 47+25

F.A.R. RTE. 1503	SECTION 10-00290-00-RS	COUNTY KANE	TOTAL SHEET SHEETS NO. 7
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	

# INDIAN TRAIL DRIVE

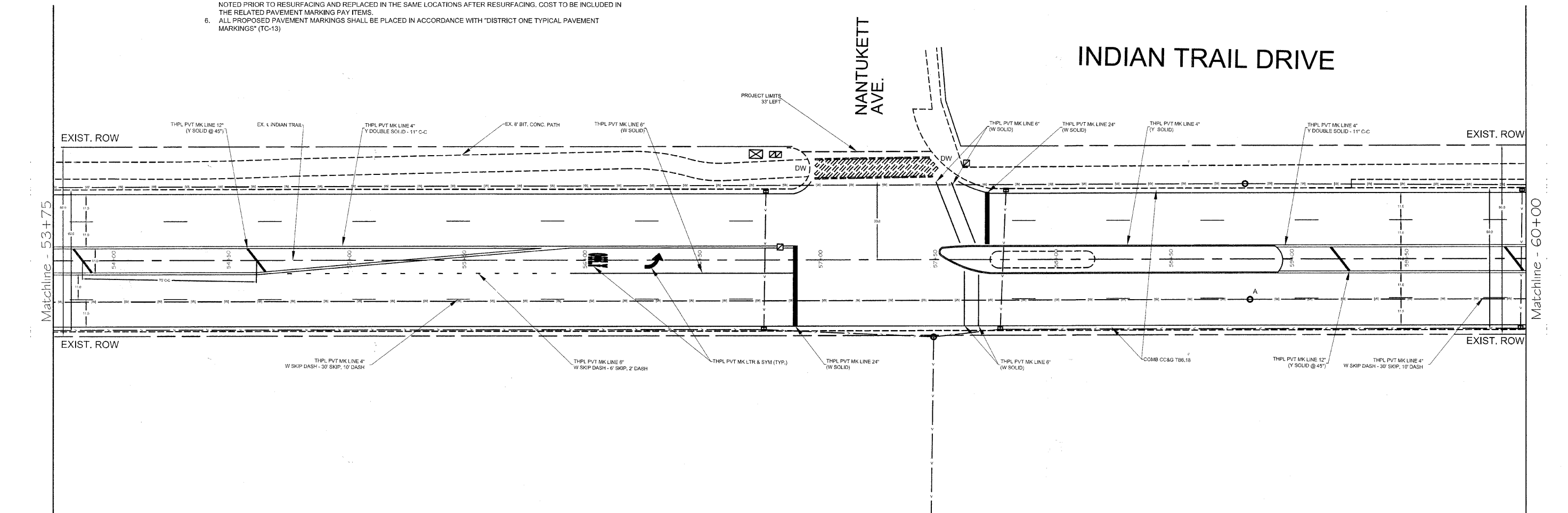


**GENERAL NOTES:**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- ALL PAVEMENT PATCHING, SIDEWALK REPLACEMENT, COMBINATION CONCRETE CURB AND GUTTER REPLACEMENT, AND DRIVEWAY REPLACEMENT LOCATIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- IN ADDITION TO FIELD REVIEW AND AERIAL DATA, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE BID PRICE FOR THE WORK.
- STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AFTER MILLING AND PLACEMENT OF LEVEL BINDER.
- THE LOCATION AND CONFIGURATION OF ALL EXISTING PAVEMENT MARKINGS, INCLUDING NO PASSING ZONES, WILL BE NOTED PRIOR TO RESURFACING AND REPLACED IN THE SAME LOCATIONS AFTER RESURFACING. COST TO BE INCLUDED IN THE RELATED PAVEMENT MARKING PAY ITEMS.
- ALL PROPOSED PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" (TC-13)

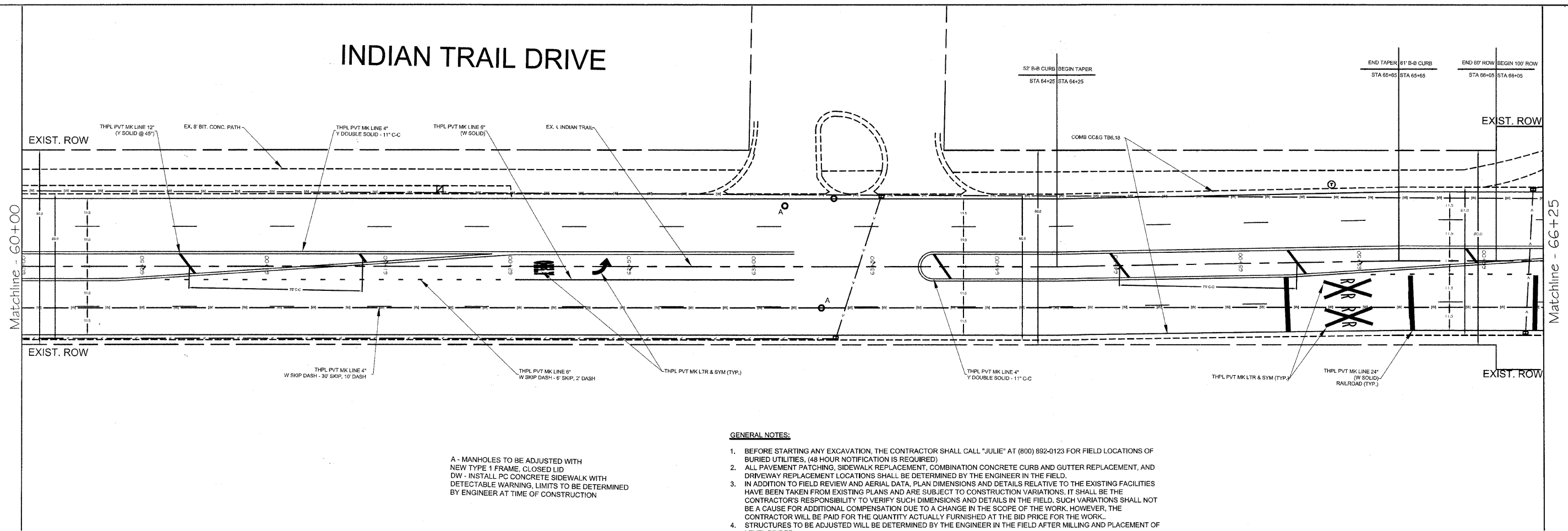
A - MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID  
 DW - INSTALL PC CONCRETE SIDEWALK WITH DETECTABLE WARNING, LIMITS TO BE DETERMINED BY ENGINEER AT TIME OF CONSTRUCTION

# INDIAN TRAIL DRIVE





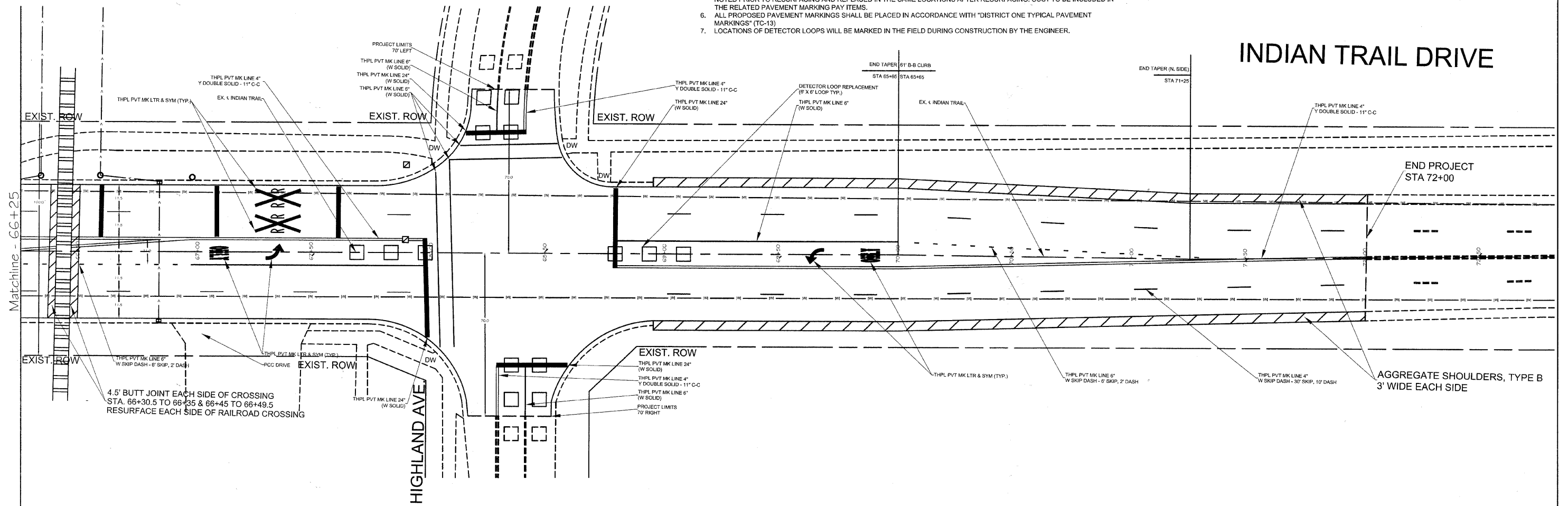
# INDIAN TRAIL DRIVE



**GENERAL NOTES:**

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES, (48 HOUR NOTIFICATION IS REQUIRED)
2. ALL PAVEMENT PATCHING, SIDEWALK REPLACEMENT, COMBINATION CONCRETE CURB AND GUTTER REPLACEMENT, AND DRIVEWAY REPLACEMENT LOCATIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
3. IN ADDITION TO FIELD REVIEW AND AERIAL DATA, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE BID PRICE FOR THE WORK.
4. STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AFTER MILLING AND PLACEMENT OF LEVEL BINDER.
5. THE LOCATION AND CONFIGURATION OF ALL EXISTING PAVEMENT MARKINGS, INCLUDING NO PASSING ZONES, WILL BE NOTED PRIOR TO RESURFACING AND REPLACED IN THE SAME LOCATIONS AFTER RESURFACING. COST TO BE INCLUDED IN THE RELATED PAVEMENT MARKING PAY ITEMS.
6. ALL PROPOSED PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" (TC-13)
7. LOCATIONS OF DETECTOR LOOPS WILL BE MARKED IN THE FIELD DURING CONSTRUCTION BY THE ENGINEER.

A - MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID  
 DW - INSTALL PC CONCRETE SIDEWALK WITH DETECTABLE WARNING. LIMITS TO BE DETERMINED BY ENGINEER AT TIME OF CONSTRUCTION



4.5' BUTT JOINT EACH SIDE OF CROSSING  
 STA. 66+30.5 TO 66+35 & 66+45 TO 66+49.5  
 RESURFACE EACH SIDE OF RAILROAD CROSSING

AGGREGATE SHOULDERS, TYPE B  
 3' WIDE EACH SIDE



CITY OF AURORA  
 ENGINEERING DIVISION  
 44 EAST DOWNER PLACE

DESIGNED - CHRIS LIROT  
 DRAWN - ALBERTO SANCHEZ  
 CHECKED - CHRIS LIROT  
 DATE - 10/1/08

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

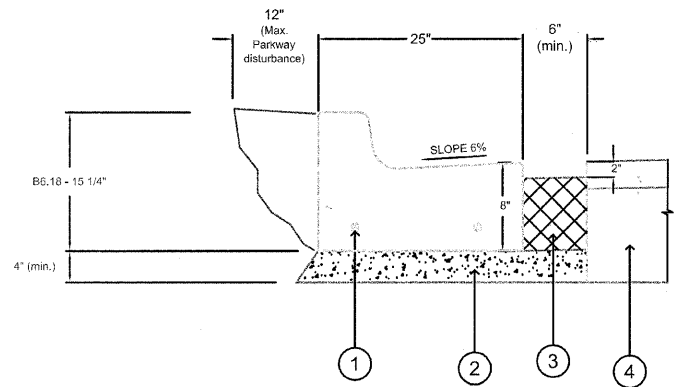
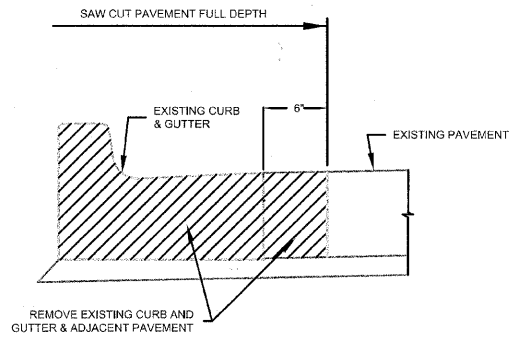
ROADWAY PLAN - INDIAN TRAIL

SCALE: 1" = 20'

SHEET NO. 5 OF 5 SHEETS

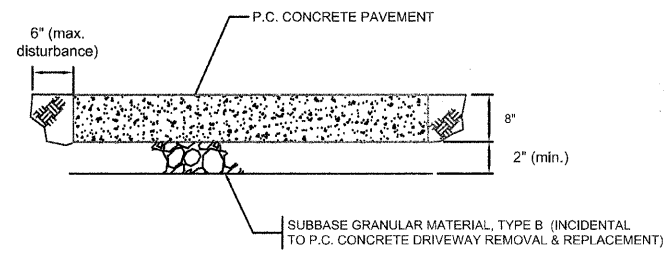
STA. 60+00 TO STA. 72+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEETS NO.
1503	10-00290-00-RS	KANE	14	9
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	

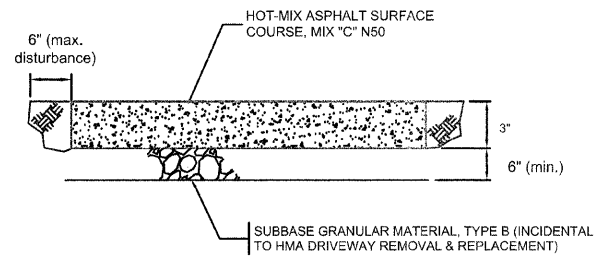


- ① CONTINUOUS REBAR, (2) - #4 - REQ'D FOR ALL CURB OVER 10' LONG
- ② SUBBASE GRANULAR MATERIAL, TYPE B (CRUSHED STONE CA-7) (COST IS INCIDENTAL TO COMB. CURB & GUTTER)
- ③ P.C. CONCRETE BASE COURSE, THE CONTRACTOR SHALL REMOVE ALL DEBRIS AND PAVEMENT FROM THE AREA PRIOR TO PLACEMENT. TEMPORARY PATCH AT DRIVEWAYS AS NEEDED. (COST IS INCIDENTAL TO COMB. CURB & GUTTER)
- ④ EXISTING PAVEMENT, VARIES

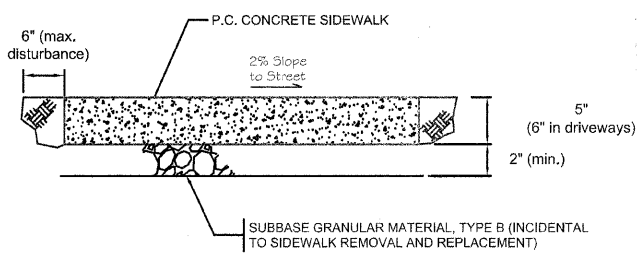
### COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT



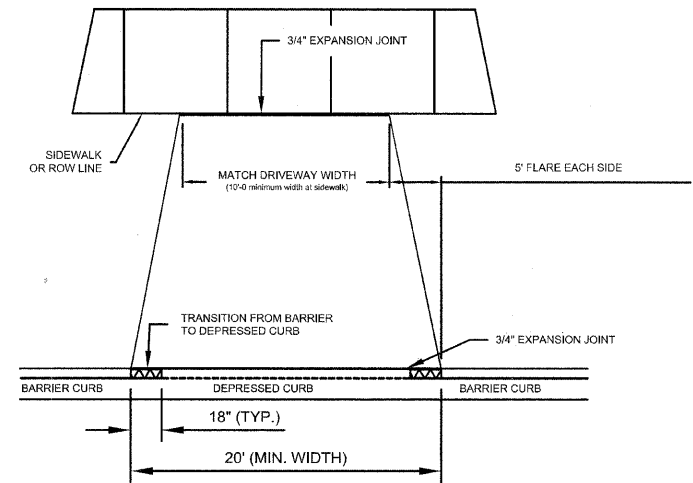
PORTLAND CEMENT CONCRETE DRIVEWAY



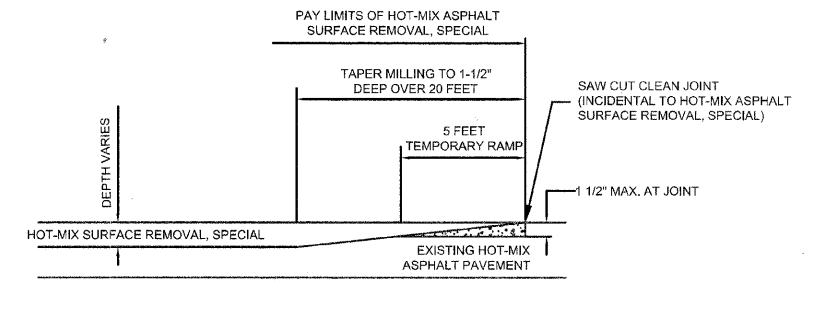
NOTE: THIS PAY ITEM MAY BE USED FOR BIKE PATH REPAIRS



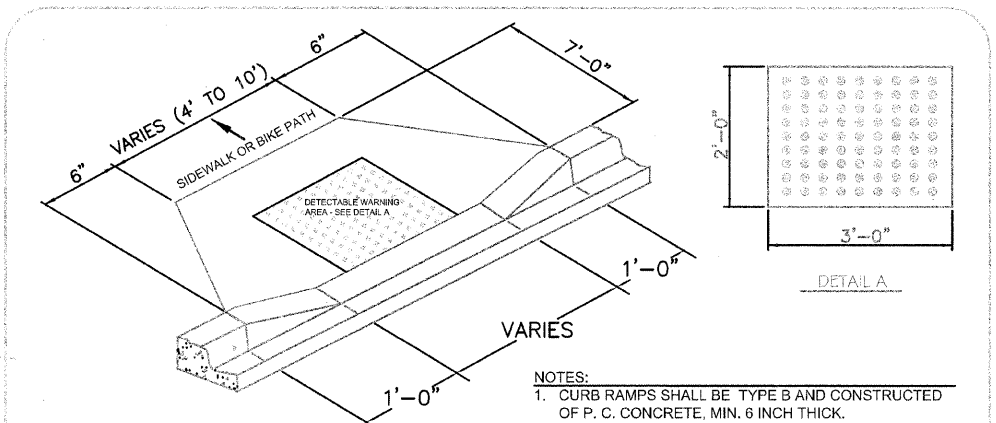
SIDEWALK REMOVAL / REPLACEMENT DETAIL



TYPICAL GRAVEL DETAIL



TYPICAL PATCH DETAIL



- NOTES:
1. CURB RAMPS SHALL BE TYPE B AND CONSTRUCTED OF P. C. CONCRETE, MIN. 6 INCH THICK.
  2. ALL WORK SHALL FOLLOW IDOT HIGHWAY STANDARD 424001 (LATEST REVISION).
  3. DETECTABLE WARNING AREA SHALL BE ADA SOLUTIONS, INC., ARMOR-TILE ADA OR APPROVED EQUAL, COLOR RED, 24" X 36"
  4. THE COLORING AND STAMPING OF CONCRETE WILL NOT BE ALLOWED.
  5. DETECTABLE WARNINGS SHALL BE INSTALLED AT ALL RAMPS INCLUDING BIKE PATHS (INSTALL 2 AT 2' X 3').

CURB RAMP DETAIL



CITY OF AURORA  
ENGINEERING DIVISION  
44 EAST DOWNER PLACE

DESIGNED - CHRIS LIROT  
DRAWN - ALBERTO SANCHEZ  
CHECKED - CHRIS LIROT  
DATE - 10/1/09

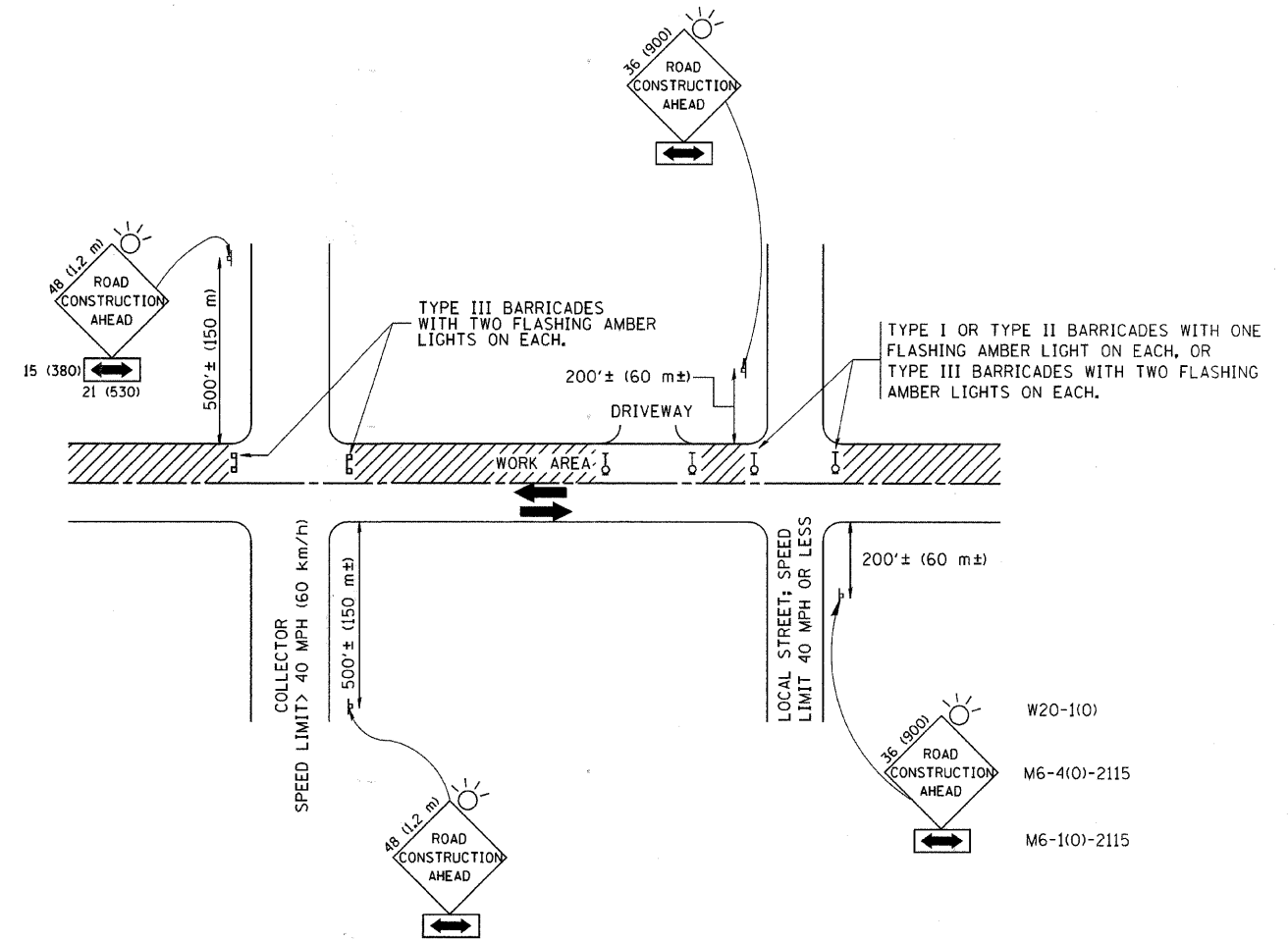
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE. 1503	SECTION 10-00290-00-RS	COUNTY KANE	TOTAL SHEETS 10
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1,2 m x 1,2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

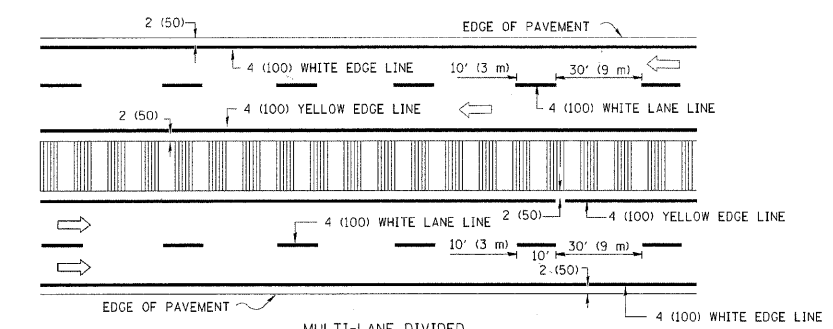
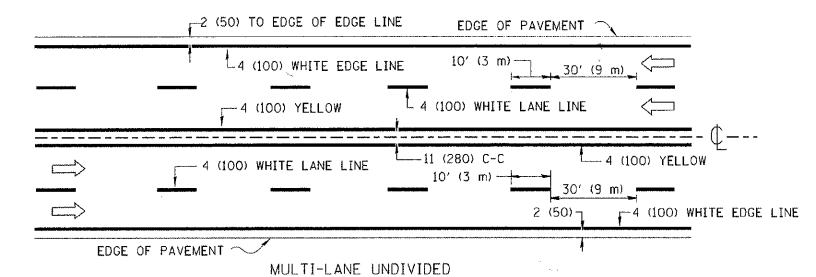
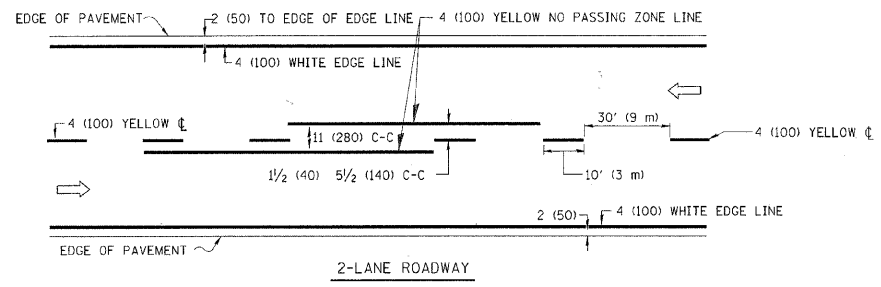
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

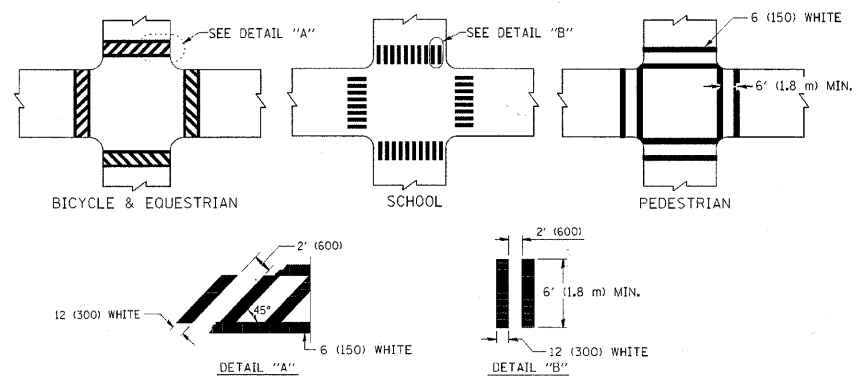
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 1503	SECTION 10-00290-00-RS	COUNTY KANE	TOTAL SHEETS 14	SHEET NO. 11
TC-10			CONTRACT NO. 63483	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

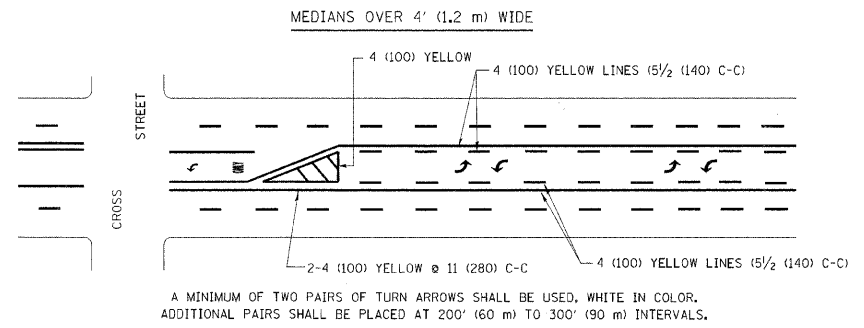
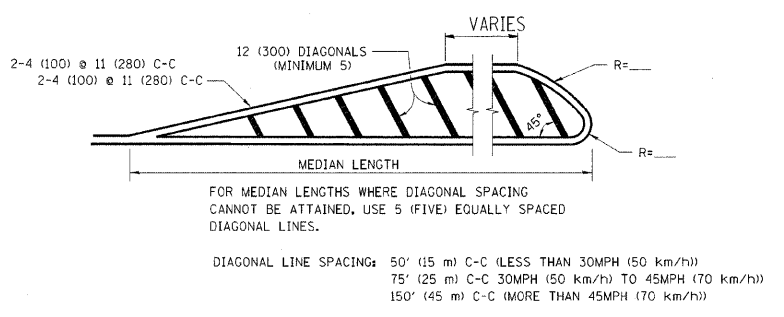
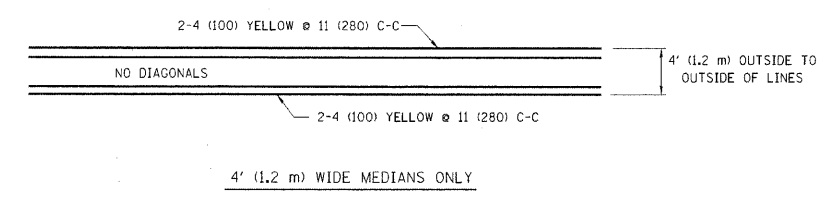


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

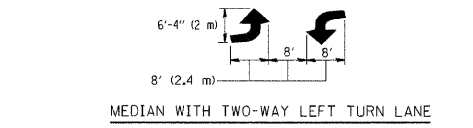
TYPICAL LANE AND EDGE LINE MARKING



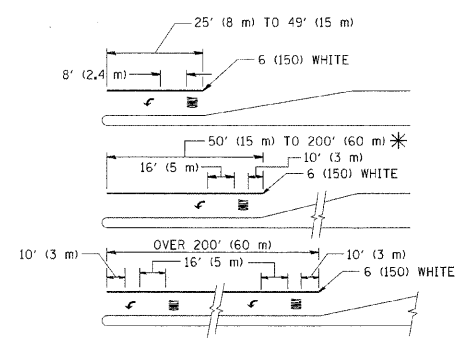
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING

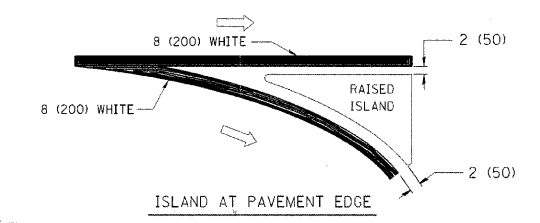
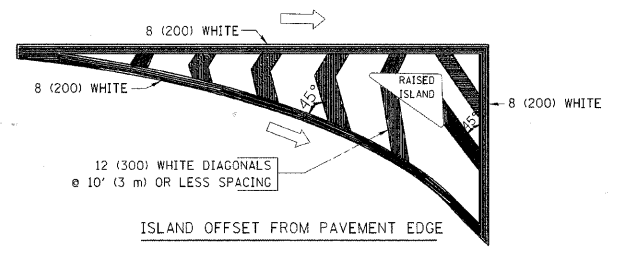


TYPICAL LEFT (OR RIGHT) TURN LANE



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING

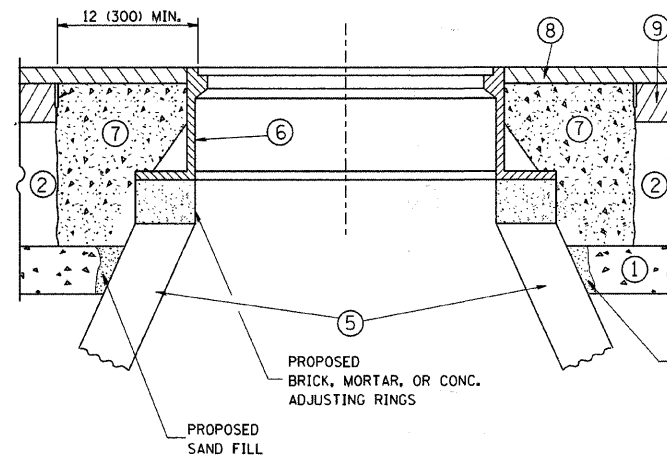
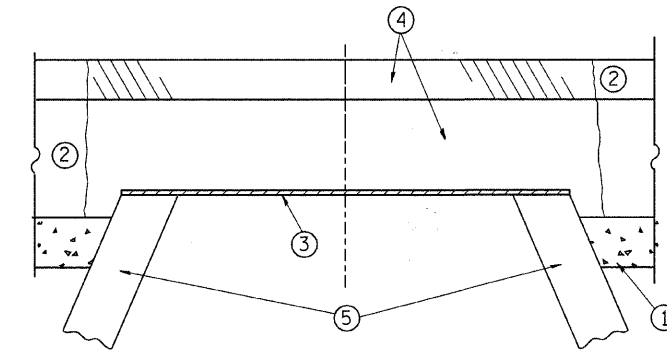


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:** THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

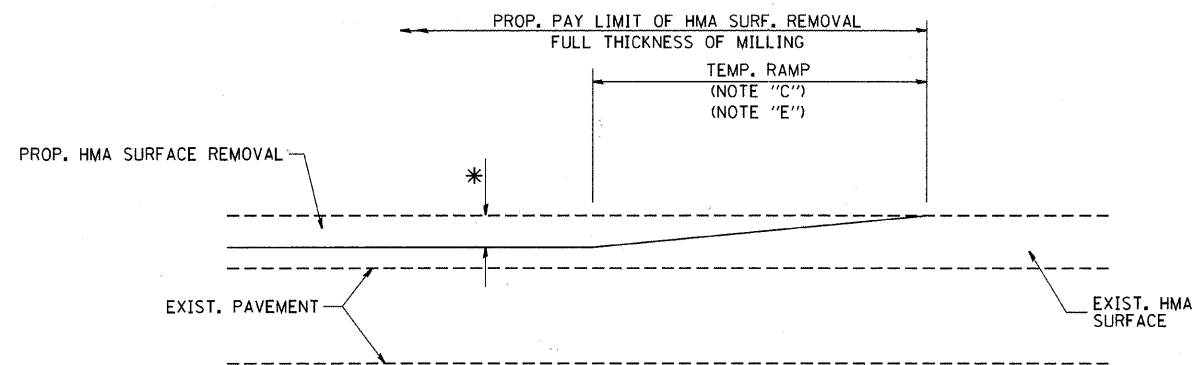
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

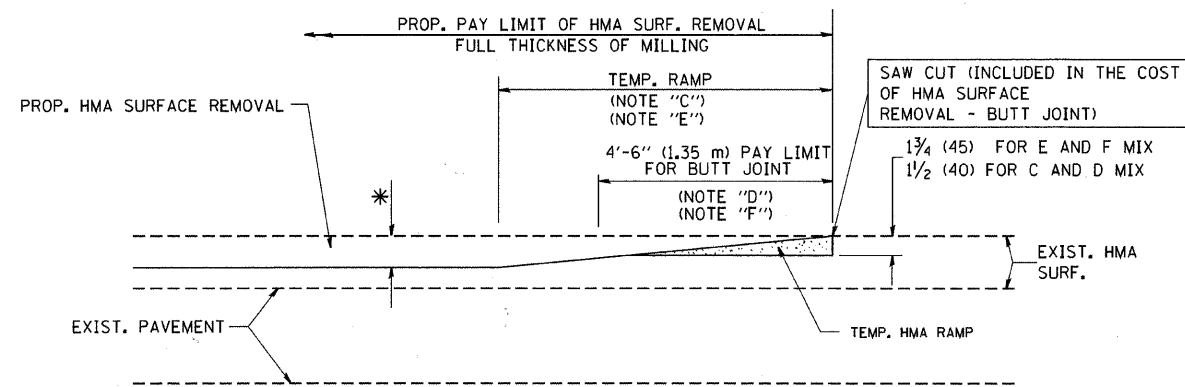
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-03 (BD-8) CONTRACT NO. 63483				
		PLOT SCALE = 50,0000 "/ IN.	REVISED - R. WIEDEMAN 05-14-04									
		PLOT DATE = 1/4/2008	REVISED - R. BORO 01-01-07									



MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

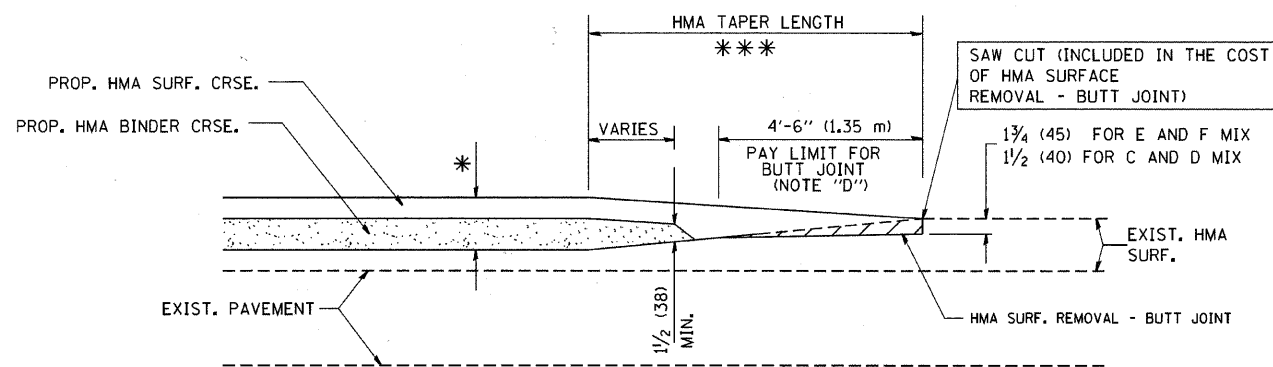
OPTION 1



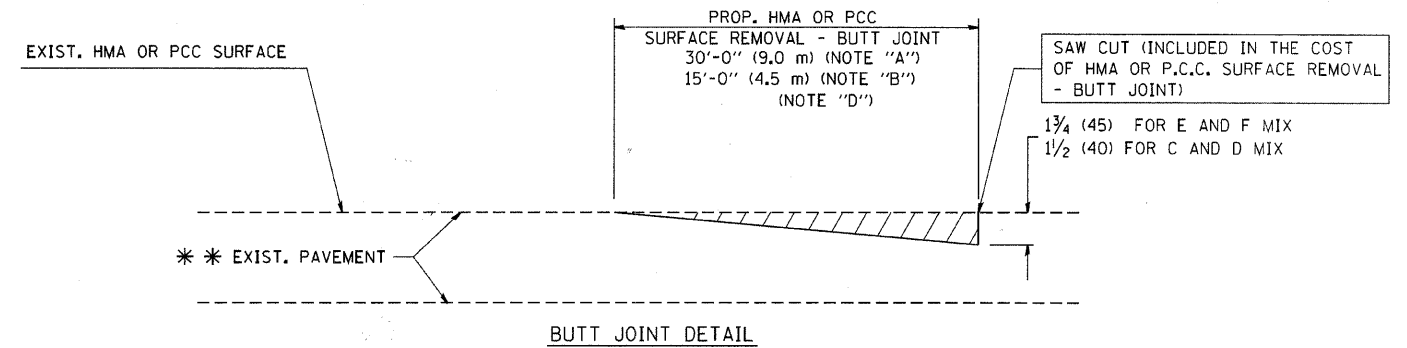
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

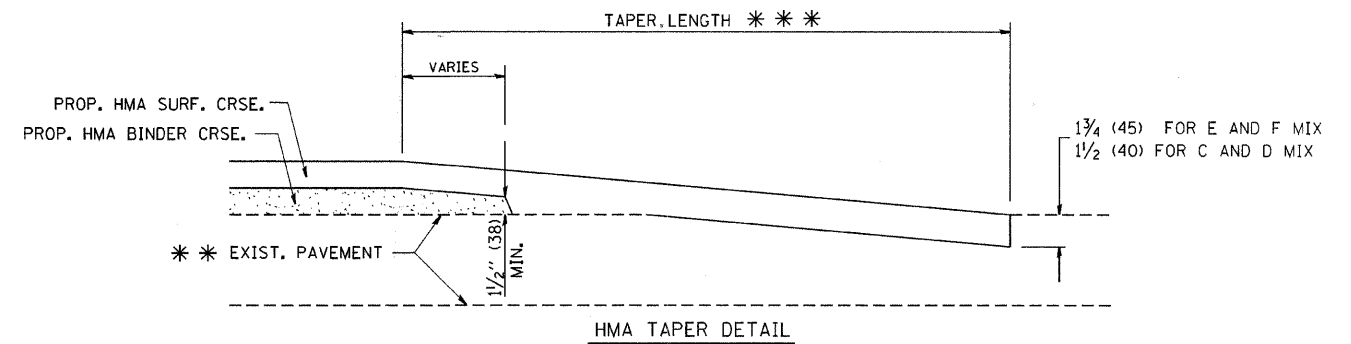
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER  
TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL  
TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dststd\22x34\bd32.dgn	USER NAME = gegl1enob1	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
		CHECKED -	REVISED - M. GOMEZ 04-06-01
		DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 1503	SECTION 10-00290-00-RS	COUNTY KANE	TOTAL SHEETS 14	SHEET NO. 14
BD400-05 BD32			CONTRACT NO. 63483	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				