

ITEM 2A
7-30-10 LETTING

TOTAL SHEETS: 12
PN005

CONSTRUCTION PLANS FOR PEKIN MUNICIPAL AIRPORT

PEKIN, TAZEWELL COUNTY, ILLINOIS

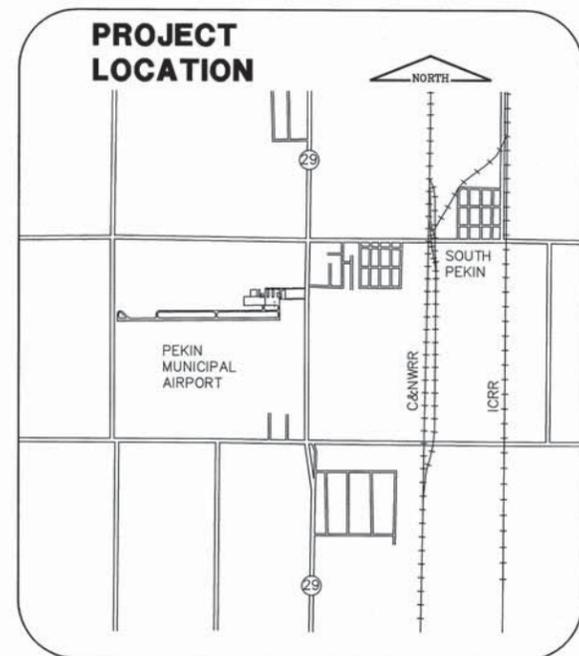
IL. PROJ. NO. C15-3990
AIP PROJ. NO. 3-17-0078-B11

REHABILITATE T-HANGAR PAVEMENT

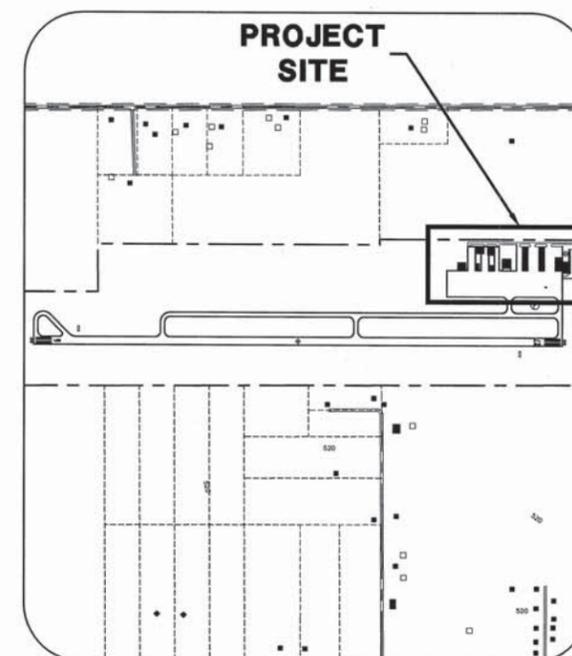
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10	INLET DETAIL
11	DRAINAGE DETAIL
12	TURFING AND EROSION CONTROL PLAN

ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR152450	SHOULDER EMBANKMENT	S.Y.	1575
AR152511	SUBGRADE REPAIR	S.Y.	1000
AR152540	SOIL STABILIZATION FABRIC	S.Y.	1000
AR209608	CRUSHED AGG. BASE COURSE - 8"	S.Y.	1100
AR401611	BIT SURFACE COURSE-METHOD 1	TON	1600
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	10675
AR401900	REMOVE BITUMINOUS PAVEMENT	S.Y.	1350
AR603510	BITUMINOUS TACK COAT	GAL	1600
AR701512	12" RCP, CLASS IV	L.F.	160
AR701518	18" RCP, CLASS IV	L.F.	48
AR701524	24" RCP, CLASS IV	L.F.	8
AR701900	REMOVE PIPE	L.F.	56
AR751410	INLET	EACH	4
AR751411	INLET-TYPE A	EACH	1
AR751900	REMOVE INLET	EACH	5
AR901510	SEEDING	ACRE	0.5
AR908510	MULCHING	ACRE	0.5
AR908520	EXCELSIOR BLANKET	SY	360

JUNE 4, 2010



LOCATION MAP



SITE PLAN

811 Know what's below. Call before you dig.
J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DESIGN INFORMATION
PAVEMENT DESIGN CRITERIA

STRUCTURAL DESIGN DATA

GROSS WEIGHT - 12,500 lbs.
AIRCRAFT SINGLE WHEEL GEAR
100 ANNUAL DEPARTURES

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: T24N
RANGE: R5W
SECTION: 33
COUNTY: TAZEWELL
CIVIL TOWNSHIP: CINCINNATI

6-11-10



Christopher B. Groth
Exp. 11-30-11

CITY OF PEKIN
AIRPORT COMMISSION

APPROVED *Robert A. Ehrlich*
DATE JUNE 10, 2010

CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

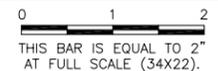
SUBMITTED BY *Christopher B. Groth*
DATE 6-11-10

CMT JOB NUMBER: 10069-02-00

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UPDATE BY: Chris Groth
PLOT DATE: 6/11/2010 3:01 PM

PN005

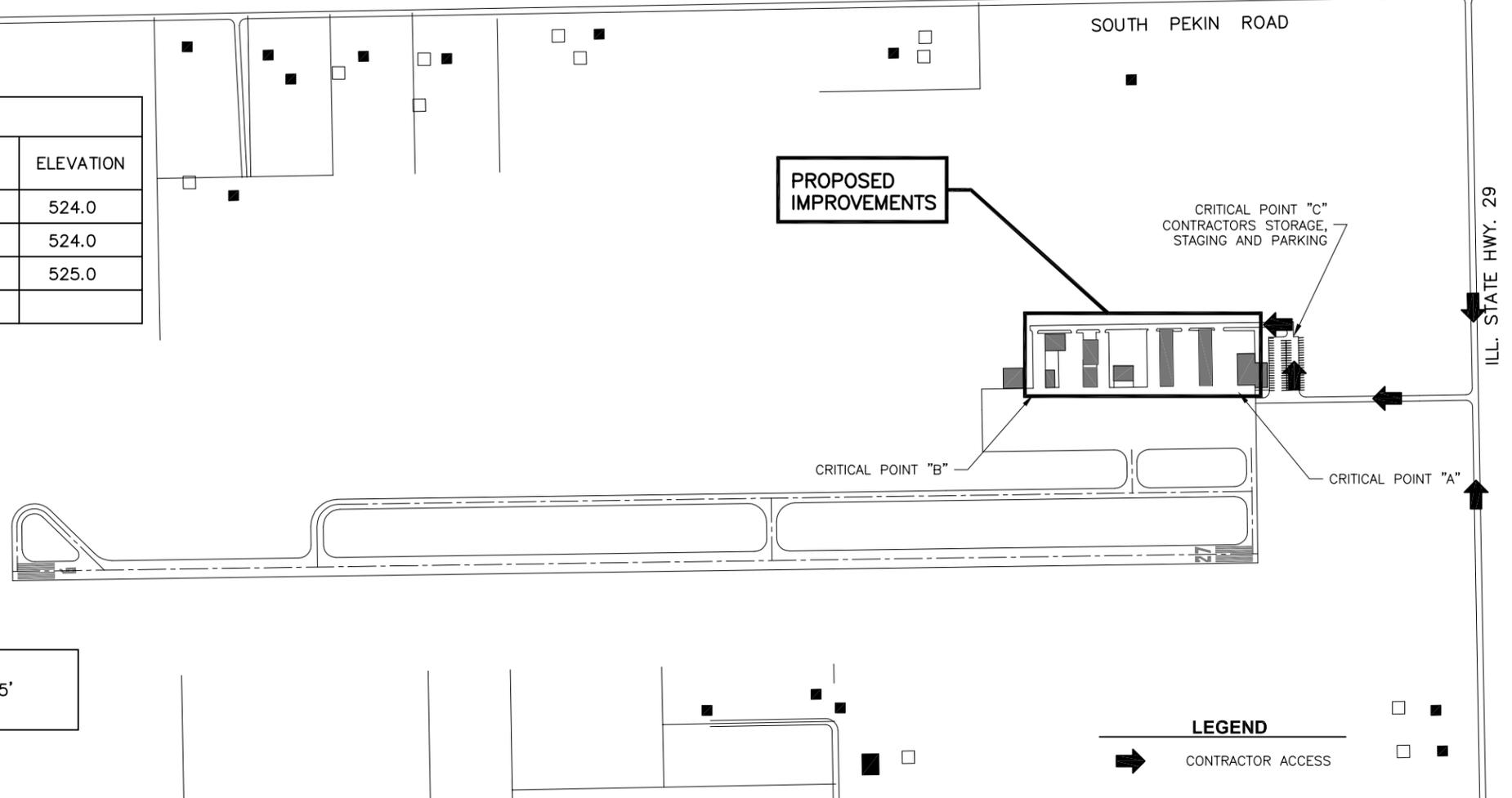
REVISIONS		
NUMBER	BY	DATE



CRITICAL POINTS			
POINT	LATITUDE N.	LONGITUDE W.	ELEVATION
A	40°29'24.14"	89°40'01.86"	524.0
B	40°29'24.06"	89°40'12.49"	524.0
C	40°29'26.29"	89°39'58.95"	525.0



UNICOM FREQUENCY: 122.80
 MAXIMUM HEIGHT OF EQUIPMENT: 25'



PEKIN MUNICIPAL AIRPORT
PEKIN, TAZEWELL COUNTY, ILLINOIS
REHABILITATE T-HANGAR PAVEMENT
SITE PLAN

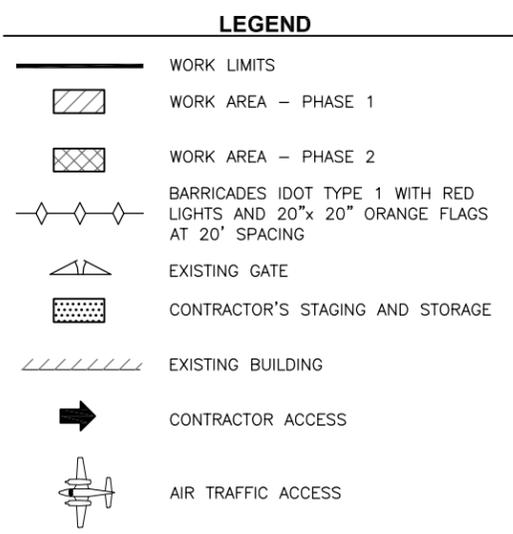
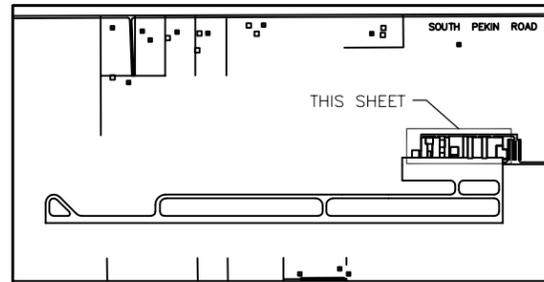
GENERAL NOTES

- 1.) ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
- 2.) ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS, AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL OF FLAGMAN IN RADIO CONTACT WITH AIR TRAFFIC. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO ARE FAMILIAR WITH AIRCRAFT OPERATIONS.
- 3.) WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT MANAGER.
- 4.) THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- 5.) BROKEN CONCRETE AND ASPHALT SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY.
- 6.) VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 40' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 200' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- 7.) ALL PAVEMENTS, DRIVES, OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT MANAGER. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- 8.) EXISTING TURF AREAS OUTSIDE THE GRADING LIMITS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT MANAGER.
- 9.) THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- 10.) IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES WITHIN THE PROJECT SITE SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- 11.) CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A.) THE CONTRACTOR'S ACCESS TO WORK SHALL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN.
 - B.) THE CONTRACTOR SHALL COMPLETE A SECURITY FORM FOR ALL PERSONNEL HE PROPOSES TO USE ON THE AIRPORT. THESE FORMS SHALL BE COMPLETED PRIOR TO THAT PERSON BEING ALLOWED ON THE AIRFIELD. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR.
 - C.) THE CONTRACTOR SHALL USE AN EXISTING GATE FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR MAY BE REQUIRED TO INSTALL AND MAINTAIN A HEAVY-DUTY PADLOCK ON THE ACCESS GATE. HE SHALL PROVIDE KEYS FOR HIS PADLOCK TO THE RESIDENT ENGINEER AND AIRPORT MANAGEMENT. NO ADDITIONAL KEYS ARE TO BE DISTRIBUTED UNLESS AUTHORIZED BY THE RESIDENT ENGINEER.
 - D.) CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL TRAFFIC CONTROL SIGNAGE REQUIRED BY THE COUNTY, TOWNSHIP, OR I.D.O.T. .
 - E.) DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - F.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS.
 - G.) THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - H.) ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - I.) THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY, TAXIWAY SAFETY AREAS, AND OBJECT FREE AREAS.
 - J.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE CROSSED BY HIS VEHICLES ACCESSING THE WORK OR DEPARTING THE WORK IMMEDIATELY FOLLOWING SAID VEHICLE.
 - K.) THE CONTRACTOR MAY USE ALTERNATE AIRPORT GATES TO DELIVER LARGER EQUIPMENT TO THE JOB SITE. THE CONTRACTOR SHALL COORDINATE ARRIVAL AND DEPARTURE OF THESE DELIVERIES WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER.

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APPROVED BY:	CET
DATE:	JUNE 4, 2010
JOB No:	10069-02-00
IL. PROJ. NO. C15-3990 AIP PROJ. NO. 3-17-0078-B11	
SHEET 02 OF 12 SHEETS	

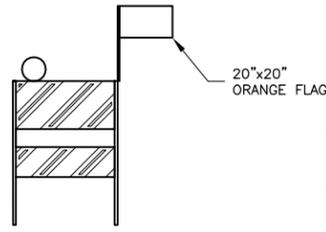
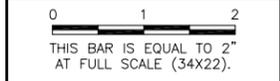


- NOTES**
1. THE AIRPORT WILL REQUIRE 7 CALENDAR DAYS NOTICE FROM THE CONTRACTOR PRIOR TO INITIATING ANY PHASE TO COORDINATE WITH THE TENANTS.
 2. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THESE PHASES.
 3. PHASE 1 BARRICADES SHALL BE PLACED FOR THE DURATION OF WORK IN PHASE 1.
 4. PHASE 1 & 2 SHALL NOT BE CONSTRUCTED CONCURRENTLY.
 5. ALL RUNWAYS, TAXIWAYS, AND MAIN APRON SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION.
 6. CONTRACTOR SHALL WORK WITH THE AIRPORT AND THE AIRPORT TENANTS SUCH THAT CONSTRUCTION OPERATIONS TIME WILL NOT EXCESSIVELY DELAY HANGAR ACCESS.
 7. IF DURING CONSTRUCTION IT IS FOUND THAT FULLY LOADED TRUCKS ARE EXCESSIVELY DAMAGING THE EXISTING SUBGRADE OR PAVEMENT, THE CONTRACTOR MAY BE REQUIRED TO LIMIT THE WEIGHT OF DELIVERY TRUCKS. THE CONTRACTOR SHALL REPAIR DAMAGE TO THE EXISTING SUBGRADE OR PAVEMENT CAUSED BY FULLY LOADED TRUCKS. COST TO REPAIR THE DAMAGED AREAS SHALL BE INCIDENTAL TO THE PROJECT.
 8. THE CONTRACTOR SHALL MAINTAIN HAUL ROAD TO SUPPORT ALL WEATHER HAULING OPERATIONS AND RESTORE TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT.

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 PLOT DATE: 6/11/2010 3:58 PM
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 KEY
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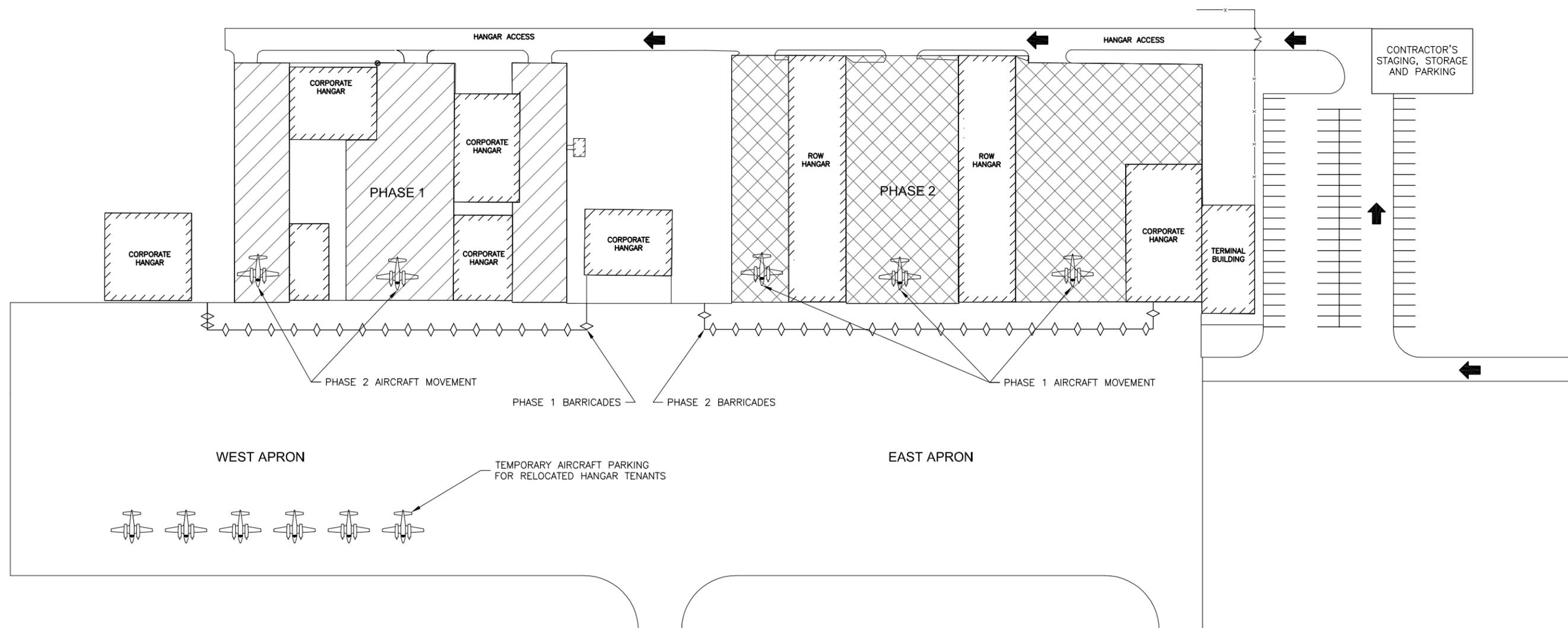
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NUMBER	BY	DATE



- FLASHER BARRICADE NOTES**
1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
 2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
 3. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
 4. PLACE AT 20' INTERVALS.

FLASHER BARRICADE DETAIL
N.T.S.

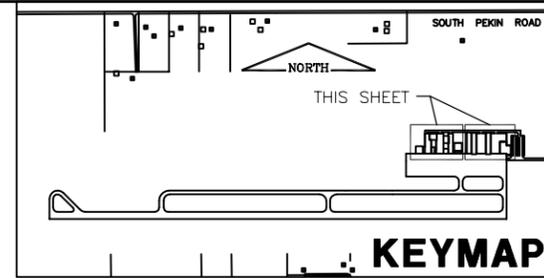
**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENT
 CONSTRUCTION ACTIVITY PLAN**



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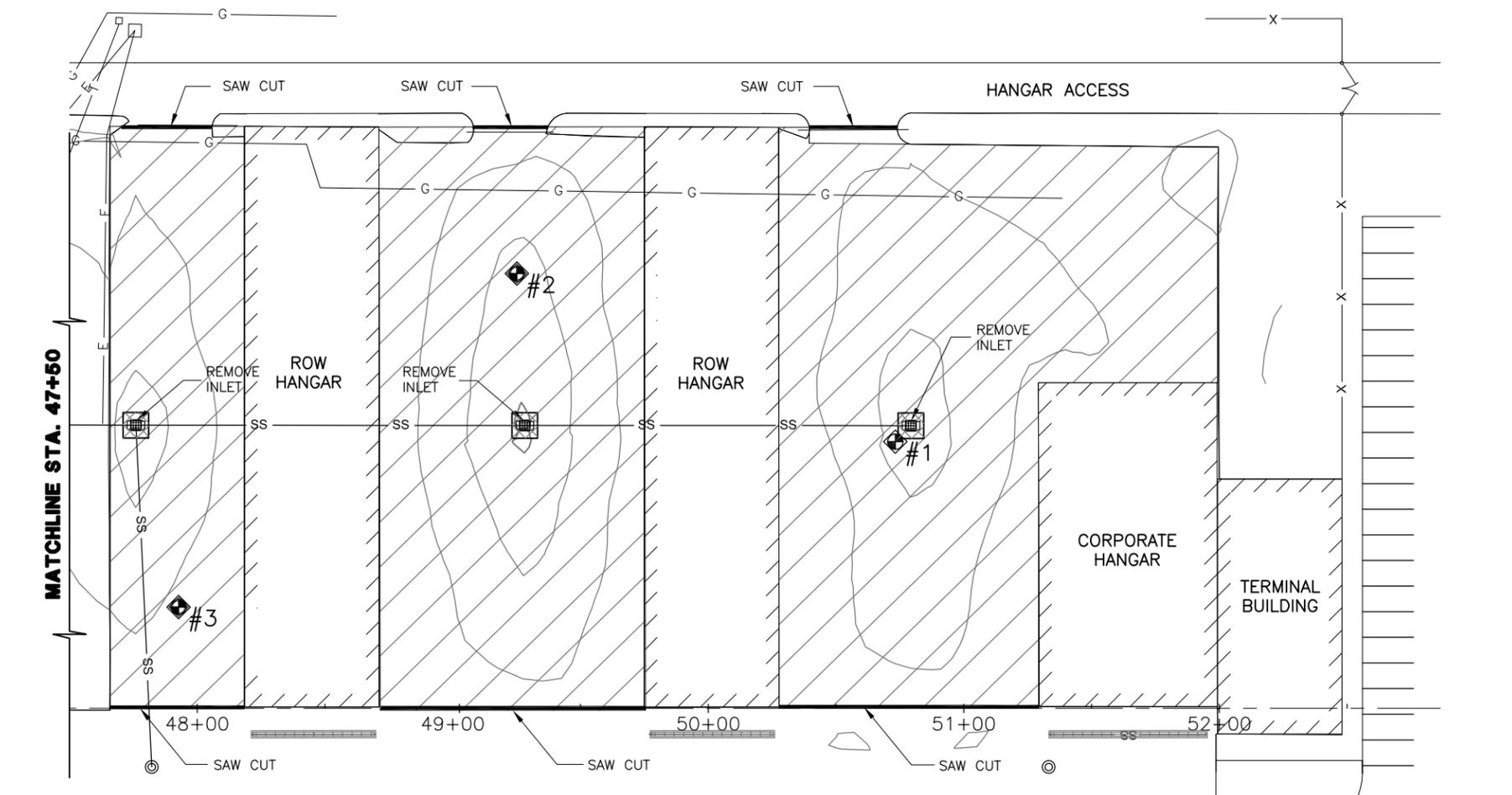
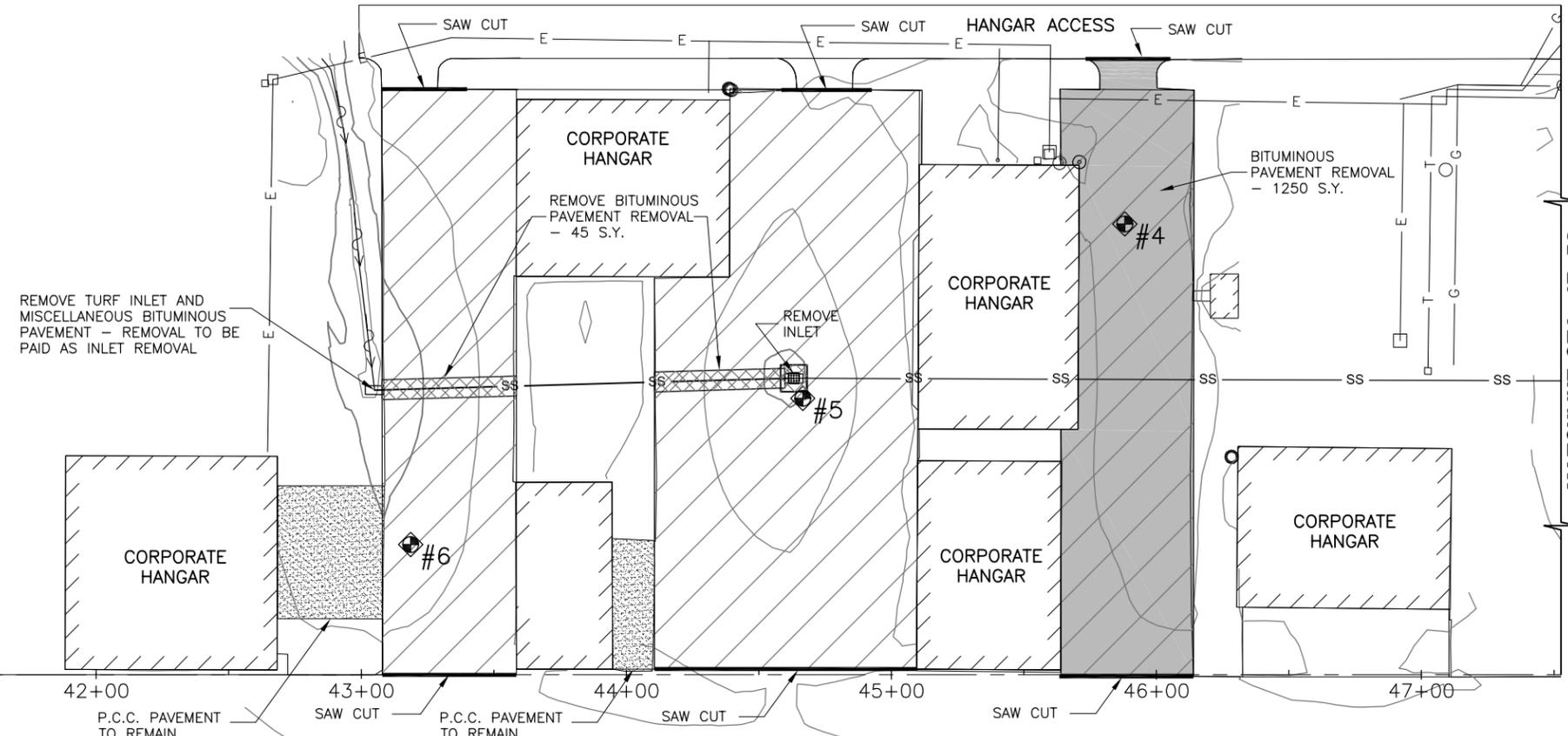
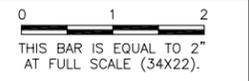
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IL. PROJ. NO. C15-3990 AIP PROJ. NO. 3-17-0078-B11	
SHEET 03 OF 12 SHEETS	



PN005

REVISIONS		
NUMBER	BY	DATE



LEGEND

	REMOVE HMA PAVEMENT
	BITUMINOUS PAVEMENT MILLING
	EXISTING PAVEMENT
	EXISTING ELECTRIC
	EXISTING GAS
	EXISTING WATER
	EXISTING STORM SEWER
	EXISTING TELEPHONE
	EXISTING DUCT
	EXISTING TIEDOWN
	REMOVAL
	EXISTING SWALE
	SAWCUT
	SOIL BORING
	EXISTING BUILDING
	EXISTING GATE
	EXISTING FENCE
	EXISTING TRENCH DRAIN

- NOTES**
- EXISTING HANGAR BUILDINGS HAVE UTILITY CONNECTIONS, SLIDING DOOR HARDWARE AND DRAINAGE HARDWARE THAT EXTENDS BEYOND THE FOOTPRINT OF THE BUILDING (NOT SHOWN).
 - THE MILLING OPERATIONS SHALL REMAIN A FIXED DISTANCE FROM THE FACE OF THE BUILDING WHERE THERE ARE NO BUILDING DOORS. THE RESIDENT ENGINEER WILL DETERMINE THE ACCEPTABLE OFFSET FOR THE MILLING EQUIPMENT. MILLING OPERATIONS AT THE BUILDING DOORS SHALL BE MILLED UP TO THE BUILDING FOUNDATION. MILLING ALONG THE BUILDING FOUNDATION MAY REQUIRE SPECIAL EQUIPMENT CAPABLE MILLING IN CLOSE PROXIMITY OF A VERTICAL FACE OR THE USE OF HAND TOOLS. COSTS ASSOCIATED WITH THIS MILLING SHALL BE INCIDENTAL TO THE MILLING PAY ITEM.
 - CONTRACTOR SHALL NOT DAMAGE EXISTING BUILDING FOUNDATIONS OR BUILDING DOORS. DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
 - SAWCUT SHOWN SHALL BE INCIDENTAL TO MILLING OPERATIONS. ADDITIONAL AREAS NOT SHOWN MAY REQUIRE SAWCUT. NO ADDITIONAL PAYMENTS WILL BE MADE FOR SAWCUT.
 - PUNCH THROUGH AREA SHOWN HAVE BEEN APPROXIMATED BASED OFF OF PAVEMENT CORES AND PROPOSED PAVEMENT GRADES. ACTUAL PUNCH THROUGH LIMITS MAY VARY.

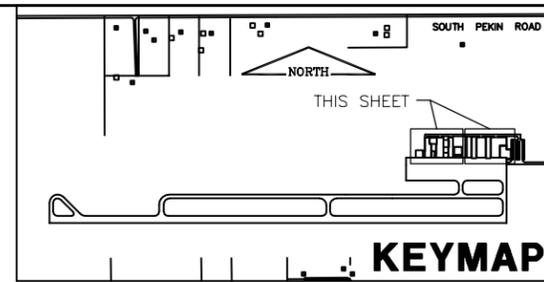
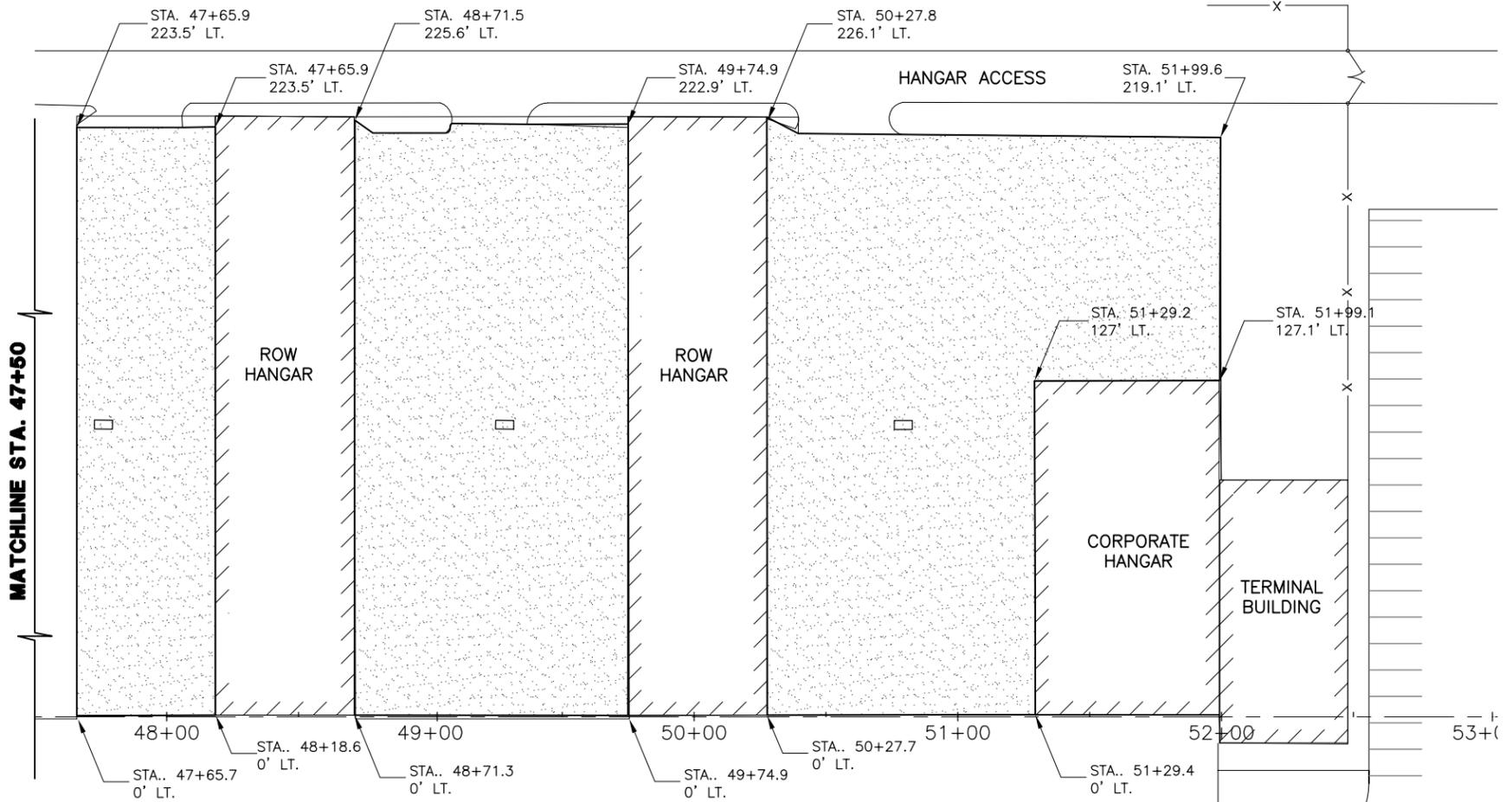
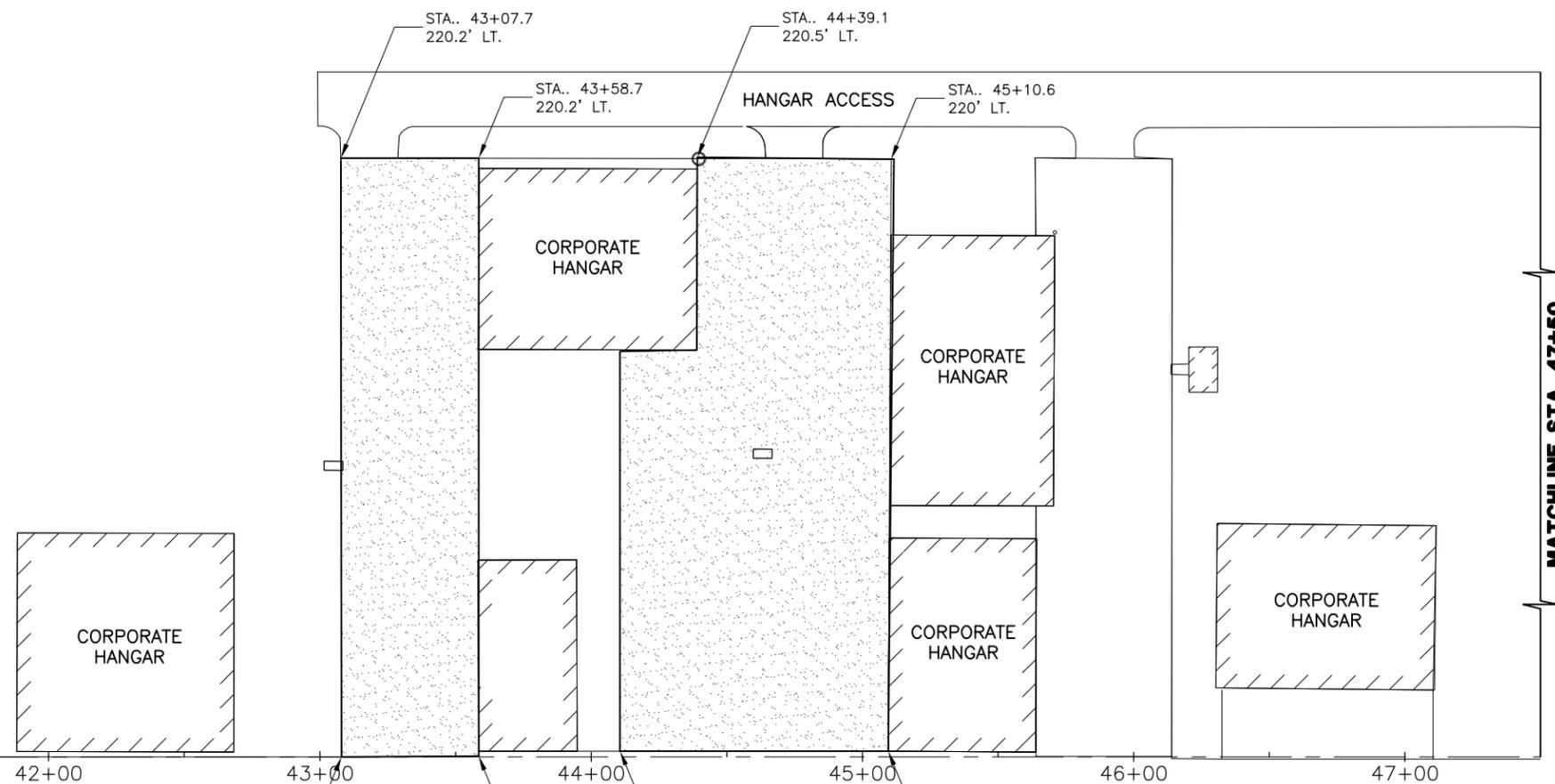
THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENT
 EXISTING CONDITIONS AND REMOVALS**

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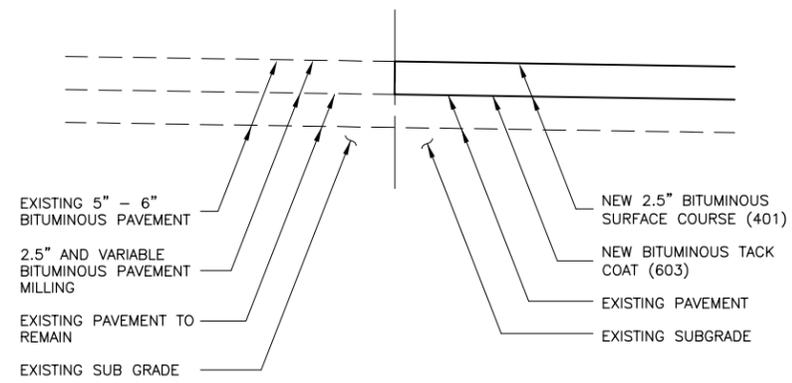
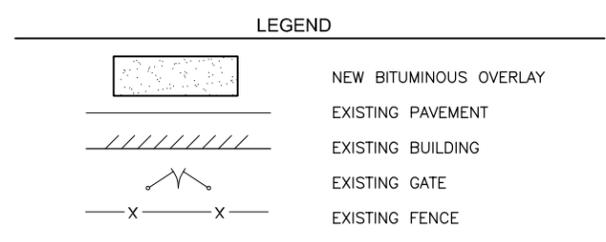
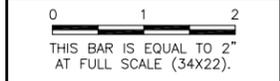


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 FILE: NEW IMPROVEMENTS 1.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 6/11/2010 3:58 PM
 Pekin_Base_Rev
 Pekin Hang EP Base
 KEY

PN005

REVISIONS		
NUMBER	BY	DATE

CONTROL POINTS			
STATION	OFFSET	NORTHING	EASTING
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51+00	0' RT.,LT.	1392796.67	2435476.97

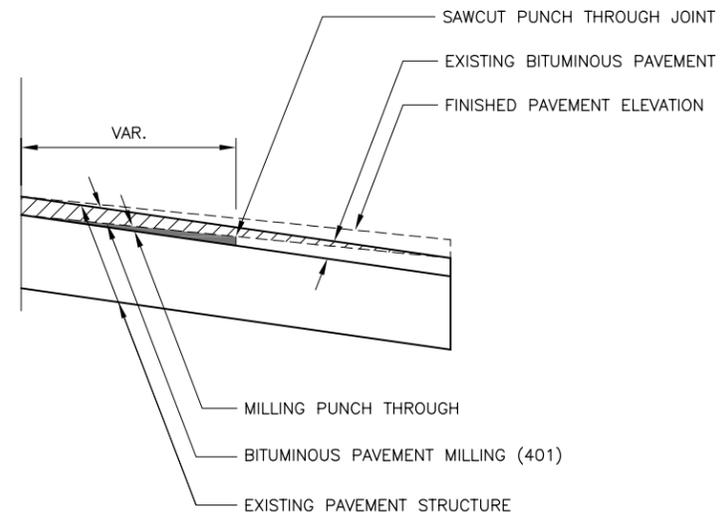


TYPICAL PAVEMENT SECTION
 NTS

PEKIN MUNICIPAL AIRPORT
PEKIN, TAZEWELL COUNTY, ILLINOIS
REHABILITATE T-HANGAR PAVEMENT
PROPOSED IMPROVEMENTS

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SHEET	05 OF 12 SHEETS

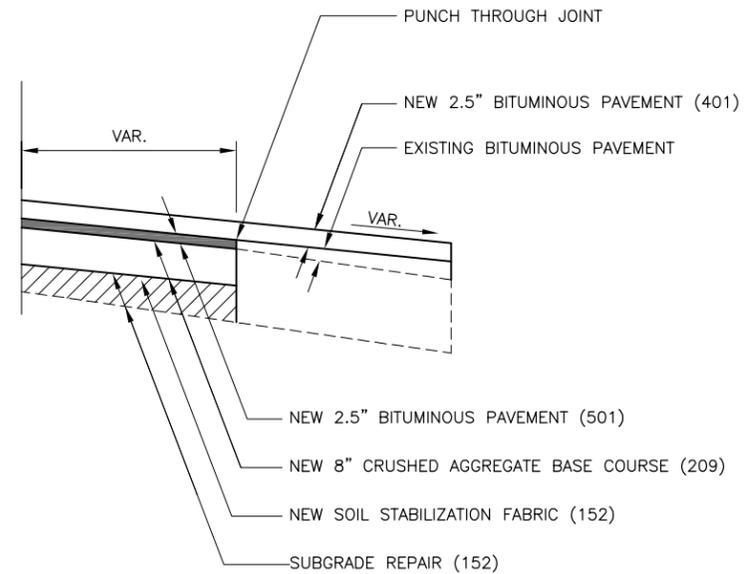


MILLING PUNCH THROUGH AREA

N.T.S.

NOTES

1. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER OF AREAS OF PAVEMENT MILLING THAT MAY REQUIRE REPAIR.
2. PRIOR TO BEGINNING SUBGRADE REPAIR, THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL AGREE ON THE LIMITS OF THE REPAIR.
3. THE CONTRACTOR SHALL SAW CUT THE FACE OF THE PUNCH THROUGH JOINT.

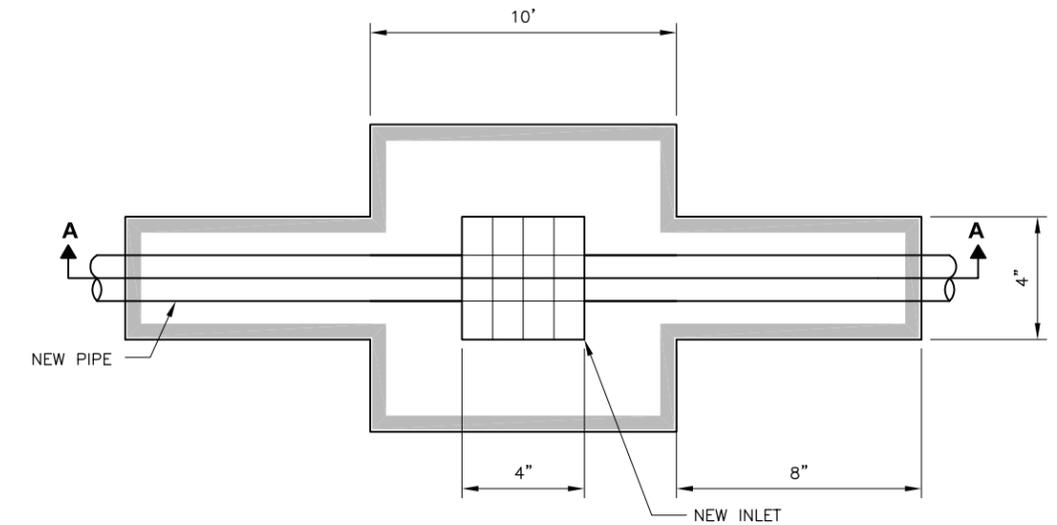


MILLING PUNCH THROUGH REPAIR

N.T.S.

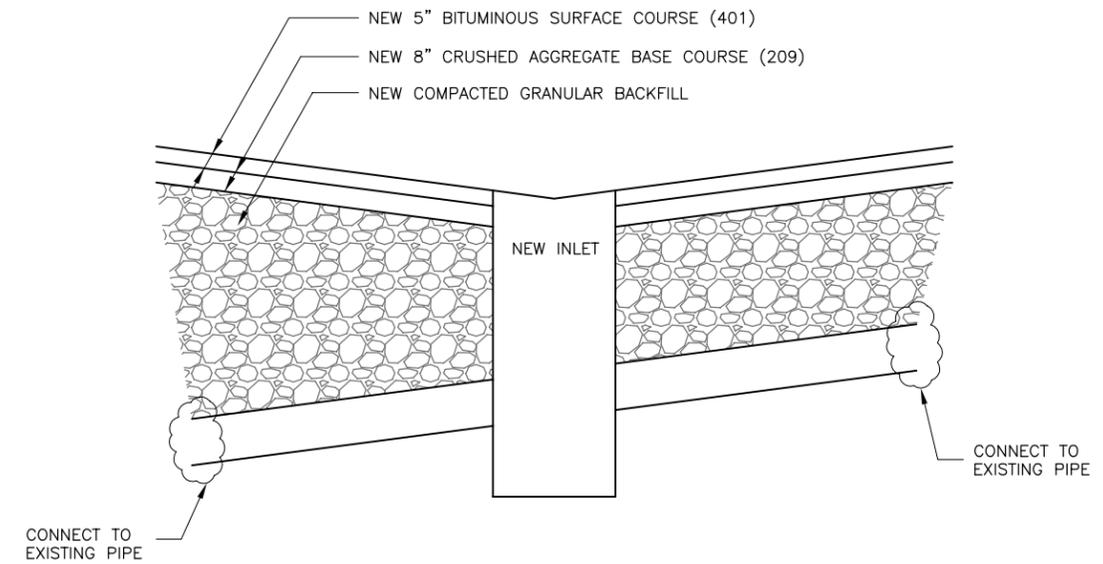
NOTES

1. CRUSHED AGGREGATE BASE COURSE (209) SHALL BE PLACED IN A MANOR APPROVED BY THE RESIDENT ENGINEER.
2. SUBGRADE REPAIR SHALL INCLUDE EXCAVATION REQUIRED TO CONSTRUCT THE AGGREGATE BASE COURSE.
3. SUBGRADE REPAIR SHALL INCLUDE COMPACTING THE SUBGRADE PER THE REQUIREMENTS OF ITEM 152.
4. CRUSHED AGGREGATE BASE COURSE (209) SHALL BE COMPACTED PER THE SPECIFICATIONS.
5. THE CONTRACTOR MAY REGRADE AND COMPACT THE EXISTING AGGREGATE SUBGRADE IN LIEU OF EXCAVATING AND ADDING NEW MATERIAL AT THE APPROVAL OF THE RESIDENT ENGINEER. COMPACTION SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER. PAYMENT FOR THIS WORK SHALL BE MADE UNDER SUBGRADE REPAIR.



PLAN VIEW

N.T.S.



INLET/PAVEMENT REMOVAL DETAIL SECTION A-A

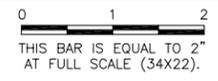
N.T.S.

NOTES:

1. CONTRACTOR SHALL SAW CUT THE REMOVAL LIMITS SHOWN, PER THE HMA REMOVED SPECIFICATION.
2. HMA REMOVAL REQUIRED OUTSIDE THE LIMITS SHOWN SHALL BE INCIDENTAL TO REMOVE PIPE AND REMOVE INLET.
3. CONTRACTOR SHALL REMOVE THE SECTION OF PIPE CONNECTED TO THE EXISTING STRUCTURE AND INSTALL NEW PIPE WHEN INSTALLING NEW INLET.

PN005

REVISIONS		
NUMBER	BY	DATE



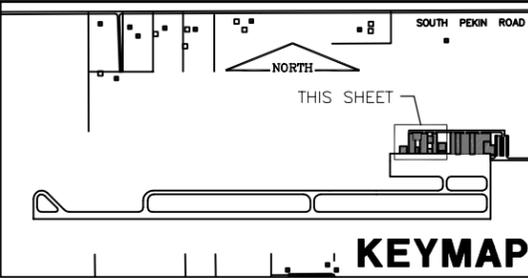
PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS

REHABILITATE T-HANGAR PAVEMENT
 PAVEMENT REPAIR AND INLET REMOVAL DETAIL

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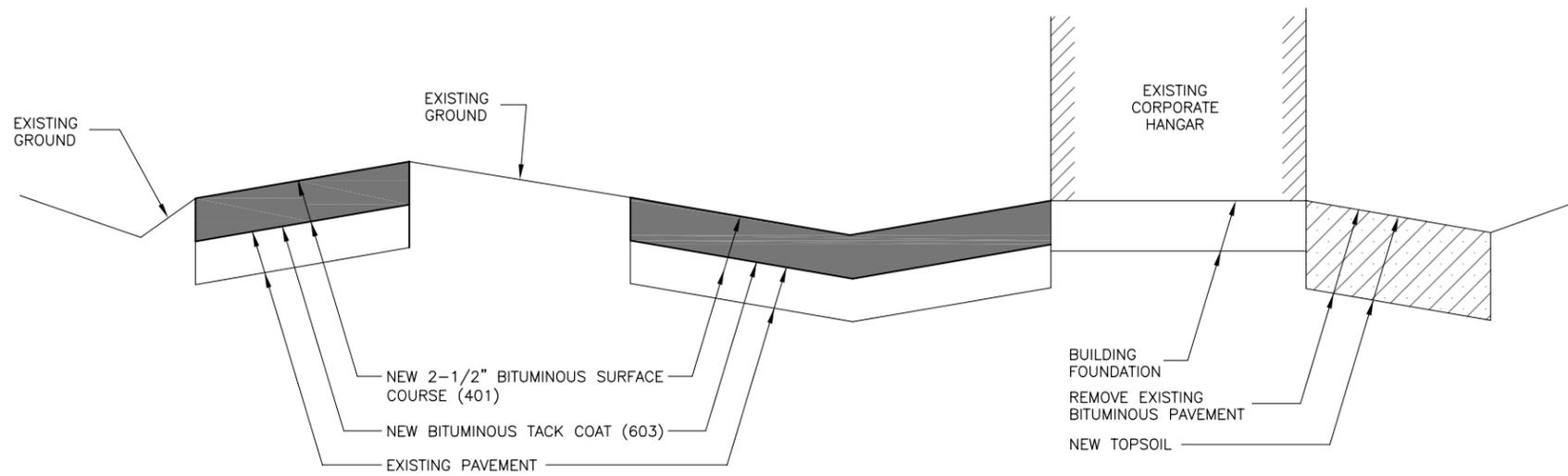
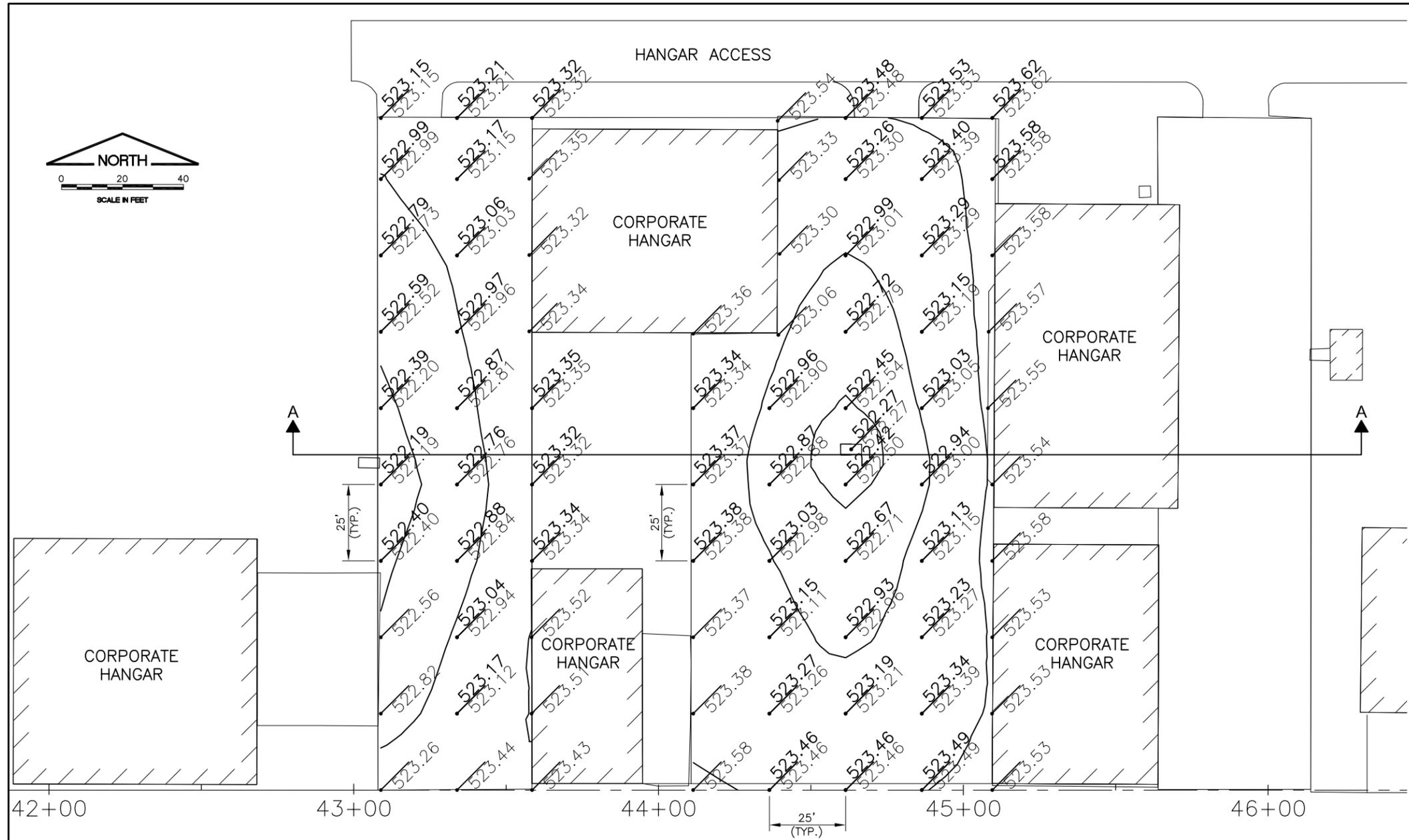
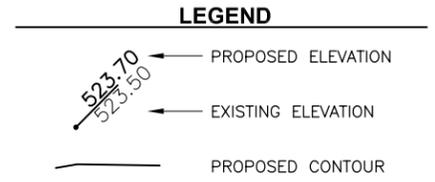
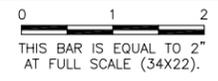


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DRAWN BY:	CMT
CHECKED BY:	CBG
APPROVED BY:	CET
DATE:	JUNE 4, 2010
JOB No:	10069-02-00
IL PROJ. NO.	C15-3990
AIP PROJ. NO.	3-17-0078-B11
SHEET	06 OF 12 SHEETS



PN005

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NUMBER	BY	DATE

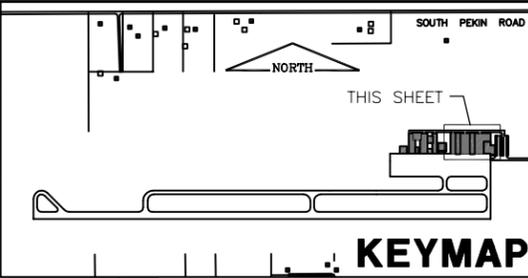


TYPICAL SECTION
 N.T.S.

**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENT
 STAKING PLAN 1**

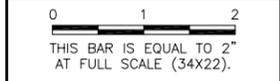
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SHEET 07 OF 12 SHEETS	



PN005

REVISIONS		
NUMBER	BY	DATE



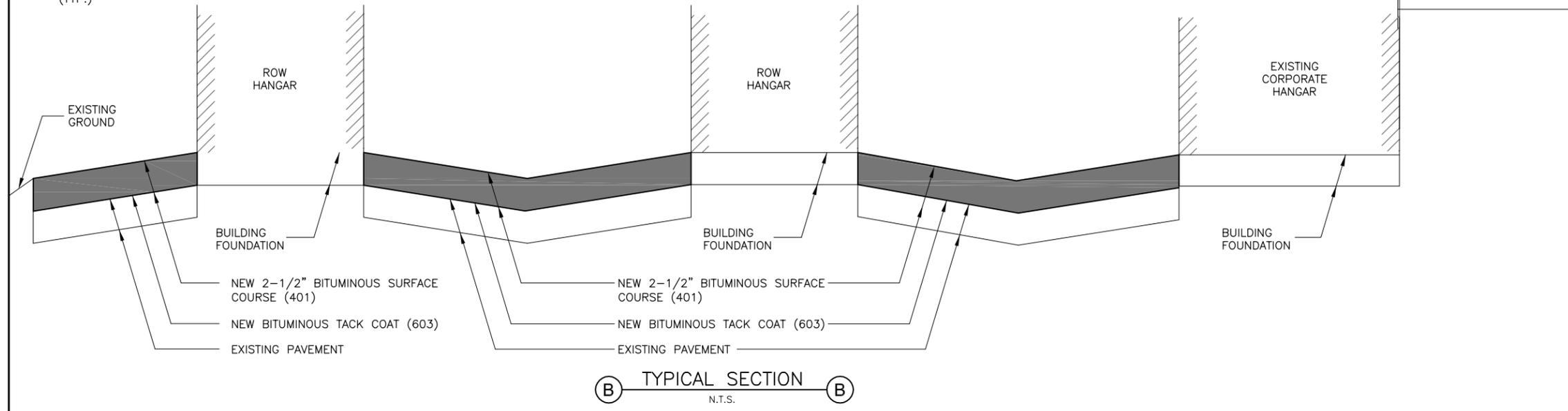
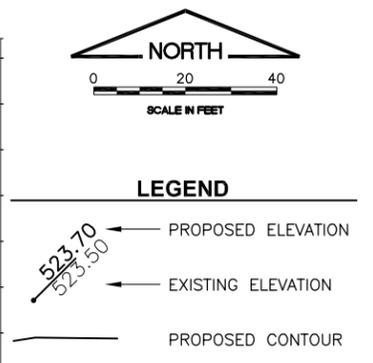
**PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS**

REHABILITATE T-HANGAR PAVEMENT

STAKING PLAN 2

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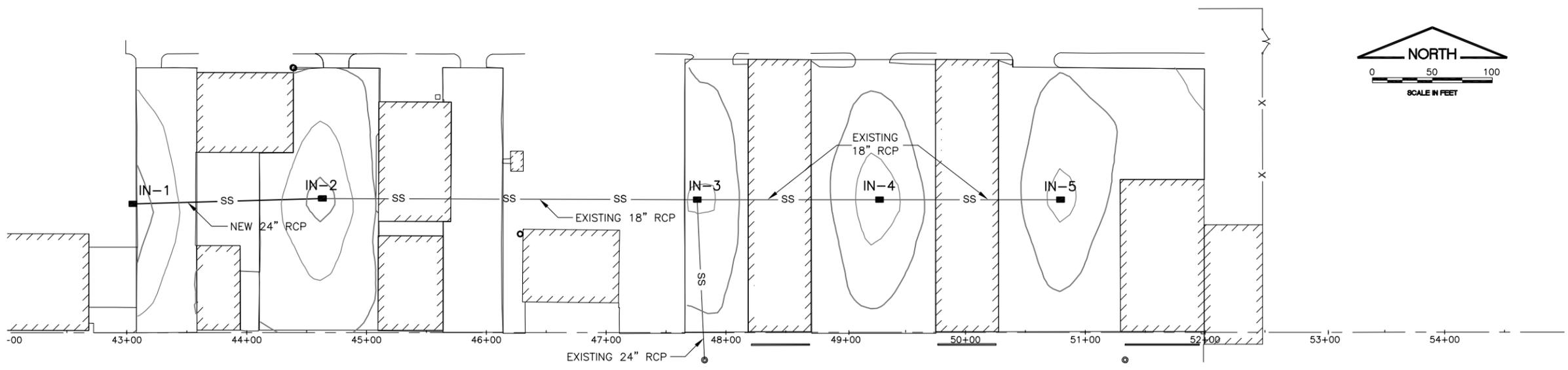
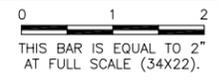
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DATE:	JUNE 4, 2010
JOB No:	10069-02-00
IL. PROJ. NO. C15-3990 AIP PROJ. NO. 3-17-0078-B11	
SHEET 08 OF 12 SHEETS	



(B) TYPICAL SECTION (B)
 N.T.S.

PN005

REVISIONS		
NUMBER	BY	DATE

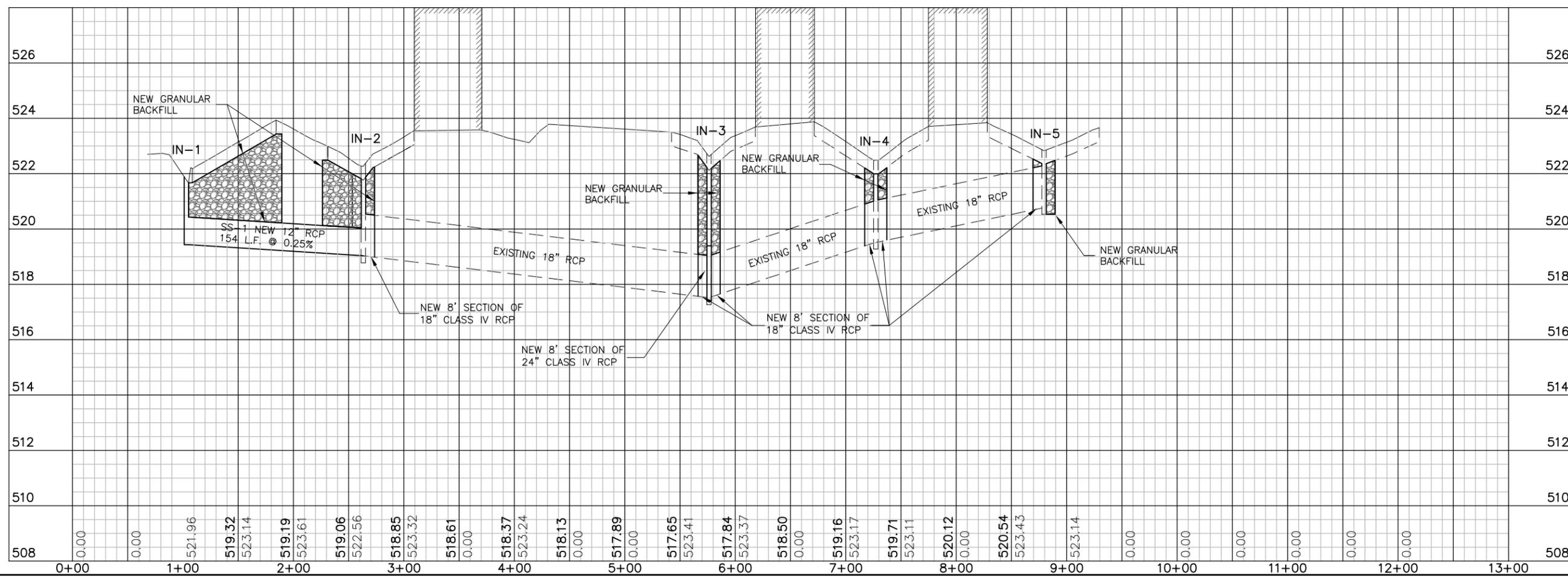


NOTE:
 1. CONTRACTOR SHALL FIELD VERIFY PIPE SIZES AND STRUCTURE INVERTS PRIOR TO CONSTRUCTING INLETS.

SCALES:
 1" = 50' HOR
 1" = 2' VER

LEGEND	
— SS —	EXISTING STORM SEWER
□	EXISTING INLET
— SS —	NEW STORM SEWER
■	NEW INLET

STRUCTURE TABLE						
STRUCTURE	STATION	OFFSET	RIM ELEV.	INVERT ELEV.	IDOT STD.	PAY ITEM
IN-1	43+05	107.1' LT.	521.66	519.43	602301-01	AR751411
IN-2	44+63.2	111.5' LT.	522.27	519.03	542546	AR751410
IN-3	47+76.1	110.5' LT.	522.62	517.52	542546	AR751410
IN-4	49+28.2	110.4' LT.	522.47	519.52	542546	AR751410
IN-5	50+79.3	110.3' LT.	522.83	520.78	542546	AR751410



PEKIN MUNICIPAL AIRPORT
PEKIN, TAZEWELL COUNTY, ILLINOIS
REHABILITATE T-HANGAR PAVEMENT
PROPOSED DRAINAGE AND PIPE PROFILE

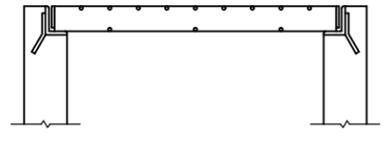
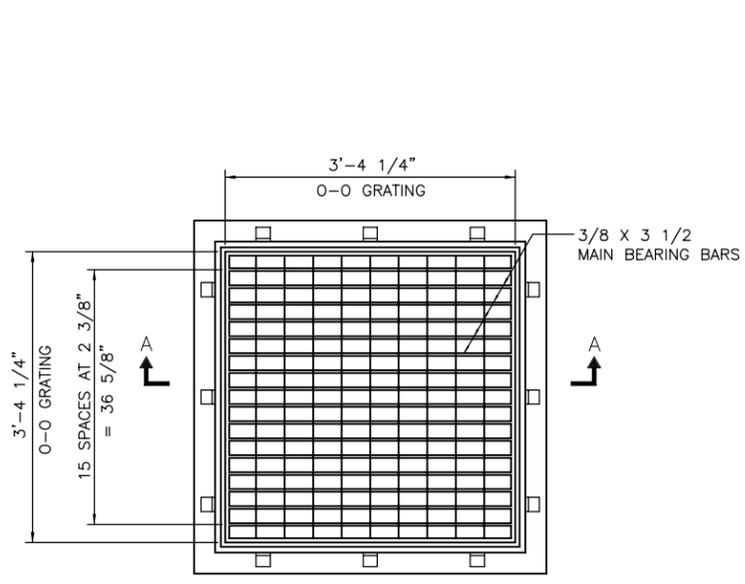
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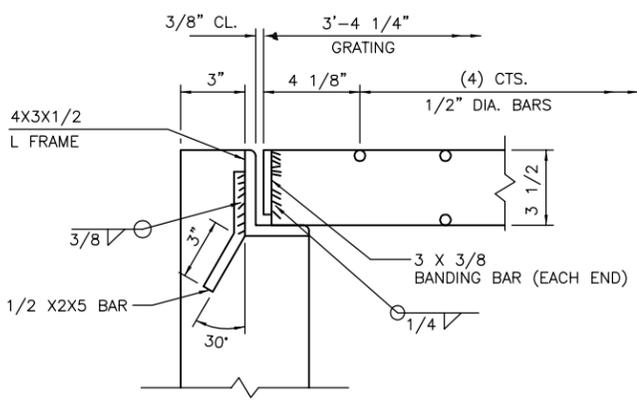
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REVISIONS		
NUMBER	BY	DATE

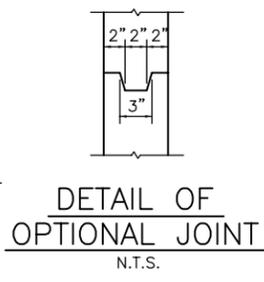
0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



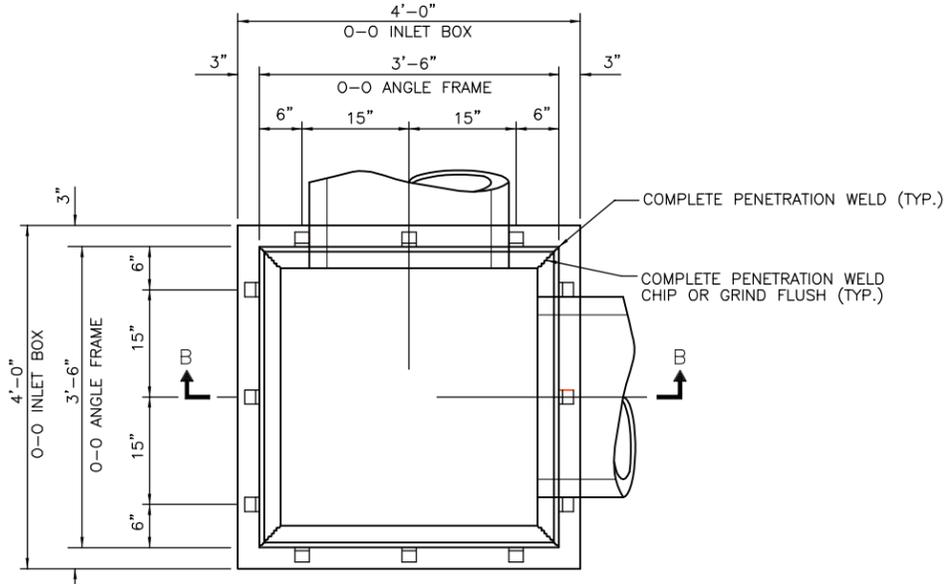
SECTION A-A
N.T.S.



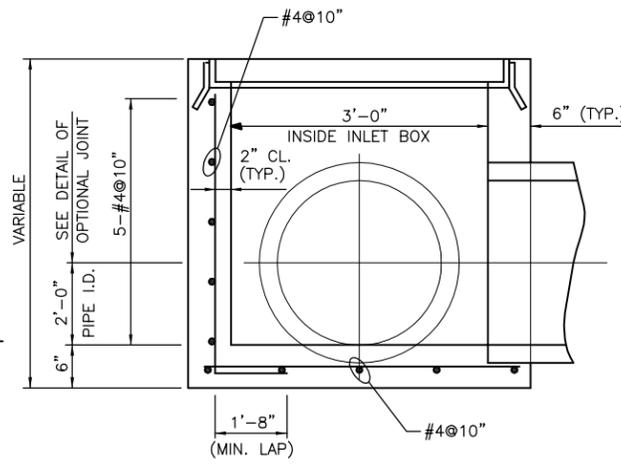
STEEL FRAME & GRATE
N.T.S.



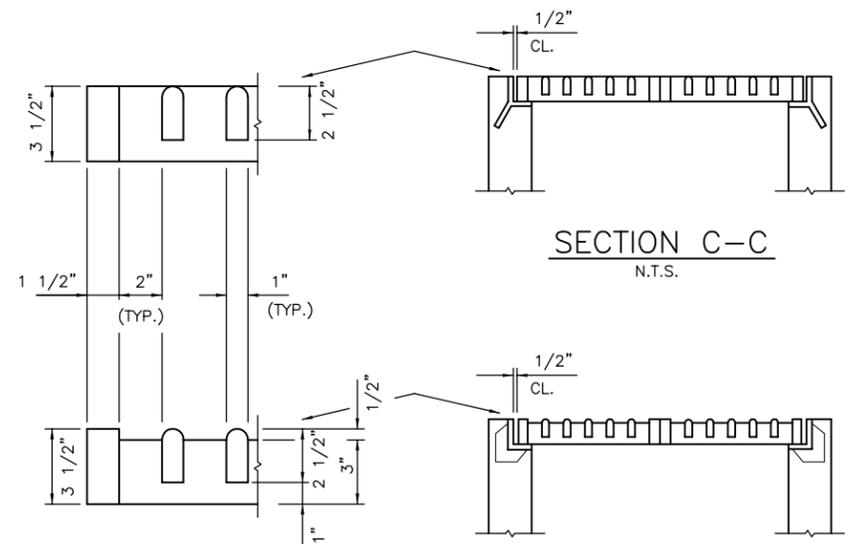
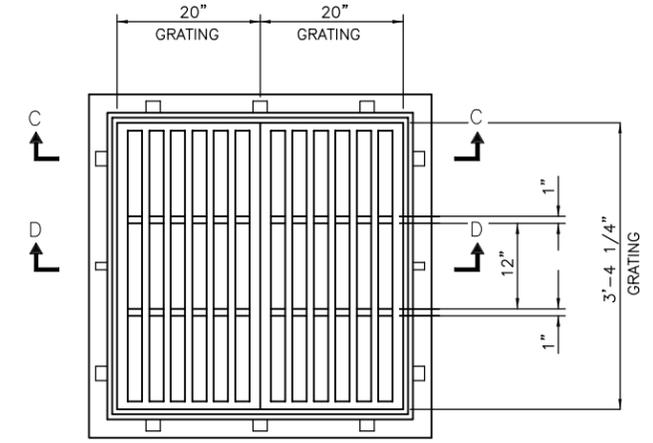
DETAIL OF OPTIONAL JOINT
N.T.S.



(GRATING OMITTED FOR CLARITY)
PLAN
N.T.S.



SECTION B-B
N.T.S.



SECTION C-C
N.T.S.

SECTION D-D
CAST FRAME & GRATE
N.T.S.

ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE SHOWN.

- NOTE:**
- FRAMES AND GRATES ARE A NEENAH R-3807 OR APPROVED EQUAL.
 - PROPOSED INLETS SHALL MEET THE REQUIREMENTS OF IDOT STANDARD 542546.

PEKIN MUNICIPAL AIRPORT
PEKIN, TAZEWELL COUNTY, ILLINOIS
REHABILITATE T-HANGAR PAVEMENT
INLET DETAIL

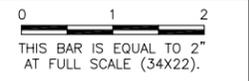


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SHEET	10 OF 12 SHEETS

PN005

REVISIONS		
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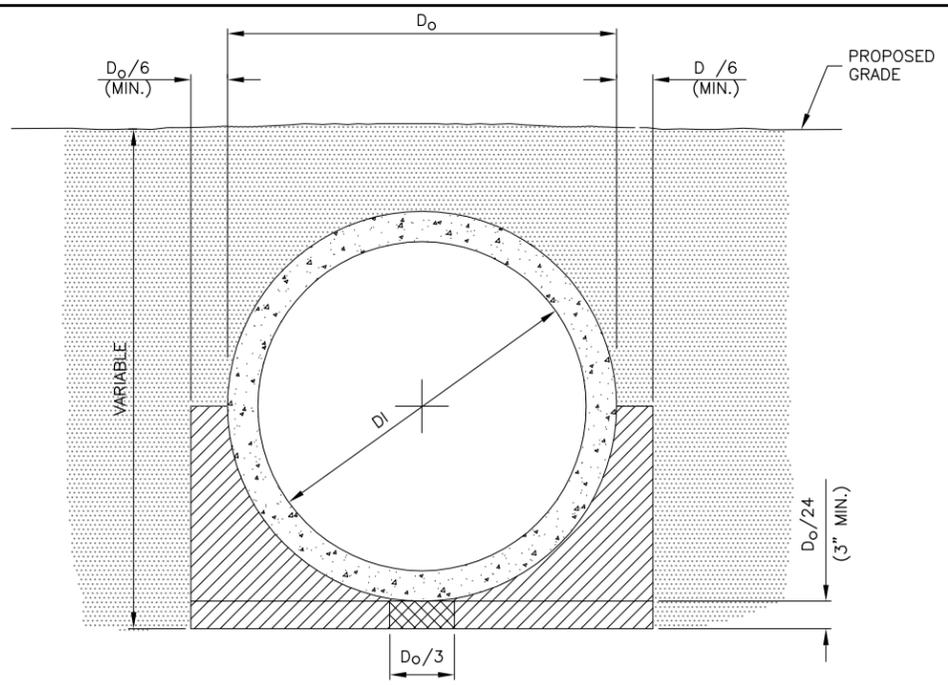


LEGEND

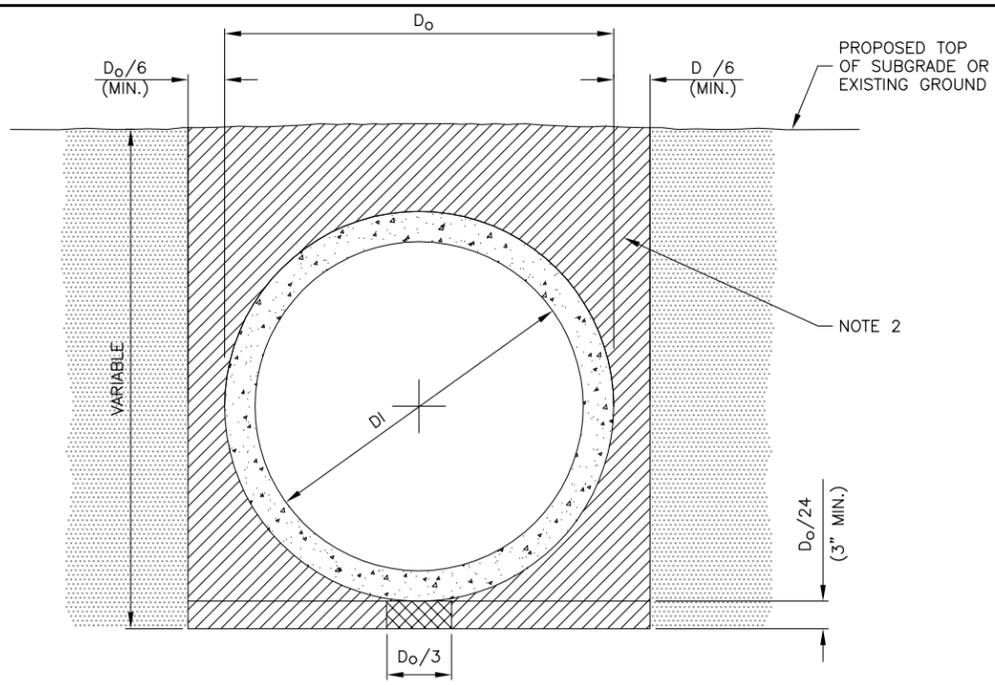
- DRAINAGE CONDUIT MATERIAL—CONCRETE
- MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
- HAUNCH AND OUTER BEDDING COMPACTION – TO ENGINEER'S SATISFACTION OF 95% STANDARD PROCTOR
- LOWER SIDE AND OVERFILL COMPACTION— SAME AS EMBANKMENT REQUIREMENTS
- D_o PIPE OUTSIDE DIAMETER
- D_i PIPE INSIDE DIAMETER

NOTES

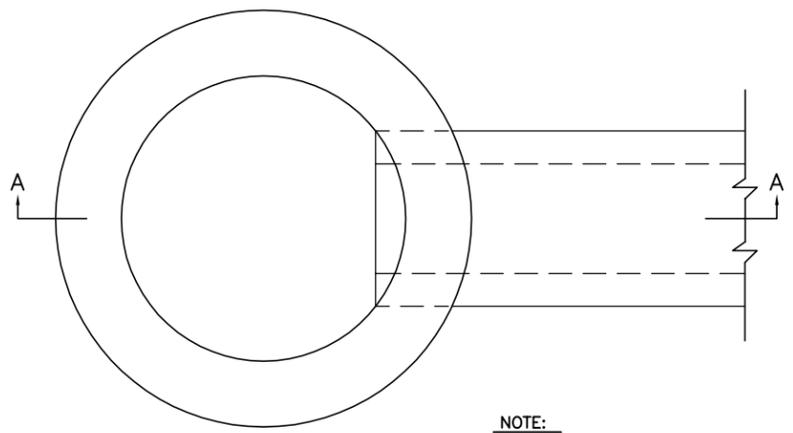
1. BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
2. BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.



STANDARD TRENCH INSTALLATION
 NON-PAVED AREA
 N.T.S.

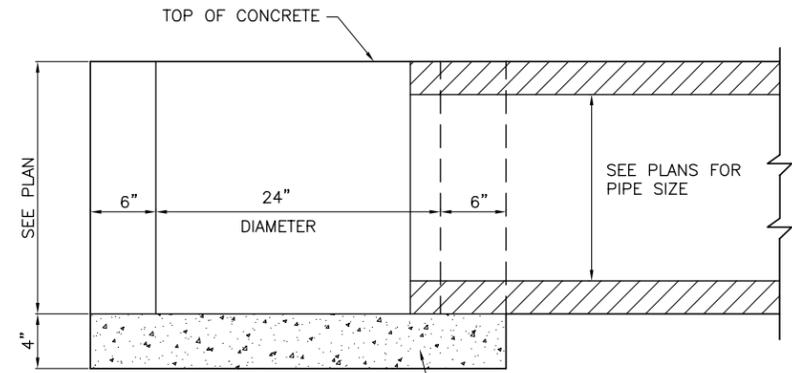


STANDARD TRENCH INSTALLATION
 PROPOSED PAVED AREAS
 N.T.S.

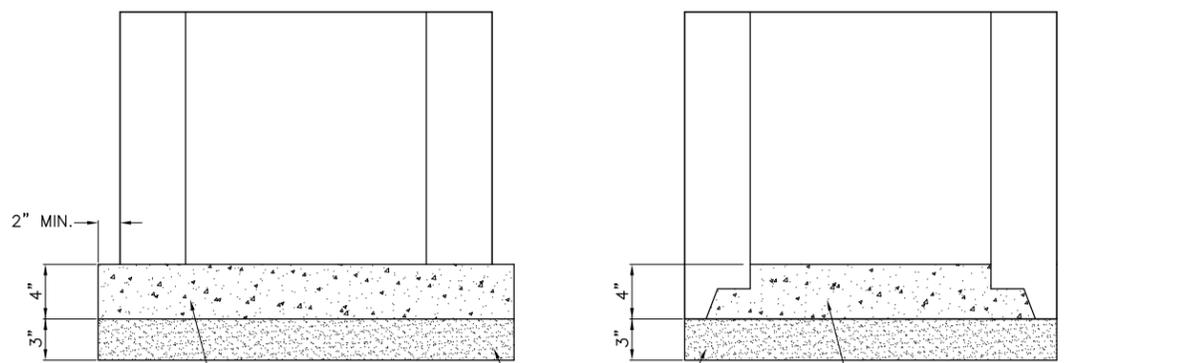


PLAN

NOTE:
 FRAME AND GRATE TO BE
 NEENAH R-2504 OR APPROVED
 EQUAL.



SECTION A-A



ALTERNATE METHODS

IDOT STANDARD 602301
 TYPE A INLET
 N.T.S.

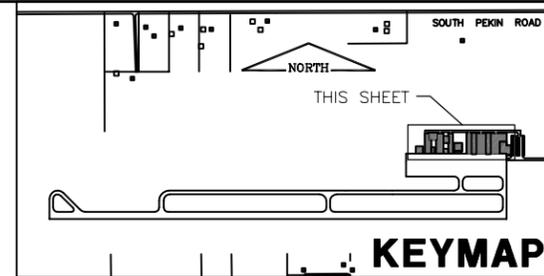
PEKIN MUNICIPAL AIRPORT
 PEKIN, TAZEWELL COUNTY, ILLINOIS
 REHABILITATE T-HANGAR PAVEMENT
 DRAINAGE DETAIL

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AIP PROJ. NO.	3-17-0078-B11

LEGEND

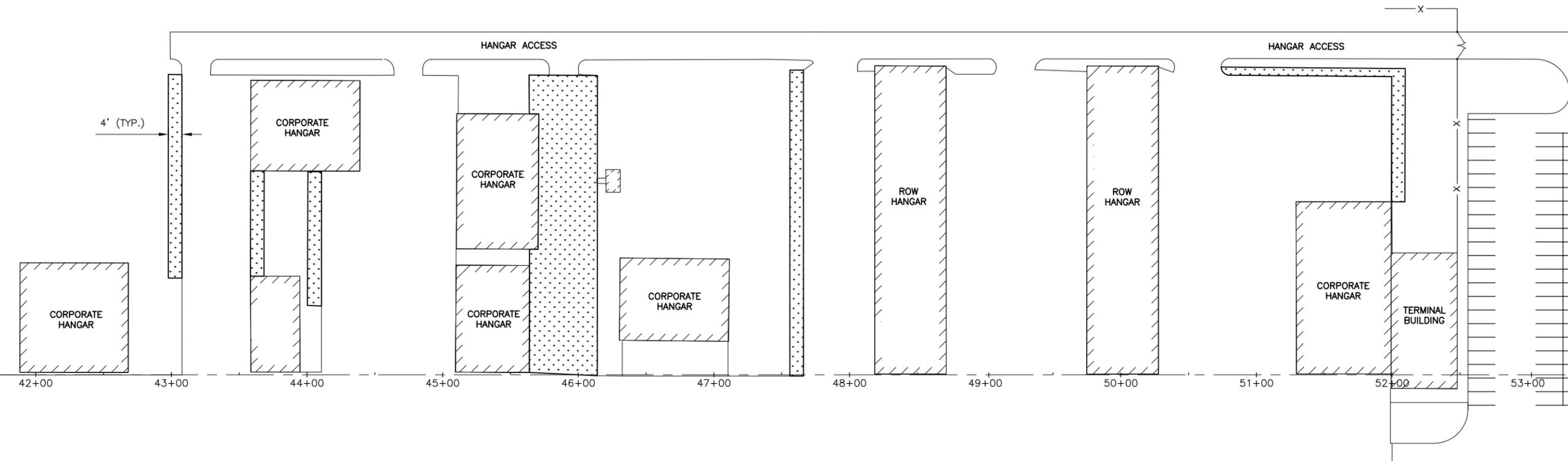
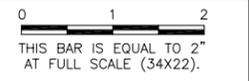
-  EXISTING PAVEMENT
-  NEW SHOULDER EMBANKMENT & TURFING



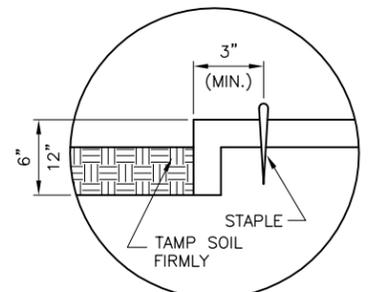
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 FILE: TURFING AND EROSION.dwg
 UPDATE BY: Chris Groth
 PLOT DATE: 6/11/2010 3:59 PM
 Pekin_Base_Rev
 KEY:
 Pekin Hang EP Base

PN005

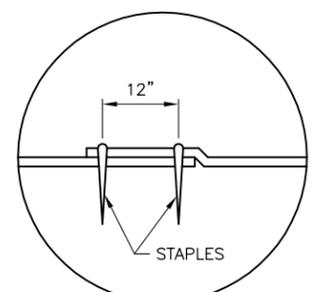
REVISIONS		
NUMBER	BY	DATE



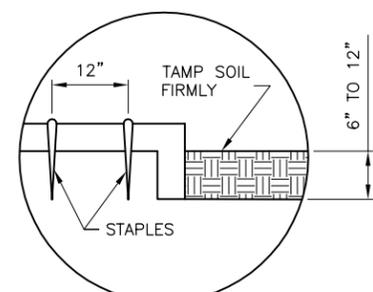
PEKIN MUNICIPAL AIRPORT
PEKIN, TAZEWELL COUNTY, ILLINOIS
REHABILITATE T-HANGAR PAVEMENT
TURFING AND EROSION CONTROL PLAN



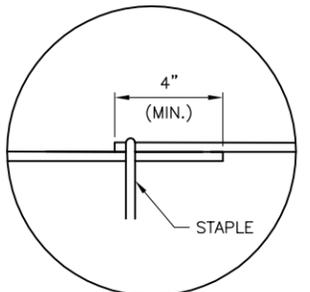
DETAIL 1 - TERMINAL FOLD



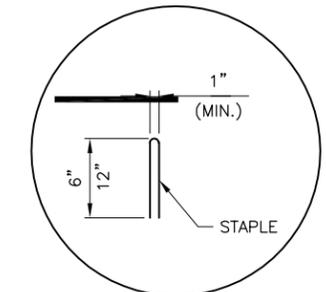
DETAIL 2 - JUNCTION SLOT



DETAIL 3 - ANCHOR SLOT



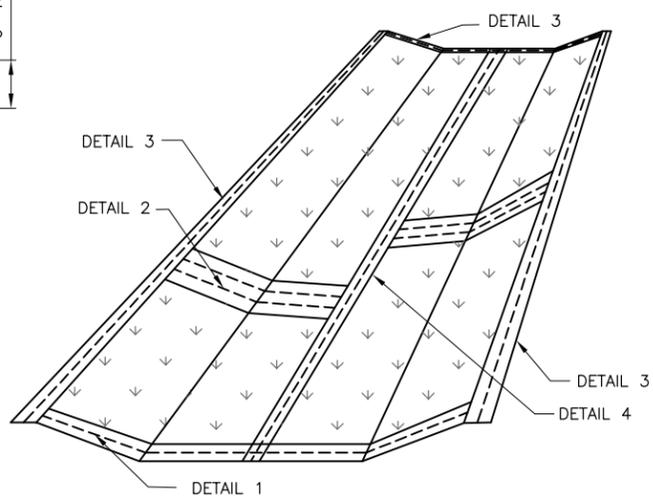
DETAIL 4 - LAP JOINT



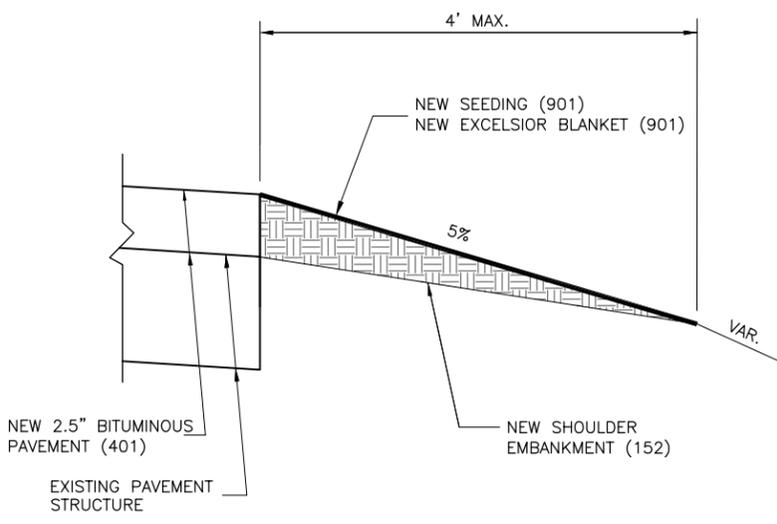
DETAIL 5 - STAPLE DETAIL

EXCELSIOR BLANKET NOTES

1. STAPLES TO BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
2. EROSION CONTROL MATERIAL SHALL BE PLACED LOOSELY OVER GROUND SURFACE. DO NOT STRETCH.
3. ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12" INTERVALS.



EXCELSIOR BLANKET DETAILS
N.T.S.



SHOULDER EMBANKMENT - TYPICAL SECTION
N.T.S.

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